

03-03-2017 LETTING ITEM 134

INDEX OF SHEETS
SEE SHEET NO. 2

HIGHWAY STANDARDS
SEE SHEET NO. 2

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED
FEDERAL AID HIGHWAY

FAP 353 US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE
INTERSECTION IMPROVEMENTS
SECTION NO.: 13-00063-00-CH
FEDERAL PROJECT NO.: M-4003(216)

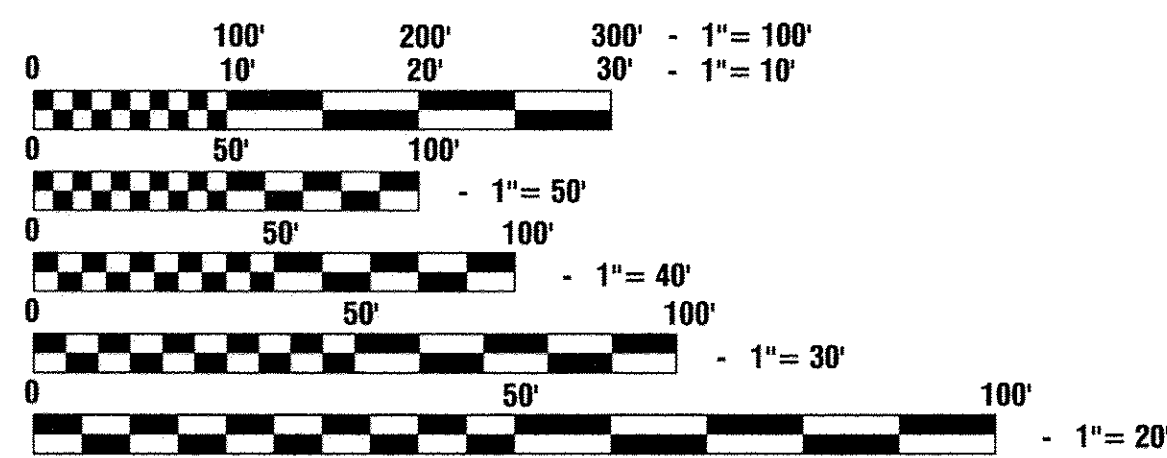
JOB NO.: C-91-392-13
VILLAGE of MATTESON
COOK COUNTY

DESIGN DESIGNATION - US ROUTE 30 (LINCOLN HIGHWAY) ADT 40,000 (2040) - SRA
PV=38,800 SU=600 MU=600
% DESIGN TRAFFIC IN DESIGN LANE
P=97% S=1.5% M=1.5%
SSS=FAIR

DESIGN DESIGNATION - KOSTNER AVENUE ADT 6,000 (2040) - MINOR COLLECTOR
PV=5,910 SU=60 MU=30
% DESIGN TRAFFIC IN DESIGN LANE
P=95.0% S=3.0% M=2.0%
SSS=FAIR

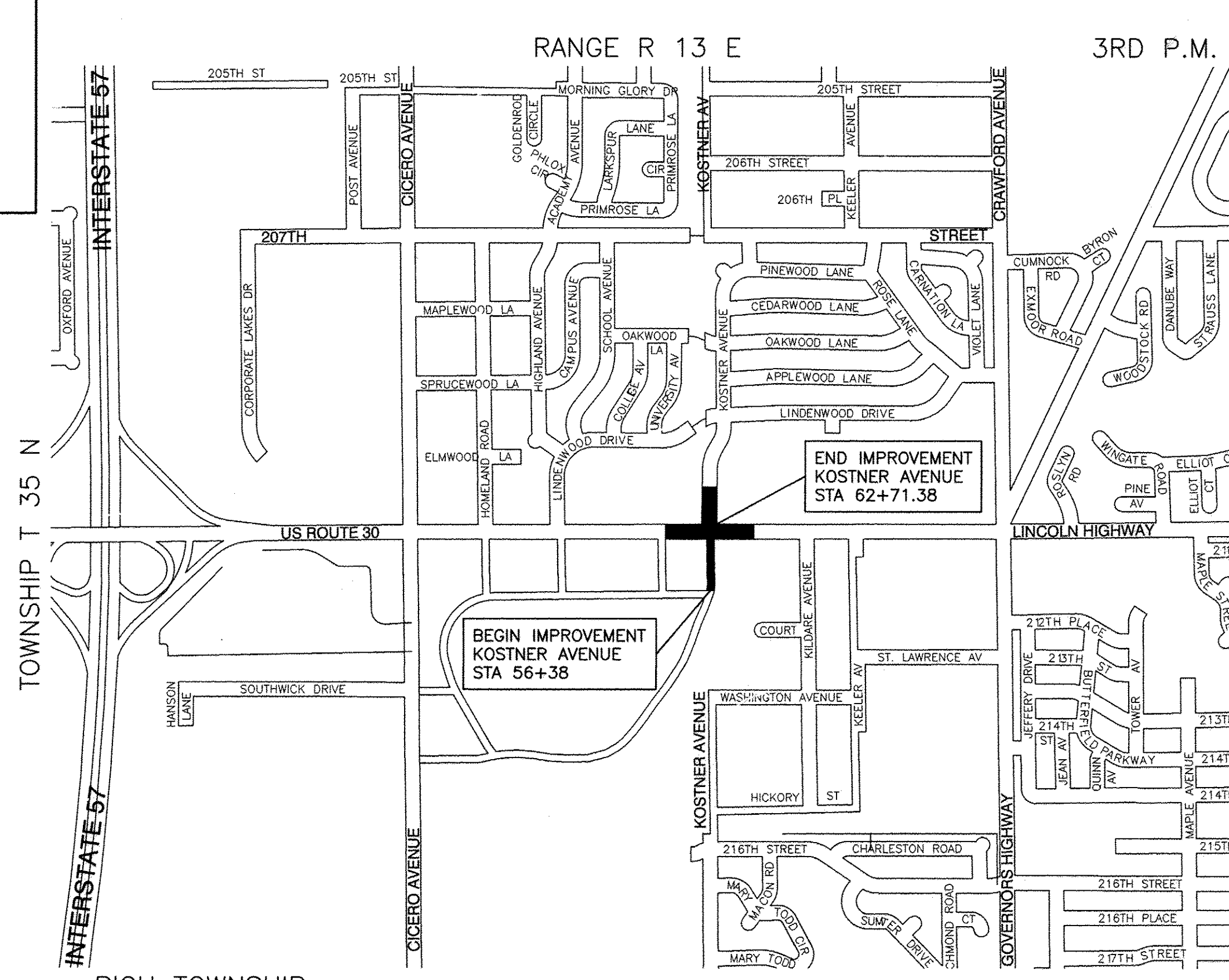
	US ROUTE 30 (LINCOLN HIGHWAY)	KOSTNER AVENUE
2013 ADT -	31,000	4,200
2040 ADT -	40,000	6,000
POSTED SPEED LIMIT -	40 mph	25 mph
DESIGN PERIOD -	20 YEARS	20 YEARS
DESIGN SPEED LIMIT -	45 mph	30 mph
STREET CLASSIFICATION -	URBAN ARTERIAL	MINOR COLLECTOR

SCALES
PLAN - 1"=50'
PROFILE HORIZ. - 1"=50'
PROFILE VERT. - 1"=5'
CROSS SECTIONS - 1"=10'



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J. U. L. I. E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123 or 811

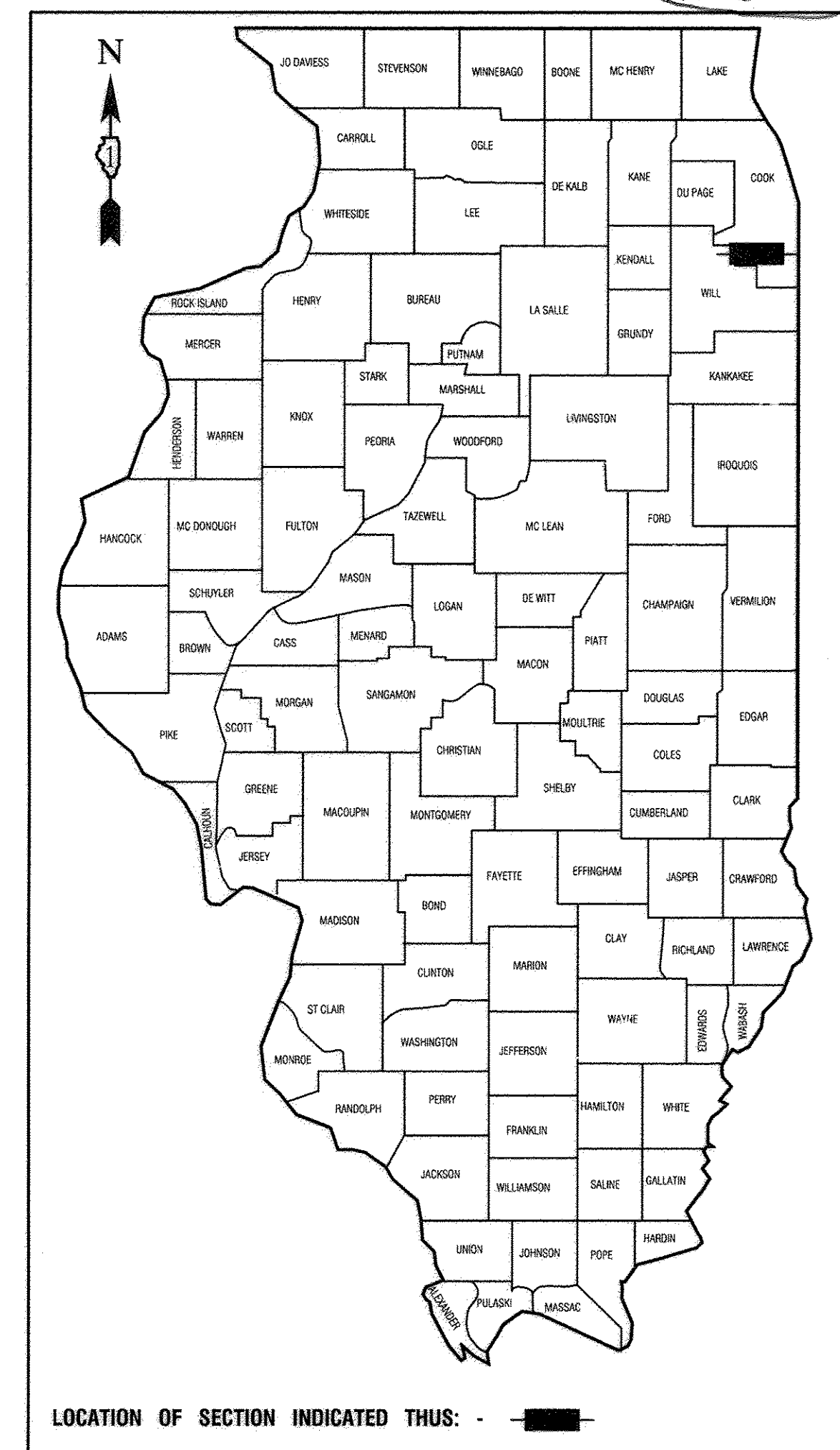


LOCATION MAP
GROSS LENGTH=633 FEET=0.12 MILES
NET LENGTH=633 FEET=0.12 MILES

F. A. P. RITE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	13-00063-00-CH	COOK	63	1
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	M-4003(216)	

CONTRACT #61C11

63 + 3 = 66



STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

Approved: [Signature] 1-22-2016
President, Village of MATTESON

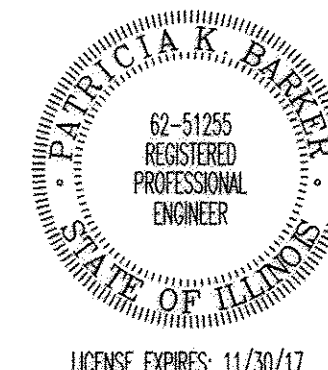
Passed: FEBRUARY 9, 2016
[Signature] CHRISTOPHER HOLT
District 1 Engineer of Local Roads & Streets

Released for Bid Based on Limited Review: February 16, 2016
[Signature] John Fortman
Deputy Director of Highways, Region 1 Engineer

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

PREPARED BY OR UNDER THE DIRECT SUPERVISION OF:

[Signature] 1/22/2016



I.D.O.T. FEDERAL AID PROGRAM ENGINEER: FAWAD AQUEEL, PE, PTOE, 847-705-4021, SCHAUMBURG, IL.
CONSULTANTS: ROBINSON ENGINEERING, LTD. 708-331-6700
CONTACT ENGINEER: PATRICIA BARKER

CONTRACT NO. 61C11

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HIGHWAY STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
420001-08	PAVEMENT JOINTS
420111-03	PCC ROUNDOUTS
442101-07	CLASS B PATCHES
442201-03	CLASS C & D PATCHES
542001-04	REINFORCED CONCRETE END SECTIONS, PIPE CULVERTS 15 INCHES THRU 84 INCHES
602001-02	CATCH BASIN, TYPE A
602301-04	INLET, TYPE A
602401-03	MANHOLE, TYPE A
602601-04	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
602701-02	MANHOLE STEPS
604001-04	FRAMES AND LIDS, TYPE 1
604051-04	FRAME AND GRATE, TYPE 11
606001-06	COMBINATION CONCRETE CURB AND GUTTER
606301-04	PC CONCRETE ISLANDS AND MEDIANS
701311-03	LANE CLOSURE, 2L, 2W MOVING OPERATIONS - DAY ONLY
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS, FOR SPEEDS <=40 MPH
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-04	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-04	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTIONS DETAILS
805001-01	ELECTRICAL SERVICE INSTALLATION DETAILS
814001-03	HANDHOLES
814006-02	DOUBLE HANDHOLES
857001-01	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
862001-01	UNINTERRUPTABLE POWER SUPPLY (UPS)
873001-02	TRAFFIC SIGNAL GROUNDING & BONDING
877001-04	STEEL MAST ARM ASSEMBLY AND POLE 16' THROUGH 55'
878001-10	CONCRETE FOUNDATION DETAILS
880006-01	TRAFFIC SIGNAL MOUNTING DETAILS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUT FOR DETECTION LOOPS

GENERAL NOTES

1. ALL ROADWAY CONSTRUCTION SHALL CONFORM TO THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED APRIL 1, 2016 BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION AND ALL AMENDMENTS THERETO, AND IN ACCORDANCE WITH THE LATEST EDITION OF THE SPECIFICATIONS FOR CONSTRUCTION IN THE VILLAGE OF MATTESON AND IN CASE OF CONFLICT, THE MORE STRINGENT CODE SHALL TAKE PRECEDENCE.
2. ALL STORM SEWER, SANITARY SEWER AND WATER MAIN CONSTRUCTION SHALL CONFORM TO THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS", PUBLISHED JUNE 2014, AND IN ACCORDANCE WITH THE SPECIFICATIONS FOR CONSTRUCTION IN THE VILLAGE OF MATTESON UNLESS OTHERWISE NOTED ON THE PLANS.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR HAVING THE UTILITY COMPANIES LOCATE THEIR FACILITIES IN THE FIELD PRIOR TO CONSTRUCTION AND SHALL ALSO BE RESPONSIBLE FOR THE MAINTENANCE AND PRESERVATION OF THESE FACILITIES. THE ENGINEER DOES NOT WARRANT THE LOCATION OF ANY EXISTING UTILITIES SHOWN ON THE PLAN. THE CONTRACTOR SHALL CALL J.U.L.I.E. AT 800-892-0123 AND THE VILLAGE OF MATTESON FOR UTILITY LOCATIONS.
4. THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING THE NATURE AND STATUS OF ALL UTILITY RELOCATION WORK PRIOR TO THE START OF CONSTRUCTION. THE CONTRACTOR SHALL TAKE APPROPRIATE MEASURES TO ENSURE THAT CONSTRUCTION OPERATIONS DO NOT INTERFERE WITH UTILITY FACILITIES AND RELOCATION WORK. THE SCHEDULE SHOULD REFLECT CONSTRUCTION SEQUENCING, WHICH COORDINATES WITH ALL UTILITY RELOCATION WORK. THE CONTRACTOR SHALL BE REQUIRED TO ADJUST THE ORDER OF ITS WORK FROM TIME TO TIME, TO COORDINATE SAME WITH UTILITY RELOCATION WORK, AND SHALL PREPARE REVISED SCHEDULE (S) IN COMPLIANCE THEREWITH AS DIRECTED BY THE OWNER. THE OWNER AND THE ENGINEER SHALL BE NOTIFIED IN WRITING BY THE CONTRACTOR AT LEAST 48 HOURS PRIOR TO THE START OF ANY OPERATION REQUIRED COOPERATION WITH OTHERS. ALL OTHER AGENCIES, UNLESS OTHERWISE NOTED, WILL BE NOTIFIED IN WRITING BY THE CONTRACTOR TEN (10) DAYS PRIOR TO THE START OF ANY SUCH OPERATION. THE UTILITY COMPANIES HAVE BEEN CONTACTED IN REFERENCE TO UTILITIES THEY OWN AND OPERATE WITHIN THE LIMITS FOR THIS PROJECT. ALL KNOWN DATA FROM THESE AGENCIES HAS BEEN INCORPORATED INTO THE PLANS. IT IS HOWEVER, THE CONTRACTOR'S RESPONSIBILITY TO CONFIRM OR ESTABLISH THE EXISTENCE OF ALL UTILITY FACILITIES AND THEIR EXACT LOCATIONS, WHETHER CONTAINED IN THE DATA SUBMITTED BY THESE AGENCIES OR NOT, AND TO SAFELY SCHEDULE ALL UTILITY RELOCATIONS.
5. ALL CONTRACTORS SHALL KEEP ACCESS AVAILABLE AT ALL TIMES FOR ALL TYPES OF TRAFFIC AS DIRECTED BY THE ENGINEER.
6. COMMENCING CONSTRUCTION
 - A. THE CONTRACTOR SHALL TAKE PHOTOS AND VIDEO RECORD WORK AREA PRIOR TO CONSTRUCTION FOR THE PURPOSE OF DOCUMENTING EXISTING CONDITIONS. SEE THE SPECIAL PROVISIONS FOR "DVD VIDEO RECORDING OF THE CONSTRUCTION ROUTE" FOR DETAILS. THIS WORK SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICES BID FOR THE VARIOUS ITEMS IN THE CONTRACT.
7. THE CONTRACTOR SHALL PRESERVE ALL CONSTRUCTION STAKES UNTIL THEY ARE NO LONGER NEEDED. ANY STAKES DESTROYED OR DISTURBED BY THE CONTRACTOR PRIOR TO THEIR USE SHALL BE RESET BY THE ENGINEER AT THE CONTRACTOR'S EXPENSE.
8. REMOVAL OF SPECIFIED ITEMS, INCLUDING BUT NOT LIMITED TO, PAVEMENT, SIDEWALK, CURB, CURB AND GUTTER, CULVERTS, ETC. SHALL BE DISPOSED OF OFF-SITE BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE. THE CONTRACTOR IS RESPONSIBLE FOR ANY PERMITS REQUIRED FOR SUCH DISPOSAL. THE REMOVAL SHALL BE ACCOMPLISHED BY MEANS OF A SAW CUT JOINT, AT THE DIRECTION OF THE ENGINEER. THIS WORK SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE VARIOUS REMOVAL ITEMS.
9. THE CONTRACTOR SHALL COLLECT AND REMOVE ALL CONSTRUCTION DEBRIS, EXCESS MATERIALS, TRASH, OIL AND GREASE RESIDUE, MACHINERY, TOOLS AND OTHER MISCELLANEOUS ITEMS WHICH WERE NOT PRESENT PRIOR TO PROJECT COMMENCEMENT AT NO ADDITIONAL EXPENSE TO THE OWNER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ACQUIRING ANY AND ALL PERMITS NECESSARY FOR THE HAULING AND DISPOSAL REQUIRED FOR CLEAN-UP AS DIRECTED BY THE ENGINEER OR OWNER. BURNING ON THE SITE IS NOT PERMITTED.
10. AT THE CLOSE OF EACH WORKING DAY AND AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES AND FLOW LINES SHALL BE FREE FORM DIRT AND DEBRIS.
11. TREES NOT MARKED FOR REMOVAL SHALL BE CONSIDERED AS DESIGNATED TO BE SAVED AND SHALL BE PROTECTED UNDER THE PROVISIONS OF ARTICLE 201.05 OF THE STANDARD SPECIFICATIONS.
12. THE TRENCHES FOR PIPE INSTALLATION SHALL BE KEPT DRY AT ALL TIMES DURING PIPE PLACEMENT. APPROPRIATE FACILITIES TO MAINTAIN THE DRY TRENCH SHALL BE PROVIDED BY THE CONTRACTOR AND THE COST OF SUCH SHALL BE INCLUDED IN THE UNIT PRICE BID AND APPROVED BY THE ENGINEER PRIOR TO IMPLEMENTATION. NO ADDITIONAL COMPENSATION SHALL BE MADE FOR DEWATERING DURING CONSTRUCTION UNLESS APPROVED IN WRITING BY THE OWNER.
13. TRENCH BACKFILL WILL BE REQUIRED TO THE FULL DEPTH ABOVE SEWERS AND WATER MAIN WITHIN TWO (2) FEET OF PROPOSED OR EXISTING PAVEMENT.
14. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARM LENGTHS.
15. THE THICKNESS OF HMA MIXTURE STATED IN THE SPECIFICATIONS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS FROM THE NOMINAL THICKNESS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA SURFACE IS PLACED.
16. ACCESS TO DRIVEWAYS SHALL BE MAINTAINED AT ALL TIMES BY LIMITING CURB AND GUTTER REPAIR TO ONE-HALF THE DRIVEWAY WIDTH AT ONE TIME AS WELL AS TEMPORARY AGGREGATE WHICH SHALL BE INCLUDED IN THE COST OF DRIVEWAY REPLACEMENT.
17. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON VILLAGE RIGHT OF WAY WITHOUT WRITTEN PERMISSION FROM THE ENGINEER.
18. THE ENGINEER SHALL CONTACT THE AREA TRAFFIC FIELD TECHNICIAN, PATRICE HARRIS, AT PATRICE.HARRIS@ILLINOIS.GOV TWO (2) WEEKS PRIOR TO PLACING PERMANENT PAVEMENT MARKINGS.
19. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847)705-4470, ROBINSON ENGINEERING (708)331-6700 AND THE VILLAGE OF MATTESON (708)748-1411 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

STORM SEWER NOTES

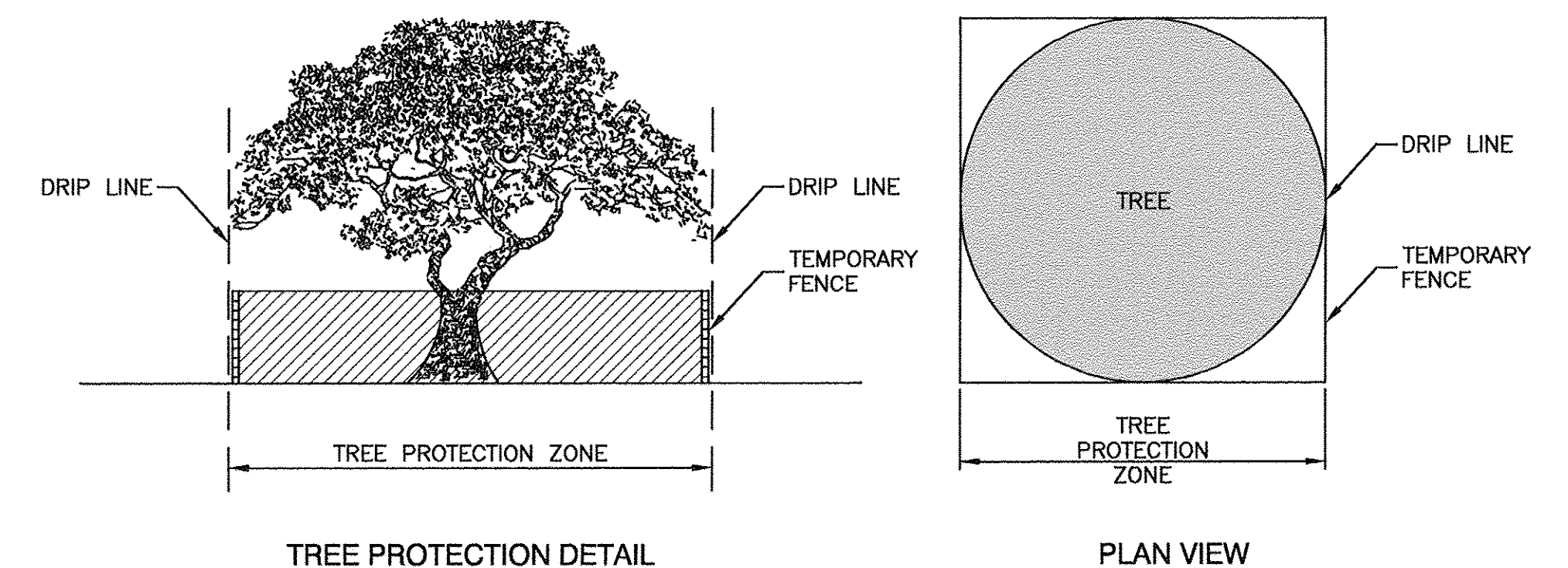
1. ON ALL IMPROVEMENTS THE FRAMES AND LIDS OF EXISTING CATCH BASINS, INLETS, MANHOLES AND VALVE VAULTS WHICH ARE TO BE ABANDONED DUE TO CONSTRUCTION OF THIS IMPROVEMENT ARE TO REMAIN THE PROPERTY OF THE VILLAGE OF MATTESON AND BE SALVAGED. THE OWNER SHALL BE NOTIFIED AS TO AVAILABILITY FOR PICK-UP.
2. THE TOP OF ALL STRUCTURES SHALL BE FLUSH WITH THE ADJACENT SURFACE OR AT THE INDICATED ELEVATIONS SHOWN ON THE PLANS.
3. FRAME ELEVATIONS ARE GIVEN ONLY TO ASSIST IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF THE STRUCTURE. FRAMES ON ALL NEW STRUCTURES WILL BE ADJUSTED TO THE FINAL ELEVATION OF THE AREA IN WHICH THEY ARE LOCATED AS PART OF THE STRUCTURE COST.

EARTHWORK NOTES

1. GENERAL
 - A. IT IS THE CONTRACTOR'S RESPONSIBILITY TO UNDERSTAND THE SOIL AND GROUNDWATER CONDITIONS AT THE SITE.
 - B. ANY QUANTITIES IN THE BID PROPOSAL ARE INTENDED AS A GUIDE FOR THE CONTRACTORS USE IN DETERMINING THE SCOPE OF THE COMPLETED PROJECT. IT IS THE CONTRACTORS RESPONSIBILITY TO DETERMINE ALL MATERIAL QUANTITIES AND APPRAISE HIMSELF OF ALL SITE CONDITIONS.
 - C. THE CONTRACTOR WILL NOTE THAT THE ELEVATIONS SHOWN ON THE CONSTRUCTION PLANS ARE FINISHED GRADE AND SUBGRADE ELEVATIONS (AS NOTED) AND THAT PAVEMENT THICKNESS, TOPSOIL, ETC. MUST BE ACCOUNTED FOR.
 - D. THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE DURING CONSTRUCTION, AND PREVENT STORMWATER FROM RUNNING INTO OR STANDING IN EXCAVATED AREAS. THE FAILURE TO PROVIDE PROPER DRAINAGE WILL NEGATE ANY POSSIBLE ADDED COMPENSATION REQUESTED DUE TO DELAYS OR UNSUITABLE MATERIALS CREATED AS A RESULT THEREOF. FINAL GRADES SHALL BE PROTECTED AGAINST DAMAGE FROM EROSION, SEDIMENTATION AND TRAFFIC.
 - E. PLANS FOR THE SITE DEWATERING, IF EMPLOYED, SHALL BE SUBMITTED AND APPROVED PRIOR TO IMPLEMENTATION. NO ADDITIONAL COMPENSATION SHALL BE MADE FOR DEWATERING DURING CONSTRUCTION.
 - F. THE CONTRACTOR SHALL BE RESPONSIBLE FOR IMPLEMENTATION OF THE "SOIL EROSION AND SEDIMENTATION CONTROL MEASURES". THE INITIAL ESTABLISHMENT OF EROSION CONTROL PROCEDURES AND THE PLACEMENT OF SILT AND FILTER FENCING, ETC. TO PROTECT ADJACENT PROPERTY, WETLANDS, ETC. SHALL OCCUR BEFORE GRADING BEGINS.
 - G. ALL STORM INLETS SHALL BE PROTECTED BY INLET FILTERS. PLACEMENTS AND MAINTENANCE OR SILT BARRIER SHALL BE AS DIRECTED BY THE ENGINEER, BASED ON ACTUAL GRADING. GRADE THE AREA WITHIN FOUR (4) FEET AROUND STRUCTURES ONE (1) FOOT BELOW RIM TO SERVE AS A SEDIMENTATION BASIN DURING CONSTRUCTION.
 - H. FINAL LOCATION OF SILT FENCE SHALL BE ADJUSTED BASED ON ACTUAL SITE GRADING CONDITIONS. ADDITIONAL MEASURES SHALL BE ADDED AS DIRECTED BY THE ENGINEER.
 - I. ALL AREAS DISTURBED DURING CONSTRUCTION SHALL BE RESEED AS SOON AS PRACTICAL.

LANDSCAPING NOTES

1. THE CONTRACTORS ATTENTION IS CALLED TO THE FACT THAT THE PRESERVATION OF EXISTING TREES IS OF UTMOST IMPORTANCE TO THE VILLAGE OF MATTESON. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO ARRANGE TREE PROTECTION WITH THE IDOT ROADSIDE DEVELOPMENT UNIT (847.705.4171) PRIOR TO SCHEDULING TREE REMOVAL. ALL TREE PROTECTION, TREE REMOVAL, PRUNING AND ROOT PRUNING SHALL BE COMPLETED BEFORE CONSTRUCTION OPERATIONS COMMENCE IN ANY AREA. AT NO TIME SHALL THE CONTRACTOR PRUNE OR REMOVE ANY TREES UNLESS SPECIFICALLY DIRECTED BY THE ENGINEER.
2. THE CONTRACTOR SHALL INSTALL TEMPORARY FENCE AROUND ALL TREES WITHIN THE CONSTRUCTION AREA TO ESTABLISH A "TREE PROTECTION ZONE" BEFORE ANY WORK BEGINS OR ANY MATERIAL IS DELIVERED TO THE JOBSITE. NO WORK IS TO BE PERFORMED (OTHER THAN ROOT PRUNING), MATERIALS STORED OR VEHICLES DRIVEN OR PARKED WITHIN THE "TREE PROTECTION ZONE". REMOVE PROTECTIVE TEMPORARY FENCE ONLY AFTER ALL CONSTRUCTION WORK HAS BEEN COMPLETED. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO MAINTAIN ALL TREE PROTECTION UNTIL CONSTRUCTION IS COMPLETED.
3. EXISTING VEGETATED AREAS (TREES, SHRUBS, VEGETATIVE BUFFERS, TURF AREAS, ETC.) WHERE DISTURBANCE IS NOT OCCURRING (INCLUDING AREAS OUTSIDE THE PROJECT LIMITS) SHALL NOT BE DISTURBED TO ENSURE THAT EXISTING VEGETATION IS PRESERVED TO MINIMIZE SOIL EROSION AND TO ELIMINATE SOIL COMPACTION. NO MATERIAL ARE TO BE STORED OR VEHICLES DRIVEN OR PARKED WITHIN THESE UNDISTURBED AREAS AT ANY TIME.
4. THE CONTRACTOR SHALL TAKE EXTRA CARE IN GRADING AND EXCAVATING NEAR TREES WHICH ARE NOT MARKED FOR REMOVAL SO AS NOT TO CAUSE INJURY TO THE ROOT SYSTEM OR TRUNKS. ROOTS OF A TREE THAT ARE TO REMAIN IN PLACE EXTENDING INTO THE EXCAVATION AREAS AT AN ELEVATION THAT WOULD INTERFERE WITH ANY PORTION OF THE PLANNED CONSTRUCTION SHALL BE SEVERED AT A POINT IMMEDIATELY OUTSIDE OF THE EXCAVATION AREA IN A MANNER THAT WILL CAUSE THE LEAST AMOUNT OF SYSTEMIC DAMAGE TO THE REMAINING TREE STRUCTURE. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE.
5. PRUNE TREE LIMBS THAT MIGHT BE DAMAGED BY EQUIPMENT OPERATIONS AT LEAST ONE WEEK PRIOR TO THE START OF CONSTRUCTION BY A CERTIFIED ARBORIST. ANY TREE LIMBS THAT ARE BROKEN BY CONSTRUCTION EQUIPMENT AFTER THE INITIAL PRUNING MUST BE PRUNED CORRECTLY WITHIN 72 HOURS.
6. SUPPLEMENTAL WATERING IS SPECIFIED FOR TREES AND SHRUBS THAT WILL BE DISTURBED BY CONSTRUCTION BUT SHALL REMAIN. NOTE THAT WATERING SHOULD BEGIN IMMEDIATELY AFTER ROOT PRUNING, TOP PRUNING OR OTHER CONSTRUCTION DISTURBANCE.



FILE NAME = 13375_02-INDX-01 - P01	USER NAME =	DESIGNED -- EMA	REVISED --	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INTERSECTION IMPROVEMENTS US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE INDEX OF SHEETS & STATE STANDARDS & GENERAL NOTES			F.A.P. RTE. 353	SECTION 13-00063-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 2
	PLOT SCALE =	CHECKED -- PKB	REVISED --									
	PLOT DATE = 05-19-16	DRAWN -- RG	REVISED --									
		CHECKED -- AG	REVISED --			SCALE: NONE	SHEET NO. 2	OF 63 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(216)

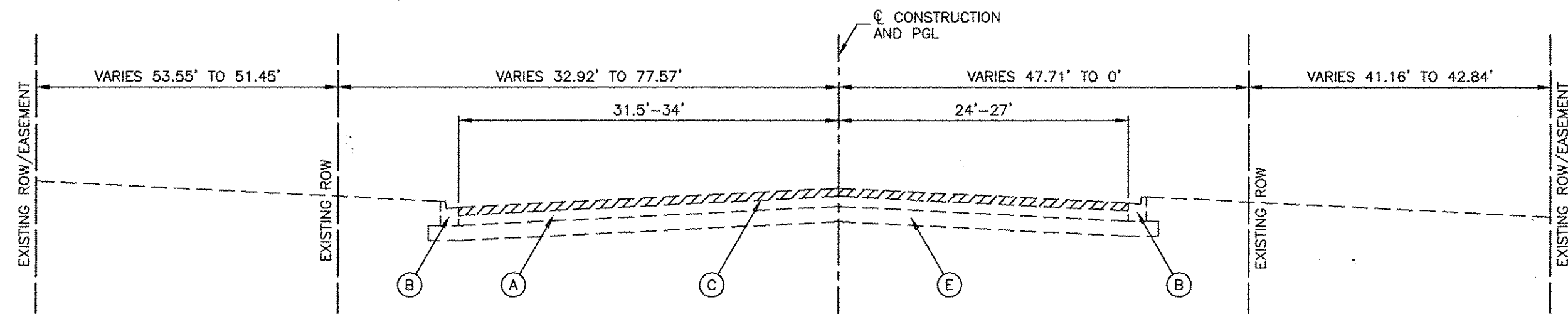
SUMMARY OF QUANTITIES					ROAD	SAFETY	LNSC	TRAINERS	CONSTRUCTION TYPE CODE			
S.I.	CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY	004	021	031	042				
	20100110	TREE REMOVAL (6 TO 15 INCH DIAMETER)	UNIT	38				38				
	20101000	TEMPORARY FENCE	FOOT	700				700				
	20101100	TREE TRUNK PROTECTION	EACH	8				8				
*	20101200	TREE ROOT PRUNING	EACH	8				8				
*	20101300	TREE PRUNING (1 TO 10 INCH DIAMETER)	EACH	6				6				
*	20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	2				2				
*	20101700	SUPPLEMENTAL WATERING	UNIT	5				5				
	20200100	EARTH EXCAVATION	CU YD	1342	1342							
	20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	100	100							
	20800150	TRENCH BACKFILL	CU YD	119	119							
	21101505	TOPSOIL EXCAVATION AND PLACEMENT	CU YD	142				142				
	21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	1075				1075				
	21301084	EXPLORATION TRENCH 84" DEPTH	FOOT	100	100							
*	25000210	SEEDING CLASS 2A	ACRE	1				1				
*	25000400	NITROGEN FERTILIZER NUTRIENT	POUND	45				45				
*	25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	45				45				
*	25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	45				45				
	28000305	TEMPORARY DITCH CHECKS	FOOT	66				66				
	28000400	PERIMETER EROSION BARRIER	FOOT	595				595				
	28000500	INLET AND PIPE PROTECTION	EACH	4				4				
	28000510	INLET FILTERS	EACH	12				12				
	28001100	TEMPORARY EROSION CONTROL BLANKET	SQ YD	2328				2328				
	28100101	STONE RIPRAP, CLASS A1	SQ YD	43				43				
	28100107	STONE RIPRAP, CLASS A4	SQ YD	43				43				
	28200200	FILTER FABRIC	SQ YD	43				43				
	30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	100	100							
	30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	2524	2524							
	31101200	SUBBASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	102	102							
	35101800	AGGREGATE BASE COURSE, TYPE B 6"	SQ YD	761	761							
	35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SQ YD	215	215							
	40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	5680	5680							
	40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	1430	1430							
	40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	48	48							
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	29	29							
	40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	611	611							
	40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	522	522							
	42000500	PORTLAND CEMENT CONCRETE PAVEMENT 10"	SQ YD	349	349							
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH	SQ FT	910	910							
	42400800	DETECTABLE WARNINGS	SQ FT	190	190							
	44000100	PAVEMENT REMOVAL	SQ YD	810	810							
	44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	1215	1215							
	44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	426	426							

* - INDICATES SPECIALTY ITEMS

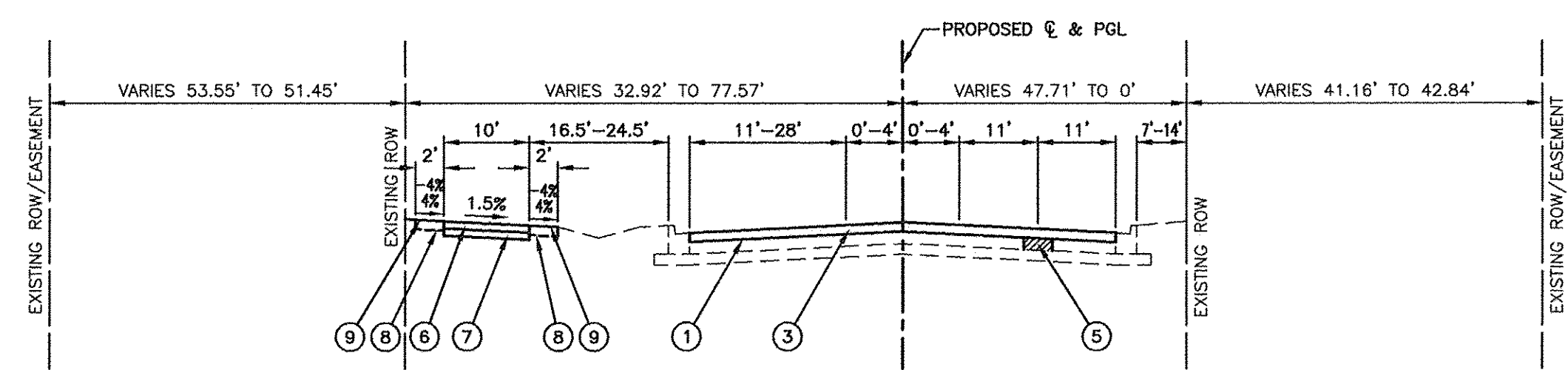
FILE NAME = 13375_02-QUAN-01 - IDOT P01	USER NAME =	DESIGNED -- EMA	REVISED -- 08-26-16 IDOT	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE INTERSECTION IMPROVEMENTS SUMMARY OF QUANTITIES			F.A.P. RTE. 353	SECTION 13-00063-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 3
	PLOT SCALE =	CHECKED -- PKB	REVISED --		SCALE: NONE	SHEET NO. 3	OF 63 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(216)
	PLOT DATE = 05-19-16	DRAWN -- RG	REVISED --									
		CHECKED -- AG	REVISED --									

SUMMARY OF QUANTITIES					ROAD	SAFETY	LNSC	TRAINEES	CONSTRUCTION TYPE CODE				
S.I.	CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY	004	021	031	042					
	44000300	CURB REMOVAL	FOOT	100	100								
	44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1132	1132								
	44000600	SIDEWALK REMOVAL	SQ FT	1187	1187								
	44003100	MEDIAN REMOVAL	SQ FT	3362	3362								
	44201713	CLASS D PATCHES, TYPE I, 6 INCH	SQ YD	10	10								
	44201717	CLASS D PATCHES, TYPE II, 6 INCH	SQ YD	10	10								
	54261318	CONCRETE END SECTION, STANDARD 542001, 18", 1:3	EACH	4	4								
	550A0050	STORM SEWERS, CLASS A, TYPE I 12"	FOOT	186	186								
	550A0070	STORM SEWERS, CLASS A, TYPE I 15"	FOOT	263	263								
	550A0090	STORM SEWERS, CLASS A, TYPE I 18"	FOOT	210	210								
	542A0223	PIPE CULVERTS, CLASS A, TYPE I 18"	FOOT	32	32								
	55100900	STORM SEWER REMOVAL 18"	FOOT	118	118								
*	56109210	WATER VALVES TO BE ADJUSTED	EACH	7	7								
*	56400100	FIRE HYDRANTS TO BE MOVED	EACH	1	1								
	60201105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE I FRAME AND GRATE	EACH	6	6								
	60218400	MANHOLES, TYPE A, 4'-DIAMETER, TYPE I FRAME, CLOSED LID	EACH	3	3								
	60221100	MANHOLES, TYPE A, 5'-DIAMETER, TYPE I FRAME, CLOSED LID	EACH	1	1								
	60236800	INLETS, TYPE A, TYPE I FRAME AND GRATE	EACH	5	5								
	60255500	MANHOLES TO BE ADJUSTED	EACH	2	2								
	60265700	VALVE VAULTS TO BE ADJUSTED	EACH	1	1								
	60500050	REMOVING CATCH BASINS	EACH	2	2								
	60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	1774	1774								
	60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	150	150								
	60618740	CONCRETE MEDIAN, TYPE M-2.12	SQ FT	2874	2874								
*	66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	320	320								
*	66900450	SPECIAL WASTE PLANS AND REPORTS	L SUM	1	1								
*	66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1								
	67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6								
	67100100	MOBILIZATION	L SUM	1	1								
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	732		732							
	70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	73		73							
	72000100	SIGN PANEL - TYPE I	SQ FT	46		46							
	72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	8		8							
	72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	2		2							
	72900100	METAL POST - TYPE A	FOOT	73		73							
	72900200	METAL POST - TYPE B	FOOT	81		81							
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	358		358							
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	1947		1947							
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	772		772							
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	970		970							

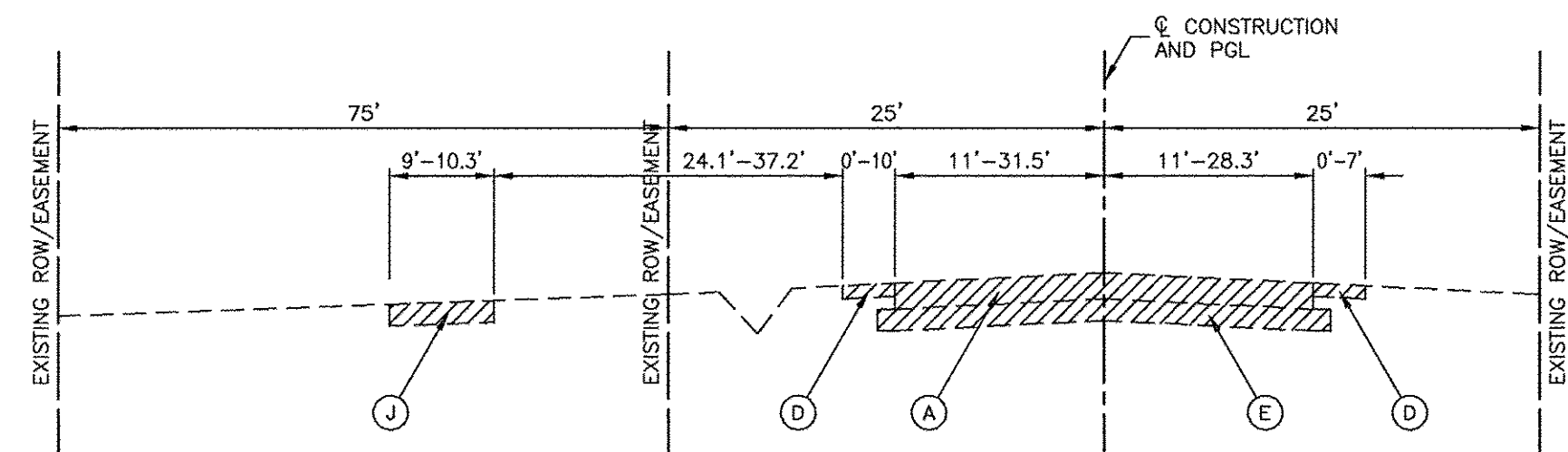
* - INDICATES SPECIALTY ITEMS



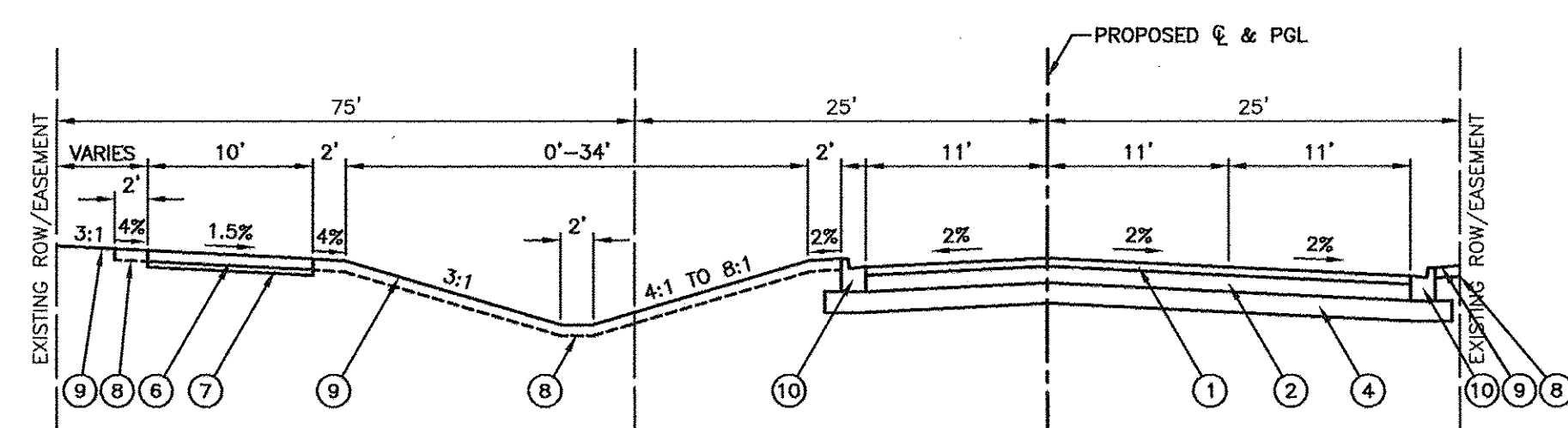
EXISTING TYPICAL SECTION
KOSTNER AVENUE
STA 56+38 TO STA 57+84.19



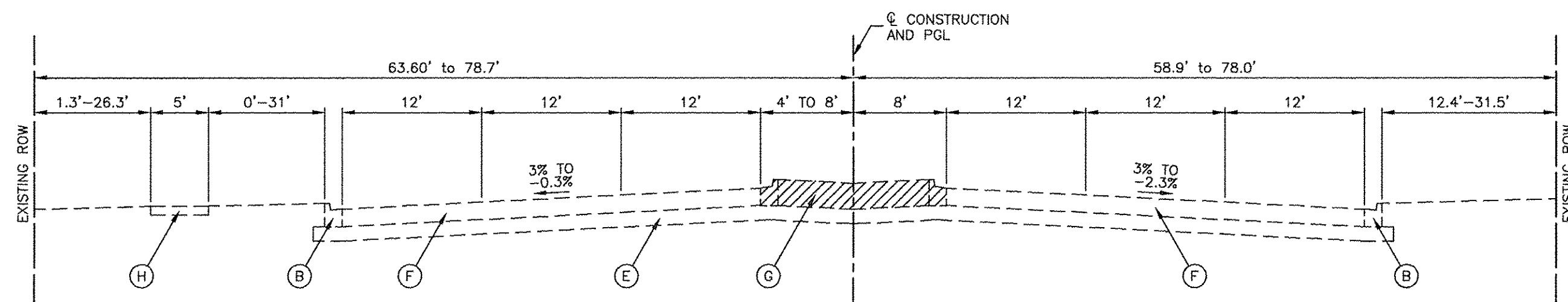
PROPOSED TYPICAL SECTION
KOSTNER AVENUE
STA 56+38 TO STA 57+84.19



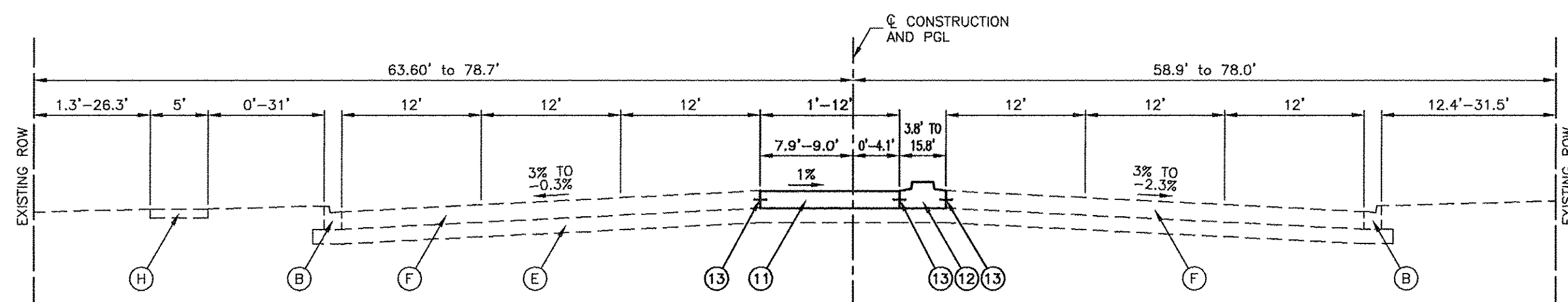
EXISTING TYPICAL SECTION
KOSTNER AVENUE
STA 57+84.19 TO STA 62+71.38



PROPOSED TYPICAL SECTION
KOSTNER AVENUE
STA 57+84.19 TO STA 62+71.38



EXISTING TYPICAL SECTION
LINCOLN HIGHWAY
STA 17+85.30 TO STA 21+35



PROPOSED TYPICAL SECTION
LINCOLN HIGHWAY
STA 17+85.30 TO STA 21+35

* TRANSVERSE CONTRACTION JOINTS SHALL BE CONSTRUCTED USING 1/2" DIA DOWEL BARS, 18" LONG AT 12" C-C SPACING ACCORDING TO HIGHWAY STANDARD 42001 AND SPACED TO ALIGN WITH THE JOINTS IN EXISTING PAVEMENT.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ Ndes
PAVEMENT RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2" (IL-9.5mm)	4% @ 50Gyr.
LEVELING BINDER (MACHINE METHOD), N50, 3/4"	4% @ 50 Gyr.
FULL DEPTH PAVEMENT	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2" (IL-9.5mm)	4% @ 50Gyr.
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 5"	4% @ 50 Gyr.
PATCHING	
CLASS D PATCHES, TYPE I, II (HMA BINDER IL-19.0mm): 6" (IN 2 LIFTS)	4% @70 Gyr.
DRIVEWAYS	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2" (IL-9.5mm)	4% @ 50Gyr.
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19.0mm); PE -6", CE - 8"	4% @ 50 Gyr.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SYIN. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR HMA FULL DEPTH "AC TYPE" SEE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISIONS.

*NOTE: CLASS D PATCHES, TYPE I & II SHALL BE CONSTRUCTED AS DIRECTED BY THE ENGINEER

EARTHWORK QUANTITIES	
TOTAL CUT =	1709 CY
TOTAL EXISTING PAVEMENT REMOVAL =	225 CY
TOTAL TOPSOIL EXCAVATION AND PLACEMENT =	142 CY
TOTAL AVAILABLE CUT TO FILL (EARTH EXCAVATION) =	1342 CY
TOTAL FILL =	105 CY
CUT TO FILL (15% SHRINKAGE)	121 CY

LEGEND

- (A) EXISTING HMA PAVEMENT, 8.5"
- (B) EXISTING CONCRETE CURB AND GUTTER
- (C) EXISTING HMA SURFACE REMOVAL, 2-1/2"
- (D) EXISTING AGGREGATE SHOULDER
- (E) EXISTING SUBGRADE
- (F) EXISTING PCC PAVEMENT, 12"
- (G) EXISTING HMA MEDIAN
- (H) EXISTING PCC SIDEWALK
- (J) EXISTING HMA PATH
- (1) HMA SURFACE COURSE, MIX "D", N50, 2"
- (2) HMA BINDER COURSE, IL-19.0, N50, 5"
- (3) LEVELING BINDER (MACHINE METHOD), N50, 3/4"
- (4) AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (5) CLASS D PATH, 6" (AS DIRECTED BY THE RESIDENT ENGINEER)
- (6) HMA SURFACE COURSE, MIX "D", N50, 3"
- (7) AGGREGATE BASE COURSE, TYPE B, 6"
- (8) TOPSOIL, FURNISH AND PLACE, 4"
- (9) SEEDING, CLASS 1A
- (10) TYPE B-6.12 COMBINATION CONCRETE CURB AND GUTTER
- (11) PCC PAVEMENT, 12"
- (12) PCC MEDIAN TYPE M-2.12
- (13) NO. 6 (NO. 19) TIE BARS - 24" LONG AT 24" C-C SPACING
- [X] ITEM TO BE REMOVED

FILE NAME = 13375_02-TYPX-01 - IDOT P01

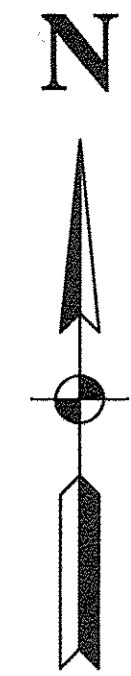
USER NAME =	DESIGNED — EMA	REVISED —
PLOT SCALE =	CHECKED — PKB	REVISED —
PLOT DATE = 05-19-16	DRAWN — RG	REVISED —
	CHECKED — AG	REVISED —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

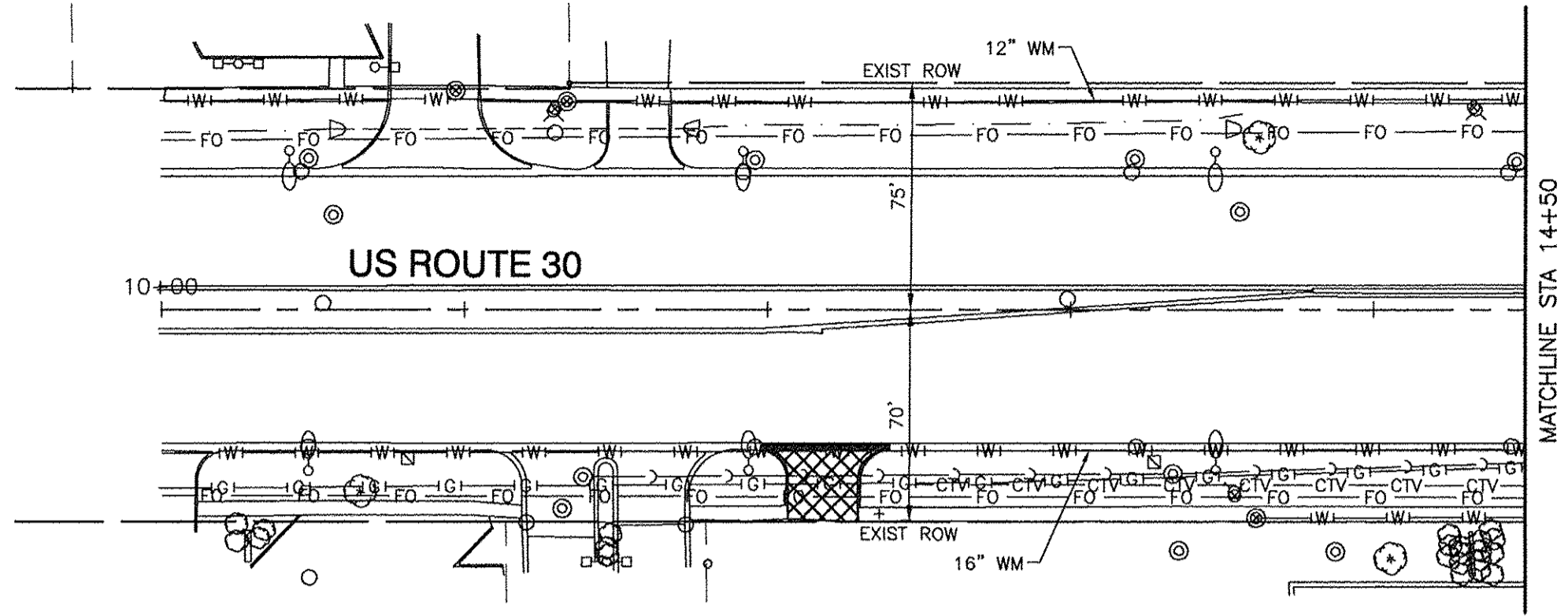
US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE
INTERSECTION IMPROVEMENTS
TYPICAL SECTIONS

SCALE: SHEET NO. 7 OF 63 SHEETS STA. TO STA.

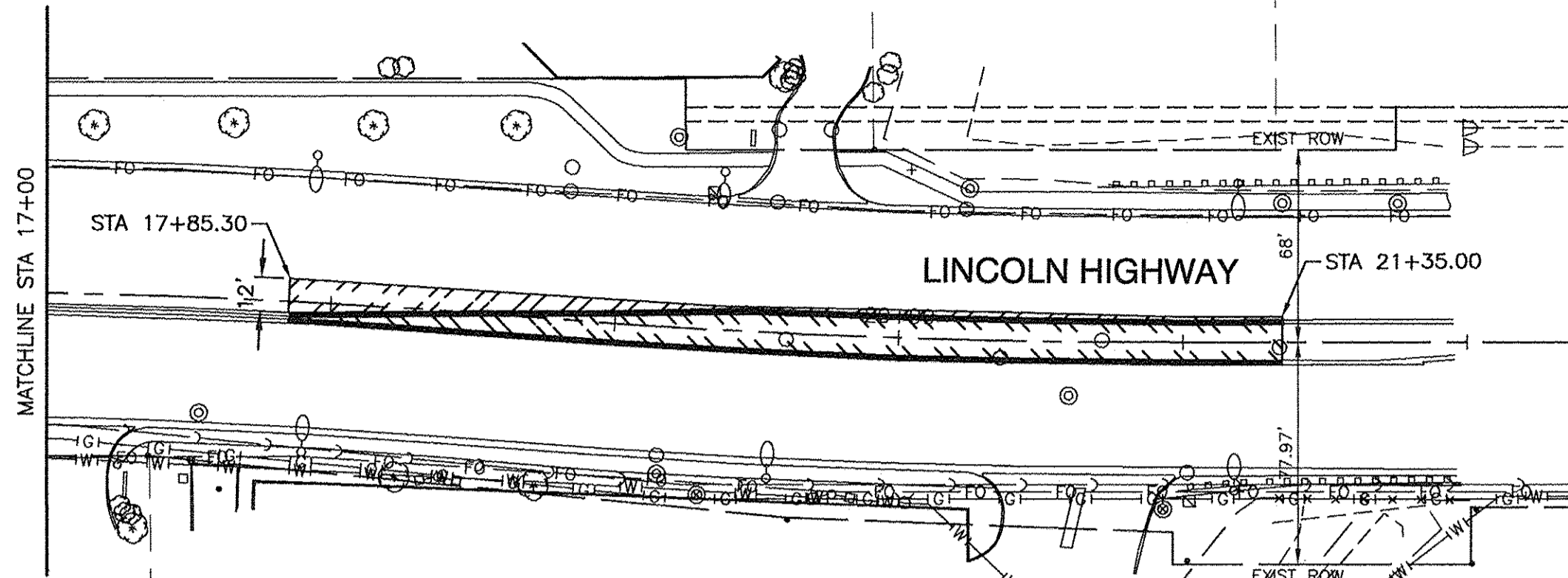
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	13-00063-00-CH	COOK	63	7
CONTRACT NO. 61C11				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	M-4003(216)	



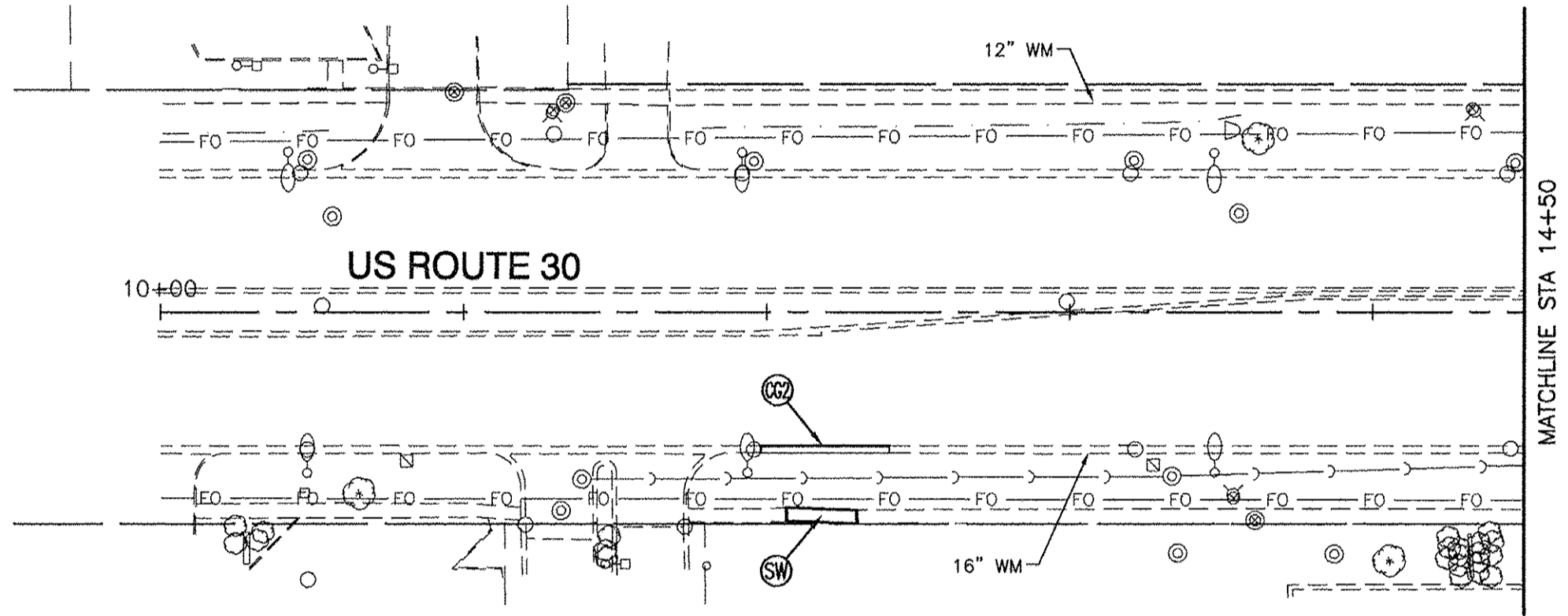
DATE	BY
PLAN	SURVEYED
NOTE BOOK NO.	ALIGNMENT CHECKED
	RT. OF WAY CHECKED
	ROAD FILE NAME



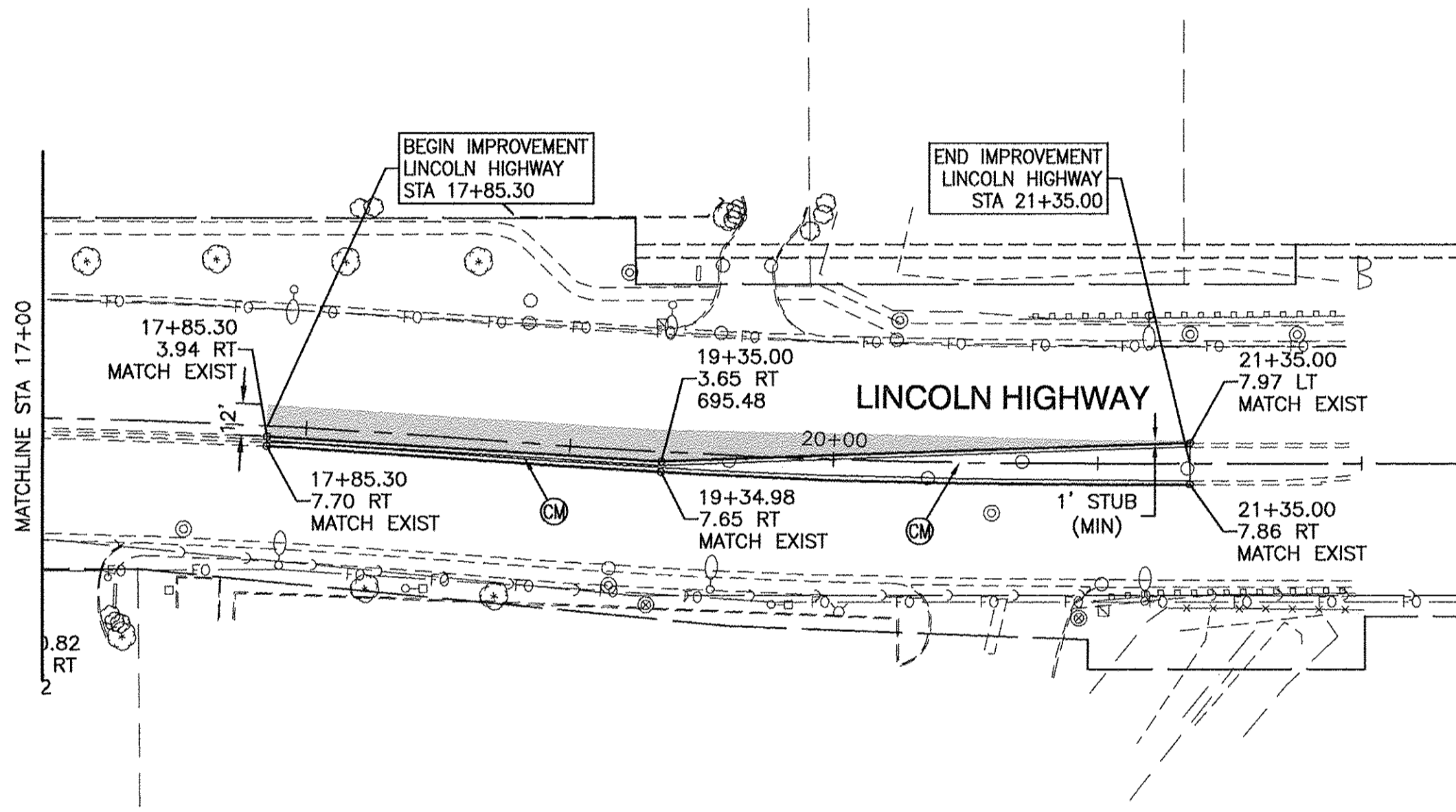
EXISTING CONDITIONS



DATE	BY
PROFILE	SURVEYED
NOTE BOOK B.M. NOTED	GRADES CHECKED
	B.M. NOTED

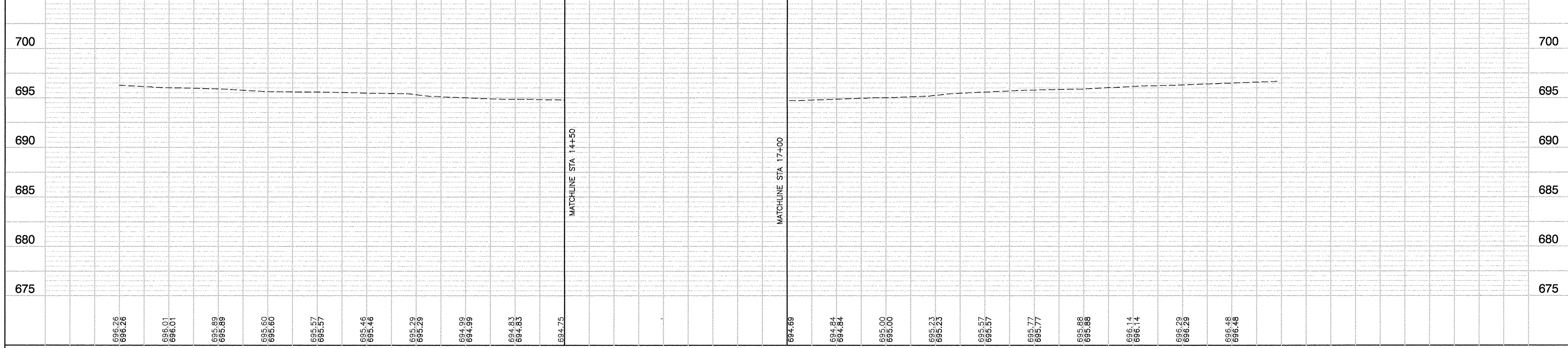


PROPOSED IMPROVEMENTS

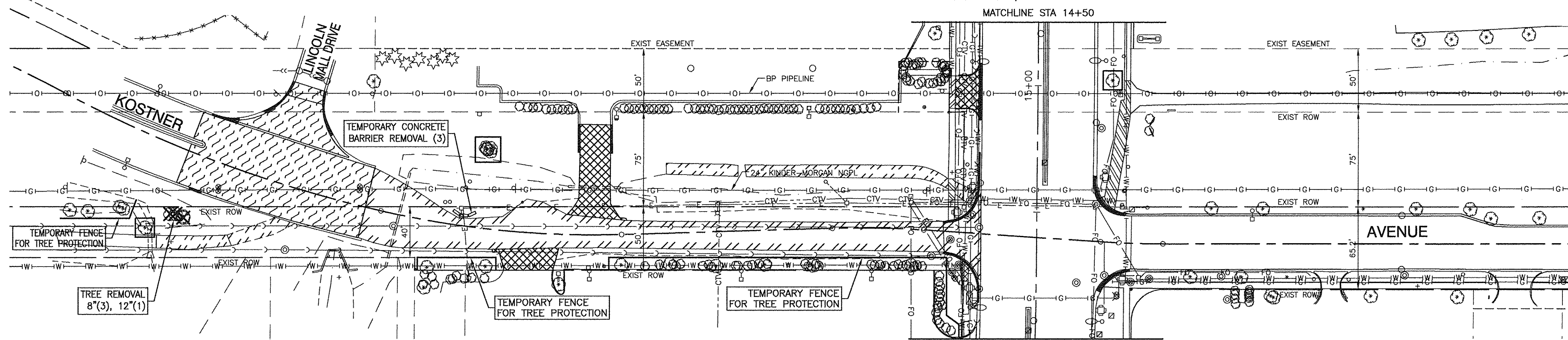


LEGEND

- BUTT JOINT
- TREE TO BE REMOVED
- CONC CURB & GUTTER REMOVAL & CURB REMOVAL
- MEDIAN REMOVAL
- PAVEMENT REMOVAL
- HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/2"
- SIDEWALK REMOVAL
- DRIVEWAY REMOVAL
- PCC PAVEMENT, 12"
- DETECTABLE WARNINGS (SF)
- PCC SIDEWALK, 5"
- AGGREGATE BASE COURSE, TYPE B, 4"
- HMA DRIVEWAY (COMMERCIAL ENTRANCE)
- HMA SURFACE COURSE, MIX "D", N50, 2"
- HMA BASE COURSE, 8"
- HMA SURFACE COURSE, MIX "D", N50, 2"
- HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 5"
- AGGREGATE SUBGRADE IMPROVEMENT, 12"
- HMA SURFACE COURSE, MIX "D", N50, 2"
- LEVELING BINDER (MACHINE METHOD) N50, 3/4"
- CONCRETE MEDIAN, TYPE M-2.12
- CURB & GUTTER, TYPE B-6.12
- CURB & GUTTER, TYPE B-6.24



FILE NAME = 13375_02-PLPR-01 - IDOT PLPR01	USER NAME =	DESIGNED - EMA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE INTERSECTION IMPROVEMENTS ROADWAY PLAN & PROFILE		F.A.P. RTE. 353	SECTION 13-00063-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 8	
	PLOT SCALE =	CHECKED - PKB	REVISED -		SCALE: H 1"=50' V 1"=5'		SHEET NO. 8	OF 63 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(216)
	PLOT DATE = 05-19-16	DRAWN - PS/RG	REVISED -									
		CHECKED - AG	REVISED -									

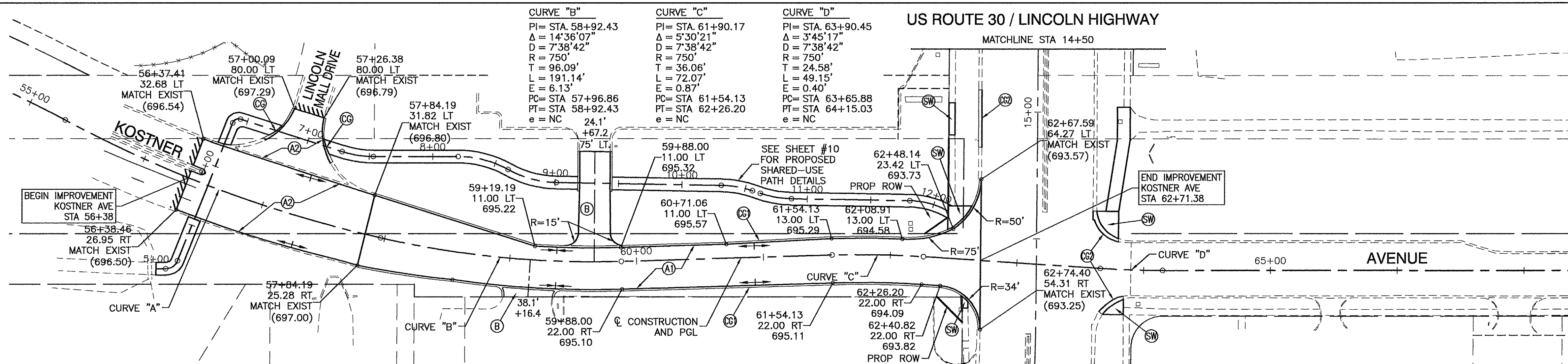


EXISTING CONDITIONS



LEGEND

- BUTT JOINT
- TREE TO BE REMOVED
- TREE TO BE PROTECTED WITH TRUNK PROTECTION & TEMPORARY FENCE
- CONC CURB & GUTTER REMOVAL & CURB REMOVAL
- MEDIAN REMOVAL
- PAVEMENT REMOVAL
- HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/2"
- SIDEWALK REMOVAL
- DRIVEWAY REMOVAL
- PCC PAVEMENT, 12"
- DETECTABLE WARNINGS (SF)
- PCC SIDEWALK, 5"
- AGGREGATE BASE COURSE, TYPE B, 4"
- HMA DRIVEWAY (COMMERCIAL ENTRANCE)
HMA SURFACE COURSE, MIX "D", N50, 2"
HMA BASE COURSE, 8"
- HMA SURFACE COURSE, MIX "D", N50, 2"
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 5"
AGGREGATE SUBGRADE IMPROVEMENT, 12"
- HMA SURFACE COURSE, MIX "D", N50, 2"
LEVELING BINDER (MACHINE METHOD) N50, 3/4"
- CONCRETE MEDIAN, TYPE M-2.12
- CURB & GUTTER, TYPE B-6.12
- CURB & GUTTER, TYPE B-6.24



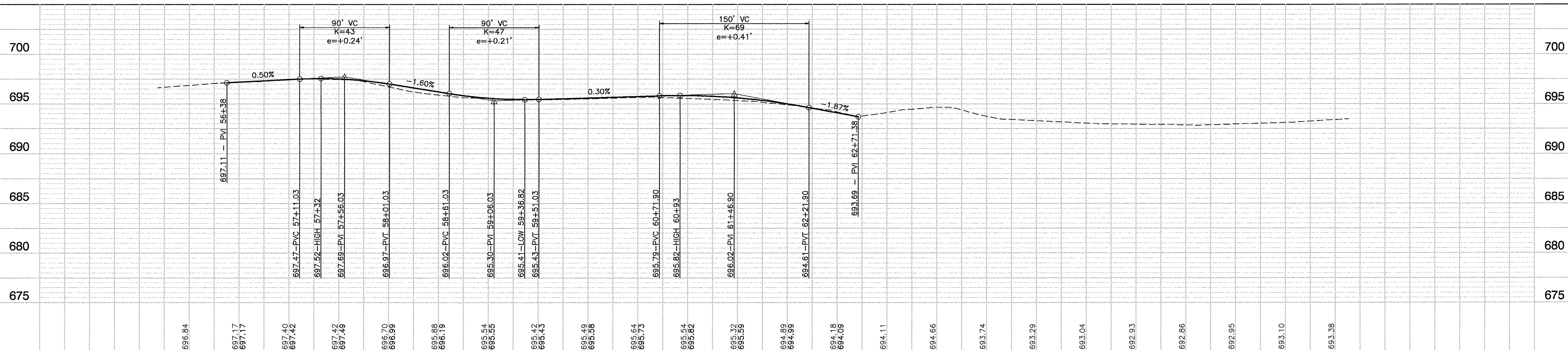
PROPOSED IMPROVEMENTS

CURVE "A"
 PI= STA 56+63.75
 $\Delta = 15'15''10"$
 $D = 5'43'55"$
 $R = 1000'$
 $T = 133.90'$
 $L = 266.21'$
 $E = 8.92'$
 PC= STA 55+30.65
 PT= STA 57+96.86
 $e = NC$

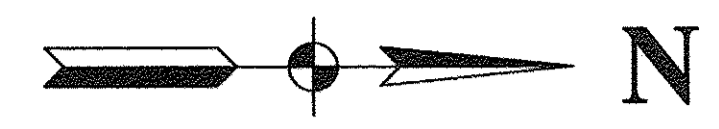
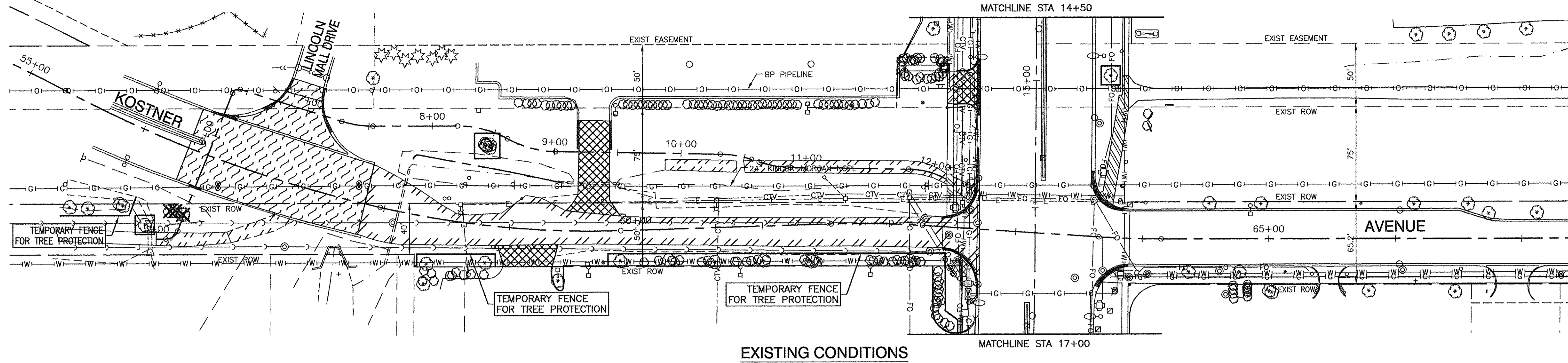
CURVE "B"
 PI= STA 58+92.43
 $\Delta = 14'36'07"$
 $D = 7'38'42"$
 $R = 750'$
 $T = 96.09'$
 $L = 191.14'$
 $E = 6.13'$
 PC= STA 57+96.86
 PT= STA 58+92.43
 $e = NC$

CURVE "C"
 PI= STA 61+90.17
 $\Delta = 5'30'21"$
 $D = 7'38'42"$
 $R = 750'$
 $T = 36.06'$
 $L = 72.07'$
 $E = 0.87'$
 PC= STA 61+54.13
 PT= STA 62+26.20
 $e = NC$

CURVE "D"
 PI= STA 63+90.45
 $\Delta = 3'45'17"$
 $D = 7'38'42"$
 $R = 750'$
 $T = 24.58'$
 $L = 49.15'$
 $E = 0.40'$
 PC= STA 63+65.88
 PT= STA 64+15.03
 $e = NC$

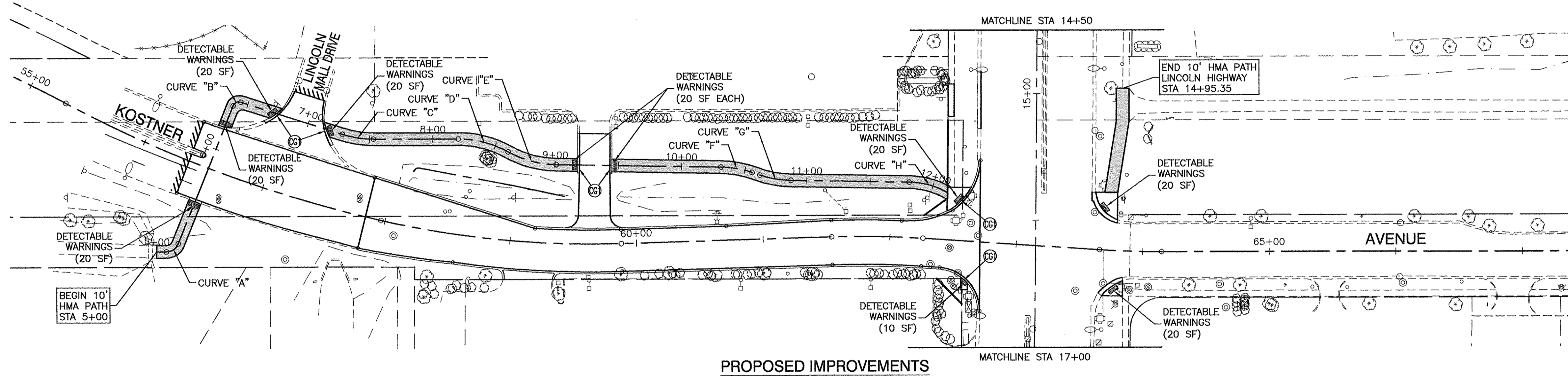


FILE NAME = 13375_02-PLPR-01 - IDOT PLPR02	USER NAME =	DESIGNED - EMA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE INTERSECTION IMPROVEMENTS ROADWAY PLAN & PROFILE			F.A.P. RTE. = 353	SECTION = 13-00063-00-CH	COUNTY = COOK	TOTAL SHEETS = 63	SHEET NO. = 9
	PLOT SCALE =	CHECKED - PKB	REVISED -					FED. ROAD DIST. NO. = 1	ILLINOIS	FED. AID PROJECT = M-4003(216)		
	PLOT DATE = 05-19-16	DRAWN - PS/RG	REVISED -					SCALE: H 1"=50' V 1"=5'	SHEET NO. 9 OF 63 SHEETS	STA. TO STA.		
		CHECKED - AG	REVISED -					CONTRACT NO. 61C11				



- LEGEND**
- TREE TO BE REMOVED
 - CONC CURB & GUTTER REMOVAL & CURB REMOVAL
 - MEDIAN REMOVAL
 - PAVEMENT REMOVAL
 - HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/2"
 - SIDEWALK REMOVAL
 - DRIVEWAY REMOVAL
 - PROPOSED 10' HMA PATH
 - HMA SURFACE COURSE, MIX "D", N50, 3"
 - AGGREGATE BASE COURSE, TYPE B, 6"
 - DETECTABLE WARNINGS (SF)
 - PCC SIDEWALK, 5"
 - AGGREGATE BASE COURSE, TYPE B, 4"
 - CURB & GUTTER, TYPE B-6.12

PLAN	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	NO. OF WAY CHECKED	
	CADD FILE NAME	
	NO.	



