

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1477	0811 RS	COOK	28	1
FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT NO. 60H99	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

F.A.U. 1477 (PERSHING ROAD)
SECTION: 0811 RS
CENTRAL AVENUE TO IL 50 (CICERO AVENUE)
RESURFACING (3P)

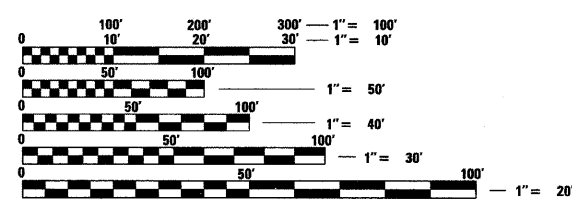
COOK COUNTY
C-91-839-09

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THESE IMPROVEMENTS ARE LOCATED WITHIN
THE TOWN OF CICERO AND VILLAGE OF STICKNEY

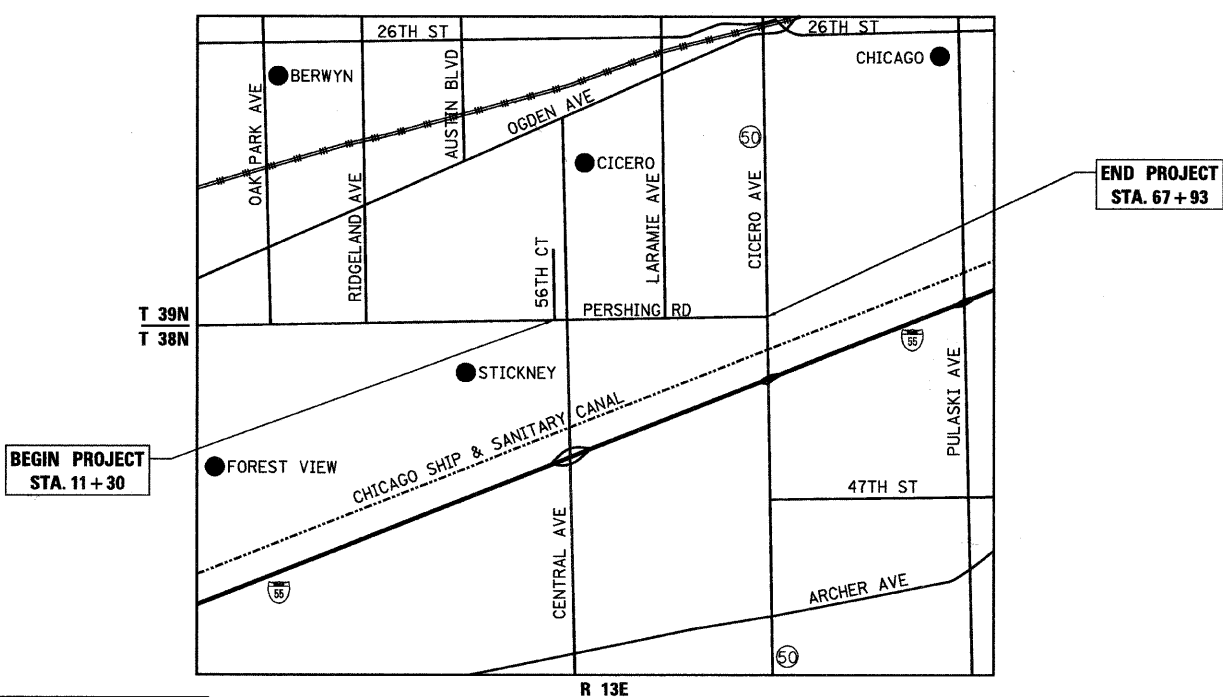
TRAFFIC DATA

2006 ADT - 13,900
POSTED SPEED LIMIT - 30 MPH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811



BEGIN PROJECT
STA. 11 + 30

END PROJECT
STA. 67 + 93

Ciorba Group, Inc.
DESIGN FIRM
REGISTRATION NUMBER
184-001016
CONSULTING ENGINEERS
SUITE 402, 5507 NORTH CUMBERLAND AVE
CHICAGO, ILLINOIS 60656 :: (773) 775-4009

CICERO & STICKNEY TOWNSHIP
LOCATION MAP
1" = 2,500'
GROSS & NET LENGTH OF PROJECT = 5,663 FT = 1.07 MI.



LOCATION OF SECTION INDICATED THUS: - [black rectangle] -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
SUBMITTED DECEMBER 8, 20 09
Diana M. D'Neefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER
January 29, 20 10
Scott E. Stitt P.E. / RD
ENGINEER OF DESIGN AND ENVIRONMENT
January 29, 20 10
Christine M. Reed RD
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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OF THE STATE OF ILLINOIS**

DISTRICT 1 DESIGN PLAN PREPARATION ENGINEER: K. ENG (847)705-4247

CONTRACT NO. 60H99

DATE: 12/7/2009
SEAL EXPIRES: 11/30/2011

INDEX OF SHEETS

SHEET NO	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES
3	SUMMARY OF QUANTITIES
4	TYPICAL SECTIONS
5-6	PROPOSED ROADWAY AND PAVEMENT MARKING PLAN
7-9	DETECTOR LOOP REPLACEMENT PLANS
10	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-8)
11	CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)
12	BUTT JOINT AND HMA TAPER DETAILS (BD-32)
13	HMA TAPER AT EDGE OF P.C.C. PAVEMENT (BD-33)
14	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)
15	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT) (TC-11)
16	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)
17	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)
18	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING (TC-16)
19	ARTERIAL ROAD INFORMATION SIGN (TC-22)
20	DISTRICT 1 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)

STATE STANDARDS

000001-05	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
442201-03	CLASS C AND D PATCHES
602601-02	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
604001-03	FRAME AND LIDS, TYPE 1
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701601-06	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701602-04	URBAN LANE CLOSURE, MULTI-LANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701701-06	URBAN LANE CLOSURE, MULTI LANE INTERSECTION
701801-04	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUTS FOR DETECTION LOOPS

GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)
- 10 FEET (3 METER) TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURB AND GUTTER AND MEDIANS IN THE FIELD UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- WHEN ARTIFICIAL LIGHTING IS USED IN NIGHT OPERATIONS THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.
- THE FOLLOWING RATES OF APPLICATION HAVE BEEN ASSUMED IN CALCULATING PLAN QUANTITIES:

BITUMINOUS MATERIALS (PRIME COAT)	0.0004 TONS/SQ YD
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE	112 LBS/SQ YD/INCH
POLYMERIZED LEVELING BINDER (MACHINE METHOD)	110 LBS/SQ YD/INCH
- THE ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISORS AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE START OF WORK.
- TWO WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS THE ENGINEER SHALL CONTACT PATRICE HARRIS, AREA TRAFFIC FIELD ENGINEER, AT (708) 597-9800.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2" (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/H) OR LESS AND 1" (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3" (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 3:1 (H:V).
- BUTT JOINTS WILL BE INSTALLED AT THE END OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT). IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- FOR PAVEMENT MARKING, REFER TO DISTRICT ONE TYPICAL MARKINGS FOR DETAILS SHOWN.
- MATCH EXISTING PAVEMENT MARKINGS AT PROJECT LIMITS AND OMISSIONS.
- ALL PATCHES OPENED ON A PARTICULAR DAY MUST BE FILLED THAT DAY TO THE TOP OF THE EXISTING PAVEMENT SURFACE.

14. IDOT TRAFFIC SIGNAL AND SYSTEM DETECTION LOOPS ARE PRESENT AT CENTRAL AVENUE, LARAMIE AVENUE, AND IL 50 (CICERO AVENUE). THE CONTRACTOR MUST NOTIFY THE IDOT AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER AT (847) 705-4139 AND THE DEPARTMENT'S ELECTRICAL MAINTENANCE CONTRACTOR PRIOR TO BEGINNING WORK, AT WHICH TIME ARRANGEMENTS WILL BE MADE TO ADJUST THE TRAFFIC CONTROLLER TIMING TO COMPENSATE FOR THE ABSENCE OF DETECTION. REPLACEMENT OF LOOPS DOES NOT REQUIRE MAINTENANCE TRANSFER, BUT DOES REQUIRE NOTIFICATION OF WORK AND INSPECTION. COORDINATION WITH THE DISTRICT SHALL BE INCLUDED WITH DETECTOR LOOP REPLACEMENT.

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PLOT DATE = 12/10/2009	DATE - 7/8/2009	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PERSHING ROAD CENTRAL AVENUE TO IL 50 (CICERO AVENUE) INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1477	0811 RS	COOK	28	2
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				CONTRACT NO. 60H99

SUMMARY OF QUANTITIES			URBAN 100% STATE	CONSTRUCTION TYPE CODE
CODE NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	ROADWAY 1000-2A
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	70	70
25200110	SODDING, SALT TOLERANT	SQ YD	70	70
35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SQ YD	5	5
40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	10	10
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	34	34
40600300	AGGREGATE (PRIME COAT)	TON	166	166
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	62	62
40600535	LEVELING BINDER (HAND METHOD), N70	TON	20	20
40600895	CONSTRUCTING TEST STRIP	EACH	2	2
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	1,870	1,870
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	1	1
40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	TON	4,100	4,100
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	10	10
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	15	15
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	400	400
44003800	MEDIAN SURFACE REMOVAL	SQ FT	210	210
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	720	720
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	150	150
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	130	130
55039700	STORM SEWERS TO BE CLEANED	FOOT	1,700	1,700
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	10	10
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	40	40
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	5	5
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	5	5
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	4	4
67100100	MOBILIZATION	L SUM	1	1

* DENOTES SPECIALTY ITEM

SUMMARY OF QUANTITIES			URBAN 100% STATE	CONSTRUCTION TYPE CODE
CODE NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	ROADWAY 1000-2A
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	6,800	6,800
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	1,875	1,875
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	19,700	19,700
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	4,500	4,500
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	300	300
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	150	150
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	750	750
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	4,200	4,200
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	625	625
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	14,200	14,200
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1,500	1,500
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	100	100
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	50	50
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	250	250
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	460	460
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	438	438
* 81400115	HANDHOLE TO BE ADJUSTED	EACH	1	1
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	900	900
X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	51	51
40600826	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	1,700	1,700
X4400100	PORTLAND CEMENT CONCRETE REMOVAL (VARIABLE DEPTH)	SQ YD	7,300	7,300
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	110	110

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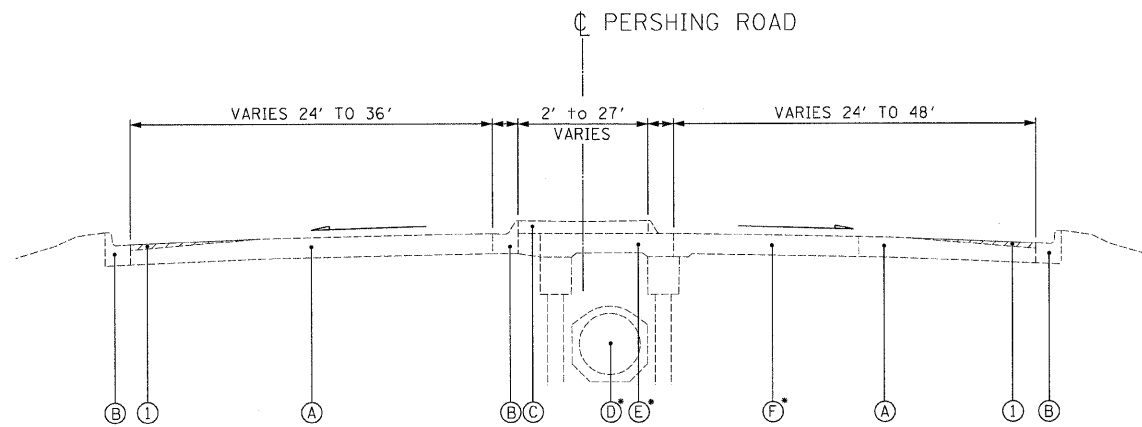
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

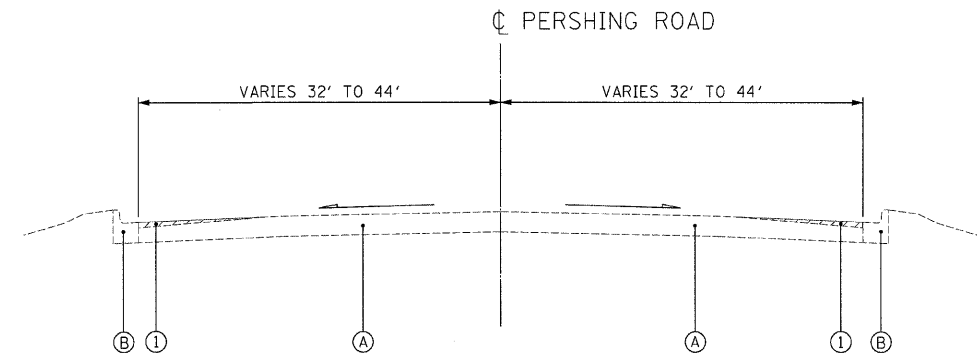
PERSHING ROAD
CENTRAL AVENUE TO IL 50 (CICERO AVENUE)
SUMMARY OF QUANTITIES

SCALE: SHEET NO. OF SHEETS STA. TO STA.

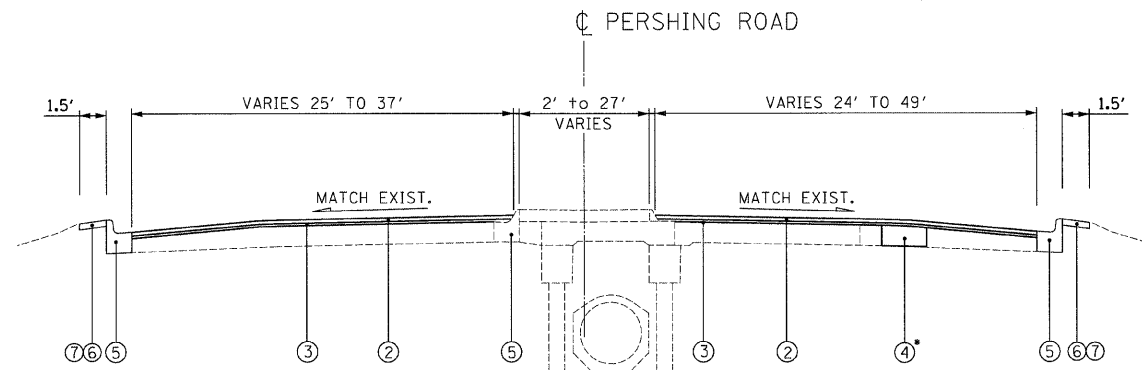
F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1477	0811 RS	COOK	28	3
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60H99	



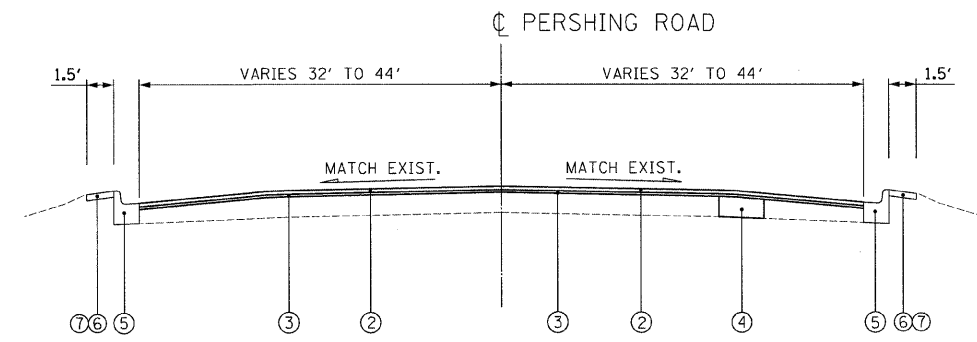
EXISTING TYPICAL SECTION
STA. 11+30 TO STA. 49+83*
STA. 64+20 TO STA. 67+93



EXISTING TYPICAL SECTION
STA. 49+83 TO STA. 64+20



PROPOSED TYPICAL SECTION
STA. 11+30 TO STA. 49+83*
STA. 64+20 TO STA. 67+93



PROPOSED TYPICAL SECTION
STA. 49+83 TO STA. 64+20

EXISTING CONDITIONS:

- (A) PORTLAND CEMENT CONCRETE PAVEMENT, 10"
- (B) COMBINATION CONCRETE CURB AND GUTTER
- (C) RAISED CONCRETE MEDIAN
- (D) UNREINFORCED 10'-6" SEWER
- (E) DRY LAND BRIDGE
- (F) APPROACH SLAB

PROPOSED IMPROVEMENTS:

- (1) PORTLAND CEMENT CONCRETE REMOVAL (VARIABLE DEPTH) (SEE SHEET 21)
- (2) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1 3/4"
- (3) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- (4) CLASS D PATCHES, 10" (DETERMINED BY ENGINEER IN FIELD)
- (5) COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY ENGINEER)
- (6) TOPSOIL FURNISH AND PLACE, 4"
- (7) SODDING, SALT TOLERANT

A QUANTITY FOR LEVELING BINDER (HAND METHOD) HAS BEEN PROVIDED FOR USE AT ENTRANCES, AROUND HAND HOLES, PRIVATE UTILITY STRUCTURE FRAMES AND ANY OTHER STRUCTURE FRAMES THAT ARE NOT ABLE TO BE LOWERED UNDER THE ITEM "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)" AFTER GRINDING OF THE EXISTING PAVEMENT.

