

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

F.A.P. ROUTE 305 /IL 53 E&W FRONTAGE & N. PALATINE FRONTAGE
ILL 68 TO N. SIDE PALATINE FRONTAGE & IL 53 E. FRONTAGE RD. TO KENNICOTT DR.
SECTION: 2009-076 RS

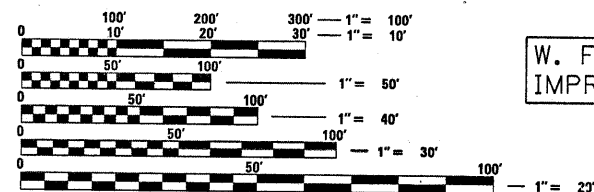
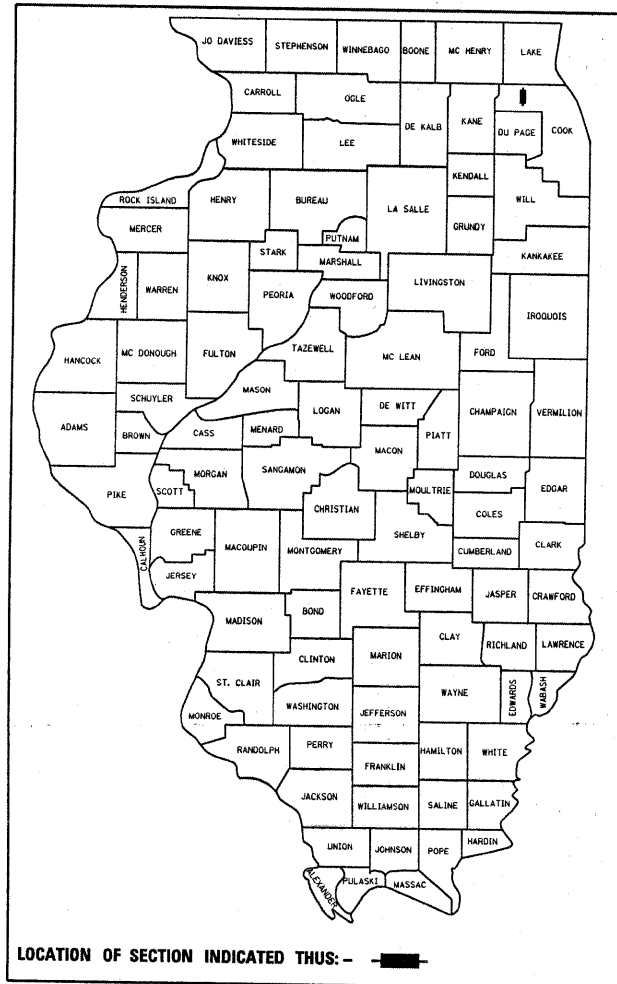
RESURFACING
COOK COUNTY
C-91-850-09

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2009-076 RS	COOK	34	1
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 60110		

FOR INDEX OF SHEETS, SEE SHEET NO. 2

IMPROVEMENT IS LOCATED IN THE VILLAGES
OF ARLINGTON HEIGHTS & PALATINE

D-91-850-09



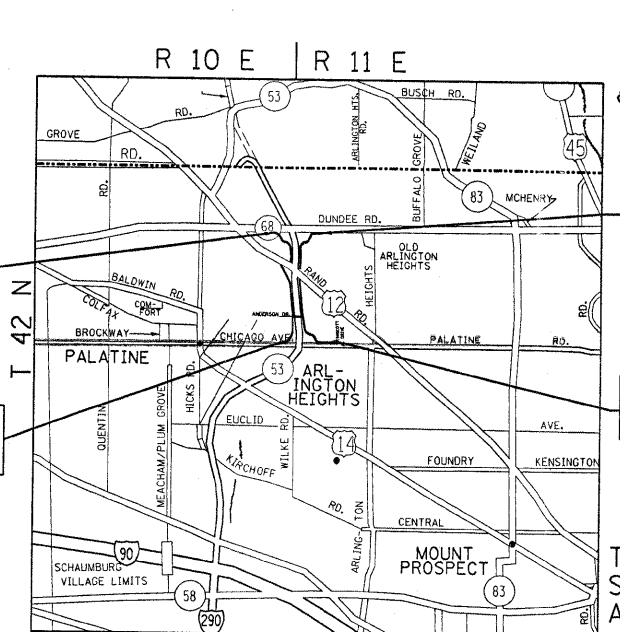
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

W. FRONTAGE RD.
IMPROVEMENT BEGINS STA. 339+23

W. FRONTAGE RD.
IMPROVEMENT BEGINS STA. 220+14

OMISSION
STA. 99+69 TO STA. 100+35
STA. 299+68 TO STA. 300+65



WHEELING TOWNSHIP

E. FRONTAGE RD.
IMPROVEMENT ENDS STA. 144+92

E. FRONTAGE RD./N. PALATINE RD.
IMPROVEMENT BEGINS STA. 18+13

TRAFFIC DATA
SPEED LIMIT: 25 TO 35 MPH
ADT: NOT KNOWN

PROJECT ENGINEER: DAN WILGREEN (847) 705-4240
PROJECT MANAGER: KEN ENG

CONTRACT NO. 60110

GROSS LENGTH OF IMPROVEMENT = 24,588 FEET (4.66 MILES)
NET LENGTH OF IMPROVEMENT = 24,425 FEET (4.63 MILES)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED DECEMBER 14, 20 09

Diane M. O'Keefe *as*
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

January 29, 20 10
Scott E. Still, P.E.
Acting ENGINEER OF DESIGN AND ENVIRONMENT

January 29, 20 10
Christine M. Reed
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

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STATE STANDARDS

000001-05	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
442201-03	CLASS C AND D PATCHES
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB & GUTTER
701011-02	OFF-ROAD MOVING OPERATIONS 2L, 2W DAY ONLY
701306-02	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS > 45 MPH
701501-05	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701701-06	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-01	TRAFFIC CONTROL DEVICES

GENERAL NOTES:

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGES OF ARLINGTON HEIGHTS AND PALATINE.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

ALL HOT-MIX ASPHALT PAVEMENT PATCHING SHALL BE CLASS D.

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40MM) WHERE THE SPEED LIMIT IS 45 MPH (80KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H), WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

10 FEET (3 METER) TRANSITION SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER TO EXISTING CURB AND GUTTERS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITION SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.

THE RESIDENT ENGINEER SHALL VERIFY THE LOCATIONS OF ALL EXISTING PAVEMENT MARKINGS PRIOR TO MILLING OR RESURFACING.

ALL PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE IMPROVEMENT ACCORDING TO DISTRICT 1 TYPICAL PAVEMENT MARKING.

TWO WEEKS PRIOR TO PLACING PERMANENT PAVEMENT MARKINGS, CONTACT WALLY CZARNY, AREA TRAFFIC FIELD ENGINEER AT (773) 685-4342.

RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED THROUGHOUT THE IMPROVEMENT ACCORDING TO THE DISTRICT STANDARDS AS NOTED IN THE DETAIL.

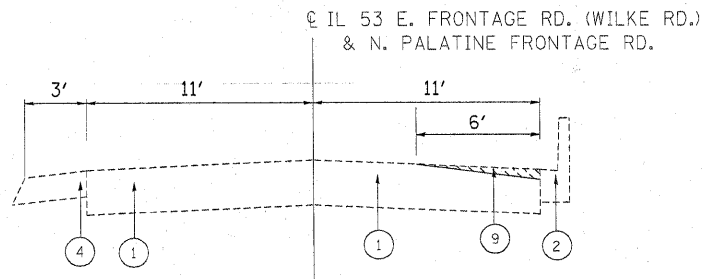
THE UNIT WEIGHT (CONVERSION FACTOR) QUOTED IS FOR THE ESTIMATING PLAN QUANTITIES ONLY. ACTUAL QUANTITIES TO FULFILL CONTRACT REQUIREMENTS WILL BE DETERMINED BASED ON UNIT WEIGHT OF APPROVED MIX DESIGN, PLAN DIMENSIONS, AND DENSITY LIMITATIONS. MAXIMUM PAYMENT WILL BE COMPUTED BASED ON WEIGHT AVERAGE DENSITIES OF THE IN-PLACE MIXTURE.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.

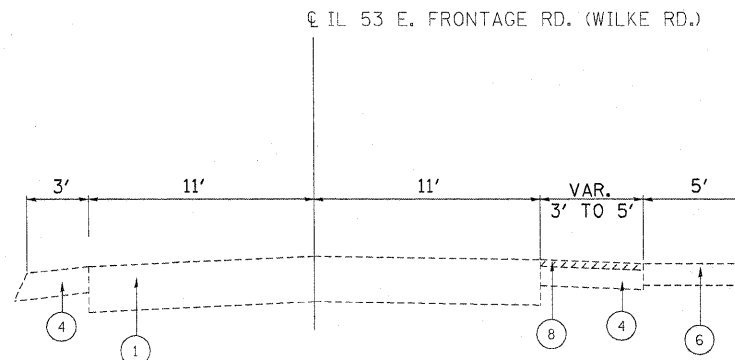
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ct:\pw\work\PW100T\ABREUAH\014610\0185409-Design.dgn		DRAWN -	REVISED -			305	2009-076 RS	COOK	34	2	
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -			CONTRACT NO. 60110					
PLOT DATE = 12/14/2009		DATE -	REVISED -			FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT					
					SCALE: 1" = 50'	SHEET NO.	OF	SHEETS	STA.	TO STA.	

SUMMARY OF QUANTITIES					CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES					CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	1000 100% STATE						CODE NO	ITEM	UNIT	TOTAL QUANTITIES	1000 100% STATE					
20201006	GRADING AND SHAPING SHOULDERS	UNIT	236	236						70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	198	198					
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	600	600						70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	67645	67645					
25200110	SODDING, SALT TOLERANT	SO YD	600	600						70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	262	262					
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	58	58						70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	201	201					
40600300	AGGREGATE (PRIME COAT)	TON	290	290						70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	1093	1093					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	23	23						78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	198	198					
40600895	CONSTRUCTING TEST STRIP	EACH	2	2						78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	67645	67645					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	1825	1825						78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	262	262					
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SO YD	1036	1036						78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	201	201					
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	6298	6298						78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	898	898					
42001300	PROTECTIVE COAT	SO YD	800	800						78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	718	718					
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SO YD	1097	1097						* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	726	726					
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SO YD	15014	15014						X0322256	TEMPORARY INFORMATION SIGNING	SO FT	102.8	102.8					
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	3600	3600						X0656100	DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT	SO YD	100	100					
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SO YD	1589	1589						40600826	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	3636	3636					
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SO YD	486	486						X4400100	PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH)	SO YD	5118	5118					
44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SO YD	296	296						X4421000	PARTIAL DEPTH PATCHING	TON	672	672					
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	1890	1890						X4422030	PARTIAL DEPTH REMOVAL 3"	SO YD	4000	4000					
55039700	STORM SEWERS TO BE CLEANED	FOOT	950	950						X6060500	CORRUGATED MEDIAN REMOVAL	SO FT	200	200					
60251740	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 24 FRAME AND GRATE	EACH	2	2															
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	11	11															
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	11	11															
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	11	11															
60624600	CORRUGATED MEDIAN	SO FT	200	200															
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6															
67100100	MOBILIZATION	L SUM	1	1															
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1															
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1															
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1															
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	7333	7333															

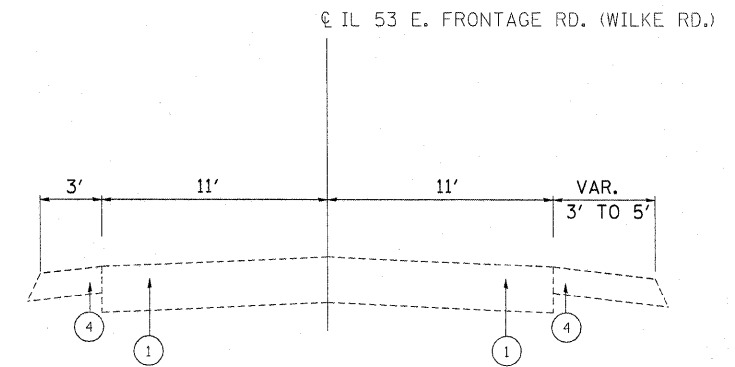
* SPECIALTY ITEM



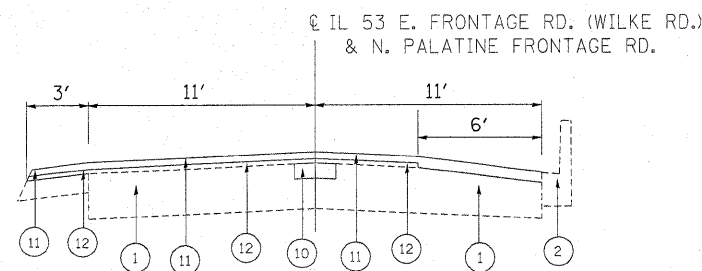
EXISTING TYPICAL SECTION
IL 53 E. FRONTAGE RD. (WILKE RD.)
& PALATINE N. FRONTAGE RD.
STA. 22+00 TO STA 30+65
STA. 59+47 TO STA 63+07



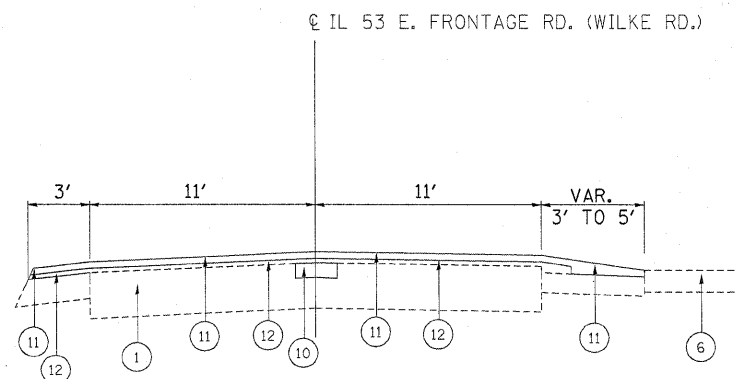
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IL 53 E. FRONTAGE RD. (WILKE RD.)
STA 30+65 TO STA. 47+17
STA 58+00 TO STA. 59+47



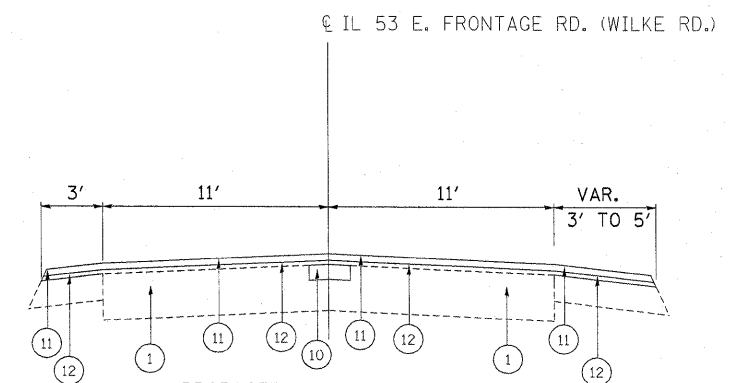
EXISTING TYPICAL SECTION
IL 53 E. FRONTAGE RD. (WILKE RD.)
STA. 47+17 TO STA. 58+00
STA. 63+07 TO STA. 84+99



PROPOSED TYPICAL SECTION
IL 53 E. FRONTAGE RD. (WILKE RD.)
& PALATINE N. FRONTAGE RD.
STA. 22+00 TO STA 30+65
STA. 59+47 TO STA 63+07



PROPOSED TYPICAL SECTION
IL 53 E. FRONTAGE RD. (WILKE RD.)
STA 30+65 TO STA. 47+17
STA 58+00 TO STA. 59+47



PROPOSED TYPICAL SECTION
IL 53 E. FRONTAGE RD. (WILKE RD.)
STA. 47+17 TO STA. 58+00
STA. 63+07 TO STA. 84+99

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS
RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5 MM)	4% @ 70 GYR
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	4% @ 50 GYR
PATCHING	
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR
PARTIAL DEPTH PATCH (HMA BINDER IL-19mm)	4% @ 70 GYR
DRIVEWAY	
HMA SURFACE COURSE, MIX "C", N50 (IL 9.5mm); 2"	4% @ 50 GYR
HMA BASE COURSE (HMA BINDER IL-19mm); PE-6", CE-8"	4% @ 50 GYR

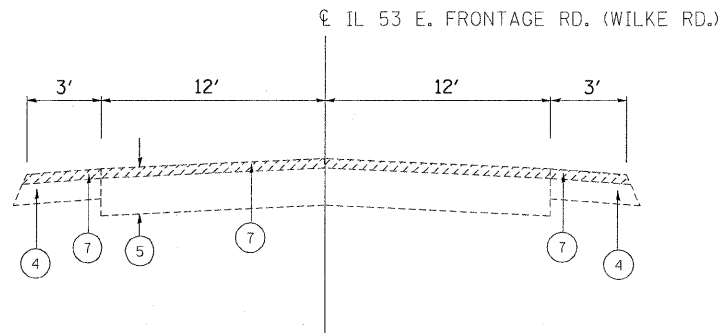
THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LB/ SQ YD/IN. FOR "AC TYPE" AND "PERCENT RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

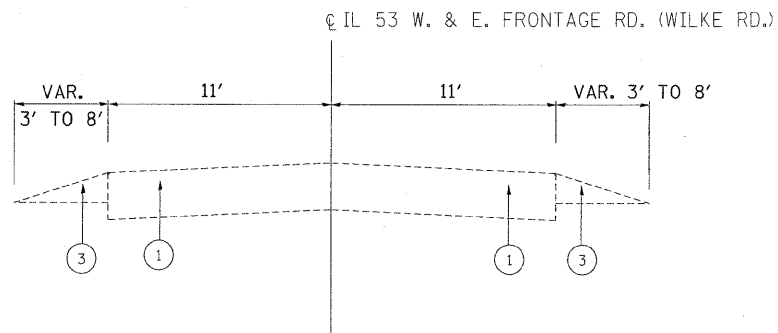
NOTE: CONTRACTOR SHALL MILL FIRST, THEN PATCH
SEE STANDARD BD-22.

