

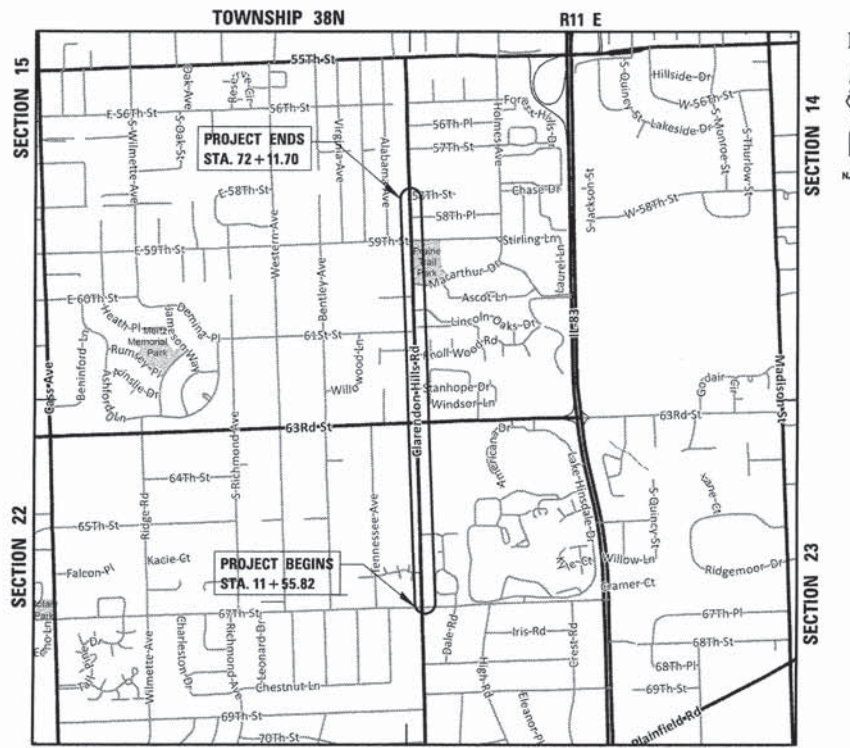
FOR INDEX OF SHEETS, SEE SHEET NO. 2

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED
FEDERAL AID HIGHWAY

FAU 2663 (CLARENDON HILLS ROAD)
67th STREET TO 58th STREET
RESURFACING
SECTION NO. 15-00022-00-RS
PROJECT NO. M-4003(597)
VILLAGE OF WILLOWBROOK
DuPAGE COUNTY
JOB NO. C-91-088-16

F.A.U. RTE.#	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2663	15-00022-00-RS	DuPAGE	26	1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO.	61C27	



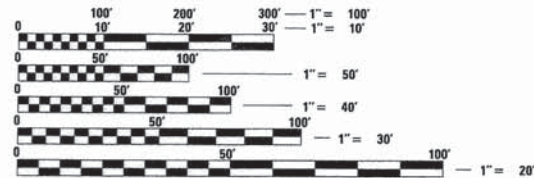
LOCATION MAP

CLARENDON HILLS ROAD
GROSS LENGTH OF PROJECT = 6,055.88 LINEAL FEET (1.15 MILES)
NET LENGTH OF PROJECT = 6,055.88 LINEAL FEET (1.15 MILES)

TRAFFIC DATA

CLARENDON HILLS ROAD
ADT (YEAR) = 5,150 (2012)
POSTED SPEED LIMIT = 30 MPH

DESIGN DESIGNATION: MAJOR COLLECTOR



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
8-1-1 OR 1-800-892-0123

CB CHRISTOPHER B. BURKE ENGINEERING LTD.
9575 West Higgins Road, Suite 600
Rosemont, Illinois 60018 (847) 823-0500

PROFESSIONAL DESIGN FIRM NO. 184-001175
EXPIRATION DATE: 04/30/17

CONTRACT NO. 61C27

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED 11/30/2015
Frank A. Tello
VILLAGE PRESIDENT, VILLAGE OF WILLOWBROOK

PASSED 12-24-2015
John H. Kelly
DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW DECEMBER 30, 2015
John E. Moran
DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

LEE M. FELL
062-053708 REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS

L.M. Fell
ENGINEER

11/30/15
DATE

LEE M. FELL
ILLINOIS REGISTRATION No. 062-053708
EXPIRATION DATE: 11/30/2017

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

FEDERAL AID PROGRAM ENGINEER: FAWAD AQUEEL, P.E. (847) 705-4021
SCHAUMBURG, ILLINOIS

GENERAL NOTES

SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2012; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE LATEST REVISION; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", (IMUTCD); THE "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS", (SSTCI), "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" JUNE 2014 SEVENTH EDITION, THE "DETAILS" IN THE PLANS AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.

ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST IDOT HIGHWAY STANDARD.

CODES OF THE IEPA TITLE 35, AND O. S. H. A. SHALL BE ADHERED TO FOR THE CONSTRUCTION OF THIS PROJECT.

ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 700 OF THE STANDARD SPECIFICATIONS.

ALL REQUIRED PERMITS FROM THE PROPER GOVERNING AGENCY SHALL BE OBTAINED FOR CONSTRUCTION ALONG OR ACROSS EXISTING STREETS OR HIGHWAYS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR THE PROPER BRACING, SHEETING, SHORING AND OTHER REQUIRED PROTECTION OF ALL ROADWAYS BEFORE CONSTRUCTION BEGINS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO THE STREETS OR ROADWAYS AND ASSOCIATED STRUCTURES AND SHALL MAKE REPAIRS AS NECESSARY TO THE SATISFACTION OF THE ENGINEER, AT THE CONTRACTOR'S OWN EXPENSE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND MAINTENANCE OF ADEQUATE SIGNS AND WARNING DEVICES TO INFORM AND PROTECT THE PUBLIC.

UTILITIES

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITY FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS.

THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES AND OTHER UTILITY LINES ARE APPROXIMATE, AND THE DEPARTMENT AND VILLAGE DO NOT GUARANTEE THEIR ACCURACY. THEIR EXACT HORIZONTAL AND VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR AT HIS OWN EXPENSE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR THE VILLAGE. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 8-1-1 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS AND CABLE.

THE CONTRACTOR SHALL CONTACT IDOT'S BUREAU OF MATERIALS (PHONE 847-705-4337) AT LEAST 24 HOURS BEFORE PLACING HOT-MIX ASPHALT OR PORTLAND CEMENT CONCRETE.

STAKING

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE ENGINEER, VILLAGE, AND ITS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCE THEIR LOCATIONS.

WATER, STORM SEWER AND SANITARY SEWER

WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF INLET FILTERS.

ALL EXISTING OR PROPOSED STORM SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR AND INCLUDED IN THE COST OF HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4".

MISCELLANEOUS

ACCESS: THE CONTRACTOR SHALL PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT, EXCEPT FOR PERIODS OF SHORT DURATION. THE COST TO PROVIDE ACCESS SHALL BE PAID FOR AND INCLUDED IN THE ITEM TEMPORARY ACCESS (ROAD) OR TEMPORARY ACCESS (PRIVATE ENTRANCE).

DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

ALL SAWCUTTING SHALL BE INCLUDED IN THE COST OF THE REMOVAL ITEMS AND SHALL BE PERFORMED PRIOR TO BEGINNING REMOVAL. ANY ITEMS OF WORK REMOVED PRIOR TO SAWCUTTING WILL NOT BE MEASURED FOR PAYMENT.

CLASS D PATCHES, 6" WILL BE DETERMINED BY THE ENGINEER IN THE FIELD AND WILL NOT EXCEED THE PLANNED QUANTITY.

THE THICKNESSES OF HOT-MIX ASPHALT MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASIS ON WHICH THEY ARE TO BE PLACED. PLAN THICKNESSES SHOULD BE CONSIDERED THE MINIMUM THICKNESS PERMITTED.

FRESH OIL SIGNS SHALL BE POSTED AT BOTH ENDS OF THE ROADWAY AND ALL SIDE STREETS AS DIRECTED BY THE ENGINEER. CONSTRUCTION AHEAD SIGNS SHALL BE PLACED AT ALL SIDE STREETS AND BOTH ENDS OF THE ROADWAY WHILE CONSTRUCTION IS IN PROGRESS. THIS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE APPLICABLE TRAFFIC CONTROL PAY ITEMS OR STANDARD 701501.

CONTRACTOR SHALL NOT PLACE SOD UNTIL THE TEMPERATURE IS 80° OR LESS AND THE FORECAST FOR THE NEXT 7 DAYS SHOWS TEMPERATURES OF 80° OR LESS. IF ALL OTHER PAY ITEMS ARE COMPLETED, THE CONTRACTOR WILL NOT BE CHARGED WORKING DAYS FOR DELAYS IN PARKWAY RESTORATION DUE TO TEMPERATURE.

NO CONSTRUCTION SHALL BEGIN UNTIL ALL PROPER TEMPORARY SIGNS AND BARRICADES HAVE BEEN INSTALLED.

AT NO TIME SHALL LESS THAN HALF OF THE STREET BE AVAILABLE FOR PARKING.

ALL ROADS MUST HAVE ONLY ONE LONGITUDINAL JOINT WHILE PAVING.

VANDALISM - SPECIAL ATTENTION IS CALLED TO THE SPECIAL PROVISION FOR VANDALISM INSPECTION AS WELL AS ARTICLE 107.30 OF THE "STANDARD SPECIFICATIONS." ANY DEFACED WORK AS DETERMINED AND DIRECTED BY THE VILLAGE SHALL BE CORRECTED OR REPLACED TO THE SATISFACTION OF THE ENGINEER BY THE CONTRACTOR AT HIS SOLE EXPENSE PRIOR TO FINAL PAYMENT. THE VILLAGE OF WILLOWBROOK WILL COOPERATE WITH THE CONTRACTOR TO MINIMIZE VANDALISM, BUT THE CONTRACTOR SHALL BE ULTIMATELY RESPONSIBLE TO CORRECT ANY DAMAGE. THE VILLAGE WILL NOT BE RESPONSIBLE FOR THE SECURITY OF THE WORK SITE IN THIS REGARD, OTHER THAN NORMAL PATROLLING AND RESPONSE TO EMERGENCIES. THE COST OF ADDITIONAL SECURITY REQUIRED TO MEET THIS SPECIAL PROVISION SHALL BE SOLELY THE CONTRACTOR'S RESPONSIBILITY.

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HIGHWAY STANDARDS

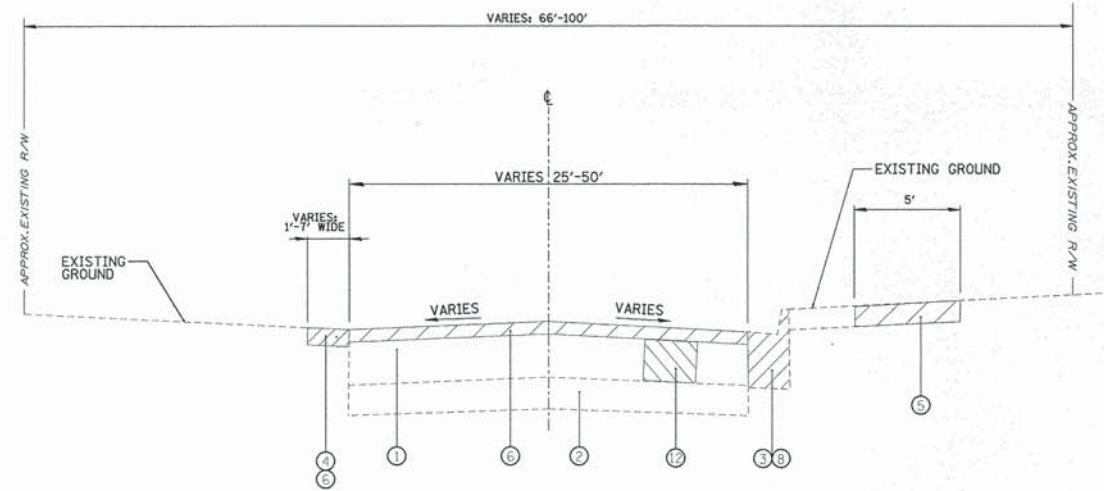
000001-06	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
424001-08	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
482001-02	HMA SHOULDER ADJACENT TO FLEXIBLE PAVEMENT
701006-05	OFF ROAD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701701-09	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-04	TRAFFIC CONTROL DEVICES

SUMMARY OF QUANTITIES

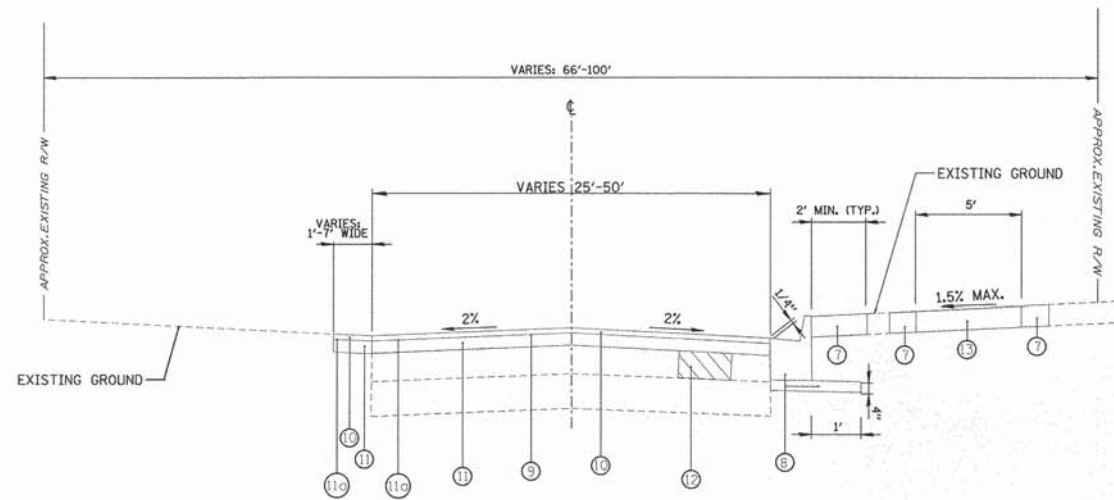
	ITEM NO.	ITEM	UNIT	CONSTRUCTION CODE 0005 Quantity
	25200110	SODDING, SALT TOLERANT	SQ YD	100
	25200200	SUPPLEMENTAL WATERING	UNIT	5
~	28000510	INLET FILTERS	EACH	50
~	40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	21,000
	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	50
	40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	1,435
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	1,000
	40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	3,010
	42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQ YD	285
~	42400800	DETECTABLE WARNINGS	SQ FT	230
	44000160	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"	SQ YD	25,800
	44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	360
	44000600	SIDEWALK REMOVAL	SQ FT	1,500
	44201713	CLASS D PATCHES, TYPE I, 6 INCH	SQ YD	300
	44201717	CLASS D PATCHES, TYPE II, 6 INCH	SQ YD	300
	44201721	CLASS D PATCHES, TYPE III, 6 INCH	SQ YD	300
	44201723	CLASS D PATCHES, TYPE IV, 6 INCH	SQ YD	300
	44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	5,900
	67100100	MOBILIZATION	L SUM	1
	70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1
	70106800	CHANGEABLE MESSAGE SIGN	CAL MO	1
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	200
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	16,750
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	885
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	275
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	315
~ *	85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	4
~ *	88600600	DETECTOR LOOP REPLACEMENT	FOOT	180
~	X0326862	STRUCTURES TO BE ADJUSTED	EACH	9
~	X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	34
~	X4023000	TEMPORARY ACCESS (ROAD)	EACH	19
~	X4240430	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL	SQ FT	1,450
~	Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	250
~	Z0013798	CONSTRUCTION LAYOUT	L SUM	1
~	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	100

