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FOR INDEX OF HIGHWAY STANDARDS, SEE SHEET NO. 3

TRAFFIC DATA

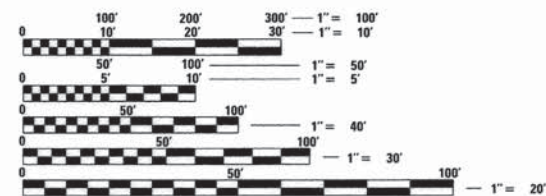
ADT:
FIRST STREET 1,900 VPD (2014)

POSTED SPEED	DESIGN SPEED
25 MPH (EXISTING)	35 MPH (EXISTING)
25 MPH (PROPOSED)	35 MPH (PROPOSED)

FUNCTIONAL CLASSIFICATION

MAJOR COLLECTOR

PROJECT IS LOCATED IN THE CITY OF ELMHURST



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

CONTRACT NO. 61C16

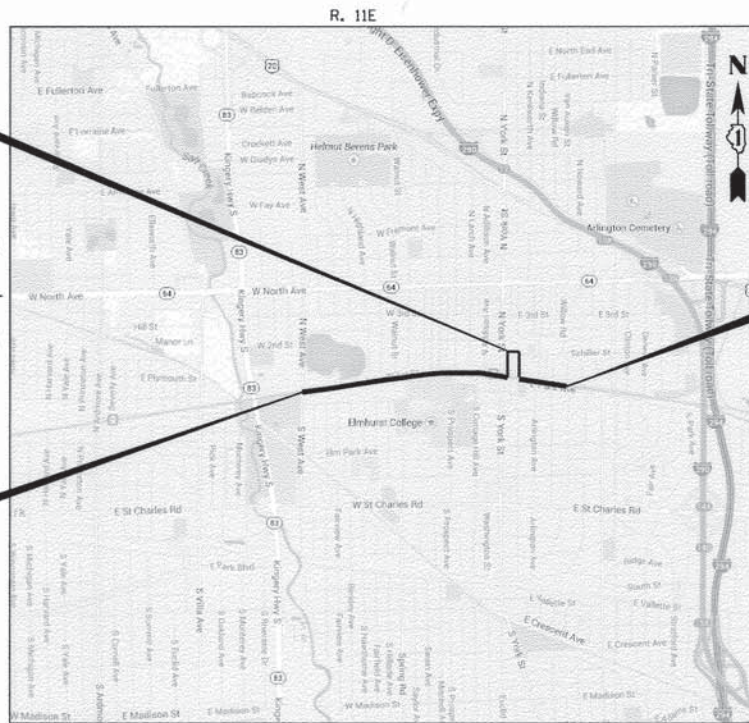
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
**PLANS FOR PROPOSED
FEDERAL AID HIGHWAY PROJECT**

FAU ROUTE 1392 (FIRST STREET)
FAU ROUTE 2659 (WEST AVENUE) TO MUN ROUTE 1300 (WILLOW ROAD)
RESURFACING
SECTION 14-00182-00-RS
PROJECT M-4003(408)
CITY OF ELMHURST
DUPAGE COUNTY
JOB NO. C-91-085-15

PROJECT OMISSION
STATION 257+17 TO
STATION 257+98
S.N. 022-6400

PROJECT BEGINS
STATION 200+41

PROJECT ENDS
STATION 270+75



LOCATION MAP

NOT TO SCALE

PROJECT LENGTH (GROSS / NET)
FIRST STREET 7,034 FT (1.33 MILES)
OMISSION = 81 FT (0.02 MILES)
TOTAL PROJECT LENGTH = 6,953 FT (1.31 MILES) (NET)



Eric Neubaier
ERIC S. NEUBAUER
NO. 062-059188
EXP. DATE 11/30/17

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1392	14-00182-00-RS	DUPAGE	31	1

CONTRACT NO. 61C16



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED ON December 18th, 2015
Kent M. Johnson
KENT M. JOHNSON
CITY OF ELMHURST, CITY ENGINEER

PASSED 12-24, 2015
Conrad
CONRAD
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID
BASED ON LIMITED
REVIEW December 30, 2015
Eric Neubaier
ERIC S. NEUBAUER
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**



GENERAL NOTES

- ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, JANUARY 1, 2012.
- ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT ENGINEER.
- PRIOR TO COMMENCEMENT OF CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS AFFECTING THEIR WORK WITH THE ACTUAL CONDITIONS AT THE JOB SITE. IN ADDITION, THE CONTRACTOR MUST VERIFY THE ENGINEER'S LINE AND GRADE STAKES. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONSTRUCTION PLANS, HE MUST IMMEDIATELY REPORT SAME TO THE ENGINEER BEFORE DOING ANY WORK, OTHERWISE THE CONTRACTOR ASSUMES FULL RESPONSIBILITY. IN THE EVENT OF DISAGREEMENT BETWEEN THE CONSTRUCTION PLANS, STANDARD SPECIFICATIONS AND/OR SPECIAL DETAILS, THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTIONS FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF THE WORK AFFECTED BY OMISSIONS OR DISCREPANCIES. FAILING TO SECURE SUCH INSTRUCTIONS, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT HIS OWN RISK AND EXPENSE IN THE EVENT OF ANY DOUBT OR QUESTION ARISING WITH RESPECT TO THE TRUE MEANING OF THE CONSTRUCTION PLANS OR SPECIFICATIONS, THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS PRIOR TO BIDDING ON THE PROJECT.
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION) AT 8-1-1, KANDICE WITH UNION PACIFIC RAILROAD UTILITIES AT (312) 496-4738, THE FIBER OPTICS HOTLINE AT (800) 336-9193, AND THE CITY OF ELMHURST AT (630) 530-3020 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOUR NOTIFICATION IS REQUIRED).
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE OR CITY PROPERTY OR RIGHT OF WAY WITHOUT WRITTEN PERMISSION FROM THE ENGINEER.
- SAW CUTTING OF PAVEMENTS, SIDEWALK, ETC. SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM BEING REMOVED.
- THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE ENGINEER, HIS AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- OFFSET LOCATIONS GIVEN IN THE PLANS FOR STRUCTURES, EDGE OF PAVEMENT, ETC. ARE FROM THE ROADWAY CENTERLINE.
- HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- QUANTITIES FOR PATCHING SHALL NOT EXCEED THOSE PROVIDED IN THE SUMMARY OF QUANTITIES UNLESS APPROVED BY THE ENGINEER. THE ENGINEER WILL VERIFY FINAL PATCH LOCATIONS IN THE FIELD, PRIOR TO REMOVAL.
- THE CONTRACTOR IS REQUIRED TO USE A PAVER SKI WHEN PLACING BITUMINOUS LIFTS.
- THE CONTRACTOR SHALL COORDINATE PAVING OPERATIONS FOR BOTH HMA LEVELING BINDER AND SURFACE COURSES SO THAT THE LONGITUDINAL JOINTS ARE CLOSED AND COMPACTED AT THE END OF EACH DAY. PAVING OPERATIONS SHALL BE SCHEDULED SO THAT ADJACENT LANES ARE PAVED IN THE SAME DIRECTION AS THE INITIAL LANE MINIMIZING THE TIME THE EDGE OF A PAVEMENT MAT IS ALLOWED TO COOL.
- THE CONTRACTOR SHALL USE 2 CHANGEABLE MESSAGE SIGNS AT LOCATIONS TO BE DETERMINED BY THE ENGINEER FOR A PERIOD FROM ONE WEEK PRIOR TO THE START OF CONSTRUCTION TO THE CONCLUSION OF THE PROJECT.
- THE CONTRACTOR'S ATTENTION IS CALLED TO THE FACT THAT THE PRESERVATION OF EXISTING TREES IS OF UTMOST IMPORTANCE TO THE CITY OF ELMHURST. ALL TREE PROTECTION, PRUNING AND ROOT PRUNING SHALL BE COMPLETED BEFORE CONSTRUCTION OPERATIONS COMMENCE IN ANY AREA. AT NO TIME SHALL THE CONTRACTOR REMOVE OR PRUNE ANY TREES UNLESS SPECIFICALLY DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL ERECT A TEMPORARY FENCE AROUND ALL TREES WITHIN THE CONSTRUCTION AREA TO ESTABLISH A "TREE PROTECTION ZONE" BEFORE ANY WORK BEGINS OR ANY MATERIAL IS DELIVERED TO THE JOBSITE. NO WORK IS TO BE PERFORMED, MATERIALS STORED, OR VEHICLES DRIVEN OR PARKED IN THE "TREE PROTECTION ZONE". REMOVE PROTECTIVE TEMPORARY FENCE ONLY AFTER ALL CONSTRUCTION WORK HAS BEEN COMPLETED.
- ALL FLEXIBLE DELINEATORS WILL BE REMOVED AND REINSTALLED BY THE CITY OF ELMHURST.
- COORDINATE CONSTRUCTION WITH PARKING ADJACENT TO FIRST STREET.

UTILITIES NOTES

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MIGHT NOT BE SHOWN ON THE PLANS. ANY UTILITY PROPERTY DAMAGE DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER IN ACCORDANCE WITH ARTICLES 105.07 AND 107.20.
- ALL UTILITY OWNERS SHALL BE NOTIFIED AT LEAST 3 DAYS PRIOR TO THE START OF CONSTRUCTION.
- THE CONTRACTOR SHALL USE ALL NECESSARY PRECAUTIONS AND PROTECTION MEASURES REQUIRED TO MAINTAIN EXISTING UTILITIES, SEWERS, AND APPURTENANCES THAT MUST BE KEPT IN OPERATION.
- THE CONTRACTOR SHALL ENSURE THAT ALL WATER SYSTEM VALVES, VALVE VAULTS, FIRE HYDRANTS, AND SANITARY SEWER MANHOLES REMAIN READILY ACCESSIBLE TO THE CITY FOR EMERGENCY OPERATIONS. THE LOCATIONS OF ALL WATER AND SANITARY FACILITIES SHALL BE MARKED AND READILY VISIBLE AT ALL TIMES.
- ALL LOOSE MATERIAL DEPOSITED IN THE FLOWLINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE COST OF INLET FILTERS.
- THE EXISTING FRAMES AND LIDS SHALL REMAIN AS PROPERTY OF THE CITY OF ELMHURST. ALL OLD FRAMES AND LIDS NOT BEING REUSED SHALL BE REMOVED FROM PARKWAYS BY THE CONTRACTOR, DELIVERED TO AND STOCKPILED AT THE CITY MUNICIPAL SERVICE FACILITY WITHIN SEVEN (7) DAYS OF THEIR REMOVAL. THE UTILITY DEPARTMENT YARD IS LOCATED AT THE NORTH END OF THE WASTE WATER TREATMENT PLANT FACILITY, 625 SOUTH ROUTE 83. PHONE NUMBER IS (630) 530-3020.
- CATCH BASINS, MANHOLES AND INLETS CONSTRUCTED IN A LOCATION WHERE AN EXISTING STRUCTURE WAS REMOVED SHALL INCLUDE UP TO FIVE FEET OF PIPE TO CONNECT EACH EXISTING PIPE. THE NECESSARY PIPE BEYOND FIVE FEET WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR "STORM SEWER" AND OF THE TYPE AND SIZE REQUIRED.
- THE CONTRACTOR SHALL CONFIRM ALL EXISTING STORM SEWER PIPE SIZES AND INVERTS PRIOR TO ORDERING STRUCTURES. ANY MODIFICATION OF STRUCTURES DUE TO THE FAILURE OF THE CONTRACTOR TO PERFORM THIS TASK SHALL BE AT THE CONTRACTOR'S EXPENSE AND MAY LEAD TO THE REJECTION OF THE STRUCTURE IN THE FIELD.

GENERAL NOTES (CONT'D)

SIGNING AND STRIPING

- SEE IDOT STANDARD DETAIL 780001, DISTRICT ONE DETAIL TC-13 AND PLAN SHEETS FOR PAVEMENT MARKING DETAILS.
- THE CONTRACTOR WILL BE REQUIRED TO TEMPORARILY RESET ALL SUCH SIGNS THAT INTERFERE WITH CONSTRUCTION OPERATIONS. ALL SUCH SIGNS MUST BE MAINTAINED STRAIGHT AND CLEAN FOR THE DURATION OF THE TEMPORARY SETTING AND MUST BE RE-ERECTED AT A TEMPORARY LOCATION AND BE VISIBLE TO THE TRAFFIC FOR WHICH IT IS INTENDED. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT IN ACCORDANCE WITH ARTICLE 107.25.

TRAFFIC CONTROL

- SEE TRAFFIC CONTROL HIGHWAY STANDARDS CONCERNING TRAFFIC CONTROL AND PROTECTION.
- THE CONTRACTOR SHALL SCHEDULE CONSTRUCTION ACTIVITIES SO THAT THERE ARE ALWAYS TWO LANES OF TRAFFIC OPEN AT THE END OF EACH DAY. DETOURS WILL ONLY BE ALLOWED UPON SUBMITTAL OF A DETOUR PLAN AND APPROVED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE TRAFFIC CONTROL STANDARD PAY ITEM.
- "ROAD CONSTRUCTION AHEAD" SIGNS SHALL BE POSTED ON ALL SIDE STREETS FROM BOTH DIRECTIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE TRAFFIC CONTROL AND PROTECTION.

STORM SEWERS, WATER MAINS, AND UTILITIES

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF ANY UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.
- THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR. THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE COST OF MOBILIZATION, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN IF NOT SHOWN ON THE PLANS. WHEN THE PLANS OR SPECIAL PROVISIONS INCLUDE INFORMATION PERTAINING TO THE LOCATION OF UNDERGROUND UTILITY FACILITIES, SUCH INFORMATION REPRESENTS ONLY THE OPINION OF THE CITY OF ELMHURST AS TO THE LOCATION OF SUCH UTILITIES AND IS ONLY INCLUDED FOR THE CONVENIENCE OF THE BIDDER. ALL UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER AT THE CONTRACTOR'S EXPENSE.
- ALL UTILITY OWNERS AND THE ENGINEER SHALL BE NOTIFIED AT LEAST 3 DAYS PRIOR TO THE START OF CONSTRUCTION.
- THE CONTRACTOR SHALL USE ALL NECESSARY PRECAUTIONS AND PROTECTION MEASURES REQUIRED TO MAINTAIN EXISTING UTILITIES, SEWERS, AND APPURTENANCES THAT MUST BE KEPT IN OPERATION.
- FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) SHALL INCLUDE REPLACEMENT OF EXISTING BROKEN ADJUSTMENT RINGS AND PATCHING INSIDE THE STRUCTURES BETWEEN PIPES AND STRUCTURES WITH HYDRAULIC CEMENT AT LOCATIONS AS DIRECTED BY THE ENGINEER. IF THE STRUCTURE IS A COMBINATION SEWER OR SANITARY MANHOLE THEN CHIMNEY SEALS SHALL BE PROVIDED AND INCLUDED IN THE COST OF FRAMES AND LIDS TO BE ADJUSTED (SPECIAL).
- ALL DRAINAGE STRUCTURE ADJUSTMENTS AND FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) SHALL USE PCC. HMA WILL NOT BE ALLOWED. EACH JOINT SHALL BE SEALED ACCORDING TO THE MANUFACTURER'S SPECIFICATIONS AS DIRECTED PER ARTICLE 602.02.
- THE CONTRACTOR SHALL ENSURE ALL WATER SYSTEM VALVES, VALVE VAULTS, AND SANITARY SEWER MANHOLES REMAIN READILY ACCESSIBLE FOR EMERGENCY OPERATIONS. THE LOCATIONS OF ALL WATER AND SANITARY FACILITIES SHALL BE MARKED AND READILY VISIBLE AT ALL TIMES.

FILE NAME =	USER NAME =	DESIGNED =	REVISED =
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		DRAWN =	REVISED =
		AJP	-
		CHECKED =	REVISED =
		ESN	-
		DATE =	REVISED =
		12/17/2015	-

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FIRST STREET RESURFACING
WEST AVENUE TO WILLOW ROAD
GENERAL NOTES AND HIGHWAY STANDARDS**

SCALE: NOT TO SCALE SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1392	14-00182-00-RS	DUPAGE	31	2
CONTRACT NO. 61C16				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

GENERAL NOTES (CONT'D)

MISCELLANEOUS

1. MATERIALS RESULTING FROM THE REMOVAL OF CONCRETE SURFACES, UTILITY STRUCTURE ADJUSTMENT, RESTORATION WORK, ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IF THE CONTRACTOR DOES NOT REMOVE THESE MATERIALS AT THE REQUEST OF THE ENGINEER, THE CITY OF ELMHURST WILL HIRE A CONTRACTOR TO HAVE THE MATERIAL REMOVED AND THE CONTRACTOR SHALL BE BILLED (CHARGED) ACCORDINGLY.
2. THE INDISCRIMINATE USE OF FIRE HYDRANTS, EXISTING STREAMS, CREEKS, WETLANDS, OR PONDS IS STRICTLY PROHIBITED. THE CONTRACTOR SHALL PROVIDE A WATER TRUCK AND DRIVER AS REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING WATER FROM AN APPROVED SOURCE. IF THIS WATER IS FROM A SOURCE OTHER THAN HIS/HER YARD, WRITTEN APPROVAL FROM THE AGENCY HAVING JURISDICTION FOR THE SOURCE OF THE WATER MUST BE RECEIVED BY THE CONTRACTOR PRIOR TO USE OF THE WATER.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SWEEPING AND CLEANING STREETS OF ANY DEBRIS AND MATERIAL THAT HAS ACCUMULATED AS A RESULT OF THE CONSTRUCTION ACTIVITY. A MECHANICAL SWEEPER, MECHANICALLY DRIVEN AIR AND HANDWORK WITH SHOVEL AND BROOM SHALL BE UTILIZED TO PROVIDE A CLEAN STREET FOR THE MOTORING PUBLIC. WITHIN 24 HOURS OF PLACING PRIME COAT AND THE LAYING OF HMA, THE CONTRACTOR SHALL SWEEP THE PAVEMENT AND REMOVE STANDING WATER, EARTH, WEEDS, LEAVES, DIRT, CONSTRUCTION DEBRIS AND ALL LOOSE MATERIAL.
4. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS, COMMERCIAL PROPERTY OWNERS, AND THE ENGINEER WHEN ACCESS TO THEIR DRIVEWAYS WILL BE ALTERED DUE TO SIDEWALK, DRIVEWAY, AND/OR CURB AND GUTTER REPLACEMENT. AT LOCATIONS WHERE THE SIDEWALK, DRIVEWAY, AND/OR CURB AND GUTTER IS SCHEDULED TO BE REMOVED, THE CONTRACTOR SHALL CONTACT THE PROPERTY OWNER 24 HOURS PRIOR TO THEIR REMOVAL. THESE ITEMS SHALL BE REMOVED AND RECONSTRUCTED HALF AT A TIME SUCH THAT THERE ARE NO DRIVEWAY CLOSURES.
5. IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO COORDINATE WITH THE UNION PACIFIC RAILROAD WHENEVER CONSTRUCTION ACTIVITY IS WITHIN 25 FEET OF THE RAILROAD ROW. THE CONTRACTOR SHALL RETAIN FLAGMEN EMPLOYED AND DESIGNATED BY THE UNION PACIFIC RAILROAD TO MONITOR ON-COMING TRAIN TRAFFIC, AND ADVISE CONTRACTOR PERSONNEL WHEN ACTIVITY ON OR NEAR THE RAILROAD RIGHT OF WAY MAY PROCEED. THIS ITEM WILL BE PAID FOR ACCORDING TO ARTICLE 107.12 AND WILL BE REIMBURSED ACCORDING TO ARTICLE 109.05.
6. WHEN REMOVING PAVEMENT, CURB AND GUTTER, SHOULDER, AND/OR ANY OTHER STRUCTURES, THE USE OF ANY TYPE OF CONCRETE BREAKERS WHICH MIGHT DAMAGE UNDERGROUND PUBLIC OR PRIVATE UTILITIES AND BUILDING FOUNDATIONS WILL NOT BE PERMITTED. UNDER NO CIRCUMSTANCES WILL THE USE OF A FROST BALL BE PERMITTED.
7. INLET FILTERS SHALL BE USED ON ALL OPEN GRATE DRAINAGE STRUCTURES WITHIN THE PROJECT LIMITS. THE QUANTITIES IN THE PLANS REFLECTS THIS.
8. ANY PAVER BRICKS REMOVED WHEN CONNECTING TO EXISTING STREETSCAPED AREAS AND NOT REUSED AT THE IMMEDIATE LOCATION SHALL REMAIN THE PROPERTY OF THE CITY OF ELMHURST. THE CONTRACTOR SHALL CRATE AND DELIVER THE PAVER BRICKS TO THE WASTE WATER TREATMENT PLANT FACILITY, 625 SOUTH ROUTE 83. THE COST WILL BE INCLUDED IN THE COST OF BRICK PAVER REMOVAL.