LEGEND

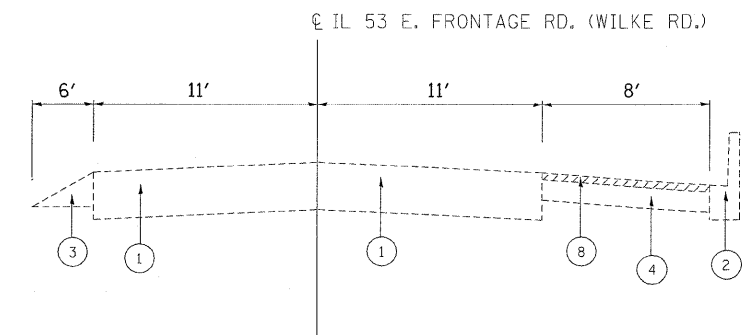
- 1 EXISTING PCC PAVEMENT ±9"
- 2 EXISTING COMBINATION CONCRETE CURB & GUTTER
- 3 EXISTING AGGREGATE SHOULDER
- 4 EXISTING HMA SHOULDER
- 5 EXISTING HMA PAVEMENT ±12"
- 6 EXISTING PCC SIDEWALK
- 7 PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- 8 PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
- 9 PROPOSED PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH)
- 10 PROPOSED PARTIAL DEPTH REMOVAL 3"
- 11 PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- 12 PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- 13 PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- 14 PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B



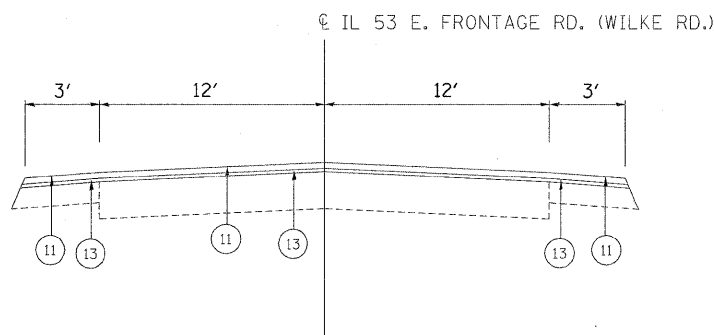
EXISTING TYPICAL SECTION
IL 53 E. FRONTAGE RD. (WILKE RD.)
STA. 84+99 TO STA 99+69
(LOOKING NORTH)



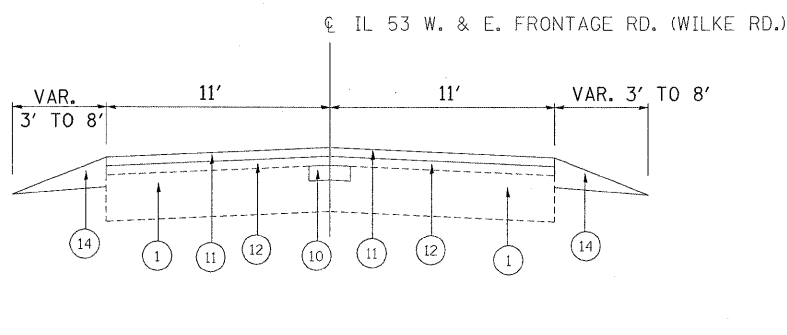
EXISTING TYPICAL SECTION
IL 53 W. & E. FRONTAGE RD. (WILKE RD.)
STA. 103+62 TO STA 132+45
STA. 135+00 TO STA 141+92
STA. 256+26 TO STA 283+77
STA. 304+41 TO STA 338+55
(LOOKING NORTH)
STA. 106+90
STA. 113+00
STA. 117+00
STA. 122+00
* CONC. DRIVEWAY



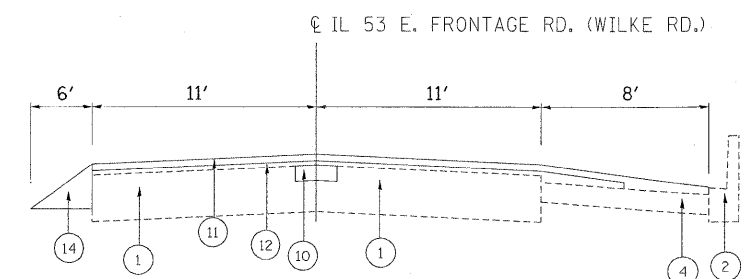
EXISTING TYPICAL SECTION
IL 53 E. FRONTAGE RD. (WILKE RD.)
STA 132+45 TO STA. 135+00



PROPOSED TYPICAL SECTION
IL 53 E. FRONTAGE RD. (WILKE RD.)
STA. 84+99 TO STA 99+69
(LOOKING NORTH)



PROPOSED TYPICAL SECTION
IL 53 W. & E. FRONTAGE RD. (WILKE RD.)
STA. 103+62 TO STA 132+45
STA. 135+00 TO STA 141+92
STA. 256+26 TO STA 283+77
STA. 304+41 TO STA 338+55
(LOOKING NORTH)



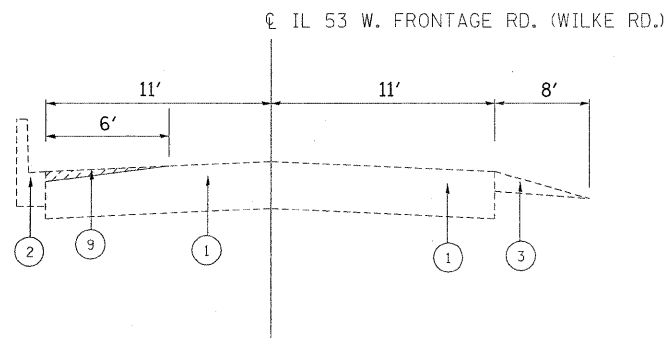
PROPOSED TYPICAL SECTION
IL 53 E. FRONTAGE RD. (WILKE RD.)
STA 132+45 TO STA. 135+00

NOTE: CONTRACTOR SHALL MILL FIRST, THEN PATCH
SEE STANDARD BD-22.

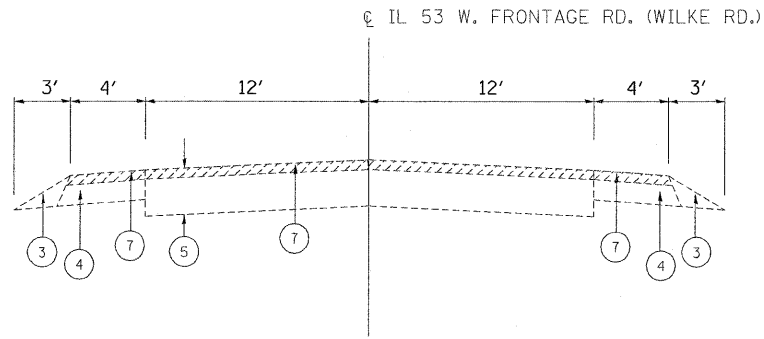
LEGEND

- ① EXISTING PCC PAVEMENT ±9"
- ② EXISTING COMBINATION CONCRETE CURB & GUTTER
- ③ EXISTING AGGREGATE SHOULDER
- ④ EXISTING HMA SHOULDER
- ⑤ EXISTING HMA PAVEMENT ±12"
- ⑥ EXISTING PCC SIDEWALK
- ⑦ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
- ⑨ PROPOSED PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH)
- ⑩ PROPOSED PARTIAL DEPTH REMOVAL 3"
- ⑪ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- ⑫ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- ⑬ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑭ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B

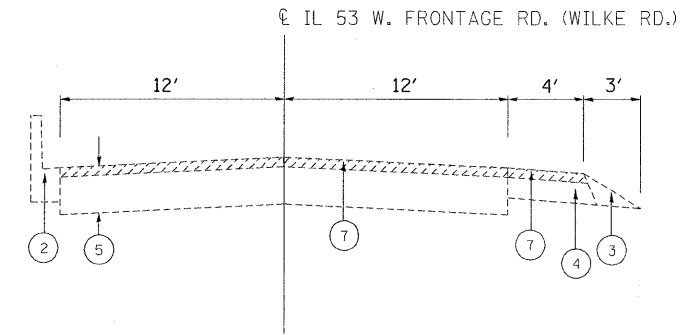
FILE NAME =	USER NAME = abreuh	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL 53 E. & W. FRONTAGE RD (WILKE RD)			RTE.	SECTION	COUNTY	SHEETS	NO.
cd:\pwork\pwork\ABREUH\0146101\0185609-Design.dgn		DRAWN -	REVISED -		TYPICAL SECTIONS			305	2009-076 RS	COOK	34	5
		CHECKED -	REVISED -		SCALE: NTS	SHEET NO.	OF SHEETS	STA. X	TO STA. X	CONTRACT NO. 60110		
		DATE -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							



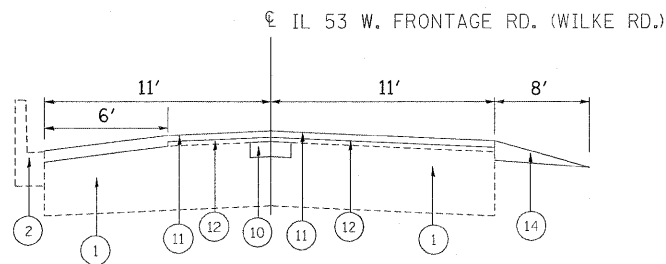
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IL 53 W. FRONTAGE RD. (WILKE RD.)
STA. 242+70 TO STA 256+26
(LOOKING NORTH)



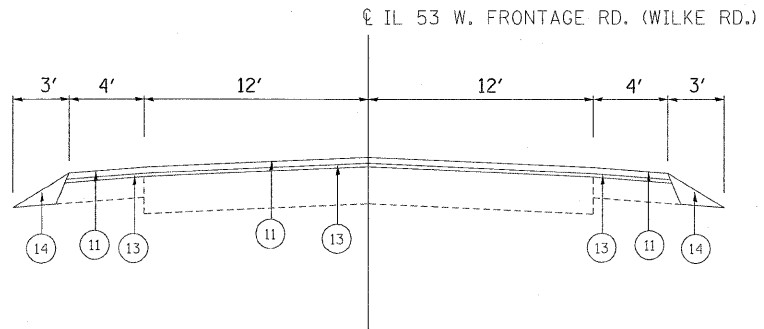
EXISTING TYPICAL SECTION
IL 53 W. FRONTAGE RD. (WILKE RD.)
STA. 283+77 TO STA 294+00
(LOOKING NORTH)



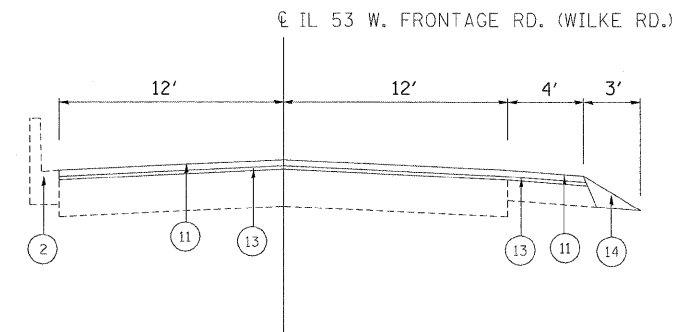
EXISTING TYPICAL SECTION
IL 53 W. FRONTAGE RD. (WILKE RD.)
STA 294+00 TO STA. 299+68
(LOOKING NORTH)



PROPOSED TYPICAL SECTION
IL 53 W. FRONTAGE RD. (WILKE RD.)
STA. 242+70 TO STA 256+26
(LOOKING NORTH)



PROPOSED TYPICAL SECTION
IL 53 W. FRONTAGE RD. (WILKE RD.)
STA. 283+77 TO STA 294+00
(LOOKING NORTH)



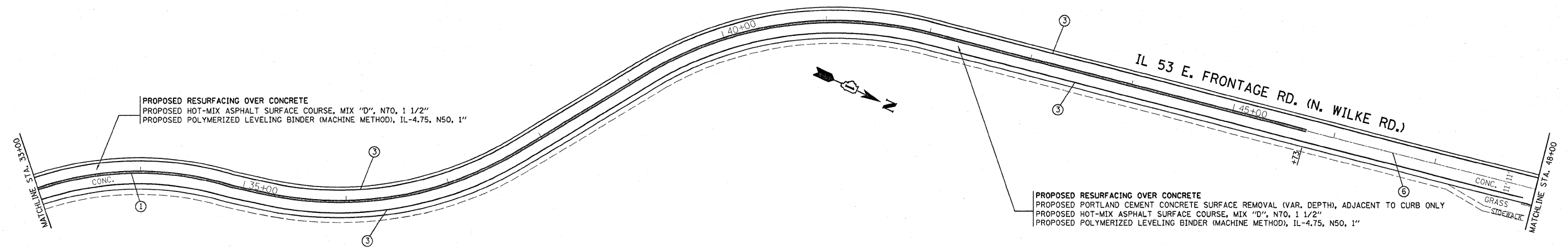
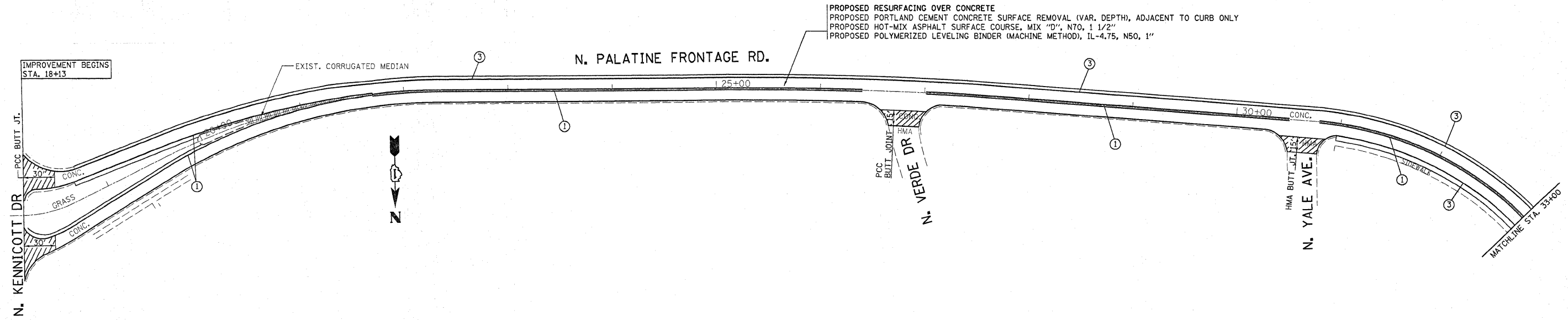
PROPOSED TYPICAL SECTION
IL 53 W. FRONTAGE RD. (WILKE RD.)
STA 294+00 TO STA. 299+68
(LOOKING NORTH)

NOTE: CONTRACTOR SHALL MILL FIRST, THEN PATCH
SEE STANDARD BD-22.

LEGEND

- ① EXISTING PCC PAVEMENT ±9"
- ② EXISTING COMBINATION CONCRETE CURB & GUTTER
- ③ EXISTING AGGREGATE SHOULDER
- ④ EXISTING HMA SHOULDER
- ⑤ EXISTING HMA PAVEMENT ±12"
- ⑥ EXISTING PCC SIDEWAK
- ⑦ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
- ⑨ PROPOSED PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH)
- ⑩ PROPOSED PARTIAL DEPTH REMOVAL 3"
- ⑪ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", NTO, 1 1/2"
- ⑫ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- ⑬ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑭ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B

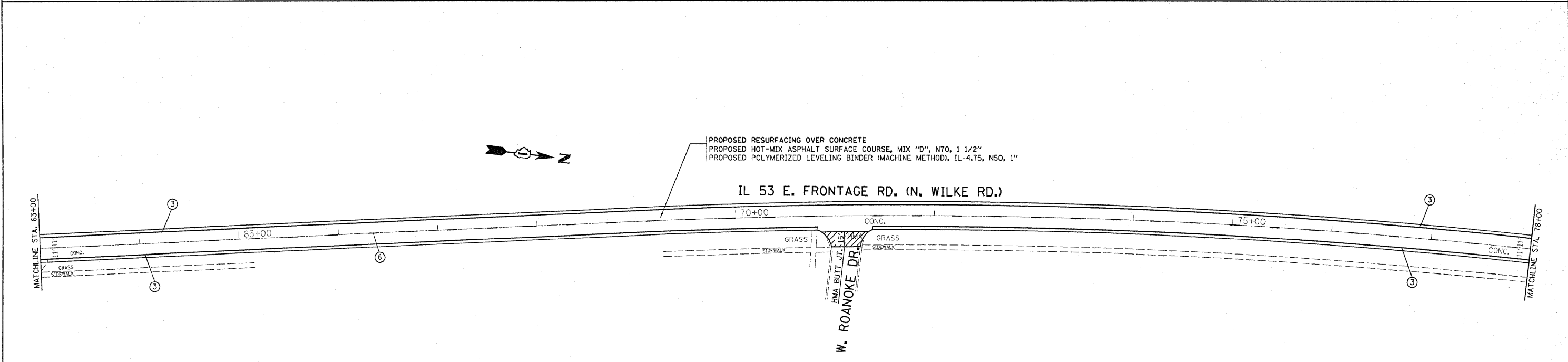
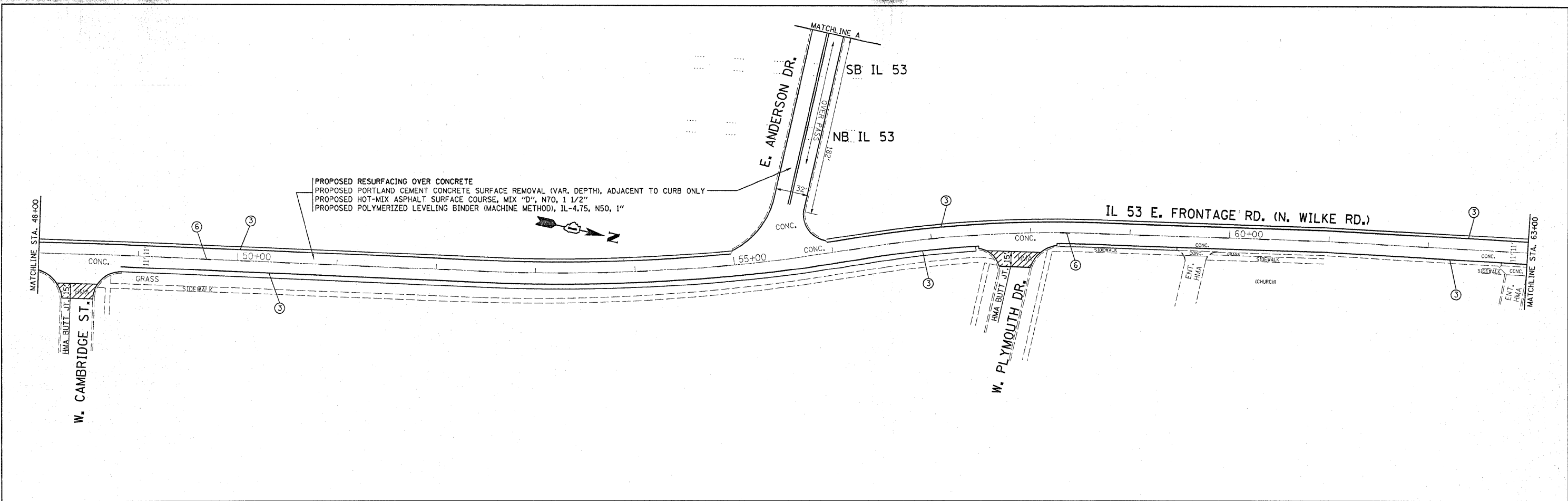
FILE NAME =	USER NAME = abreuah	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL 53 W. FRONTAGE RD (WILKE RD)		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
es:\pw\work\VPWIDOT\ABREUAH\2014610\185409-Design.dgn		DRAWN -	REVISED -		TYPICAL SECTIONS		305	2009-076 RS	COOK	34	7	
		CHECKED -	REVISED -		SCALE: NTS	SHEET NO. OF SHEETS	STA. X	TO STA. X	CONTRACT NO. 60110			
		DATE -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							