*INDICATES SPECIALTY ITEM
~INDICATES SPECIAL PROVISION

FILE NAME = N:\WILLWBROOK\150469\Civil\Que_150469.dwg	USER NAME = jlapaglia	DESIGNED - JAL	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CLARENDON HILLS ROAD SUMMARY OF QUANTITIES	F.A.U RTE. 2663	SECTION 15-00022-00-RS	COUNTY DU PAGE	TOTAL SHEETS 26	SHEET NO. 3	
PLOT SCALE = NOT TO SCALE						SCALE: N.T.S.		SHEET NO. 3 OF 26 SHEETS		STA. TO STA.	
PLOT DATE = 12/15/2015								CONTRACT NO. 61C27		ILLINOIS FED. AID PROJECT	
DRAWN - EDT											
CHECKED - LMF											
DATE - #DATE#											
REVISED -											
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EXISTING TYPICAL SECTION
STA. 11+55.82 TO STA 72+11.70, CLARENDON HILLS ROAD



PROPOSED TYPICAL SECTION
STA. 11+55.82 TO STA 72+11.70, CLARENDON HILLS ROAD

NOTES:

1. THE HIGH SIDE OF THE ROADWAY SHALL BE PAVED FIRST.
2. ANY EXCAVATION OF STONE NECESSARY TO OBTAIN THE NECESSARY DEPTH FOR THE PROPOSED PAVEMENT SHALL BE INCLUDED IN THE COST OF THE PAVEMENT RESURFACING.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS ITEM	VOIDS
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm), 2"	4% @ 50 GYR.
LEVELING BINDER (MACHINE METHOD), IL 9.5 mm, N50, 1" (SHALL BE PLACED IN 1 LIFT) (WITH STRIP REFLECTIVE CRACK CONTROL)	4% @ 50 GYR.
CLASS D PATCHES, 6" (HMA BINDER IL-19 mm)	4% @ 70 GYR.

MIXTURE REQUIREMENT NOTES:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE IS 112 LBS/SY/IN. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
2. FOR "PERCENT OF RAP AND RAS" SEE DISTRICT ONE SPECIAL PROVISIONS.

CORE DETAILS

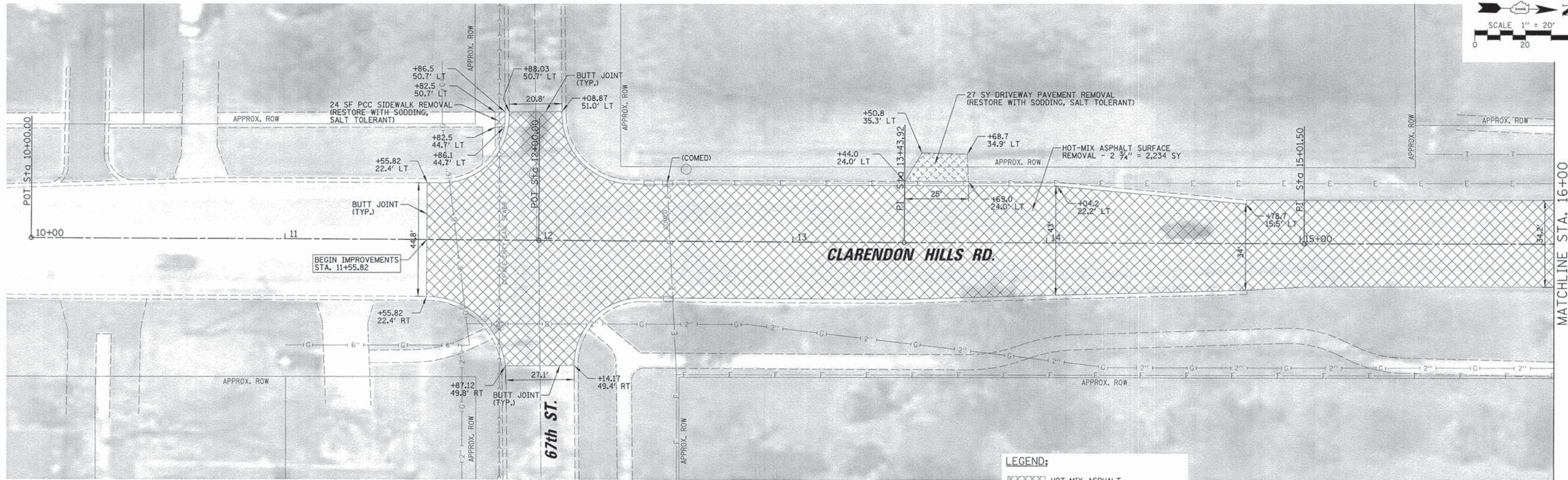
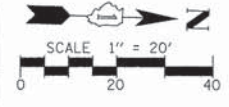
CORE NUMBER	CORE LOCATION	ASPHALT THICKNESS	BASE THICKNESS
1	50' SOUTH OF 58th PLACE	8"	9"
2	5914 CLARENDON HILLS ROAD	9.25"	5.75"
3	1/2 WAY BETWEEN 60th COURT AND McARTHUR DRIVE	6"	9"
4	6048 CLARENDON HILLS ROAD	2"	7"
5	6144 CLARENDON HILLS ROAD	9"	7"
6	6242 CLARENDON HILLS ROAD	13"	25"
7	6317-6337 CLARENDON HILLS ROAD	8.5"	6.5"
8	6350 CLARENDON HILLS ROAD	9"	6"
9	6401-6425 CLARENDON HILLS ROAD	6.5"	10.5"
10	6502 CLARENDON HILLS ROAD	14.75"	5.25"
11	50' NORTH OF EAGLES NEST DRIVE	9"	8"
12	100' NORTH OF 67th STREET	13.5"	5.5"

LEGEND

- ① EXISTING HOT-MIX ASPHALT PAVEMENT
- ② EXISTING AGGREGATE BASE
- ③ EXISTING CURB AND GUTTER
- ④ EXISTING ASPHALT SHOULDER
- ⑤ EXISTING PCC SIDEWALK
- ⑥ HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"
- ⑦ SODDING, SALT TOLERANT
- ⑧ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER). INCLUDES 4" SUBBASE GRANULAR MATERIAL TYPE B.
- ⑨ PROPOSED BITUMINOUS MATERIAL (PRIME COAT)
- ⑩ *HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 2"
- ⑪ *LEVELING BINDER (MACHINE METHOD), IL 9.5 mm, N50 - 1" WITH STRIP REFLECTIVE CRACK CONTROL
- ⑫ CLASS D PATCHES, 6" (AS DIRECTED BY THE ENGINEER)
- ⑬ PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL AS DIRECTED BY THE ENGINEER (SIDEWALKS THROUGH DRIVEWAYS SHALL BE 6 INCHES THICK - THIS WORK WILL BE INCLUDED IN THE PAY ITEM FOR PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH, SPECIAL)

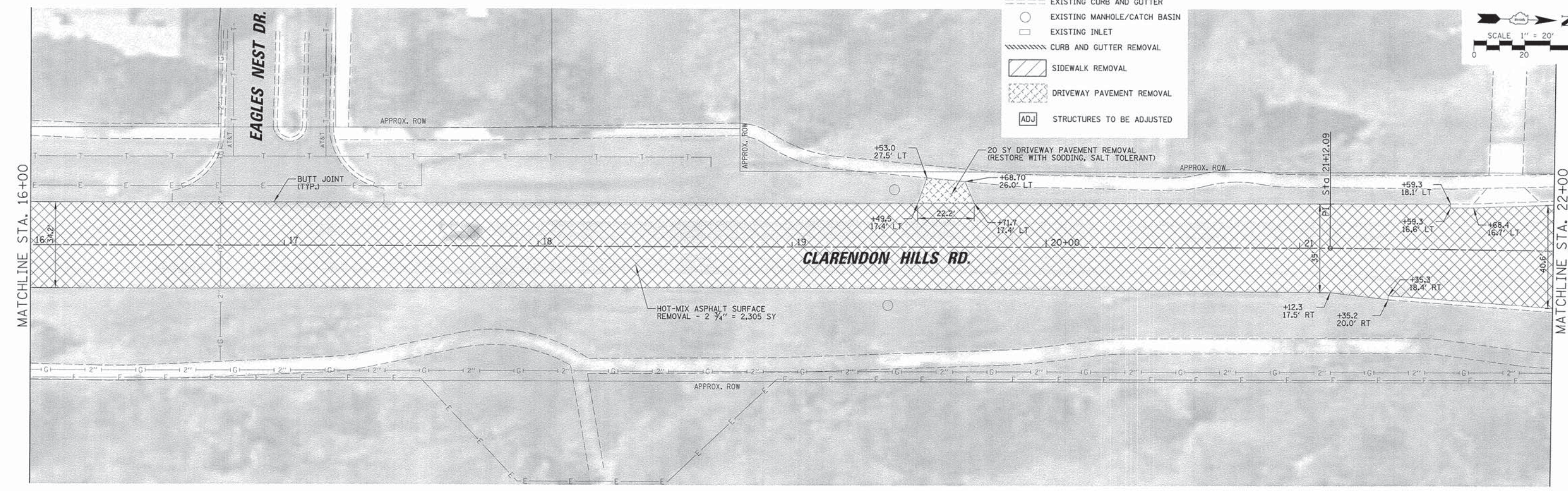
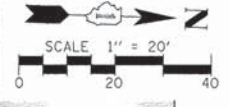
NOTE:

1. PAVING OF THE FULL ROADWAY WIDTH SHALL BE COMPLETED AT THE END OF EACH DAY OF PAVING TO PREVENT A LONGITUDINAL COLD JOINT FROM APPEARING WHEN OPPOSITE SIDES OF THE ROAD ARE PAVED ON DIFFERENT DAYS. THE CONTRACTOR SHALL ALSO ENSURE THAT AT THE END OF EACH DAY EACH PASS ENDS AT APPROXIMATELY THE SAME STATION TO PREVENT A COLD JOINT.
2. HMA SHOULDERS SHALL BE CONSTRUCTED PER IDOT HIGHWAY STD. 482001-02.

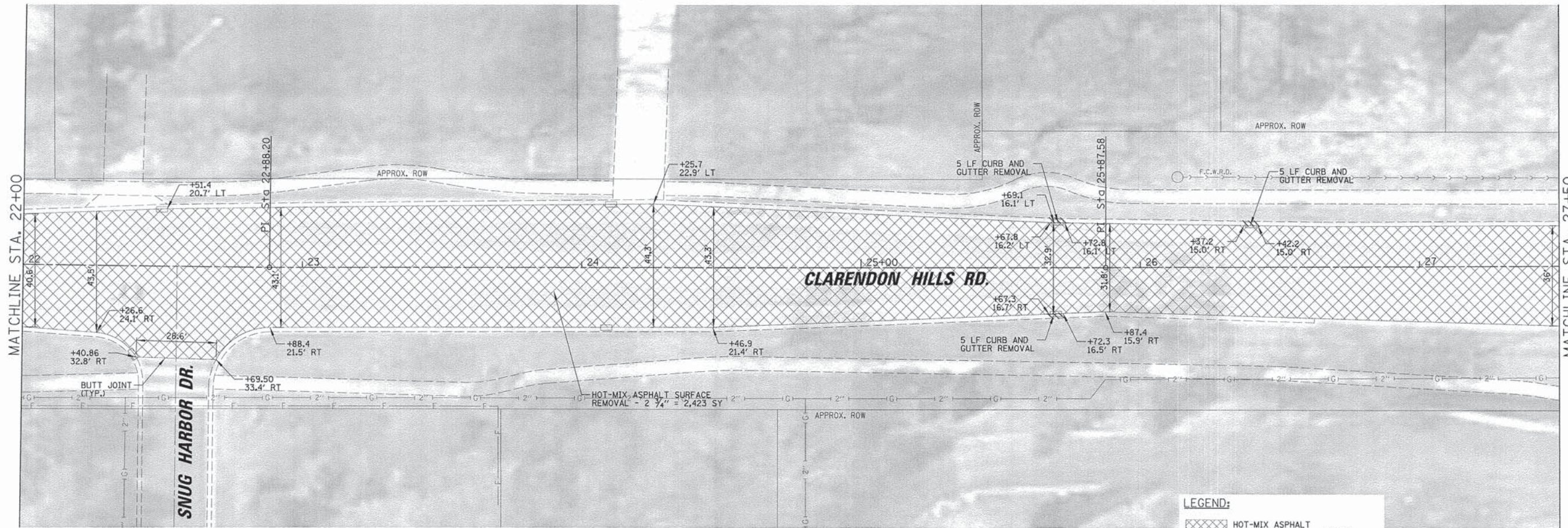
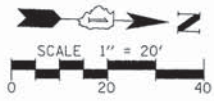