STD. NO.
 000001-06
 424001-08
 424006-02
 424011-02
 424016-02
 442201-03
 602301-04
 602401-03
 602701-02
 604001-04
 604056-04
 606001-06
 701301-04
 701311-03
 701501-06
 701601-09
 701701-09
 701801-05
 701901-04
 780001-05
 886001-01
 886006-01

HIGHWAY STANDARDS
DESCRIPTION

STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
 PERPENDICULAR CURB RAMPS FOR SIDEWALKS
 DIAGONAL CURB RAMPS FOR SIDEWALKS
 CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
 MID-BLOCK CURB RAMPS FOR SIDEWALKS
 CLASS C AND D PATCHES
 INLET - TYPE A
 MANHOLE TYPE A
 MANHOLE STEPS
 FRAME AND LIDS TYPE 1
 FRAME AND GRATE TYPE 11V
 CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
 LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
 LANE CLOSURE, 2L, 2W, MOVING OPERATIONS-DAY ONLY
 URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
 URBAN LANE CLOSURE MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
 URBAN LANE CLOSURE, MULTILANE INTERSECTION
 SIDEWALK, CORNER, OR CROSSWALK CLOSURE
 TRAFFIC CONTROL DEVICES
 TYPICAL PAVEMENT MARKINGS
 DETECTOR LOOP INSTALLATIONS
 TYPICAL LAYOUTS FOR DETECTION LOOPS

FILE NAME = G:\CHP\BIBS\Road\Sheets\G-1R2-GenNotes.dwg	USER NAME = ESN\ehs@es.com	DESIGNED - AS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FIRST STREET RESURFACING WEST AVENUE TO WILLOW ROAD GENERAL NOTES AND HIGHWAY STANDARDS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE =	CHECKED - ESN	REVISED -	1392			14-00182-00-RS	DUPAGE	31	3	
PLOT DATE = 12/18/2015	DATE - 12/17/2015	REVISED -	CONTRACT NO. 61C16							
						SCALE: NOT TO SCALE	SHEET NO. 2 OF 2 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

Item Number	Items	Unit	Quantity	0005 ROADWAY 50% STP/50% LA
20101100	TREE TRUNK PROTECTION	EACH	12	12
20101200	TREE ROOT PRUNING	EACH	2	2
20101300	TREE PRUNING (1 TO 10 INCH DIAMETER)	EACH	2	2
20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	1	1
20200100	EARTH EXCAVATION	CU YD	39	39
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	20	20
21101625	TOPSOIL FURNISH AND PLACE, 6"	SQ YD	281	281
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	0.39	0.39
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	0.39	0.39
25200110	SODDING, SALT TOLERANT	SQ YD	2,093	2,093
25200200	SUPPLEMENTAL WATERING	UNIT	6	6
28000510	INLET FILTERS	EACH	110	110
30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	20	20
30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	1,551	1,551
35800100	PREPARATION OF BASE	SQ YD	100	100
31101200	SUBBASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	1,784	1,784
40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	1,499	1,499
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGWAYS	TON	36	36
40600825	POLYMERIZED LEVELING BINDER (MACHINE METHOD),IL-4.75, N50	TON	958	958
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	246	246
40600990	TEMPORARY RAMP	SQ YD	246	246
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	1,917	1,917
42000500	PORTLAND CEMENT CONCRETE PAVEMENT 10"	SQ YD	1,551	1,551
42001300	PROTECTIVE COAT	SQ YD	2,903	2,903
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	1,437	1,437
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	4,176	4,176
42400800	DETECTABLE WARNINGS	SQ FT	425	425
44000160	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"	SQ YD	21,341	21,341
44000100	PAVEMENT REMOVAL	SQ YD	1,551	1,551
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	121	121
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	3,397	3,397
44000600	SIDEWALK REMOVAL	SQ FT	3,194	3,194
44201749	CLASS D PATCHES, TYPE I, 9 INCH	SQ YD	244	244
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	274	274
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	257	257
44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SQ YD	1,264	1,264
60236825	INLETS, TYPE A, TYPE 11V FRAME AND GRATE	EACH	2	2
60266600	VALVE BOXES TO BE ADJUSTED	EACH	1	1

Item Number	Items	Unit	Quantity	0005 ROADWAY 50% STP/50% LA
60404805	FRAMES AND GRATES, TYPE 11V	EACH	2	2
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	1	1
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	2,566	2,566
60608300	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12	FOOT	914	914
67100100	MOBILIZATION	LSUM	1	1
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	10	10
* 66900205	SPECIAL WASTE DISPOSAL	CU YD	10	10
* 66900450	SPECIAL WASTE PLANS AND REPORTS	LSUM	1	1
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	LSUM	1	1
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	LSUM	1	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	LSUM	1	1
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	12	12
70300100	SHORT TERM PAVEMENT MARKING	FOOT	100	100
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	60	60
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	75	75
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	155	155
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	76	76
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	205	205
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	50	50
* 78005100	EPOXY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	82	82
* 78005110	EPOXY PAVEMENT MARKING - LINE 4"	FOOT	1,904	1,904
* 78005130	EPOXY PAVEMENT MARKING - LINE 6"	FOOT	2,188	2,188
* 78005140	EPOXY PAVEMENT MARKING - LINE 8"	FOOT	290	290
* 78005150	EPOXY PAVEMENT MARKING - LINE 12"	FOOT	293	293
* 78005180	EPOXY PAVEMENT MARKING - LINE 24"	FOOT	365	365
* 78300100	PAVEMENT MARKING REMOVAL	SQ FT	2,074	2,074
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	379	379
X0326144	TACTILE/DETECTABLE WARNING SURFACE	SQ FT	137	137
X4403300	CONCRETE MEDIAN REMOVAL	SQ FT	653	653
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	LSUM	1	1

* SPECIALTY ITEM

FILE NAME = C:\CH\1005\Road\Sheets\10-103-Guaranteed.k

USER NAME = mawcdoald
 PLOT SCALE = 1/8"=1'-0"
 PLOT DATE = 12/22/2015

DESIGNED - AS
 DRAWN - AJP
 CHECKED - ESN
 DATE - 12/17/2015

REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

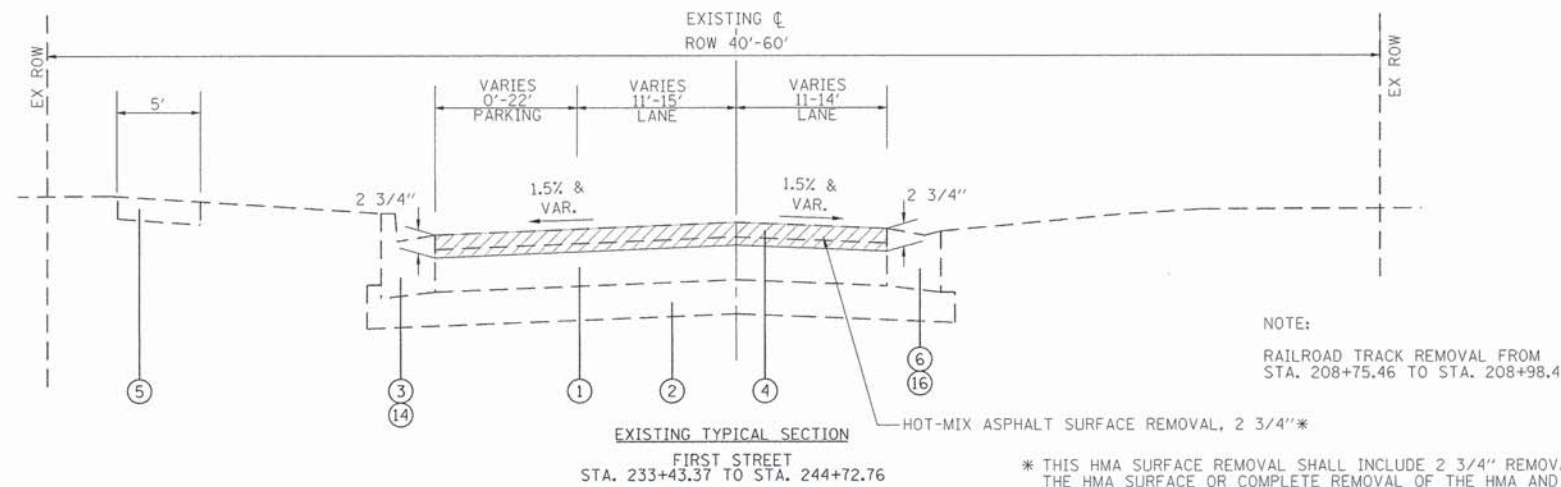
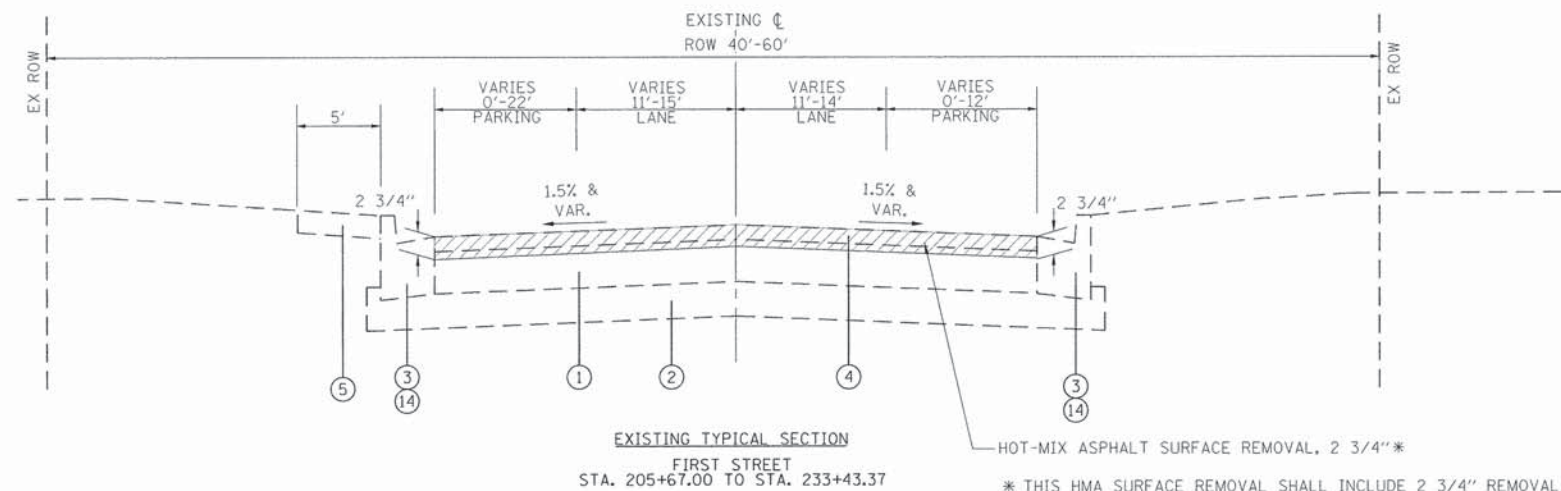
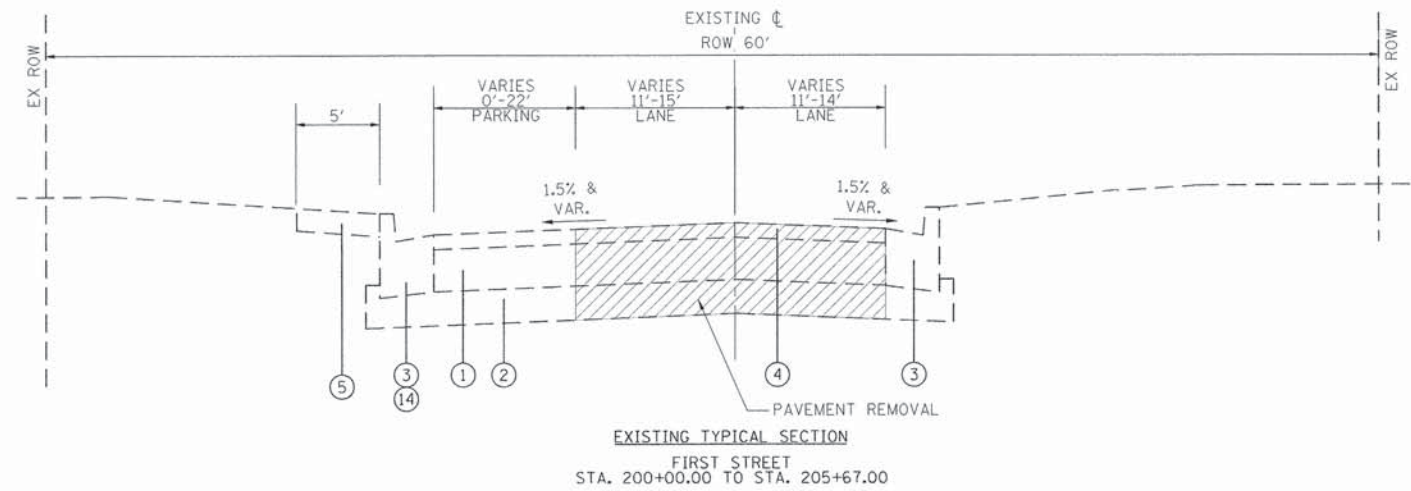
FIRST STREET RESURFACING
 WEST AVENUE TO WILLOW ROAD
 SUMMARY OF QUANTITIES

SCALE: NOT TO SCALE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RATE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1392	14-00182-00-RS	DUPAGE	31	4
CONTRACT NO. 61C16				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

Item Number	Items	Unit	Quantity	0005 ROADWAY 5% STP. LA
* X7830068	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS	SQ FT	60	60
* X7830070	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT	1,904	1904
* X7830074	GROOVING FOR RECESSED PAVEMENT MARKING 7"	FOOT	2,188	2188
* X7830078	GROOVING FOR RECESSED PAVEMENT MARKING 13"	FOOT	293	293
* X7830090	GROOVING FOR RECESSED PAVEMENT MARKING 25"	FOOT	355	355
* X8140115	HANDHOLE TO BE ADJUSTED	EACH	6	6
XX000445	SAWCUT AND SEAL NEW JOINTS	FOOT	121	121
XX001621	BRICK PAVER REMOVAL	SQ FT	812	812
XX004688	BRICK PAVER SIDEWALK	SQ FT	694	694
Z0013798	CONSTRUCTION LAYOUT	LSUM	1	1
Z0018400	DRAINAGE STRUCTURES TO BE ADJUSTED	EACH	97	97
Z0018600	DRAINAGE STRUCTURES TO BE RECONSTRUCTED	EACH	5	5
* Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	89	89
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	LSUM	1	1
Z0048900	RAILROAD TRACK REMOVAL	FOOT	195	195
Z0038224	PORTLAND CEMENT CONCRETE SURFACE REMOVAL 3"	SQ YD	1,484	1484

* SPECIALTY ITEM



LEGEND

- ① EXISTING PCC PAVEMENT, 6"
- ② EXISTING SUBBASE GRANULAR MATERIAL, CA-6, 4"
- ③ EXISTING COMBINATION CONCRETE CURB & GUTTER TYPE B-6.12
- ④ EXISTING HMA PAVEMENT, VARIABLE DEPTH
- ⑤ EXISTING PCC SIDEWALK, 5"
- ⑥ EXISTING COMBINATION CONCRETE CURB & GUTTER TYPE M-2.12
- ⑦ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"
- ⑧ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- ⑨ CLASS D PATCHES, 9" (AS SHOWN ON THE PLANS AND AS DIRECTED BY THE ENGINEER)
- ⑩ REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
AGGREGATE SUBGRADE IMPROVEMENT (AS DIRECTED BY ENGINEER)
- ⑪ PROPOSED PCC SIDEWALK, 5" (AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
- ⑫ AGGREGATE SUBGRADE IMPROVEMENT, 12"
- ⑬ SODDING, SALT TOLERANT
TOPSOIL FURNISH AND PLACE, 6"
- ⑭ COMBINATION CONCRETE CURB AND GUTTER REMOVAL
COMBINATION CONCRETE CURB AND GUTTER TYPE B-6.12
(AS SHOWN ON THE PLANS AND AS DIRECTED BY THE ENGINEER)
- ⑮ SUBBASE GRANULAR MATERIAL, TYPE B 4"
(AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
- ⑯ COMBINATION CONCRETE CURB AND GUTTER REMOVAL
COMBINATION CONCRETE CURB AND GUTTER TYPE M-2.12
- ⑰ CONCRETE MEDIAN REMOVAL
- ⑱ PORTLAND CEMENT CONCRETE PAVEMENT 10"

NOTES:

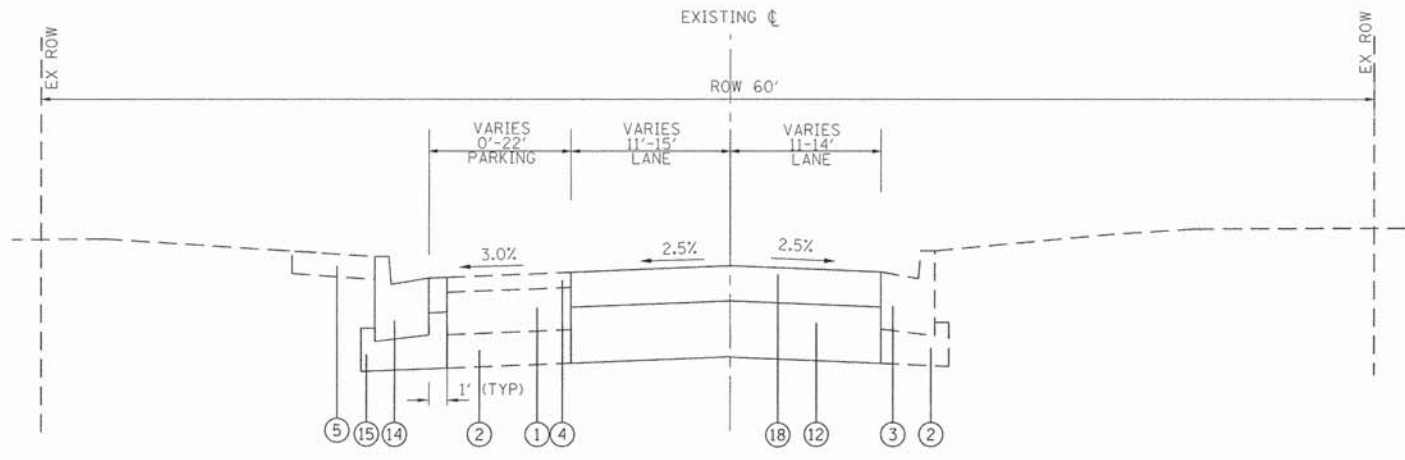
- 1. ALL PROPOSED ROADWAY CROSS SLOPES ARE ADA COMPLIANT.

* THIS HMA SURFACE REMOVAL SHALL INCLUDE 2 3/4" REMOVAL OF THE HMA SURFACE OR COMPLETE REMOVAL OF THE HMA AND REMOVAL OF THE PCC PAVEMENT SURFACE BELOW TO THE DEPTHS AS SHOWN IN THE TYPICAL SECTION ABOVE.

NOTE:
RAILROAD TRACK REMOVAL FROM
STA. 208+75.46 TO STA. 208+98.46

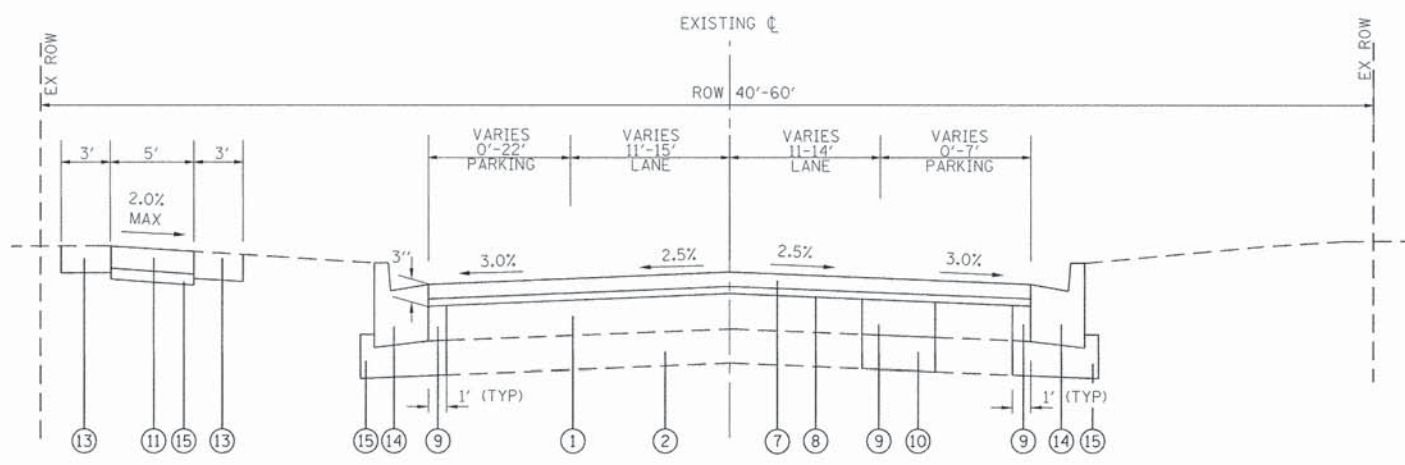
* THIS HMA SURFACE REMOVAL SHALL INCLUDE 2 3/4" REMOVAL OF THE HMA SURFACE OR COMPLETE REMOVAL OF THE HMA AND REMOVAL OF THE PCC PAVEMENT SURFACE BELOW TO THE DEPTHS AS SHOWN IN THE TYPICAL SECTION ABOVE.

FILE NAME =	USER NAME = mamadonald	DESIGNED =	REVISED =	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FIRST STREET RESURFACING WEST AVENUE TO WILLOW ROAD TYPICAL SECTIONS	F.A.U. RTE. =	SECTION =	COUNTY =	TOTAL SHEETS =	SHEET NO. =
EG:\CHP\18165\Road\Sheets\18165-Typ Sects.SHT		DRAWN =	REVISED =			1392	14-00182-00-RS	DUPAGE	31	6
PLOT SCALE =		CHECKED =	REVISED =			CONTRACT NO. 61C16				
PLOT DATE = 12/22/2015		DATE =	REVISED =			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
					NOT TO SCALE SHEET NO. OF SHEETS STA. TO STA.					



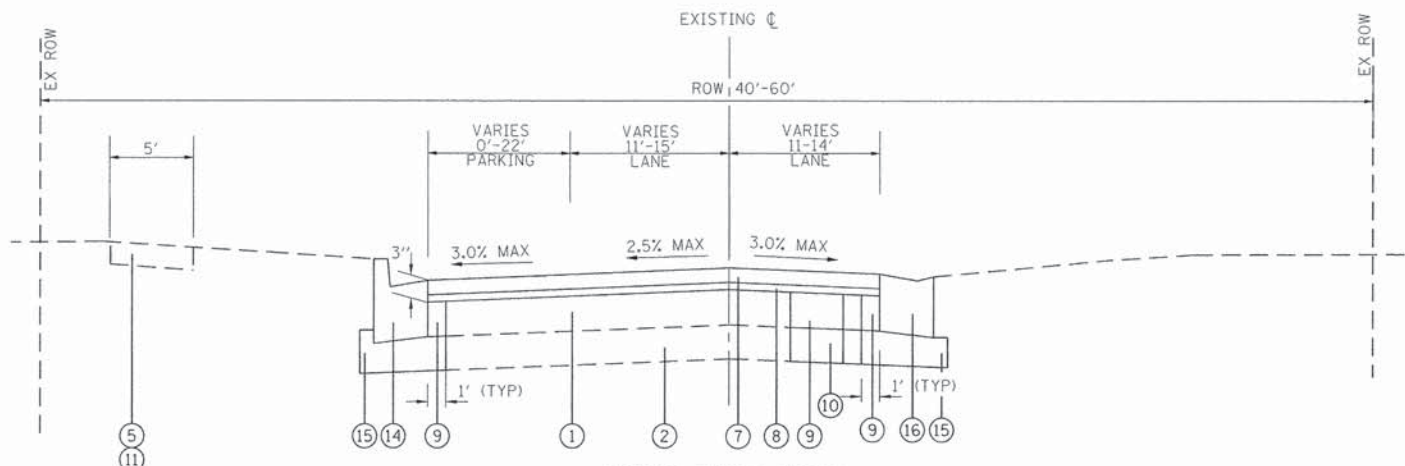
PROPOSED TYPICAL SECTION
FIRST STREET
STA. 200+00.00 TO STA. 205+67.00

NOTE: HMA SURFACE COURSE SHALL BE 1/4" ABOVE THE EDGE OF PAVEMENT.



PROPOSED TYPICAL SECTION
FIRST STREET
STA. 205+67.00 TO STA. 233+43.37

NOTE: HMA SURFACE COURSE SHALL BE 1/4" ABOVE THE EDGE OF PAVEMENT.



PROPOSED TYPICAL SECTION
FIRST STREET
STA. 233+43.37 TO STA. 244+72.76

NOTE: HMA SURFACE COURSE SHALL BE 1/4" ABOVE THE EDGE OF PAVEMENT.