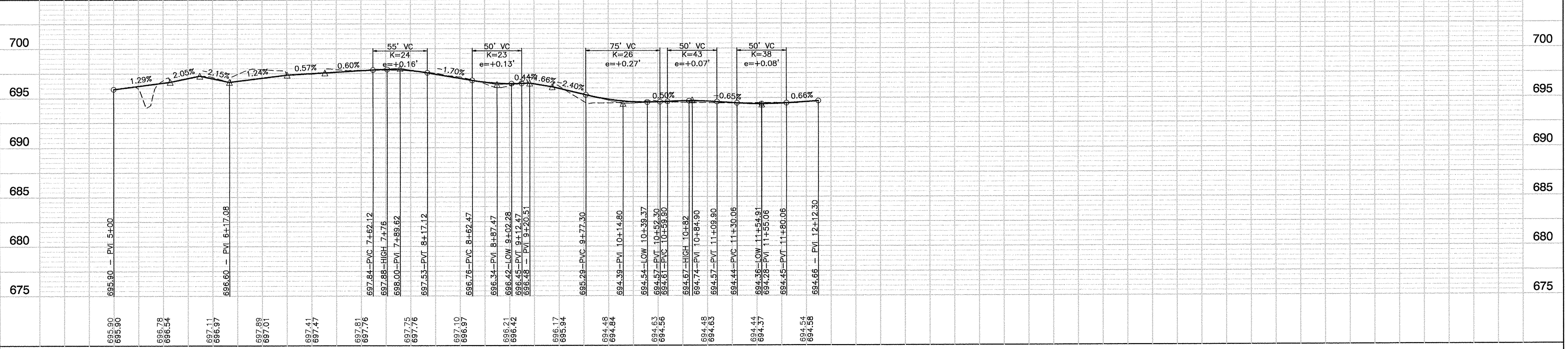
PATH

CURVE "A"	CURVE "B"	CURVE "C"
PI= STA. 5+13.61	PI= STA. 6+40.47	PI= STA. 7+40.78
$\Delta = 69^{\circ}02'03"$	$\Delta = 85^{\circ}59'41"$	$\Delta = 17^{\circ}41'12"$
D = 572'57'28"	D = 1145'54'56"	D = 70'44'08"
R = 10'	R = 5'	R = 81'
T = 6.88'	T = 4.66'	T = 12.60'
L = 12.05'	L = 7.50'	L = 25.00'
E = 2.14'	E = 1.84'	E = 0.97'
PC= STA 5+07.59	PC= STA 6+35.81	PC= STA 7+28.18
PT= STA 5+19.63	PT= STA 6+43.31	PT= STA 7+53.18
e = NC	e = NC	e = NC

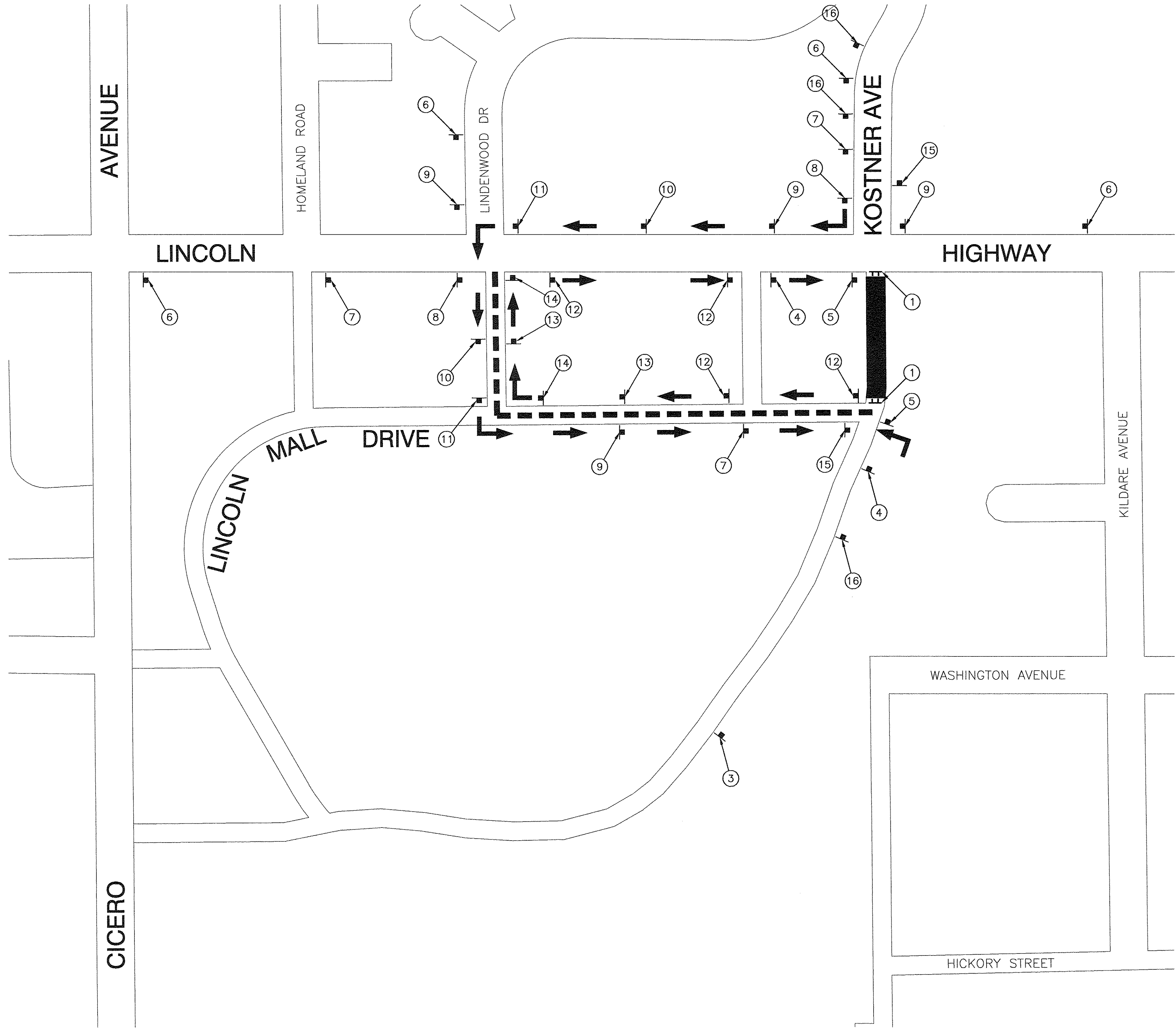
CURVE "D"	CURVE "E"	CURVE "F"
PI= STA. 8+41.06	PI= STA. 8+81.47	PI= STA. 10+43.53
$\Delta = 29^{\circ}06'18"$	$\Delta = 28^{\circ}07'08"$	$\Delta = 17^{\circ}41'02"$
D = 70'44'08"	D = 70'44'08"	D = 70'44'08"
R = 81'	R = 81'	R = 81'
T = 21.03'	T = 20.28'	T = 12.60'
L = 41.15'	L = 39.75'	L = 25.00'
E = 2.68'	E = 2.50'	E = 0.97'
PC= STA 8+20.03	PC= STA 8+61.19	PC= STA 10+30.93
PT= STA 8+61.19	PT= STA 9+00.94	PT= STA 10+55.93
e = NC	e = NC	e = NC

CURVE "G"	CURVE "H"
PI= STA. 10+75.12	PI= STA. 11+96.89
$\Delta = 17^{\circ}41'02"$	$\Delta = 28^{\circ}20'39"$
D = 70'44'08"	D = 90'02'38"
R = 81'	R = 63.63'
T = 12.60'	T = 16.07'
L = 25.00'	L = 31.48'
E = 0.97'	E = 2.00'
PC= STA 10+62.52	PC= STA 11+80.82
PT= STA 10+87.52	PT= STA 12+12.3
e = NC	e = NC

PROFILE	SURVEYED	DATE
	PLOTTED	
	CHECKED	
	BY	
	GRADES CHECKED	
	DATE	



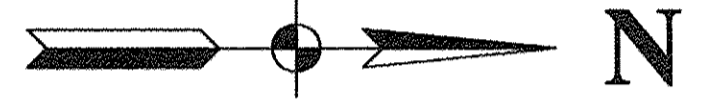
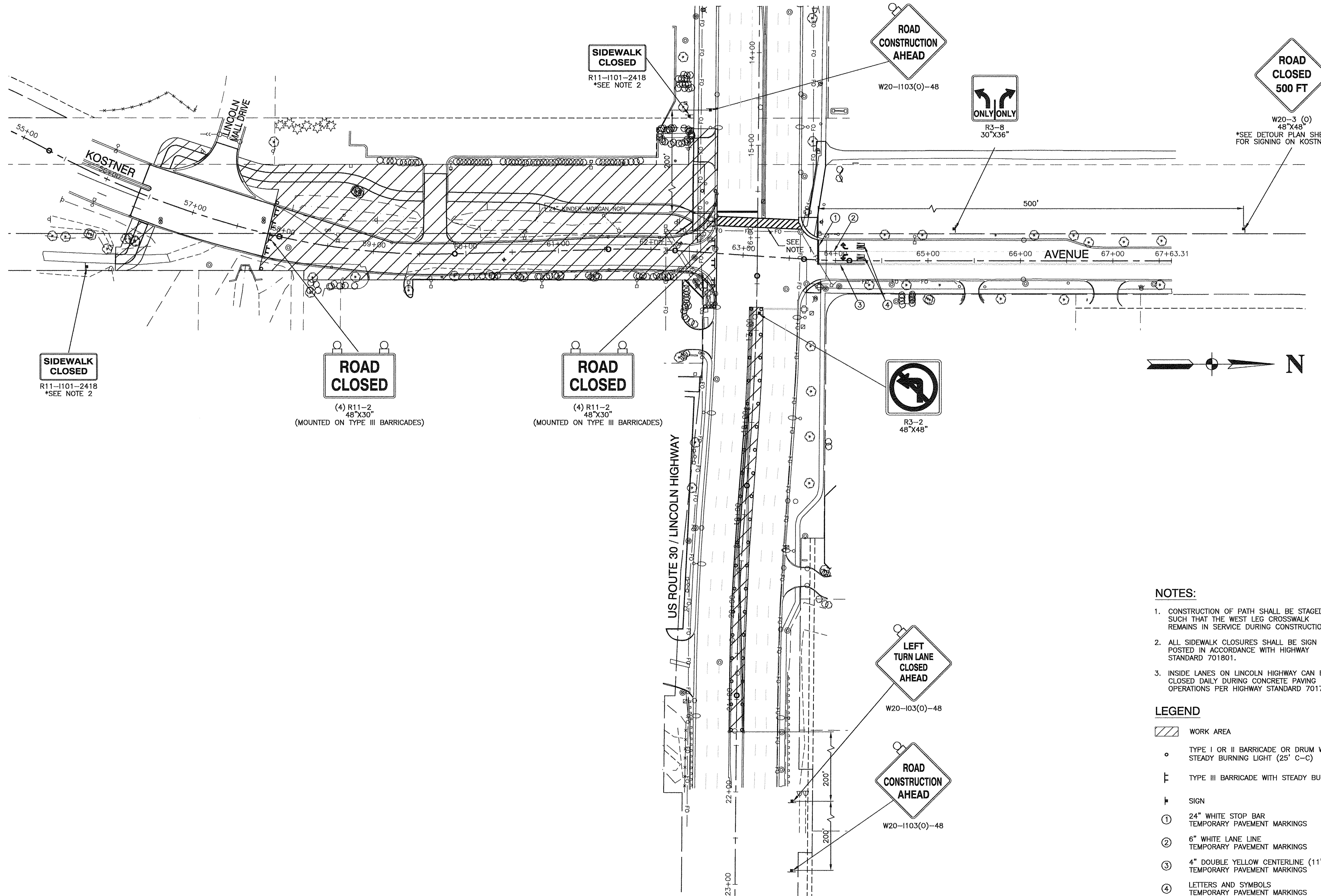
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	PLOT SCALE =	CHECKED - PKB	REVISED -			CONTRACT NO. 61C11					
	PLOT DATE = 05-19-16	DRAWN - PS/RG	REVISED -			SCALE: H 1"=50' V 1"=5'					
		CHECKED - AG	REVISED -			SHEET NO. 10 OF 63 SHEETS STA. TO STA.					
						FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(216)			



<p>1 ROAD CLOSED R11-2 48"x30" TYPE III BARRICADES (4)</p> <p>2 ROAD CLOSED AHEAD W20-3 (0) 48"x48"</p> <p>3 DETOUR AHEAD W20-2 48"x48"</p> <p>4 NORTH M3-1 (0) 24"x12" Kostner Ave D3-1 DETOUR M4-9 30"x24" (TYP)</p> <p>5 NORTH M3-1 (0) Kostner Ave D3-1 DETOUR M4-9 (L)</p> <p>6 DETOUR AHEAD W20-2 48"x48" SOUTH M3-3 (0) 24"x12" Kostner Ave D3-1</p>	<p>7 SOUTH M3-3 (0) 24"x12" Kostner Ave D3-1 DETOUR M4-9</p> <p>8 SOUTH M3-3 (0) 24"x12" Kostner Ave D3-1 DETOUR M4-9 (R)</p> <p>9 SOUTH M3-3 (0) 24"x12" Kostner Ave D3-1 DETOUR M4-9</p> <p>10 SOUTH M3-3 (0) 24"x12" Kostner Ave D3-1 DETOUR M4-9</p> <p>11 SOUTH M3-3 (0) 24"x12" Kostner Ave D3-1 DETOUR M4-9 (L)</p>	<p>12 NORTH M3-1 (0) 24"x12" Kostner Ave D3-1 DETOUR M4-9</p> <p>13 NORTH M3-1 (0) 24"x12" Kostner Ave D3-1 DETOUR M4-9</p> <p>14 NORTH M3-1 (0) 24"x12" Kostner Ave D3-1 DETOUR M4-9 (R)</p> <p>15 END DETOUR M4-8a 24"x18" Kostner Ave D3-1</p> <p>16 ROAD CLOSED 500 FT W20-3 (0) 48"x48"</p>
---	---	--

LEGEND

▬▬▬	TYPE III BARRICADE
■	SIGN
--- --	DETOUR ROUTE
→	DIRECTION OF TRAFFIC
■	CONSTRUCTION AREA CLOSED TO TRAFFIC



- NOTES:**
1. CONSTRUCTION OF PATH SHALL BE STAGED SUCH THAT THE WEST LEG CROSSWALK REMAINS IN SERVICE DURING CONSTRUCTION.
 2. ALL SIDEWALK CLOSURES SHALL BE SIGN POSTED IN ACCORDANCE WITH HIGHWAY STANDARD 701801.
 3. INSIDE LANES ON LINCOLN HIGHWAY CAN BE CLOSED DAILY DURING CONCRETE PAVING OPERATIONS PER HIGHWAY STANDARD 701701.

- LEGEND**
- WORK AREA
 - TYPE I OR II BARRICADE OR DRUM WITH STEADY BURNING LIGHT (25' C-C)
 - TYPE III BARRICADE WITH STEADY BURNING LIGHT
 - SIGN
 - 24" WHITE STOP BAR
TEMPORARY PAVEMENT MARKINGS
 - 6" WHITE LANE LINE
TEMPORARY PAVEMENT MARKINGS
 - 4" DOUBLE YELLOW CENTERLINE (11" C-C)
TEMPORARY PAVEMENT MARKINGS
 - LETTERS AND SYMBOLS
TEMPORARY PAVEMENT MARKINGS

FILE NAME = 13375_02-TC0N-01 - IDOT P01

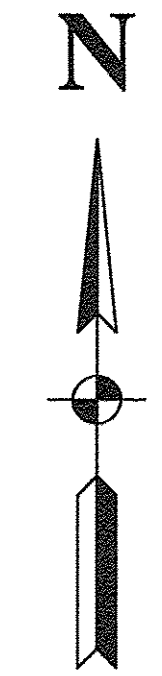
USER NAME =	DESIGNED — EMA	REVISED —
	CHECKED — PKB	REVISED —
PLOT SCALE =	DRAWN — RG	REVISED —
PLOT DATE = 05-19-16	CHECKED — AG	REVISED —

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE
INTERSECTION IMPROVEMENTS
TEMPORARY TRAFFIC CONTROL PLAN**

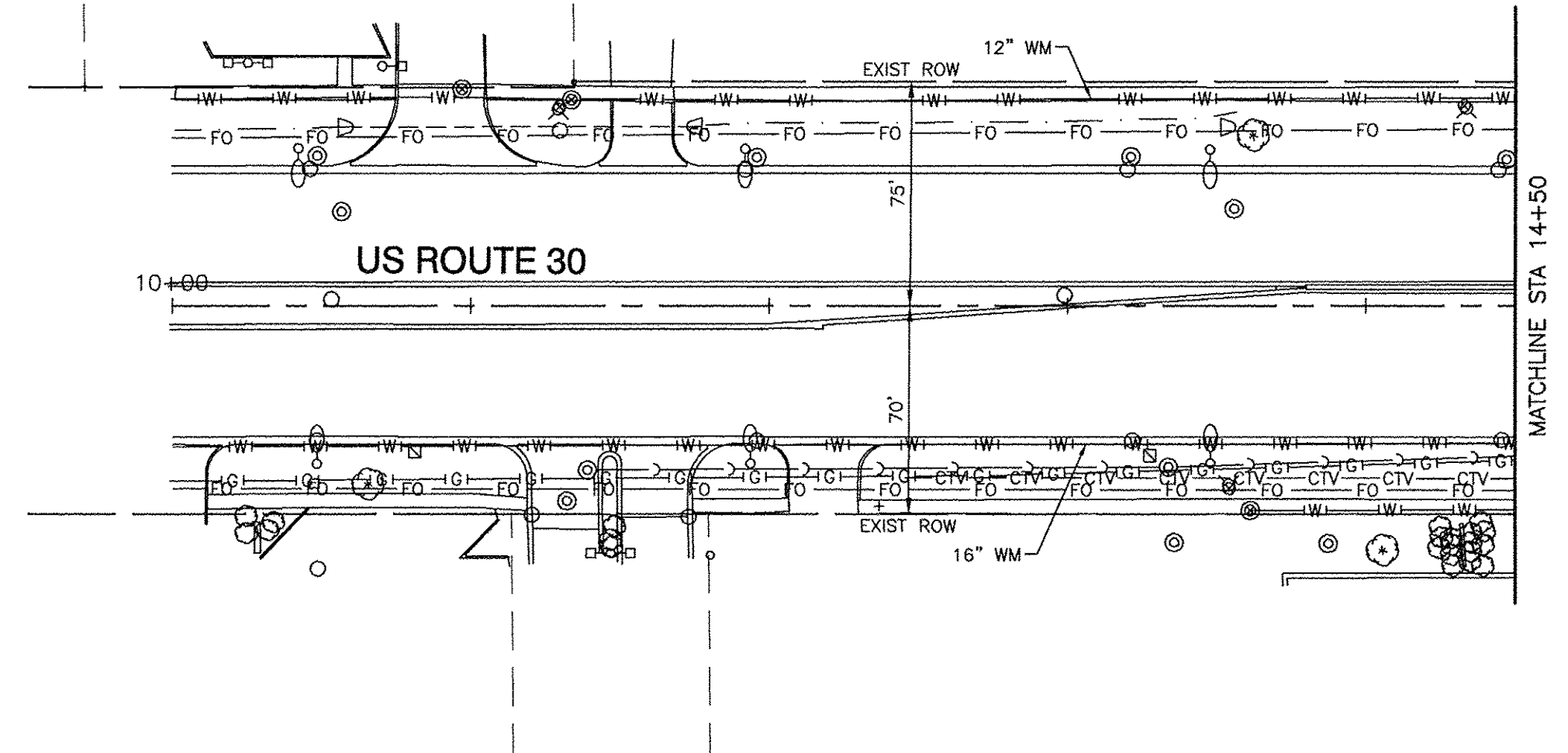
SCALE: SHEET NO. 12 OF 63 SHEETS STA. TO STA.

F.A.P. RTE. 353	SECTION 13-00063-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 12
FED. ROAD DIST. NO. 1 ILLINOIS			FED. AID PROJECT M-4003(216)	

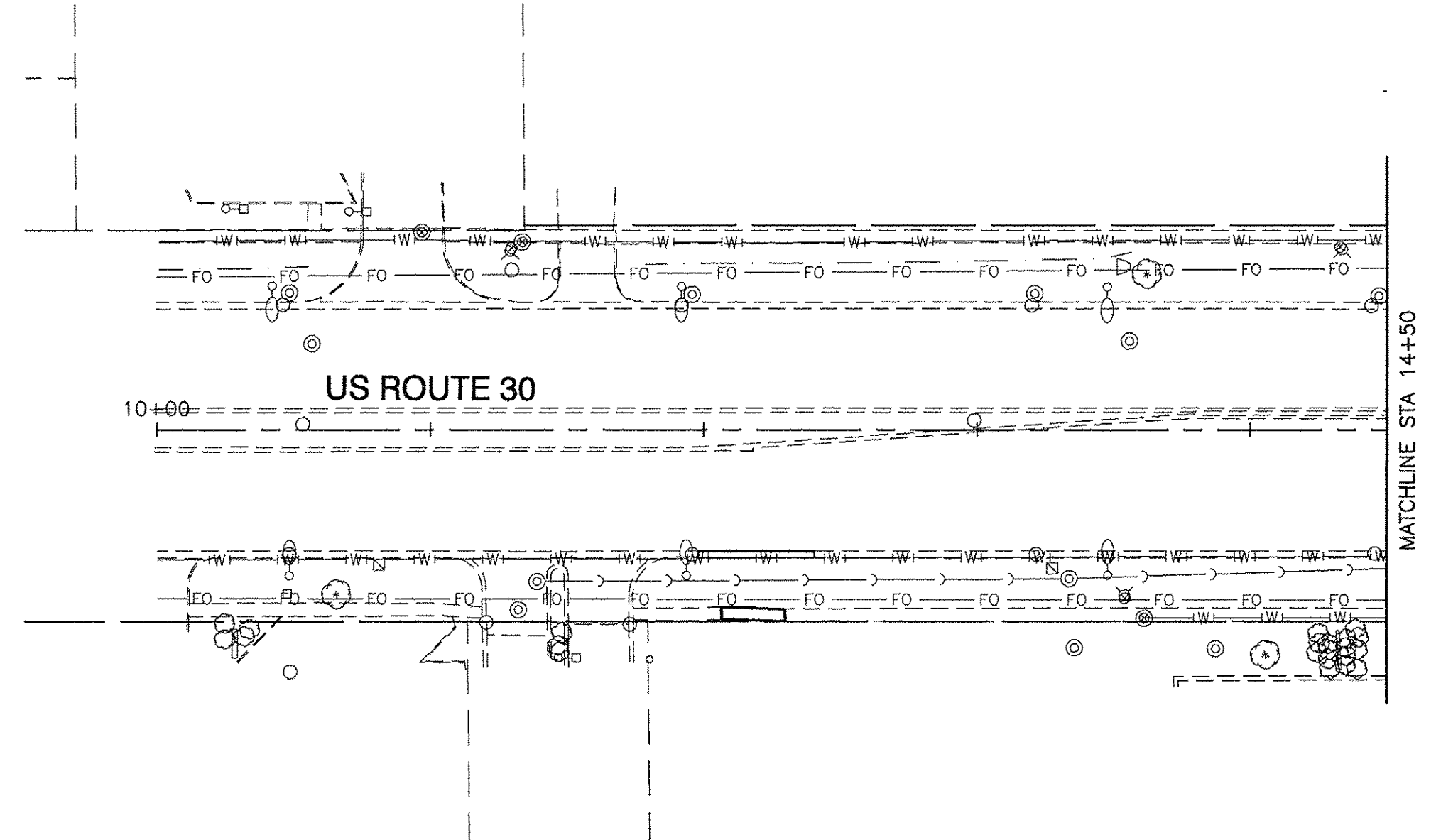
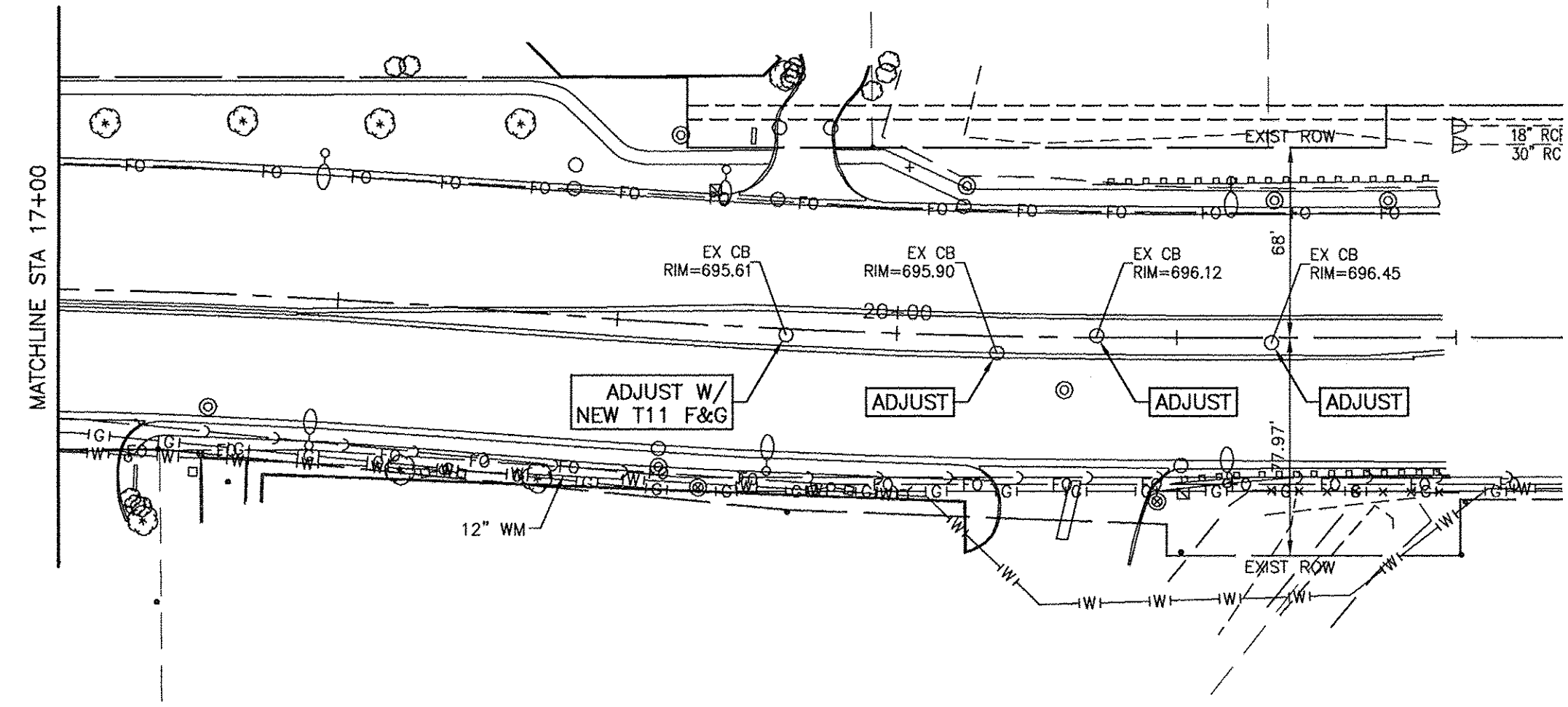


PLAN	SURVEYED	DATE
	PLOTTED	
NOTE BOOK NO.	ALIGNMENT CHECKED	
	CAAD FILE NAME	

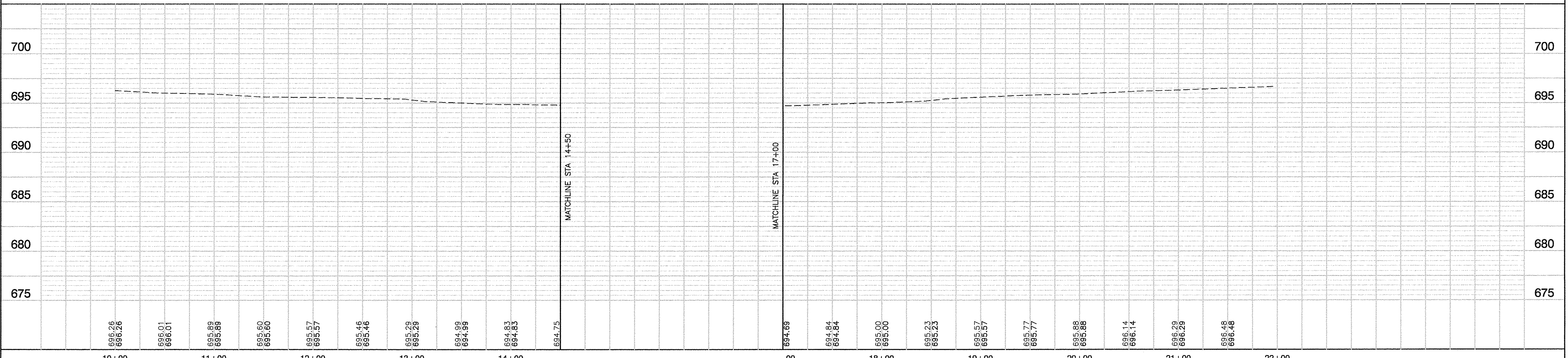
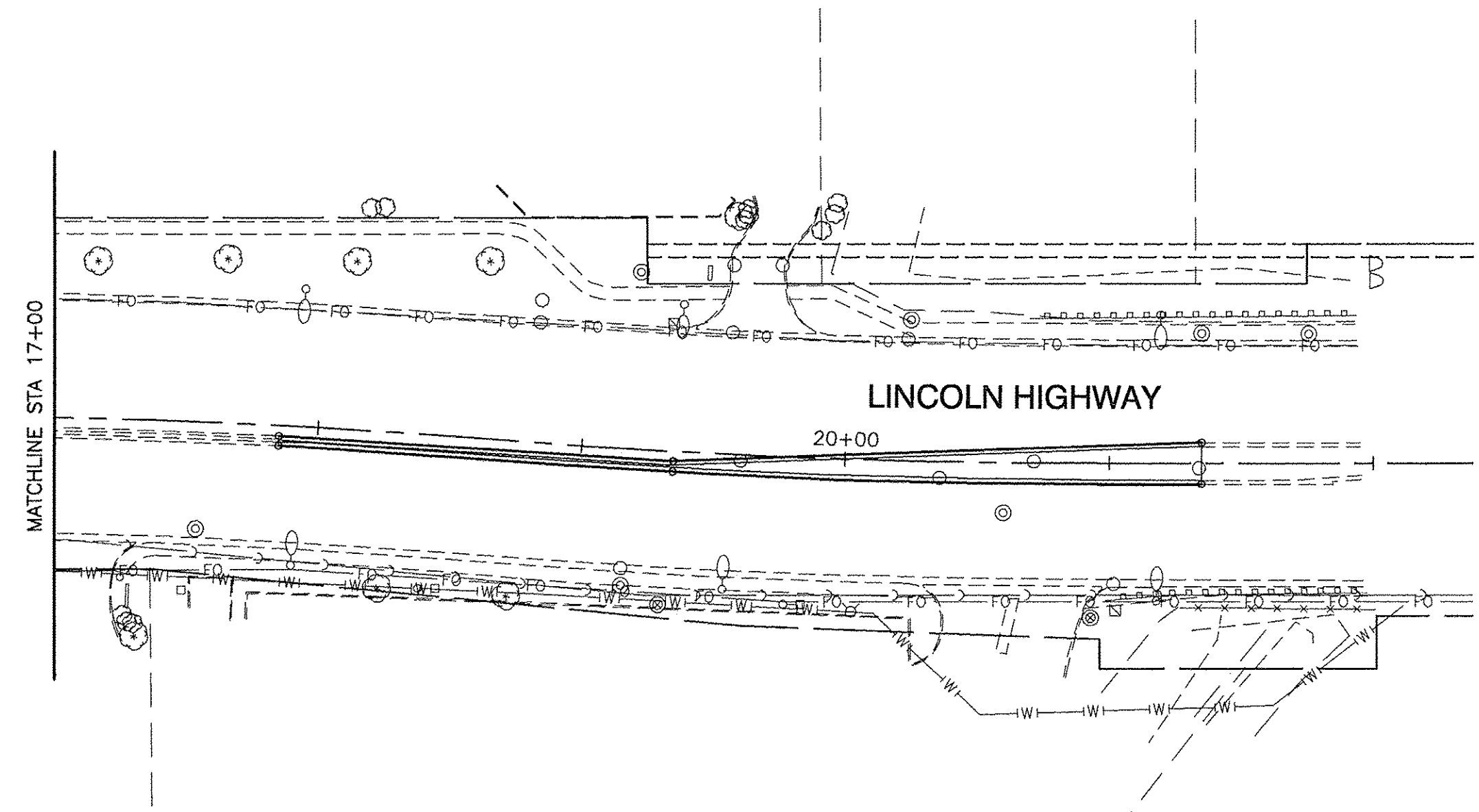
PROFILE	SURVEYED	DATE
	PLOTTED	
NOTE BOOK NO.	GRADES CHECKED	
	B.M. NOTED	



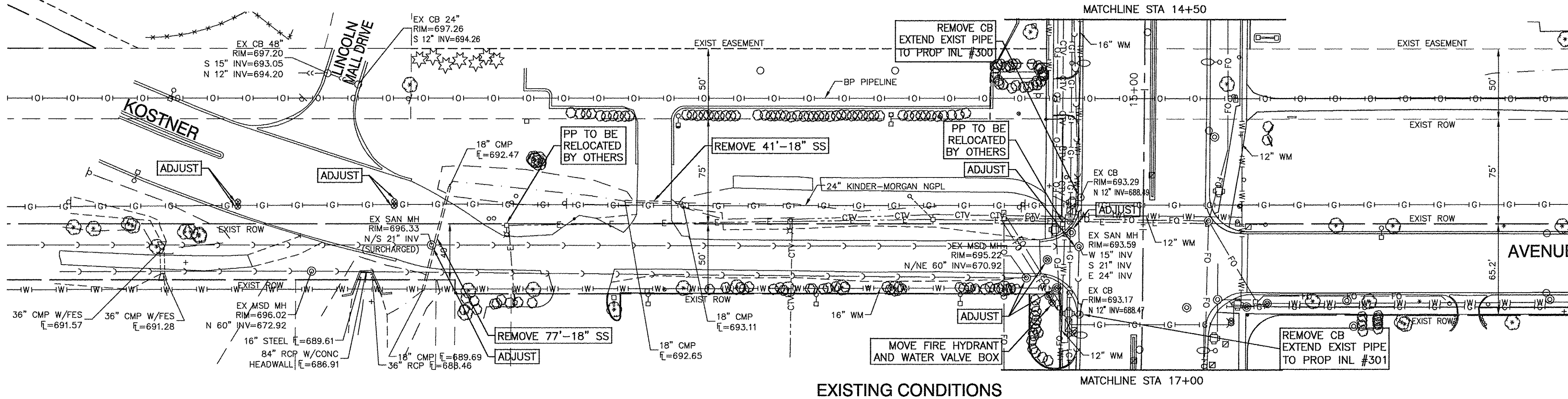
EXISTING CONDITIONS



PROPOSED IMPROVEMENTS

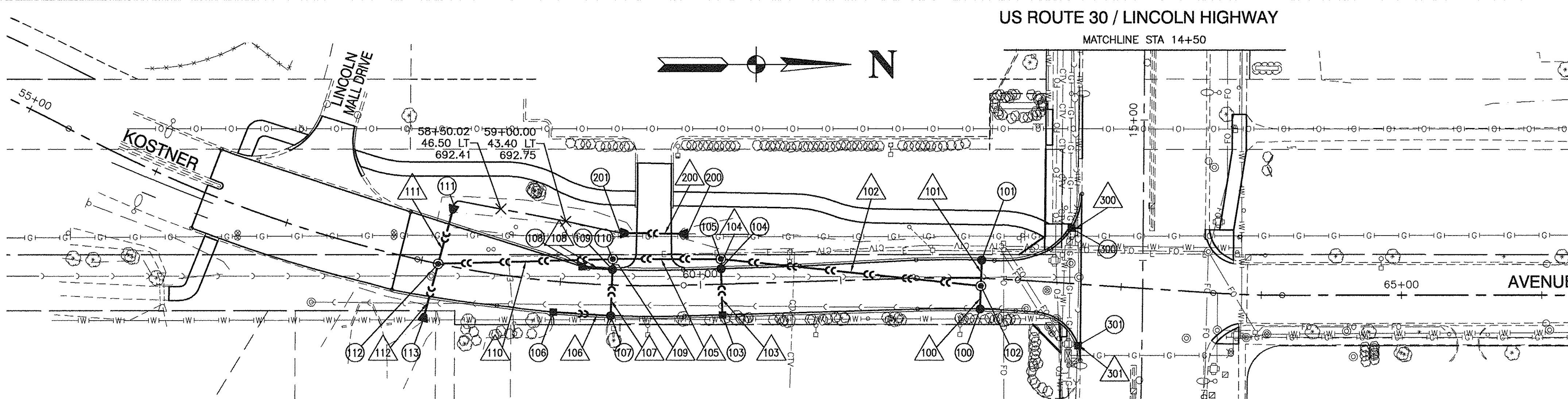


FILE NAME = 13375_02-PLPR-01 - IDOT UTIL01	USER NAME =	DESIGNED — EMA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE INTERSECTION IMPROVEMENTS DRAINAGE & UTILITIES			F.A.P. RTE 353	SECTION 13-00063-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 14
	PLOT SCALE =	CHECKED — PKB	REVISED —		SCALE: H 1"=50' V 1"=5'	SHEET NO. 14	OF 63 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(216)
	PLOT DATE = 05-19-16	DRAWN — PS/RG	REVISED —									
		CHECKED — AG	REVISED —									



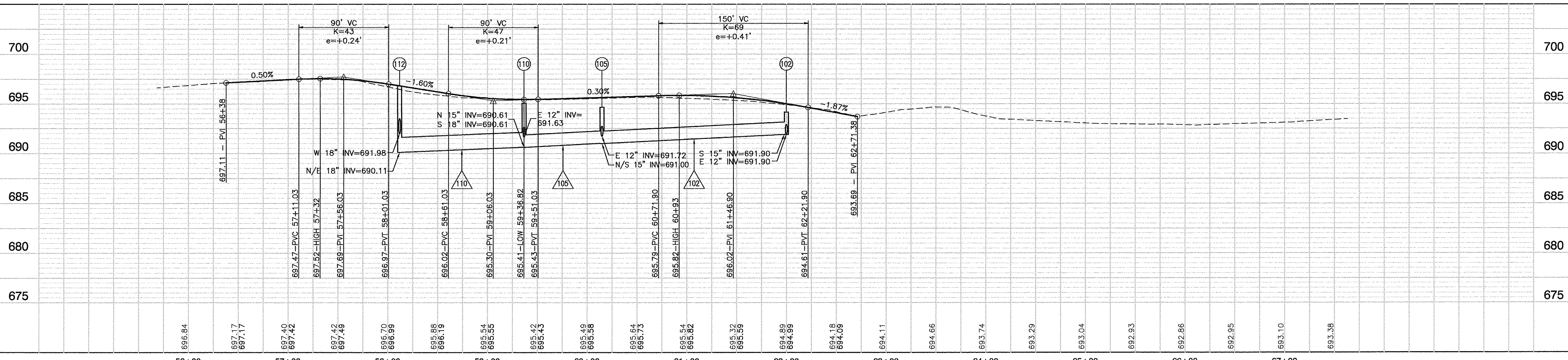
EXISTING CONDITIONS

100	STA 62+00, 22' RT CB TA, 4' DIA, T11 F&G RIM EL=694.55 W 12\"	108	STA 59+16.82, 11.58' LT INLET TA, T11 F&G RIM EL=695.21 N 12\"	300	STA 62+61.76, 39.56' LT INLET TA, T11 F&G RIM EL=693.30 N 12\"
101	STA 62+00, 13' LT CB TA, 4' DIA, T11 F&G RIM EL=694.73 E 12\"	109	STA 59+36.82, 11' LT CB TA, 4' DIA, T11 F&G RIM EL=695.19 S 12\"	301	STA 62+72.18, 43.79' RT INLET TA, T11 F&G RIM EL=693.17 N 12\"
102	STA 62+00, 5.5' RT MH TA, 4' DIA, T1F CL RIM EL=694.88 E 12\"	110	STA 59+36.82, 18.5' LT MH TA, 4' DIA, T1F CL RIM EL=695.06 E 12\"	100	17'-12\" RCP SST1 @ 0.50% (0.8)
103	STA 60+15, 22' RT INLET TA, T11 F&G RIM EL=695.18 W 12\"	111	STA 58+14.61, 43.02' LT 18\" END SECTION W 18\"	101	19'-12\" RCP SST1 @ 0.50% (1.1)
				102	186'-15\" RCP SST1 @ 0.50% (21.4)
				103	33'-12\" RCP SST1 @ 0.50% (3.6)
				104	7'-12\" RCP SST1 @ 0.50% (1.9)
				105	77'-15\" RCP SST1 @ 0.50% (9.3)



PROPOSED IMPROVEMENTS

104	STA 60+15, 11' LT CB TA, 4' DIA, T11 F&G RIM EL=695.40 E 12\"	112	STA 58+12, 0' RT MH TA, 5' DIA, T1F CL RIM EL=695.80 N 18\"	106	41'-12\" RCP SST1 @ 0.50% (3.0)
105	STA 60+15, 18' LT MH TA, 4' DIA, T1F CL RIM EL=694.64 E 12\"	113	STA 58+09.64, 42.34' RT 18\" END SECTION E 18\"	107	33'-12\" RCP SST1 @ 0.50% (3.5)
106	STA 58+97, 22' RT INLET TA, T11 F&G RIM EL=695.14 N 12\"	200	STA 59+91.77, 36.69' LT 18\" END SECTION N 18\"	108	20'-12\" RCP SST1 @ 0.50% (1.9)
107	STA 59+36.82, 22' RT CB TA, 4' DIA, T11 F&G RIM EL=694.97 S 12\"	201	STA 59+41.64, 37.36' LT 18\" END SECTION S 18\"	109	8'-12\" RCP SST1 @ 0.50% (2.3)
				110	125'-18\" RCP SST1 @ 0.40% (35.7)
				111	43'-18\" RCP SST1 @ 1.00% (6.4)
				112	42'-18\" RCP SST1 @ 1.00% (20.4)
				200	32'-18\" PIPE CULVERT T1 @ 1.00% (5.6)
				300	6'-12\" RCP SST1 @ 1.00% (1.6)
				301	2'-12\" RCP SST1 @ 1.00% (0.5)



FILE NAME = 13375_02_LPR-01 - IDOT UT1.02

USER NAME =	DESIGNED - EMA	REVISED -
CHECKED - PKB	CHECKED -	REVISED -
DRAWN - PS/RG	DRAWN -	REVISED -
CHECKED - AG	CHECKED -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

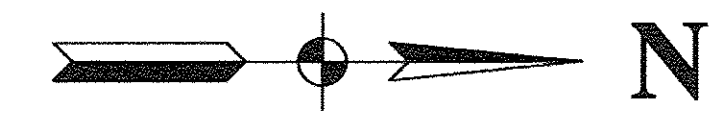
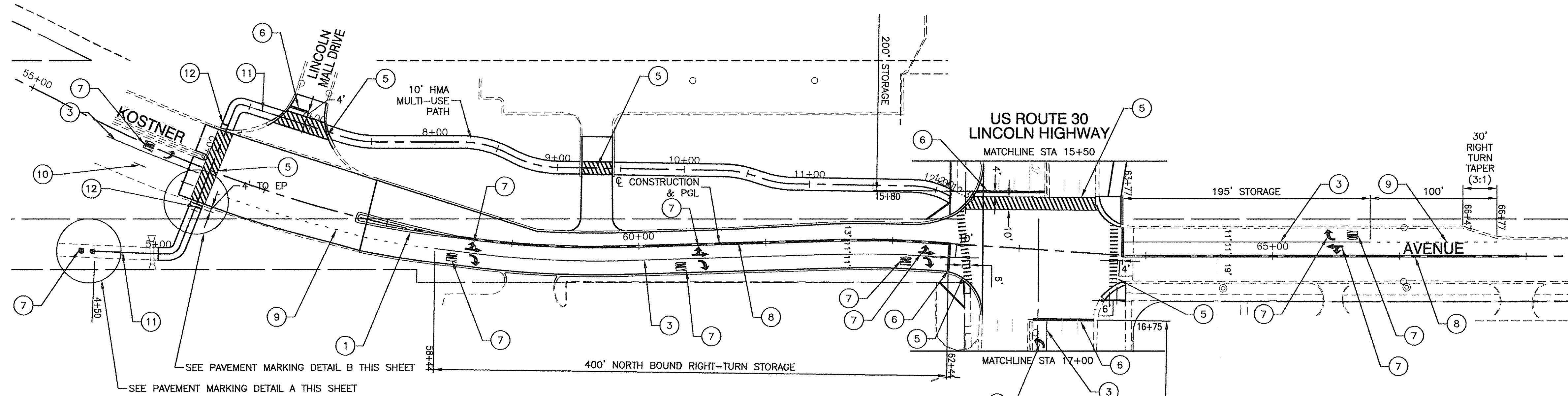
US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE
INTERSECTION IMPROVEMENTS
DRAINAGE & UTILITIES

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	13-00063-00-CH	COOK	63	15
CONTRACT NO. 61C11				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(216)		

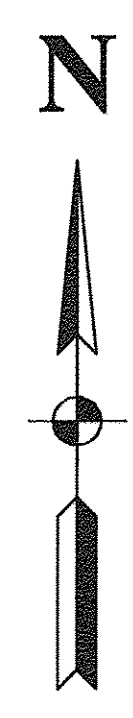
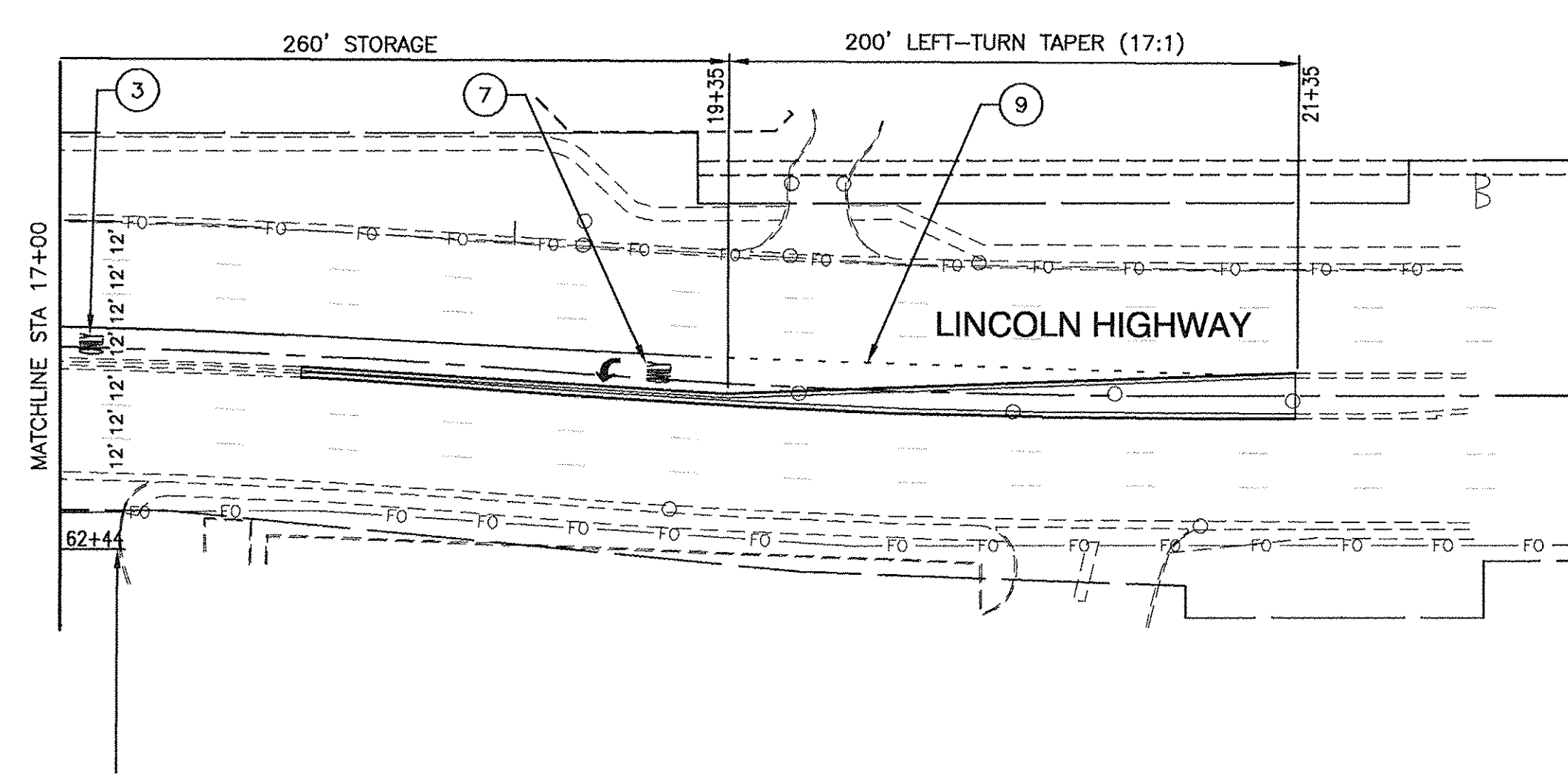
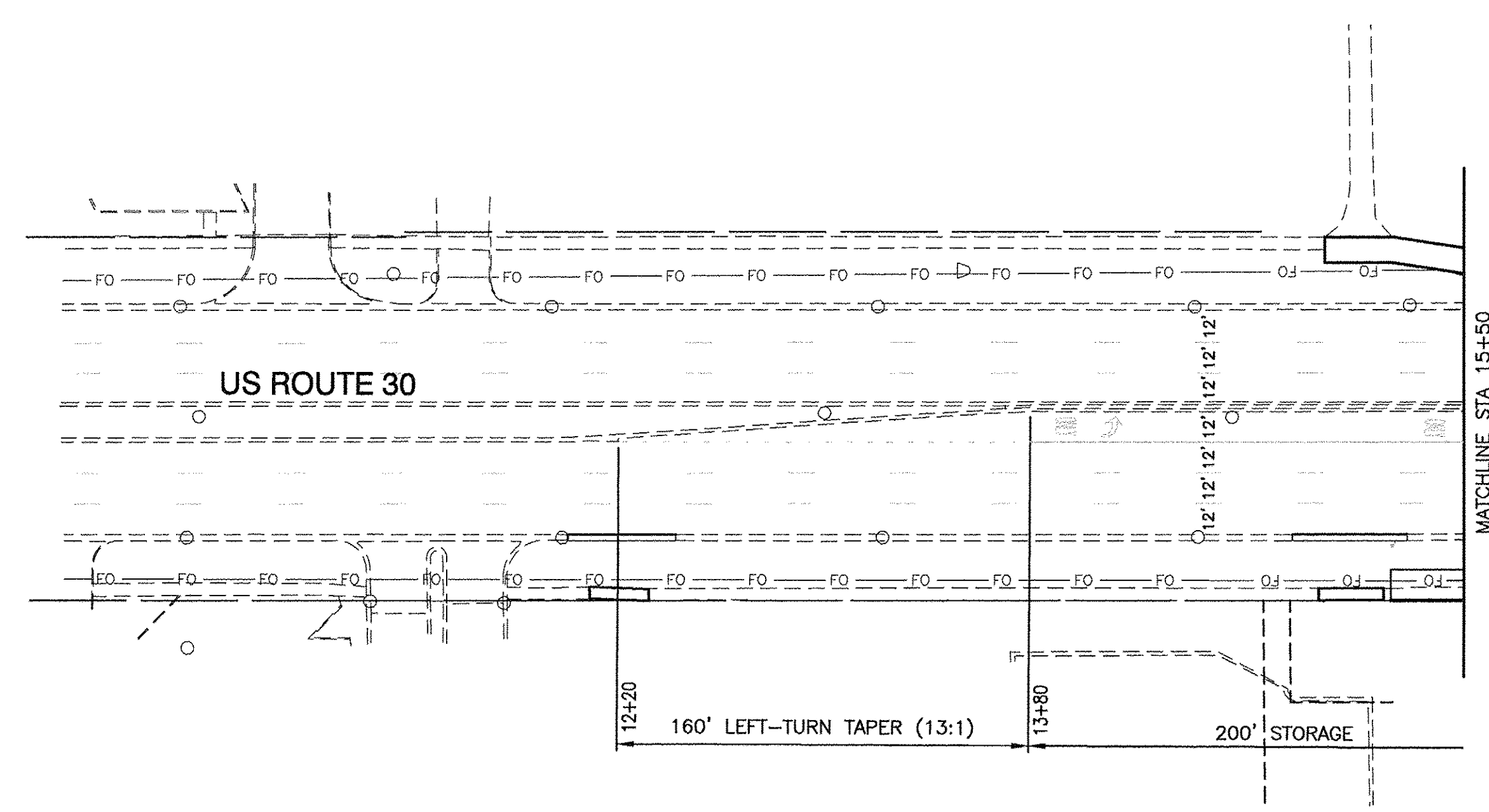
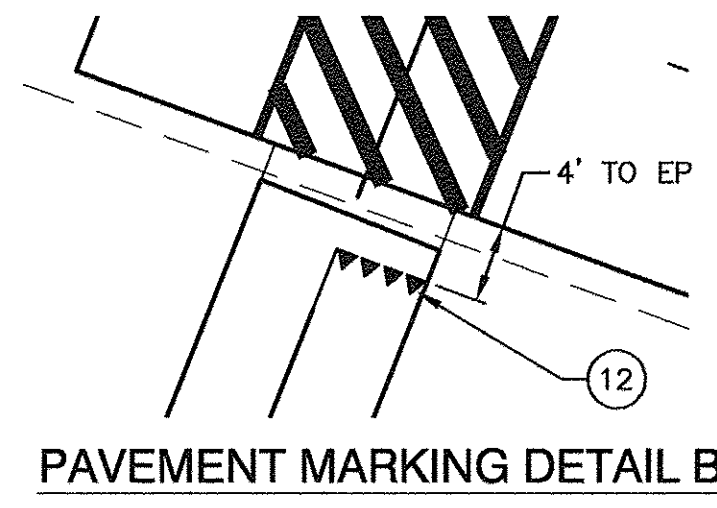
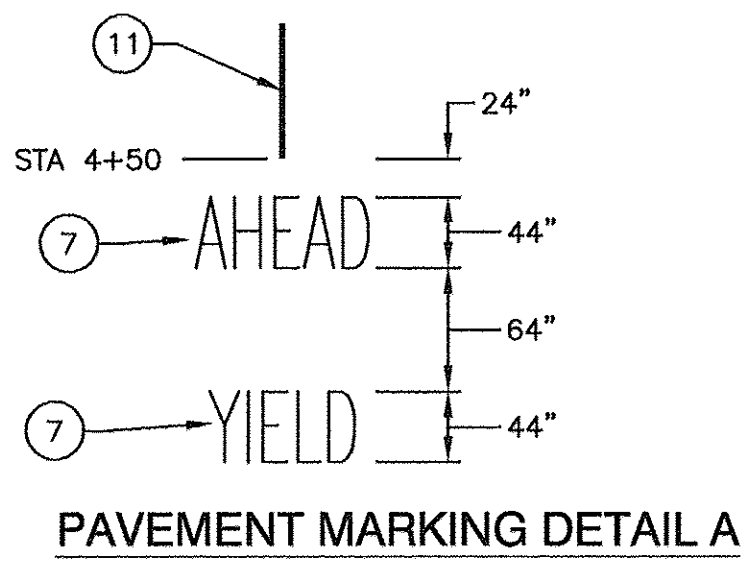
SCALE: H 1"=50' V 1"=5' SHEET NO. 15 OF 63 SHEETS STA. TO STA.

DATE	BY
DATE	BY
DATE	BY

DATE	BY
DATE	BY
DATE	BY



DATE	
BY	
DESIGNED	
CHECKED	
IN CHARGE	
FILE NAME	
NO.	
PLAN	
NOTE BOOK	



PAVEMENT MARKING LEGEND

- ① DOUBLE YELLOW MEDIAN OUTLINE (11" C/C)
- ② 12" YELLOW DIAGONAL LINE (15' C/C)
- ③ 6" WHITE LANE LINE
- ④ 6" WHITE CROSSWALK LINE (6' C/C)
- ⑤ 12" WHITE CROSSWALK LINE (3' C/C)
- ⑥ 24" WHITE STOP BAR
- ⑦ WHITE LETTERS & SYMBOLS
- ⑧ DOUBLE 4" YELLOW CENTERLINE (11" C/C)
- ⑨ 6" WHITE SKIP-DASH LINE (2' LINE - 6' SPACE/BLANK)
- ⑩ 4" WHITE SKIP-DASH (10' LINE - 30' SPACE)
- ⑪ 4" SOLID YELLOW CENTERLINE
- ⑫ 12" YIELD LINE (12" TRIANGLE, 3" GAP)

NOTES

- 1. MODIFIED URETHANE PAVEMENT MARKINGS SHALL BE APPLIED TO ALL PCC PAVEMENT SECTIONS.
- 2. THERMOPLASTIC PAVEMENT MARKINGS SHALL BE APPLIED TO ALL HMA PAVEMENT SECTIONS.

FILE NAME = 13375_02-PMKG-01 - IDOT PMKG

USER NAME =	DESIGNED — EMA	REVISED —
	CHECKED — PKB	REVISED —
PLOT SCALE =	DRAWN — PS	REVISED —
PLOT DATE = 05-19-16	CHECKED — AG	REVISED —

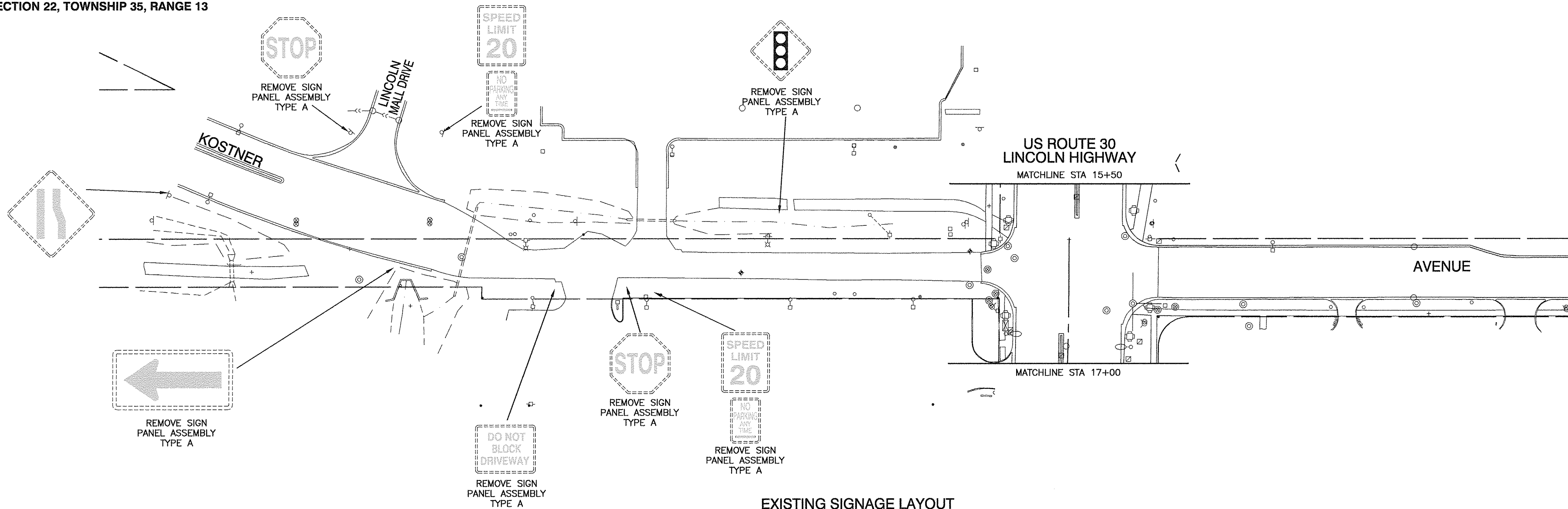
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE
INTERSECTION IMPROVEMENTS
PAVEMENT MARKING PLAN

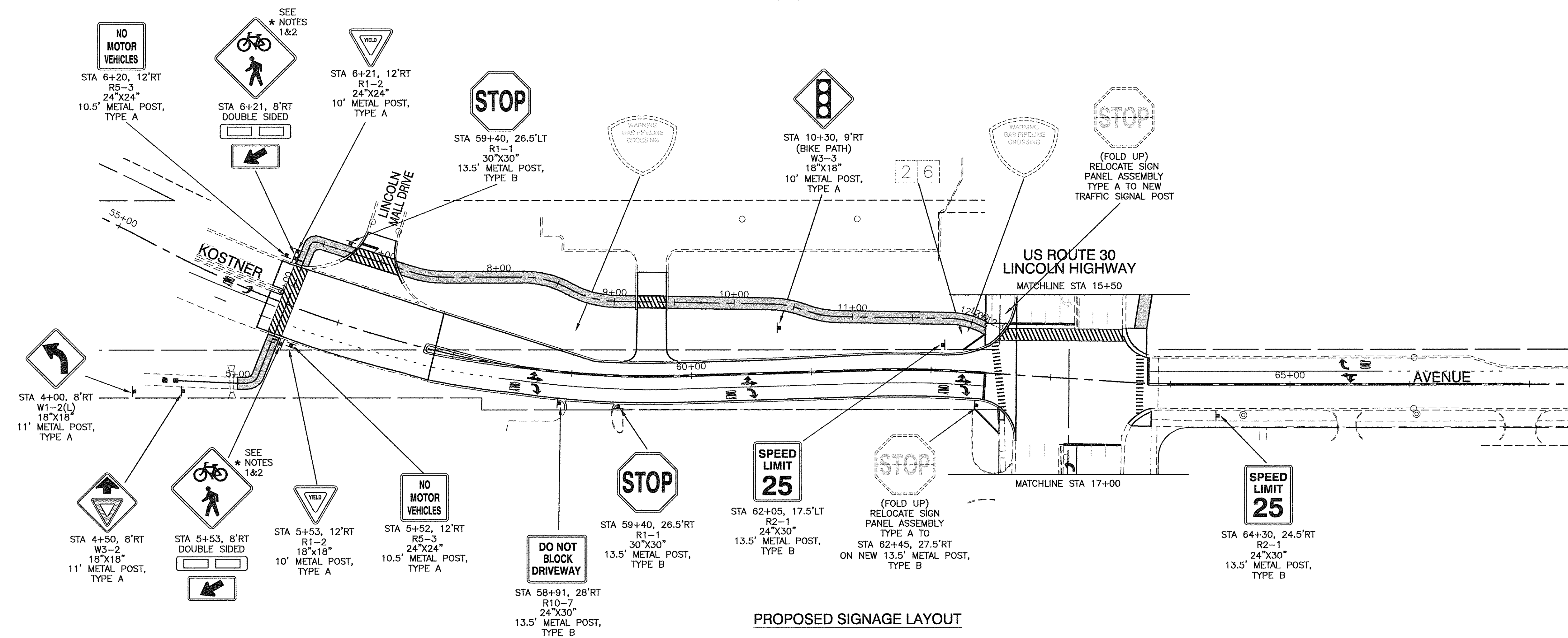
SCALE: SHEET NO. 16 OF 63 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	13-00063-00-CH	COOK	63	16
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 61C11	
FED. AID PROJECT M-4003(216)				

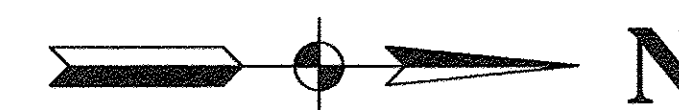
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FIELD FILE NAME	
PLAN NO.	
NOTE BOOK NO.	



EXISTING SIGNAGE LAYOUT



PROPOSED SIGNAGE LAYOUT



NOTES:

1. SEE SHEET 43 FOR DETAILS OF PUSH BUTTON ACTIVATED RECTANGULAR RAPID FLASHING BEACONS (RRFB) AND ASSOCIATED CROSSWALK SIGNS.
2. SIGN PANELS SHALL FACE BOTH DIRECTIONS OF TRAVEL ON KOSTNER AVE ON EACH ASSEMBLY.
3. ALL OFFSETS ARE FROM CENTERLINE UNLESS OTHERWISE NOTED.

FILE NAME = 13375_02-PMKG-01 - IDOT SIGN

USER NAME =	DESIGNED — EMA	REVISIONS —
PLOT SCALE =	CHECKED — PKB	REVISIONS —
PLOT DATE = 05-19-16	DRAWN — PS	REVISIONS —
	CHECKED — AG	REVISIONS —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE
INTERSECTION IMPROVEMENTS
SIGNING PLAN

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	13-00083-00-CH	COOK	63	17
CONTRACT NO. 61C11				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(216)				

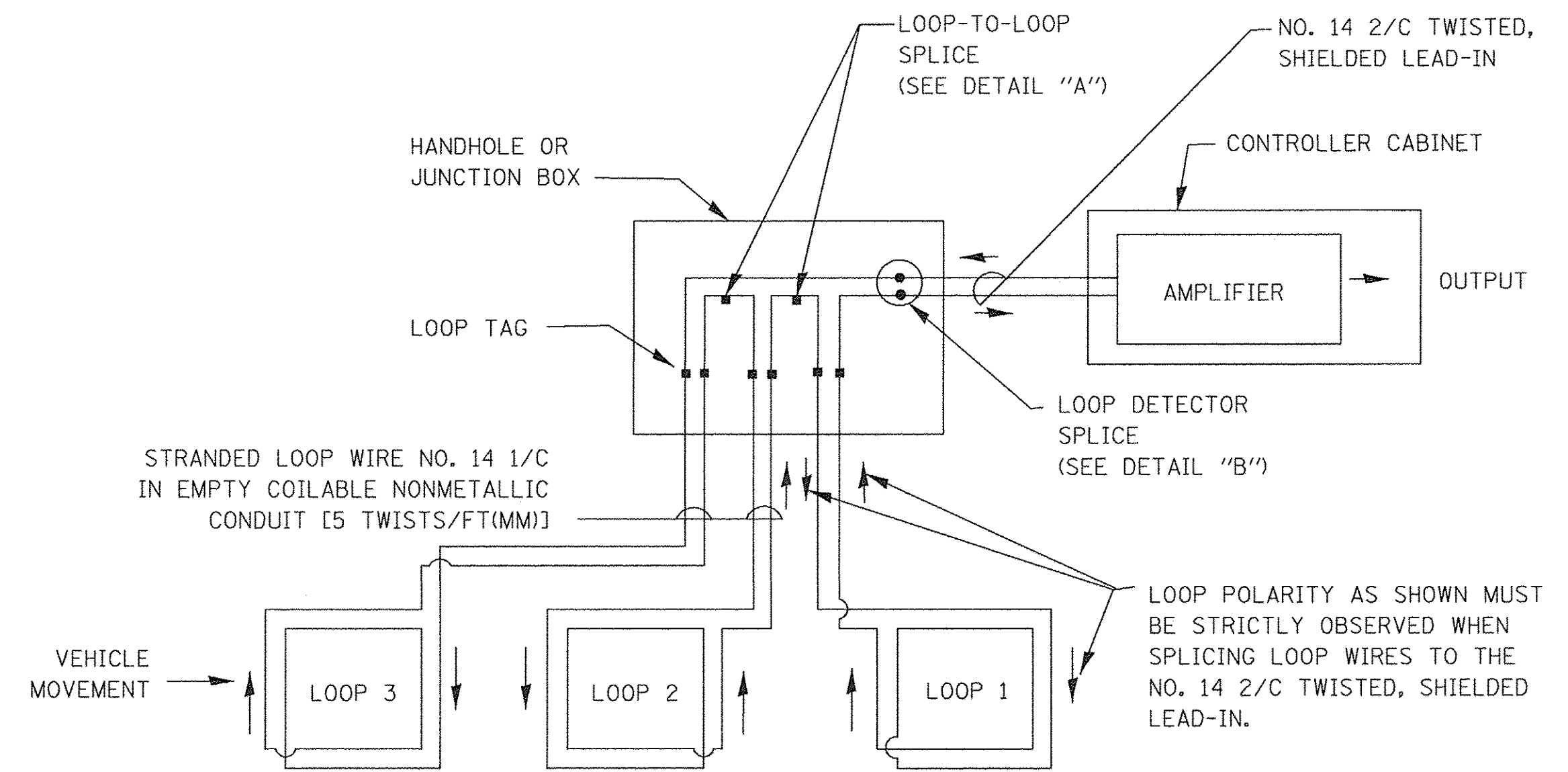
SCALE: SHEET NO. 17 OF 63 SHEETS STA. TO STA.

TRAFFIC SIGNAL LEGEND

ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED																		
CONTROLLER CABINET				EMERGENCY VEHICLE LIGHT DETECTOR				ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE																					
RAILROAD CONTROL CABINET				CONFIRMATION BEACON				COAXIAL CABLE																					
COMMUNICATIONS CABINET				HANDHOLE				VENDOR CABLE FOR CAMERA																					
MASTER CONTROLLER				HEAVY DUTY HANDHOLE				COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED																					
MASTER MASTER CONTROLLER				DOUBLE HANDHOLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F																					
UNINTERRUPTABLE POWER SUPPLY				JUNCTION BOX				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F																					
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT				UNDERGROUND CONDUIT, GALVANIZED STEEL (UC)				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM24F																					
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT				TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE				GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE																					
STEEL MAST ARM ASSEMBLY AND POLE				COMMON TRENCH				CONTROLLER CABINET AND FOUNDATION TO BE REMOVED																					
ALUMINUM MAST ARM ASSEMBLY AND POLE				COILABLE NONMETALLIC CONDUIT (EMPTY)				STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED																					
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE				SYSTEM ITEM				ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED																					
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA				INTERSECTION ITEM				STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED																					
SIGNAL POST				REMOVE ITEM				SIGNAL POST AND FOUNDATION TO BE REMOVED																					
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM				RELOCATE ITEM				INTERSECTION & SAMPLING (SYSTEM) DETECTOR																					
GUY WIRE				ABANDON ITEM				SAMPLING (SYSTEM) DETECTOR																					
SIGNAL HEAD				12" (300mm) TRAFFIC SIGNAL SECTION				QUEUE DETECTOR																					
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)				12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE				PREFORMED QUEUE DETECTOR																					
SIGNAL HEAD WITH BACKPLATE				SIGNAL FACE				PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR																					
SIGNAL HEAD OPTICALLY PROGRAMMED				SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD				PREFORMED SAMPLING (SYSTEM) DETECTOR																					
FLASHER INSTALLATION (S DENOTES SOLAR POWER)				"RB" INDICATES REFLECTIVE BACKPLATE				<h2 style="margin: 0;">RAILROAD SYMBOLS</h2> <table style="width: 100%; border: none;"> <thead> <tr> <th style="width: 50%;"></th> <th style="width: 25%;">EXISTING</th> <th style="width: 25%;">PROPOSED</th> </tr> </thead> <tbody> <tr> <td>RAILROAD CONTROL CABINET</td> <td></td> <td></td> </tr> <tr> <td>RAILROAD CANTILEVER MAST ARM</td> <td></td> <td></td> </tr> <tr> <td>FLASHING SIGNAL</td> <td></td> <td></td> </tr> <tr> <td>CROSSING GATE</td> <td></td> <td></td> </tr> <tr> <td>CROSSBUCK</td> <td></td> <td></td> </tr> </tbody> </table>					EXISTING	PROPOSED	RAILROAD CONTROL CABINET			RAILROAD CANTILEVER MAST ARM			FLASHING SIGNAL			CROSSING GATE			CROSSBUCK		
	EXISTING	PROPOSED																											
RAILROAD CONTROL CABINET																													
RAILROAD CANTILEVER MAST ARM																													
FLASHING SIGNAL																													
CROSSING GATE																													
CROSSBUCK																													
PEDESTRIAN SIGNAL HEAD				12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL																									
PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED																									
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID																									
ILLUMINATED SIGN "NO LEFT TURN"				PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER																									
ILLUMINATED SIGN "NO RIGHT TURN"				RADIO INTERCONNECT																									
DETECTOR LOOP, TYPE I				RADIO REPEATER																									
PREFORMED DETECTOR LOOP				DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED																									
MICROWAVE VEHICLE SENSOR				GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)																									
VIDEO DETECTION CAMERA																													
VIDEO DETECTION ZONE																													
PAN, TILT, ZOOM CAMERA																													
WIRELESS DETECTOR SENSOR																													
WIRELESS ACCESS POINT																													

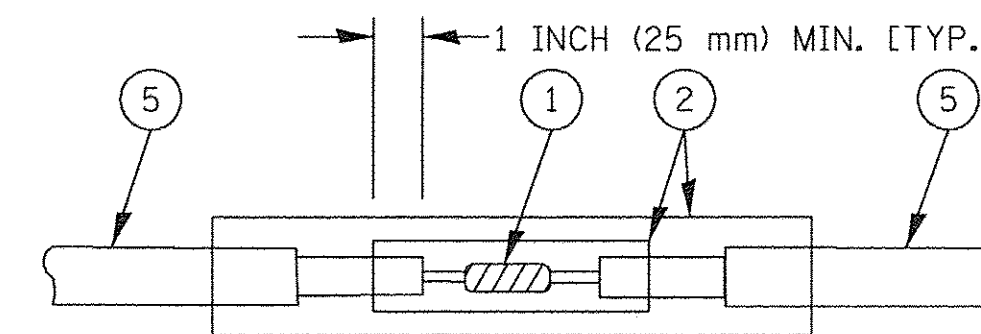
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

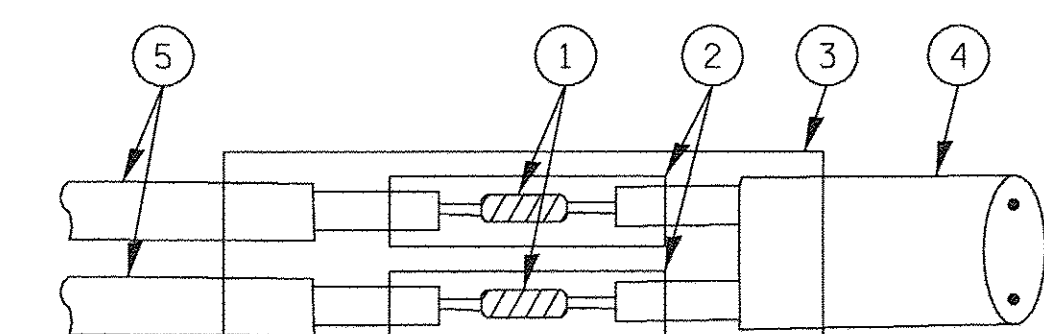


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.