LEGEND:

- HOT-MIX ASPHALT SURFACE REMOVAL - 2 3/4"
- EXISTING CURB AND GUTTER
- EXISTING MANHOLE/CATCH BASIN
- EXISTING INLET
- CURB AND GUTTER REMOVAL
- SIDEWALK REMOVAL
- DRIVEWAY PAVEMENT REMOVAL
- STRUCTURES TO BE ADJUSTED

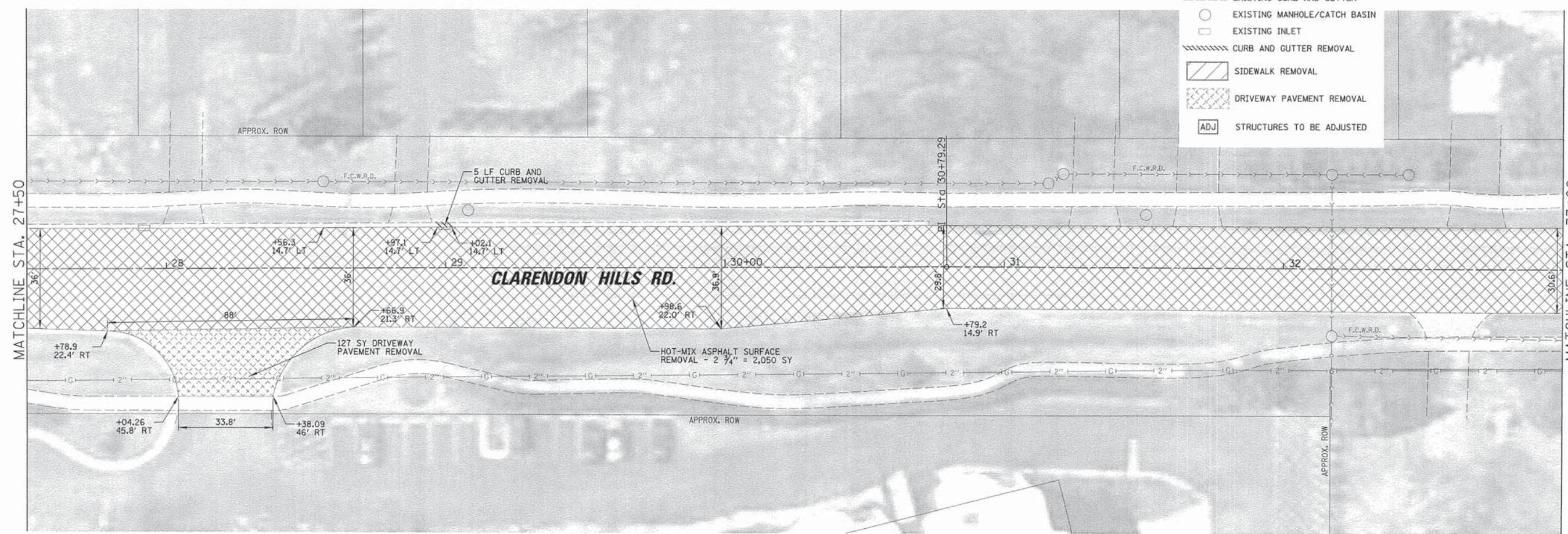
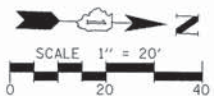


FILE NAME =	USER NAME = jlapaglia	DESIGNED - JAL	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CLARENDON HILLS ROAD EXISTING CONDITIONS AND REMOVAL PLAN	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
N:\WILLLOWBROOK\150469\Civil\rem.150469.dwg	PLOT SCALE = 20'	DRAWN - EDT	REVISED -			2663	15-00022-00-RS	DuPAGE	26	5	
	PLOT DATE = 12/15/2015	CHECKED - LMF	REVISED -			CONTRACT NO. 61C27					
		DATE - \$DATE\$	REVISED -			ILLINOIS FED. AID PROJECT					

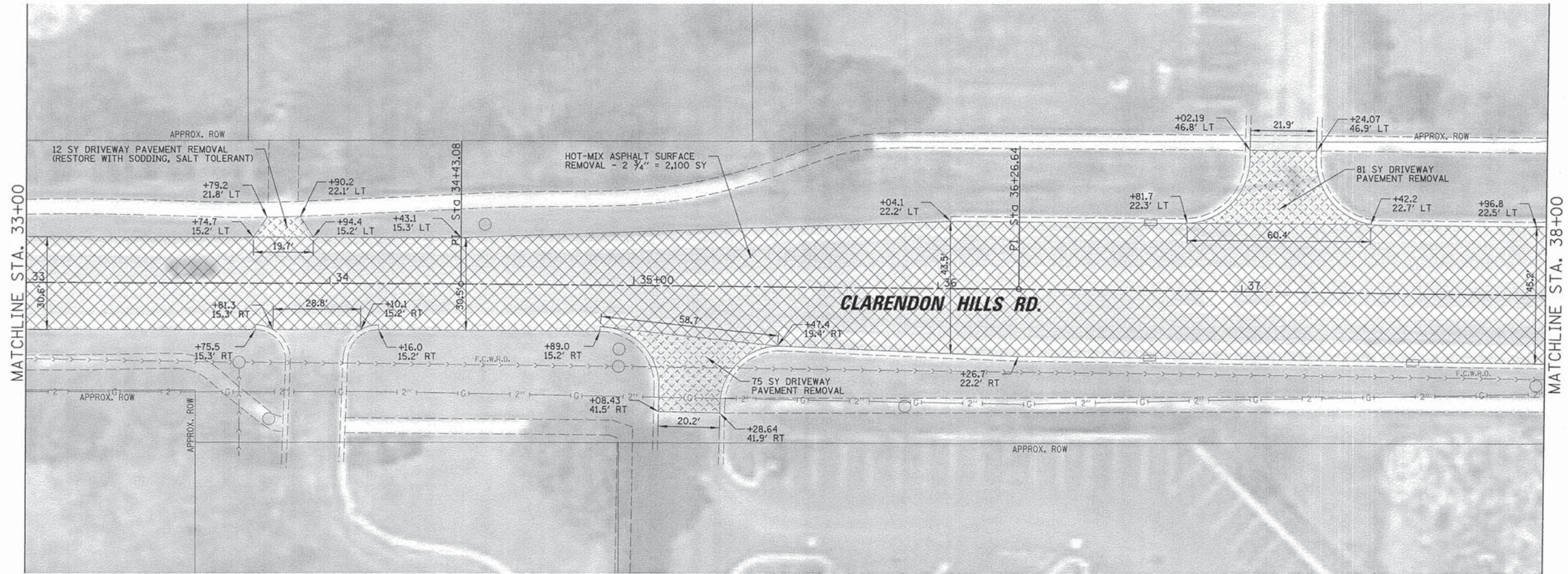
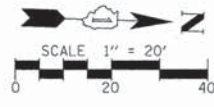


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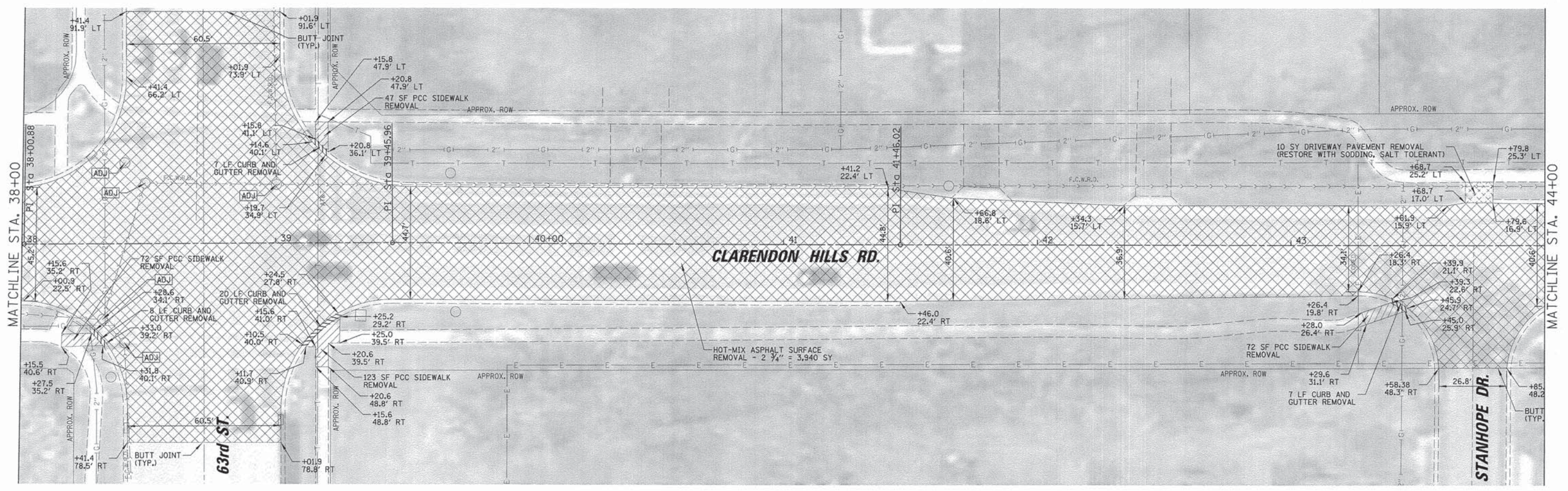
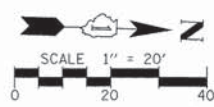
- HOT-MIX ASPHALT SURFACE REMOVAL - 2 3/4"
- EXISTING CURB AND GUTTER
- EXISTING MANHOLE/CATCH BASIN
- EXISTING INLET
- CURB AND GUTTER REMOVAL
- SIDEWALK REMOVAL
- DRIVEWAY PAVEMENT REMOVAL
- STRUCTURES TO BE ADJUSTED



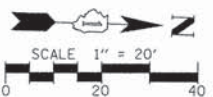
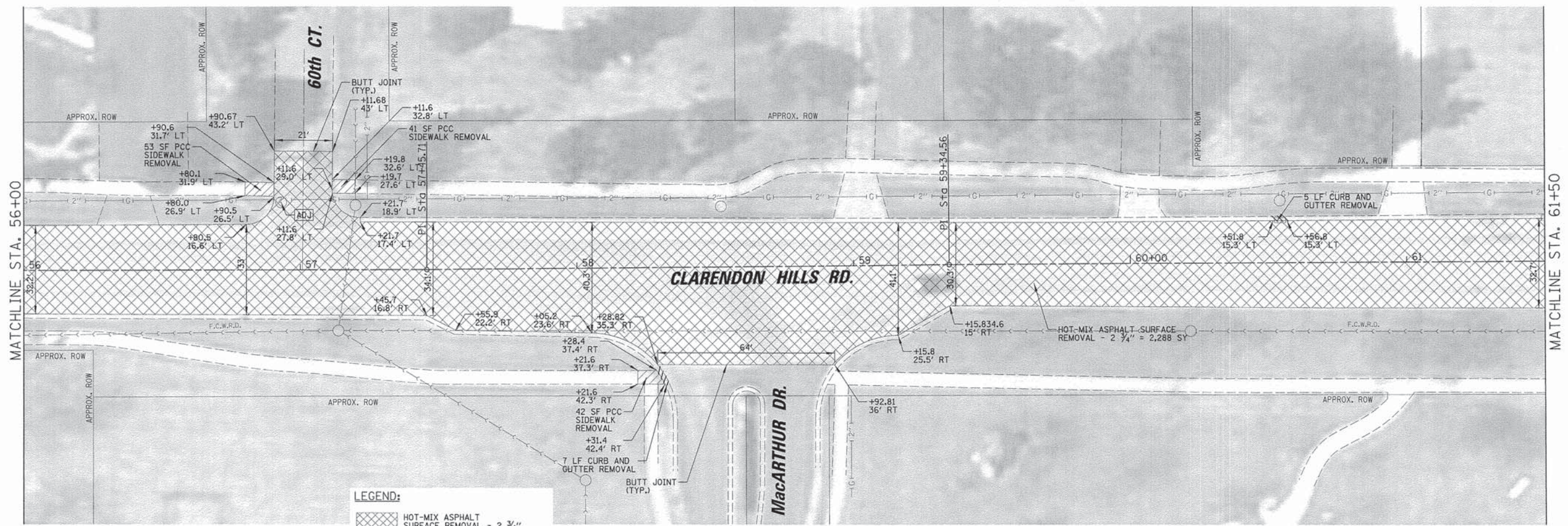
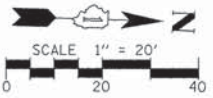
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	PLOT DATE = 12/15/2015	CHECKED - LMF	REVISED -								
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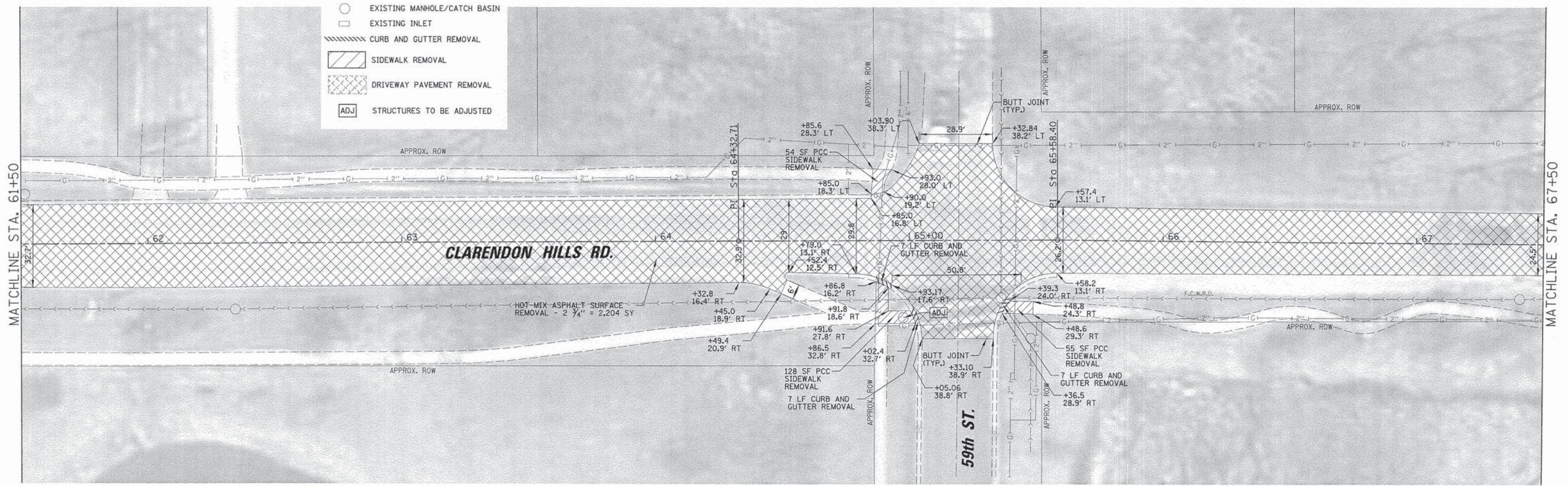
- LEGEND:**
- HOT-MIX ASPHALT SURFACE REMOVAL - 2 3/4"
 - EXISTING CURB AND GUTTER
 - EXISTING MANHOLE/CATCH BASIN
 - EXISTING INLET
 - CURB AND GUTTER REMOVAL
 - SIDEWALK REMOVAL
 - DRIVEWAY PAVEMENT REMOVAL
 - STRUCTURES TO BE ADJUSTED



