

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS
**PROPOSED
 HIGHWAY PLANS**
 FAP ROUTE 310 (IL ROUTE 255)
 SECTIONS 60-15, 60-15B
 PROJECT ACNHF-0310(139)
 MADISON COUNTY

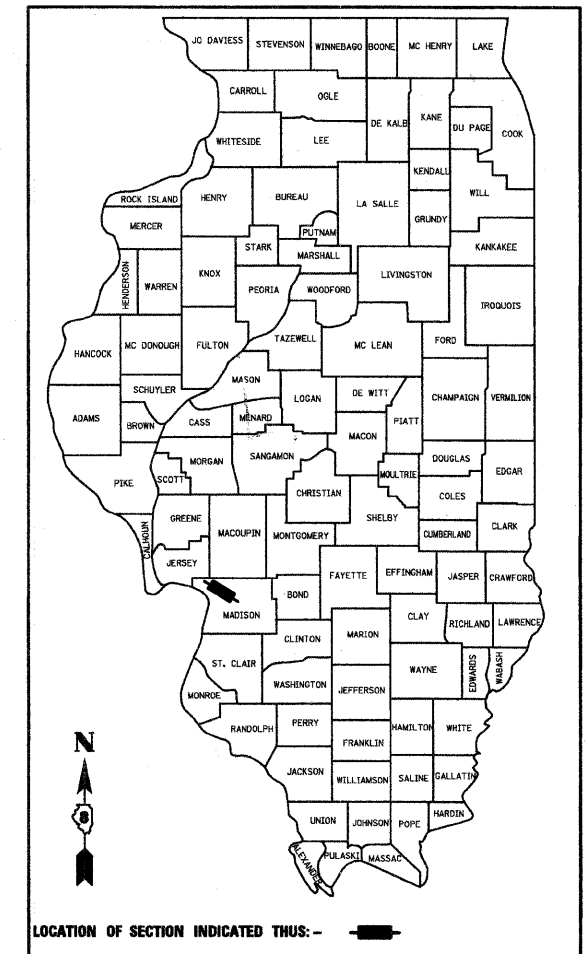
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|---------------|---------|--------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 1 |

*474+1 = 475

CONTRACT NO. 76323

FOR INDEX OF SHEETS AND HIGHWAY STANDARDS SEE SHEET NO. 2

D-98-038-92



C-98-108-03
 MAINLINE GRADING AND PAVING
 CAST-IN-PLACE BOX CULVERT AT ROCK CREEK
 FAP ROUTE 310

MICROFILMED _____
 REEL NUMBER _____
 AWARDED _____
 RESIDENT ENGINEER _____
 AS BUILT CHANGES WERE MADE ON THE FOLLOWING SHEETS

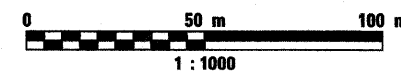
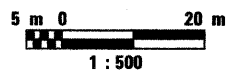
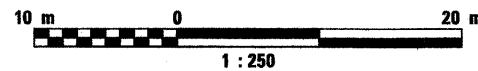
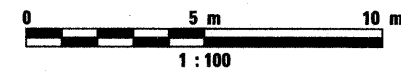
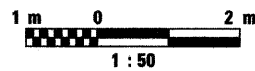
SECTION 60-15
 PROJECT ENDS
 STA 40+635.165 (SBL)
 STA 40+665.645 (NBL)

BRIDGE OMISSION
 STA 39+885.770 TO STA 39+943.450 (NBL)
 STA 39+885.770 TO STA 39+943.450 (SBL)

BRIDGE OMISSION
 STA 39+133.793 TO STA 39+191.756 (NBL)
 STA 39+126.063 TO STA 39+187.431 (SBL)
 STA 0+284.426 TO STA 0+341.063 (RAMP C)
 STA 0+077.907 TO STA 0+012.622 (RAMP D)

BRIDGE OMISSION
 STA 38+799.695 TO STA 38+865.865 (NBL)
 STA 38+798.239 TO STA 38+863.008 (SBL)

METRIC RATIOS



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION
 FOR EXCAVATION (1-800-892-0123)

CONTRACT NO. 76323

ERIC B. BARNES
 LICENSED PROFESSIONAL ENGINEER
 062-052141
 DATE 11/12/07
 REGISTERED PROFESSIONAL ENGINEER
 STATE OF ILLINOIS NO. 062-052141
 LICENSE EXPIRES NOVEMBER 30, 2009



GROSS LENGTH OF SECTION 60-15 = 5515.645 METERS = 5 516 KILOMETERS
 NET LENGTH OF SECTION 60-15 = 5331.828 METERS = 5 332 KILOMETERS

DESIGN DESIGNATION
 2230(24) ARTERIAL 7.11 (FD-20)

PLANS PREPARED BY:
KLINGNER & ASSOCIATES, P.C.
 Engineers • Architects • Surveyors

656 North 24th Street, Quincy, IL
 4508 Persh Gravel Road, Hannibal, MO
 608 N. 4th Street, Suite 100, Burlington, IA
 41 North Prairie Street, Galena, IL
 Internet Address: www.klingner.com
 Ph (217) 223-3570 - Fax (217) 223-3583
 Ph (573) 221-8828 - Fax (573) 221-8812
 Ph (319) 753-1636 - Fax (319) 752-3626
 Ph (309) 342-4842 - Fax (309) 341-3781
 STATE OF ILLINOIS DESIGN FIRM # 1842738

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS
 SUBMITTED Oct 14 20 09
Ma L. Ramo
 DEPUTY DIRECTOR OF HIGHWAYS, REGION FIVE ENGINEER
January 09 20 10
Scott E. Stitt P.E. / ea
 ENGINEER OF DESIGN AND ENVIRONMENT
January 09 20 10
Christine M. Reed / ea
 DIRECTOR, DIVISION OF HIGHWAYS

PRINTED BY THE AUTHORITY
 OF THE STATE OF ILLINOIS

PROJECT ENGINEER: PATTI LEBEAU (618)346-3179
 SQUAD CONTACT: ARTHUR MUEHLFELD (618)346-3209

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






| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------------|---------------------------|--------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 2 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

CONTRACT NO. 76323

IDOT HIGHWAY STANDARDS

- 000001-05 STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
- 001001-02 AREAS OF REINFORCEMENT BARS
- 202001-01 EARTH MEDIAN DITCH CHECK
- 280001-05 TEMPORARY EROSION CONTROL SYSTEMS
- 406001-05 ENTRANCE RAMP TERMINAL (FLEXIBLE RAMP PAVEMENT ADJACENT TO FLEXIBLE MAINLINE PAVEMENT)
- 406101-04 EXIT RAMP TERMINAL (FLEXIBLE RAMP PAVEMENT ADJACENT TO FLEXIBLE MAINLINE PAVEMENT)
- 420401-08 BRIDGE APPROACH PAVEMENT
- 482001-02 HMA SHOULDER ADJACENT TO FLEXIBLE PAVEMENT
- 542206-01 REINFORCED CONCRETE END SECTIONS FOR PIPE CULVERTS
1050 mm (42") THRU 1500 mm (60") DIAMETER SKEWED WITH ROADWAY
- 542301-02 PRECAST REINFORCED CONCRETE FLARED END SECTION
- 542311-01 GRATING FOR CONCRETE FLARED END SECTION FOR 600 mm (24") THRU 1300 mm (54") PIPE
- 542401-01 METAL END SECTION FOR PIPE CULVERTS
- 542606-01 REINFORCED CONCRETE PIPE TEE
- 601001-03 SUB-SURFACE DRAINS
- 601101-01 CONCRETE HEADWALL FOR PIPE DRAIN
- 602401-02 MANHOLE, TYPE A
- 602601-02 PRECAST REINFORCED CONCRETE FLAT SLAB TOP
- 602701-02 CAST IRON STEPS
- 604001-03 FRAME AND LIDS, TYPE 1
- 604101-01 MEDIAN INLET FOR 600 mm (24") REINFORCED CONCRETE PIPE
- 609006-05 BRIDGE APPROACH PAVEMENT (DRAIN DETAIL)
- 630001-08 STEEL PLATE BEAM GUARDRAIL
- 630201-06 PCC/HMA STABILIZATION AT STEEL PLATE BEAM GUARDRAIL
- 631011-06 TRAFFIC BARRIER TERMINAL TYPE 2
- 631026-05 TRAFFIC BARRIER TERMINAL TYPE 5 & 5A
- 631031-08 TRAFFIC BARRIER TERMINAL TYPE 6
- 635001-01 DELINEATORS
- 635006-03 REFLECTOR AND TERMINAL MARKER PLACEMENT
- 635011-02 REFLECTOR MARKER AND MOUNTING DETAILS
- 642001-01 SHOULDER RUMBLE STRIPS
- 665001-02 WOVEN WIRE FENCE
- 666001-01 RIGHT OF WAY MARKERS
- 667101-01 PERMANENT SURVEY MARKERS
- 701001-02 OFF-ROAD OPERATIONS, 2L, 2W, MORE THAN 4.5 m AWAY
- 701006-03 OFF-ROAD OPERATIONS, 2L, 2W, 4.5 m TO 600 mm FROM PAVEMENT EDGE
- 701101-02 OFF-ROAD OPERATIONS, MULTILANE, 4.5 m TO 600 mm FROM PAVEMENT EDGE
- 701106-02 OFF-ROAD OPERATIONS, MULTILANE, MORE THAN 4.5 m AWAY
- 701602-04 URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
- 702001-06 TRAFFIC CONTROL DEVICES
- 720011-01 METAL POSTS FOR SIGNS, MARKERS & DELINEATORS
- 780001-02 TYPICAL PAVEMENT MARKINGS
- 781001-03 TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS

LEGEND

-  EXISTING DITCH
-  PROPOSED STANDARD DITCH (SEE TYPICAL SECTIONS)
-  PROPOSED SPECIAL DITCH
-  PROPOSED DITCH SUMMIT
- BOP** BEGINNING OF PROFILE GRADE
- EOP** END OF PROFILE GRADE
-  ITEM TO BE REMOVED
-  RIPRAP
-  TREE TO BE REMOVED (INDIVIDUAL)
-  EARTH MEDIAN DITCH CHECK (STD 202001)
-  TREES TO BE REMOVED (AREA)

INDEX OF SHEETS

- 1 COVER SHEET
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- 34-44 HORIZONTAL CONTROL TIES
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- 196-243 SOIL STRATA PLAN AND PROFILE SHEETS
- 244-252 ROCK CREEK CULVERT PLANS
- 253-258 GUARDRAIL AND SHOULDER WIDENING DETAILS
- 259-260 HEADWALL DETAILS
- 261-262 BRIDGE APPROACH PAVEMENT (SPECIAL) DETAILS
- 263-265 MISCELLANEOUS DETAILS
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- 358-372 CULVERT CROSS SECTIONS
- 373-457 RAMP CROSS SECTIONS
- 458-474 MISCELLANEOUS GRADING CROSS SECTIONS

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION INDEX OF SHEETS FAP 310 (IL 255) SECTION 60-15 MADISON COUNTY DRAWN BY EBB CHECKED BY |
|-----------|------|---|
| NAME | DATE | |
| | | DATE |
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------------|----------|------------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 3 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |
| CONTRACT NO. 76323 | | | | |

GENERAL NOTES

- ALL ELEVATIONS REFER TO USGS MEAN SEA LEVEL DATUM.
- WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL MONUMENTS UNTIL AN AUTHORIZED SURVEYOR OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION. THE CONTRACTOR WILL BE RESPONSIBLE FOR HAVING AN AUTHORIZED SURVEYOR RE-ESTABLISH ANY SECTION OR SUBSECTION MONUMENTS DESTROYED BY HIS OPERATIONS.
- ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE NUMBER LISTED OR THE COPY OF STANDARD INCLUDED IN THESE PLANS. THE STANDARDS AND REVISION NUMBERS SHALL APPLY TO THIS PROJECT.
- STRAW BALES, HAY BALES, PERIMETER EROSION BARRIER, AND SILT FENCES WILL NOT BE PERMITTED FOR TEMPORARY OR PERMANENT DITCH CHECKS. DITCH CHECKS SHALL BE COMPOSED OF AGGREGATE, ROLLED EXCELSIOR, URETHANE FOAM, GEOTEXTILE, SILT PANELS OR SILT WEDGES.
- ABANDONED UNDERGROUND UTILITIES THAT CONFLICT WITH CONSTRUCTION SHALL BE DISPOSED OUTSIDE THE LIMITS OF RIGHT OF WAY ACCORDING TO ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF EARTH EXCAVATION AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- SEEDING SHALL BE PLACED ON ALL AREAS THAT ARE DISTURBED BY CONSTRUCTION OPERATIONS ACCORDING TO SECTION 250 OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND AS DIRECTED BY THE ENGINEER. SEEDING NOT FALLING WITHIN THE CONSTRUCTION LIMITS OR NOT DIRECTLY RELATED TO THE CONTRACT WORK SHALL BE CONSIDERED THE RESPONSIBILITY OF THE CONTRACTOR.
- THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- PROTECTIVE COATING SHALL BE APPLIED TO THE SURFACE OF NEW CONCRETE ACCORDING TO SECTION 420 OF THE STANDARD SPECIFICATIONS.
- ALL TREES, BRUSH, AND SHRUBS WITHIN THE CONSTRUCTION LIMITS WILL BE REMOVED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS. ONLY THOSE TREES DESIGNATED BY THE ENGINEER OR LISTED IN THE TREE REMOVAL SCHEDULE SHALL BE REMOVED. TREES ALONG THE EDGE OF THE RIGHT OF WAY SHALL BE SAVED IF, IN THE OPINION OF THE ENGINEER, THEY DO NOT INTERFERE WITH CONSTRUCTION OPERATIONS. THE CONTRACTOR WILL PROTECT ALL REMAINING TREES FROM DAMAGE DUE TO HIS OPERATIONS.
- ILLINOIS STATE LAW REQUIRES A 48-HOUR NOTICE TO BE GIVEN TO UTILITIES BEFORE DIGGING. FIELD MARKING OF FACILITIES MAY BE OBTAINED BY CONTACTING J.U.L.I.E. OR FOR NON-MEMBERS, THE UTILITY COMPANY DIRECTLY. AGENCIES KNOWN TO HAVE FACILITIES WITHIN THE PROJECT ARE AS FOLLOWS:

ABOVE GROUND

- AMEREN UE

BELOW GROUND

- SBC
- FOSTERBURG WATER DISTRICT
- IL AMERICAN WATER - ALTON
- AMEREN UE
- MCI WORLDCOM
- AT&T CORPORATION
- SPRINT
- VILLAGE OF GODFREY
- CHARTER COMMUNICATIONS

MEMBERS OF J.U.L.I.E. (800)-892-0123 ARE INDICATED BY *. NON J.U.L.I.E. MEMBERS MUST BE NOTIFIED INDIVIDUALLY.

- IF ARCHAEOLOGICAL CLEARANCE HAS NOT BEEN OBTAINED FOR THE ENTIRE PROJECT, THE RESIDENT ENGINEER SHALL PROVIDE THE CONTRACTOR THOSE AREAS OF THE PROJECT WHICH HAVE BEEN CLEARED AND IN WHICH THE CONTRACTOR MAY WORK. THE RESIDENT ENGINEER SHALL ALSO NOTIFY THE CONTRACTOR WHEN ADDITIONAL SITES BECOME AVAILABLE.
- THE CONTRACTOR SHALL BE AWARE THAT HE MAY FIND ARCHEOLOGICAL EXCAVATIONS THAT HAVE NOT BEEN FILLED IN. THE CONTRACTOR SHALL DEWATER THE EXCAVATION, IF NECESSARY, AND FILL AND COMPACT THE HOLE WITH DIRT IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS AND TO THE SATISFACTION OF THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE CONSIDERED INCLUDED IN THE COST FOR EARTHWORK, AND NO OTHER COMPENSATION WILL BE ALLOWED.
- MIXTURE REQUIREMENTS:

| MIXTURE USE | POLY SURFACE | BINDER | SHOULDERS |
|-------------------------------------|----------------|----------------|------------------|
| AC/PG | SBS 76-22 | PG 64-22 | PG 64-22 |
| RAP % (MAX) | 10% | 10% | 30% |
| DESIGN AIR VOIDS | 4.0% @ Ndes=90 | 4.0% @ Ndes=90 | **2.0% @ Ndes=30 |
| MIX COMPOSITION (GRADATION MIXTURE) | | IL 19.0 | |
| FRICITION AGG | MIXTURE "D" | MIXTURE "B" | BAM |

** TOP LIFT SHOULDERS - DESIGN THIS MIX AT 2.0% VOIDS AND ADD ASPHALT TO REDUCE VOIDS TO 1.5%.

PLAN QUANTITIES FOR BITUMINOUS CONCRETE SURFACE COURSE ITEMS ARE CALCULATED USING A WEIGHT OF 112 LB/SQ YD/IN (59.8 KG/SQ M/25 MM THICKNESS)

GENERAL NOTES

- FERTILIZER NUTRIENTS SHALL BE APPLIED TO BOTH THE SEEDED AREAS AND THE AREAS COVERED WITH EROSION CONTROL BLANKET.
- DO NOT INCLUDE MULCH OR EMULSIFIED ASPHALT ON EROSION CONTROL BLANKET AREAS.
- ALL EROSION CONTROL PRODUCTS FURNISHED SHALL BE SPECIFICALLY RECOMMENDED BY THE MANUFACTURER FOR THE USE SPECIFIED IN THE EROSION CONTROL PLAN. PRIOR TO THE APPROVAL AND USE OF THE PRODUCT, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER A NOTARIZED CERTIFICATION BY THE PRODUCER STATING THE INTENDED USE OF THE PRODUCT AND THAT THE PHYSICAL PROPERTIES REQUIRED FOR THIS APPLICATION ARE MET OR EXCEEDED. THE CONTRACTOR SHALL PROVIDE MANUFACTURER RECOMMENDED INSTALLATION PROCEDURES TO FACILITATE THE ENGINEER IN CONSTRUCTION INSPECTION.
- IF ANY UNSUITABLE MATERIAL IS ENCOUNTERED DURING CONSTRUCTION, IT WILL BE NECESSARY TO REMOVE THE UNSUITABLE MATERIAL AND REPLACE IT WITH A SUITABLE MATERIAL AS APPROVED BY THE ENGINEER. THIS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04.
- THE FOLLOWING STRUCTURES HAVE BEEN STRUCTURALLY ANALYZED AS TO THEIR ABILITY TO BE CROSSED BY AN EMPTIED MATERIAL TRANSFER DEVICE AS PER THE CONDITIONS STATED IN THE SPECIAL PROVISION ENTITLED "MATERIAL TRANSFER DEVICE":

| | | |
|-------------|-------------|-------------|
| SN 060-0308 | SN 060-0310 | SN 060-0330 |
| SN 060-0309 | SN 060-0311 | SN 060-0331 |

IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE IF THE MATERIAL TRANSFER DEVICE MUST CROSS ANY ADDITIONAL STRUCTURES (BEYOND THOSE SPECIFIED ABOVE). THE CONTRACTOR MUST PROVIDE A LIST OF ADDITIONAL STRUCTURES TO THE RESIDENT ENGINEER/RESIDENT TECHNICIAN A MINIMUM OF 30 DAYS IN ADVANCE OF THE DATE THE MATERIAL TRANSFER DEVICE WILL BE USED, IN ORDER THAT A STRUCTURAL ANALYSIS CAN BE PERFORMED BY THE DEPARTMENT'S BUREAU OF BRIDGES AND STRUCTURES.
- THE MATERIAL TRANSFER DEVICE (MTD) MAY BE ALLOWED TO TRAVEL OVER STRUCTURES IF GIVEN APPROVAL FROM THE ENGINEER, THE MTD IS EMPTIED OF HMA MATERIAL PRIOR TO CROSSING THE STRUCTURE, AND THE TIRES OF THE MTD TRAVEL ON OR IN CLOSE PROXIMITY AND PARALLEL TO THE BEAM AND/OR GIRDER LINES OF THE STRUCTURE. THE RESIDENT ENGINEER SHOULD MARK THE DECK SURFACE FOR THE LOCATION OF THE BEAM AND/OR GIRDERS. THE COST TO EMPTY THE MTD OF HMA MATERIAL WILL BE AT THE EXPENSE OF THE CONTRACTOR. IF THE NBIS RATING OF A STRUCTURE IS LESS THAN OR EQUAL TO 4.0, THE DEPARTMENT WILL PAY TO HAVE THE MTD MOVED ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- RIGHT-OF-WAY MARKERS SHALL BE SET SO THE BACK OF THE POST IS TWELVE (12") INCHES INSIDE THE RIGHT OF WAY BOUNDARY. RIGHT OF WAY CORNERS ARE MARKED BY A 5/8" IRON ROD WITH IDOT ALUMINUM CAP AND SHALL NOT BE REMOVED OR DAMAGED WHEN SETTING THE RIGHT OF MARKERS.
- THE PROPOSED EMBANKMENT SHALL BE BENCHED INTO THE EXISTING SLOPES TO THE SATISFACTION OF THE ENGINEER.
- PERFORMED PLASTIC PAVEMENT MARKING, TYPE "B" FOR THE CENTERLINE MARKINGS SHALL BE PLACED USING THE TYPE "A" METHOD.
- IF THERE IS A DISCREPANCY BETWEEN THE RIGHT OF WAY SHOWN ON THE PLANS AND THE PLAT OF HIGHWAYS, THE PLAT OF HIGHWAYS ALWAYS TAKES PRECEDENCE.
- ANY PAVEMENT MARKING OR PAVEMENT MARKING REMOVAL NOT SHOWN IN THESE PLANS BUT REQUIRED TO OPEN TRAFFIC WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- THE REMOVAL AND DISPOSAL OF ABANDONED FENCING, DEBRIS, BRUSH, RIPRAP, STONE, CONCRETE SLABS, TILE, SIDEWALK, ETC. NOT PAID FOR SPECIFICALLY ON THE PLANS WILL BE CONSIDERED TO BE INCLUDED IN THE EARTH EXCAVATION PAY ITEM.
- GRADING WILL BE DONE BY HAND AROUND LIGHT POLES, UTILITY POLES, SIGN POSTS, EXISTING SHRUBS AND TREES, OR OTHER NATURAL OR MAN-MADE OBJECTS WHERE SHALLOW FILL OR CUTS ARE ADJACENT TO THE ITEMS. THE ITEMS TO REMAIN IN PLACE WILL BE ACCORDING TO THE ENGINEER. THIS WORK WILL BE CONSIDERED TO BE INCLUDED IN THE EARTH EXCAVATION PAY ITEM.
- THE FOLLOWING RATES OF APPLICATION HAVE BEEN ASSUMED IN CALCULATING PLAN QUANTITIES:

| | |
|---|----------------------------|
| GRANULAR MATERIALS | 2.5 M TONS / CU M |
| HOT-MIX ASPHALT BINDER COURSE AND SHOULDERS | 0.00244 M TONS / SQ M / MM |
| HOT-MIX ASPHALT SURFACE COURSE | 0.00262 M TONS / SQ M / MM |
| LIME | 68.56 KG / CU M |

1 METRIC TON = 1000 KG
 BITUMINOUS MATERIALS: 1 LITER = 0.00091 M TONS

COMMITMENTS

NO COMMITMENTS HAVE BEEN MADE FOR THIS PROJECT.

| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
GENERAL NOTES
FAP 310 (IL 255)
SECTION 60-15
MADISON COUNTY
 DRAWN BY EBB
 CHECKED BY
 DATE

SUMMARY OF QUANTITIES

| | | | | |
|---------------------|---------------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 4 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |
| CONTRACT NO. 76323 | | | | |

| SUMMARY OF QUANTITIES | | | | CONSTRUCTION TYPE CODE | | |
|-----------------------|--|------------------|------------------|--------------------------|-----------------------|--|
| CODE NO | ITEM | UNIT | TOTAL QUANTITIES | URBAN | | |
| | | | | 80% FEDERAL 20% STATE | | |
| | | | | SEC 60-15 I000 | SEC 60-15B X028-2A | |
| 28000300 | TEMPORARY DITCH CHECKS | EACH | 430 | 430 | | |
| M28000315 | AGGREGATE DITCH CHECKS | M TON | 1290 | 1290 | | |
| 28000500 | INLET AND PIPE PROTECTION | EACH | 34 | 34 | | |
| 40702700 | FURNISH PROFILOGRAPH | L SUM | 1 | 1 | | |
| 51500100 | NAME PLATES | EACH | 1 | | 1 | |
| 60100060 | CONCRETE HEADWALL FOR PIPE DRAINS | EACH | 109 | 109 | | |
| 60246605 | MEDIAN INLET (604101) | EACH | 8 | 8 | | |
| 60500060 | REMOVING INLETS | EACH | 1 | 1 | | |
| 60900240 | TYPE C INLET BOX, STANDARD 609006 | EACH | 6 | 6 | | |
| 60900315 | TYPE D INLET BOX, STANDARD 609006 | EACH | 6 | 6 | | |
| 60900515 | CONCRETE THRUST BLOCKS | EACH | 6 | 6 | | |
| * 63100167 | TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT | EACH | 14 | 14 | | |
| * 63100045 | TRAFFIC BARRIER TERMINAL, TYPE 2 | EACH | 8 | 8 | | |
| * 63100070 | TRAFFIC BARRIER TERMINAL, TYPE 5 | EACH | 6 | 6 | | |
| * 63100085 | TRAFFIC BARRIER TERMINAL, TYPE 6 | EACH | 12 | 12 | | |
| 63500105 | DELINEATORS | EACH | 317 | 317 | | |
| 66600105 | FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS | EACH | 94 | 94 | | |
| 66700205 | PERMANENT SURVEY MARKERS, TYPE I | EACH | 37 | 37 | | |
| 66700305 | PERMANENT SURVEY MARKERS, TYPE II | EACH | 16 | 16 | | |
| 66700705 | FURNISHING AND ERECTING DRAINAGE MARKERS | EACH | 2 | 2 | | |
| 67000400 | ENGINEER'S FIELD OFFICE, TYPE A | CAL MO | 29 | 29 | | |
| 67000600 | ENGINEER'S FIELD LABORATORY | CAL MO | 29 | 29 | | |
| 67100100 | MOBILIZATION | L SUM | 1 | 1 | | |
| 70102632 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701602 | L SUM | 1 | 1 | | |
| * 78100100 | RAISED REFLECTIVE PAVEMENT MARKER | EACH | 941 | 941 | | |
| * 78100105 | RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE) | EACH | 58 | 58 | | |
| * 78200410 | GUARDRAIL MARKERS, TYPE A | EACH | 80 | 80 | | |
| * 78200520 | BARRIER WALL MARKERS, TYPE B | EACH | 48 | 48 | | |
| * 78201000 | TERMINAL MARKER - DIRECT APPLIED | EACH | 14 | 14 | | |
| Z0007601 | BUILDING REMOVAL NO. 1 | L SUM | 1 | 1 | | |
| Z0007608 | BUILDING REMOVAL NO. 8 | L SUM | 1 | 1 | | |
| Z0007609 | BUILDING REMOVAL NO. 9 | L SUM | 1 | 1 | | |
| Z0007610 | BUILDING REMOVAL NO. 10 | L SUM | 1 | 1 | | |
| Z0007611 | BUILDING REMOVAL NO. 11 | L SUM | 1 | 1 | | |
| Z0007612 | BUILDING REMOVAL NO. 12 | L SUM | 1 | 1 | | |
| Z0013798 | CONSTRUCTION LAYOUT | L SUM | 1 | 1 | | |
| * Z0049809 | REMOVAL AND DISPOSAL OF FRIABLE ASBESTOS, BUILDING NO. 9 | L SUM | 1 | 1 | | |
| * Z0049810 | REMOVAL AND DISPOSAL OF FRIABLE ASBESTOS, BUILDING NO. 10 | L SUM | 1 | 1 | | |
| * Z0049811 | REMOVAL AND DISPOSAL OF FRIABLE ASBESTOS, BUILDING NO. 11 | L SUM | 1 | 1 | | |
| * Z0049909 | REMOVAL AND DISPOSAL OF NON-FRIABLE ASBESTOS, BUILDING NO. 9 | L SUM | 1 | 1 | | |
| * Z0049910 | REMOVAL AND DISPOSAL OF NON-FRIABLE ASBESTOS, BUILDING NO. 10 | L SUM | 1 | 1 | | |
| * Z0049912 | REMOVAL AND DISPOSAL OF NON-FRIABLE ASBESTOS, BUILDING NO. 12 | L SUM | 1 | 1 | | |
| * Z0049911 | REMOVAL AND DISPOSAL OF NON-FRIABLE ASBESTOS, BUILDING NO. 11 | L SUM | 1 | 1 | | |

* SPECIALTY ITEM

| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES
 FAP 310 (IL 255)
 SECTION 60-15
 MADISON COUNTY
 DRAWN BY EBB
 CHECKED BY
 DATE

Rev.