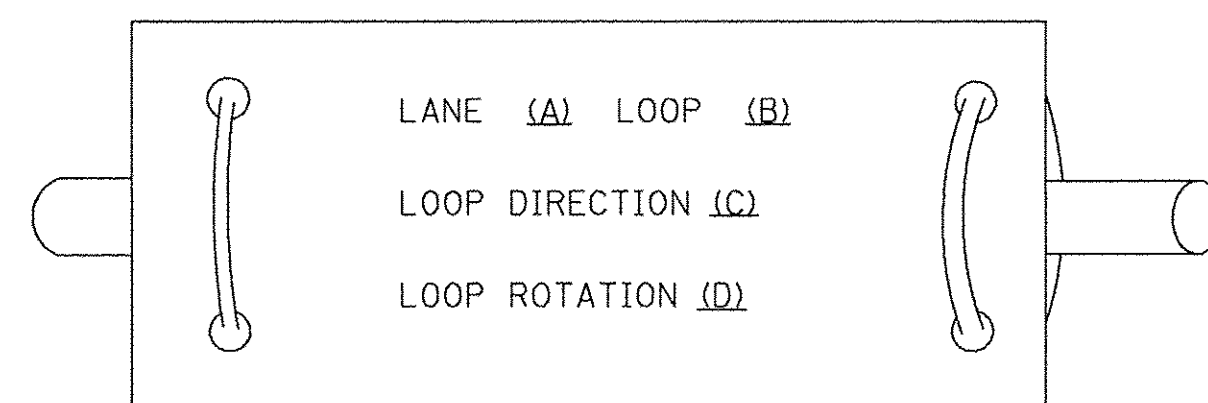
DETAIL "A"
LOOP-TO-LOOP SPLICE



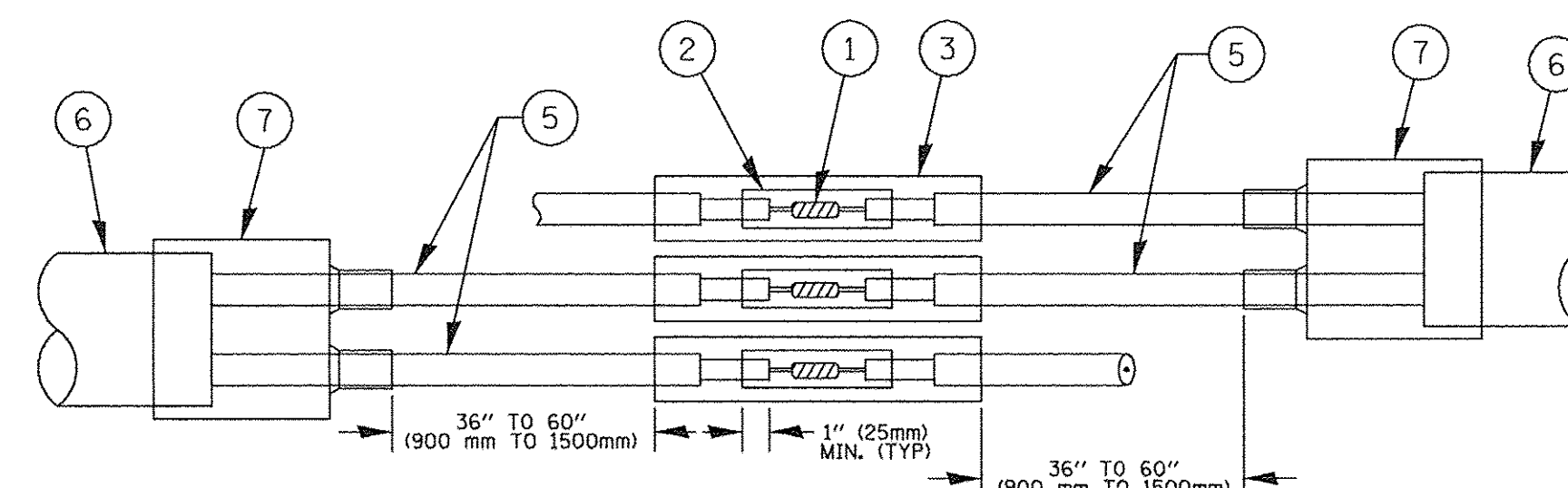
DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP

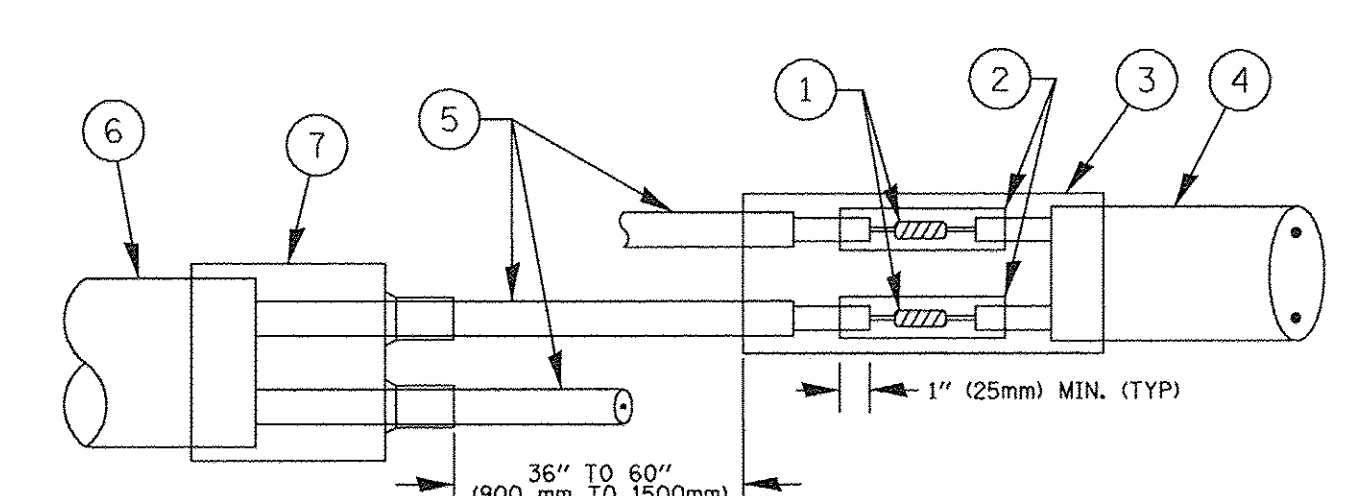
LOOP LEAD-IN CABLE TAG



- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PREFORMED LOOP

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PREFORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

FILE NAME = 13375_02-SGNL-DTLS-03 - P02

USER NAME = foatemj	DESIGNED — DAD	REVISED — DAG 1-1-14
	CHECKED — BCK	REVISED —
PLOT SCALE = 5/8" @ 1" = 1 in.	DRAWN — DAD	REVISED —
PLOT DATE = 1/13/2014	CHECKED — 10-28-09	REVISED —

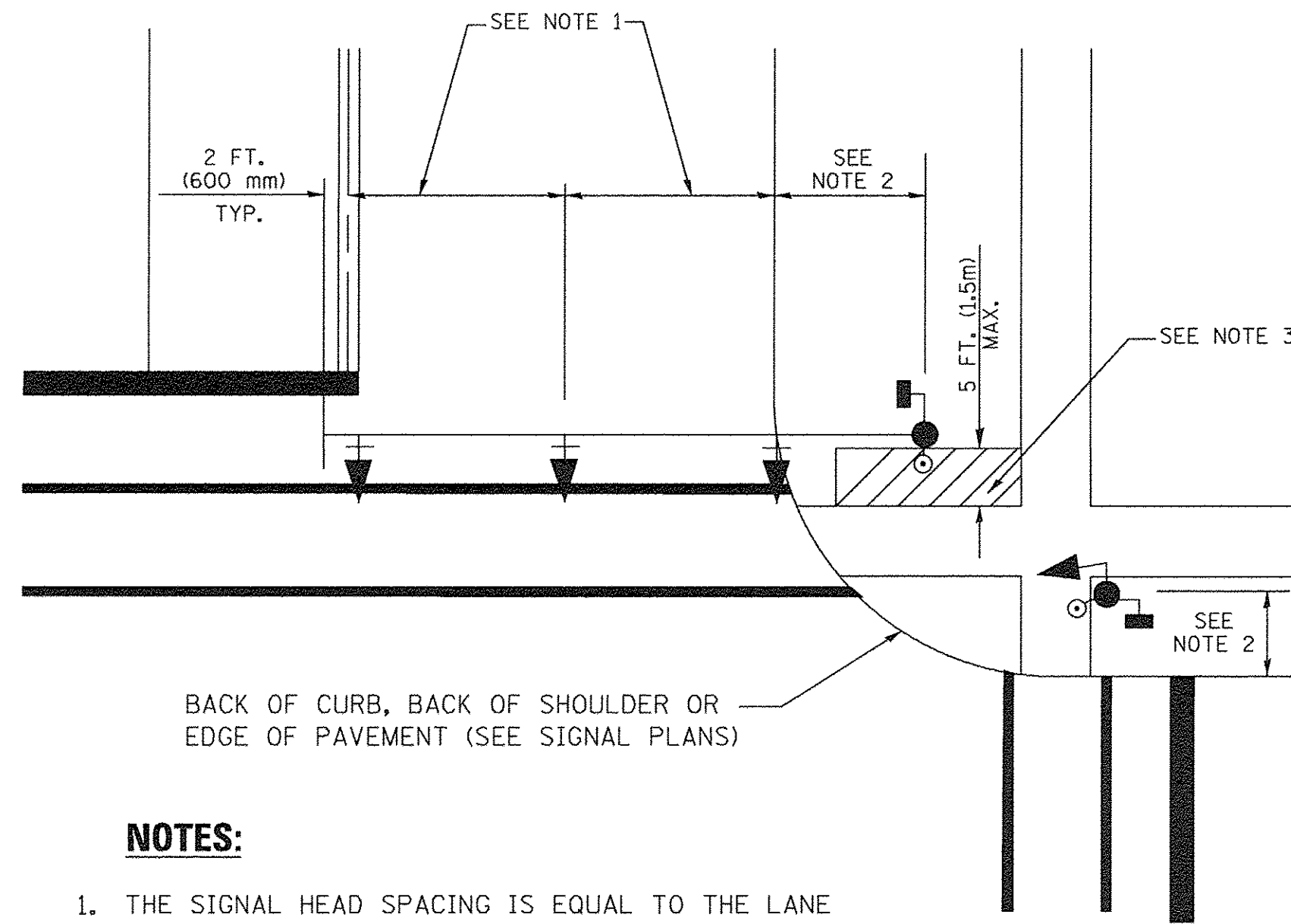
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE SHEET NO. 2 OF 7 SHEETS STA. TO STA.

F.A.P. RTE. 353	SECTION 13-00063-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 19
TS-05		CONTRACT NO. 61C11		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(216)				

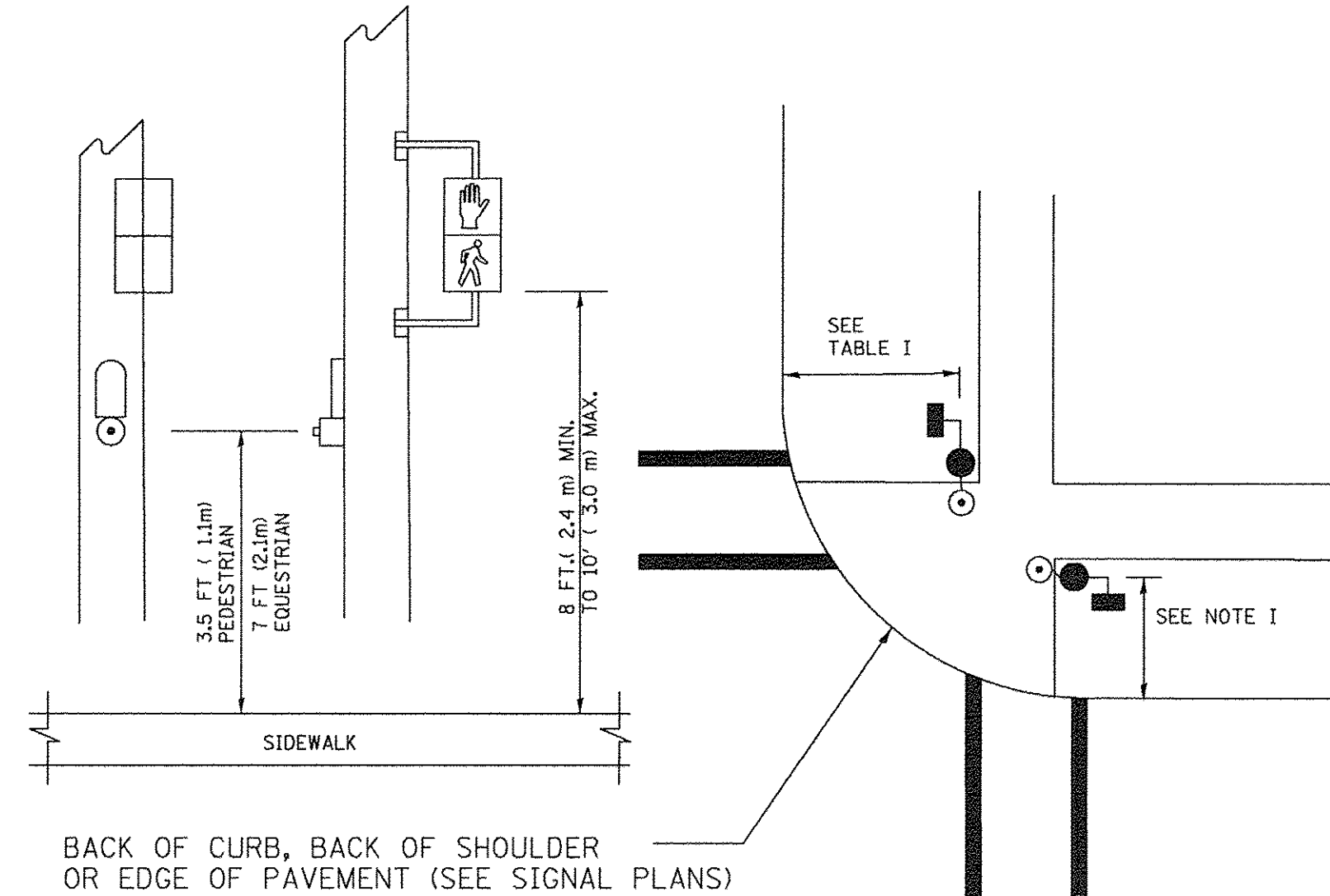
**TRAFFIC SIGNAL MAST ARM AND SIGNAL POST
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR
FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN
WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.**



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

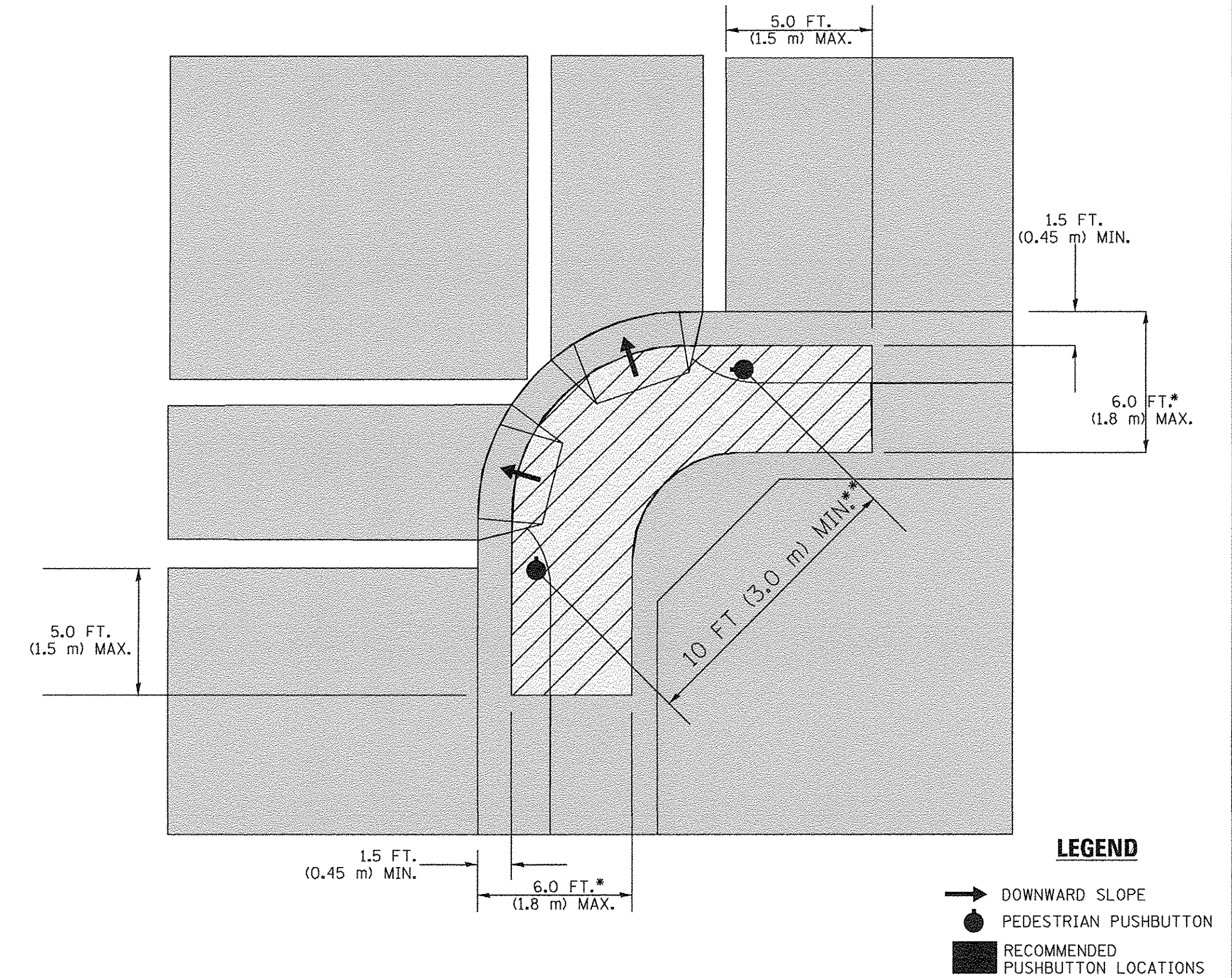
**PEDESTRIAN SIGNAL POST
AND
PEDESTRIAN PUSH BUTTON POST**



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



LEGEND

- DOWNWARD SLOPE
- PEDESTRIAN PUSHBUTTON
- RECOMMENDED PUSHBUTTON LOCATIONS

- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD AFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

FILE NAME = 13375_02-SGNL-DTLS-03 - P03

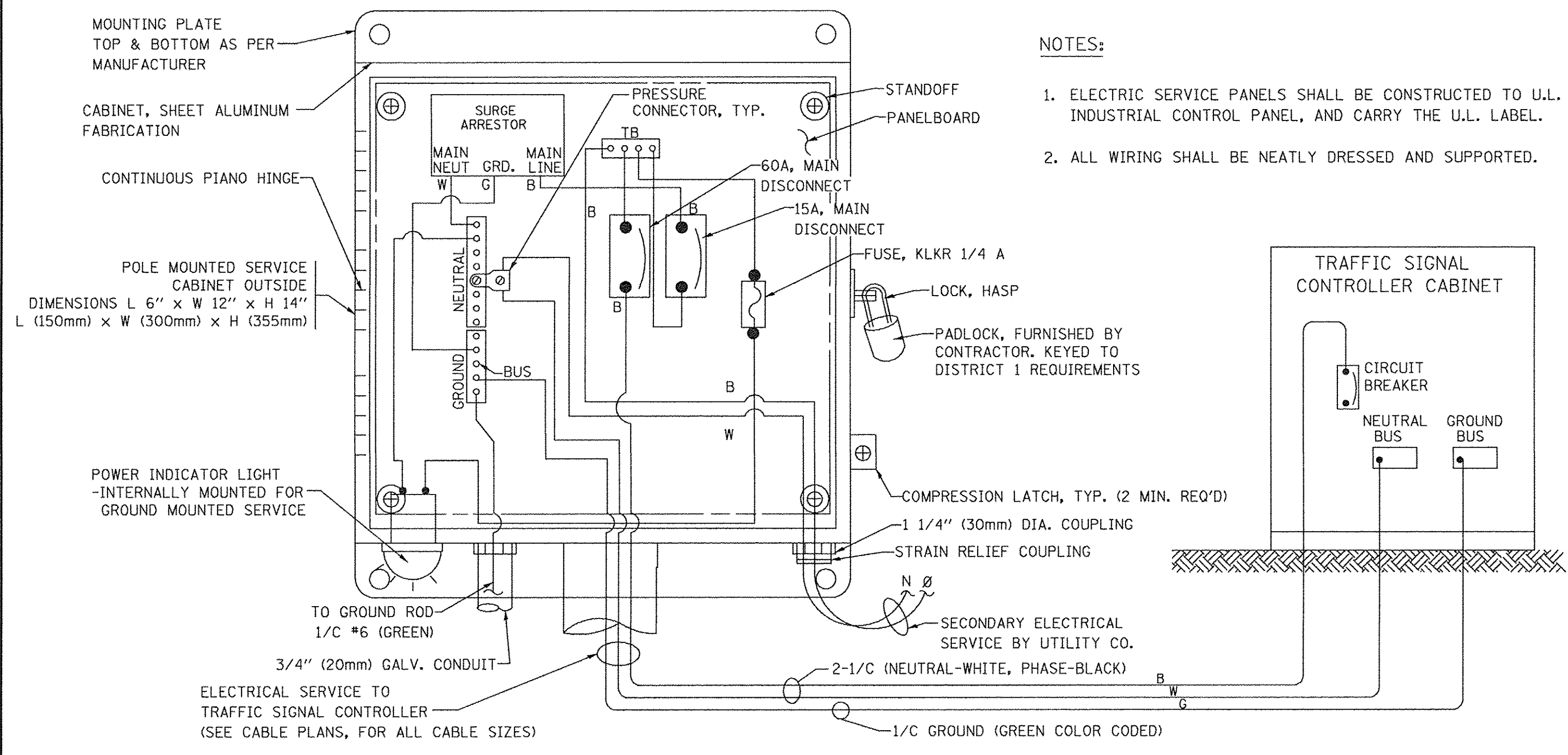
USER NAME = footejm	DESIGNED — DAD	REVISED — DAG 1-1-14
	CHECKED — BCK	REVISED —
PLOT SCALE = 50.0000' / 1"	DRAWN — DAD	REVISED —
PLOT DATE = 1/13/2014	CHECKED — 10-28-09	REVISED —

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

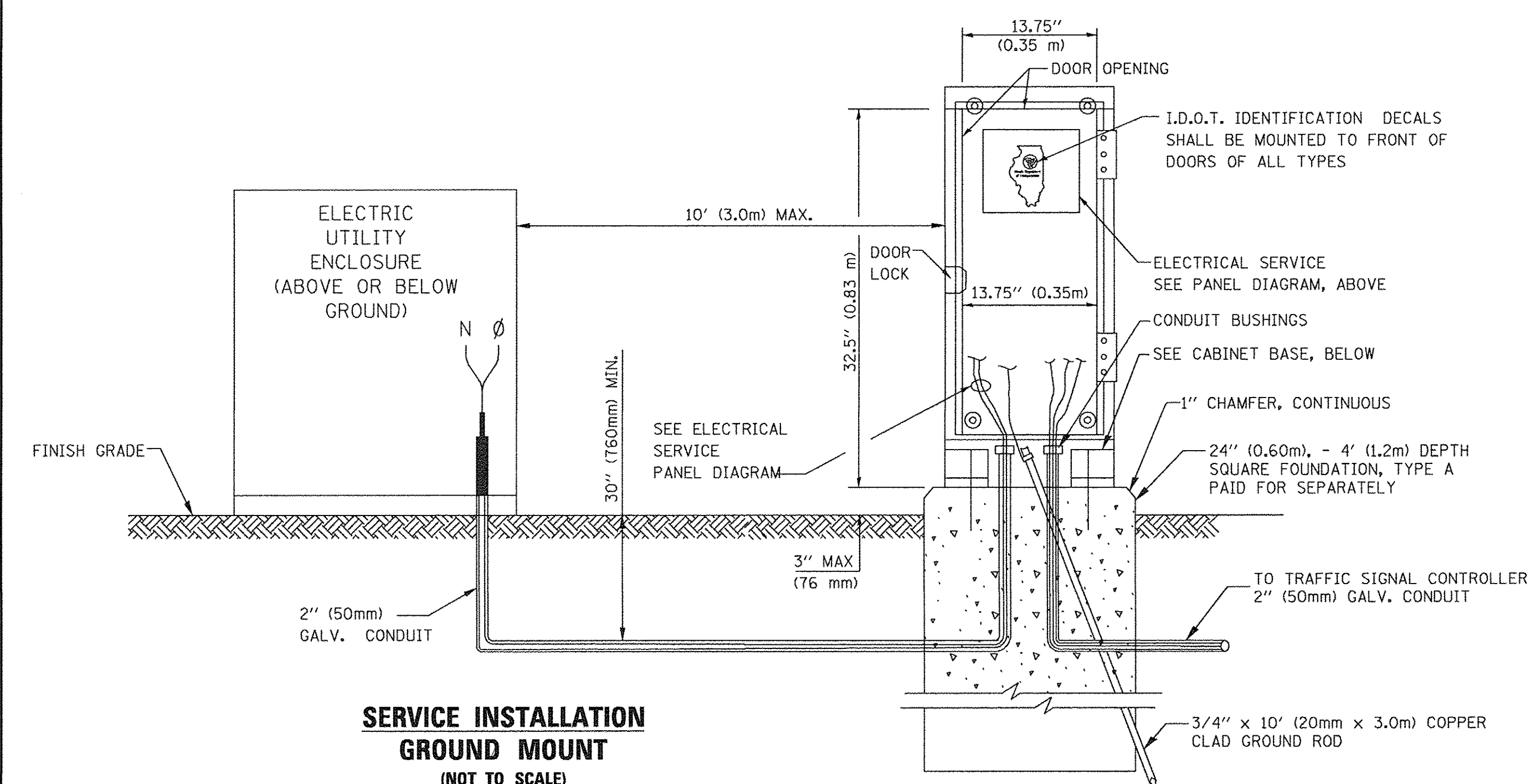
**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET NO. 3 OF 7 SHEETS STA. TO STA.

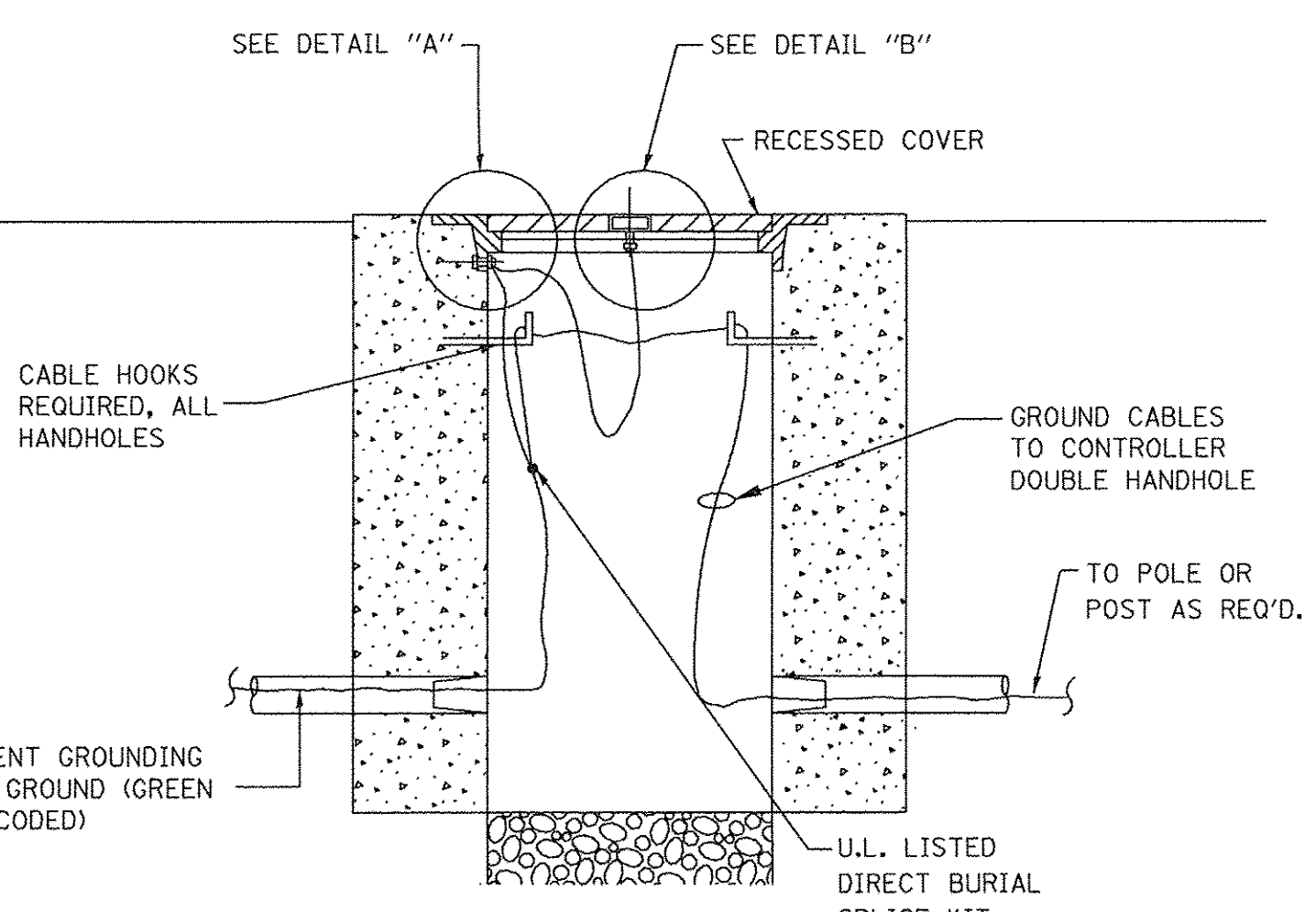
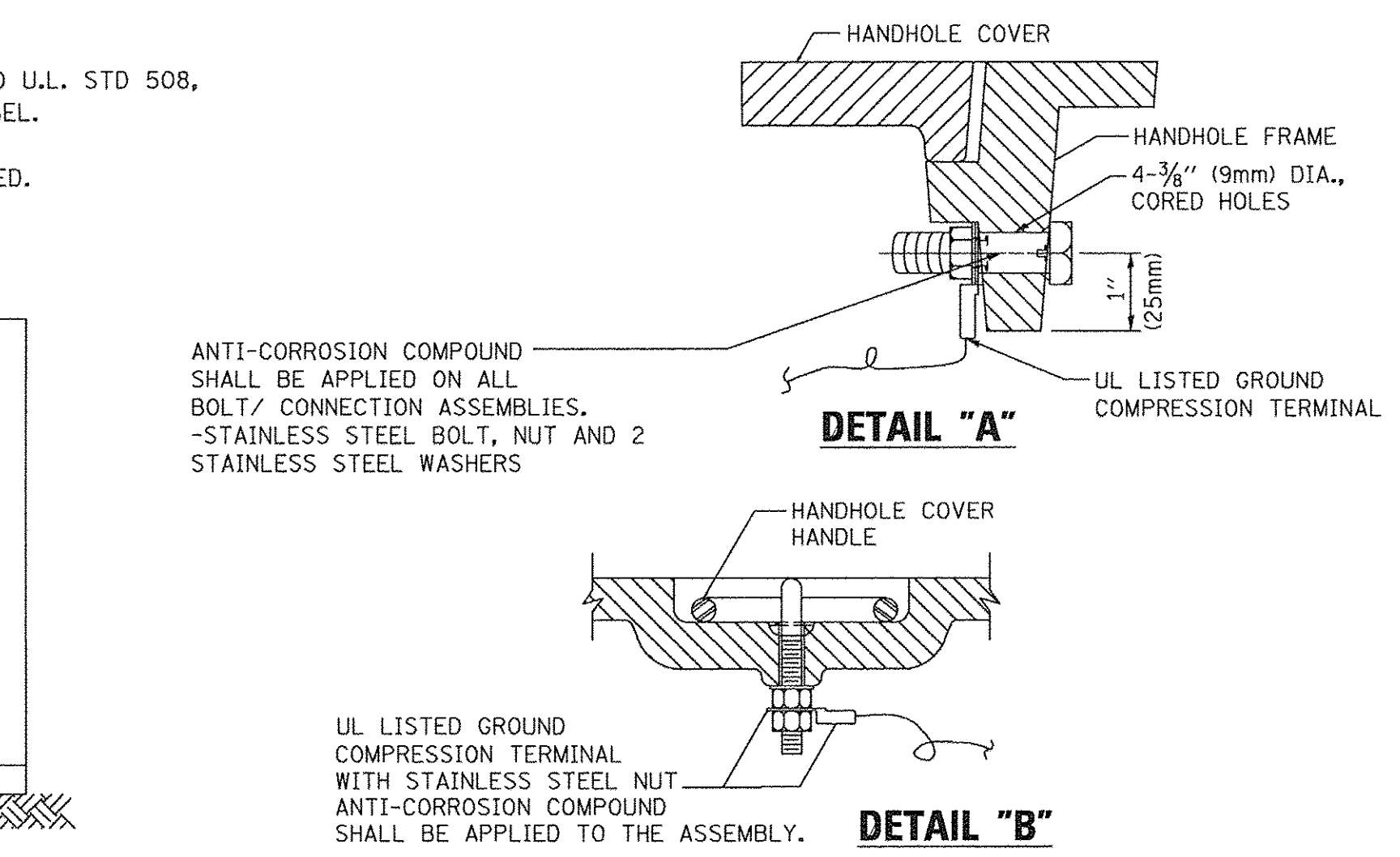
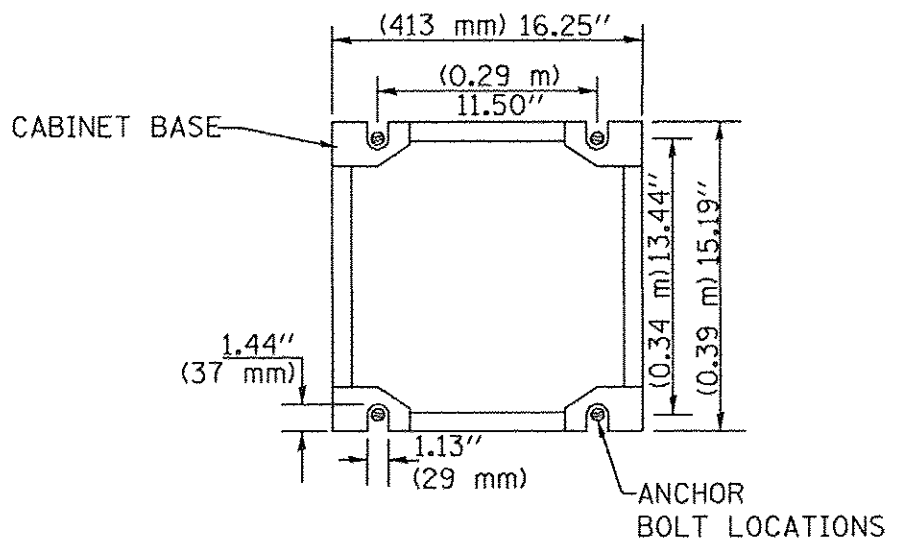
F.A.P. RTE. 353	SECTION 13-00063-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 20
TS-05		CONTRACT NO. 61C11		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(216)				



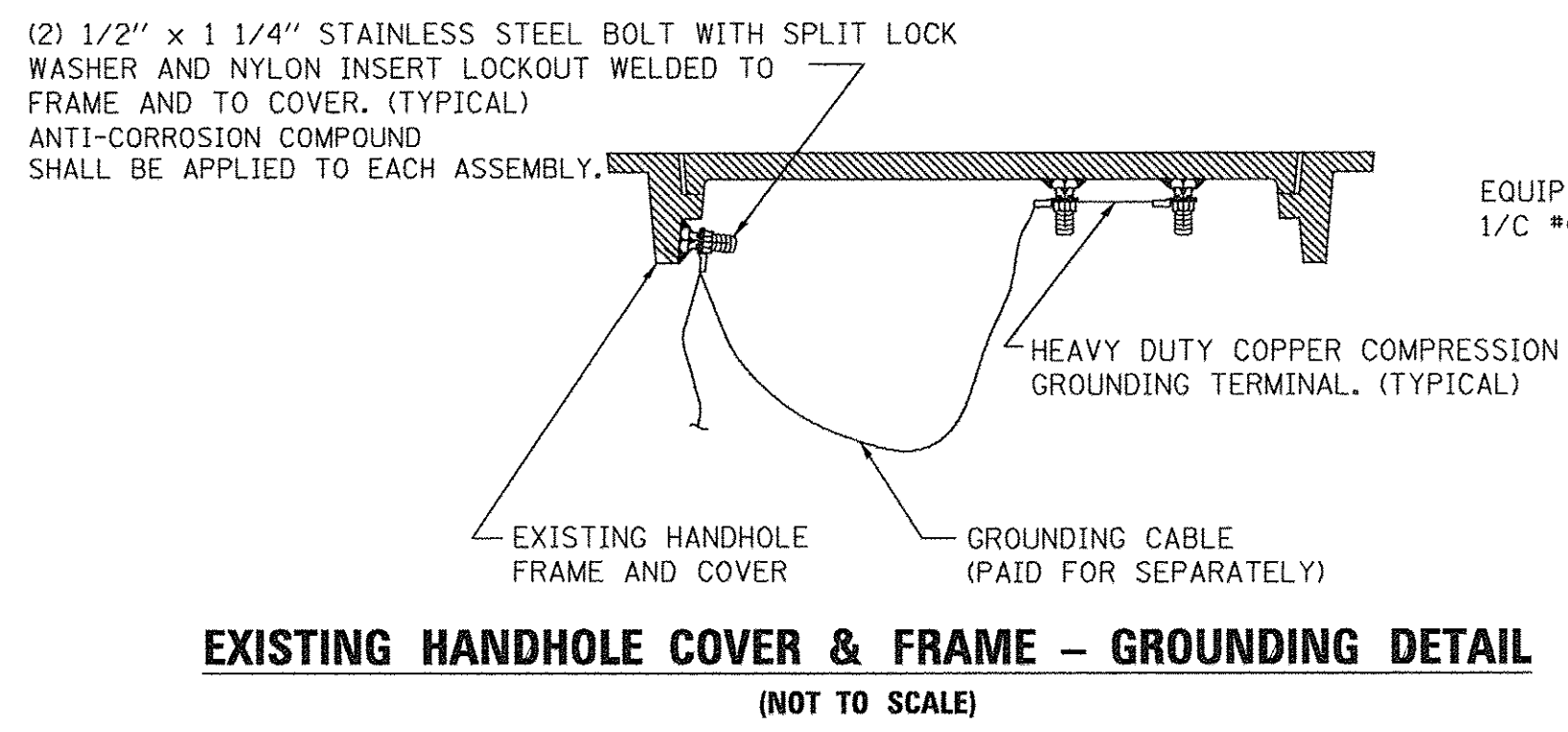
ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE) SERVICE INSTALLATION POLE MOUNT (SHOWN) (NOT TO SCALE)



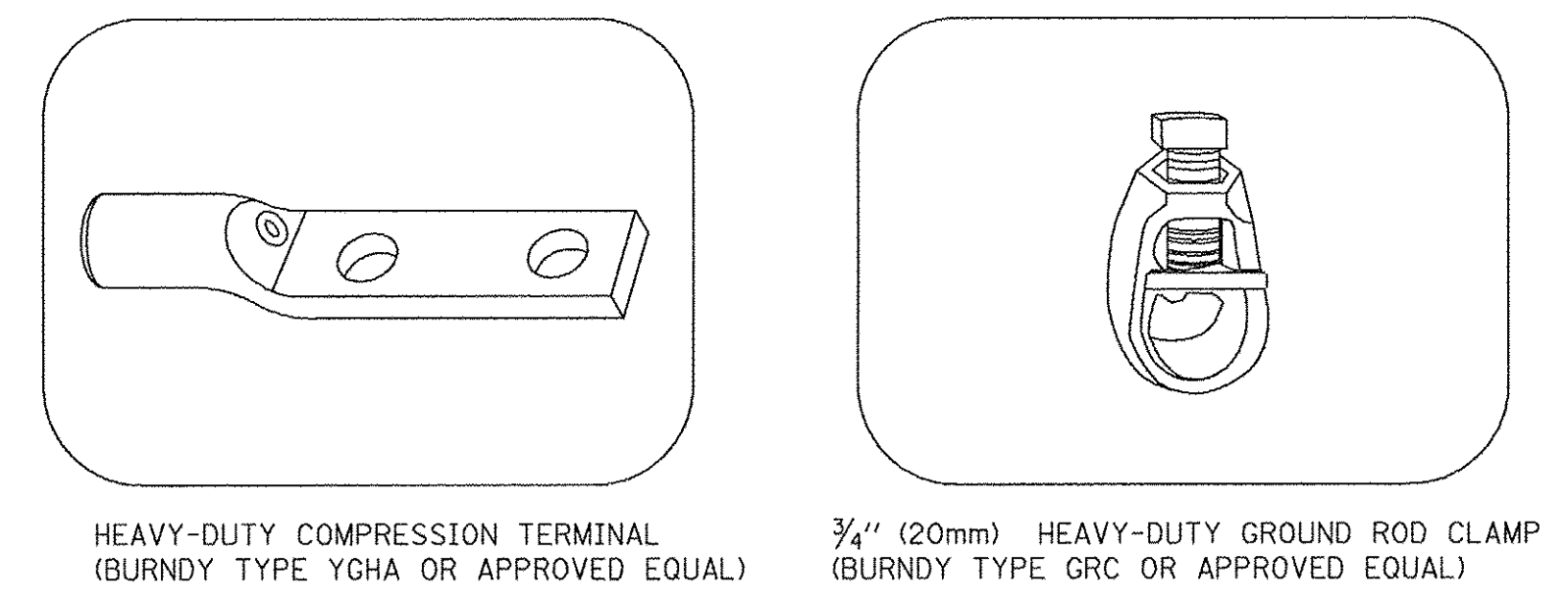
CABINET - BASE BOLT PATTERN (NOT TO SCALE)



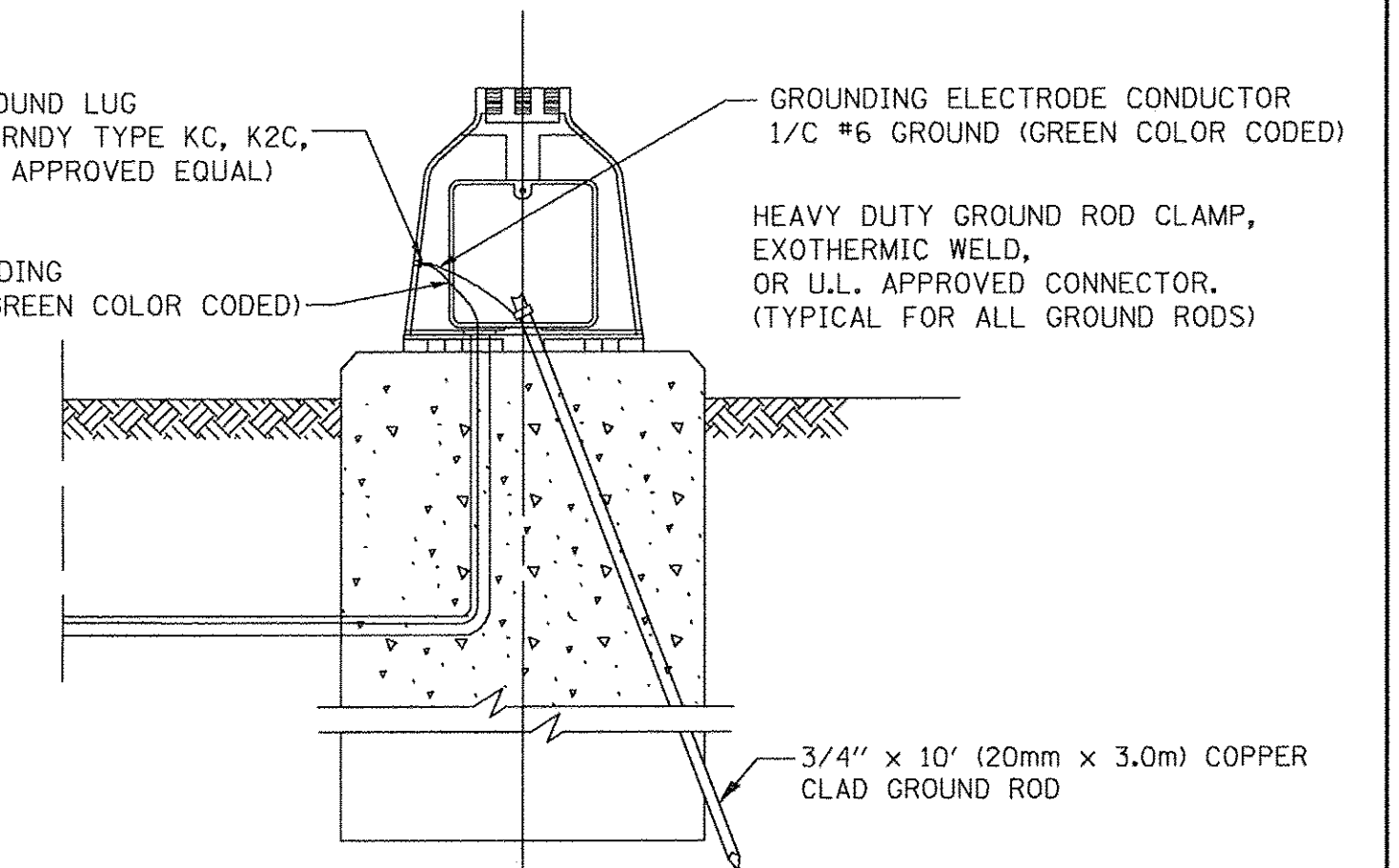
HANDHOLE COVER & FRAME - GROUNDING DETAIL (NOT TO SCALE)



EXISTING HANDHOLE COVER & FRAME - GROUNDING DETAIL (NOT TO SCALE)



- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
 - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES
 - 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES
 - 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES.
 - 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



MAST ARM POLE / POST-GROUNDING DETAIL (NOT TO SCALE)

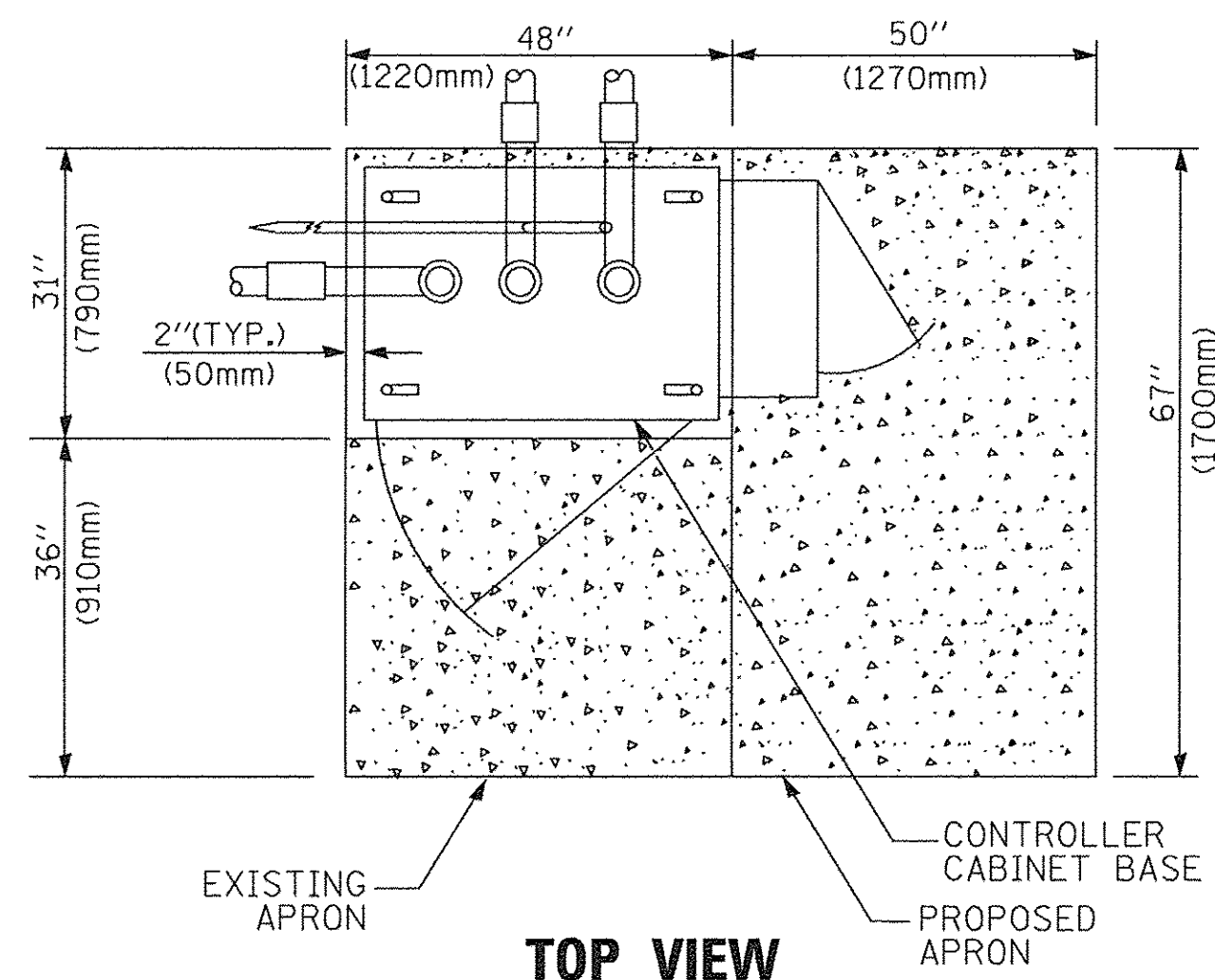
FILE NAME = 13375_02-SGNL_DTLS-03 - P04

USER NAME = Footemj	DESIGNED - DAD	REVISED - DAG 1-1-14
PLOT SCALE = 50.0000 / in.	CHECKED - BCK	REVISED -
PLOT DATE = 1/13/2014	DRAWN - DAD	REVISED -
	CHECKED - 10-28-09	REVISED -

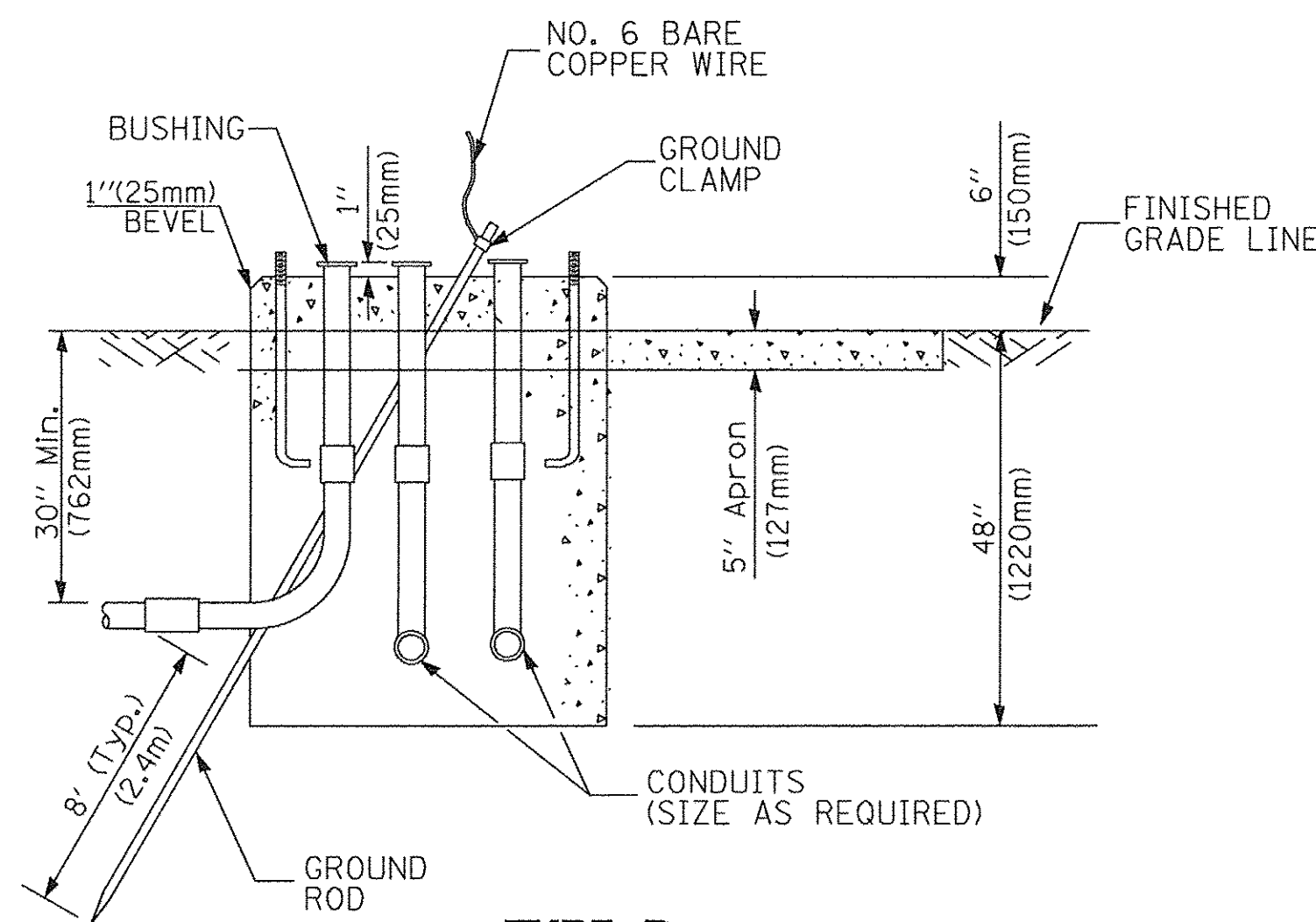
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE	
STANDARD TRAFFIC SIGNAL DESIGN DETAILS	
SCALE: NONE	SHEET NO. 4 OF 7 SHEETS STA. TO STA.

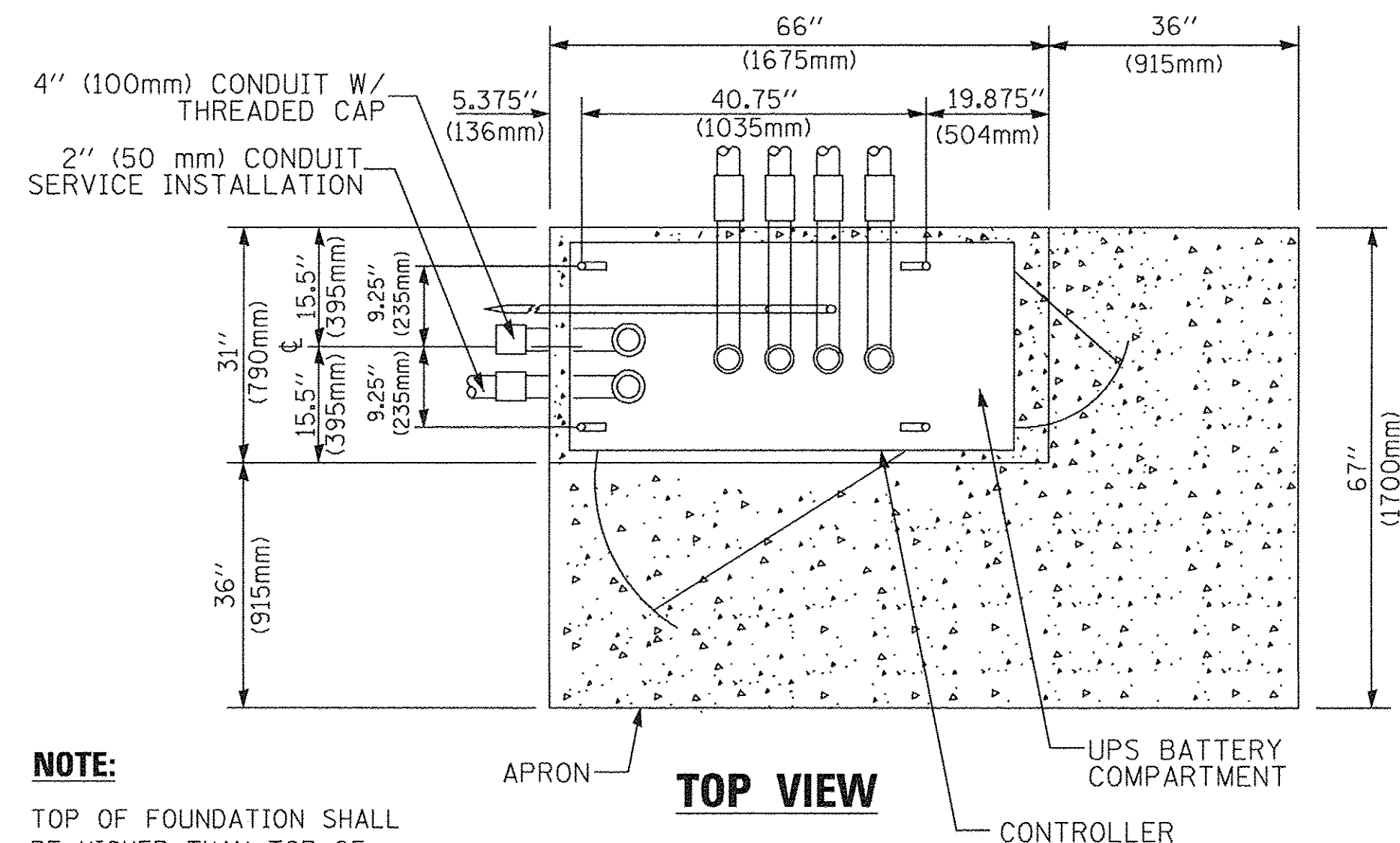
F.A.P. RTE. 353	SECTION 13-00083-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 21
TS-05		CONTRACT NO. 61C11		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(216)		



TOP VIEW



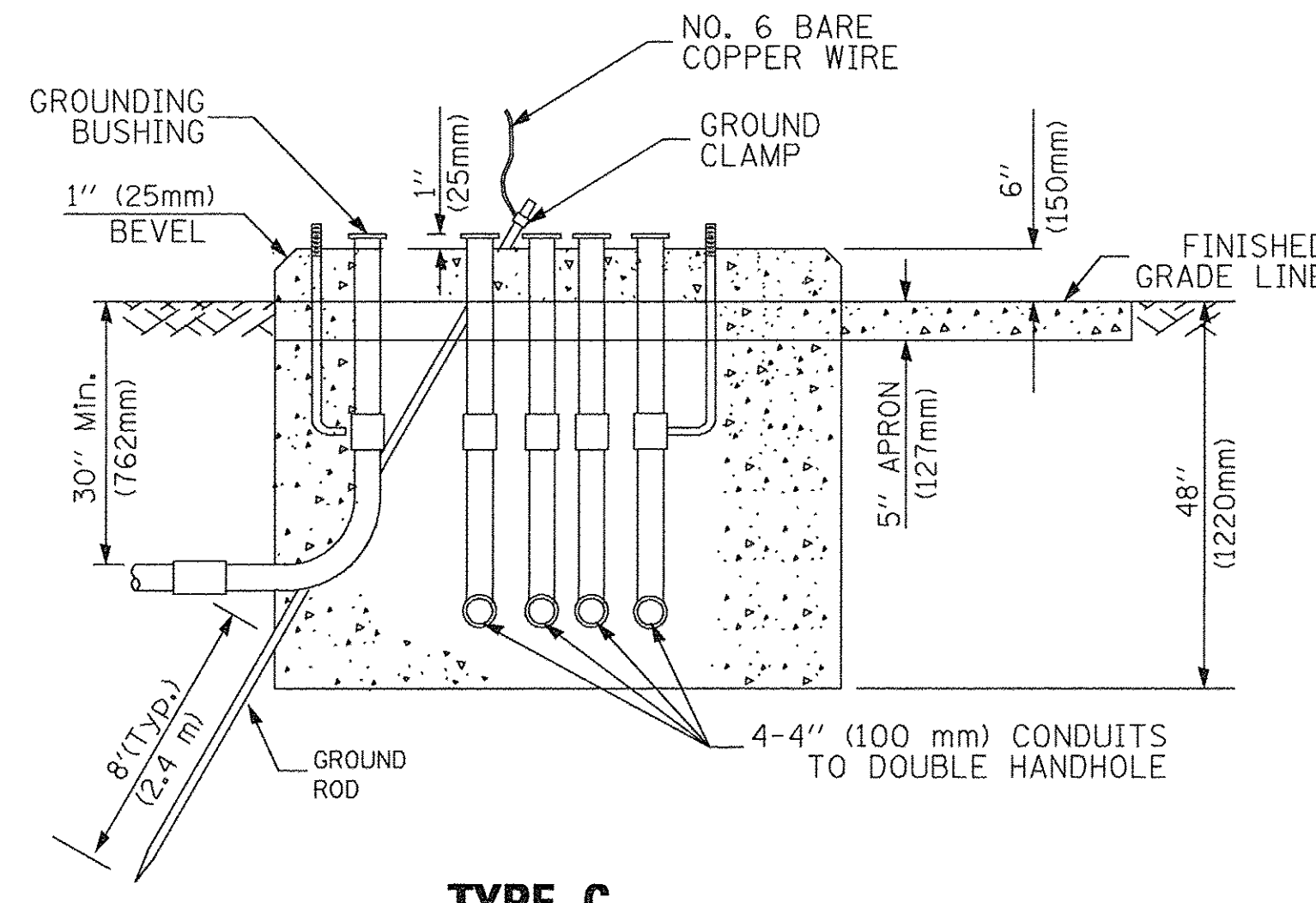
**TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET**



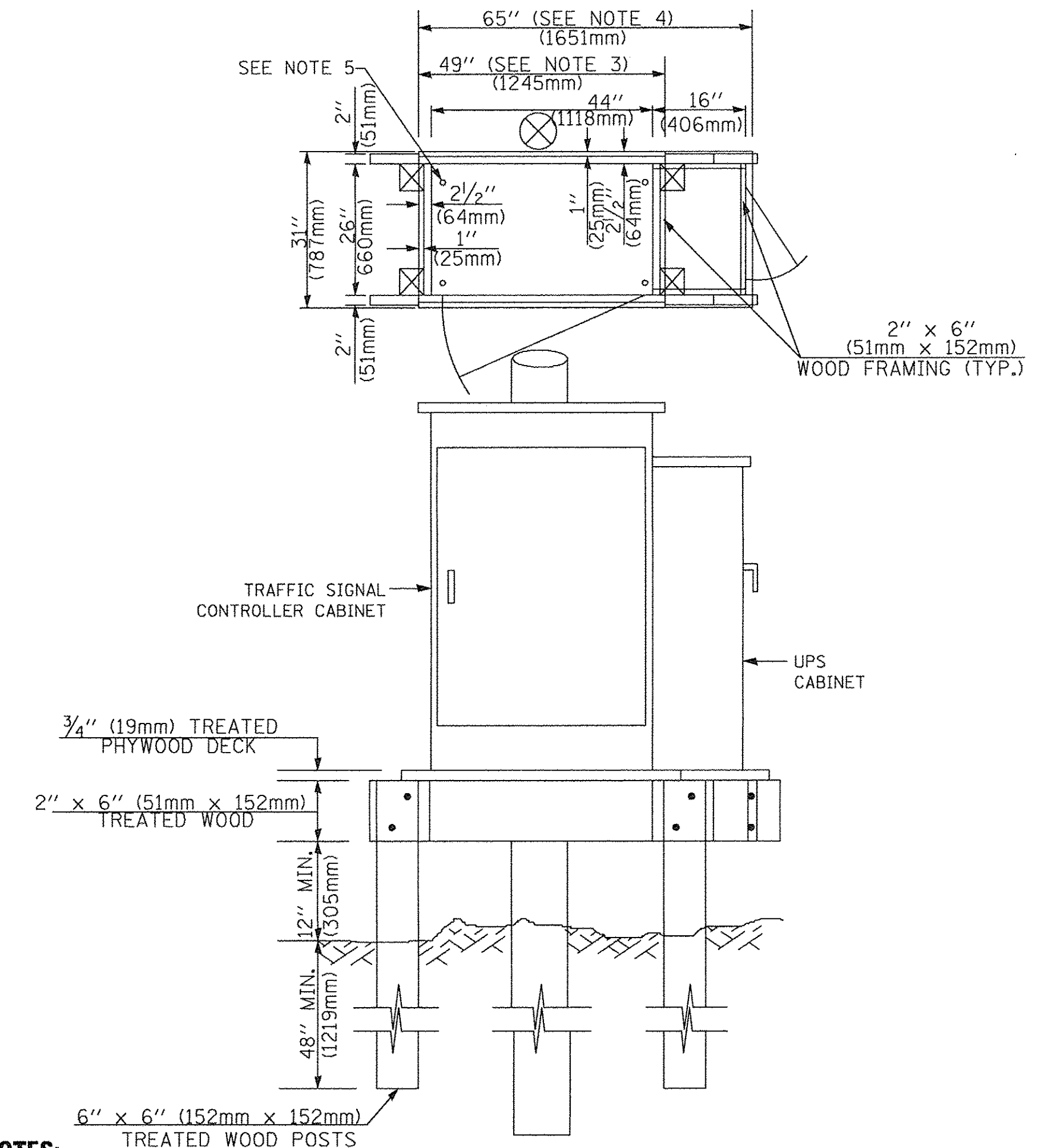
TOP VIEW

NOTE:

TOP OF FOUNDATION SHALL BE HIGHER THAN TOP OF DOUBLE HANDHOLE



**TYPE C
FOR GROUND MOUNTED
SUPER P (TYPE IV) AND SUPER R (TYPE V)
CONTROLLER CABINETS**



NOTES:

- BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
- BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
- PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
- PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
- DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
- FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

**TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

Mast Arm Length	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and up to 56' (16.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

- These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
- Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
- Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
- For mast arm assemblies with dual arms refer to state standard 878001.

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

FILE NAME = 13375_02-SGNL_DTLS-03 - P05

USER NAME = footemj
PLOT SCALE = 50,0000' / 1" =
PLOT DATE = 1/13/2014

DESIGNED -- DAG
CHECKED -- BCK
DRAWN -- DAD
CHECKED -- 10-28-09

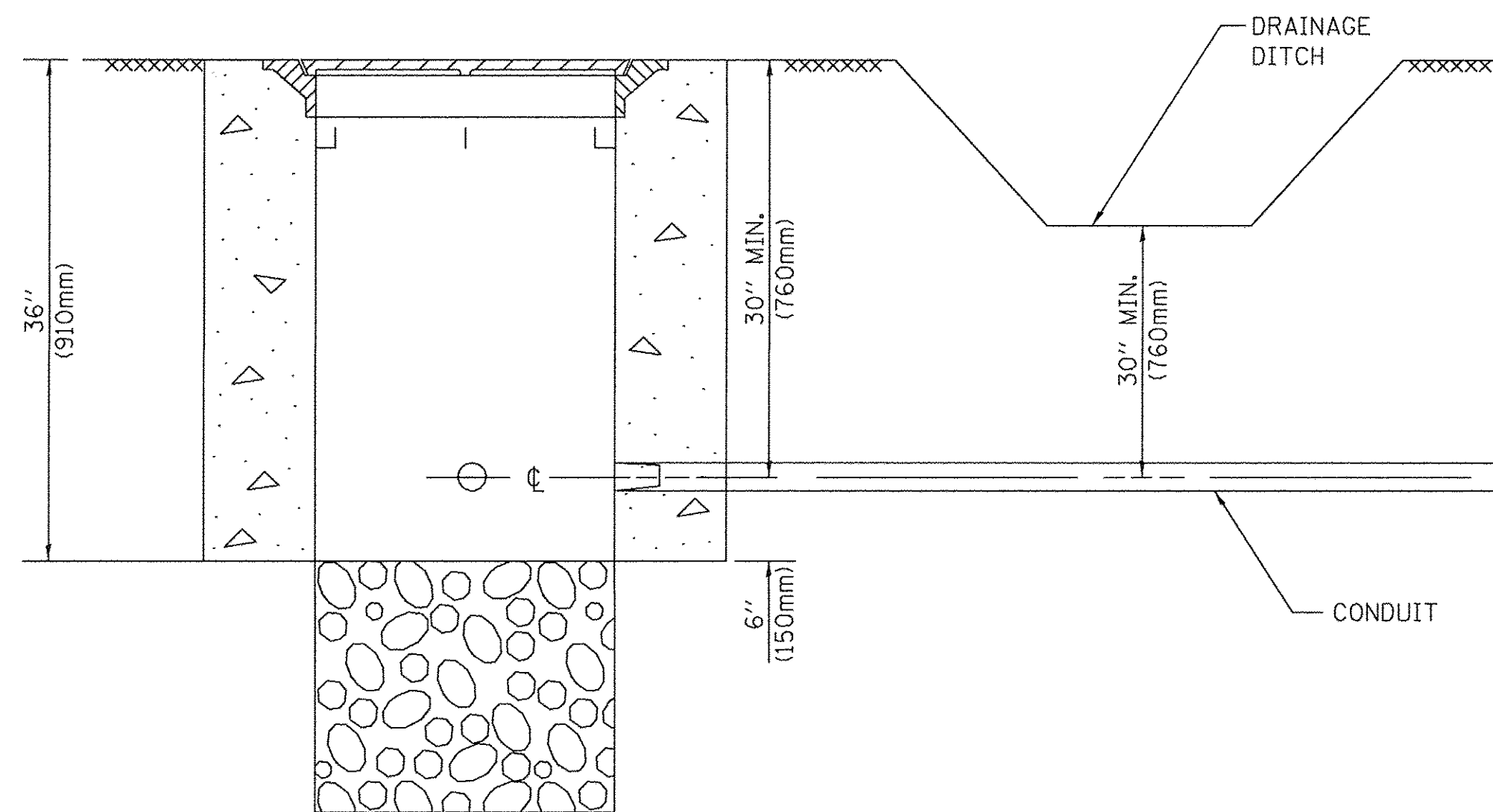
REVISED -- DAG 1-1-14
REVISED --
REVISED --
REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	13-00063-00-CH	COOK	63	22
TS-05			CONTRACT NO. 61C11	
FED. ROAD DIST. NO. 1			ILLINOIS FED. AID PROJECT M-4003(216)	

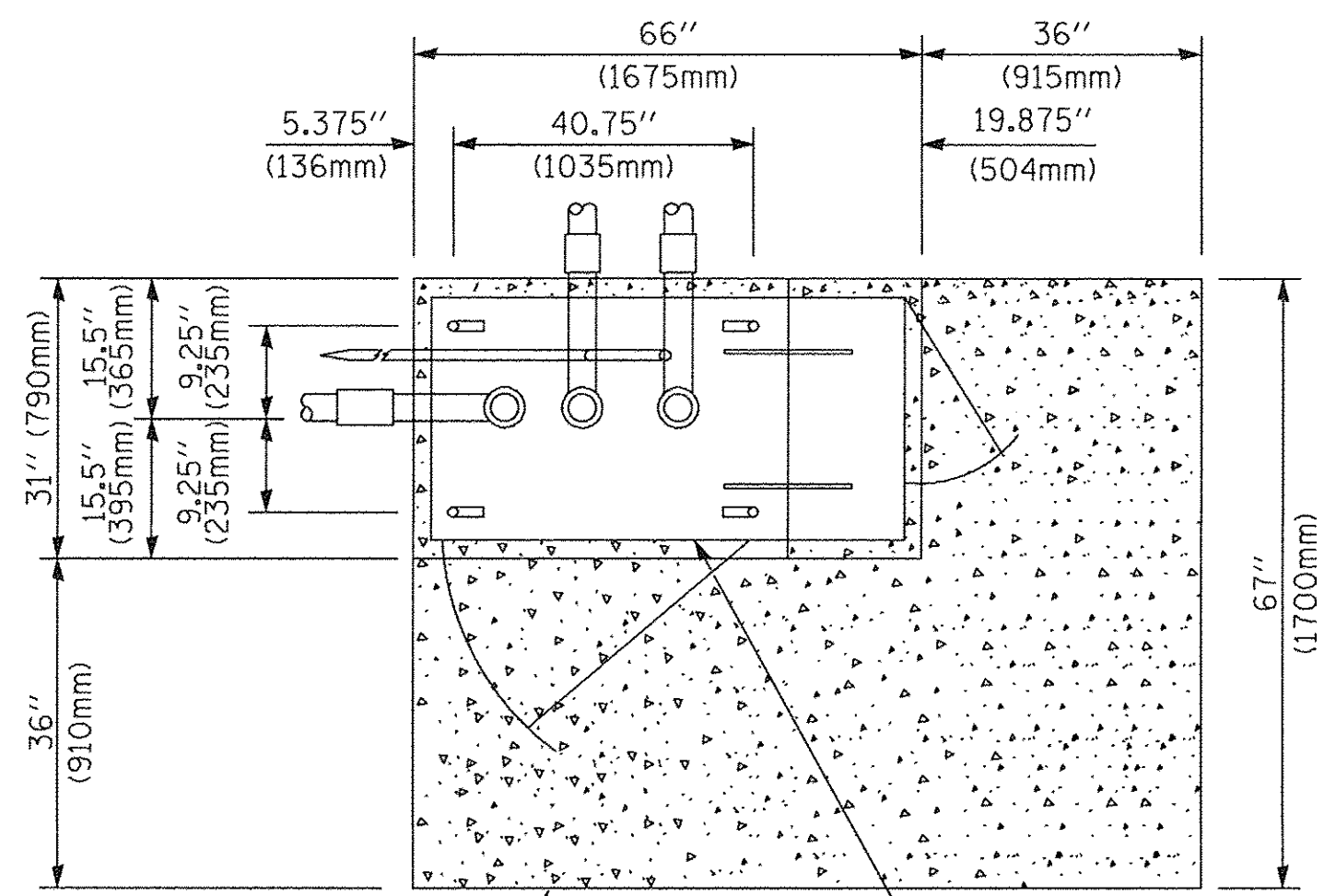
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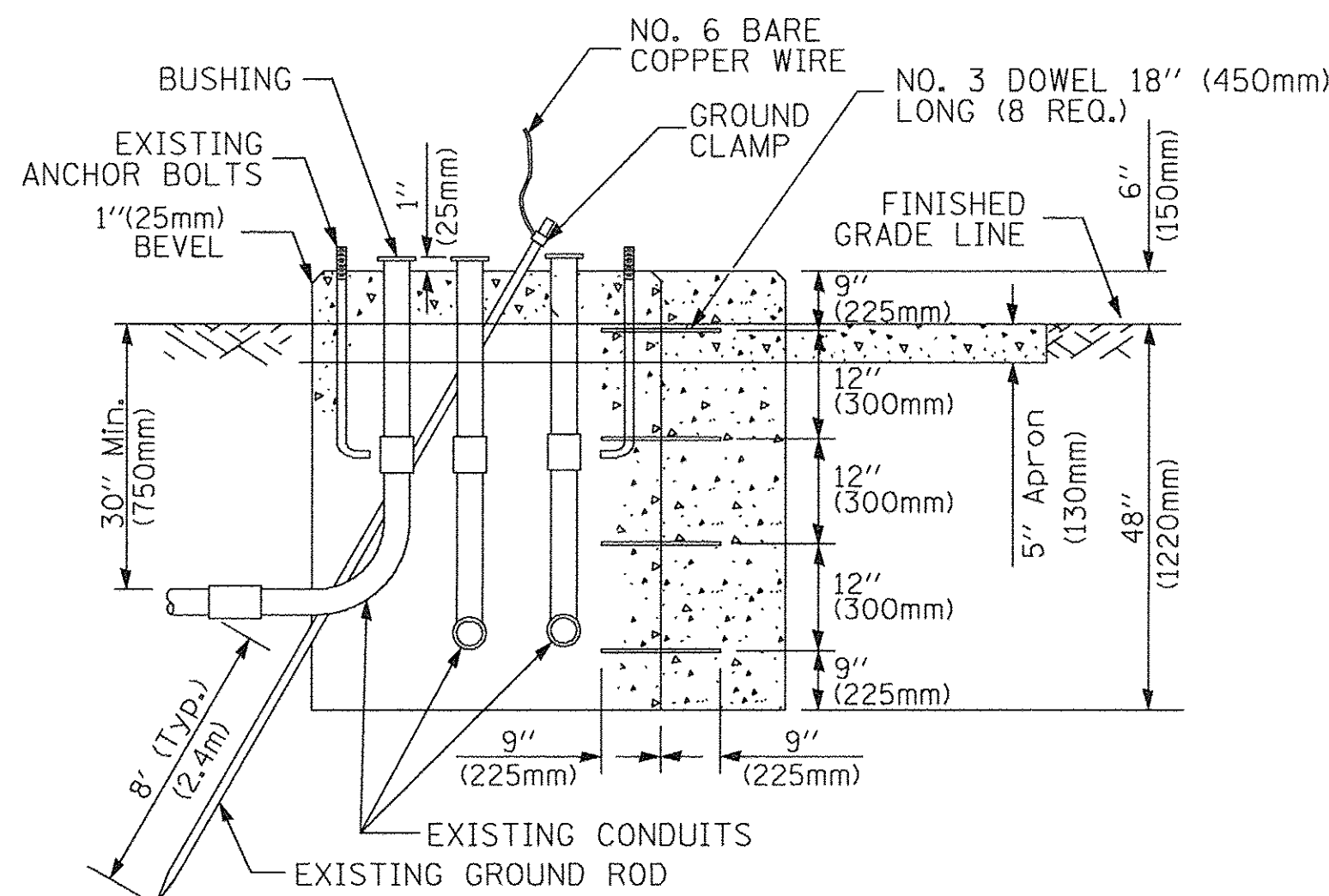
NOTES:

1. CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

HANDHOLE WITH MINIMUM CONDUIT DEPTH
(NOT TO SCALE)



TOP VIEW
(NOT TO SCALE)

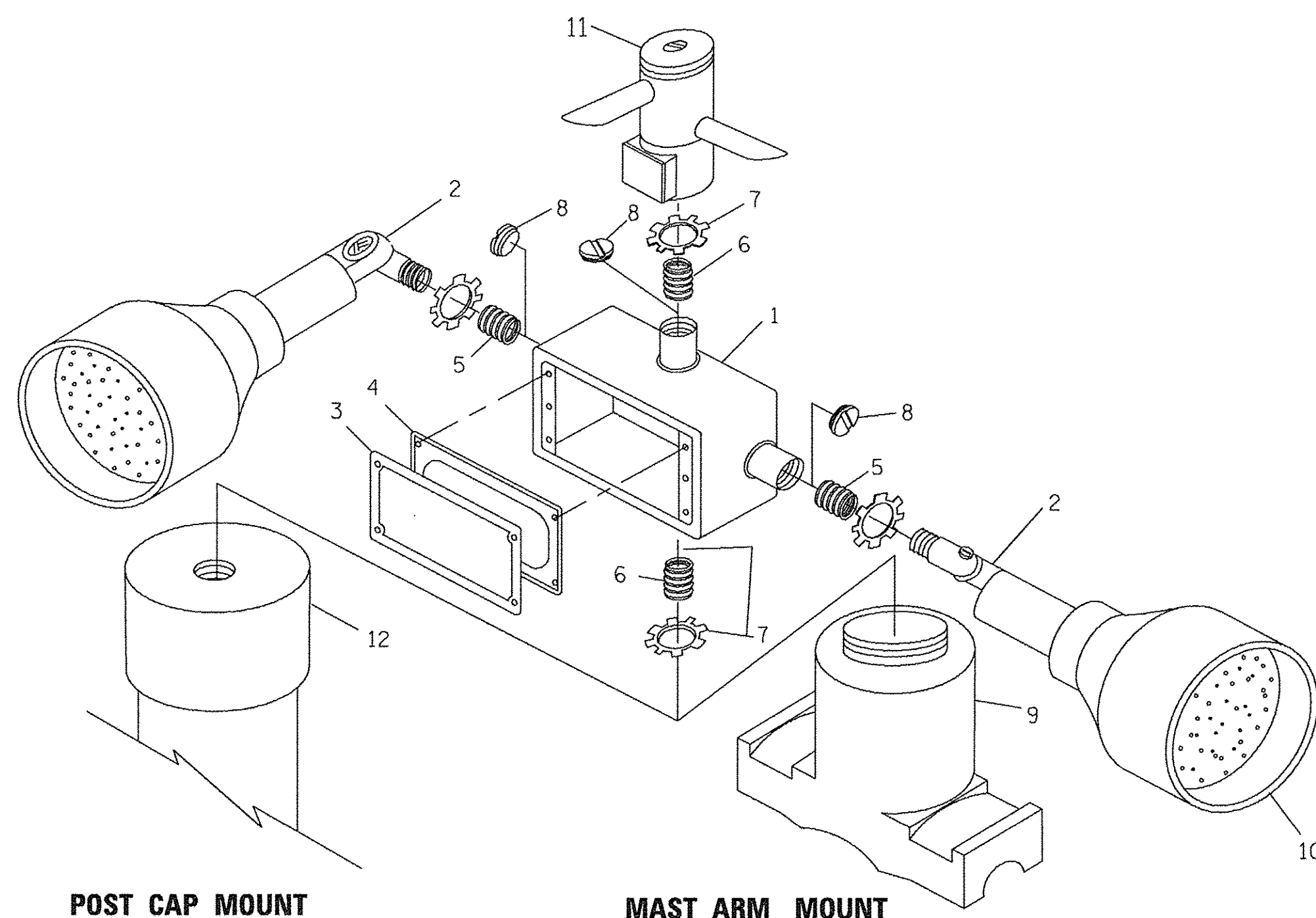


MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION
(NOT TO SCALE)