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PLOT DATE 12/15/2015	DATE - \$DATE\$	CHECKED - LMF	REVISED -			SCALE: 20'		SHEET NO. 7 OF 26 SHEETS		STA. TO STA.	
		DATE - \$DATE\$	REVISED -			CONTRACT NO. 61C27		ILLINOIS FED. AID PROJECT			



- LEGEND:**
- HOT-MIX ASPHALT SURFACE REMOVAL - 2 3/4"
 - EXISTING CURB AND GUTTER
 - EXISTING MANHOLE/CATCH BASIN
 - EXISTING INLET
 - CURB AND GUTTER REMOVAL
 - SIDEWALK REMOVAL
 - DRIVEWAY PAVEMENT REMOVAL
 - STRUCTURES TO BE ADJUSTED



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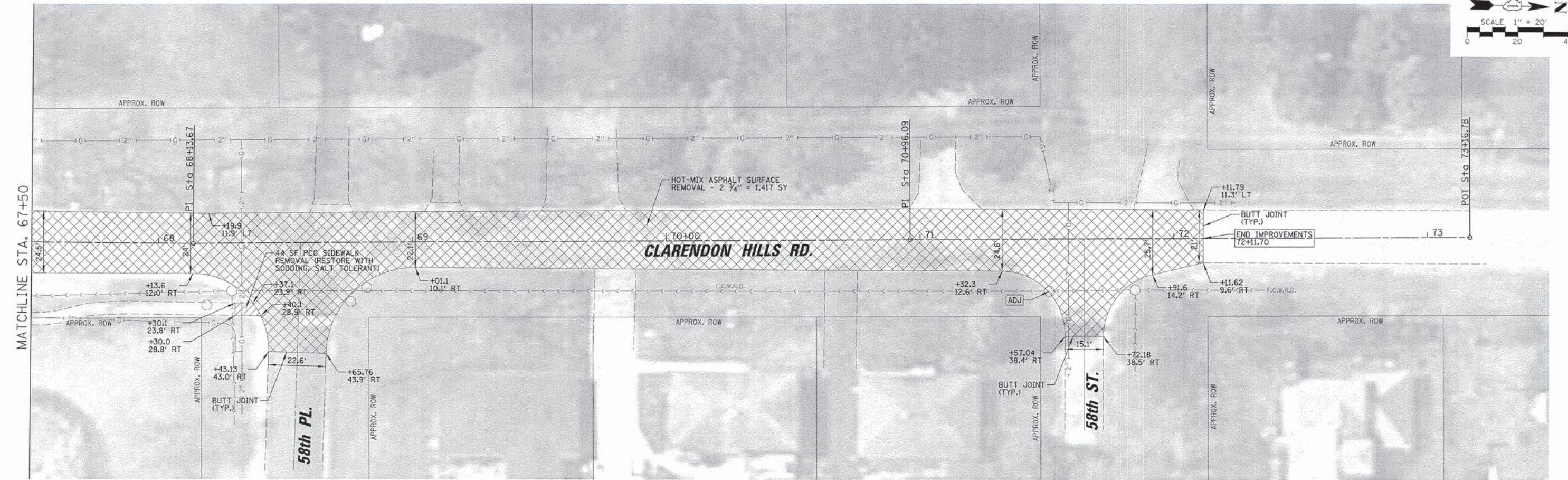
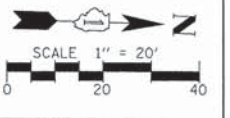
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DATE - \$DATE\$	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CLARENDON HILLS ROAD
EXISTING CONDITIONS AND REMOVAL PLAN**

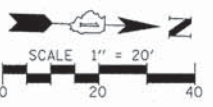
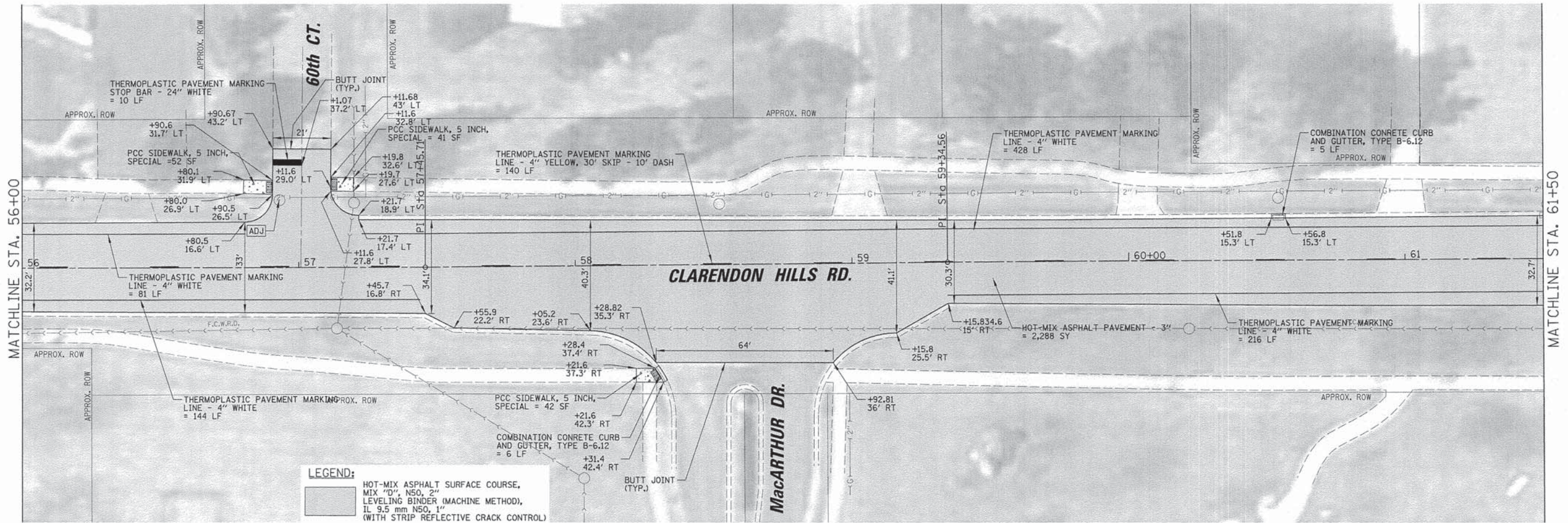
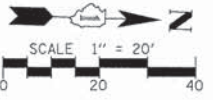
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2663	15-00022-00-RS	DuPAGE	26	9
CONTRACT NO. 61C27				
ILLINOIS FED. AID PROJECT				



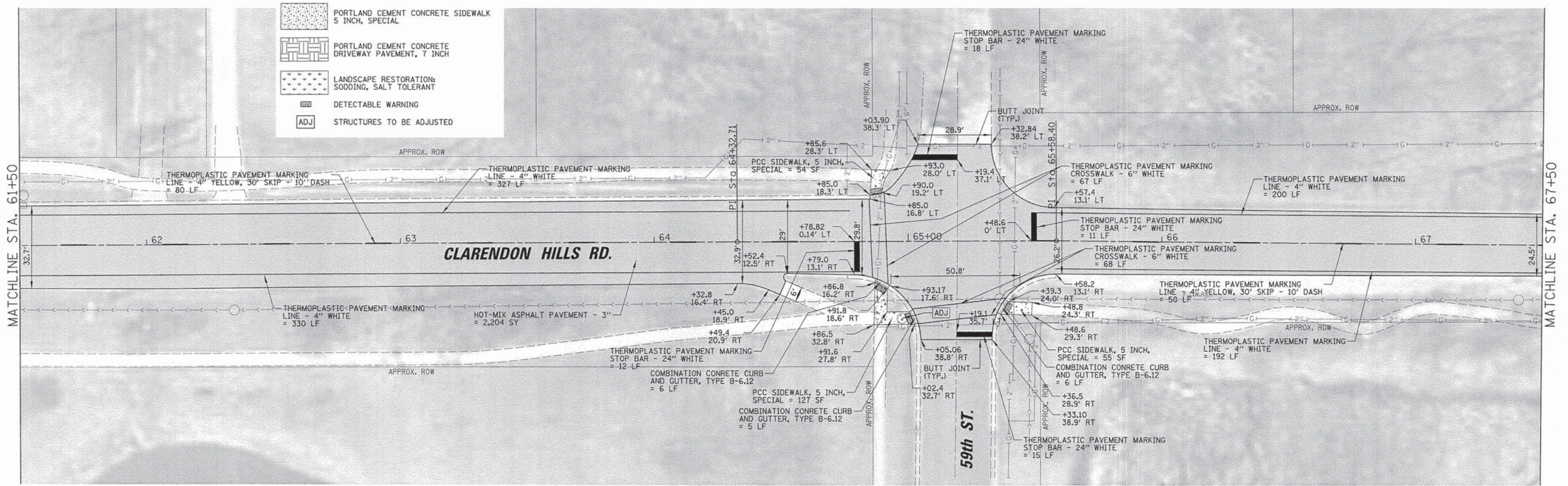
- LEGEND:**
- HOT-MIX ASPHALT SURFACE REMOVAL - 2 3/4"
 - EXISTING CURB AND GUTTER
 - EXISTING MANHOLE/CATCH BASIN
 - EXISTING INLET
 - CURB AND GUTTER REMOVAL
 - SIDEWALK REMOVAL
 - DRIVEWAY PAVEMENT REMOVAL
 - STRUCTURES TO BE ADJUSTED

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	PLOT DATE = 12/15/2015	CHECKED - LMF	REVISED -			ILLINOIS FED. AID PROJECT						
		DATE - #DATE#	REVISED -									



LEGEND:

	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2" LEVELING BINDER (MACHINE METHOD), IL 9.5 mm N50, 1" (WITH STRIP REFLECTIVE CRACK CONTROL)
	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL
	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH
	LANDSCAPE RESTORATION: SODDING, SALT TOLERANT
	DETECTABLE WARNING
	STRUCTURES TO BE ADJUSTED



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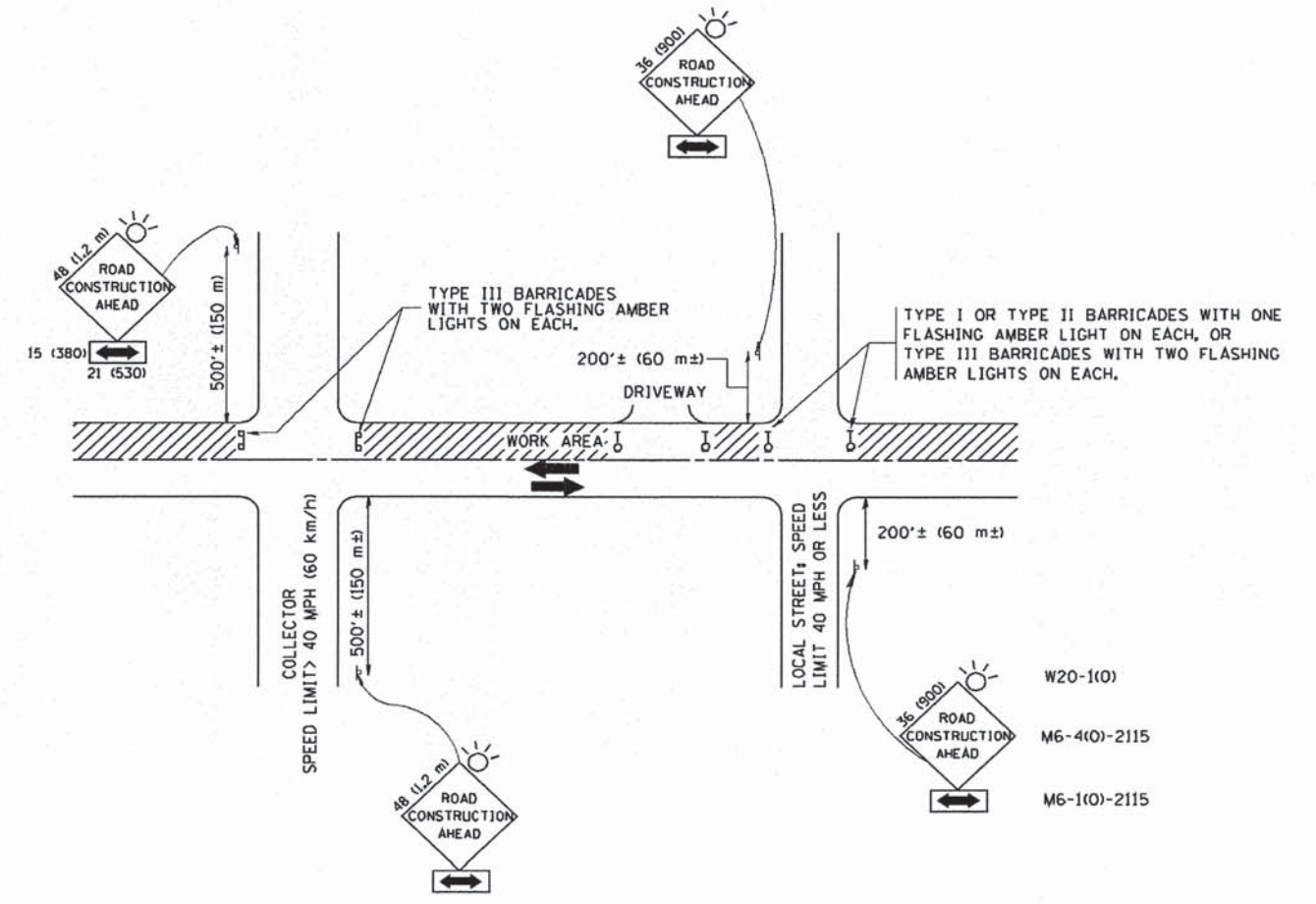
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DATE - #DATE#	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CLARENDON HILLS ROAD
PROPOSED ROADWAY IMPROVEMENTS PLAN**

SCALE: 20' SHEET NO. 15 OF 26 SHEETS STA. TO STA.