SUMMARY OF QUANTITIES

| | | | | |
|---------------------|---------------|------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 5 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

CONTRACT NO. 76323

| SUMMARY OF QUANTITIES | | | | CONSTRUCTION TYPE CODE | | |
|-----------------------|--|-------|------------------|--------------------------|-----------------------|--|
| CODE NO | ITEM | UNIT | TOTAL QUANTITIES | URBAN | | |
| | | | | 80% FEDERAL 20% STATE | | |
| | | | | SEC 60-15 I000 | SEC 60-15B X028-2A | |
| Z0030150 | IMPACT ATTENUATORS (NON-REDIRECTIVE), TEST LEVEL 3 | EACH | 2 | 2 | | |
| Z0038500 | PERMANENT BARRICADES | EACH | 12 | 12 | | |
| Z0050900 | REMOVE CONCRETE FOUNDATION | EACH | 1 | 1 | | |
| Z0065100 | SETTLEMENT PLATFORMS | EACH | 1 | 1 | | |
| M2010110 | TREE REMOVAL (6 TO 15 UNITS DIAMETER) | UNIT | 934 | 934 | | |
| M2010210 | TREE REMOVAL (OVER 15 UNITS DIAMETER) | UNIT | 408 | 408 | | |
| M2010500 | TREE REMOVAL, HECTARES | HA | 23.4 | 23.4 | | |
| M2020010 | EARTH EXCAVATION | CU M | 775915 | 775915 | | |
| M2040800 | FURNISHED EXCAVATION | CU M | 371210 | 371210 | | |
| M2500200 | SEEDING, CLASS 2 | HA | 45.3 | 45.3 | | |
| M2080150 | TRENCH BACKFILL | CU M | 430 | 430 | | |
| M2500400 | NITROGEN FERTILIZER NUTRIENT | KG | 4530 | 4530 | | |
| M2500500 | PHOSPHORUS FERTILIZER NUTRIENT | KG | 4530 | 4530 | | |
| M2500600 | POTASSIUM FERTILIZER NUTRIENT | KG | 4530 | 4530 | | |
| M2510115 | MULCH, METHOD 2 | HA | 45.3 | 45.3 | | |
| M2510630 | EROSION CONTROL BLANKET | SQ M | 36525 | 36525 | | |
| M2800200 | EARTH EXCAVATION FOR EROSION CONTROL | CU M | 300 | 300 | | |
| M2800250 | TEMPORARY EROSION CONTROL SEEDING | KG | 15000 | 15000 | | |
| M2800400 | PERIMETER EROSION BARRIER | METER | 549 | 549 | | |
| M2801000 | AGGREGATE (EROSION CONTROL) | M TON | 3495 | 3495 | | |
| M2810109 | STONE RIPRAP, CLASS A5 | SQ M | 432 | | 432 | |
| M2810707 | STONE DUMPED RIPRAP, CLASS A4 | SQ M | 7826 | 7826 | | |
| M2810725 | STONE DUMPED RIPRAP, CLASS B3 | SQ M | 4360 | 4360 | | |
| M2811840 | RIPRAP FOR STILLING BASIN | M TON | 5695 | 5695 | | |
| M2820200 | FILTER FABRIC | SQ M | 10917 | 10485 | 432 | |
| M3020456 | PROCESSING MODIFIED SOIL 300MM | SQ M | 163342 | 163342 | | |
| M3101500 | LIME | M TON | 3359.7 | 3359.7 | | |
| M3112010 | SUB-BASE GRANULAR MATERIAL, TYPE C | M TON | 29014 | 29014 | | |
| M4060895 | CONSTRUCTING TEST STRIP | EACH | 2 | 2 | | |
| M4075390 | HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 390MM | SQ M | 82448 | 82448 | | |
| M4075400 | HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 400MM | SQ M | 14463 | 14463 | | |
| M4205000 | BRIDGE APPROACH PAVEMENT | SQ M | 437 | 437 | | |
| M4205050 | BRIDGE APPROACH PAVEMENT (SPECIAL) | SQ M | 1225 | 1225 | | |
| M4205200 | PROTECTIVE COAT | SQ M | 1662 | 1662 | | |
| M4812000 | AGGREGATE SHOULDERS, TYPE B | M TON | 14526 | 14526 | | |
| M4820600 | HOT-MIX ASPHALT SHOULDERS, 200MM | SQ M | 57234 | 57234 | | |
| M5010522 | PIPE CULVERT REMOVAL | METER | 54 | 54 | | |
| M5080105 | REINFORCEMENT BARS | KG | 529240 | 2020 | 527220 | |

| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
 SUMMARY OF QUANTITIES
 FAP 310 (IL 255)
 SECTION 60-15
 MADISON COUNTY
 DRAWN BY EBB
 CHECKED BY
 DATE

Rev.

SUMMARY OF QUANTITIES

| | | | | |
|---------------------|---------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 6 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

CONTRACT NO. 76323

| SUMMARY OF QUANTITIES | | | | CONSTRUCTION TYPE CODE | | | |
|-----------------------|---|-------|------------------|--------------------------|------------|--|--|
| | | | | URBAN | | | |
| | | | | 80% FEDERAL 20% STATE | | | |
| CODE NO | ITEM | UNIT | TOTAL QUANTITIES | SEC 60-15 | SEC 60-15B | | |
| | | | | I000 | X028-2A | | |
| M5110200 | SLOPE WALL 150 MM | SQ M | 293 | 293 | | | |
| M5401110 | PRECAST CONCRETE BOX CULVERT 2.4M X 2.1M | METER | 181.4 | 181.4 | | | |
| M5403000 | CONCRETE BOX CULVERTS | CU M | 2878.2 | | 2878.2 | | |
| M542C528 | REINFORCED CONCRETE PIPE TEE, 600MM PIPE WITH 600MM RISER | EACH | 5 | 5 | | | |
| M542C544 | REINFORCED CONCRETE PIPE TEE, 900MM PIPE WITH 600MM RISER | EACH | 1 | 1 | | | |
| M542C552 | REINFORCED CONCRETE PIPE TEE, 1200MM PIPE WITH 600MM RISER | EACH | 1 | 1 | | | |
| M542E112 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 300MM | EACH | 1 | 1 | | | |
| M542E128 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 600MM | EACH | 9 | 9 | | | |
| M542E144 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 900MM | EACH | 11 | 11 | | | |
| M542E148 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 1050MM | EACH | 2 | 2 | | | |
| M542E156 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 1350MM | EACH | 2 | 2 | | | |
| M542E168 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 1800MM | EACH | 2 | 2 | | | |
| M542F012 | METAL END SECTIONS 300MM | EACH | 12 | 12 | | | |
| M542F252 | CAST-IN-PLACE REINFORCED CONCRETE END SECTIONS 1200MM | EACH | 2 | 2 | | | |
| M542G035 | GRATING FOR CONCRETE FLARED END SECTION 600MM | EACH | 1 | 1 | | | |
| M542G055 | GRATING FOR CONCRETE FLARED END SECTION 900MM | EACH | 3 | 3 | | | |
| M542I225 | PIPE CULVERTS, TYPE 1 RCCP 600MM | METER | 0.5 | 0.5 | | | |
| M542I240 | PIPE CULVERTS, TYPE 1 RCCP 900MM | METER | 16.0 | 16.0 | | | |
| M5423425 | PIPE CULVERTS, TYPE 2 RCCP 600MM | METER | 156.5 | 156.5 | | | |
| M5423445 | PIPE CULVERTS, TYPE 2 RCCP 900MM | METER | 110.5 | 110.5 | | | |
| M5425605 | PIPE CULVERTS, TYPE 3 RCCP 300MM | METER | 9.0 | 9.0 | | | |
| M5425625 | PIPE CULVERTS, TYPE 3 RCCP 600MM | METER | 18.5 | 18.5 | | | |
| M5427925 | PIPE CULVERTS, TYPE 4 RCCP 600MM | METER | 36.5 | 36.5 | | | |
| M5427940 | PIPE CULVERTS, TYPE 4 RCCP 900MM | METER | 69.0 | 69.0 | | | |
| M5427955 | PIPE CULVERTS, TYPE 4 RCCP 1350MM | METER | 73.5 | 73.5 | | | |
| M5427970 | PIPE CULVERTS, TYPE 4 RCCP 1800MM | METER | 41.5 | 41.5 | | | |
| M5428545 | PIPE CULVERTS, TYPE 5 RCCP 1050MM | METER | 126.0 | 126.0 | | | |
| M5429125 | PIPE CULVERTS, TYPE 6 RCCP 600MM | METER | 56.5 | 56.5 | | | |
| M5429750 | PIPE CULVERTS, TYPE 7 RCCP 1200MM | METER | 112.5 | 112.5 | | | |
| M5429900 | CONCRETE HEADWALLS | CU M | 40.4 | 40.4 | | | |
| M5930100 | CONTROLLED LOW-STRENGTH MATERIAL | CU M | 2 | 2 | | | |
| M6010125 | PIPE DRAINS 300MM | METER | 151.0 | 151.0 | | | |
| M6010605 | PIPE UNDERDRAINS 100MM | METER | 15933.5 | 15933.5 | | | |
| M6010705 | PIPE UNDERDRAINS 100MM (SPECIAL) | METER | 652.5 | 652.5 | | | |
| M6021511 | MANHOLES, TYPE A, 1.2M DIAMETER, WITH MEDIAN INLET (604101) | EACH | 2 | 2 | | | |
| M6021605 | MANHOLES, TYPE A, 1.5M DIAMETER, TYPE 1 FRAME, OPEN LID | EACH | 4 | 4 | | | |
| M6023105 | MANHOLES, SPECIAL, 1.5M DIAMETER | EACH | 1 | 1 | | | |
| * M6300101 | STEEL PLATE BEAM GUARD RAIL, TYPE A, 1.83M Posts | METER | 2514.60 | 2514.60 | | | |

* SPECIALTY ITEM

| REVISIONS | |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES
 FAP 310 (IL 255)
 SECTION 60-15
 MADISON COUNTY
 DRAWN BY EBB
 CHECKED BY
 DATE

8/11/2009

SUMMARY OF QUANTITIES

| | | | | |
|---------------------|---------------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 7 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

CONTRACT NO. 76323

| SUMMARY OF QUANTITIES | | | | CONSTRUCTION TYPE CODE | | |
|-----------------------|---|-------|------------------|--------------------------|------------|--|
| CODE NO | ITEM | UNIT | TOTAL QUANTITIES | URBAN | | |
| | | | | SEC 60-15 | SEC 60-15B | |
| | | | | 80% FEDERAL 20% STATE | | |
| | | | | 1000 | X028-2A | |
| M6420015 | SHOULDER RUMBLE STRIP | METER | 19261 | 19261 | | |
| M6640200 | CHAIN LINK FENCE, 1.2 METER (SPECIAL) | METER | 712 | 712 | | |
| M6650100 | WOVEN WIRE FENCE, 1.2 METER | METER | 11615 | 11615 | | |
| M6650340 | WOVEN WIRE GATES, 1.2M X 4.9M DOUBLE | EACH | 14 | 14 | | |
| * M7800100 | THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS | SQ M | 3 | 3 | | |
| * M7800105 | THERMOPLASTIC PAVEMENT MARKING - LINE 100MM | METER | 25839 | 25839 | | |
| * M7800120 | THERMOPLASTIC PAVEMENT MARKING - LINE 200MM | METER | 1806 | 1806 | | |
| * M7800125 | THERMOPLASTIC PAVEMENT MARKING - LINE 300MM | METER | 190 | 190 | | |
| * M7800140 | THERMOPLASTIC PAVEMENT MARKING - LINE 600MM | METER | 22 | 22 | | |
| * M7800205 | PAINT PAVEMENT MARKING - LINE 100MM | METER | 1500 | 1500 | | |
| * M7800405 | PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 100MM | METER | 2344 | 2344 | | |
| * M7800600 | EPOXY PAVEMENT MARKING - LETTERS AND SYMBOLS | SQ M | 14 | 14 | | |
| * M7800605 | EPOXY PAVEMENT MARKING - LINE 100MM | METER | 1969 | 1969 | | |
| * M7800620 | EPOXY PAVEMENT MARKING - LINE 200MM | METER | 399 | 399 | | |
| * M7800625 | EPOXY PAVEMENT MARKING - LINE 300MM | METER | 262 | 262 | | |
| * M7800640 | EPOXY PAVEMENT MARKING - LINE 600MM | METER | 58 | 58 | | |
| M7830100 | PAVEMENT MARKING REMOVAL | SQ M | 104 | 104 | | |
| X0322461 | DRUM AND CHEVRON SIGN | EACH | 104 | 104 | | |
| X0325445 | RIGHT-OF-WAY AND PROPERTY CORNERS | EACH | 27 | 27 | | |
| Z0064505 | SECTION CORNER MARKERS | EACH | 3 | 3 | | |
| MX033793 | 100mm PVC C-900 WATERLINE | METER | 87 | 87 | | |
| MX033794 | 100mm CERTA-LOK IN 400mm STEEL CASING (OPEN CUT) | METER | 70 | 70 | | |
| 0 Z0076000 | TRAINEES | HR | 3,000 | 3,000 | | |

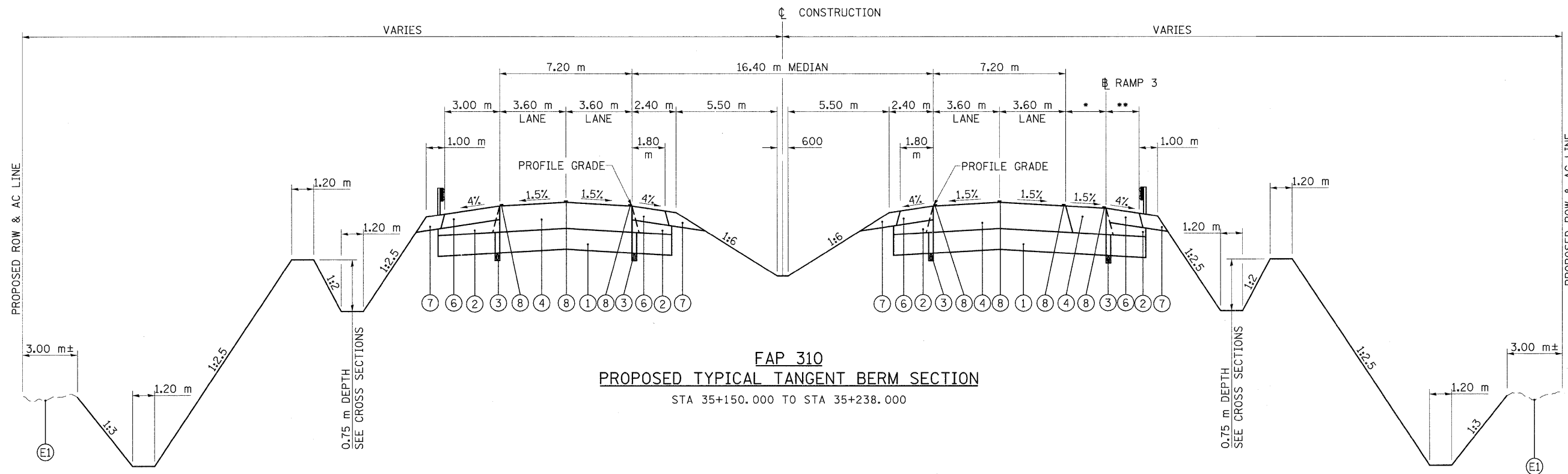
* SPECIALTY ITEM @ Y080

| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES
 FAP 310 (IL 255)
 SECTION 60-15
 MADISON COUNTY
 DRAWN BY EBB
 CHECKED BY
 DATE

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NOTE: THE BERM BEGINS AT STATION 35+200 ON THE LEFT SIDE.

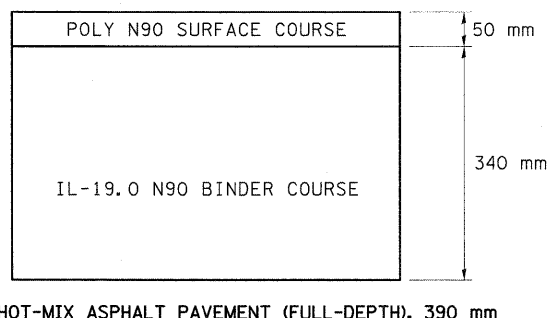
NOTE: RAMP 3 ENDS AT STATION 35+244.733
 * VARIES 2.195 m TO 0.300 m FROM STATION 35+150.000 TO STATION 35+244.733
 ** 1.800 m FROM STATION 35+150.000 TO STATION 35+199.750
 VARIES 1.800 m TO 2.700 m FROM STATION 35+199.750 TO STATION 35+244.733

PROPOSED LEGEND

- (E1) EXISTING GROUND
- (1) LIME STABILIZED SOIL MIXTURE, 300 mm
- (2) SUBBASE GRANULAR MATERIAL, TYPE C, VARIABLE DEPTH (100 mm MINIMUM)
- (3) SUB-SURFACE DRAIN, STD 601001
- (4) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 390 mm (SEE DETAIL)
- (5) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 400 mm (SEE DETAIL)
- (6) HOT-MIX ASPHALT SHOULDERS, 200 mm (RUMBLE STRIPS CONSTRUCTED AS PER STANDARD 642001)
- (7) AGGREGATE SHOULDER, TYPE B, 200 mm
- (8) PAVEMENT MARKING (SEE PAVEMENT MARKING PLAN SHEETS)

FAP 310 (IL 255)

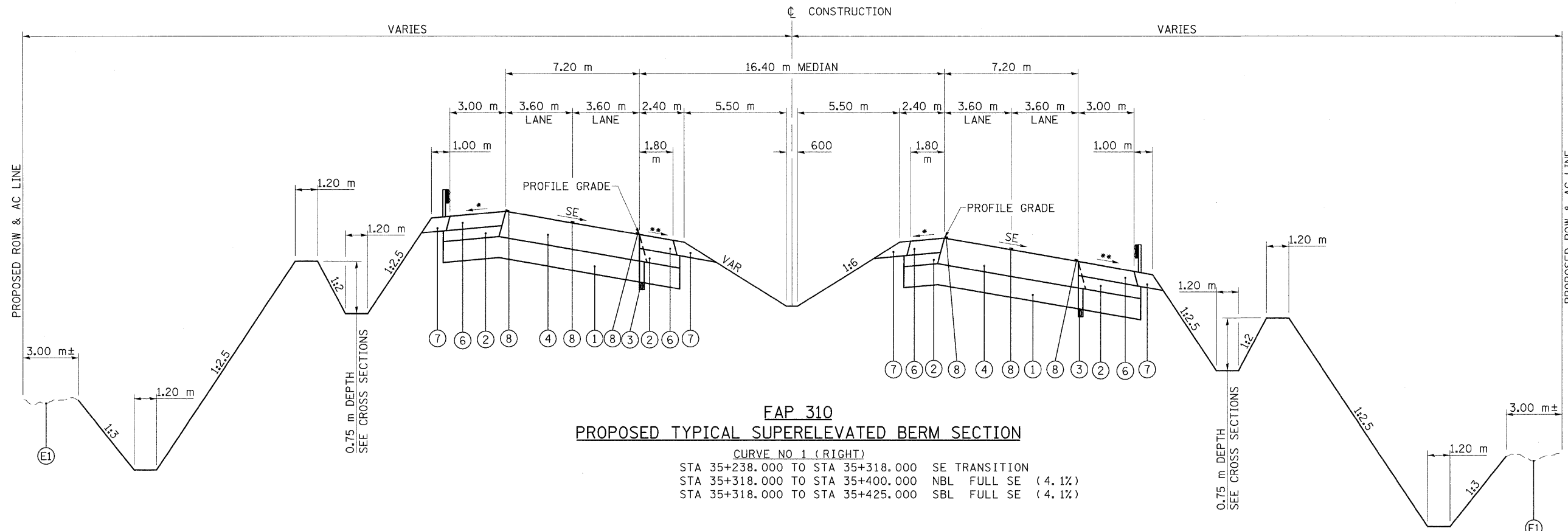
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|---|---|
| STRUCTURAL DESIGN TRAFFIC: | YEAR 2014 |
| PV = 16836 | SU = 549 MU = 915 |
| ROAD/STREET CLASSIFICATION: | CLASS I |
| PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE: | |
| P = 32 | S = 45 M = 45 |
| TRAFFIC FACTOR: | ACTUAL TF = 4.64 AC TYPE = 20 |
| | MINIMUM TF = 7.11 |
| AC GRADE: | BINDER = PG 64-22 SURFACE = SBS 76-22 |
| SUBGRADE SUPPORT RATING: | SSR = POOR (STA 35+150.000 TO STA 40+665.645) |



| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
FAP 310 (IL 255)
SECTION 60-15
MADISON COUNTY
 DRAWN BY **EBB**
 CHECKED BY
 DATE

ALL DIMENSIONS ARE IN mm
 UNLESS NOTED OTHERWISE



FAP 310
PROPOSED TYPICAL SUPERELEVATED BERM SECTION

CURVE NO. 1 (RIGHT)
 STA 35+238.000 TO STA 35+318.000 SE TRANSITION
 STA 35+318.000 TO STA 35+400.000 NBL FULL SE (4.1%)
 STA 35+318.000 TO STA 35+425.000 SBL FULL SE (4.1%)

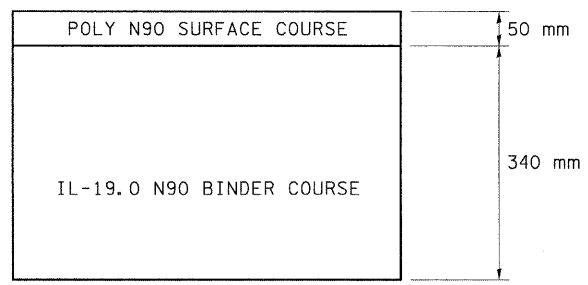
* 4% AND VARIES, 8% MAXIMUM BREAKOVER
 ** 4% OR SE, WHICHEVER IS GREATER

PROPOSED LEGEND

- (E1) EXISTING GROUND
- (1) LIME STABILIZED SOIL MIXTURE, 300 mm
- (2) SUBBASE GRANULAR MATERIAL, TYPE C, VARIABLE DEPTH (100 mm MINIMUM)
- (3) SUB-SURFACE DRAIN, STD 601001
- (4) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 390 mm (SEE DETAIL)
- (5) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 400 mm (SEE DETAIL)
- (6) HOT-MIX ASPHALT SHOULDERS, 200 mm (RUMBLE STRIPS CONSTRUCTED AS PER STANDARD 642001)
- (7) AGGREGATE SHOULDER, TYPE B, 200 mm
- (8) PAVEMENT MARKING (SEE PAVEMENT MARKING PLAN SHEETS)

FAP 310 (IL 255)

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|---|---|
| STRUCTURAL DESIGN TRAFFIC: | YEAR 2014 |
| PV = 16836 | SU = 549 MU = 915 |
| ROAD/STREET CLASSIFICATION: | CLASS I |
| PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE: | |
| P = 32 | S = 45 M = 45 |
| TRAFFIC FACTOR: | ACTUAL TF = 4.64 AC TYPE = 20 |
| | MINIMUM TF = 7.11 |
| AC GRADE: | BINDER = PG 64-22 SURFACE = SBS 76-22 |
| SUBGRADE SUPPORT RATING: | SSR = POOR (STA 35+150.000 TO STA 40+665.645) |



HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 390 mm

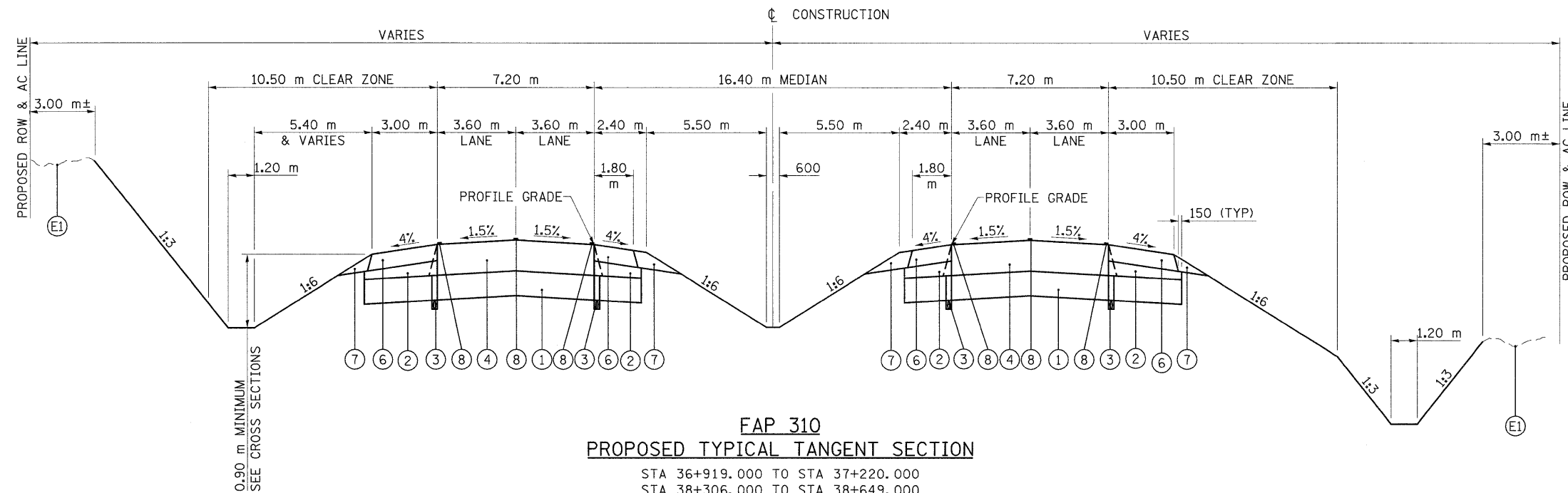
ALL DIMENSIONS ARE IN mm
UNLESS NOTED OTHERWISE

| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL SECTIONS
 FAP 310 (IL 255)
 SECTION 60-15
 MADISON COUNTY

DRAWN BY **EBB**
 CHECKED BY
 DATE

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|---------------------|---------------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 10 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |
| CONTRACT NO. 76323 | | | | |



**FAP 310
PROPOSED TYPICAL TANGENT SECTION**

- STA 36+919.000 TO STA 37+220.000
- STA 38+306.000 TO STA 38+649.000
- STA 39+622.000 TO STA 39+876.770 SBL
- STA 39+767.068 TO STA 39+876.770 NBL
- STA 39+876.770 TO STA 39+885.770 BRIDGE APPROACH PAVEMENT
- STA 39+885.770 TO STA 39+943.450 BRIDGE OMISSION
- STA 39+943.450 TO STA 39+952.450 BRIDGE APPROACH PAVEMENT
- STA 39+952.450 TO STA 40+026.000
- STA 40+558.000 TO STA 40+635.165 SBL
- STA 40+558.000 TO STA 40+665.645 NBL

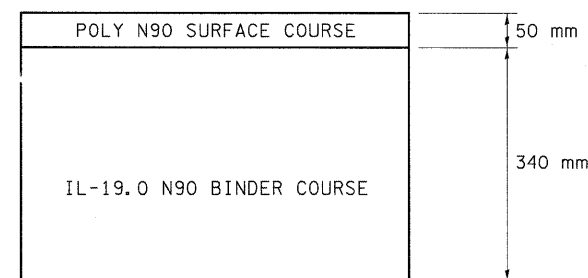
NOTE: WHERE GUARDRAIL IS TO BE INSTALLED, INCREASE SHOULDER WIDTH BY 1.0 m

PROPOSED LEGEND

- (E1) EXISTING GROUND
- (1) LIME STABILIZED SOIL MIXTURE, 300 mm
- (2) SUBBASE GRANULAR MATERIAL, TYPE C, VARIABLE DEPTH (100 mm MINIMUM)
- (3) SUB-SURFACE DRAIN, STD 601001
- (4) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 390 mm (SEE DETAIL)
- (5) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 400 mm (SEE DETAIL)
- (6) HOT-MIX ASPHALT SHOULDERS, 200 mm (RUMBLE STRIPS CONSTRUCTED AS PER STANDARD 642001)
- (7) AGGREGATE SHOULDER, TYPE B, 200 mm
- (8) PAVEMENT MARKING (SEE PAVEMENT MARKING PLAN SHEETS)

FAP 310 (IL 255)

| | |
|---|---|
| STRUCTURAL DESIGN TRAFFIC: | YEAR 2014 |
| PV = 16836 | SU = 549 MU = 915 |
| ROAD/STREET CLASSIFICATION: | CLASS I |
| PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE: | |
| P = 32 | S = 45 M = 45 |
| TRAFFIC FACTOR: | ACTUAL TF = 4.64 AC TYPE = 20 |
| | MINIMUM TF = 7.11 |
| AC GRADE: | BINDER = PG 64-22 SURFACE = SBS-76-22 |
| SUBGRADE SUPPORT RATING: | SSR = POOR (STA 35+150.000 TO STA 40+665.645) |



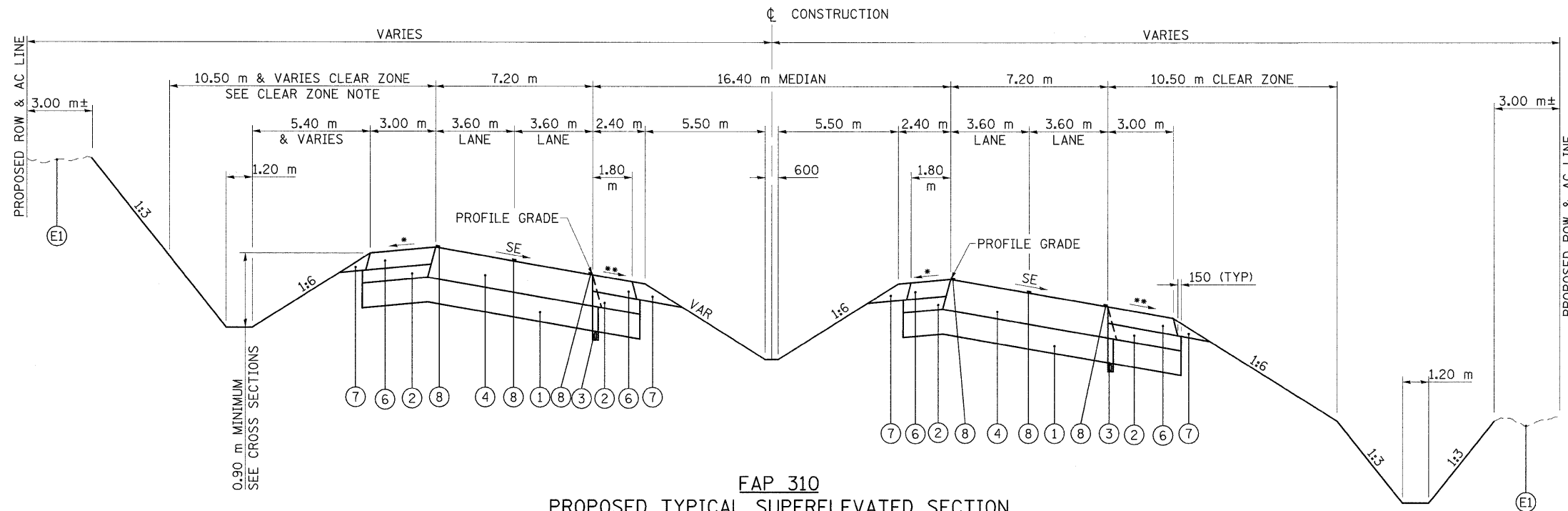
HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 390 mm

ALL DIMENSIONS ARE IN mm UNLESS NOTED OTHERWISE

| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
FAP 310 (IL 255)
SECTION 60-15
MADISON COUNTY
DRAWN BY **EBB**
CHECKED BY
DATE

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|---------------------|---------------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 11 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |
| CONTRACT NO. 76323 | | | | |



**FAP 310
PROPOSED TYPICAL SUPERELEVATED SECTION**

CURVE NO. 1 (RIGHT)