*** NOTE TO CONTRACTOR**

THERE IS AN EXISTING DRY LAND BRIDGE OVER A 10'-6" SANITARY SEWER PIPE THAT RUNS ALONG PERSHING ROAD FROM CENTRAL AVENUE TO LARAMIE AVE UNDER THE MEDIAN (APPROXIMATE STA. 15+00 TO STA. 41+73). THE BRIDGE AND THE APPROACH SLAB(S) ARE LOCATED IN THE MEDIAN AND THE INSIDE EASTBOUND LANE OF PERSHING ROAD. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING THIS BRIDGE WITHIN THE PROJECT LIMITS PRIOR TO PATCHING PERSHING ROAD. NO PATCHING SHOULD BE DONE WITHIN THE LIMITS OF THE BRIDGE OR THE APPROACH SLABS. SEE THE EXISTING BRIDGE PLANS INCLUDED IN THIS PLANSET FOR FURTHER INFORMATION.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS CHART

OPERATIONS	MIXTURE TYPE	AIR VOIDS @ Ndes
ROADWAY RESURFACING	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1 3/4"	4% @ 90 GYR
	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"	4% @ 50 GYR
INCIDENTAL USE	LEVELING BINDER (HAND METHOD), N70	4% @ 70 GYR
PAVEMENT PATCHING	CLASS D PATCHES, 10" (HMA BINDER IL-19 MM)	4% @ 70 GYR
DRIVES BEHIND CURB	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 (IL-9.5MM) 2" THICKNESS	4% @ 50 GYR
	HOT-MIX ASPHALT BASE COURSE, 8" (HMA BINDER IL-19 MM)	4% @ 50 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

FOR "AC TYPE" AND "PERCENT RAP" SEE DISTRICT ONE SPECIAL PROVISIONS

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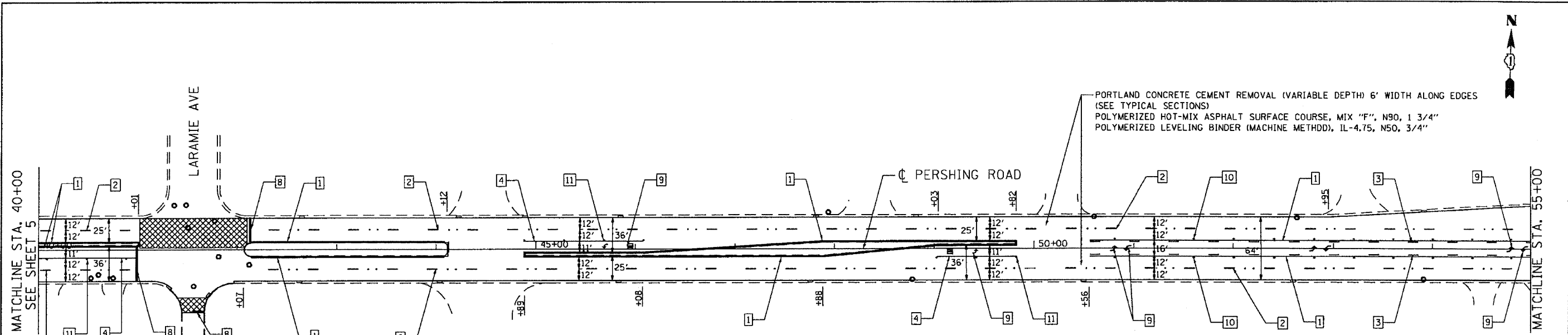
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	DATE - 7/8/2009	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PERSHING ROAD
CENTRAL AVENUE TO IL 50 (CICERO AVENUE)
TYPICAL SECTIONS**

SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE. 1477	SECTION 0811 RS	COUNTY COOK	TOTAL SHEETS 28	SHEET NO. 4
CONTRACT NO. 60H99				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

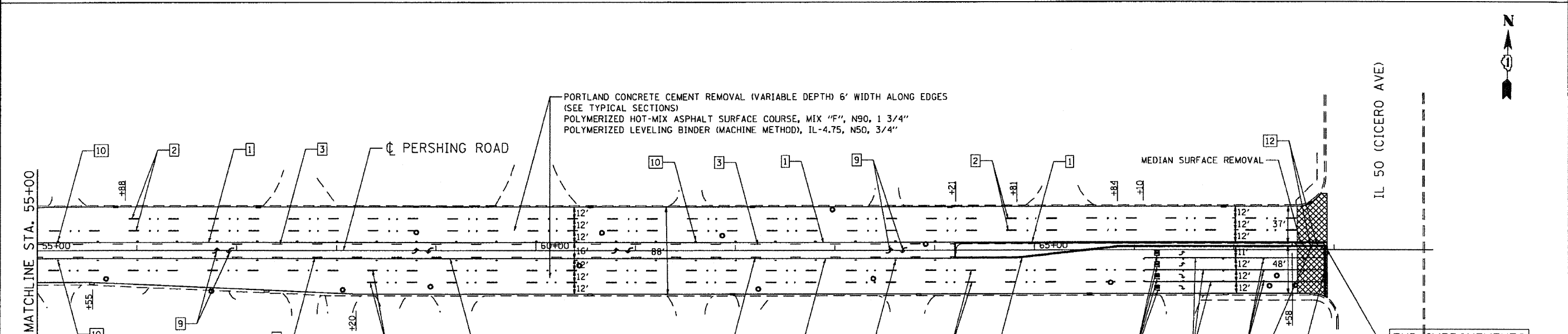


PORTLAND CONCRETE CEMENT REMOVAL (VARIABLE DEPTH) 6' WIDTH ALONG EDGES
(SEE TYPICAL SECTIONS)
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1 3/4"
POLYMERIZED LEVELING BINDER (MACHINE METHDD), IL-4.75, N50, 3/4"

LEGEND:

- 1 THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW SOLID LINE)
- 2 THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE LANE LINE - 10' DASH, 30' SKIP) WITH RAISED REFLECTIVE PAVEMENT MARKERS (2 ONE-WAY CRYSTAL MARKER) (80' C-C)
- 3 THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW LANE LINE - 10' DASH, 30' SKIP)
- 4 THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE SOLID LINE)
- 5 THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE LANE LINE - 2' DASH, 6' SKIP)
- 6 THERMOPLASTIC PAVEMENT MARKING - LINE 8" (WHITE SOLID LINE)
- 7 THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE DIAGONAL LINE)
- 8 THERMOPLASTIC PAVEMENT MARKING - LINE 24" (WHITE STOP LINE)
- 9 THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS (TYP.)
- 10 RAISED REFLECTIVE PAVEMENT MARKERS (TWO-WAY AMBER MARKER) (40' C-C)
- 11 RAISED REFLECTIVE PAVEMENT MARKERS (ONE-WAY CRYSTAL MARKER) (40' C-C)
- 12 THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW DIAGONAL LINE)

PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT



PORTLAND CONCRETE CEMENT REMOVAL (VARIABLE DEPTH) 6' WIDTH ALONG EDGES
(SEE TYPICAL SECTIONS)
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1 3/4"
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

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PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT

END IMPROVEMENTS
STA. 67+93
MATCH EXISTING
PAVEMENT MARKINGS

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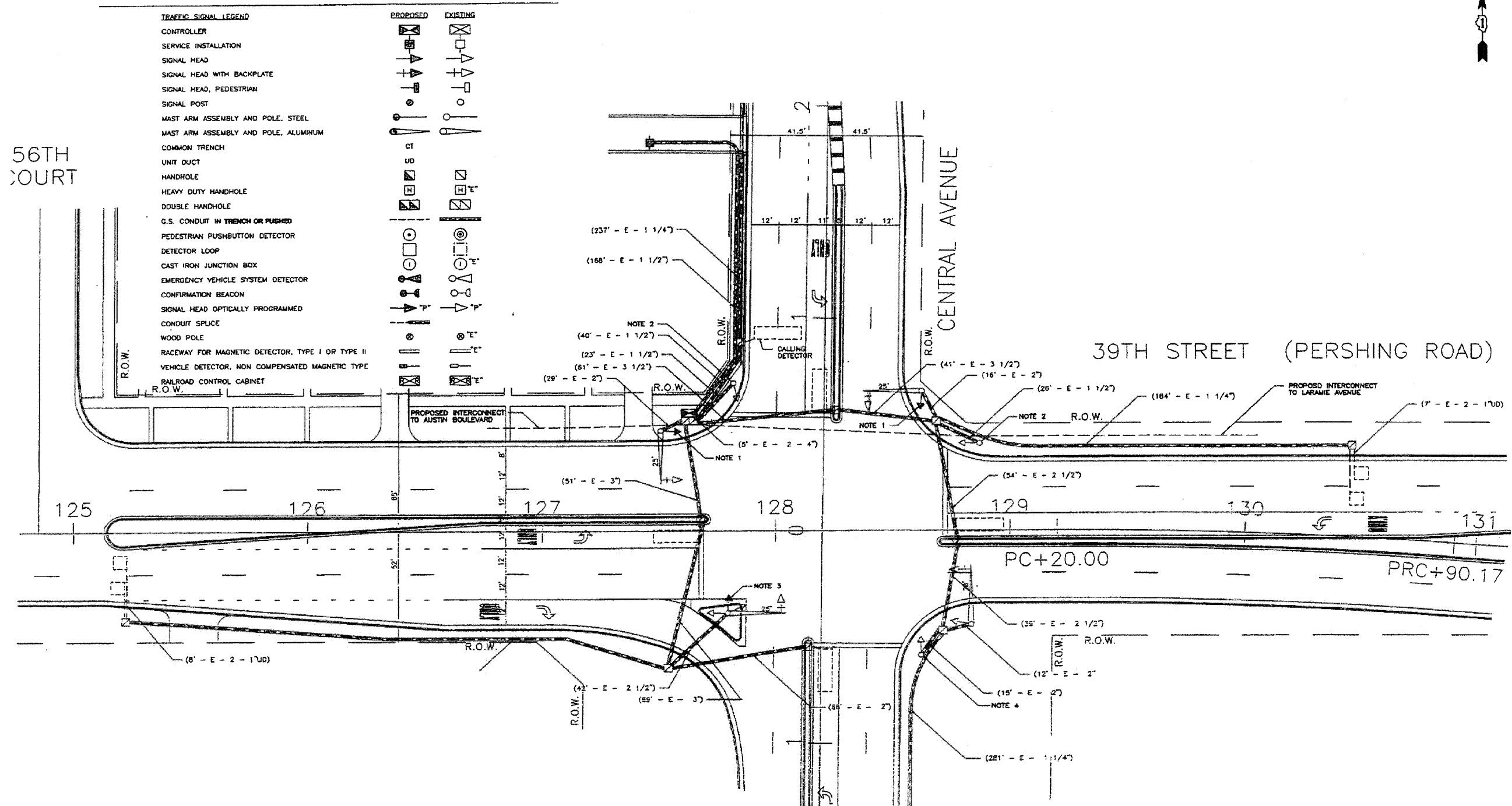
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PERSHING ROAD
CENTRAL AVENUE TO IL 50 (CICERO AVENUE)
PROPOSED ROADWAY AND PAVEMENT MARKING PLAN**

SCALE: 1"=50' SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1477	0811 RS	COOK	28	6
CONTRACT NO. 60H99			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	



REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN PROJECT LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
86600600	258	FOOT	DETECTOR LOOP REPLACEMENT

FILE NAME: J:\Projects\39th\39th.dwg; 31/10/2009 10:30:00 AM; 31/10/2009 10:30:00 AM

Ciorba Group, Inc.
CONSULTING ENGINEERS
5507 North Cumberland Avenue, Suite 402
Chicago, Illinois 60658
Tel. 773.775.4009 Fax 773.775.4014

USER NAME = wlaneaster	DESIGNED - MWR	REVISED -
PLOT SCALE = 2.0000' / IN.	DRAWN - MWR	REVISED -
PLOT DATE = 12/4/2009	CHECKED - SNS	REVISED -
	DATE - 7/8/2009	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

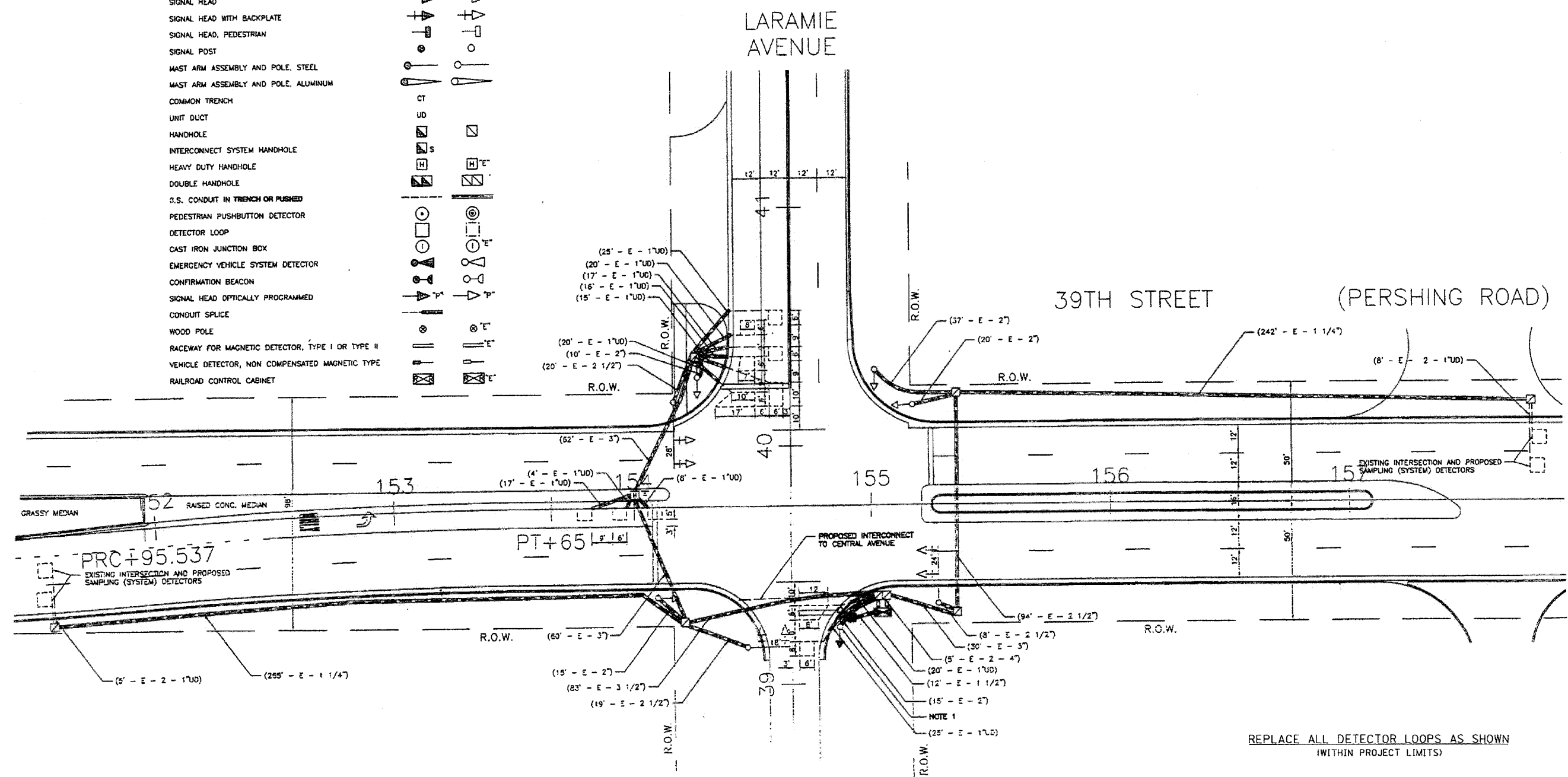
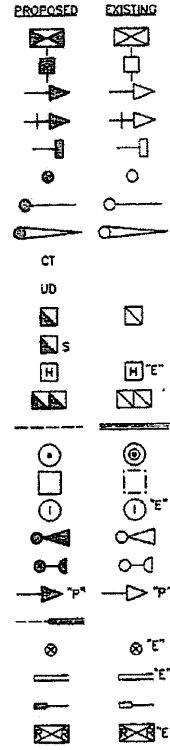
**PERSHING ROAD
CENTRAL AVENUE TO IL 50 (CICERO AVENUE)
DETECTOR LOOP REPLACEMENT PLAN**

SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE. 1477	SECTION 0811 RS	COUNTY COOK	TOTAL SHEETS 28	SHEET NO. 7
FED. ROAD DIST. NO. 1 ILLINOIS			FED. AID PROJECT CONTRACT NO. 60H99	



- TRAFFIC SIGNAL LEGEND**
- CONTROLLER
 - SERVICE INSTALLATION
 - SIGNAL HEAD
 - SIGNAL HEAD WITH BACKPLATE
 - SIGNAL HEAD, PEDESTRIAN
 - SIGNAL POST
 - MAST ARM ASSEMBLY AND POLE, STEEL
 - MAST ARM ASSEMBLY AND POLE, ALUMINUM
 - COMMON TRENCH
 - UNIT DUCT
 - HANDHOLE
 - INTERCONNECT SYSTEM HANDHOLE
 - HEAVY DUTY HANDHOLE
 - DOUBLE HANDHOLE
 - U.S. CONDUIT IN TRENCH OR PUSHED
 - PEDESTRIAN PUSHBUTTON DETECTOR
 - DETECTOR LOOP
 - CAST IRON JUNCTION BOX
 - EMERGENCY VEHICLE SYSTEM DETECTOR
 - CONFIRMATION BEACON
 - SIGNAL HEAD OPTICALLY PROGRAMMED
 - CONDUIT SPLICE
 - WOOD POLE
 - RAWEAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II
 - VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE
 - RAILROAD CONTROL CABINET



CODE NO.	QUANTITY	UNIT	ITEM
86600600	262	FOOT	DETECTOR LOOP REPLACEMENT

FILE NAME: c:\p\proj\33881\33881\33881\SIGNAL\33881_31-1.dwg

Ciorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60655
 Tel. 773.775.4009 Fax 773.775.4014

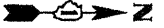
USER NAME: wjancoster	DESIGNED: MWR	REVISED: -
PLOT SCALE: 2.0000' / IN.	DRAWN: MWR	REVISED: -
PLOT DATE: 12/4/2009	CHECKED: SNS	REVISED: -
	DATE: 7/8/2009	REVISED: -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

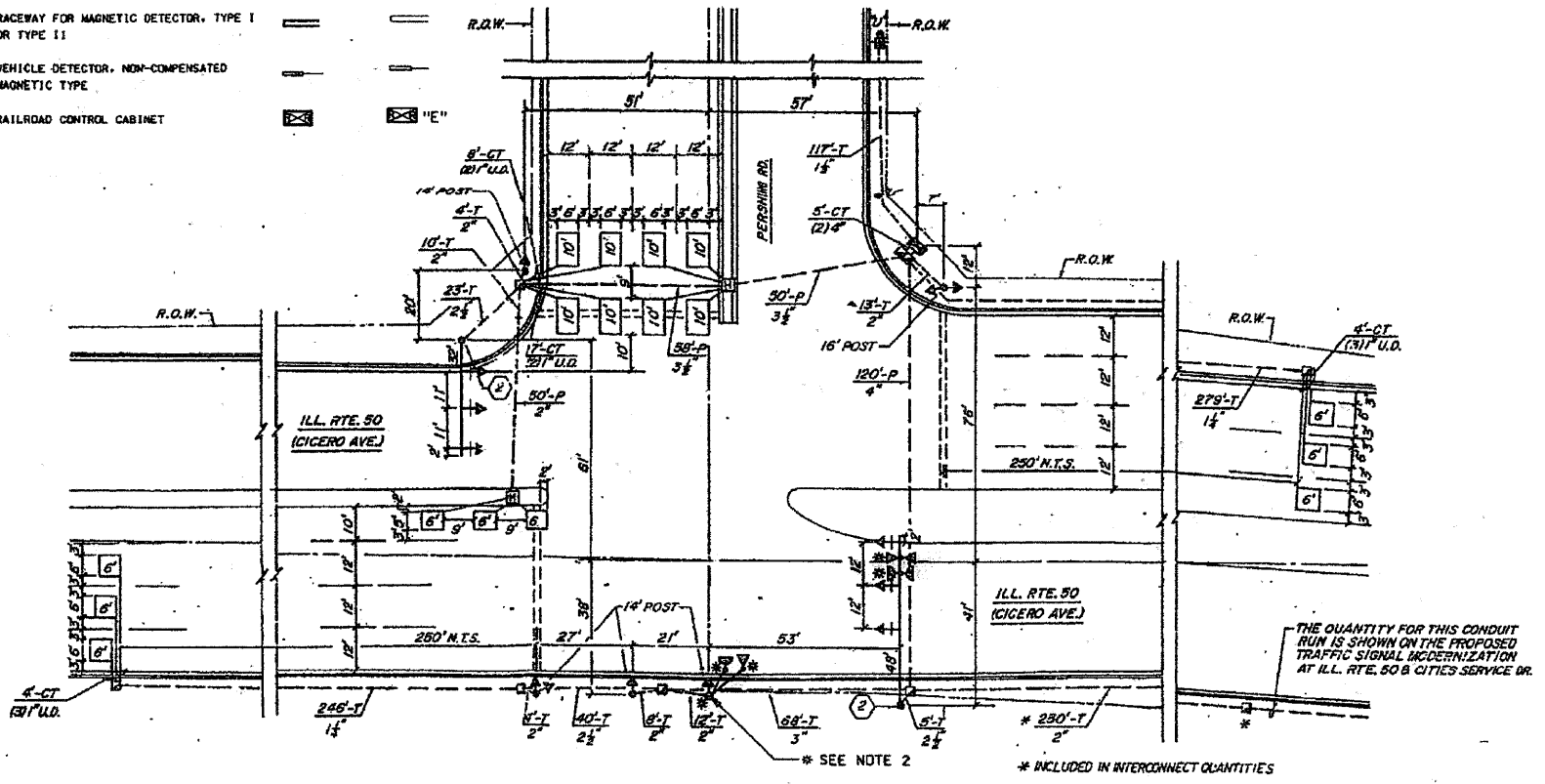
**PERSHING ROAD
 CENTRAL AVENUE TO IL 50 (CICERO AVENUE)
 DETECTOR LOOP REPLACEMENT PLAN**

SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE. 1477	SECTION 0B11 RS	COUNTY COOK	TOTAL SHEETS 28	SHEET NO. 8
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60H99	



TRAFFIC SIGNAL LEGEND			
	PROPOSED	EXISTING	
CONTROLLER	[Symbol]	[Symbol]	C.S. CONDUIT IN TRENCH OR PUSHED
SERVICE INSTALLATION	[Symbol]	[Symbol]	PEDESTRIAN PUSHBUTTON DETECTOR
SIGNAL HEAD	[Symbol]	[Symbol]	DETECTOR LOOP
SIGNAL HEAD WITH BACKPLATE	[Symbol]	[Symbol]	CAST IRON JUNCTION BOX
SIGNAL HEAD, PEDESTRIAN	[Symbol]	[Symbol]	EMERGENCY VEHICLE SYSTEM DETECTOR
SIGNAL POST	[Symbol]	[Symbol]	CONFIRMATION BEACON
MAST ARM ASSEMBLY AND POLE, STEEL	[Symbol]	[Symbol]	SIGNAL HEAD OPTICALLY PROGRAMMED
MAST ARM ASSEMBLY AND POLE, ALUMINUM	[Symbol]	[Symbol]	CONDUIT SPLICE
COMMON TRENCH	CT		WOOD POLE
UNIT DUCT	UD		RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II
HANDHOLE	[Symbol]	[Symbol]	VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE
HEAVY DUTY HANDHOLE	[Symbol]	[Symbol]	RAILROAD CONTROL CABINET
DOUBLE HANDHOLE	[Symbol]	[Symbol]	



REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN PROJECT LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
86600600	340	FOOT	DETECTOR LOOP REPLACEMENT

FILE NAME: c:\p\proj\3368\3368_31\dwg\3368_31-Comp483.dwg

Ciorba Group, Inc.
CONSULTING ENGINEERS
5507 North Cumberland Avenue, Suite 402
Chicago, Illinois 60658
Tel. 773.775.4009 Fax 773.775.4014

USER NAME = *lancoaster	DESIGNED - MWR	REVISED -
	DRAWN - MWR	REVISED -
PLOT SCALE = 2.0000" / IN.	CHECKED - SNS	REVISED -
PLOT DATE = 12/4/2009	DATE = 7/8/2009	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PERSHING ROAD
CENTRAL AVENUE TO ILL 50 (CICERO AVENUE)
DETECTOR LOOP REPLACEMENT PLAN**

SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE. 1477	SECTION 0811 RS	COUNTY CODK	TOTAL SHEETS 28	SHEET NO. 9
FED. ROAD DIST. NO. 1 ILLINOIS			FED. AIO PROJECT	
			CONTRACT NO. 60H99	

INDEX OF SHEETS ON SHEET NO. 2

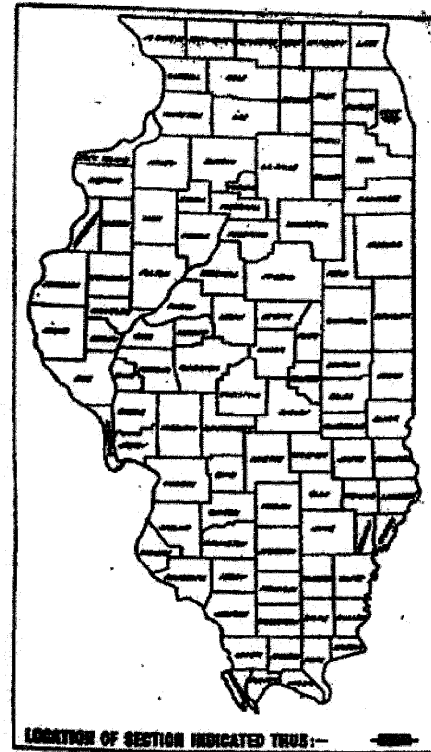
Pushing Rd
Dryland Bidge
&
MWRD Sanitary Sewer
Clearo to Central Ave

SECTION	SECTION	SECTION	SECTION
98	081-R	COOK	88
P-80-088-70			

F.A. ROUTE 98 - SECTION 081-R - COOK COUNTY

C-01-180-72 GRADING, PAVING & DRAINAGE

GROSS LENGTH - 6,213.00 LIN. FT. OR 0.89 MILES
NET LENGTH - 6,203.70 LIN. FT. OR 0.89 MILES

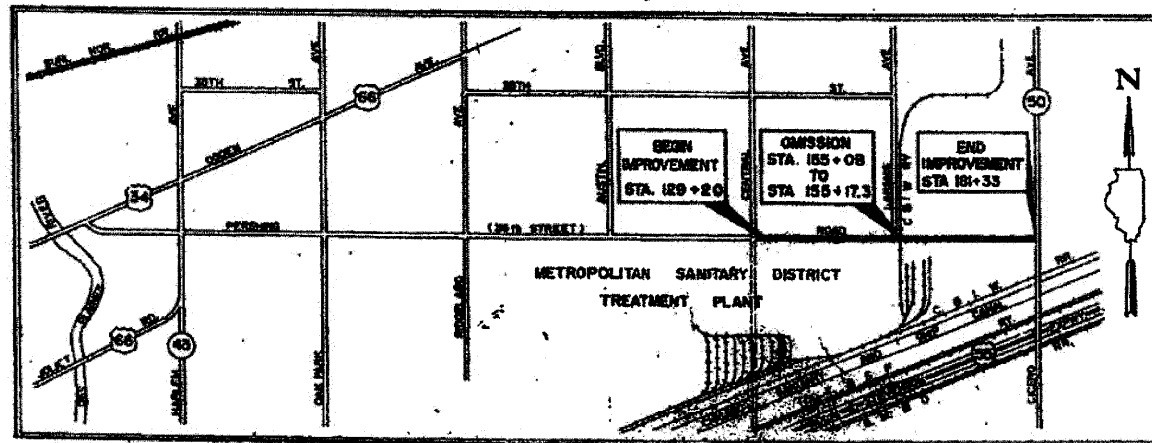


INFORMATION ONLY

OMISSION
STATION 155+08 TO STATION 155+123

DESIGN DESIGNATION
1,270 (81) TS - 2 - 2.87 (PCC-20)

LOCATION MAP



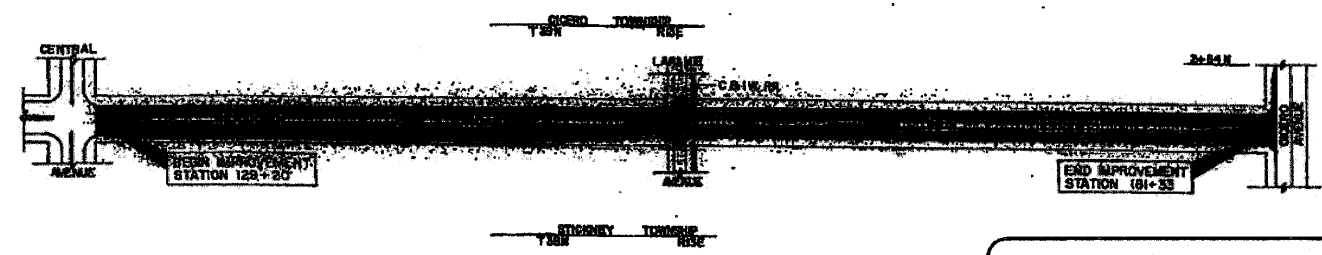
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

5-11 1972
[Signature]
[Signature]
[Signature]
[Signature]
[Signature]

CONTRACT NO. 8Q148

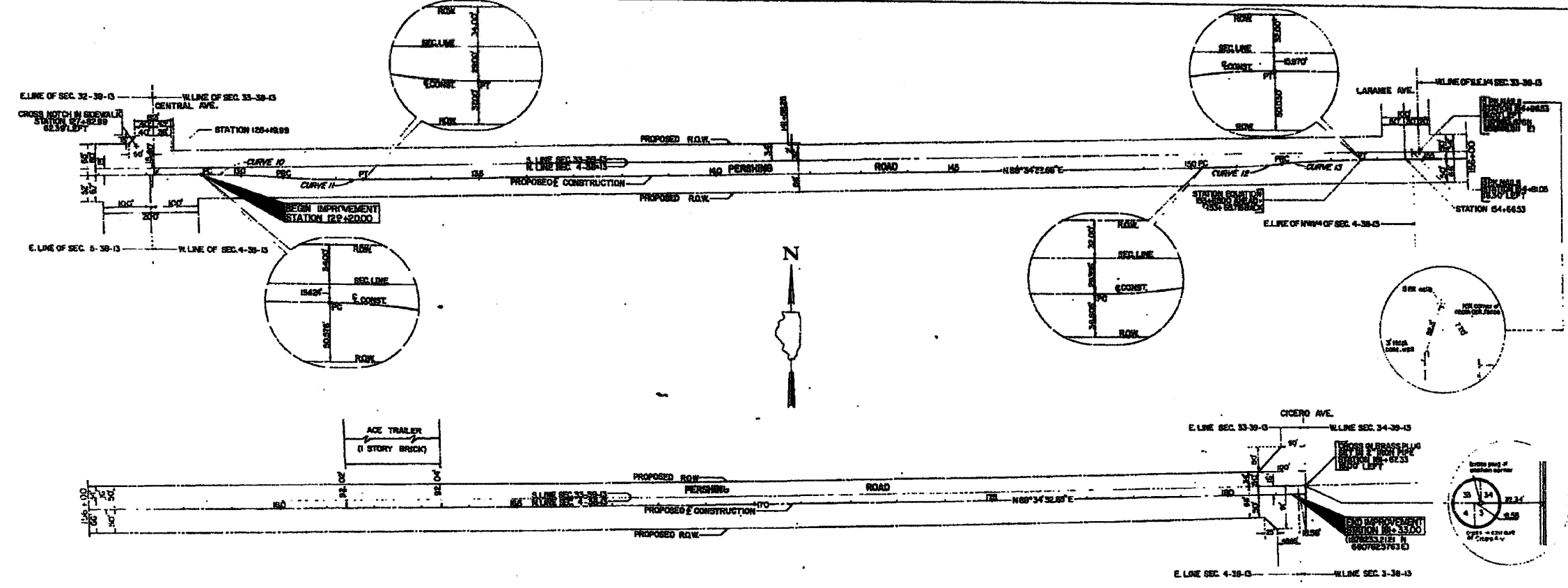
COOK COUNTY SECTION 081-R EARLY 88

NO.	DATE	BY	CHKD.	APP'D.
58	0811-R	COOK	58	5
129+20.00		131+33.00		
BY: [Signature]				