F.A.U. RTE. 2663	SECTION 15-00022-00-RS	COUNTY DuPAGE	TOTAL SHEETS 26	SHEET NO. 15
CONTRACT NO. 61C27			ILLINOIS FED. AID PROJECT	



W20-1(0)
M6-4(0)-2115
M6-1(0)-2115

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS:
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

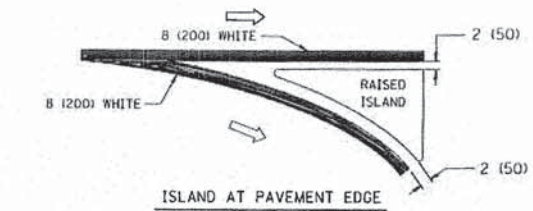
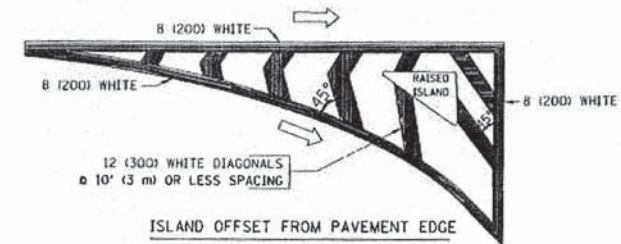
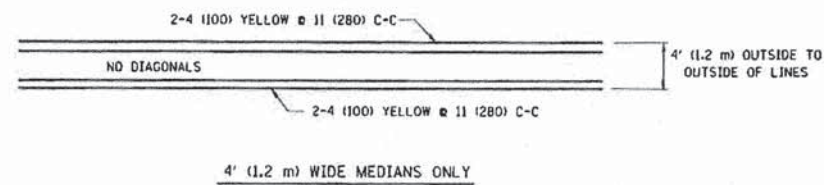
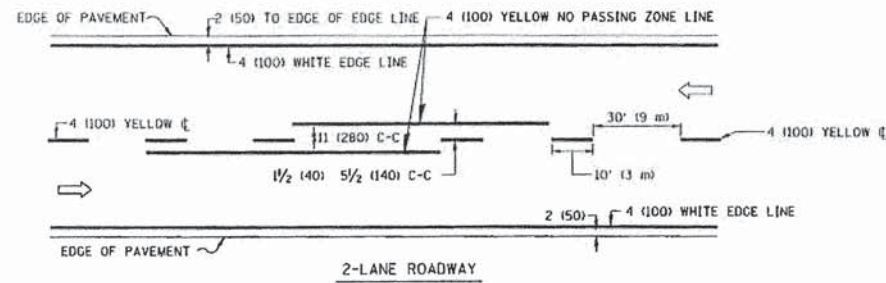
All dimensions are in millimeters (inches) unless otherwise shown.

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		DRAWN -	REVISED - A. HOUSEH 03-06-96
		CHECKED -	REVISED - A. HOUSEH 10-15-96
		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

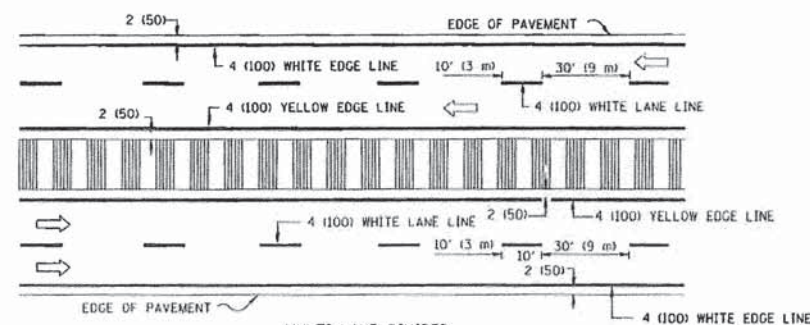
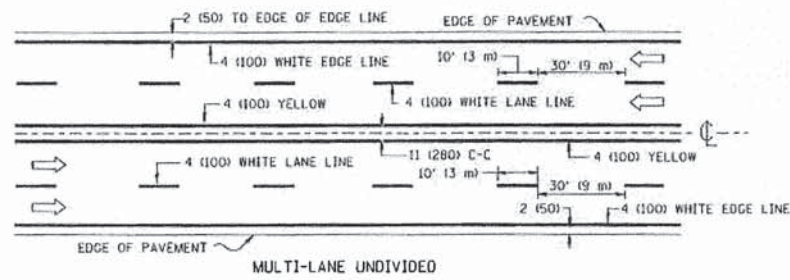
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			
SCALE: NONE	SHEET NO.	OF SHEETS	STA. TO STA.

F.A. RTE. 2663	SECTION 15-00022-00-RS	COUNTY DuPAGE	TOTAL SHEETS 26	SHEET NO. 17
TC-10			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

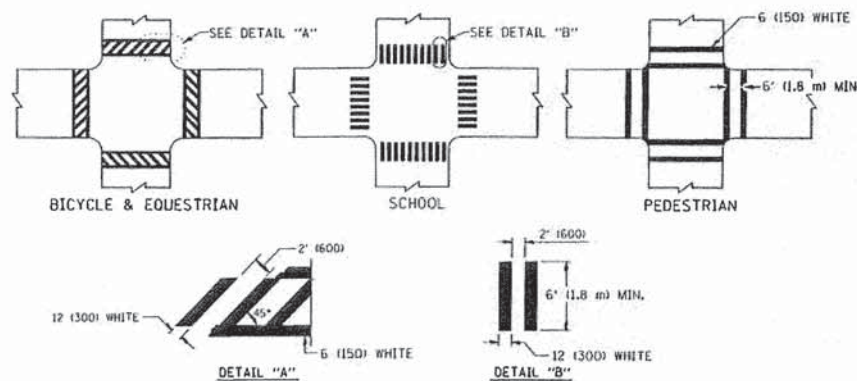


TYPICAL ISLAND MARKING

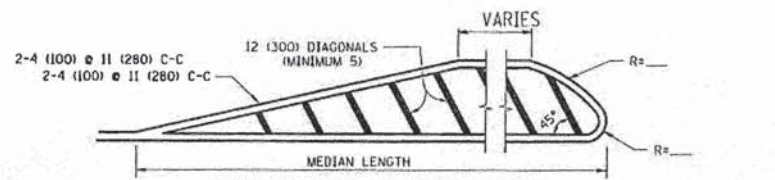


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



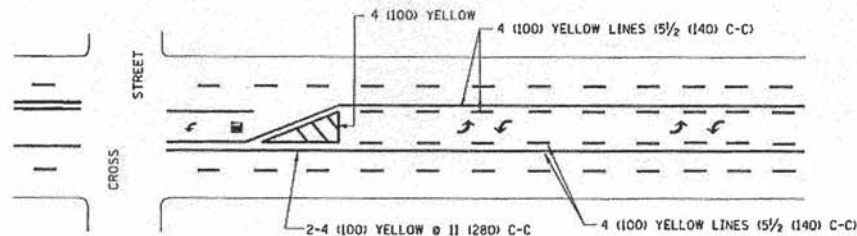
TYPICAL CROSSWALK MARKING



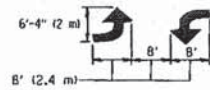
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

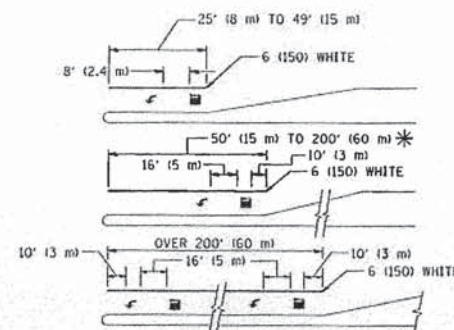


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²); AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

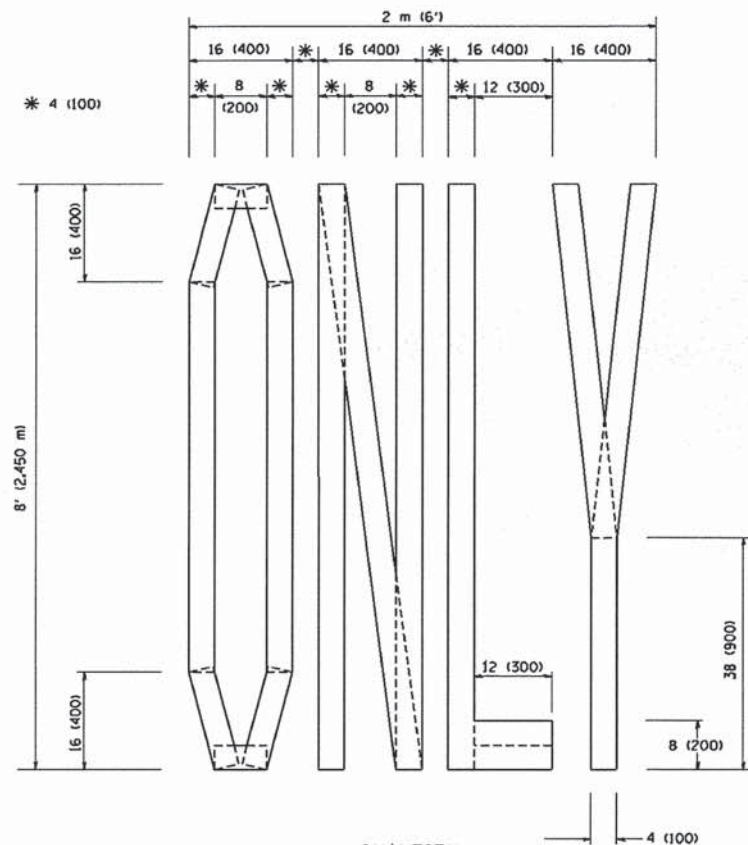
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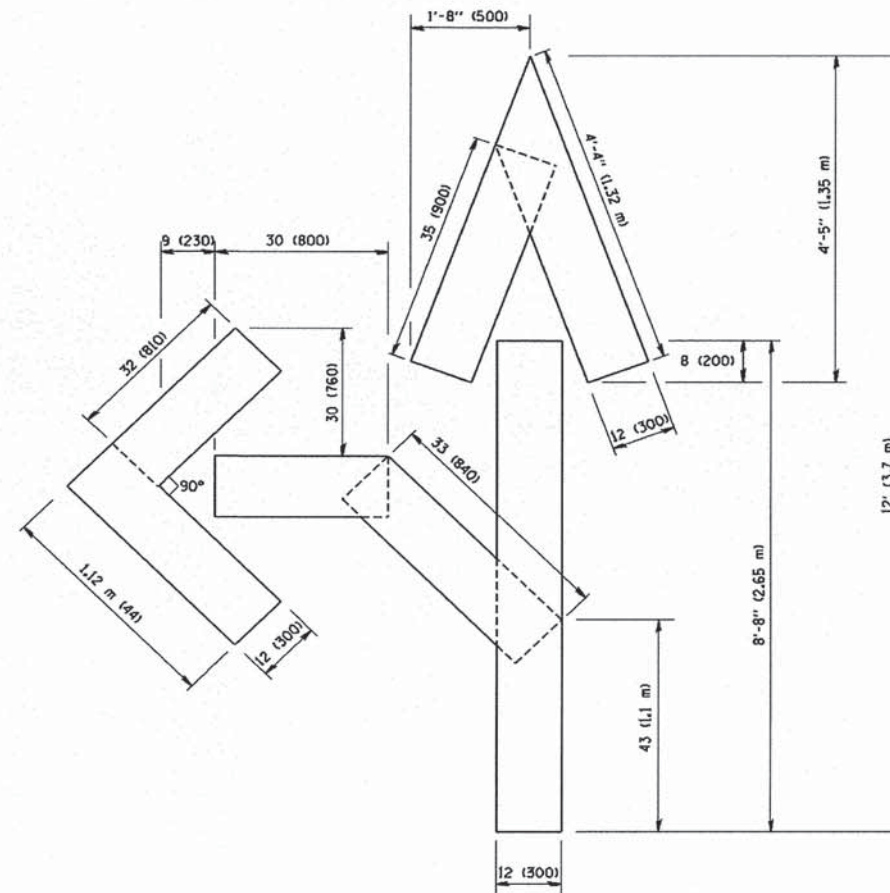
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE	
TYPICAL PAVEMENT MARKINGS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

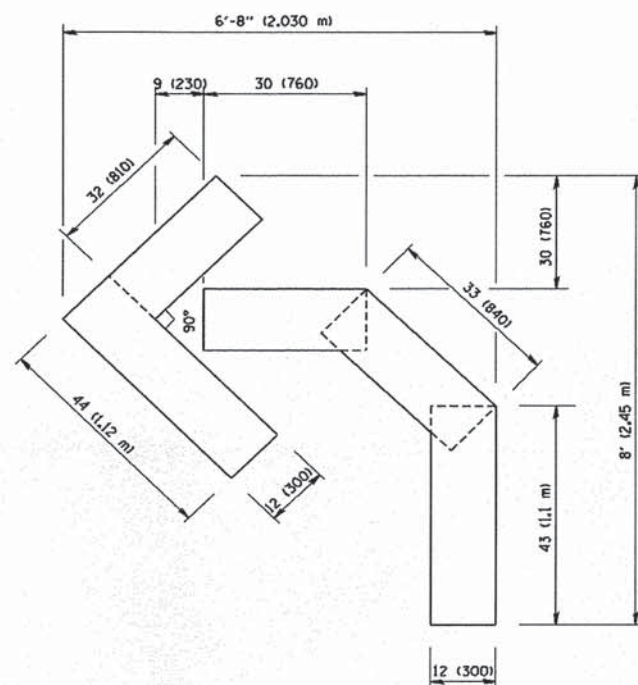
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2663	15-00022-00-RS	DuPAGE	26	18
TC-13		CONTRACT NO.		
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in Inches (millimeters) unless otherwise shown.