STA 35+400.000 TO STA 36+839.000 NBL FULL SE (4.1%)
 STA 35+425.000 TO STA 36+839.000 SBL FULL SE (4.1%)
 STA 36+839.000 TO STA 36+919.000 SE TRANSITION

CURVE NO. 2 (LEFT)

STA 37+220.000 TO STA 37+300.000 SE TRANSITION
 STA 37+300.000 TO STA 38+226.000 FULL SE (5.5%)
 STA 38+226.000 TO STA 38+306.000 SE TRANSITION

CURVE NO. 3 (RIGHT)

STA 38+649.000 TO STA 38+729.000 SE TRANSITION
 STA 38+729.000 TO STA 38+789.316 SBL FULL SE (5.5%)
 STA 38+729.000 TO STA 38+790.610 NBL FULL SE (5.5%)
 STA 38+789.316 TO STA 38+798.239 SBL BRIDGE APPROACH PAVEMENT
 STA 38+790.610 TO STA 38+799.695 NBL BRIDGE APPROACH PAVEMENT
 STA 38+799.695 TO STA 38+865.865 NBL BRIDGE OMISSION
 STA 38+798.239 TO STA 38+863.008 SBL BRIDGE OMISSION
 STA 38+865.865 TO STA 38+874.979 NBL BRIDGE APPROACH PAVMENT
 STA 38+874.979 TO STA 39+124.810 NBL FULL SE (5.5%)
 STA 38+863.008 TO STA 38+871.897 SBL BRIDGE APPROACH PAVEMENT
 STA 38+871.897 TO STA 38+943.322 SBL FULL SE (5.5%)
 STA 39+124.810 TO STA 39+133.793 NBL BRIDGE APPROACH PAVEMENT
 STA 39+133.793 TO STA 39+191.756 NBL BRIDGE OMISSION
 STA 39+191.756 TO STA 39+201.033 NBL BRIDGE APPROACH PAVEMENT
 STA 39+201.033 TO STA 39+370.000 NBL FULL SE (5.5%)
 STA 39+410.000 TO STA 39+542.000 SBL FULL SE (5.5%)
 STA 39+542.000 TO STA 39+622.000 SBL SE TRANSITION

CURVE NO. 4 (LEFT)

STA 40+026.000 TO STA 40+106.000 SE TRANSITION
 STA 40+106.000 TO STA 40+478.000 FULL SE (4.8%)
 STA 40+478.000 TO STA 40+558.000 SE TRANSITION

NOTE: WHERE GUARDRAIL IS TO BE INSTALLED, INCREASE SHOULDER WIDTH BY 1.0 m

* 4% AND VARIES, 8% MAXIMUM BREAKOVER

** 4% OR SE, WHICHEVER IS GREATER

CLEAR ZONE ADJUSTED FOR CURVE

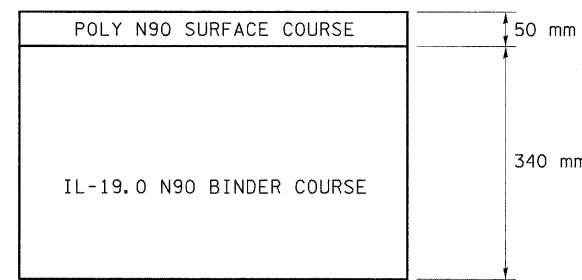
SEE SHEETS 137-142 FOR CLEAR ZONE OFFSETS AND TRANSITIONS

PROPOSED LEGEND

- (E1) EXISTING GROUND
- (1) LIME STABILIZED SOIL MIXTURE, 300 mm
- (2) SUBBASE GRANULAR MATERIAL, TYPE C, VARIABLE DEPTH (100 mm MINIMUM)
- (3) SUB-SURFACE DRAIN, STD 601001
- (4) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 390 mm (SEE DETAIL)
- (5) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 400 mm (SEE DETAIL)
- (6) HOT-MIX ASPHALT SHOULDERS, 200 mm (RUMBLE STRIPS CONSTRUCTED AS PER STANDARD 642001)
- (7) AGGREGATE SHOULDER, TYPE B, 200 mm
- (8) PAVEMENT MARKING (SEE PAVEMENT MARKING PLAN SHEETS)

FAP 310 (IL 255)

| | |
|---|---|
| STRUCTURAL DESIGN TRAFFIC: | YEAR 2014 |
| PV = 16836 | SU = 549 MU = 915 |
| ROAD/STREET CLASSIFICATION: | CLASS I |
| PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE: | |
| P = 32 | S = 45 M = 45 |
| TRAFFIC FACTOR: | ACTUAL TF = 4.64 AC TYPE = 20 |
| | MINIMUM TF = 7.11 |
| AC GRADE: | BINDER = PG 64-22 SURFACE = SBS 76-22 |
| SUBGRADE SUPPORT RATING: | SSR = POOR (STA 35+150.000 TO STA 40+665.645) |

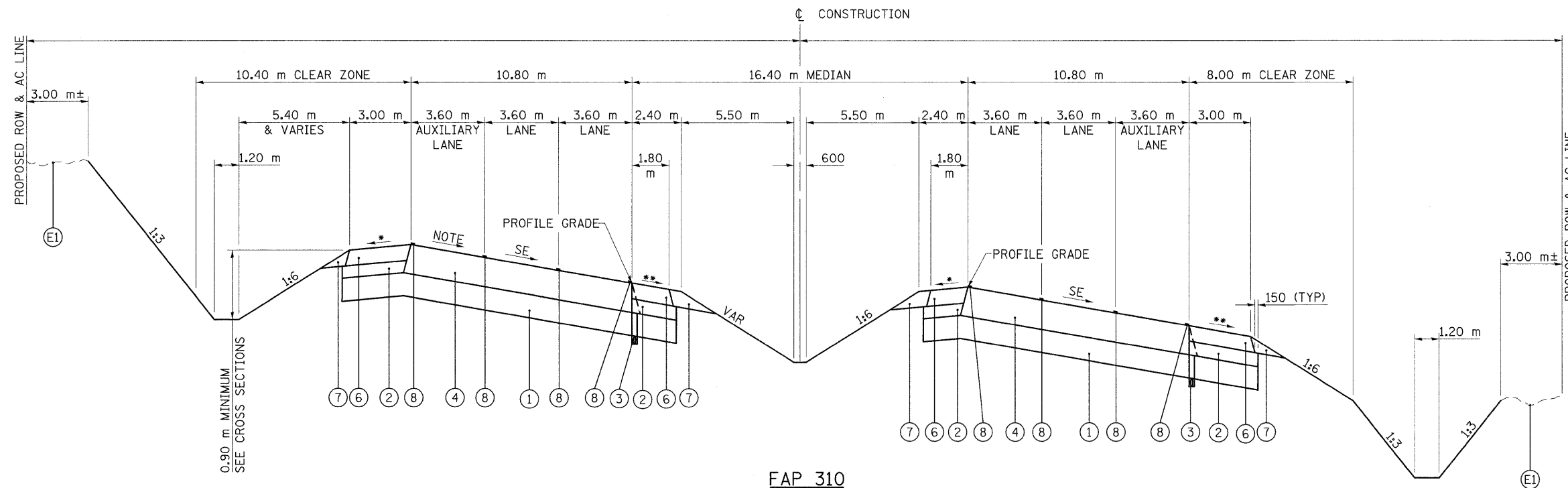


HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 390 mm

ALL DIMENSIONS ARE IN mm UNLESS NOTED OTHERWISE

| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL SECTIONS
 FAP 310 (IL 255)
 SECTION 60-15
 MADISON COUNTY
 DRAWN BY **EBB**
 CHECKED BY
 DATE



FAP 310
PROPOSED TYPICAL SUPERELEVATED SECTION WITH AUXILIARY LANE

AUXILIARY LANE NOTE:

STA 39+108.322 TO STA 39+195.000 SBL 4.0%
 STA 39+195.000 TO STA 39+260.000 SBL SE TRANSITION
 STA 39+260.000 TO STA 39+410.000 SBL FULL SE (5.5%)

CURVE NO. 3 (RIGHT)

STA 39+108.322 TO STA 39+117.295 SBL FULL SE (5.5%)
 STA 39+117.295 TO STA 39+126.063 SBL BRIDGE APPROACH PAVEMENT
 STA 39+126.063 TO STA 39+187.431 SBL BRIDGE OMISSION
 STA 39+187.431 TO STA 39+196.493 SBL BRIDGE APPROACH PAVEMENT
 STA 39+196.493 TO STA 39+410.000 SBL FULL SE (5.5%)
 STA 39+370.000 TO STA 39+542.000 NBL FULL SE (5.5%)
 STA 39+542.000 TO STA 39+602.068 NBL SE TRANSITION

* 4% AND VARIES, 8% MAXIMUM BREAKOVER

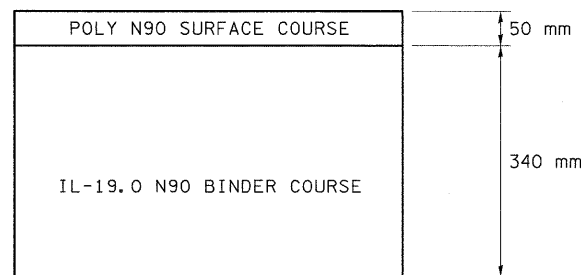
** 4% OR SE, WHICHEVER IS GREATER

PROPOSED LEGEND

- (E1) EXISTING GROUND
- (1) LIME STABILIZED SOIL MIXTURE, 300 mm
- (2) SUBBASE GRANULAR MATERIAL, TYPE C, VARIABLE DEPTH (100 mm MINIMUM)
- (3) SUB-SURFACE DRAIN, STD 601001
- (4) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 390 mm (SEE DETAIL)
- (5) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 400 mm (SEE DETAIL)
- (6) HOT-MIX ASPHALT SHOULDERS, 200 mm (RUMBLE STRIPS CONSTRUCTED AS PER STANDARD 642001)
- (7) AGGREGATE SHOULDER, TYPE B, 200 mm
- (8) PAVEMENT MARKING (SEE PAVEMENT MARKING PLAN SHEETS)

FAP 310 (IL 255)

| | | |
|---|---|---------------------|
| STRUCTURAL DESIGN TRAFFIC: | YEAR 2014 | |
| PV = 16836 | SU = 549 | MU = 915 |
| ROAD/STREET CLASSIFICATION: | CLASS I | |
| PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE: | | |
| P = 32 | S = 45 | M = 45 |
| TRAFFIC FACTOR: | ACTUAL TF = 4.64 | AC TYPE = 20 |
| | MINIMUM TF = 7.11 | |
| AC GRADE: | BINDER = PG 64-22 | SURFACE = SBS 76-22 |
| SUBGRADE SUPPORT RATING: | SSR = POOR (STA 35+150.000 TO STA 40+665.645) | |



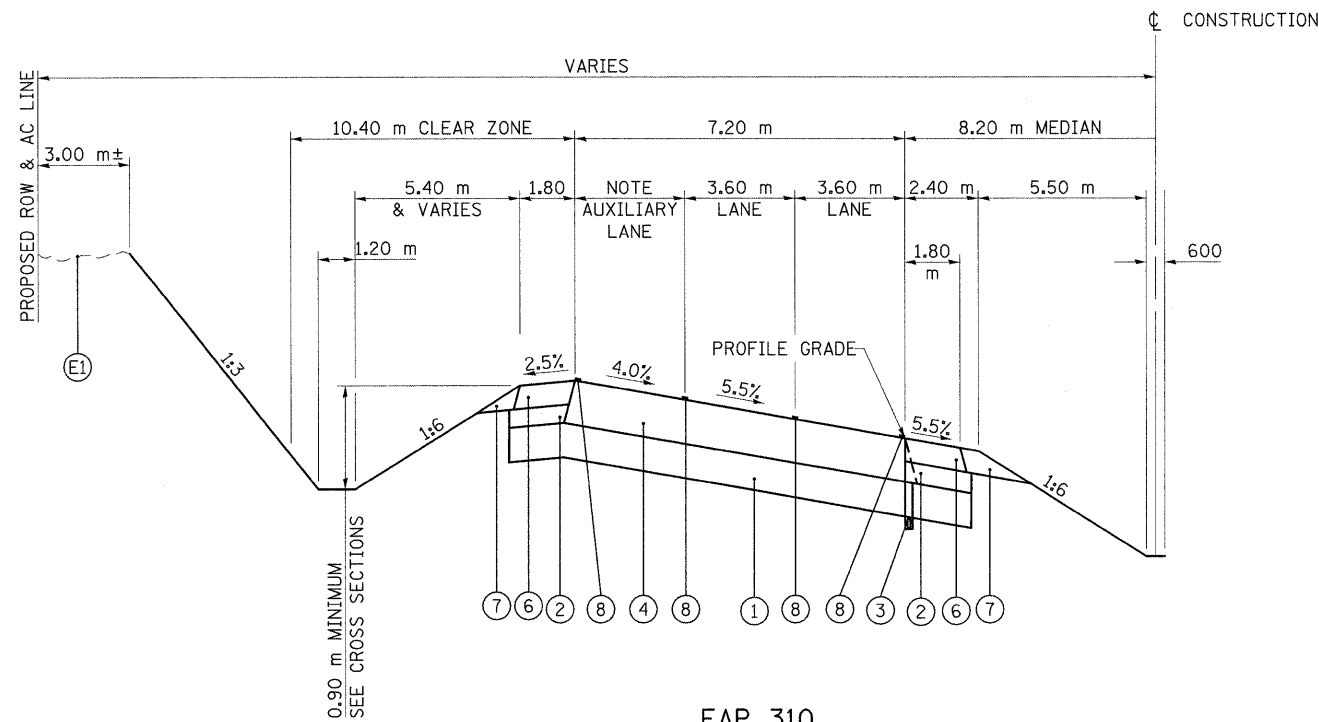
HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 390 mm

ALL DIMENSIONS ARE IN mm
UNLESS NOTED OTHERWISE

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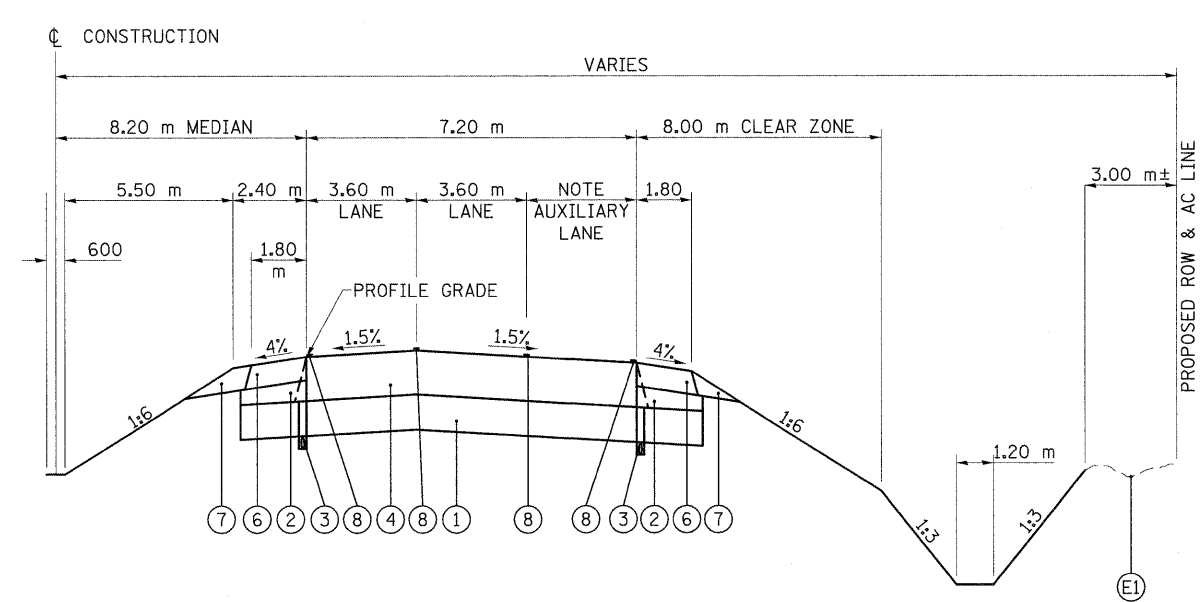
ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
FAP 310 (IL 255)
SECTION 60-15
MADISON COUNTY
 DRAWN BY **EBB**
 CHECKED BY
 DATE

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|---------------------|---------------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 13 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |
| CONTRACT NO. 76323 | | | | |



FAP 310
PROPOSED TYPICAL SUPERELEVATED SECTION WITH AUXILIARY LANE
 CURVE NO 3 (RIGHT)
 STA 38+943.322 TO STA 39+108.322 SBL FULL SE (5.5%)

NOTE: THE AUXILIARY LANE TRANSITIONS FROM THE 3.60 m LANE TO A 0.3 m STUB.



FAP 310
PROPOSED TYPICAL TANGENT SECTION WITH AUXILIARY LANE
 CURVE NO 3 (RIGHT)
 STA 39+602.068 TO STA 39+622.000 NBL SE TRANSITION
 STA 39+622.000 TO STA 39+767.068 NBL

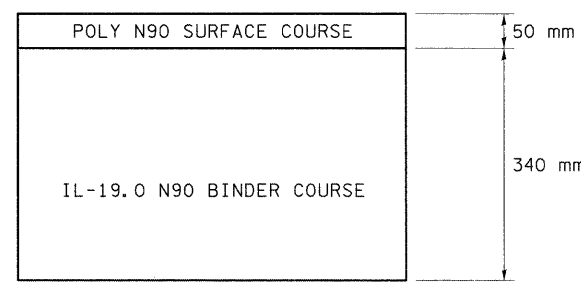
NOTE: THE AUXILIARY LANE TRANSITIONS FROM THE 3.60 m LANE TO A 0.3 m STUB.

PROPOSED LEGEND

- (E1) EXISTING GROUND
- (1) LIME STABILIZED SOIL MIXTURE, 300 mm
- (2) SUBBASE GRANULAR MATERIAL, TYPE C, VARIABLE DEPTH (100 mm MINIMUM)
- (3) SUB-SURFACE DRAIN, STD 601001
- (4) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 390 mm (SEE DETAIL)
- (5) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 400 mm (SEE DETAIL)
- (6) HOT-MIX ASPHALT SHOULDERS, 200 mm (RUMBLE STRIPS CONSTRUCTED AS PER STANDARD 642001)
- (7) AGGREGATE SHOULDER, TYPE B, 200 mm
- (8) PAVEMENT MARKING (SEE PAVEMENT MARKING PLAN SHEETS)

FAP 310 (IL 255)

| | |
|---|---|
| STRUCTURAL DESIGN TRAFFIC: | YEAR 2014 |
| PV = 16836 | SU = 549 MU = 915 |
| ROAD/STREET CLASSIFICATION: | CLASS I |
| PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE: | |
| P = 32 | S = 45 M = 45 |
| TRAFFIC FACTOR: | ACTUAL TF = 4.64 AC TYPE = 20 |
| | MINIMUM TF = 7.11 |
| AC GRADE: | BINDER = PG 64-22 SURFACE = SBS 76-22 |
| SUBGRADE SUPPORT RATING: | SSR = POOR (STA 35+150.000 TO STA 40+665.645) |



HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 390 mm

ALL DIMENSIONS ARE IN mm UNLESS NOTED OTHERWISE

| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL SECTIONS
 FAP 310 (IL 255)
 SECTION 60-15
 MADISON COUNTY
 DRAWN BY EBB
 CHECKED BY
 DATE

RAMPS

STRUCTURAL DESIGN TRAFFIC: YEAR 2014
 PV = 4600 SU = 150 MU = 250

ROAD/STREET CLASSIFICATION: CLASS I

PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:
 P = 100 S = 100 M = 100

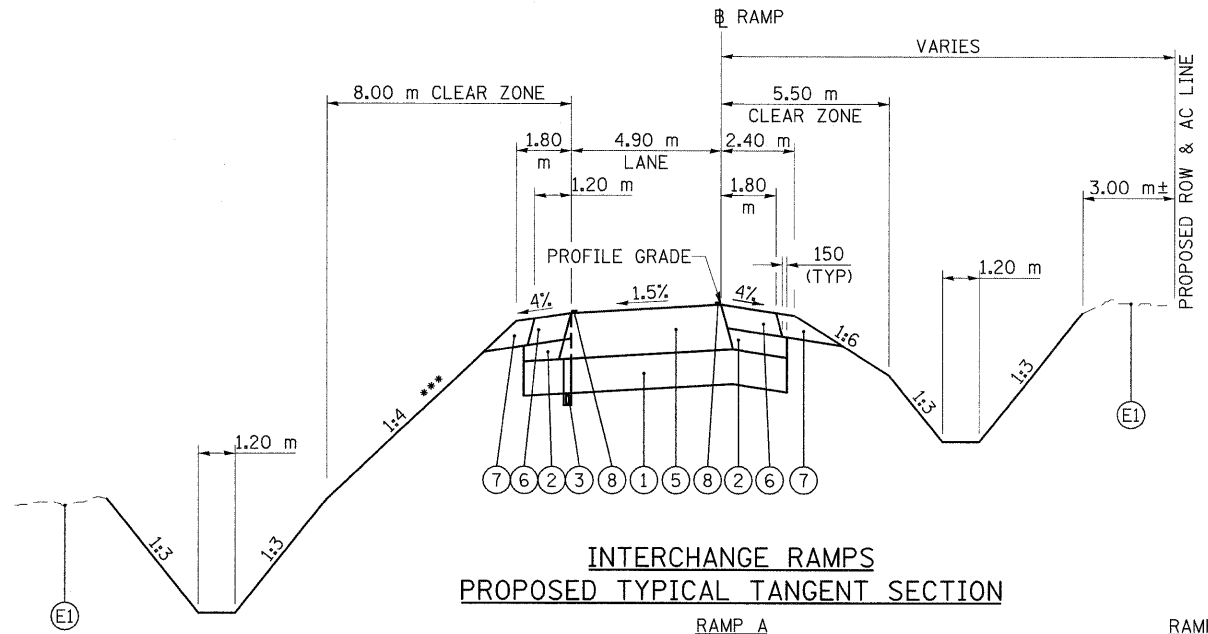
TRAFFIC FACTOR: ACTUAL TF = 2.82 AC TYPE = 20
 MINIMUM TF = 7.90

AC GRADE: BINDER = PG 64-22 SURFACE = SBS 76-22

SUBGRADE SUPPORT RATING:
 SSR = POOR (ALL RAMPS)

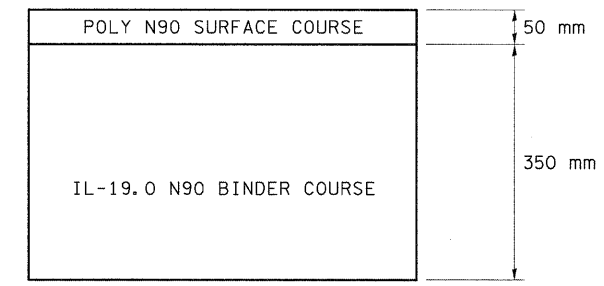
RAMP TERMINAL LOCATIONS

| STATION TO STATION | |
|--------------------|-----------|
| RAMP A | |
| 0+322.355 | 0+703.081 |
| RAMP B | |
| 0+000.000 | 0+200.080 |
| HUMBERT RAMP C | |
| 0+281.133 | 0+508.671 |
| HUMBERT RAMP D | |
| 0+000.000 | 0+089.181 |
| RAMP F | |
| 0+318.817 | 0+533.866 |
| RAMP E | |
| 0+000.000 | 0+130.213 |
| IL 111 RAMP D | |
| 0+360.141 | 0+762.532 |
| IL 111 RAMP C | |
| 0+000.000 | 0+196.244 |



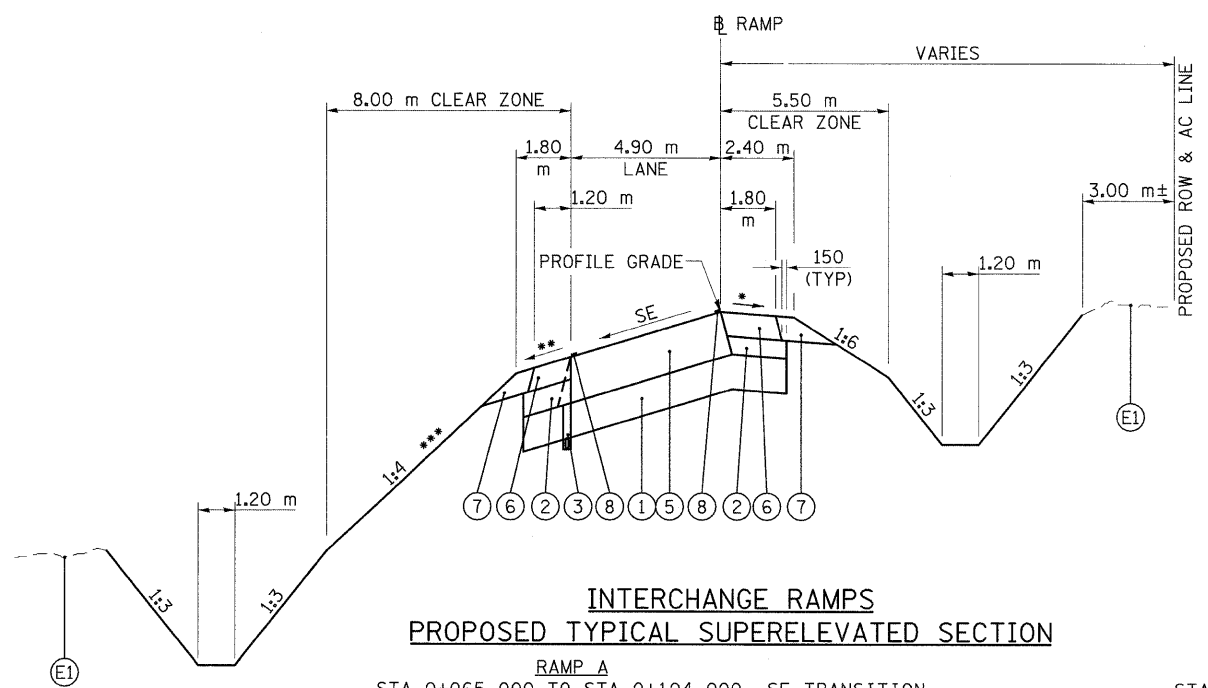
**INTERCHANGE RAMPS
PROPOSED TYPICAL TANGENT SECTION**

| | |
|---|--|
| RAMP A STA 0+048.889 TO STA 0+065.000 | RAMP F STA 0+040.493 TO STA 0+053.000 |
| HUMBERT RAMP C STA 0+043.131 TO STA 0+106.000 | RAMP E STA 0+209.000 TO STA 0+376.895 |
| HUMBERT RAMP D STA 0+089.181 TO STA 0+250.000 STA 0+380.000 TO STA 0+378.864 | IL 111 RAMP D STA 0+040.791 TO STA 0+360.141 |
| | IL 111 RAMP C STA 0+549.000 TO STA 0+579.503 |



HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 400 mm

*** 1:6 SLOPE WITHIN THE MAINLINE CLEARZONE



**INTERCHANGE RAMPS
PROPOSED TYPICAL SUPERELEVATED SECTION**

| | |
|---|--|
| RAMP A STA 0+065.000 TO STA 0+104.000 SE TRANSITION STA 0+104.000 TO STA 0+151.000 FULL SE (6.0%) STA 0+151.000 TO STA 0+203.000 SE TRANSITION STA 0+203.000 TO STA 0+263.000 SE TRANSITION STA 0+263.000 TO STA 0+312.486 FULL SE (6.0%) STA 0+312.486 TO STA 0+322.355 SE TRANSITION | RAMP F STA 0+053.000 TO STA 0+091.000 SE TRANSITION STA 0+091.000 TO STA 0+126.000 FULL SE (5.6%) STA 0+126.000 TO STA 0+174.000 SE TRANSITION STA 0+174.000 TO STA 0+234.000 SE TRANSITION STA 0+234.000 TO STA 0+308.692 FULL SE (6.0%) STA 0+308.692 TO STA 0+318.817 SE TRANSITION |
| RAMP B STA 0+200.080 TO STA 0+264.080 SE TRANSITION STA 0+264.080 TO STA 0+333.000 FULL SE (6.0%) STA 0+333.000 TO STA 0+388.220 SE TRANSITION | RAMP E STA 0+130.213 TO STA 0+151.213 SE TRANSITION STA 0+151.213 TO STA 0+164.000 FULL SE (6.0%) STA 0+164.000 TO STA 0+209.000 SE TRANSITION |
| HUMBERT RAMP C STA 0+106.000 TO STA 0+147.000 SE TRANSITION STA 0+147.000 TO STA 0+272.000 FULL SE (6.0%) STA 0+272.000 TO STA 0+275.682 SE TRANSITION STA 0+275.682 TO STA 0+284.426 BRIDGE APPROACH PAVEMENT STA 0+284.426 TO STA 0+281.133 BRIDGE OMISSION | IL 111 RAMP C STA 0+196.244 TO STA 0+260.384 SE TRANSITION STA 0+260.384 TO STA 0+361.000 FULL SE (6.0%) STA 0+361.000 TO STA 0+421.000 SE TRANSITION STA 0+421.000 TO STA 0+459.000 SE TRANSITION STA 0+459.000 TO STA 0+511.000 FULL SE (3.9%) STA 0+511.000 TO STA 0+549.000 SE TRANSITION |
| HUMBERT RAMP D STA 0+250.000 TO STA 0+288.000 SE TRANSITION STA 0+288.000 TO STA 0+342.000 FULL SE (5.8%) STA 0+342.000 TO STA 0+380.000 SE TRANSITION | |

* 4% AND VARIES, 8% MAXIMUM BREAKOVER

** 4% OR SE, WHICHEVER IS GREATER

*** 1:6 SLOPE WITHIN THE MAINLINE CLEARZONE

- * STA 0+281.133 TO STA 0+341.063 BRIDGE OMISSION
- STA 0+341.063 TO STA 0+350.179 BRIDGE APPROACH PAVEMENT
- ** STA 0+003.148 TO STA 0+012.622 BRIDGE APPROACH PAVEMENT
- STA 0+012.622 TO STA 0+077.907 BRIDGE OMISSION
- STA 0+077.907 TO STA 0+087.382 BRIDGE APPROACH PAVEMENT