INFORMATION ONLY

GENERAL PLAN OF IMPROVEMENT
1" = 300'



— CURVE 10 —

PC STATION	129+20.00 (6780356402N, 65525088E)
PT STATION	130+90.179 (6780356402N, 65525088E)
Δ	4° 32' 32.5"
D	2' 40.20'
R	244.116'
T	85.134'
L	170.179'

— CURVE 11 —

PC STATION	130+90.179
PT STATION	132+60.358 (6780356402N, 65525088E)
Δ	4° 32' 32.5"
D	2' 40.20'
R	244.116'
T	85.134'
L	170.179'

— CURVE 12 —

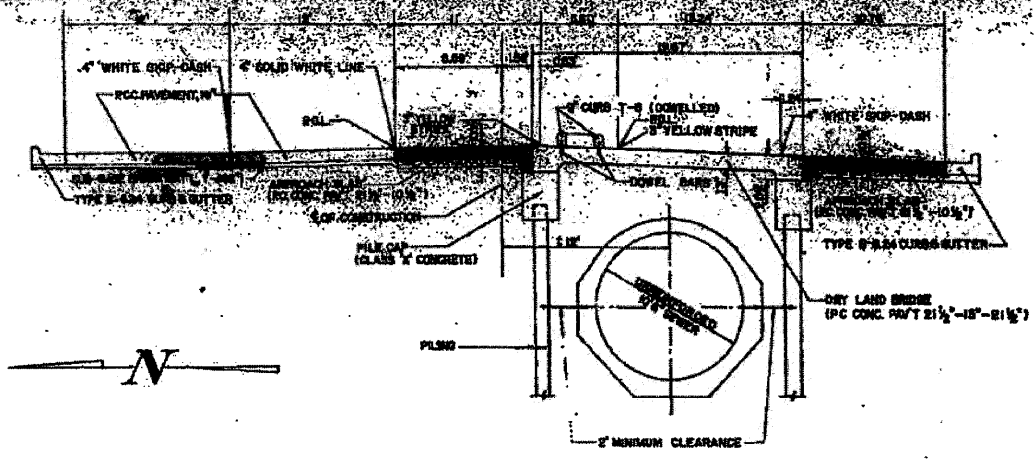
PC STATION	130+90.179 (6780356402N, 65525088E)
PT STATION	131+91.537 (6780356402N, 65525088E)
Δ	4° 32' 32.5"
D	2' 40.20'
R	244.116'
T	85.134'
L	170.179'

— CURVE 13 —

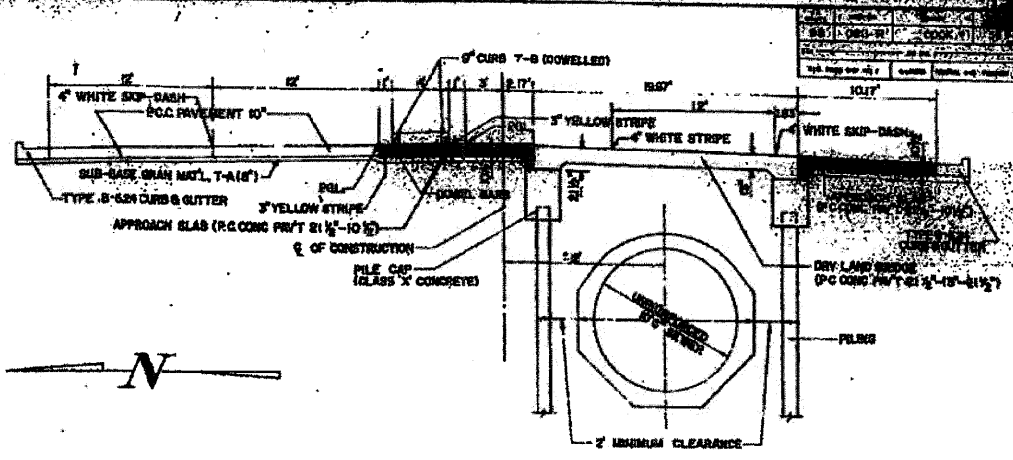
PC STATION	131+91.537
PT STATION	133+61.716 (6780356402N, 65525088E)
Δ	4° 32' 32.5"
D	2' 40.20'
R	244.116'
T	85.134'
L	170.179'

RIGHT OF WAY AND ALIGNMENT
1" = 100'

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
**PERSHING ROAD
 GENERAL PLAN
 OF IMPROVEMENT
 AND
 RIGHT OF WAY,
 ALIGNMENT**
 SCALE: AS SHOWN
 DATE: _____ DRAWN BY: LK BRM
 CHECKED BY: WGN



SECTION A-A



SECTION D-D

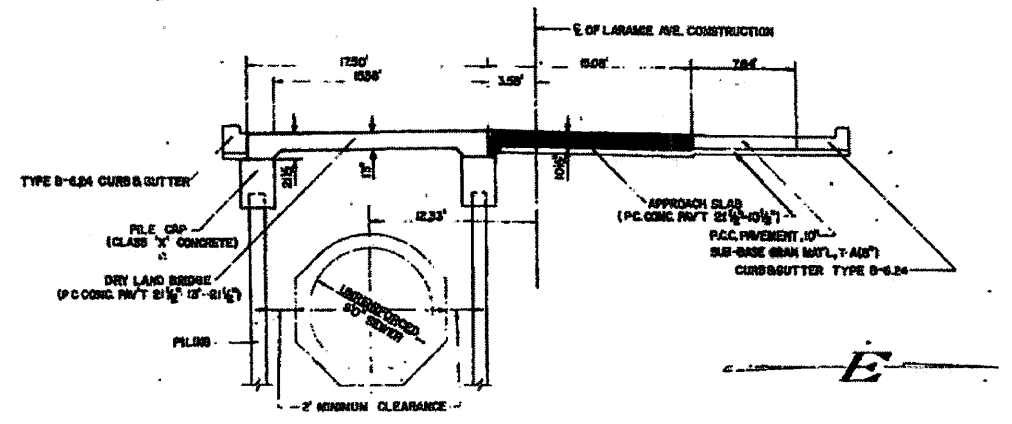
PILE DATA

	Sta. 129+20.00 to 132+60.36 Central Ave.	Sta. 150+85.34 to 154+76.50 Laramie Ave.
Type	Steel HP 10 x 42	HP 10 x 42
Capacity	45 Tons	45 Tons
Est. Length	43 Ft.	25 Ft.
No. Req'd	56	83
** Test Pile	1	3
* Pre-Core	18 Ft.	14 Ft.

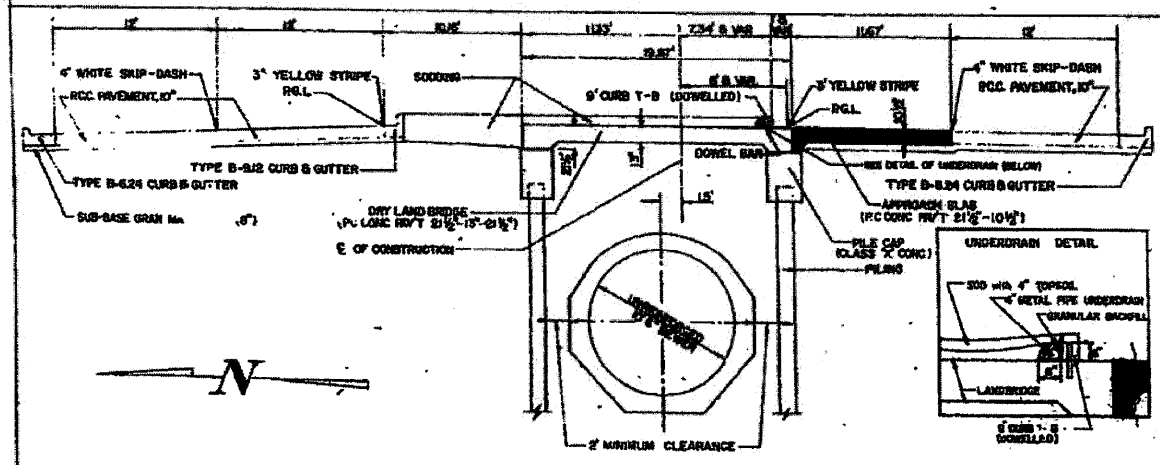
* These lengths are approximate. Piles shall be pre-cored thru the fill material or at least one (1) foot below the invert elevation of the sewer, which ever is deeper. Piles shall then be driven to the req'd capacity.

** In permanent location as directed by the Engineer.

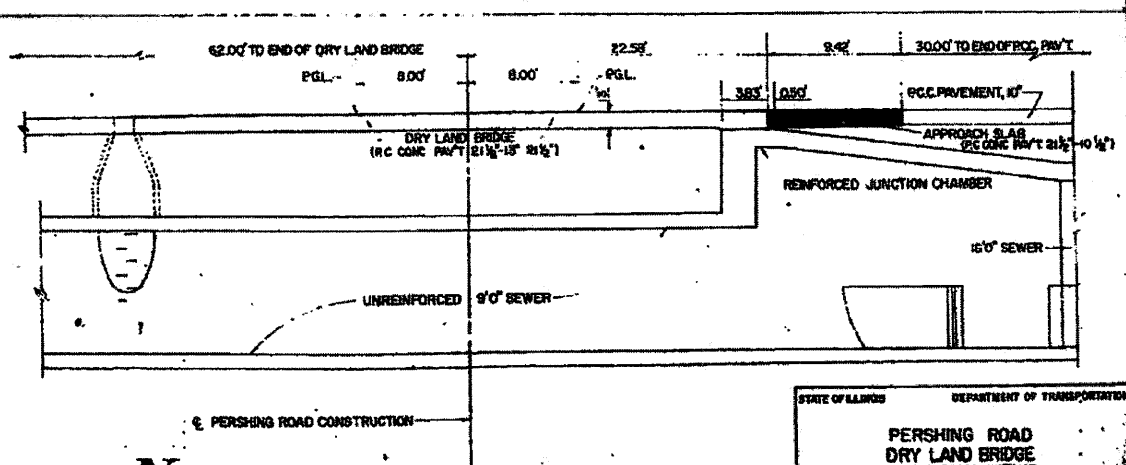
INFORMATION ONLY



SECTION E-E



SECTION C-C



SECTION F-F

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

**PERSHING ROAD
DRY LAND BRIDGE
SECTIONAL VIEWS**

SCALE 1"=5'
DATE _____ DRAWN BY: L.S.
CHECKED BY: J.M.

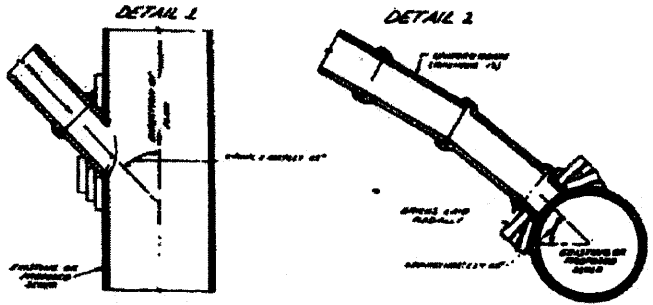
SHEET 23 OF 55

AS REVISED 8-30-78 L.W.

AS REVISED



Note: For Pipe Data see s/n. # 23.



DESCRIPTION.
 All pipe, manholes and inlet connections shall be made in accordance with the specifications of the Standard Specifications for Sewerage and Sanitation, 1938 Edition, as amended, and the cover shall be made in accordance with the specifications of the Standard Specifications for Sewerage and Sanitation, 1938 Edition, as amended, and the cover shall be made in accordance with the specifications of the Standard Specifications for Sewerage and Sanitation, 1938 Edition, as amended.

DETAIL 1
 SECTIONAL PLAN
 This detail shows a vertical pipe with a horizontal branch pipe. The branch pipe is shown with a flange and a gasket. The vertical pipe is shown with a flange and a gasket. The connection is shown with a gasket and a nut and washer.

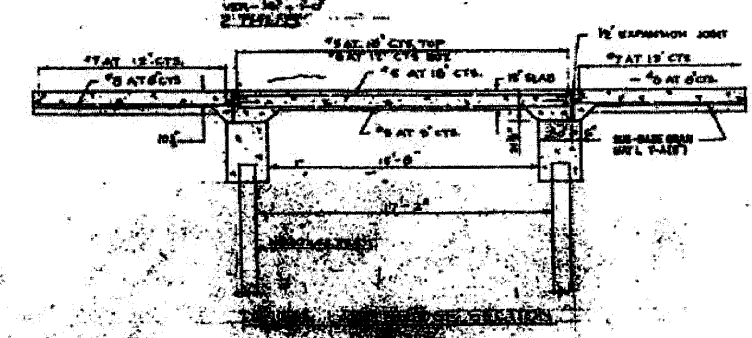
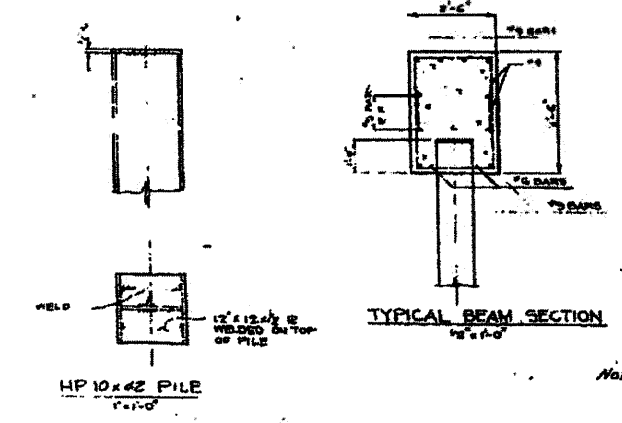
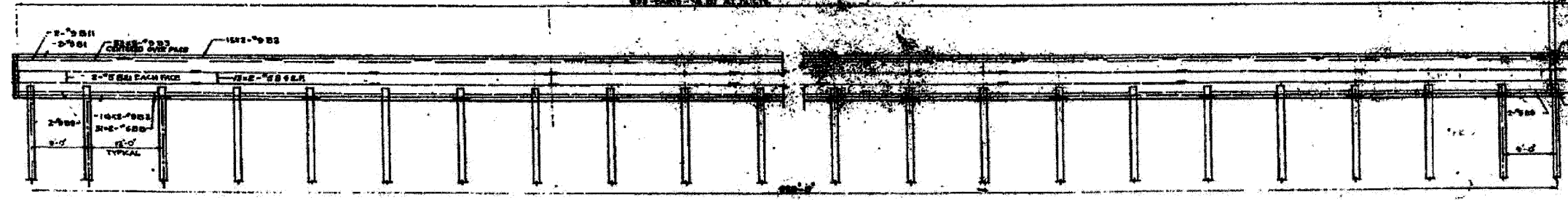
DETAIL 2
 SECTIONAL VIEW
 This detail shows a pipe with a flange and a gasket. The pipe is shown with a flange and a gasket. The connection is shown with a gasket and a nut and washer.