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 PLOT DATE = 1/4/2008

DESIGNED -
 DRAWN -
 CHECKED -
 DATE - 09-18-94

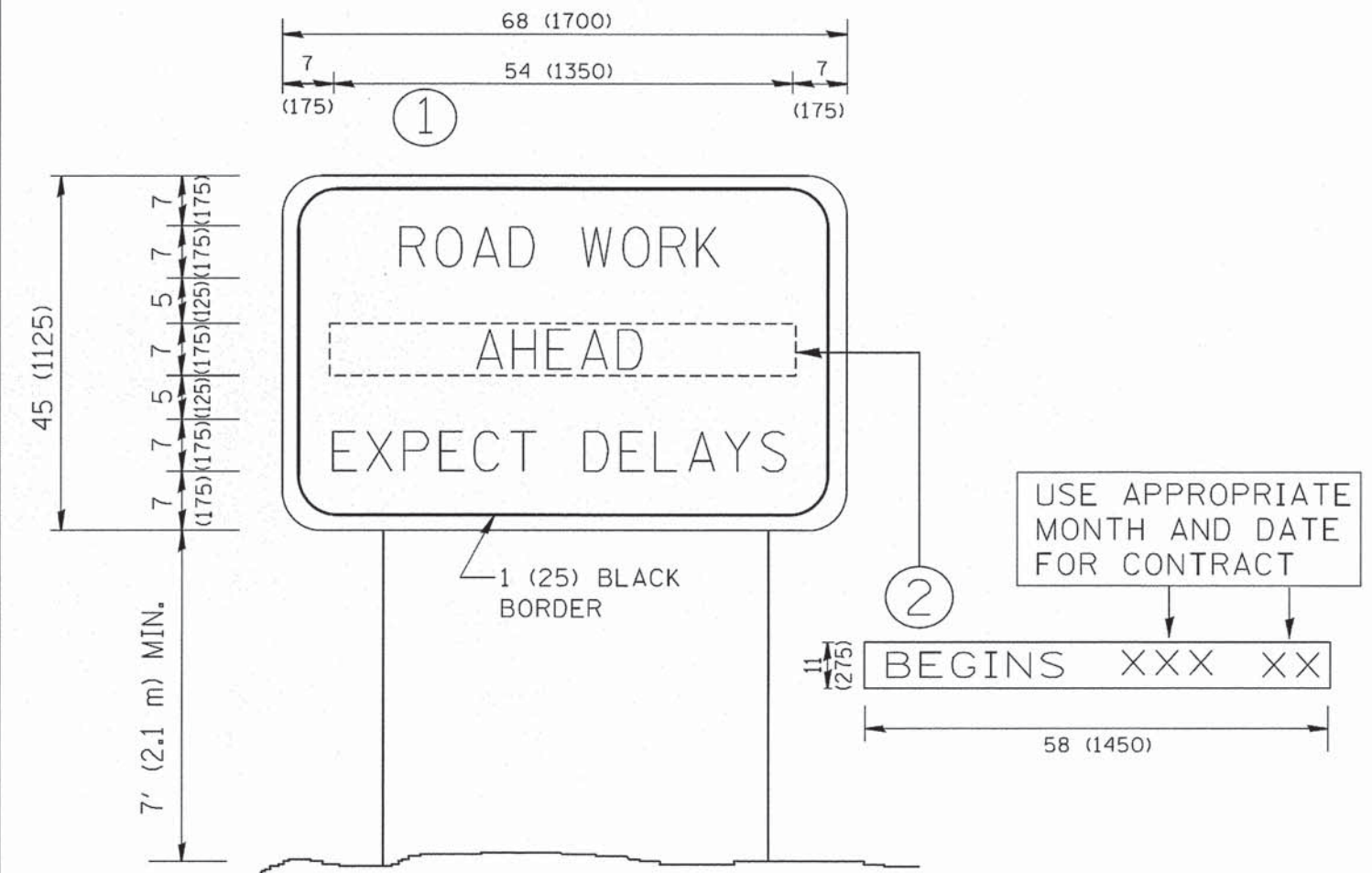
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 REVISED -T. RAMMACHER 11-04-97
 REVISED -T. RAMMACHER 03-02-98
 REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2663	15-00022-00-RS	DuPAGE	26	19
TC-16			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

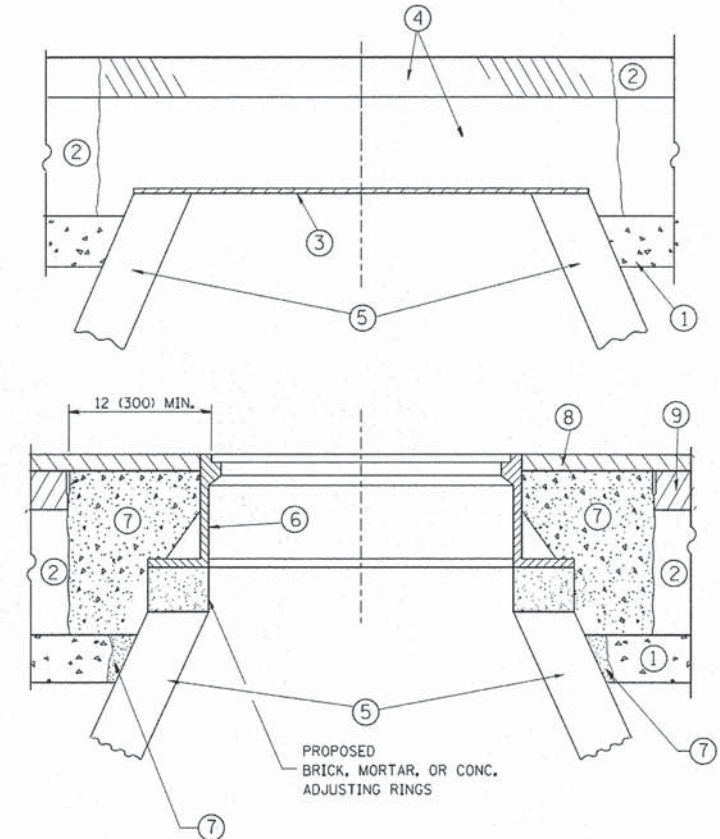
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	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD
INFORMATION SIGN

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2663	15-00022-00-RS	DuPAGE	26	20
TC-22			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
 - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
 - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
 - D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

- STAGE 2 (AFTER PAVEMENT MILLING)**
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- *UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

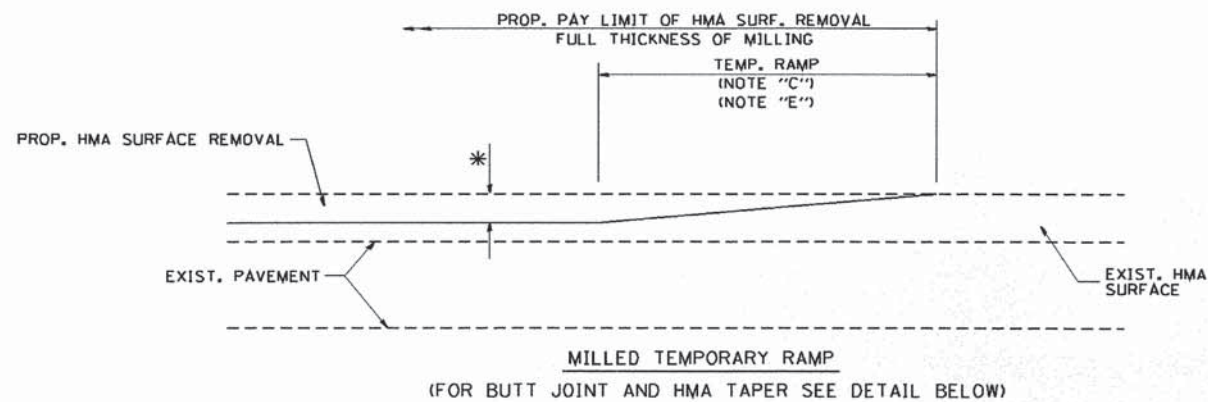
THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

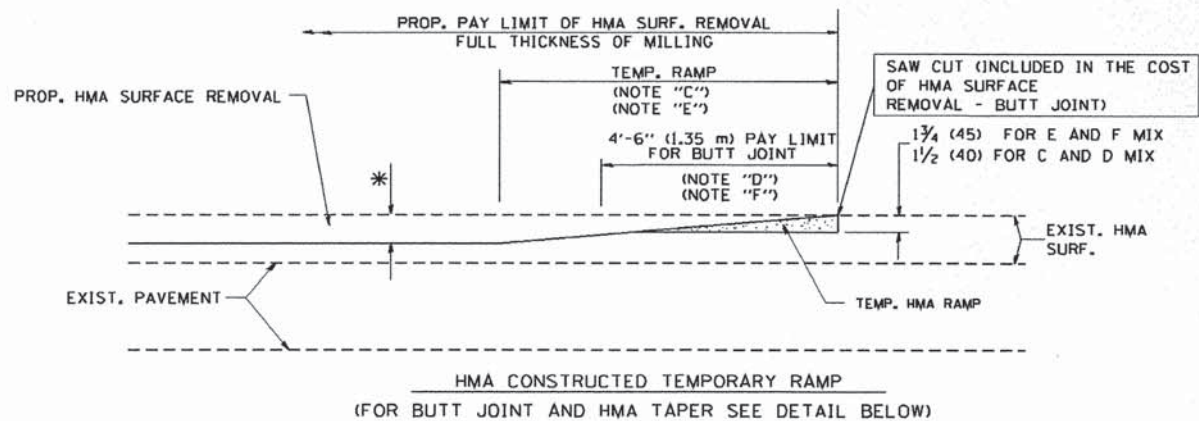
DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING	F.A. RTE. =	SECTION =	COUNTY =	TOTAL SHEETS =	SHEET NO. =	
ct:\pw\work\pvidot\baueardl\d0108315\bd08.dgn		DRAWN -	REVISED - R. BORO 01-01-07			2663	15-00022-00-RS	DUPAGE	26	21	
		PLOT SCALE = 1/648.5000' / m	REVISED - R. BORO 03-09-11			BD600-03 (BD-8)		CONTRACT NO.			
		PLOT DATE = 12/6/2011	REVISED - R. BORO 12-06-11			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					
					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS		STA.	TO STA.		

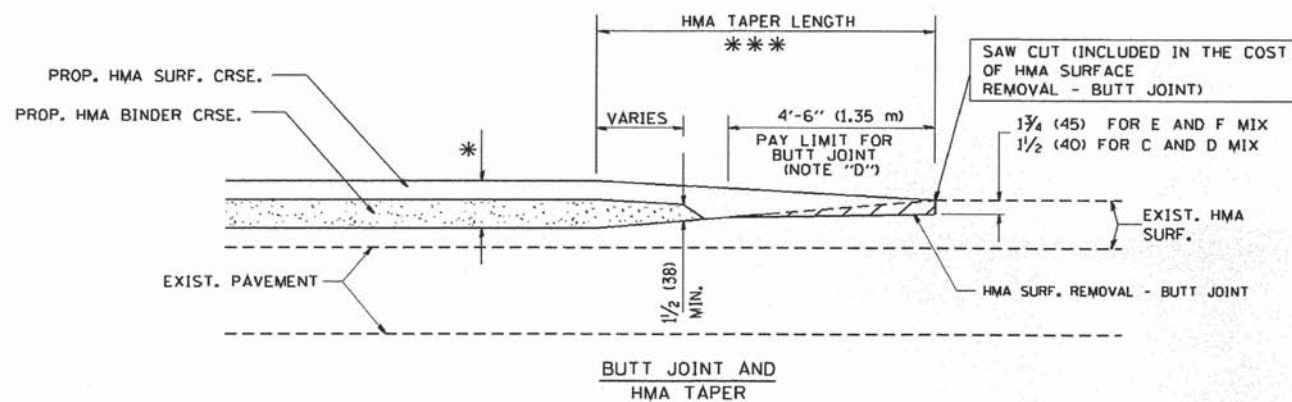


OPTION 1

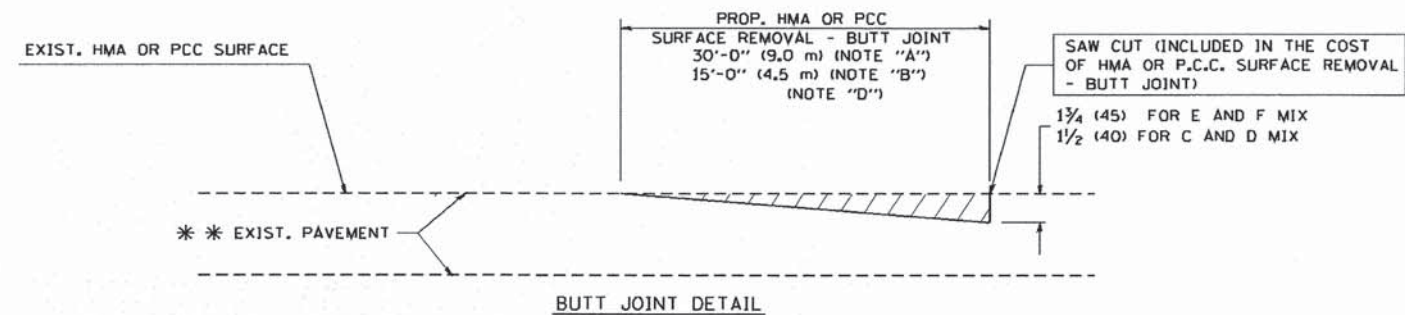


OPTION 2

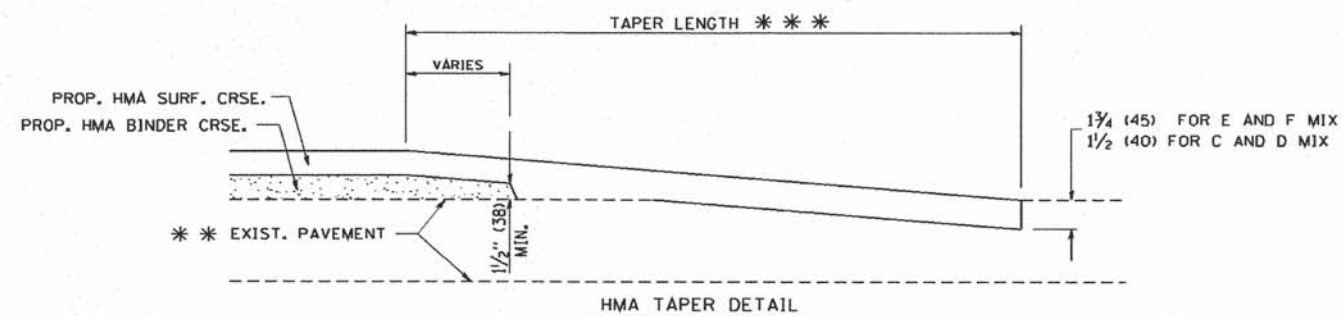
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- * * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