PROPOSED LEGEND

- (E1) EXISTING GROUND
- (1) LIME STABILIZED SOIL MIXTURE, 300 mm
- (2) SUBBASE GRANULAR MATERIAL, TYPE C, VARIABLE DEPTH (100 mm MINIMUM)
- (3) SUB-SURFACE DRAIN, STD 601001
- (4) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 390 mm (SEE DETAIL)
- (5) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 400 mm (SEE DETAIL)
- (6) HOT-MIX ASPHALT SHOULDERS, 200 mm (RUMBLE STRIPS CONSTRUCTED AS PER STANDARD 642001)
- (7) AGGREGATE SHOULDER, TYPE B, 200 mm
- (8) PAVEMENT MARKING (SEE PAVEMENT MARKING PLAN SHEETS)

| REVISIONS | |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL SECTIONS
 FAP 310 (IL 255)
 SECTION 60-15
 MADISON COUNTY

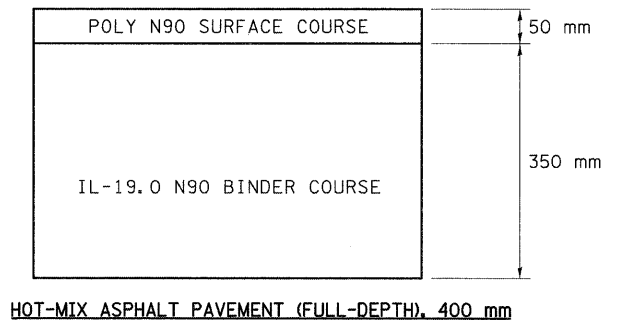
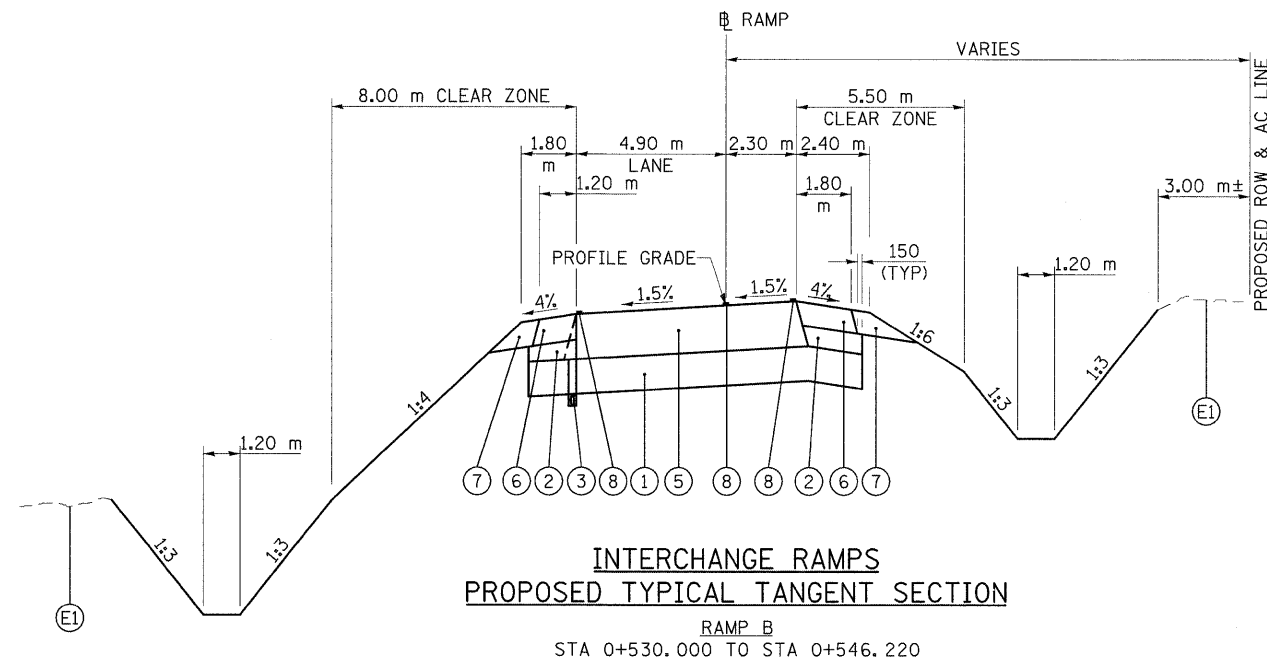
DATE _____ DRAWN BY **E88**
 CHECKED BY _____

ALL DIMENSIONS ARE IN mm
 UNLESS NOTED OTHERWISE

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------------|---------------------------|--------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 15 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |

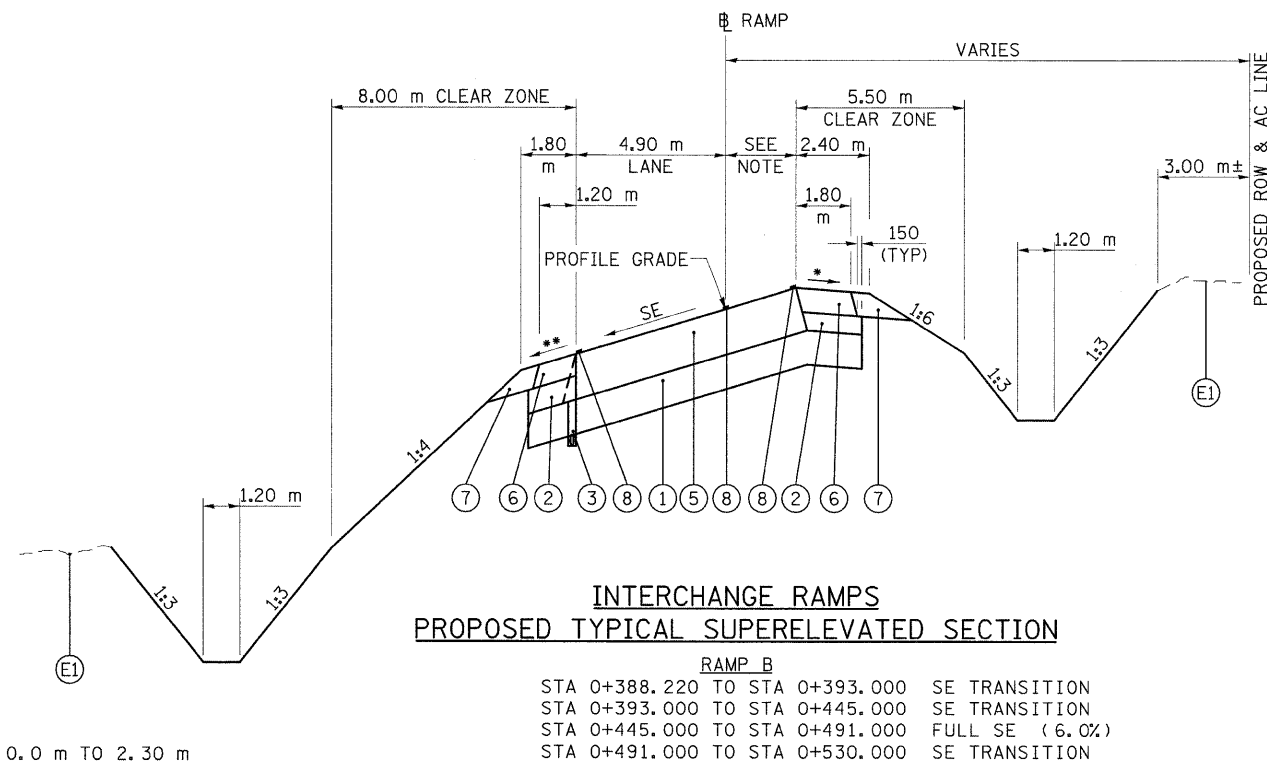
RAMPS

STRUCTURAL DESIGN TRAFFIC: YEAR 2014
 PV = 4600 SU = 150 MU = 250
ROAD/STREET CLASSIFICATION: CLASS I
PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:
 P = 100 S = 100 M = 100
TRAFFIC FACTOR: ACTUAL TF = 2.82 AC TYPE = 20
 MINIMUM TF = 7.90
AC GRADE: BINDER = PG 64-22 SURFACE = SBS 76-22
SUBGRADE SUPPORT RATING:
 SSR = POOR (ALL RAMPS)



PROPOSED LEGEND

- Ⓔ EXISTING GROUND
- ① LIME STABILIZED SOIL MIXTURE, 300 mm
- ② SUBBASE GRANULAR MATERIAL, TYPE C, VARIABLE DEPTH (100 mm MINIMUM)
- ③ SUB-SURFACE DRAIN, STD 601001
- ④ HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 390 mm (SEE DETAIL)
- ⑤ HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 400 mm (SEE DETAIL)
- ⑥ HOT-MIX ASPHALT SHOULDERS, 200 mm (RUMBLE STRIPS CONSTRUCTED AS PER STANDARD 642001)
- ⑦ AGGREGATE SHOULDER, TYPE B, 200 mm
- ⑧ PAVEMENT MARKING (SEE PAVEMENT MARKING PLAN SHEETS)



NOTE: THE WIDTH VARIES FROM 0.0 m TO 2.30 m FROM STA 0+388.220 TO STA 0+446.220
 THE WIDTH REMAINS A CONSTANT 2.30 m FROM STA 0+446.220 TO STA 0+582.158

* 4% AND VARIES, 8% MAXIMUM BREAKOVER
 ** 4% OR SE, WHICHEVER IS GREATER

| REVISIONS | |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL SECTIONS
 FAP 310 (IL 255)
 SECTION 60-15
 MADISON COUNTY
 DRAWN BY **EBB**
 CHECKED BY
 DATE

ALL DIMENSIONS ARE IN mm
 UNLESS NOTED OTHERWISE

SCHEDULES OF QUANTITIES

| | | | | |
|---------------------|---------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 16 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |

GUARDRAIL SCHEDULE

| LOCATION | | | | | SIDE | TRAFFIC BARRIER TERM. | | | | SPBGR |
|------------|----------------|----|------------|----------------|------------|--------------------------|--------|--------|--------|---------|
| STATIONING | | | | | | TYPE 1 SPECIAL (TANGENT) | TYPE 2 | TYPE 5 | TYPE 6 | TYPE A |
| | | | | | | EACH | | | | FOOT |
| 1+503.865 | RAMP 3 | to | 1+519.105 | RAMP 3 | RT OUTSIDE | 1 | | | | |
| 1+519.105 | RAMP 3 | to | 35+455.585 | FAP 310 | | | | | | 400.05 |
| 35+455.585 | FAP 310 | to | 35+460.000 | FAP 310 | | | 1 | | | |
| 35+146.750 | FAP 310 | to | 35+151.100 | FAP 310 | LT OUTSIDE | | 1 | | | |
| 35+151.100 | FAP 310 | to | 35+509.980 | FAP 310 | | | | | | 361.95 |
| 35+509.980 | FAP 310 | to | 35+525.000 | FAP 310 | | 1 | | | | |
| 38+625.301 | FAP 310 | to | 38+640.541 | FAP 310 | RT OUTSIDE | 1 | | | | |
| 38+640.541 | FAP 310 | to | 38+791.068 | FAP 310 | | | | | | 148.59 |
| 38+791.068 | FAP 310 | to | 38+801.418 | FAP 310 | | | | 1 | | |
| 38+656.076 | FAP 310 | to | 38+671.316 | FAP 310 | RT MEDIAN | 1 | | | | |
| 38+671.316 | FAP 310 | to | 38+790.064 | FAP 310 | | | | | | 118.11 |
| 38+790.064 | FAP 310 | to | 38+800.239 | FAP 310 | | | | 1 | | |
| 38+744.478 | FAP 310 | to | 38+748.729 | FAP 310 | LT OUTSIDE | | 1 | | | |
| 38+748.729 | FAP 310 | to | 38+793.358 | FAP 310 | | | | | | 45.72 |
| 38+793.358 | FAP 310 | to | 38+797.298 | FAP 310 | | | | 1 | | |
| 38+860.456 | FAP 310 | to | 38+870.318 | FAP 310 | LT OUTSIDE | | | | 1 | |
| 38+870.318 | FAP 310 | to | 39+015.453 | FAP 310 | | | | | | 148.59 |
| 39+015.453 | FAP 310 | to | 39+030.327 | FAP 310 | | 1 | | | | |
| 38+862.600 | FAP 310 | to | 38+872.621 | FAP 310 | LT MEDIAN | | | | 1 | |
| 38+872.621 | FAP 310 | to | 38+985.992 | FAP 310 | | | | | | 114.3 |
| 38+985.992 | FAP 310 | to | 39+001.108 | FAP 310 | | 1 | | | | |
| 38+867.714 | FAP 310 | to | 38+871.850 | FAP 310 | RT OUTSIDE | | | 1 | | |
| 38+871.850 | FAP 310 | to | 38+953.793 | FAP 310 | | | | | | 80.01 |
| 38+953.793 | FAP 310 | to | 38+958.250 | FAP 310 | | | 1 | | | |
| 38+984.161 | FAP 310 | to | 38+999.527 | FAP 310 | RT MEDIAN | 1 | | | | |
| 38+999.527 | FAP 310 | to | 39+118.605 | FAP 310 | | | | | | 118.11 |
| 39+118.605 | FAP 310 | to | 39+128.781 | FAP 310 | | | | 1 | | |
| 0+216.965 | HUMBERT RAMP C | to | 0+232.314 | HUMBERT RAMP C | RT OUTSIDE | 1 | | | | |
| 0+232.314 | HUMBERT RAMP C | to | 0+270.684 | HUMBERT RAMP C | | | | | | 38.10 |
| 0+270.684 | HUMBERT RAMP C | to | 0+280.817 | HUMBERT RAMP C | | | | 1 | | |
| 0+130.099 | HUMBERT RAMP D | to | 0+125.748 | HUMBERT RAMP D | LT OUTSIDE | | 1 | | | |
| 0+125.748 | HUMBERT RAMP D | to | 0+087.657 | HUMBERT RAMP D | | | | | | 38.10 |
| 0+087.657 | HUMBERT RAMP D | to | 0+083.619 | HUMBERT RAMP D | | | | 1 | | |
| 39+185.373 | FAP 310 | to | 39+195.189 | FAP 310 | LT OUTSIDE | | | | 1 | |
| 39+195.189 | FAP 310 | to | 39+313.765 | FAP 310 | | | | | | 121.92 |
| 39+313.765 | FAP 310 | to | 39+328.586 | FAP 310 | | 1 | | | | |
| 39+191.978 | FAP 310 | to | 39+201.998 | FAP 310 | LT MEDIAN | | | | 1 | |
| 39+201.998 | FAP 310 | to | 39+315.367 | FAP 310 | | | | | | 114.30 |
| 39+315.367 | FAP 310 | to | 39+330.483 | FAP 310 | | 1 | | | | |
| 0+347.710 | HUMBERT RAMP C | to | 0+351.772 | HUMBERT RAMP C | RT OUTSIDE | | | 1 | | |
| 0+351.772 | HUMBERT RAMP C | to | 0+412.936 | HUMBERT RAMP C | | | | | | 60.96 |
| 0+412.936 | HUMBERT RAMP C | to | 0+417.297 | HUMBERT RAMP C | | | 1 | | | |
| 39+722.379 | FAP 310 | to | 39+737.619 | FAP 310 | RT OUTSIDE | 1 | | | | |
| 39+737.619 | FAP 310 | to | 39+874.779 | FAP 310 | | | | | | 137.16 |
| 39+874.779 | FAP 310 | to | 39+884.882 | FAP 310 | | | | 1 | | |
| 39+749.049 | FAP 310 | to | 39+764.289 | FAP 310 | RT MEDIAN | 1 | | | | |
| 39+764.289 | FAP 310 | to | 39+874.779 | FAP 310 | | | | | | 110.49 |
| 39+874.779 | FAP 310 | to | 39+884.882 | FAP 310 | | | | 1 | | |
| 39+830.011 | FAP 310 | to | 39+834.362 | FAP 310 | LT OUTSIDE | | 1 | | | |
| 39+834.362 | FAP 310 | to | 39+880.082 | FAP 310 | | | | | | 45.72 |
| 39+880.082 | FAP 310 | to | 39+884.120 | FAP 310 | | | | 1 | | |
| 39+944.338 | FAP 310 | to | 39+954.441 | FAP 310 | LT OUTSIDE | | | | 1 | |
| 39+954.441 | FAP 310 | to | 40+114.993 | FAP 310 | | | | | | 160.02 |
| 40+114.993 | FAP 310 | to | 40+130.521 | FAP 310 | | 1 | | | | |
| 39+944.338 | FAP 310 | to | 39+954.441 | FAP 310 | LT MEDIAN | | | | 1 | |
| 39+954.441 | FAP 310 | to | 40+064.931 | FAP 310 | | | | | | 110.49 |
| 40+064.931 | FAP 310 | to | 40+080.171 | FAP 310 | | 1 | | | | |
| 39+945.100 | FAP 310 | to | 39+949.138 | FAP 310 | RT OUTSIDE | | | 1 | | |
| 39+949.138 | FAP 310 | to | 39+991.048 | FAP 310 | | | | | | 41.91 |
| 39+991.048 | FAP 310 | to | 39+995.399 | FAP 310 | | | 1 | | | |
| TOTALS | | | | | | 14 | 8 | 6 | 12 | 2514.60 |

BRIDGE APPROACH PAVEMENT DRAINS

| STATION | SIDE | INLET BOX STANDARD 609006 | | CONCRETE THRUST BLOCKS | PIPE DRAINS 300MM | METAL END SECTIONS 300MM |
|----------------|------|---------------------------|--------|------------------------|-------------------|--------------------------|
| | | TYPE C | TYPE D | | | |
| | | EACH | | | | |
| FAP 310 | | | | | | |
| 38+794.4 | LT | 1 | | | 3.3 | 1 |
| 38+796.5 | RT | | 1 | 1 | 20.6 | 1 |
| 39+195.9 | LT | 1 | | | 3.7 | 1 |
| 39+879.0 | RT | 1 | | | 3.3 | 1 |
| 39+880.0 | LT | 1 | | | 3.1 | 1 |
| 39+881.0 | RT | | 1 | 1 | 18.7 | 1 |
| 39+881.5 | LT | | 1 | 1 | 18.7 | 1 |
| 39+947.8 | RT | | 1 | 1 | 20.4 | 1 |
| 39+948.0 | RT | 1 | | | 3.3 | 1 |
| 39+948.2 | LT | 1 | | | 16.5 | 1 |
| 39+948.2 | LT | | 1 | 1 | 3.4 | 1 |
| HUMBERT RAMP C | | | | | | |
| 0+349.1 | RT | | 1 | 1 | 35.7 | 1 |
| TOTALS | | 6 | 6 | 6 | 150.7 | 12 |
| USE | | 6 | 6 | 6 | 151.0 | 12 |

IMPACT ATTENUATORS

| STATION | SIDE | EACH |
|----------|------|------|
| FAP 310 | | |
| 36+324.7 | CL | 1 |
| 36+347.3 | CL | 1 |
| TOTAL | | 2 |

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION SCHEDULES OF QUANTITIES FAP 310 (IL 255) SECTION 60-15 MADISON COUNTY DRAWN BY EBB CHECKED BY |
|-----------|------|---|
| NAME | DATE | |
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| | | |
| | | |
| DATE | | |

SCHEDULES OF QUANTITIES

| | | | | |
|---------------------|---------------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 17A |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |
| CONTRACT NO. 76323 | | | | |

FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS CONT.

| Description/Location | Station | Offset (Meters) | Right of Way Markers @ 1' offset | Property corners @ Intersect w/ ROW or PE | Drainage Markers @ 1' Offset |
|--------------------------|------------|-----------------|----------------------------------|---|------------------------------|
| FAP 310 | 35+239.713 | 91.440 RT | | | 1 |
| FAP 310 | 35+335.000 | 91.440 RT | | | 1 |
| FAP 310 | 35+640.453 | 45.720 RT | 1 | | |
| FAP 310 | 35+690.450 | 45.720 LT | 1 | | |
| FAP 310 | 35+780.906 | 45.720 RT | 1 | | |
| FAP 310 | 36+501.065 | 44.196 RT | | 1 | |
| FAP 310 | 36+998.000 | 70.104 LT | 1 | | |
| FAP 310 | 37+137.255 | 70.104 LT | 1 | | |
| FAP 310 | 37+620.000 | 64.008 LT | 1 | | |
| FAP 310 | 37+740.000 | 64.008 LT | 1 | | |
| FAP 310 | 39+114.649 | 94.083 LT | 1 | | |
| FAP 310 | 39+172.593 | 8.181 LT | 1 | | |
| FAP 310 | 39+177.209 | 87.130 RT | 1 | | |
| FAP 310 | 39+180.079 | 11.147 LT | 1 | | |
| FAP 310 | 39+208.664 | 75.498 RT | 1 | | |
| FAP 310 | 39+232.549 | 121.057 RT | 1 | | |
| FAP 310 | 39+304.960 | 67.803 RT | 1 | | |
| FAP 310 Ramp B | 0+156.960 | 52.032 RT | | 1 | |
| FAP 310 Ramp B | 0+237.288 | 47.652 RT | | 1 | |
| FAP 310 Ramp B | 0+260.535 | 46.068 RT | | 1 | |
| Humbert Road | 20+129.116 | 78.468 RT | | 1 | |
| Humbert Road | 20+169.230 | 15.240 RT | | 1 | |
| Humbert Road | 20+248.275 | 32.004 LT | | 1 | |
| Humbert Road | 20+409.465 | 15.240 RT | | 1 | |
| Humbert Road | 19+730.815 | 19.515 RT | | 1 | |
| Ramp B | 0+290.229 | 41.559 RT | | 1 | |
| Ramp B | 0+307.592 | 37.443 RT | | 1 | |
| Ramp B | 0+339.289 | 15.240 RT | | 1 | |
| Ramp B | 0+349.757 | 22.093 RT | | 1 | |
| Ramp B | 0+381.333 | 16.764 RT | | 1 | |
| Ramp B | 0+415.486 | 16.764 RT | | 1 | |
| Ramp B | 0+447.525 | 18.077 RT | | 1 | |
| Ramp B | 0+476.616 | 22.618 RT | | 1 | |
| Seller Rd. | 0+853.033 | 15.240 LT | | 1 | |
| Seller Road | 0+683.127 | 15.240 LT | | 1 | |
| Seller Road | 0+723.021 | 15.240 LT | | 1 | |
| Seller Road | 0+762.973 | 15.240 LT | | 1 | |
| Seller Road | 0+792.961 | 15.240 LT | | 1 | |
| Seller Road | 0+822.979 | 15.240 LT | | 1 | |
| Seller Road | 0+853.033 | 15.240 LT | | 1 | |
| Seller Road | 0+883.127 | 15.240 LT | | 1 | |
| Wenzel Rd. | 10+421.937 | 29.921 LT | | 1 | |
| Wenzel Rd. | 10+431.027 | 28.973 LT | | 1 | |
| Wenzel Rd. | 10+431.178 | 32.919 RT | 1 | | |
| Wenzel Rd. | 9+672.778 | 6.174 RT | 1 | | |
| FAP 310-Ramp B @ Humbert | 0+576.268 | 23.772 RT | 1 | | |
| Total Quantities | | | * 17 | 27 | 2 |

Section Corner Markers

| Section Corners | Station | Offset | Quantity |
|---|-----------------------------------|-----------|----------|
| Center of Section 24 T6NR10W | 37+278.914 | 78.326 LT | 1 |
| NW corner E 1/2 of NE 1/4 = NE corner W 1/2 of NE 1/4 Sec. 23 T6NR10W | Humbert Rd & Seller Rd 20+248.441 | 2.048 LT | 1 |
| NW corner E 1/2 of SE 1/4 of SW 1/4 Sec. 14, T6NR10W | 39+805.316 | 76.037 RT | 1 |
| Total Quantity | | | 3 |

*THIS IS NOT A TOTAL QUANTITY
 Right of Way Markers shall be set so the back of the post is twelve (12") inches inside the right of way boundary. Right of way corners are marked by a 5/8" iron rod with IDOT aluminum cap and shall not be removed or damaged when setting the right of way markers.

| REVISIONS | |
|-----------|------|
| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
 SCHEDULES OF QUANTITIES
 FAP 310 (IL 255)
 SECTION 60-15
 MADISON COUNTY
 DRAWN BY EBB
 CHECKED BY
 DATE

SCHEDULES OF QUANTITIES

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------------|---------------------------|--------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 18 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |

RAISED REFLECTIVE PAVEMENT MARKERS

| STATION TO STATION | SIDE | SPACING | ONE-WAY CRYSTAL EACH | ONE-WAY AMBER EACH |
|-----------------------|----------|---------|----------------------|--------------------|
| FAP 310 | | | | |
| 35+150.0 | 38+790.6 | RT | 24.4 | 300 |
| 35+150.0 | 38+789.3 | LT | 24.4 | 300 |
| 38+336.5 | 38+452.0 | RT | 12.2 | 10 |
| 38+871.9 | 39+117.3 | LT | 24.4 | 22 |
| 39+196.5 | 39+876.8 | LT | 24.4 | 58 |
| 39+952.5 | 40+635.2 | LT | 24.4 | 58 |
| 39+105.8 | 39+117.3 | LT | 12.2 | 1 |
| 39+196.5 | 39+473.3 | LT | 24.4 | 24 |
| 40+330.0 | 40+440.3 | LT | 12.2 | 10 |
| RAMP B | | | | |
| 0+000.0 | 0+200.1 | RT | 6.1 | 34 |
| 0+084.4 | 0+243.1 | LT | 12.2 | 13 |
| 0+243.1 | 0+352.7 | LT | 12.2 | 10 |
| HUMBERT RAMP D | | | | |
| 0+000.0 | 0+003.1 | RT | 6.1 | 1 |
| 0+087.4 | 0+091.5 | RT | 6.1 | 1 |
| 0+087.4 | 0+091.5 | LT | 12.2 | 1 |
| RAMP E | | | | |
| 0+000.0 | 0+130.2 | RT | 6.1 | 22 |
| 0+130.2 | 0+178.6 | LT | 12.2 | 4 |
| IL 111 RAMP C | | | | |
| 0+000.0 | 0+196.2 | RT | 6.1 | 33 |
| 0+083.2 | 0+196.2 | LT | 12.2 | 10 |
| 0+196.2 | 0+381.0 | LT | 12.2 | 15 |
| IL 111 | | | | |
| 29+828.5 | 29+872.7 | RT | 24.4 | 2 |
| 29+897.5 | 29+947.5 | LT | 12.2 | 5 |
| 30+007.5 | 30+073.0 | RT | 12.2 | 6 |
| 30+057.5 | 30+073.0 | RT | 12.2 | 1 |
| TOTALS | | | 912 | 29 |
| USE | | | | 941 |

** DOUBLE REFLECTORS

RAISED REFLECTIVE PAVEMENT MARKERS (BRIDGE)

| STATION TO STATION | SIDE | SPACING | ONE-WAY CRYSTAL EACH |
|-----------------------|----------|---------|----------------------|
| FAP 310 | | | |
| 38+789.3 | 38+871.9 | LT | 24.4 |
| 38+790.6 | 38+875.0 | RT | 24.4 |
| 39+117.3 | 39+196.5 | LT | 24.4 |
| 39+117.3 | 39+179.0 | LT | 12.2 |
| 39+876.8 | 39+952.5 | LT | 24.4 |
| HUMBERT RAMP D | | | |
| 0+003.1 | 0+087.4 | RT | 6.1 |
| 0+016.1 | 0+087.4 | LT | 12.2 |
| TOTALS | | | 58 |

** DOUBLE REFLECTORS

SHOULDER RUMBLE STRIP

| STATION TO STATION | SIDE | METER |
|--------------------|----------|----------------|
| FAP 310 | | |
| 35+150.0 | 38+790.6 | RT |
| 35+150.0 | 38+789.3 | LT |
| 35+150.0 | 38+140.0 | LT |
| 35+244.7 | 38+252.0 | RT |
| 38+452.1 | 38+791.2 | RT |
| 38+523.7 | 38+788.8 | LT |
| 38+870.6 | 39+108.3 | LT |
| 38+871.9 | 39+117.3 | LT |
| 38+875.0 | 39+124.8 | RT |
| 38+876.4 | 39+127.2 | RT |
| 39+195.0 | 39+410.0 | LT |
| 39+196.5 | 39+876.8 | LT |
| 39+201.0 | 39+876.8 | RT |
| 39+370.0 | 39+505.0 | RT |
| 39+621.7 | 39+876.8 | LT |
| 39+636.8 | 39+876.8 | RT |
| 39+952.5 | 40+260.2 | RT |
| 39+952.5 | 40+665.6 | RT |
| 39+952.5 | 40+635.2 | LT |
| 39+952.5 | 40+330.1 | LT |
| 40+530.0 | 40+635.2 | LT |
| 40+657.8 | 40+665.6 | RT |
| TOTAL | | 19260.5 |
| USE | | 19261 |

BARRIER WALL MARKERS, TYPE B

| STATION TO STATION | SIDE | SPACING | EACH |
|-----------------------|----------|---------|-----------|
| FAP 310 | | | |
| 38+797.3 | 38+861.2 | LT | 21.3 |
| 38+798.4 | 38+863.4 | LT | 21.7 |
| 38+799.5 | 38+865.4 | RT | 22.0 |
| 38+800.6 | 38+867.7 | RT | 22.4 |
| 39+122.2 | 39+192.7 | LT | 23.5 |
| 39+128.0 | 39+195.0 | RT | 22.3 |
| 39+884.1 | 39+945.1 | RT | 20.3 |
| 39+884.1 | 39+945.1 | RT | 20.3 |
| 39+884.1 | 39+945.1 | LT | 20.3 |
| 39+884.1 | 39+945.1 | LT | 20.3 |
| HUMBERT RAMP C | | | |
| 0+280.1 | 0+347.7 | RT | 22.6 |
| HUMBERT RAMP D | | | |
| 0+009.3 | 0+083.6 | RT | 24.8 |
| TOTAL | | | 48 |

TERMINAL MARKER - DIRECT APPLIED

| STATION | SIDE | EACH |
|-----------------------|------|-----------|
| FAP 310 | | |
| 35+525.0 | LT | 1 |
| 38+625.3 | RT | 1 |
| 38+656.1 | RT | 1 |
| 38+984.2 | RT | 1 |
| 39+001.1 | LT | 1 |
| 39+030.3 | LT | 1 |
| 39+328.6 | LT | 1 |
| 39+330.5 | LT | 1 |
| 39+722.4 | RT | 1 |
| 39+749.0 | RT | 1 |
| 40+080.2 | LT | 1 |
| 40+130.5 | LT | 1 |
| RAMP 3 | | |
| 1+503.9 | RT | 1 |
| HUMBERT RAMP C | | |
| 0+217.0 | RT | 1 |
| TOTAL | | 14 |