DETAIL 1
 SECTIONAL PLAN
 This detail shows a vertical pipe with a horizontal branch pipe. The branch pipe is shown with a flange and a gasket. The vertical pipe is shown with a flange and a gasket. The connection is shown with a gasket and a nut and washer.

DETAIL 2
 SECTIONAL VIEW
 This detail shows a pipe with a flange and a gasket. The pipe is shown with a flange and a gasket. The connection is shown with a gasket and a nut and washer.

INFORMATION ONLY

10-88-812

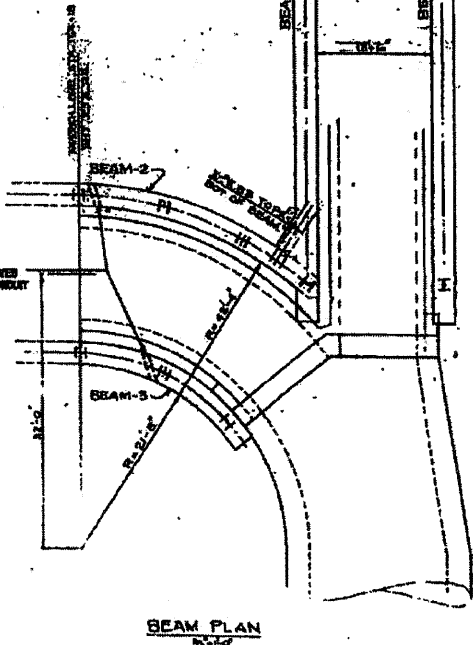
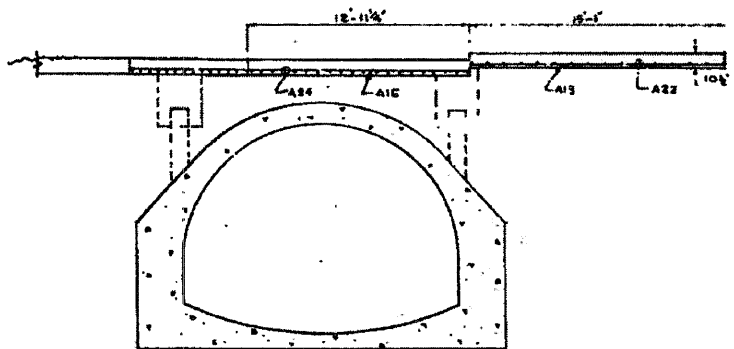
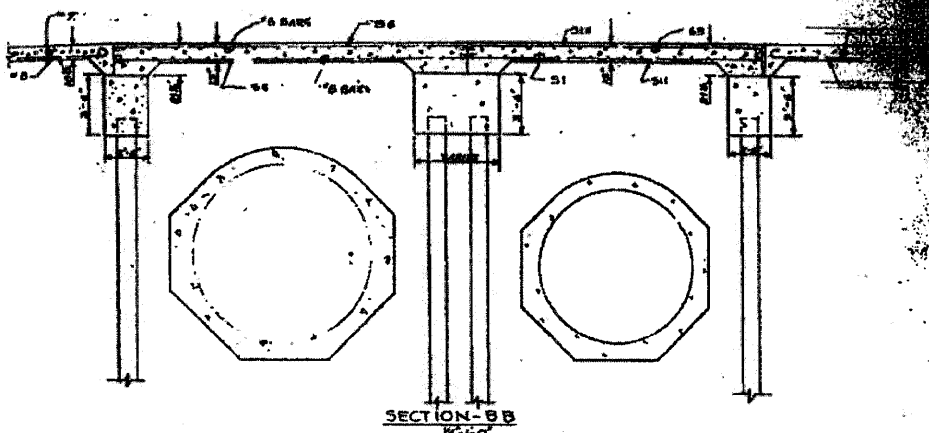
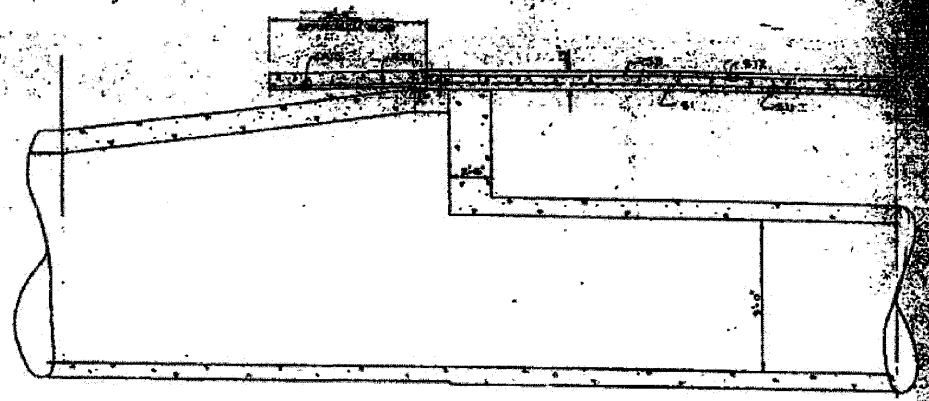
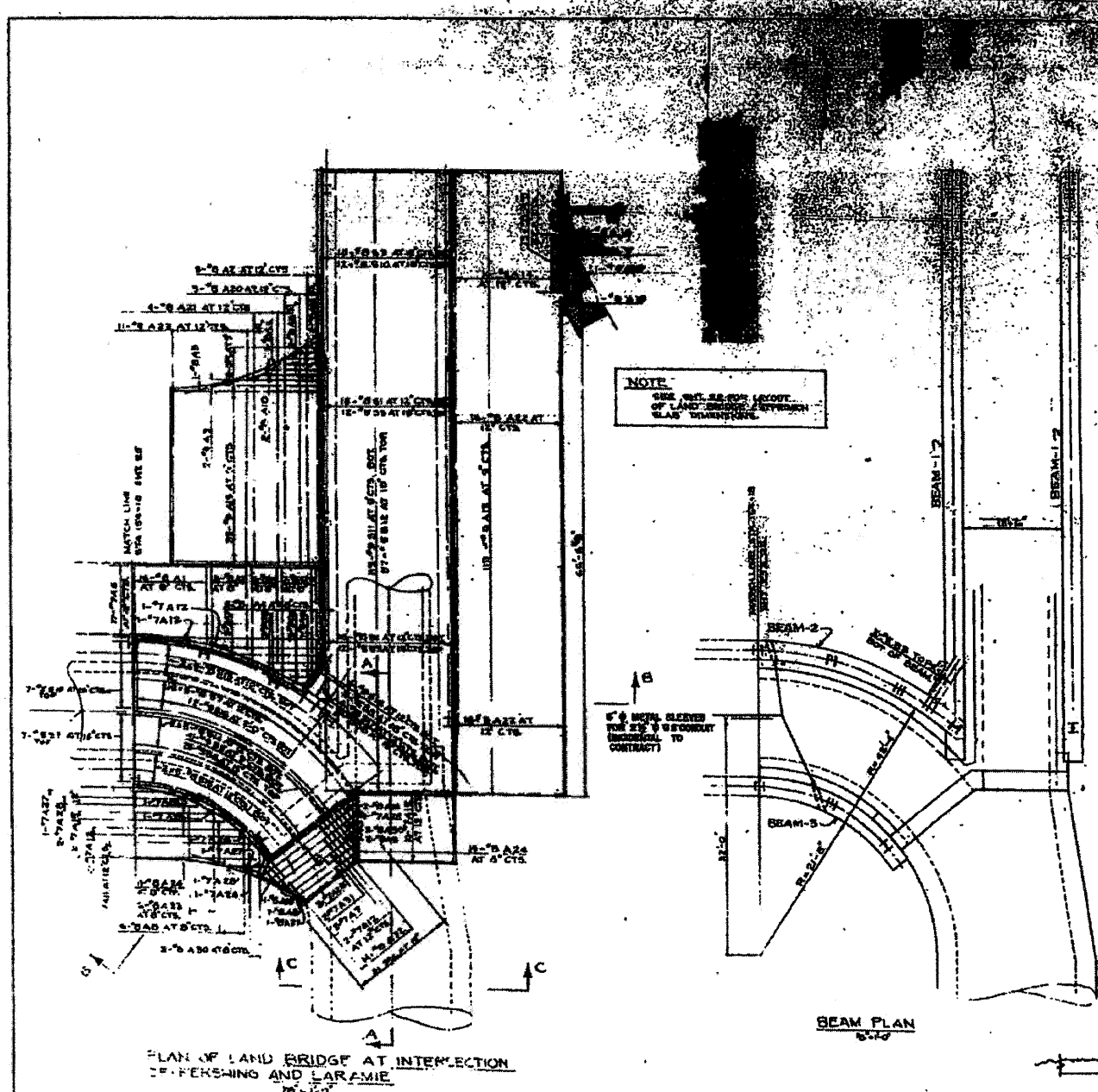


INFORMATION ONLY

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
 PERSHING ROAD
 DRY LAND BRIDGE,
 APPROACH SLAB & BEAM

SCALE: AS NOTED
 DATE: _____
 DRAWN BY: A.L.
 CHECKED BY: W.S.

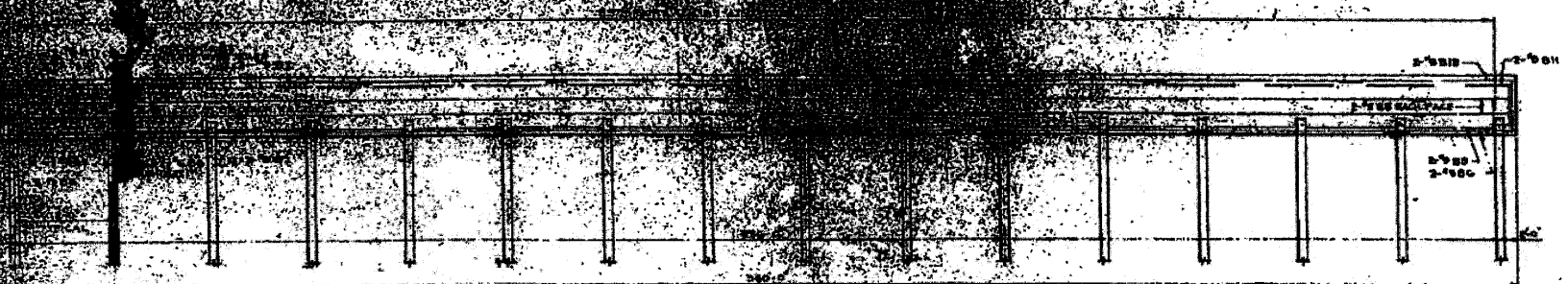
Note: For Pile Data see sheet #25.



INFORMATION ONLY

Note: For File Data see sh. 423.

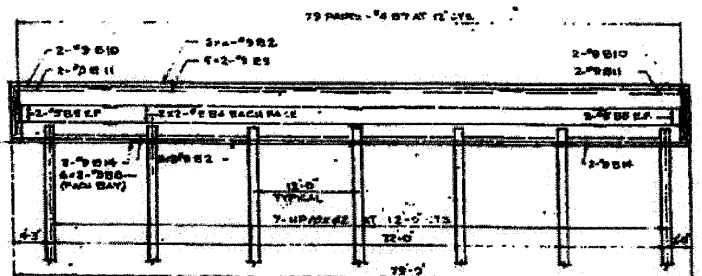
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
 PERSHING ROAD
 DRY LAND BRIDGE
 INTERSECTION PERSHING
 AND LARAMIE
 SCALE: AS NOTED
 DATE: _____
 SHEET 26 OF 30



BEAM ELEVATION BEAMS (2 TO STA. 132+60.36, 2 THUS)

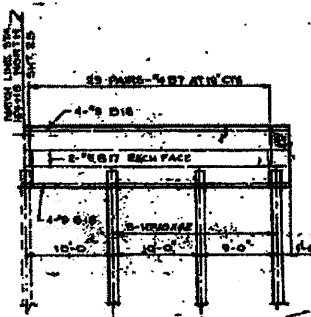
BAR	NO.	SIZE	LENGTH	SHAPE
B1	1	1-0"	20'-0"	
B2	2	1-0"	20'-0"	
B3	3	1-0"	20'-0"	
B4	4	1-0"	20'-0"	
B5	5	1-0"	20'-0"	
B6	6	1-0"	20'-0"	
B7	7	1-0"	20'-0"	
B8	8	1-0"	20'-0"	
B9	9	1-0"	20'-0"	
B10	10	1-0"	20'-0"	
B11	11	1-0"	20'-0"	
B12	12	1-0"	20'-0"	
B13	13	1-0"	20'-0"	
B14	14	1-0"	20'-0"	
B15	15	1-0"	20'-0"	
B16	16	1-0"	20'-0"	
B17	17	1-0"	20'-0"	
B18	18	1-0"	20'-0"	

INFORMATION ONLY

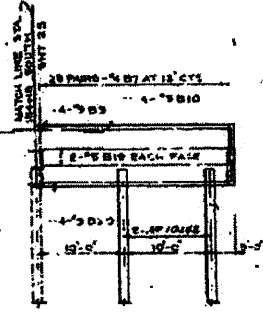


ELEVATION BEAM B-1 (2 THUS)

LOOKING WEST
(LARGE ARCADE)



ELEVATION BEAM B-2



ELEVATION BEAM B-3

BAR	NO.	SIZE	LENGTH	SHAPE
A1	1	1-0"	20'-0"	
A2	2	1-0"	20'-0"	
A3	3	1-0"	20'-0"	
A4	4	1-0"	20'-0"	
A5	5	1-0"	20'-0"	
A6	6	1-0"	20'-0"	
A7	7	1-0"	20'-0"	
A8	8	1-0"	20'-0"	
A9	9	1-0"	20'-0"	
A10	10	1-0"	20'-0"	
A11	11	1-0"	20'-0"	
A12	12	1-0"	20'-0"	
A13	13	1-0"	20'-0"	
A14	14	1-0"	20'-0"	
A15	15	1-0"	20'-0"	
A16	16	1-0"	20'-0"	
A17	17	1-0"	20'-0"	
A18	18	1-0"	20'-0"	
A19	19	1-0"	20'-0"	
A20	20	1-0"	20'-0"	
A21	21	1-0"	20'-0"	
A22	22	1-0"	20'-0"	

BAR	NO.	SIZE	LENGTH	SHAPE
B1	1	1-0"	20'-0"	
B2	2	1-0"	20'-0"	
B3	3	1-0"	20'-0"	
B4	4	1-0"	20'-0"	
B5	5	1-0"	20'-0"	
B6	6	1-0"	20'-0"	
B7	7	1-0"	20'-0"	
B8	8	1-0"	20'-0"	
B9	9	1-0"	20'-0"	
B10	10	1-0"	20'-0"	
B11	11	1-0"	20'-0"	
B12	12	1-0"	20'-0"	
B13	13	1-0"	20'-0"	
B14	14	1-0"	20'-0"	
B15	15	1-0"	20'-0"	
B16	16	1-0"	20'-0"	
B17	17	1-0"	20'-0"	
B18	18	1-0"	20'-0"	

BAR	E	G
B1	1-0"	20'-0"
B2	1-0"	20'-0"
B3	1-0"	20'-0"
B4	1-0"	20'-0"
B5	1-0"	20'-0"
B6	1-0"	20'-0"
B7	1-0"	20'-0"
B8	1-0"	20'-0"
B9	1-0"	20'-0"
B10	1-0"	20'-0"
B11	1-0"	20'-0"
B12	1-0"	20'-0"
B13	1-0"	20'-0"
B14	1-0"	20'-0"
B15	1-0"	20'-0"
B16	1-0"	20'-0"
B17	1-0"	20'-0"
B18	1-0"	20'-0"

BILL OF MATERIALS

ITEM	QUANTITY	UNIT
REINFORCEMENT BARS		TONS
CLASS II CONCRETE		CU YD
PROTECTIVE COAT		SQ YD
STRIP EXCAVATION		CU YD
PURCHASING STEEL PILES (2 1/2" DIA)		TONS
DRIVING STEEL PILES		TONS
TEST PILE STEEL		TONS
SUB BASE GRAB MATL. 7 A		TONS
P.C.C. FILLMENT (2 1/2" DIA)		CU YD
PILE POINTS (2 1/2" DIA)		TONS

NOTES

- ALL REINFORCEMENT BARS SHALL BE LAPPIED 24 DIAM LENGTH UNLESS OTHERWISE NOTED.
- CLEAR COVER OF CONCRETE OVER REINFORCEMENT BARS SHALL BE 2" UNLESS OTHERWISE NOTED.
- ABBREVIATION S.E. INDICATES SOUTH FACE.
- DESIGNATION SHALL BE INDICATED TO LINES OF BARS WITH II LENGTHS PER LINE.
- TEST PILE SHALL BE GIVEN IN THEIR PERMANENT LOCATIONS APPROXIMATELY AT 340 FT INTERVAL (APPROXIMATELY).
- REINFORCEMENT TO BE MADE TO SHEET 83 FOR STRUCTURAL PURPOSES AND SHEET 82 FOR DRY LAND PURPOSES.
- ALL DIMENSIONS SHALL BE USED FOR THE BEAMS AND PILE CAPS.
- ALL DIMENSIONS SHALL BE APPROXIMATE WITH THE APPLICABLE TOLERANCES.