LEGEND

- ① EXISTING PCC PAVEMENT, 6"
- ② EXISTING SUBBASE GRANULAR MATERIAL, CA-6, 4"
- ③ EXISTING COMBINATION CONCRETE CURB & GUTTER TYPE B-6.12
- ④ EXISTING HMA PAVEMENT, VARIABLE DEPTH
- ⑤ EXISTING PCC SIDEWALK, 5"
- ⑥ EXISTING COMBINATION CONCRETE CURB & GUTTER TYPE M-2.12
- ⑦ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"
- ⑧ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- ⑨ CLASS D PATCHES, 9" (AS SHOWN ON THE PLANS AND AS DIRECTED BY THE ENGINEER)
- ⑩ REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL AGGREGATE SUBGRADE IMPROVEMENT (AS DIRECTED BY ENGINEER)
- ⑪ PROPOSED PCC SIDEWALK, 5" (AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
- ⑫ AGGREGATE SUBGRADE IMPROVEMENT, 12"
- ⑬ SODDING, SALT TOLERANT TOPSOIL FURNISH AND PLACE, 6"
- ⑭ COMBINATION CONCRETE CURB AND GUTTER REMOVAL COMBINATION CONCRETE CURB AND GUTTER TYPE B-6.12 (AS SHOWN ON THE PLANS AND AS DIRECTED BY THE ENGINEER)
- ⑮ SUBBASE GRANULAR MATERIAL, TYPE B 4" (AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
- ⑯ COMBINATION CONCRETE CURB AND GUTTER REMOVAL COMBINATION CONCRETE CURB AND GUTTER TYPE M-2.12
- ⑰ CONCRETE MEDIAN REMOVAL
- ⑱ PORTLAND CEMENT CONCRETE PAVEMENT 10"

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

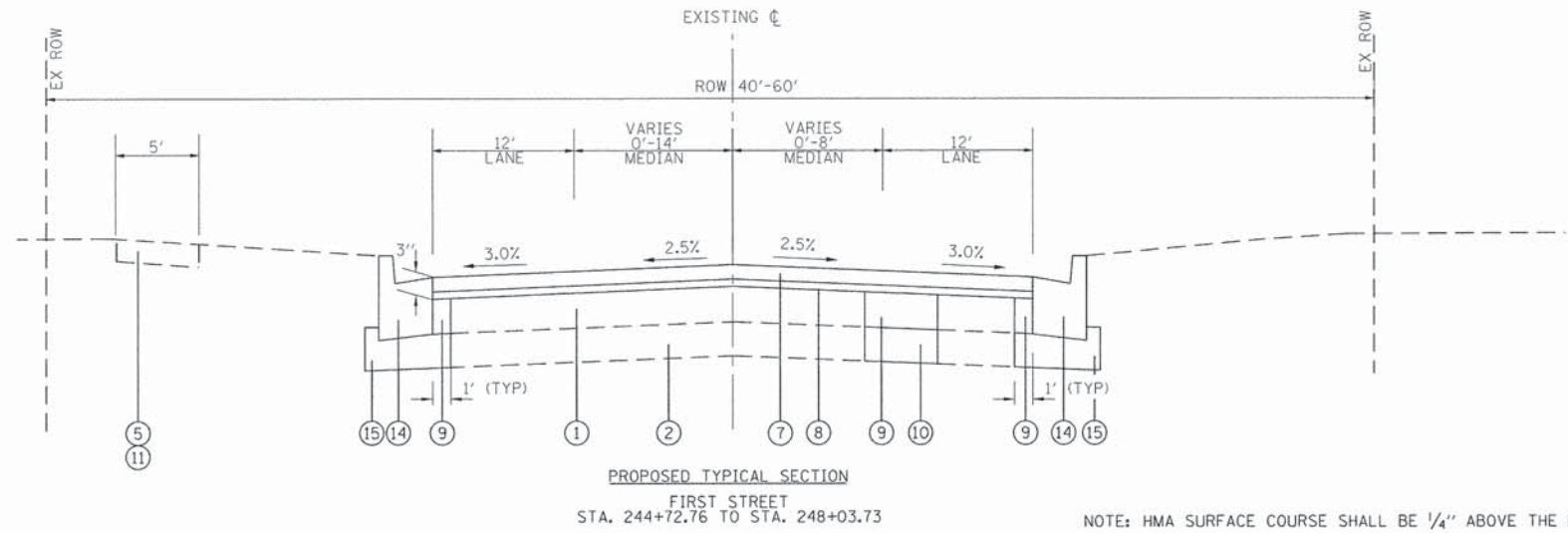
THE CONTRACTOR SHALL MILL BEFORE PATCHING.

MIXTURE TYPE	AIR VOIDS @ Ndes
HMA SURFACE COURSE, MIX "D", N50 (IL 9.5mm), 2"	4% @ 50 GYRATIONS
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"	3.5% @ 50 GYRATIONS
CLASS D PATCHES (HMA BINDER IL-19 mm), 9" (IN 3 LIFTS)	4% @ 70 GYRATIONS
HOT-MIX ASPHALT DRIVEWAYS, 8"	
HMA SURFACE COURSE, MIX "D", N50 (IL-9.5mm) 2"	4% @ 50 GYRATIONS
HMA BASE COURSE (HMA BINDER IL-19 mm), 6" (IN 2 LIFTS)	4% @ 50 GYRATIONS

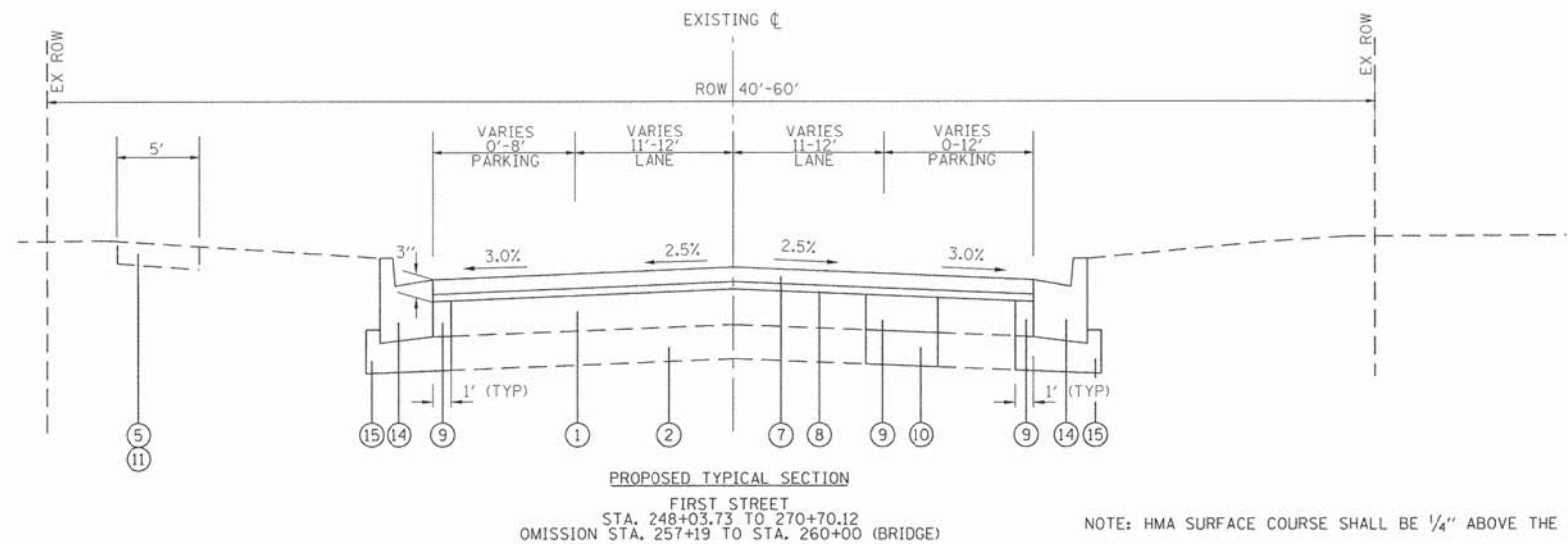
- NOTES:
- 1) THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
 - 2) THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.
 - 3) THE CONTRACTOR SHALL MILL BEFORE PATCHING.

LEGEND

- ① EXISTING PCC PAVEMENT, 6"
- ② EXISTING SUBBASE GRANULAR MATERIAL, CA-6, 4"
- ③ EXISTING COMBINATION CONCRETE CURB & GUTTER TYPE B-6.12
- ④ EXISTING HMA PAVEMENT, VARIABLE DEPTH
- ⑤ EXISTING PCC SIDEWALK, 5"
- ⑥ EXISTING COMBINATION CONCRETE CURB & GUTTER TYPE M-2.12
- ⑦ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"
- ⑧ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- ⑨ CLASS D PATCHES, 9" (AS SHOWN ON THE PLANS AND AS DIRECTED BY THE ENGINEER)
- ⑩ REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
AGGREGATE SUBGRADE IMPROVEMENT (AS DIRECTED BY ENGINEER)
- ⑪ PROPOSED PCC SIDEWALK, 5" (AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
- ⑫ AGGREGATE SUBGRADE IMPROVEMENT, 12"
- ⑬ SODDING, SALT TOLERANT
TOPSOIL FURNISH AND PLACE, 6"
- ⑭ COMBINATION CONCRETE CURB AND GUTTER REMOVAL
COMBINATION CONCRETE CURB AND GUTTER TYPE B-6.12
(AS SHOWN ON THE PLANS AND AS DIRECTED BY THE ENGINEER)
- ⑮ SUBBASE GRANULAR MATERIAL, TYPE B 4"
(AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
- ⑯ COMBINATION CONCRETE CURB AND GUTTER REMOVAL
COMBINATION CONCRETE CURB AND GUTTER TYPE M-2.12
- ⑰ CONCRETE MEDIAN REMOVAL
- ⑱ PORTLAND CEMENT CONCRETE PAVEMENT 10"

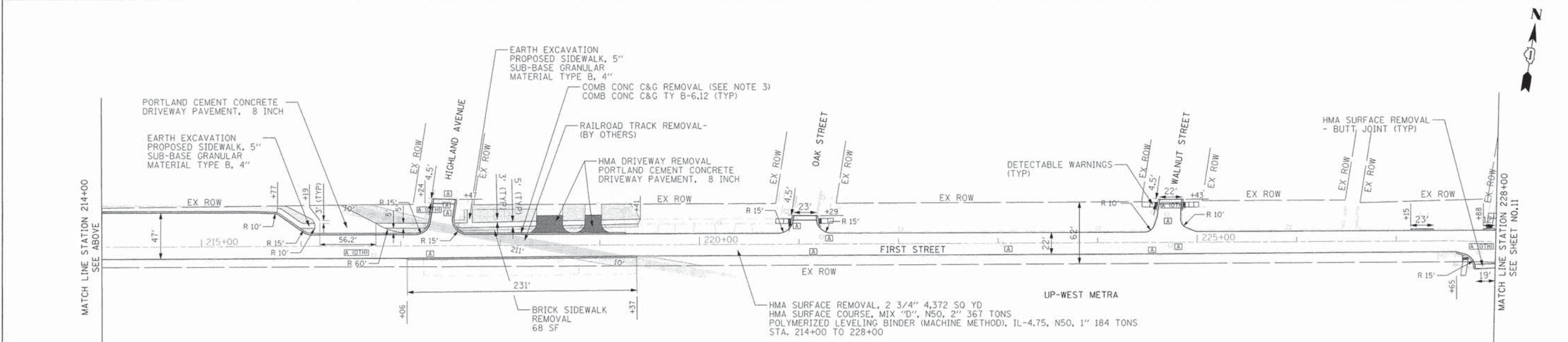
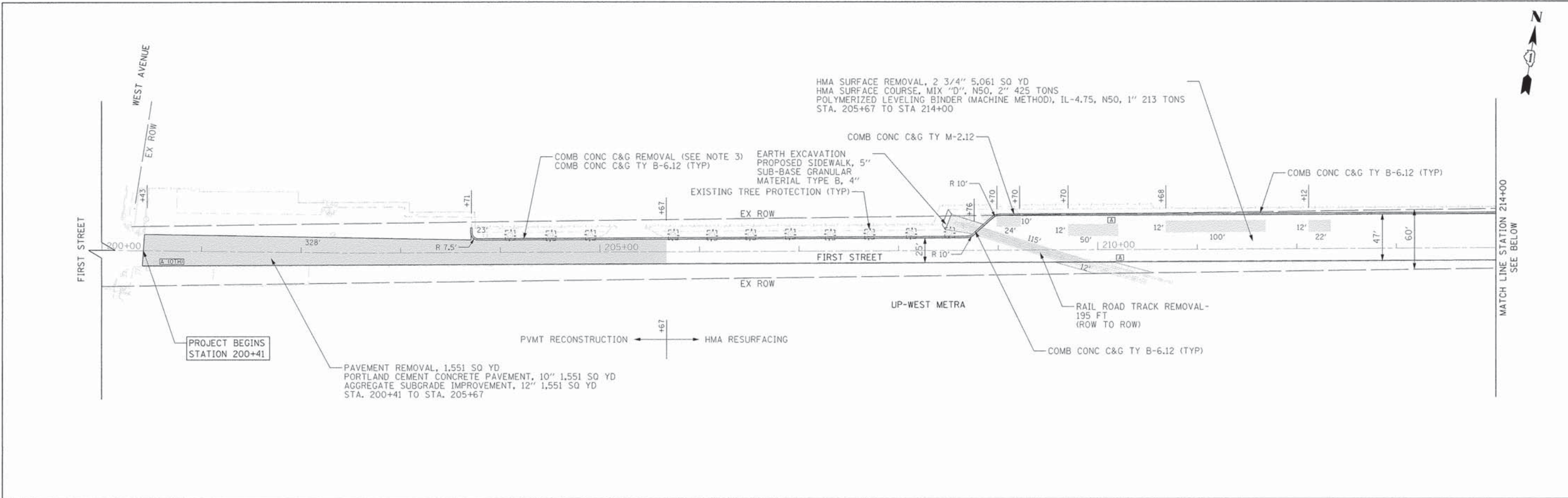


NOTE: HMA SURFACE COURSE SHALL BE 1/4" ABOVE THE EDGE OF PAVEMENT.



NOTE: HMA SURFACE COURSE SHALL BE 1/4" ABOVE THE EDGE OF PAVEMENT.

FILE NAME +	USER NAME + mamadnoid	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FIRST STREET RESURFACING WEST AVENUE TO WILLOW ROAD TYPICAL SECTIONS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
In:\CHP\1185\Road\Sheets\1185-Typ Sects.dwg		DRAWN -	REVISED -			1392	14-00182-00-RS	DUPAGE	31	9	
PLOT SCALE +		CHECKED -	REVISED -			CONTRACT NO. 61C16					
PLOT DATE + 12/22/2015		DATE -	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

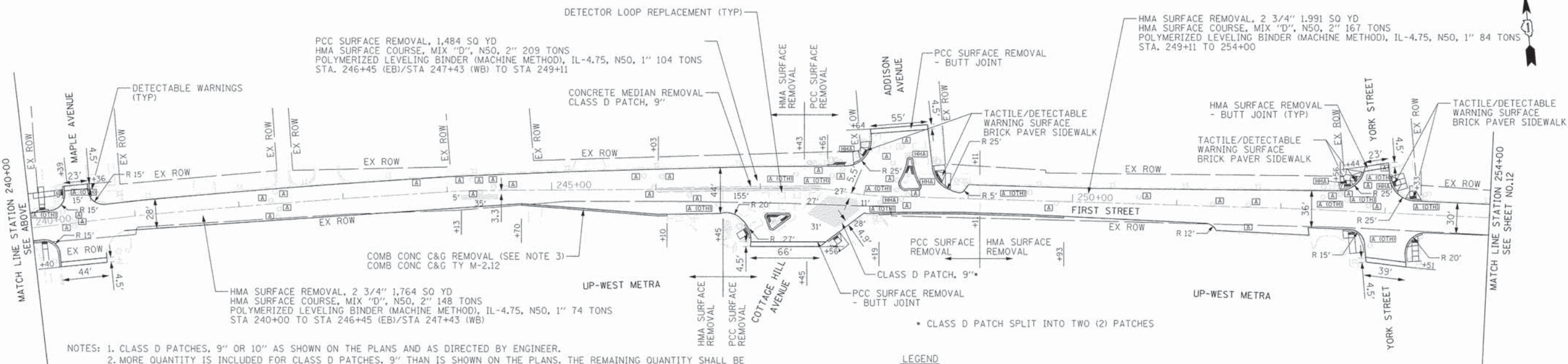
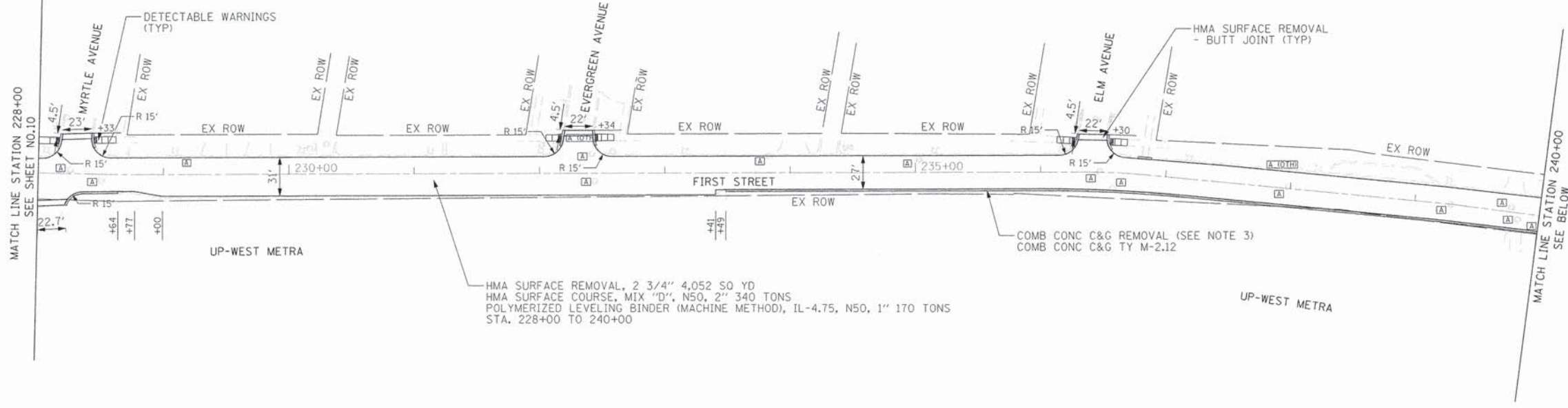


- NOTES:
1. CLASS D PATCHES, 9" OR 10" AS SHOWN ON THE PLANS AND AS DIRECTED BY ENGINEER.
 2. MORE QUANTITY IS INCLUDED FOR CLASS D PATCHES, 9" THAN IS SHOWN ON THE PLANS. THE REMAINING QUANTITY SHALL BE USED AT THE DIRECTION OF THE ENGINEER.
 3. REMOVAL AND DISPOSAL OF THE MATERIAL UNDER THE PROPOSED CURB AND GUTTER IN ORDER TO INSTALL THE PROPOSED SUB-BASE GRANULAR MATERIAL, TY B 4" SHALL BE CONSIDERED INCLUDED IN THE COST OF COMB CURB AND GUTTER REMOVAL.
 4. IF UNSUITABLE MATERIAL IS ENCOUNTERED UNDER THE EXISTING DRIVEWAYS, A 4" DEPTH OF THE MATERIAL SHALL BE REMOVED AND DISPOSED OF AND PAID FOR AS REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL. SUB-BASE GRANULAR MATERIAL SHALL BE INSTALLED IN ITS PLACE AND SHALL BE PAID FOR AS SUB-BASE GRANULAR MATERIAL, TYPE B 4".
 5. THE ADJUSTMENT OF THE DRAINAGE STRUCTURES IN THE CURB AND GUTTER LINE SHALL BE PAID FOR AS DRAINAGE STRUCTURES TO BE ADJUSTED.
 6. ALL PROPOSED SIDEWALK ADJACENT TO EXISTING SIDEWALK SHALL BE TIED WITH TWO 12"-#4 REINFORCING BARS, EPOXY COATED, THIS WORK SHALL BE INCLUDED IN THE COST OF THE PCC SIDEWALK PAY ITEM OF THE CORRESPONDING DEPTH.

LEGEND

[A] DRAINAGE STRUCTURE TO BE ADJUSTED	[VBA] VALVE BOX TO BE ADJUSTED
[A (OTH)] FOR ADJUSTMENT OF OTHER UTILITIES	[PATCH] PAVEMENT PATCHING (CLASS D PATCHES, 9" UNLESS OTHERWISE NOTED)
[REC] DRAINAGE STRUCTURE TO BE RECONSTRUCTED	[TP] TREE PROTECTION
[F&G I] FRAMES AND LIDS TO BE ADJUSTED, SPECIAL	[SODDING] SODDING, SALT TOLERANT TOPSOIL FURNISH AND PLACE, 6"
[F&G IIV] NEW FRAME & GRATE, TYPE IIV	
[F&L I CL] NEW FRAME & LID, TYPE I, CLOSED LID	
FOC FACE OF CURB	
FOS FACE OF SIDEWALK	

FILE NAME = G:\10\11\1165\Road\Sheets\VP-281aht	USER NAME = macdonald	DESIGNED - AS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FIRST STREET RESURFACING WEST AVENUE TO WILLOW ROAD IMPROVEMENT PLAN	F.A.U. RTE. = 1392	SECTION = 14-00182-00-RS	COUNTY = DUPAGE	TOTAL SHEETS = 31	SHEET NO. = 10
PLOT SCALE =	DRAWN - AJP	REVISED -	SCALE: 1"=50'			SHEET NO. 1 OF 3 SHEETS	STA. 200+00 TO STA. 228+00	CONTRACT NO. 61C16		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT
PLOT DATE = 12/22/2015	CHECKED - ESN	REVISED -								
	DATE = 12/17/2015	REVISED -								

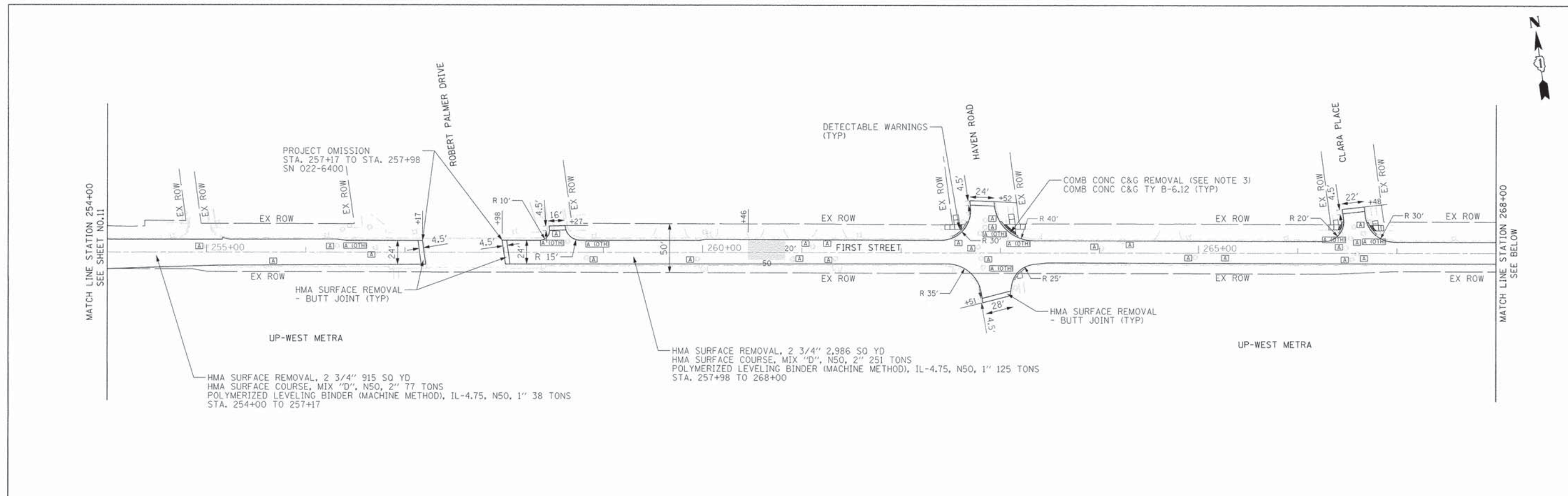


- NOTES:
1. CLASS D PATCHES, 9" OR 10" AS SHOWN ON THE PLANS AND AS DIRECTED BY ENGINEER.
 2. MORE QUANTITY IS INCLUDED FOR CLASS D PATCHES, 9" THAN IS SHOWN ON THE PLANS. THE REMAINING QUANTITY SHALL BE USED AT THE DIRECTION OF THE ENGINEER.
 3. REMOVAL AND DISPOSAL OF THE MATERIAL UNDER THE PROPOSED CURB AND GUTTER IN ORDER TO INSTALL THE PROPOSED SUB-BASE GRANULAR MATERIAL, TY B 4" SHALL BE CONSIDERED INCLUDED IN THE COST OF COMB CURB AND GUTTER REMOVAL.
 4. IF UNSUITABLE MATERIAL IS ENCOUNTERED UNDER THE EXISTING DRIVEWAYS, A 4" DEPTH OF THE MATERIAL SHALL BE REMOVED AND DISPOSED OF AND PAID FOR AS REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL. SUB-BASE GRANULAR MATERIAL SHALL BE INSTALLED IN ITS PLACE AND SHALL BE PAID FOR AS SUB-BASE GRANULAR MATERIAL, TYPE B 4".
 5. THE ADJUSTMENT OF THE DRAINAGE STRUCTURES IN THE CURB AND GUTTER LINE SHALL BE PAID FOR AS DRAINAGE STRUCTURES TO BE ADJUSTED.
 6. ALL PROPOSED SIDEWALK ADJACENT TO EXISTING SIDEWALK SHALL BE TIED WITH TWO 12"-#4 REINFORCING BARS, EPOXY COATED, THIS WORK SHALL BE INCLUDED IN THE COST OF THE PCC SIDEWALK PAY ITEM OF THE CORRESPONDING DEPTH.