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4" (19 mm) CLOSE NIPPLE
7	3/4" (19 mm) LOCKNUT
8	3/4" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

NOTES:

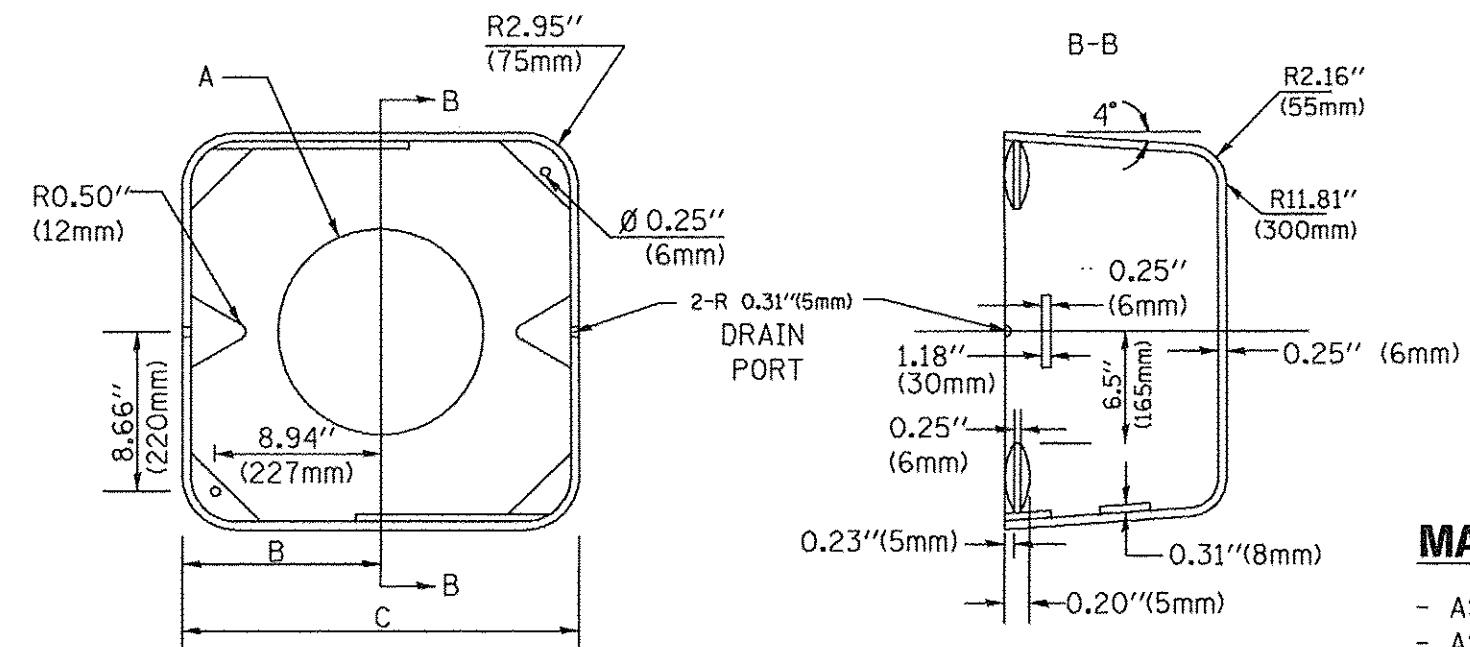
1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



POST CAP MOUNT

MAST ARM MOUNT

EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL



MATERIAL:
- ASTM A36 STEEL
- ASTM A-123 HOT DIPPED GALVANIZED

A	B	C	HEIGHT	WEIGHT
VARIES	9.5"(241mm)	19"(483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIES	10.75"(273mm)	21.5"(546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIES	13.0"(330mm)	26"(660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIES	18.5"(470mm)	37"(940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

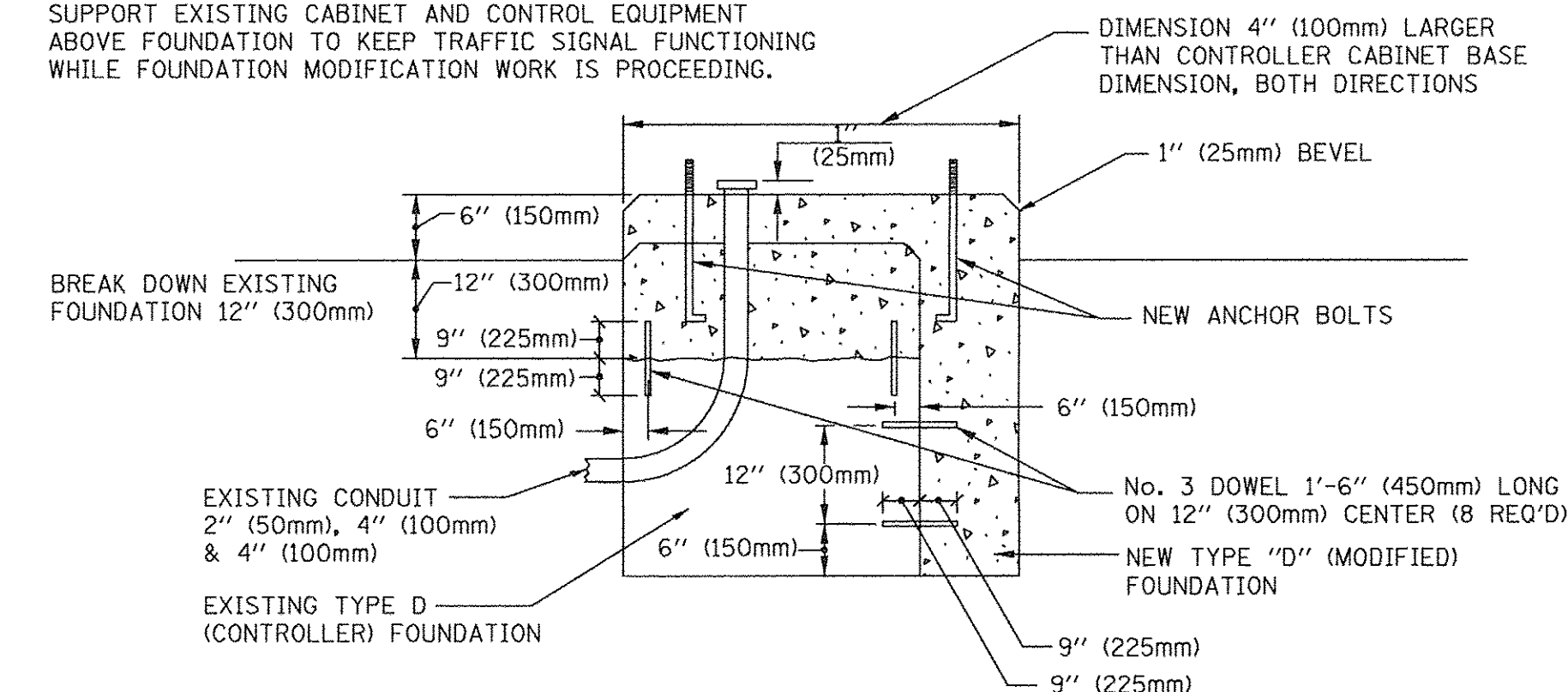
SHROUD

NOTES:

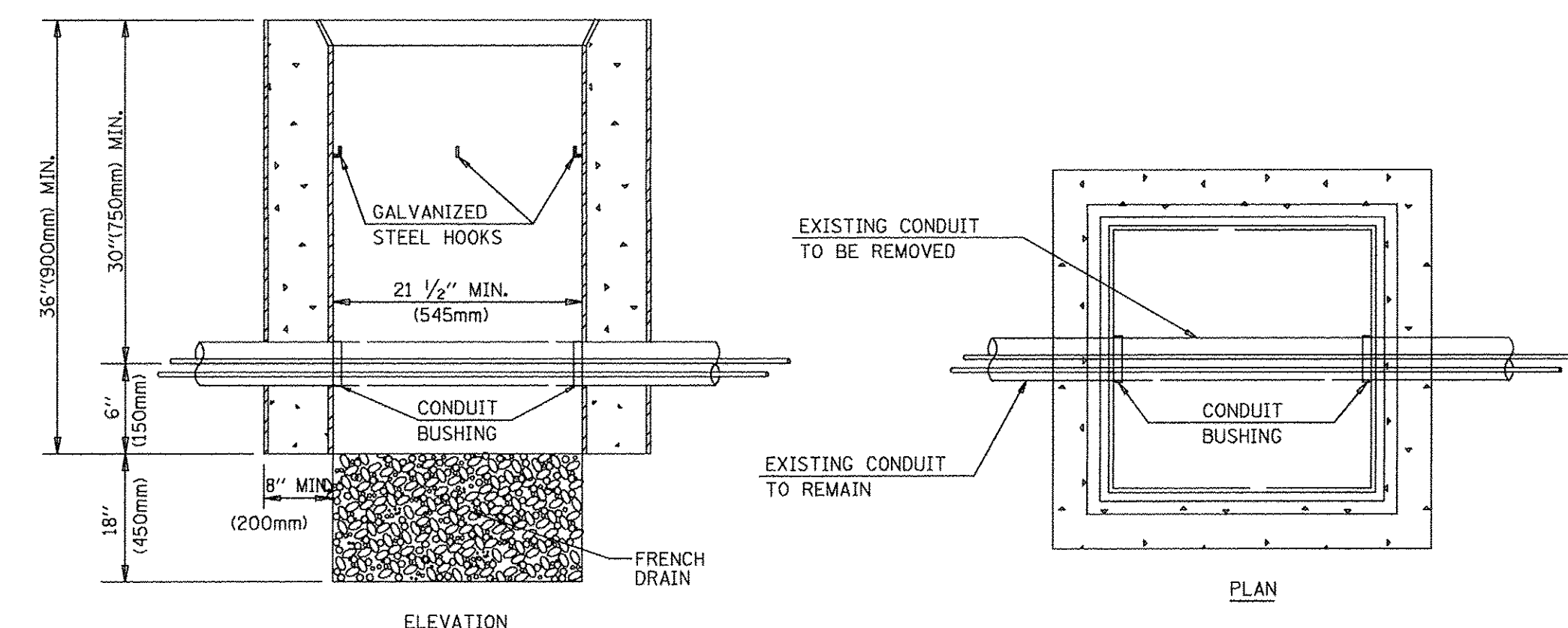
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION



NOTES:

1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

FILE NAME = 13375_02-SGNL-DTLS-03 - P06

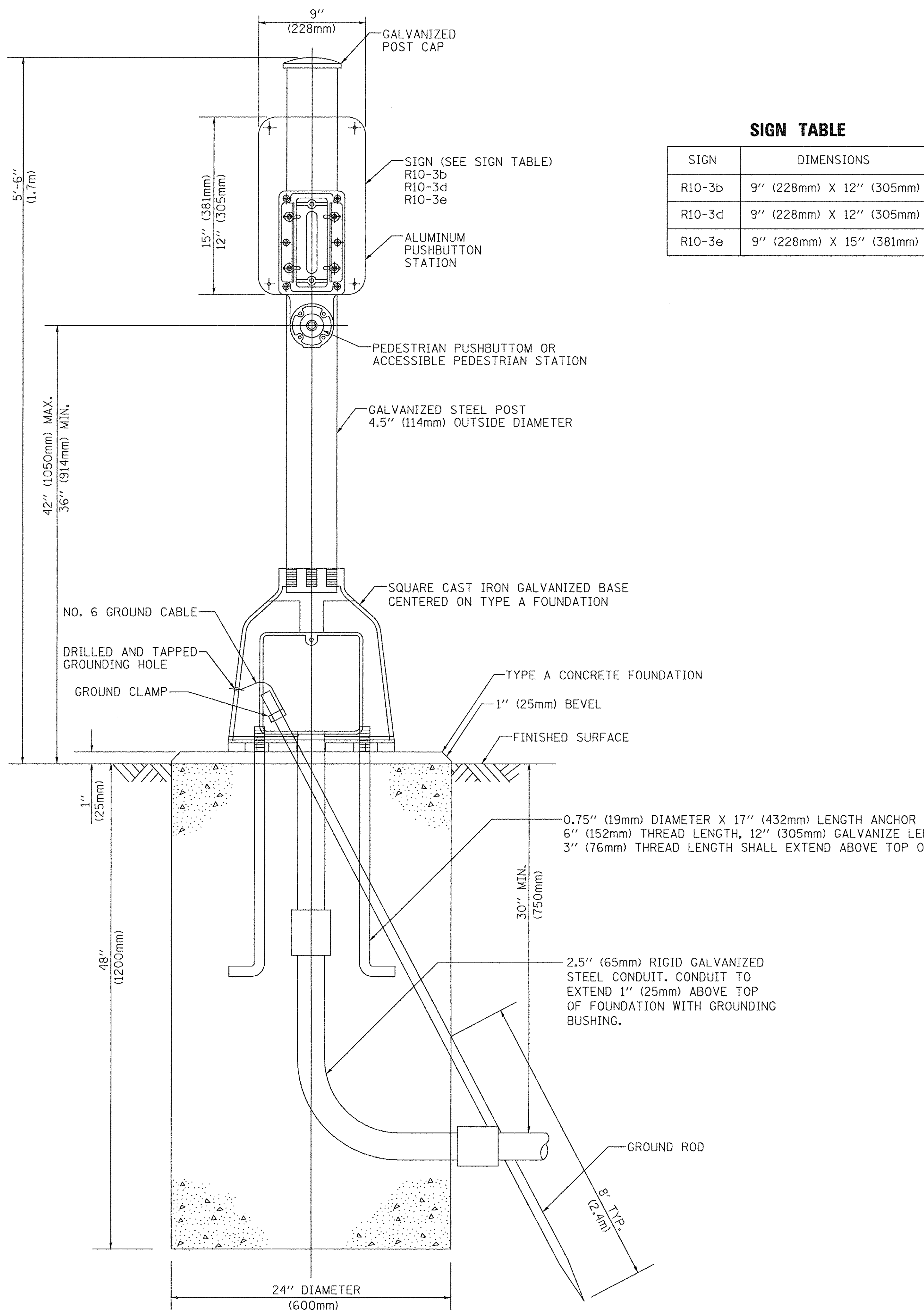
USER NAME = footemj	DESIGNED -- DAD	REVISED -- DAG 1-1-14
PLOT SCALE = 5/8" @ 1" = 1'	CHECKED -- BCK	REVISED --
PLOT DATE = 1/13/2014	DRAWN -- DAD	REVISED --
	CHECKED -- 10-28-09	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

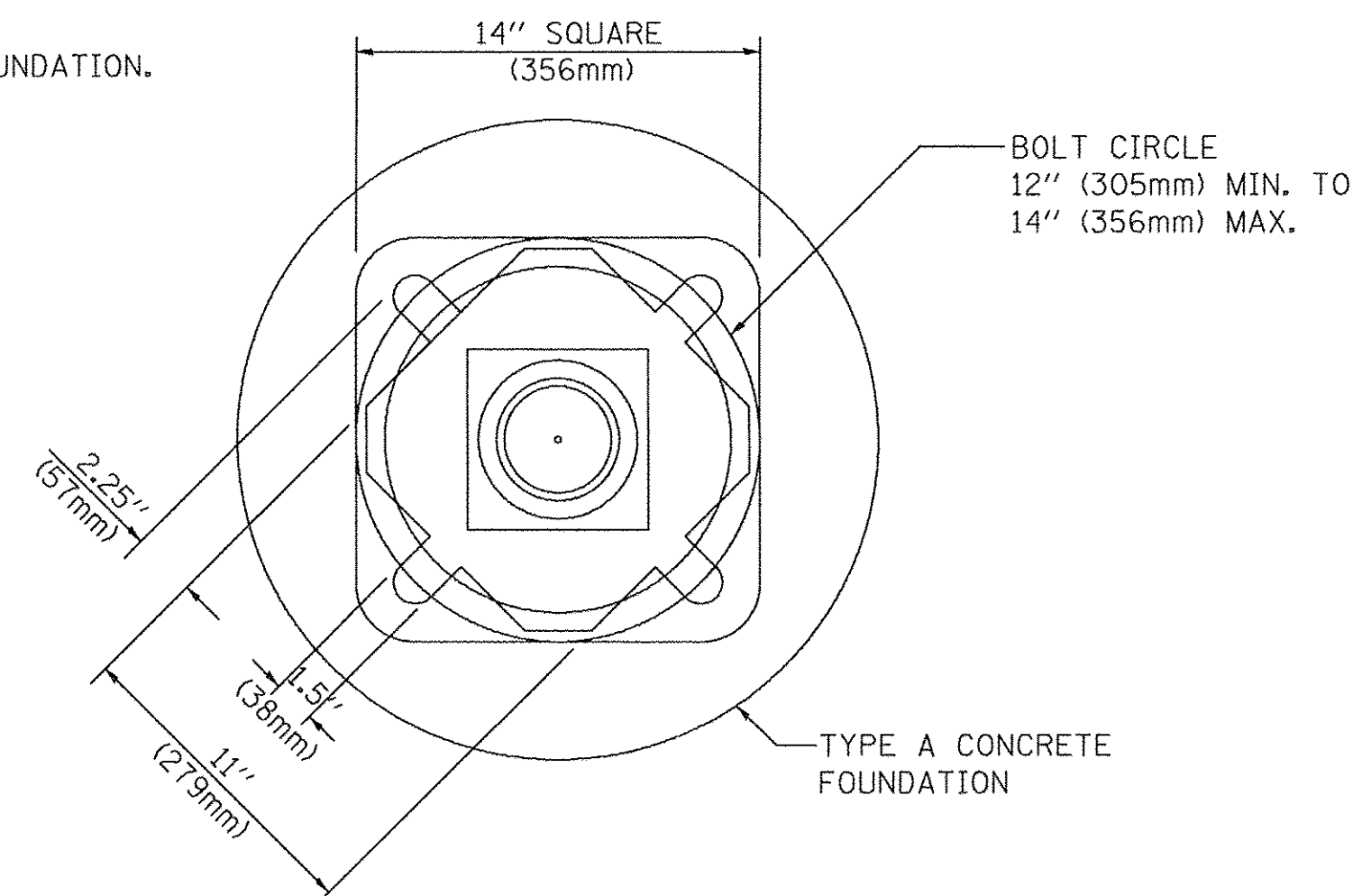
F.A.P. RTE. 353	SECTION 13-00063-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 23
TS-05		CONTRACT NO. 61C11		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT M-4003(216)		

SCALE: NONE SHEET NO. 6 OF 7 SHEETS STA. TO STA.



SIGN TABLE

SIGN	DIMENSIONS
R10-3b	9" (228mm) X 12" (305mm)
R10-3d	9" (228mm) X 12" (305mm)
R10-3e	9" (228mm) X 15" (381mm)



BOLT PATTERN
PEDESTRIAN PUSH BUTTON POST, TYPE A

FILE NAME = 13375_02-SGNL_DTLS-03 - P07

USER NAME = footemj	DESIGNED -- DAG	REVISED -- DAG 1-1-14
PLOT SCALE = 50.0000' / 1" =	CHECKED -- GND	REVISED --
PLOT DATE = 1/13/2014	DRAWN -- DAD	REVISED --
	CHECKED -- 10/1/2012	REVISED --

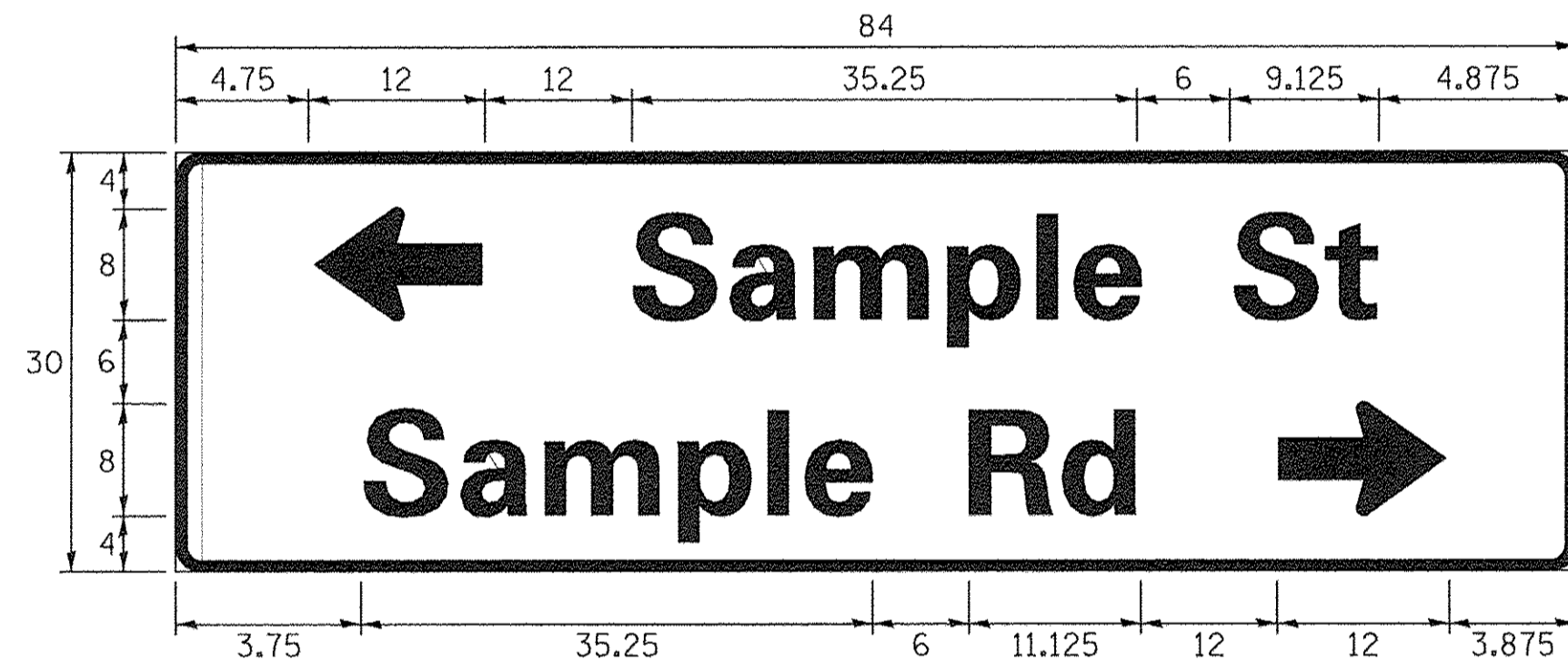
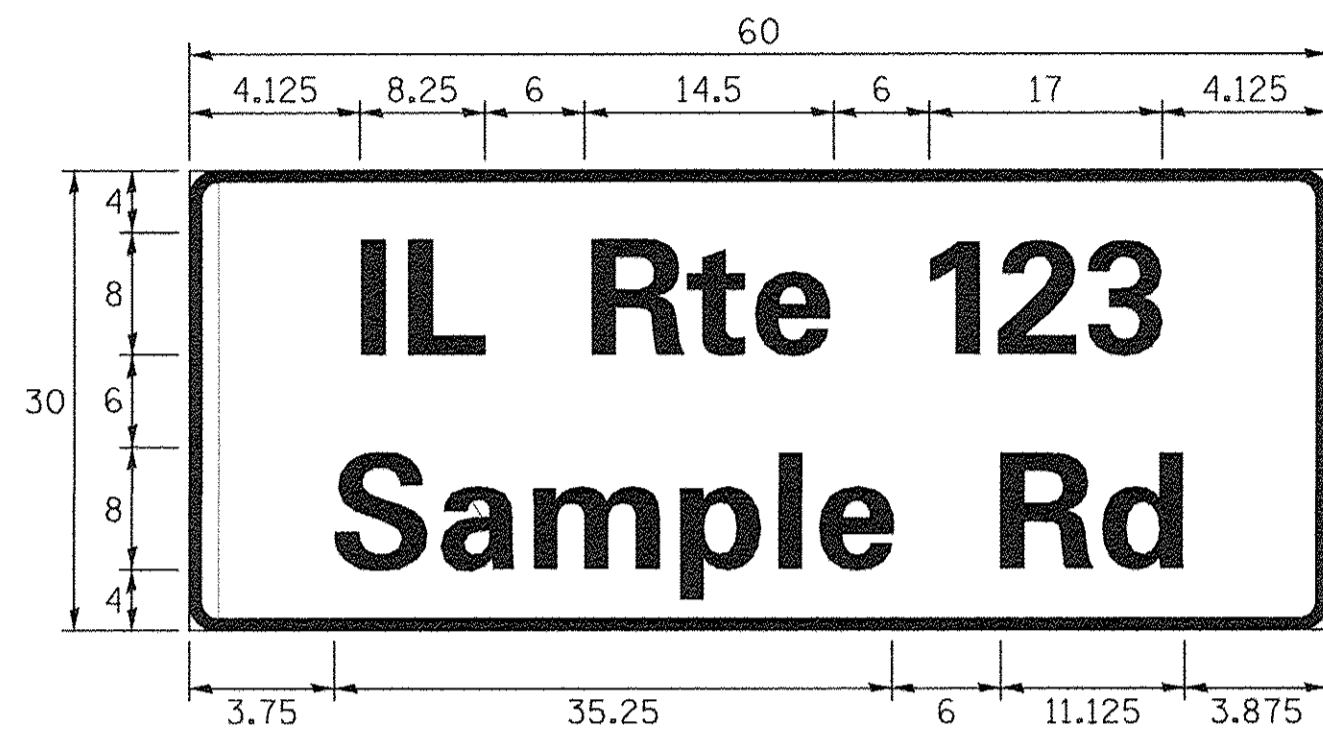
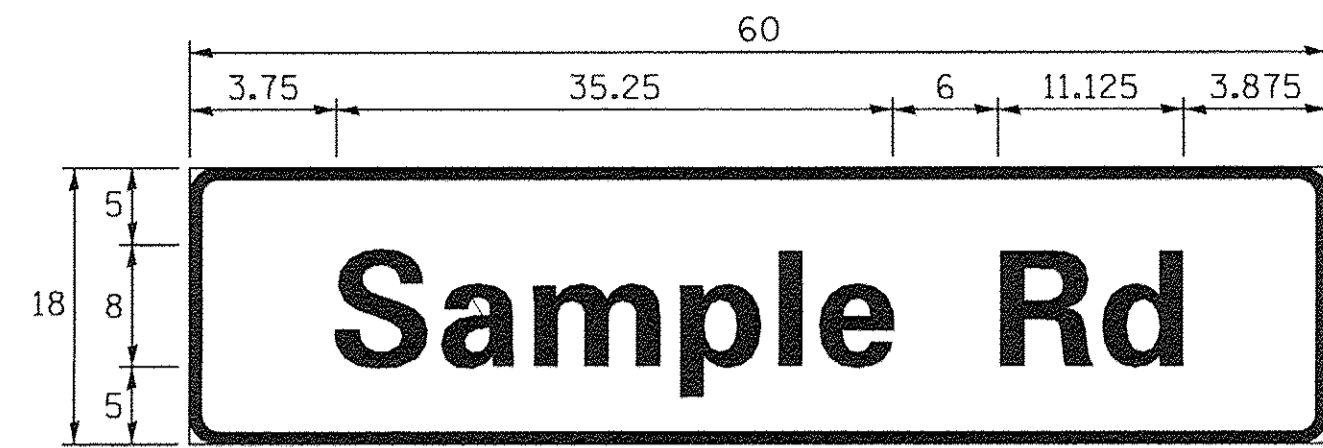
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

F.A.P. RTE. 353	SECTION 13-00063-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 24
TS-05		CONTRACT NO. 61C11		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(216)				

SCALE: NONE SHEET NO. 7 OF 7 SHEETS STA. TO STA.

SIGN PANEL – TYPE 1 OR TYPE 2



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY. REQUIRED
D OR C	-	1 OR 2	ZZ	-

COMMON STREET NAME ABBREVIATIONS AND WIDTHS

NAME	ABBREVIATION	WIDTH (INCH)	
		SERIES "C"	SERIES "D"
AVENUE	Ave	15.000	18.250
BOULEVARD	Blvd	17.125	20.000
CIRCLE	Cir	11.125	13.000
COURT	Ct	8.250	9.625
DRIVE	Dr	8.625	10.125
HIGHWAY	Hwy	18.375	22.000
ILLINOIS	IL	7.000	8.250
LANE	Ln	9.125	10.750
PARKWAY	Pkwy	23.375	27.375
PLACE	Pl	7.125	7.750
ROAD	Rd	9.625	11.125
ROUTE	Rte	12.625	14.500
STREET	St	8.000	9.125
TERRACE	Ter	12.625	14.625
TRAIL	Tr	7.750	9.125
UNITED STATES	US	10.375	12.250

GENERAL NOTES

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 877001, 877002, 877006, 877011 AND 877012, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 8'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- ALL SIGNS SHALL CONSIST OF A WHITE LEGEND AND BORDER (TYPE ZZ SHEETING) ON A GREEN BACKGROUND (TYPE ZZ SHEETING)
- THE SIGN LENGTH SHALL BE IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHALL NOT EXCEED 8'-0". ALL BORDERS SHALL BE 3/4" WIDE. CORNER RADIUS SHALL BE 1-7/8". THE SPACING BETWEEN THE WORDS SHOULD BE 6", IF POSSIBLE, BUT MAY BE REDUCED TO 5" WHEN SPACING IS CRITICAL. A MINIMUM OF 2-1/2" SHALL BE INCLUDED BETWEEN THE WORD AND THE RIGHT AND LEFT EDGES OF THE SIGN.
- A PREFERRED METHOD FOR THE SIGN DESIGN IS TO USE SERIES "D" LETTER ON A ONE-LINE SIGN 18" IN HEIGHT AND A MAXIMUM OF 8'-0" IN WIDTH. IF SERIES "D" DOES NOT FIT ON A 8'-0" SIGN, THEN SERIES "C" SHOULD BE TRIED. IF SERIES "C" DOES NOT FIT ON A 8'-0" SIGN, A 30" HIGH TWO-LINE SIGN CAN BE USED. THE CROSSROAD DESIGNATION AS TO STREET, AVENUE, ETC. SHOULD BE SPELLED OUT ON THE SECOND LINE, IF THERE IS SPACE AVAILABLE.
- LED ILLUMINATED STREET NAME SIGNS CAN BE USED IN PLACE OF REGULAR SIGN PANELS BUT ANY SPECIAL WORDING AND SYMBOLOGY MUST BE APPROVED BY THE DEPARTMENT. GENERAL DESIGN REQUIREMENT AS LISTED ABOVE (COLOR, FONT, SIZE, ETC.) MUST BE FOLLOWED.
- SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS.

LOCAL SUPPLIERS:

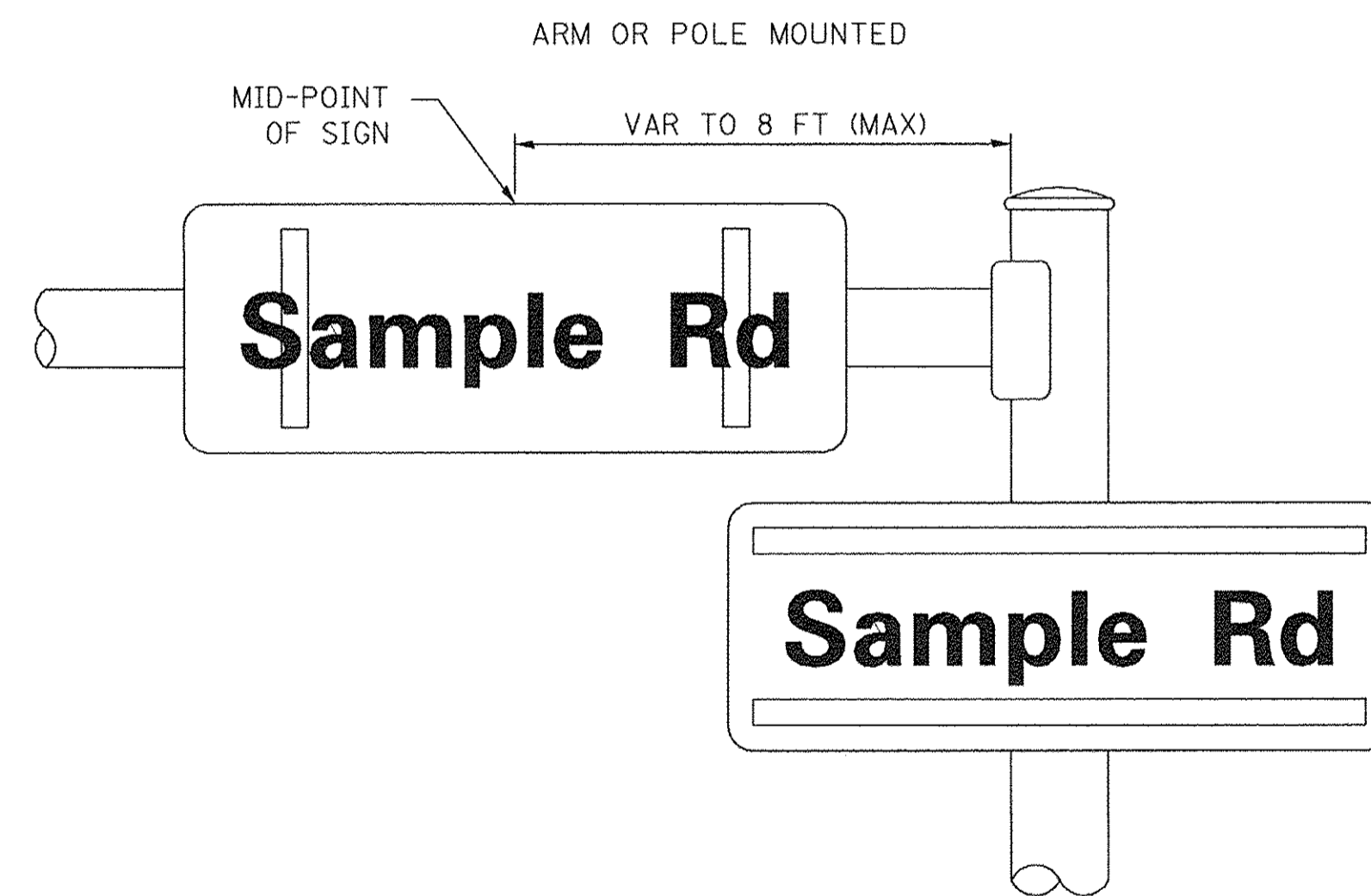
- J.O. HERBERT COMPANY, INC
MIDLOTHIAN, VA
- WESTERN REMAC, INC.
WOODRIDGE, IL

PARTS LISTING:

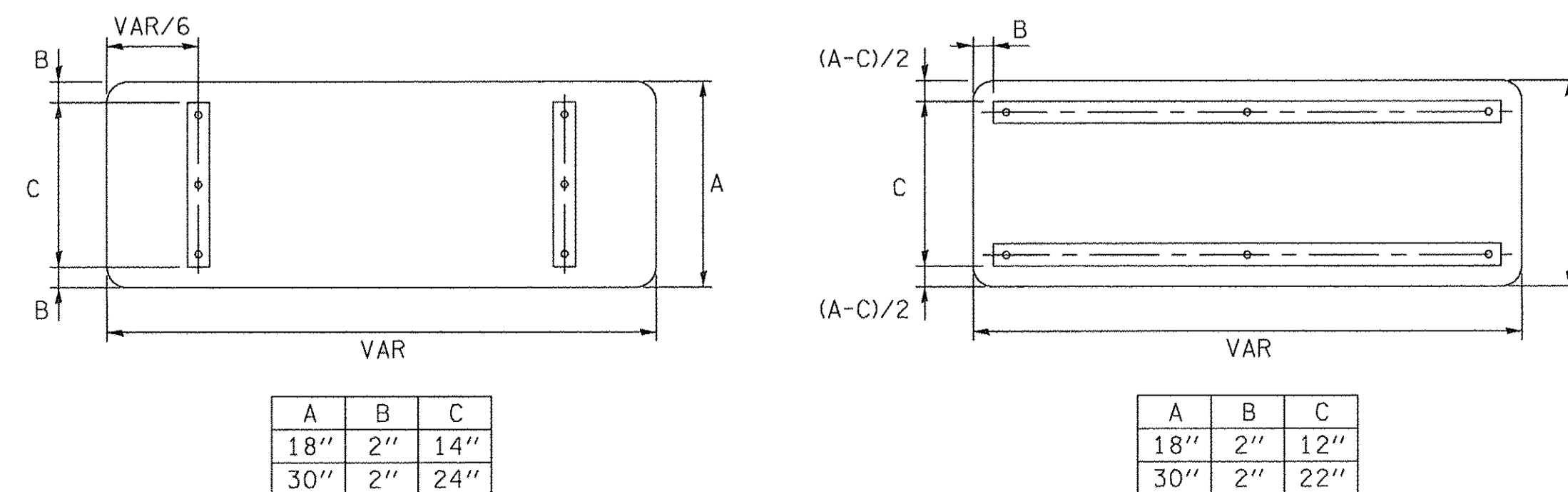
- SIGN CHANNEL PART #HPN053 (MED. CHANNEL)
1/4" x 14 x 1" H.W.H. #3
SELF TAPPING WITH NEOPRENE WASHER
- SIGN SCREWS PART #HPN034 (UNIVERSAL)
CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING
- BRACKETS

OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BACKET OF THE ABOVE PRODUCT.

MOUNTING LOCATION



SUPPORTING CHANNELS



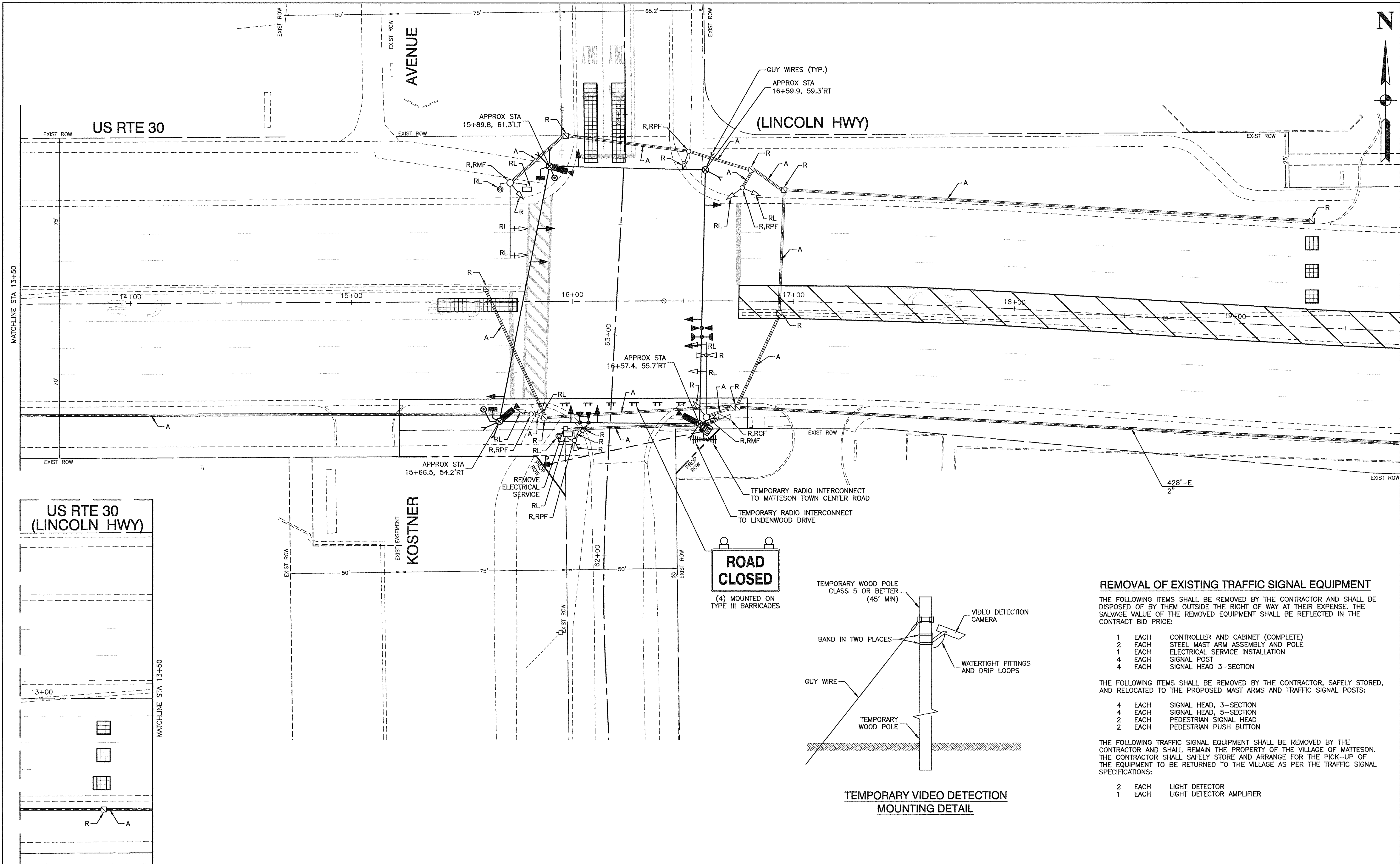
A	B	C
18"	2"	14"
30"	2"	24"

A	B	C
18"	2"	12"
30"	2"	22"

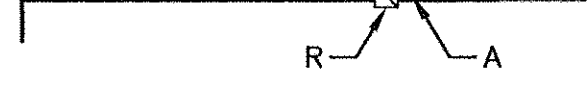
STANDARD ALPHABETS SPACING CHART

(8") UPPER CASE AND (6") LOWER CASE

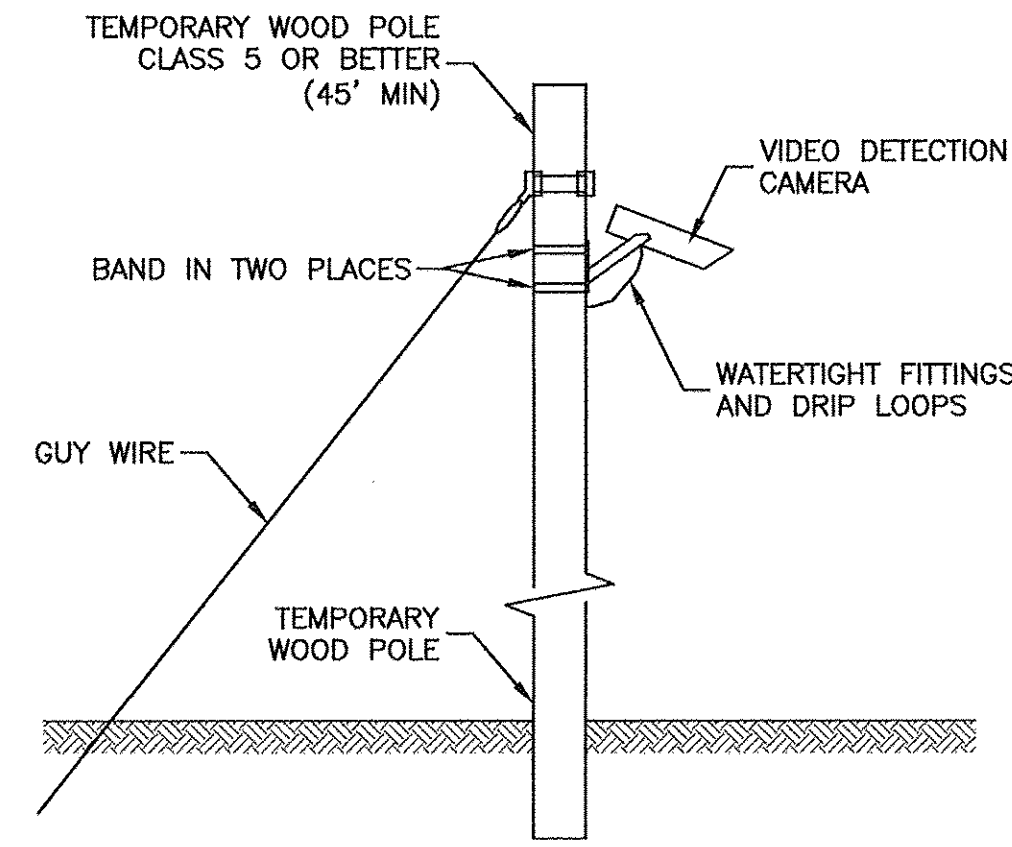
FHWA SERIES "C"				FHWA SERIES "D"			
CHARACTER	LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)	CHARACTER	LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)
A	0.240	5.122	0.240	A	0.240	6.804	0.240
B	0.880	4.482	0.480	B	0.960	5.446	0.400
C	0.720	4.482	0.720	C	0.800	5.446	0.800
D	0.880	4.482	0.720	D	0.960	5.446	0.800
E	0.880	4.082	0.480	E	0.960	4.962	0.400
F	0.880	4.082	0.240	F	0.960	4.962	0.240
G	0.720	4.482	0.720	G	0.800	5.446	0.800
H	0.880	4.482	0.880	H	0.960	5.446	0.960
I	0.880	1.120	0.880	I	0.960	1.280	0.960
J	0.240	4.082	0.880	J	0.240	5.122	0.960
K	0.880	4.482	0.480	K	0.960	5.604	0.400
L	0.880	4.082	0.240	L	0.960	4.962	0.240
M	0.880	5.284	0.880	M	0.960	6.244	0.960
N	0.880	4.482	0.880	N	0.960	5.446	0.960
O	0.720	4.722	0.720	O	0.800	5.684	0.800
P	0.880	4.482	0.720	P	0.960	5.446	0.240
Q	0.720	4.722	0.720	Q	0.800	5.684	0.800
R	0.880	4.482	0.480	R	0.960	5.446	0.400
S	0.480	4.482	0.480	S	0.400	5.446	0.400
T	0.240	4.082	0.240	T	0.240	4.962	0.240
U	0.880	4.482	0.880	U	0.960	5.446	0.960
V	0.240	4.962	0.240	V	0.240	6.084	0.240
W	0.240	6.084	0.240	W	0.240	7.124	0.240
X	0.240	4.722	0.240	X	0.400	5.446	0.400
Y	0.240	5.122	0.240	Y	0.240	6.884	0.240
Z	0.480	4.482	0.480	Z	0.400	5.446	0.400
a	0.320	3.842	0.640	a	0.400	4.562	0.720
b	0.720	4.082	0.480	b	0.800	4.802	0.480
c	0.480	4.002	0.240	c	0.480	4.722	0.240
d	0.480	4.082	0.720	d	0.480	4.802	0.800
e	0.480	4.082	0.320	e	0.480	4.722	0.320
f	0.320	2.480	0.160	f	0.320	2.882	0.160
g	0.480	4.082	0.720	g	0.480	4.802	0.800
h	0.720	4.082	0.640	h	0.800	4.722	0.720
i	0.720	1.120	0.720	i	0.800	1.280	0.800
j	0.000	2.320	0.720	j	0.000	2.642	0.800
k	0.720	4.322	0.160	k	0.800	5.122	0.160
l	0.720	1.120	0.720	l	0.800	1.280	0.800
m	0.720	6.724	0.640	m	0.800	7.926	0.720
n	0.720	4.082	0.640	n	0.800	4.722	0.720
o	0.480	4.082	0.480	o	0.480	4.882	0.480
p	0.720	4.082	0.480	p	0.800	4.802	0.480
q	0.480	4.082	0.720	q	0.480	4.802	0.800
r	0.720	2.642	0.160	r	0.800	3.042	0.160
s	0.320	3.362	0.240	s	0.320	3.762	0.240
t	0.080	2.882	0.080	t	0.080	3.202	0.080
u	0.640	4.082	0.720	u	0.720	4.722	0.800
v	0.160	4.722	0.160	v	0.160	5.684	0.160
w	0.160	7.524	0.160	w	0.160	9.046	0.160
x	0.000	5.202	0.000	x	0.000	6.244	0.000
y	0.160	4.962	0.160	y	0.160	6.004	0.160
z	0.240	3.362	0.240	z	0.240	4.002	0.240
1	0.720	1.680	0.880	1	0.800	2.000	0.960
2	0.480	4.482	0.480	2	0.800	5.446	0.800
3	0.480	4.482	0.480	3	1.440	5.446	0.800
4	0.240	4.962	0.720	4	0.160	6.004	0.960
5	0.480	4.482	0.480	5	0.800	5.446	0.800
6	0.720	4.482	0.720	6	0.800	5.446	0.800
7	0.240	4.482	0.720	7	0.560	5.446	0.560
8	0.480	4.482	0.480	8	0.800	5.446	0.800
9	0.480	4.482	0.480	9	0.800	5.446	0.800
0	0.720	4.722	0.720	0	0.800	5.684	0.800
-	0.240	2.802	0.240	-	0.240	2.802	0.240



**US RTE 30
(LINCOLN HWY)**



(4) MOUNTED ON TYPE III BARRICADES



TEMPORARY VIDEO DETECTION MOUNTING DETAIL

REMOVAL OF EXISTING TRAFFIC SIGNAL EQUIPMENT

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT OF WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE:

- 1 EACH CONTROLLER AND CABINET (COMPLETE)
- 2 EACH STEEL MAST ARM ASSEMBLY AND POLE
- 1 EACH ELECTRICAL SERVICE INSTALLATION
- 4 EACH SIGNAL POST
- 4 EACH SIGNAL HEAD 3-SECTION

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR, SAFELY STORED, AND RELOCATED TO THE PROPOSED MAST ARMS AND TRAFFIC SIGNAL POSTS:

- 4 EACH SIGNAL HEAD, 3-SECTION
- 4 EACH SIGNAL HEAD, 5-SECTION
- 2 EACH PEDESTRIAN SIGNAL HEAD
- 2 EACH PEDESTRIAN PUSH BUTTON

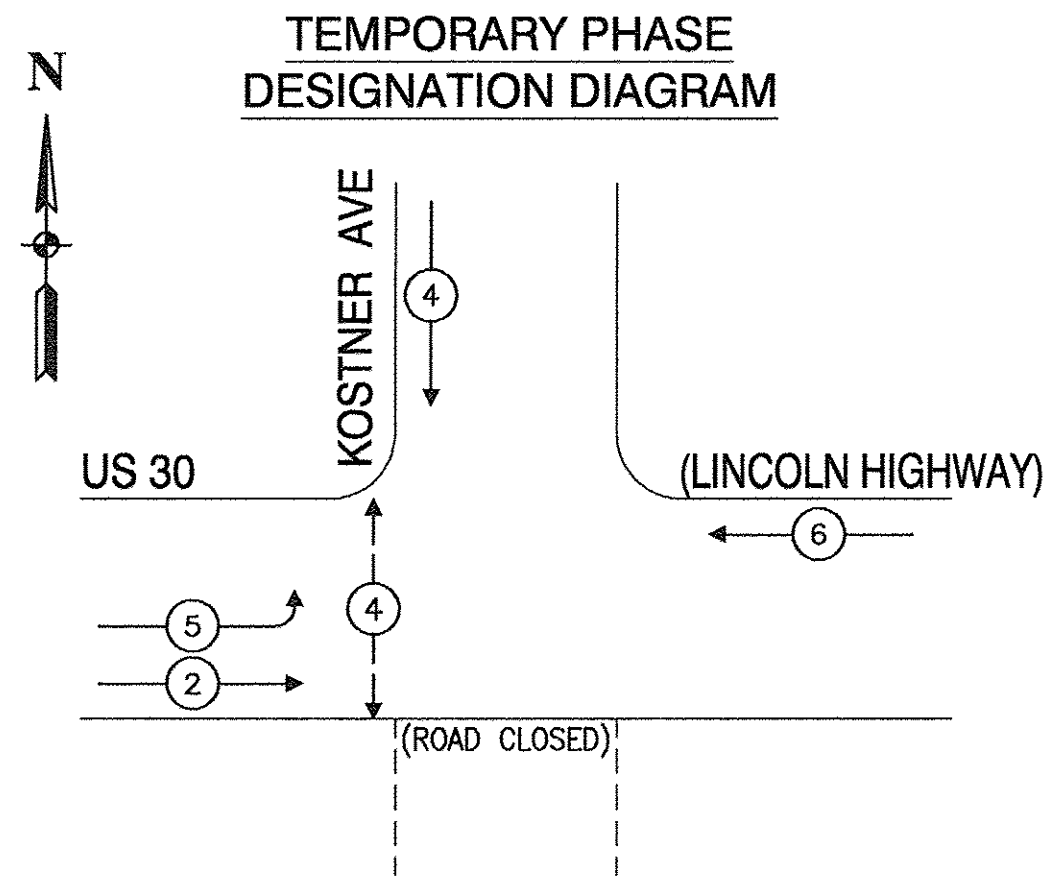
THE FOLLOWING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE VILLAGE OF MATTESON. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR THE PICK-UP OF THE EQUIPMENT TO BE RETURNED TO THE VILLAGE AS PER THE TRAFFIC SIGNAL SPECIFICATIONS:

- 2 EACH LIGHT DETECTOR
- 1 EACH LIGHT DETECTOR AMPLIFIER

THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

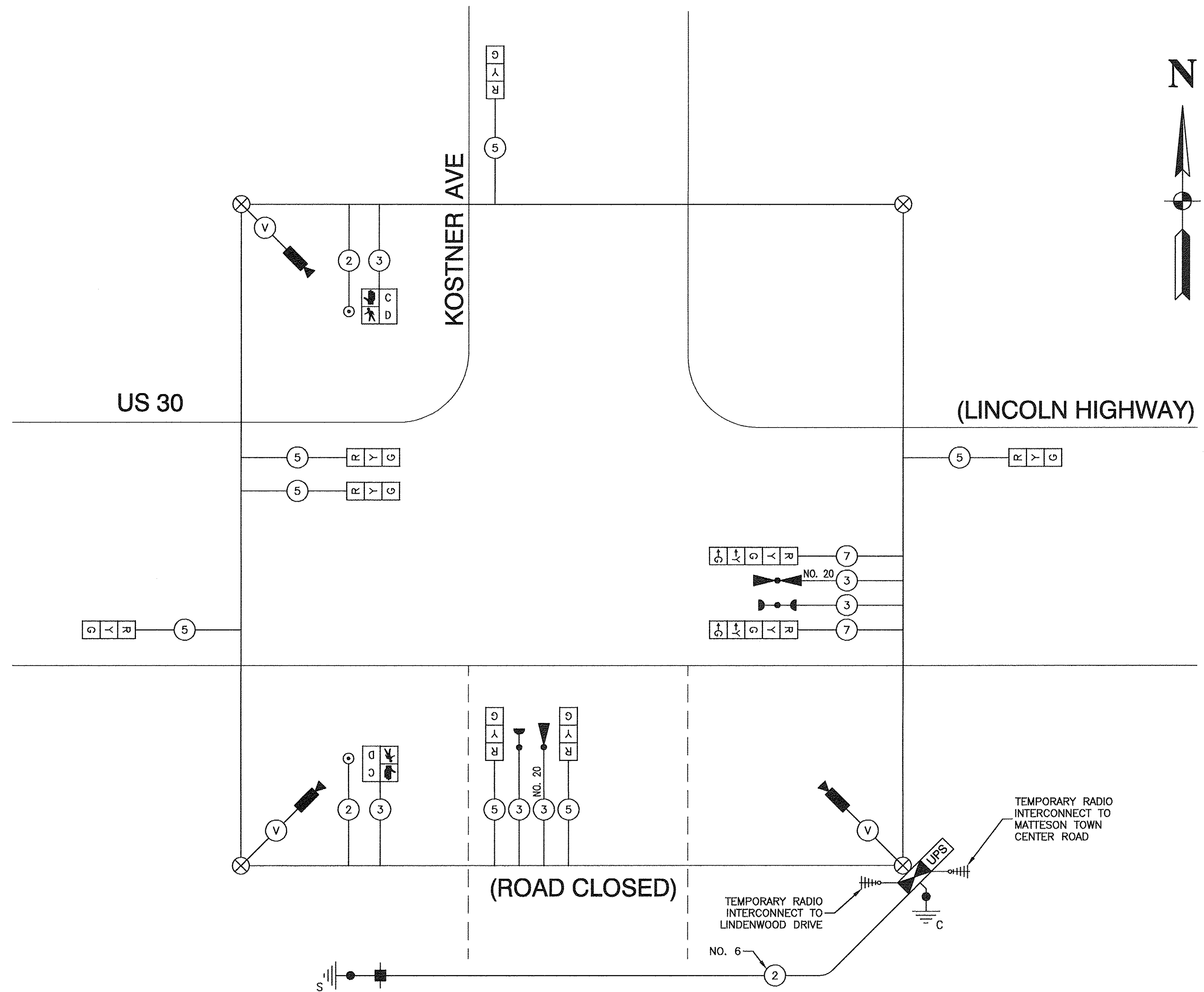
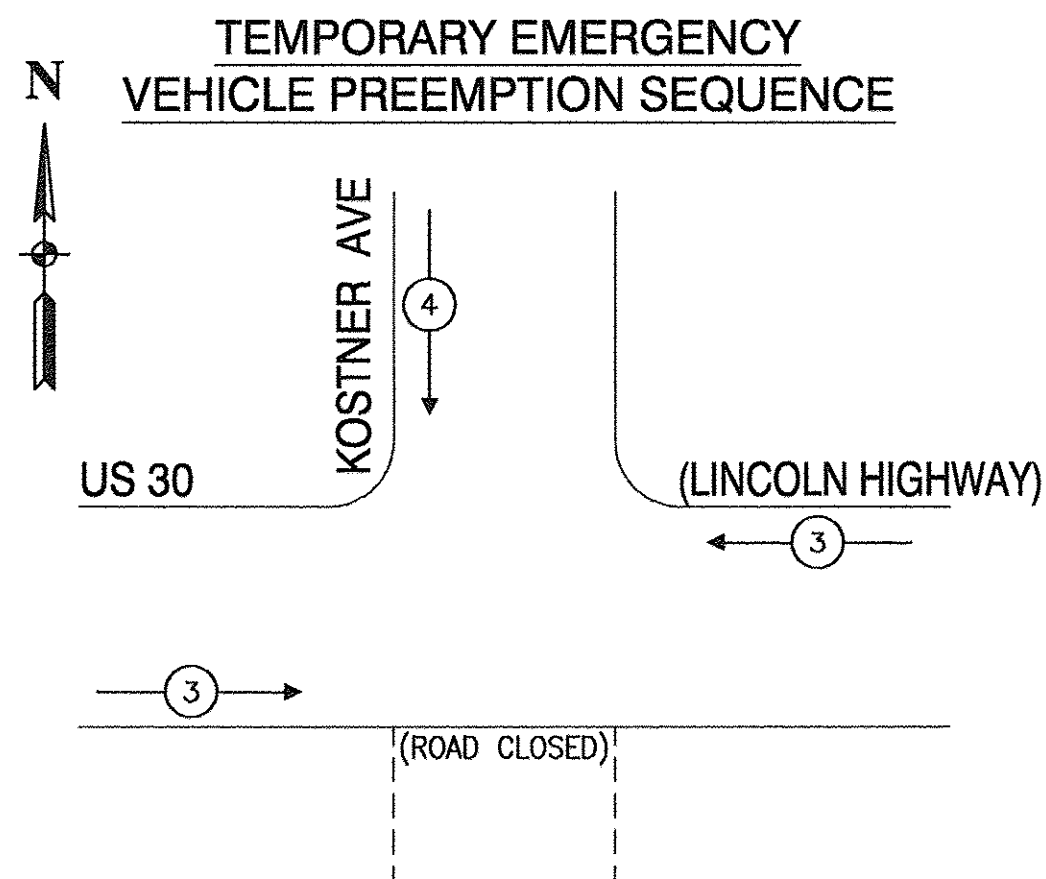
TS 1450

FILE NAME = 13375_02-SGNL-01 - TEMP TS	USER NAME =	DESIGNED — EMA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE INTERSECTION IMPROVEMENTS TEMPORARY TRAFFIC SIGNAL INSTALLATION	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	PLOT SCALE =	CHECKED — PKB	REVISED —			353	13-00063-00-CH	COOK	63	26	
PLOT DATE = 06-19-16	DRAWN — ACAD	REVISED —		SCALE: 1" = 20'	SHEET NO. 26 OF 63 SHEETS	STA.	TO STA.	CONTRACT NO. 61C11			
	CHECKED — ACAD	REVISED —				FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	M-4003(216)		



LEGEND

- ← (4) → DUAL ENTRY PHASE
- ◇ OVERLAP
- ← (5) → PEDESTRIAN PHASE
- * NUMBER REFERS TO ASSOCIATED PHASE



TEMPORARY CABLE PLAN

NOT TO SCALE

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	LED	% OPERATION	
SIGNAL (RED)	9	17		0.50	76.5
(YELLOW)	9	25		0.25	56.3
(GREEN)	9	15		0.25	33.8
PERMISSIVE ARROW	4	12		0.10	4.8
PED. SIGNAL	2	25		1.00	50
CONTROLLER	1	100		1.00	100
UPS	1	25		1.00	25
VIDEO SYSTEM	0	150		1.00	25
ILLUM. SIGN	0	25		0.05	0
FLASHER	-	-		0.50	-
ENERGY COSTS TO: TOTAL =					371.3

ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS/DISTRICT I
201 WEST CENTER CT. SCHUMBERG, IL 60196-1096
ENERGY SUPPLY CONTACT: LASHAWN LAO
PHONE: (708) 235-2346
COMPANY: COMMONWEALTH EDISON

NOTE:
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT
SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

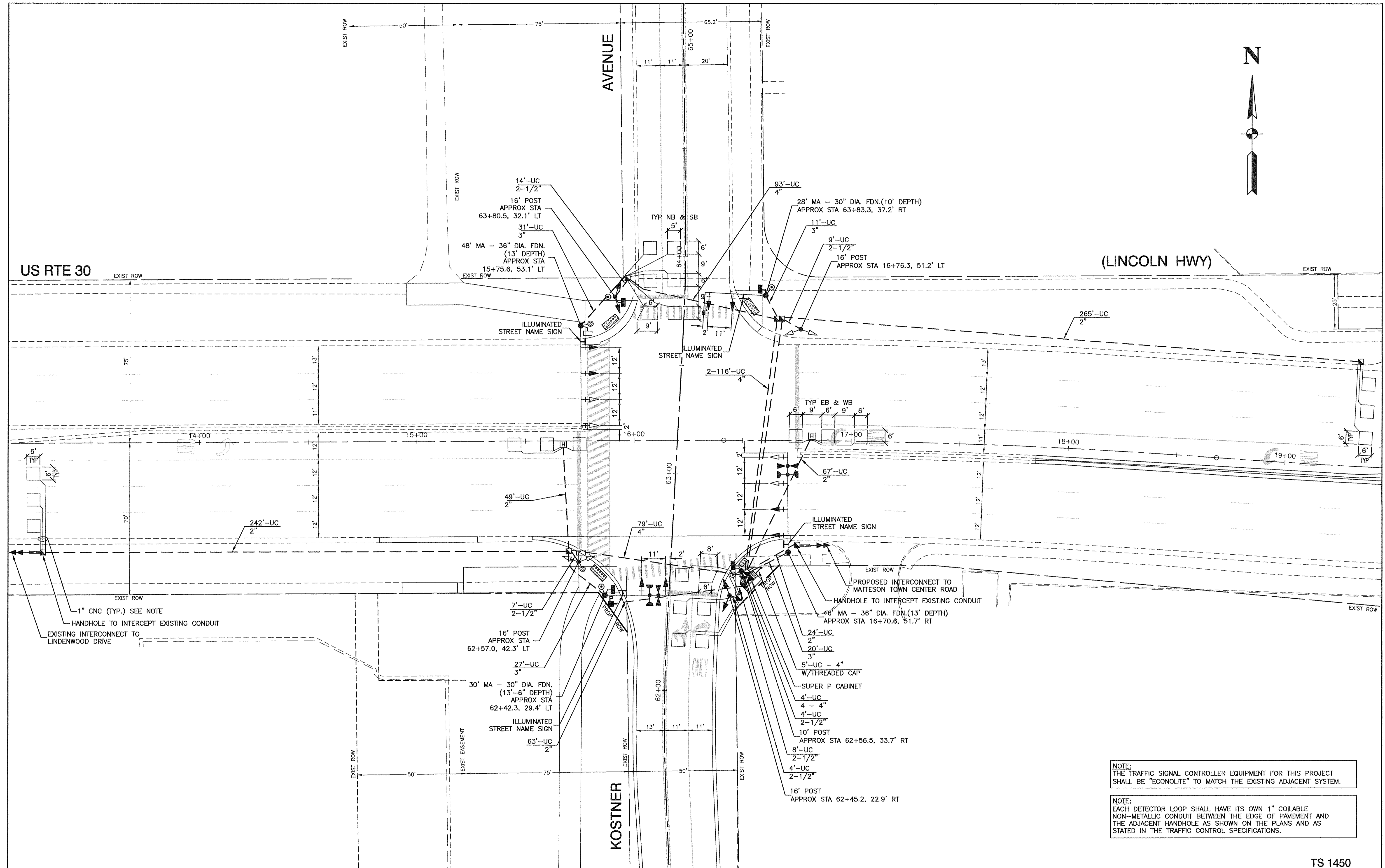
FILE NAME = 13375_02-SGNL_CBLE-01 - IDOT P01	USER NAME =	DESIGNED — EMA	REVISED —
		CHECKED — PKB	REVISED —
	PLOT SCALE =	DRAWN — JJB	REVISED —
	PLOT DATE = 05-19-16	CHECKED — APG	REVISED —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE INTERSECTION IMPROVEMENTS		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		353	13-00063-00-CH	COOK	63	27
		CONTRACT NO. 61C11				
SCALE:	SHEET NO. 27 OF 63 SHEETS	STA.	TO STA.			

FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(216)
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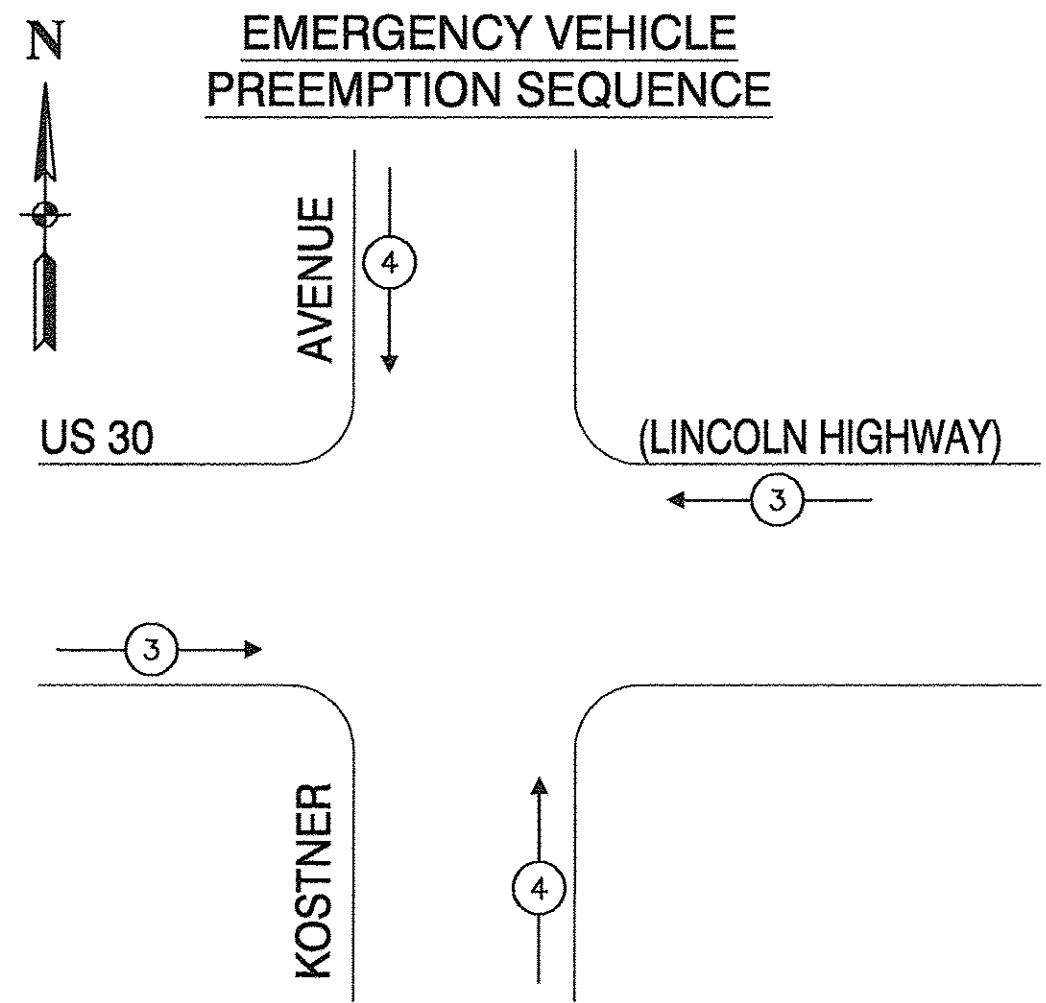
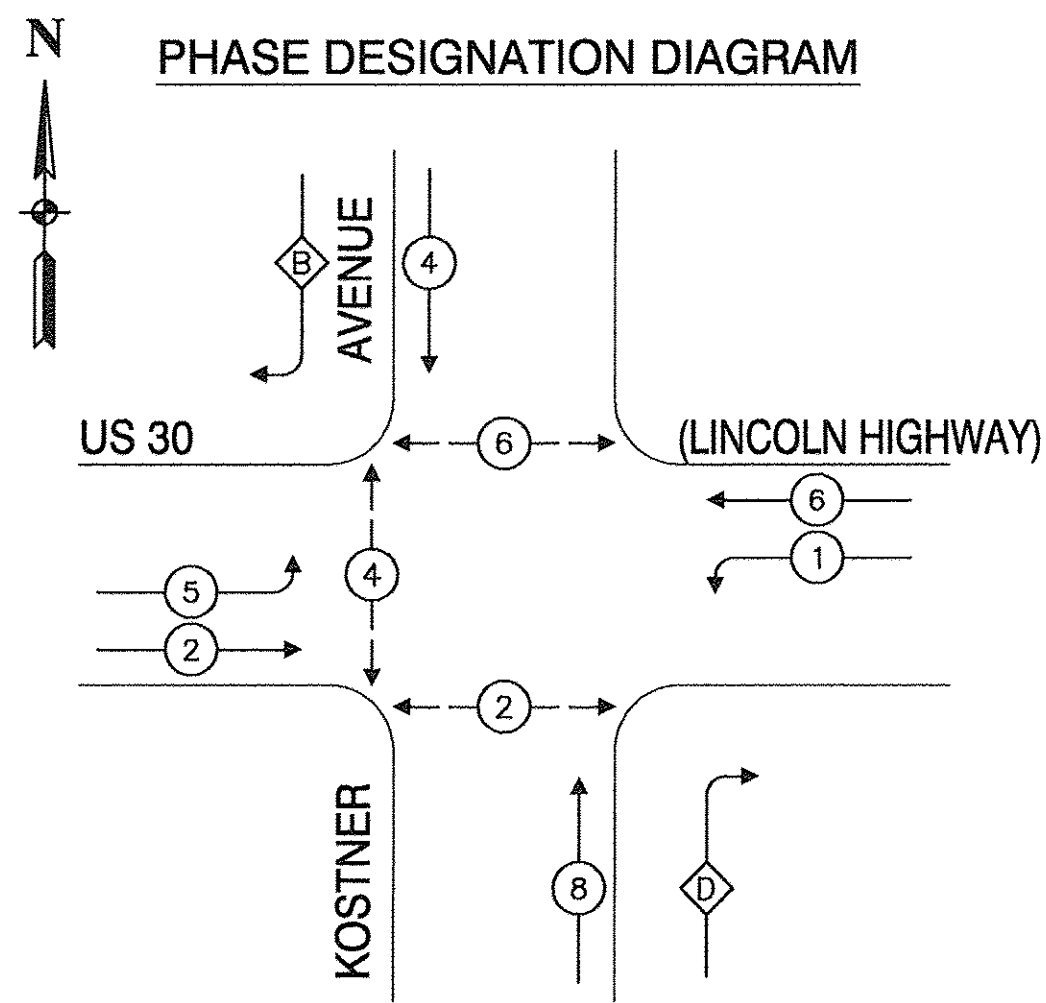
TS 1450



NOTE:
EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC CONTROL SPECIFICATIONS.

FILE NAME = 13375_02-SGNL-02 - PROP TS	USER NAME =	DESIGNED — EMA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE INTERSECTION IMPROVEMENTS PROPOSED TRAFFIC SIGNAL INSTALLATION	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE =	CHECKED — PKB	REVISED —			353	13-00063-00-CH	COOK	63	28
	PLOT DATE = 05-19-16	DRAWN — ACAD	REVISED —			CONTRACT NO. 61C11				
	CHECKED — ACAD	REVISED —		SCALE: 1" = 20'	SHEET NO. 28 OF 63 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(216)	

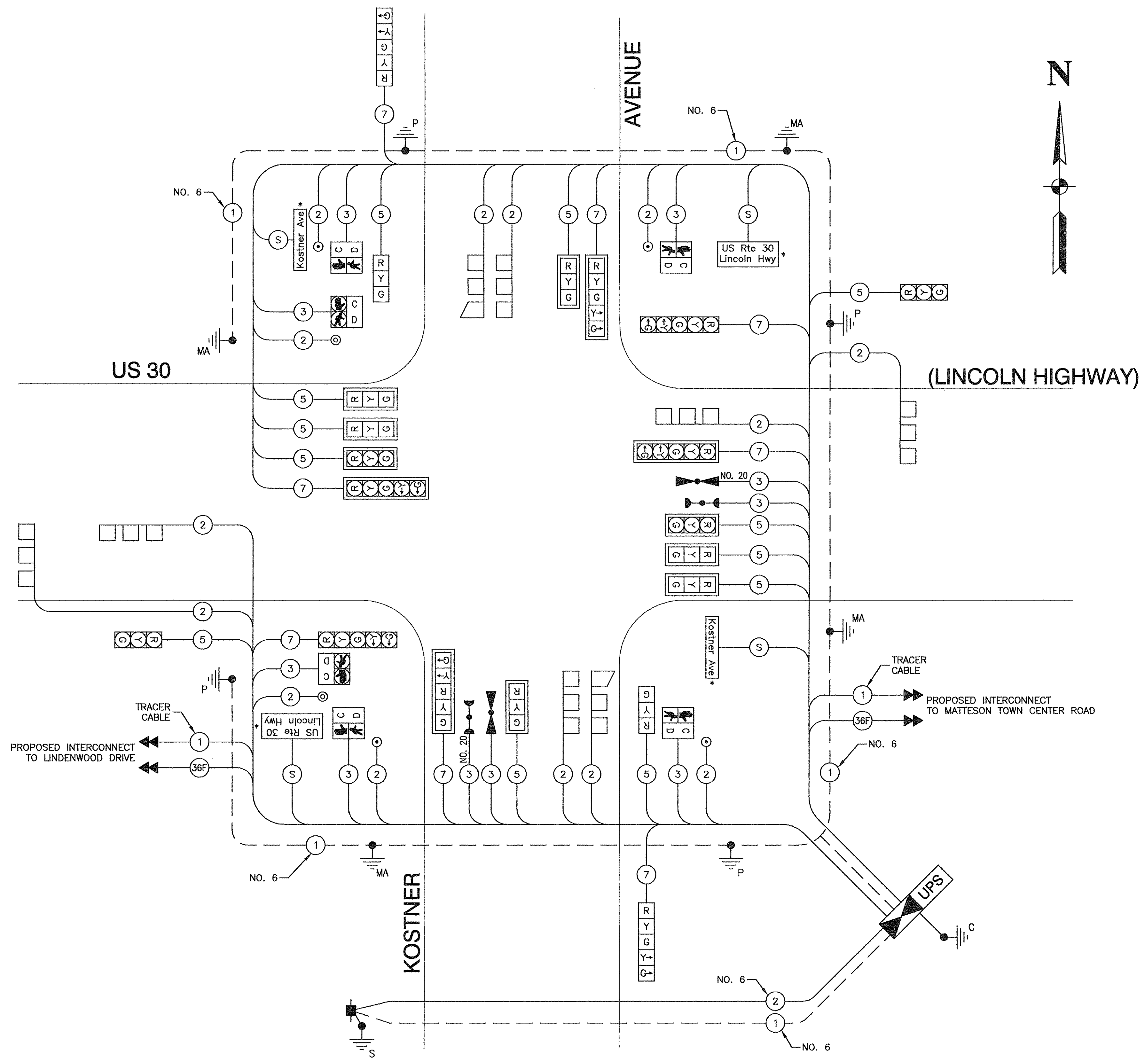
TS 1450



OVERLAP LETTER	PERMISSIVE PHASE	PROTECTED PHASE
B	= 4 + 5	
D	= 8 + 1	

LEGEND

- ← (⊙) → DUAL ENTRY PHASE
 - ← (◇) → OVERLAP
 - ← (⊙) → PEDESTRIAN PHASE
- * NUMBER REFERS TO ASSOCIATED PHASE



CABLE PLAN
NOT TO SCALE

NOTE:
*AN ASTERISK INDICATES ILLUMINATED STREET NAME SIGN

NOTE:
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE	% OPERATION		
		INCAND.	LED		
SIGNAL (RED)	20	17	0.50		170
(YELLOW)	20	25	0.25		125
(GREEN)	20	15	0.25		75
PERMISSIVE ARROW	16	12	0.10		19.2
PED. SIGNAL	6	25	1.00		150
CONTROLLER	1	100	1.00		100
UPS	1	25	1.00		25
VIDEO SYSTEM	0	150	1.00		0
ILLUM. SIGN	4	25	0.50		50
FLASHER			0.50		

ENERGY COSTS TO: TOTAL = 714.2
 ILLINOIS DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS/DISTRICT 1
 201 WEST CENTER CT. SCHAMBURG, IL 60196-1096
 ENERGY SUPPLY CONTACT: LASHAWN LAO
 PHONE: (708)235-2346
 COMPANY: COMMONWEALTH EDISON

FILE NAME = 13375_02-SGNL_CBL-02 - IDOT P01	USER NAME =	DESIGNED - EMA	REVISED -
		CHECKED - PKB	REVISED -
		PLOT SCALE =	DRAWN - JJB
			REVISED -
		PLOT DATE = 05-19-16	CHECKED - APG
			REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE INTERSECTION IMPROVEMENTS
 CABLE PLAN, PHASE DESIGNATION DIAGRAM, AND
 EMERGENCY VEHICLE PREEMPTION SEQUENCE
 SCALE: SHEET NO. 29 OF 63 SHEETS STA. TO STA.