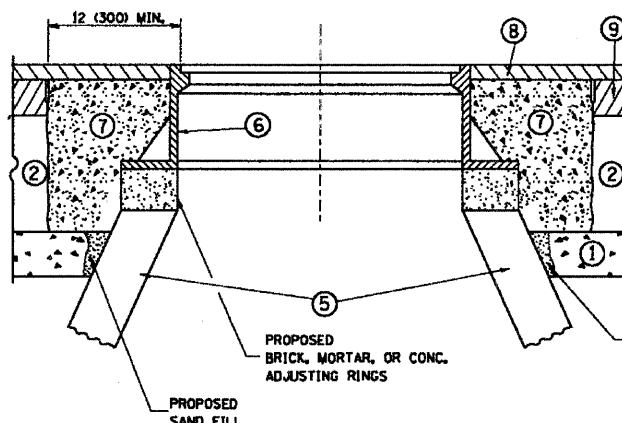
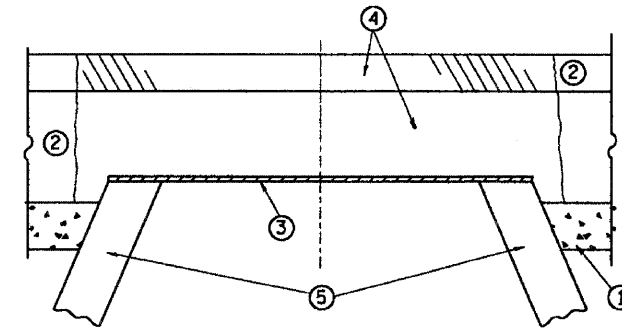
AS REVISED

Notes for Pile Data see sheet #83.

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

PERSHING ROAD
DRY LAND BRIDGE
BEAMS

SCALE: AS NOTED



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

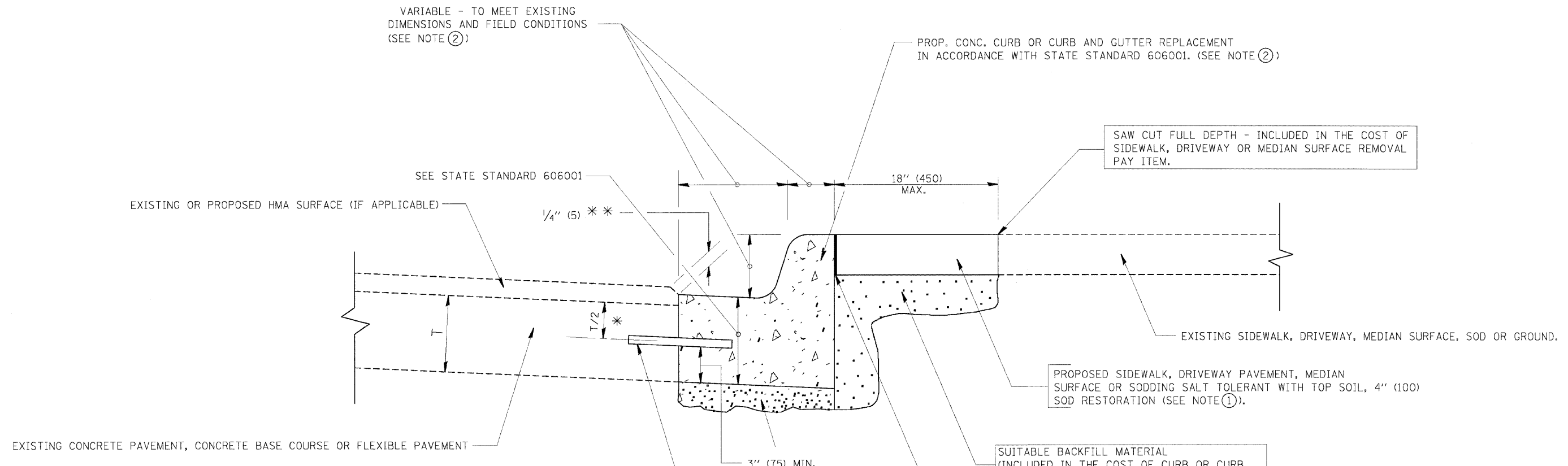
THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"
NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME = W:\data\22x34\bd08.dgn	USER NAME = geglianobt	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING		F.A.L. RTE. 1477	SECTION 0811 RS	COUNTY COOK	TOTAL SHEETS 28	SHEET NO. 18
	PLOT SCALE = 50.0000' / 1" =	CHECKED -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	BD600-03 (BD-8)		CONTRACT NO. 60H99
	PLOT DATE = 1/4/2008	DATE - 10-25-94	REVISED - R. WIEDEMAN 05-14-04						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		
			REVISED - R. BORO 01-01-07								



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
 - * * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

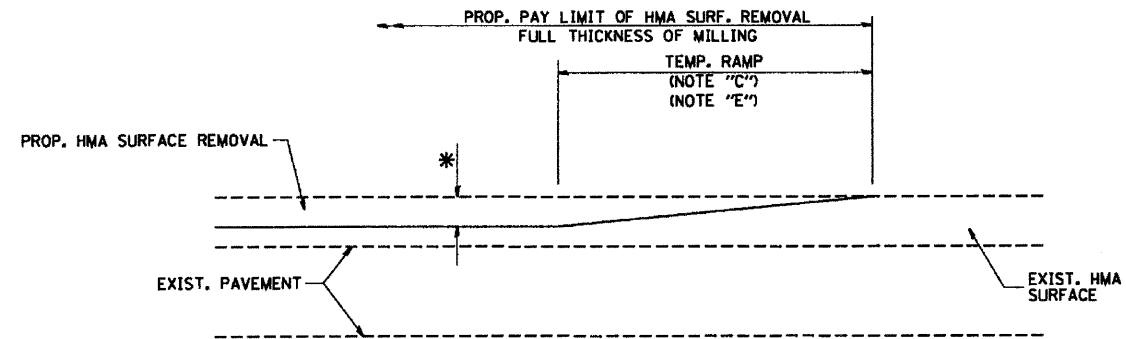
- SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)
- PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

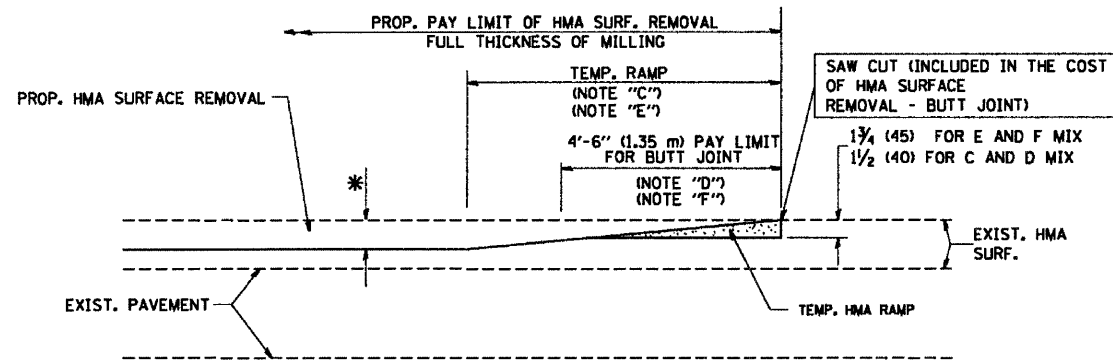
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = drvakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ci:\pawork\pawdot\drvakosgn\d018315\bd24.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97		1477	0811 RS	COOK	28	19	
PLOT SCALE = 50.000 1/ IN.		CHECKED -	REVISED - M. GOMEZ 01-22-01		BD600-06 (BD-24)		CONTRACT NO. 60H99			
PLOT DATE = 12/15/2009		DATE - 03-11-94	REVISED - R. BORO 12-15-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

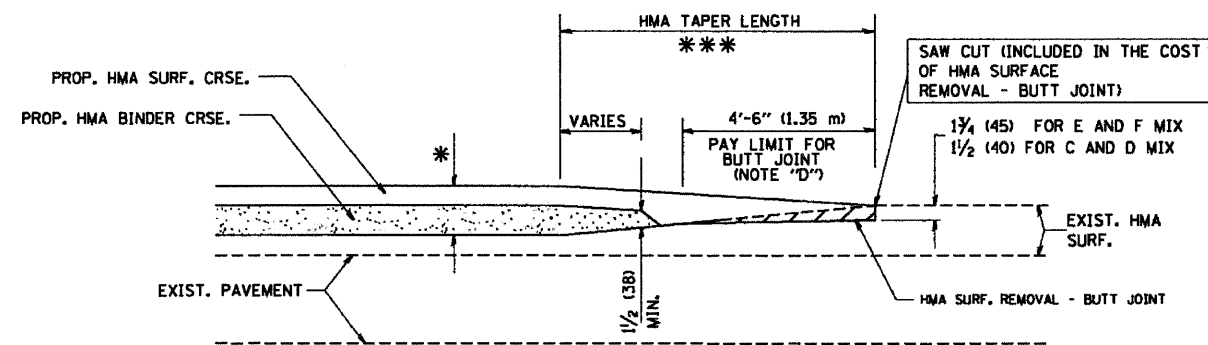
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

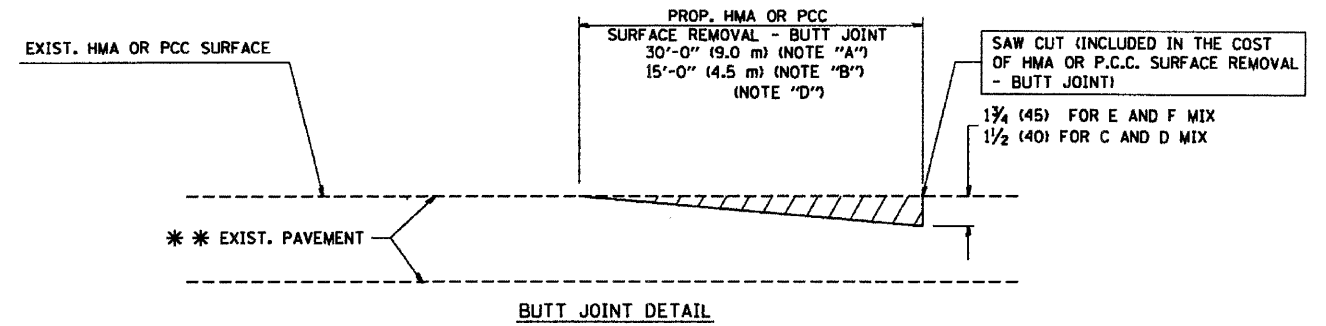
OPTION 2

TYPICAL TEMPORARY RAMP

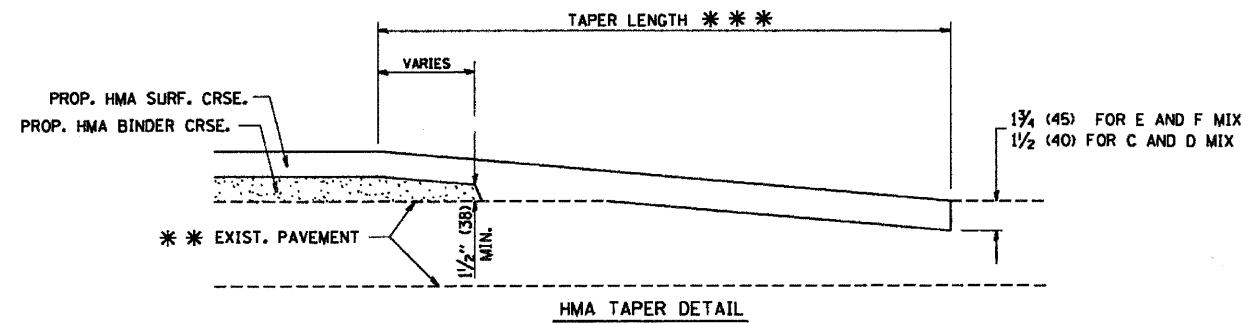


BUTT JOINT AND
HMA TAPER

**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

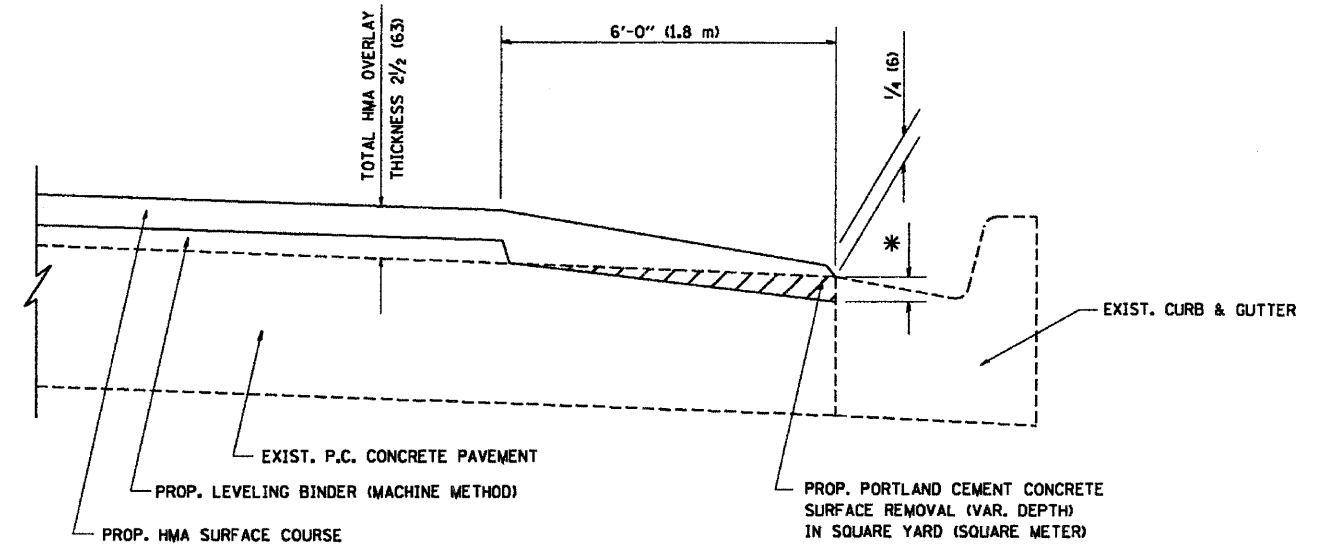
THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\data\22x34\bd32.dgn	USER NAME = gagliardi	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 5/8" = 1' IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS		F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	1477	0811 RS	COOK	28	20
TO STA.		BD400-05 BD32		CONTRACT NO. 60H99		
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT						