GUARDRAIL MARKERS, TYPE A

| STATION TO STATION | SIDE | SPACING | SILVER EACH | AMBER EACH |
|-----------------------|----------|---------|-------------|------------|
| FAP 310 | | | | |
| 35+146.8 | 35+405.0 | LT | 2 | |
| 35+244.7 | 35+460.0 | RT | 2 | |
| 35+405.0 | 35+525.0 | LT | 5 | |
| 38+625.3 | 38+745.3 | RT | 24 | |
| 38+656.1 | 38+776.1 | RT | 24 | 5 |
| 38+744.5 | 38+797.3 | LT | 45 | 1 |
| 38+745.3 | 38+800.6 | RT | 45 | 1 |
| 38+861.2 | 38+910.3 | LT | 45 | 1 |
| 38+867.7 | 38+958.3 | RT | 45 | 2 |
| 38+881.1 | 39+001.1 | LT | 24 | 5 |
| 38+910.3 | 39+030.3 | LT | 24 | 5 |
| 38+984.2 | 39+104.2 | RT | 24 | 5 |
| 39+208.6 | 39+328.6 | LT | 24 | 5 |
| 39+210.5 | 39+330.5 | LT | 24 | 5 |
| 39+722.4 | 39+842.4 | RT | 24 | 5 |
| 39+749.0 | 39+869.0 | RT | 24 | 5 |
| 39+960.2 | 40+080.2 | LT | 24 | 5 |
| 40+010.5 | 40+130.5 | LT | 24 | 5 |
| RAMP 3 | | | | |
| 1+503.9 | 1+623.9 | RT | 24 | 5 |
| 1+623.9 | 1+710.6 | RT | 30 | 2 |
| HUMBERT RAMP C | | | | |
| 0+217.0 | 0+280.1 | RT | 24 | 2 |
| 0+347.7 | 0+417.3 | RT | 45 | 1 |
| HUMBERT RAMP D | | | | |
| 0+083.6 | 0+130.1 | RT | 30 | 1 |
| TOTALS | | | 50 | 30 |
| USE | | | | 80 |

EARTHWORK SUMMARY

| LOCATION STATION TO STATION | EARTH EXCAVATION | EARTH EXCAVATION ADJUSTED 25% | EMBANKMENT | EARTHWORK BALANCE |
|----------------------------------|------------------|-------------------------------|---------------|-------------------|
| FAP 310 | | | | |
| 35+150 | 35+500 | 41800 | 31350 | 273415 |
| 35+500 | 35+850 | 59215 | 44410 | 11915 |
| 35+850 | 36+200 | 31750 | 23815 | 12185 |
| 36+200 | 36+550 | 69930 | 52450 | 40 |
| 36+550 | 36+900 | 83580 | 62685 | 26270 |
| 36+900 | 37+250 | 82915 | 62110 | 16630 |
| 37+250 | 37+600 | 51005 | 38255 | 64320 |
| 37+600 | 37+950 | 120215 | 90160 | 6125 |
| 37+950 | 38+300 | 56075 | 42055 | 37720 |
| 38+300 | 38+650 | 6450 | 4840 | 25350 |
| 38+650 | 39+000 | 4250 | 3190 | 99690 |
| 39+000 | 39+350 | 1235 | 925 | 91015 |
| 39+350 | 39+700 | 895 | 670 | 49250 |
| 39+700 | 40+050 | 7140 | 5355 | 78630 |
| 40+050 | 40+400 | 5240 | 3930 | 16560 |
| 40+400 | 40+658 | 7435 | 5575 | 5575 |
| SUBTOTALS | 629030 | 471775 | 809115 | (337340) |
| RAMP A | | | | |
| RAMP A | 15125 | 11345 | 13420 | (2075) |
| RAMP B | | | | |
| RAMP B | 34200 | 25650 | 5695 | 19955 |
| HUMBERT RAMP C | | | | |
| HUMBERT RAMP C | 2040 | 1530 | 33275 | (31745) |
| HUMBERT RAMP D | | | | |
| HUMBERT RAMP D | 2380 | 1785 | 47045 | (45260) |
| RAMP F | | | | |
| RAMP F | 4670 | 3505 | 23335 | (19830) |
| RAMP E | | | | |
| RAMP E | 4570 | 3430 | 18220 | (14790) |
| IL 111 RAMP D | | | | |
| IL 111 RAMP D | 18600 | 13950 | 2970 | 10980 |
| IL 111 RAMP C | | | | |
| IL 111 RAMP C | 24145 | 18110 | 90 | 18020 |
| SUBTOTALS | 105730 | 79305 | 144050 | (64745) |
| RAMP A INFIELD | | | | |
| RAMP A INFIELD | 2905 | 2180 | | 2180 |
| RAMP B INFIELD | | | | |
| RAMP B INFIELD | 6405 | 4805 | | 4805 |
| HUMBERT RAMP C INFIELD | | | | |
| HUMBERT RAMP C INFIELD | 445 | 335 | | 335 |
| HUMBERT RAMP D INFIELD | | | | |
| HUMBERT RAMP D INFIELD | 1160 | 870 | | 870 |
| RAMP F INFIELD | | | | |
| RAMP F INFIELD | 9405 | 7055 | | 7055 |
| RAMP E INFIELD | | | | |
| RAMP E INFIELD | 560 | 420 | | 420 |
| IL 111 RAMP D INFIELD | | | | |
| IL 111 RAMP D INFIELD | 925 | 695 | | 695 |
| IL 111 RAMP C INFIELD | | | | |
| IL 111 RAMP C INFIELD | 14140 | 10605 | | 10605 |
| SUBTOTALS | 35945 | 26965 | 0 | 26965 |
| HUMBERT ROAD RIGHT SIDE | | | | |
| 19+920 | 19+970 | 415 | 310 | 310 |
| 20+030 | 20+090 | 785 | 590 | 590 |
| HUMBERT ROAD LEFT SIDE | | | | |
| 19+920 | 19+970 | 580 | 435 | 435 |
| 20+030 | 20+090 | 135 | 100 | 100 |
| IL 111 RIGHT SIDE | | | | |
| 29+880 | 29+960 | 1585 | 1190 | 1190 |
| 30+040 | 30+070 | 170 | 130 | 130 |
| IL 111 LEFT SIDE | | | | |
| 29+880 | 29+960 | 440 | 330 | 330 |
| 30+040 | 30+070 | 190 | 145 | 145 |
| NE QUADRANT RAMP F | | | | |
| NE QUADRANT RAMP F | 395 | 295 | | 295 |
| NW QUADRANT RAMP F | | | | |
| NW QUADRANT RAMP F | 195 | 145 | | 145 |
| NW QUADRANT IL 111 RAMP D | | | | |
| NW QUADRANT IL 111 RAMP D | 320 | 240 | | 240 |
| SUBTOTALS | 5210 | 3910 | 0 | 3910 |
| TOTALS | 775915 | 581955 | 953165 | (371210) |

PIPE CULVERT REMOVAL

| STATION | OFFSET | SIDE | DESCRIPTION | METER |
|----------------|--------|------|-------------|-------------|
| FAP 310 | | | | |
| 36+790.7 | 25.4 | LT | 300 mm RCCP | 53.5 |
| TOTAL | | | | 53.5 |
| USE | | | | 54 |

REMOVING INLETS

| STATION | OFFSET | SIDE | EACH |
|----------------|--------|------|----------|
| FAP 310 | | | |
| 36+813.0 | 39.2 | LT | 1 |
| TOTAL | | | 1 |

| REVISIONS | |
|-----------|------|
| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
SCHEDULES OF QUANTITIES
FAP 310 (IL 255)
SECTION 60-15
MADISON COUNTY
 DRAWN BY **EBB**
 CHECKED BY
 DATE

SCHEDULES OF QUANTITIES

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------------|---------------------------|--------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 20 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |

SEEDING SCHEDULE

| STATION TO STATION | SIDE | WIDTH | SEEDING CLASS 2 | MULCH, METHOD 2 | NITROGEN FERTILIZER NUTRIENT | PHOSPHORUS FERTILIZER NUTRIENT | POTASSIUM FERTILIZER NUTRIENT |
|------------------------|------|-------|-----------------|-----------------|------------------------------|--------------------------------|-------------------------------|
| | | | HA | HA | KG | | |
| FAP 310 | | | | | | | |
| 35+150 35+500 | LT | 19-68 | 1.69 | 1.69 | 169 | 169 | 169 |
| 35+150 35+500 | RT | 21-79 | 1.88 | 1.88 | 188 | 188 | 188 |
| 35+150 38+799 | CL | 11.6 | 4.23 | 4.23 | 423 | 423 | 423 |
| 35+500 35+850 | LT | 18-40 | 0.90 | 0.90 | 90 | 90 | 90 |
| 35+500 35+850 | RT | 10-22 | 0.59 | 0.59 | 59 | 59 | 59 |
| 35+850 36+200 | LT | 12-29 | 0.68 | 0.68 | 68 | 68 | 68 |
| 35+850 36+200 | RT | 13-34 | 0.72 | 0.72 | 72 | 72 | 72 |
| 36+200 36+550 | LT | 16-34 | 0.82 | 0.82 | 82 | 82 | 82 |
| 36+200 36+550 | RT | 14-24 | 0.68 | 0.68 | 68 | 68 | 68 |
| 36+550 36+900 | LT | 15-52 | 1.07 | 1.07 | 107 | 107 | 107 |
| 36+550 36+900 | RT | 15-39 | 1.04 | 1.04 | 104 | 104 | 104 |
| 36+900 37+250 | LT | 20-50 | 1.29 | 1.29 | 129 | 129 | 129 |
| 36+900 37+250 | RT | 21-34 | 1.12 | 1.12 | 112 | 112 | 112 |
| 37+250 37+600 | LT | 14-50 | 1.29 | 1.29 | 129 | 129 | 129 |
| 37+250 37+600 | RT | 20-38 | 1.25 | 1.25 | 125 | 125 | 125 |
| 37+600 37+950 | LT | 8-42 | 0.65 | 0.65 | 65 | 65 | 65 |
| 37+600 37+950 | RT | 20-39 | 1.22 | 1.22 | 122 | 122 | 122 |
| 37+950 38+140 | LT | 16-41 | 0.52 | 0.52 | 52 | 52 | 52 |
| 37+950 38+252 | RT | 31-56 | 1.51 | 1.51 | 151 | 151 | 151 |
| 38+864 39+131 | CL | 11.6 | 0.31 | 0.31 | 31 | 31 | 31 |
| 39+152 39+410 | LT | 24-67 | 1.06 | 1.06 | 106 | 106 | 106 |
| 39+188 39+884 | CL | 11.6 | 0.81 | 0.81 | 81 | 81 | 81 |
| 39+370 39+505 | RT | 10-21 | 0.20 | 0.20 | 20 | 20 | 20 |
| 39+945 40+665 | CL | 11.6 | 0.84 | 0.84 | 84 | 84 | 84 |
| 40+530 40+681 | LT | VAR | 0.23 | 0.23 | 23 | 23 | 23 |
| 40+658 40+719 | RT | VAR | 0.16 | 0.16 | 16 | 16 | 16 |
| RAMP A | | | | | | | |
| 0+049 0+703 | RT | 10-41 | 1.28 | 1.28 | 128 | 128 | 128 |
| RAMP B | | | | | | | |
| 0+000 0+546 | RT | 9-48 | 1.18 | 1.18 | 118 | 118 | 118 |
| HUMBERT RAMP C | | | | | | | |
| 0+043 0+316 | RT | 8-55 | 0.81 | 0.81 | 81 | 81 | 81 |
| 0+347 0+509 | RT | 22-42 | 0.55 | 0.55 | 55 | 55 | 55 |
| HUMBERT RAMP D | | | | | | | |
| 0+088 0+379 | RT | 9-49 | 1.04 | 1.04 | 104 | 104 | 104 |
| RAMP F | | | | | | | |
| 0+040 0+534 | RT | 10-39 | 0.94 | 0.94 | 94 | 94 | 94 |
| RAMP E | | | | | | | |
| 0+000 0+377 | RT | 10-24 | 0.66 | 0.66 | 66 | 66 | 66 |
| IL 111 RAMP D | | | | | | | |
| 0+041 0+762 | RT | 9-22 | 1.05 | 1.05 | 105 | 105 | 105 |
| IL 111 RAMP C | | | | | | | |
| 0+000 0+579 | RT | 9-19 | 0.72 | 0.72 | 72 | 72 | 72 |
| RAMP A INFIELD | LT | VAR | 1.34 | 1.34 | 134 | 134 | 134 |
| RAMP B INFIELD | LT | VAR | 1.68 | 1.68 | 168 | 168 | 168 |
| HUMBERT RAMP C INFIELD | LT | VAR | 0.87 | 0.87 | 87 | 87 | 87 |
| HUMBERT RAMP D INFIELD | LT | VAR | 1.27 | 1.27 | 127 | 127 | 127 |
| RAMP F INFIELD | LT | VAR | 1.57 | 1.57 | 157 | 157 | 157 |
| RAMP E INFIELD | LT | VAR | 0.77 | 0.77 | 77 | 77 | 77 |
| IL 111 RAMP D INFIELD | LT | VAR | 0.94 | 0.94 | 94 | 94 | 94 |
| IL 111 RAMP C INFIELD | LT | VAR | 1.79 | 1.79 | 179 | 179 | 179 |
| TOTALS | | | 45.24 | 45.24 | 4524 | 4524 | 4524 |
| USE | | | 45.3 | 45.3 | 4530 | 4530 | 4530 |

RATES:
FERTILIZER NUTRIENTS = 100 KG / HA

RIPRAP SCHEDULE

| STATION TO STATION | SIDE | WIDTH | STONE RIPRAP CLASS A5 | STONE DUMPED RIPRAP CLASS A4 | STONE DUMPED RIPRAP CLASS B3 | FILTER FABRIC |
|-----------------------|------|-------|-----------------------|------------------------------|------------------------------|---------------|
| | | | SQ M | | | SQ M |
| FAP 310 | | | | | | |
| 35+145.0 35+289.6 | RT | 3.9 | | 569.7 | | 569.7 |
| 35+264.8 35+290.4 | LT | 11.0 | 264.0 | | | 264.0 |
| 35+289.6 35+319.6 | RT | 7.0 | 168.0 | | | 168.0 |
| 35+290.4 35+500.0 | LT | 3.9 | | 872.2 | | 872.2 |
| 35+319.6 35+600.0 | RT | 3.9 | | 1089.2 | | 1089.2 |
| 35+500.0 35+750.0 | LT | 3.9 | | | 1013.0 | |
| 35+947.2 35+952.8 | RT | 5.4 | | 52.5 | | 52.5 |
| 36+393.1 36+397.1 | RT | 3.9 | | 195.7 | | 195.7 |
| 36+393.1 36+450.0 | LT | 3.9 | | | 227.2 | |
| 36+397.6 36+435.0 | RT | 3.9 | | | 142.6 | |
| 36+625.0 36+701.6 | RT | 3.9 | | | 294.4 | |
| 36+690.0 36+758.0 | LT | 3.9 | | | 299.1 | |
| 36+714.4 36+800.0 | RT | 3.9 | | | 319.3 | |
| 36+758.0 36+763.7 | LT | VAR | | 129.4 | | 129.4 |
| 36+763.7 36+777.7 | LT | 3.9 | | | 71.9 | |
| 36+923.0 36+977.6 | RT | 4.5 | | 443.9 | | 443.9 |
| 36+973.1 36+986.8 | LT | 3.9 | | 102.2 | | 102.2 |
| 37+244.9 37+351.8 | RT | 3.9 | | | 444.8 | |
| 37+310.0 37+405.3 | LT | 3.9 | | | 360.3 | |
| 37+376.7 37+485.0 | RT | 3.9 | | 448.2 | | 448.2 |
| 37+405.3 37+625.0 | LT | VAR | | 1747.3 | | 1747.3 |
| 37+966.1 37+974.5 | RT | 8.7 | | 157.7 | | 157.7 |
| 37+974.5 38+137.6 | RT | 8.4 | | 1515.2 | | 1515.2 |
| 38+154.5 38+158.7 | LT | 3.9 | | 121.5 | | 121.5 |
| 38+158.7 38+212.3 | LT | 3.9 | | | 205.5 | |
| 39+790.0 39+803.3 | LT | VAR | | 141.3 | | 141.3 |
| 40+643.1 40+651.1 | RT | 3.9 | | | 22.8 | |
| RAMP A | | | | | | |
| 0+173.1 0+200.0 | RT | 3.9 | | | 103.8 | |
| HUMBERT RAMP D | | | | | | |
| 0+338.2 0+341.8 | RT | 3.9 | | | 15.2 | |
| RAMP F | | | | | | |
| 0+090.0 0+145.1 | RT | 3.9 | | | 231.7 | |
| 0+155.8 0+217.5 | RT | 3.9 | | 239.3 | | 239.3 |
| 0+229.8 0+310.0 | RT | 3.9 | | | 291.7 | |
| RAMP E | | | | | | |
| 0+112.5 0+201.9 | RT | 3.9 | | | 316.7 | |
| TOTALS | | | 432.0 | 7825.3 | 4360.0 | 8257.3 |
| USE | | | 432 | 7826 | 4360 | 8258 |

RIPRAP FOR STILLING BASIN

| STATION | SIDE | CLASS | RIPRAP FOR STILLING BASIN | FILTER FABRIC |
|----------------|------|-------|---------------------------|---------------|
| | | | M TON | SQ M |
| FAP 310 | | | | |
| 36+395.0 | RT | B-3 | 13.3 | |
| 36+711.6 | RT | A-6 | 1674.8 | 733.7 |
| 37+371.1 | RT | A-6 | 2364.6 | 1034.8 |
| 37+942.6 | LT | A-6 | 1037.0 | 450.1 |
| 38+143.5 | RT | A-5 | 277.7 | 181.8 |
| 39+700.0 | LT | A-4 | 134.6 | 106.6 |
| RAMP F | | | | |
| 0+150.0 | RT | A-4 | 192.2 | 151.6 |
| TOTALS | | | 5694.2 | 2658.6 |
| USE | | | 5695 | 2659 |

RATE:
RIPRAP = 2.5 M TONS / CU M

SLOPEWALL 150MM

| STATION | SIDE | WIDTH | SQ M |
|---------------|------|-------|--------------|
| 0+265.0 | RT | 7.5 | 112.5 |
| RAMP E | | | |
| 0+120.0 | RT | 5.5 | 82.5 |
| 0+295.5 | RT | 6.5 | 97.5 |
| TOTAL | | | 292.5 |
| USE | | | 293 |

| REVISIONS | |
|-----------|------|
| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
SCHEDULES OF QUANTITIES
FAP 310 (IL 255)
SECTION 60-15
MADISON COUNTY
 DRAWN BY EBB
 CHECKED BY
 DATE

SCHEDULES OF QUANTITIES

| | | | | |
|---------------------|---------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 21 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |

PROCESSING LIME STABILIZED SOIL MIXTURE, 300MM

| STATION TO STATION | SIDE | WIDTH | LIME STABILIZED SOILS | LIME | |
|--------------------|----------|-------|-----------------------|---------|--------|
| | | | SQ M | M TON | |
| FAP 310 | | | | | |
| 35+150.0 | 35+244.7 | RT | 9.4 | 885.8 | 18.22 |
| 35+150.0 | 38+140.0 | LT | 12.7 | 37973.0 | 781.03 |
| 35+244.7 | 38+252.0 | RT | 12.7 | 38192.3 | 785.54 |
| 38+140.0 | 38+523.7 | LT | 9.4 | 3587.1 | 73.78 |
| 38+252.0 | 38+452.1 | RT | 9.4 | 1870.5 | 38.47 |
| 38+336.5 | 38+452.1 | RT | 0.0-6.3 | 364.0 | 7.49 |
| 38+372.9 | 38+523.7 | LT | 0.0-4.8 | 362.6 | 7.46 |
| 38+452.1 | 38+753.9 | RT | 12.7 | 3833.2 | 78.84 |
| 38+523.7 | 38+750.0 | LT | 12.7 | 2874.8 | 59.13 |
| 38+750.0 | 38+778.9 | LT | 12.7-13.4 | 377.5 | 7.76 |
| 38+753.9 | 38+774.5 | RT | 12.7-13.2 | 267.2 | 5.50 |
| 38+774.5 | 38+791.4 | RT | 13.2-13.8 | 228.2 | 4.69 |
| 38+778.9 | 38+789.5 | LT | 13.4-13.8 | 143.8 | 2.96 |
| 38+870.0 | 38+890.1 | LT | 12.9-13.6 | 267.6 | 5.50 |
| 38+874.5 | 38+885.0 | RT | 13.3-13.6 | 141.9 | 2.92 |
| 38+885.0 | 38+910.1 | RT | 12.7-13.3 | 325.0 | 6.68 |
| 38+890.1 | 38+900.8 | LT | 12.7-12.9 | 136.3 | 2.80 |
| 38+900.8 | 38+988.4 | LT | 12.7 | 1112.6 | 22.88 |
| 38+910.1 | 39+080.2 | RT | 12.7 | 2161.1 | 44.45 |
| 38+988.4 | 39+106.9 | LT | 12.7-15.1 | 1646.0 | 33.86 |
| 39+106.9 | 39+108.3 | LT | 15.1 | 21.4 | 0.44 |
| 39+108.3 | 39+117.9 | LT | 13.0-13.1 | 124.6 | 2.56 |
| 39+108.0 | 39+124.0 | RT | 12.7-13.3 | 570.2 | 11.73 |
| 39+108.3 | 39+114.5 | LT | 5.5-6.3 | 36.1 | 0.74 |
| 39+195.0 | 39+208.1 | LT | 16.3-16.4 | 215.2 | 4.43 |
| 39+199.7 | 39+240.0 | RT | 9.4-10.6 | 402.1 | 8.27 |
| 39+205.1 | 39+303.4 | RT | 0.0-2.0 | 95.9 | 1.97 |
| 39+208.1 | 39+410.0 | LT | 16.3 | 3290.2 | 67.67 |
| 39+240.0 | 39+370.0 | RT | 9.4 | 1215.5 | 25.00 |
| 39+370.0 | 39+505.0 | RT | 16.3 | 2200.5 | 45.26 |
| 39+410.0 | 39+621.7 | LT | 9.4 | 1979.8 | 40.72 |
| 39+473.4 | 39+621.7 | LT | 0.0-4.8 | 356.8 | 7.34 |
| 39+505.0 | 39+602.1 | RT | 13.0 | 1257.0 | 25.85 |
| 39+576.3 | 39+602.1 | RT | 0.0-1.8 | 26.6 | 0.55 |
| 39+602.1 | 39+636.8 | RT | 1.8-4.9 | 116.5 | 2.40 |
| 39+602.1 | 39+636.8 | RT | 12.3-13.0 | 437.2 | 8.99 |
| 39+621.7 | 39+876.8 | LT | 12.7 | 3238.9 | 66.62 |
| 39+636.8 | 39+722.1 | RT | 12.7-14.4 | 1156.7 | 23.79 |
| 39+722.1 | 39+876.8 | RT | 12.7 | 1964.5 | 40.41 |
| 39+952.5 | 40+260.2 | RT | 12.7 | 3908.5 | 80.39 |
| 39+952.5 | 40+330.1 | LT | 12.7 | 4795.7 | 98.64 |
| 40+260.2 | 40+657.8 | RT | 9.4 | 3717.3 | 76.46 |
| 40+260.2 | 40+429.3 | RT | 0.0-4.8 | 405.7 | 8.35 |
| 40+330.1 | 40+530.0 | LT | 9.4 | 1869.4 | 38.45 |
| 40+330.1 | 40+445.7 | LT | 0.0-6.3 | 364.3 | 7.49 |
| 40+530.0 | 40+635.2 | LT | 12.7 | 1335.6 | 27.47 |
| 40+657.8 | 40+665.6 | RT | 12.7 | 99.9 | 2.06 |
| SUBTOTALS | | | 131952.5 | 2714.00 | |

PROCESSING LIME STABILIZED SOIL MIXTURE, 300MM CONTINUED

| STATION TO STATION | SIDE | WIDTH | LIME STABILIZED SOILS | LIME | |
|--------------------|---------|---------|-----------------------|---------|-------|
| | | | SQ M | M TON | |
| RAMP 3 | | | | | |
| 1+615.8 | 1+665.6 | LT & RT | 3.4-4.4 | 191.4 | 3.94 |
| 1+665.6 | 1+710.6 | LT & RT | 3.4 | 150.8 | 3.10 |
| RAMP A | | | | | |
| 0+048.9 | 0+322.4 | LT & RT | 8.6 | 2351.8 | 48.37 |
| 0+322.4 | 0+472.5 | LT & RT | 7.1 | 1058.6 | 21.77 |
| 0+472.5 | 0+658.0 | LT & RT | 3.4-7.1 | 964.4 | 19.84 |
| 0+658.0 | 0+703.1 | LT & RT | 3.4 | 151.1 | 3.11 |
| RAMP B | | | | | |
| 0+000.0 | 0+016.6 | LT & RT | 3.4 | 55.5 | 1.14 |
| 0+016.6 | 0+084.4 | LT & RT | 3.4-7.1 | 352.5 | 7.25 |
| 0+084.4 | 0+200.1 | LT & RT | 7.1 | 815.8 | 16.78 |
| 0+200.1 | 0+388.2 | LT & RT | 8.6 | 1618.0 | 33.28 |
| 0+388.2 | 0+446.2 | LT & RT | 8.6-10.9 | 565.5 | 11.63 |
| 0+446.2 | 0+546.2 | LT & RT | 10.9 | 1090.0 | 22.42 |
| HUMBERT RAMP C | | | | | |
| 0+043.1 | 0+273.8 | LT & RT | 8.6 | 1983.7 | 40.80 |
| 0+273.8 | 0+276.2 | LT & RT | 8.6-8.7 | 20.6 | 0.42 |
| 0+351.8 | 0+409.1 | LT & RT | 7.1-9.0 | 461.2 | 9.49 |
| 0+409.1 | 0+443.8 | LT & RT | 7.1 | 244.8 | 5.03 |
| 0+443.8 | 0+448.8 | LT & RT | 7.0-7.1 | 34.4 | 0.71 |
| 0+448.8 | 0+508.7 | LT & RT | 7.0 | 416.4 | 8.56 |
| HUMBERT RAMP D | | | | | |
| 0+000.0 | 0+004.6 | LT & RT | 3.4-3.6 | 15.9 | 0.33 |
| 0+088.7 | 0+089.0 | LT & RT | 7.8 | 2.3 | 0.05 |
| 0+089.0 | 0+116.2 | LT & RT | 8.6-9.2 | 242.1 | 4.98 |
| 0+116.2 | 0+378.9 | LT & RT | 8.6 | 2258.7 | 46.46 |
| RAMP F | | | | | |
| 0+040.5 | 0+318.8 | LT & RT | 8.6 | 2393.6 | 49.23 |
| 0+318.8 | 0+469.0 | LT & RT | 7.1 | 1058.6 | 21.77 |
| 0+469.0 | 0+473.9 | LT & RT | 7.0-7.1 | 34.5 | 0.71 |
| 0+473.9 | 0+533.9 | LT & RT | 7.0 | 407.8 | 8.39 |
| RAMP E | | | | | |
| 0+000.0 | 0+017.2 | LT & RT | 3.4 | 57.7 | 1.19 |
| 0+017.2 | 0+069.6 | LT & RT | 3.4-7.1 | 272.6 | 5.61 |
| 0+069.6 | 0+130.2 | LT & RT | 7.1 | 427.1 | 8.78 |
| 0+130.2 | 0+376.9 | LT & RT | 8.6 | 2121.5 | 43.63 |
| IL 111 RAMP D | | | | | |
| 0+040.8 | 0+360.1 | LT & RT | 8.6 | 2746.4 | 56.49 |
| 0+360.1 | 0+532.7 | LT & RT | 7.1 | 1216.6 | 25.02 |
| 0+532.7 | 0+717.5 | LT & RT | 3.4-7.1 | 960.9 | 19.76 |
| 0+717.5 | 0+762.5 | LT & RT | 3.4 | 150.8 | 3.10 |
| IL 111 RAMP C | | | | | |
| 0+000.0 | 0+016.6 | LT & RT | 3.4 | 55.5 | 1.14 |
| 0+016.6 | 0+083.2 | LT & RT | 3.4-7.1 | 346.2 | 7.12 |
| 0+083.2 | 0+196.2 | LT & RT | 7.1 | 797.3 | 16.40 |
| 0+196.2 | 0+579.5 | LT & RT | 8.6 | 3296.0 | 67.79 |
| TOTALS | | | 163341.3 | 3359.60 | |
| USE | | | 163342 | 3359.7 | |

RATES:

LIME = 68.56 KG / CU M
1 M TON = 1000 KG

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION SCHEDULES OF QUANTITIES FAP 310 (IL 255) SECTION 60-15 MADISON COUNTY |
|-----------|------|---|
| NAME | DATE | |
| | | DRAWN BY EBB CHECKED BY DATE |
| | | |
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6/28/2007

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCHEDULES OF QUANTITIES

| | | | | |
|---------------------|---------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 22 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |

CULVERT SCHEDULE

| STATION | SIDE | PIPE CULVERTS | | | | | | | | | | | | REINFORCED CONCRETE PIPE TEE | | |
|----------------|---------|-------------------------|-------------------------|-------------------------|-------------------------|-------------------------|-------------------------|-------------------------|-------------------------|--------------------------|--------------------------|--------------------------|-------------------------|------------------------------|-----------------------------------|-----------------------------------|
| | | TYPE 1 RCCP 600MM | TYPE 1 RCCP 900MM | TYPE 2 RCCP 600MM | TYPE 2 RCCP 900MM | TYPE 3 RCCP 300MM | TYPE 3 RCCP 600MM | TYPE 4 RCCP 600MM | TYPE 4 RCCP 900MM | TYPE 4 RCCP 1350MM | TYPE 4 RCCP 1800MM | TYPE 5 RCCP 1050MM | TYPE 6 RCCP 600MM | TYPE 7 RCCP 1200MM | 600MM PIPE WITH 600MM RISER | 900MM PIPE WITH 600MM RISER |
| METER | | | | | | | | | | | | | | | | |
| FAP 310 | | | | | | | | | | | | | | | | |
| 35+291.0 | LT & RT | | | | | | | | | | | | 48.3 | | | |
| 35+495.0 | LT | | | | | | | | | | | | | | | |
| 35+950.0 | RT | | | | | | | | | 36.5 | | | | | | |
| 36+395.0 | LT & RT | | | | 50.6 | | | | | | | | | | 1 | |
| 36+737.0 | LT & RT | | | | | | | | | | | | 8.2 | 112.2 | | 1 |
| 36+975.0 | LT & RT | | | | | | 8.7 | | | 69.0 | | | | | | |
| 37+397.0 | LT & RT | | | | | | | | | | | | | | | |
| 37+450.0 | RT | 0.5 | | | 29.8 | | | | | | | | | | 1 | |
| 37+700.0 | LT | | | | 22.3 | | | | | | | | | | 1 | |
| 37+956.0 | LT & RT | | | | 3.5 | | | | | | | | | | | |
| 38+150.0 | LT & RT | | | | | | | | | 73.4 | | | | | | |
| 38+373.0 | RT | | | | 29.7 | | | | | | | | | | 1 | |
| 39+540.0 | LT | | | | 29.4 | | | | | | | | | | 1 | |
| 39+603.0 | RT | | | | | | | | 18.4 | | | | | | | |
| 39+700.0 | LT & RT | | | | | | | | | | | 125.7 | | | | |
| 40+310.5 | RT | | | | 30.3 | | | | | | | | | | 1 | |
| 40+405.0 | LT | | | | 11.3 | | | | | | | | | | | |
| RAMP A | | | | | | | | | | | | | | | | |
| 0+175.0 | LT & RT | | | | | | | | | | | | | | | |
| RAMP B | | | | | | | | | | | | | | | | |
| 0+375.0 | LT & RT | | | | 22.1 | | | | | | | | | | | |
| HUMBERT RAMP C | | | | | | | | | | | | | | | | |
| 0+065.0 | LT & RT | | | | 15.6 | | | | | | | | | | | |
| HUMBERT RAMP D | | | | | | | | | | | | | | | | |
| 0+340.0 | LT & RT | | | | | | 18.1 | | | | | | | | | |
| RAMP F | | | | | | | | | | | | | | | | |
| 0+150.0 | LT & RT | | | | | | 19.6 | | | | | 41.2 | | | | |
| TOTALS | | 0.5 | 15.6 | 156.3 | 110.4 | 8.7 | 18.4 | 36.5 | 69.0 | 73.4 | 41.2 | 125.7 | 56.5 | 112.2 | 5 | 1 |
| USE | | 0.5 | 16.0 | 156.5 | 110.5 | 9.0 | 18.5 | 36.5 | 69.0 | 73.5 | 41.5 | 126.0 | 56.5 | 112.5 | 5 | 1 |

CULVERT SCHEDULE CONTINUED

| STATION | SIDE | PRECAST REINFORCED CONCRETE FLARED END SECTIONS | | | | | | CAST-IN-PLACE REINFORCED CONCRETE END SECTIONS 1200MM EACH | GRATING FOR CONCRETE FLARED END SECTION | | MEDIAN INLET (604101) EACH | REINFORCEMENT BARS KG | PRECAST CONCRETE BOX CULVERT 2.4M X 2.1M METER | CONCRETE BOX CULVERTS CU M | CONCRETE HEADWALLS CU M | MANHOLES, TYPE A | | MANHOLES SPECIAL 1.5M DIAMETER |
|----------------|---------|---|-------|-------|--------|--------|--------|--|---|-------|-------------------------------------|-----------------------------|--|-------------------------------------|-------------------------------|--|---|--------------------------------------|
| | | 300MM | 600MM | 900MM | 1050MM | 1350MM | 1800MM | | 600MM | 900MM | | | | | | 1.2M DIAMETER WITH MEDIAN INLET (604101) | 1.5M DIAMETER TYPE 1 FRAME OPEN LID EACH | |
| FAP 310 | | | | | | | | | | | | | | | | | | |
| 35+291.0 | LT & RT | | | | | | | | | | 527220 | | 2878.2 | | | | | |
| 35+495.0 | LT | | 1 | | | | | | | | | | | | | 1 | | |
| 35+950.0 | RT | | 1 | | | | | | | | | | | | | 1 | | |
| 36+395.0 | LT & RT | | | 2 | | | | | | 1 | | | | | | | | |
| 36+737.0 | LT & RT | | | | | | | 2 | | 1 | | | | | | | | |
| 36+975.0 | LT & RT | 1 | | | 1 | | | | | | 1060 | 103.3 | | 21.4 | | | 1 | |
| 37+397.0 | LT & RT | | | | | | | | | | | | | | | | | |
| 37+450.0 | RT | | 1 | | | | | | | 1 | | | | | | | | |
| 37+700.0 | LT | | 1 | | | | | | | 1 | | | | | | | | |
| 37+956.0 | LT & RT | | | | | | | | | 1 | | | | | | | | |
| 38+150.0 | LT & RT | | | | | 2 | | | | | 960 | 78.1 | | 19.0 | | | | |
| 38+373.0 | RT | | 1 | | | | | | | 1 | | | | | | | 1 | |
| 39+540.0 | LT | | 1 | | | | | | | 1 | | | | | | | 1 | |
| 39+603.0 | RT | | 1 | | | | | | | 1 | | | | | | | 1 | |
| 39+700.0 | LT & RT | | | | 2 | | | | | | | | | | | | | |
| 40+310.5 | RT | | 1 | | | | | | | 1 | | | | | | | 1 | |
| 40+405.0 | LT | | 1 | | | | | | | | | | | | | | 1 | |
| RAMP A | | | | | | | | | | | | | | | | | | |
| 0+175.0 | LT & RT | | | 2 | | | | | | | | | | | | | | |
| RAMP B | | | | | | | | | | | | | | | | | | |
| 0+375.0 | LT & RT | | | 2 | | | | | | | | | | | | | | |
| HUMBERT RAMP C | | | | | | | | | | 1 | | | | | | | | |
| 0+065.0 | LT & RT | | | 2 | | | | | | | | | | | | | | |
| HUMBERT RAMP D | | | | | | | | | | 1 | | | | | | | | |
| 0+340.0 | LT & RT | | | 2 | | | | | | | | | | | | | | |
| RAMP F | | | | | | | | | | 1 | | | | | | | | |
| 0+150.0 | LT & RT | | | | | | 2 | | | | | | | | | | | |
| TOTALS | | 1 | 9 | 11 | 2 | 2 | 2 | 2 | 2 | 1 | 3 | 8 | 529240 | 181.4 | 2878.2 | 40.4 | 2 | 4 |
| USE | | 1 | 9 | 11 | 2 | 2 | 2 | 2 | 2 | 1 | 3 | 8 | 529240 | 181.4 | 2878.2 | 40.4 | 2 | 4 |

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION SCHEDULES OF QUANTITIES FAP 310 (IL 255) SECTION 60-15 MADISON COUNTY DRAWN BY EBB CHECKED BY |
|-----------|------|---|
| NAME | DATE | |
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SCHEDULES OF QUANTITIES

| | | | | |
|---------------------|---------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 24 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |

PIPE UNDERDRAIN SCHEDULE

| STATION TO STATION | DEPTH OR GRADE % | | | | PIPE UNDERDRAINS 100MM METER | PIPE UNDERDRAINS 100MM (SPECIAL) METER | CONCRETE HEADWALL FOR PIPE DRAINS EACH |
|--------------------|------------------|--------|------------------|---------|------------------------------|--|--|
| | SOUTHBOUND LANES | | NORTHBOUND LANES | | | | |
| | OUTSIDE | MEDIAN | MEDIAN | OUTSIDE | | | |
| FAP 310 | | | | | | | |
| 35+150.0 | 35+259.4 | | 790 | | 109.4 | 5.6 | 1 |
| 35+150.0 | 35+300.0 | | 790 | | 150.0 | 5.8 | 1 |
| 35+150.0 | 35+259.4 | 790 | | | 109.4 | 5.2 | 1 |
| 35+244.7 | 35+300.0 | | | 790 | 55.3 | 5.2 | 1 |
| 35+300.0 | 35+425.0 | | | 790 | 125.0 | | |
| 35+300.0 | 35+450.0 | | 790 | | 150.0 | 5.6 | 1 |
| 35+425.0 | 35+450.0 | | | -0.40% | 25.0 | 5.5 | 1 |
| 35+450.0 | 35+496.2 | | | -0.40% | 46.2 | 7.0 | 1 |
| 35+450.0 | 35+496.2 | -0.40% | | | 46.2 | 6.5 | 1 |
| 35+496.2 | 35+590.0 | | | +0.40% | 93.8 | | |
| 35+496.2 | 35+590.0 | +0.40% | | | 93.8 | | |
| 35+590.0 | 35+650.0 | | | 790 | 60.0 | | |
| 35+590.0 | 35+650.0 | 790 | | | 60.0 | | |
| 35+650.0 | 35+800.0 | | | 790 | 150.0 | 6.1 | 1 |
| 35+650.0 | 35+800.0 | 790 | | | 150.0 | 5.6 | 1 |
| 35+800.0 | 35+950.0 | | | 790 | 150.0 | 6.1 | 1 |
| 35+800.0 | 35+950.0 | 790 | | | 150.0 | 5.6 | 1 |
| 35+950.0 | 36+100.0 | | | 790 | 150.0 | 6.1 | 1 |
| 35+950.0 | 36+100.0 | 790 | | | 150.0 | 5.6 | 1 |
| 36+100.0 | 36+250.0 | | | 790 | 150.0 | 6.1 | 1 |
| 36+100.0 | 36+250.0 | 790 | | | 150.0 | 5.6 | 1 |
| 36+250.0 | 36+400.0 | | | 790 | 150.0 | 6.1 | 1 |
| 36+250.0 | 36+400.0 | 790 | | | 150.0 | 5.6 | 1 |
| 36+400.0 | 36+550.0 | | | 790 | 150.0 | 6.1 | 1 |
| 36+400.0 | 36+550.0 | 790 | | | 150.0 | 5.6 | 1 |
| 36+550.0 | 36+700.0 | | | 790 | 150.0 | 6.1 | 1 |
| 36+550.0 | 36+700.0 | 790 | | | 150.0 | 5.6 | 1 |
| 36+700.0 | 36+850.0 | | | 790 | 150.0 | 6.1 | 1 |
| 36+700.0 | 36+850.0 | 790 | | | 150.0 | 5.6 | 1 |
| 36+850.0 | 36+870.0 | | | 790 | 20.0 | 6.1 | 1 |
| 36+850.0 | 36+949.6 | +0.40% | | | 99.6 | 6.2 | 1 |
| 36+870.0 | 36+949.6 | | | +0.42% | 79.6 | | |
| 36+897.6 | 36+949.6 | | | +0.40% | 52.0 | 6.5 | 1 |
| 36+897.6 | 36+949.6 | +0.40% | | | 52.0 | 7.4 | 1 |
| 36+949.6 | 37+100.0 | | | -0.46% | 150.4 | 6.1 | 1 |
| 36+949.6 | 37+100.0 | | | -0.46% | 150.4 | 5.6 | 1 |
| 36+949.6 | 37+100.0 | -0.46% | | | 150.4 | 6.6 | 1 |
| 36+949.6 | 37+100.0 | | | | 150.4 | 6.1 | 1 |
| 37+100.0 | 37+237.1 | | | 790 | 137.1 | 6.1 | 1 |
| 37+100.0 | 37+250.0 | | | 790 | 150.0 | 5.6 | 1 |
| 37+100.0 | 37+237.1 | | | 790 | 137.1 | 5.6 | 1 |
| 37+100.0 | 37+250.0 | 790 | | | 150.0 | 6.1 | 1 |
| 37+250.0 | 37+400.0 | | | 790 | 150.0 | 5.4 | 1 |
| 37+250.0 | 37+400.0 | 790 | | | 150.0 | 5.8 | 1 |
| 37+400.0 | 37+448.5 | | | -0.40% | 48.5 | 6.1 | 1 |
| 37+400.0 | 37+448.5 | -0.40% | | | 48.5 | 6.6 | 1 |
| 37+448.5 | 37+500.0 | | | +0.40% | 51.5 | | |
| 37+448.5 | 37+500.0 | +0.40% | | | 51.5 | | |
| 37+500.0 | 37+600.0 | | | 790 | 100.0 | | |
| 37+500.0 | 37+600.0 | 790 | | | 100.0 | | |
| 37+600.0 | 37+750.0 | | | 790 | 150.0 | 5.4 | 1 |
| 37+600.0 | 37+750.0 | 790 | | | 150.0 | 5.8 | 1 |
| 37+750.0 | 37+900.0 | | | 790 | 150.0 | 5.4 | 1 |
| SUBTOTALS | | | | | 6153.1 | 254.5 | 43 |

** DOUBLE PIPE OUTLET

PIPE UNDERDRAIN SCHEDULE CONTINUED

| STATION TO STATION | DEPTH OR GRADE % | | | | PIPE UNDERDRAINS 100MM METER | PIPE UNDERDRAINS 100MM (SPECIAL) METER | CONCRETE HEADWALL FOR PIPE DRAINS EACH |
|--------------------|------------------|--------|------------------|---------|------------------------------|--|--|
| | SOUTHBOUND LANES | | NORTHBOUND LANES | | | | |
| | OUTSIDE | MEDIAN | MEDIAN | OUTSIDE | | | |
| FAP 310 | | | | | | | |
| 37+750.0 | 37+900.0 | | | 790 | 150.0 | 5.8 | 1 |
| 37+900.0 | 38+050.0 | | | 790 | 150.0 | 5.4 | 1 |
| 37+900.0 | 38+050.0 | 790 | | | 150.0 | 5.8 | 1 |
| 38+050.0 | 38+200.0 | | | 790 | 150.0 | 5.4 | 1 |
| 38+050.0 | 38+140.0 | 790 | | | 90.0 | 5.8 | 1 |
| 38+200.0 | 38+350.0 | | | 790 | 150.0 | 5.4 | 1 |
| 38+288.9 | 38+425.0 | | | 790 | 136.1 | 5.6 | 1 |
| 38+350.0 | 38+500.0 | | | 790 | 150.0 | 5.6 | 1 |
| 38+373.0 | 38+550.0 | | | 790 | 177.0 | | |
| 38+425.0 | 38+550.0 | | | 790 | 125.0 | 5.6 | 1 |
| 38+432.9 | 38+575.0 | 790 | | | 142.1 | 11.7 | 1 |
| 38+500.0 | 38+666.1 | | | 790 | 166.1 | 5.6 | 1 |
| 38+550.0 | 38+700.0 | | | 790 | 150.0 | 6.1 | 1 |
| 38+550.0 | 38+675.0 | | | 790 | 125.0 | 5.6 | 1 |
| 38+575.0 | 38+666.1 | 790 | | | 91.1 | 6.1 | 1 |
| 38+675.0 | 38+789.3 | | | 790 | 114.3 | 5.6 | 1 |
| 38+700.0 | 38+791.2 | | | 790 | 91.2 | 5.9 | 1 |
| 38+871.9 | 38+970.0 | | | 790 | 98.1 | 6.3 | 1 |
| 38+877.0 | 38+970.0 | | | 790 | 93.0 | 4.9 | 1 |
| 38+970.0 | 39+040.0 | | | +0.41% | 70.0 | | |
| 38+970.0 | 39+040.0 | | | +0.41% | 70.0 | | |
| 39+040.0 | 39+127.2 | | | -0.51% | 87.2 | 13.9 | 1 |
| 39+040.0 | 39+105.0 | | | -0.62% | 65.0 | 6.5 | 1 |
| 39+105.0 | 39+117.3 | | | +0.40% | 12.3 | | |
| 39+196.5 | 39+325.0 | | | 790 | 128.5 | 5.7 | 1 |
| 39+325.0 | 39+450.0 | | | 790 | 125.0 | 5.4 | 1 |
| 39+370.0 | 39+505.0 | | | 790 | 135.0 | | |
| 39+450.0 | 39+520.0 | | | 790 | 70.0 | | |
| 39+520.0 | 39+540.0 | | | -0.40% | 20.0 | | |
| 39+540.0 | 39+580.0 | | | +0.42% | 40.0 | | |
| 39+580.0 | 39+650.0 | | | 790 | 70.0 | | |
| 39+603.0 | 39+750.0 | | | 790 | 147.0 | | |
| 39+604.9 | 39+740.0 | | | 790 | 135.1 | 5.6 | 1 |
| 39+604.9 | 39+740.0 | 790 | | | 135.1 | | |
| 39+650.0 | 39+775.0 | | | 790 | 125.0 | 5.6 | 1 |
| 39+740.0 | 39+876.8 | | | 790 | 136.8 | 5.6 | 1 |
| 39+740.0 | 39+876.8 | 790 | | | 136.8 | 5.6 | 1 |
| 39+750.0 | 39+876.8 | | | 790 | 126.8 | 6.1 | 1 |
| 39+775.0 | 39+876.8 | | | 790 | 101.8 | 5.6 | 1 |
| 39+952.5 | 40+045.0 | | | 790 | 92.6 | 5.2 | 1 |
| 39+952.5 | 40+100.0 | | | 790 | 147.6 | 5.6 | 1 |
| 39+952.5 | 40+045.0 | | | 790 | 92.6 | 6.0 | 1 |
| 39+952.5 | 40+100.0 | 790 | | | 147.6 | 6.3 | 1 |
| 40+100.0 | 40+250.0 | | | 790 | 150.0 | 5.5 | 1 |
| 40+100.0 | 40+200.0 | 790 | | | 100.0 | 8.6 | 1 |
| 40+200.0 | 40+350.0 | 790 | | | 150.0 | | |
| 40+250.0 | 40+400.0 | | | 790 | 150.0 | 5.5 | 1 |
| 40+350.0 | 40+405.0 | 790 | | | 55.0 | | |
| 40+400.0 | 40+550.0 | | | 790 | 150.0 | 5.6 | 1 |
| 40+530.0 | 40+635.2 | -0.68% | | | 105.2 | 6.1 | 1 |
| 40+539.0 | 40+635.2 | | | 790 | 96.2 | 5.6 | 1 |
| 40+550.0 | 40+665.6 | | | 790 | 115.6 | 5.6 | 1 |
| 40+657.8 | 40+665.6 | | | 790 | 7.9 | 6.1 | 1 |
| TOTALS | | | | | 12149.9 | 500.0 | 83 |
| USE | | | | | 12150.0 | 500.0 | 83 |

** DOUBLE PIPE OUTLET

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION SCHEDULES OF QUANTITIES FAP 310 (IL 255) SECTION 60-15 MADISON COUNTY |
|-----------|------|---|
| NAME | DATE | |
| | | DRAWN BY EBB CHECKED BY DATE |
| | | |
| | | |
| | | |
| | | |

11/30/2009

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCHEDULES OF QUANTITIES

| | | | | |
|---------------------|---------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 25 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |

PIPE UNDERDRAIN SCHEDULE CONTINUED

| STATION TO STATION | DEPTH OR GRADE % | | PIPE UNDERDRAINS 100MM METER | PIPE UNDERDRAINS 100MM (SPECIAL) METER | CONCRETE HEADWALL FOR PIPE DRAINS EACH |
|---------------------------|------------------|---------|------------------------------|--|--|
| | LT SIDE | RT SIDE | | | |
| RAMP 3 1+615.8 1+710.6 | | 790 | 94.8 | | |
| RAMP A | | | | | |
| 0+048.9 0+104.0 | 800 | | 55.1 | | |
| 0+104.0 0+203.0 | 800 | | 99.0 | 4.9 | 1 ** |
| 0+203.0 0+228.9 | | +0.40% | 25.9 | 5.7 | 1 |
| 0+228.9 0+255.0 | | -0.40% | 26.1 | | |
| 0+255.0 0+350.0 | 800 | | 95.0 | 5.4 | 1 |
| 0+350.0 0+500.0 | 790 | | 150.0 | 5.6 | 1 |
| 0+500.0 0+650.0 | 790 | | 150.0 | 5.4 | 1 |
| 0+650.0 0+703.1 | 790 | | 53.1 | | |
| RAMP B | | | | | |
| 0+036.8 0+195.0 | 800 | | 158.2 | 5.7 | 1 |
| 0+195.0 0+310.0 | 800 | | 115.0 | 5.7 | 1 |
| 0+310.0 0+351.3 | | +0.43% | 41.3 | | |
| 0+351.3 0+393.0 | | -0.40% | 41.7 | 5.8 | 1 |
| 0+393.0 0+445.0 | -0.97% | | 52.0 | 4.1 | 1 ** |
| 0+445.0 0+475.0 | +0.40% | | 30.0 | | |
| 0+475.0 0+546.2 | 800 | | 71.2 | | |
| HUMBERT RAMP C | | | | | |
| 0+043.1 0+160.0 | 800 | | 116.9 | 5.7 | 1 |
| 0+160.0 0+275.7 | 800 | | 115.7 | 4.6 | 1 |
| 0+275.7 0+500.0 | 790 | | 149.8 | 5.1 | 1 |
| 0+500.0 0+508.7 | 790 | | 8.7 | | |
| HUMBERT RAMP D | | | | | |
| 0+089.2 0+120.0 | +0.49% | | 30.8 | | |
| 0+120.0 0+240.0 | 800 | | 120.0 | 4.4 | 1 |
| 0+240.0 0+378.9 | 800 | | 138.9 | 4.0 | 1 |
| RAMP F | | | | | |
| 0+040.5 0+060.0 | 800 | | 19.5 | | |
| 0+060.0 0+080.0 | -0.41% | | 20.0 | 3.9 | 1 ** |
| 0+080.0 0+095.0 | +0.43% | | 15.0 | | |
| 0+095.0 0+174.0 | 800 | | 79.0 | | |
| 0+174.0 0+290.0 | 800 | | 116.0 | 5.7 | 1 |
| 0+290.0 0+335.0 | -0.59% | | 45.0 | | |
| 0+335.0 0+370.0 | +0.40% | | 35.0 | 14.4 | 1 ** |
| 0+370.0 0+439.0 | 800 | | 69.0 | | |
| RAMP E | | | | | |
| 0+000.0 0+036.0 | 790 | | 36.0 | 5.4 | 1 ** |
| 0+036.0 0+135.0 | 790 | | 99.1 | | |
| 0+135.0 0+171.8 | | +0.44% | 36.8 | | |
| 0+171.8 0+275.0 | 800 | | 103.2 | 5.7 | 1 |
| 0+275.0 0+376.9 | 800 | | 101.9 | 5.7 | 1 |
| IL 111 RAMP D | | | | | |
| 0+040.8 0+055.0 | +0.40% | | 14.2 | 4.3 | 1 |
| 0+055.0 0+165.0 | 800 | | 110.0 | | |
| 0+165.0 0+230.0 | 800 | | 65.0 | 4.2 | 1 |
| 0+230.0 0+261.8 | +0.42% | | 31.8 | | |
| 0+261.8 0+320.0 | -0.40% | | 58.2 | | |
| 0+320.0 0+411.4 | 800 | | 91.4 | | |
| 0+411.4 0+762.5 | 790 | | 118.8 | | |
| IL 111 RAMP C | | | | | |
| 0+000.0 0+050.0 | +0.40% | | 50.0 | | |
| 0+050.0 0+177.1 | 800 | | 127.1 | 5.6 | 1 |
| 0+177.1 0+300.0 | 800 | | 122.9 | 15.7 | 1 |
| 0+300.0 0+352.4 | +0.41% | | 52.4 | | |
| 0+352.4 0+421.0 | -0.79% | | 68.6 | 5.7 | 1 |
| 0+421.0 0+515.0 | 800 | | 94.0 | | |
| 0+515.0 0+535.0 | -0.44% | | 20.0 | 4.0 | 1 ** |
| 0+535.0 0+579.5 | +0.58% | | 44.5 | | |
| TOTALS | | | 3783.4 | 152.4 | 26 |
| USE | | | 3783.5 | 152.5 | 26 |

** DOUBLE PIPE OUTLET

AGGREGATE SHOULDERS, TYPE B

| STATION TO STATION | SIDE | WIDTH | M TON |
|--------------------|------|---------|---------|
| FAP 310 | | | |
| 35+137.8 35+143.8 | LT | 0.0-1.0 | 2.3 |
| 35+143.8 35+528.0 | LT | 1.0 | 243.0 |
| 35+528.0 38+650.7 | RT | 0.6 | 2302.3 |
| 35+528.0 38+798.8 | LT | 0.6 | 2322.0 |
| 35+528.0 38+825.0 | RT | 1.0 | 130.1 |
| 35+528.0 38+825.0 | RT | 0.0-1.0 | 3.1 |
| 35+528.0 38+825.0 | RT | 0.0 | 924.1 |
| 35+528.0 38+825.0 | RT | 0.0-1.0 | 3.6 |
| 35+528.0 38+825.0 | LT | 0.0 | 939.4 |
| 38+452.1 38+616.3 | RT | 0.0 | 56.1 |
| 38+452.1 38+742.3 | LT | 0.0 | 61.9 |
| 38+452.1 38+742.3 | RT | 0.0-1.0 | 3.3 |
| 38+452.1 38+742.3 | RT | 1.0 | 120.4 |
| 38+452.1 38+742.3 | RT | 0.6-1.0 | 1.8 |
| 38+452.1 38+742.3 | RT | 1.0 | 120.6 |
| 38+452.1 38+742.3 | LT | 0.0-1.0 | 0.8 |
| 38+452.1 38+742.3 | LT | 1.0 | 31.2 |
| 38+452.1 38+742.3 | LT | 1.0 | 119.0 |
| 38+452.1 38+742.3 | LT | 1.0 | 126.4 |
| 38+452.1 38+742.3 | RT | 0.6 | 68.4 |
| 38+452.1 38+742.3 | RT | 1.0 | 56.2 |
| 38+452.1 38+742.3 | RT | 0.0-1.0 | 0.8 |
| 38+452.1 38+742.3 | RT | 0.0 | 31.6 |
| 38+452.1 38+742.3 | RT | 0.6-1.0 | 1.7 |
| 38+452.1 38+742.3 | RT | 1.0 | 117.9 |
| 38+452.1 38+742.3 | LT | 0.6-1.0 | 1.9 |
| 38+452.1 38+742.3 | LT | 0.6 | 80.7 |
| 38+452.1 38+742.3 | LT | 0.0-1.0 | 3.3 |
| 38+452.1 38+742.3 | LT | 0.0 | 20.9 |
| 38+452.1 38+742.3 | LT | 1.0 | 98.6 |
| 38+452.1 38+742.3 | LT | 1.0 | 126.3 |
| 38+452.1 38+742.3 | RT | 0.6 | 338.4 |
| 38+452.1 38+742.3 | LT | 0.0-1.0 | 3.1 |
| 38+452.1 38+742.3 | LT | 0.6-1.0 | 1.9 |
| 38+452.1 38+742.3 | LT | 0.6 | 365.1 |
| 38+452.1 38+742.3 | LT | 0.0 | 21.6 |
| 38+452.1 38+742.3 | RT | 0.0 | 48.7 |
| 38+452.1 38+742.3 | LT | 0.0 | 57.1 |
| 38+452.1 38+742.3 | RT | 0.0 | 26.4 |
| 38+452.1 38+742.3 | RT | 0.0-1.0 | 3.6 |
| 38+452.1 38+742.3 | RT | 1.0 | 116.5 |
| 38+452.1 38+742.3 | RT | 0.6-1.0 | 1.8 |
| 38+452.1 38+742.3 | RT | 1.0 | 116.9 |
| 38+452.1 38+742.3 | LT | 0.0-1.0 | 0.8 |
| 38+452.1 38+742.3 | LT | 1.0 | 34.3 |
| 38+452.1 38+742.3 | RT | 1.0 | 32.0 |
| 38+452.1 38+742.3 | RT | 0.6 | 474.9 |
| 38+452.1 38+742.3 | LT | 1.0 | 116.9 |
| 38+452.1 38+742.3 | LT | 1.0 | 112.2 |
| 38+452.1 38+742.3 | RT | 0.0-1.0 | 0.8 |
| 38+452.1 38+742.3 | RT | 0.0 | 74.2 |
| 38+452.1 38+742.3 | LT | 0.6-1.0 | 1.8 |
| 38+452.1 38+742.3 | LT | 0.6 | 364.5 |
| 38+452.1 38+742.3 | LT | 0.0-1.0 | 14.6 |
| 38+452.1 38+742.3 | LT | 0.0 | 70.4 |
| 38+452.1 38+742.3 | LT | 0.0 | 36.3 |
| 38+452.1 38+742.3 | RT | 0.0 | 2.7 |
| SUBTOTAL | | | 10556.8 |

BUILDING REMOVAL

| NUMBER | STATION | OFFSET | SIDE | DESCRIPTION | L SUM |
|--------|----------|--------|------|---------------------|-------|
| 1 | 35+949.9 | 35.6 | LT | GARAGE | 1 |
| 2 | 36+572.3 | 32.9 | RT | SHED | * |
| 3 | 36+596.2 | 13.7 | RT | BARN | * |
| 4 | 36+609.9 | 4.8 | RT | GARAGE | * |
| 5 | 36+867.5 | 32.5 | LT | CONCRETE FOUNDATION | * |
| 6 | 38+882 | 130 | LT | BARN | * |
| 7 | 39+024 | 117 | RT | GARAGE | * |
| 8 | 40+233 | 29.6 | LT | COMMERCIAL | 1 |
| 9 | 40+417 | 47 | LT | GARAGE | 1 |
| 10 | 40+418 | 32 | LT | GARAGE | 1 |
| 11 | 40+426 | 27 | LT | SHED | * |
| 12 | 35+920 | 48 | LT | HOUSE | 1 |