ALL BUTT JOINTS ARE INCIDENTAL TO THE SURFACE COURSE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\drtstd\22x34\bd32.dgn

USER NAME = gegljanobt
PLOT SCALE = 50.0000 / IN.
PLOT DATE = 1/4/2008

DESIGNED - M. DE YONG
DRAWN -
CHECKED -
DATE - 06-13-90
REVISED - R. SHAH 10-25-94
REVISED - A. ABBAS 03-21-97
REVISED - M. GOMEZ 04-06-01
REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

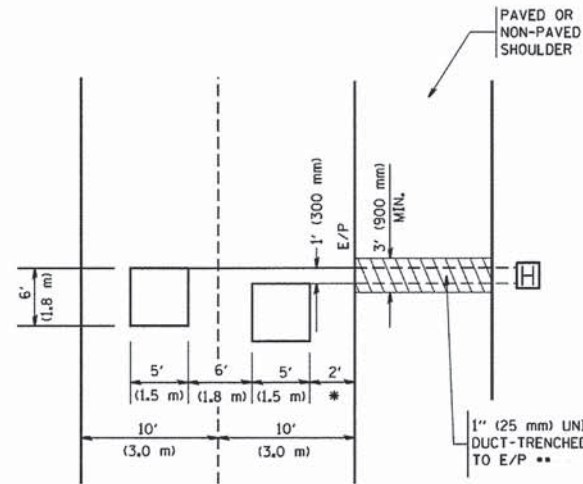
BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2663	15-00022-00-RS	DuPAGE	26	22
BD400-05 BD32			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

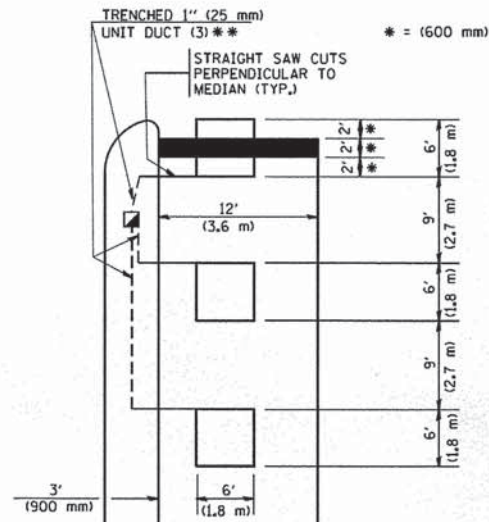


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

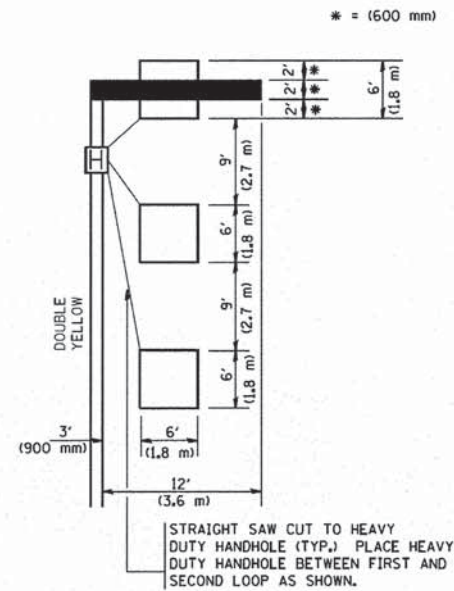
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

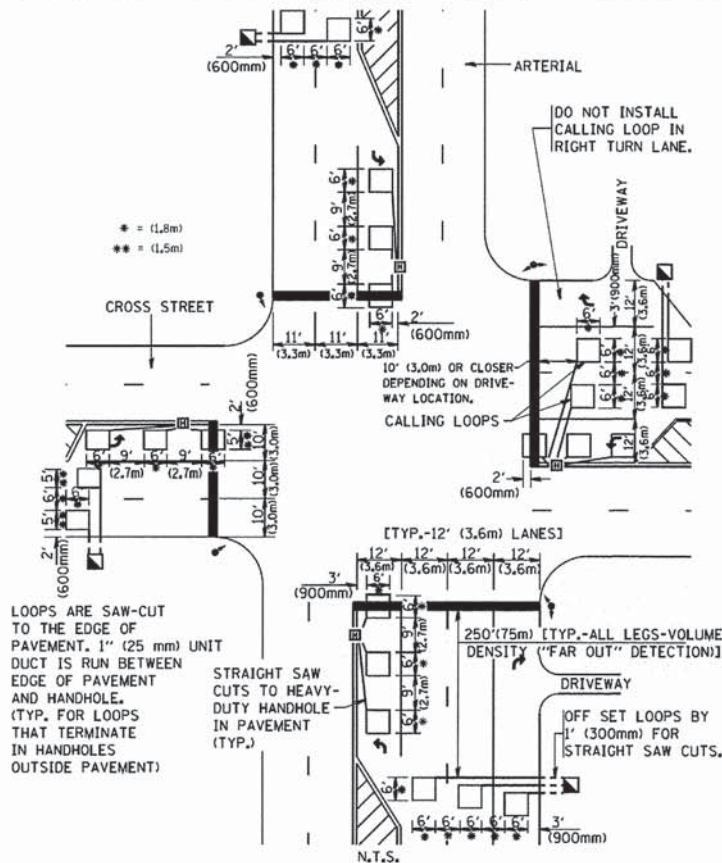
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



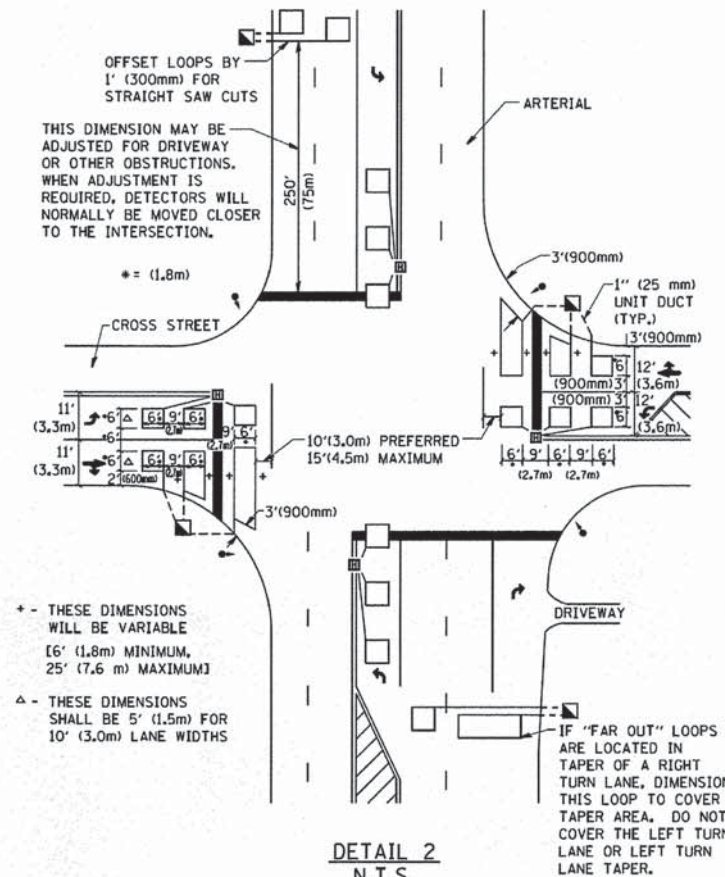
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\diststd\22x34\ts07.dgn

USER NAME = gaglianobt
PLOT SCALE = 50.0000 / IN.
PLOT DATE = 1/4/2008

DESIGNED -
DRAWN -
CHECKED - R.K.F.
DATE -

REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

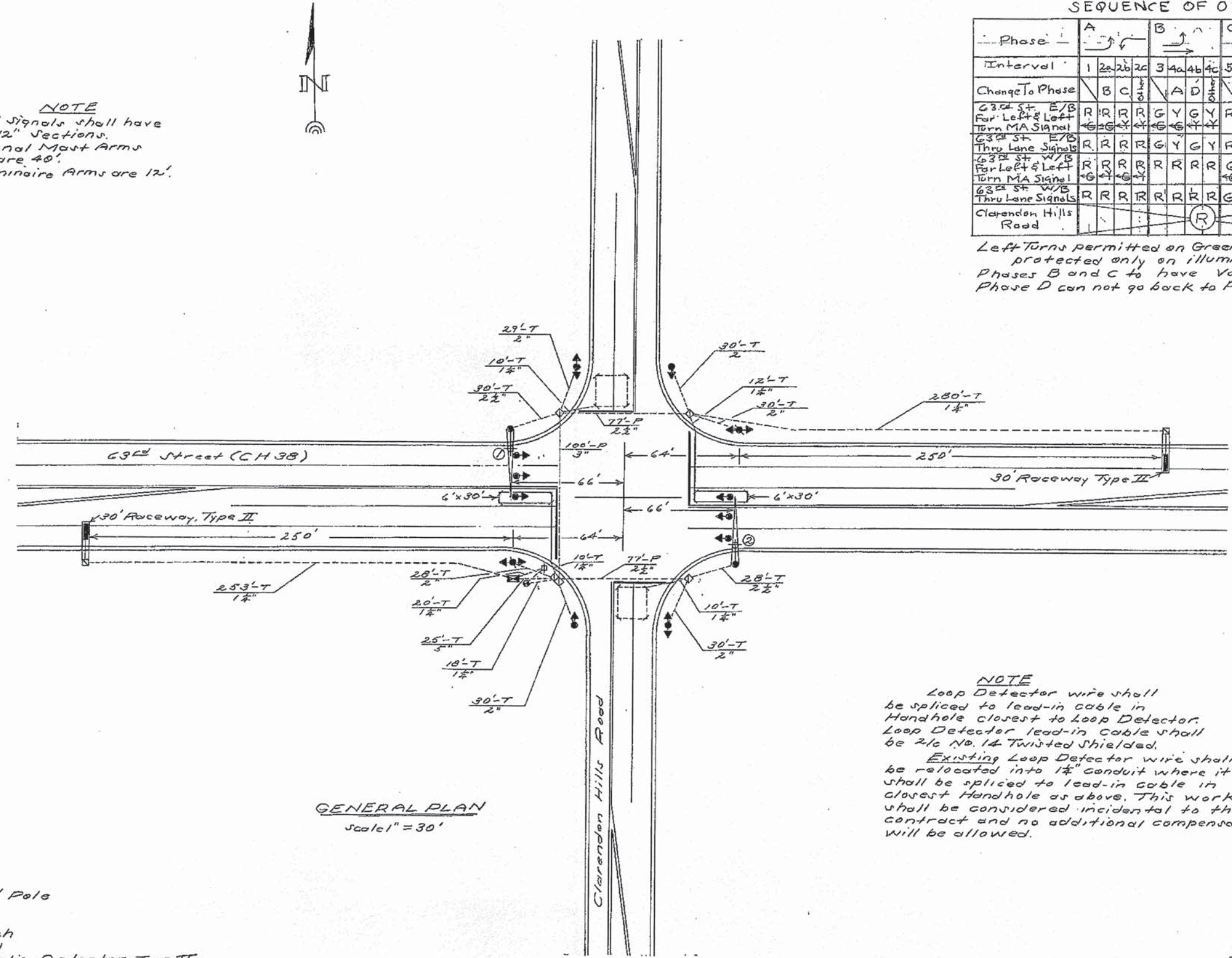
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2663	15-00022-00-RS	DuPAGE	26	24
TS-07		CONTRACT NO.		
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				

SEQUENCE OF OPERATION

Phase	A		B			C			D		E		Fig. OP.		
Interval	1	2a	2b	2c	3	4a	4b	4c	5	6a	6b	7		8	9
Change To Phase		B	C	Other	A	D	Other	Other	A	D	Other	Other	Other	Other	Other
63rd St. E/B For Left & Left Turn MA Signal	R	R	R	R	G	Y	G	Y	R	R	R	R	G	Y	
63rd St. E/B Thru Lane Signals	R	R	R	R	G	Y	G	Y	R	R	R	R	G	Y	(R)
63rd St. W/B For Left & Left Turn MA Signal	R	R	R	R	R	R	R	R	G	Y	G	Y	G	Y	
63rd St. W/B Thru Lane Signals	R	R	R	R	R	R	R	R	G	Y	G	Y	G	Y	
Clarendon Hills Road									(R)					G	Y

Left Turns permitted on Green Signal but protected only on illuminated Arrow.
 Phases B and C to have Volume Density.
 Phase D can not go back to Phases A, B, and C.