LEGEND

[A]	DRAINAGE STRUCTURE TO BE ADJUSTED	[VBA]	VALVE BOX TO BE ADJUSTED
[A (OTH)]	FOR ADJUSTMENT OF OTHER UTILITIES	[Patching]	PAVEMENT PATCHING (CLASS D PATCHES, 9" UNLESS OTHERWISE NOTED)
[HHA]	HANDHOLE TO BE ADJUSTED	[Tree]	TREE PROTECTION
[REC]	DRAINAGE STRUCTURE TO BE RECONSTRUCTED	[Sodding]	SODDING, SALT TOLERANT TOPSOIL FURNISH AND PLACE, 6"
[RISP]	FRAMES AND LIDS TO BE ADJUSTED, SPECIAL	[FOS]	FACE OF CURB
[F&G IIV]	NEW FRAME & GRATE, TYPE IIV	[FOS]	FACE OF SIDEWALK
[F&L I CL]	NEW FRAME & LID, TYPE I, CLOSED LID		

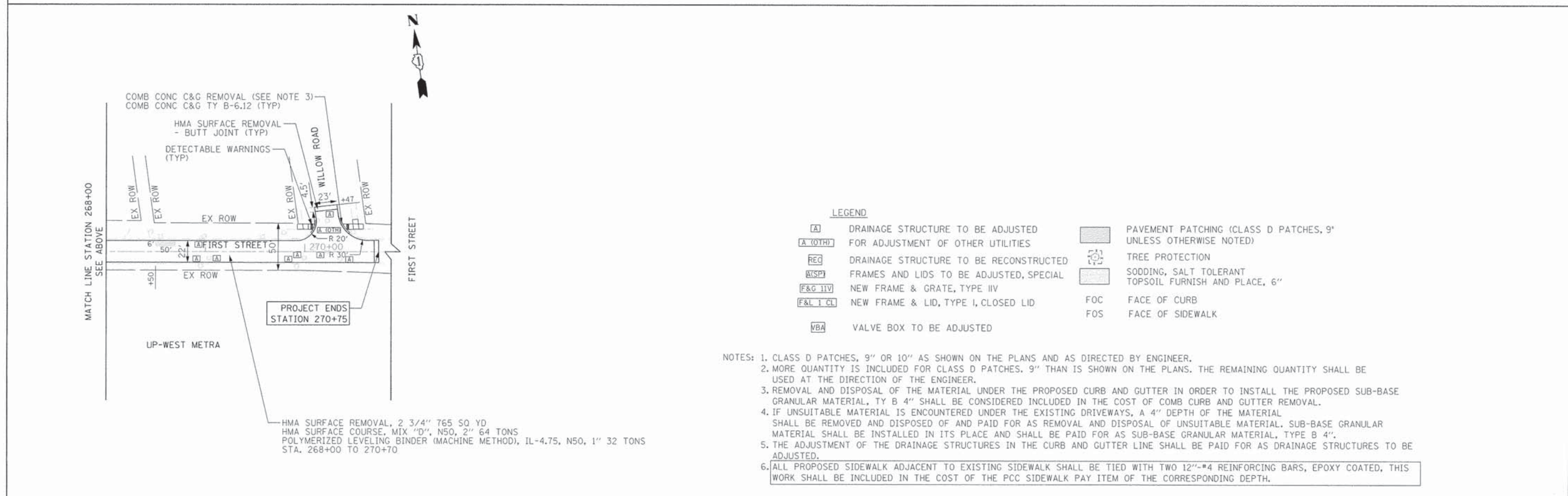
FILE NAME =	USER NAME =	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FIRST STREET RESURFACING WEST AVENUE TO WILLOW ROAD IMPROVEMENT PLAN	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Es:\DIR\BIB\B5\Road\Sheets\1PP-282.dwg		DRAWN -	REVISED -			1392	14-00182-00-RS	DUPAGE	31	11	
PLOT SCALE =		CHECKED -	REVISED -			CONTRACT NO. 61C16					
PLOT DATE =		DATE -	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					
						SCALE: 1"=50' SHEET NO. 2 OF 3 SHEETS STA. 200+00 TO STA. 254+00					



HMA SURFACE REMOVAL, 2 3/4" 915 SQ YD
 HMA SURFACE COURSE, MIX "D", N50, 2" 77 TONS
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1" 38 TONS
 STA. 254+00 TO 257+17

HMA SURFACE REMOVAL, 2 3/4" 2,986 SQ YD
 HMA SURFACE COURSE, MIX "D", N50, 2" 251 TONS
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1" 125 TONS
 STA. 257+98 TO 268+00

UP-WEST METRA



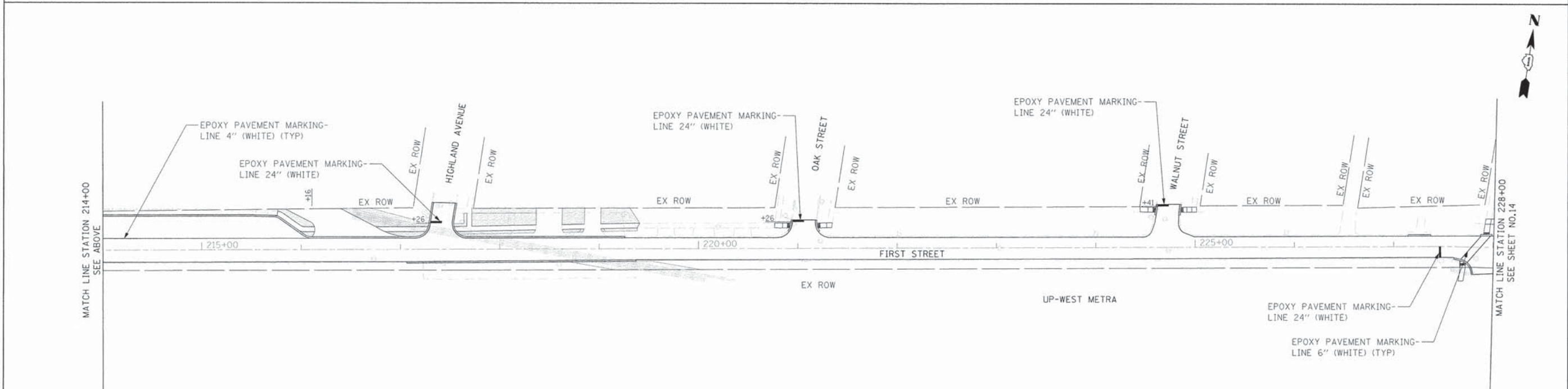
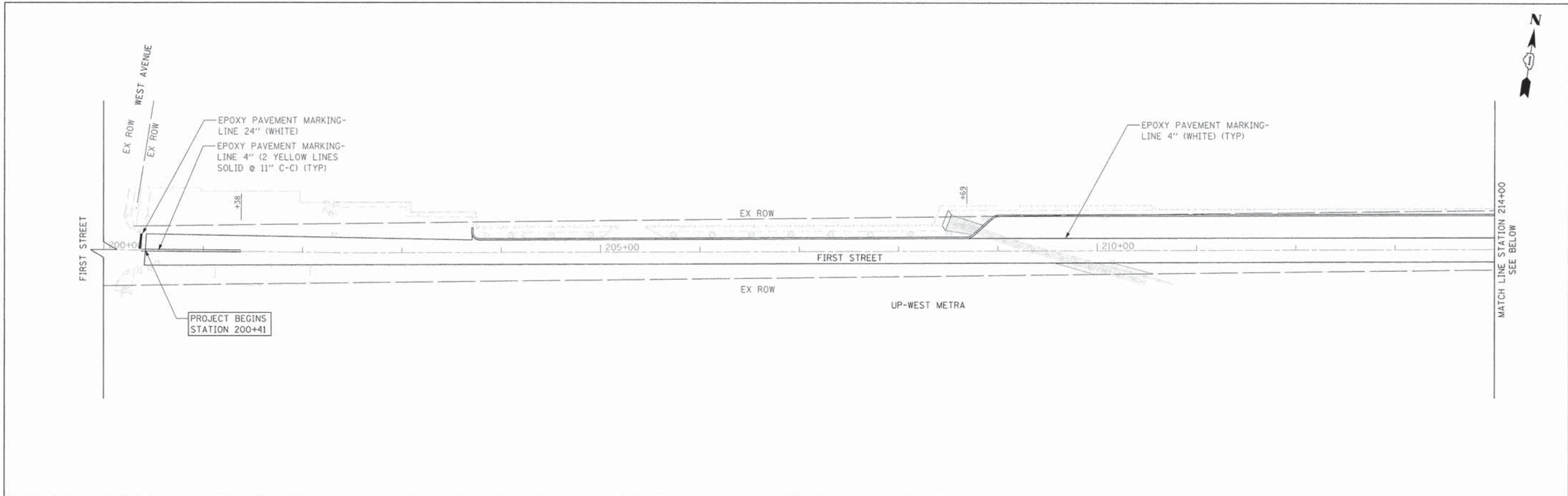
HMA SURFACE REMOVAL, 2 3/4" 765 SQ YD
 HMA SURFACE COURSE, MIX "D", N50, 2" 64 TONS
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1" 32 TONS
 STA. 268+00 TO 270+75

LEGEND

- [A] DRAINAGE STRUCTURE TO BE ADJUSTED
- [A (OTH)] FOR ADJUSTMENT OF OTHER UTILITIES
- [REC] DRAINAGE STRUCTURE TO BE RECONSTRUCTED
- [ATSP] FRAMES AND LIDS TO BE ADJUSTED, SPECIAL
- [F&G IIV] NEW FRAME & GRATE, TYPE IIV
- [F&L I CL] NEW FRAME & LID, TYPE I, CLOSED LID
- [VBA] VALVE BOX TO BE ADJUSTED
- [Patching] PAVEMENT PATCHING (CLASS D PATCHES, 9" UNLESS OTHERWISE NOTED)
- [Tree] TREE PROTECTION
- [Sodding] SODDING, SALT TOLERANT TOPSOIL FURNISH AND PLACE, 6"
- [FOC] FACE OF CURB
- [FOS] FACE OF SIDEWALK

- NOTES: 1. CLASS D PATCHES, 9" OR 10" AS SHOWN ON THE PLANS AND AS DIRECTED BY ENGINEER.
 2. MORE QUANTITY IS INCLUDED FOR CLASS D PATCHES, 9" THAN IS SHOWN ON THE PLANS. THE REMAINING QUANTITY SHALL BE USED AT THE DIRECTION OF THE ENGINEER.
 3. REMOVAL AND DISPOSAL OF THE MATERIAL UNDER THE PROPOSED CURB AND GUTTER IN ORDER TO INSTALL THE PROPOSED SUB-BASE GRANULAR MATERIAL, TY B 4" SHALL BE CONSIDERED INCLUDED IN THE COST OF COMB CURB AND GUTTER REMOVAL.
 4. IF UNSUITABLE MATERIAL IS ENCOUNTERED UNDER THE EXISTING DRIVEWAYS, A 4" DEPTH OF THE MATERIAL SHALL BE REMOVED AND DISPOSED OF AND PAID FOR AS REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL. SUB-BASE GRANULAR MATERIAL SHALL BE INSTALLED IN ITS PLACE AND SHALL BE PAID FOR AS SUB-BASE GRANULAR MATERIAL, TYPE B 4".
 5. THE ADJUSTMENT OF THE DRAINAGE STRUCTURES IN THE CURB AND GUTTER LINE SHALL BE PAID FOR AS DRAINAGE STRUCTURES TO BE ADJUSTED.
 6. ALL PROPOSED SIDEWALK ADJACENT TO EXISTING SIDEWALK SHALL BE TIED WITH TWO 12"-#4 REINFORCING BARS, EPOXY COATED, THIS WORK SHALL BE INCLUDED IN THE COST OF THE PCC SIDEWALK PAY ITEM OF THE CORRESPONDING DEPTH.

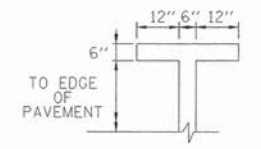
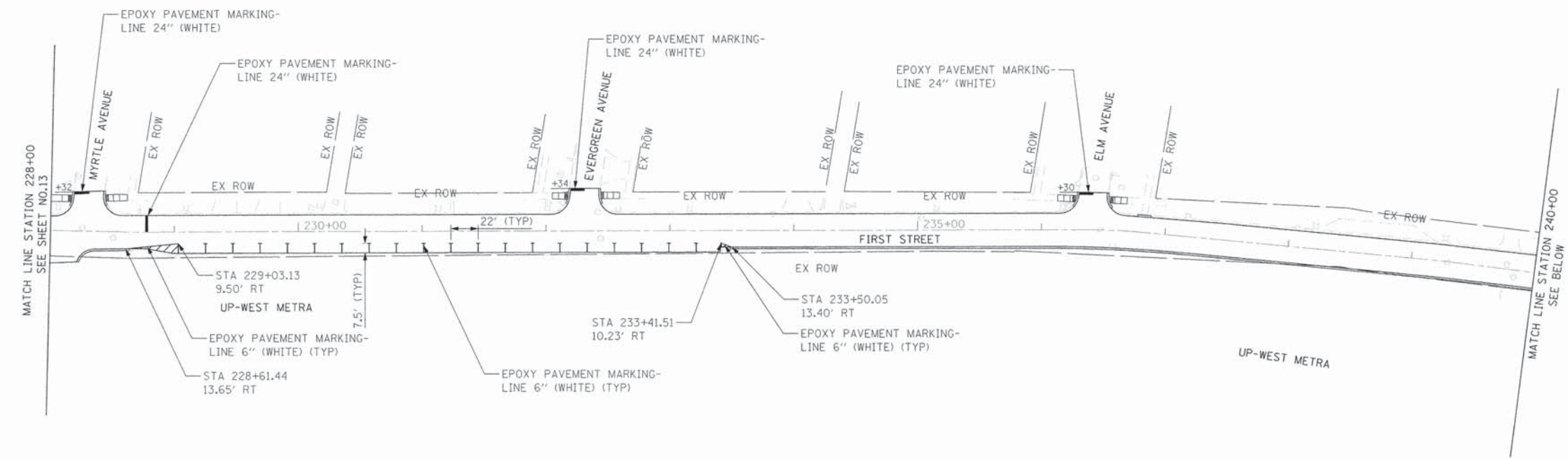
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	PLOT SCALE :	DRAWN - AJP	REVISED -			1392	14-00182-00-RS	DUPAGE	31	12
PLOT DATE : 12/22/2015	CHECKED - ESN	REVISED -	CONTRACT NO. 61C16							
	DATE - 12/17/2015	REVISED -	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
						SCALE: 1"=50'		SHEET NO. 2 OF 3 SHEETS		STA. 254+00 TO STA. 270+75



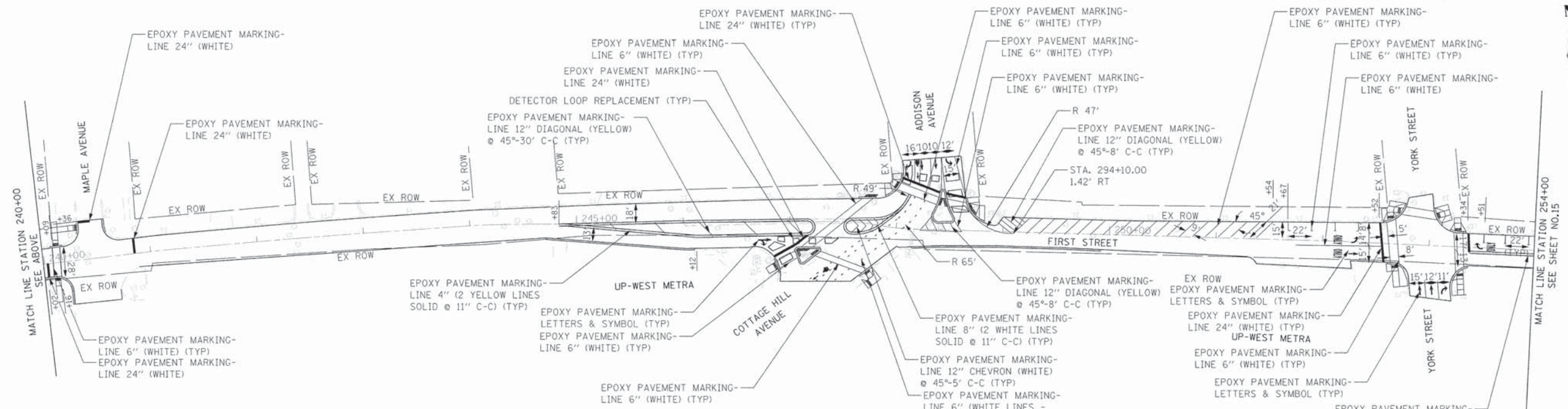
PAVEMENT MARKING ITEM	CORRESPONDING RECESSED GROOVING ITEM
EPOXY PAVEMENT MARKING - LETTERS AND SYMBOLS	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS
EPOXY PAVEMENT MARKING - LINE 4"	GROOVING FOR RECESSED PAVEMENT MARKING 5"
EPOXY PAVEMENT MARKING - LINE 6"	GROOVING FOR RECESSED PAVEMENT MARKING 7"
EPOXY PAVEMENT MARKING - LINE 12"	GROOVING FOR RECESSED PAVEMENT MARKING 13"
EPOXY PAVEMENT MARKING - LINE 24"	GROOVING FOR RECESSED PAVEMENT MARKING 25"

NOTES: 1) EPOXY PAVEMENT MARKINGS SHALL BE INSTALLED IN GROOVED RECESSED CHANNELS CONSTRUCTED 0.040 INCHES BELOW THE SURFACE AND 1 INCH WIDER THAN THE PAVEMENT MARKING LINE. CONSTRUCTION OF THE RECESSED CHANNELS SHALL BE PAID FOR SEPARATELY PER FOOT AS GROOVING FOR RECESSED PAVEMENT MARKING OF THE WIDTH SPECIFIED AND PER SQUARE FOOT FOR GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS.

2) SEE TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKING FOR GUIDANCE.

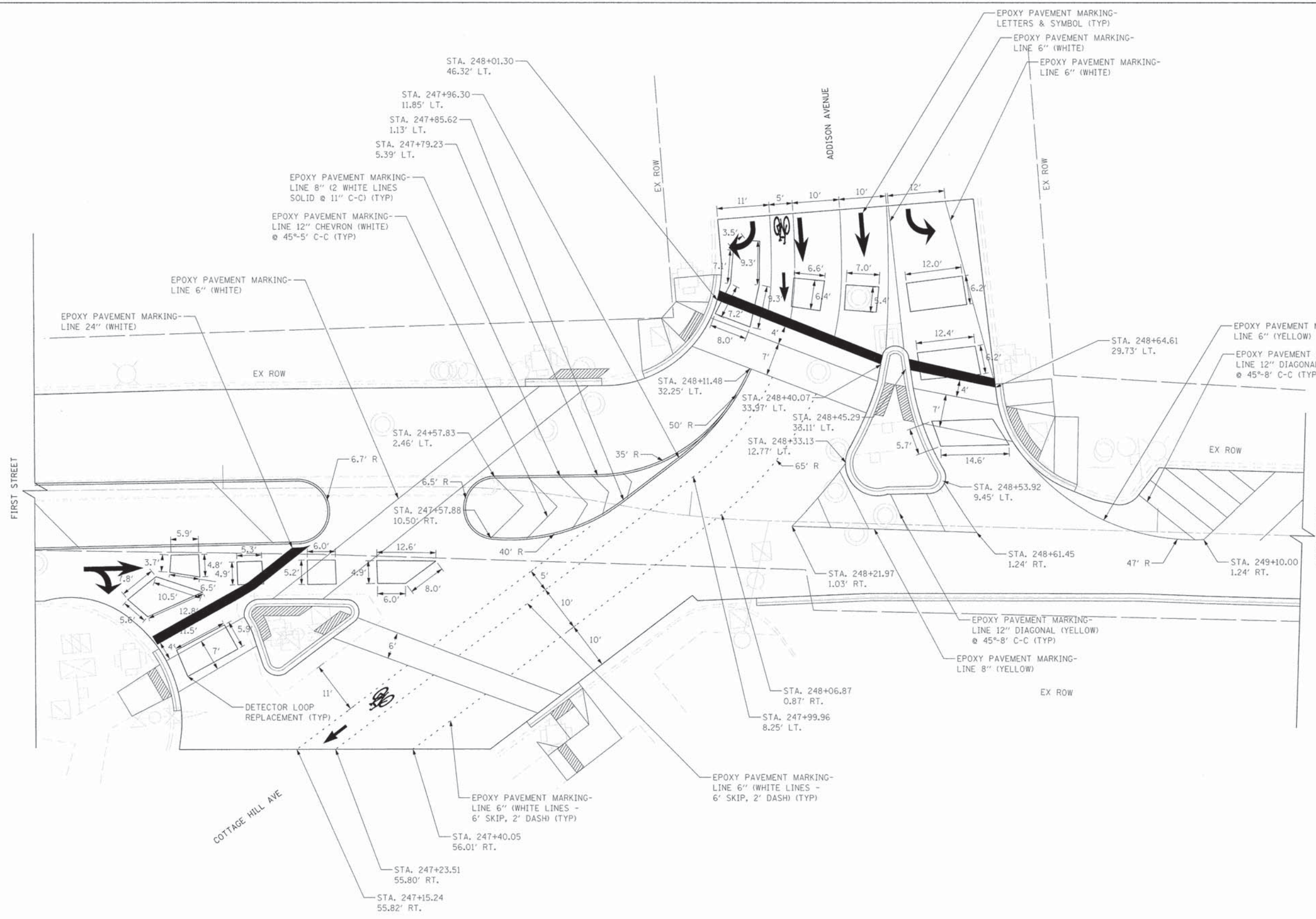


PARKING STALL MARKING DETAIL
 NOTE: PARKING STALL PAVEMENT MARKING SHALL BE WHITE ALONG FIRST STREET








PAVEMENT MARKING ITEM	CORRESPONDING RECESSED GROOVING ITEM
EPOXY PAVEMENT MARKING - LETTERS AND SYMBOLS	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS
EPOXY PAVEMENT MARKING - LINE 4"	GROOVING FOR RECESSED PAVEMENT MARKING 5"
EPOXY PAVEMENT MARKING - LINE 6"	GROOVING FOR RECESSED PAVEMENT MARKING 7"
EPOXY PAVEMENT MARKING - LINE 12"	GROOVING FOR RECESSED PAVEMENT MARKING 13"
EPOXY PAVEMENT MARKING - LINE 24"	GROOVING FOR RECESSED PAVEMENT MARKING 25"