* BUILDINGS REMOVED BY OTHERS

AGGREGATE SHOULDERS, TYPE B CONTINUED

| STATION TO STATION | SIDE | WIDTH | M TON |
|---------------------------|------|---------|---------|
| RAMP 3 1+615.8 1+710.6 | RT | 1.0 | 55.8 |
| RAMP A | | | |
| 0+024.3 0+322.4 | LT | 0.6 | 163.0 |
| 0+039.0 0+658.0 | RT | 0.6 | 406.7 |
| 0+658.0 0+688.0 | RT | 0.0-0.6 | 16.5 |
| 0+688.0 0+703.1 | RT | 0.0 | 6.0 |
| RAMP B | | | |
| 0+000.0 0+005.6 | RT | 0.0 | 1.9 |
| 0+005.6 0+016.6 | RT | 0.0-0.6 | 5.5 |
| 0+016.6 0+556.1 | RT | 0.6 | 351.3 |
| 0+200.1 0+570.8 | LT | 0.6 | 201.5 |
| HUMBERT RAMP C | | | |
| 0+027.7 0+274.1 | LT | 0.6 | 117.5 |
| 0+033.3 0+211.6 | RT | 0.6 | 102.9 |
| 0+211.6 0+214.0 | RT | 0.6-1.0 | 1.4 |
| 0+214.0 0+280.0 | RT | 1.0 | 43.9 |
| 0+347.7 0+420.3 | RT | 1.0 | 48.0 |
| 0+420.3 0+421.1 | RT | 0.6-1.0 | 0.5 |
| 0+421.1 0+448.8 | RT | 0.6 | 14.1 |
| 0+448.8 0+478.7 | RT | 0.0-0.6 | 11.6 |
| 0+478.7 0+508.7 | RT | 0.0 | 8.1 |
| HUMBERT RAMP D | | | |
| 0+083.6 0+133.1 | RT | 1.0 | 31.5 |
| 0+089.2 0+388.7 | LT | 0.6 | 169.7 |
| 0+133.1 0+133.9 | RT | 0.6-1.0 | 0.5 |
| 0+133.9 0+388.7 | RT | 0.6 | 137.6 |
| RAMP F | | | |
| 0+040.5 0+473.9 | RT | 0.6 | 286.9 |
| 0+040.5 0+318.8 | LT | 0.6 | 148.5 |
| 0+473.9 0+503.3 | RT | 0.0-0.6 | 14.6 |
| 0+503.3 0+533.9 | RT | 0.0 | 9.8 |
| RAMP E | | | |
| 0+000.0 0+008.7 | RT | 0.0 | 3.5 |
| 0+008.7 0+017.2 | RT | 0.0-0.6 | 4.7 |
| 0+017.2 0+376.9 | RT | 0.6 | 236.0 |
| 0+130.2 0+376.9 | LT | 0.6 | 138.0 |
| IL 111 RAMP D | | | |
| 0+040.8 0+717.5 | RT | 0.6 | 432.4 |
| 0+040.8 0+360.1 | LT | 0.6 | 183.8 |
| 0+717.5 0+747.5 | RT | 0.0-0.6 | 14.9 |
| 0+747.5 0+762.5 | RT | 0.0 | 5.2 |
| IL 111 RAMP C | | | |
| 0+000.0 0+005.6 | RT | 0.0 | 1.9 |
| 0+005.6 0+016.6 | RT | 0.0-0.6 | 5.5 |
| 0+016.6 0+579.5 | RT | 0.6 | 378.3 |
| 0+196.2 0+579.5 | LT | 0.6 | 209.6 |
| TOTAL | | | 14525.2 |
| USE | | | 14526 |

RATE:
AGGREGATE = 2.5 M TONS / CU M

SETTLEMENT PLATFORMS

| STATION | EACH |
|---------|------|
| FAP 310 | |
| 35+298 | 1 |
| TOTAL | 1 |

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION SCHEDULES OF QUANTITIES FAP 310 (IL 255) SECTION 60-15 MADISON COUNTY DRAWN BY EBB CHECKED BY |
|-----------|------|---|
| NAME | DATE | |
| | | DATE |
| | | |
| | | |
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SCHEDULES OF QUANTITIES

| | | | | |
|---------------------|---------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 27 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |

PAVEMENT MARKING SCHEDULE CONTINUED

| STATION TO STATION | SIDE | DESCRIPTION | THERMOPLASTIC PAVEMENT MARKING | | | | | PREFORMED PLASTIC PAVEMENT MARKING TYPE B - LINE 100MM METER | EPOXY PAVEMENT MARKING | | | | | | |
|--------------------|---------|-------------|--------------------------------|----------------------|------------------------------|---------------------|---------------------|--|-----------------------------|----------------------|------------------------------|---------------------|---------------------|------|------|
| | | | LETTERS AND SYMBOLS SQ M | YELLOW LINE 100MM | WHITE LINE 200MM METER | WHITE LINE 300MM | WHITE LINE 600MM | | LETTERS AND SYMBOLS SQ M | YELLOW LINE 100MM | WHITE LINE 200MM METER | WHITE LINE 300MM | WHITE LINE 600MM | | |
| RAMP 3 | | | | | | | | | | | | | | | |
| 1+615.8 | 1+710.6 | RT | | | 94.8 | | | | | | | | | | |
| RAMP A | | | | | | | | | | | | | | | |
| 0+015.4 | 0+019.7 | LT | | | | | | | | | | 13.4 | | | |
| 0+016.6 | 0+022.8 | RT | | | | | | | | | | 13.4 | | | |
| 0+018.4 | | LT | | | | | | | | | | 4.9 | | | |
| 0+048.9 | 0+703.1 | RT | | | 654.2 | | | | | | | | | | |
| 0+048.9 | 0+322.4 | LT | | 273.5 | | | | | | | | | | | |
| 0+322.4 | 0+472.5 | LT | | | | 150.2 | | | | | | | | | |
| RAMP B | | | | | | | | | | | | | | | |
| 0+000.0 | 0+546.2 | RT | | | 546.2 | | | | | | | | | | |
| 0+084.4 | 0+200.1 | LT | | | | 115.7 | | | | | | | | | |
| 0+200.1 | 0+546.2 | LT | | 346.1 | | | | | | | | | | | |
| 0+446.2 | 0+546.2 | LT | | | 100.0 | | | | | | | | | | |
| 0+546.2 | 0+575.0 | LT | | | | | | | | 28.9 | | | | | |
| 0+568.7 | | RT | | | | | | | 1.47 | | | | | | |
| 0+570.0 | | LT | | | | | | | 1.47 | | | | | | |
| 0+570.0 | | LT & RT | | | | | | | 1.47 | | | | | | |
| 0+573.0 | 0+579.0 | RT | | | | | | | | | | 13.4 | | | |
| 0+573.4 | | RT | | | | | | | | | | 4.9 | | | |
| 0+575.3 | | LT & RT | | | | | | | | | | 7.2 | | | |
| 0+578.3 | 0+580.1 | LT & RT | | | | | | | | | | 19.8 | | | |
| HUMBERT RAMP C | | | | | | | | | | | | | | | |
| 0+043.1 | 0+275.7 | RT | | | 232.6 | | | | | | | | | | |
| 0+043.1 | 0+274.4 | LT | | 231.3 | | | | | | | | | | | |
| 0+274.4 | 0+348.2 | LT | | | | | | | | | | 73.7 | | | |
| 0+275.7 | 0+350.2 | RT | | | | | | | | 74.5 | | | | | |
| 0+348.2 | 0+443.8 | LT | | | 95.7 | | | | | | | | | | |
| 0+350.2 | 0+508.7 | RT | | | 158.5 | | | | | | | | | | |
| HUMBERT RAMP D | | | | | | | | | | | | | | | |
| 0+000.0 | 0+003.1 | RT | | | 3.1 | | | | | | | | | | |
| 0+003.1 | 0+087.4 | RT | | | | | | | | 84.2 | | | | | |
| 0+016.1 | 0+084.9 | LT | | | | | | | | | | 68.8 | | | |
| 0+084.9 | 0+087.4 | LT | | | | 2.5 | | | | | | | | | |
| 0+087.4 | 0+378.9 | RT | | | 291.5 | | | | | | | | | | |
| 0+089.2 | 0+378.9 | LT | | 289.7 | | | | | | | | | | | |
| 0+400.7 | | LT | | | | | | | | | | 4.9 | | | |
| 0+401.2 | | LT & RT | | | | | | | | | | 4.9 | | | |
| 0+401.3 | 0+406.3 | LT & RT | | | | | | | | | | 13.4 | | | |
| 0+401.9 | 0+405.4 | LT | | | | | | | | | | 14.6 | | | |
| RAMP F | | | | | | | | | | | | | | | |
| 0+040.5 | 0+533.9 | RT | | | 493.4 | | | | | | | | | | |
| 0+040.5 | 0+318.8 | LT | | 278.3 | | | | | | | | | | | |
| 0+318.8 | 0+469.0 | LT | | | | 150.2 | | | | | | | | | |
| RAMP E | | | | | | | | | | | | | | | |
| 0+000.0 | 0+376.9 | RT | | | 376.9 | | | | | | | | | | |
| 0+130.2 | 0+376.9 | LT | | 246.7 | | | | | | | | | | | |
| 0+400.2 | | LT | | | | | | | | | | 5.5 | | | |
| 0+401.3 | | RT | | | | | | | | | | 5.5 | | | |
| IL 111 RAMP D | | | | | | | | | | | | | | | |
| 0+040.8 | 0+762.5 | RT | | | 721.7 | | | | | | | | | | |
| 0+040.8 | 0+360.1 | LT | | 319.4 | | | | | | | | | | | |
| 0+360.1 | 0+532.7 | LT | | | | 172.6 | | | | | | | | | |
| IL 111 RAMP C | | | | | | | | | | | | | | | |
| 0+000.0 | 0+579.5 | RT | | | 579.5 | | | | | | | | | | |
| 0+083.2 | 0+196.2 | LT | | | 113.1 | | | | | | | | | | |
| 0+196.2 | 0+579.5 | LT | | 383.3 | | | | | | | | | | | |
| SUBTOTALS | | | 0.00 | 2368.3 | 4252.4 | 800.0 | 0.0 | 11.0 | 0.0 | 4.41 | 0.0 | 187.6 | 142.5 | 88.0 | 26.8 |

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION SCHEDULES OF QUANTITIES FAP 310 (IL 255) SECTION 60-15 MADISON COUNTY |
|-----------|------|---|
| NAME | DATE | |
| | | DRAWN BY EBB CHECKED BY DATE |
| | | |
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| | | |

6/28/2007

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCHEDULES OF QUANTITIES

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------------|---------------------------|--------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 28 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |

PAVEMENT MARKING SCHEDULE CONTINUED

| STATION TO STATION | SIDE | DESCRIPTION | THERMOPLASTIC PAVEMENT MARKING | | | | | PREFORMED PLASTIC PAVEMENT MARKING TYPE B - LINE 100MM METER | EPOXY PAVEMENT MARKING | | | | | | | | | | | | | | |
|--------------------|----------|-------------|--------------------------------|----------------------|------------------------------|---------------------|---------------------|--|-----------------------------|----------------------|------------------------------|---------------------|---------------------|-------|------|--|--|--|--|--|--|--|--|
| | | | LETTERS AND SYMBOLS SQ M | YELLOW LINE 100MM | WHITE LINE 200MM METER | WHITE LINE 300MM | WHITE LINE 600MM | | LETTERS AND SYMBOLS SQ M | YELLOW LINE 100MM | WHITE LINE 200MM METER | WHITE LINE 300MM | WHITE LINE 600MM | | | | | | | | | | |
| HUMBERT ROAD | | | | | | | | | | | | | | | | | | | | | | | |
| 19+762.7 | 19+898.1 | RT | | | | | | | | | | | | | | | | | | | | | |
| 19+820.0 | 19+875.2 | RT | | | | | | | | | | | | | | | | | | | | | |
| 19+835.2 | | RT | | | | | | | | | | | | | | | | | | | | | |
| 19+860.0 | | RT | | | | | | | | | | | | | | | | | | | | | |
| 19+862.8 | 19+900.8 | LT | | | | | | | | | | | | | | | | | | | | | |
| 19+875.2 | 19+904.0 | RT | | | | | | | | | | | | | | | | | | | | | |
| 19+875.2 | 19+904.0 | RT | | | | | | | | | | | | | | | | | | | | | |
| 19+887.0 | 19+901.7 | LT | | | | | | | | | | | | | | | | | | | | | |
| 19+887.0 | 19+901.7 | LT | | | | | | | | | | | | | | | | | | | | | |
| 19+893.3 | | RT | | | | | | | | | | | | | | | | | | | | | |
| 19+894.5 | 19+896.3 | RT | | | | | | | | | | | | | | | | | | | | | |
| 19+894.5 | 19+896.3 | LT | | | | | | | | | | | | | | | | | | | | | |
| 19+903.0 | 19+910.3 | RT | | | | | | | | | | | | | | | | | | | | | |
| 19+905.6 | 19+907.4 | LT | | | | | | | | | | | | | | | | | | | | | |
| 19+910.3 | 19+930.0 | RT | | | | | | | | | | | | | | | | | | | | | |
| 19+914.0 | 19+971.0 | LT | | | | | | | | | | | | | | | | | | | | | |
| 19+914.0 | | LT | | | | | | | | | | | | | | | | | | | | | |
| 19+930.0 | 20+070.0 | LT & RT | | | | | | | | | | | | | | | | | | | | | |
| 19+937.6 | | LT | | | | | | | | | | | | | | | | | | | | | |
| 19+963.4 | | LT | | | | | | | | | | | | | | | | | | | | | |
| 20+029.0 | 20+089.0 | RT | | | | | | | | | | | | | | | | | | | | | |
| 20+035.4 | 20+092.1 | LT | | | | | | | | | | | | | | | | | | | | | |
| 20+036.6 | | RT | | | | | | | | | | | | | | | | | | | | | |
| 20+062.4 | | RT | | | | | | | | | | | | | | | | | | | | | |
| 20+078.5 | 20+102.0 | LT & RT | | | | | | | | | | | | | | | | | | | | | |
| 20+089.0 | | RT | | | | | | | | | | | | | | | | | | | | | |
| 20+092.1 | 20+095.7 | LT | | | | | | | | | | | | | | | | | | | | | |
| 20+093.2 | 20+112.5 | LT | | | | | | | | | | | | | | | | | | | | | |
| 20+095.4 | 20+099.9 | RT | | | | | | | | | | | | | | | | | | | | | |
| 20+100.6 | 20+137.6 | LT | | | | | | | | | | | | | | | | | | | | | |
| 20+103.7 | 20+118.9 | RT | | | | | | | | | | | | | | | | | | | | | |
| 20+103.7 | 20+118.9 | RT | | | | | | | | | | | | | | | | | | | | | |
| 20+107.1 | 20+143.1 | RT | | | | | | | | | | | | | | | | | | | | | |
| 20+109.5 | 20+112.1 | RT | | | | | | | | | | | | | | | | | | | | | |
| 20+111.0 | 20+114.3 | LT | | | | | | | | | | | | | | | | | | | | | |
| 20+117.0 | | LT | | | | | | | | | | | | | | | | | | | | | |
| IL 111 | | | | | | | | | | | | | | | | | | | | | | | |
| 29+828.5 | 29+867.5 | RT | | | | | | | | | | | | | | | | | | | | | |
| 29+828.5 | 29+872.7 | RT | | | | | | | | | | | | | | | | | | | | | |
| 29+872.2 | 29+877.3 | RT | | | | | | | | | | | | | | | | | | | | | |
| 29+877.3 | 29+897.5 | RT | | | | | | | | | | | | | | | | | | | | | |
| 29+897.5 | 29+947.5 | LT | | | | | | | | | | | | | | | | | | | | | |
| 29+897.5 | | LT | | | | | | | | | | | | | | | | | | | | | |
| 29+912.5 | | LT | | | | | | | | | | | | | | | | | | | | | |
| 29+932.5 | | LT | | | | | | | | | | | | | | | | | | | | | |
| 30+057.5 | 30+073.0 | RT | | | | | | | | | | | | | | | | | | | | | |
| 30+057.5 | 30+073.0 | RT | | | | | | | | | | | | | | | | | | | | | |
| 30+073.0 | | RT | | | | | | | | | | | | | | | | | | | | | |
| 30+080.7 | 30+082.1 | RT | | | | | | | | | | | | | | | | | | | | | |
| 30+086.9 | 30+124.9 | RT | | | | | | | | | | | | | | | | | | | | | |
| 30+087.0 | 30+099.4 | RT | | | | | | | | | | | | | | | | | | | | | |
| 30+087.0 | 30+099.4 | RT | | | | | | | | | | | | | | | | | | | | | |
| TOTALS | | | 2.94 | 12954.1 | 12884.4 | 1805.3 | 189.4 | 21.8 | 2343.6 | 13.23 | 810.5 | 1157.6 | 398.9 | 262.2 | 57.2 | | | | | | | | |
| USE | | | 3 | 25839 | | 1806 | 190 | 22 | 2344 | 14 | 1969 | 399 | 262 | 58 | | | | | | | | | |

PAVEMENT MARKING REMOVAL

| STATION TO STATION | SIDE | DESCRIPTION | SQ M | |
|--------------------|----------|-------------|------------------------------------|------|
| HUMBERT ROAD | | | | |
| 19+762.7 | 19+910.3 | RT | 100MM SOLID WHITE PAINT EDGE LINE | 14.8 |
| 19+762.7 | 19+885.6 | RT | 200MM SOLID WHITE PAINT DIAGONALS | 10.9 |
| 19+868.9 | 19+907.4 | LT | 100MM SOLID WHITE PAINT EDGE LINE | 3.9 |
| 19+892.7 | 19+917.1 | RT | 100MM SKIP-DASH WHITE PAINT | 0.4 |
| 19+894.2 | 19+914.6 | LT | 100MM SKIP-DASH WHITE PAINT | 0.4 |
| 19+930.0 | 20+025.1 | LT | 100MM SOLID YELLOW PAINT EDGE LINE | 9.5 |
| 19+930.0 | 20+025.1 | LT | 200MM SOLID YELLOW PAINT DIAGONALS | 8.0 |
| 19+971.0 | 20+070.0 | RT | 100MM SOLID YELLOW PAINT EDGE LINE | 9.9 |
| 19+971.0 | 20+070.0 | RT | 200MM SOLID YELLOW PAINT DIAGONALS | 7.4 |
| 20+042.3 | 20+131.4 | LT | 100MM SOLID WHITE PAINT EDGE LINE | 8.9 |
| 20+086.1 | 20+117.6 | LT | 100MM SKIP-DASH WHITE PAINT | 0.7 |
| 20+088.4 | 20+111.7 | RT | 100MM SKIP-DASH WHITE PAINT | 0.7 |
| 20+095.4 | 20+143.1 | RT | 100MM SOLID WHITE PAINT EDGE LINE | 4.8 |
| IL 111 | | | | |
| 29+828.5 | 29+855.0 | RT | 100MM SOLID WHITE PAINT EDGE LINE | 2.7 |
| 29+897.5 | 30+007.5 | LT | 100MM SOLID YELLOW PAINT EDGE LINE | 11.0 |
| 29+897.5 | 30+007.5 | LT | 200MM SOLID YELLOW PAINT DIAGONALS | 13.5 |
| TOTAL | | | 107.4 | |
| USE | | | 108 | |

PAINT PAVEMENT MARKING - LINE 100MM

| STATION TO STATION | SIDE | DESCRIPTION | YELLOW METER | |
|--------------------|----------|-------------|----------------|-------|
| FAP 310 | | | | |
| 38+883.0 | 39+505.0 | RT | CHANNELIZATION | 622.0 |
| RAMP E | | | | |
| 0+000.0 | 0+130.2 | LT | CHANNELIZATION | 130.2 |
| TOTAL | | | 752.2 | |
| USE | | | 753 | |

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION SCHEDULES OF QUANTITIES FAP 310 (IL 255) SECTION 60-15 MADISON COUNTY |
|-----------|------|---|
| NAME | DATE | |
| | | DRAWN BY EBB CHECKED BY DATE |
| | | |
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SCHEDULES OF QUANTITIES

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------------|---------------------------|--------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 29 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |

HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH)

| STATION TO STATION | SIDE | WIDTH | 390MM | | 400MM | |
|--------------------|------------|---------|---------|---------|---------|--|
| | | | SQ M | | SQ M | |
| FAP 310 | | | | | | |
| 35+150.000 | 38+790.610 | RT | 7.2 | 26212.4 | | |
| 35+150.000 | 38+789.302 | LT | 7.2 | 26203.0 | | |
| 38+871.897 | 39+117.295 | LT | 7.2 | 1766.9 | | |
| 38+874.979 | 39+124.810 | RT | 7.2 | 1798.8 | | |
| 38+943.322 | 39+108.322 | LT | 0.3-3.6 | 321.8 | | |
| 39+108.322 | 39+115.115 | LT | 3.6 | 24.5 | | |
| 39+193.501 | 39+410.000 | LT | 3.6 | 779.4 | | |
| 39+196.493 | 39+876.770 | LT | 7.2 | 4898.0 | | |
| 39+201.033 | 39+876.770 | RT | 7.2 | 4865.3 | | |
| 39+370.000 | 39+602.068 | RT | 3.6 | 835.4 | | |
| 39+602.068 | 39+767.068 | RT | 0.3-3.6 | 321.8 | | |
| 39+952.450 | 40+665.645 | RT | 7.2 | 5135.0 | | |
| 39+952.450 | 40+635.165 | LT | 7.2 | 4915.5 | | |
| RAMP 3 | | | | | | |
| 1+615.833 | 1+710.585 | LT | 0.3-2.2 | 118.2 | | |
| RAMP A | | | | | | |
| 0+048.889 | 0+412.498 | LT | 4.9 | | 1781.7 | |
| 0+412.498 | 0+703.081 | LT | 0.3-6.1 | 929.9 | | |
| RAMP B | | | | | | |
| 0+000.000 | 0+119.995 | LT | 0.3-6.8 | 428.4 | | |
| 0+119.995 | 0+388.220 | LT | 4.9 | | 1314.3 | |
| 0+388.220 | 0+446.220 | LT & RT | 4.9-7.2 | | 350.9 | |
| 0+446.220 | 0+546.220 | LT & RT | 7.2 | | 720.0 | |
| HUMBERT RAMP C | | | | | | |
| 0+043.131 | 0+275.682 | LT | 4.9 | | 1139.5 | |
| 0+350.179 | 0+383.910 | LT | 4.9 | | 165.3 | |
| 0+383.910 | 0+508.671 | LT | 3.6-6.1 | 605.1 | | |
| HUMBERT RAMP D | | | | | | |
| 0+000.000 | 0+003.148 | LT | 0.0-0.2 | 0.3 | | |
| 0+087.382 | 0+378.864 | LT | 4.9 | | 1428.3 | |
| RAMP F | | | | | | |
| 0+040.493 | 0+408.894 | LT | 4.9 | | 1805.2 | |
| 0+408.894 | 0+533.866 | LT | 3.6-6.1 | 606.1 | | |
| RAMP E | | | | | | |
| 0+000.000 | 0+095.441 | LT | 0.3-6.7 | 334.0 | | |
| 0+095.441 | 0+376.895 | LT | 4.9 | | 1379.1 | |
| IL 111 RAMP D | | | | | | |
| 0+040.791 | 0+472.771 | LT | 4.9 | | 2116.7 | |
| 0+472.771 | 0+762.532 | LT | 0.3-6.1 | 927.2 | | |
| IL 111 RAMP C | | | | | | |
| 0+000.000 | 0+117.939 | LT | 0.3-6.8 | 421.0 | | |
| 0+117.939 | 0+579.503 | LT | 4.9 | | 2261.7 | |
| TOTALS | | | | 82448.0 | 14462.6 | |
| USE | | | | 82448 | 14463 | |

HOT-MIX ASPHALT SHOULDERS, 200MM & SUB-BASE GRANULAR MATERIAL, TYPE C

| STATION TO STATION | SIDE | WIDTH | HMA SHOULDERS | | SUB-BASE MATERIAL | |
|--------------------|------------|-------|---------------|---------|-------------------|--|
| | | | SQ M | | M TON | |
| FAP 310 | | | | | | |
| 35+150.000 | 38+753.876 | RT | 1.8 | 6487.0 | 3365.9 | |
| 35+150.000 | 38+778.906 | LT | 1.8 | 6532.0 | 3386.6 | |
| 35+150.000 | 38+140.000 | LT | 3.0 | 8970.0 | 4429.5 | |
| 35+244.733 | 38+252.000 | RT | 3.0 | 9021.8 | 4478.1 | |
| 38+372.083 | 38+452.050 | RT | 1.9-6.3 | 329.5 | 159.8 | |
| 38+432.900 | 38+523.651 | LT | 1.2-4.8 | 272.3 | 132.7 | |
| 38+452.050 | 38+774.527 | RT | 3.0 | 967.4 | 410.5 | |
| 38+523.651 | 38+750.010 | LT | 3.0 | 679.1 | 292.6 | |
| 38+750.010 | 38+788.484 | LT | 3.0-4.0 | 134.1 | 68.4 | |
| 38+753.876 | 38+790.402 | RT | 1.8-2.6 | 81.3 | 42.4 | |
| 38+778.906 | 38+789.462 | LT | 1.8-1.9 | 19.7 | 10.5 | |
| 38+774.527 | 38+791.446 | RT | 3.0-3.3 | 52.9 | 27.3 | |
| 38+869.953 | 38+900.753 | LT | 3.0-3.6 | 102.0 | 51.9 | |
| 38+872.282 | 38+890.112 | LT | 1.8-2.1 | 34.8 | 18.6 | |
| 38+874.485 | 38+910.064 | RT | 1.8-2.6 | 78.4 | 41.3 | |
| 38+876.967 | 38+885.037 | RT | 3.0-3.1 | 24.7 | 12.6 | |
| 38+890.112 | 39+106.908 | LT | 1.8 | 390.2 | 210.6 | |
| 39+106.908 | 39+117.885 | LT | 1.8-1.9 | 20.4 | 10.9 | |
| 38+885.037 | 39+128.149 | RT | 3.0 | 729.3 | 374.7 | |
| 38+900.753 | 38+943.322 | LT | 3.0 | 127.7 | 65.6 | |
| 38+910.064 | 39+080.231 | RT | 1.8 | 306.3 | 165.3 | |
| 38+943.322 | 38+988.360 | LT | 1.8-2.7 | 101.3 | 53.4 | |
| 38+988.360 | 39+108.322 | LT | 1.8 | 215.9 | 116.5 | |
| 39+080.231 | 39+124.023 | RT | 1.8-2.6 | 97.2 | 50.9 | |
| 39+108.322 | 39+114.040 | LT | 5.2-6.3 | 32.8 | 16.0 | |
| 39+190.733 | 39+410.000 | LT | 3.0 | 657.8 | 338.0 | |
| 39+197.303 | 39+208.147 | LT | 1.8-1.9 | 20.2 | 10.8 | |
| 39+208.147 | 39+876.770 | LT | 1.8 | 1203.5 | 605.9 | |
| 39+199.706 | 39+240.000 | RT | 1.8-3.1 | 97.9 | 51.6 | |
| 39+204.202 | 39+241.839 | RT | 1.2-1.9 | 59.1 | 28.4 | |
| 39+240.000 | 39+876.770 | RT | 1.8 | 1146.2 | 577.4 | |
| 39+370.000 | 39+505.000 | RT | 3.0 | 405.0 | 208.1 | |
| 39+531.881 | 39+621.741 | LT | 1.2-4.8 | 269.6 | 131.4 | |
| 39+602.068 | 39+636.755 | RT | 1.8-4.9 | 116.5 | 56.6 | |
| 39+621.741 | 39+876.770 | LT | 3.0 | 765.1 | 306.2 | |
| 39+636.755 | 39+722.379 | RT | 1.8 | 154.1 | 71.2 | |
| 39+722.379 | 39+767.068 | RT | 1.8-2.7 | 100.6 | 43.3 | |
| 39+767.068 | 39+876.770 | RT | 3.0 | 329.1 | 131.7 | |
| 39+952.450 | 40+260.203 | RT | 3.0 | 923.3 | 435.7 | |
| 39+952.450 | 40+665.645 | RT | 1.8 | 1283.8 | 651.0 | |
| 39+952.450 | 40+635.165 | LT | 1.8 | 1228.9 | 631.0 | |
| 39+952.450 | 40+330.064 | LT | 3.0 | 1132.8 | 536.9 | |
| 40+260.203 | 40+370.566 | RT | 1.2-4.8 | 331.1 | 161.4 | |
| 40+330.064 | 40+410.215 | LT | 1.9-6.3 | 330.2 | 160.2 | |
| 40+530.000 | 40+635.165 | LT | 3.0 | 315.5 | 126.3 | |
| 40+657.776 | 40+665.645 | RT | 3.0 | 23.6 | 9.4 | |
| SUBTOTAL | | | | 46702.0 | 23265.1 | |

NOTE: WIDTH IS FOR HMA SHOULDERS, 200MM.

HOT-MIX ASPHALT SHOULDERS, 200MM & SUB-BASE GRANULAR MATERIAL, TYPE C CONTINUED

| STATION TO STATION | SIDE | WIDTH | HMA SHOULDERS | | SUB-BASE MATERIAL | |
|--------------------|-----------|-------|---------------|---------|-------------------|--|
| | | | SQ M | | M TON | |
| RAMP 3 | | | | | | |
| 1+615.833 | 1+665.558 | RT | 1.8 | 89.5 | 41.4 | |
| 1+665.558 | 1+710.585 | RT | 1.8-2.7 | 101.3 | 41.4 | |
| RAMP A | | | | | | |
| 0+048.889 | 0+657.980 | RT | 1.8 | 1096.4 | 577.0 | |
| 0+048.889 | 0+322.355 | LT | 1.2 | 328.2 | 191.5 | |
| 0+657.980 | 0+703.081 | RT | 1.8-2.7 | 101.5 | 53.5 | |
| RAMP B | | | | | | |
| 0+000.000 | 0+016.580 | RT | 1.8-2.7 | 37.3 | 18.0 | |
| 0+016.580 | 0+546.220 | RT | 1.8 | 953.4 | 493.3 | |
| 0+200.080 | 0+546.220 | LT | 1.2 | 415.4 | 245.9 | |
| HUMBERT RAMP C | | | | | | |
| 0+043.131 | 0+273.796 | RT | 1.8 | 415.2 | 224.7 | |
| 0+043.131 | 0+274.137 | LT | 1.2 | 277.2 | 167.5 | |
| 0+273.796 | 0+276.178 | RT | 1.8-1.9 | 4.4 | 2.5 | |
| 0+351.759 | 0+409.119 | RT | 1.8-3.8 | 160.1 | 85.9 | |
| 0+409.119 | 0+448.758 | RT | 1.8 | 71.4 | 38.5 | |
| 0+448.758 | 0+403.081 | RT | 1.8-3.0 | 143.8 | 75.3 | |
| HUMBERT RAMP D | | | | | | |
| 0+088.187 | 0+116.223 | RT | 1.8-2.5 | 60.6 | 33.6 | |
| 0+089.181 | 0+378.864 | LT | 1.2 | 347.6 | 195.5 | |
| 0+116.223 | 0+378.864 | RT | 1.8 | 472.8 | 269.2 | |
| RAMP F | | | | | | |
| 0+040.493 | 0+473.898 | RT | 1.8 | 780.1 | 424.2 | |
| 0+040.493 | 0+318.817 | LT | 1.2 | 334.0 | 197.2 | |
| 0+473.898 | 0+533.866 | RT | 1.8-3.0 | 143.9 | 75.3 | |
| RAMP E | | | | | | |
| 0+000.000 | 0+017.220 | RT | 1.8-3.0 | 41.3 | 21.6 | |
| 0+017.220 | 0+376.895 | RT | 1.8 | 647.4 | 327.4 | |
| 0+130.213 | 0+376.895 | LT | 1.2 | 296.0 | 178.8 | |
| IL 111 RAMP D | | | | | | |
| 0+040.791 | 0+717.505 | RT | 1.8 | 1218.1 | 667.3 | |
| 0+040.791 | 0+360.141 | LT | 1.2 | 383.2 | 208.2 | |
| 0+717.505 | 0+762.532 | RT | 1.8-2.7 | 101.3 | 43.6 | |
| IL 111 RAMP C | | | | | | |
| 0+000.000 | 0+016.580 | RT | 1.8-2.7 | 37.3 | 16.0 | |
| 0+016.580 | 0+579.503 | RT | 1.8 | 1013.3 | 562.3 | |
| 0+196.244 | 0+579.503 | LT | 1.2 | 459.9 | 272.3 | |
| TOTAL | | | | 57233.9 | 29014.0 | |
| USE | | | | 57234 | 29014 | |

NOTE: WIDTH IS FOR HMA SHOULDERS, 200MM.