LEGEND

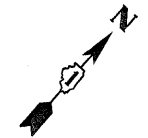
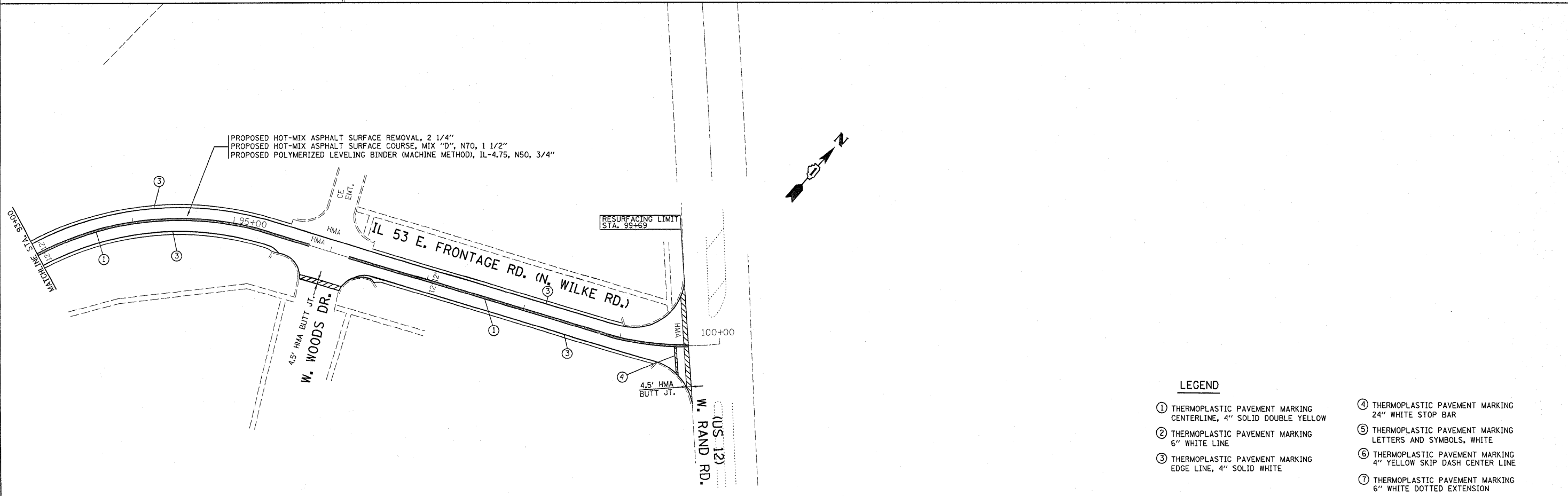
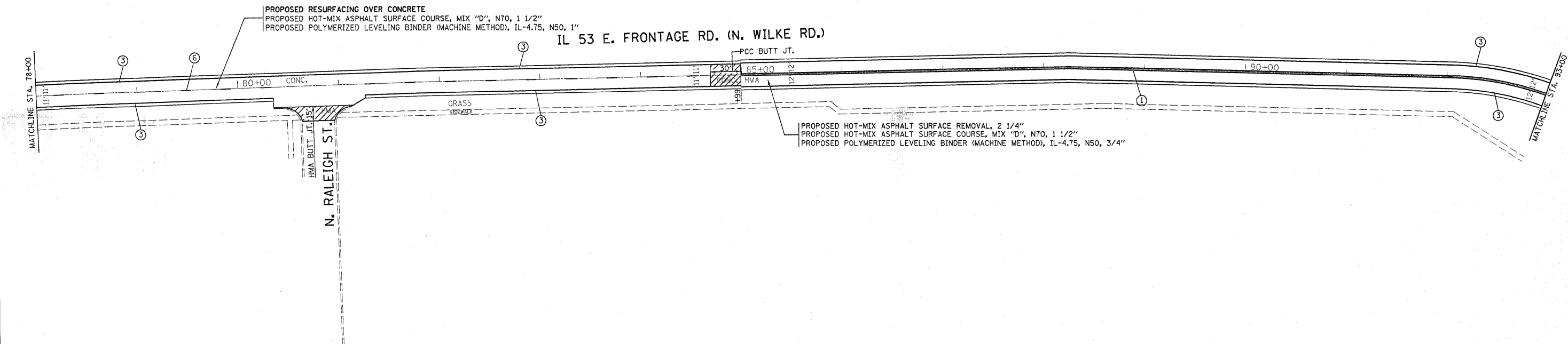
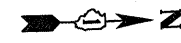
- ① THERMOPLASTIC PAVEMENT MARKING CENTERLINE, 4" SOLID DOUBLE YELLOW
- ② THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE
- ③ THERMOPLASTIC PAVEMENT MARKING EDGE LINE, 4" SOLID WHITE
- ④ THERMOPLASTIC PAVEMENT MARKING 24" WHITE STOP BAR
- ⑤ THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE
- ⑥ THERMOPLASTIC PAVEMENT MARKING 4" YELLOW SKIP DASH CENTER LINE
- ⑦ THERMOPLASTIC PAVEMENT MARKING 6" WHITE DOTTED EXTENSION

FILE NAME =	USER NAME = abreuah	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY AND PAVEMENT MARKING PLAN IL 53 E & W FRONTAGE RD. & N & S PALATINE FRONTAGE RD.			F.A.P. RTE. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 50,0000' / IN.		CHECKED -	REVISED -		CONTRACT NO. 60110							
PLOT DATE = 12/14/2009		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							
				SCALE:	SHEET NO.	OF	SHEETS	STA.	TO	STA.		



- LEGEND**
- ① THERMOPLASTIC PAVEMENT MARKING CENTERLINE, 4" SOLID DOUBLE YELLOW
 - ② THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE
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 - ⑥ THERMOPLASTIC PAVEMENT MARKING 4" YELLOW SKIP DASH CENTER LINE
 - ⑦ THERMOPLASTIC PAVEMENT MARKING 6" WHITE DOTTED EXTENSION

FILE NAME =	USER NAME = abreugh	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY AND PAVEMENT MARKING PLAN IL 53 E & W FRONTAGE RD. & N & S PALATINE FRONTAGE RD.			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pw\work\p\WIDOT\ABREUGH\0146101\0185409-sht-plan.dgn		DRAWN -	REVISED -		305	2009-076 RS	COOK	34	9			
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.				CONTRACT NO. 60110			
PLOT DATE = 12/14/2009		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							



LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING CENTERLINE, 4" SOLID DOUBLE YELLOW
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- ⑥ THERMOPLASTIC PAVEMENT MARKING 4" YELLOW SKIP DASH CENTER LINE
- ⑦ THERMOPLASTIC PAVEMENT MARKING 6" WHITE DOTTED EXTENSION

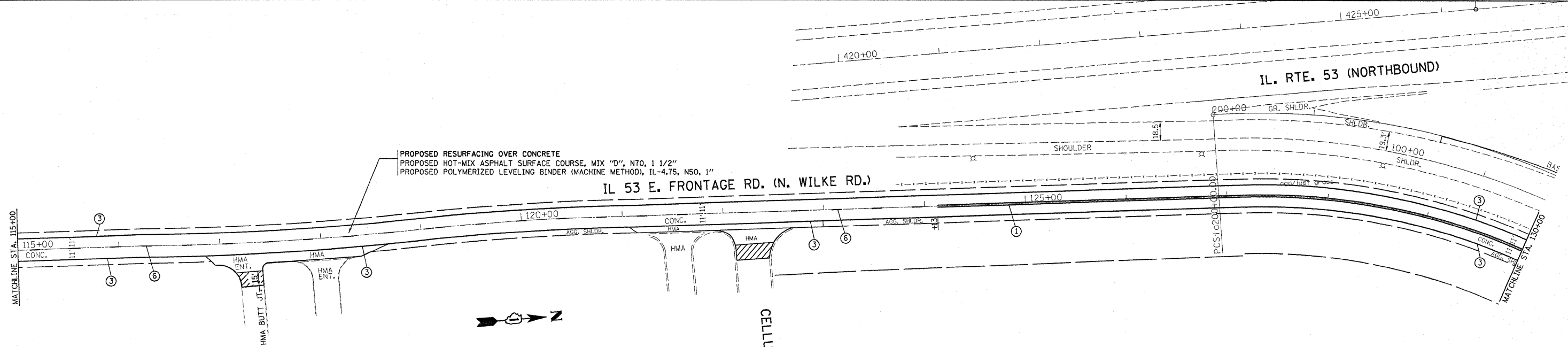
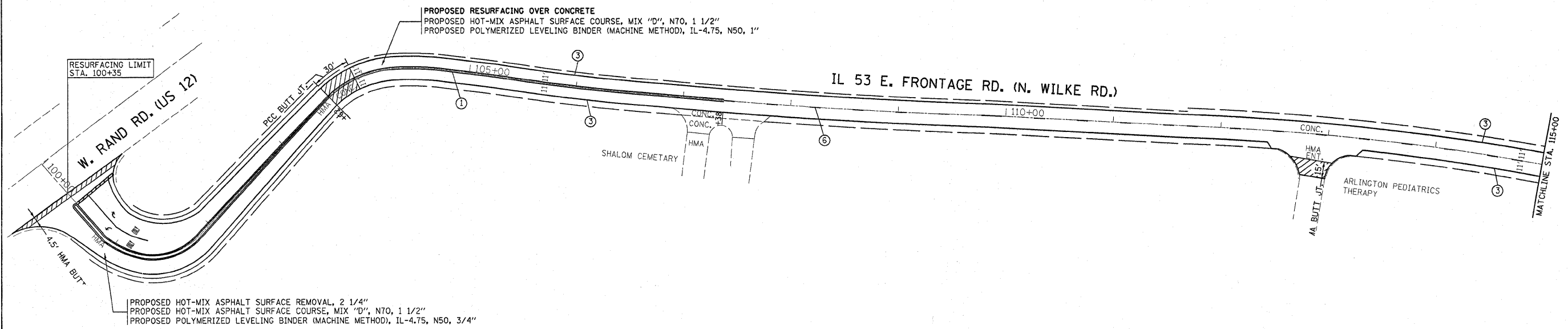
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	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 12/14/2009	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLAN
IL 53 E & W FRONTAGE RD. & N & S PALATINE FRONTAGE RD.**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

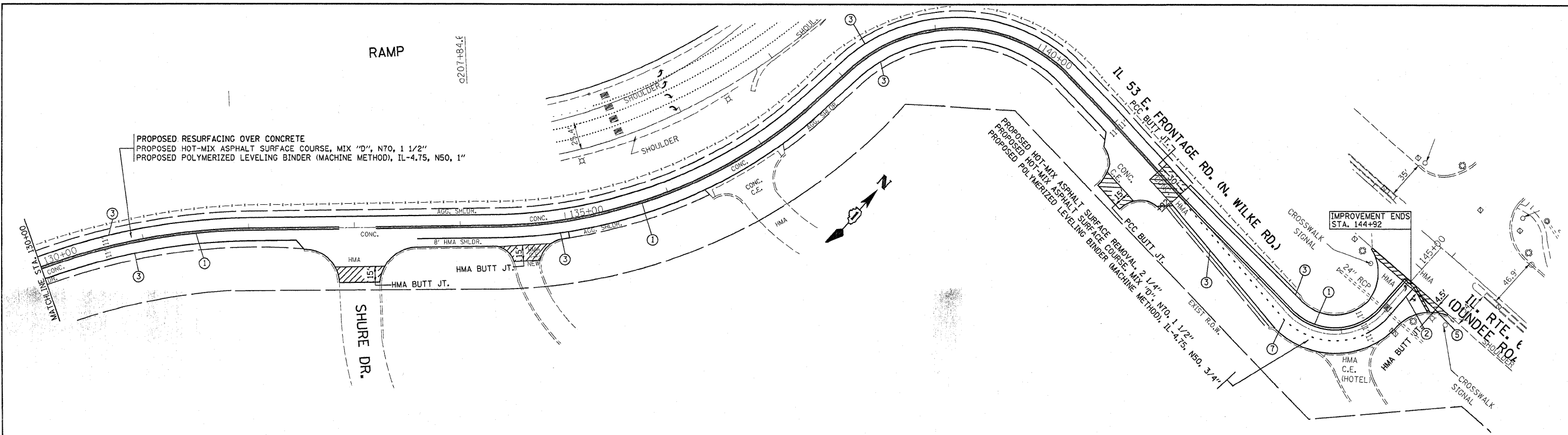
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2009-076 RS	COOK	34	10
CONTRACT NO. 60110				
ILLINOIS FED. AID PROJECT				



LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING CENTERLINE, 4" SOLID DOUBLE YELLOW
- ② THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE
- ③ THERMOPLASTIC PAVEMENT MARKING EDGE LINE, 4" SOLID WHITE
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- ⑥ THERMOPLASTIC PAVEMENT MARKING 4" YELLOW SKIP DASH CENTER LINE
- ⑦ THERMOPLASTIC PAVEMENT MARKING 6" WHITE DOTTED EXTENSION

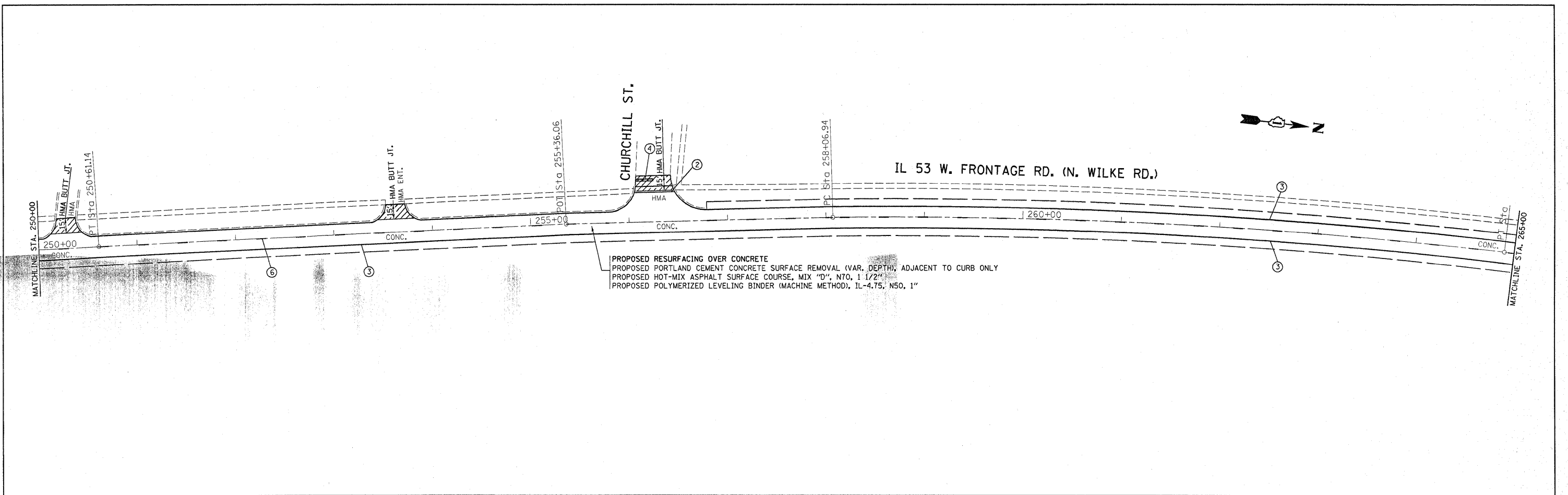
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PLOT SCALE = 50,0000' / IN.		CHECKED -	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	CONTRACT NO. 60110		
PLOT DATE = 12/14/2009		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							



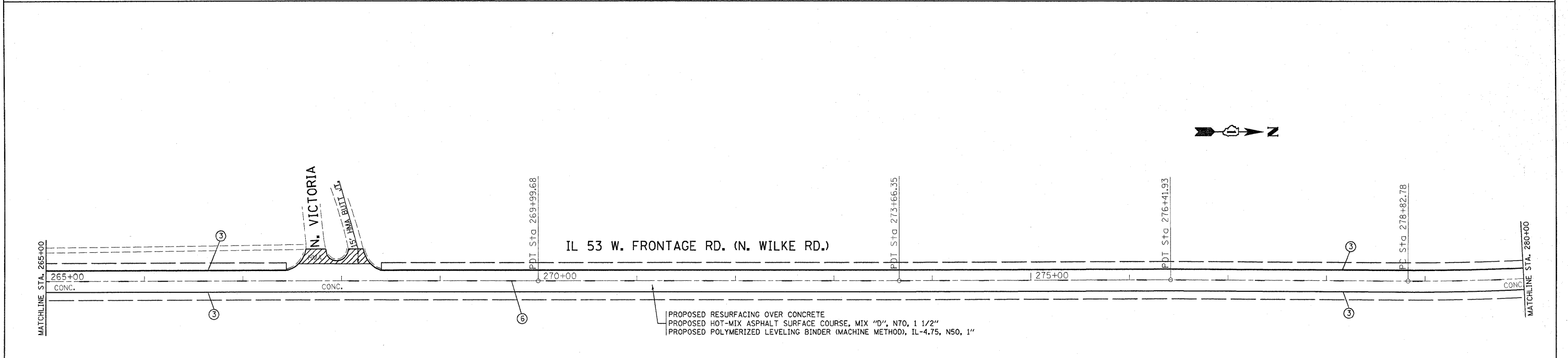
LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING CENTERLINE, 4" SOLID DOUBLE YELLOW
- ② THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE
- ③ THERMOPLASTIC PAVEMENT MARKING EDGE LINE, 4" SOLID WHITE
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- ⑥ THERMOPLASTIC PAVEMENT MARKING 4" YELLOW SKIP DASH CENTER LINE
- ⑦ THERMOPLASTIC PAVEMENT MARKING 6" WHITE DOTTED EXTENSION

FILE NAME =	USER NAME = abreuah	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY AND PAVEMENT MARKING PLAN IL 53 E & W FRONTAGE RD. & N & S PALATINE FRONTAGE RD.	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cr:\pw\work\p\dot\abreuah\0146101\01050099-sht-plan.dgn	PLOT SCALE = 50,0000' / IN.	DRAWN -	REVISED -			305	2009-076 RS	COOK	34	12
PLOT DATE = 12/14/2009	DATE -	CHECKED -	REVISED -			SCALE: SHEET NO. OF SHEETS STA. TO STA.		CONTRACT NO. 60110 ILLINOIS FED. AID PROJECT		