NOTES: 1) EPOXY PAVEMENT MARKINGS SHALL BE INSTALLED IN GROOVED RECESSED CHANNELS CONSTRUCTED 0.040 INCHES BELOW THE SURFACE AND 1 INCH WIDER THAN THE PAVEMENT MARKING LINE. CONSTRUCTION OF THE RECESSED CHANNELS SHALL BE PAID FOR SEPARATELY PER FOOT AS GROOVING FOR RECESSED PAVEMENT MARKING OF THE WIDTH SPECIFIED AND PER SQUARE FOOT FOR GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS.
 2) SEE TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKING FOR GUIDANCE






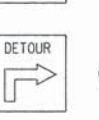






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PLOT SCALE =	CHECKED = ESN	REVISED =	SCALE: 1"=10'			SHEET NO. 1 OF 1 SHEETS	STA. 247+50 TO STA. 249+31	FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT	CONTRACT NO. 61C16			
PLOT DATE = 12/18/2015	DATE = 12/17/2015	REVISED =											

LEGEND

-  WORK ZONE
-  DETOUR ROUTE
-  TRAFFIC FLOW DIRECTION
-  TYPE III BARRICADE
-  PROPOSED TRAFFIC CONTROL SIGN

DETOUR SIGNS

- | | | | | | |
|---|---|---|--|---|---|
| R11-2 (48X30) |  | ① | M4-9L (48X36) |  | ⑥ |
| M4-10L (48X18) |  | ② | AS SHOWN OR M4-8 "DETOUR" (24X12) WITH M5-1L (21X15) |  | ⑦ |
| SIGN FLAGS, MONODIRECTIONAL FLASHING LIGHT, W20-2 (36X36) |  | ③ | AS SHOWN OR M4-8 "DETOUR" (24X12) WITH M5-1R (21X15) |  | ⑧ |
| M4-8A (24X18) |  | ④ | AS SHOWN OR M4-8 "DETOUR" (24X12) WITH M6-3 (21X15) |  | ⑨ |
| M4-9R (48X36) |  | ⑤ | |  | ⑩ |
| | | | R1-1 (30X30) | | |

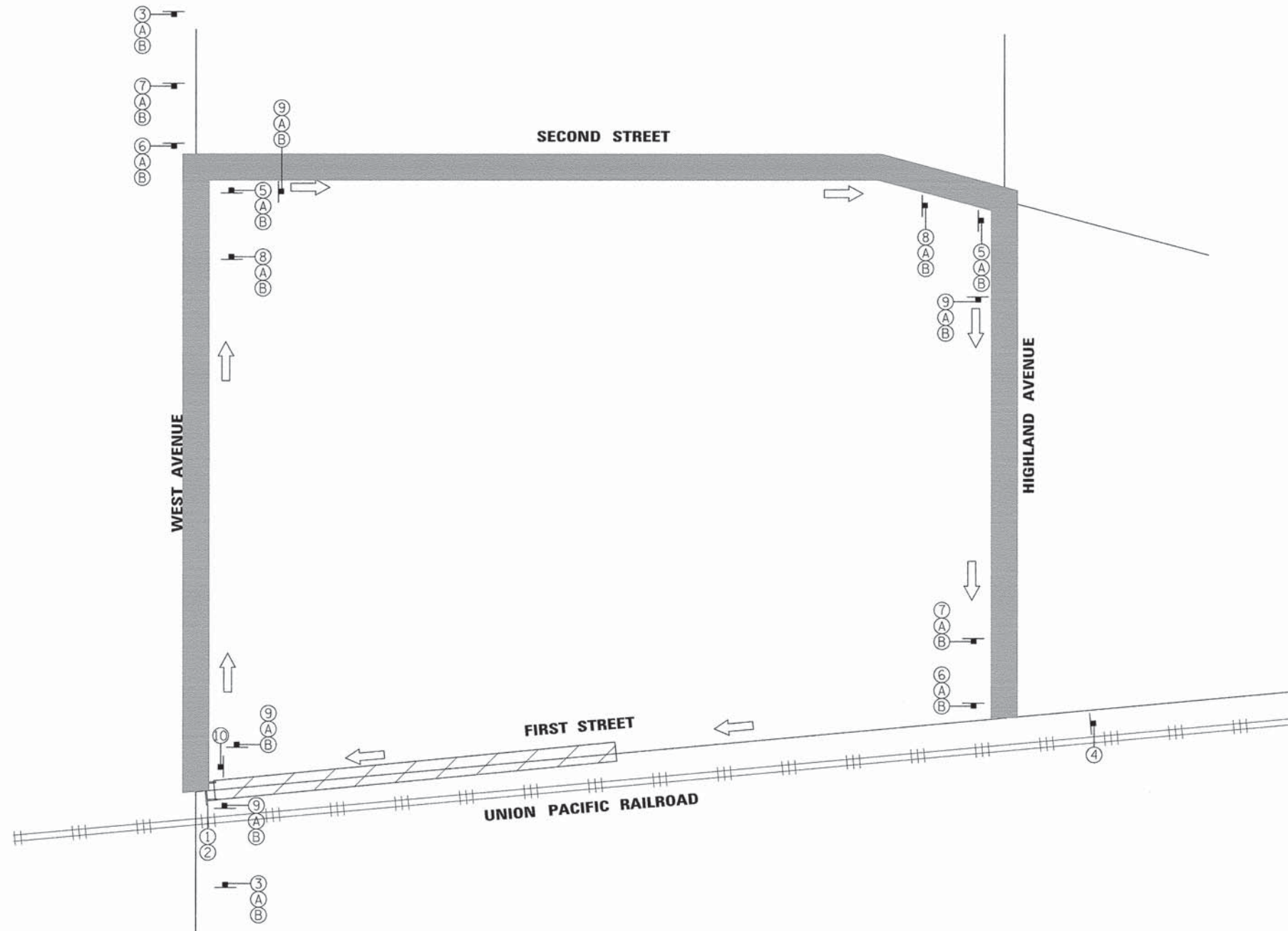
CARDINAL DIRECTION & SPECIAL SIGNS

- MIN. 5" BLACK LETTERING AND ORANGE BACKGROUND
M3-2 (24X12)
- | | |
|--------------|-----|
| First Street | (A) |
| EAST | (B) |

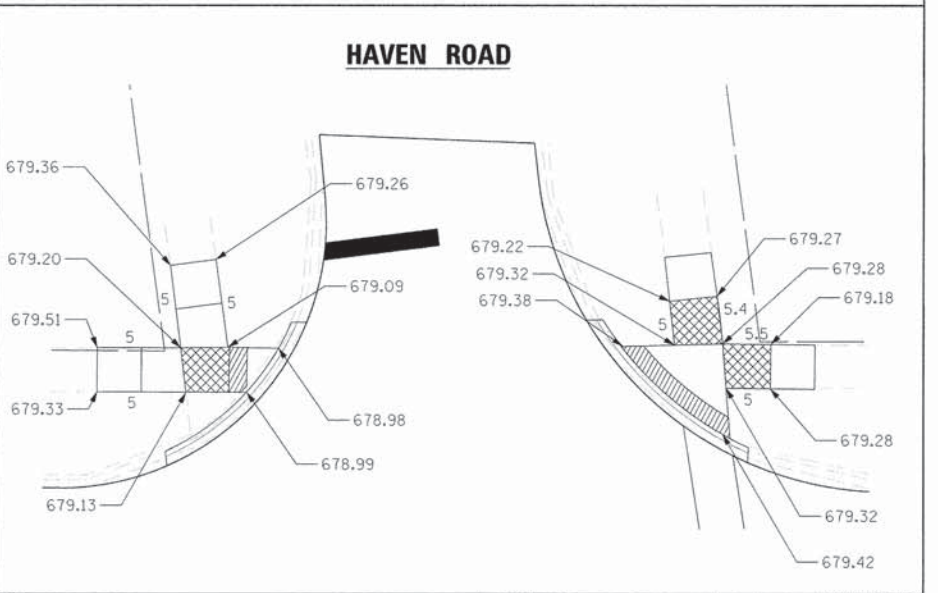
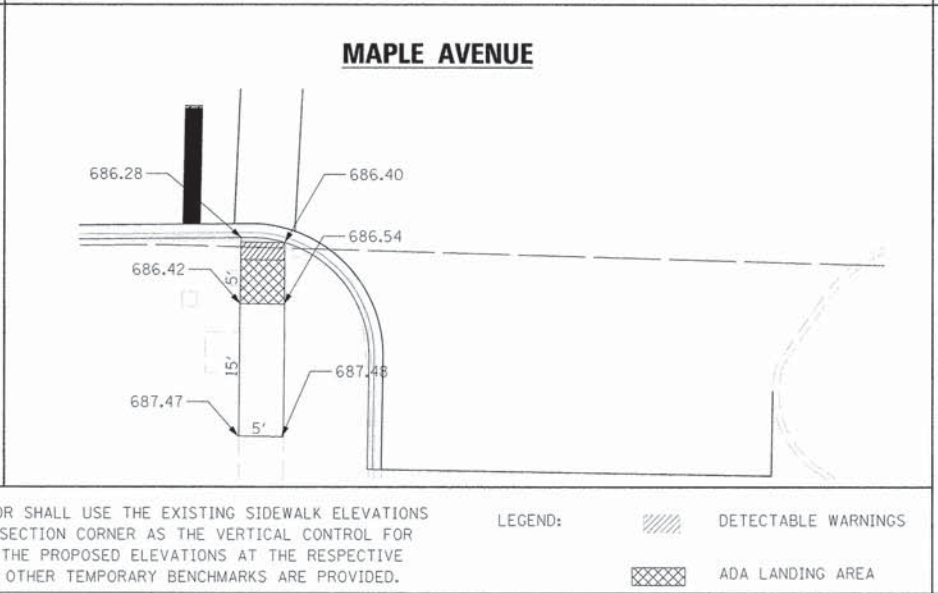
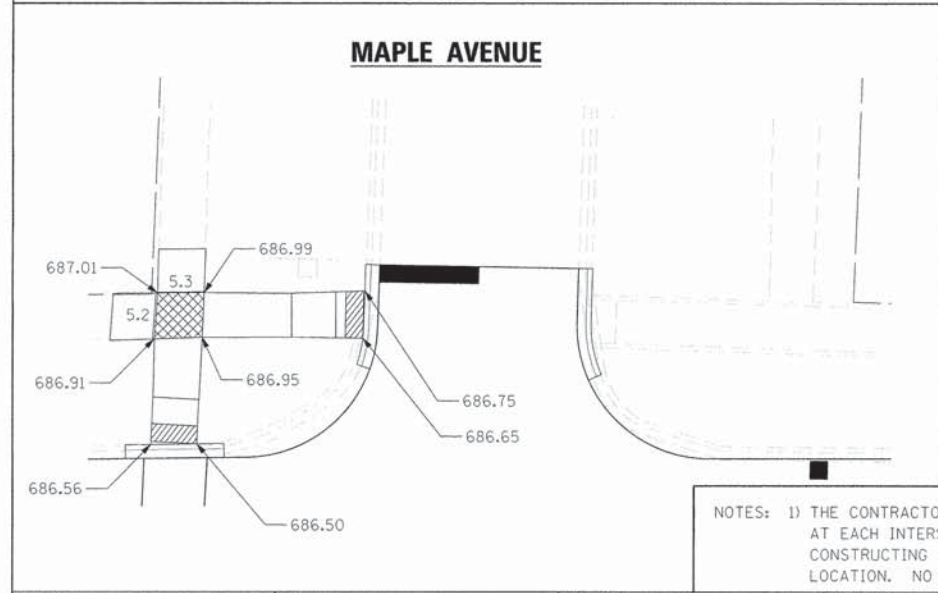
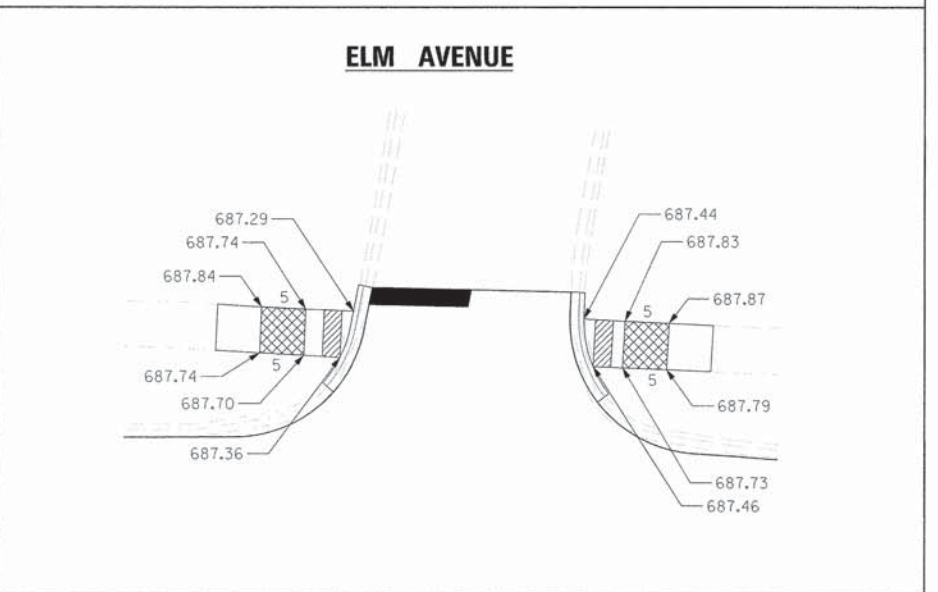
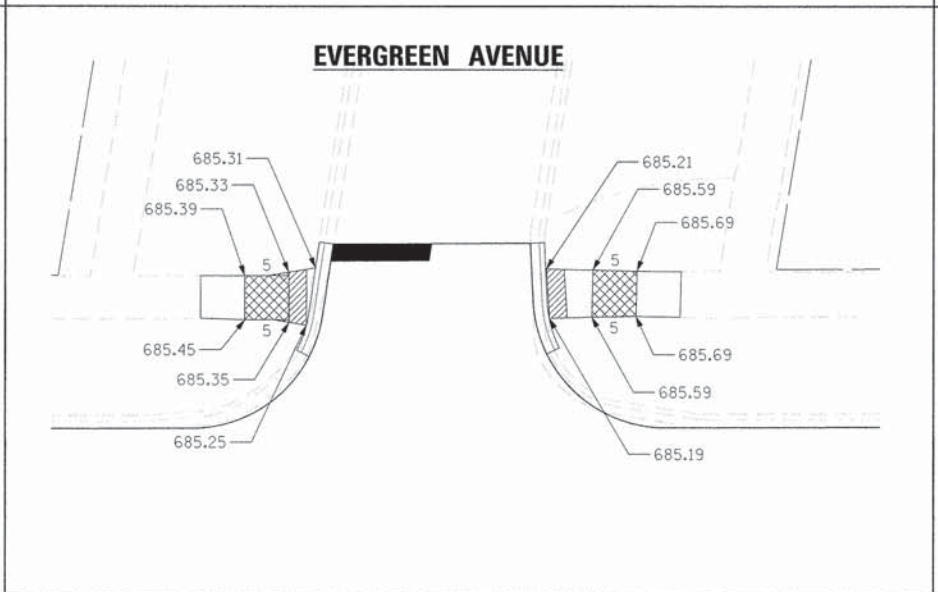
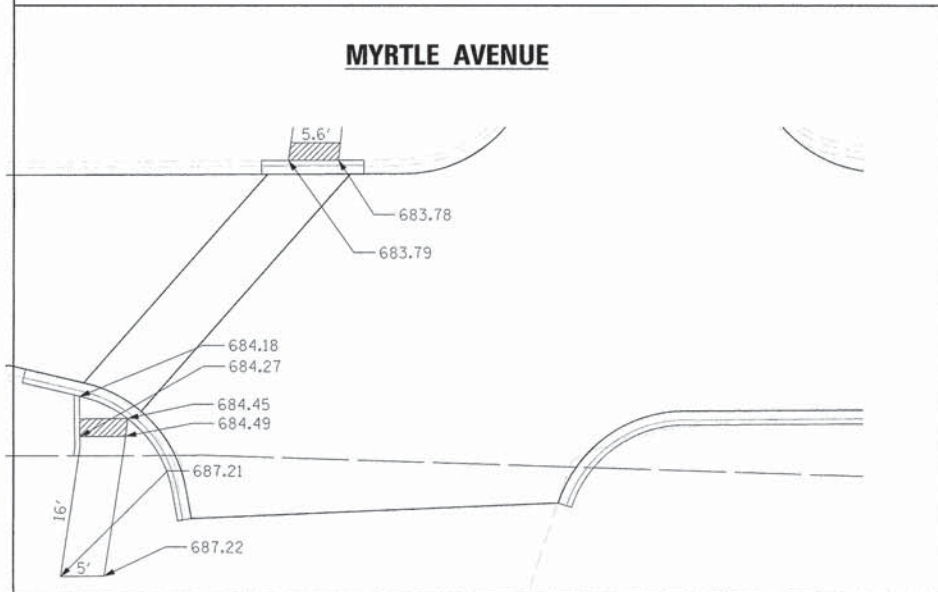
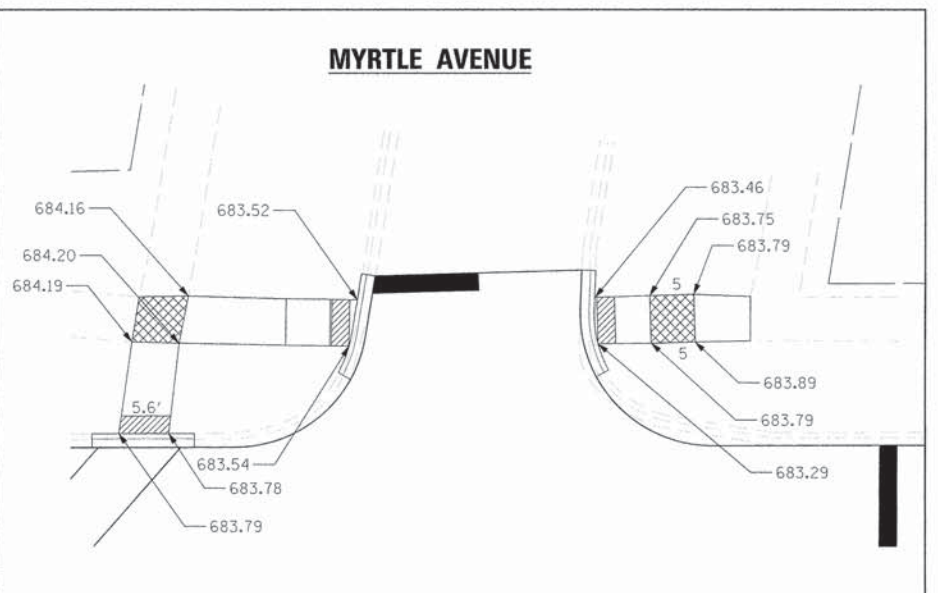
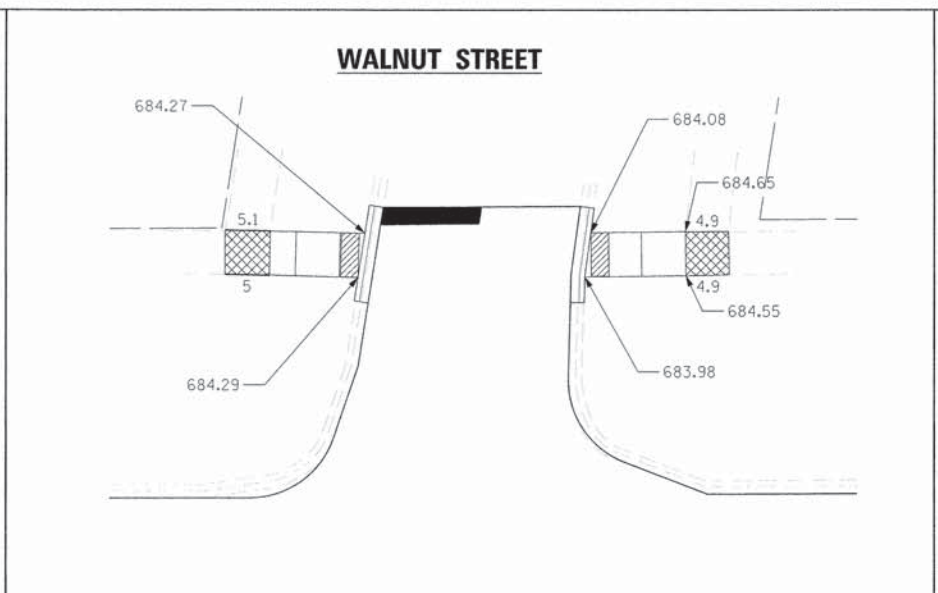
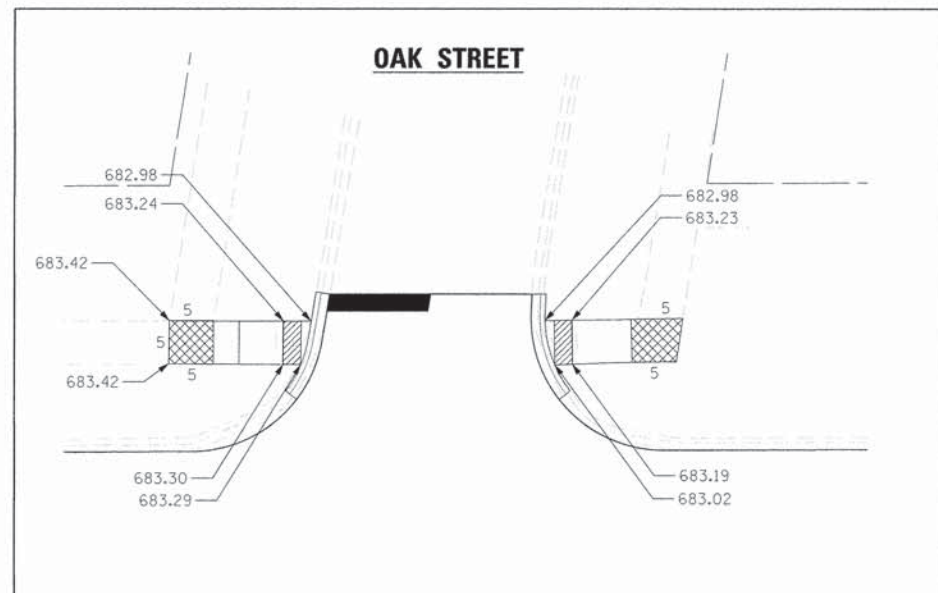
HIGHWAY STANDARDS

701901 TRAFFIC CONTROL DEVICES

- NOTES:**
1. THIS DETOUR SHALL ONLY BE USED FOR THE STAGED CONSTRUCTION OF THE CONCRETE PAVEMENT. FIRST STREET WILL REMAIN WESTBOUND BETWEEN HIGHLAND AVENUE AND WEST AVENUE. THE CONCRETE PAVEMENT SHALL BE CONSTRUCTED IN TWO STAGES. SIGNS AND BARRICADES MUST BE SHIFTED AS NECESSARY TO ACCOMMODATE THE STAGING.
 2. LOCATIONS SHOWN ARE APPROXIMATE. LOCATIONS TO BE DETERMINED BY THE ENGINEER BASED ON FIELD CONDITIONS.
 3. ONE 12 FOOT WESTBOUND TRAVEL LANE SHALL BE MAINTAINED THROUGH THE CONSTRUCTION WORK ZONE.
 4. PORTABLE/CHANGEABLE ELECTRONIC MESSAGE BOARDS TO BE USED 3 DAYS IN ADVANCE AND THROUGHOUT THE DURATION OF CONSTRUCTION.
 5. STREET NAME SHALL BE ABOVE DETOUR SIGN TO COMPLY WITH MUTCD (TYPICAL APPLICATION 19).