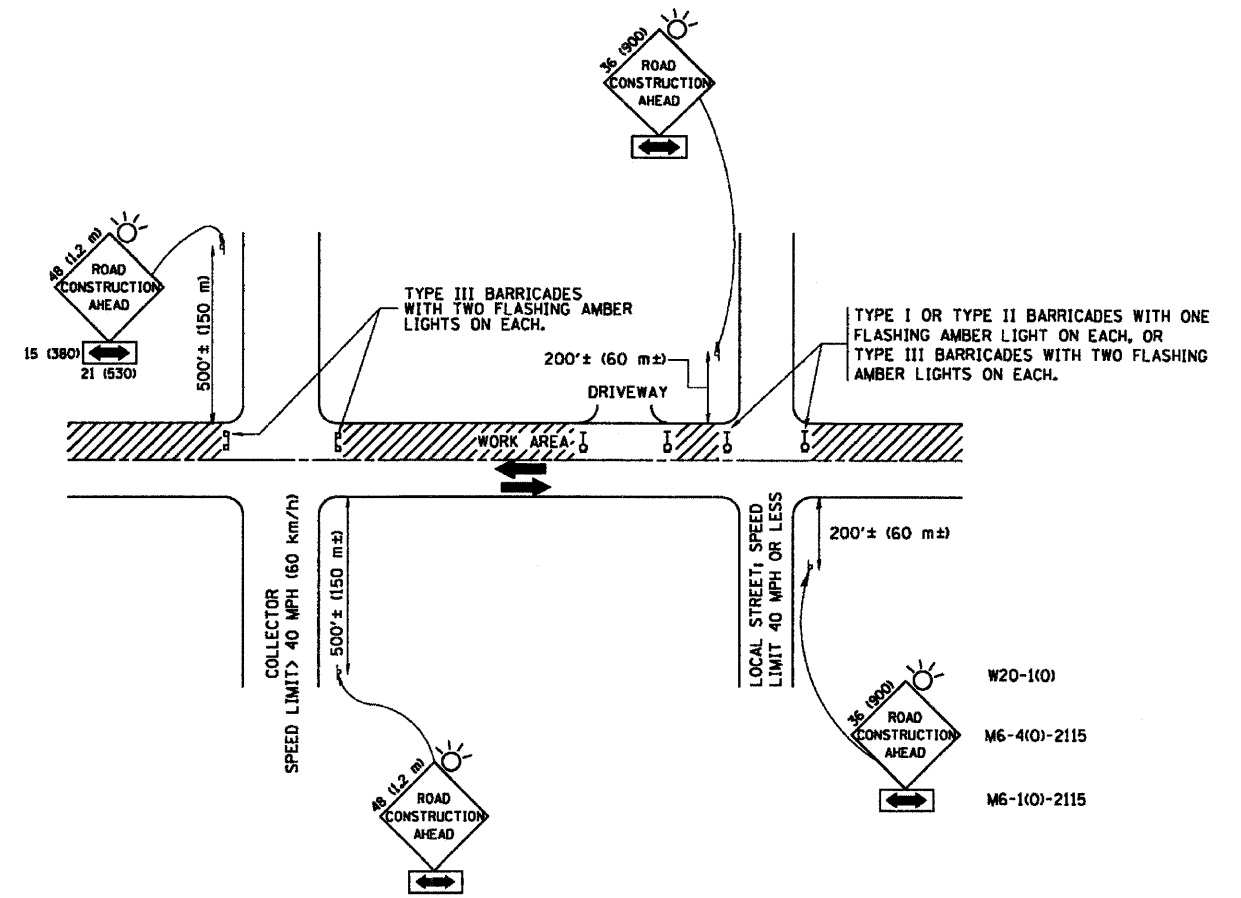


HMA TAPER AT
EDGE OF P.C.C. PAVEMENT

HMA SURFACE	LEVELING BINDER	* MILLING AT GUTTER FLAG
MIX	THICKNESS	THICKNESS
C OR D	1 1/2 (38)	1 (25)
F	1 3/4 (44)	3/4 (19)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\bd33.dgn	USER NAME = geglienobt	DESIGNED - R. SHAH	REVISED - R. SHAH 10-25-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HMA TAPER AT EDGE OF P.C.C. PAVEMENT		F.A.J. RTE. 1477	SECTION 0811 RS	COUNTY COOK	TOTAL SHEETS 28	SHEET NO. 21
	PLOT SCALE = 58.0000' / IN.	DRAWN - JIS	REVISED - A. ABBAS 05-05-99		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	BD400-06 (BD33)		CONTRACT NO. 60H99
	PLOT DATE = 1/4/2000	CHECKED - A. ABBAS	REVISED - E. GOMEZ 12-21-00		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						
		DATE - 09-10-94	REVISED - R. BORO 01-01-07								



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS**
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:**
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.**
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.**

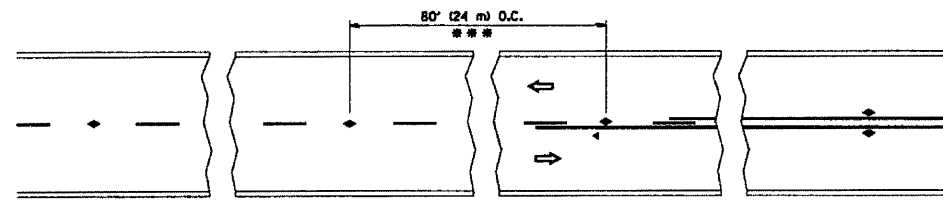
All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME = W:\distato\22x34\tbl8.dgn	USER NAME = gegliemobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
		PLOT SCALE = 58.888 / IN.	REVISED - A. HOUSEH 10-15-96
		PLOT DATE = 1/1/2008	REVISED - T. RANMACHER 01-06-00
		DATE - 06-89	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

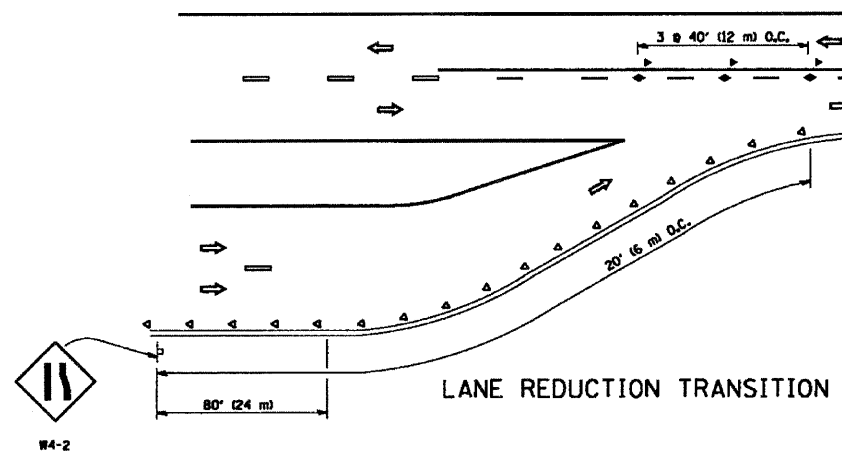
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 1477	SECTION 0811 RS	COUNTY COOK	TOTAL SHEETS 28	SHEET NO. 22
TC-10			CONTRACT NO. 60H99	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

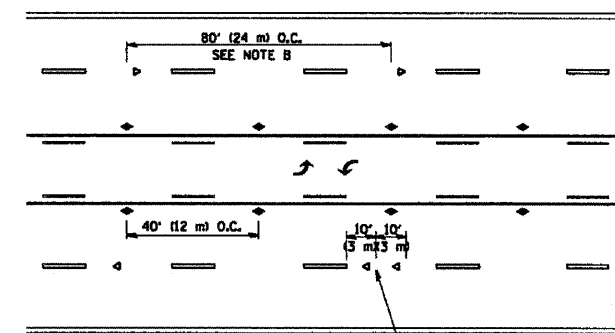


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

TWO-LANE/TWO-WAY

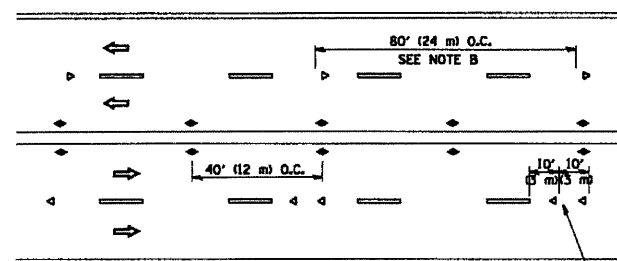


LANE REDUCTION TRANSITION

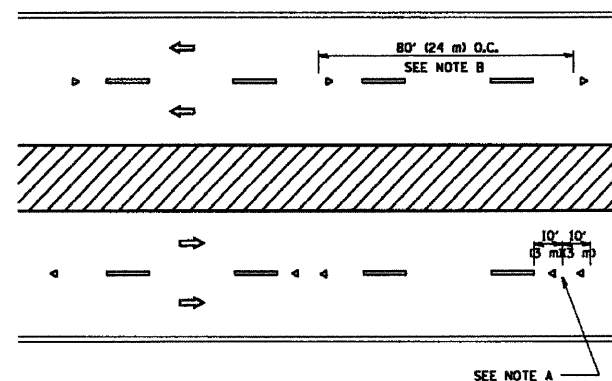


SEE NOTE A

TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

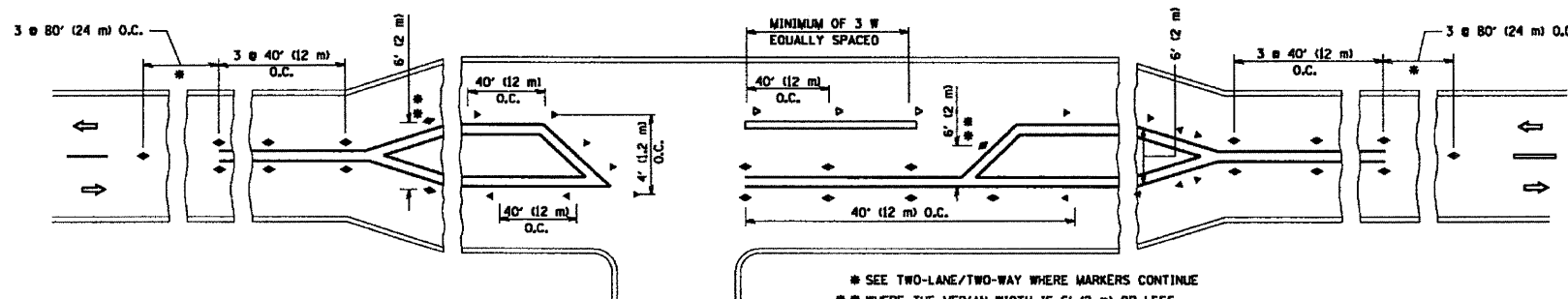
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◄ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

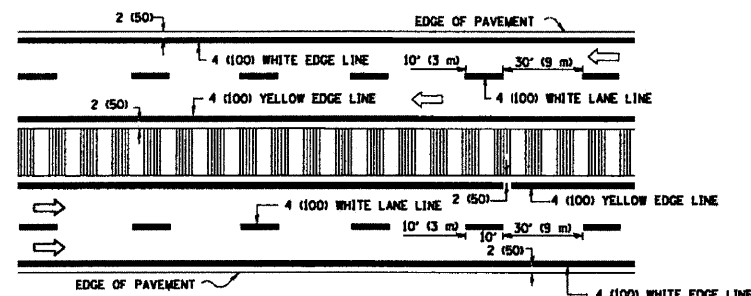
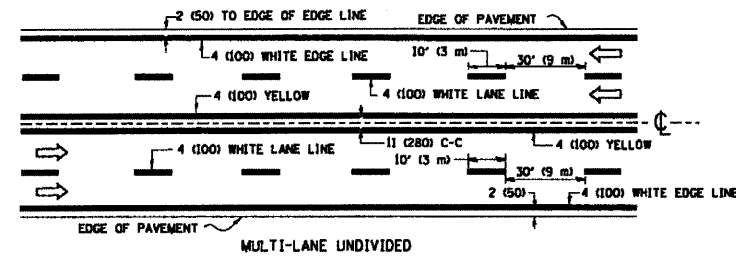
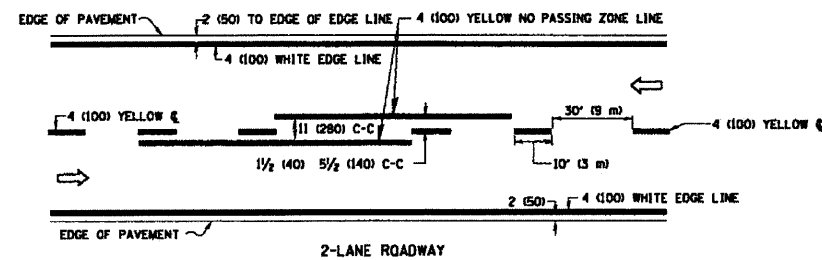


* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

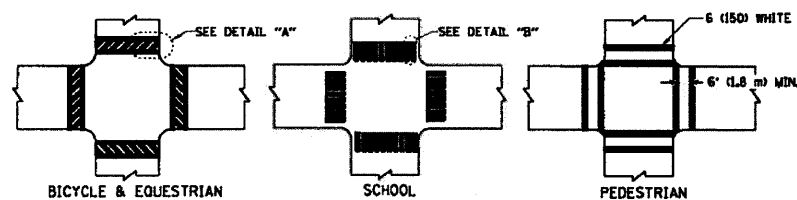
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = M:\distato\22\34\tcl1.dgn	USER NAME = gegionobt	DESIGNED - DRAWN -	REVISED - T. RAMMACHER 09-19-94 REVISED - T. RAMMACHER 03-12-99	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			F.A.L. RTE. 1477	SECTION 0811 RS	COUNTY COOK	TOTAL SHEETS 28	SHEET NO. 23
PLOT SCALE = 58.000' / IN.	CHECKED -	REVISED - T. RAMMACHER 01-06-00	SCALE: NONE		SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	TC-11		CONTRACT NO. 60H99		
PLOT DATE = 1/4/2000	DATE -	REVISED -				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						

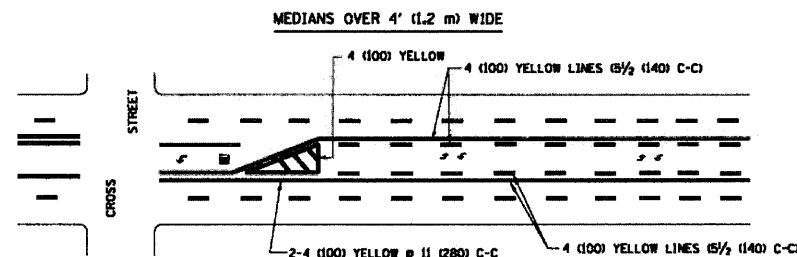
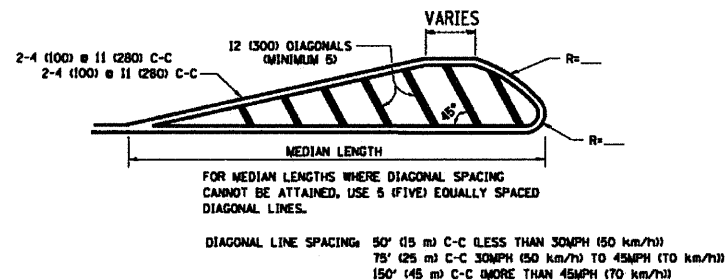
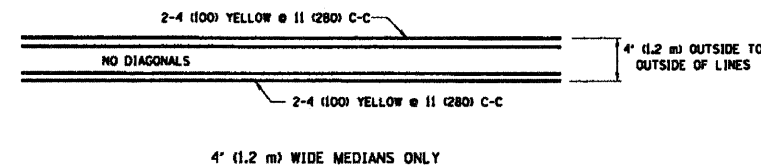


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

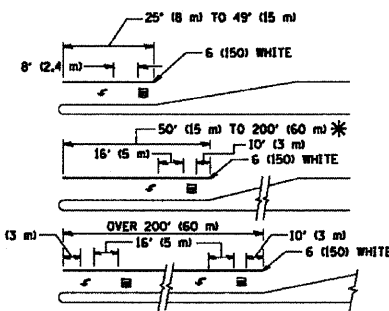


TYPICAL CROSSWALK MARKING



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

TYPICAL PAINTED MEDIAN MARKING

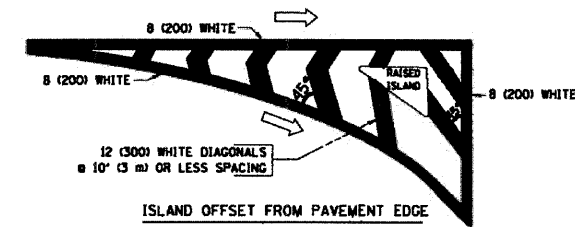


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m²) AREA = 20.8 SQ. FT. (1.9 m²)

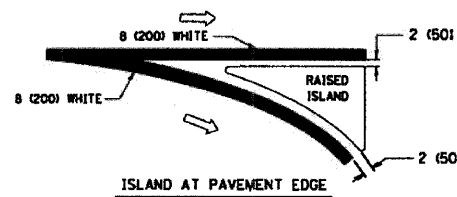
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