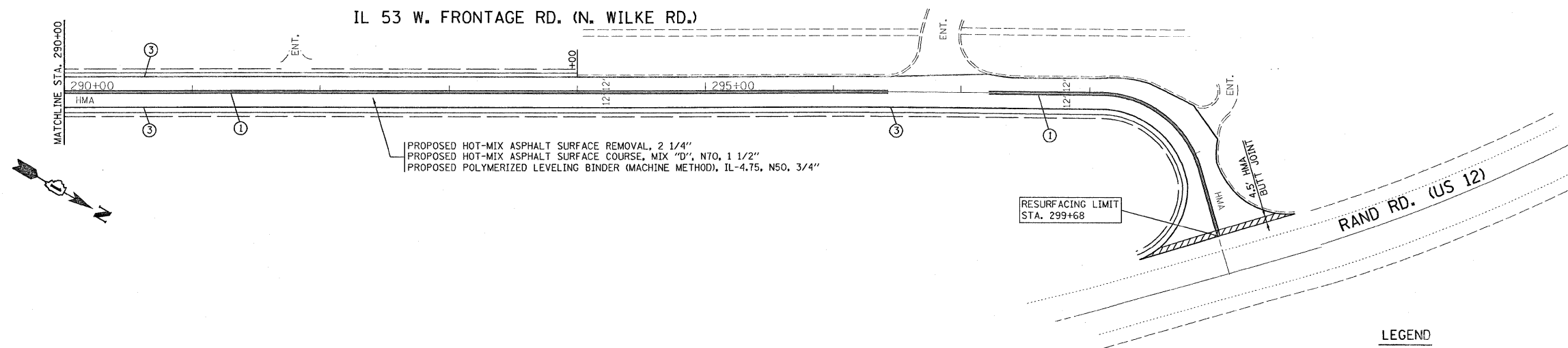
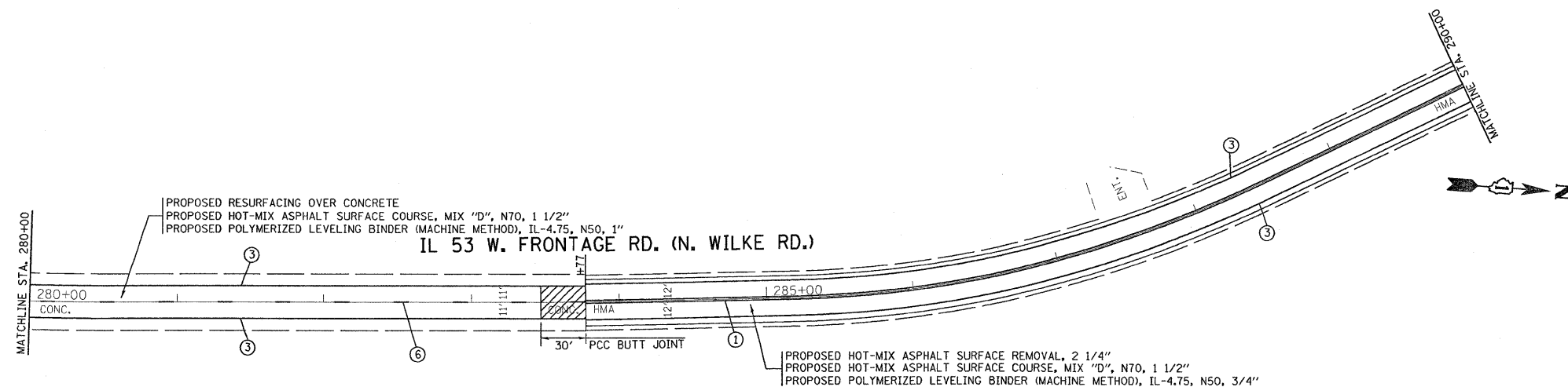
PROPOSED RESURFACING OVER CONCRETE
 PROPOSED PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VAR. DEPTH), ADJACENT TO CURB ONLY
 PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
 PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"



PROPOSED RESURFACING OVER CONCRETE
 PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
 PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"

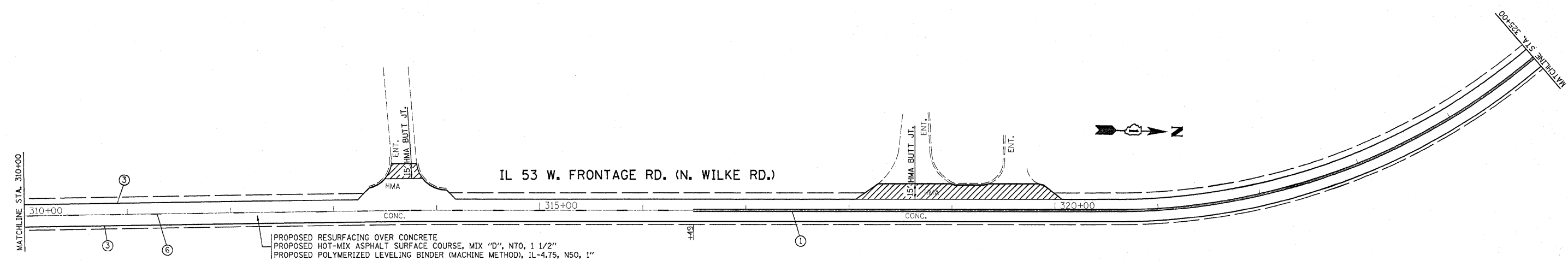
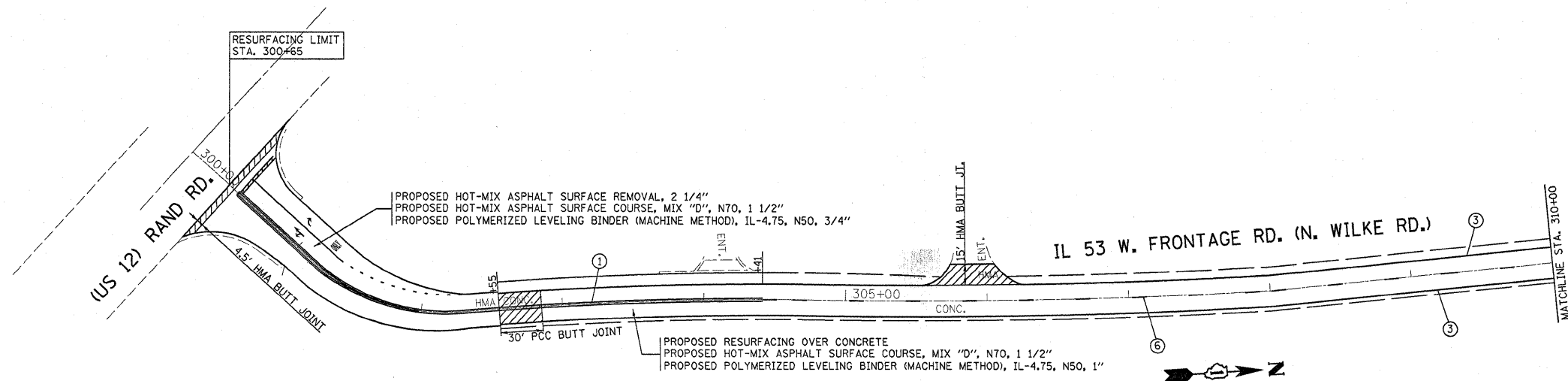
- LEGEND**
- ① THERMOPLASTIC PAVEMENT MARKING CENTERLINE, 4" SOLID DOUBLE YELLOW
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 - ⑦ THERMOPLASTIC PAVEMENT MARKING 6" WHITE DOTTED EXTENSION

FILE NAME =	USER NAME = abreuah	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY AND PAVEMENT MARKING PLAN IL 53 E & W FRONTAGE RD. & N & S PALATINE FRONTAGE RD.	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\p1dot\abreuah\d014610\10185209-shs-plan.dgn		DRAWN -	REVISED -			305	2009-076 RS	COOK	34	14
PLOT SCALE = 50,0000' / IN.		CHECKED -	REVISED -			CONTRACT NO. 60110				
PLOT DATE = 12/14/2009		DATE -	REVISED -			ILLINOIS FED. AID PROJECT				
					SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.



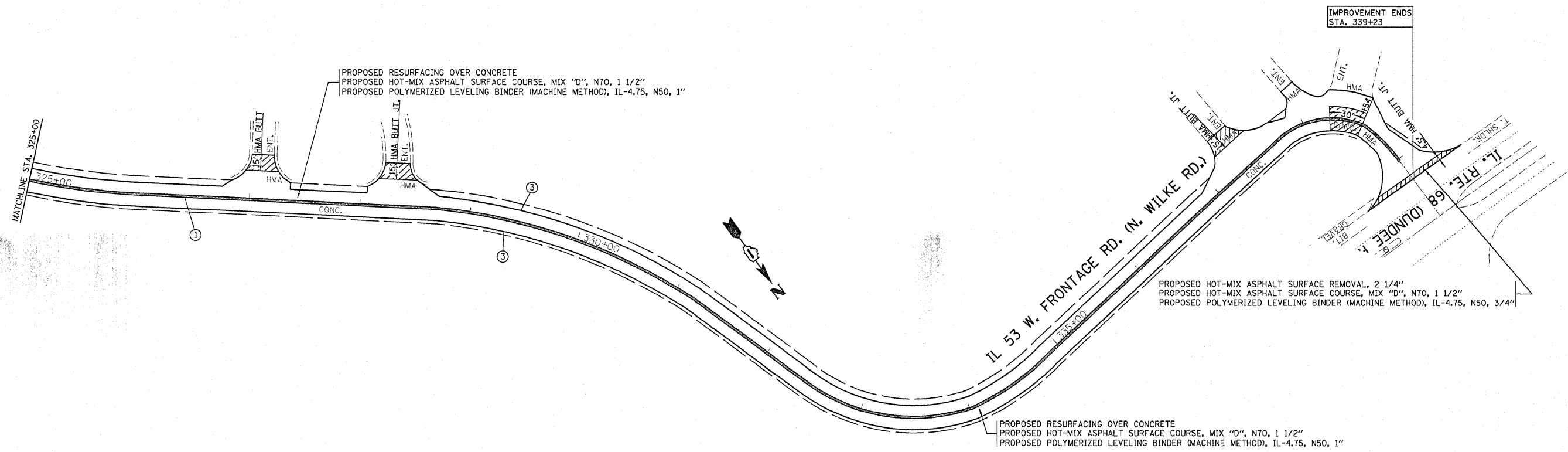
- LEGEND**
- ① THERMOPLASTIC PAVEMENT MARKING CENTERLINE, 4" SOLID DOUBLE YELLOW
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 - ⑥ THERMOPLASTIC PAVEMENT MARKING 4" YELLOW SKIP DASH CENTER LINE
 - ⑦ THERMOPLASTIC PAVEMENT MARKING 6" WHITE DOTTED EXTENSION

FILE NAME =	USER NAME = abrauh	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY AND PAVEMENT MARKING PLAN			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT DATE = 8/7/2009		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							



- LEGEND**
- ① THERMOPLASTIC PAVEMENT MARKING CENTERLINE, 4" SOLID DOUBLE YELLOW
 - ② THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE
 - ③ THERMOPLASTIC PAVEMENT MARKING EDGE LINE, 4" SOLID WHITE
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 - ⑥ THERMOPLASTIC PAVEMENT MARKING 4" YELLOW SKIP DASH CENTER LINE
 - ⑦ THERMOPLASTIC PAVEMENT MARKING 6" WHITE DOTTED EXTENSION

FILE NAME =	USER NAME = abreuah	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY AND PAVEMENT MARKING PLAN IL 53 E & W FRONTAGE RD. & N & S PALATINE FRONTAGE RD.			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT DATE = 12/14/2009		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							
				SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.			



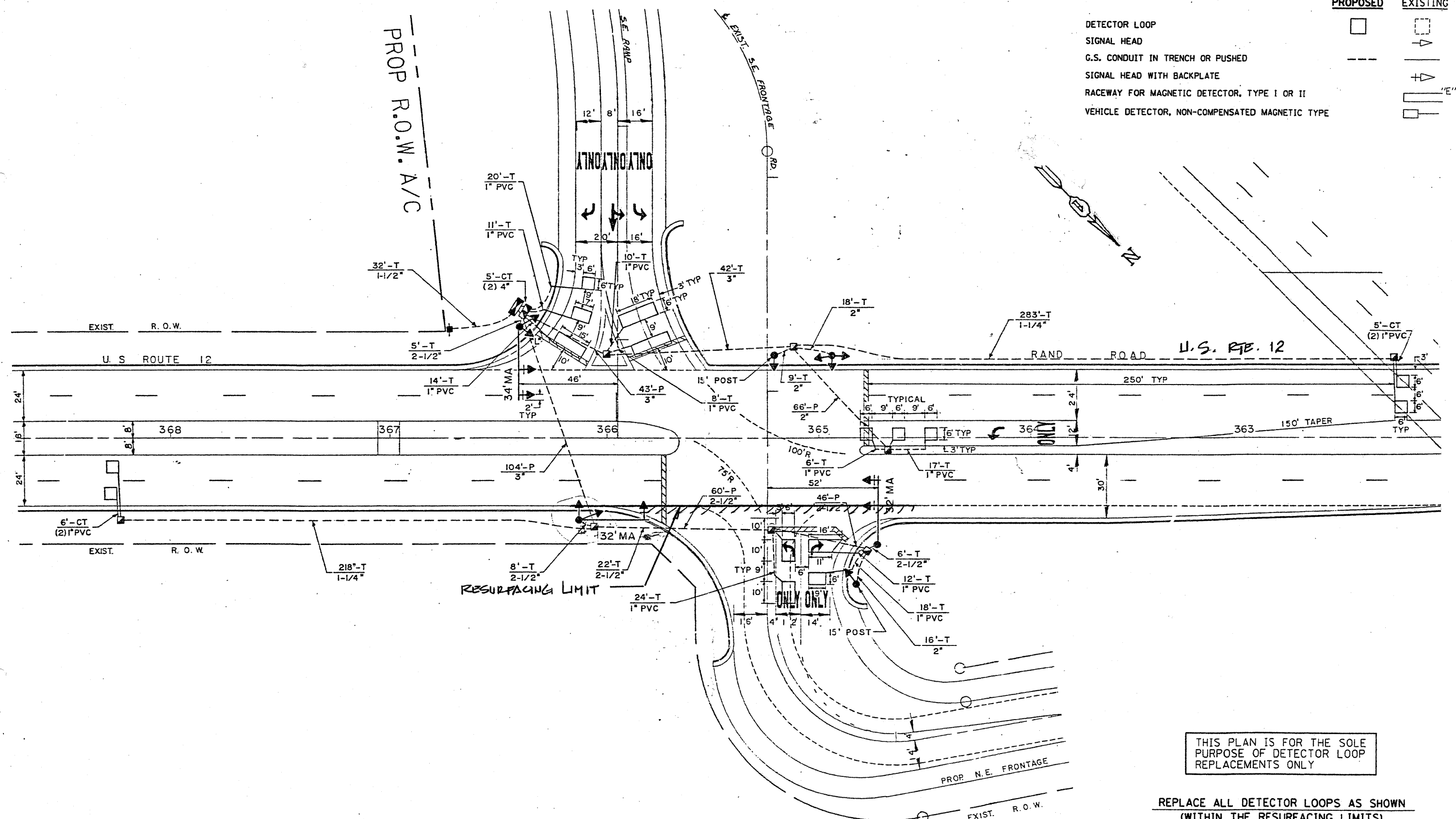
LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING CENTERLINE, 4" SOLID DOUBLE YELLOW
- ② THERMOPLASTIC PAVEMENT MARKING 6" WHITE LINE
- ③ THERMOPLASTIC PAVEMENT MARKING EDGE LINE, 4" SOLID WHITE
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- ⑥ THERMOPLASTIC PAVEMENT MARKING 4" YELLOW SKIP DASH CENTER LINE
- ⑦ THERMOPLASTIC PAVEMENT MARKING 6" WHITE DOTTED EXTENSION

FILE NAME = c:\pwork\pwidot\abreuh\d0146101\0185099-sht-plan.dgn	USER NAME = abreuh	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY AND PAVEMENT MARKING PLAN IL 53 E & W FRONTAGE RD. & N & S PALATINE FRONTAGE RD.			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED -		305	2009-076 RS	COOK	34	17			
	PLOT DATE = 12/14/2009	CHECKED -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.			CONTRACT NO. 60110		ILLINOIS FED. AID PROJECT		

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP	□	□
SIGNAL HEAD	—	△
G.S. CONDUIT IN TRENCH OR PUSHED	---	—
SIGNAL HEAD WITH BACKPLATE	—	△
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II	—	— "E"
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE	□	□



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	153	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = kanthephixaybc	DESIGNED - BCK	REVISED -
ca:\p\work\p\WIDOT\KANTHAPHIXAYBC\d81126	4\traffic.legend.v7.dgn	DRAWN - BCK	REVISED -
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	PLOT DATE = 4/3/2009	DATE -	REVISED -

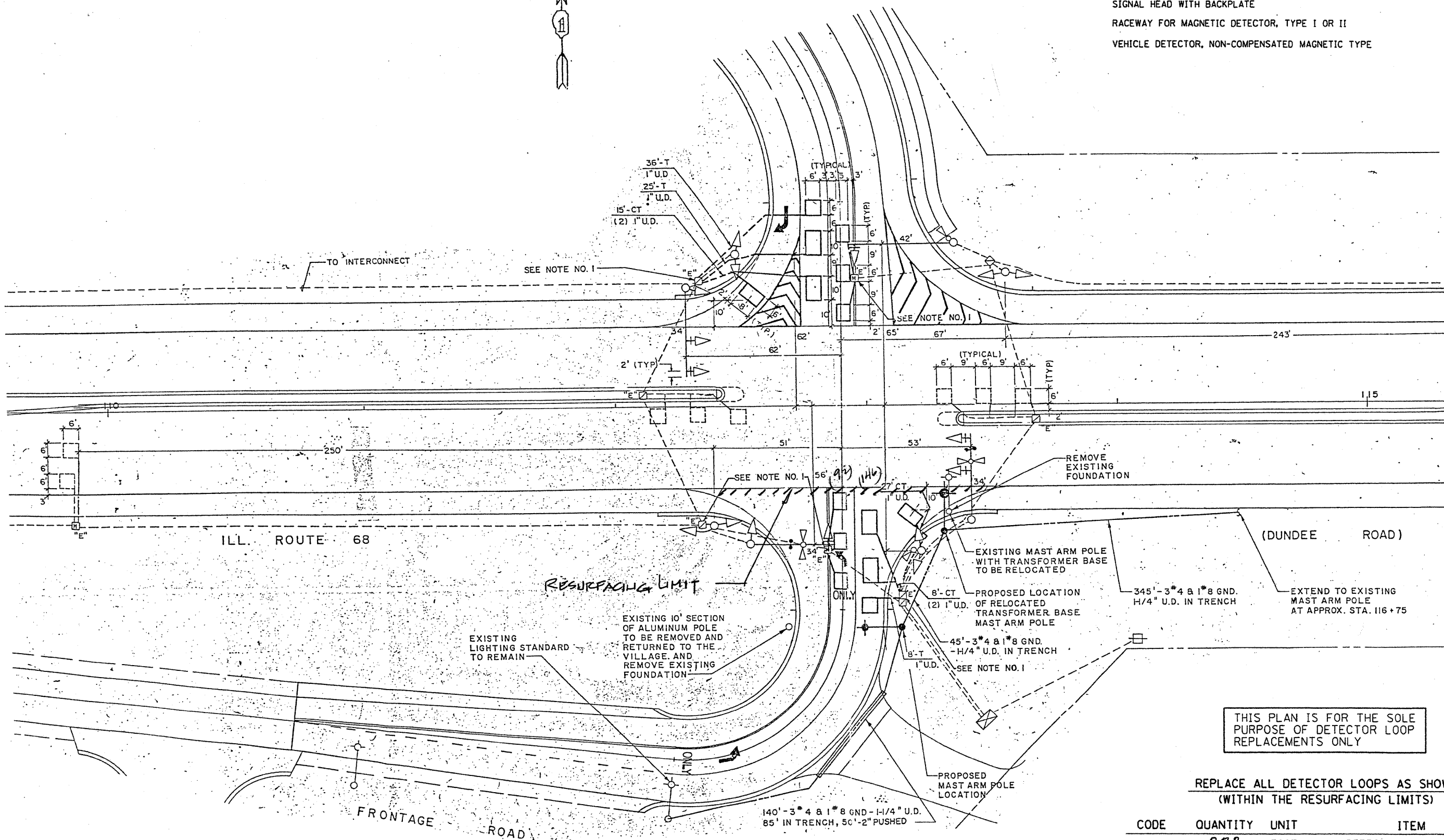
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE - DETECTOR LOOP REPLACEMENT
IL. RTE. 53 (E. FRONTAGE) @ U.S. 12**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2009-076 RS	Cook	34	18
CONTRACT NO.				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	238	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME *	USER NAME = kentaphixaybo	DESIGNED - BCK	REVISED -
c:\pwork\PIWIDOT\KANTAPHIXAYBC\d01126	4\traffic.legend.v7.dgn	DRAWN - BCK	REVISED -
	PLOT SCALE = 3/4" = 1' IN.	CHECKED - DAD	REVISED -
	PLOT DATE = 4/3/2009	DATE	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