FILE NAME: G:\CHP\118185\Road\Sheet\DETOUR PLAN.dgn	USER NAME: wmcndonald	DESIGNED - AS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FIRST STREET RESURFACING WEST AVENUE TO WILLOW ROAD DETOUR PLAN	F.A.U. RTE. 1392	SECTION 14-00182-00-RS	COUNTY DUPAGE	TOTAL SHEETS 31	SHEET NO. 17
PLOT SCALE: -	PLOT DATE: 12/22/2015	DRAWN - AJP	REVISER -	SCALE: NTS SHEET NO. 1 OF 1 SHEETS STA. TO STA.		CONTRACT NO. 61C16 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES: 1) THE CONTRACTOR SHALL USE THE EXISTING SIDEWALK ELEVATIONS AT EACH INTERSECTION CORNER AS THE VERTICAL CONTROL FOR CONSTRUCTING THE PROPOSED ELEVATIONS AT THE RESPECTIVE LOCATION. NO OTHER TEMPORARY BENCHMARKS ARE PROVIDED.

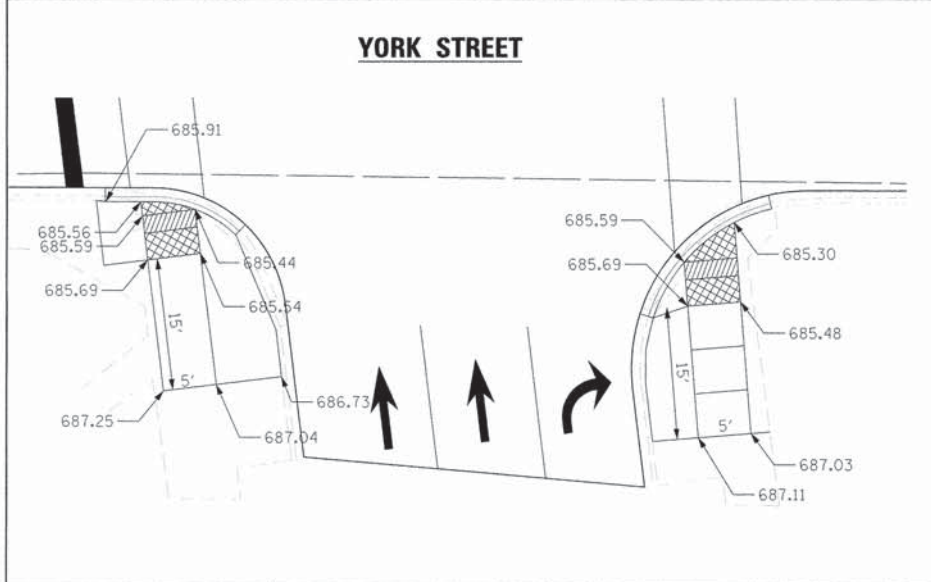
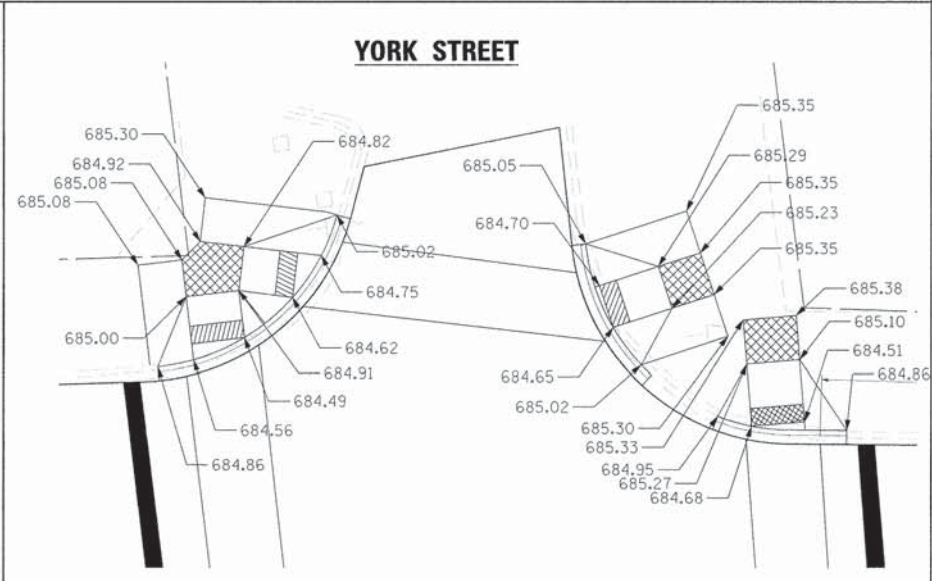
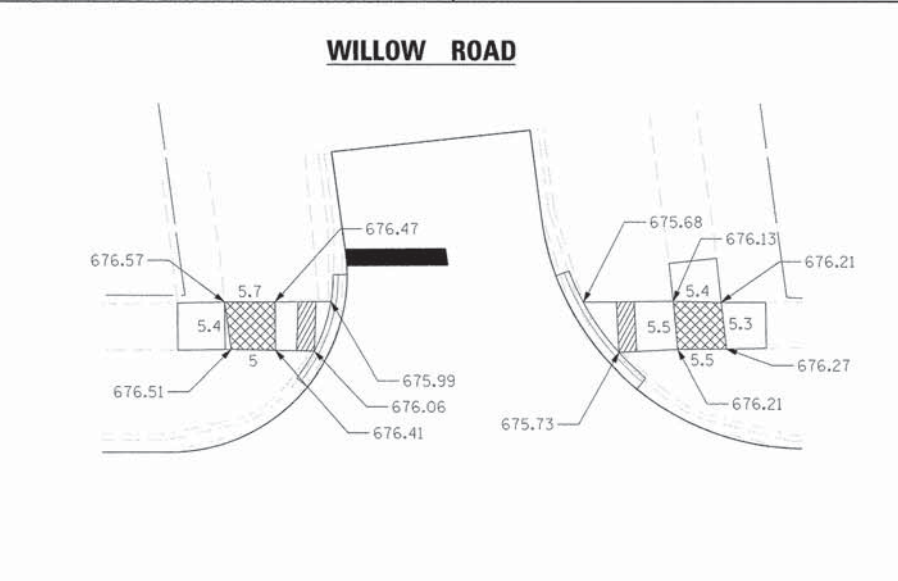
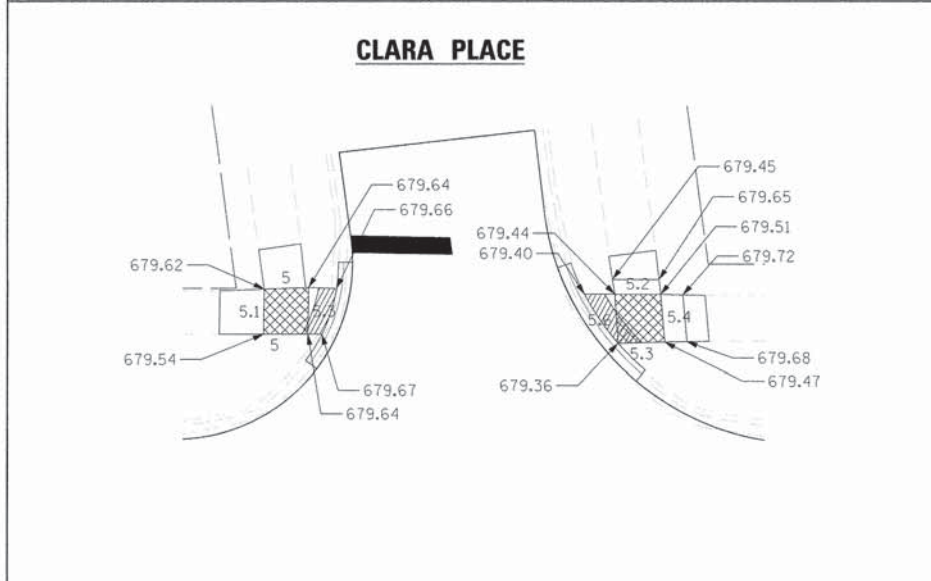
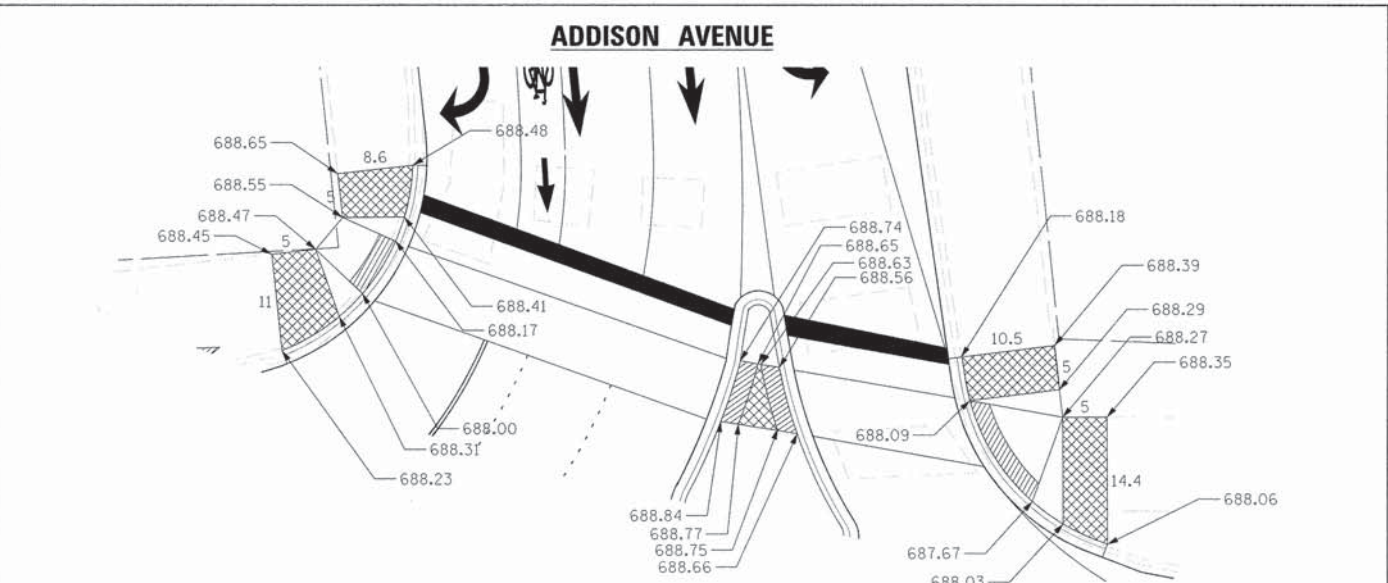
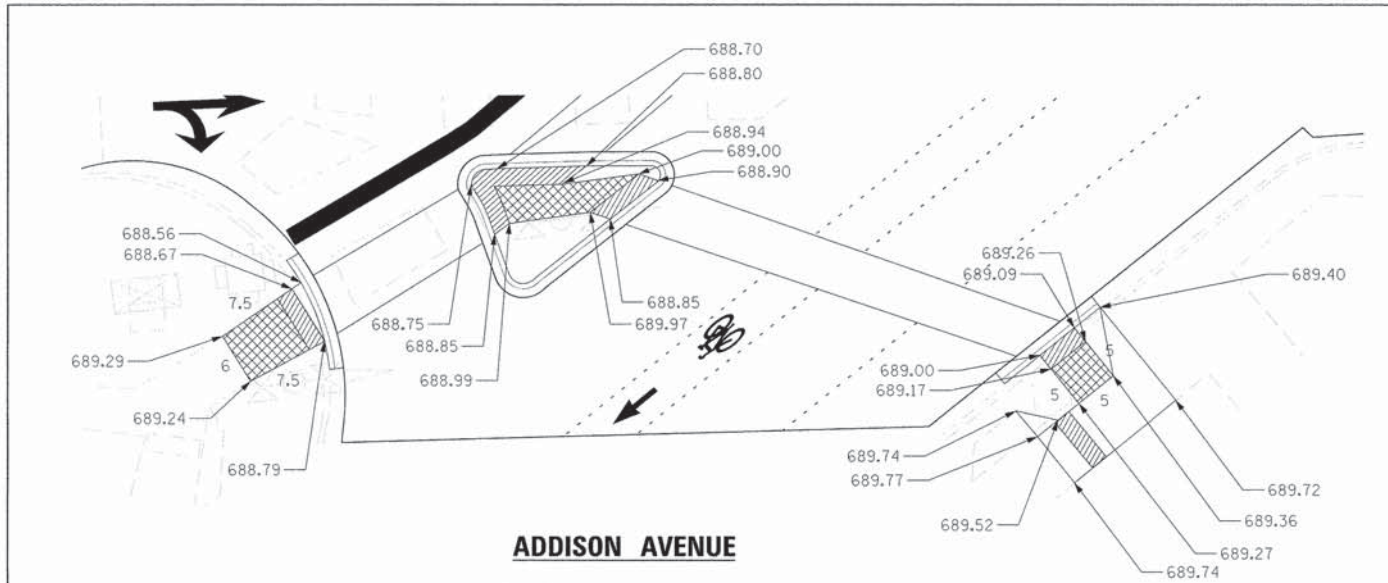
LEGEND:
 DETECTABLE WARNINGS
 ADA LANDING AREA

FILE NAME * GeO\PI\P\1815\Road\Sheets\VC-S&I-ADA Details\Sheet	USER NAME * ESNeubauer	DESIGNED - AS	REVISED -
		DRAWN - AJP	REVISED -
PLOT SCALE * 1"=10'		CHECKED - ESN	REVISED -
PLOT DATE * 12/18/2015		DATE - 12/17/2015	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FIRST STREET RESURFACING
WEST AVENUE TO WILLOW ROAD
CURB RAMP AND DETECTABLE WARNING DETAILS
SCALE: 1"=10' SHEET NO. 1 OF 2 SHEETS STA. TO STA.

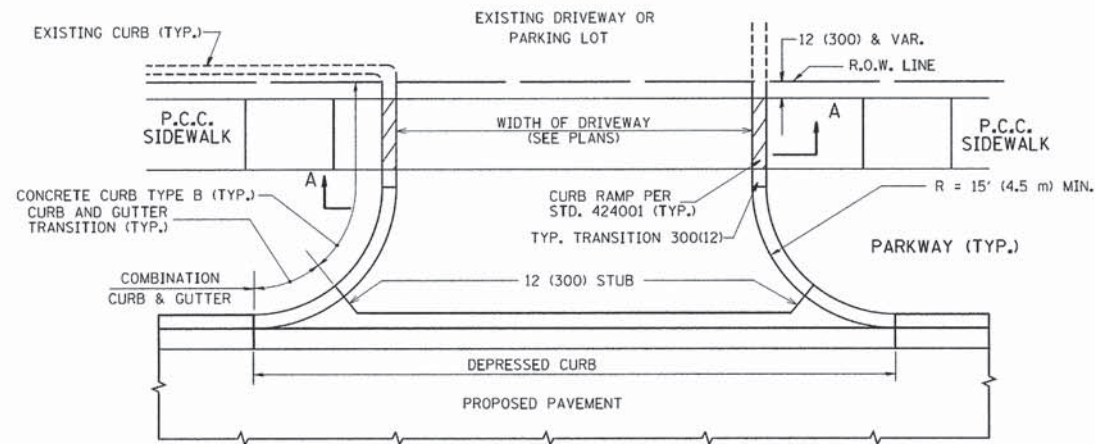
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CONTRACT NO. 61C16				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



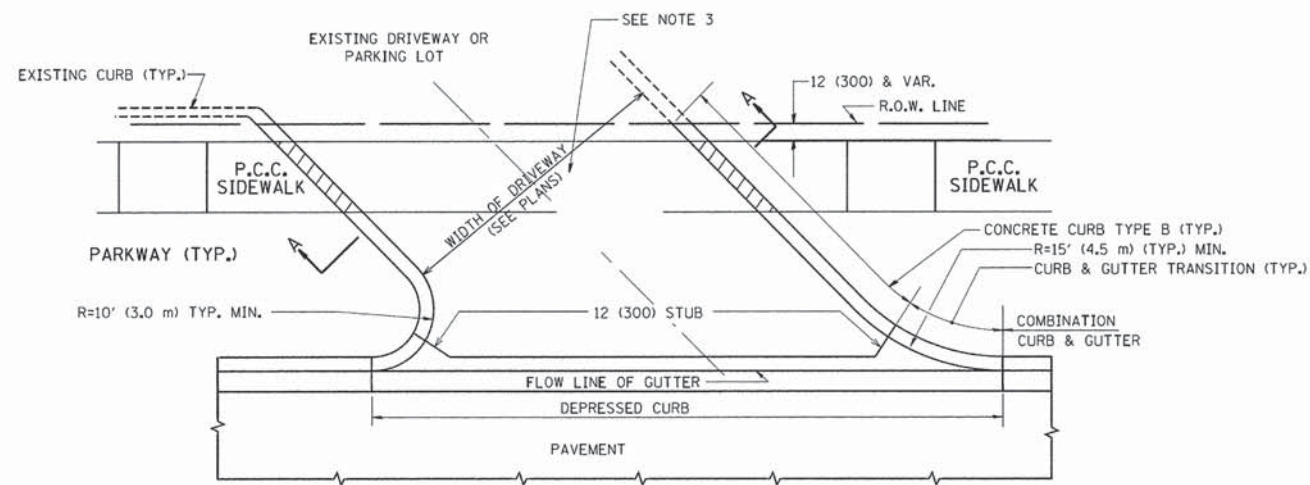
NOTES: 1) THE CONTRACTOR SHALL USE THE EXISTING SIDEWALK ELEVATIONS AT EACH INTERSECTION CORNER AS THE VERTICAL CONTROL FOR CONSTRUCTING THE PROPOSED ELEVATIONS AT THE RESPECTIVE LOCATION. NO OTHER TEMPORARY BENCHMARKS ARE PROVIDED.

LEGEND: DETECTABLE WARNINGS ADA LANDING AREA

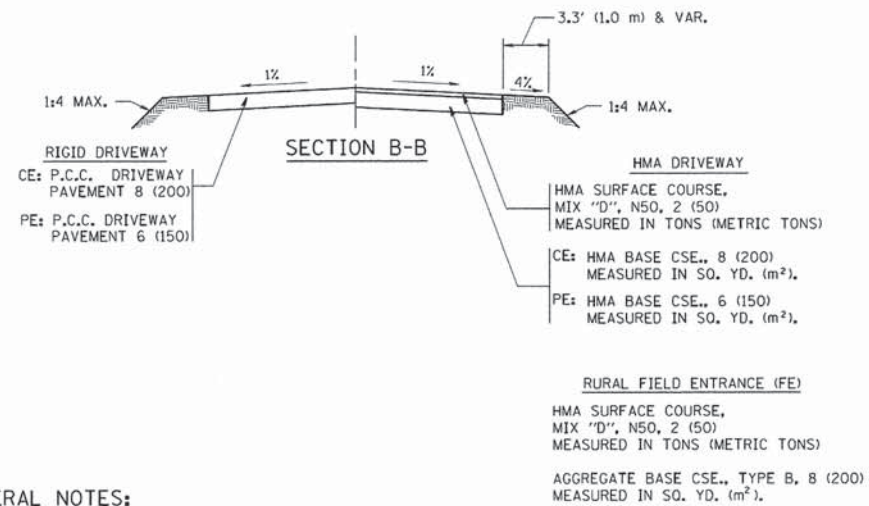
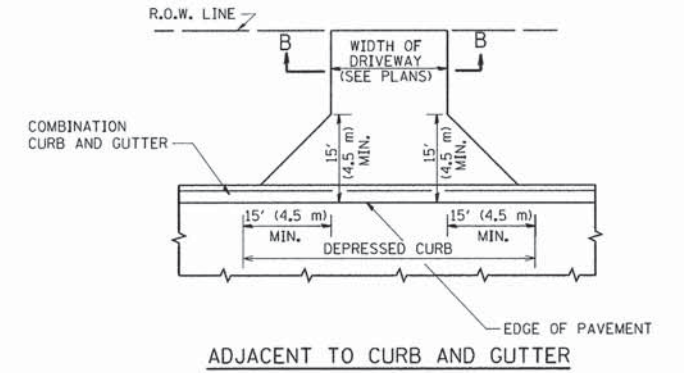
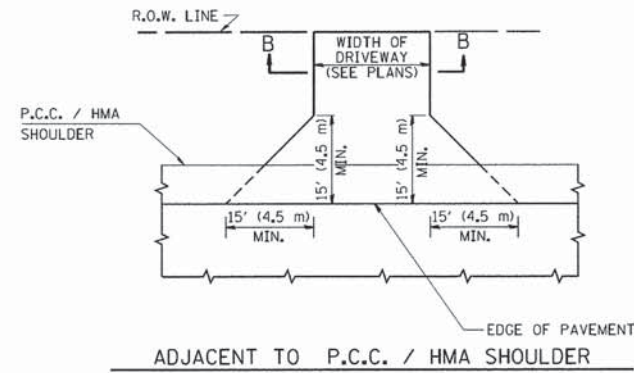
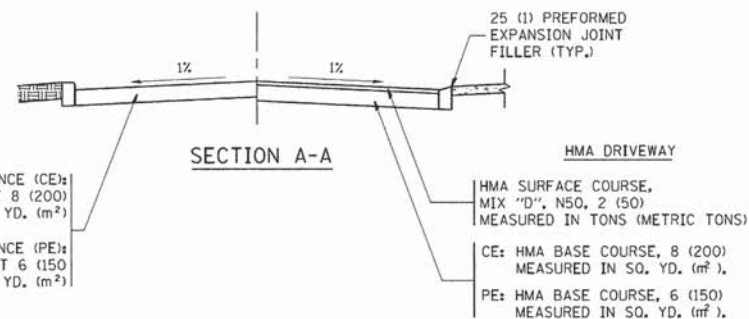
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PLOT SCALE =	CHECKED = ESN	REVISED =	SCALE: 1"=10'			SHEET NO. 2 OF 2 SHEETS	STA. TO STA.	CONTRACT NO. 61C16				
PLOT DATE = 12/18/2015	DATE = 12/17/2015	REVISED =	FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT									