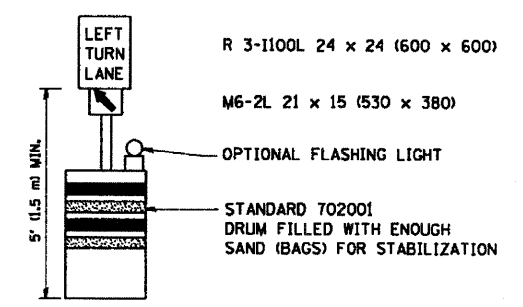
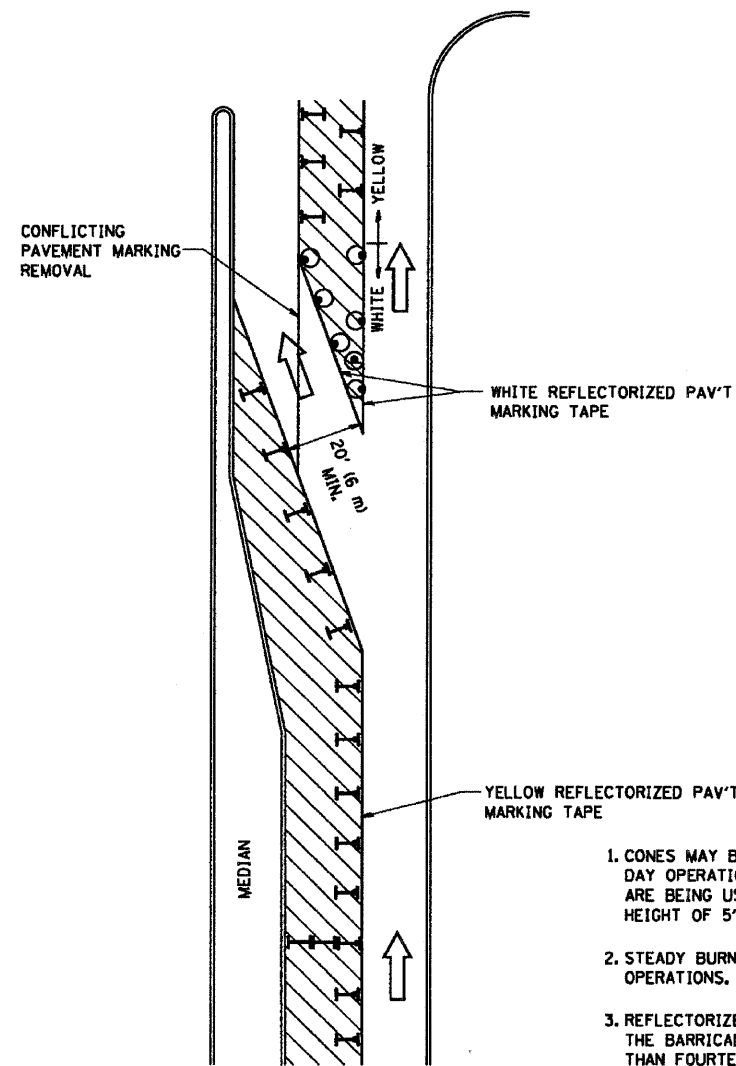
ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW. EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN & BICYCLE & EQUESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
DORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES "RR" 15 6' (1.8 m) LETTERS (6 (1400) LINE FOR "X")	SOLID	WHITE	SEE STATE STANDARD T80001 AREA OF: "R" = 3.6 SQ. FT. (0.33 m ²) EACH "X" = 54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (23 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD T80001.

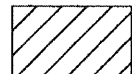
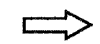




All dimensions are in inches (millimeters) unless otherwise shown.



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

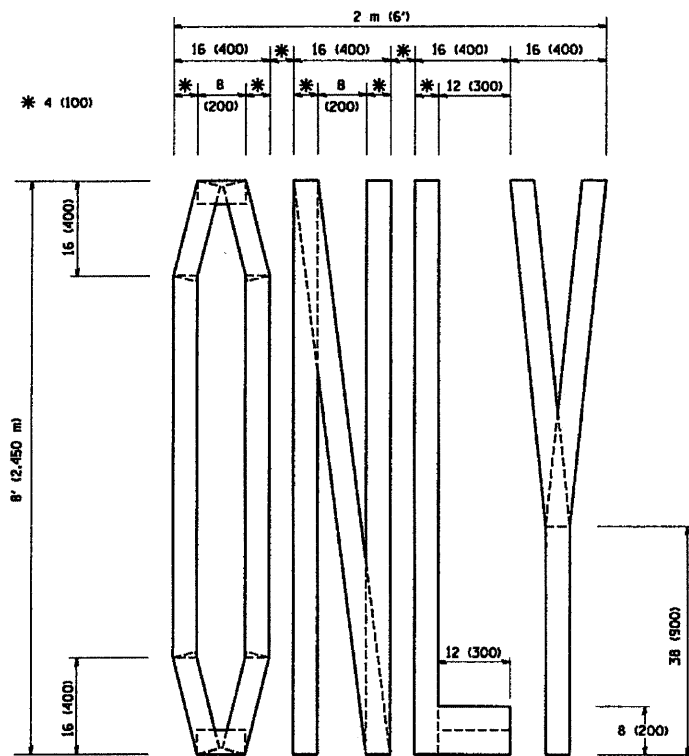
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\distr\td\22x34\td14.dgn	USER NAME = geglienobt	DESIGNED -	REVISED - T. RAMMACHER 09-08-94
		DRAWN -	REVISED - A. HOUSEH 11-07-95
		PLOT SCALE = 58.0000 ' / IN.	REVISED - A. HOUSEH 10-12-96
		CHECKED -	REVISED - T. RAMMACHER 01-06-00
		PLOT DATE = 1/1/2008	

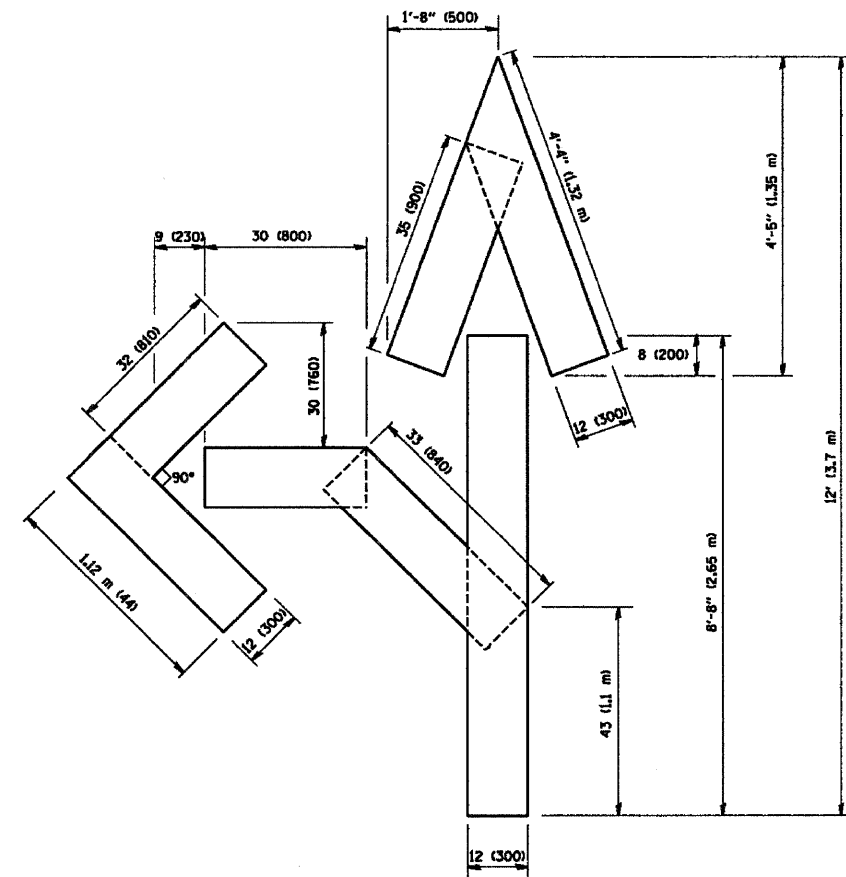
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

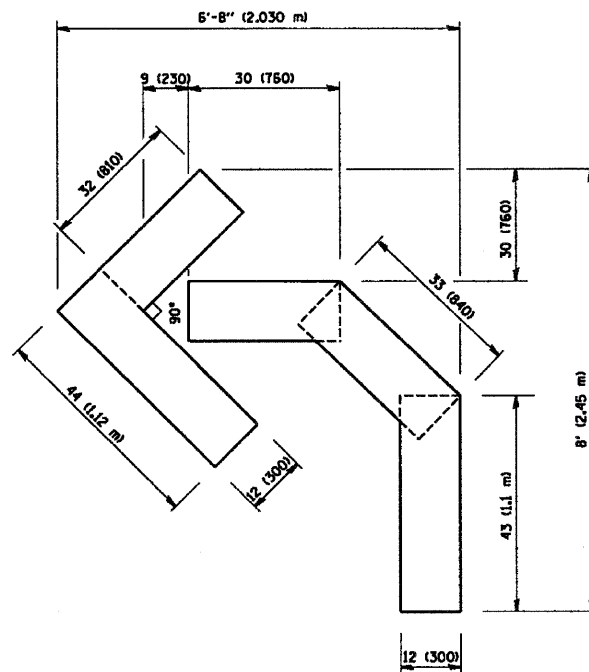
F.A.U. RTE. 1477	SECTION 0B11 RS	COUNTY COOK	TOTAL SHEETS 28	SHEET NO. 25
TC-14		CONTRACT NO. 60H99		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



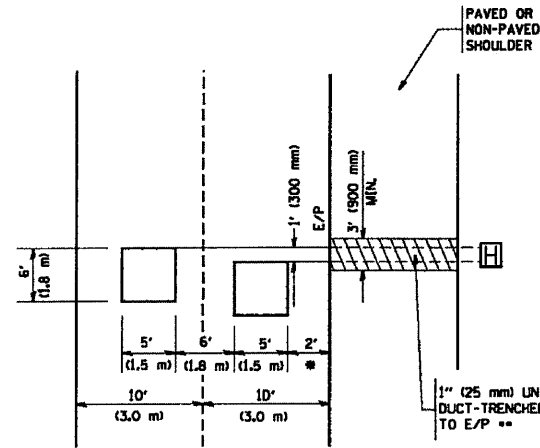
QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\dvtatd\22\34\td16.dgn	USER NAME = goglienobt	DESIGNED -	REVISED - T. RAMMACHER 06-05-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING			F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - T. RAMMACHER 11-04-97		1477	0811 RS	COOK	28	26			
		PLOT SCALE = 58.8888" / IN.	REVISED - T. RAMMACHER 03-02-98		TC-16			CONTRACT NO. 60H99				
		PLOT DATE = 1/4/2000	REVISED - E. GOMEZ 08-28-00		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. (ILLINOIS) FED. AID PROJECT			

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

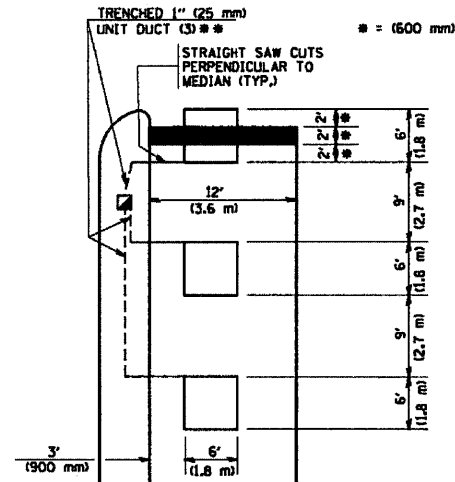


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

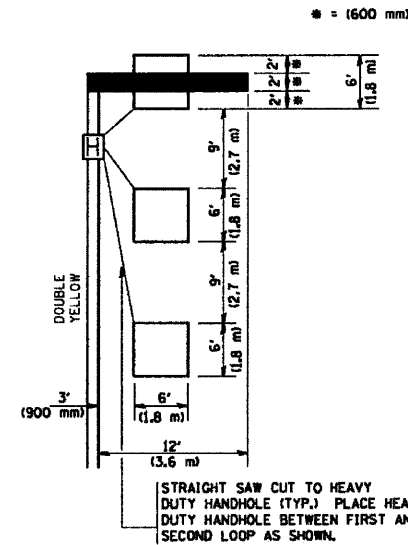
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

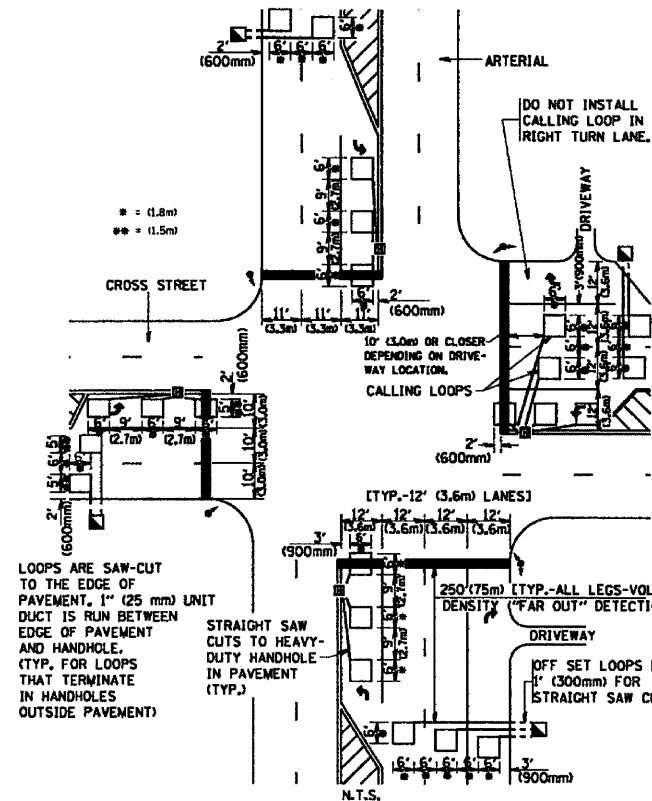
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



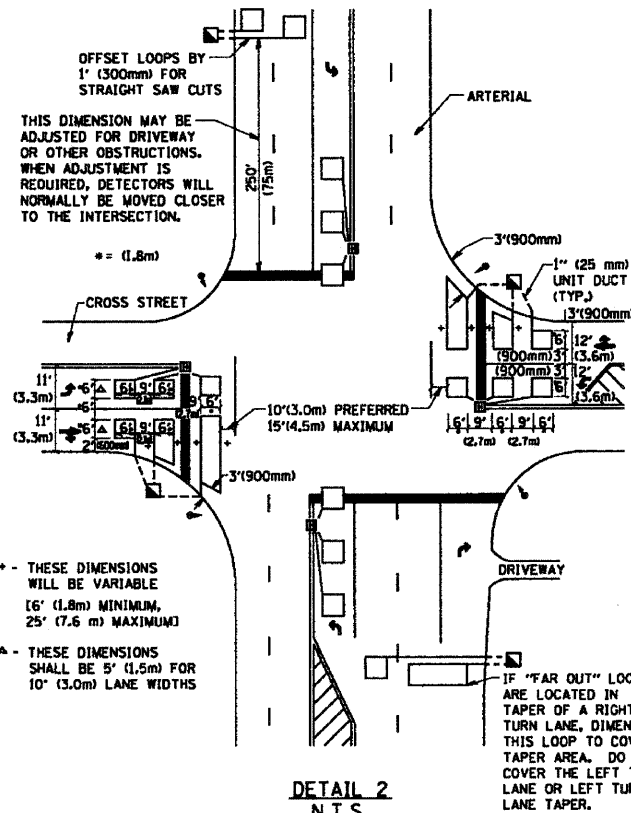
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2
N.T.S.**

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\distatd\22x34\ts87.dgn	USER NAME = goglienob	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING		F.A.U. RTE. 1477	SECTION DB11 RS	COUNTY COOK	TOTAL SHEETS 28	SHEET NO. 28
PLOT SCALE = 5/8" = 1' IN.	CHECKED - R.J.F.	DATE -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	TS-07		CONTRACT NO. 60H99	
PLOT DATE = 1/4/2008	DATE -	REVISED -	REVISED -				FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				