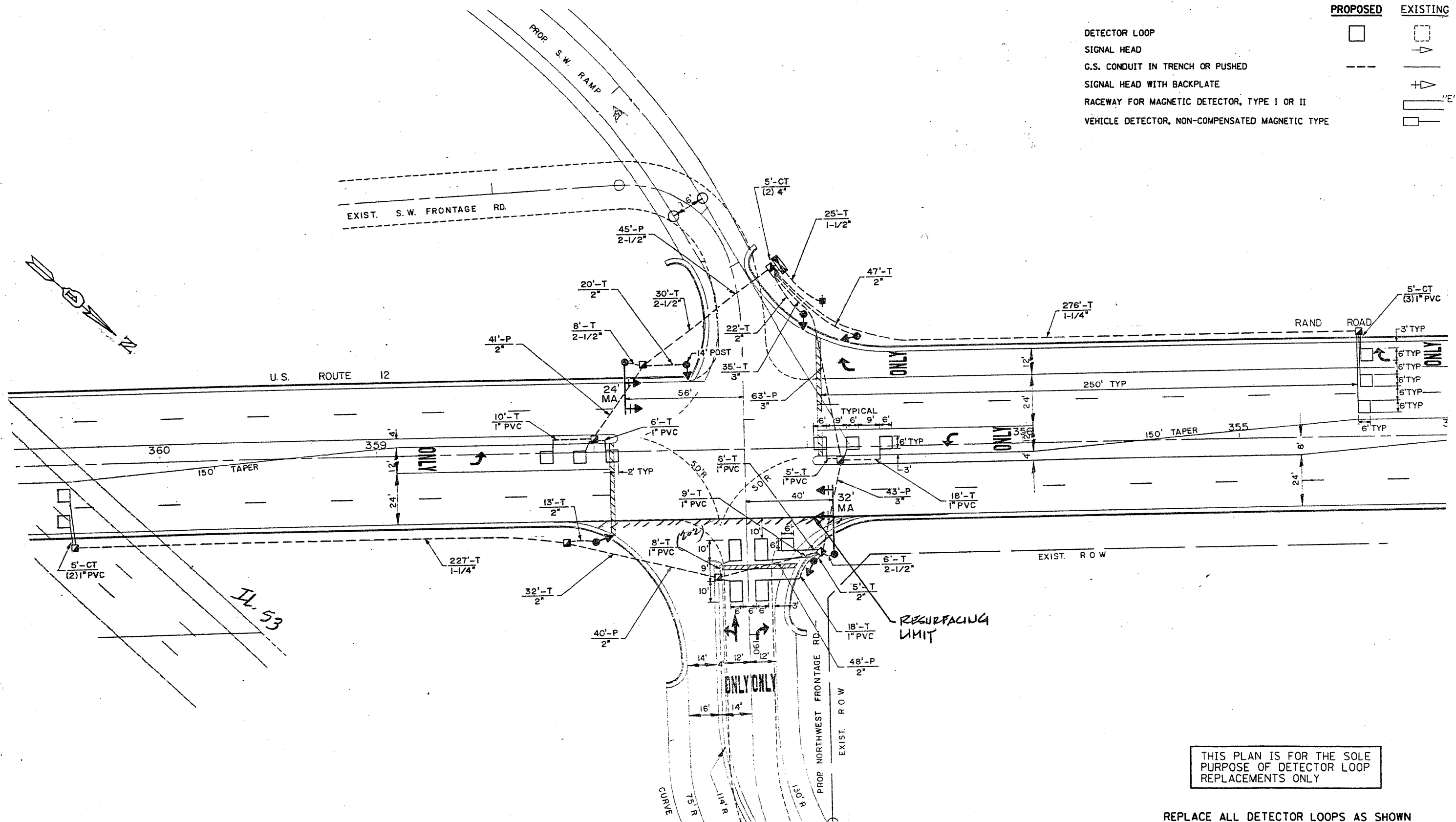
**DISTRICT ONE - DETECTOR LOOP REPLACEMENT
ILL. ROUTE 68 @ E. FRONTAGE RD.**

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2009-076RS	Cook	39	19
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO.	

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	182	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = kanthapixaybo	DESIGNED - BCK	REVISED -
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PLOT DATE = 4/3/2009		DATE -	REVISED -

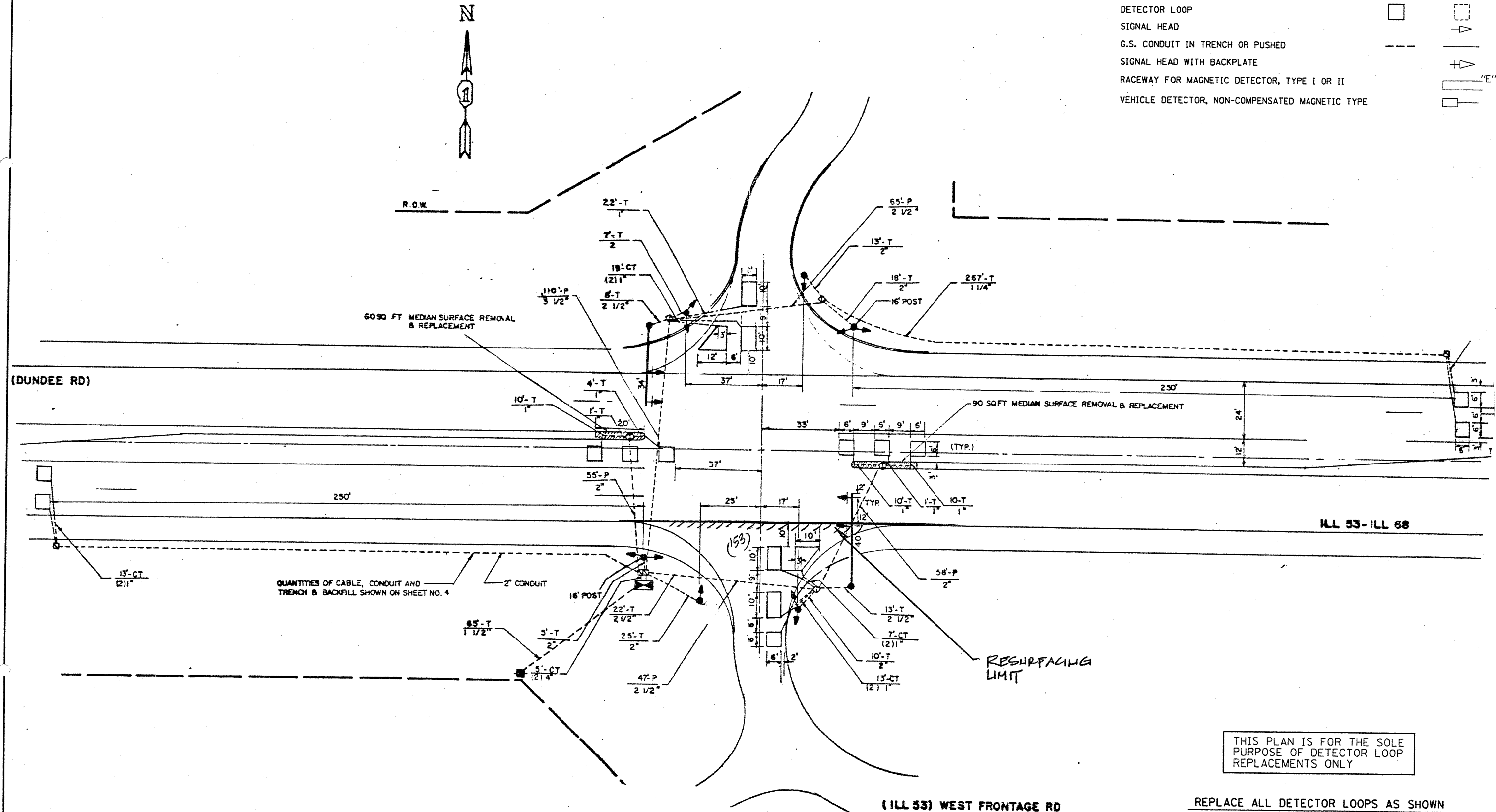
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE - DETECTOR LOOP REPLACEMENT
IL. RTE. 53 (W. FRONTAGE) @ U.S. 12**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2009-076 RS	COOK	34	20
CONTRACT NO.				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	153	FOOT	DETECTOR LOOP, REPLACEMENT

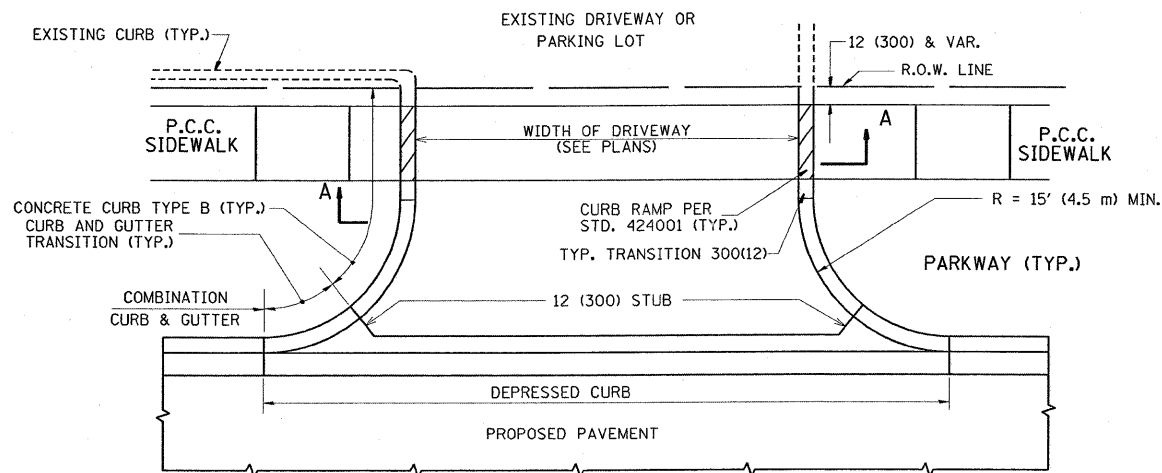
FILE NAME =	USER NAME = kanthaphixaybc	DESIGNED - BCK	REVISED -
ca:\pwwork\pwwid\KANTHAPHIXAYBC\d01126	4\traffic.legend_v7.dgn	DRAWN - BCK	REVISED -
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	PLOT DATE = 4/3/2009	DATE	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

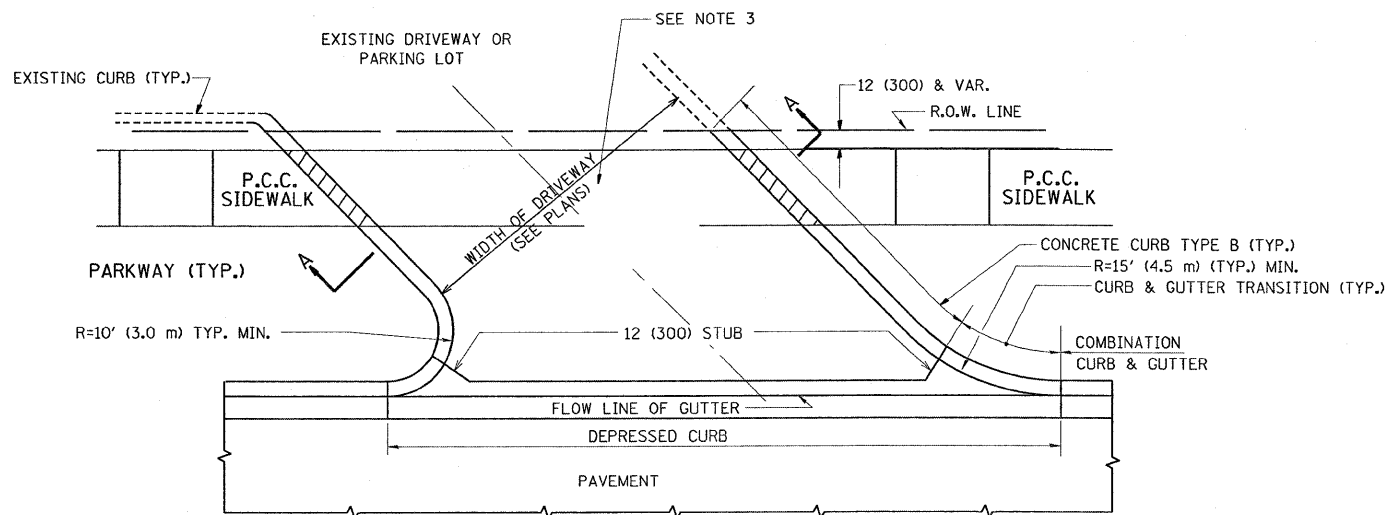
**DISTRICT ONE - DETECTOR LOOP REPLACEMENT
ILL. ROUTE 53/68 @ W. FRONTAGE RD.**

F.A.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2009-076 R3	COOK	34	21
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				

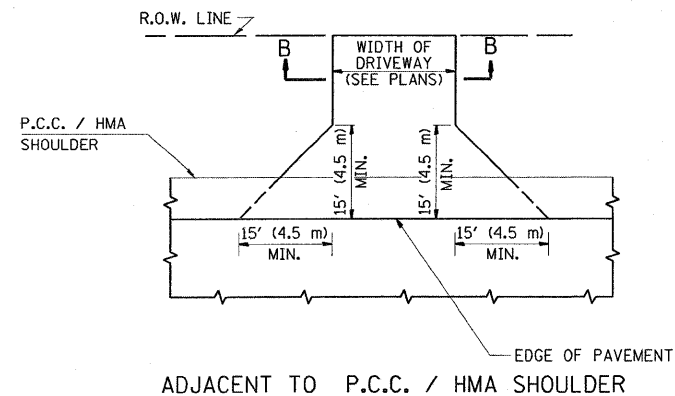
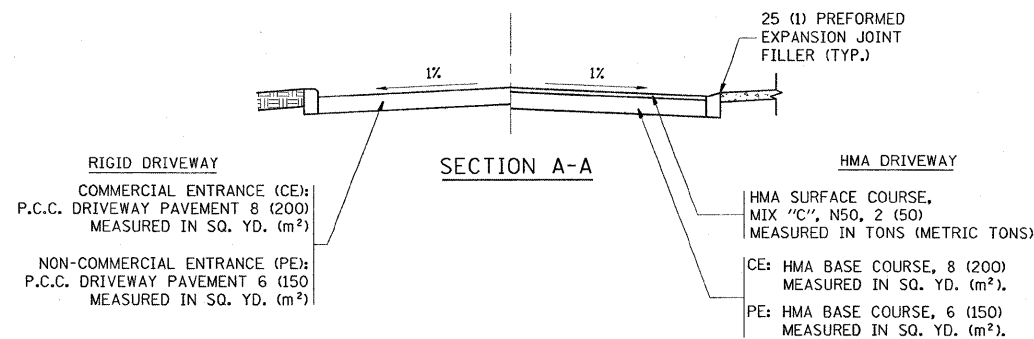
SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.



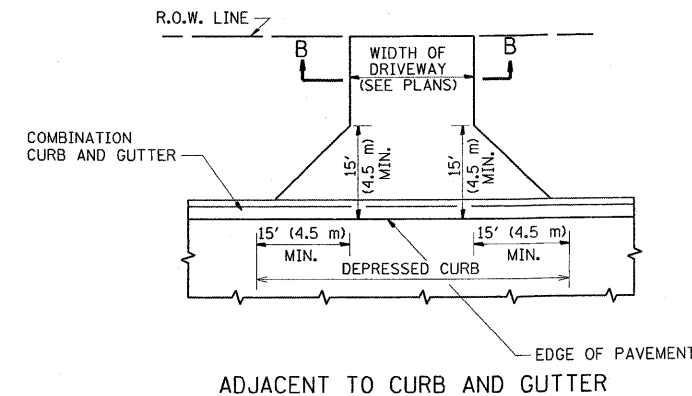
WITH CONCRETE CURB, TYPE B



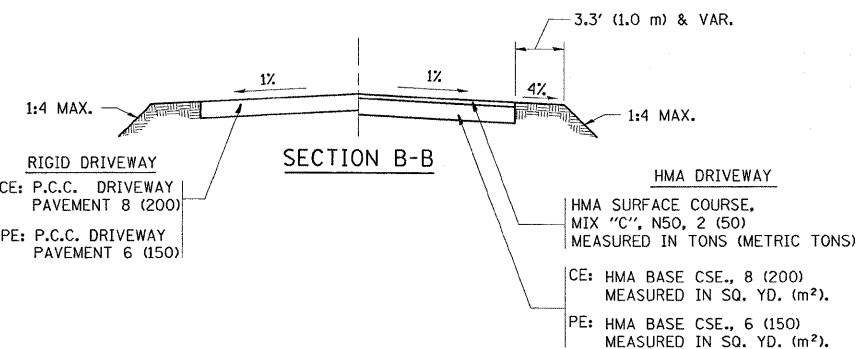
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



RURAL FIELD ENTRANCE (FE)
 HMA SURFACE COURSE,
 MIX "C", N50, 2 (50)
 MEASURED IN TONS (METRIC TONS)
 AGGREGATE BASE CSE., TYPE B, 8 (200)
 MEASURED IN SQ. YD. (m²).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

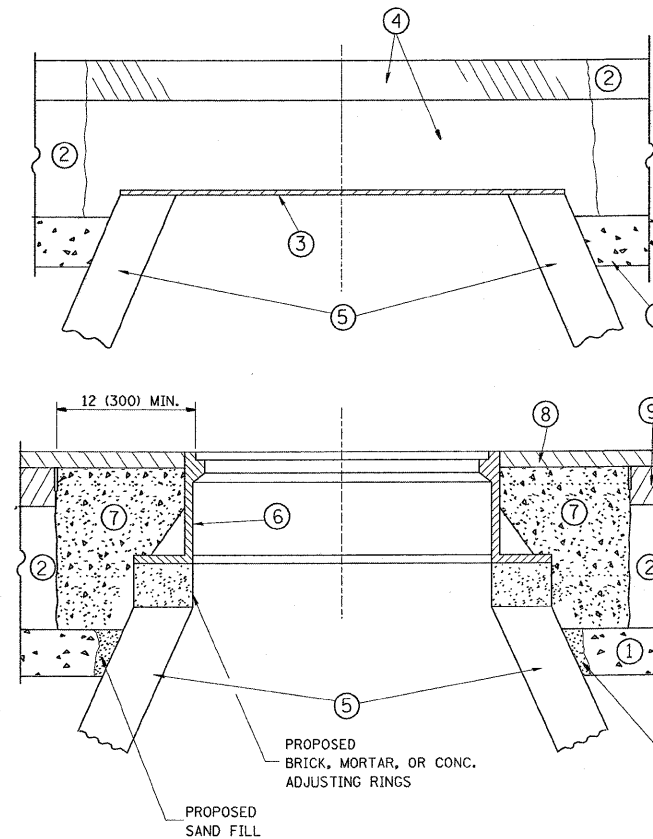
THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

FILE NAME =	USER NAME = abreuah	DESIGNED - R. SHAH	REVISED - M. GOMEZ 04-06-01	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
21\pwork\pwork\abreuah\d0146101\01st5.dgn		DRAWN -	REVISED - P. LOFLUER 04-15-03			305	2009-076 RS	COOK	34	22
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED - R. BORO 01-01-07			BD0156-07 (BD-01)			CONTRACT NO. 60110	
PLOT DATE = 8/5/2009		DATE - 11-04-95	REVISED - R. BORO 06-11-08			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	



CONSTRUCTION PROCEDURES

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
 - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
 - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
 - D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.
- STAGE 2 (AFTER PAVEMENT MILLING)**
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - B) INSTALL THE FRAME AND LID; ADJUST ITS FINAL SURFACE ELEVATION.
 - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

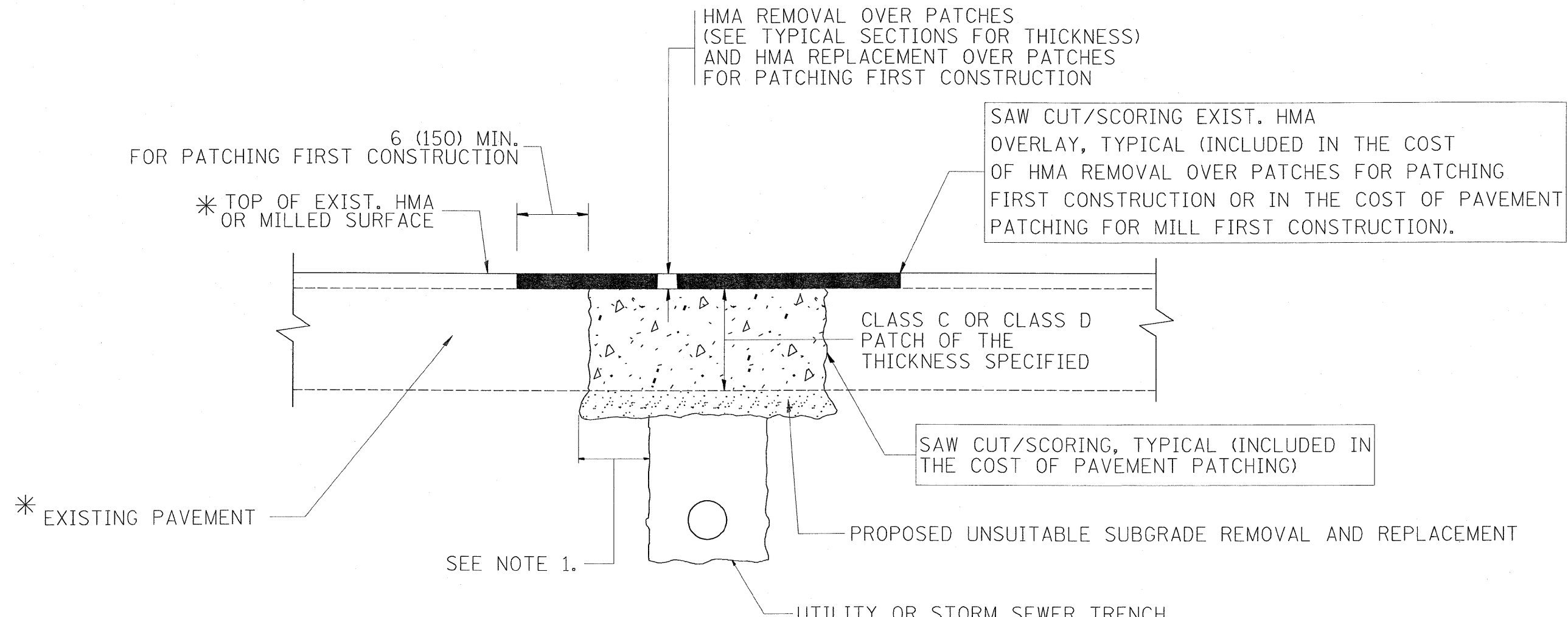
THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL" NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = abroush	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING	F.A.P. RTE. 305	SECTION 2009-076 RS	COUNTY COOK	TOTAL SHEETS 34	SHEET NO. 23	
ct:\pwork\pwork\abroush\d0146101\Draw5.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		
		CHECKED -	REVISED - R. WIEDEMAN 05-14-04		CONTRACT NO. 60110						
		DATE - 10-25-94	REVISED - R. BORO 01-01-07								



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = abreuah	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et\pwork\pwork\abreuah\d0146181\DistS		DRAWN -	REVISED - R. BORO 01-01-07		305	2009-076 RS	COOK	34	24			
PLOT SCALE = 50,0000' / IN.		CHECKED -	REVISED - R. BORO 09-04-07		BD400-04 (BD-22)			CONTRACT NO. 60110				
PLOT DATE = 8/5/2009		DATE - 10-25-94	REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

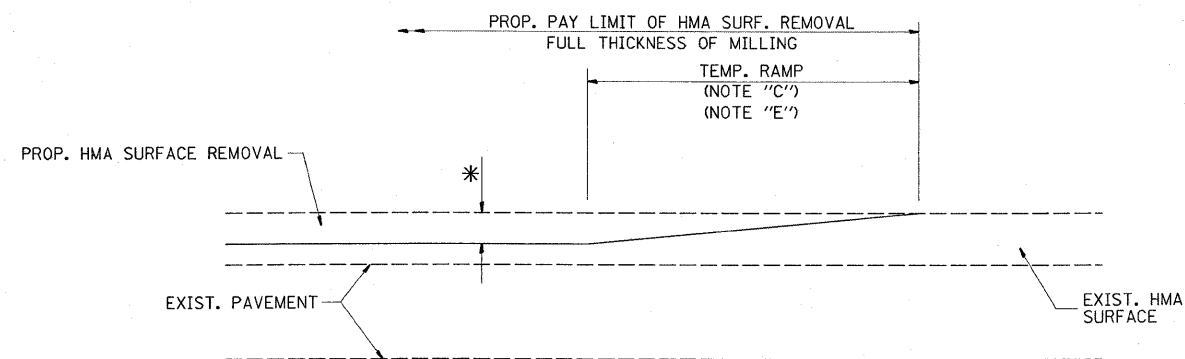
BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

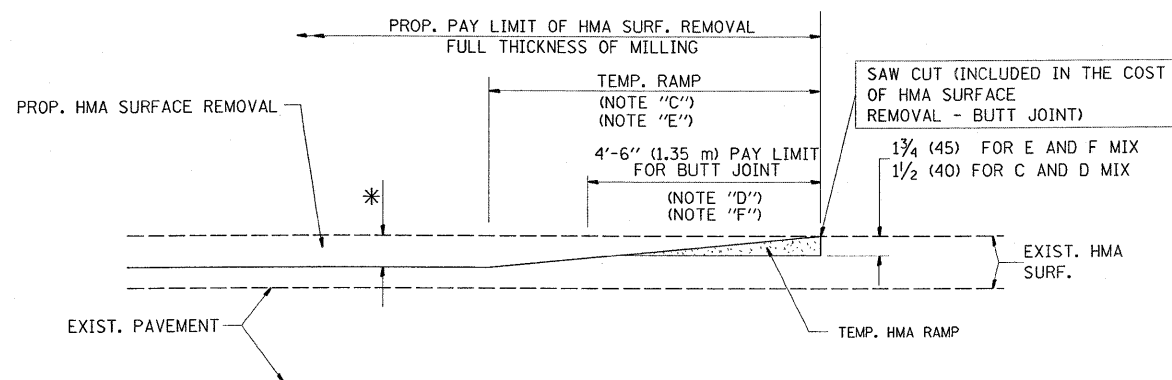
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = abreuah	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et\pw\work\PKWIDOT\ABREUAH\d014610\1\01.st	sd.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97		305	2009-076 RS	COOK	34	25			
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISOR - M. GOMEZ 01-22-01	REVISOR - R. BORO 12-15-09		BD600-06 (BD-24)		CONTRACT NO. 60110					
PLOT DATE = 12/16/2009	DATE - 03-11-94				SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	



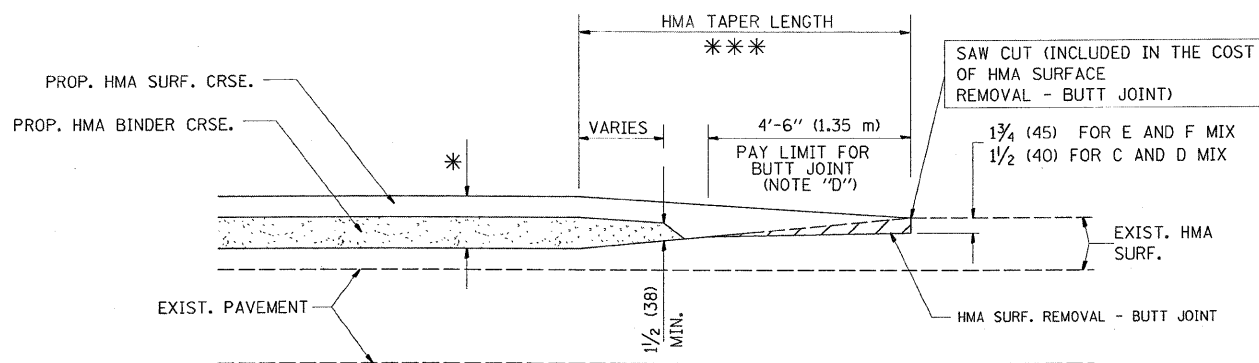
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1



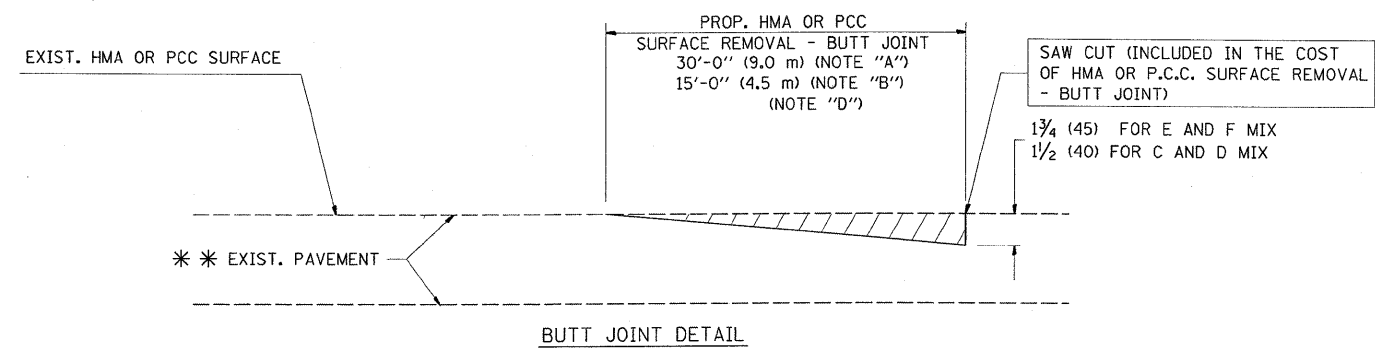
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2
TYPICAL TEMPORARY RAMP

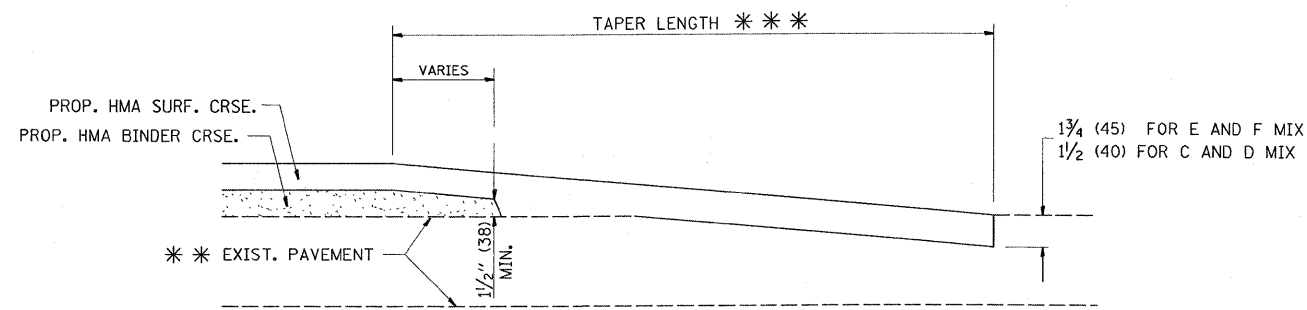


BUTT JOINT AND
HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- ** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

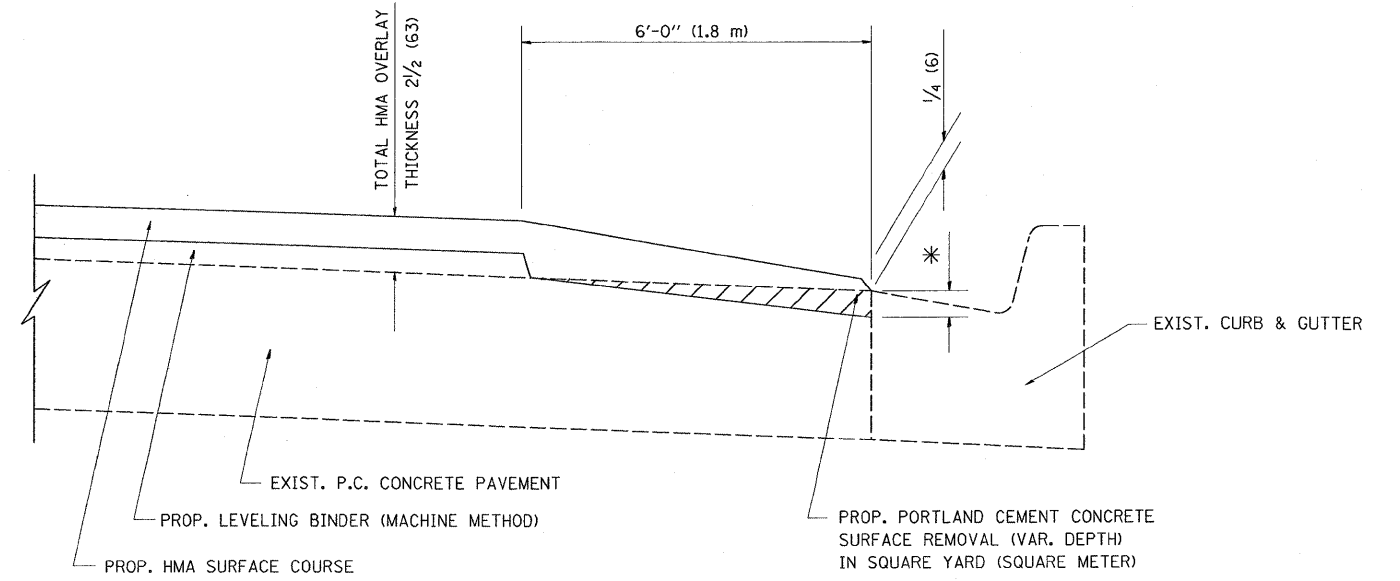
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PLOT SCALE = 50.0000' / IN.	CHECKED -	DATE - 06-13-90	REVISED - M. GOMEZ 04-06-01
PLOT DATE = 8/5/2001	DATE -		REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2009-076 RS	COOK	34	26
BD400-05 BD32			CONTRACT NO. 60110	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



HMA TAPER AT
EDGE OF P.C.C PAVEMENT

MIX	HMA SURFACE		LEVELING BINDER		* MILLING AT GUTTER FLAG
	MIX	THICKNESS	THICKNESS	THICKNESS	
C OR D	1 1/2 (38)	1 1/2 (38)	1 (25)	1 (25)	1/4 (33)
F	1 3/4 (44)	1 3/4 (44)	3/4 (19)	3/4 (19)	1/2 (38)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

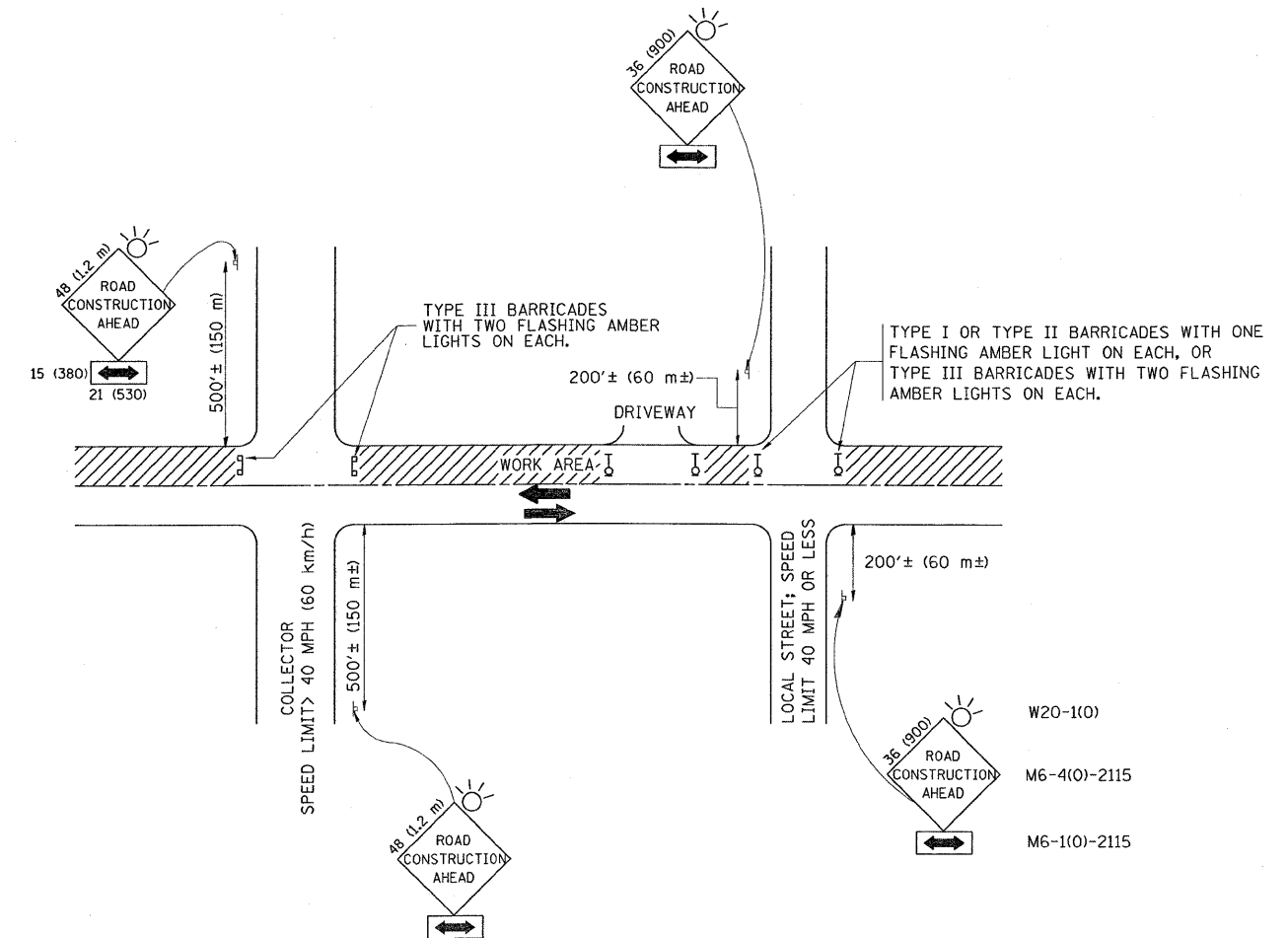
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	PLOT SCALE = 50.0000' / IN.	CHECKED - A. ABBAS	REVISED - E. GOMEZ 12-21-00
	PLOT DATE = 8/5/2009	DATE - 09-10-94	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HMA TAPER AT
EDGE OF P.C.C PAVEMENT

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2009-076 RS	COOK	27	27
BD400-06 (BD33)		CONTRACT NO. 60110		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

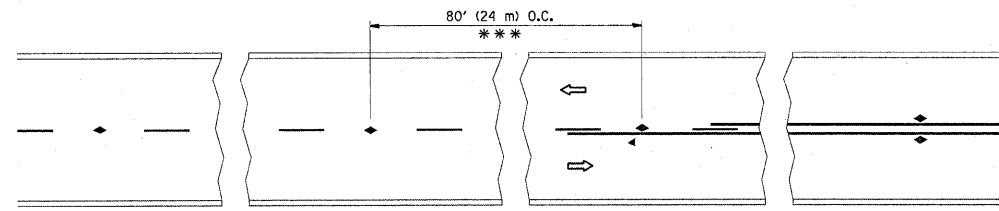
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		CHECKED -	REVISED - A. HOUSEH 10-15-96
		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

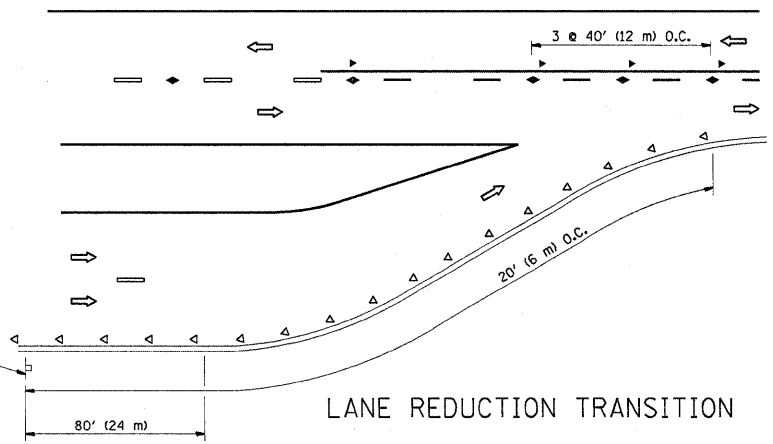
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2009-076 RS	COOK	34	28
TC-10			CONTRACT NO. 60110	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

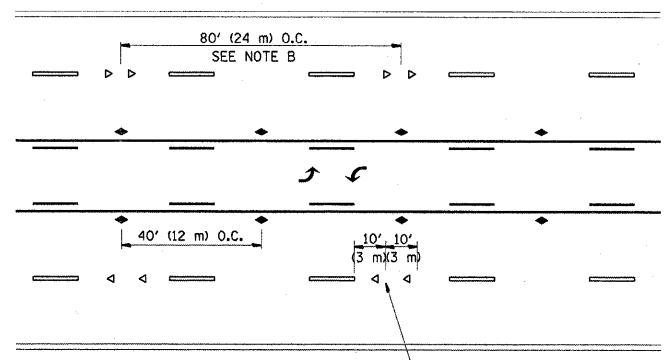


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

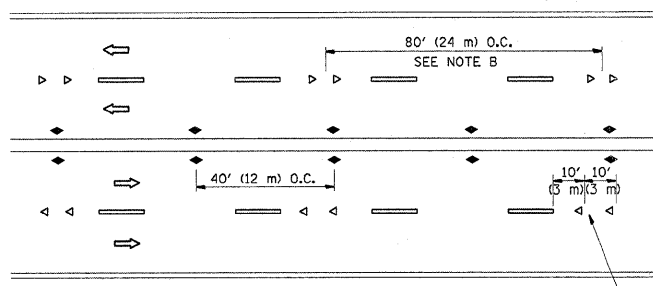
TWO-LANE/TWO-WAY



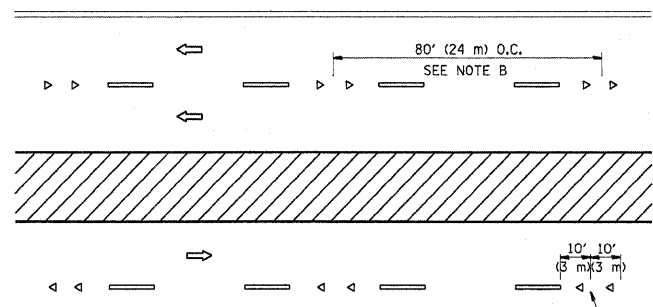
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

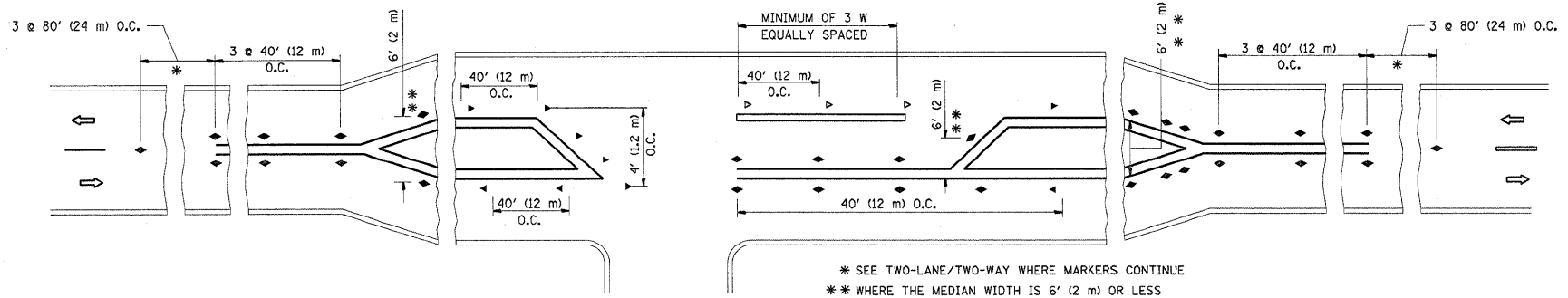
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

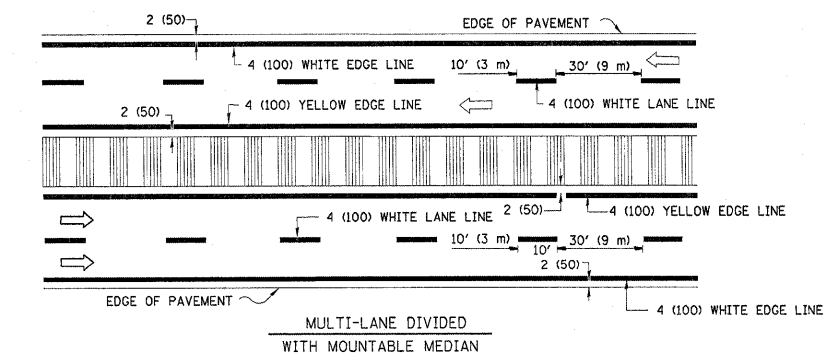
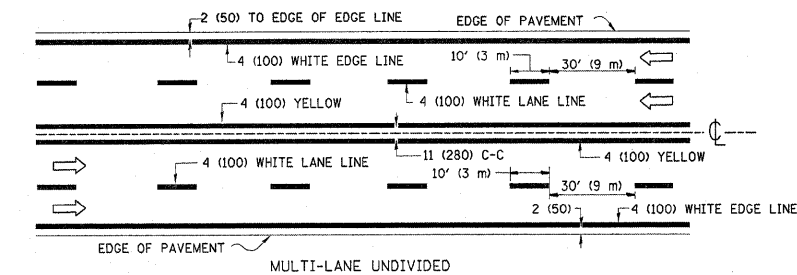
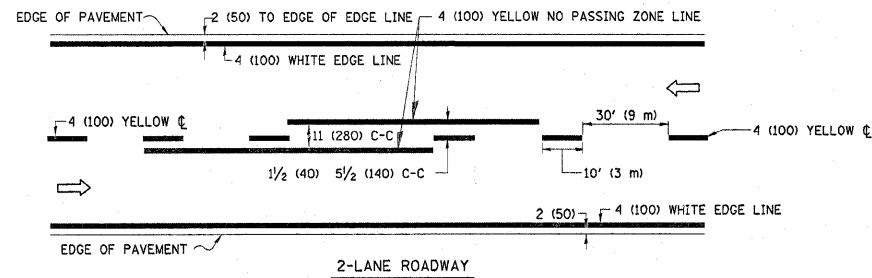
1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

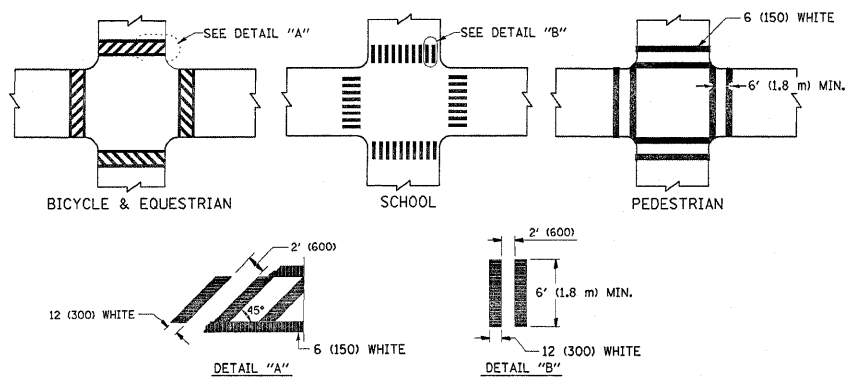
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = abrauh	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pwork\pwork\ABREUAH\0146101\01st	ed.dgn	DRAWN -	REVISED - T. RAMMACHER 03-12-99		305	2009-076 RS	COOK	34	29			
		PLOT SCALE = 50.0000' / IN.	REVISED - T. RAMMACHER 01-06-00		TC-11			CONTRACT NO. 60110				
		PLOT DATE = 12/14/2009	REVISED - C. JUCIUS 09-09-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

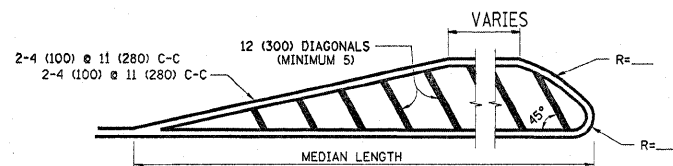
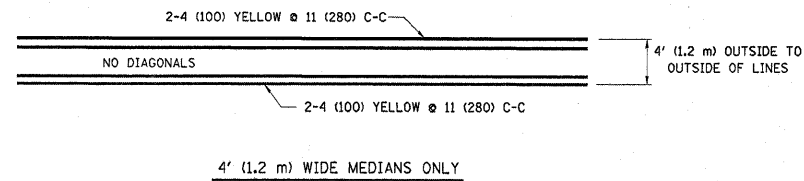


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



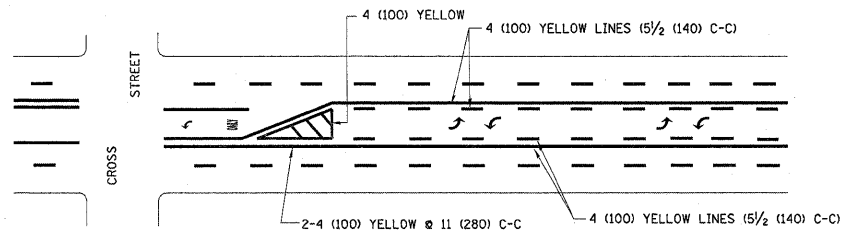
TYPICAL CROSSWALK MARKING



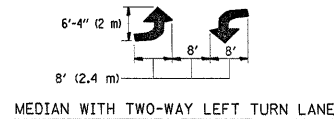
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

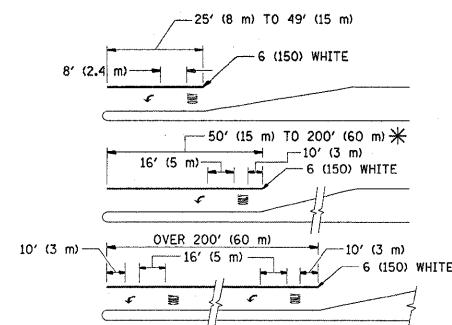
MEDIANS OVER 4' (1.2 m) WIDE



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



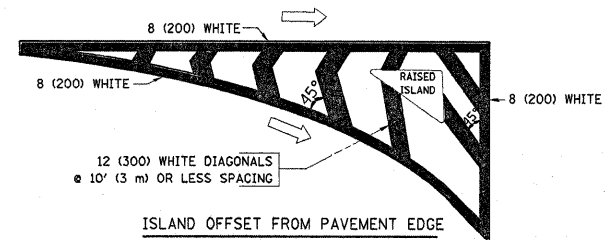
TYPICAL PAINTED MEDIAN MARKING



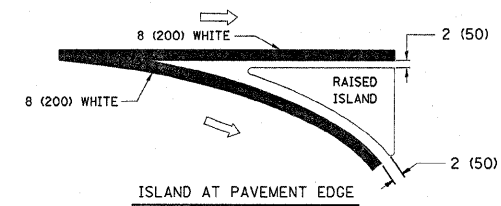
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100)	SKIP-DASH	WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
LANE LINES	5 (125) ON FREEWAYS	SKIP-DASH	WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
TWO WAY LEFT TURN MARKING	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 30 MPH (50 km/h) TO 45MPH (70 km/h) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

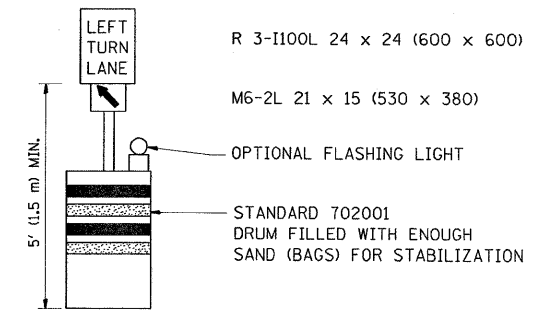
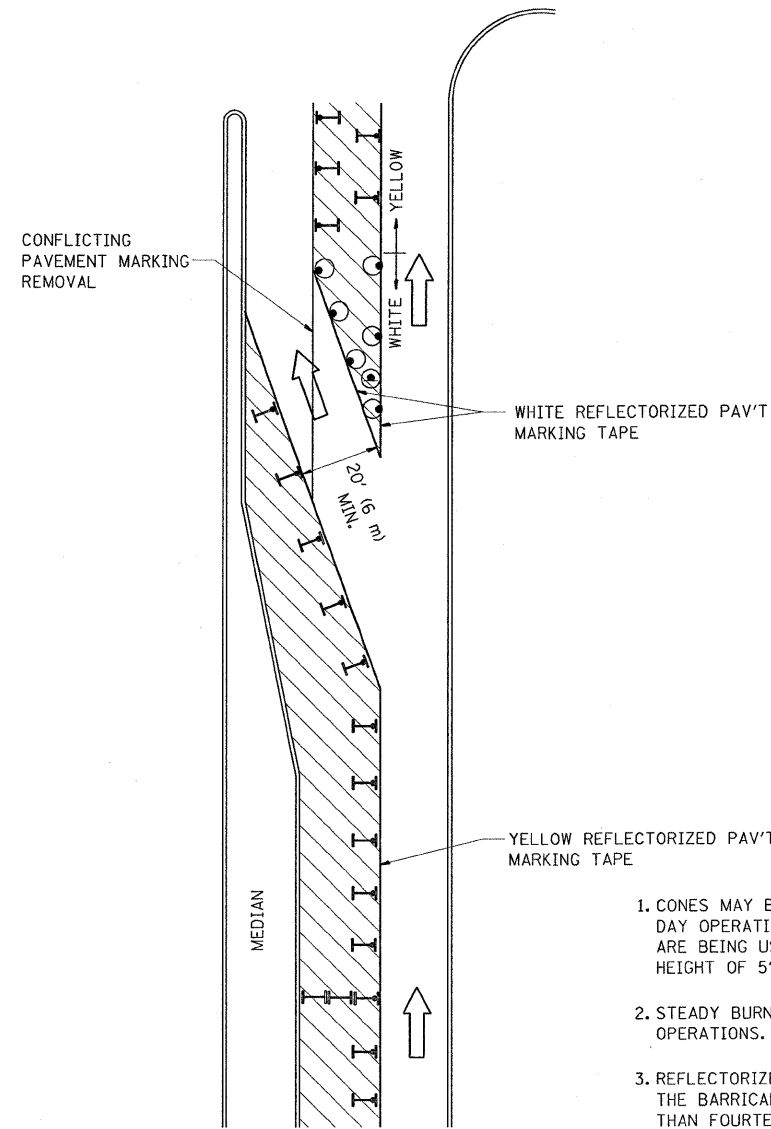
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PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -	REVISED -
PLOT DATE = 12/14/2009	DATE - 03-19-90	REVISED -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE. 305	SECTION 2009-076 RS	COUNTY COOK	TOTAL SHEETS 34	SHEET NO. 30
TC-13		CONTRACT NO. 60110		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

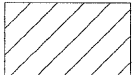







GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

FILE NAME =	USER NAME = abreuh	DESIGNED -	REVISED -T. RAMMACHER 09-08-94
os\pr\work\pwidot\obreuh\d0146101\Draw5	d.dgn	DRAWN -	REVISED - A. HOUSEH 11-07-95
	PLOT SCALE = 50.0000 1/ IN.	CHECKED -	REVISED - A. HOUSEH 10-12-96
	PLOT DATE = 8/5/2009	DATE -	REVISED -T. RAMMACHER 01-06-00

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

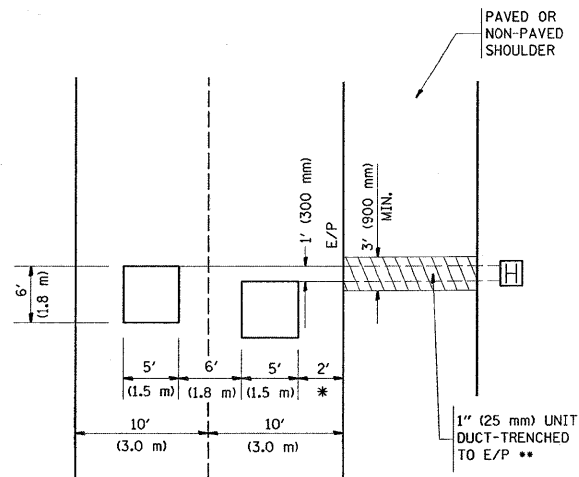
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2009-076 RS	COOK	34	31
TC-14		CONTRACT NO.	60110	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

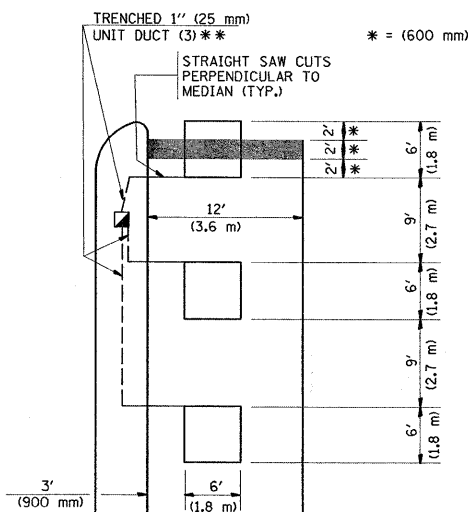


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

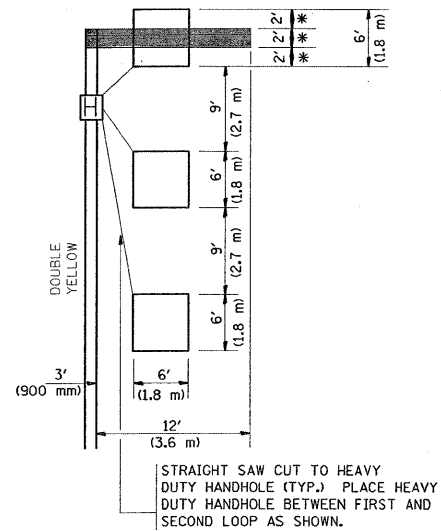


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

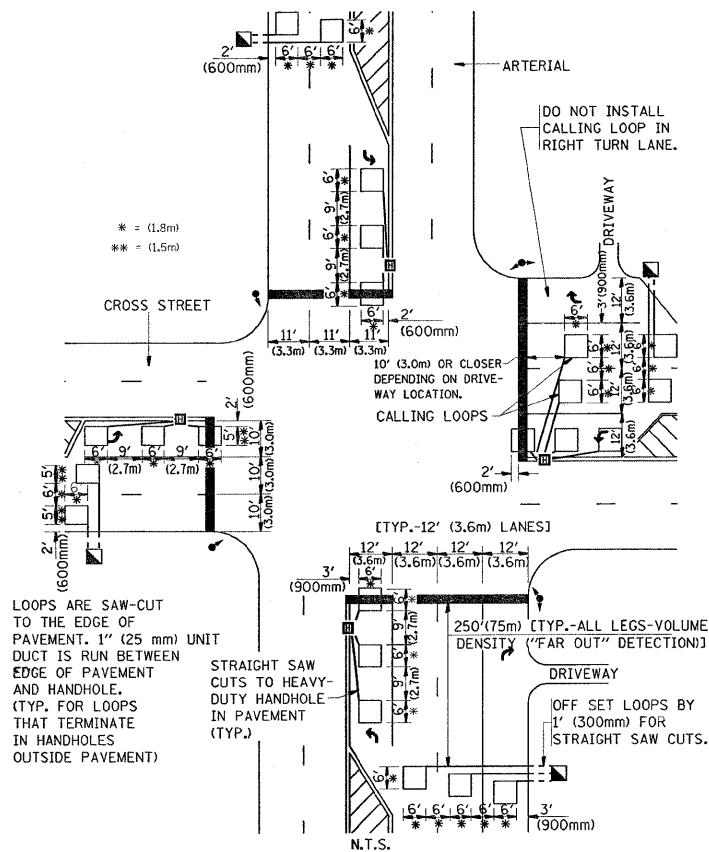
**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

* = (600 mm)



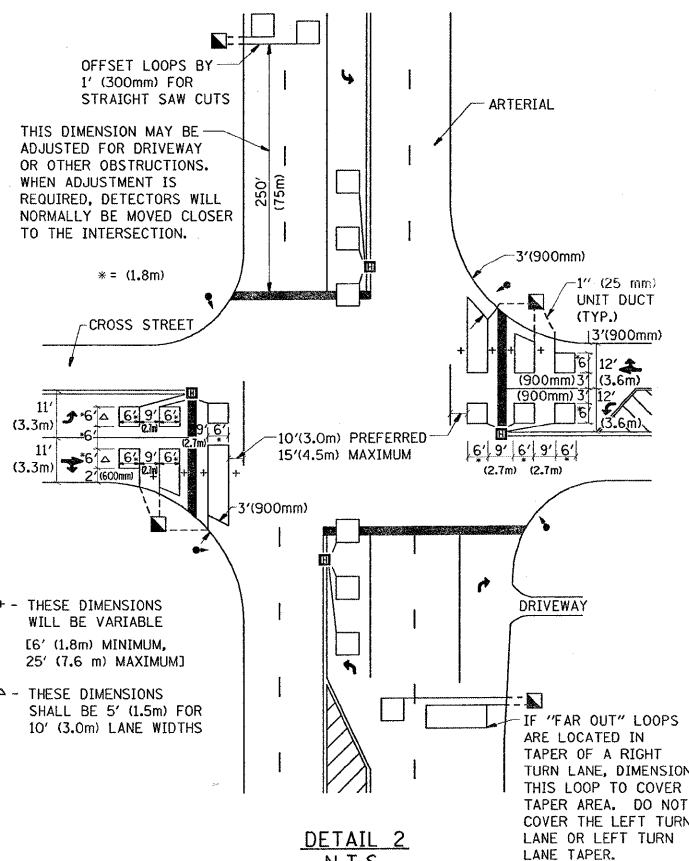
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

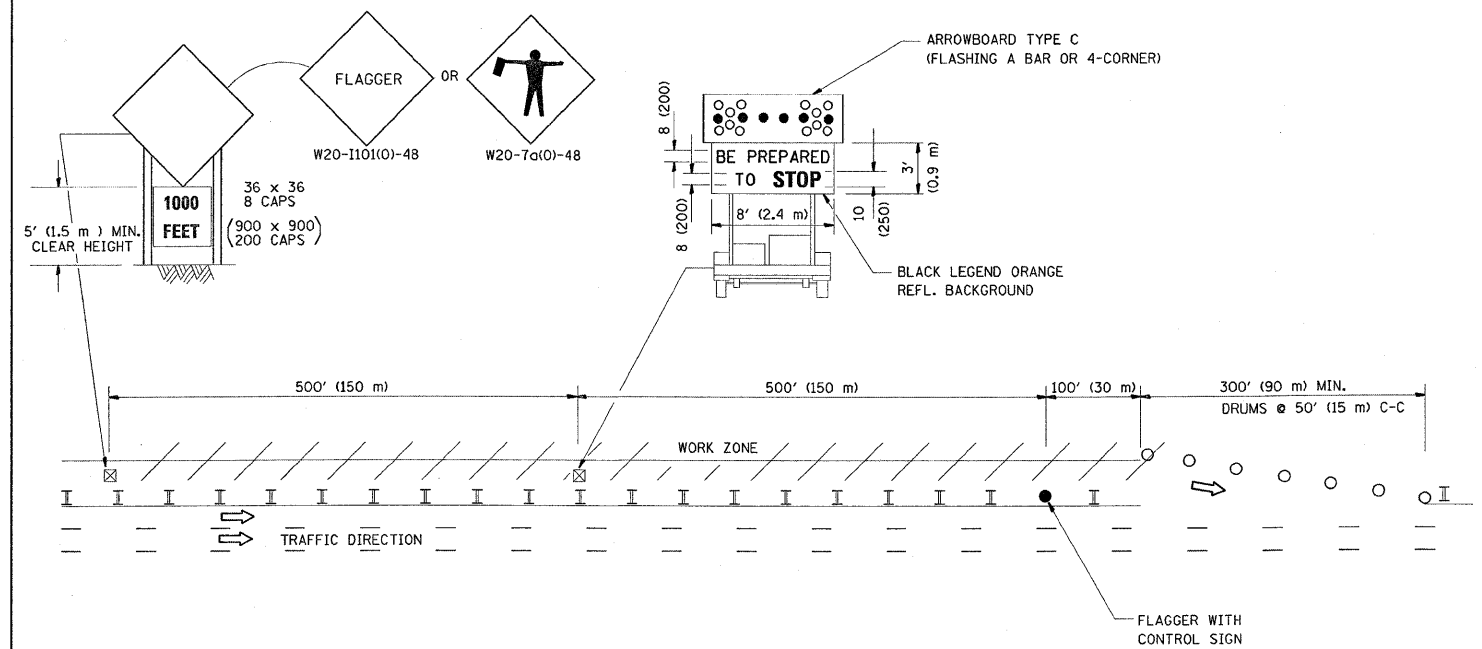
ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

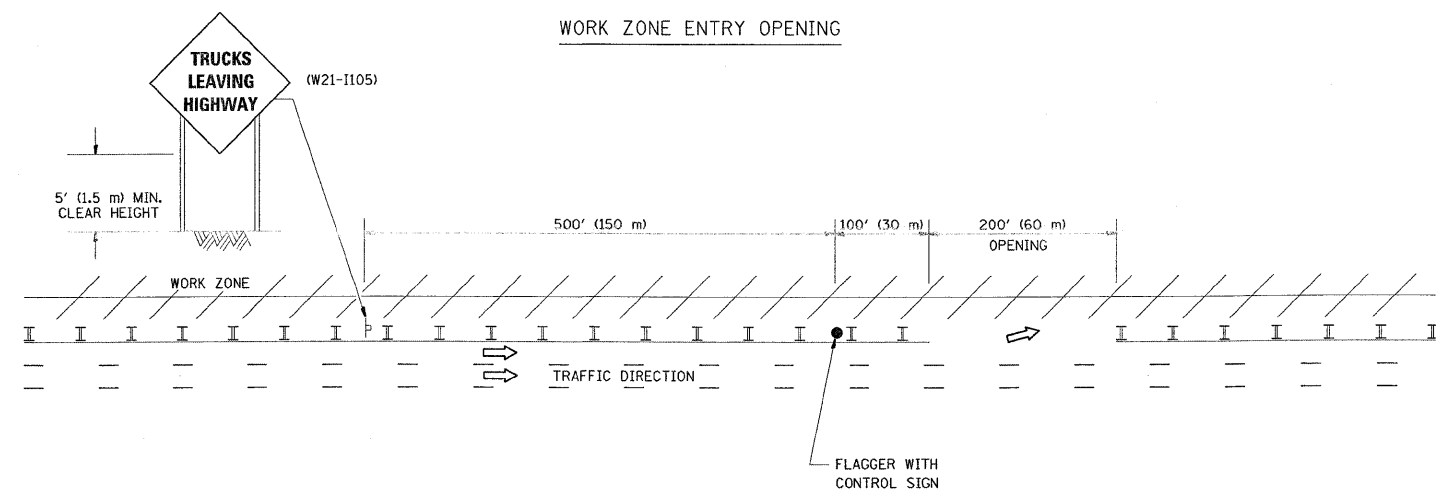
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21\pwwork\pwwork\abraush\d0146101\01st5.dgn		DRAWN -	REVISED -		305	2009-076 RS	COOK	34	32			
PLOT SCALE = 50.0000' / IN.		CHECKED - R.K.F.	REVISED -		TS-07			CONTRACT NO. 60110				
PLOT DATE = 8/5/2009		DATE -	REVISED -		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:

1. The Arrowboard, the Flagger Ahead trailer mounted sign, and the Trucks Leaving Highway sign shall be removed or turned away from traffic and the exit and entry openings shall be closed when the flagging operation ceases.
2. Work Zone Exit Openings should be a minimum of one half mile apart.
3. Exiting the work zone at any place other than at a Work Zone Exit Opening will be prohibited.
4. All vehicles shall enter the work zone at entry openings, using their turn signals to warn motorists

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

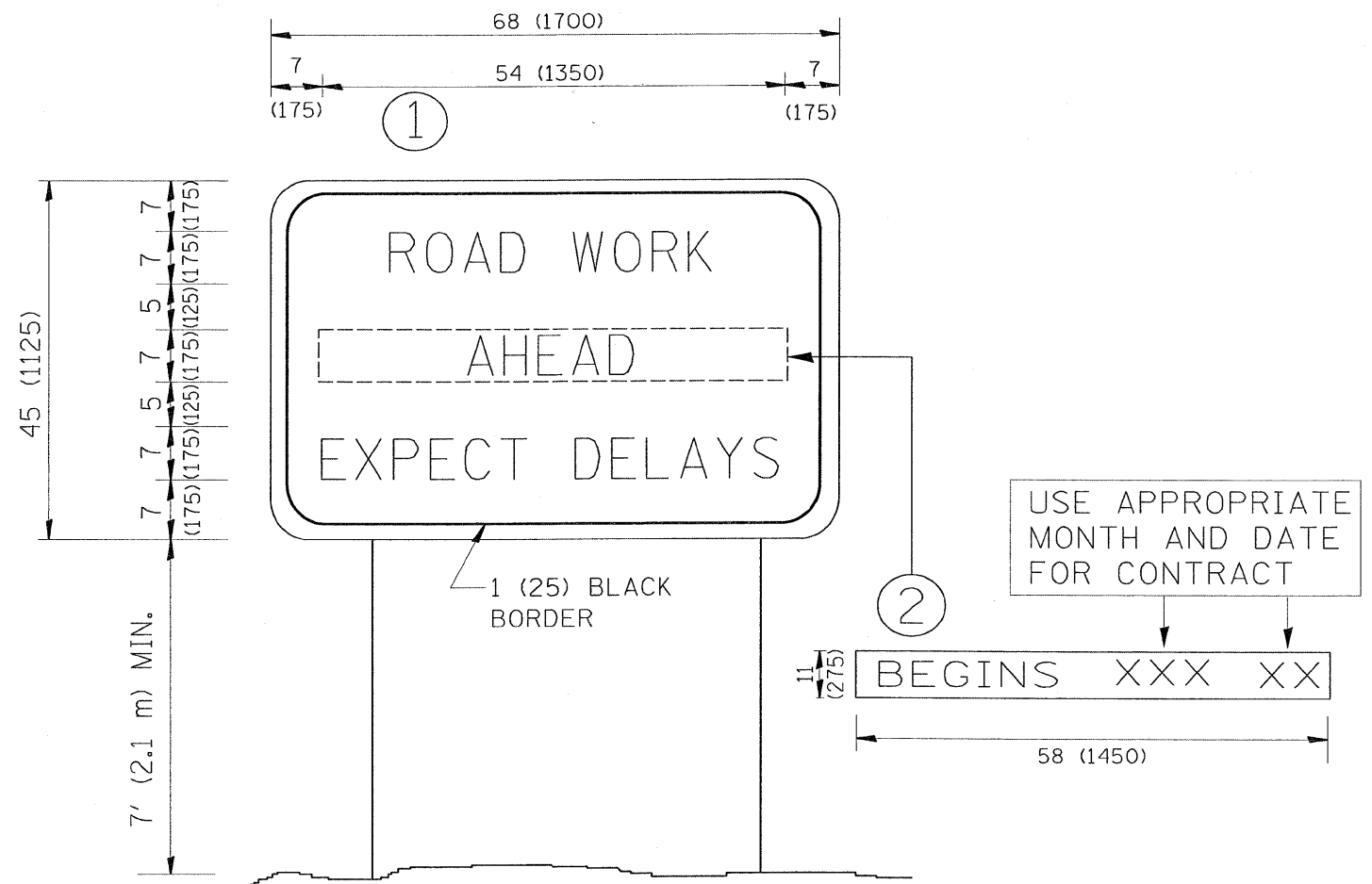
FILE NAME =	USER NAME = abreaush	DESIGNED -	REVISED - D.W.S. 08-98
ct:\pw\work\pwidot\abreaush\08146101\01st5.dgn		DRAWN -	REVISED - J.A.F. 04-03
	PLOT SCALE = 50.0000 "/td> <td>CHECKED -</td> <td>REVISED - J.A.F. 02-06</td>	CHECKED -	REVISED - J.A.F. 02-06
	PLOT DATE = 8/5/2009	DATE -	REVISED - S.P.B. 01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIGNING FOR FLAGGING OPERATIONS
AT WORK ZONE OPENINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2009-076 RS	COOK	34	33
TC-18		CONTRACT NO. 60110		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = abrouh	DESIGNED -	REVISED - R. MIRS 09-15-97
c:\pwwork\pwwork\abrouh\d0146101\dsts.dgn		DRAWN -	REVISED - R. MIRS 12-11-97
		CHECKED -	REVISED - T. RAMMACHER 02-02-99
		DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2009-076 RS	COOK	34	34
TC-22			CONTRACT NO. 60110	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				