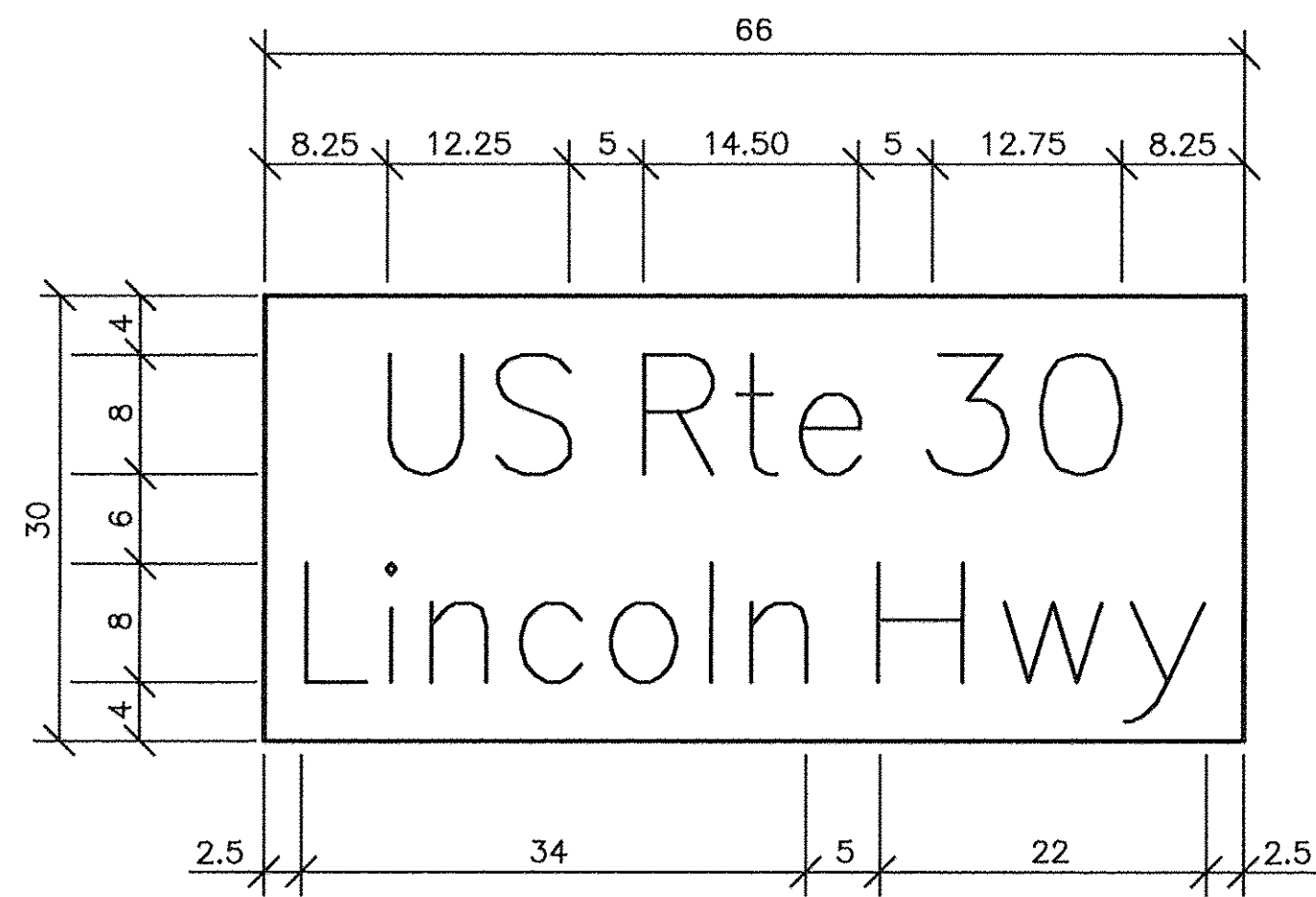
F.A.P. RTE. 353	SECTION 13-00063-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 29
FED. ROAD DIST. NO. 1 ILLINOIS			FED. AID PROJECT M-4003(216)	

TS 1450

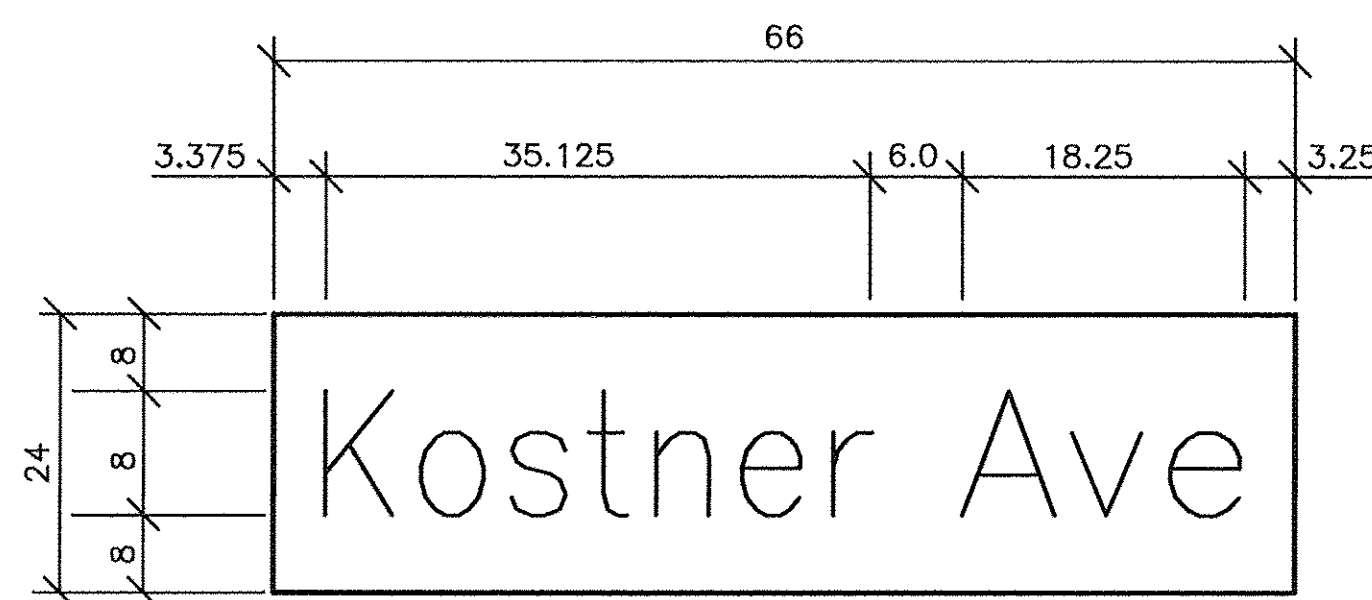
SCHEDULE OF QUANTITIES

ITEM DESCRIPTION	UNITS	TOTAL QTY
SERVICE INSTALLATION - POLE MOUNTED	EACH	1
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	719
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	44
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	92
UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	438
HANDHOLE	EACH	5
HEAVY-DUTY HANDHOLE	EACH	2
DOUBLE HANDHOLE	EACH	2
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	1085
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	1424
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	2352
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	1528
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	1656
ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	97
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	623
TRAFFIC SIGNAL POST, GALVANIZED STEEL 10 FT.	EACH	1
TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	4
STEEL MAST ARM ASSEMBLY AND POLE, 28 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 30 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 46 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 48 FT.	EACH	1
CONCRETE FOUNDATION, TYPE A	FOOT	20
CONCRETE FOUNDATION, TYPE C	FOOT	4
CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	24
CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	26
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	6
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	2
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	2
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	2
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	4
TRAFFIC SIGNAL BACKPLATE, LOUVERED, FORMED PLASTIC	EACH	12
INDUCTIVE LOOP DETECTOR	EACH	8
DETECTOR LOOP, TYPE I	FOOT	870
LIGHT DETECTOR	EACH	2
LIGHT DETECTOR AMPLIFIER	EACH	1
PEDESTRIAN PUSH-BUTTON	EACH	4
TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1
RELOCATE EXISTING SIGNAL HEAD	EACH	8
RELOCATE EXISTING PEDESTRIAN SIGNAL HEAD	EACH	2
RELOCATE EXISTING PEDESTRIAN PUSH-BUTTON	EACH	2
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REMOVE EXISTING HANDHOLE	EACH	7
REMOVE EXISTING DOUBLE HANDHOLE	EACH	1
REMOVE EXISTING CONCRETE FOUNDATION	EACH	7
TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1
LED INTERNALLY ILLUMINATED STREET NAME SIGN	EACH	4
FULL-ACTUATED CONTROLLER AND TYPE SUPER P CABINET (SPECIAL)	EACH	1
EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C	FOOT	303
UNINTERRUPTABLE POWER SUPPLY, SPECIAL	EACH	1

**SIGN PANEL
LED INTERNALLY ILLUMINATED STREET NAME SIGN**



DESIGN SERIES	AREA (SQ FT)	SHEETING TYPE	QTY. REQUIRED
D	13.75	ZZ	2



DESIGN SERIES	AREA (SQ FT)	SHEETING TYPE	QTY. REQUIRED
D	8.25	ZZ	2

NOTE:
FOR ADDITIONAL DESIGN AND INSTALLATION INFORMATION SEE DISTRICT 1 MAST ARM MOUNTED STREET NAME SIGNS DETAIL, AND ILLUMINATED STREET NAME SIGN MOUNTING DETAIL.

FILE NAME = 13375_02-SGNL_DTLS-01 - IDOT P01

USER NAME =	DESIGNED -- EMA	REVISED --
	CHECKED -- PKB	REVISED --
PLOT SCALE =	DRAWN -- JJB	REVISED --
PLOT DATE = 05-19-16	CHECKED -- APG	REVISED --

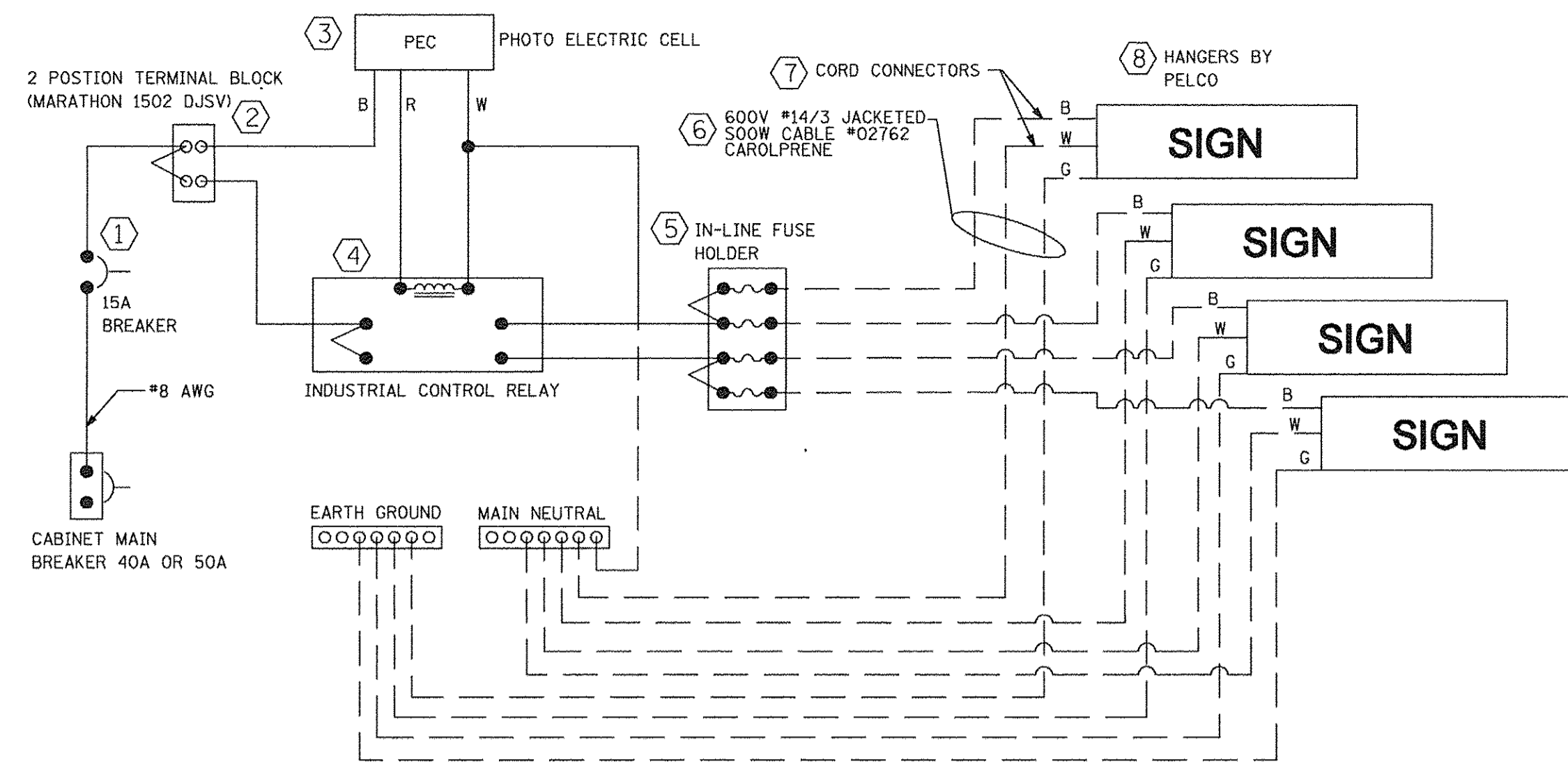
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE INTERSECTION IMPROVEMENTS
TEMPORARY CABLE PLAN, PHASE DESIGNATION DIAGRAM, AND
EMERGENCY VEHICLE PREEMPTION SEQUENCE

SCALE: SHEET NO. 30 OF 63 SHEETS STA. TO STA.

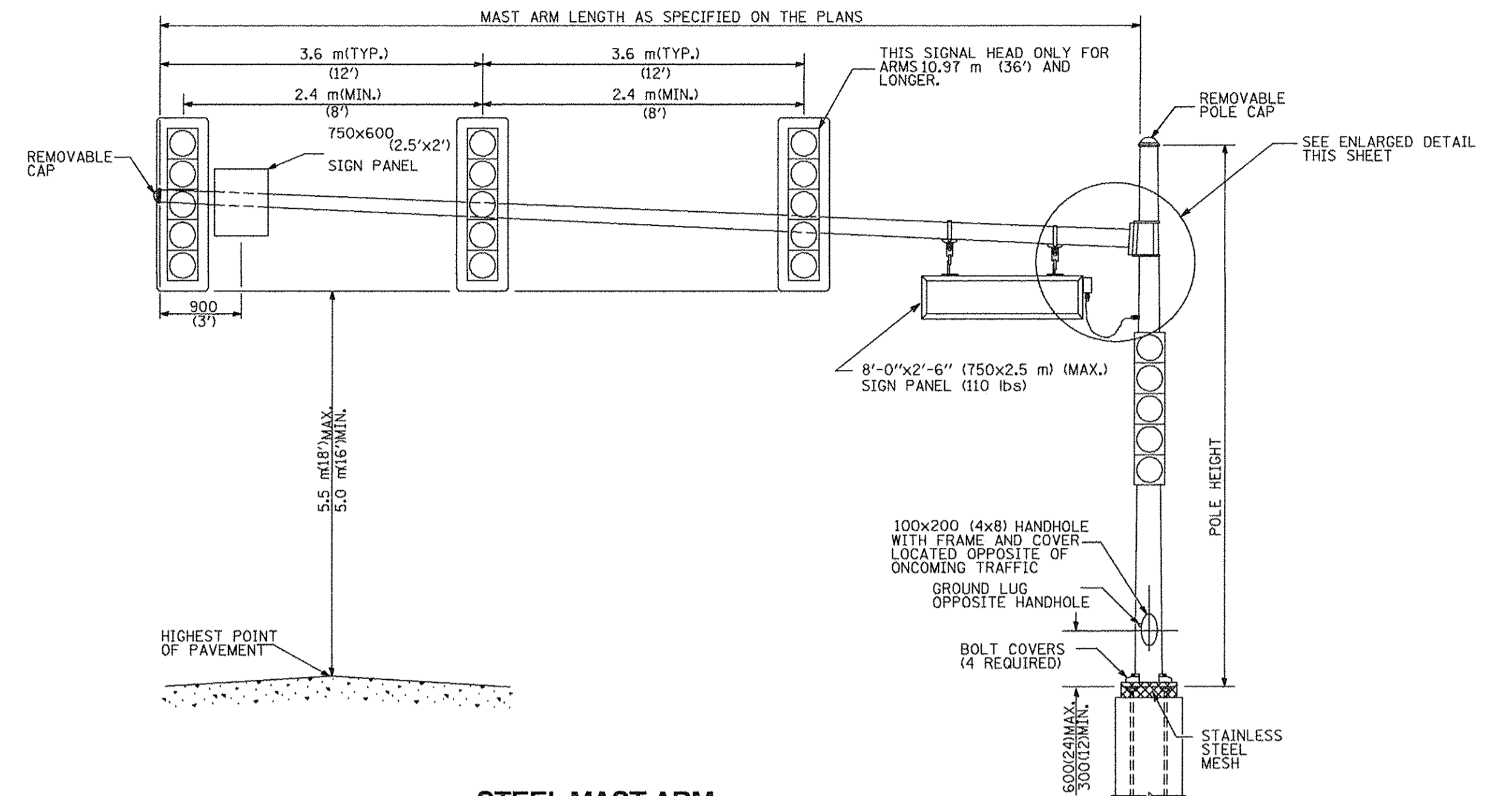
F.A.P. RTE. 353	SECTION 13-00063-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 30
CONTRACT NO. 61C11			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(216)	

TS 1450

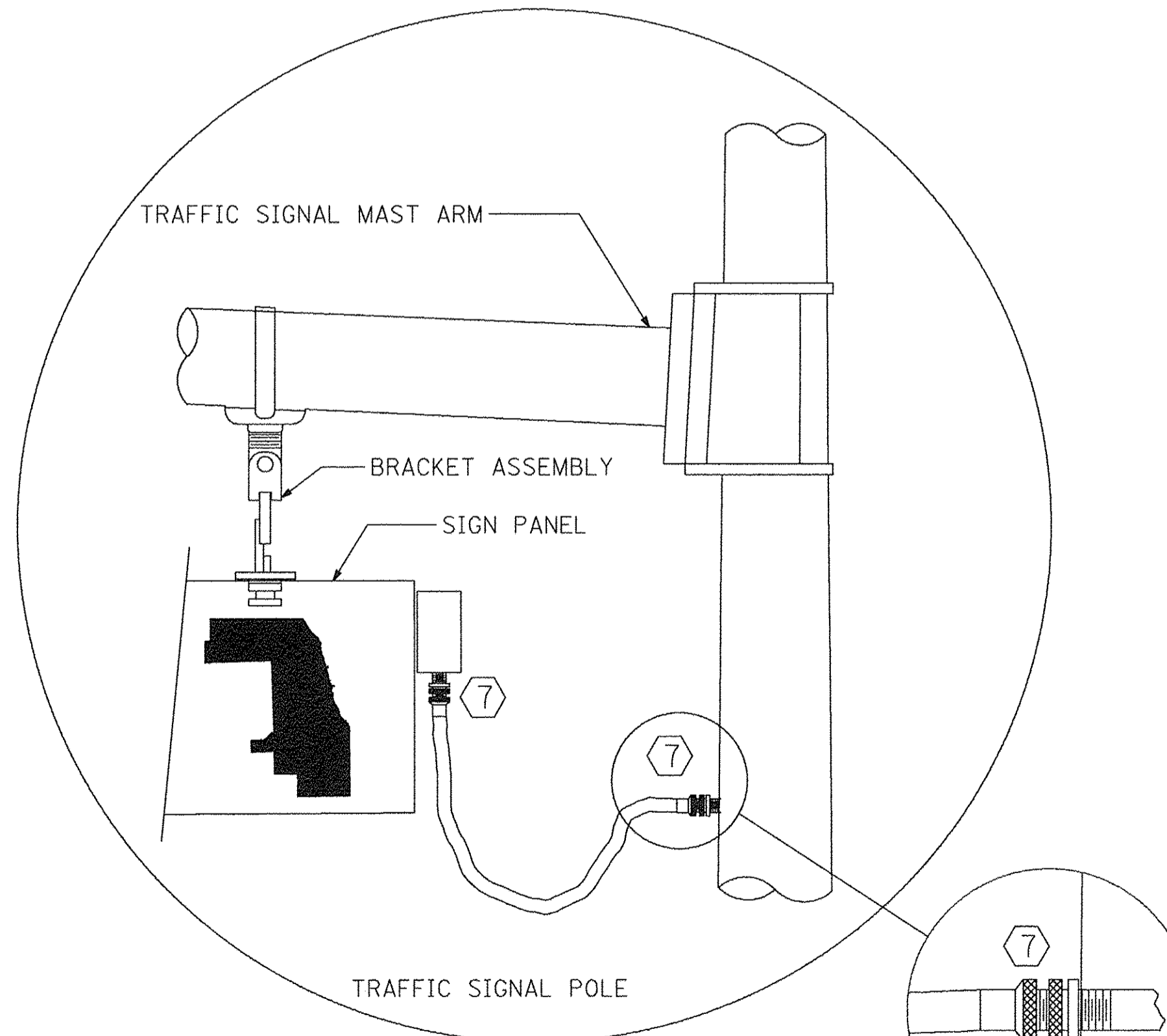
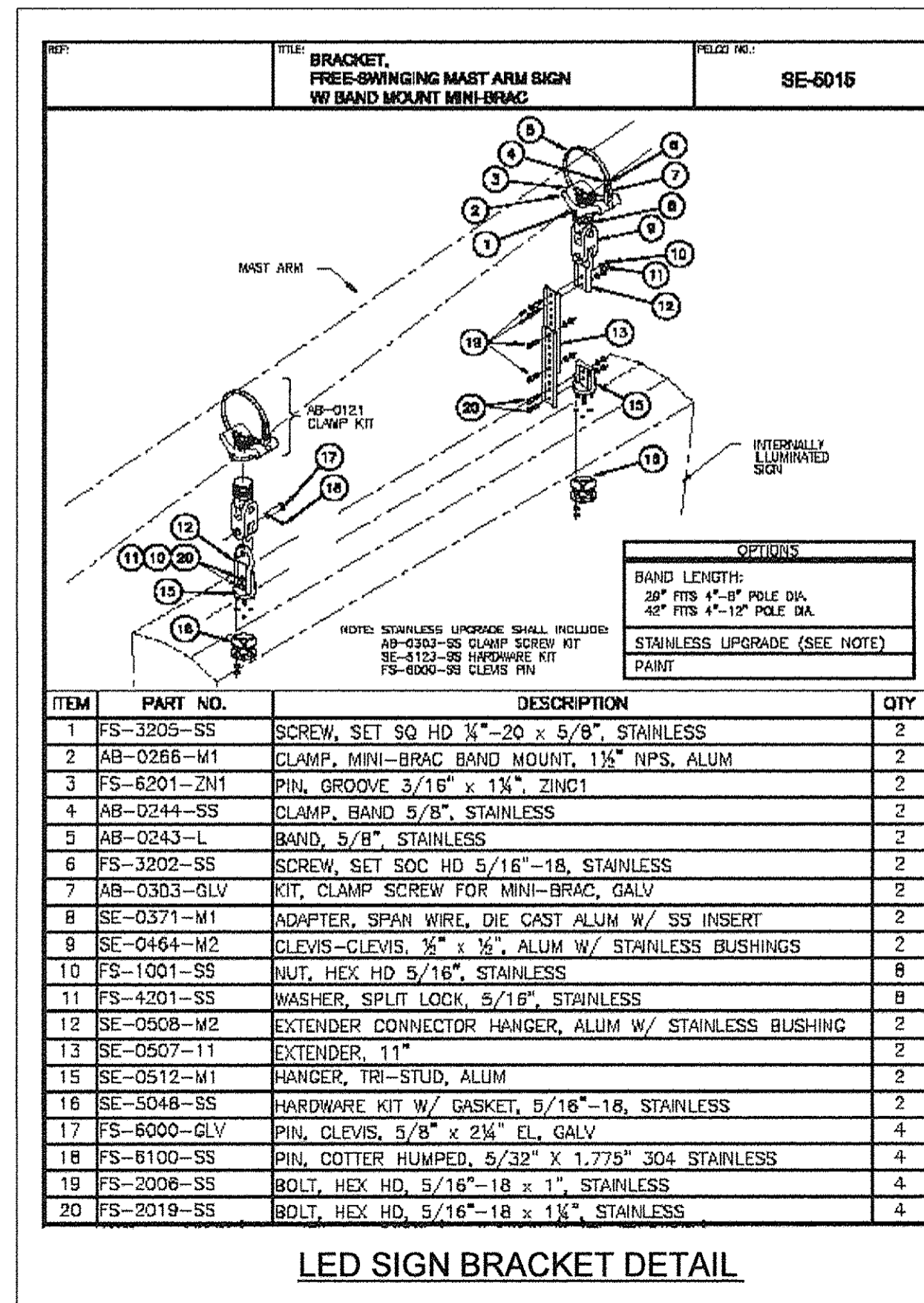


LED SIGN WIRING DETAIL

DESCRIPTION	MANUFACTURER	MODEL	NOTES
1. CIRCUIT BREAKER		15 AMPERE	
2. TERMINAL BLOCK	MARATHON	1502 DJSV	
3. PHOTO ELECTRIC CONTROL	FISHER PIERCE	B124-1.5-07762	
4. CONTRACTOR (INDUSTRIAL CONTROL RELAY)	SQUARE D	8501X020V02	BOLT ON W/ SCREW TERMINAL
5. IN-LINE FUSE HOLDER WITH 5 AMP FUSE	BUSSMANN	S-8000 BK/S-8-3-4-R	
6. ELECTRIC CABLE, No. 14, 3/C (BLACK, WHITE, GREEN)	CAROLPRENE /SOOW	02762	
7. CORD/CABLE CONNECTOR	APPLETON	CG5050S (STEEL)	
8. SIGN MOUNTING HARDWARE	PELCO	SE-5015	



STEEL MAST ARM ASSEMBLY AND POLE



LED SIGN ENLARGED CABLE CONNECTOR DETAIL

LED SIGN ENLARGED CABLE CONNECTOR DETAIL

- GENERAL NOTE:
- SIGNAL HEADS, SIGN PANELS, AND OTHER ATTACHMENTS ARE SHOWN FOR MINIMUM DESIGN LOADING PURPOSES ONLY. EACH SIGNAL HEAD SHALL WEIGH 36 Kg (80 lb) AND HAVE A PROJECTED AREA OF 1.37 sq. m (14.7 sq. ft.).
 - PHOTO ELECTRIC CELL IS TO BE MOUNTED ABOVE CABINET DOOR.
 - THE SIGN SHALL BE LOCATED AT A MAXIMUM OF 8' FROM CENTER OF SIGN TO POLE.
 - SIGN IS TO BE MOUNTED A MINIMUM OF 16' ABOVE PAVEMENT.

FILE NAME = 13375_02-SGNL-DTLS-04-IDOT P01

USER NAME =	DESIGNED = EMA	REVISED =
PLOT SCALE =	CHECKED = PKB	REVISED =
PLOT DATE = 05-19-16	DRAWN = K.W.M.	REVISED =
	CHECKED = APG	REVISED =

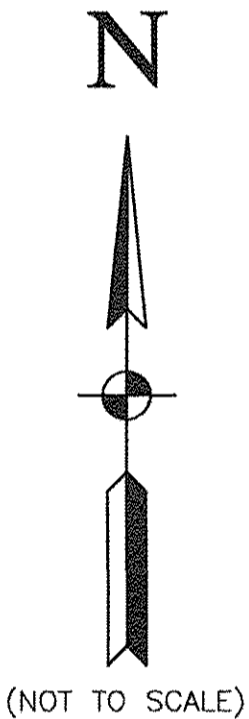
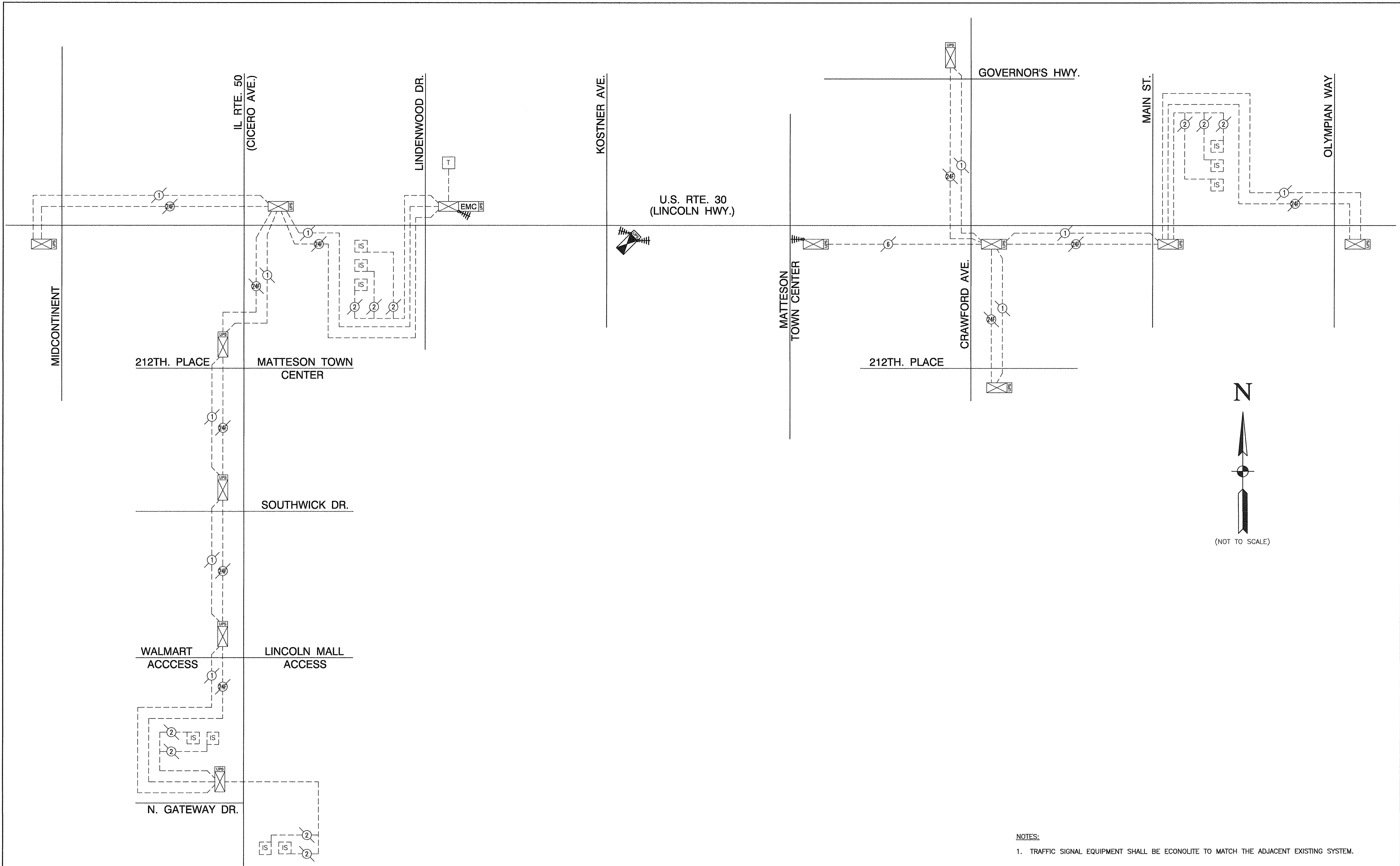
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE
 INTERSECTION IMPROVEMENTS
 ILLUMINATED STREET NAME SIGN MOUNTING DETAIL

SCALE: SHEET NO. 30A OF 63 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	13-00063-00-CH	COOK	63	30A
CONTRACT NO. 61C11				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(216)				

TS 1450

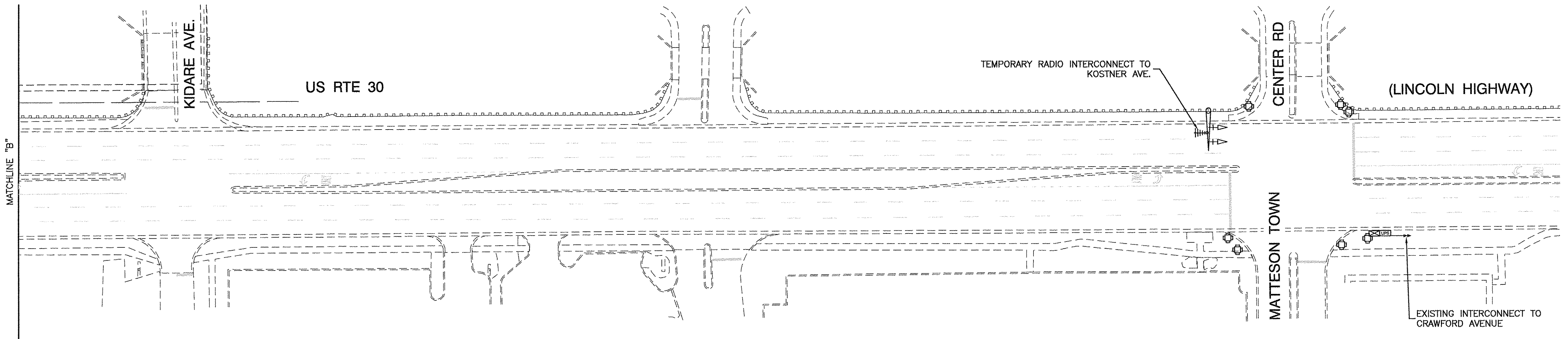
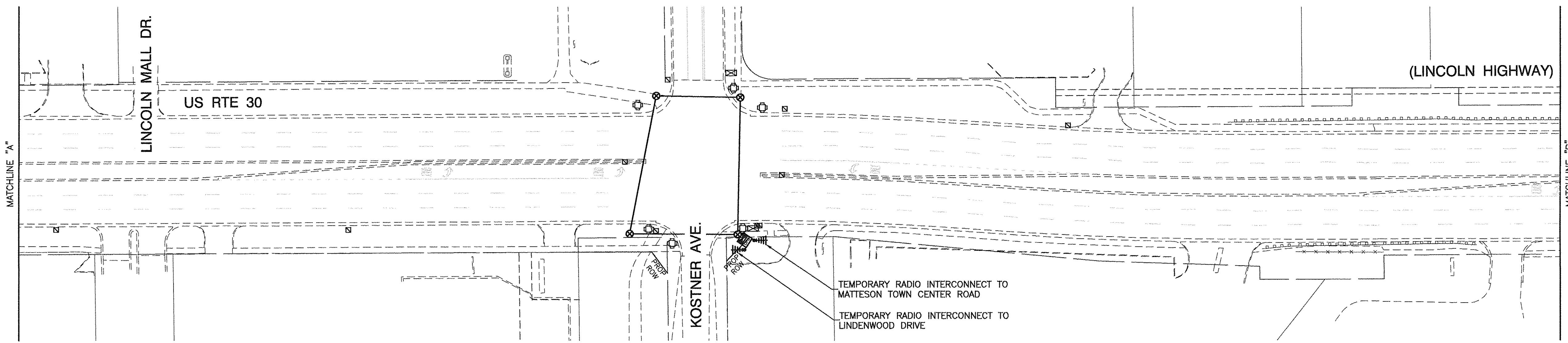
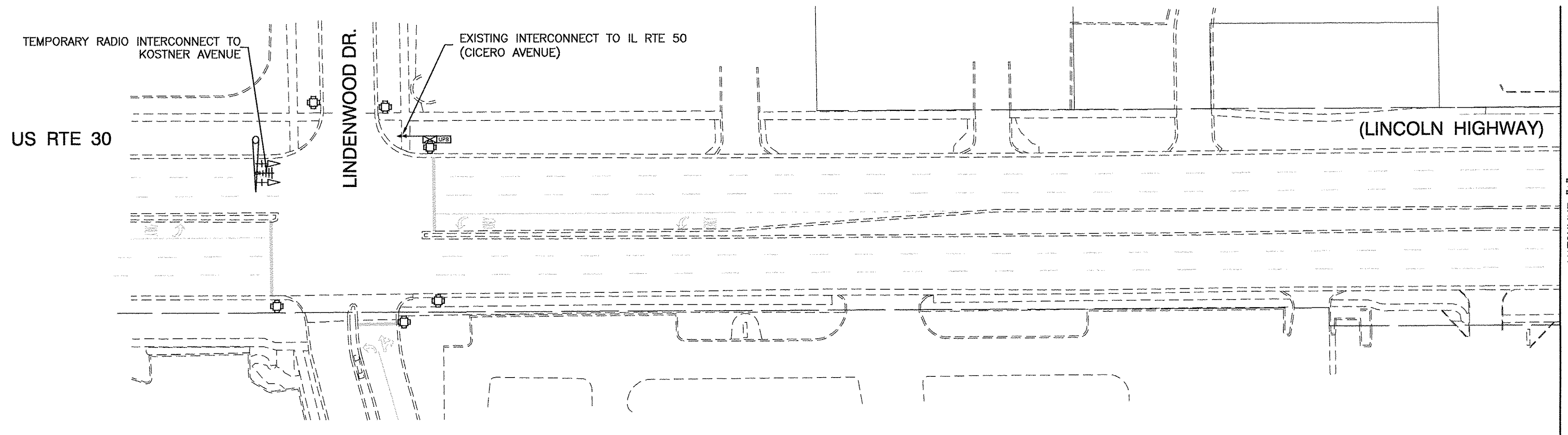
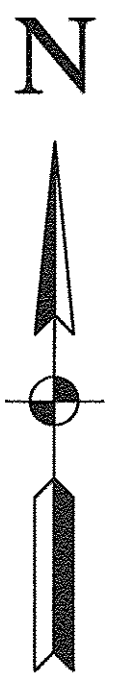


(NOT TO SCALE)

- NOTES:
 1. TRAFFIC SIGNAL EQUIPMENT SHALL BE ECONOLITE TO MATCH THE ADJACENT EXISTING SYSTEM.

ECON 54

FILE NAME = 13375_02-SGNL-04 - TEMP-INCON	USER NAME =	DESIGNED — EMA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE INTERSECTION IMPROVEMENTS TEMPORARY RADIO INTERCONNECT SCHEMATIC			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE =	CHECKED — PKB	REVISED —					353	13-00063-00-CH	COOK	63	31
	PLOT DATE = 05-19-16	DRAWN — JJB	REVISED —					CONTRACT NO. 61C11				
		CHECKED — EA	REVISED —					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(216)				
				SCALE: NONE		SHEET NO. 31 OF 63 SHEETS		STA. TO STA.				



FILE NAME = 13375_02-SGNL-INTR-01 - TEMP-INTR

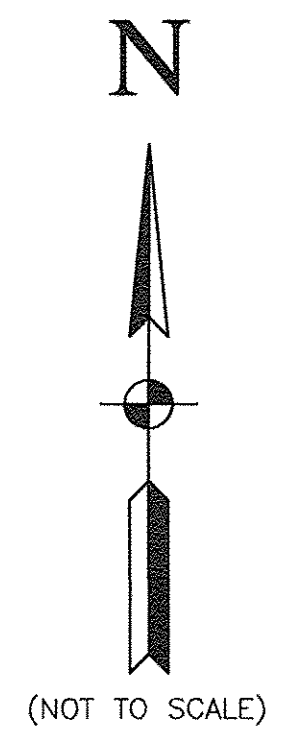
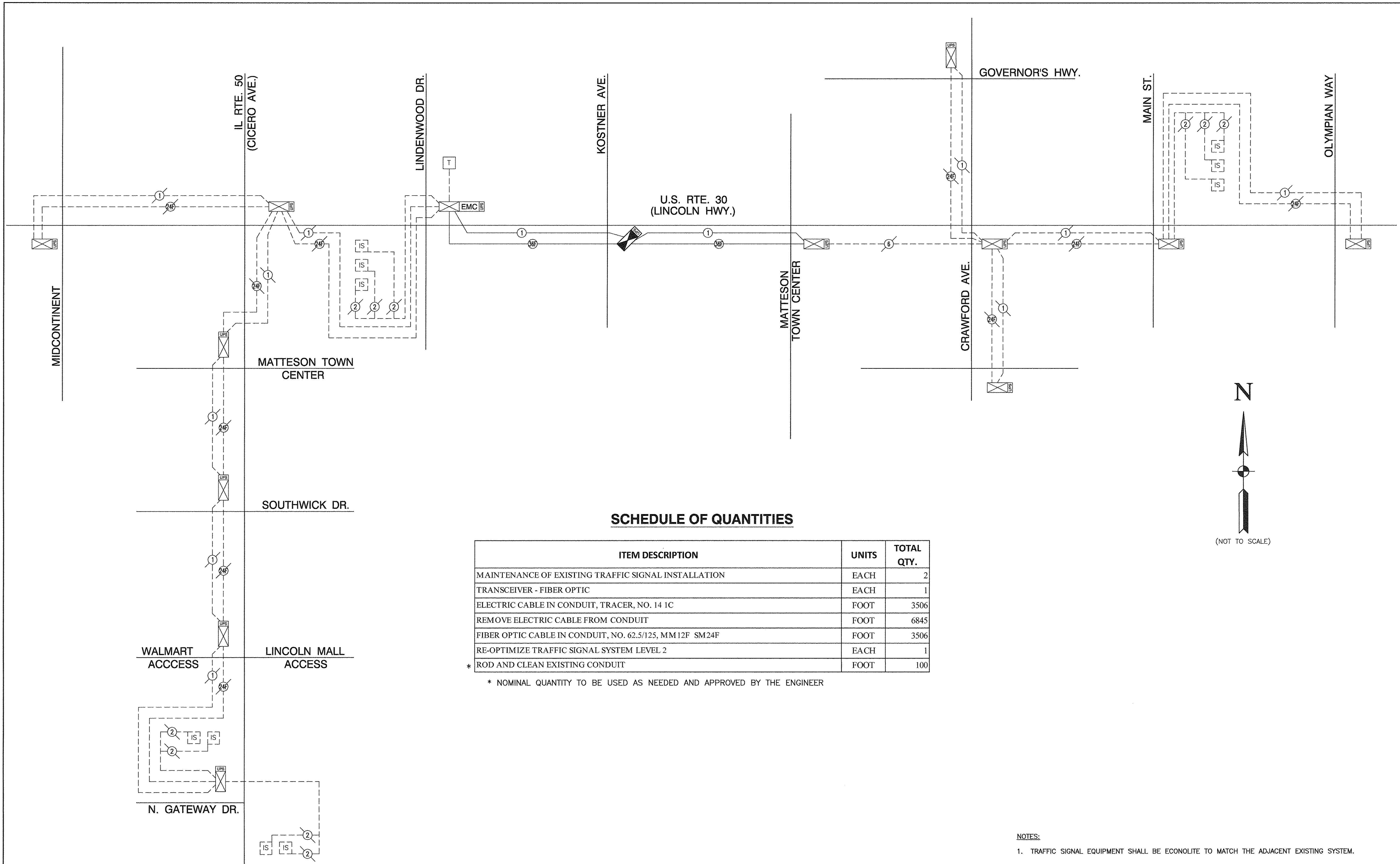
USER NAME =	DESIGNED — EMA	REVISED —
	CHECKED — PKB	REVISED —
PLOT SCALE =	DRAWN — JJB	REVISED —
PLOT DATE = 05-19-16	CHECKED — EA	REVISED —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE INTERSECTION IMPROVEMENTS TEMPORARY RADIO INTERCONNECT PLAN		
SCALE: 1' = 50'	SHEET NO. 32 OF 63 SHEETS	STA. TO STA.

F.A.P. RTE. 353	SECTION 13-00083-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 32
CONTRACT NO. 61C11				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(216)				

ECON 54



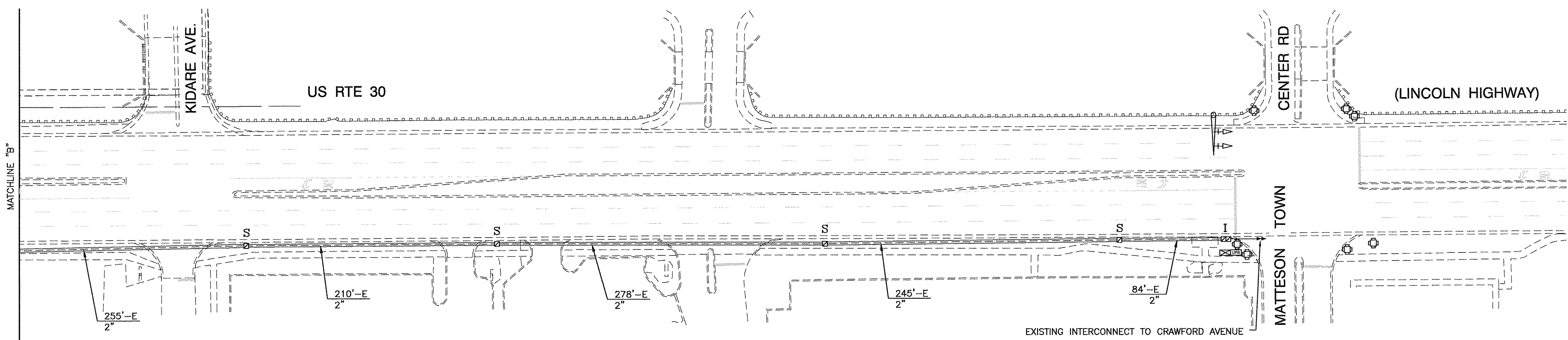
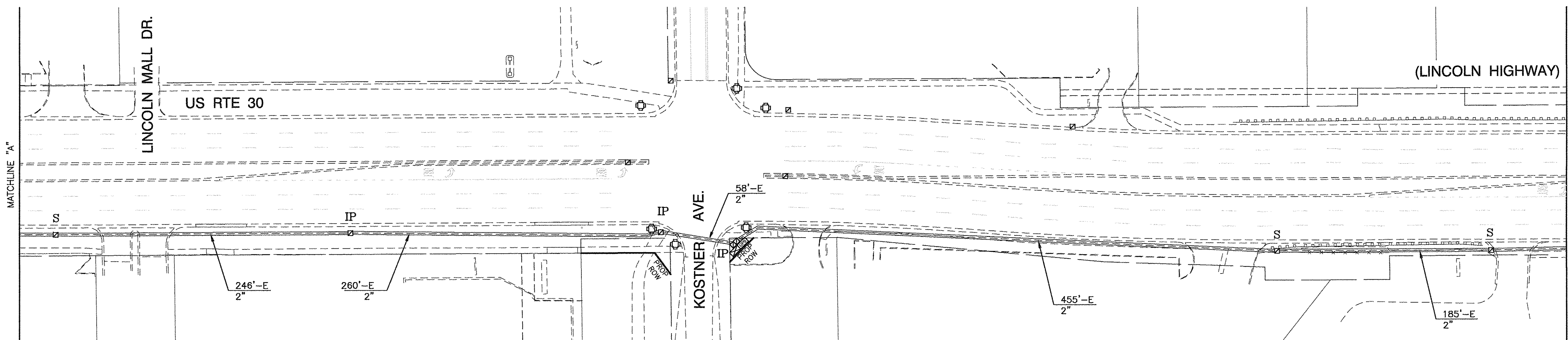
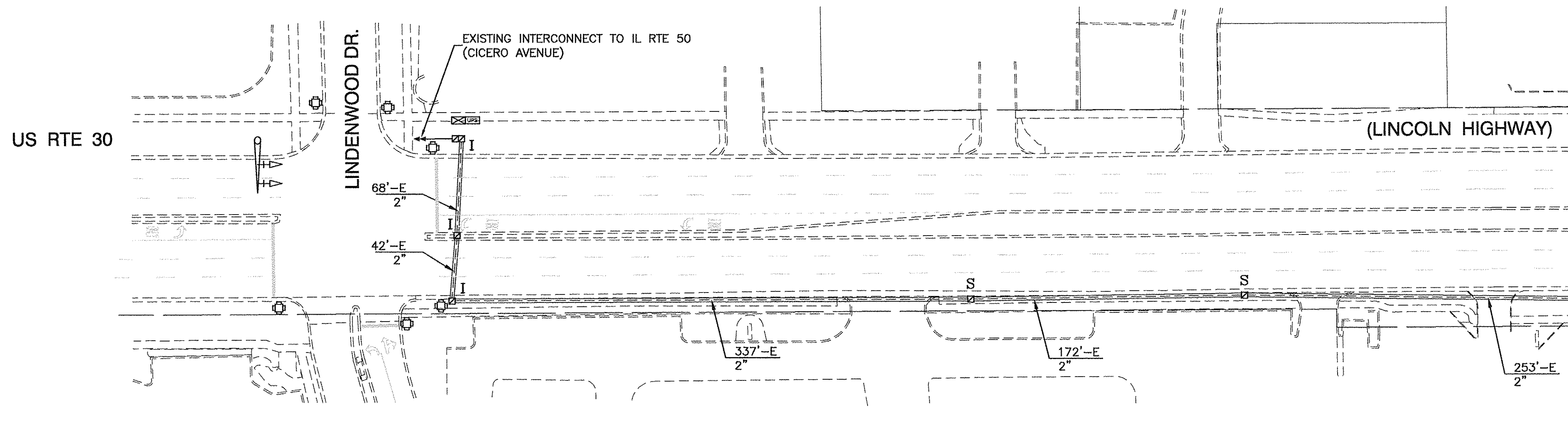
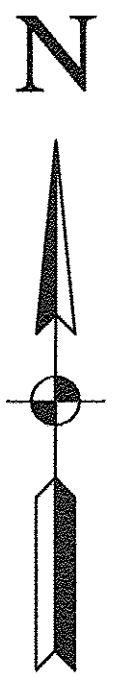
SCHEDULE OF QUANTITIES

ITEM DESCRIPTION	UNITS	TOTAL QTY.
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2
TRANSCIVER - FIBER OPTIC	EACH	1
ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	3506
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	6845
FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM24F	FOOT	3506
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 2	EACH	1
* ROD AND CLEAN EXISTING CONDUIT	FOOT	100

* NOMINAL QUANTITY TO BE USED AS NEEDED AND APPROVED BY THE ENGINEER

NOTES:

1. TRAFFIC SIGNAL EQUIPMENT SHALL BE ECONOLITE TO MATCH THE ADJACENT EXISTING SYSTEM.



FILE NAME = 13375_02-SGNL-INTR-02 - EX-INTR

USER NAME =	DESIGNED -- EMA	REVISD --
	CHECKED -- PKB	REVISD --
PLOT SCALE =	DRAWN -- JJB	REVISD --
PLOT DATE = 05-19-16	CHECKED -- EA	REVISD --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE
INTERSECTION IMPROVEMENTS
EXISTING INTERCONNECT PLAN

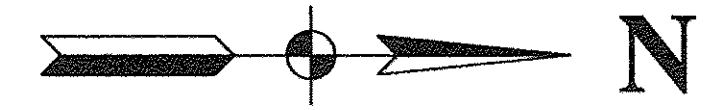
SCALE: 1" = 50'

SHEET NO. 34 OF 63 SHEETS

STA. TO STA.

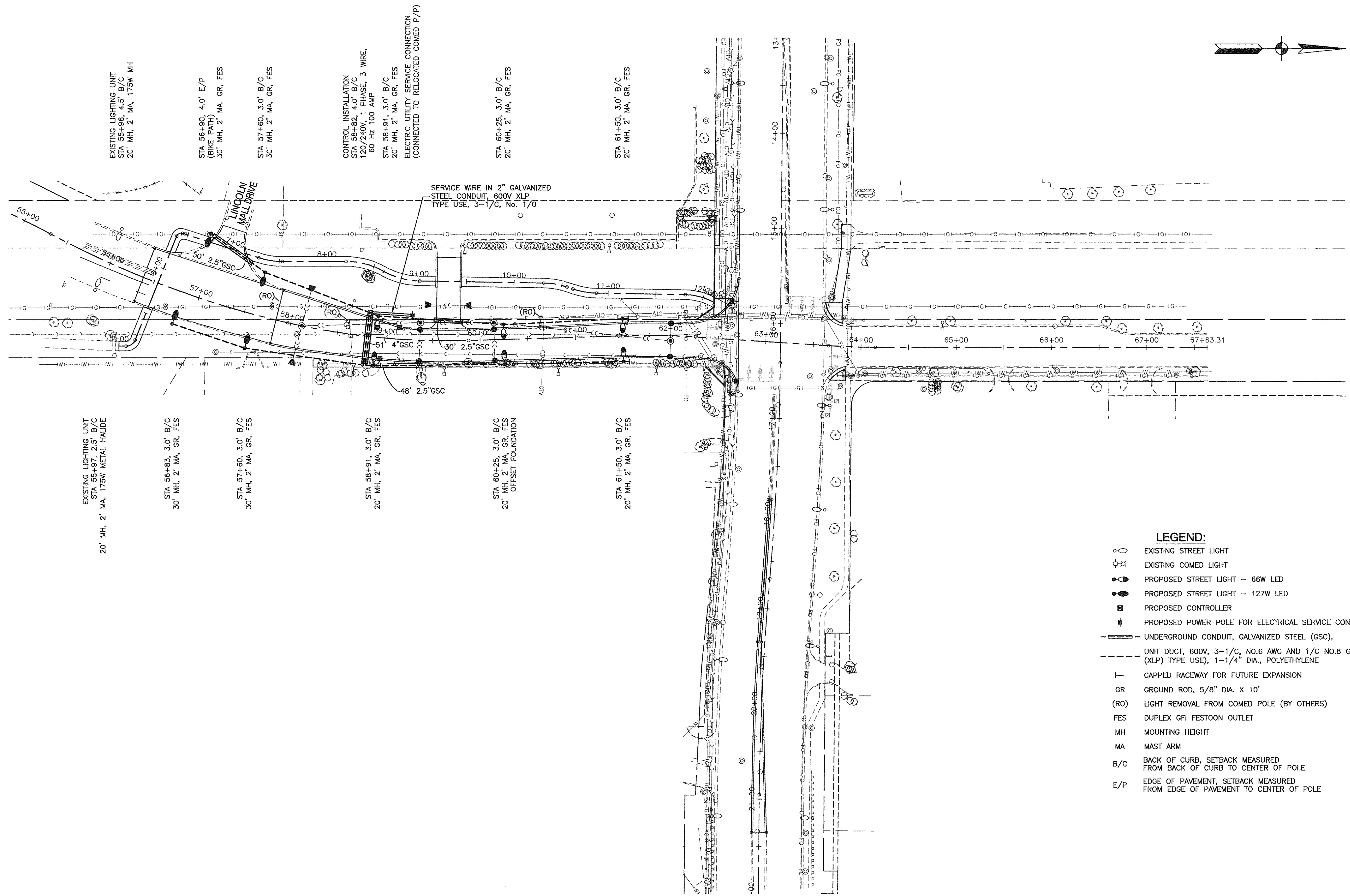
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	13-00063-00-CH	COOK	63	34
CONTRACT NO. 61C11				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(216)				

ECON 54



PLAN	SURVEYED	BY	DATE
	PLANNED		
NOTE BOOK NO.	CHECKED		
	FILE NAME		

PROFILE	SURVEYED	BY	DATE
	PLANNED		
NOTE BOOK NO.	CHECKED		
	FILE NAME		



EXISTING LIGHTING UNIT
STA 56+97, 2.5' B/C
20' MH, 2' MA, 175W METAL HALIDE

STA 56+83, 3.0' B/C
30' MH, 2' MA, GR, FES

STA 57+60, 3.0' B/C
30' MH, 2' MA, GR, FES

STA 58+91, 3.0' B/C
20' MH, 2' MA, GR, FES

STA 60+25, 3.0' B/C
20' MH, 2' MA, GR, FES
OFFSET FOUNDATION

STA 61+50, 3.0' B/C
20' MH, 2' MA, GR, FES

EXISTING LIGHTING UNIT
STA 56+96, 4.5' B/C
20' MH, 2' MA, 175W MH

STA 56+90, 4.0' E/P
(BIKE PATH)
30' MH, 2' MA, GR, FES

STA 57+60, 3.0' B/C
30' MH, 2' MA, GR, FES

CONTROL INSTALLATION
STA 58+82, 4.0' B/C
120/240V, 1 PHASE, 3 WIRE,
60 Hz 100 AMP

STA 58+91, 3.0' B/C
20' MH, 2' MA, GR, FES
ELECTRIC UTILITY SERVICE CONNECTION
(CONNECTED TO RELOCATED COMED P/P)

STA 60+25, 3.0' B/C
20' MH, 2' MA, GR, FES

STA 61+50, 3.0' B/C
20' MH, 2' MA, GR, FES

- LEGEND:**
- EXISTING STREET LIGHT
 - ⊕ EXISTING COMED LIGHT
 - PROPOSED STREET LIGHT - 66W LED
 - PROPOSED STREET LIGHT - 127W LED
 - ⊞ PROPOSED CONTROLLER
 - ⊞ PROPOSED POWER POLE FOR ELECTRICAL SERVICE CONNECTION
 - GSC UNDERGROUND CONDUIT, GALVANIZED STEEL (GSC)
 - UNIT DUCT, 600V, 3-1/C, NO.6 AWG AND 1/C NO.8 GROUND, (XLP) TYPE USE, 1-1/4" DIA., POLYETHYLENE
 - ┌ CAPPED RACEWAY FOR FUTURE EXPANSION
 - GR GROUND ROD, 5/8" DIA. X 10'
 - (RO) LIGHT REMOVAL FROM COMED POLE (BY OTHERS)
 - FES DUPLEX GFI FESTOON OUTLET
 - MH MOUNTING HEIGHT
 - MA MAST ARM
 - B/C BACK OF CURB, SETBACK MEASURED FROM BACK OF CURB TO CENTER OF POLE
 - E/P EDGE OF PAVEMENT, SETBACK MEASURED FROM EDGE OF PAVEMENT TO CENTER OF POLE

FILE NAME = 13375_02-LIGHT-01 - LT-1



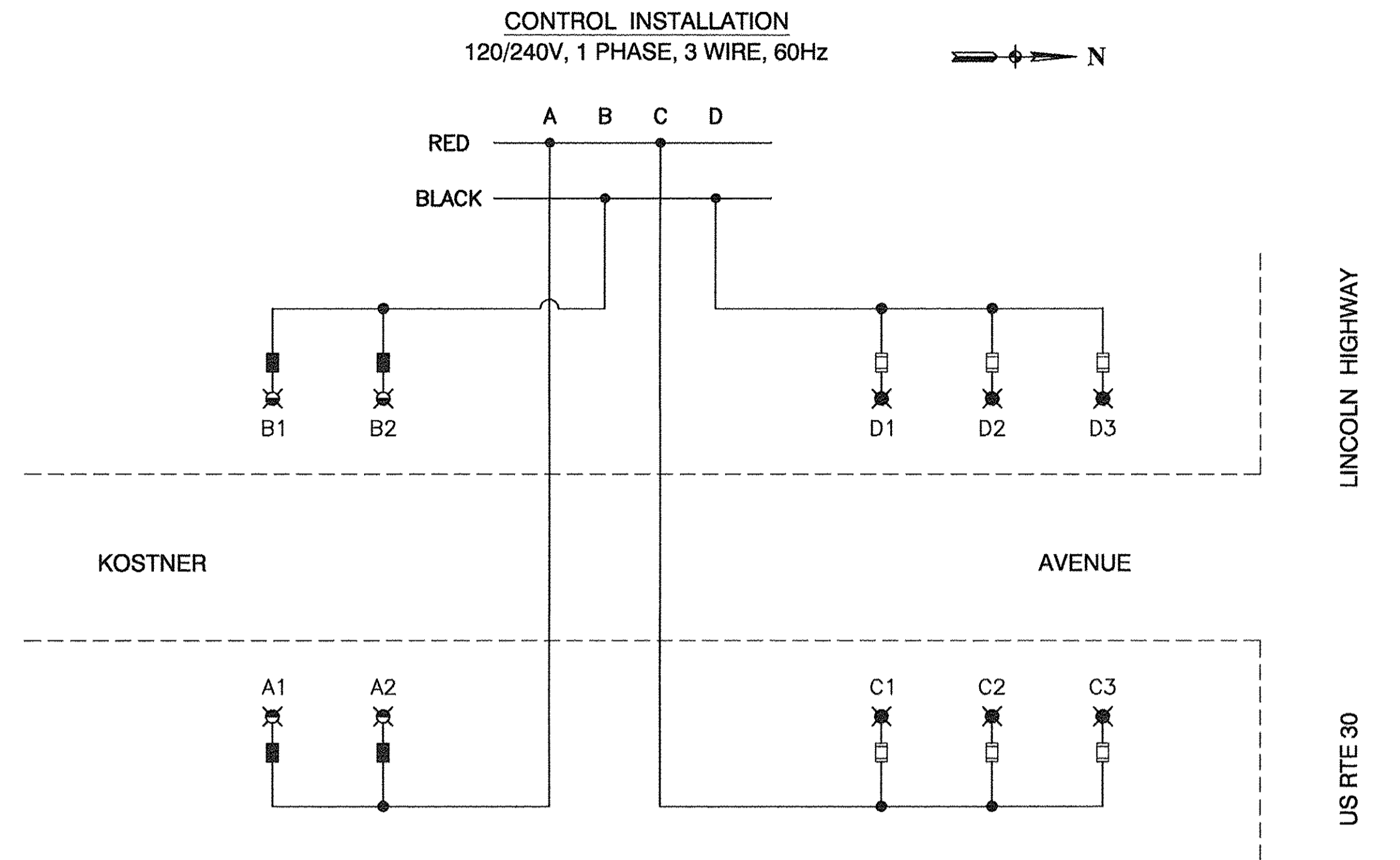
USER NAME =	DESIGNED - EMA	REVISED -
	CHECKED - PKB	REVISED -
PLOT SCALE =	DRAWN - PS	REVISED -
PLOT DATE = 05-19-16	CHECKED - AG	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE
INTERSECTION IMPROVEMENTS
STREET LIGHTING PLAN

SCALE: SHEET NO. 35 OF 63 SHEETS STA. TO STA.

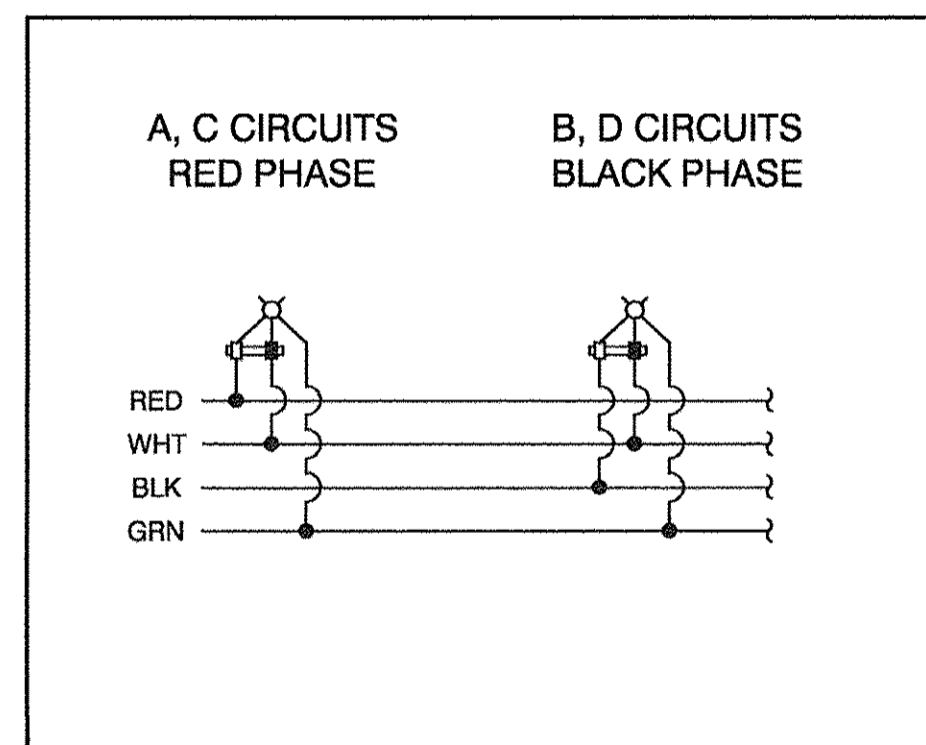
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	13-00063-00-CH	COOK	63	35
CONTRACT NO. 61C11				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	M-4003(216)	



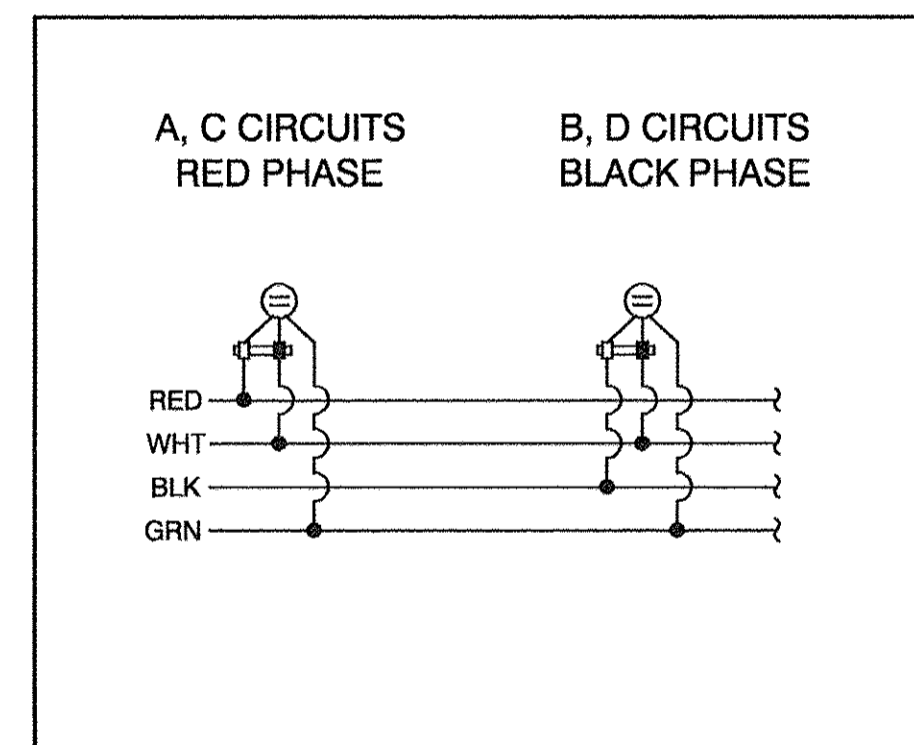
LOAD TABULATIONS CONTROLLER #1

CIRCUIT	WATTS	AMPS @ 120V	
		RED PHASE	BLACK PHASE
A	254	2.12	
B	254		2.12
C	198	1.65	
D	198		1.65
SUBTOTAL	904	3.77	3.77
TOTAL AMPS @ 120 V		7.54	

TYPICAL LUMINAIRE WIRING

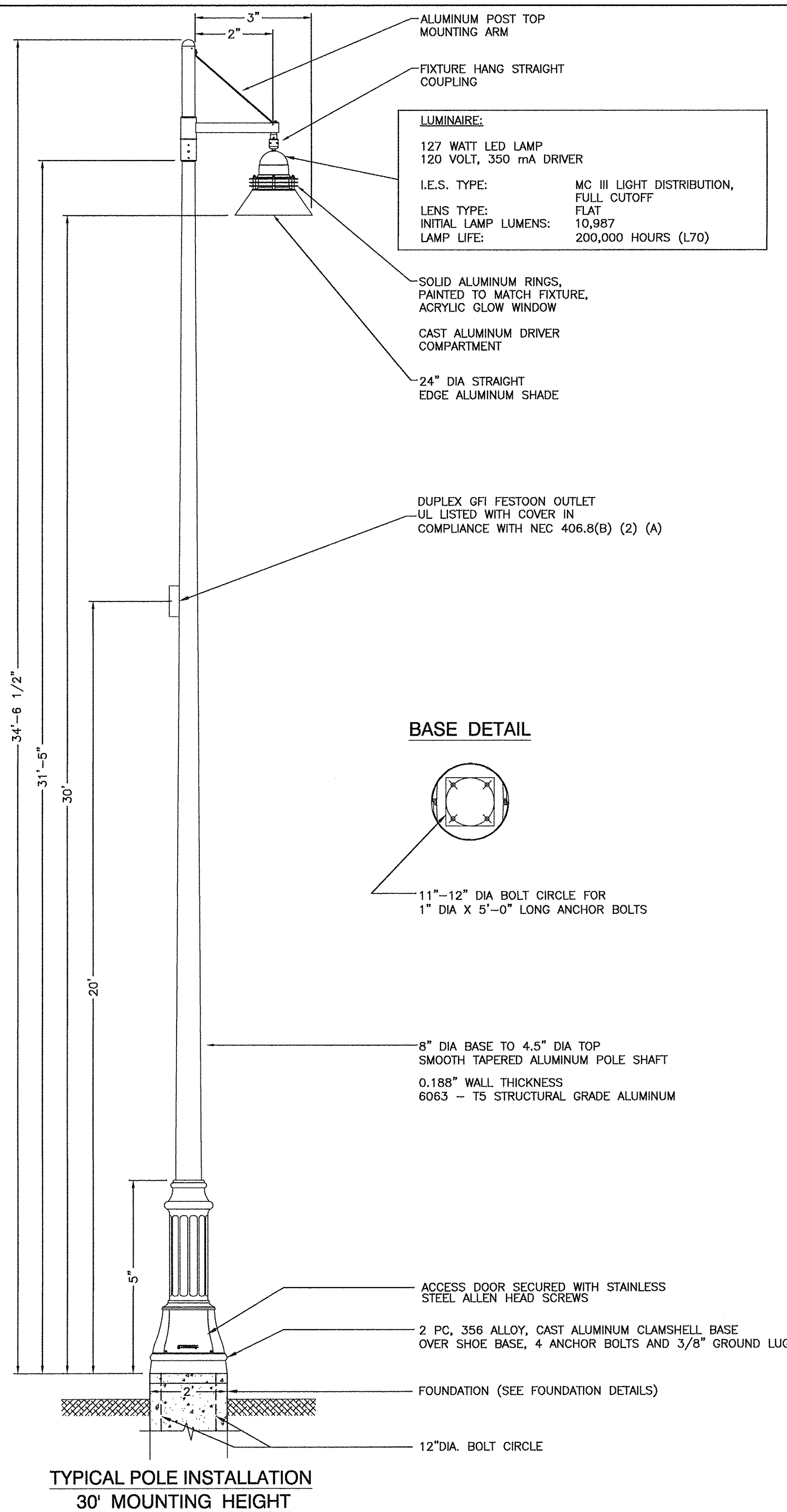
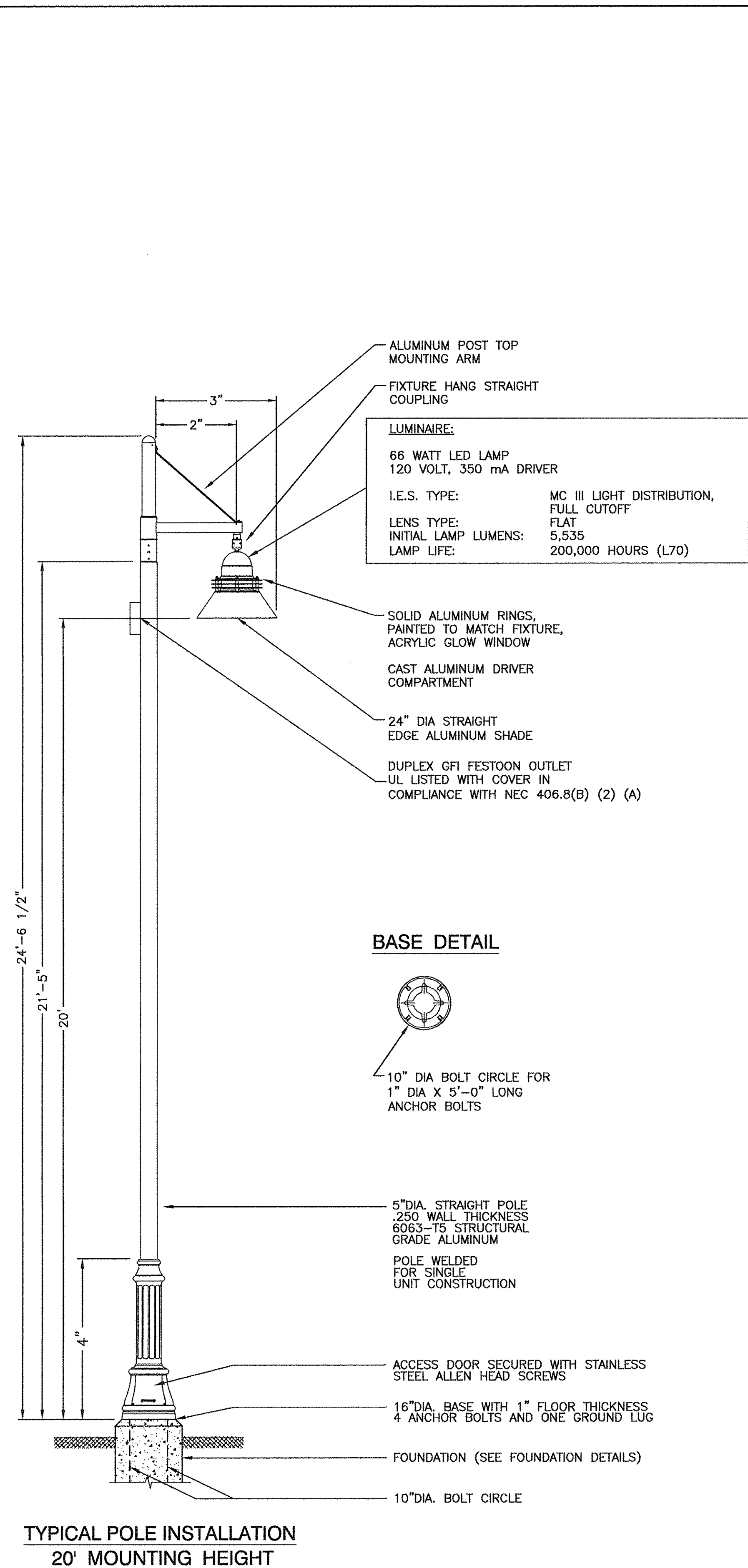


TYPICAL FESTOON WIRING

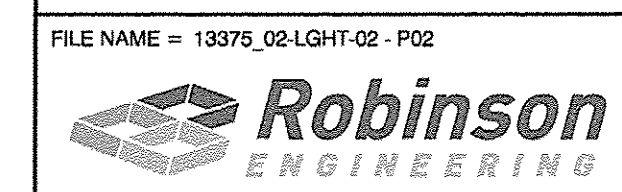


LEGEND

- ⊗ LUMINAIRE, 127W LED
- ⊗ LUMINAIRE, 66W LED
- FUSE, 2.5 AMP LUMINAIRE, 6.5 AMP FESTOON
- FUSE, 1.5 AMP LUMINAIRE, 6.5 AMP FESTOON
- A1 LUMINAIRE CIRCUIT
- ⌋ CIRCUIT BREAKER
- CONNECTION



- NOTES:**
1. THE LIGHTING UNITS SHALL MEET AASHTO DESIGN CRITERIA. DESIGN FOR 90 M.P.H. WIND WITH 30% GUST AND 75 POUND LUMINAIRE HAVING AN E.P.A. OF 1.6 SQ. FT. AND PROPER ICE LOADING.
 2. LIGHT POLE AND ASSOCIATED EQUIPMENT TO BE U.L. LISTED



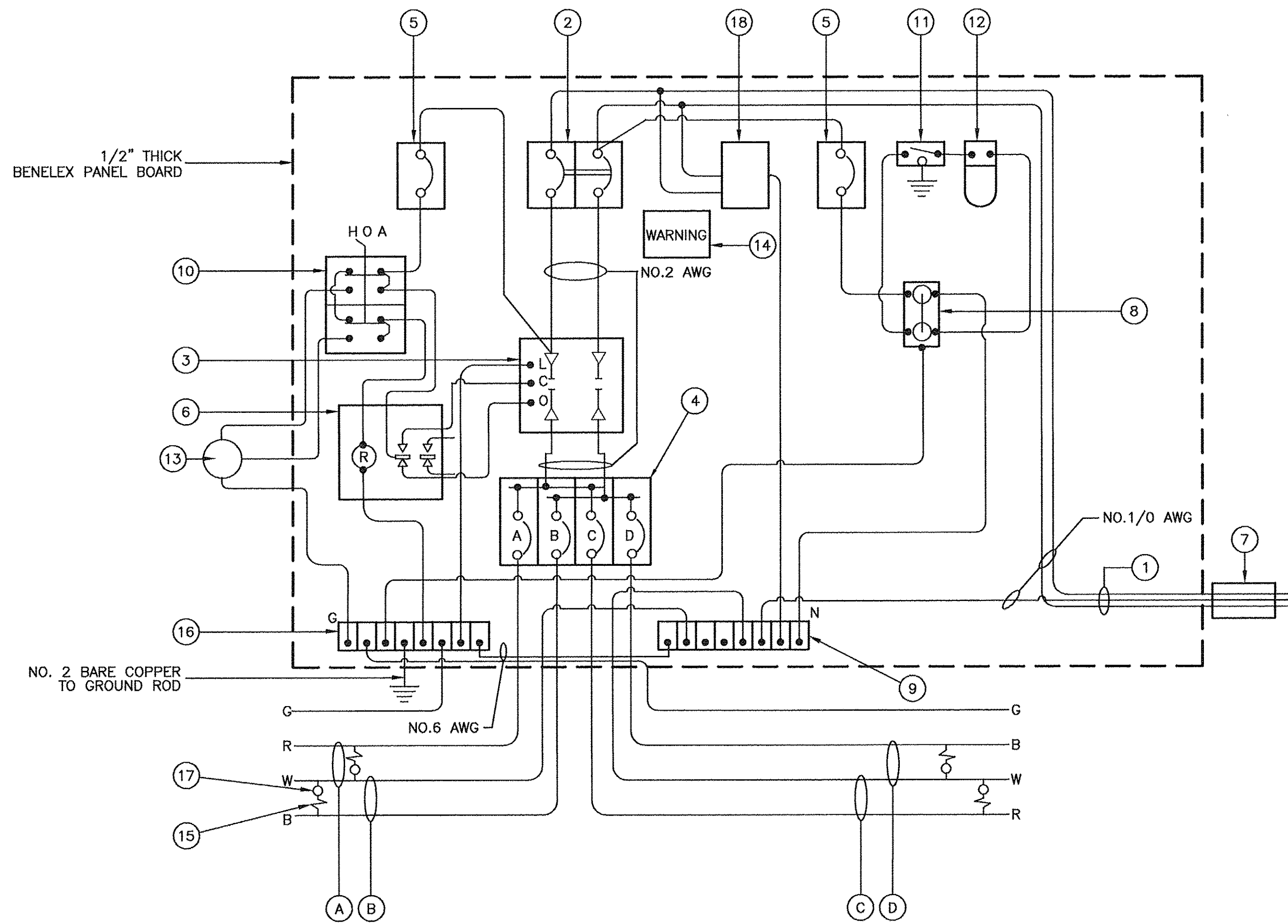
FILE NAME = 13375_02-LGHT-02 - P02	USER NAME =	DESIGNED — EMA	REVISED —
		CHECKED — PKB	REVISED —
	PLOT SCALE =	DRAWN — PS	REVISED —
	PLOT DATE = 05-19-16	CHECKED — AG	REVISED —

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE
 INTERSECTION IMPROVEMENTS
 STREET LIGHTING DETAILS

SCALE: SHEET NO. 37 OF 63 SHEETS STA. TO STA.