WITH CONCRETE CURB, TYPE B



WITH CONCRETE CURB, TYPE B



GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

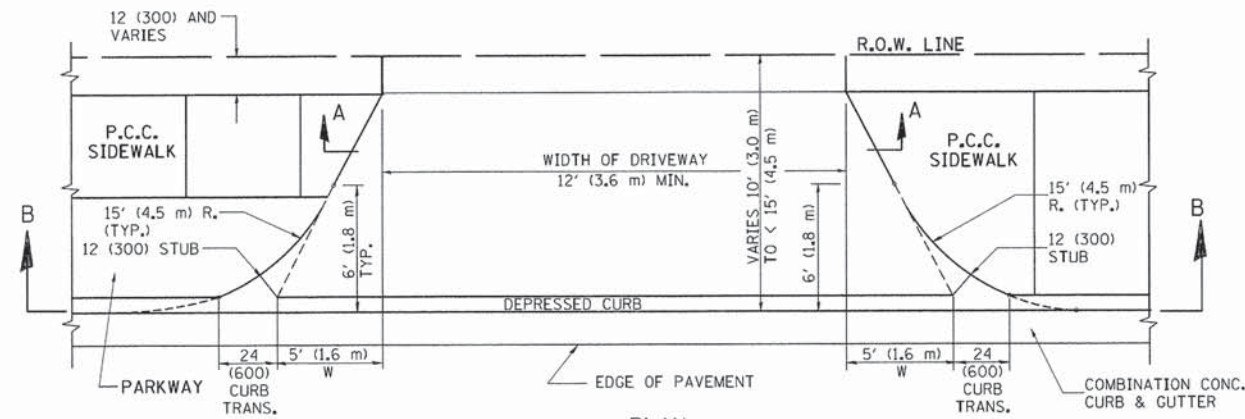
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

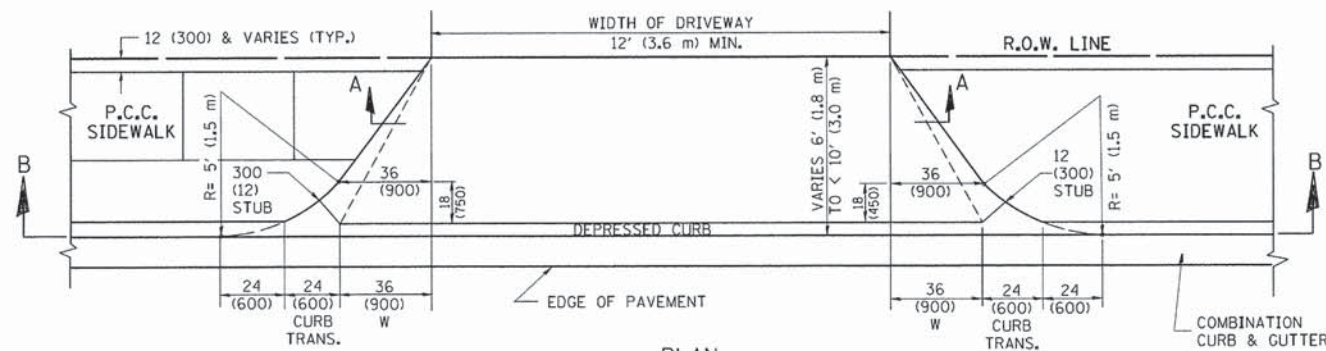
**DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W.
AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

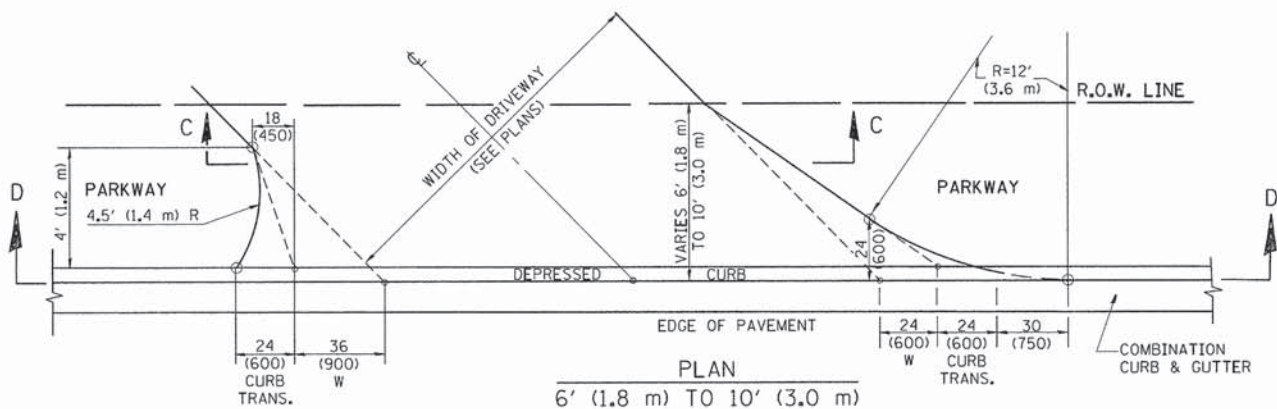
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BD0156-07 (BD-01)		CONTRACT NO.	61C16	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



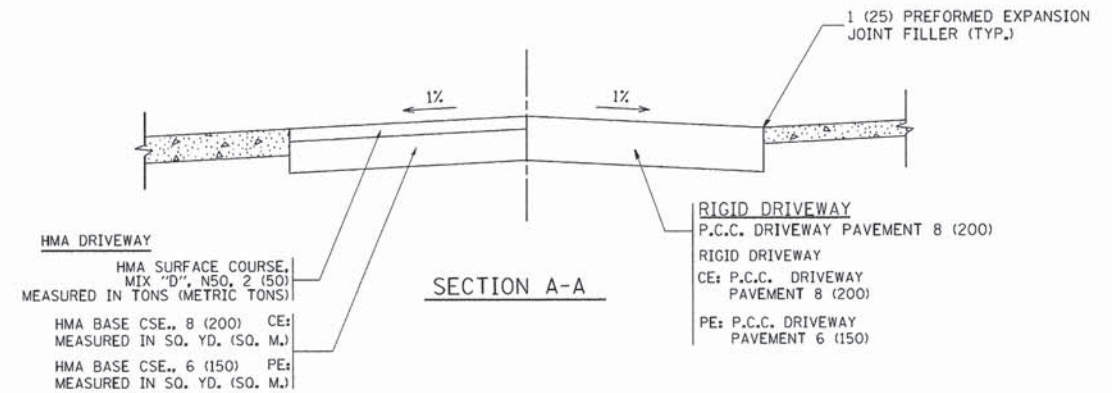
PLAN
10' (3.0 m) TO < 15' (4.5 m)



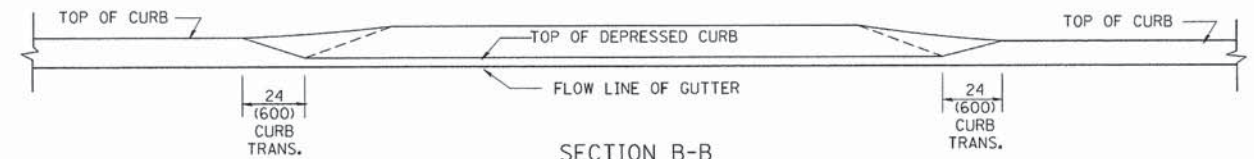
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6' (1.8 m) TO < 10' (3.0 m)



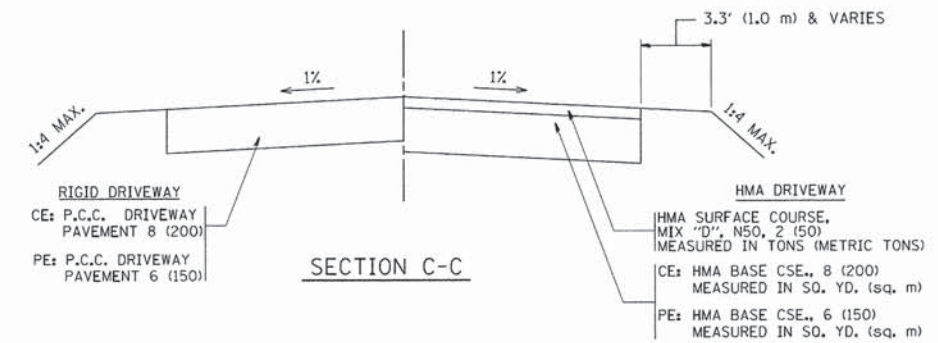
PLAN
6' (1.8 m) TO 10' (3.0 m)



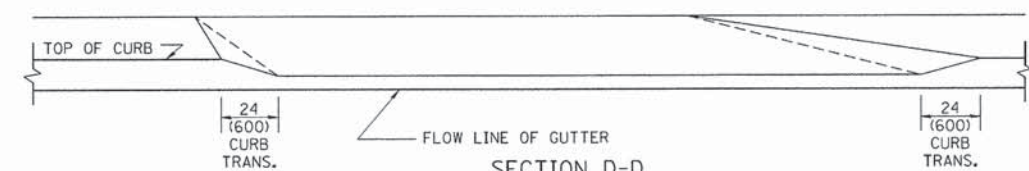
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

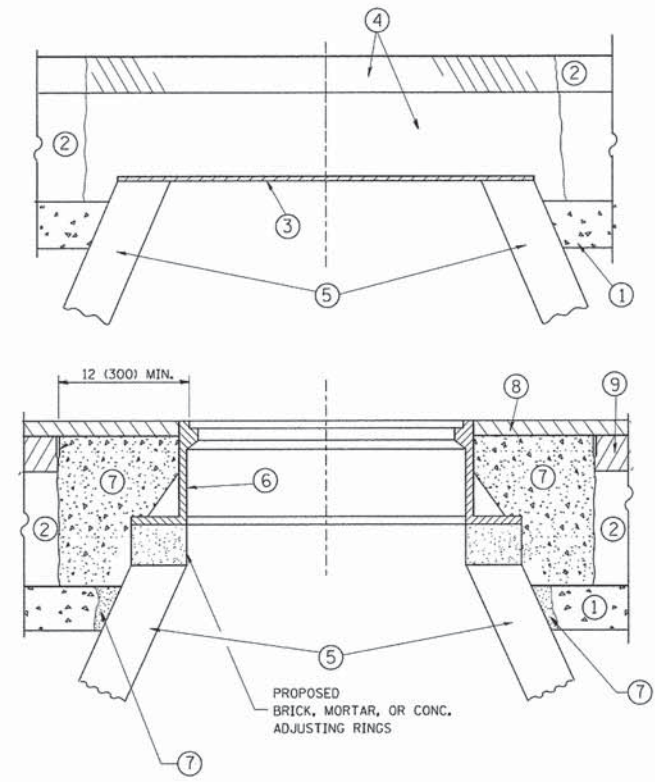
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	PLOT SCALE = 50.0000' / 1" =	CHECKED -	REVISED - R. BORO 01-01-07
	PLOT DATE = 10/28/2011	DATE - 11-06-95	REVISED - R. BORO 09-06-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1392	14-00182-00-RS	DUPAGE	31	21
BD400-02 (BD-02)			CONTRACT NO. 61C16	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				



CONSTRUCTION PROCEDURES

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
 - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
 - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
 - D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

- STAGE 2 (AFTER PAVEMENT MILLING)**
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- * UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

~~CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.~~

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

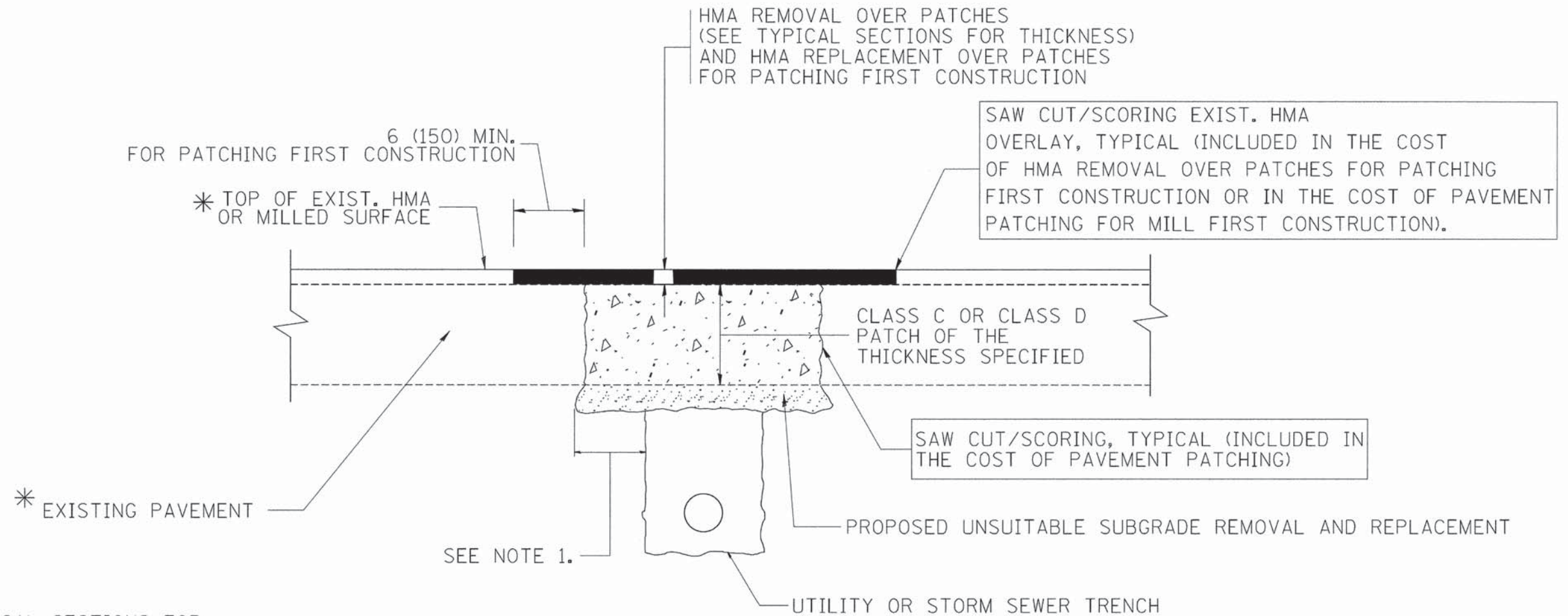
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	PLOT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1392	14-00182-00-RS	DUPAGE	31	22
BD600-03 (BD-8)		CONTRACT NO. 61C16		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

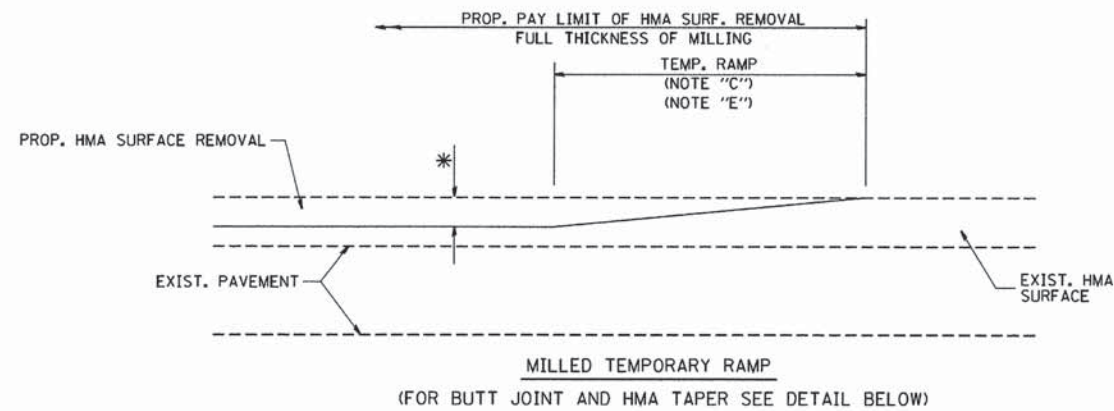
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		PLOT SCALE = 50,000' / 1"	REVISOR - R. BORO 09-04-07
		PLOT DATE = 10/27/2008	DATE - 10-25-94
			REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

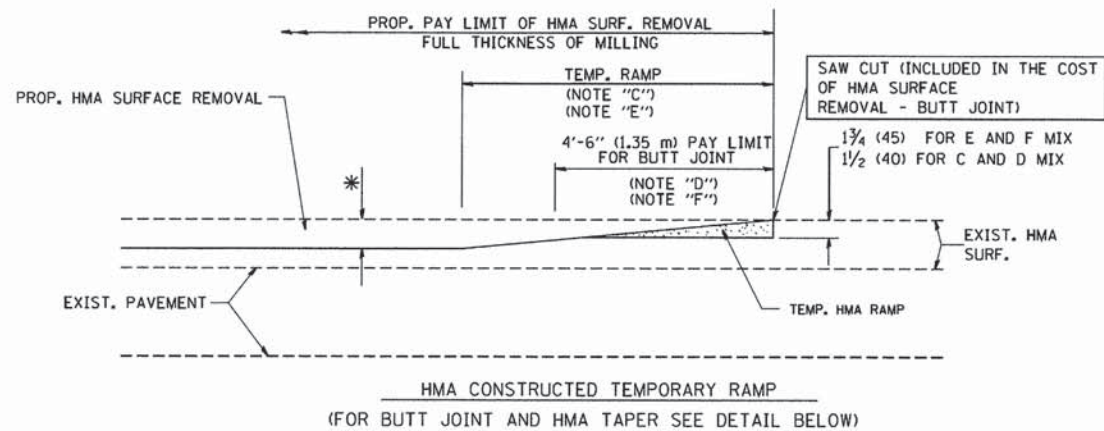
**PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 1392	SECTION 14-00182-00-RS	COUNTY DUPAGE	TOTAL SHEETS 31	SHEET NO. 23
BD400-04 (BD-22)		CONTRACT NO. 61C16		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

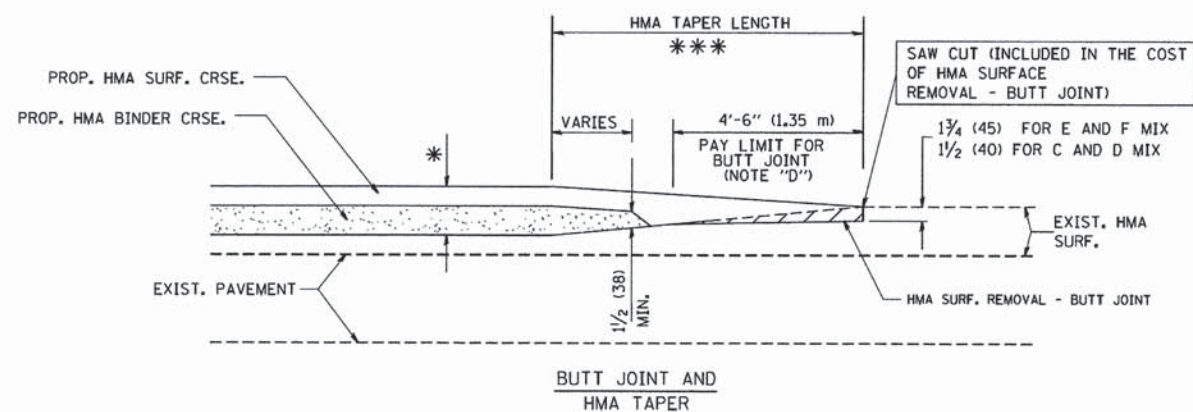


OPTION 1

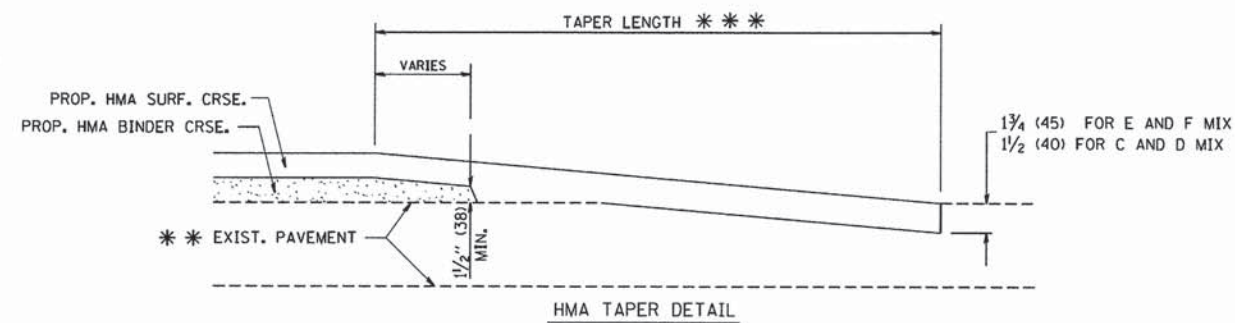
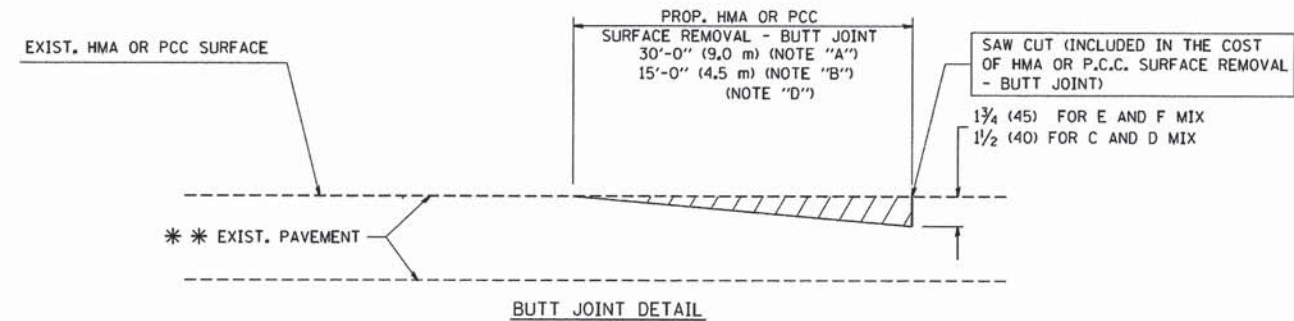


OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

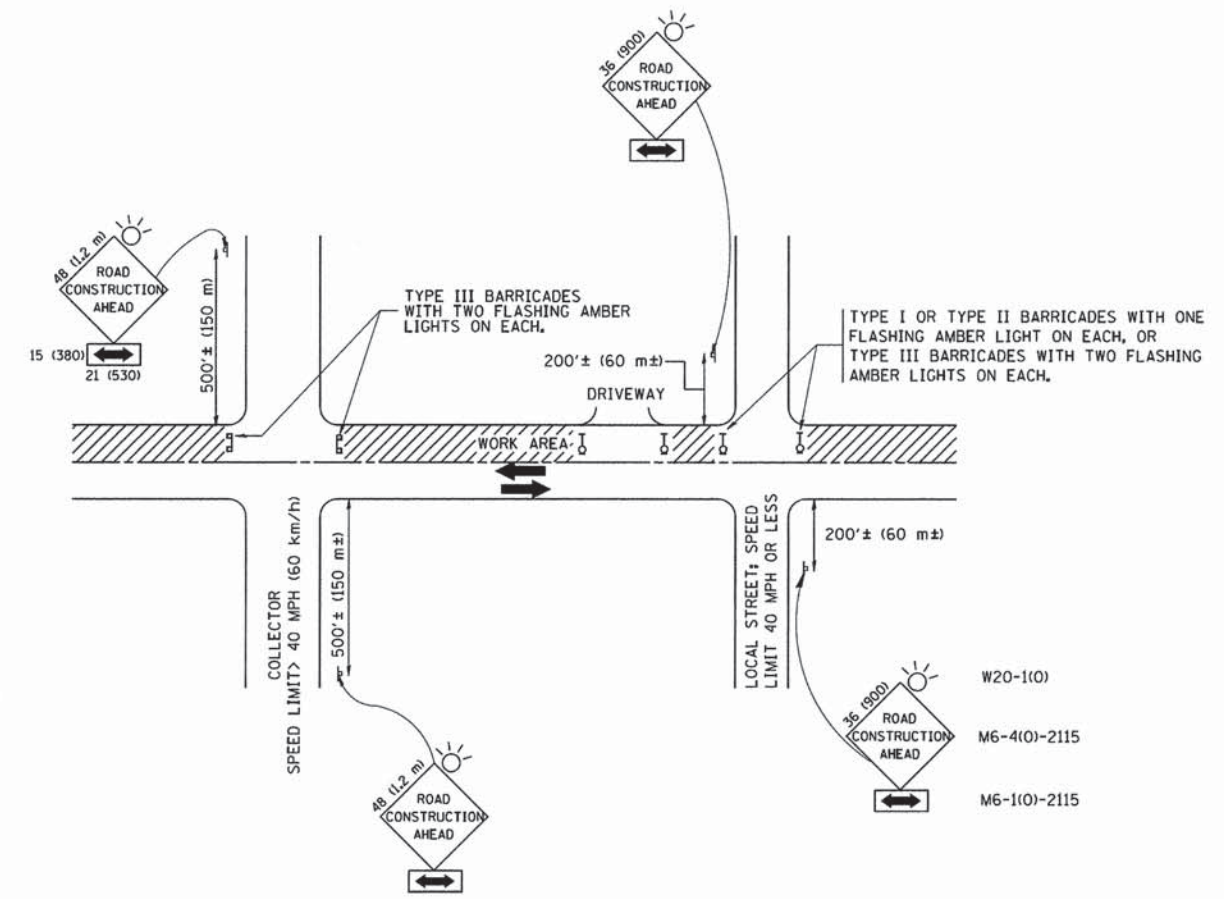
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	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.11. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1392	14-00182-00-RS	DUPAGE	31	24
BD400-05 BD32		CONTRACT NO. 61C16		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

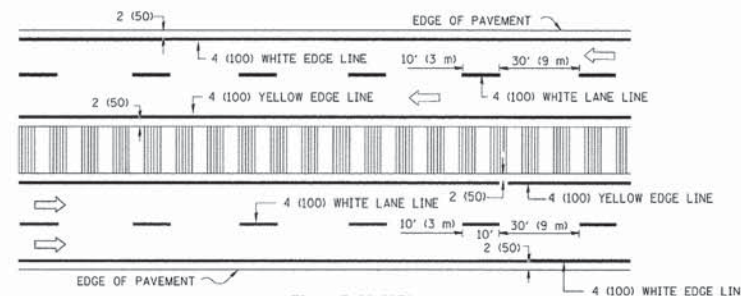
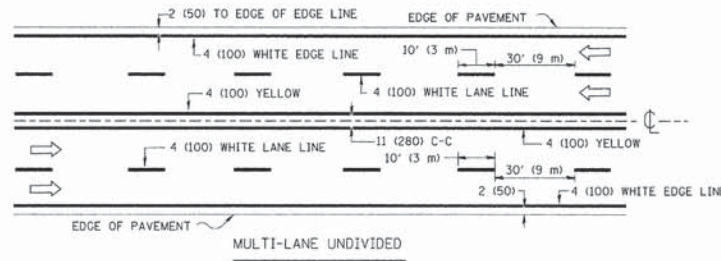
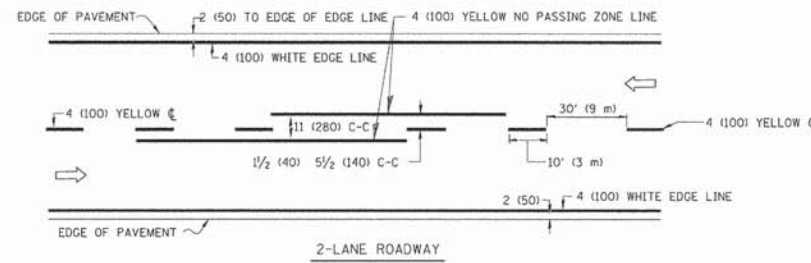
- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1,2 m x 1,2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

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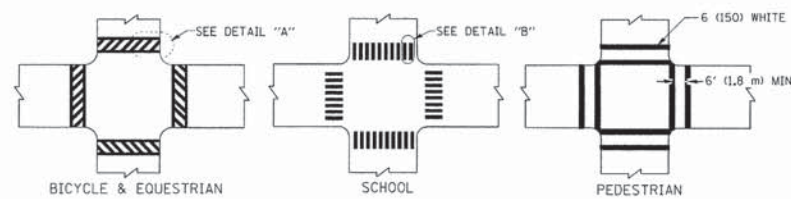
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE		1392	14-00182-00-RS	DUPAGE	31	25
SHEET NO. 1 OF 1 SHEETS		TC-10		CONTRACT NO. 61C16		
STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

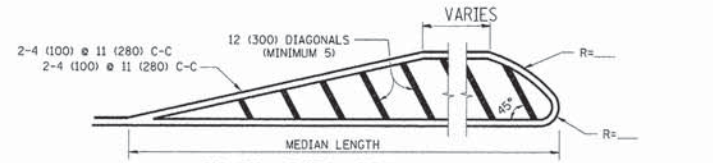
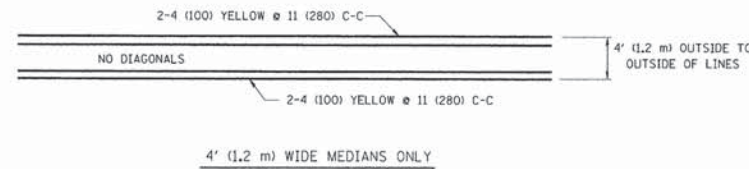


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

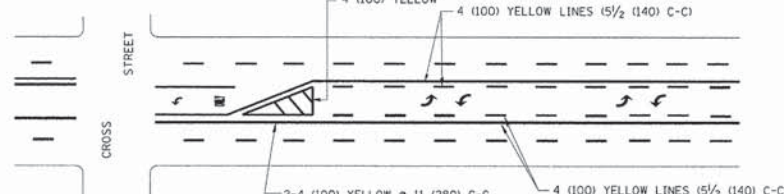
TYPICAL LANE AND EDGE LINE MARKING



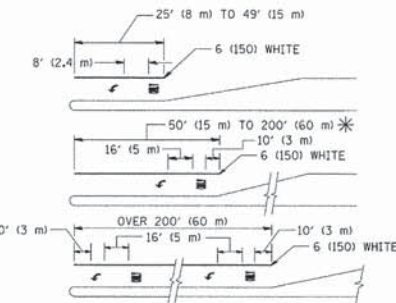
TYPICAL CROSSWALK MARKING



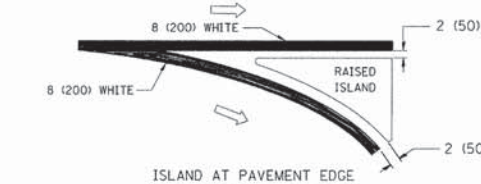
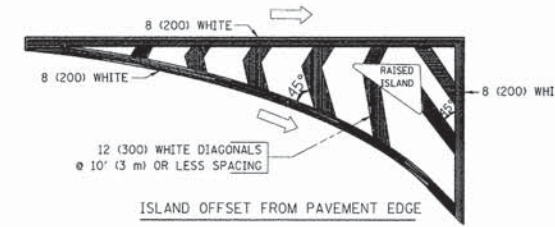
MEDIANS OVER 4' (1.2 m) WIDE



TYPICAL PAINTED MEDIAN MARKING



TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15' (4.5 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

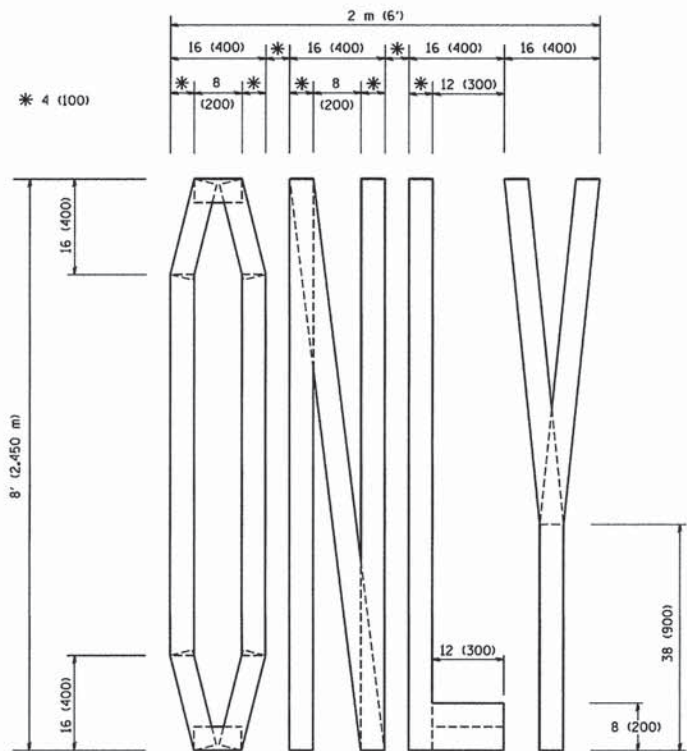
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

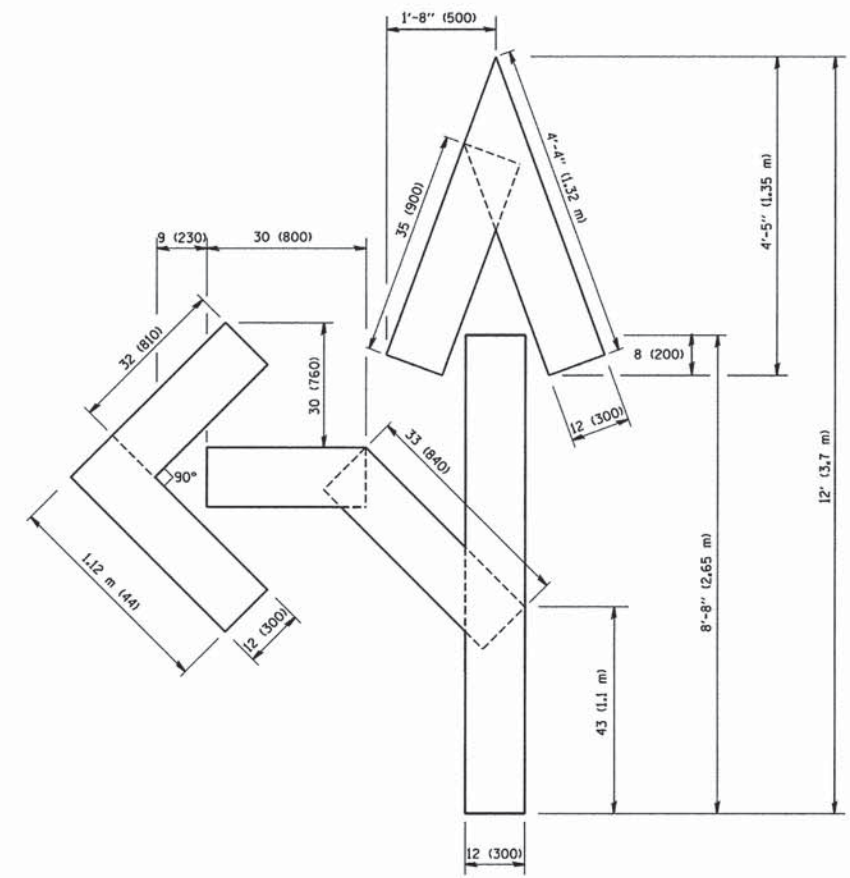
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

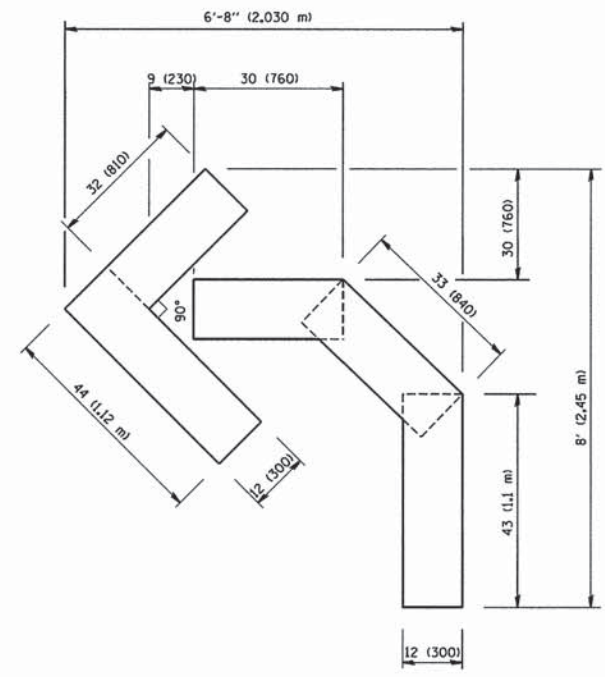
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TYPICAL PAVEMENT MARKINGS		1392	14-00182-00-RS	DUPAGE	31	26
SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		CONTRACT NO. 61C16		
STA. TO STA.		FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

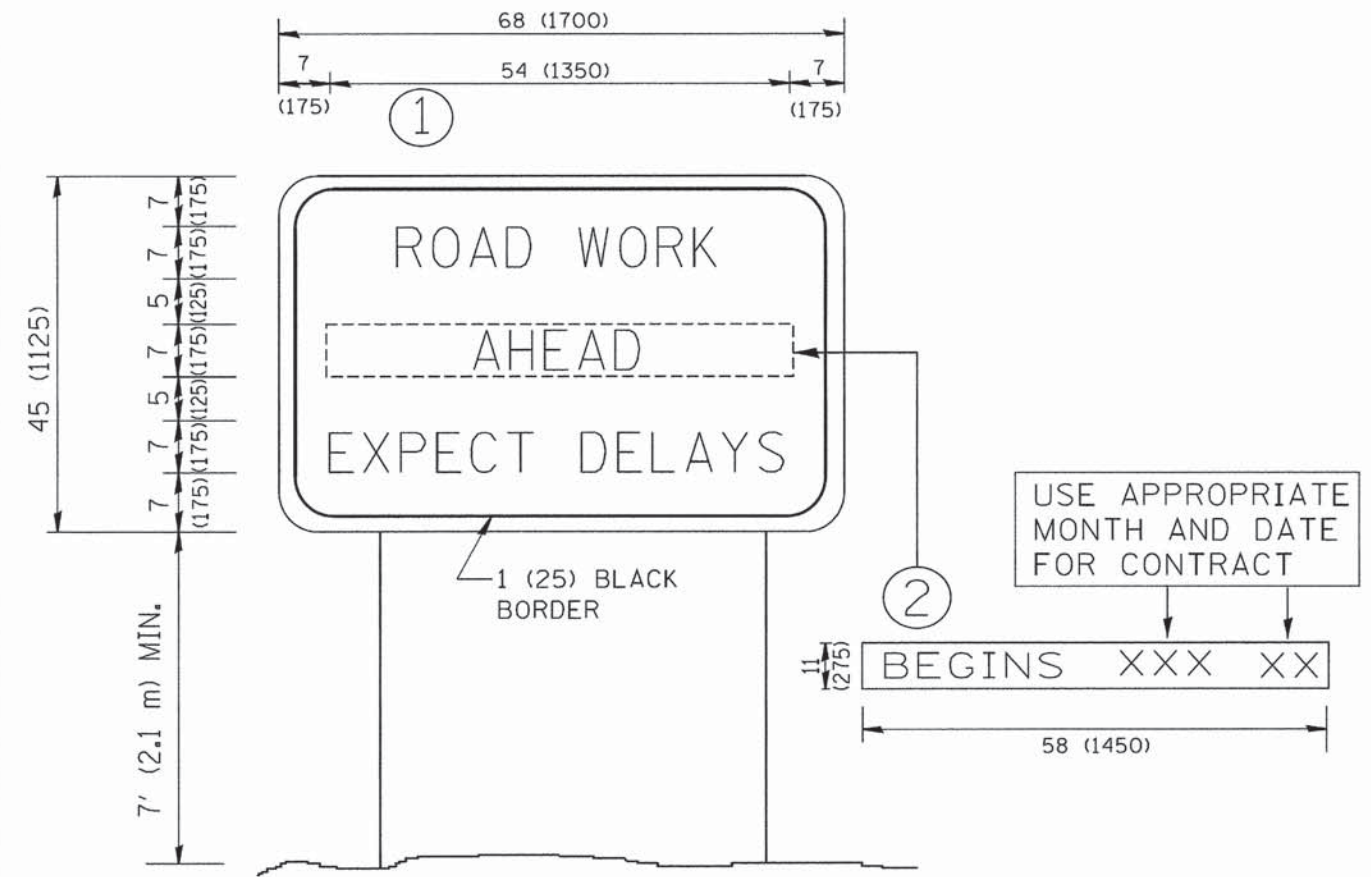
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	PLOT DATE = 1/4/2008	DATE = 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1392	14-00182-00-RS	DUPAGE	31	27
TC-16		CONTRACT NO. 61C16		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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	PLOT SCALE = 50,000' / IN.	DRAWN - -	REVISED - R. MIRS 12-11-97		SCALE: NONE	SHEET NO. 1	OF 1	SHEETS	STA.	TO STA.	TC-22	CONTRACT NO. 61C16
	PLOT DATE = 1/4/2008	CHECKED - -	REVISED - T. RAMMACHER 02-02-99		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
	DATE - -		REVISED - C. JUCIUS 01-31-07									



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME =	USER NAME = gaglianob	DESIGNED -	REVISED - C. JUCIUS 02-15-07
c:\pe_work\psidot\gaglianob\080315\to	6.dgn	DRAWN -	REVISED -
	PLOT SCALE = 50.000' / 1"	CHECKED -	REVISED -
	PLOT DATE = 12/13/2012	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

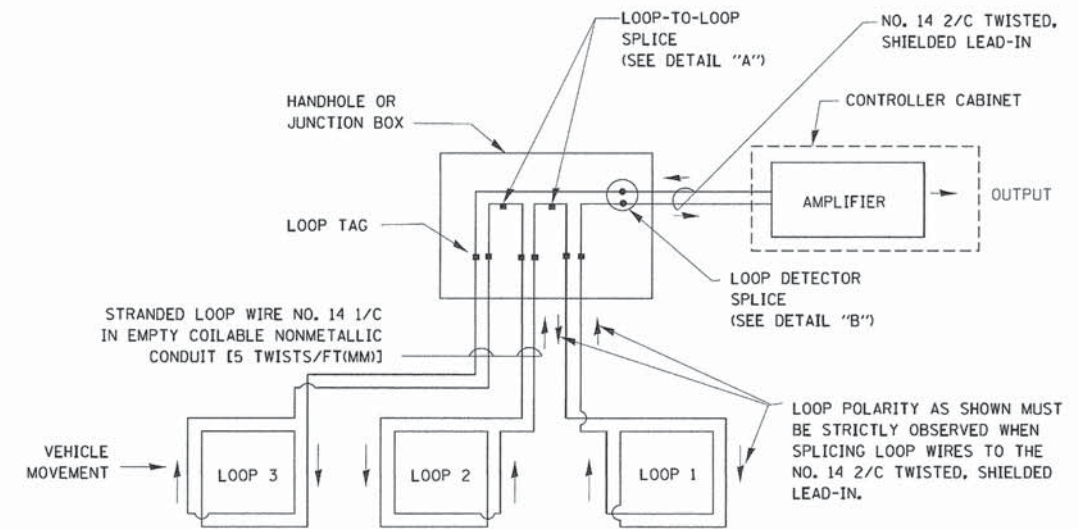
DRIVEWAY ENTRANCE SIGNING

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1392	14-00182-00-RS	DUPAGE	31	29
TC-26			CONTRACT NO. 61C16	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

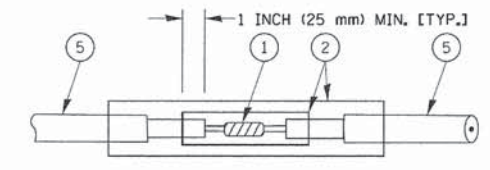
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVESHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

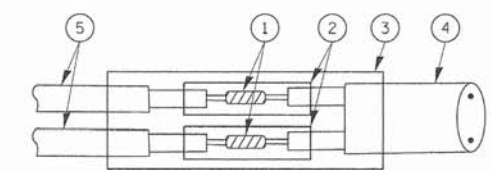


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

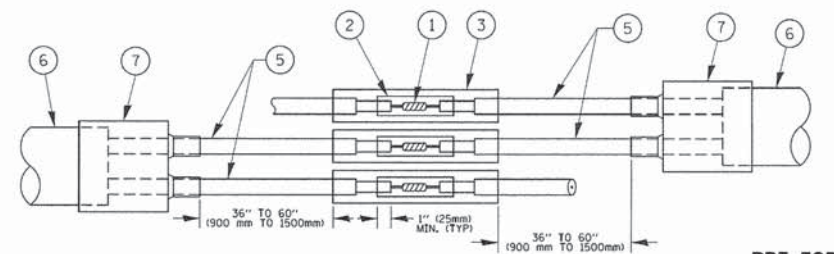


DETAIL "A"
LOOP-TO-LOOP SPLICE

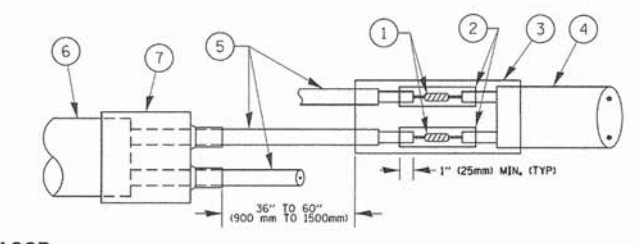


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



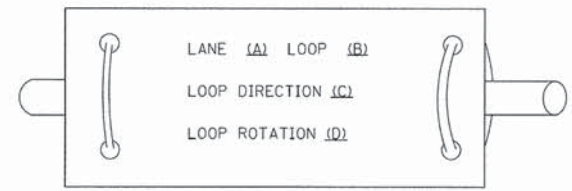
DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PREFORMED LOOP

LOOP DETECTOR SPLICE

- ① WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- ② WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- ③ WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- ④ NO. 14 2/C TWISTED, SHIELDED CABLE.
- ⑤ LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- ⑥ PREFORMED LOOP
- ⑦ XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

LOOP LEAD-IN CABLE TAG



- A. LANE #1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

FILE NAME =	USER NAME = fcoatesj	DESIGNED - DAD	REVISED - DAG 1-1-14	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ar\pwwork\pwwork\footem\j\00100315\ts05.dgn		DRAWN - BCK	REVISED -		SCALE: NONE	SHEET NO. 2 OF 7	SHEETS STA.	TO STA.	1392	14-00182-00-RS	DUPAGE	31	31
		CHECKED - DAD	REVISED -					TS-05		CONTRACT NO. 61C16			
		DATE - 10-28-09	REVISED -		FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT								