3-5-10 Letting, Item 089

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2530	09-00075-00-RS	DUPAGE	15	1
FEDERAL ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63350		

INDEX OF SHEETS

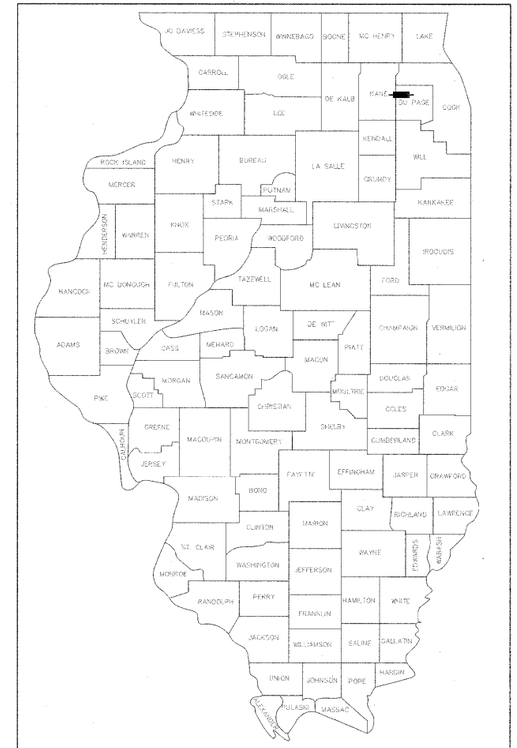
IDOT STATEWIDE STANDARDS

000001-05	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
001006	DECIMAL OF AN INCH AND OF A FOOT
442201-03	CLASS C AND D PATCHES
630001-08	STEEL PLATE BEAM GUARDRAIL
630301-05	SHOULDER WIDENING FOR TYPE 1 (SPECIAL GUARDRAIL TERMINALS
635001-01	DELINEATORS
635006-03	REFLECTOR AND TERMINAL MARKER PLACEMENT
635011-02	REFLECTOR MARKER AND MOUNTING DETAILS
701006-03	OFF-ROAD OPERATIONS, 2L, 2W, 15' (4.5M) TO 24' (600M) FROM PAVEMENT EDGE
701011-02	OFF-ROAD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701301-03	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS-DAY ONLY
701501-05	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701701-06	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS
781001-03	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUTS FOR DETECTION LOOPS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED FEDERAL AID HIGHWAY

F.A.U. ROUTE 2530 (POWIS ROAD)
IL ROUTE 64 (NORTH AVENUE) TO SMITH ROAD
LAPP RESURFACING
SECTION NO. 09-00075-00-RS
PROJECT NO. ARA-9003(403)
JOB NO. C-91-804-09
CITY OF WEST CHICAGO
DUPAGE COUNTY

THE PROJECT IS LOCATED IN THE CITY OF WEST CHICAGO

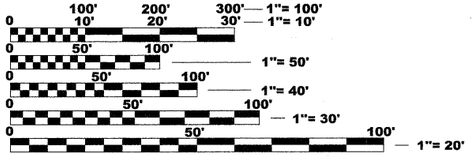


LOCATION OF SECTION INDICATED THUS: -

DESIGN DESIGNATION:
LOCAL ROAD

ADT:
4,100 VPD (2002)
7,000 VPD (2030)

POSTED SPEED LIMIT(S):
40 MPH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

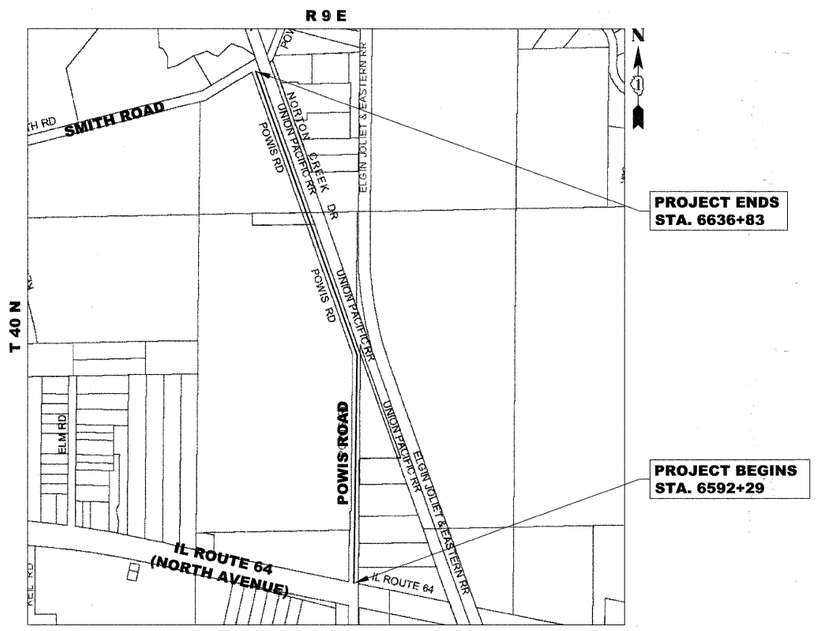
PLANS PREPARED BY:

CITY OF WEST CHICAGO
475 MAIN STREET
PHONE: 630-293-2200
FAX: 630-293-3028
P.O. BOX 488
WEST CHICAGO, IL 60185

thomas engineering group, llc
238 south kenilworth avenue
suite 100
oak park, il 60302
phone: 708-533-1700

PROJECT ENGINEER: BRIAN L. PAWULA, P.E., (847) 922-6125
PROJECT MANAGER: KEVIN C. VANDEWOESTYNE, P.E., (847) 815-9500

CONTRACT NO. 63350



LOCATION MAP

N.T.S.

PROJECT LENGTH

GROSS = 4,454 FT = 0.84 MI
NET = 4,454 FT = 0.84 MI

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED 12/14 **20** 09
CITY OF WEST CHICAGO, DIRECTOR OF PUBLIC WORKS

PASSED December 16 **2009**
DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID
BASED ON LIMITED REVIEW December 16 **20** 09
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

BRIAN L. PAWULA 062-061268 LICENSED PROFESSIONAL ENGINEER OF ILLINOIS

BY: **DATE:** 12/16/09
THOMAS ENGINEERING GROUP, LLC
LICENSE EXPIRES: 11/30/11

RICHARD A. YOUNG 062-054695 LICENSED PROFESSIONAL ENGINEER OF ILLINOIS

BY: **DATE:** 12/14/09
EJM ENGINEERING, INC.
LICENSE EXPIRES: 11/30/11
THIS SEAL APPLIES TO SHEET(S): 4

FEDERAL AID DESIGN ENGINEER: MARILIN SOLOMON, P.E. (847) 705-4407

GENERAL NOTES:

1. ALL IMPROVEMENTS SHALL BE IN ACCORDANCE WITH IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JANUARY 1, 2007 OR LATER.

2. BEFORE STARTING EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)

3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ALL UTILITIES THAT ARE DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE OWNER. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE.

4. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER AND THE CITY OF WEST CHICAGO IF UNMARKED UTILITIES ARE DISCOVERED AND IF MUNICIPAL UTILITIES ARE DAMAGED DURING CONSTRUCTION. THE CONTRACTOR WILL COOPERATE WITH THE CITY OF WEST CHICAGO IF MUNICIPAL UTILITY IMPROVEMENTS ARE REQUIRED WITHIN THE DURATION OF THE CONTRACT.

5. THE THICKNESSES OF HMA MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASIS ON WHICH THEY ARE TO BE PLACED. PLAN THICKNESSES SHOULD BE CONSIDERED THE MINIMUM THICKNESSES PERMITTED.

6. QUANTITIES FOR MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS AND STRIP REFLECTIVE CRACK CONTROL TREATMENT HAVE BEEN PROVIDED. AFTER THE HMA SURFACE REMOVAL OPERATIONS ARE COMPLETE ALL OPEN CRACKS AND OPEN EXPANSION JOINTS HAVING A WIDTH OF 1/2 IN. OR MORE SHALL BE CLEANED AND FILLED AND STRIP REFLECTIVE CRACK CONTROL WILL BE APPLIED ACCORDING TO ARTICLE 406 AND 443, RESPECTIVELY. THE ACTUAL NEED FOR THESE ITEMS SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER. IF CRACK SEALING IS NOT REQUIRED THE QUANTITIES WILL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.

7. QUANTITIES FOR PAVEMENT PATCHING (CLASS D PATCHES AND PAVEMENT PATCHING - PARTIAL DEPTH) HAVE BEEN PROVIDED IN THE CONTRACT BASED ON FIELD OBSERVATIONS OF EXISTING CONDITIONS. LOCATIONS SHOWN IN THE PLANS MAY DIFFER AND WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE ENGINEER. IF PAVEMENT PATCHING IS NOT REQUIRED THE QUANTITY WILL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.

8. PAVEMENT PATCHING SHALL BE SCHEDULED IMMEDIATELY FOLLOWING HMA SURFACE REMOVAL TO REDUCE DEGRADATION OF THE EXISTING BASE.

9. THE MAXIMUM ALLOWABLE VERTICAL PAVEMENT LANE DROP DIFFERENTIAL WILL BE 1-1/2".

10. THE CONTRACTOR SHALL PLACE SHORT-TERM PAVEMENT MARKINGS IMMEDIATELY FOLLOWING PLACEMENT OF THE FINAL HMA SURFACE COURSE. PAVEMENT MARKING GROOVING CANNOT OCCUR UNTIL SEVEN (7) DAYS AFTER PLACEMENT OF FINAL HMA SURFACE COURSE (SEE SPECIAL PROVISIONS).

11. THE CONTRACTOR SHALL MAINTAIN PROPER DRAINAGE AT ALL TIMES DURING THE COURSE OF CONSTRUCTION, AND SHALL PREVENT STORMWATER FROM RUNNING INTO OR STANDING IN EXCAVATED AREAS.

12. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE BUTT JOINT AND HMA TAPER DETAILS SHEET INCLUDED IN THE PLANS (BD-32).

13. ALL PAVEMENTS, CURB AND GUTTER, SIDEWALKS, AND DRIVEWAYS TO BE REMOVED SHALL BE SAW CUT PRIOR TO REMOVAL TO PREVENT DAMAGE TO THE ITEMS TO REMAIN. THE COST OF SAW CUTTING SHALL BE INCLUDED IN THE COST OF THE ITEMS BEING CONSTRUCTED.

14. PARKWAY RESTORATION SHALL INCLUDE REPLACEMENT OF DAMAGED AND DESTROYED LANDSCAPE, IN KIND, OF THE EXISTING TURF ADJACENT TO THE WORKING LIMITS AND WHERE THE CONTRACTOR'S EQUIPMENT HAS DESTROYED OR DAMAGED THE TURF. THE CONTRACTOR SHALL PREPARE THE GROUND ACCORDING TO SECTION 211 OF THE STANDARD SPECIFICATIONS. SALT TOLERANT SEED AND FERTILIZER SHALL BE PLACED ACCORDING TO SECTION 250 OF THE STANDARD SPECIFICATIONS. THIS WORK SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR ITEMS BEING CONSTRUCTED SUCH AS AGGREGATE WEDGE SHOULDERS, TYPE B; GRADING EXISTING SHOULDER; AND STEEL PLATE BEAM GUARDRAIL, TYPE A (SEE SPECIAL PROVISIONS).

15. ALL STREETS AND COMMERCIAL PARKING LOT ENTRANCES SHALL REMAIN OPEN DURING CONSTRUCTION. TRAFFIC SHALL BE MAINTAINED IN ACCORDANCE WITH IDOT STANDARDS 701006, 701011, 701301, 701311, AND 701901 (SEE SPECIAL PROVISIONS).

16. WORK SHALL BE CAREFULLY PLANNED BY THE CONTRACTOR TO REDUCE DISRUPTION TO RESIDENTS, BUSINESSES, AND THE PUBLIC SEEKING TO ACCESS THE BUSINESSES. AT LEAST ONE LANE OF TRAFFIC MUST REMAIN OPEN AT ALL TIMES.

17. DURING AND AFTER CONSTRUCTION OPERATIONS, LOOSE MATERIAL ON ROADWAYS AS A RESULT OF CONTRACTOR OPERATIONS, INCLUDING BUT NOT LIMITED TO HMA SURFACE REMOVAL, BINDER COURSE AND SURFACE COURSE INSTALLATION, SHALL BE REMOVED AND DEPOSITED OFF SITE BY THE CLOSE OF EACH BUSINESS DAY. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE CONTRACT. THIS APPLIES TO EXCESSIVE PRIMER LEFT ON ROADWAYS.

18. GUARDRAIL REMOVAL SHALL INCLUDE DISMANTLING, LOADING, HAULING, DISPOSAL, AND RESTORATION OF THE SITE IN PREPARATION FOR INSTALLATION OF NEW GUARDRAIL.

19. THE COST OF REMOVING AND REPLACING EARTH AND SHOULDER MATERIAL NECESSARY FOR INSTALLATION OF GUARDRAIL WILL NOT BE PAID FOR SEPERATELY. THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR STEEL PLATE BEAM GUARDRAIL, TYPE A. IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO CHECK ALL FIELD CONDITIONS WHICH RELATE TO THIS ITEM AND ALL OTHER ITEMS ASSOCIATED WITH THIS CONTRACT.

20. ONCE WORK HAS BEGUN ON A CONTINUOUS SECTION OF GUARDRAIL, ALL OF THE GUARDRAIL THAT IS REMOVED FROM THAT SECTION MUST BE REPLACED WITH NEW GUARDRAIL IN THE SAME DAY. NEW GUARDRAIL SHALL BE TEMPORARILY CONNECTED TO REMAINING OLD GUARDRAIL IN A MANNER APPROVED BY THE ENGINEER.

21. GUARDRAIL AND TERMINAL MARKERS SHALL BE INSTALLED THE SAME DAY THAT THE GUARDRAIL AND TERMINAL SECTIONS TO WHICH THEY ARE TO BE ATTACHED. GUARDRAIL MARKERS SHALL NOT BE ATTACHED TO THE TERMINAL SECTIONS.

22. THE CONTRACTOR SHALL PLACE "NO PASSING ZONES NOT STRIPED NEXT _ MILES" SIGNS AT THE BEGINNING OF UNSTRIPED AREAS, JUST BEYOND EACH MAJOR INTERSECTION WITHIN THE UPSTRIPED AREA, AND AT OTHER LOCATIONS AS DIRECTED BY THE ENGINEER. THE SIGNS SHALL BE PLACED JUST PRIOR TO REMOVAL OR COVERING OF THE STRIPING AND SHALL REMAIN IN PLACE UNTIL FULL NO PASSING ZONE STRIPING HAS BEEN RESTORED. THESE SIGNS SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR MOBILIZATION.

23. THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT 847-705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.

[ ] DENOTES ITEM(S) OR WORK NOT PAID FOR SEPARATELY

SUMMARY OF QUANTITIES

SPEC. ITEM	PAY CODE	DESCRIPTION	UNIT	TOTAL QUANTITY I000
	20201010	GRADING EXISTING SHOULDER	SQ YD	145
	40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	1253
	40600300	AGGREGATE (PRIME COAT)	TON	25
	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	6.3
	40600895	CONSTRUCTING TEST STRIP	EACH	2
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	80
	40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	1053
	44000153	HOT-MIX ASPHALT SURFACE REMOVAL, 1"	SQ YD	12530
	44201713	CLASS D PATCHES, TYPE I, 6 INCH	SQ YD	100
	44201717	CLASS D PATCHES, TYPE II, 6 INCH	SQ YD	100
	44201721	CLASS D PATCHES, TYPE III, 6 INCH	SQ YD	150
	44201723	CLASS D PATCHES, TYPE IV, 6 INCH	SQ YD	150
	44212900	PAVEMENT PATCHING (PARTIAL DEPTH)	SQ YD	50
	44300300	AREA REFLECTIVE CRACK CONTROL TREATMENT, SYSTEM A	SQ YD	12530
	44301200	STRIP REFLECTIVE CRACK CONTROL TREATMENT, SYSTEM B, 24 INCH	FOOT	500
	48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	180
*	63000003	STEEL PLATE BEAM GUARDRAIL, TYPE A, 9 FOOT POSTS	FOOT	825
*	63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	2
	63200310	GUARDRAIL REMOVAL	FOOT	325
	67100100	MOBILIZATION	L SUM	1
	70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1
	70300100	SHORT-TERM PAVEMENT MARKING	FOOT	3300
	70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	18977
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	98
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	18977
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	150
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	44
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	84
*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	147
*	78200410	GUARDRAIL MARKERS, TYPE A	EACH	9
*	78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	2
*	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	37
*	88600600	DETECTOR LOOP REPLACEMENT	FOOT	285
	40600826	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	526

* = SPECIALTY ITEM

FILE NAME = 02_Gen Notes, Sum of Qtys_Powis LAPP
PLOT DATE = 12/15/09

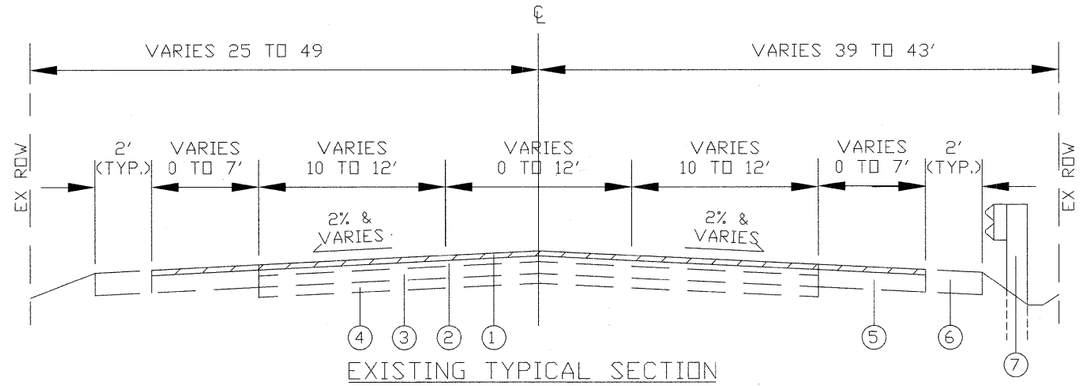
thomas engineering group, llc
238 south kenilworth avenue
suite 100
oak park, il 60302
phone: 708-533-1700

DESIGNED - DMM	REVISED -	
DRAWN - DMM	REVISED -	
CHECKED - KCV	REVISED -	
DATE - 12/16/09	REVISED -	

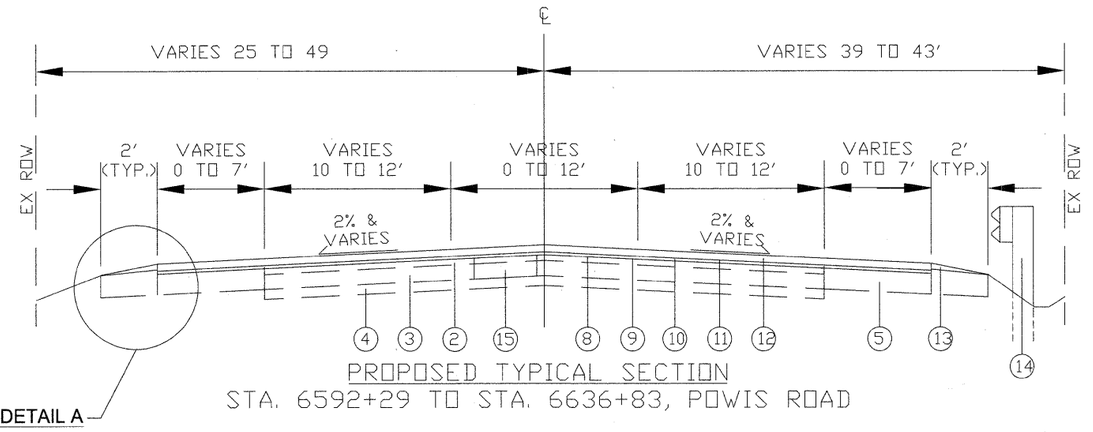
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

POWIS ROAD - IL RTE 64 (NORTH AVENUE) TO SMITH ROAD
GENERAL NOTES AND SUMMARY OF QUANTITIES
SCALE: NTS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2530	09-00075-00-RS	DUPAGE	15	2
		CONTRACT NO. 63350		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ARA-9003(403)		



EXISTING TYPICAL SECTION
STA. 6592+29 TO STA. 6636+83, POWIS ROAD



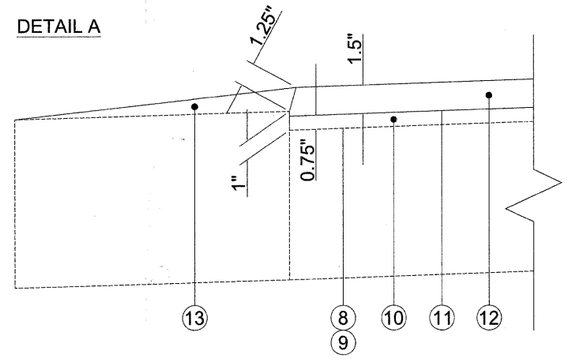
PROPOSED TYPICAL SECTION
STA. 6592+29 TO STA. 6636+83, POWIS ROAD

SEE DETAIL A

LEGEND:

1. PR HOT-MIX ASPHALT SURFACE REMOVAL, 1"
2. EX HOT-MIX ASPHALT SURFACE COURSE AND (LEVELING) BINDER COURSE, 3" AND VARIES (1" TO BE MILLED)
3. EX HOT-MIX ASPHALT BASE COURSE, VARIES 2" TO 4"
4. EX SUBBASE GRANULAR MATERIAL, TYPE B, VARIES 4" TO 18"
5. EX HOT-MIX ASPHALT SHOULDERS
6. EX AGGREGATE SHOULDERS
7. EX GUARDRAIL, REMOVAL (STA. 6609+84 TO STA. 6613+04)
8. PR AGGREGATE (PRIME COAT)
9. PR BITUMINOUS MATERIALS (PRIME COAT)
10. PR POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 0.75"
11. PR AREA REFLECTIVE CRACK CONTROL TREATMENT, SYSTEM A
12. PR HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1.5"
13. PR AGGREGATE WEDGE SHOULDERS, TYPE B (SEE DETAIL A THIS SHEET)
14. PR GUARDRAIL, INSTALLATION (STA. 6609+84 TO STA. 6618+03)
15. PR INTERMITTENT PAVEMENT PATCH

DETAIL A



CONTRACTOR SHALL MILL BEFORE PATCHING

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ Ndes
PAVEMENT RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm)	4% @ 50 Gyr.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	4% @ 50 Gyr.
PATCHING	
CLASS D PATCH (HMA BINDER IL-19 mm)	4% @ 70 Gyr.
PAVEMENT PATCHING (PARTIAL DEPTH) (HMA BINDER IL-19 mm)	4% @ 70 Gyr.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

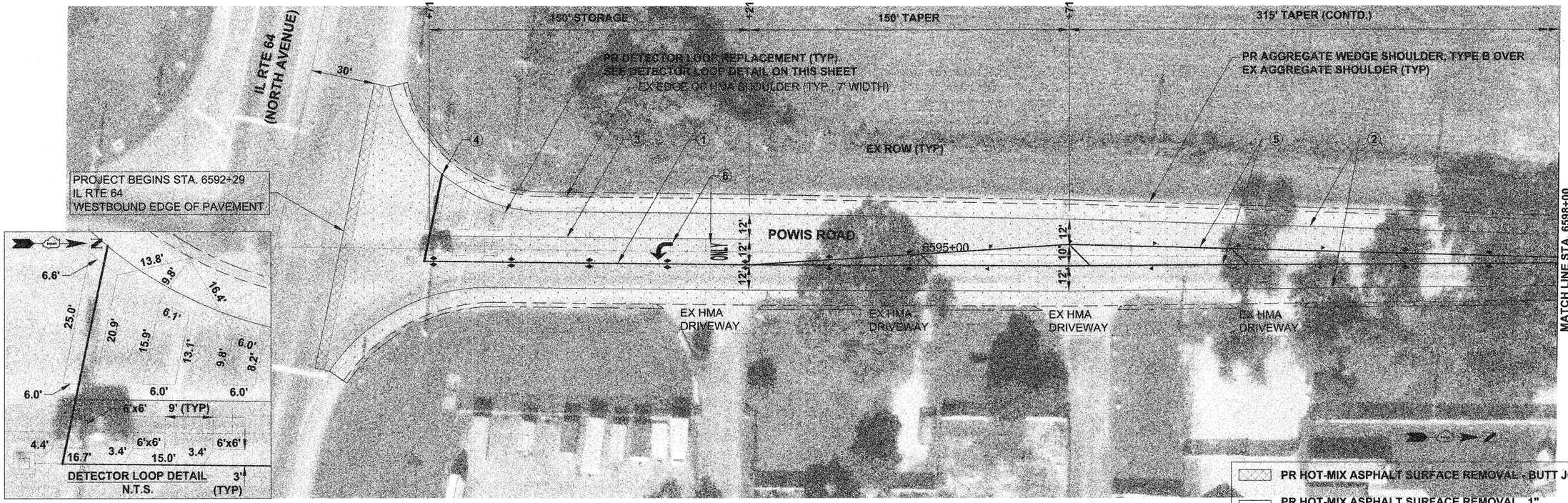
THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

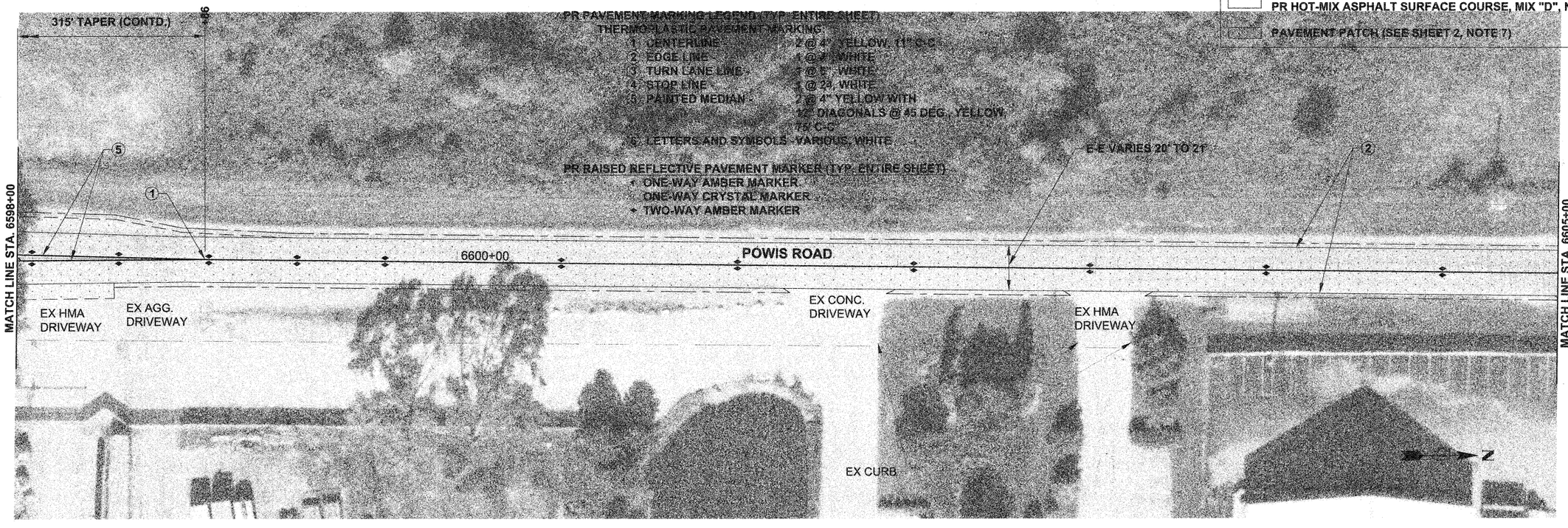
FILE NAME = 03_Typical Sections_Powis LAPP	thomas engineering group, llc 238 south kenilworth avenue suite 100 oak park, il 60302 phone: 708-533-1700	DESIGNED - DMM	REVISED -
PLOT DATE = 12/15/09		DRAWN - DMM	REVISED -
		CHECKED - KCV	REVISED -
		DATE - 12/16/09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

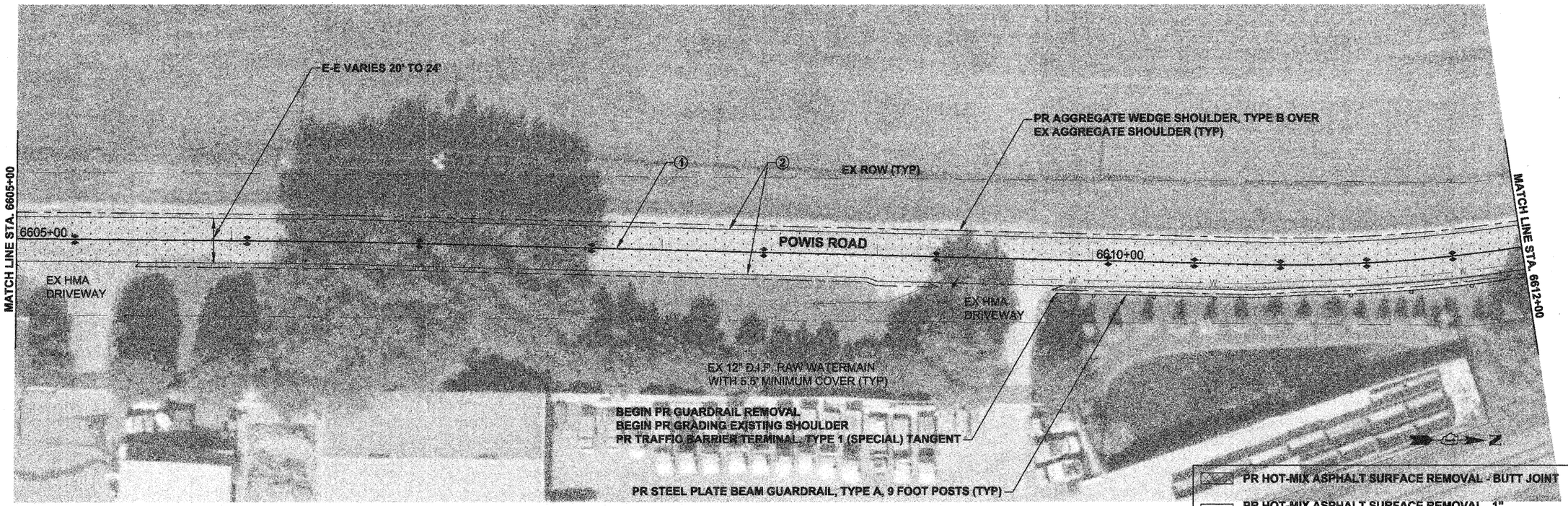
POWIS ROAD - IL RTE 64 (NORTH AVENUE) TO SMITH ROAD
TYPICAL SECTIONS
SCALE: NTS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2530	09-00075-00-RS	DUPAGE	15	3
		CONTRACT NO. 63350		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ARA-9003(403)		





FILE NAME = 04-07_Plan Sheets_Powis LAPP	thomas engineering group, llc 238 south kenilworth avenue suite 100 oak park, il 60302 phone: 708-533-1700	DESIGNED - BLP	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	POWIS ROAD - IL RTE 64 (NORTH AVENUE) TO SMITH ROAD	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT DATE = 12/15/09		DRAWN - BLP	REVISED -		PLAN SHEET	2530	09-00075-00-RS	DUPAGE	15	4
		CHECKED - DMM	REVISED -					CONTRACT NO. 63350		
		DATE - 12/16/09	REVISED -		SCALE: 1" = 50' STA. 6591+98 TO STA. 6605+00	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ARA-9003(403)		



PR HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

PR HOT-MIX ASPHALT SURFACE REMOVAL, 1"
PR POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, ¾"
PR HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1½"

PAVEMENT PATCH (SEE SHEET 2, NOTE 7)

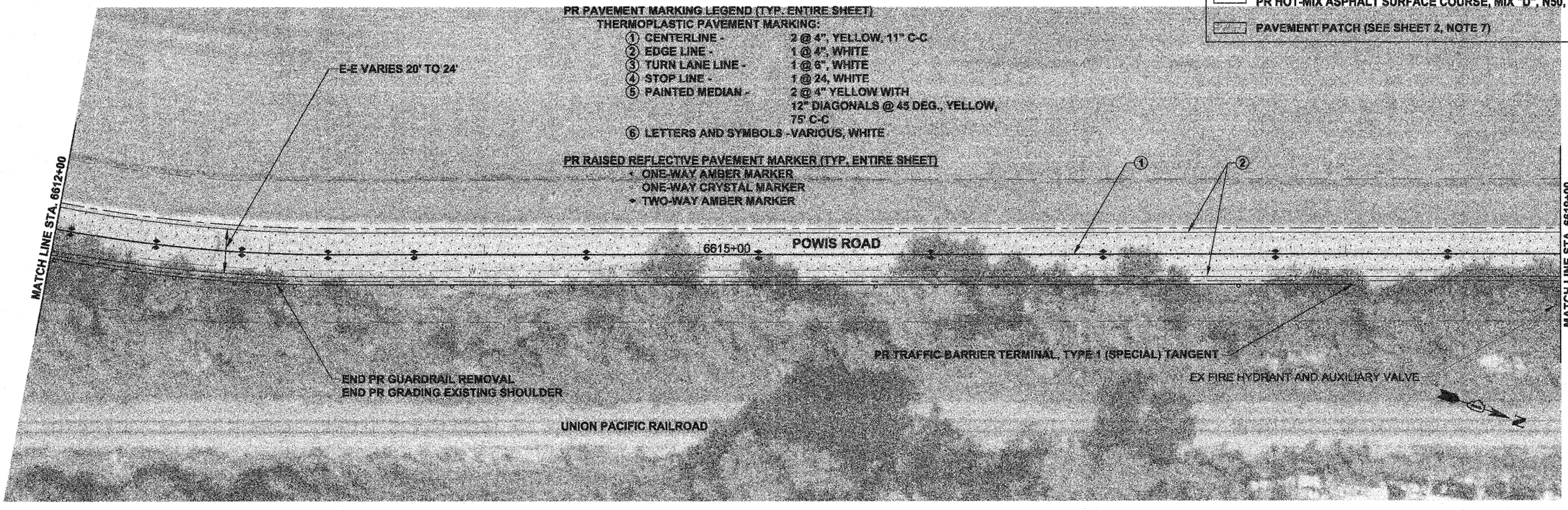
PR PAVEMENT MARKING LEGEND (TYP. ENTIRE SHEET)

THERMOPLASTIC PAVEMENT MARKING:

①	CENTERLINE -	2 @ 4", YELLOW, 11" C-C
②	EDGE LINE -	1 @ 4", WHITE
③	TURN LANE LINE -	1 @ 6", WHITE
④	STOP LINE -	1 @ 24, WHITE
⑤	PAINTED MEDIAN -	2 @ 4" YELLOW WITH 12" DIAGONALS @ 45 DEG., YELLOW, 75' C-C
⑥	LETTERS AND SYMBOLS -	VARIOUS, WHITE

PR RAISED REFLECTIVE PAVEMENT MARKER (TYP. ENTIRE SHEET)

- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER
- TWO-WAY AMBER MARKER



FILE NAME = 84-07_Plan Sheets_Powis LAPP
PLOT DATE = 12/15/09

thomas engineering group
engineering group
service at the highest grade
thomas engineering group, llc
238 south kenilworth avenue
suite 100
oak park, il 60302
phone: 708-533-1700

DESIGNED	BLP	REVISED	-
DRAWN	BLP	REVISED	-
CHECKED	DMM	REVISED	-
DATE	12/16/09	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

POWIS ROAD - IL RTE 64 (NORTH AVENUE) TO SMITH ROAD

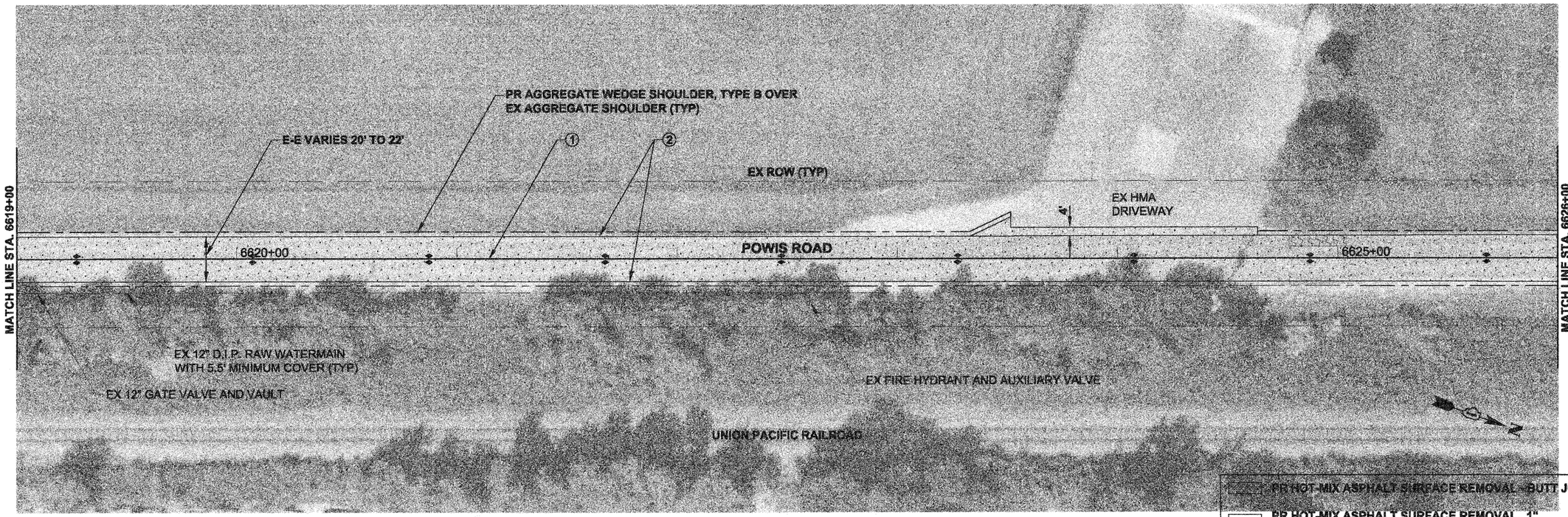
PLAN SHEET

SCALE: 1" = 80' | STA. 6605+00 TO STA. 6619+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2530	09-00075-00-RS	DUPAGE	15	5

CONTRACT NO. 63350

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT ARA-9003(403)



PR HOT-MIX ASPHALT SURFACE REMOVAL, BUTT JOINT

PR HOT-MIX ASPHALT SURFACE REMOVAL, 1"
PR POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, ¾"
PR HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1½"

PAVEMENT PATCH (SEE SHEET 2, NOTE 7)

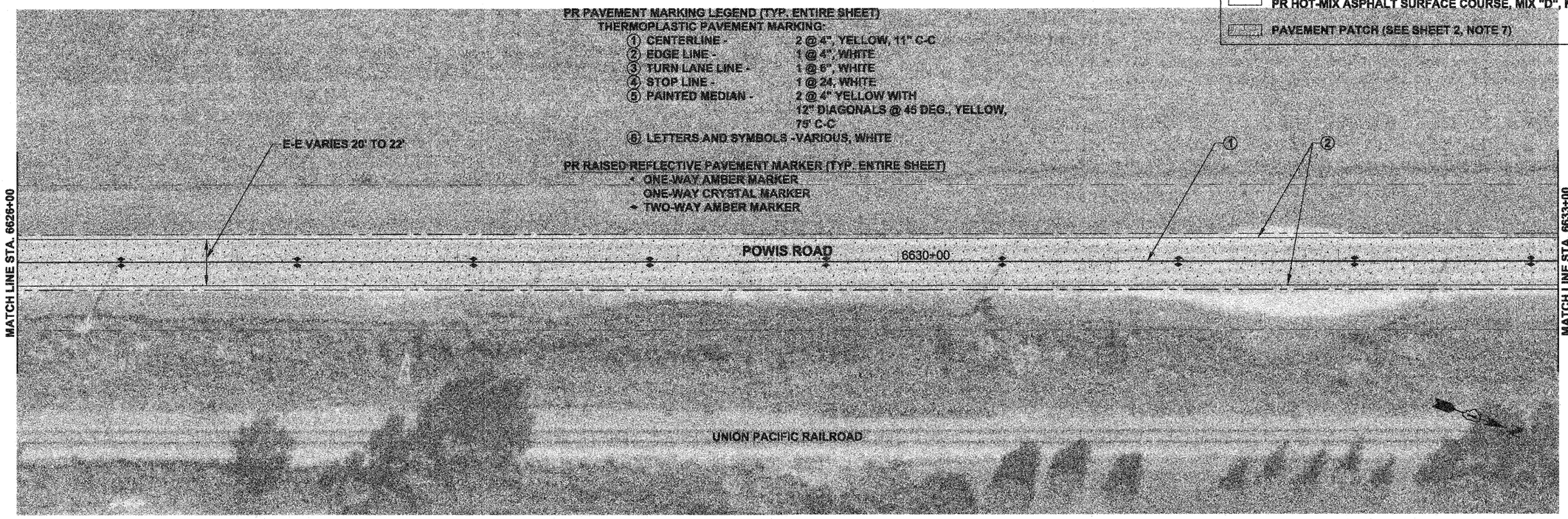
PR PAVEMENT MARKING LEGEND (TYP. ENTIRE SHEET)

THERMOPLASTIC PAVEMENT MARKING:

①	CENTERLINE -	2 @ 4", YELLOW, 11" C-C
②	EDGE LINE -	1 @ 4", WHITE
③	TURN LANE LINE -	1 @ 6", WHITE
④	STOP LINE -	1 @ 24, WHITE
⑤	PAINTED MEDIAN -	2 @ 4" YELLOW WITH 12" DIAGONALS @ 45 DEG., YELLOW, 75' C-C
⑥	LETTERS AND SYMBOLS -	VARIOUS, WHITE

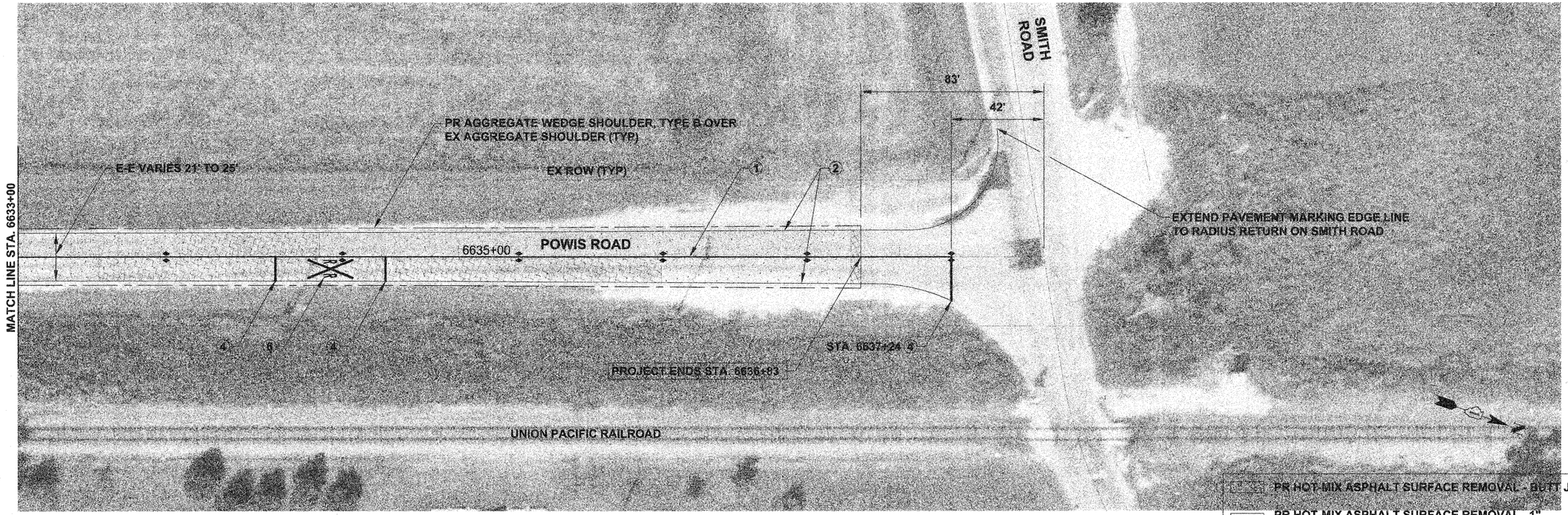
PR RAISED REFLECTIVE PAVEMENT MARKER (TYP. ENTIRE SHEET)

- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER
- TWO-WAY AMBER MARKER



FILE NAME = 04-07_Plan Sheets_Powis LAPP	thomas engineering group, llc 238 south kenilworth avenue suite 100 oak park, il 60302 phone: 708-533-1700	DESIGNED - BLP	REVISED -	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**	**POWIS ROAD - IL RTE 64 (NORTH AVENUE) TO SMITH ROAD**
PLOT DATE = 12/16/09		DRAWN - BLP	REVISED -		**PLAN SHEET**
		CHECKED - DMM	REVISED -		SCALE: 1" = 50' STA. 6619+00 TO STA. 6633+00
		DATE - 12/16/09	REVISED -		

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2530	09-00076-00-RS	DUPAGE	15	6
		CONTRACT NO. 63350		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ARA-9603(403)		



PR HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

PR HOT-MIX ASPHALT SURFACE REMOVAL, 1"
PR POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, ¾"
PR HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1½"

PAVEMENT PATCH (SEE SHEET 2, NOTE 7)

PR PAVEMENT MARKING LEGEND (TYP. ENTIRE SHEET)

THERMOPLASTIC PAVEMENT MARKING:

①	CENTERLINE -	2 @ 4", YELLOW, 11" C-C
②	EDGE LINE -	1 @ 4", WHITE
③	TURN LANE LINE -	1 @ 6", WHITE
④	STOP LINE -	1 @ 24, WHITE
⑤	PAINTED MEDIAN -	2 @ 4" YELLOW WITH 12" DIAGONALS @ 45 DEG., YELLOW, 75' C-C
⑥	LETTERS AND SYMBOLS -	VARIOUS, WHITE

PR RAISED REFLECTIVE PAVEMENT MARKER (TYP. ENTIRE SHEET)

- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER
- TWO-WAY AMBER MARKER

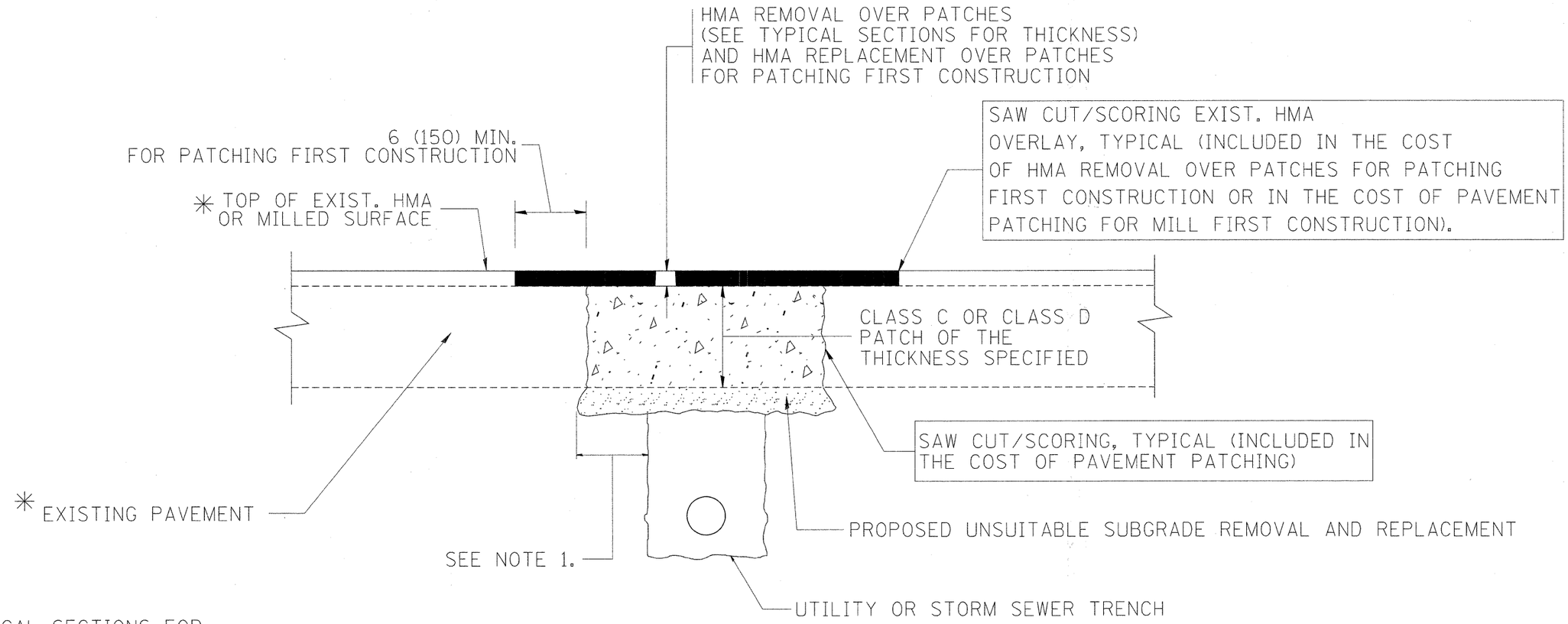
FILE NAME = 04-07_Plan Sheets_Powis LAPP	thomas engineering group, llc 238 south kenilworth avenue suite 100 oak park, il 60302 phone: 708-533-1700	DESIGNED - BLP	REVISED -
PLOT DATE = 12/15/09		DRAWN - BLP	REVISED -
		CHECKED - DMM	REVISED -
		DATE - 12/16/09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

POWIS ROAD - IL RTE 64 (NORTH AVENUE) TO SMITH ROAD
PLAN SHEET

SCALE: 1" = 50' | STA. 6633+00 TO STA. 6637+66

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2530	09-00075-00-RS	DUPAGE	15	7
		CONTRACT NO. 63350		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ARA-9003(403)		



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

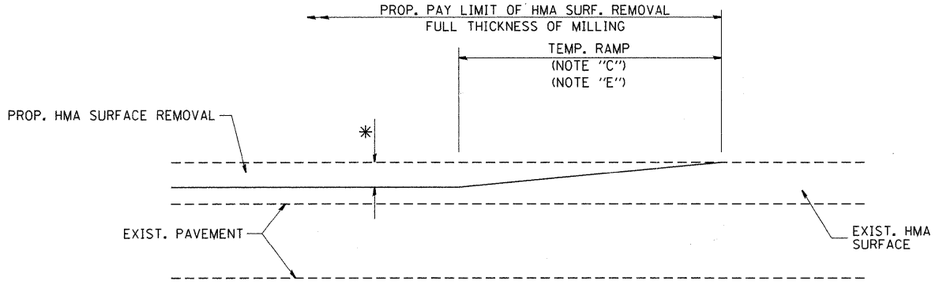
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST $4\frac{1}{2}$ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

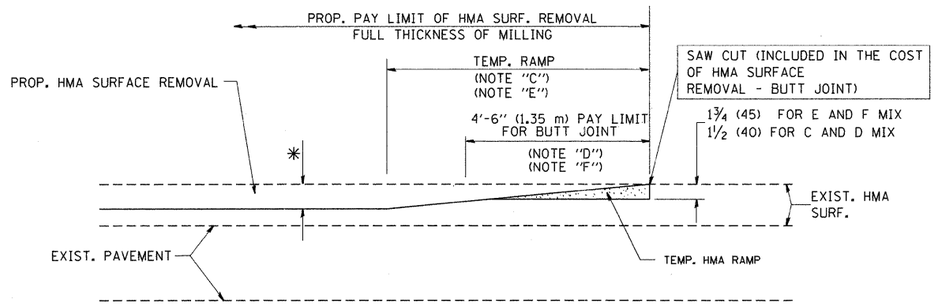
FILE NAME = c:\projects\diststd22x34\bd22.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**	**PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT**				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R. BORO 01-01-07						2530	09-00075-00-RS	DUPAGE	15	8
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - R. BORO 09-04-07							**BD400-04 (BD-22)**	CONTRACT NO. 63350		
	PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	ARA-9003(403)	



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

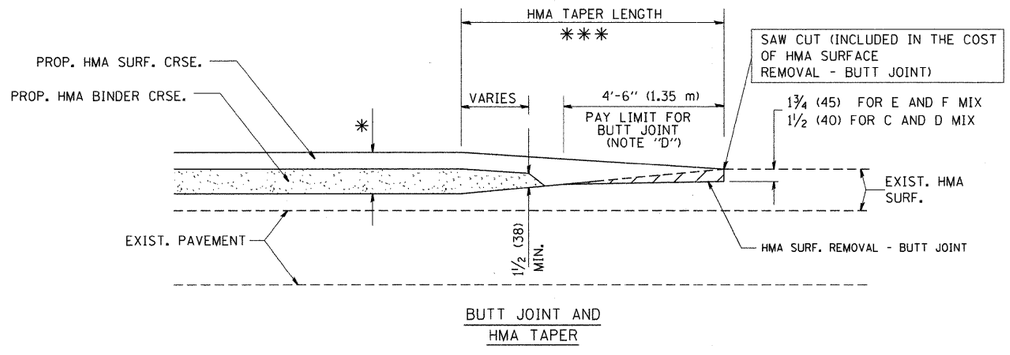


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

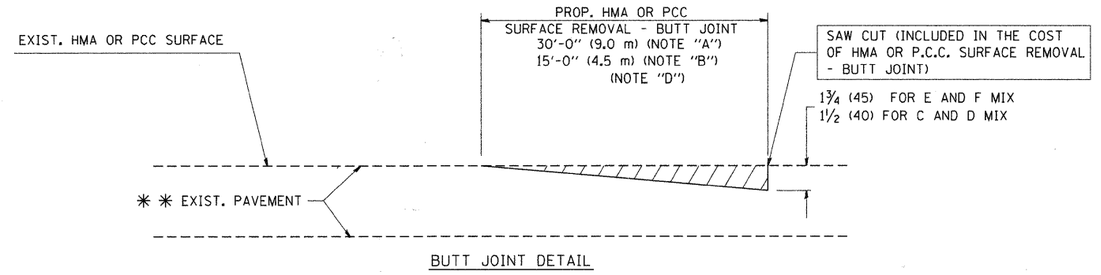
OPTION 2

TYPICAL TEMPORARY RAMP

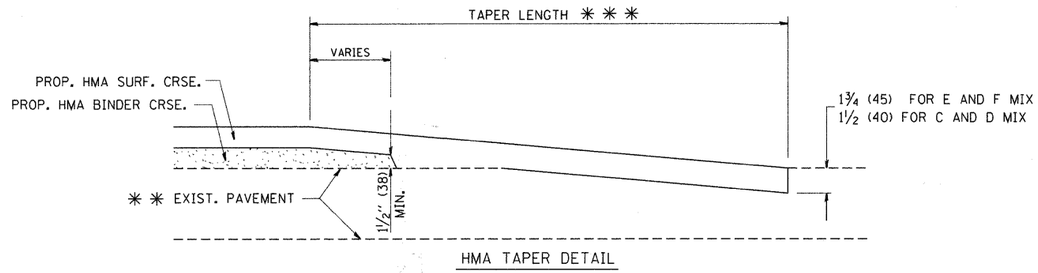


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.

B: MINOR SIDE ROADS.

C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.

F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

* * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

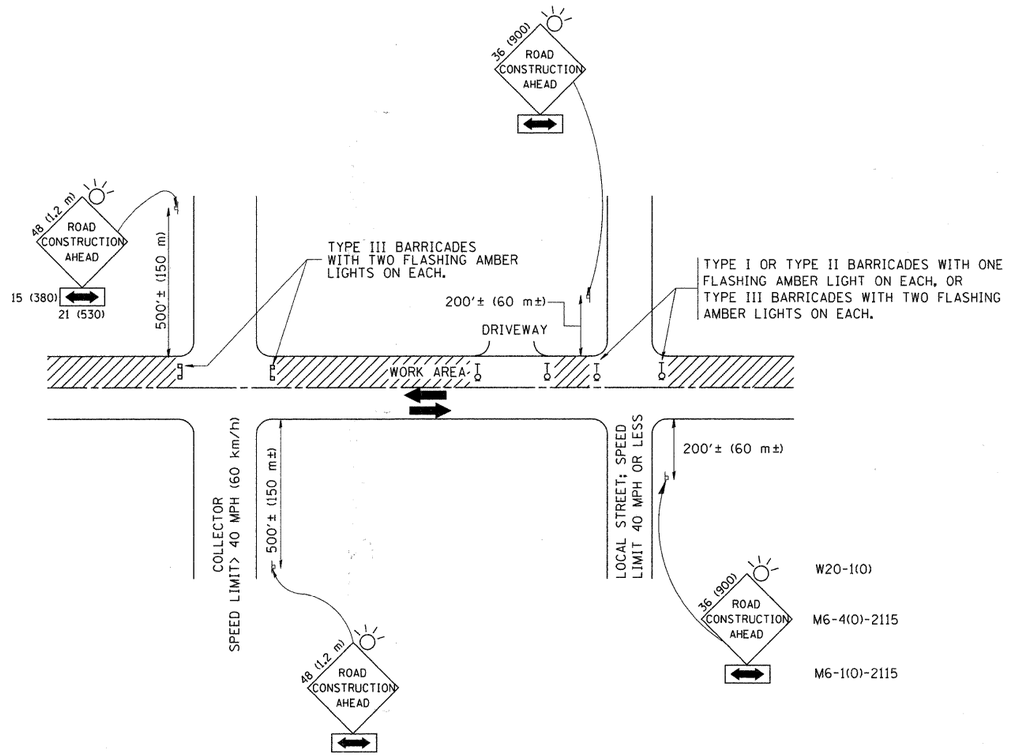
FILE NAME = W:\diststd\22x34\bd32.dgn	USER NAME = gaglianobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2530	09-00075-00-RS	DUPAGE	15	9
	BD400-05 BD32	CONTRACT NO. 63350		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	ARA-9003(403)	



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

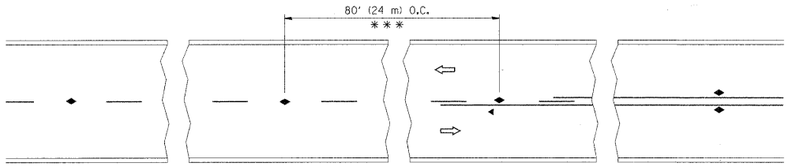
FILE NAME = W:\diststd\22x34\tc10.dgn	USER NAME = gaglianobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED -T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

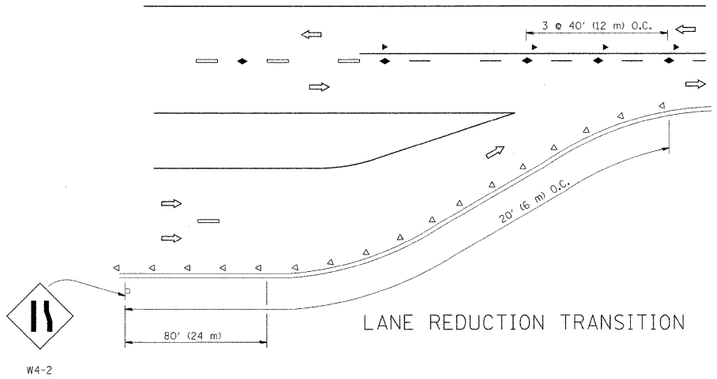
SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2530	09-00075-00-RS	DUPAGE	15	10
	TC-10	CONTRACT NO. 63350		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	ARA-9003(403)	

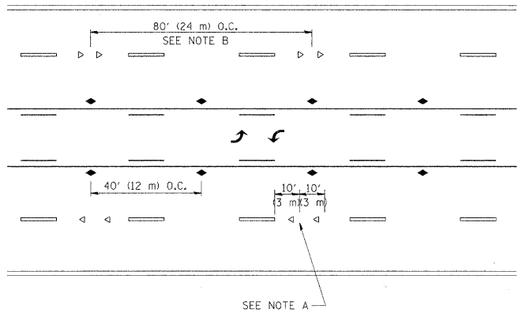


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

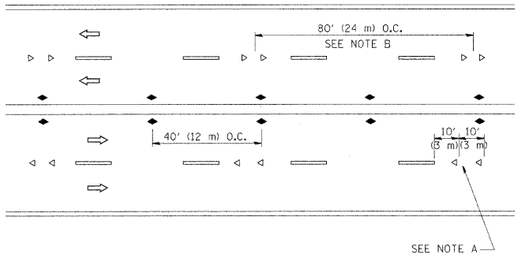
TWO-LANE/TWO-WAY



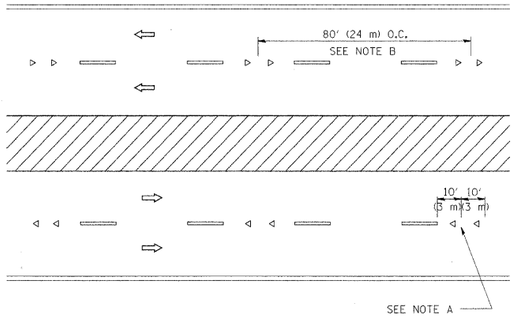
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

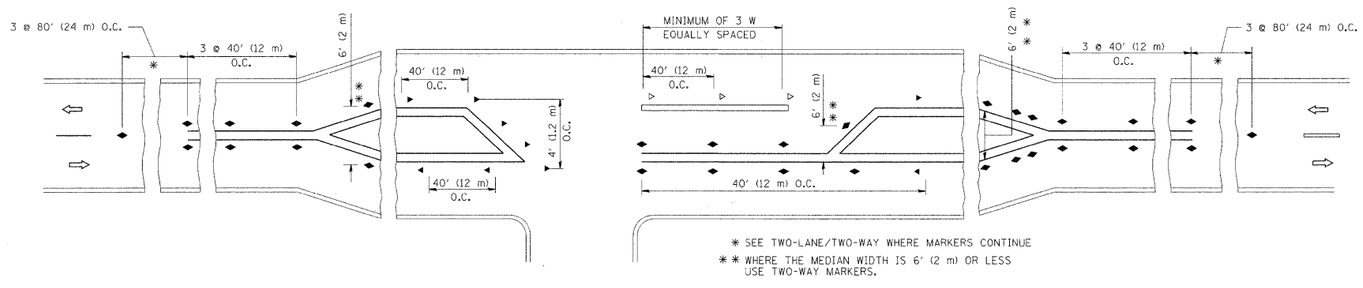
SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

LANE MARKER NOTES

A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE

** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

All dimensions are in inches (millimeters) unless otherwise shown.

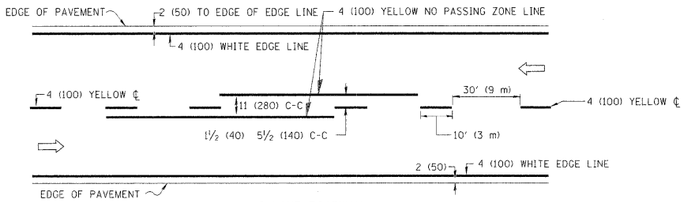
FILE NAME = c:\pw_work\pwidot\drivakosgn\d0108315\tc11.dgn	USER NAME = drivakosgn	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
		DRAWN -	REVISED - T. RAMMACHER 03-12-99
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	PLOT DATE = 9/9/2009	DATE -	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

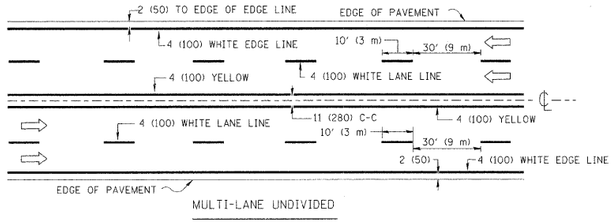
TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

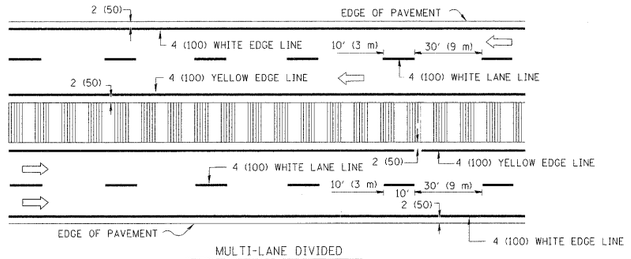
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2530	09-00075-00-RS	DUPAGE	15	11
	TC-11	CONTRACT NO. 63350		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	ARA-9003(403)	



2-LANE ROADWAY



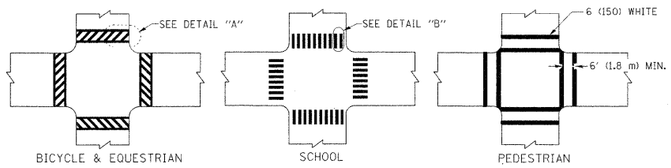
MULTI-LANE UNDIVIDED

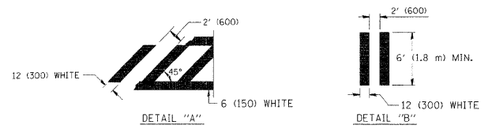


MULTI-LANE DIVIDED
WITH MOUNTABLE MEDIAN

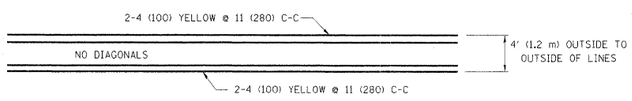
NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

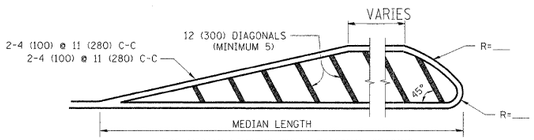




TYPICAL CROSSWALK MARKING



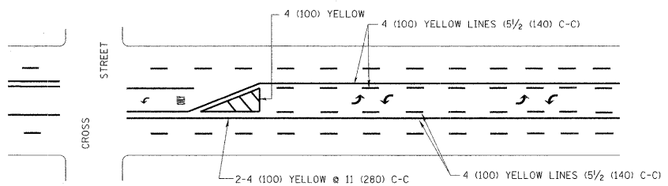
4' (1.2 m) WIDE MEDIANS ONLY



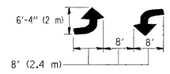
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

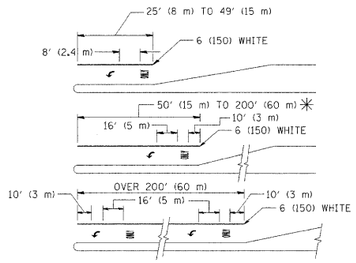


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED. WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

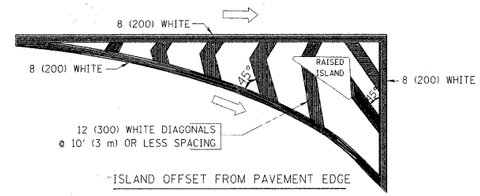


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
↰ AREA = 15.6 SQ. FT. (1.5 m^2) **ONLY** AREA = 20.8 SQ. FT. (1.9 m^2)

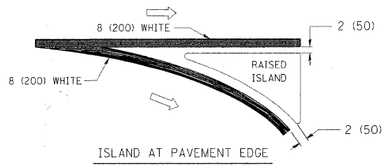
✳ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m^2) EACH "X"=54.0 SQ. FT. (5.0 m^2)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

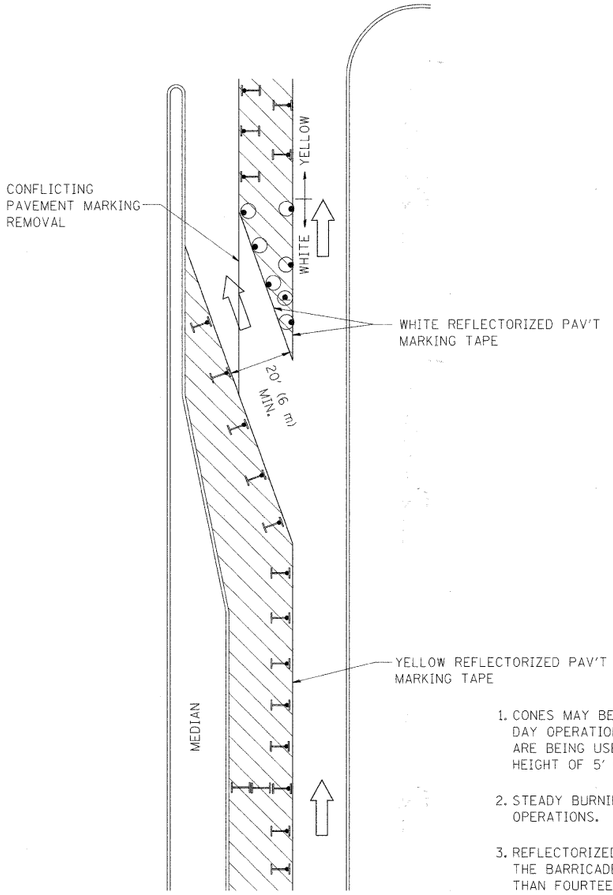
FILE NAME = c:\pw_work\pwidot\drivakosgn\d0108315\tc13.dgn	USER NAME = drivakosgn	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
		DRAWN -	REVISED - C. JUCIUS 09-09-09
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

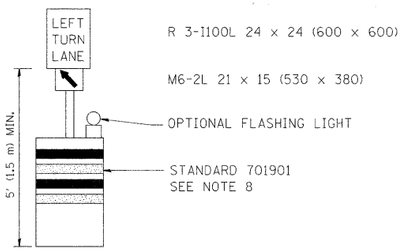
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2530	09-00075-00-RS	DUPAGE	15	12
	TC-13	CONTRACT NO. 63350		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	ARA-9003(403)	





GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHR 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

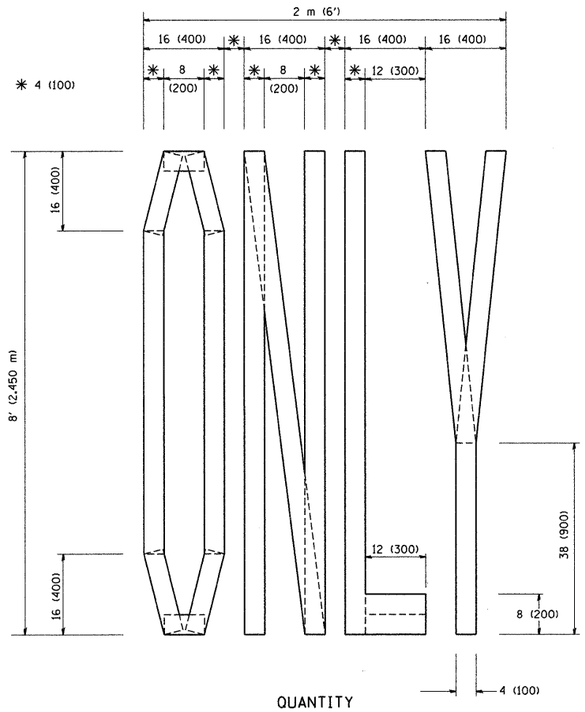
FILE NAME = c:\pw_work\PWIDOT\DRIVAKOSGN\d0106315\tc14.dgn	USER NAME = drivakosgn	REVISED -T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
	PLOT SCALE = 49.9999 ' / IN.	REVISED - A. HOUSEH 11-07-95	REVISED -
	PLOT DATE = 9/14/2009	REVISED -T. RAMMACHER 01-06-00	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

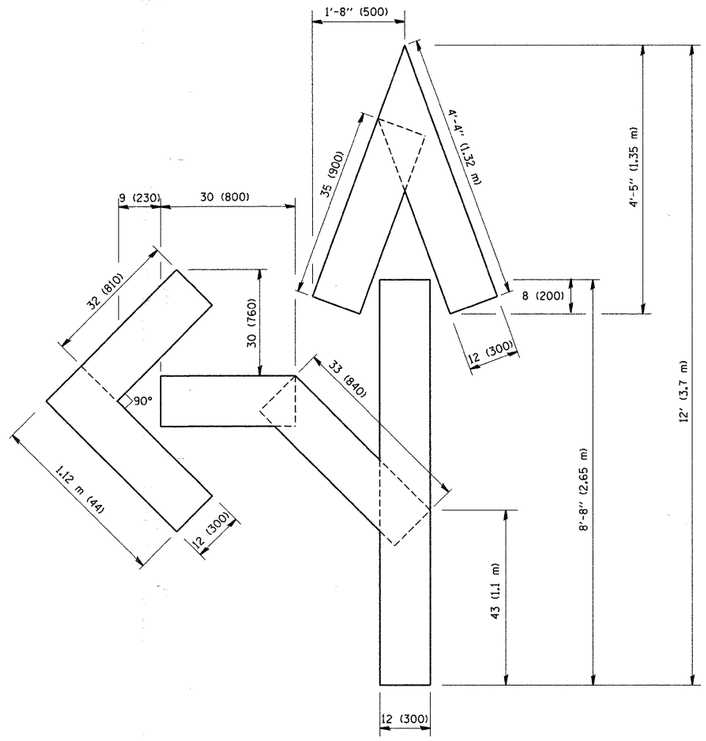
TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

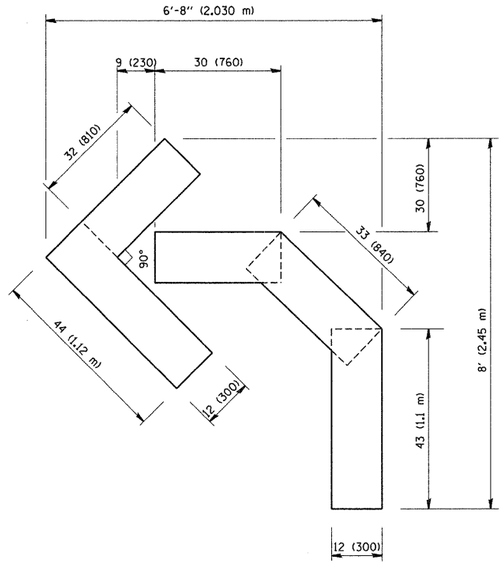
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2530	09-00075-00-RS	DUPAGE	15	13
	TC-14	CONTRACT NO. 63350		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	ARA-9003(403)	



QUANTITY
4 (100) LINE = 64.1 ft. (19.7 m)
21.1 sq. ft. (1.97 sq. m)



QUANTITY
4 (100) LINE = 82.5 ft. (25.3 m)
27.5 sq. ft. (2.53 sq. m)



QUANTITY
4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\diststd\22x34\tc16.dgn	USER NAME = gaglienobt	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
		DRAWN -	REVISED -T. RAMMACHER 11-04-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 1/4/2008	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

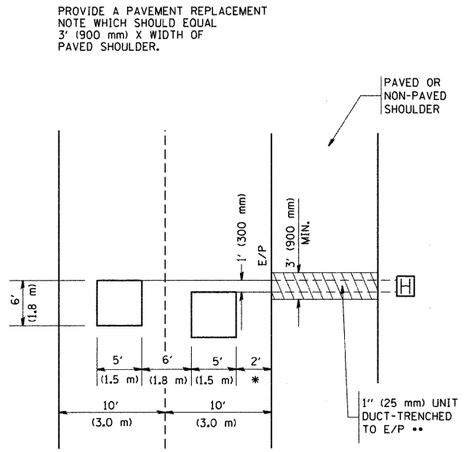
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2530	09-00075-00-RS	DUPAGE	15	14
	TC-16	CONTRACT NO. 63350		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	ARA-9003(403)	

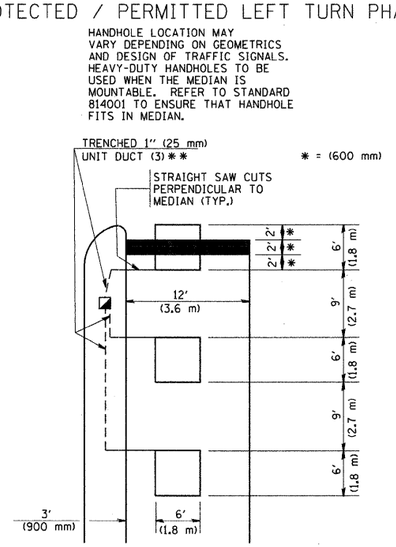
LOOPS NEXT TO SHOULDERS



* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

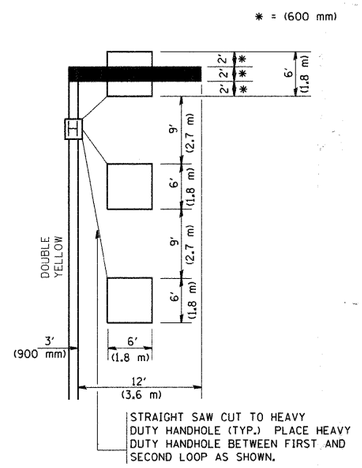
LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

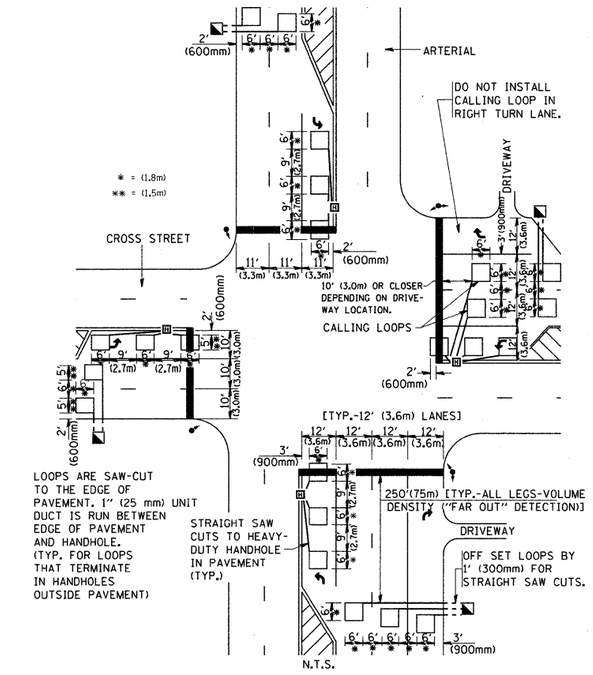
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



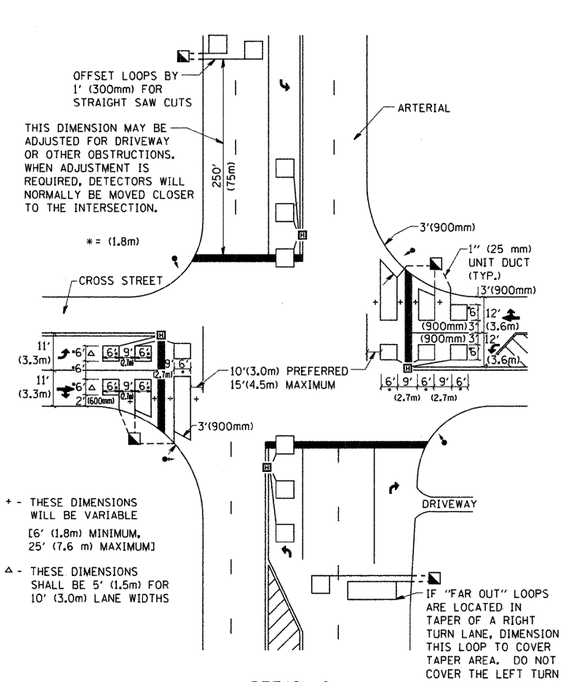
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\diststd\22x34\ts07.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 50.0000 ' / IN.	CHECKED - R.K.F.	REVISED -
	PLOT DATE = 1/4/2008	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 – DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2530	09-00075-00-RS	DUPAGE	15	15
	TS-07	CONTRACT NO. 63350		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	ARA-9003(403)	