F.A.P. RTE. 353	SECTION 13-00063-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 37
CONTRACT NO. 61C11				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(216)				



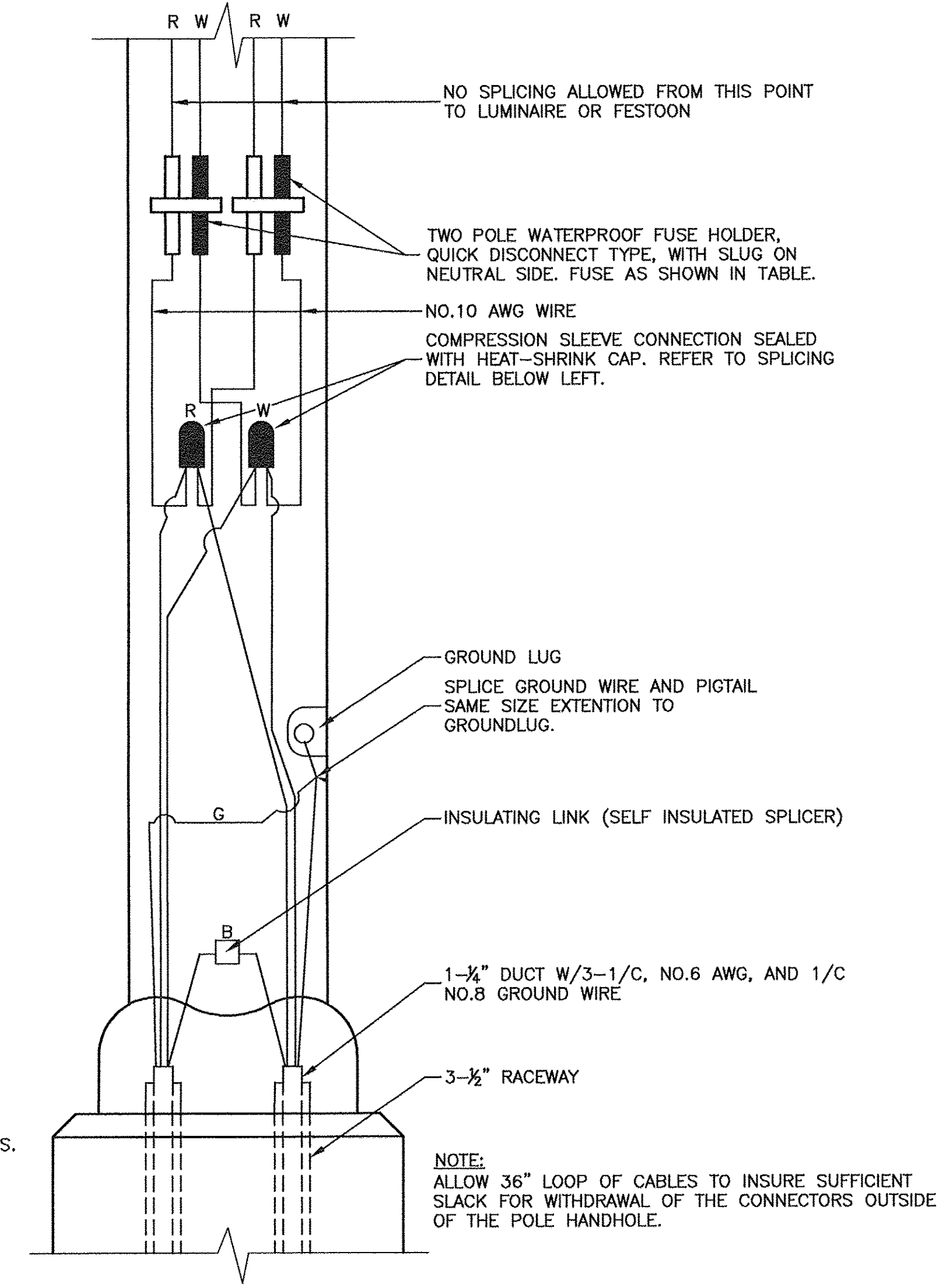
CONTROLLER WIRING DIAGRAM

CONTROLLER WIRING DIAGRAM LEGEND

- ① 3-1/2" DIA. GALVANIZED STEEL CONDUIT FOR 120/240 VOLT, 1Ø, 3 WIRE, 60HZ. SERVICE.
- ② (1) 100 AMP MAIN CIRCUIT BREAKER, 2 POLE, 600 VOLT, 100 AMP BASE, NON-INTERCHANGEABLE TRIP INTERRUPTING RATING NEMA - 14000 AMP AT 480 V.
- ③ (1) 100 AMP REMOTE CONTROL CONTACTOR SWITCH, ELECTRICALLY OPERATED, ELECTRICALLY HELD, 2 POLE, 600 VOLT
- ④ (4) 30 AMP CIRCUIT BREAKER, 1 POLE, 120 VOLT, 100 AMP BASE, NON-INTERCHANGEABLE TRIP RATING NEMA - 14000 AMP AT 240 VOLTS.
- ⑤ (2) 20 AMP CONTROL CIRCUIT-CIRCUIT BREAKER, 1 POLE, 120 VOLT, 100 AMP BASE, NON-INTERCHANGEABLE TRIP INTERRUPTING RATING NEMA 14000 AMP AT 240 V.
- ⑥ (1) 20 AMP, 1 POLE DOUBLE THROW, 120 VOLT RELAY
- ⑦ METER SOCKET, 1Ø, 3 WIRE, 100A
- ⑧ (1) 20 AMP, 120 VOLT DUPLEX GFCI RECEPTACLE MOUNTED IN BOX.
- ⑨ NEUTRAL BUS BAR, 1/4"x1"x12" MINIMUM LENGTH MOUNTED ON PANEL WITH LUGS.
- ⑩ 3 POSITION SELECTOR SWITCH, 120V.
- ⑪ SWITCH FOR LIGHTING FIXTURE MOUNTED IN BOX, 20 AMP.
- ⑫ WEATHER-PROOF LIGHTING FIXTURE WITH 1600 LUMEN, 120 V LED LAMP.
- ⑬ PHOTOCELL MOUNTED TO CABINET, 120 V, 1000 VA BALLAST RATING, 1-4 FC ON 3-12 FC OFF, 30 SECOND MINIMUM DELAY.
- ⑭ WARNING PLATE TO READ: WARNING, MAINTENANCE CIRCUIT IS LIVE WHEN MAIN BREAKER IS SWITCHED OFF
- ⑮ IN-LINE FUSEHOLDER WITH FUSE AS NOTED IN FUSE TABLE
- ⑯ GROUND BUS BAR 1/4"x1"x12" MINIMUM LENGTH MOUNTED ON PANEL WITH LUGS
- ⑰ LUMINAIRE OR FESTOON OUTLET
- ⑱ SURGE ARRESTER, UL LISTED, NEC 280 COMPLIANT, 1Ø, 3 WIRE 60 HZ. MAX. 3,405 JOULES
- (A) CIRCUIT (RED)
- (B) CIRCUIT (BLACK)
- (C) CIRCUIT (RED)
- (D) CIRCUIT (BLACK)

NOMINAL WATTAGE	FUSE SIZE
66	1.5 AMP
127	2.5 AMP
FESTOON	6.5 AMP

LUMINAIRE FUSE SIZE TABLE



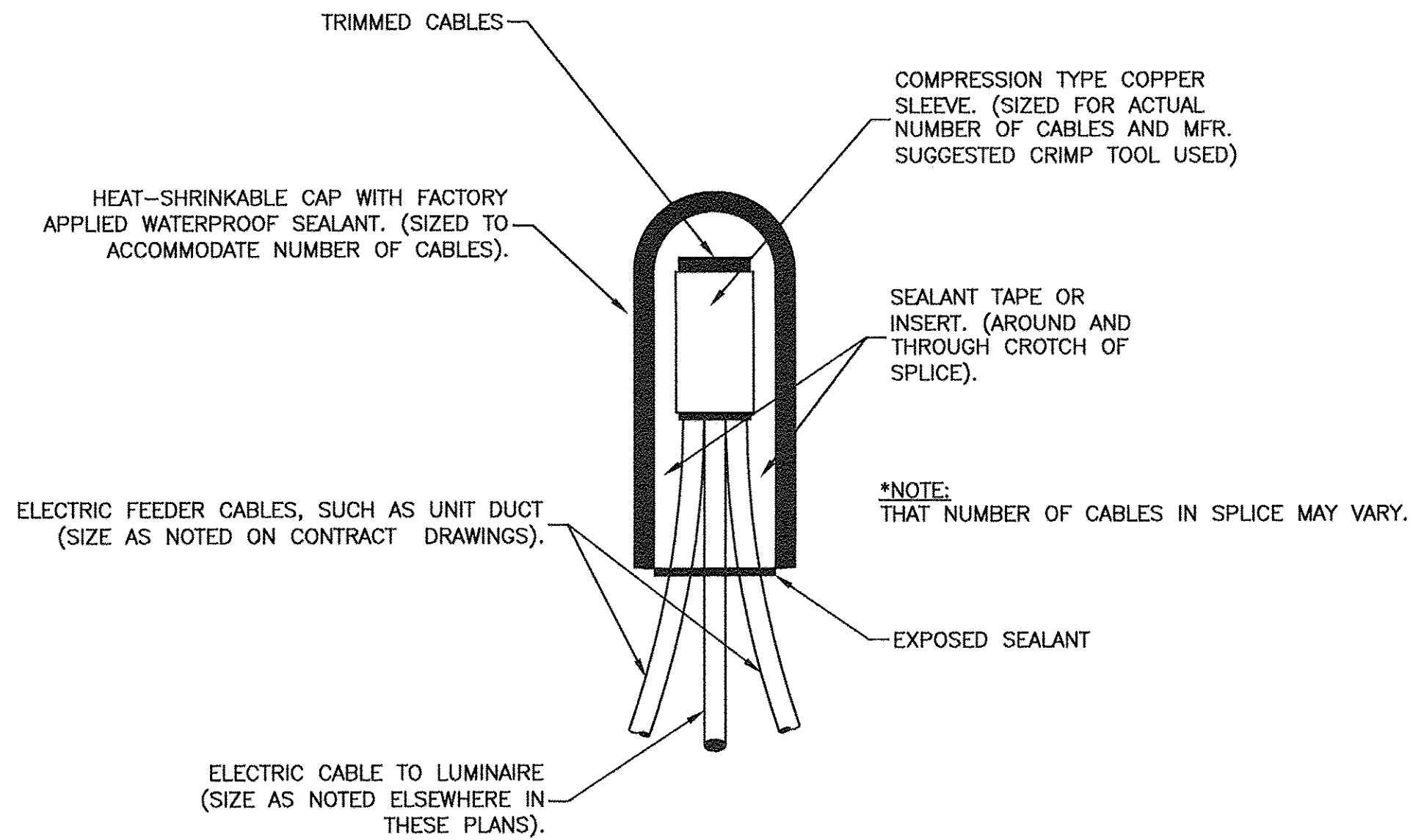
POLE HANDHOLE WIRING DIAGRAM
(TYPICAL FOR SINGLE LUMINAIRE INSTALLATION)
(RED PHASE SHOWN)

GENERAL NOTES FOR CONTROL CABINET

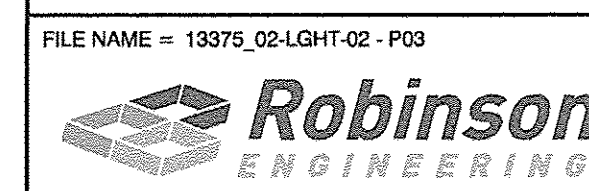
- ENTIRE CONTROL CABINET SHALL BE GROUNDED.
- ALL WIRING SHALL BE TAGGED WITH SELF-STICKING WIRE MARKERS.
- GROUND BUS TO BE COLOR CODED GREEN, NEUTRAL BUS WHITE, AND BONDED TO CABINET ENCLOSURE, BY LISTED PRESSURE CONNECTORS OR LISTED CLAMPS.
- ALL INTERNAL CONTROLLER WIRING TO BE NO.12 AWG UNLESS OTHERWISE SPECIFIED.
- CABINET WIRING INSULATION TO BE TYPE XHHW OR APPROVED EQUAL.
- THE CONTROLLER SHALL BE UL LISTED, NEMA 3R AND SUITABLE FOR USE AS SERVICE ENTRANCE

LIGHTING GENERAL NOTES

- ALL WORK TO CONFORM TO THE MOST RECENT NATIONAL ELECTRICAL CODE AND ANY APPLICABLE LOCAL CODES.
- CONTRACTOR TO VERIFY LOCATION OF ALL UNDERGROUND UTILITIES BEFORE TRENCHING OR AUGERING.
- BEFORE INSTALLING STANDARDS NEAR OVERHEAD FACILITIES CALL C.E. Co. FOR APPROVAL OF LOCATION.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR THE ESTABLISHMENT OF FINISHED GRADE. THE RESIDENT ENGINEER MAY ASSIST THE CONTRACTOR, AS APPLICABLE, BUT THE RESPONSIBILITY COORDINATING THE FINISHED GRADE ELEVATION WITH THE TOP OF THE FOUNDATION HEIGHTS AND THE LIGHT SHALL REMAIN WITH THE CONTRACTOR.
- NO POLES SHALL BE ERECTED UNTIL THE RESPECTIVE FOUNDATIONS HAVE CURED, AS APPROVED BY THE ENGINEER.
- THE CONTRACTOR SHALL MAKE SPECIAL NOTE OF THE REQUIREMENTS FOR WIRE MARKERS AND SHALL TAG ALL WIRE MARKERS AND SHALL TAG ALL WIRE ACCORDINGLY.
- EQUIPMENT GROUND CONDUCTORS SHALL BE SPLICED AND BONDED AT EACH LIGHT POLE OR OTHER PIECE OF EQUIPMENT.
- CONDUITS AND UNIT DUCTS MUST BE POSITIONED IN THE FIELD TO AVOID CONFLICT WITH TREES, BUSHES, DRAINS AND OTHER UTILITIES.



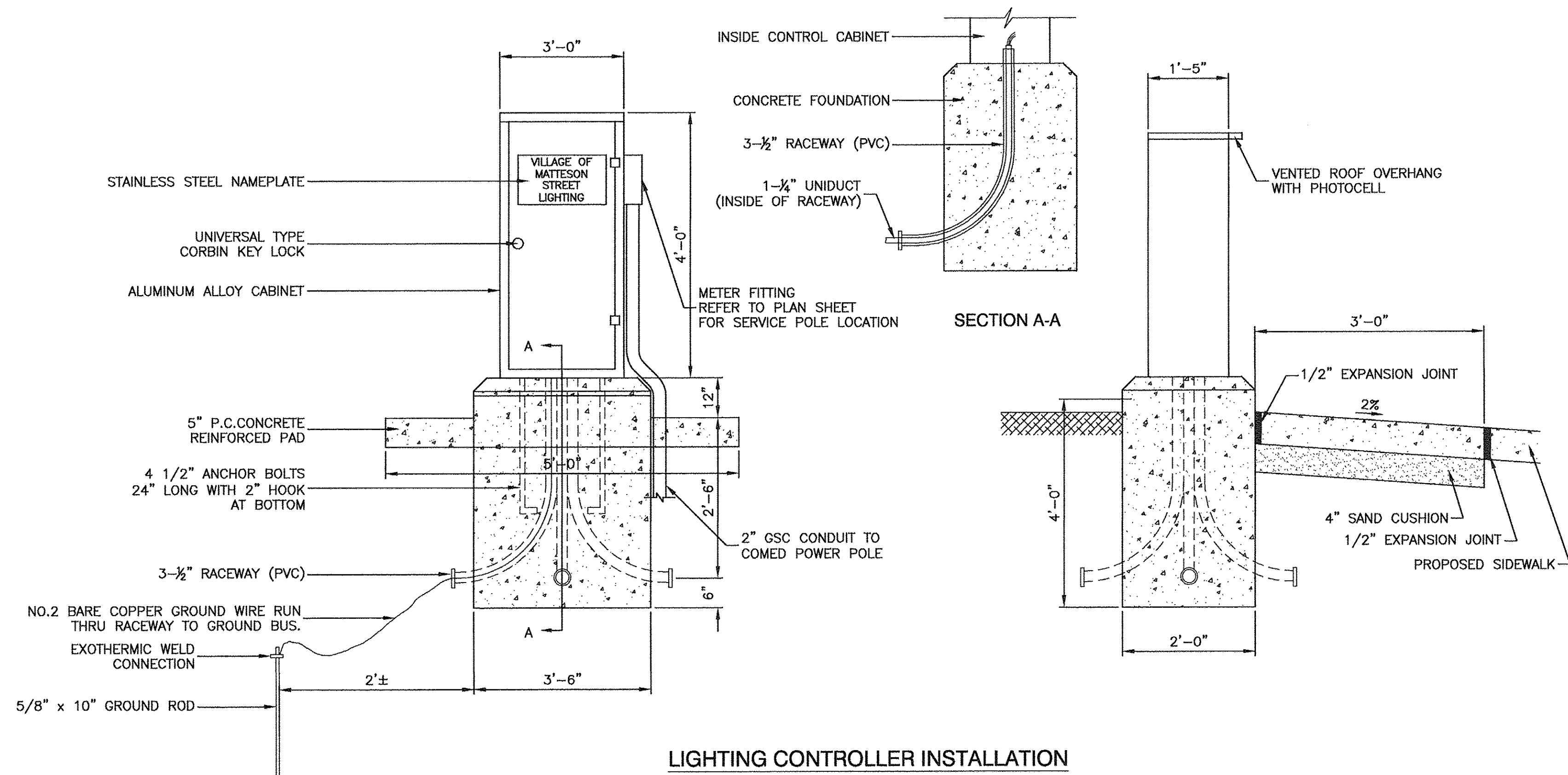
SPLICING ELECTRIC CABLES
BASIC MATERIALS AND METHODS



USER NAME =	DESIGNED -- EMA	REVISED --
PLOT SCALE =	CHECKED -- PKB	REVISED --
PLOT DATE = 05-19-16	DRAWN -- PS	REVISED --
	CHECKED -- AG	REVISED --

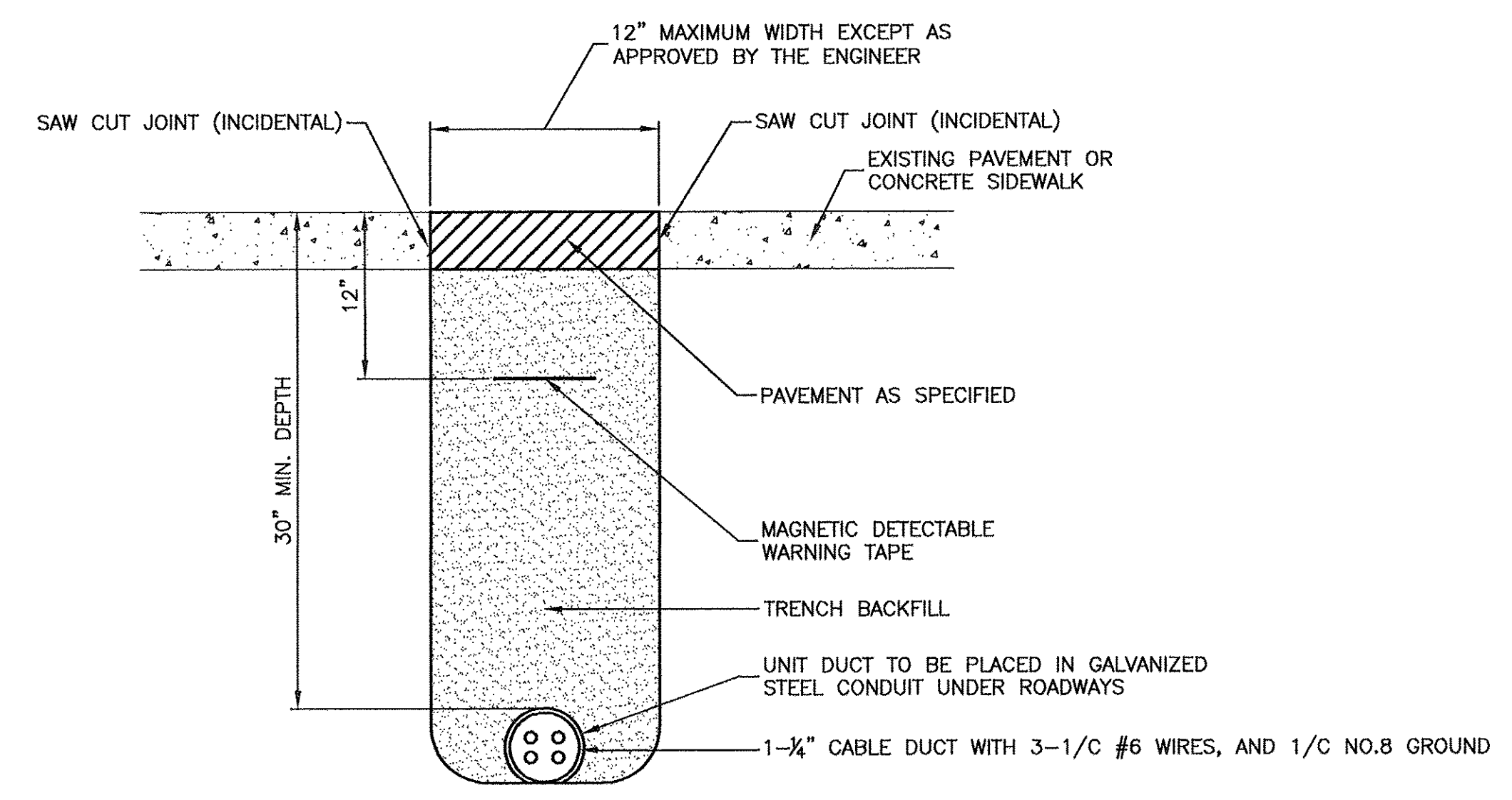
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FILE NAME = 13375_02-LGHT-02 - P03		US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE INTERSECTION IMPROVEMENTS STREET LIGHTING DETAILS		F.A.P. RTE. 353	SECTION 13-00063-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 38
SCALE:		SHEET NO. 38 OF 63 SHEETS		STA. TO STA.		CONTRACT NO. 61C11		
				FED. ROAD DIST. NO. 1		ILLINOIS		FED. AID PROJECT M-4003(216)

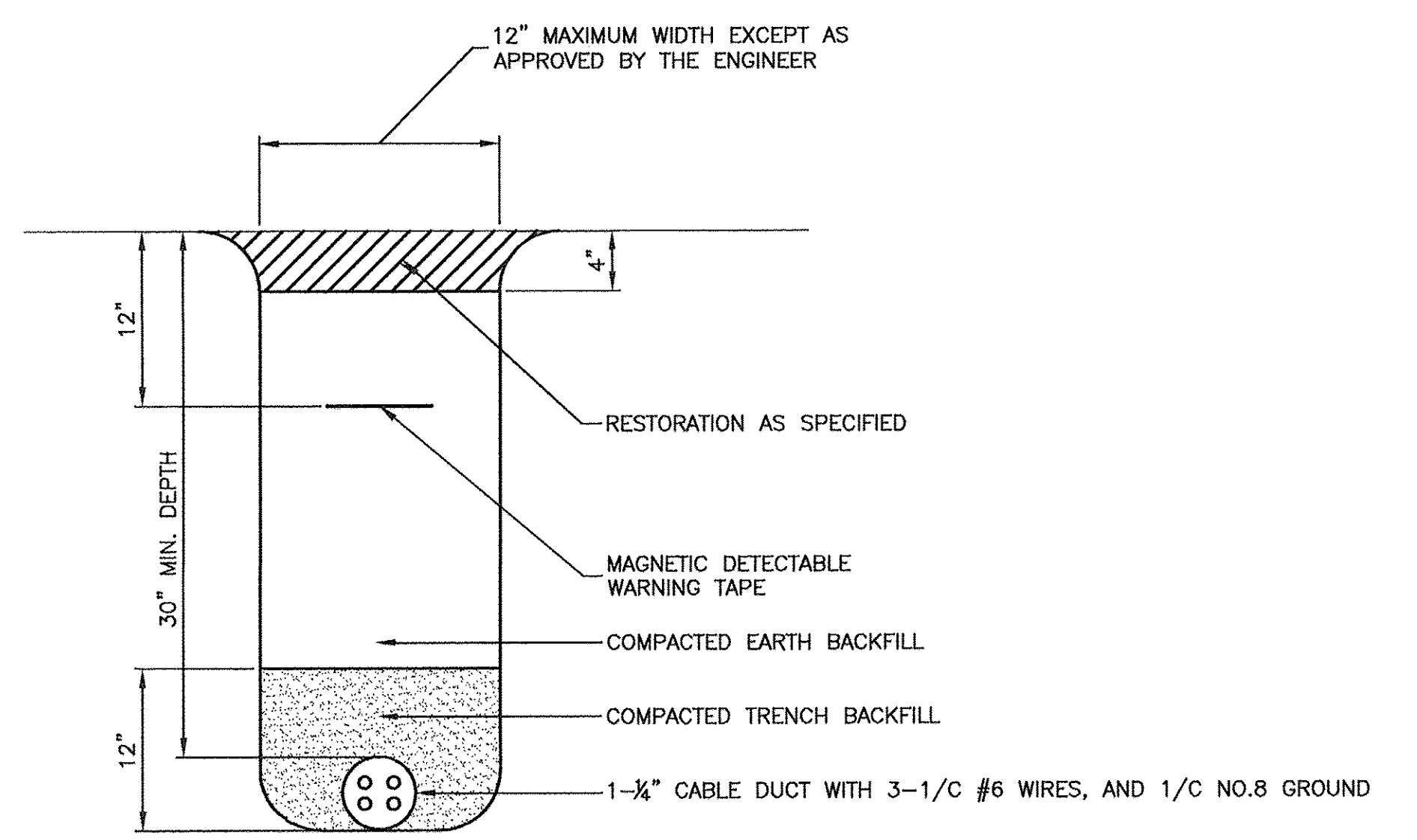


LIGHTING CONTROLLER INSTALLATION

- NOTE:
1. ORIENT CABINET SO DOOR OPENS AWAY FROM TRAFFIC.
 2. METER FITTING TO BE LOCATED ON THE SIDE OF THE CABINET CLOSEST TO SERVICE POLE.



TRENCH DETAIL - PAVED AREAS



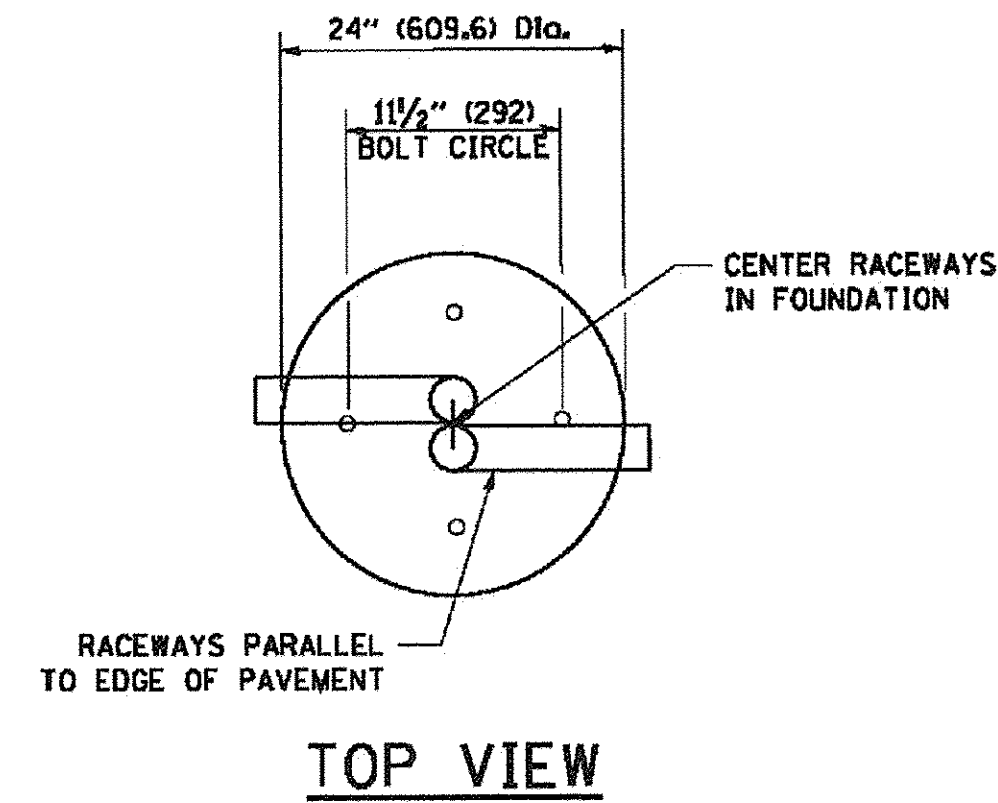
TRENCH DETAIL - UNPAVED AREAS

FILE NAME = 13375_02-LGHT-02 - P04	USER NAME =	DESIGNED -- EMA	REVISED --
		CHECKED -- PKB	REVISED --
	PLOT SCALE =	DRAWN -- PS	REVISED --
	PLOT DATE = 05-19-16	CHECKED -- AG	REVISED --

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	13-00063-00-CH	COOK	63	39
CONTRACT NO. 61C11				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(216)				

LIGHT POLE FOUNDATION DEPTH TABLE
30 FT. (9.144 m) TO 35 FT. (10.668 m) MOUNTING HEIGHT

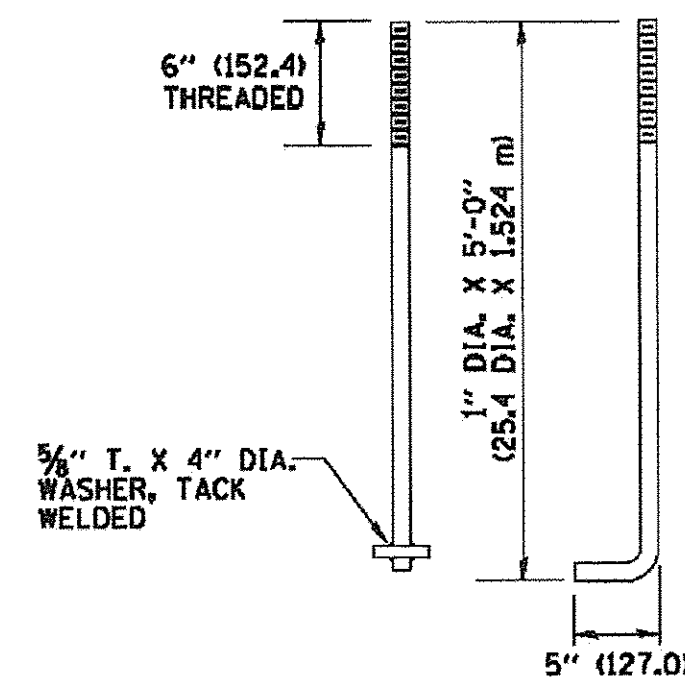
SOIL CONDITIONS	DESIGN DEPTH "D" OF FOUNDATION	
	SINGLE ARM POLE	TWIN ARM POLE
SOFT CLAY Qu = 0.375 TON/SO. FT.	11'-0" (3.35 m)	12'-8" (3.85 m)
MEDIUM CLAY Qu = 0.75 TON/SO.FT	9'-0" (2.74 m)	14'-10" (4.52 m)
STIFF CLAY Qu = 1.50 TON/SO. FT.	7'-6" (2.29 m)	8'-7" (2.61 m)
LOOSE SAND φ = 34°	9'-6" (2.90 m)	10'-7" (3.22 m)
MEDIUM SAND φ = 37.5°	9'-0" (2.74 m)	9'-10" (2.99 m)
DENSE SAND φ = 40°	8'-3" (2.51 m)	9'-7" (2.91 m)



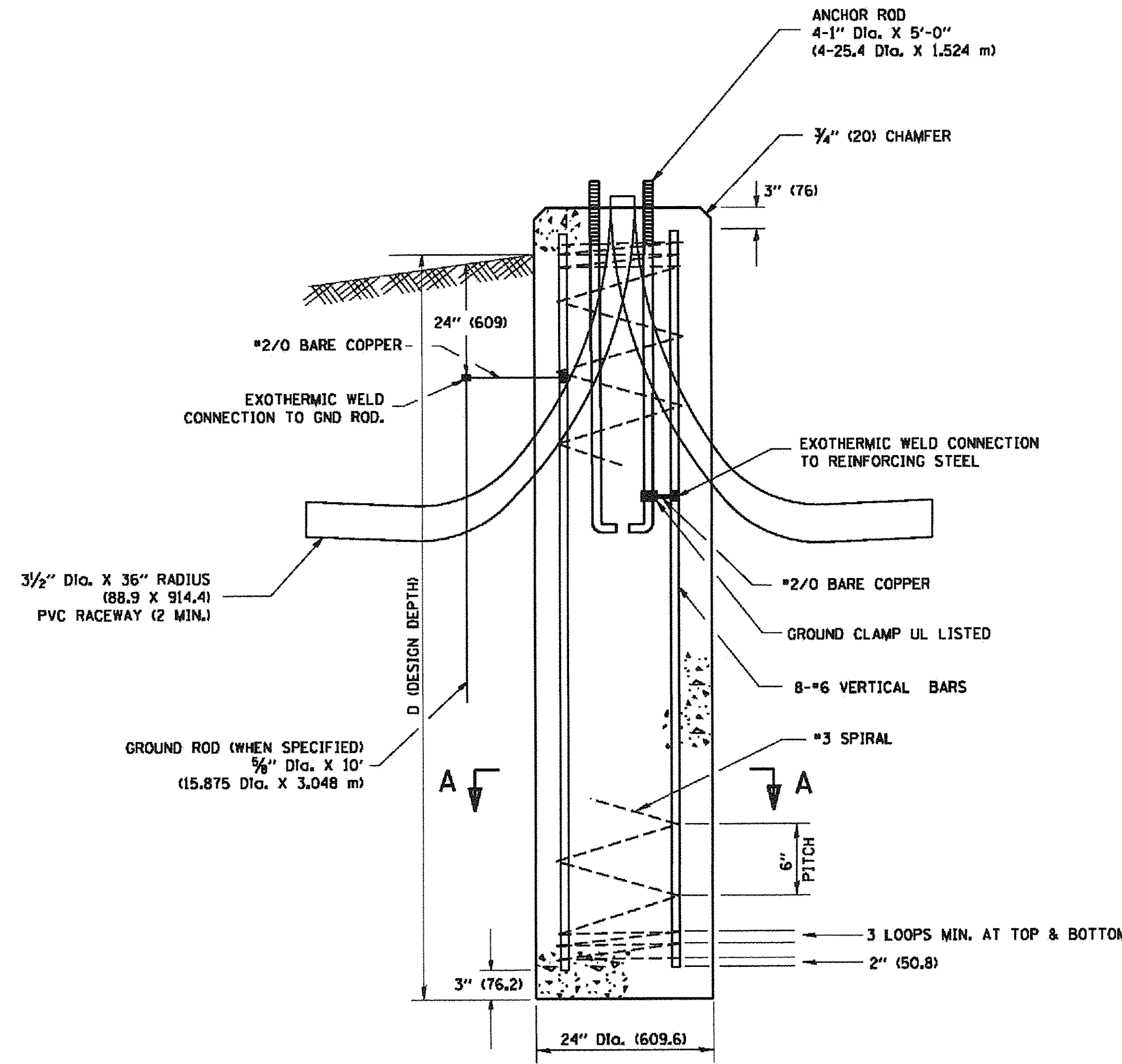
NOTE: FOUNDATION BOLT CIRCLE SHALL MATCH THE POLE BOLT CIRCLE.

NOTES

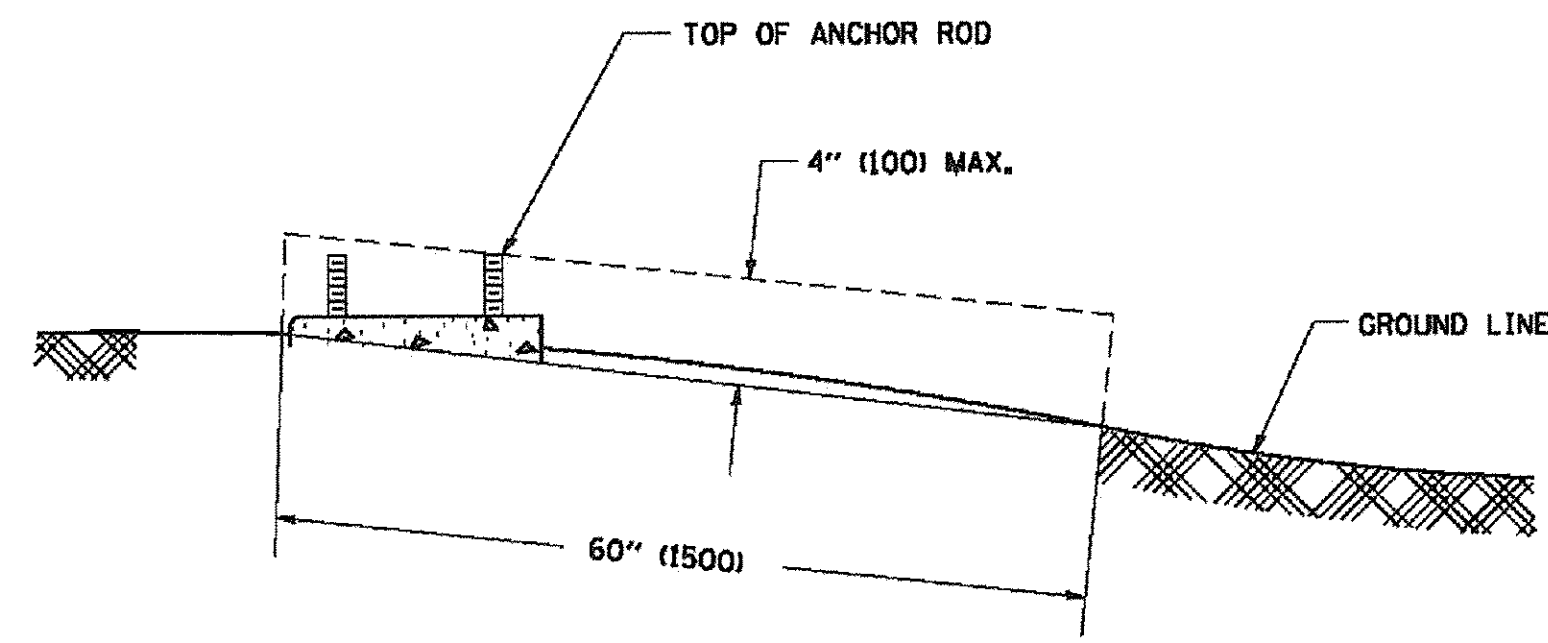
- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- THE ANCHOR RODS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED.
- THE FOUNDATION SHALL NOT PROTRUDE MORE THAN 4 IN. (100 mm) ABOVE THE FINISHED GRADE WITHIN A 60 IN. (1.5 m) CHORD ACROSS THE FOUNDATION, WITH ANCHOR RODS INCLUDED, IN ACCORDANCE WITH AASHTO GUIDELINES. IF THE FOUNDATION HEIGHT, INCLUDING ANCHOR RODS, EXTENDS BEYOND THESE SPECIFIED LIMITS, THE FOUNDATION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE. SEE FOUNDATION EXTENSION DETAIL.
- THE HOLE FOR THE FOUNDATION SHALL BE MADE BY DRILLING WITH AN AUGER, OF THE SAME DIAMETER AS THE FOUNDATION. IF SOIL CONDITIONS REQUIRE THE USE OF A LINER TO FORM THE HOLE, THE LINER SHALL BE WITHDRAWN AS THE CONCRETE IS DEPOSITED.
- THE TOP OF THE FOUNDATION SHALL BE CONSTRUCTED LEVEL. A LINER OR FORM SHALL BE USED TO PRODUCE A UNIFORM SMOOTH SIDE TO THE TOP OF THE FOUNDATION. FOUNDATION TOP SHALL BE CHAMFERED 3/4-IN. (20 mm).
- THE CONCRETE SHALL BE CLASS S1. CONCRETE SHALL CURE ACCORDING TO ARTICLE 1020.13 BEFORE LIGHT POLES ARE INSTALLED.
- THE ANCHOR ROD SHALL BE A HOOK ROD TYPE. COLD BENDING OF THE ANCHOR ROD WILL NOT BE ALLOWED. THE RADIUS OF THE HOOK BEND SHALL NOT BE LESS THAN 4 TIMES THE NOMINAL DIAMETER OF THE ANCHOR ROD. A TACK WELDED ANCHOR ROD MAY BE SUBSTITUTED WITH THE APPROVAL OF THE ENGINEER.
- THE ANCHOR RODS SHALL BE ACCORDING TO ASTM F1554 GRADE 725 (GRADE 105). NUTS SHALL BE HEXAGON NUTS ACCORDING TO ASTM A 194 2H OR ASTM A 563 DH, AND WASHERS SHALL BE ACCORDING TO ASTM F 436.
- ANCHOR RODS, NUTS AND WASHERS SHALL BE COMPLETELY GALVANIZED BY EITHER THE HOT-DIPPED PROCESS CONFORMING WITH AASHTO M 232, THE MECHANICAL PLATING METHOD CONFORMING TO AASHTO M 298, CLASS 50 WITH A MAXIMUM COATING THICKNESS OF 150 UM(6 MILS) OR THE ELECTROLYTIC PROCESS ACCORDING TO ASTM F 1136.
- THE ANCHOR RODS SHALL BE THREADED A MINIMUM OF 6 INCHES (150 mm) WITH A MINIMUM OF 3 INCHES (75 mm) OF THREADED ANCHOR ROD EMBEDDED IN THE FOUNDATION.
- ANCHOR RODS SHALL PROJECT 2 3/4" (69.9 mm) ABOVE THE TOP OF THE FOUNDATION. IF BREAKAWAY COUPLINGS ARE SPECIFIED, THE CONTRACTOR SHALL CAREFULLY COORDINATE THE ANCHOR ROD PROJECTION WITH THE INSTALLATION REQUIREMENTS OF THE BREAKAWAY COUPLINGS.
- THE CONTRACTOR SHALL USE A #3 SPIRAL AT 6" (152.4 mm) PITCH OR MAY SUBSTITUTE #3 TIES AT 12" (304.8 mm) O.C. WITH THE APPROVAL OF THE ENGINEER.
- THE CABLE TRENCHES AND FOUNDATION SHALL BE BACK FILLED AND COMPACTED AS SPECIFIED BEFORE THE LIGHT POLE IS ERECTED.
- THE RACEWAYS SHALL PROJECT 1" (25.4 mm) ABOVE THE TOP OF THE FOUNDATION.



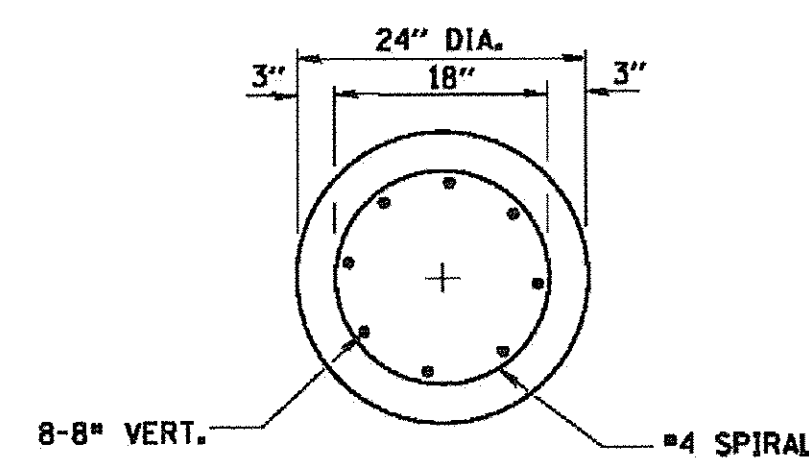
ANCHOR BOLT DETAIL



FOUNDATION DETAIL



FOUNDATION EXTENSION DETAIL



SECTION A-A

FILE NAME = 13375_02-LIGHT-03 - P01



USER NAME = geglianobt
 CHECKED —
 PLOT SCALE = 50.0000' / IN.
 DRAWN —
 PLOT DATE = 1/4/2008

DESIGNED —
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REVISED —
 REVISED —
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 REVISED —

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

LIGHT POLE FOUNDATION

30' (9.144 m) TO 35' (10.668 m) M.H. 11 1/2" (292 mm) BOLT CIRCLE

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

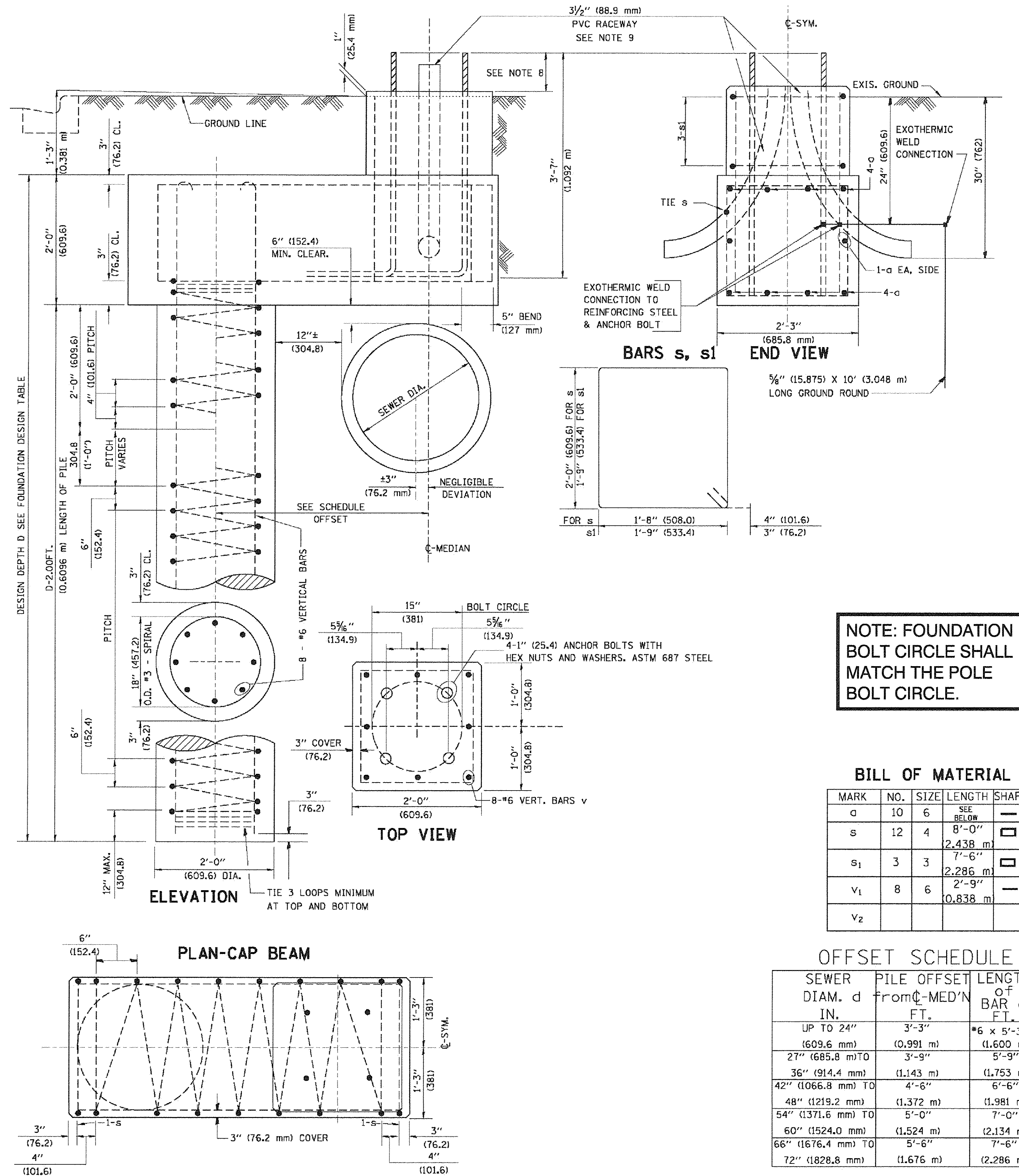
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	13-00063-00-CH	COOK	63	40
BE-300		CONTRACT NO. 61C11		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT M-4003(216)		

FOUNDATION DESIGN TABLE

TYPE OF SOIL	DESIGN DEPTH OF FOUNDATION		REINFORCEMENT IN FOUNDATION			
	SINGLE ARM D	TWIN ARM D	SINGLE ARM		TWIN ARM	
			VERT BARS	SPIRAL	VERT BARS	SPIRAL
SOFT CLAY	13'-0" (3.962 m)	15'-0" (4.572 m)	8-#6X12'-6" (3.810 m)	*3X122' (37.186 m)	8-#6X14'-3" (4.343 m)	*3X141' (42.977 m)
MEDIUM CLAY	9'-6" (2.896 m)	10'-9" (3.277 m)	8-#6X9'-0" (2.743 m)	*3X90' (27.432 m)	8-#6X10'-0" (3.048 m)	*3X100' (30.480 m)
STIFF CLAY	7'-0" (2.134 m)	8'-0" (2.438 m)	8-#6X6'-6" (1.981 m)	*3X66' (20.112 m)	8-#6X7'-6" (2.286 m)	*3X76' (23.165 m)
LOOSE SAND	9'-0" (2.743 m)	10'-0" (3.048 m)	8-#6X8'-6" (2.591 m)	*3X85' (25.908 m)	8-#6X9'-6" (2.896 m)	*3X94' (28.651 m)
MEDIUM SAND	8'-3" (2.515 m)	9'-0" (2.743 m)	8-#6X8'-0" (2.438 m)	*3X78' (23.774 m)	8-#6X8'-6" (2.591 m)	*3X85' (25.908 m)
DENSE SAND	7'-9" (2.362 m)	9'-0" (2.743 m)	8-#6X7'-6" (2.286 m)	*3X73' (22.250 m)	8-#6X8'-6" (2.591 m)	*3X85' (25.908 m)
ROCK OR SOLIDIFIED SLAG	5'-0" (1.524 m)	5'-0" (1.524 m)	NONE	NONE	NONE	NONE

NOTES

- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- THE ENGINEER SHALL DETERMINE THE CLASS OF SOIL DURING EXCAVATION AND SELECT THE DESIGN DEPTH OF FOUNDATION FROM THE DESIGN TABLE.
- EXCAVATION OF THE POLE FOUNDATION SHALL BE MADE WITH AN AUGER, 24" (609.6 mm) OR 30" (762.0 mm) IN DIAMETER.
- THE ANCHOR ROD SHALL BE A HOOK ROD TYPE. COLD BENDING OF THE ANCHOR ROD WILL NOT BE ALLOWED. THE RADIUS OF THE HOOK BEND SHALL NOT BE LESS THAN 4 TIMES THE NOMINAL DIAMETER OF THE ANCHOR ROD. A TACK WELDED ANCHOR ROD MAY BE SUBSTITUTED WITH THE APPROVAL OF THE ENGINEER.
- THE ANCHOR BOLTS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED IN THE FORM.
- THE ANCHOR RODS SHALL BE ACCORDING TO ASTM F1554 GRADE 725 (GRADE 105). NUTS SHALL BE HEXAGON NUTS ACCORDING TO ASTM A 194 2H OR ASTM A 563 DH, AND WASHERS SHALL BE ACCORDING TO ASTM F 436.
- THE CONTRACTOR SHALL COORDINATE EXTENSION OF ANCHOR BOLTS ABOVE TOP OF FOUNDATION WITH THE BREAKAWAY DEVICE MANUFACTURER'S REQUIREMENTS. IF LIGHT POLE IS MOUNTED WITHOUT BREAKAWAY DEVICE, ANCHOR BOLTS SHALL PROJECT 2 3/4" (69.9 mm) ABOVE TOP OF THE FOUNDATION. THE CONTRACTOR SHALL CONFIRM ANCHOR BOLT EXTENTION WITH ENGINEER.
- RACEWAYS SHALL PROJECT 1" (25.4 mm) ABOVE THE TOP OF THE FOUNDATION.
- THE CABLE TRENCH SHALL BE BACKFILLED AND FIRMLY COMPACTED BEFORE THE LIGHT IS ERCTED.



NOTE: FOUNDATION BOLT CIRCLE SHALL MATCH THE POLE BOLT CIRCLE.

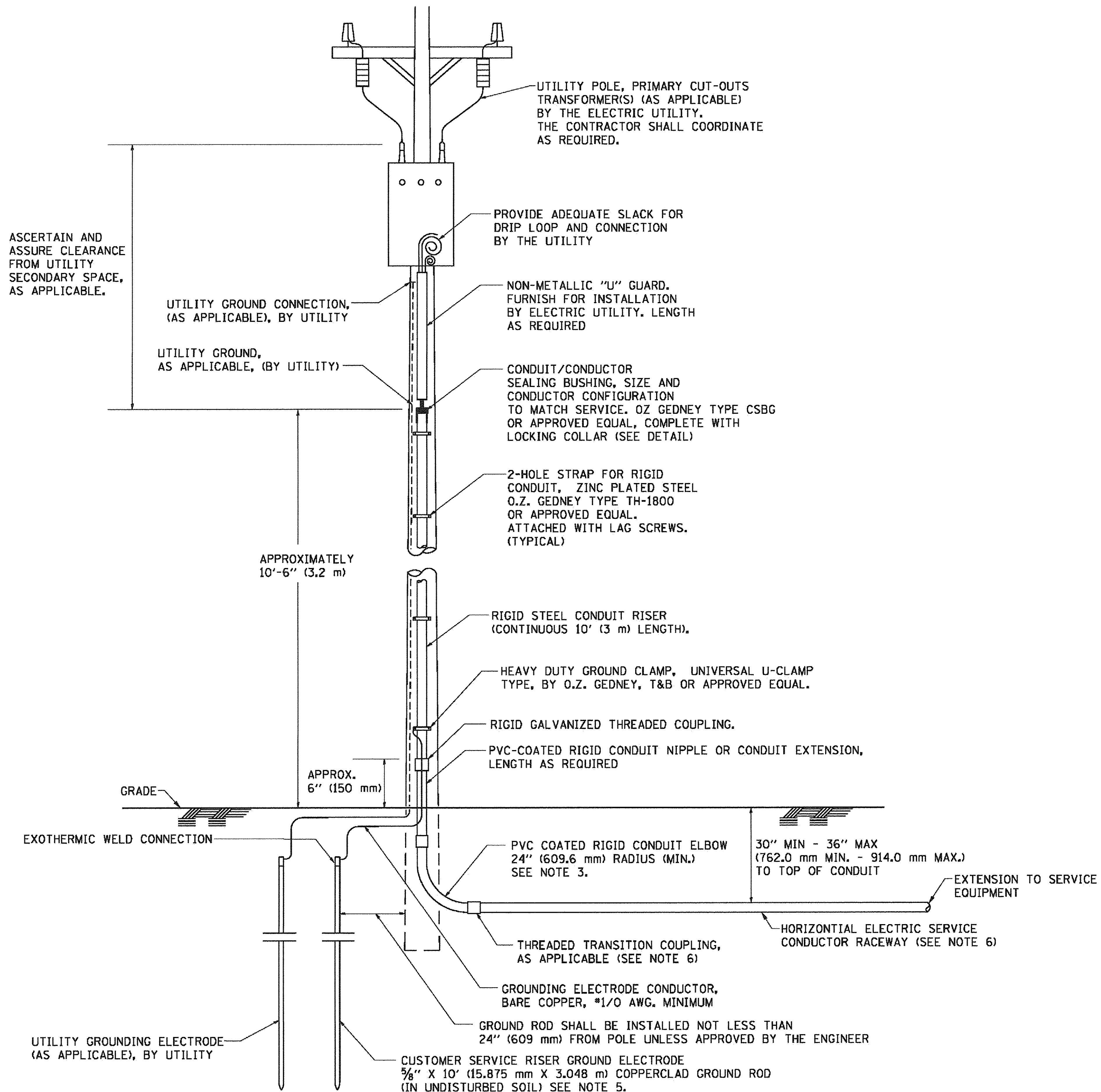
BILL OF MATERIAL

MARK	NO.	SIZE	LENGTH	SHAPE
c	10	6	SEE BELOW	—
s	12	4	8'-0" (2.438 m)	□
s ₁	3	3	7'-6" (2.286 m)	□
v ₁	8	6	0.838 m	—
v ₂				

OFFSET SCHEDULE

SEWER DIAM. d IN.	PILE OFFSET FROM C-MEDIAN FT.	LENGTH OF BAR d FT.
UP TO 24" (609.6 mm)	3'-3" (0.991 m)	*6 x 5'-3" (1.600 m)
27" (685.8 mm) TO	3'-9" (1.143 m)	5'-9"
36" (914.4 mm)	4'-6" (1.372 m)	(1.753 m)
42" (1066.8 mm) TO	5'-0" (1.524 m)	(1.981 m)
48" (1219.2 mm)	5'-6" (1.676 m)	7'-0"
54" (1371.6 mm) TO	6'-0" (1.828 m)	(2.134 m)
60" (1524.0 mm)	6'-6" (1.981 m)	7'-6"
66" (1676.4 mm) TO	7'-0" (2.134 m)	(2.286 m)
72" (1828.8 mm)	7'-6" (2.286 m)	

ASCERTAIN AND ASSURE CLEARANCE FROM UTILITY SECONDARY SPACE, AS APPLICABLE.

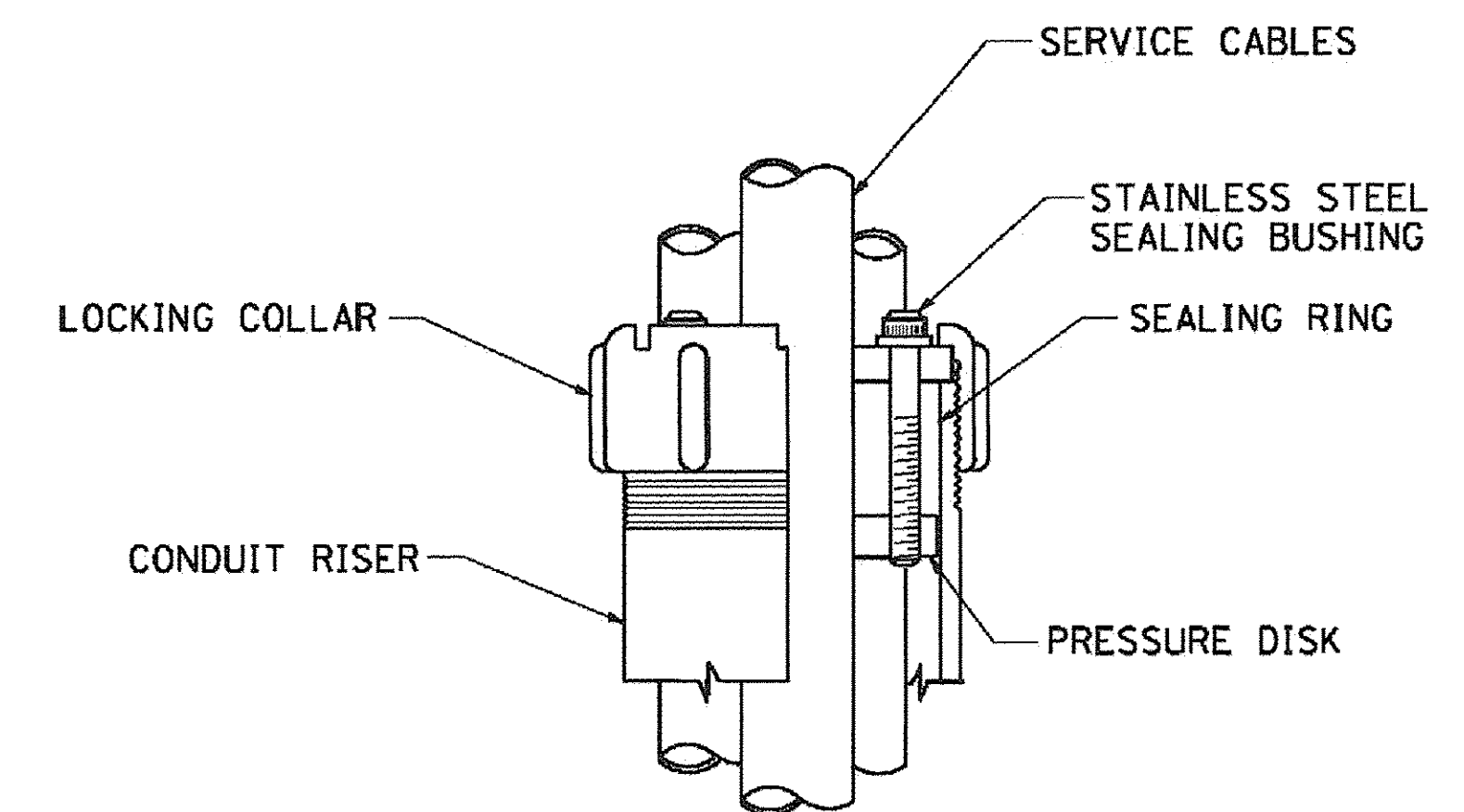


APPLICATION

THIS DETAIL APPLIES FOR LOW VOLTAGE ELECTRIC SERVICE (660 V OR LESS) FROM AN OVERHEAD UTILITY SUPPLY TO SEPERATLY-MOUNTED SERVICE EQUIPMENT.

NOTES

- SERVICE VOLTAGE SHALL BE AS INDICATED ELSEWHERE IN THE DRAWINGS.
- UNLESS OTHERWISE INDICATED, ITEMS AND WORK SHALL BE INCLUDED AND PAID AS PART OF THE ELECTRIC UTILITY SERVICE INSTALLATION PAY ITEM.
- CONDUIT AND CONNECTOR DIAMETER SHALL MATCH THE DIAMETER OF THE SERVICE CONDUCTOR RACEWAY AS INDICATED ON THE PLANS.
- PVC COATED RACEWAYS AND ACCESSORIES SHALL BE CAREFULLY INSTALLED WITH MFR RECOMMENDED TOOLS AND PROCEDURES TO AVOID DAMAGE. ANY DAMAGE SHALL BE REPAIRED WITH COMPATIBLE PVC TOUCH-UP MATERIAL TO THE SATISFACTION OF THE ENGINEER OR THE DAMAGED MATERIAL SHALL BE REPLACED AT NO ADDITIONAL COST.
- THE CONTRACTOR SHALL OBTAIN INSPECTION AND APPROVAL BY THE ENGINEER OF SERVICE RISER GROUND ELECTRODE, RISER ELBOW, NIPPLE AND CONNECTION TO SERVICE CONDUCTOR RACEWAY EXTENSION BEFORE BACKFILL AND SHALL ALSO OBTAIN INSPECTION OF SERVICE RISER AND SEALING BUSHING BEFORE UTILITY "U" GUARD INSTALLATION AND SERVICE CONNECTION.
- THE HORIZONTAL ELECTRIC SERVICE CONDUCTOR RACEWAY SHALL BE AS INDICATED AND SHALL BE MEASURED SEPARATELY FOR PAYMENT. WHEN THE RACEWAY IS PVC-COATED RIGID GALVANIZED STEEL, THE COUPLING SHALL BE THE SAME. WHEN THE RACEWAY IS PVC CONDUIT (IN CONCRETE), THE COUPLING SHALL BE A METALIC TO NON METALIC ADAPTER. WHEN THE RACEWAY IS ENCASED IN CONCRETE, THE CONCRETE SHALL EXTEND TO COVER THE COUPLING.
- PLANS AND DETAILS INDICATE THE GENERAL NATURE AND REQUIREMENTS. THEY DO NOT SHOW EVERY ACCESSORY AND ATTACHMENT, AND THEY DO NOT RELIEVE THE CONTRACTOR OF THE REQUIREMENTS OF THE SPECIFICATIONS AND SPECIAL PROVISIONS TO ASCERTAIN UTILITY REQUIREMENTS AND TO COORDINATE ACCORDINGLY, FURNISHING ALL ITEMS AND WORK NOT PROVIDED BY THE UTILITY, BUT NECESSARY FOR A COMPLETE SERVICE INSTALLATION IS REQUIRED AND SHALL BE INCLUDED IN THE ELECTRIC UTILITY SERVICE INSTALLATION PAY ITEM.



SEALING BUSHING DETAIL

FILE NAME = 13375_02-LGHT-05 - P01



USER NAME = geglanoht
 PLOT SCALE = 50.0000' / IN.
 PLOT DATE = 1/4/2008

DESIGNED —
 CHECKED —
 DRAWN — MEA
 CHECKED —

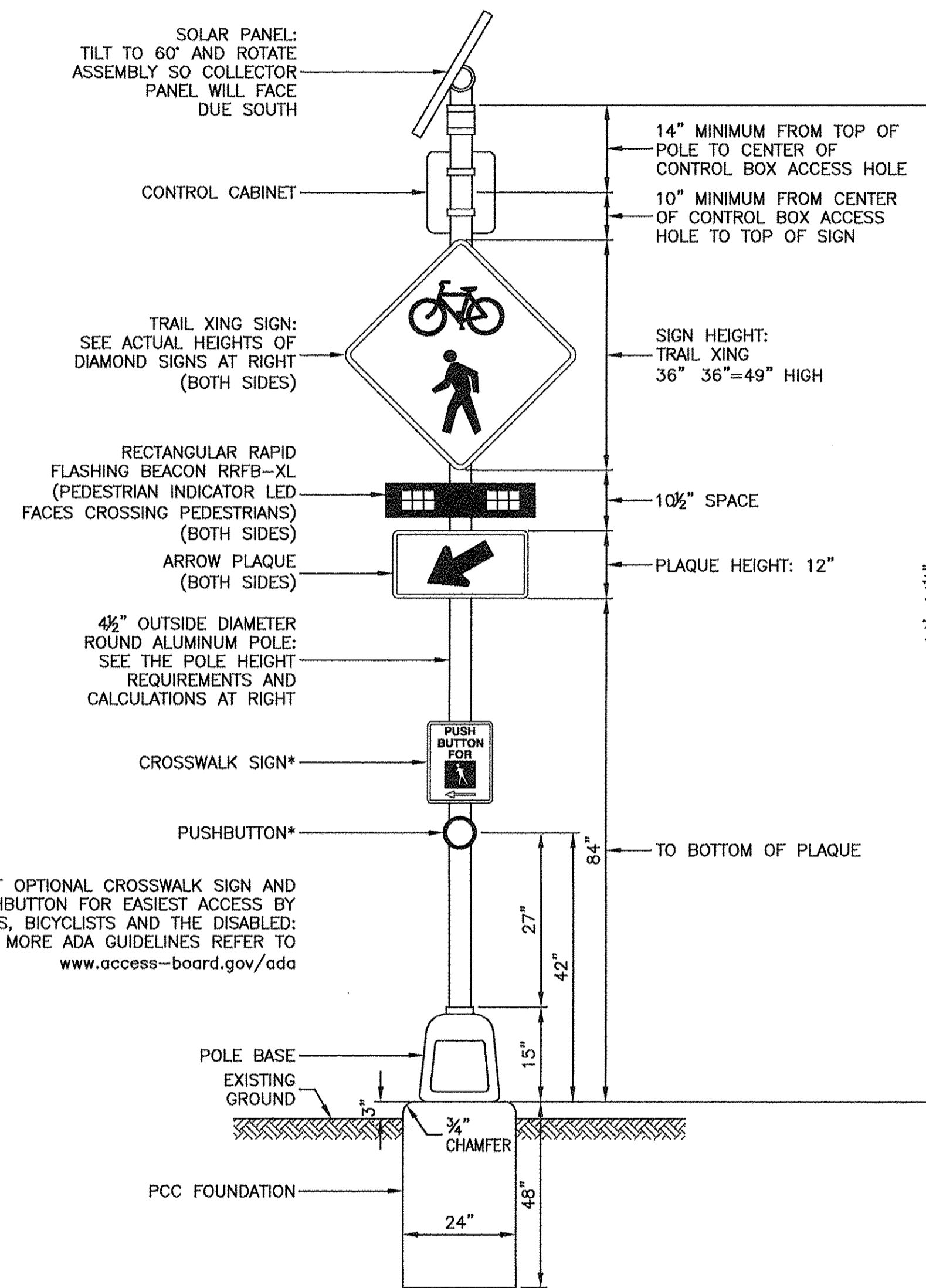
REVISED — 03-03-06
 REVISED —
 REVISED —
 REVISED —

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ELECTRIC SERVICE INSTALLATION
 AERIAL, REMOTE DISCONNECT

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	13-00063-00-CH	COOK	63	42
BE-220		CONTRACT NO. 61C11		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT M-4003(216)		



TYPICAL RECTANGULAR RAPID FLASHING BEACON CONFIGURATION

NOTES

1. THE SCHEDULE OF QUANTITIES LISTS ITEMS INCLUDED FOR ONE COMPLETE ASSEMBLY.
2. SEE THE SPECIAL PROVISIONS FOR SOLAR POWERED FLASHING BEACON ASSEMBLY (COMPLETE) FOR FURTHER DETAILS.
3. TRAIL CROSSING, ARROW PLAQUES, AND RRFB SHALL BE INSTALLED FACING BOTH DIRECTIONS OF VEHICLE TRAFFIC ON EACH SIDE OF THE KOSTNER AVE TRAIL CROSSING.

SCHEDULE OF QUANTITIES PER ASSEMBLY

ITEM	UNIT	QUAN
SOLAR PANEL	EACH	1
CONTROL CABINET AND ELECTRICAL COMPONENTS	EACH	1
RRFB-XL	EACH	2
PEDESTRIAN PUSHBUTTON	EACH	1
4 1/2" DIAMETER ALUMINUM POLE	EACH	1
P.C.C. FOUNDATION	EACH	1
TRAIL CROSSING SIGN	EACH	2
ARROW PLAQUE	EACH	2
CROSSWALK SIGN	EACH	1

FILE NAME = 13375_02-DTLS-02 - IDOT P01

USER NAME =	DESIGNED — EMA	REVISED —
	CHECKED — PKB	REVISED —
PLOT SCALE =	DRAWN — RG	REVISED —
PLOT DATE = 05-19-16	CHECKED — AG	REVISED —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

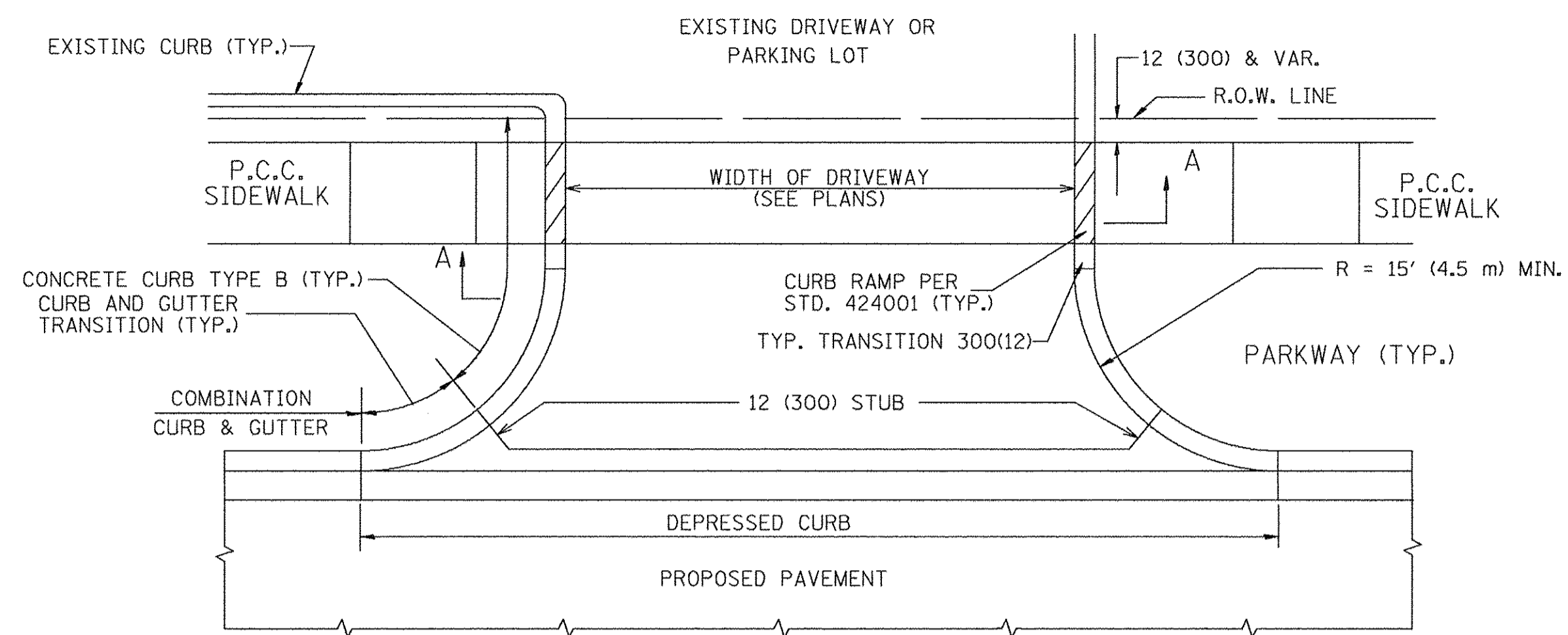
INTERSECTION IMPROVEMENTS
US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE
FLASHING BEACON ASSEMBLY DETAILS

SCALE: NONE

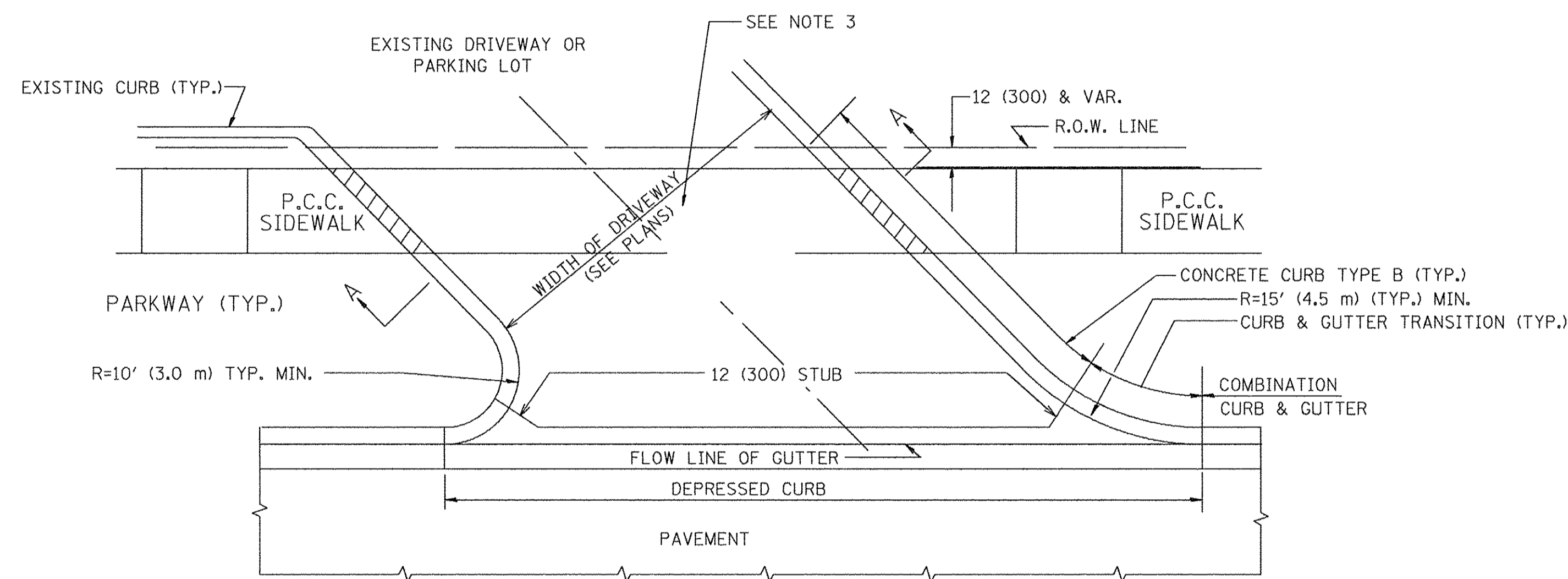
SHEET NO. 43 OF 63 SHEETS

STA. TO STA.