NOTE
 All Signals shall have 12" Sections.
 Signal Mast Arms are 40'.
 Luminaire Arms are 12'.



GENERAL PLAN
 Scale 1" = 30'

NOTE
 Loop Detector wire shall be spliced to lead-in cable in Handhole closest to Loop Detector. Loop Detector lead-in cable shall be 2/c No. 14 Twisted Shielded.
 Existing Loop Detector wire shall be relocated into 1 1/2" conduit where it shall be spliced to lead-in cable in closest Handhole as above. This work shall be considered incidental to the contract and no additional compensation will be allowed.

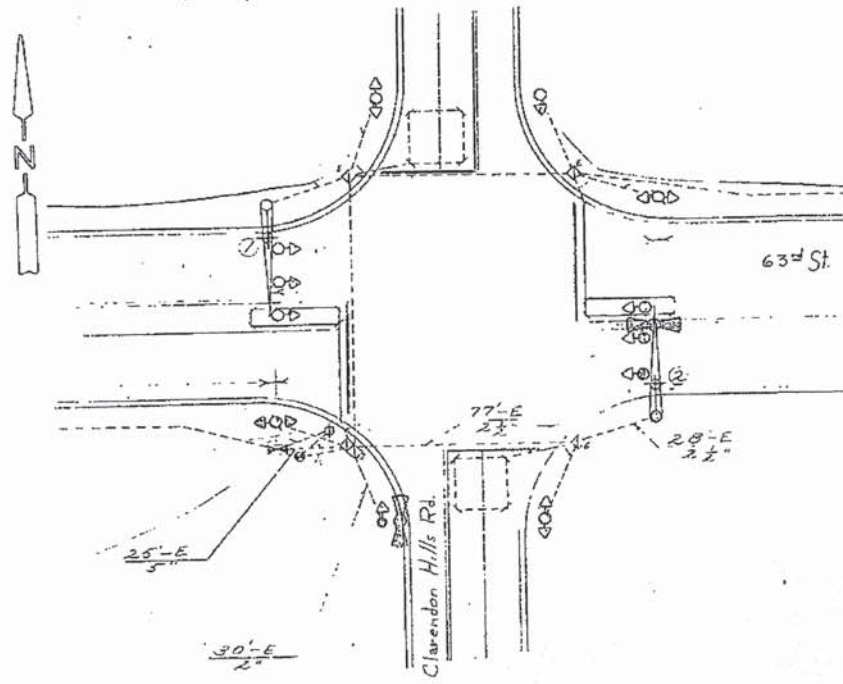
LEGEND

- Controller (Signals)
- Controller (Lighting)
- Signal Head
- Signal Head Split
- Mast Arm Assembly & Pole
- Handhole
- Double Handhole
- G.S. Conduit in Trench
- G.S. Conduit Pushed
- Raceway For Magnetic Detector, Type II
- Magnetic Detector
- Luminaire
- Detector Loop
- Existing Detector Loop

"FOR INFORMATION ONLY"

DUPAGE COUNTY HWY. DEPT.
 TRAFFIC CONTROL SIGNALS
 63rd St & Clarendon Hills Rd.
 Section 80-00241-03-TL

PROPOSED TRAFFIC SIGNAL PLAN

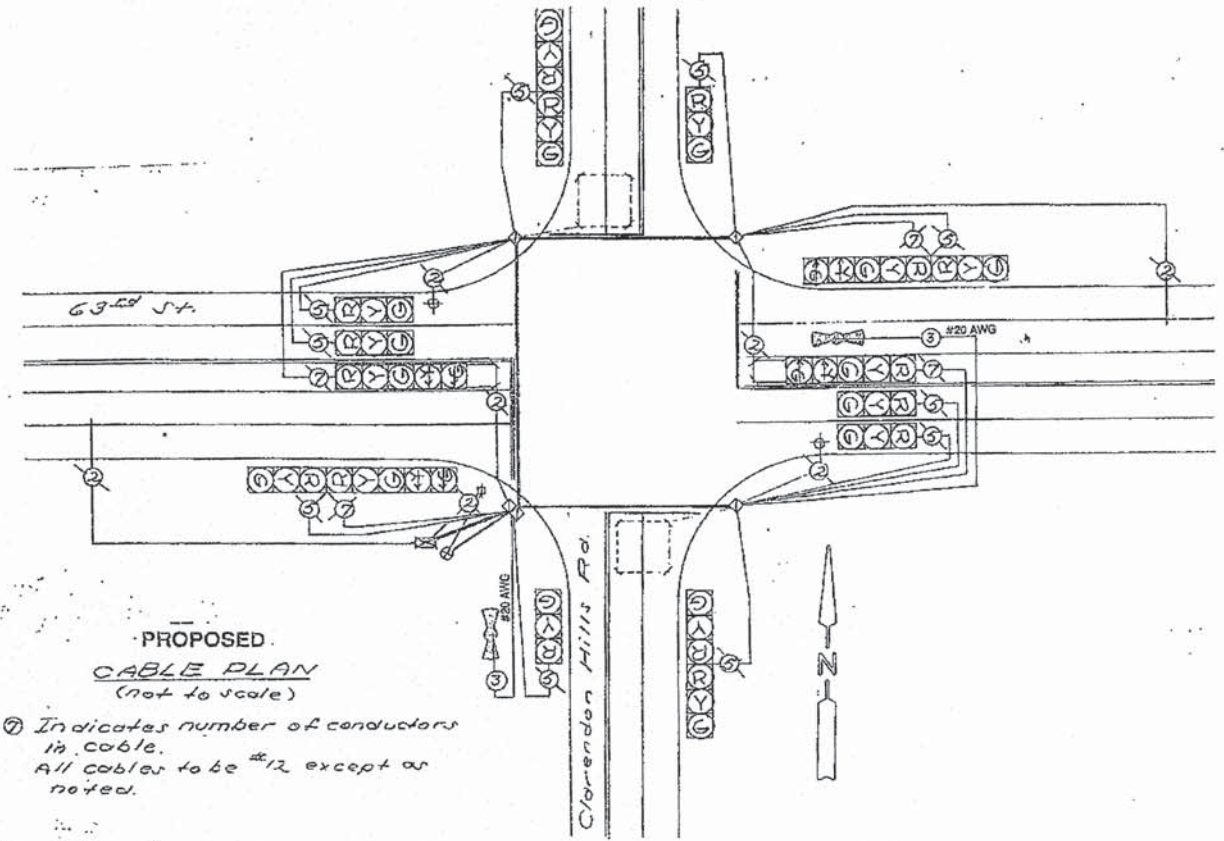


PRIORITY SEQUENCE FOR CONTROLLER SEQUENCE IV

PROPOSED PRIORITY LANES	
PRIORITY LANE INTERVAL	1 2
MOVEMENT	← → ↑ ↓

NOTES:

1. TERMINATION OF PHASES 1+5 OR 3+7 SHALL BE WITH A YELLOW ARROW DISPLAYED TOGETHER WITH A CIRCULAR RED.
2. TERMINATION OF PHASES 1, 3, 5, OR 7 ALONE IN PHASES 1+6, 3+8, 2+5, OR 4+7 SHALL BE WITH A YELLOW ARROW DISPLAYED TOGETHER WITH A CIRCULAR GREEN WHEN FOLLOWED BY A PRIORITY LANE INTERVAL WHICH DISPLAYS THE CIRCULAR GREEN.
3. TERMINATION OF PHASES 1+6, 3+8, 2+5, OR 4+7 SHALL BE WITH A CIRCULAR YELLOW DISPLAY WHEN FOLLOWED BY A PRIORITY LANE INTERVAL WHICH DISPLAYS A CIRCULAR RED.
4. TERMINATION OF PHASES 2+6 OR 4+8 SHALL BE WITH A CIRCULAR YELLOW WHEN FOLLOWED BY A PRIORITY LANE INTERVAL WHICH DISPLAYS A CIRCULAR RED. WHEN PHASES 2+6 OR 4+8 CIRCULAR GREEN IS TO BE DISPLAYED IN THE PRIORITY LANE INTERVAL, IT SHALL REMAIN GREEN.
5. TERMINATION OF ALL PEDESTRIAN INTERVALS SHALL INCLUDE A FULL FLASHING "DON'T WALK" CLEARANCE INTERVAL.
6. TERMINATION OF ALL PRIORITY INTERVALS SHALL BE WITH A CIRCULAR YELLOW EXCEPT WHEN THE GREEN DISPLAYED DURING THE PRIORITY INTERVAL IS TO REMAIN GREEN WHEN THE NORMAL SEQUENCE OF OPERATIONS RESUMES CONTROL OF THE INTERSECTION.
7. IF ALL RED CLEARANCE IS USED IN THE NORMAL SEQUENCE OF OPERATIONS, IT MUST BE DISPLAYED AFTER THE YELLOW CLEARANCE INTERVAL WHEN ENTERING OR LEAVING THE PRIORITY SEQUENCE.



PROPOSED CABLE PLAN (not to scale)

Ⓢ Indicates number of conductors in cable. All cables to be #12 except as noted.

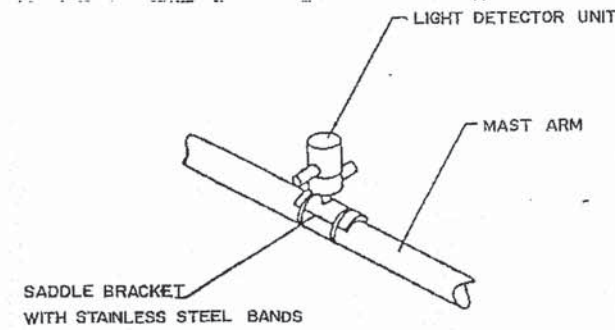
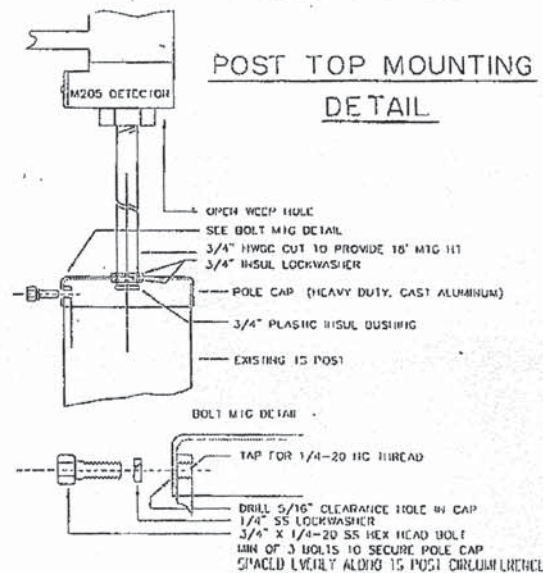
Ⓢ LIGHT DETECTOR

PROPOSED TRAFFIC SIGNAL LEGEND

- Ⓢ Controller (Signals)
- Ⓢ Controller (Lighting)
- Ⓢ Signal Head
- Ⓢ Signal Head Split
- Ⓢ Mast Arm Assembly & Pole
- Ⓢ Handhole
- Ⓢ Double Handhole
- Ⓢ G S Conduit in Trench
- Ⓢ G S Conduit Pushed
- Ⓢ Raceway For Magnetic Detector, Type II
- Ⓢ Magnetic Detector
- Ⓢ Luminaire
- Ⓢ Detector Loop
- Ⓢ Existing Detector Loop

Ⓢ LIGHT DETECTOR

POST TOP MOUNTING DETAIL



MAST ARM MOUNTING DETAIL FOR EMERGENCY VEHICLE PRE-EMPTION SYSTEM LIGHT DETECTOR

NOT TO SCALE

EXISTING SEQUENCE OF OPERATIONS

Phase	A	B	C	D	E	Op.
Interval	1 2a 2b 2c	3 4a 4b 4c	5 6a 6b 6c	7 8	9 10	Fig.
Change To Phase	B C	A D	A D	ALL	Other	
63rd St. E/B For Left & Right Turn MA Signal	R R R R	G Y G Y	R R R R	G Y		R
63rd St. E/B Thru Lane Signals	R R R R	G Y G Y	R R R R	G Y		R
63rd St. W/B For Left & Right Turn MA Signal	R R R R	R R R R	G Y G Y	G Y		R
63rd St. W/B Thru Lane Signals	R R R R	R R R R	G Y G Y	G Y		R
Clarendon Hills Road						R

Left Turns permitted on Green Signal but protected only on illuminated Arrow. Phases B and C to have Volume Density. Phase D can not go back to Phases A, B, and C.

PROPOSED SCHEDULE OF QUANTITIES

- 2 EACH LIGHT DETECTOR
- 1 EACH LIGHT DETECTOR AMPLIFIER
- 285 LIN FT EMERG VEH PRIORITY SYST LEAD-IN CABLE IN CONDUIT
- 1 EACH MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION
- 1 EACH LUMP SUM TRAFFIC CONTROL AND PROTECTION
- 1 EACH LUMP SUM MOBILIZATION
- 1 EACH TRAFFIC SIGNAL POST, FERROUS 18'
- 1 EACH RELOCATE EXISTING TRAFFIC SIGNAL

"FOR INFORMATION ONLY"

63rd St. & Clarendon Hills Rd.			
SCALE: None	APPROVED BY:	DRAWN BY PER	
DATE: 1-24-95		REVISED	
Pinner Electric Inc.			
8525 Southview Avenue Brookfield, Illinois 60513			
F.A.U. RTE. 2663	SECTION 15-00022-00-RS	COUNTY DUPAGE	TOTAL SHEETS 26
			SHEET NO. 26
			CONTRACT NO. 61C27
ILLINOIS FED. AID PROJECT			

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CLARENDON HILLS ROAD
TRAFFIC SIGNAL PLANS AND DETAILS

SCALE: N.T.S. SHEET NO. 26 OF 26 SHEETS STA. TO STA.

FILED IN: 150469	USER NAME: jlapaglia	DESIGNED: JAL	REVISED: -
NAWILLOWBROOK\150469\Civil\traf_150469	PLT: slst	DRAWN: EDT	REVISED: -
	PLOT SCALE: NOT TO SCALE	CHECKED: LMF	REVISED: -
	PLOT DATE: 12/15/2015	DATE: \$DATE\$	REVISED: -