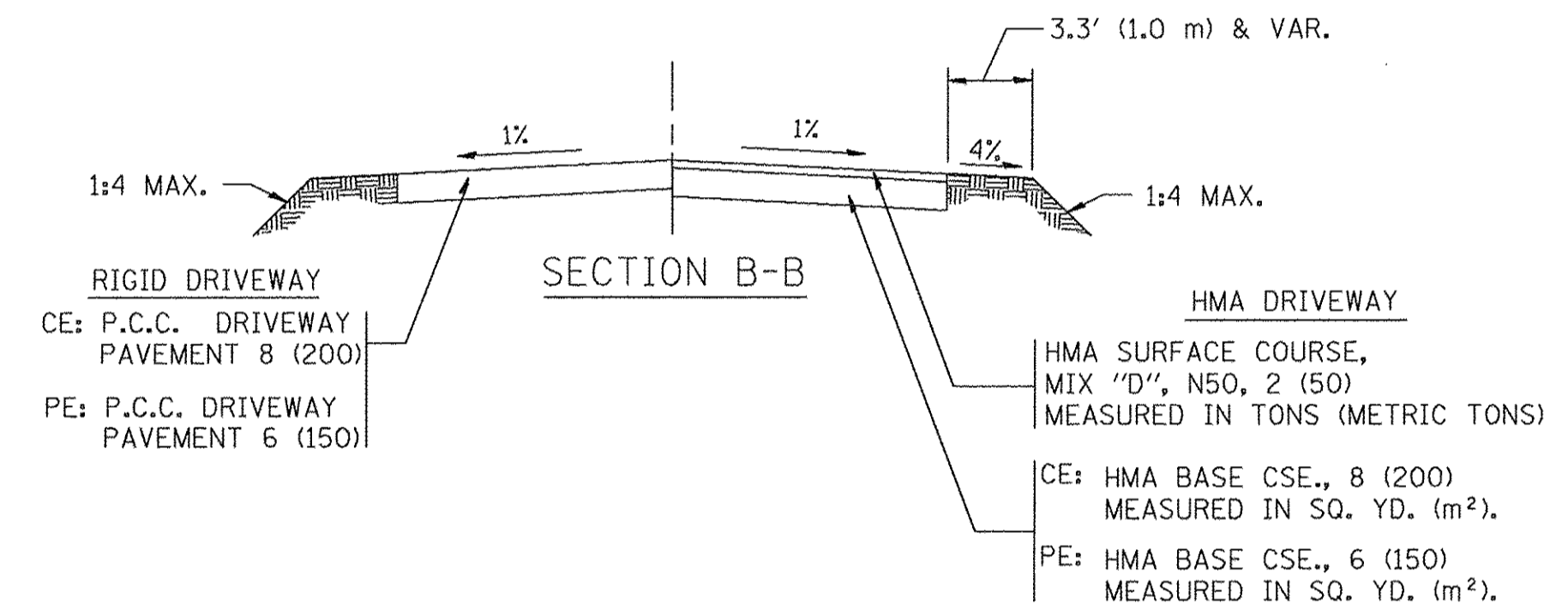
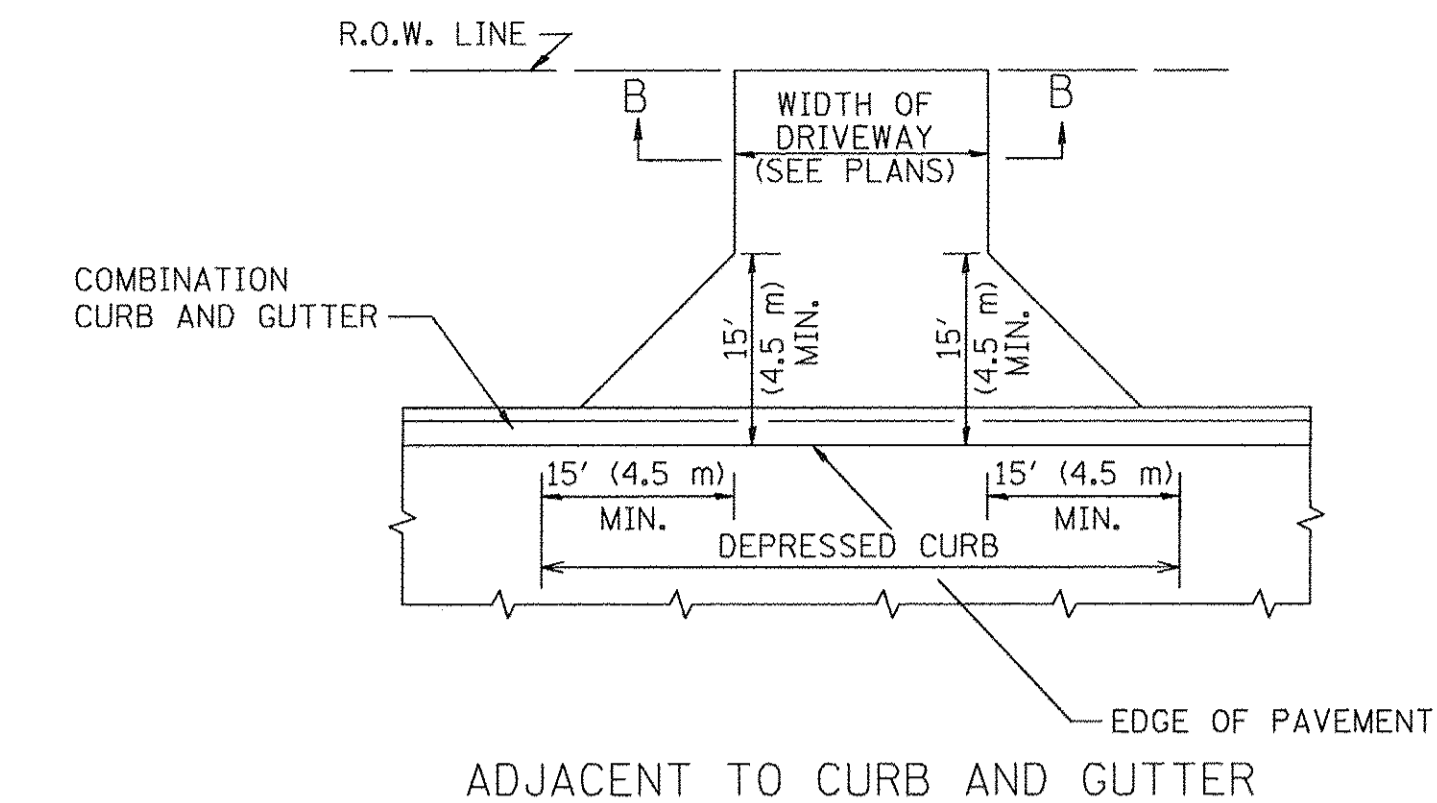
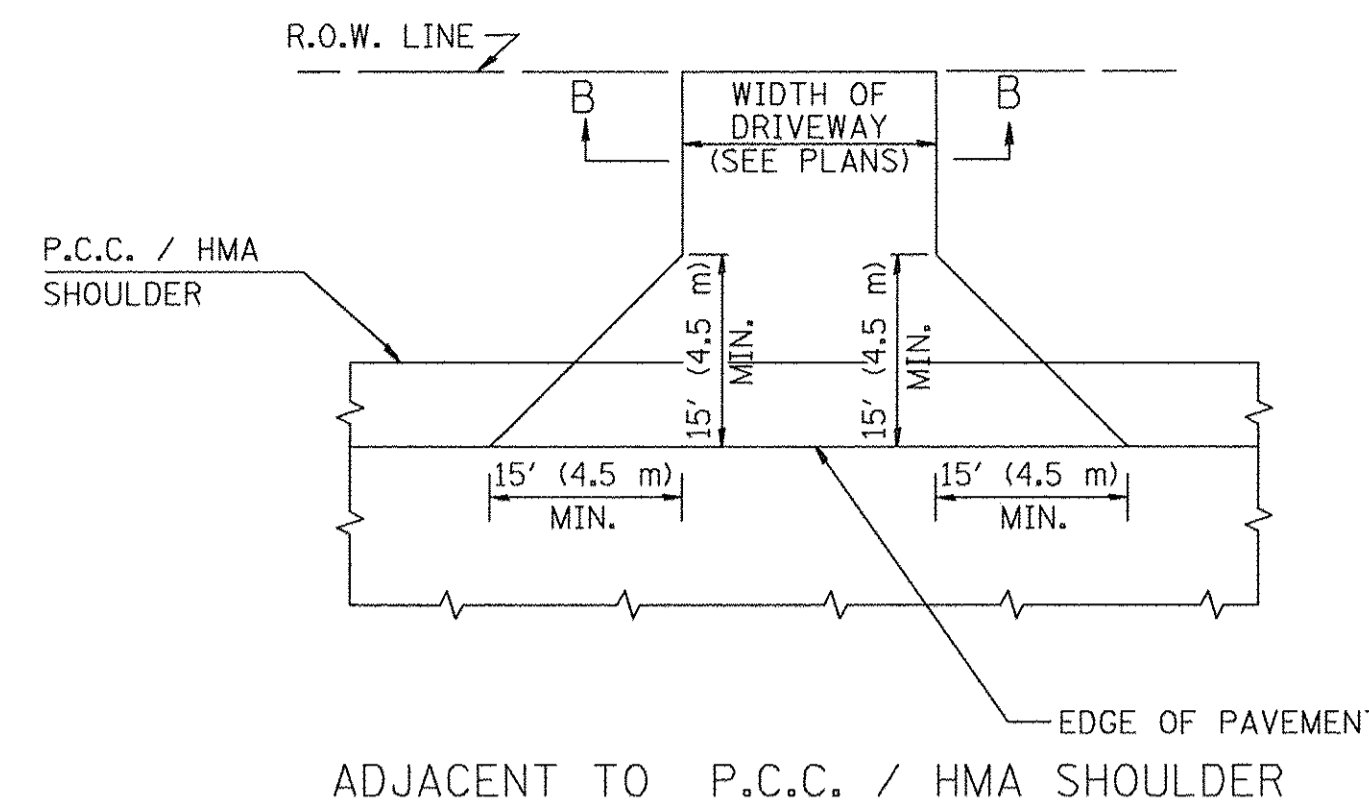
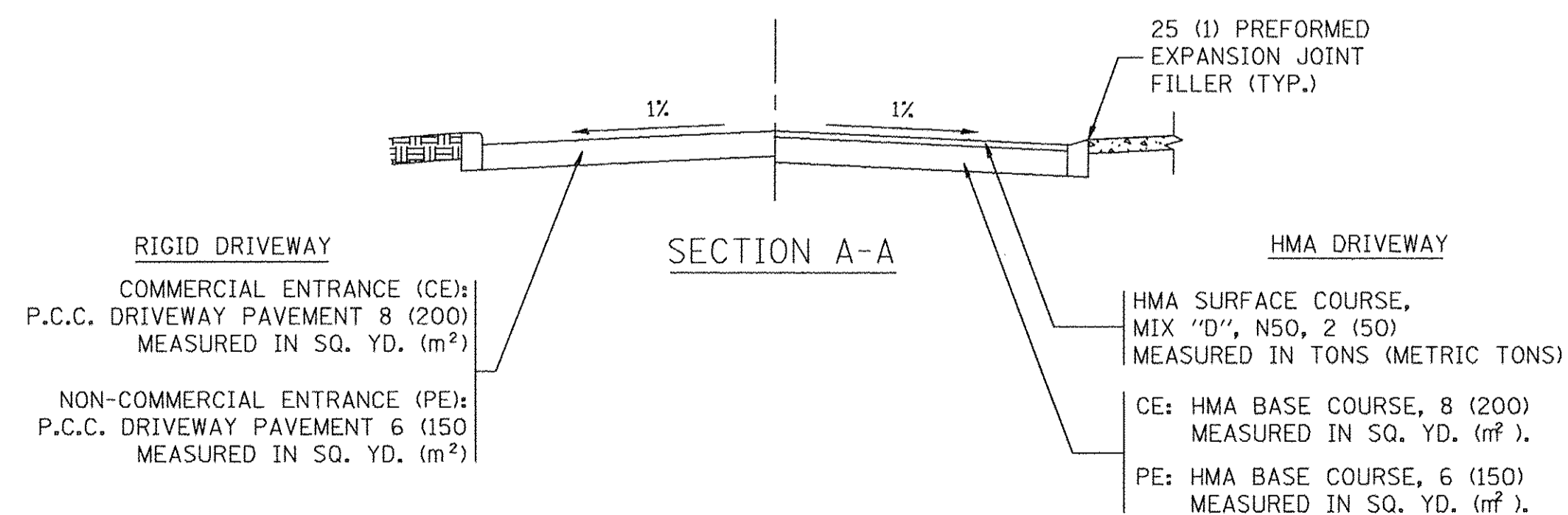
F.A.P. RTE. 353	SECTION 13-00063-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 43
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT M-4003(216)	
CONTRACT NO. 61C11				



WITH CONCRETE CURB, TYPE B



WITH CONCRETE CURB, TYPE B



GENERAL NOTES:
 DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

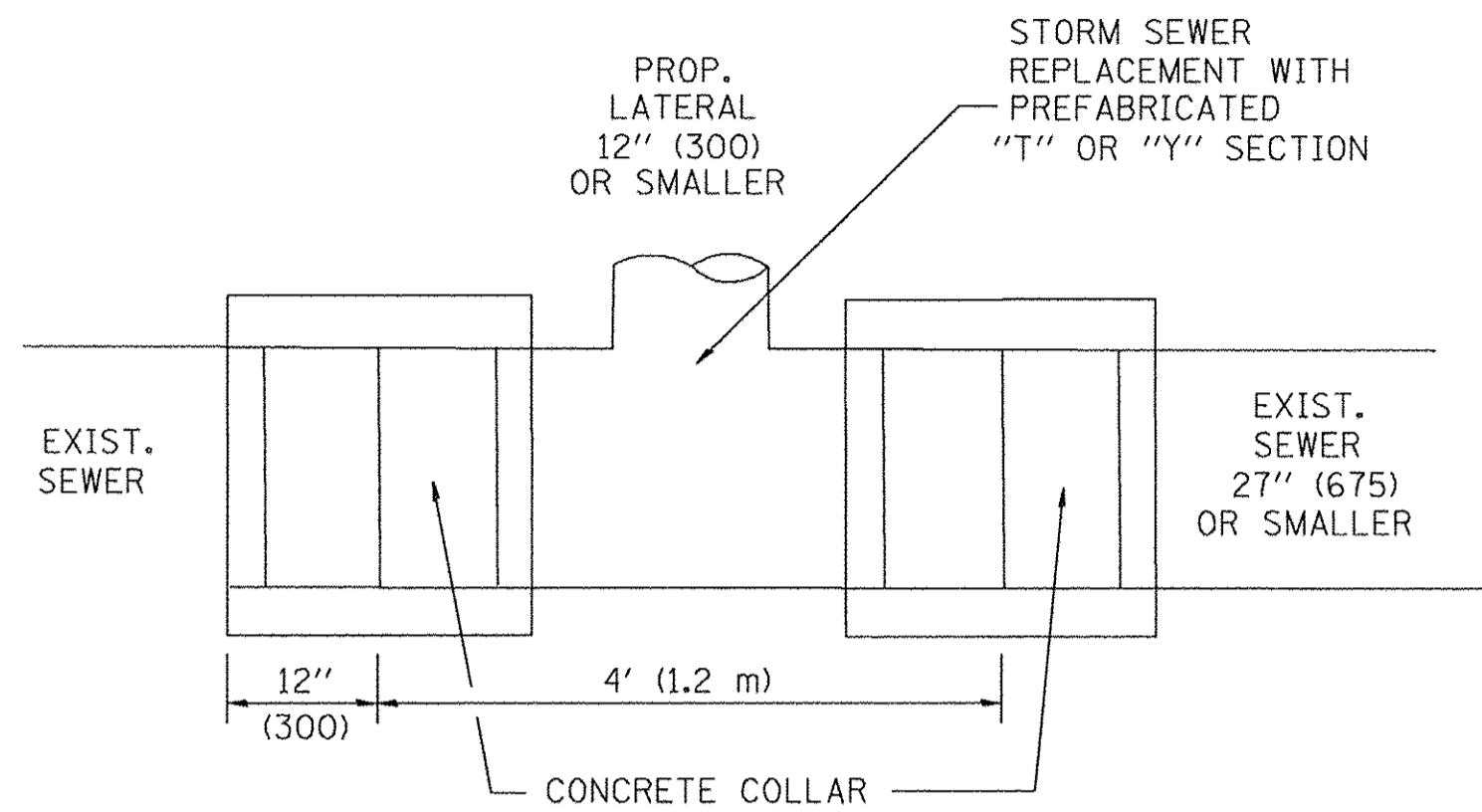
FILE NAME = 13375_02-DTLS-01 - BD01

USER NAME = leyan	DESIGNED -- R. SHAH	REVISED -- P. LGFLUER 04-15-03
PLOT SCALE = 50.0000' / 1" =	CHECKED --	REVISED -- R. BORO 01-01-07
PLOT DATE = 9/6/2011	DRAWN --	REVISED -- R. BORO 06-11-08
	CHECKED -- 11-04-95	REVISED -- R. BORO 09-06-11

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

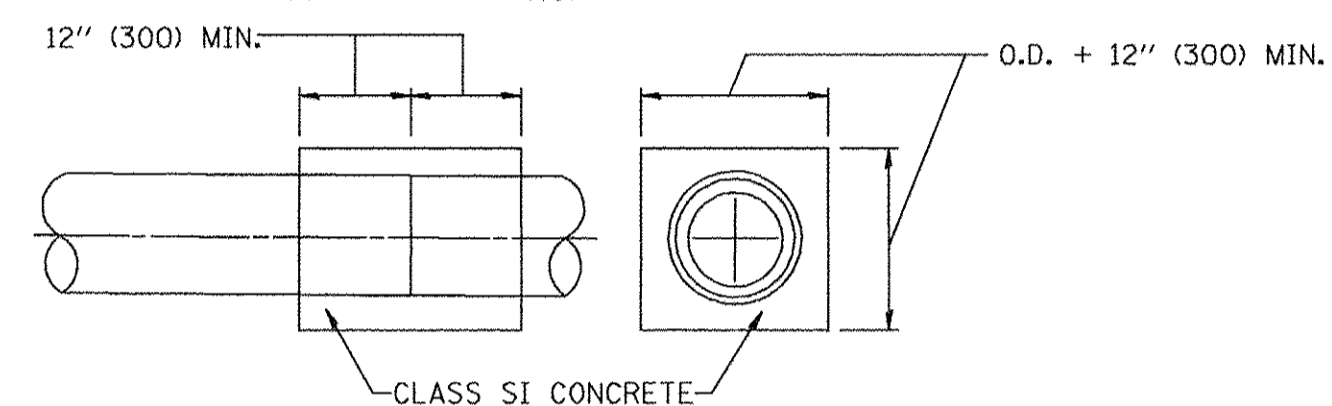
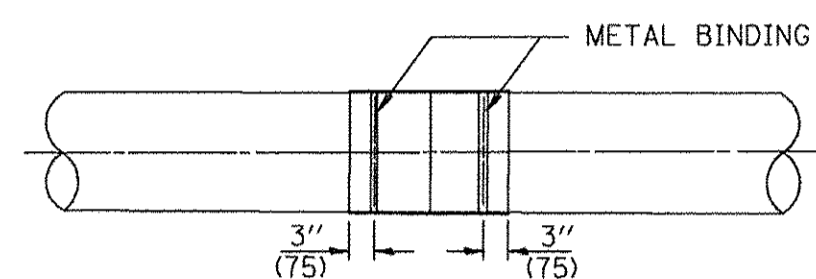
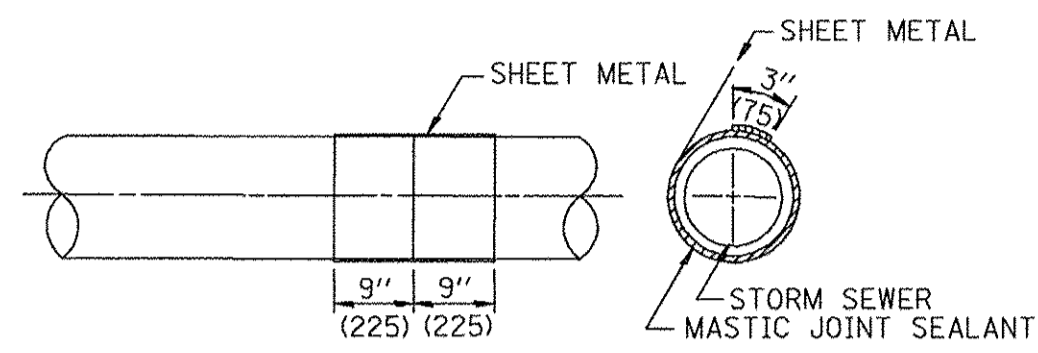
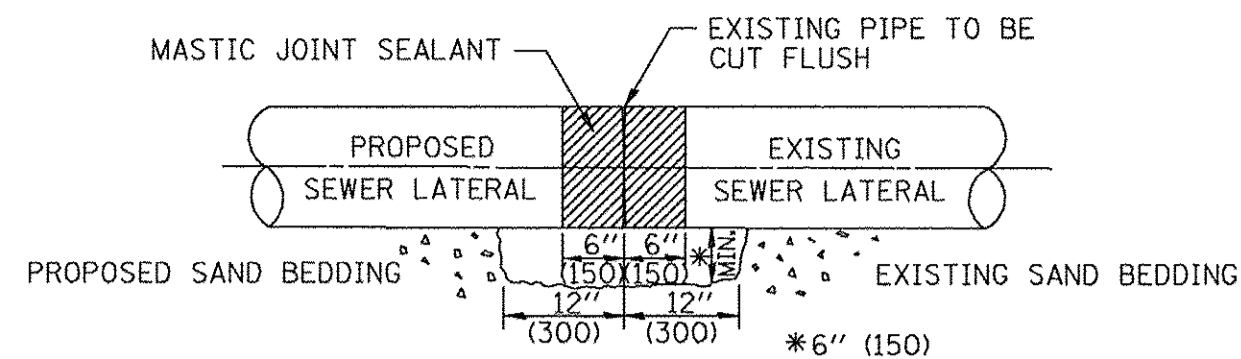
DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)	
SCALE: NONE	SHEET NO. 44 OF 63 SHEETS
STA. TO STA.	

F.A.P. RTE. 353	SECTION 13-00063-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 44
BD0156-07 (BD-01)		CONTRACT NO. 61C11		
FED. ROAD DIST. NO. 1		ILLINOIS		FED. AID PROJECT M-4003(216)



DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER

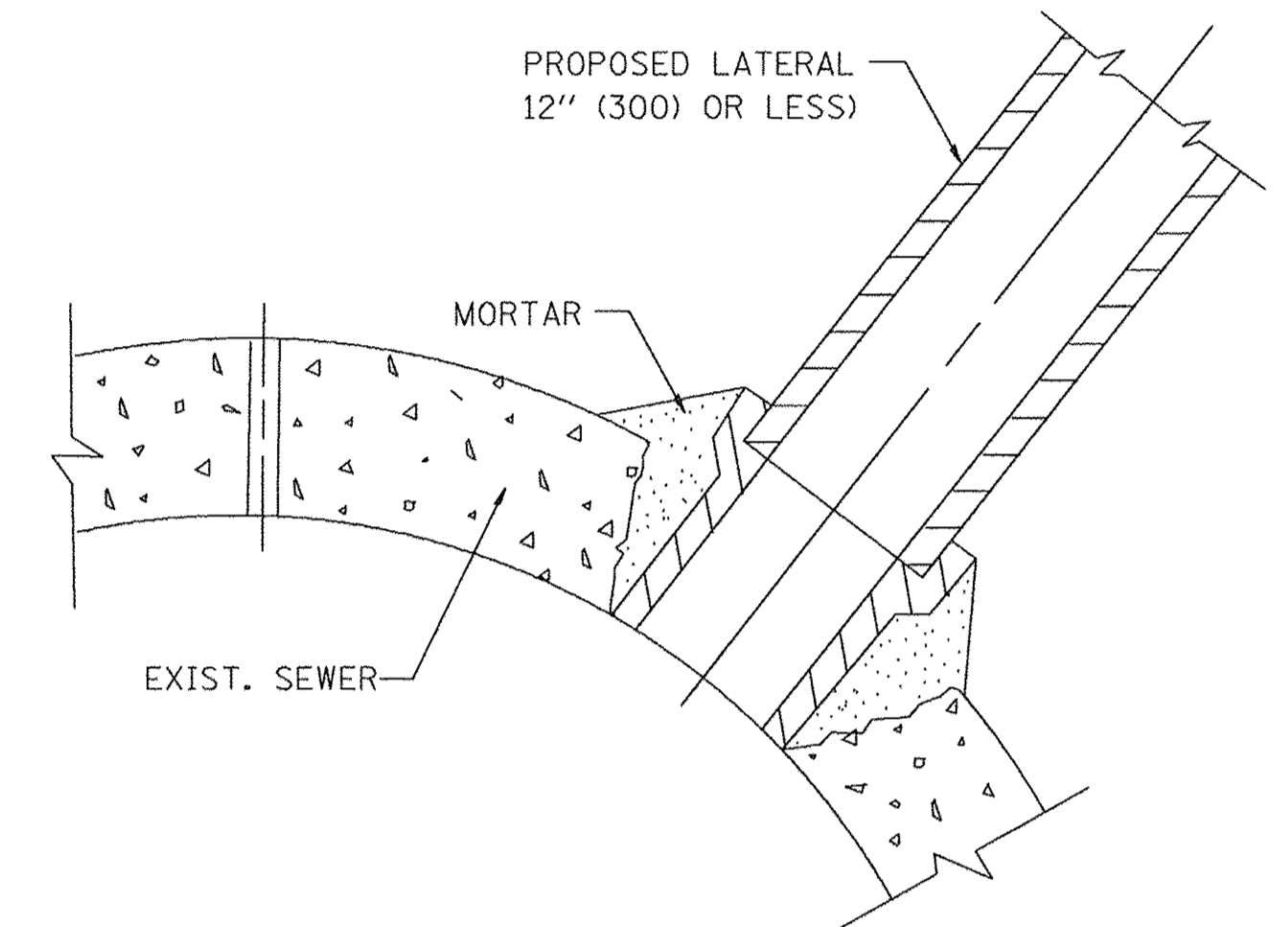


DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

- CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
- APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
- BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12' x 6' (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
- CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
- WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
- LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
- PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
- WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
- PLACE CLASS SI CONCRETE AROUND THE JOINT.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

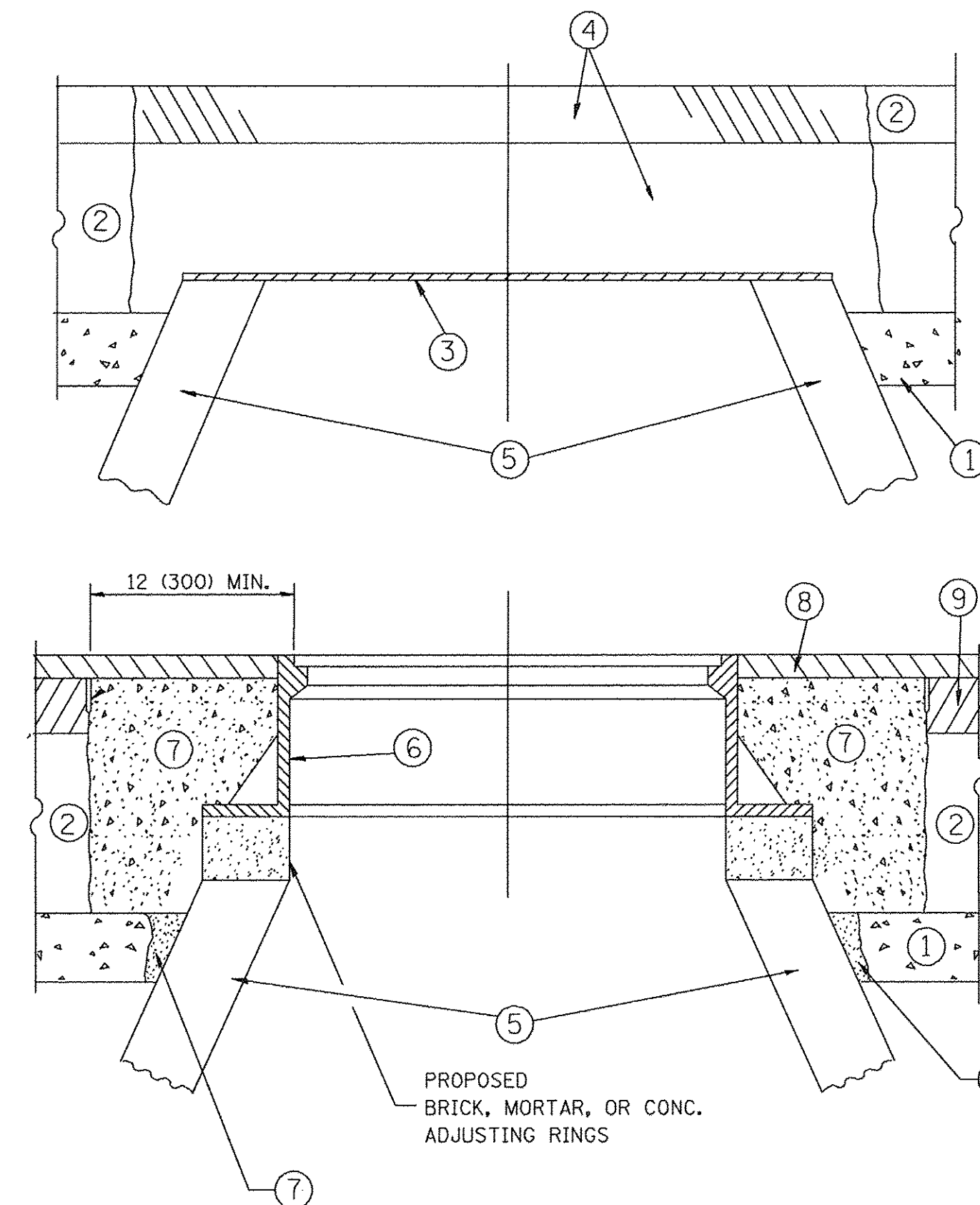
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = 13375_02-DTLS-01 - BD7

USER NAME = geglmanobt	DESIGNED — M. DE YONG	REVISED — V. DE YONG 05-08-92
	CHECKED —	REVISED — R. SHAH 09-09-94
PLOT SCALE = 50.000' / IN.	DRAWN —	REVISED — R. SHAH 10-25-94
PLOT DATE = 1/4/2008	CHECKED — 07-25-90	REVISED — R. SHAH 06-12-96

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE		353	13-00063-00-CH	COOK	63	46
SHEET NO. 46 OF 63 SHEETS		BD500-01 (BD-7)		CONTRACT NO. 61C11		
STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS		FED. AID PROJECT M-4003(216)		



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

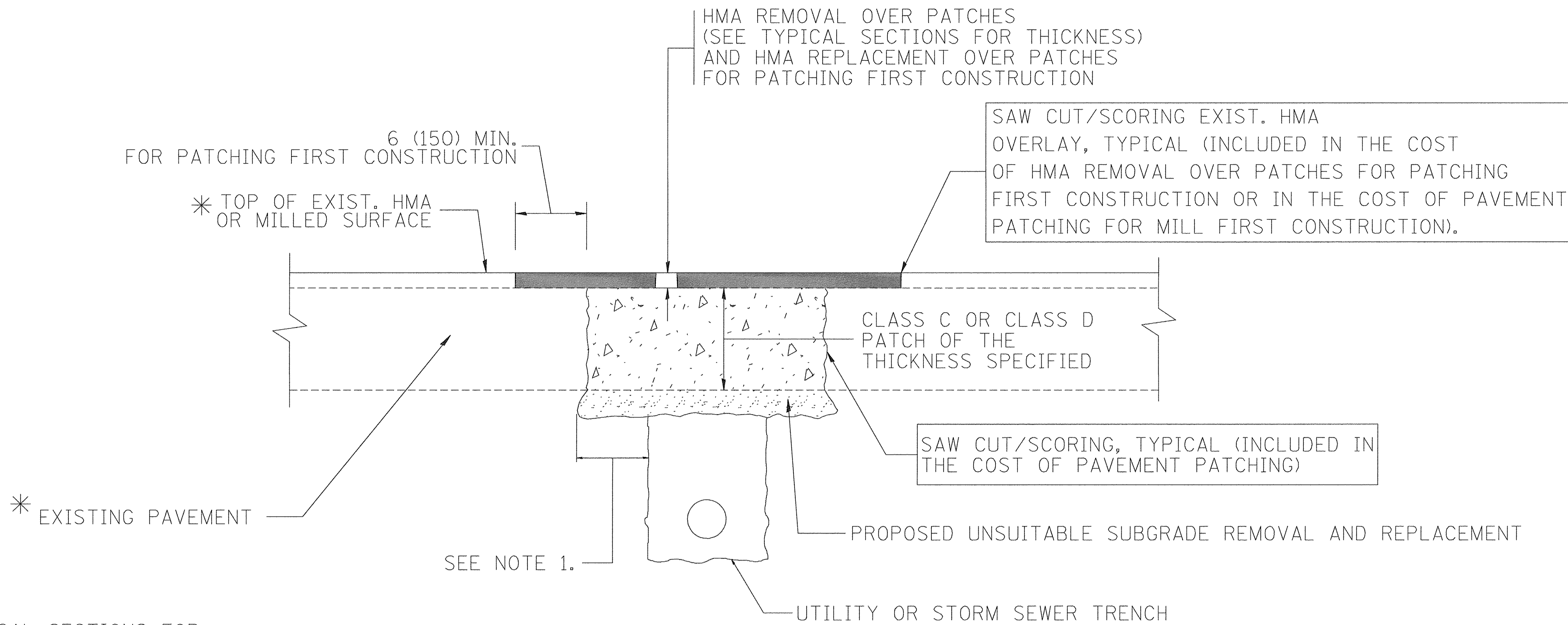
THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME = 13375_02-DTLS-01 - BD08	USER NAME = bauerdl	DESIGNED — R. SHAH	REVISED — R. WIEDEMAN 05-14-04	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING	F.A.P. RTE. 353	SECTION 13-00063-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 47			
	PLOT SCALE = 1/8" = 1' / m	CHECKED —	REVISED — R. BORO 01-01-07			SCALE: NONE	SHEET NO. 47 OF 63 SHEETS	STA. TO STA.	BD600-03 (BD-B)		CONTRACT NO. 61C11		
	PLOT DATE = 12/6/2011	DRAWN —	REVISED — R. BORO 03-09-11					FED. ROAD DIST. NO. 1		ILLINOIS		FED. AID PROJECT M-4003(218)	
		CHECKED — 10-25-94	REVISED — R. BORO 12-06-11										



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

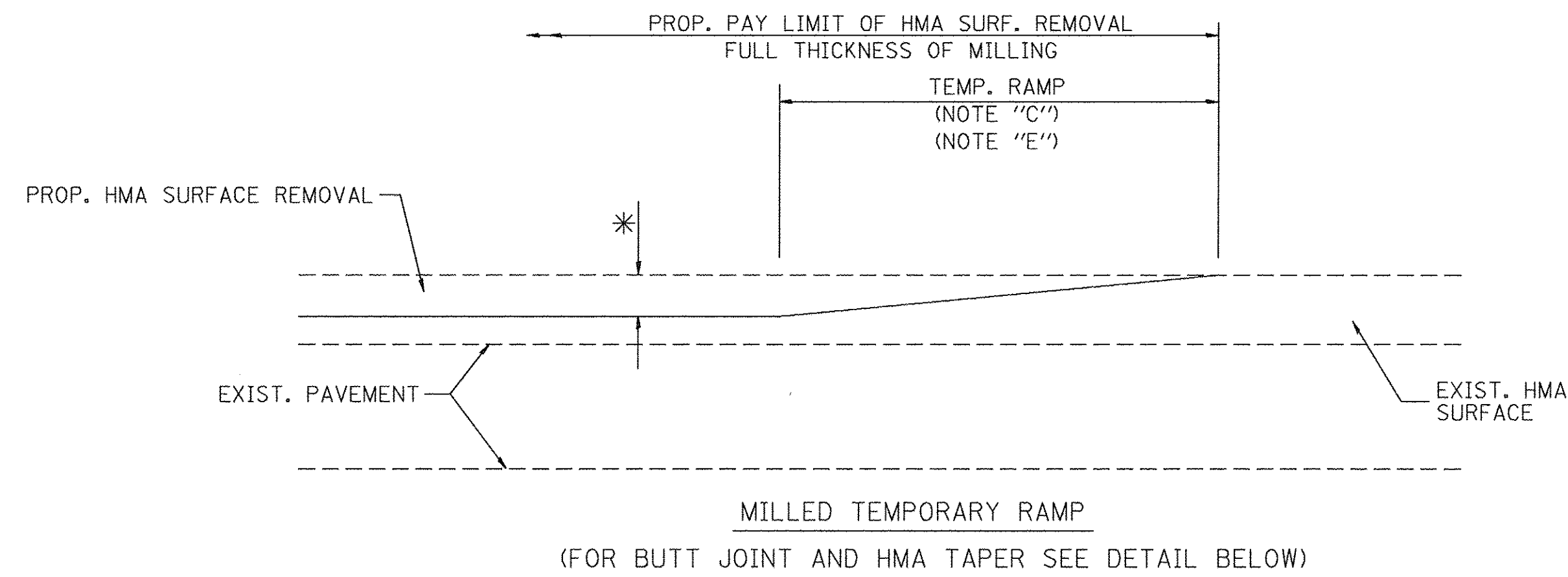
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

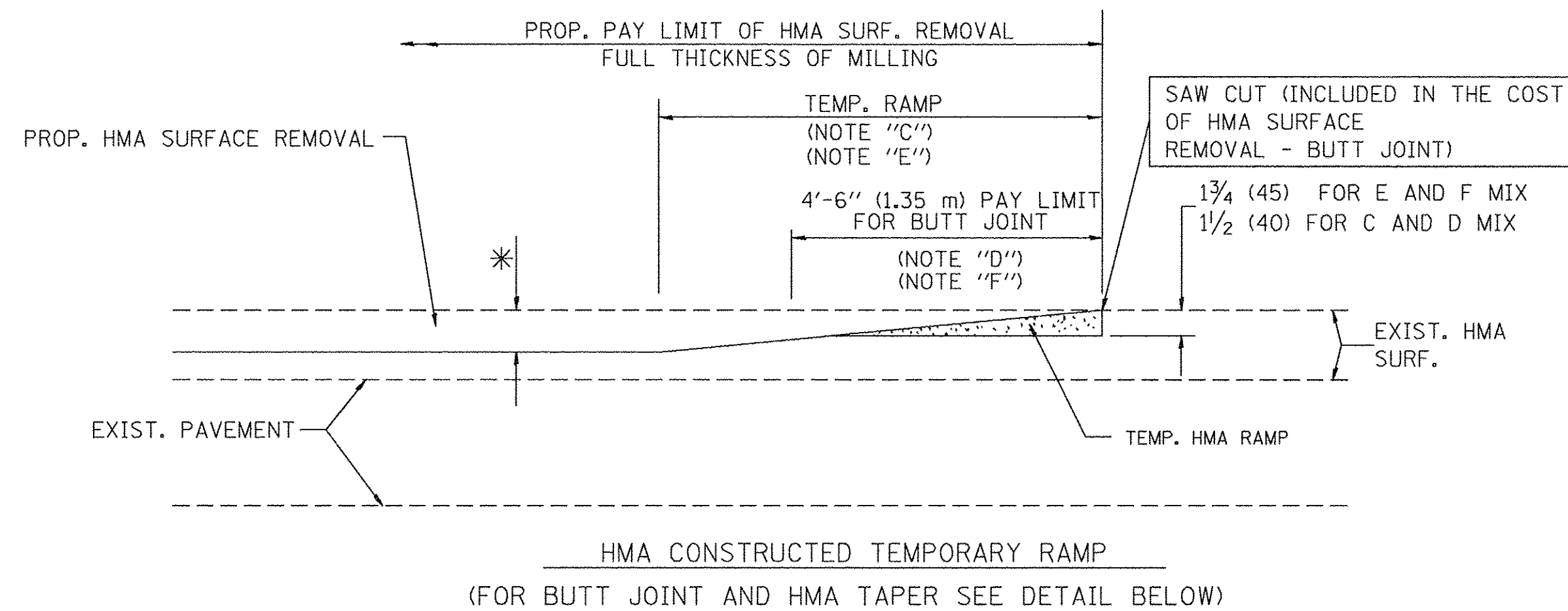
1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

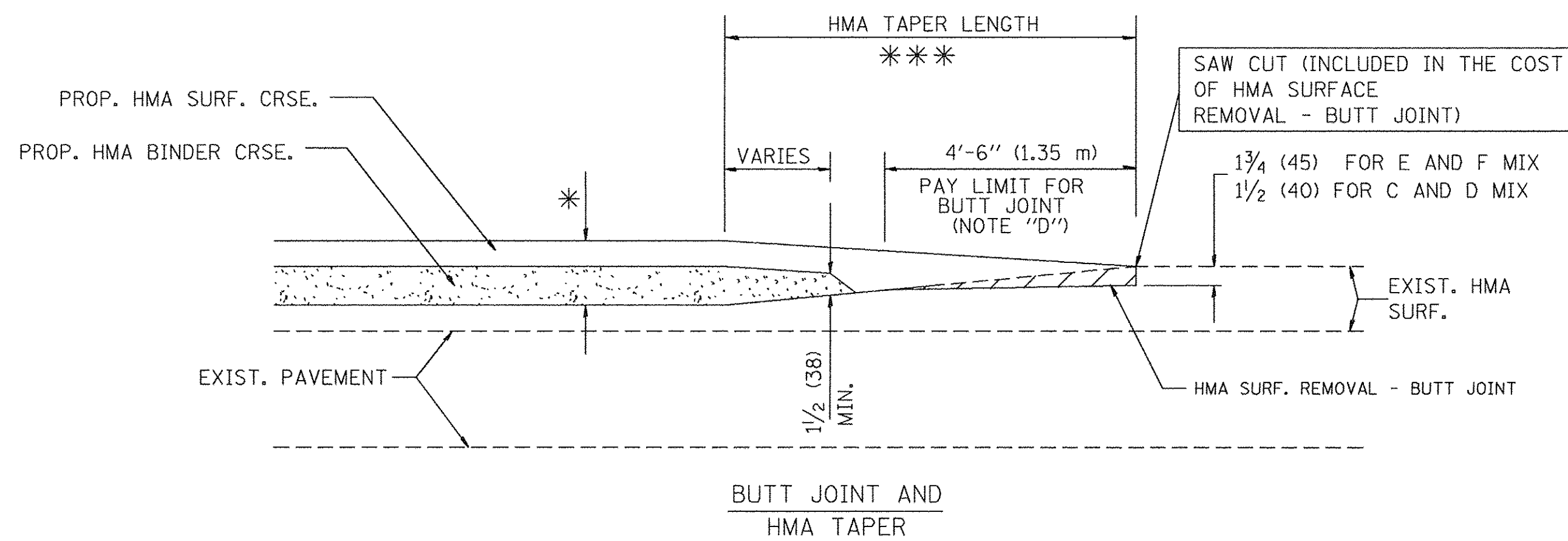
FILE NAME = 13375_02-DTLS-01 - BD22	USER NAME = bauerdl	DESIGNED -- R. SHAH	REVISED -- A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	F.A.P. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		CHECKED --	REVISED -- R. BORO 01-01-07			353	13-00063-00-CH	COOK	63	48	
	PLOT SCALE = 50.000' / 1"	DRAWN --	REVISED -- R. BORO 09-04-07			BD400-04 (BD-22)		CONTRACT NO. 61C11			
	PLOT DATE = 10/27/2008	CHECKED -- 10-25-94	REVISED -- K. ENG 10-27-08			SCALE: NONE	SHEET NO. 48 OF 63 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS



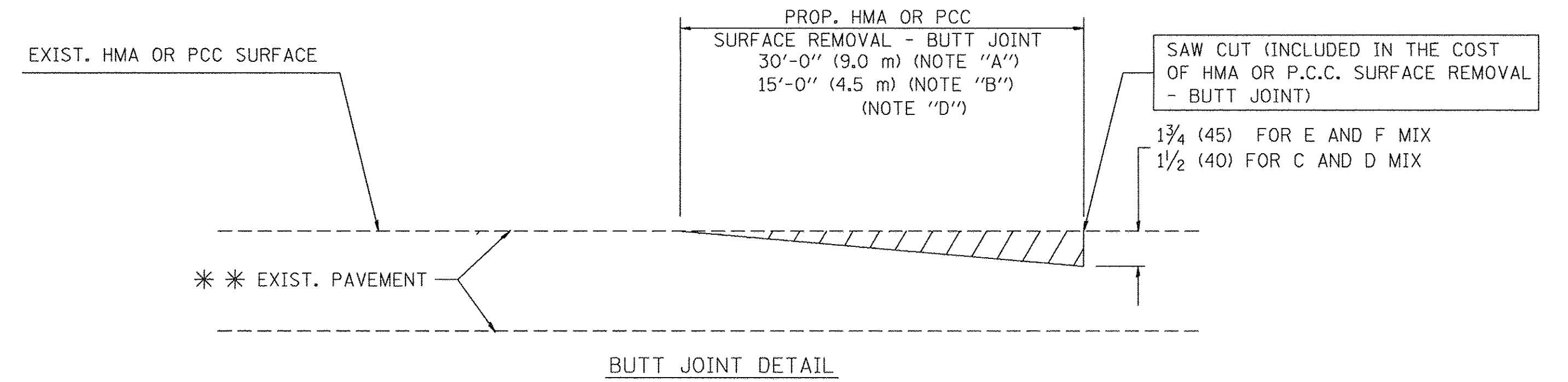
OPTION 1



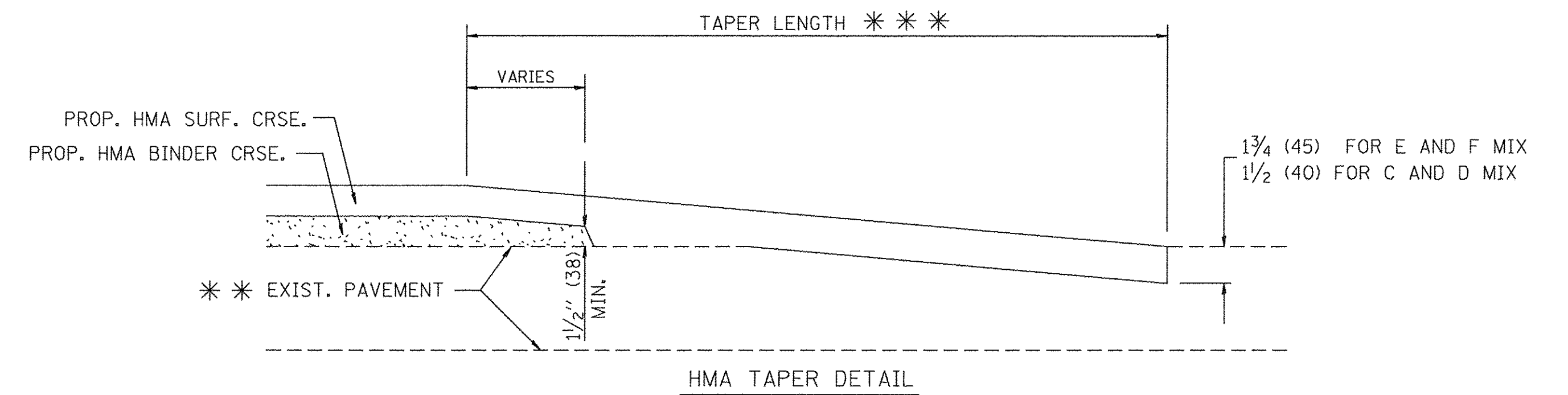
OPTION 2
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = 13375_02-DTLS-01 - BD32

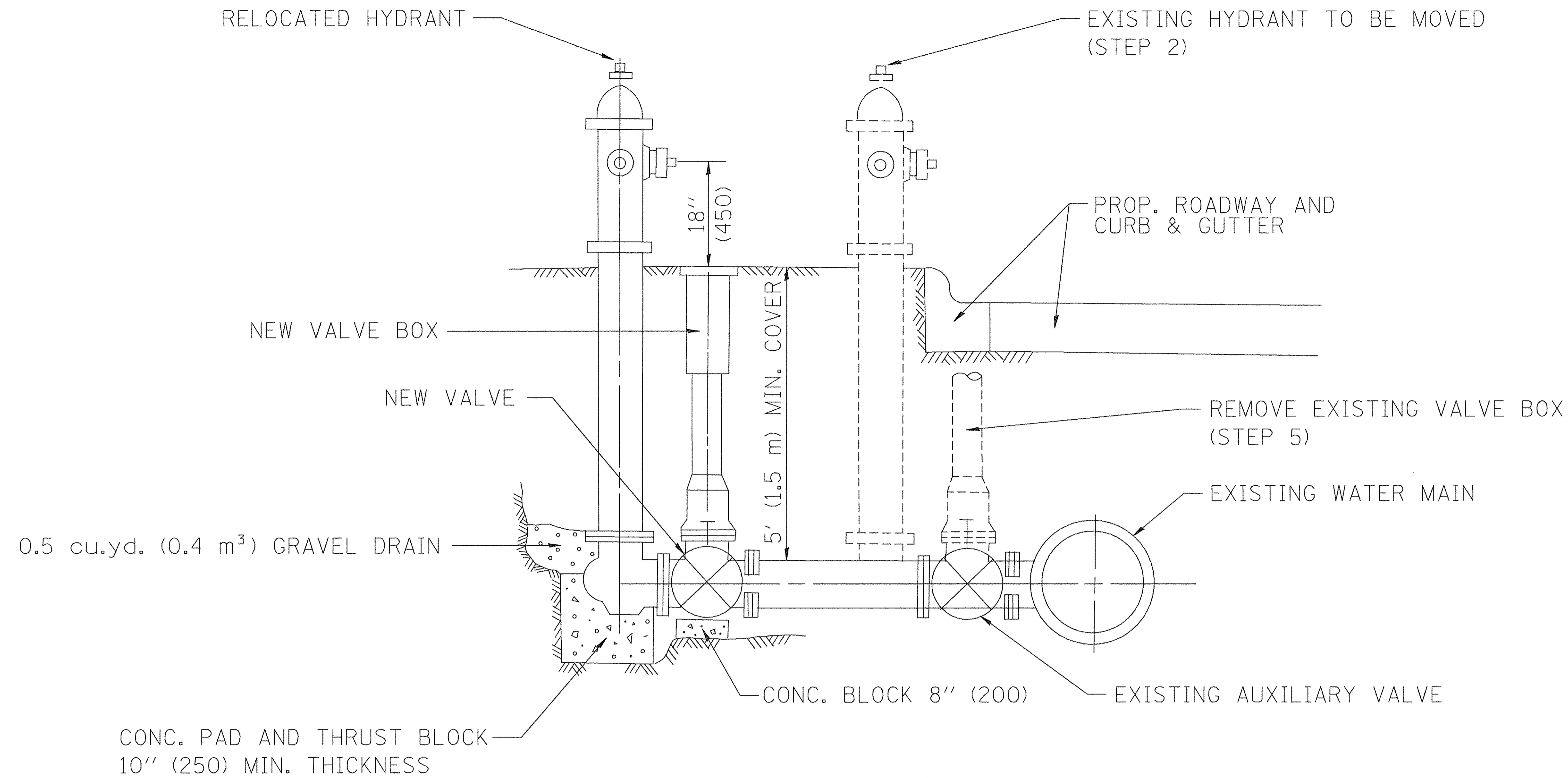
USER NAME = gaglianob	DESIGNED — M. DE YONG	REVISED — R. SHAH 10-25-94
PLOT SCALE = 50.0000 / / IN.	CHECKED —	REVISED — A. ABBAS 03-21-97
PLOT DATE = 1/4/2008	DRAWN —	REVISED — M. GOMEZ 04-06-01
	CHECKED — 06-13-90	REVISED — R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 49 OF 63 SHEETS STA. TO STA.

F.A.P. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	13-00063-00-CH	COOK	63	49
BD400-05 BD32		CONTRACT NO. 61C11		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT M-4003(216)	



SEQUENCE OF CONSTRUCTION:

1. CLOSE EXISTING VALVE.
2. REMOVE EXISTING HYDRANT.
3. INSTALL HYDRANT EXTENSION AND NEW VALVE.
4. RELOCATE EXISTING HYDRANT.
5. OPEN EXISTING VALVE, REMOVE BOX.
6. BACKFILL.
7. FLUSH AND TEST FOR CHLORIDE RESIDUAL AND PROVIDE TEST.

ALL WORK TO BE DONE IN ACCORDANCE WITH ARTICLE 564 OF THE STANDARD SPECIFICATIONS. NEW VALVE AND BOX SHALL BE SAME MAKE AND MODEL AS EXISTING.

FIRE HYDRANT TO BE MOVED

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

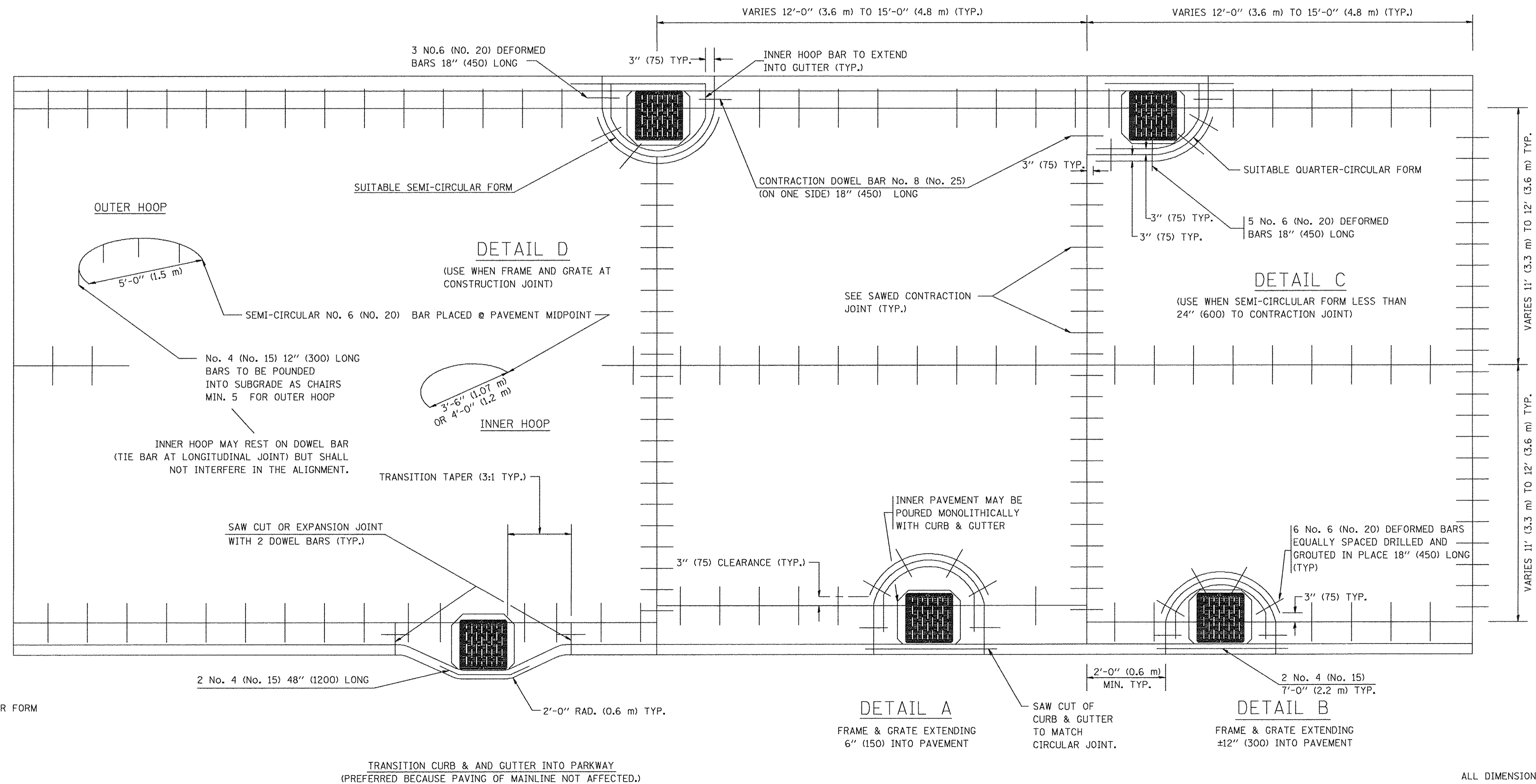
FILE NAME = 13375_02-DTLS-01 - B036	USER NAME = geglianobt	DESIGNED —	REVISED — R. SHAH 09-09-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FIRE HYDRANT TO BE MOVED			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50.0000' / IN.	DRAWN —	REVISED — R. SHAH 10-25-94					353	13-00063-00-CH	COOK	63	49A
	PLOT DATE = 1/4/2008	CHECKED —	REVISED —		SCALE: NONE SHEET NO. 49A OF 63 SHEETS STA. TO STA.			BD-36		CONTRACT NO. 61C11		
								FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(216)				

FRAME EXTENSION INTO PAVEMENT	INNER HOOP REINFORCEMENT DIAMETER	SEMI CIRCULAR FORM DIAMETER	OUTER HOOP REINFORCEMENT DIAMETER
UP TO 8" (200)	3'-6" (1.1 m)	4'-0" (1.2 m)	5'-0" (1.5 m)
> 8" (200) TO 14" (360)	4'-0" (1.2 m)	4'-6" (1.4 m)	5'-0" (1.5 m)

DESIGNER NOTE:
THIS DETAIL IS TO BE USED
WHEN THE GUTTER FLAG IS
LESS THAN 24"

NOTES :

1. THE ROUNDOUT AND ADDED REINFORCEMENT WILL NOT BE PAID SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE PAVEMENT.
2. TRANSVERSE JOINTS MAY BE MOVED TO ACCOMMODATE ROUNDOUT, EDGE OF CIRCULAR JOINT SHALL BE MINIMUM 12" (300) FROM TRANSVERSE JOINT. RELOCATED TRANSVERSE JOINT SHALL BE CONTINUOUS FROM EDGE OF PAVEMENT TO EDGE OF PAVEMENT.
3. SEMI-CIRCULAR FORM SHALL BE REMOVED PRIOR TO DRILL AND GROUT OF TIE BARS.
4. ALL REINFORCED BARS SHALL BE EPOXY COATED.
5. DRILL AND GROUT IS PREFERRED, HOWEVER TIE BARS CAN BE POURED IN PLACE IF CLEARANCE IS PROVIDED TO OUTER EDGE OF FRAME. MINIMUM 2" (50) CLEARANCE.
6. WOOD SHIMS SHALL BE USED TO ADJUST ALL FRAMES. AFTER ADJUSTING MORTAR HAS CURED, THE WOOD SHIMS SHALL BE REMOVED AND THE VOIDS UNDER THE FRAMES FILLED WITH NON SHRINK GROUT.
7. HOOP REINFORCEMENT SHALL BE ONE PIECE CONSTRUCTION.
8. CIRCULAR FRAMES AND GRATES MAY BE SUBSTITUTED.
9. CURB DOWELS MUST BE PLACED LEVEL & TRUE TO ALLOW CONTRACTION MOVEMENT.



LEGEND:

- CASTING
- SUITABLE SEMI-CIRCULAR FORM

ALL DIMENSIONS ARE IN INCHES
(MILLIMETERS) UNLESS OTHERWISE NOTED

FILE NAME = 13375_02-DTLS-01 - BD48

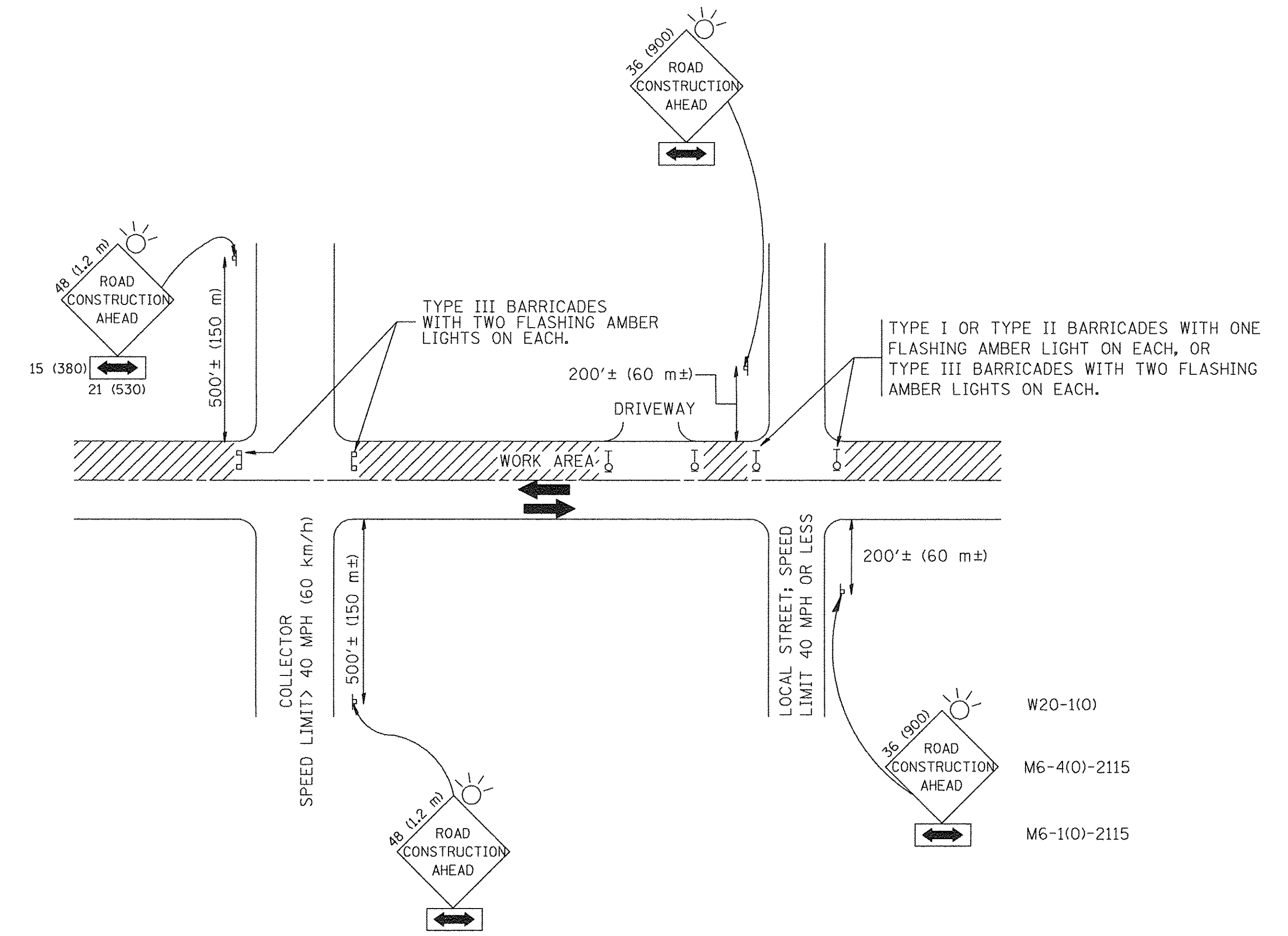
USER NAME = gaglianob	DESIGNED — A. ABBAS	REVISED — T. MATOUSEK 08-28-00
	CHECKED — TOM MATOUSEK	REVISED — T. MATOUSEK 10-02-00
PLOT SCALE = 5/8" = 1' IN.	DRAWN — A. ABBAS	REVISED — T. MATOUSEK 04-25-02
PLOT DATE = 1/4/2008	CHECKED — 01-04-99	REVISED — P. LAFLEUR 08-27-02

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PCC PAVEMENT ROUNDOUTS AT
CURB AND GUTTER

SCALE: NONE SHEET NO. 50 OF 63 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	13-00063-00-CH	COOK	63	50
BD-48			CONTRACT NO. 61C11	
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(216)		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (Inches) unless otherwise shown.

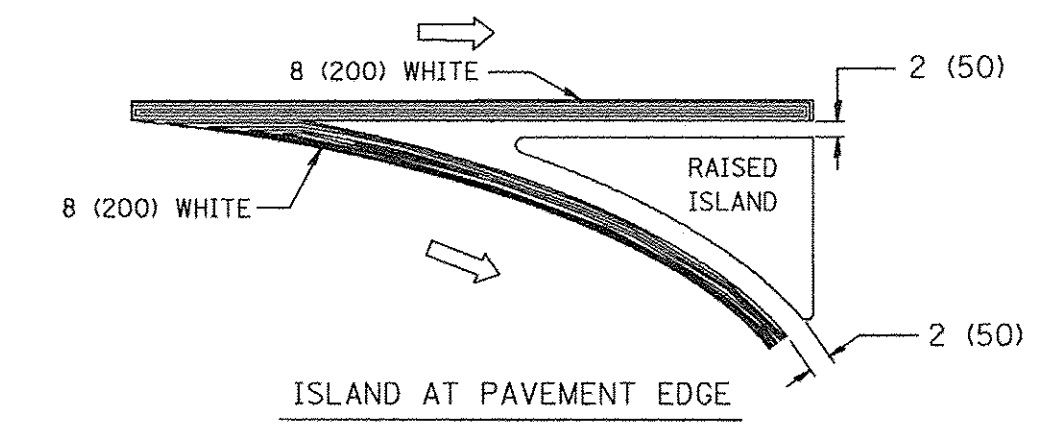
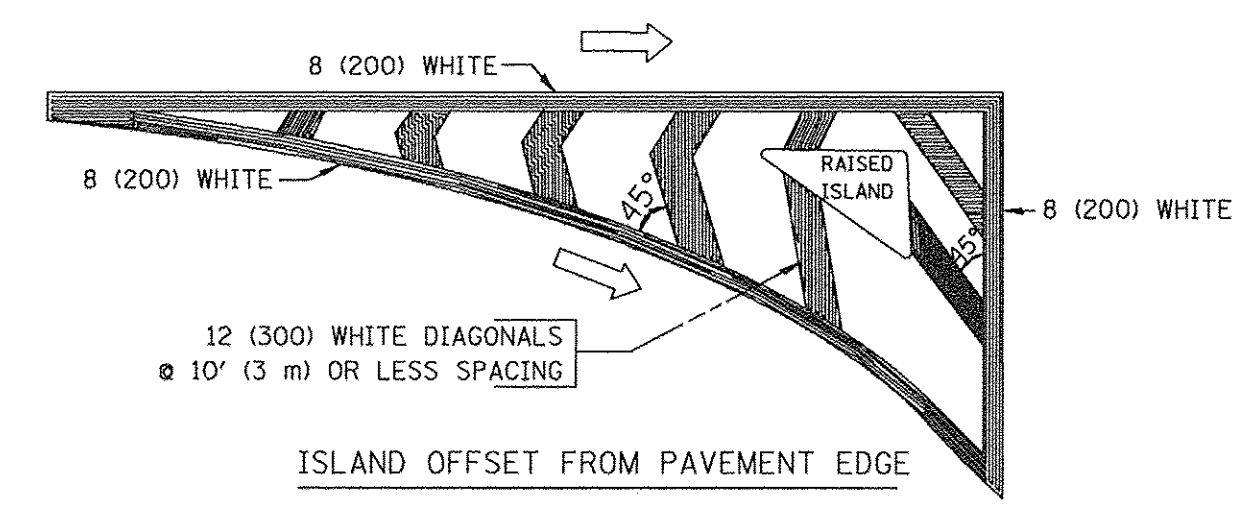
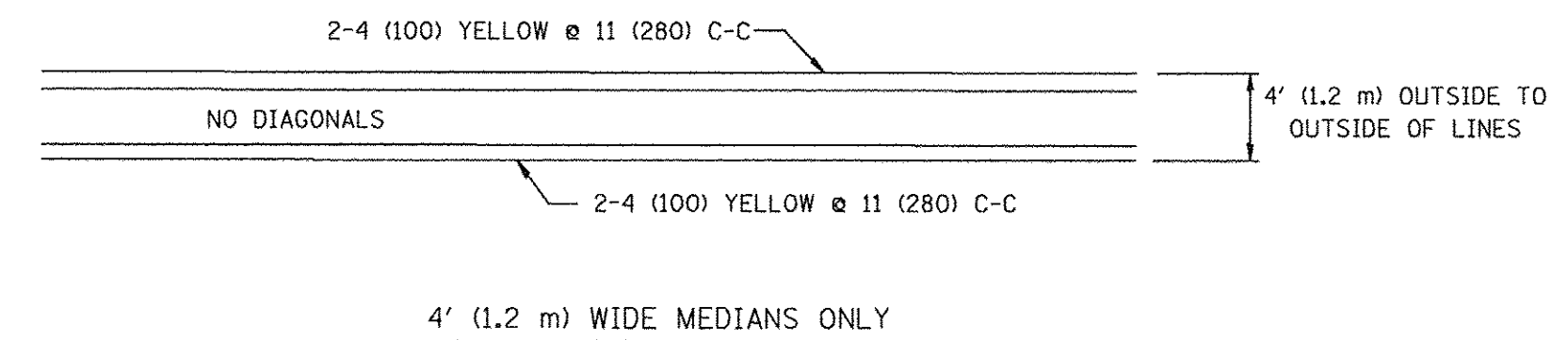
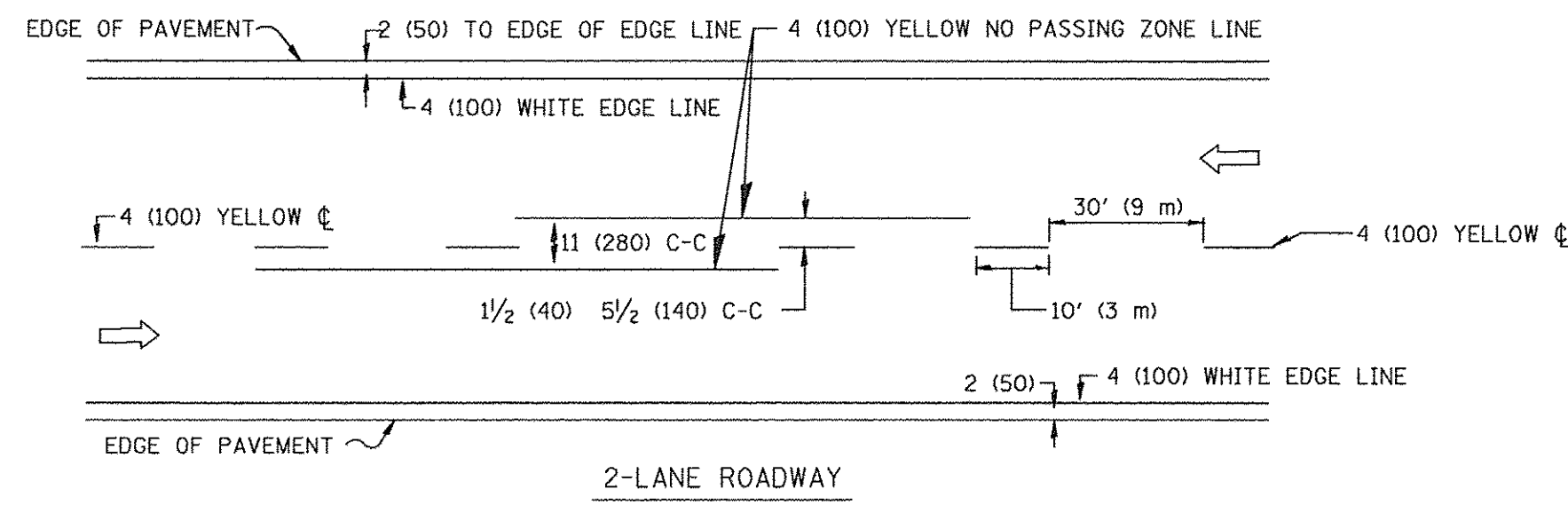
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		CHECKED --	REVISED -- A. HOUSEH 03-06-96
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	PLOT DATE = 1/4/2008	CHECKED -- 06-89	REVISED -- T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

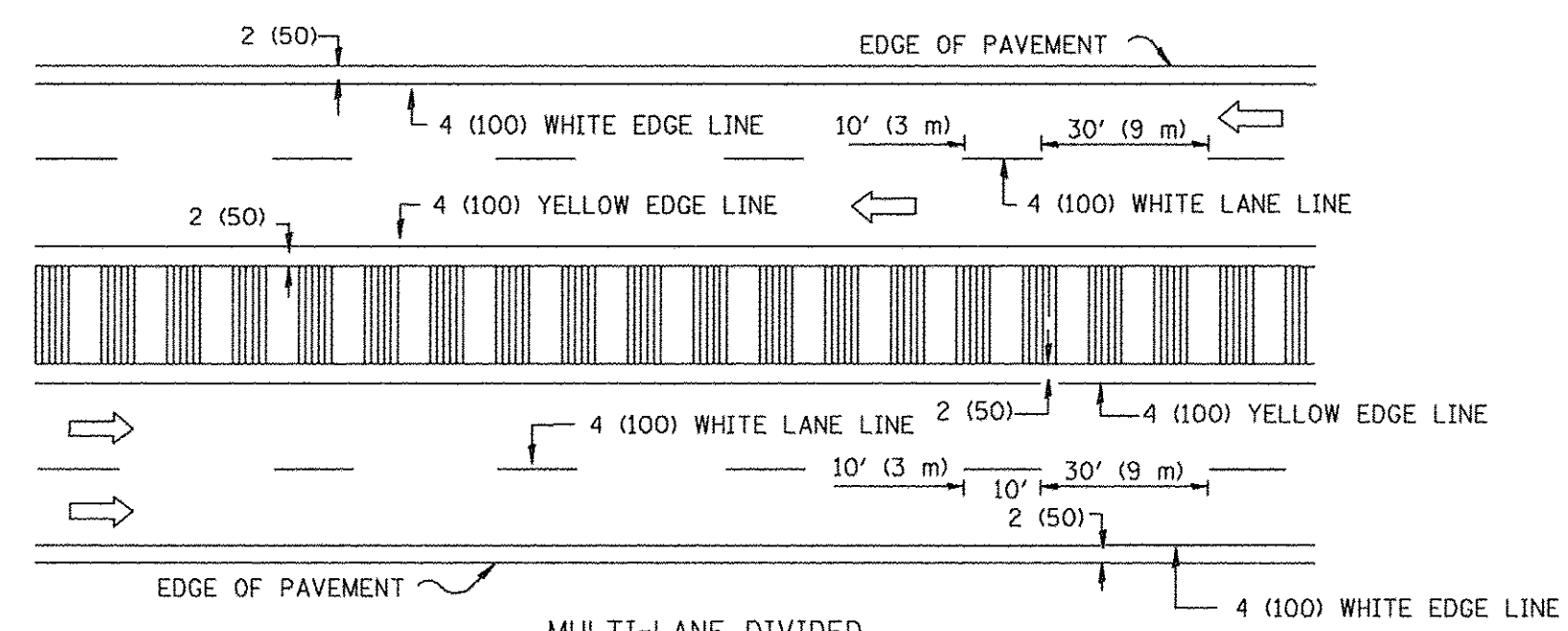
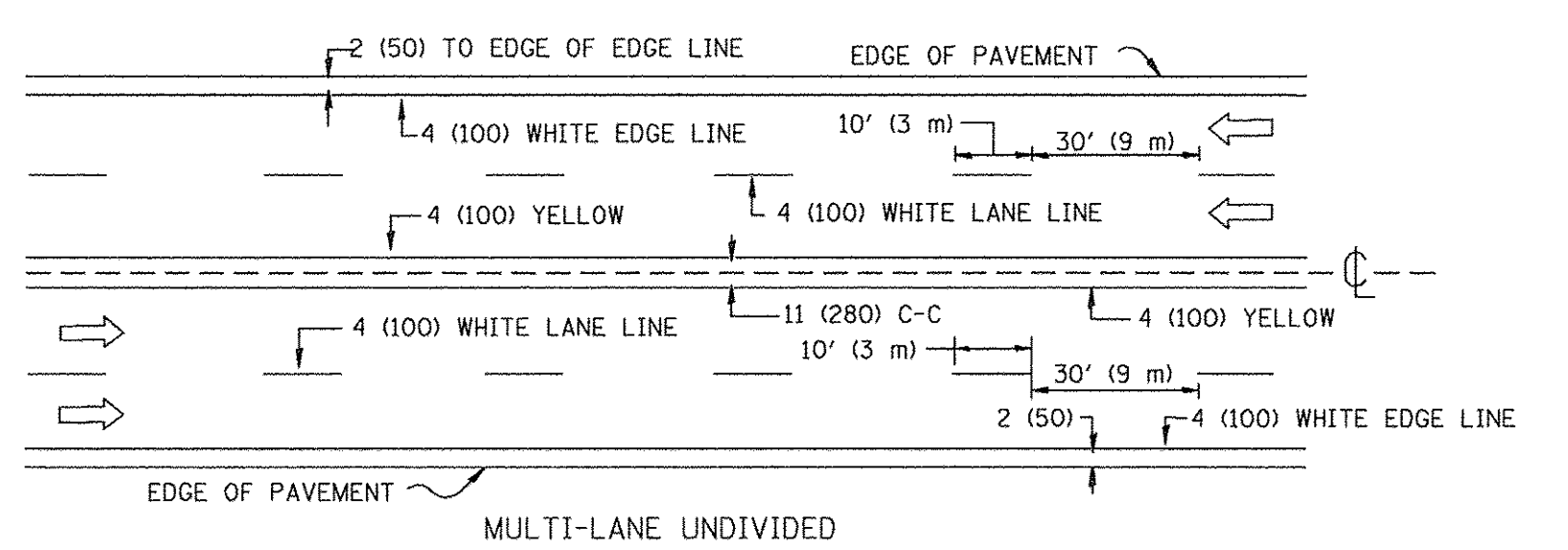
TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET NO. 50A OF 63 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	13-00063-00-CH	COOK	63	50A
TC-10		CONTRACT NO. 61C11		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(216)		

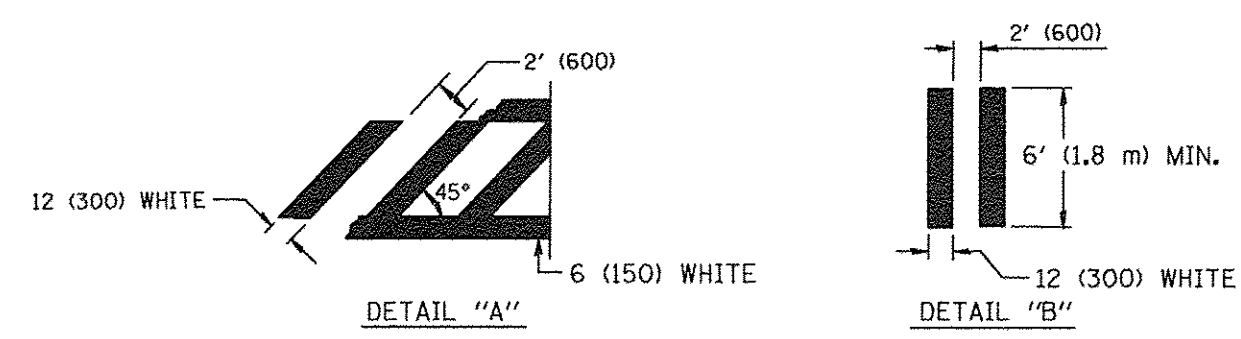
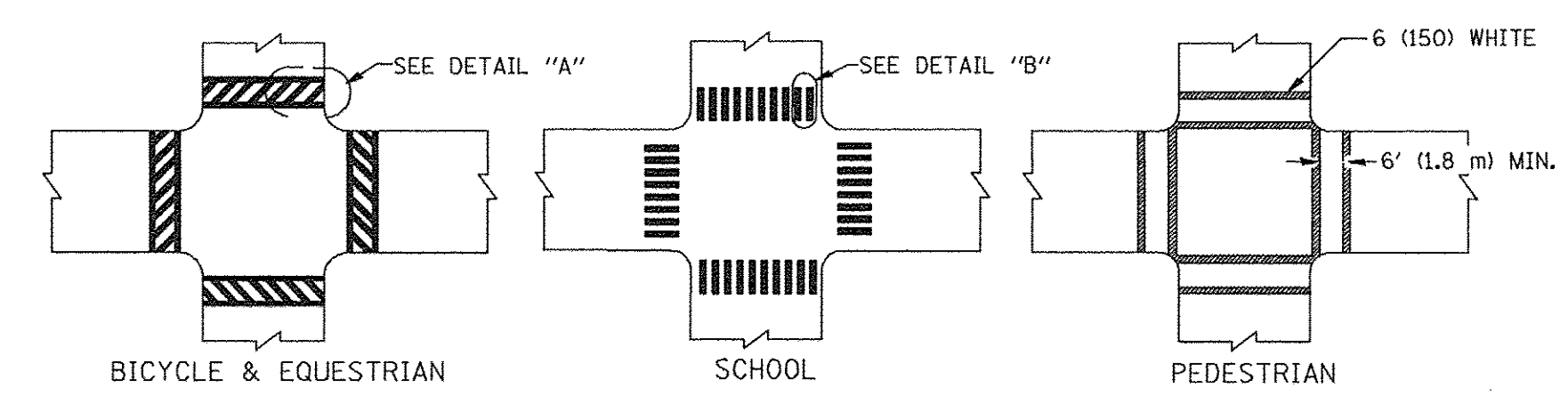


TYPICAL ISLAND MARKING

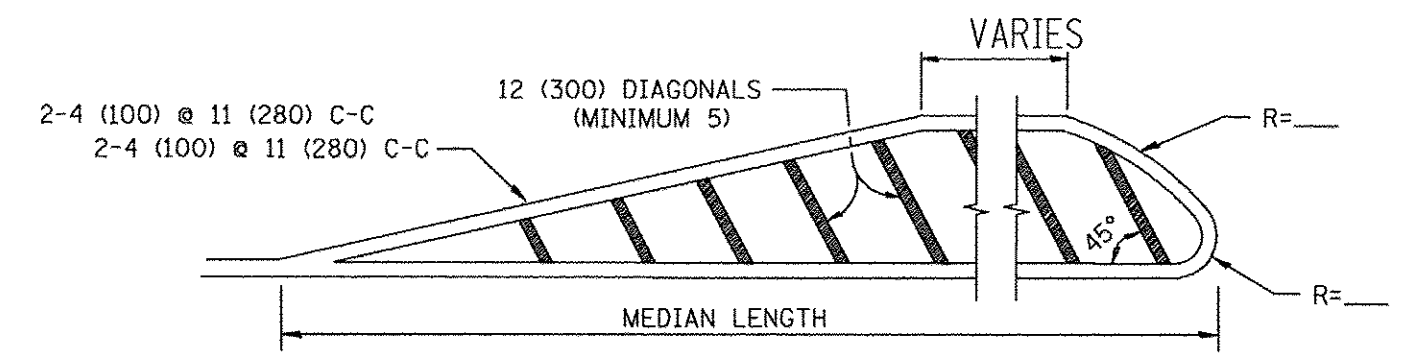


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



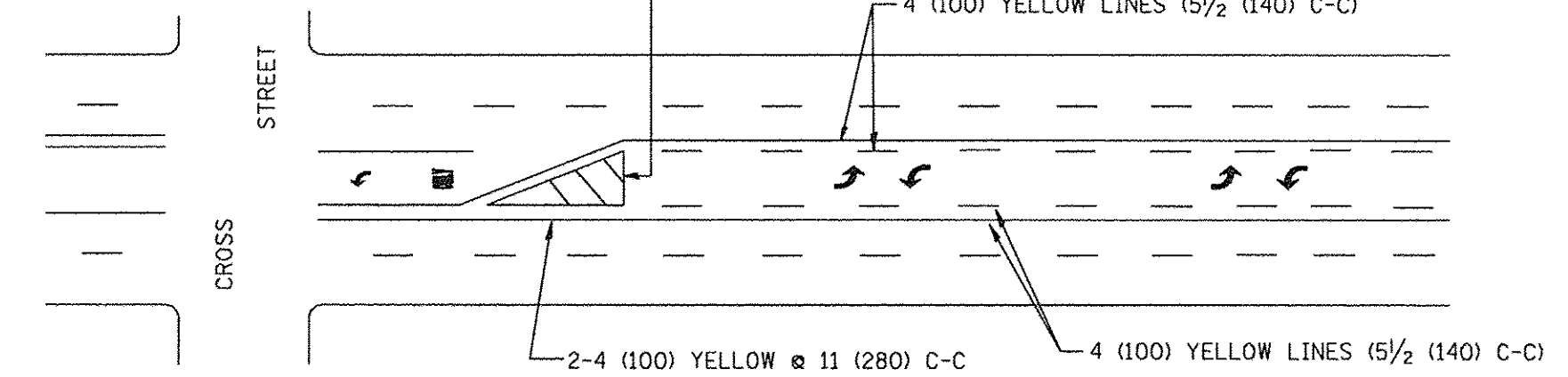
TYPICAL CROSSWALK MARKING



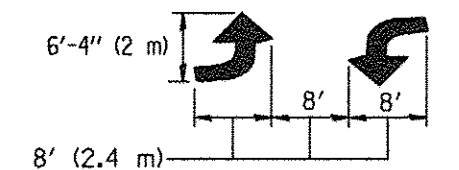
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

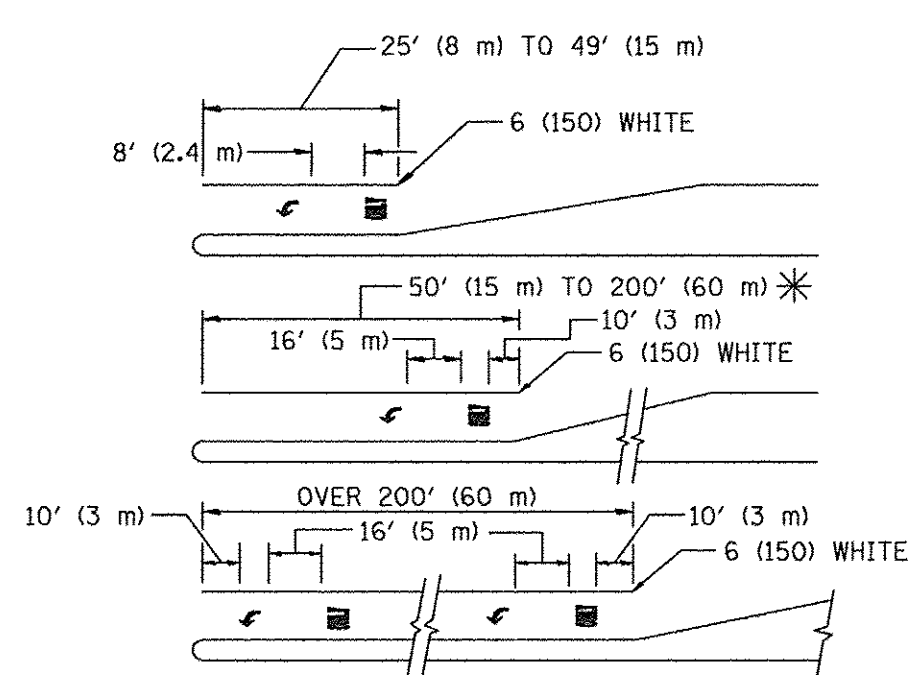


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN)	2 @ 6 (150)	SOLID	WHITE	NOT LESS THAN 6' (1.8 m) APART
A. DIAGONALS (BIKE & EQUESTRIAN)	12 (300) @ 45°	SOLID	WHITE	2' (600) APART
B. LONGITUDINAL BARS (SCHOOL)	12 (300) @ 90°	SOLID	WHITE	2' (600) APART
				SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

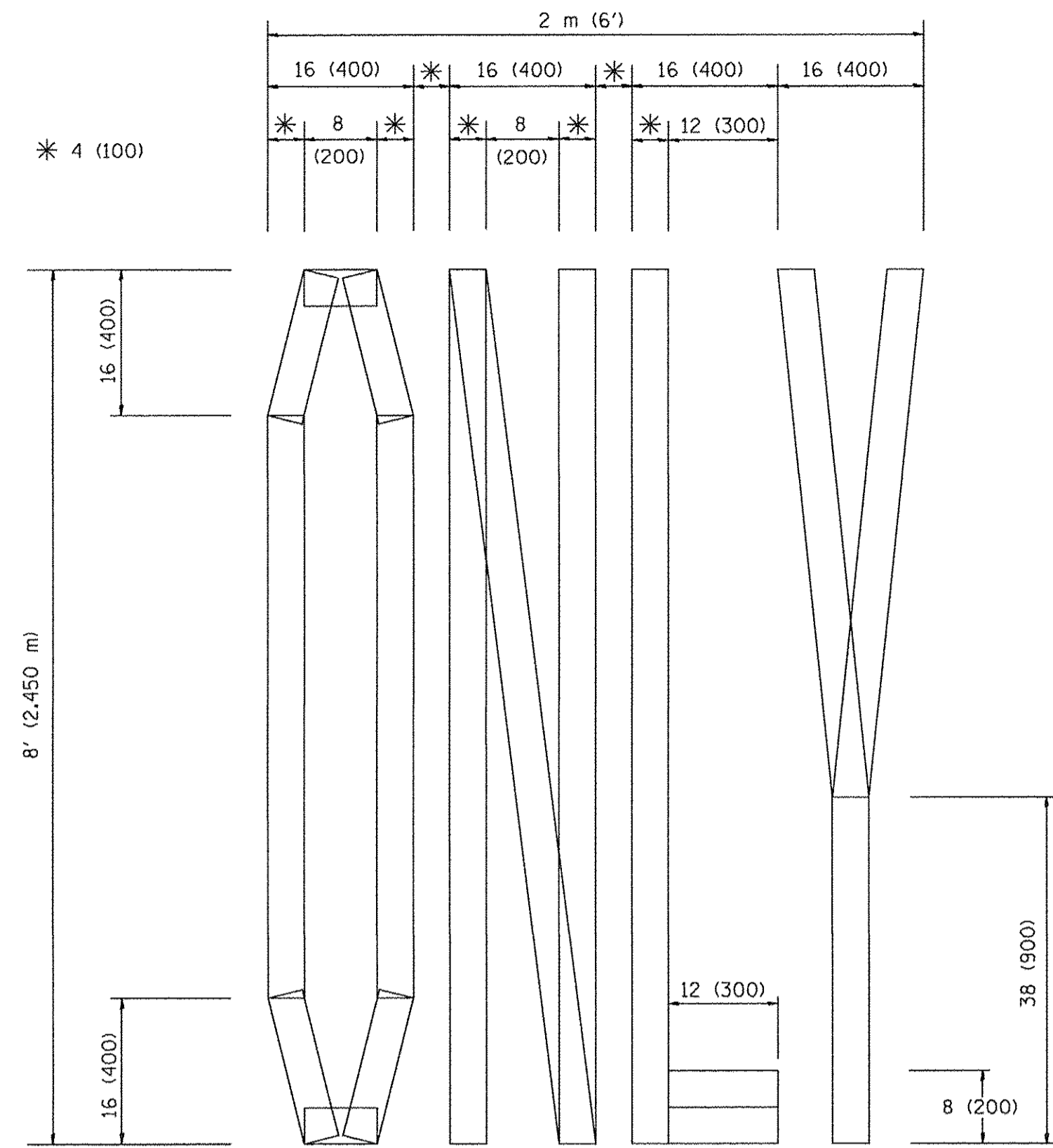
All dimensions are in inches (millimeters) unless otherwise shown.

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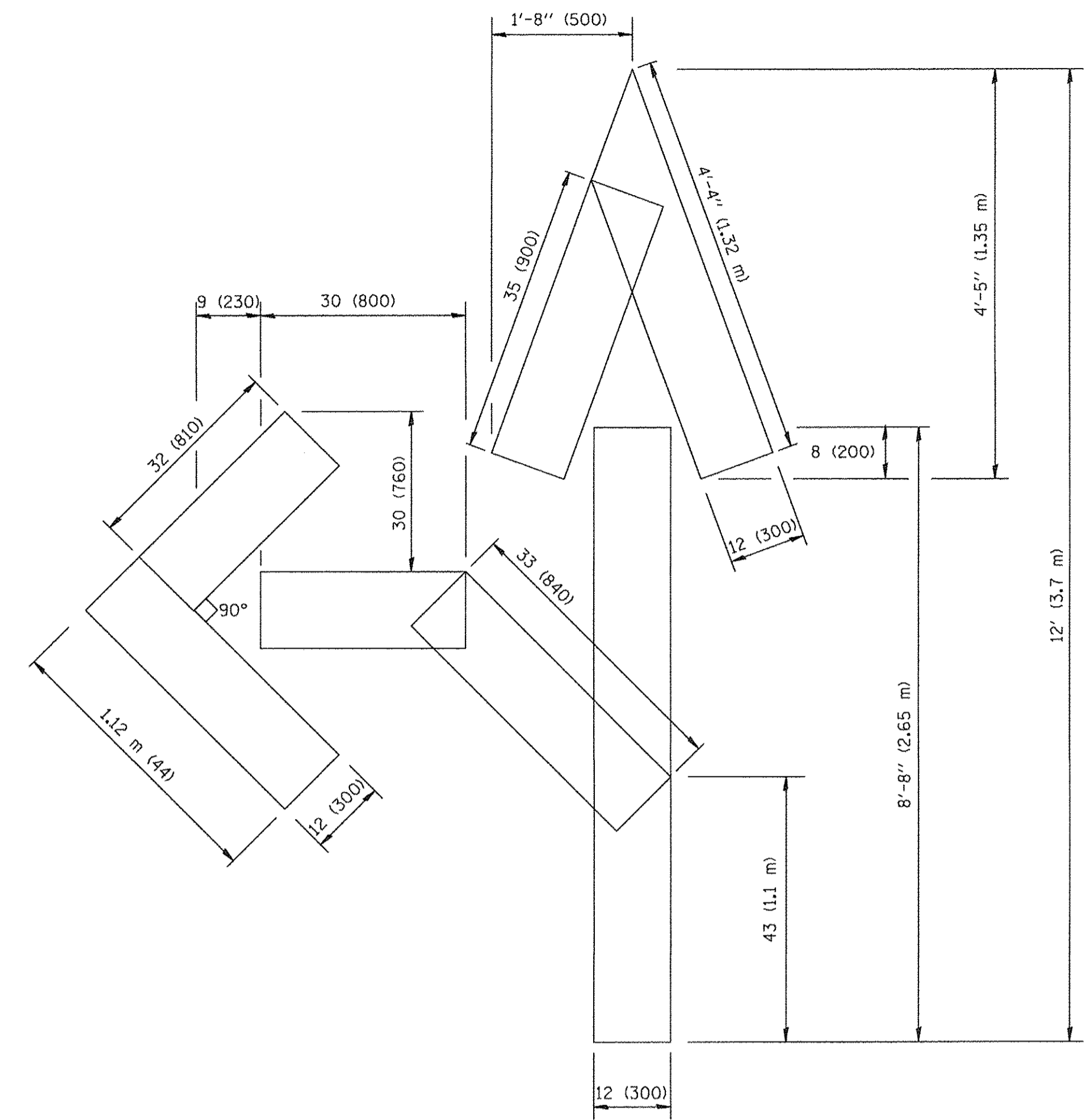
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	CHECKED --	REVISED -- C. JUCIUS 09-09-09
PLOT SCALE = 5/8" = 1'	DRAWN --	REVISED --
PLOT DATE = 9/9/2009	CHECKED -- 03-19-90	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

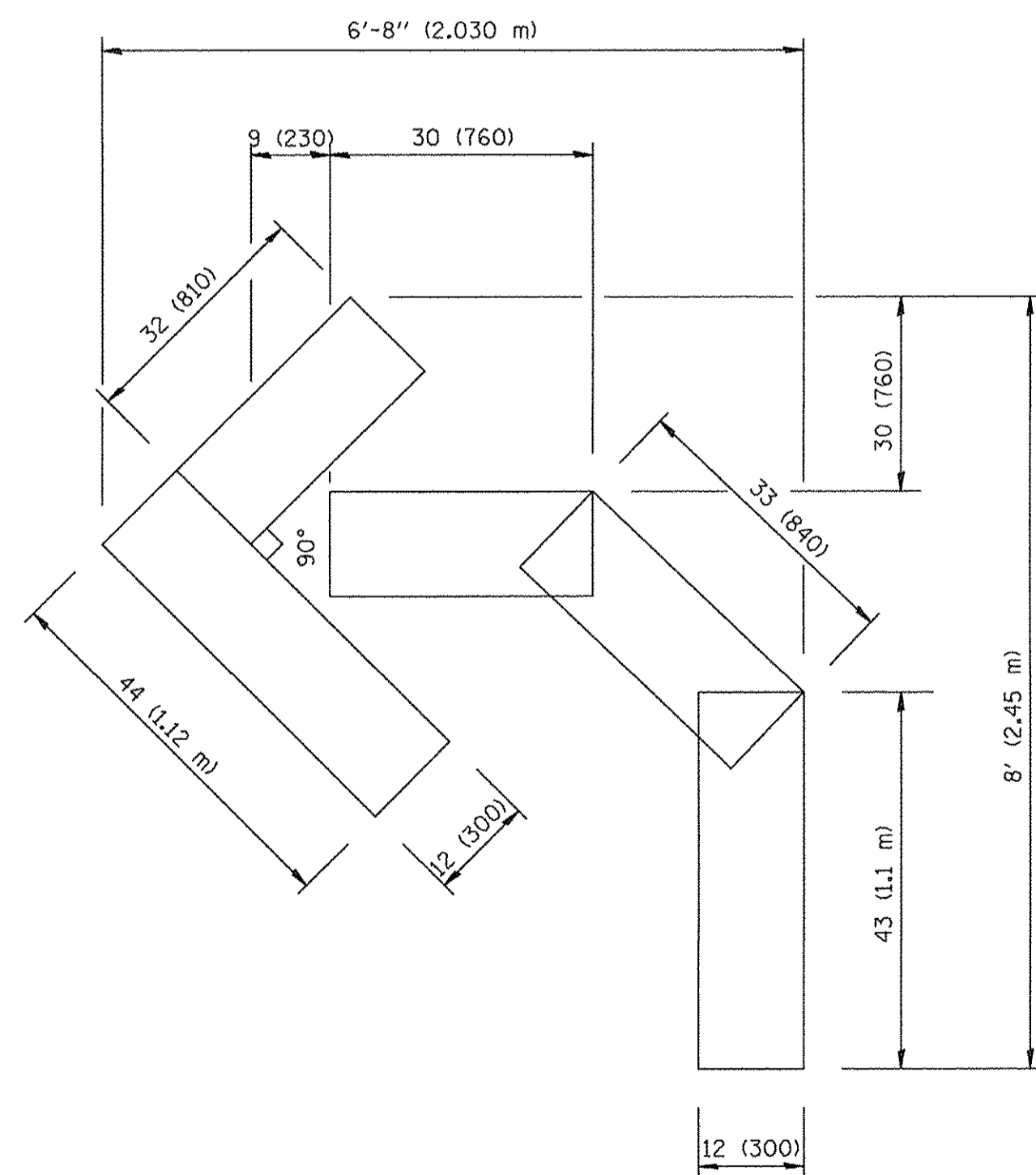
DISTRICT ONE		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TYPICAL PAVEMENT MARKINGS		353	13-00063-00-CH	COOK	63	51
SCALE: NONE		SHEET NO. 51 OF 63 SHEETS		STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(216)	



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

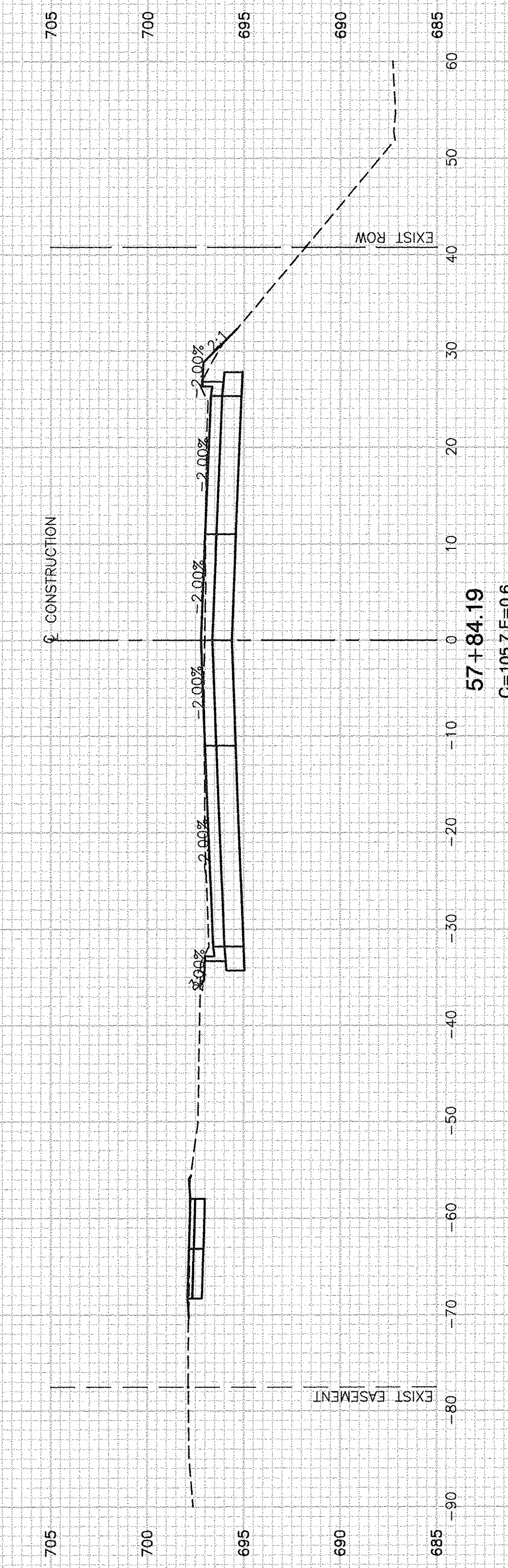
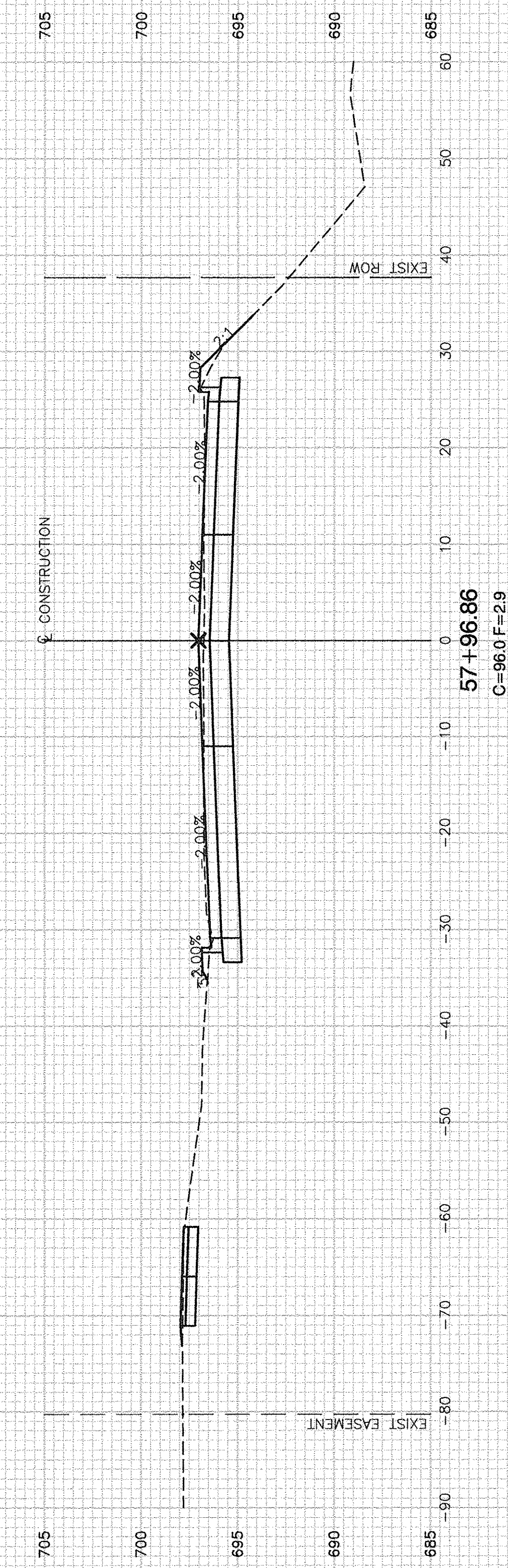
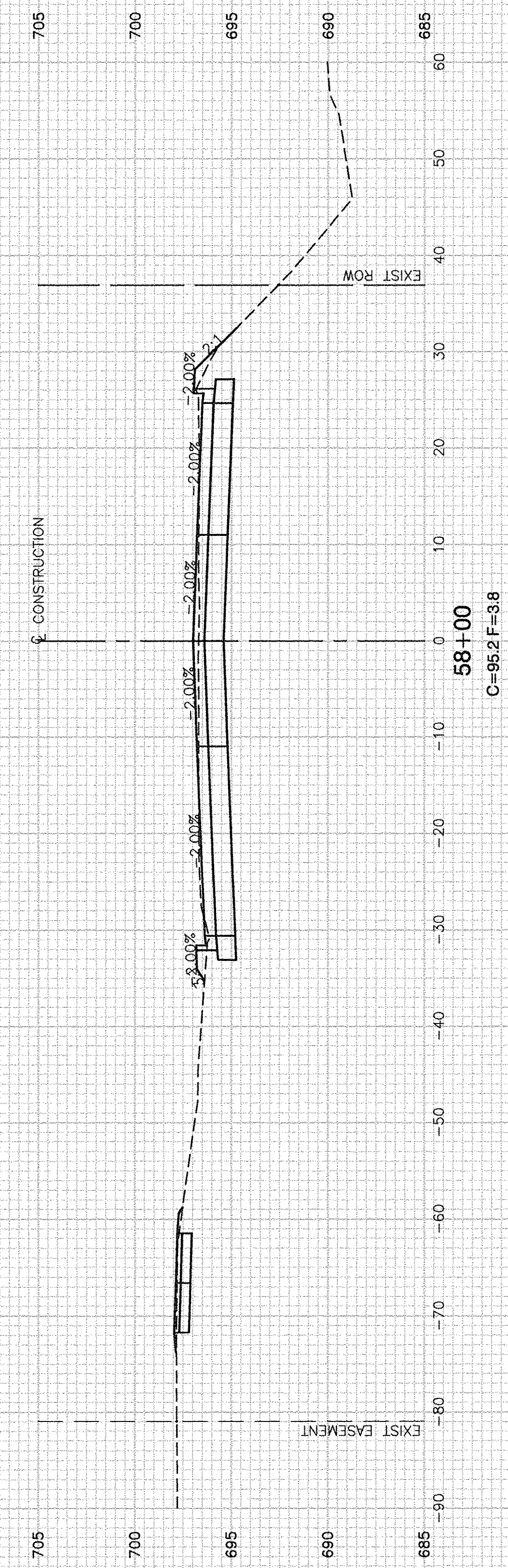
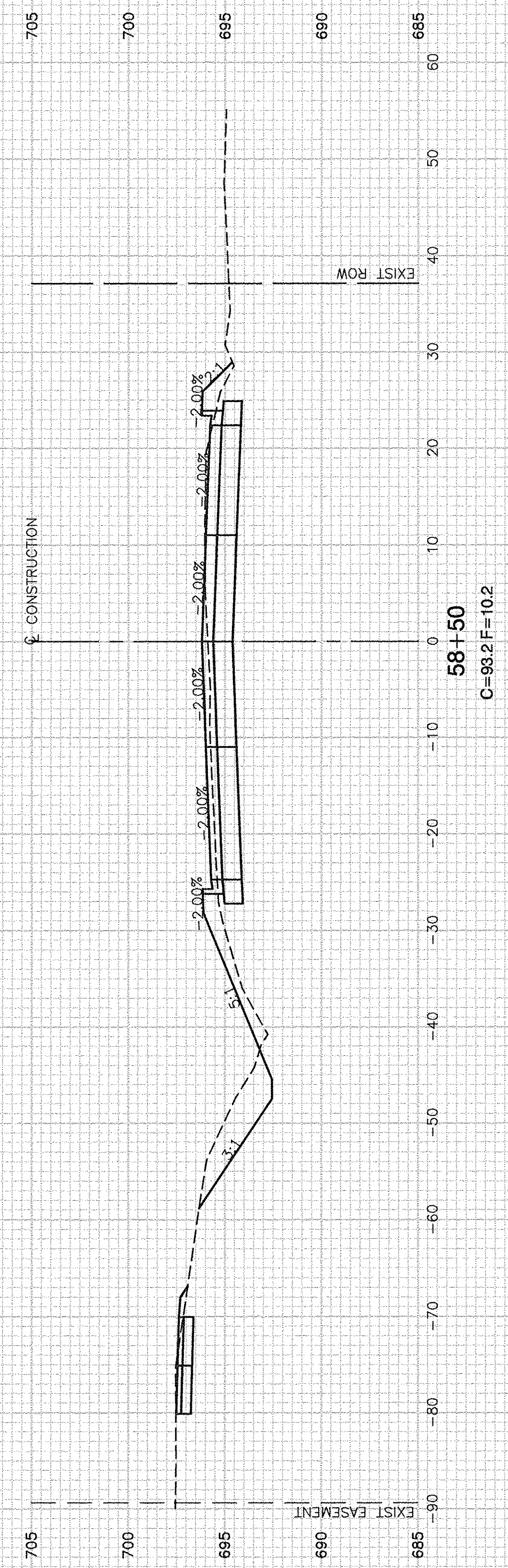
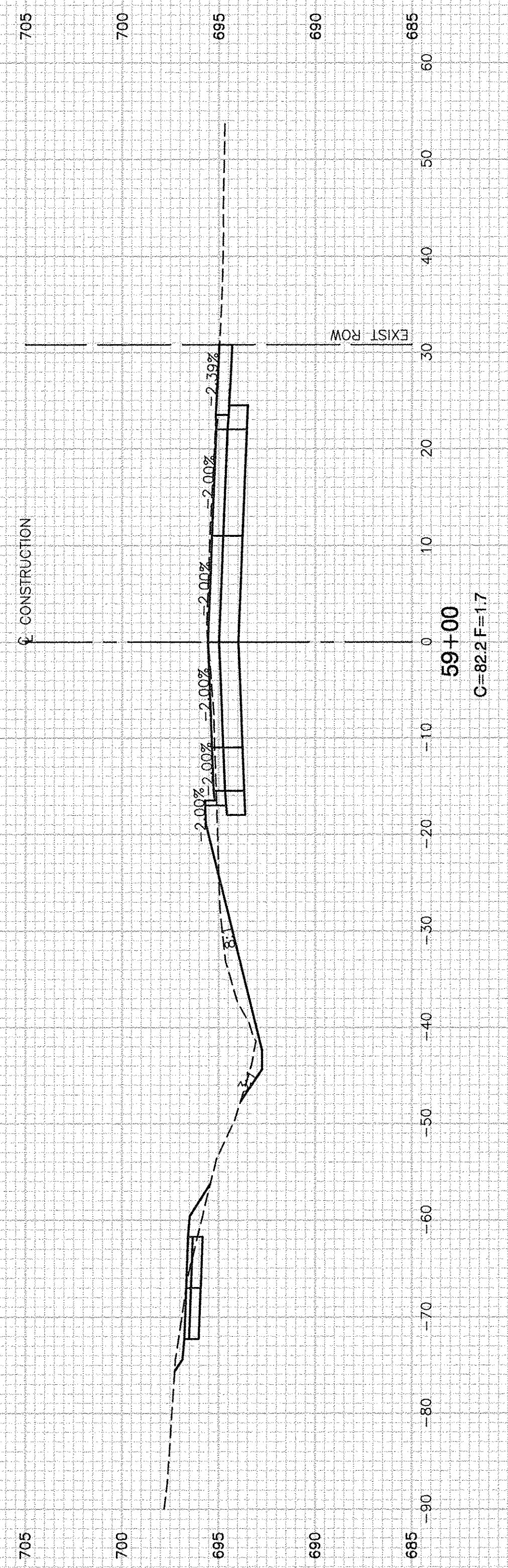
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	PLOT DATE = 1/4/2008	CHECKED — 09-18-94	REVISED —E. GOMEZ 08-28-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 52 OF 63 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	13-00063-00-CH	COOK	63	52
TC-16		CONTRACT NO. 61C11		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(216)		



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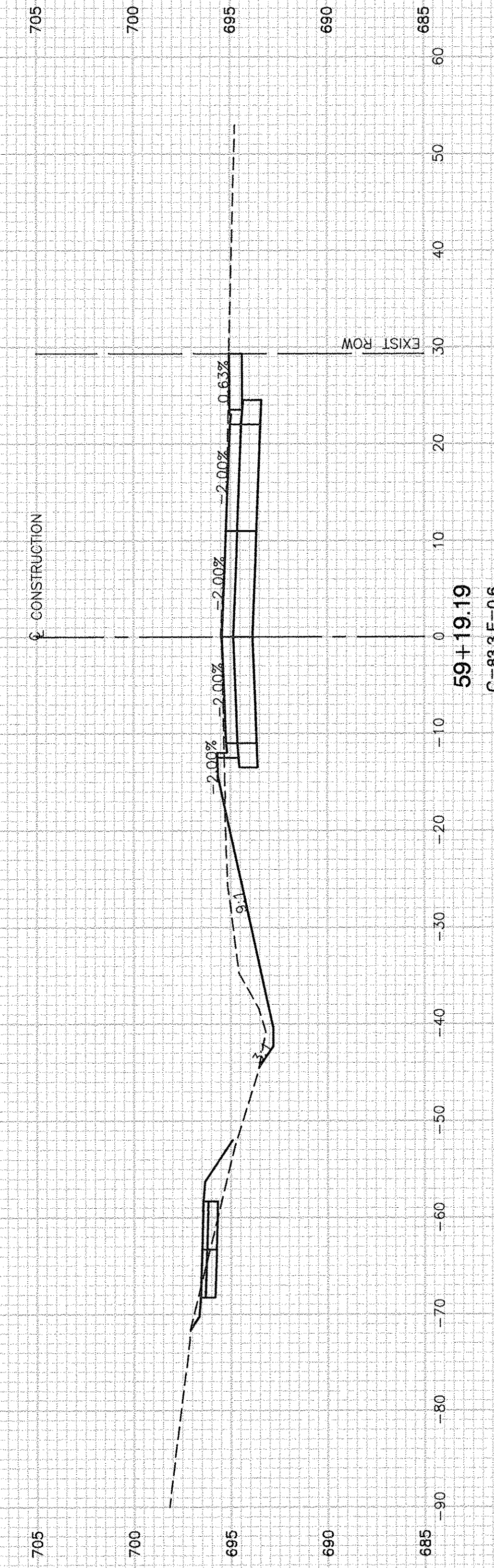
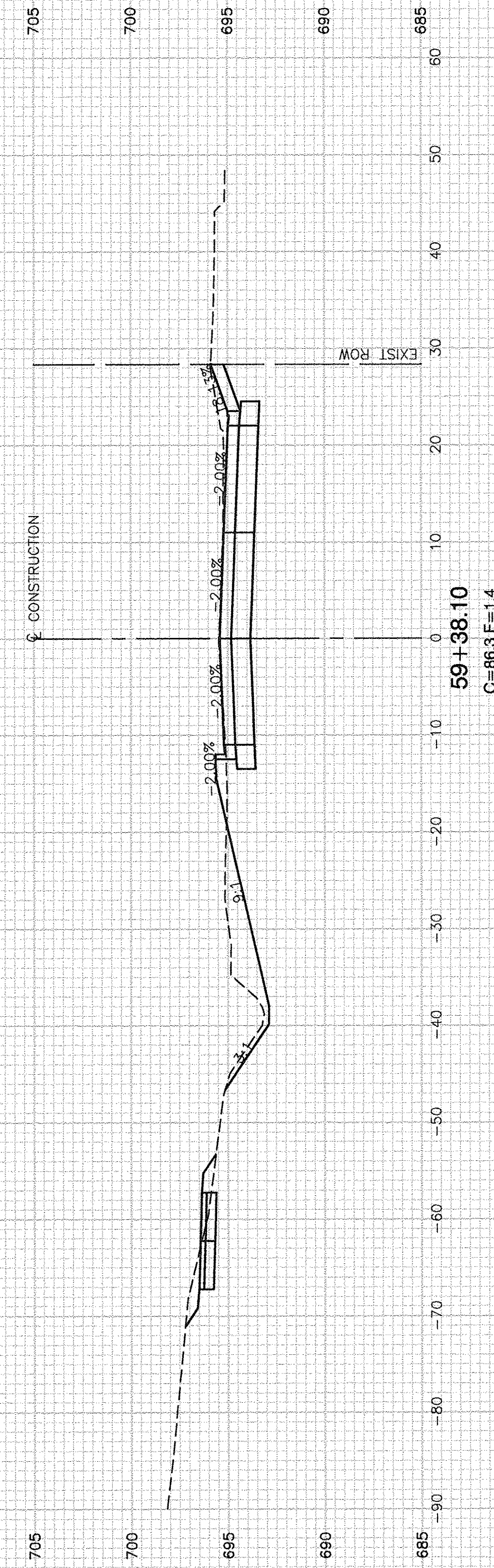
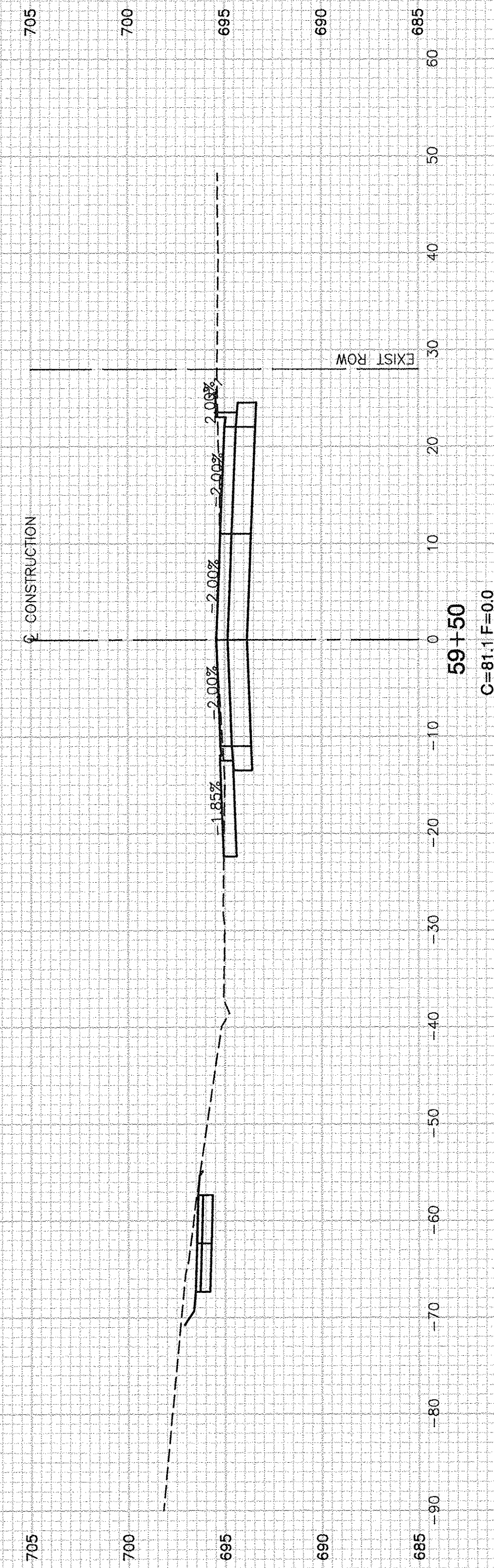
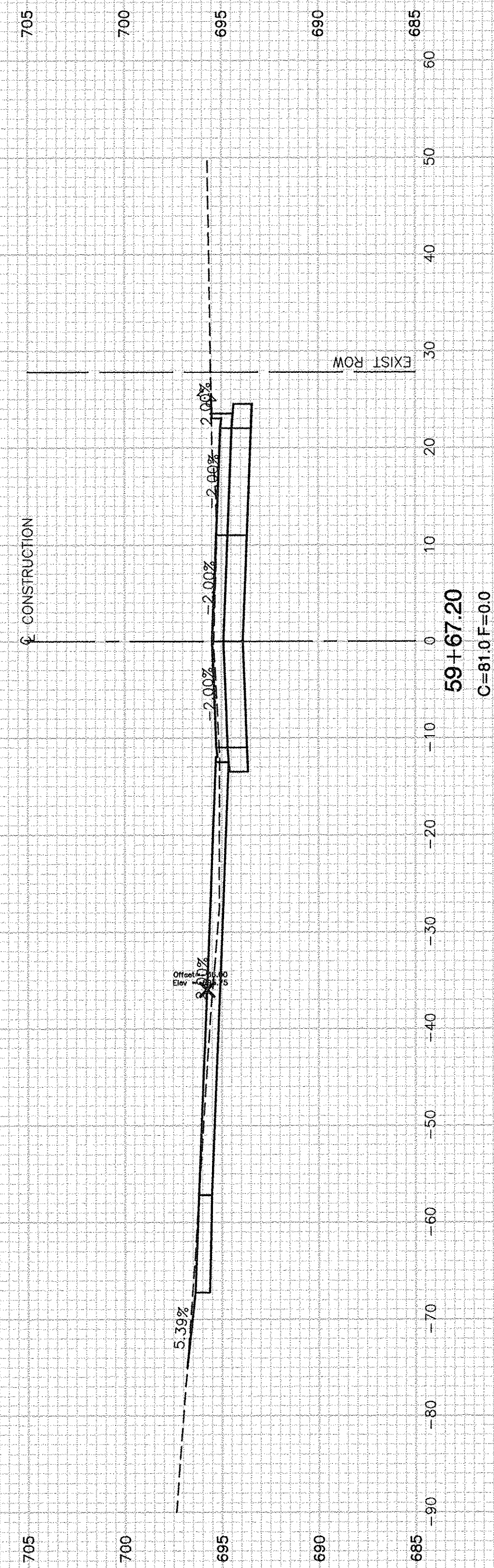
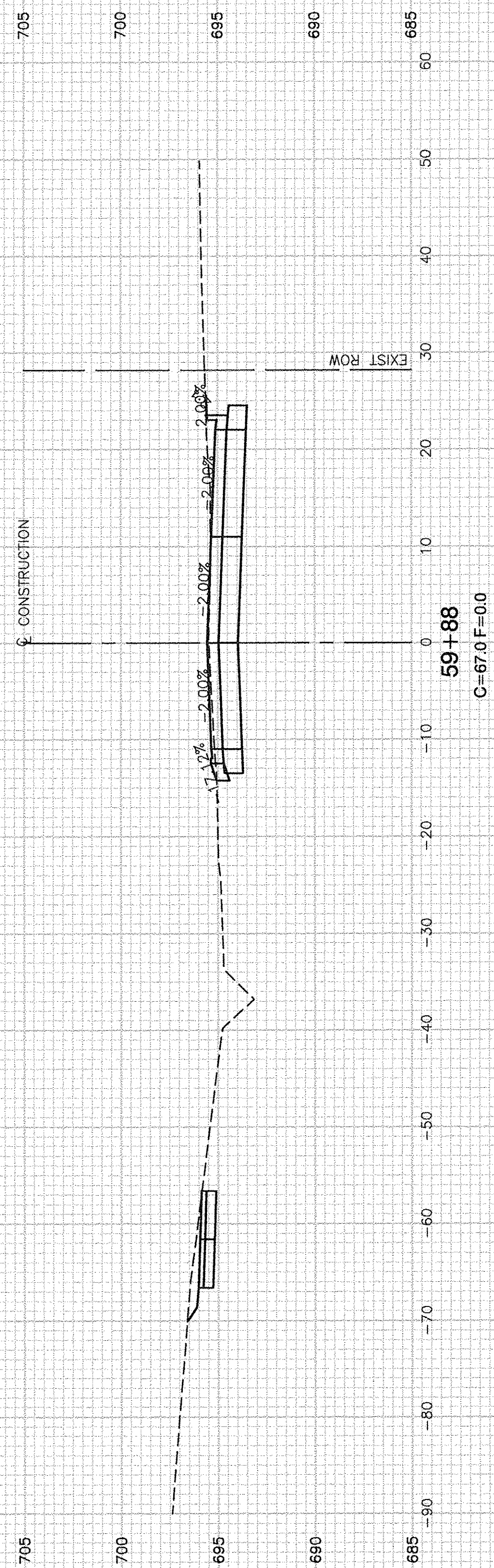
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PLOT SCALE =	CHECKED — PKB	REVISED —
PLOT DATE = 05-19-16	DRAWN — KWM	REVISED —
	CHECKED — AG	REVISED —

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE
INTERSECTION IMPROVEMENTS
CROSS SECTIONS**

SCALE: SHEET NO. 53 OF 63 SHEETS STA. TO STA.

F.A.P. RTE. 353	SECTION 13-00063-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 53
CONTRACT NO. 61C11				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(216)		



FILE NAME = 13375_02-BASE01 - XSEC02

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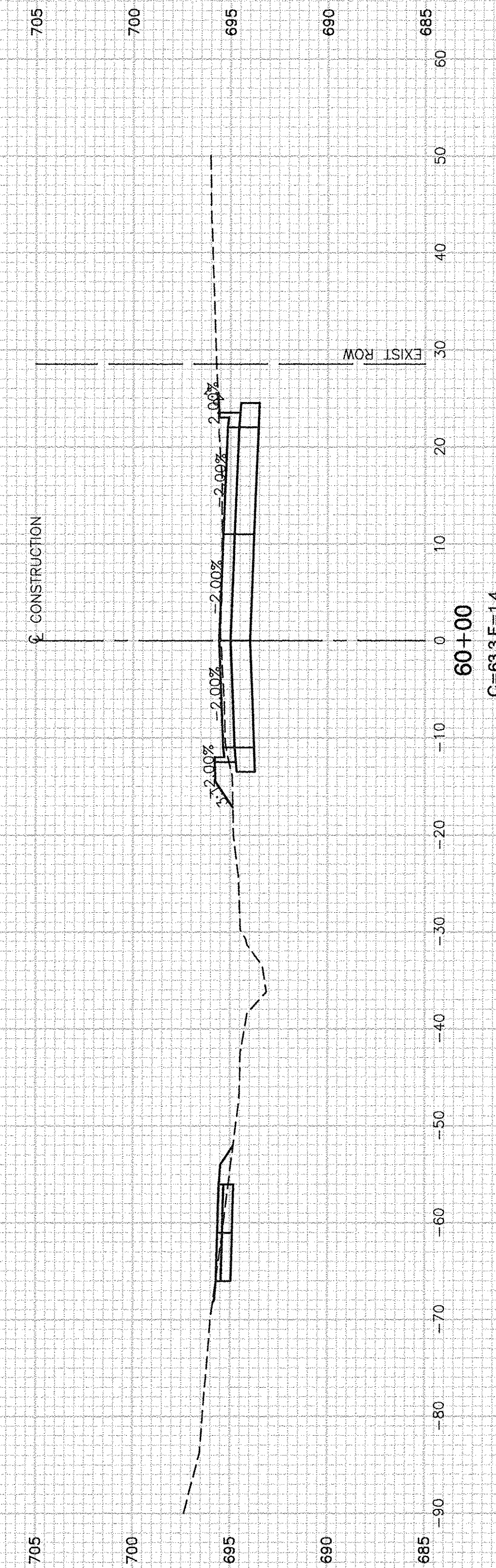
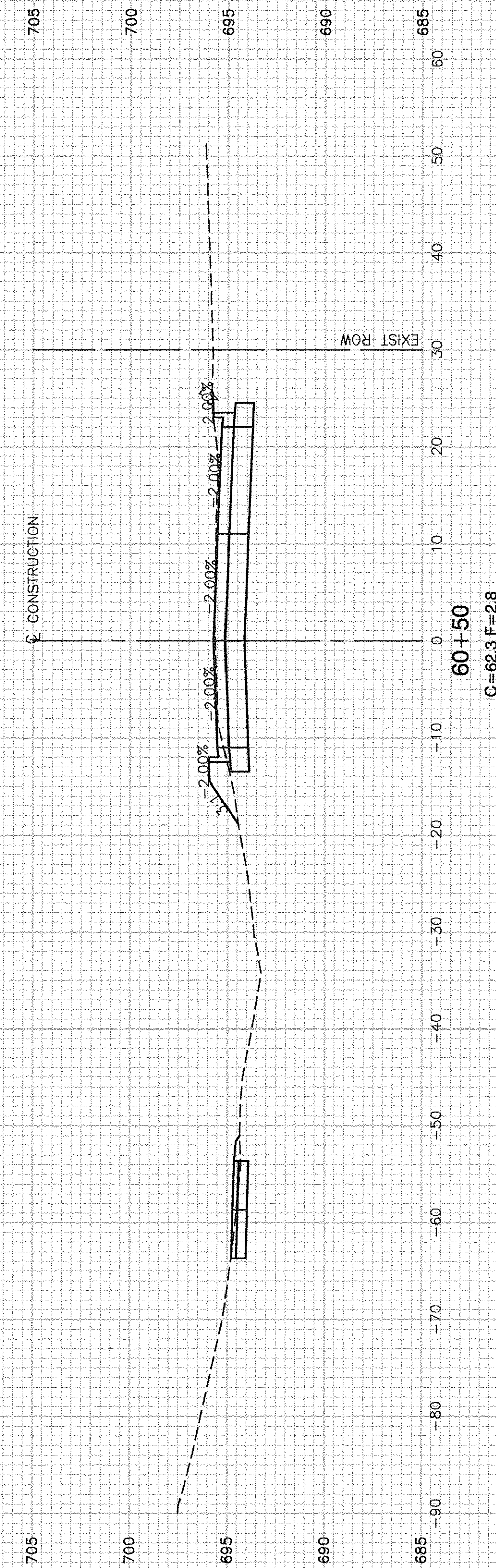
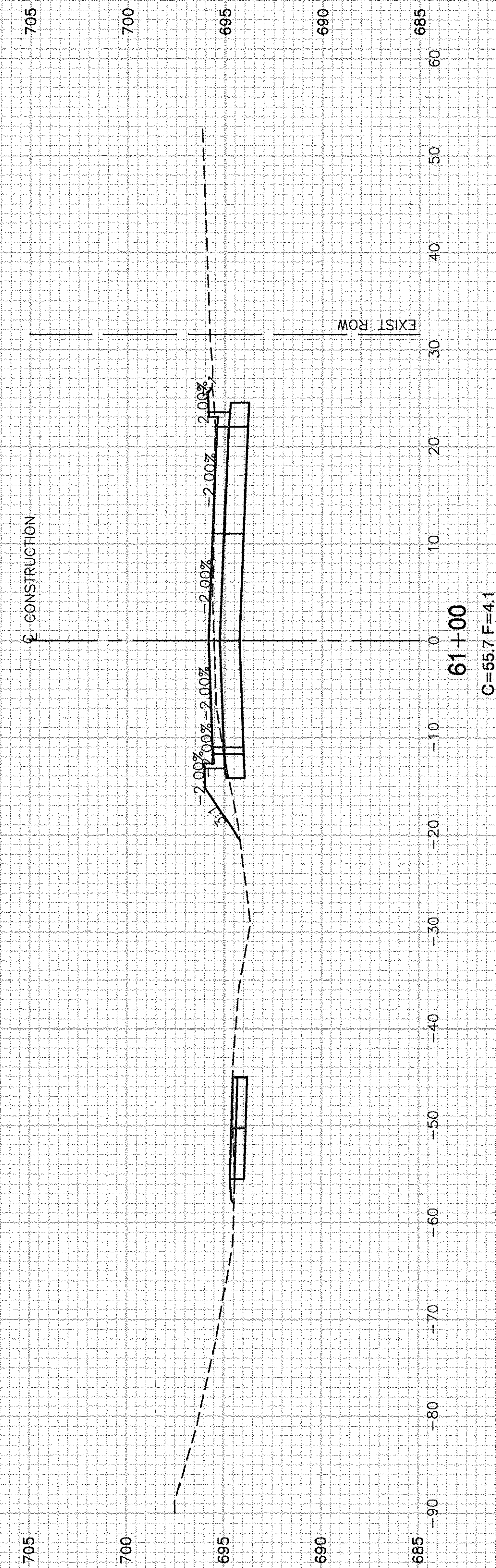
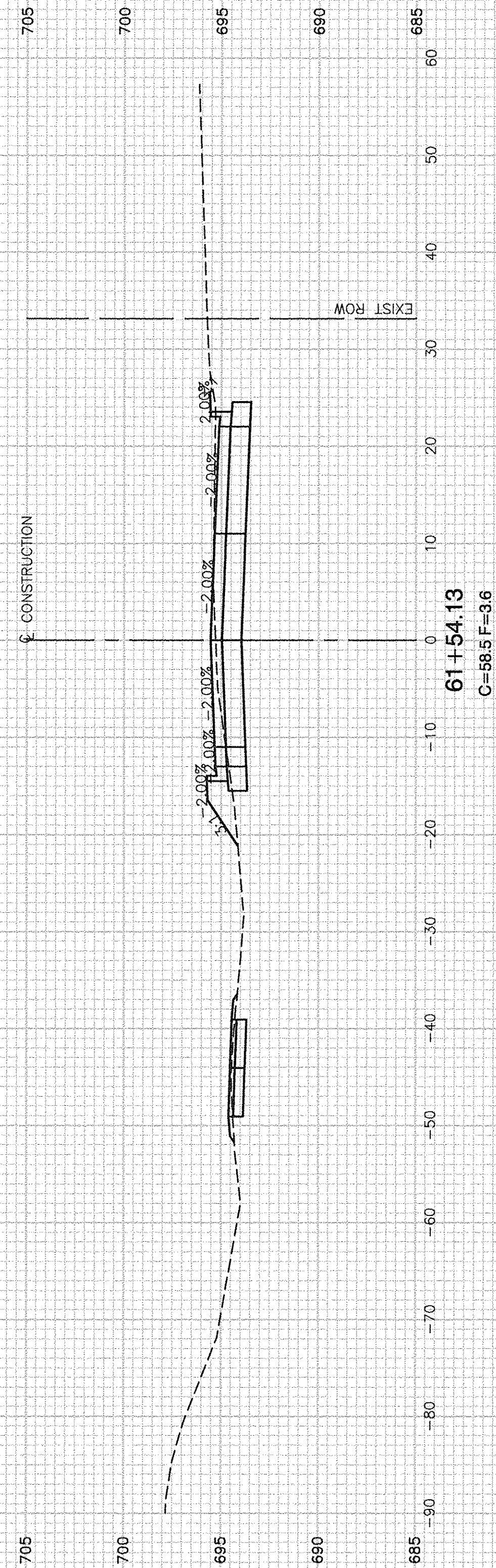
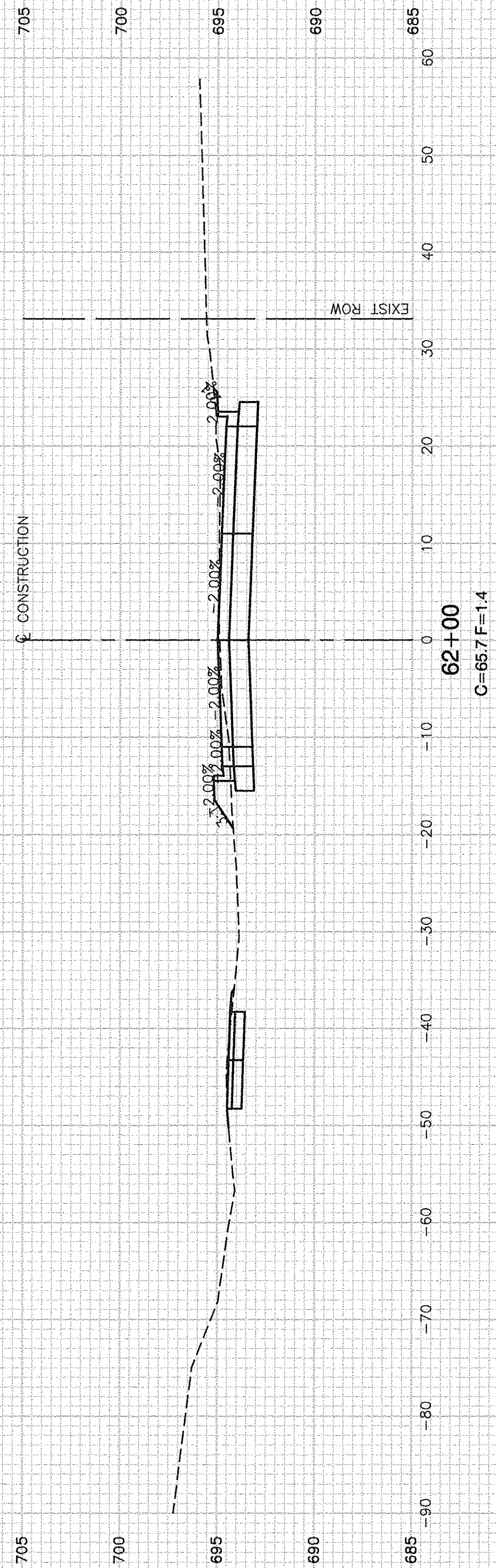
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE
 INTERSECTION IMPROVEMENTS
 CROSS SECTIONS

SCALE: SHEET NO. 54 OF 63 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	13-00063-00-CH	COOK	63	54
CONTRACT NO. 61C11				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(216)				



FILE NAME = 13375_02-BASE-01 - XSEC03

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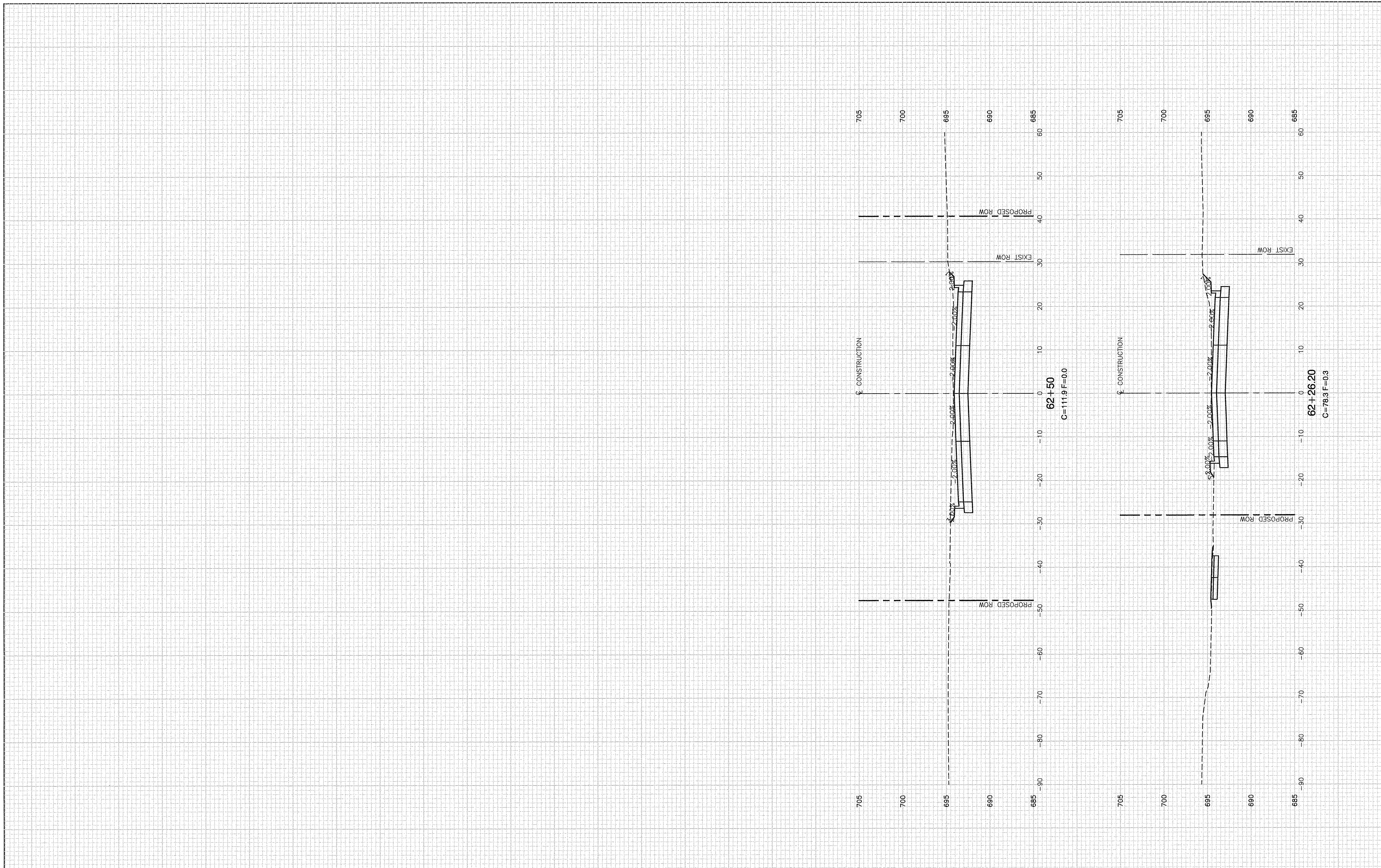
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE
 INTERSECTION IMPROVEMENTS
 CROSS SECTIONS

SCALE: SHEET NO. 55 OF 63 SHEETS STA. TO STA.

F.A.P. RTE. 353	SECTION 13-00063-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 55
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT M-4003(216)	
CONTRACT NO. 61C11				



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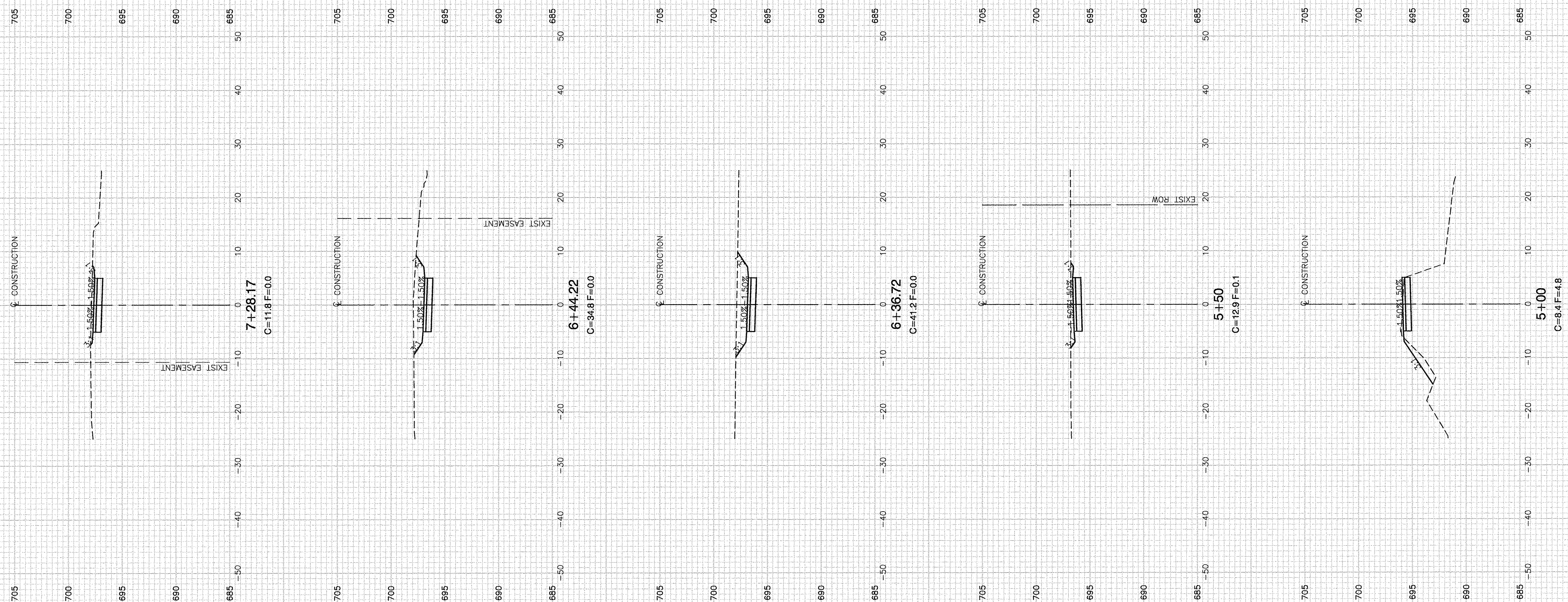
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE
 INTERSECTION IMPROVEMENTS
 CROSS SECTIONS

SCALE: SHEET NO. 56 OF 63 SHEETS STA. TO STA.

F.A.P. RTE. 353	SECTION 13-00063-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 56
CONTRACT NO. 61C11				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(216)				



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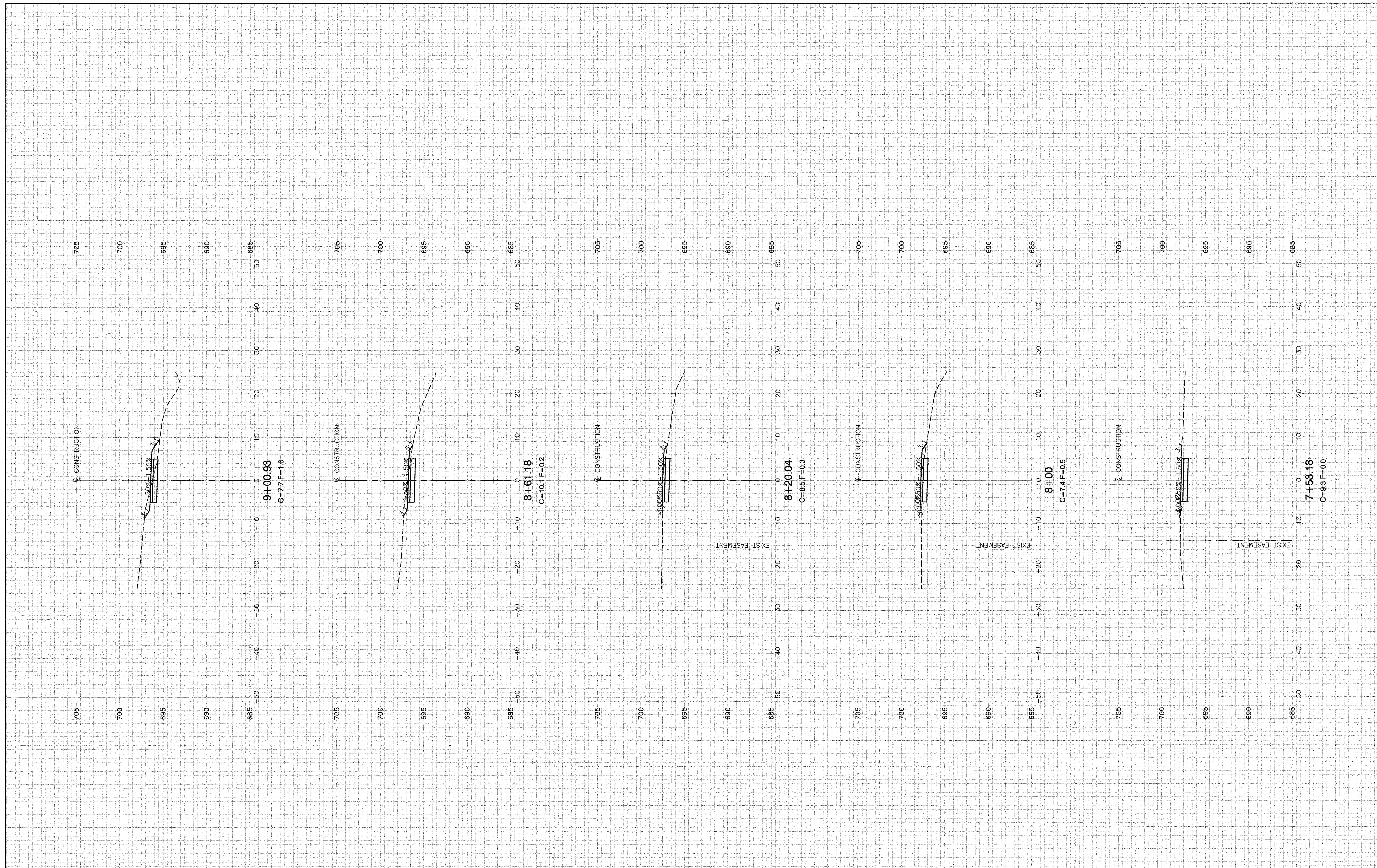
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PLOT DATE = 05-19-16	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE
INTERSECTION IMPROVEMENTS
CROSS SECTIONS

SCALE: SHEET NO. 57 OF 63 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	13-00063-00-CH	COOK	63	57
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(216)			CONTRACT NO. 61C11	



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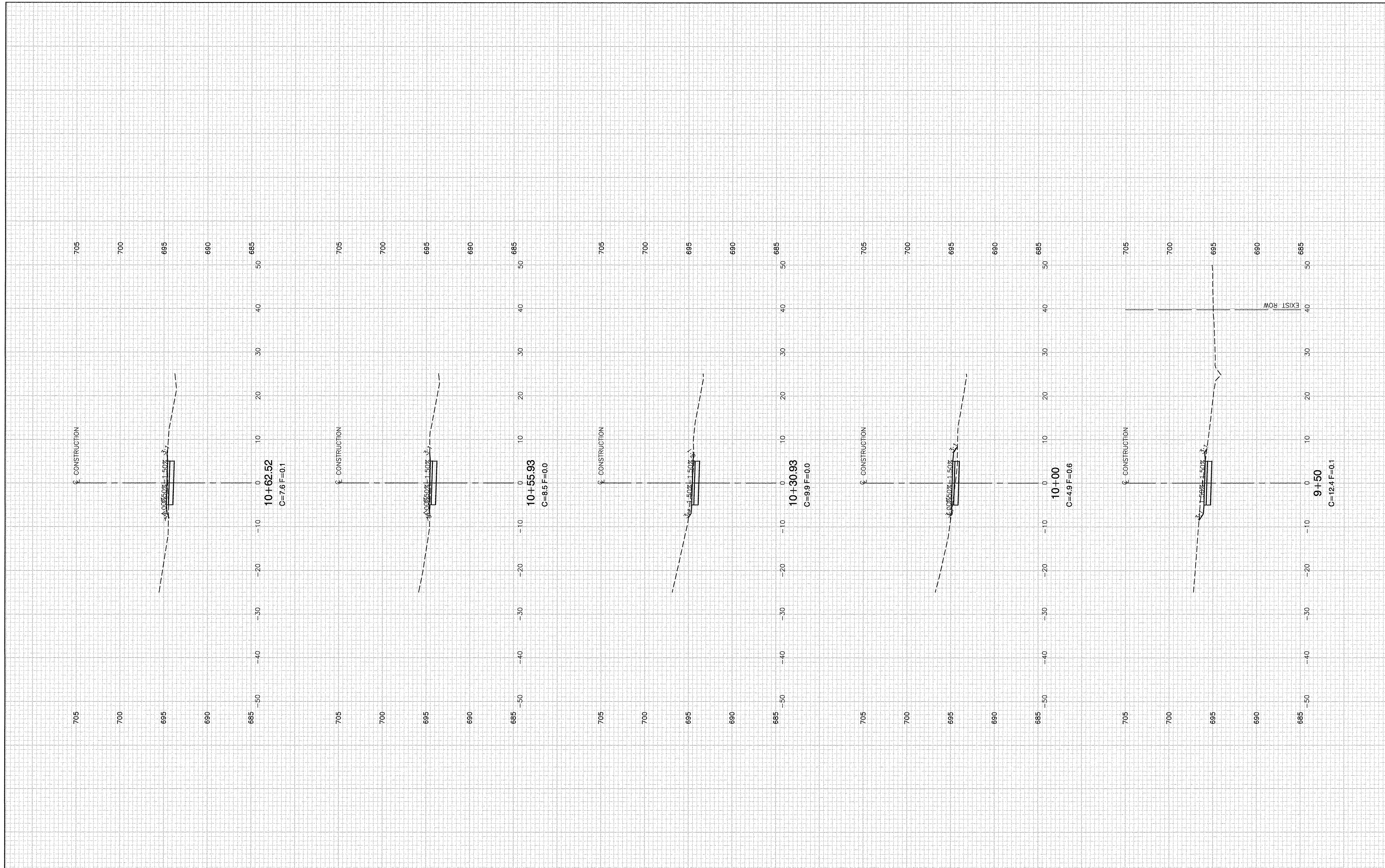
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	CHECKED — PKB	REVISED —
PLOT SCALE =	DRAWN — KWM	REVISED —
PLOT DATE = 05-19-16	CHECKED — AG	REVISED —

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE
INTERSECTION IMPROVEMENTS
CROSS SECTIONS**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	13-00063-00-CH	COOK	63	58
CONTRACT NO. 61C11				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	M-4003(216)	

SCALE: SHEET NO. 58 OF 63 SHEETS STA. TO STA.



FILE NAME = 13375_02-BASE-01 - XSEC7

USER NAME =	DESIGNED -- EMA	REVISED --
	CHECKED -- PKB	REVISED --
PLOT SCALE =	DRAWN -- KWM	REVISED --
PLOT DATE = 05-19-16	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE
INTERSECTION IMPROVEMENTS
CROSS SECTIONS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	13-00063-00-CH	COOK	63	59
CONTRACT NO. 61C11				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	M-4003(216)	

SCALE: SHEET NO. 59 OF 63 SHEETS STA. TO STA.

700

700

695

695

690

690

685

685

680

680

50

50

40

40

30

30

20

20

10

10

0

0

-10

-10

-20

-20

-30

-30

-40

-40

-50

-50

CL CONSTRUCTION

12+12.30
C=8.9 F=0.0

CL CONSTRUCTION

11+80.82
C=8.5 F=0.1

CL CONSTRUCTION

11+50
C=9.2 F=0.0

CL CONSTRUCTION

11+00
C=6.2 F=0.4

CL CONSTRUCTION

10+87.52
C=6.1 F=0.4

695

695

695

695

690

690

690

690

685

685

685

685

680

680

680

680

700

CL CONSTRUCTION

700

695

695

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695

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700

CL CONSTRUCTION

700

695

695

695

695

690

690

690

690

685

685

685

685

680

680

680

680

FILE NAME = 13375_02-BASE-01 - XSEC8

USER NAME =

DESIGNED -- EMA

REVISED --

CHECKED -- PKB

REVISIONS

REVISED --

PLOT SCALE =

DRAWN -- KWM

REVISED --

PLOT DATE = 05-19-16

CHECKED -- AG

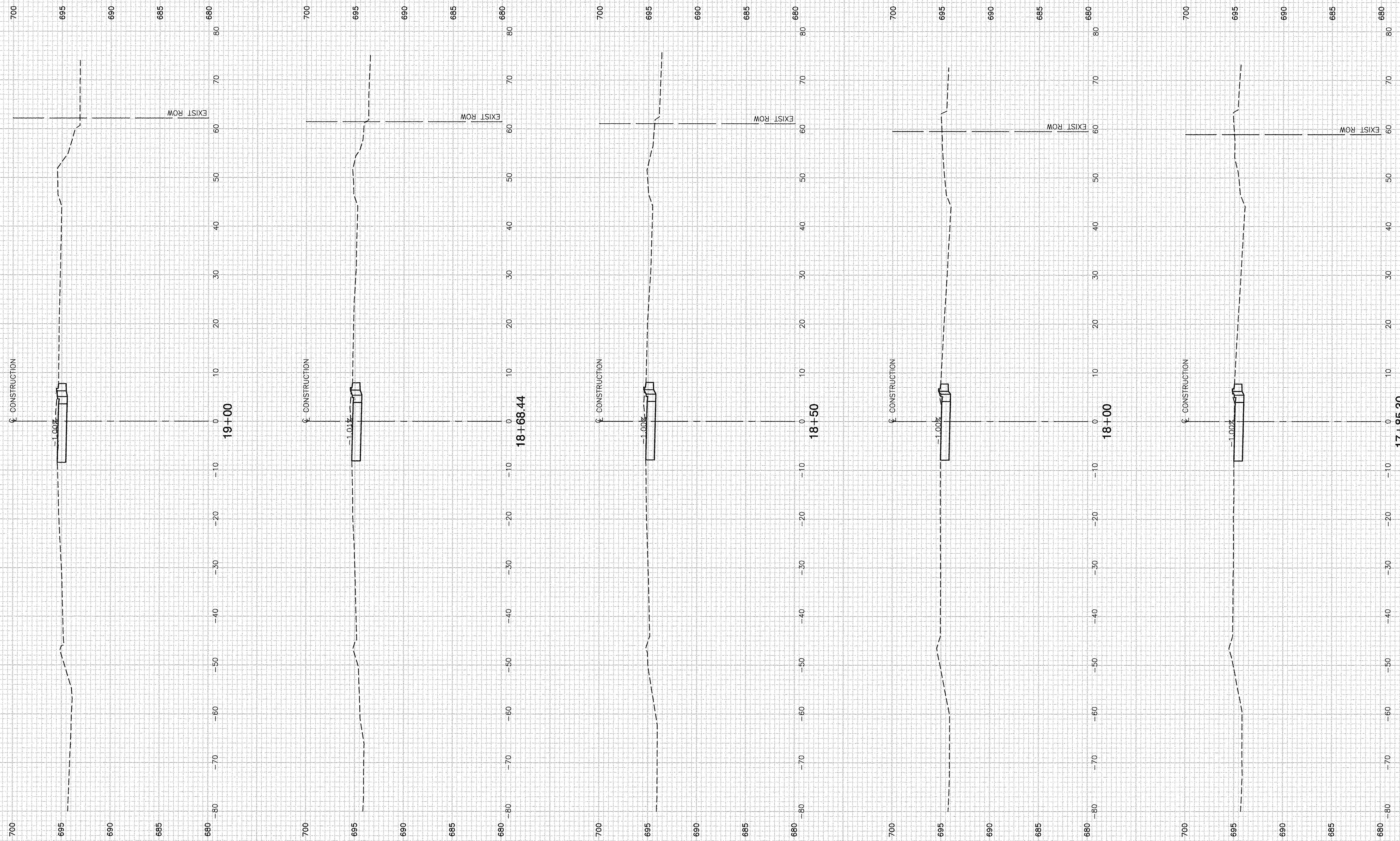
REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE
INTERSECTION IMPROVEMENTS
CROSS SECTIONS

SCALE: SHEET NO. 60 OF 63 SHEETS STA. TO STA.

F.A.P. RTE. 353	SECTION 13-00063-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 60
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 61C11	
FED. AID PROJECT M-4003(216)				



FILE NAME = 13375_02-BASE-01 - XSEC9

USER NAME =
 PLOT SCALE =
 PLOT DATE = 05-19-16

DESIGNED — EMA
 CHECKED — PKB
 DRAWN — KWM
 CHECKED — AG

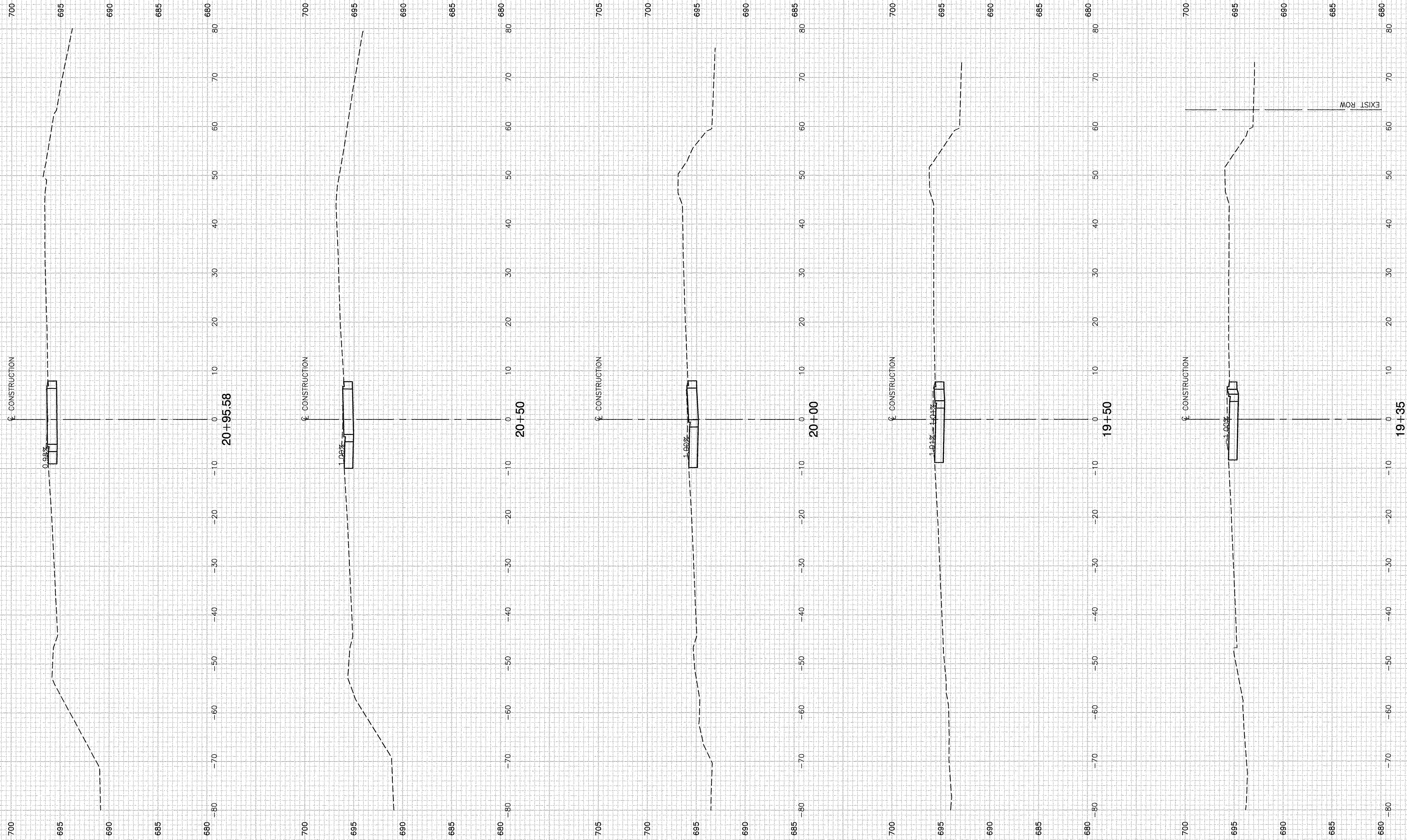
REVISED —
 REVISED —
 REVISED —
 REVISED —

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE
 INTERSECTION IMPROVEMENTS
 CROSS SECTIONS

SCALE: SHEET NO. 61 OF 63 SHEETS STA. TO STA.

F.A.P. RTE. 353	SECTION 13-00063-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 61
CONTRACT NO. 61C11				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(216)				



FILE NAME = 13375_02-BASE-01 - XSECT10

USER NAME =
 PLOT SCALE =
 PLOT DATE = 05-19-16

DESIGNED — EMA
 CHECKED — PKB
 DRAWN — KWM
 CHECKED — AG

REVISED —
 REVISED —
 REVISED —
 REVISED —

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE
 INTERSECTION IMPROVEMENTS
 CROSS SECTIONS

SCALE: SHEET NO. 62 OF 63 SHEETS STA. TO STA.

F.A.P. RTE. 353	SECTION 13-00063-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 62
FED. ROAD DIST. NO. 1 ILLINOIS			FED. AID PROJECT M-4003(216)	

EXIST ROW

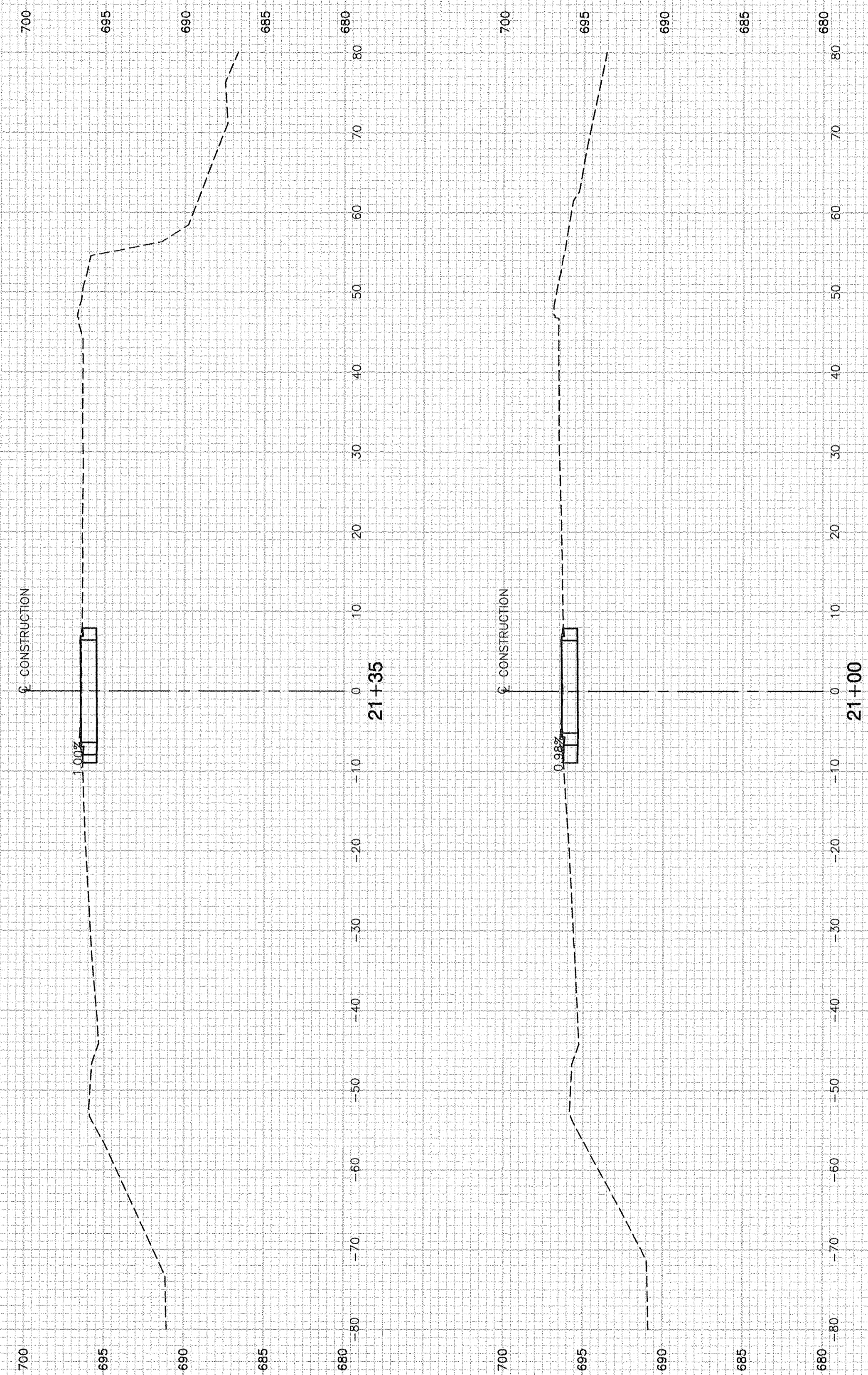
20+95.58

20+50

20+00

19+50

19+35



FILE NAME = 13375_02-BASE-01 - XSEC11

USER NAME =	DESIGNED -- EMA	REVISED --
	CHECKED -- PKB	REVISED --
PLOT SCALE =	DRAWN -- KWM	REVISED --
PLOT DATE = 05-19-16	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US 30 (LINCOLN HIGHWAY) AT KOSTNER AVENUE
INTERSECTION IMPROVEMENTS
CROSS SECTIONS

SCALE: SHEET NO. 63 OF 63 SHEETS STA. TO STA.

F.A.P. RTE. 353	SECTION 13-00063-00-CH	COUNTY COOK	TOTAL SHEETS 63	SHEET NO. 63
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(216)			CONTRACT NO. 61C11	