QC/QA CONCRETE SCHEDULE

| PAY ITEM NUMBER | DESCRIPTION | PLAN QUANTITY | CONVERSION FACTOR | CU M |
|-----------------|---|---------------|-------------------|--------|
| 60246605 | MEDIAN INLET (604101) | 8 each | 0.36 cu m x each | 2.9 |
| 60900515 | CONCRETE THRUST BLOCKS | 6 each | 0.22 cu m x each | 1.3 |
| M4205000 | BRIDGE APPROACH PAVEMENT | 437 sq m | 0.385 cu m x sq m | 168.3 |
| M4205050 | BRIDGE APPROACH PAVEMENT (SPECIAL) | 1225 sq m | 0.385 cu m x sq m | 471.7 |
| M5110200 | SLOPE WALL 150MM | 293 sq m | 0.150 cu m x sq m | 44.0 |
| M5403000 | CONCRETE BOX CULVERTS | 2878.2 cu m | n/a | 2878.2 |
| M5429900 | CONCRETE HEADWALLS | 40.4 cu m | n/a | 40.4 |
| M542F252 | CAST-IN-PLACE REINFORCED CONCRETE END SECTIONS 1200MM | 2 each | 3.4 cu m x each | 6.8 |
| M6021511 | MANHOLES, TYPE A, 1.2M DIAMETER, WITH MEDIAN INLET (604101) | 2 each | 0.36 cu m x each | 0.8 |
| M6023105 | MANHOLES, SPECIAL, 1.5M DIAMETER | 1 each | 0.48 cu m x each | 0.5 |
| TOTAL | | | | 3614.9 |

WATER LINE RELOCATION SCHEDULE

| PAY ITEM NUMBER | DESCRIPTION | PLAN QUANTITY |
|-----------------|--|---------------|
| M2080150 | TRENCH BACKFILL | 430 CU M |
| M5930100 | CONTROLLED LOW-STRENGTH MATERIAL | 2 CU M |
| | 100mm PVC C-900 WATERLINE | 87 M |
| | 100mm CERTA-LOC IN 400mm STEEL CASING (OPEN CUT) | 70 M |

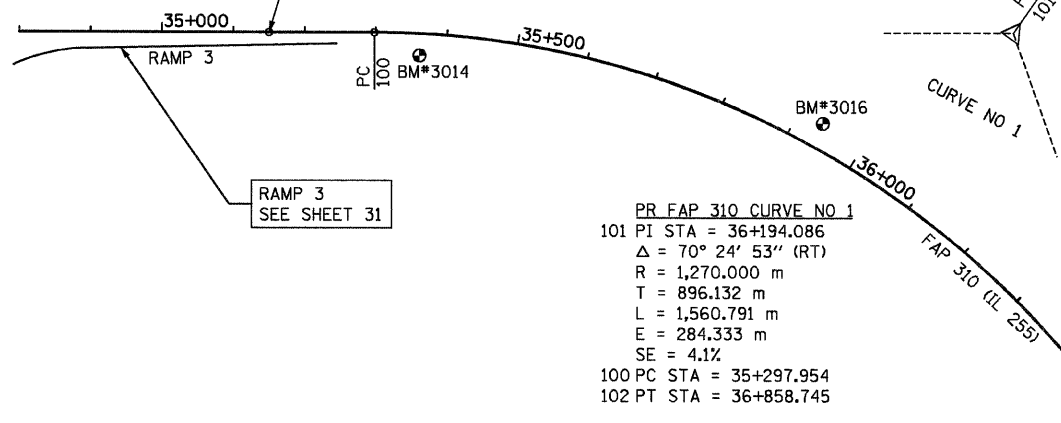
| REVISIONS | |
|-----------|------|
| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
SCHEDULES OF QUANTITIES
FAP 310 (IL 255)
SECTION 60-15
MADISON COUNTY
 DRAWN BY EBB
 CHECKED BY
 DATE

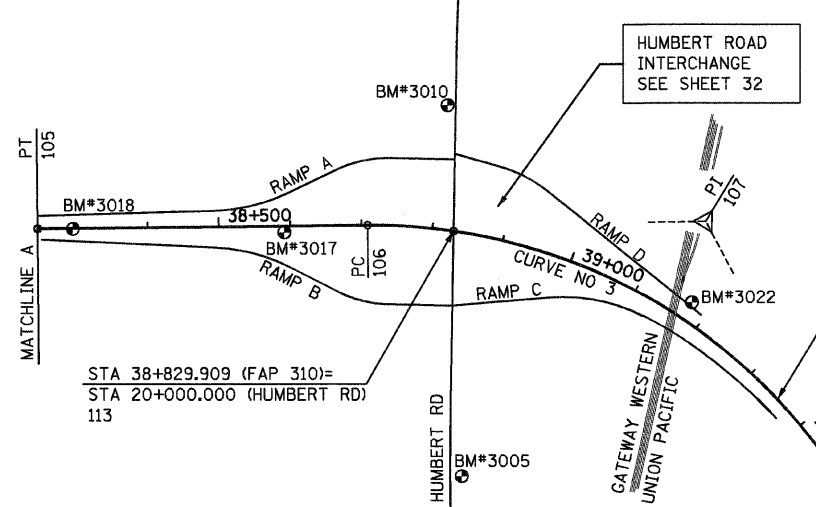
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------------|---------------------------|--------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 30 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |

| CONTROL POINTS | | |
|----------------|--------------|--------------|
| POINT | NORTHING | EASTING |
| 100 | 253,215.1909 | 701,492.4699 |
| 101 | 253,215.1990 | 700,596.3380 |
| 102 | 254,059.4870 | 700,295.9546 |
| 103 | 254,455.9892 | 700,154.8859 |
| 104 | 254,980.1650 | 699,968.3930 |
| 105 | 254,974.4927 | 699,412.0589 |
| 106 | 254,969.7738 | 698,949.2235 |
| 107 | 254,964.9360 | 698,474.7360 |
| 108 | 255,384.0772 | 698,252.2896 |
| 109 | 255,847.4601 | 698,006.3632 |
| 110 | 256,032.0913 | 697,908.3758 |
| 111 | 256,161.6866 | 697,744.3781 |
| 112 | 253,215.1896 | 701,640.4235 |
| 113 | 254,977.8964 | 698,828.7196 |
| 114 | 255,695.9215 | 698,086.7876 |
| 115 | 256,265.4685 | 697,613.0463 |

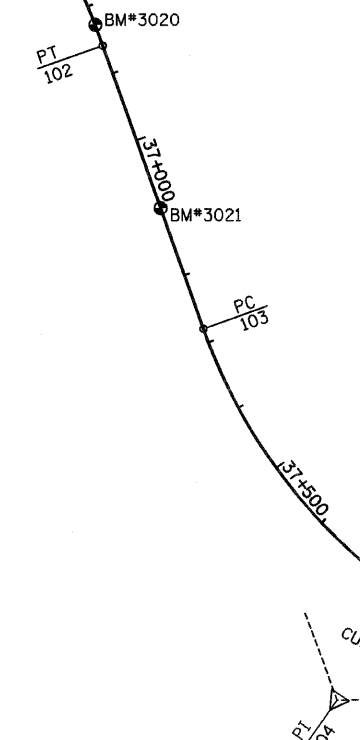
BEGIN PROJECT (FAP 310)
STA 35+150.000
112



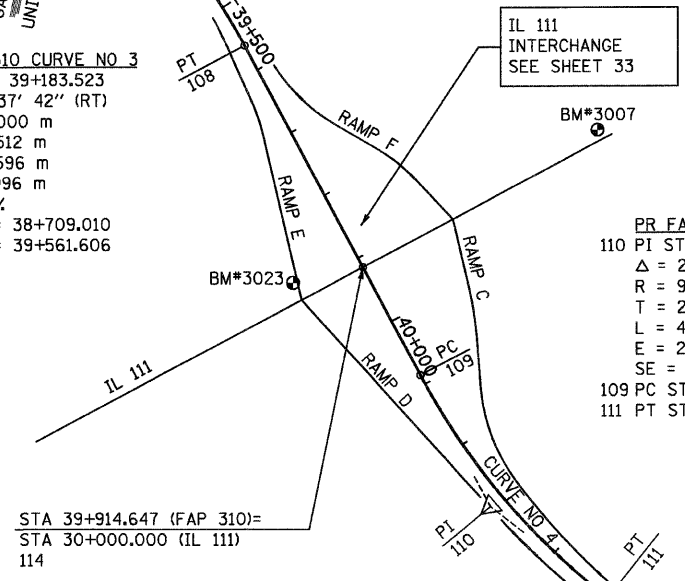
PR FAP 310 CURVE NO 1
101 PI STA = 36+194.086
 $\Delta = 70^\circ 24' 53''$ (RT)
R = 1,270.000 m
T = 896.132 m
L = 1,560.791 m
E = 284.333 m
SE = 4.1%
100 PC STA = 35+297.954
102 PT STA = 36+858.745



PR FAP 310 CURVE NO 3
107 PI STA = 39+183.523
 $\Delta = 62^\circ 37' 42''$ (RT)
R = 780.000 m
T = 474.512 m
L = 852.596 m
E = 132.996 m
SE = 5.5%
106 PC STA = 38+709.010
108 PT STA = 39+561.606



PR FAP 310 CURVE NO 2
104 PI STA = 37+835.958
 $\Delta = 70^\circ 59' 58''$ (LT)
R = 780.000 m
T = 556.363 m
L = 966.556 m
E = 178.092 m
SE = 5.5%
103 PC STA = 37+279.595
105 PT STA = 38+246.151

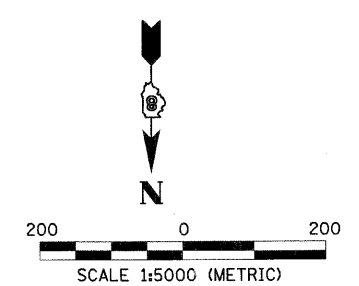


PR FAP 310 CURVE NO 4
110 PI STA = 40+295.227
 $\Delta = 23^\circ 43' 39''$ (LT)
R = 995.000 m
T = 209.022 m
L = 412.052 m
E = 21.718 m
SE = 4.8%
109 PC STA = 40+086.205
111 PT STA = 40+498.257

STA 39+914.647 (FAP 310)=
STA 30+000.000 (IL 111)
114

END PROJECT (FAP 310)
POT STA 40+665.645
115

| BENCHMARKS | | |
|------------|-----------|--|
| MARK | ELEVATION | DESCRIPTION |
| BM# 3007 | 188.205 | CUT "4" ON WEST FLANGE BOLT OF FIRE HYDRANT LOCATED AT SOUTHEAST CORNER OF THE INTERSECTION OF IL 111 & US 67 - IL 111 STA = 29+619.3, 15.1 m RT |
| BM# 3008 | 186.070 | CUT "4" ON WEST FLANGE BOLT ON FIRE HYDRANT LOCATED AT NORTHEAST CORNER INTERSECTION US 67 & INGHAM LANE - FAP 310 STA = 40+754.8, 8.5 m RT |
| BM# 3009 | 184.977 | 60d NAIL IN POWER POLE LOCATED ON WEST SIDE OF US 67 AND SOUTH OF MAILBOX 7406 |
| BM# 3010 | 187.953 | CUT "4" IN WEST FLANGE BOLT OF FIRE HYDRANT LOCATED ON EAST SIDE HUMBERT ROAD AND NORTH OF MAILBOX 6415 AT HUMBERT RD (ACROSS FROM CHURCH) - HUMBERT STA = 19+822.2, 11.2 m RT |
| BM# 3014 | 149.445 | RAILROAD SPIKE IN 30" DIA TREE LOCATED NEAR WEST EDGE OF FIELD - FAP 310 STA = 35+362.7, 30.8 m RT |
| BM# 3016 | 171.485 | 40d NAIL LIGHT POLE LOCATED NORTH OF HILGENDORF HOUSE, AND EAST OF METAL GARAGE - FAP 310 STA = 35+936.7, 34.8 m LT |
| BM# 3017 | 188.966 | RAILROAD SPIKE IN 12" DIA WALNUT - FAP 310 STA = 38+592.0, 8.7 m RT |
| BM# 3018 | 185.947 | RAILROAD SPIKE IN 8" DIA TREE - FAP 310 = STA 38+295.5, 0.8 m RT |
| BM# 3019 | 175.948 | RAILROAD SPIKE IN 12" DIA HICKORY - FAP 310 STA = 37+997.0, 1.0 m LT |
| BM# 3020 | 178.758 | RAILROAD SPIKE IN 12" DIA TREE - FAP 310 STA = 36+828.0, 0.5 m LT |
| BM# 3021 | 178.523 | RAILROAD SPIKE IN 20" DIA TREE - FAP 310 STA = 37+101.0, 0.0 m RT |
| BM# 3022 | 188.010 | RAILROAD SPIKE IN POWER POLE LOCATED ON WEST SIDE OF RAILROAD TRACKS BETWEEN RAMP D & FAP 310 - FAP 310 STA = 39+173.5, 27.0 m LT |
| BM# 3023 | 187.116 | CHSLD "X" ON WEST FLANGE BOLT OF FIRE HYDRANT LOCATED ON SOUTH SIDE OF IL 111 IN FRONT OF RISTER'S AUTO |

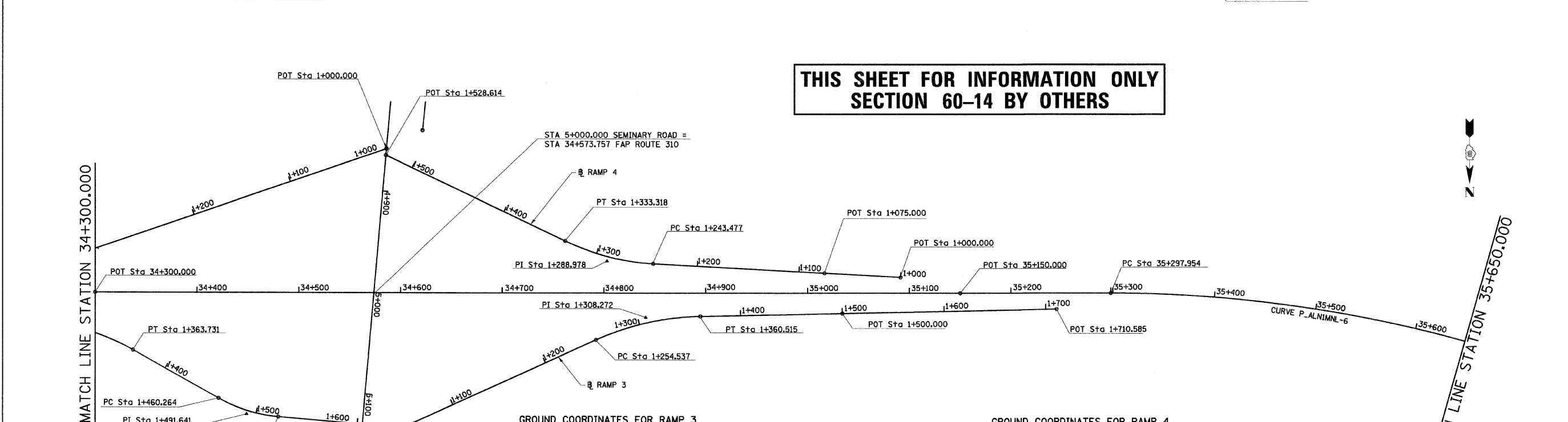
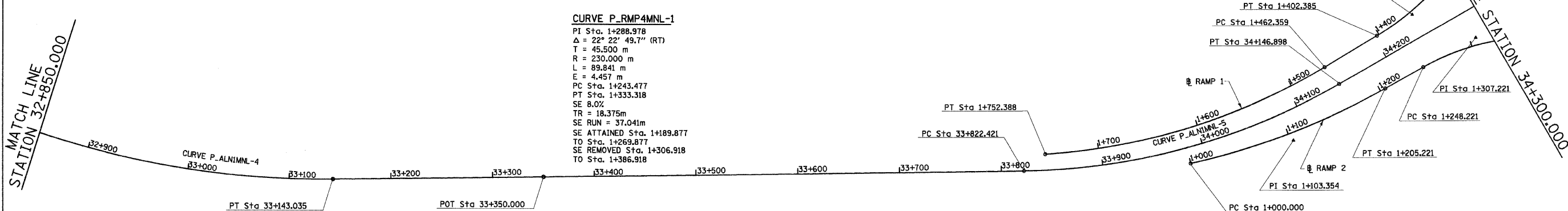


| REVISIONS | |
|-----------|------|
| NAME | DATE |
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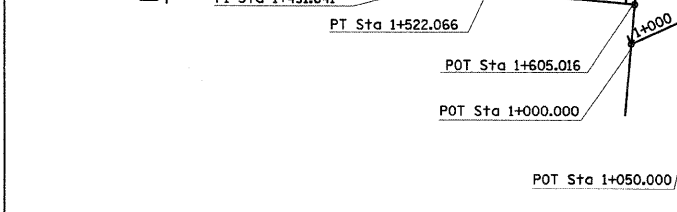
ILLINOIS DEPARTMENT OF TRANSPORTATION
**GENERAL LAYOUT &
HORIZONTAL CONTROL
FOR MAINLINE FAP 310 (IL 255)**

DATE _____ DRAWN BY BGJ
CHECKED BY _____

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------------|---------------------------|--------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 31 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| SECTION 60-14 | | CONTRACT NO. 76323 | | |

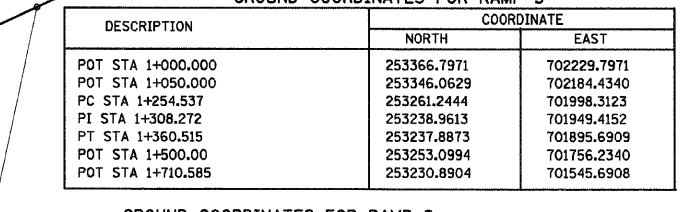


**THIS SHEET FOR INFORMATION ONLY
SECTION 60-14 BY OTHERS**



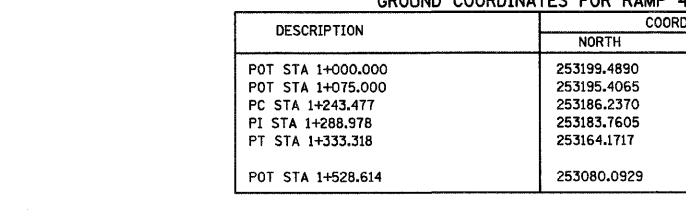
GROUND COORDINATES FOR RAMP 1

| DESCRIPTION | COORDINATE | |
|-------------------|-------------|-------------|
| | NORTH | EAST |
| POT STA 1+000.000 | 253073.7248 | 702204.2891 |
| PC STA 1+321.391 | 253178.3567 | 702508.1708 |
| PI STA 1+362.205 | 253191.6441 | 702546.7616 |
| PT STA 1+402.358 | 253192.4681 | 702587.5675 |
| PC STA. 1+462.359 | 253193.5414 | 702647.5582 |
| PI STA 1+610.213 | 253196.1861 | 702795.3885 |
| PT STA 1+752.388 | 253130.3823 | 702927.7920 |



GROUND COORDINATES FOR RAMP 3

| DESCRIPTION | COORDINATE | |
|-------------------|-------------|-------------|
| | NORTH | EAST |
| POT STA 1+000.000 | 253366.7971 | 702229.7971 |
| POT STA 1+050.000 | 253346.0629 | 702184.4340 |
| PC STA 1+254.537 | 253261.2444 | 701998.3123 |
| PI STA 1+308.272 | 253238.9613 | 701949.4152 |
| PT STA 1+360.515 | 253237.8873 | 701895.6909 |
| POT STA 1+500.000 | 253253.0994 | 701756.2340 |
| POT STA 1+710.585 | 253230.8904 | 701545.6908 |



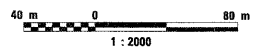
GROUND COORDINATES FOR RAMP 4

| DESCRIPTION | COORDINATE | |
|-------------------|-------------|-------------|
| | NORTH | EAST |
| POT STA 1+000.000 | 253199.4890 | 701698.9584 |
| POT STA 1+075.000 | 253195.4065 | 701773.8472 |
| PC STA 1+243.477 | 253186.2370 | 701942.0747 |
| PI STA 1+288.978 | 253183.7605 | 701987.5077 |
| PT STA 1+333.318 | 253164.1717 | 702028.5756 |
| POT STA 1+528.614 | 253080.0929 | 702204.8463 |



GROUND COORDINATES FOR FAP 310, (MAINLINE)

| DESCRIPTION | COORDINATE | |
|---------------------|-------------|-------------|
| | NORTH | EAST |
| PI STA 32+930.726 | 252693.5320 | 703739.7940 |
| PT STA 33+143.035 | 252801.8136 | 703546.6639 |
| POT STA 33+350.000 | 252903.0287 | 703366.1375 |
| PC STA 33+822.421 | 253134.0643 | 702954.0644 |
| PI STA 33+988.284 | 253215.1790 | 702809.3890 |
| PT STA 34+146.898 | 253215.1805 | 702643.5259 |
| POT STA 34+300.000 | 253215.1819 | 702490.4235 |
| POT STA. 34+573.757 | 253215.1844 | 702216.6665 |
| POT STA 35+150.000 | 253215.1896 | 701640.4235 |
| PC STA. 35+297.954 | 253215.1909 | 701492.4699 |
| PI STA 36+194.086 | 253215.1990 | 700596.3380 |
| PT STA 36+858.745 | 254059.4870 | 700295.9546 |



| REVISIONS | |
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| NAME | DATE |
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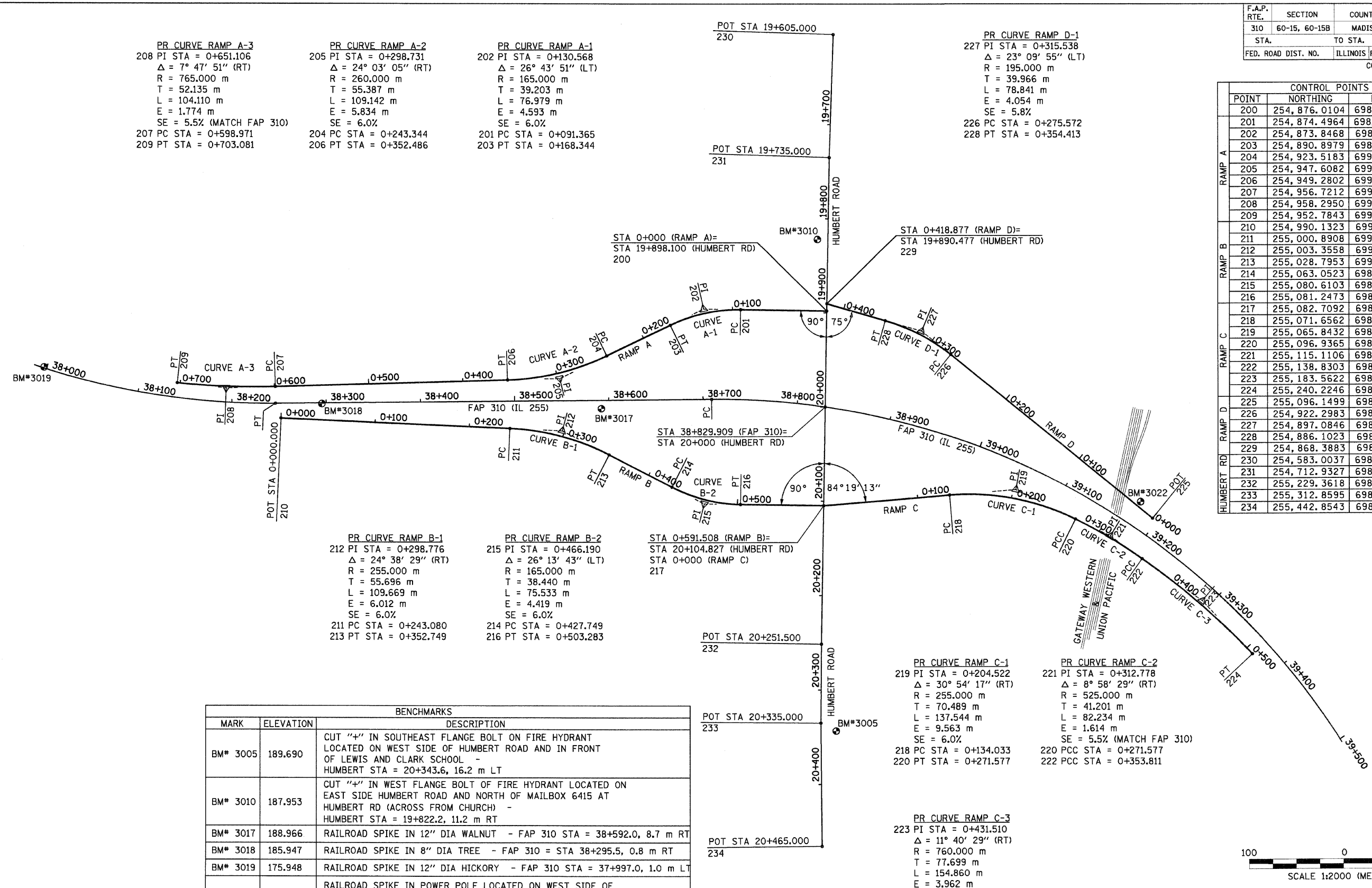
ILLINOIS DEPARTMENT OF TRANSPORTATION
HORIZONTAL CONTROL
FAP ROUTE 310 SECTION 60-14
MADISON COUNTY
DRAWN BY BANDY
CHECKED BY STEUART
DATE

6/28/2007

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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------------|---------------------------|--------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 32 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |

| CONTROL POINTS | | |
|----------------|--------------|--------------|
| POINT | NORTHING | EASTING |
| 200 | 254,876.0104 | 698,827.0311 |
| 201 | 254,874.4964 | 698,918.3831 |
| 202 | 254,873.8468 | 698,957.5811 |
| 203 | 254,890.8979 | 698,992.8822 |
| 204 | 254,923.5183 | 699,060.4166 |
| 205 | 254,947.6082 | 699,110.2902 |
| 206 | 254,949.2802 | 699,165.6518 |
| 207 | 254,956.7212 | 699,412.0244 |
| 208 | 254,958.2950 | 699,464.1360 |
| 209 | 254,952.7843 | 699,515.9792 |
| 210 | 254,990.1323 | 699,406.0501 |
| 211 | 255,000.8908 | 699,163.2083 |
| 212 | 255,003.3558 | 699,107.5671 |
| 213 | 255,028.7953 | 699,058.0207 |
| 214 | 255,063.0523 | 698,991.3013 |
| 215 | 255,080.6103 | 698,957.1052 |
| 216 | 255,081.2473 | 698,918.6701 |
| 217 | 255,082.7092 | 698,830.4566 |
| 218 | 255,071.6562 | 698,696.8803 |
| 219 | 255,065.8432 | 698,626.6310 |
| 220 | 255,096.9365 | 698,563.3699 |
| 221 | 255,115.1106 | 698,526.3936 |
| 222 | 255,138.8303 | 698,492.7051 |
| 223 | 255,183.5622 | 698,429.1737 |
| 224 | 255,240.2246 | 698,376.0084 |
| 225 | 255,096.1499 | 698,481.6737 |
| 226 | 254,922.2983 | 698,695.4855 |
| 227 | 254,897.0846 | 698,726.4946 |
| 228 | 254,886.1023 | 698,764.9224 |
| 229 | 254,868.3883 | 698,826.9048 |
| 230 | 254,583.0037 | 698,820.0350 |
| 231 | 254,712.9327 | 698,824.3285 |
| 232 | 255,229.3618 | 698,832.8870 |
| 233 | 255,312.8595 | 698,833.5144 |
| 234 | 255,442.8543 | 698,832.3511 |



PR CURVE RAMP A-3
 208 PI STA = 0+651.106
 $\Delta = 7^\circ 47' 51''$ (RT)
 R = 765.000 m
 T = 52.135 m
 L = 104.110 m
 E = 1.774 m
 SE = 5.5% (MATCH FAP 310)
 207 PC STA = 0+598.971
 209 PT STA = 0+703.081

PR CURVE RAMP A-2
 205 PI STA = 0+298.731
 $\Delta = 24^\circ 03' 05''$ (RT)
 R = 260.000 m
 T = 55.387 m
 L = 109.142 m
 E = 5.834 m
 SE = 6.0%
 204 PC STA = 0+243.344
 206 PT STA = 0+352.486

PR CURVE RAMP A-1
 202 PI STA = 0+130.568
 $\Delta = 26^\circ 43' 51''$ (LT)
 R = 165.000 m
 T = 39.203 m
 L = 76.979 m
 E = 4.593 m
 SE = 6.0%
 201 PC STA = 0+091.365
 203 PT STA = 0+168.344

PR CURVE RAMP D-1
 227 PI STA = 0+315.538
 $\Delta = 23^\circ 09' 55''$ (LT)
 R = 195.000 m
 T = 39.966 m
 L = 78.841 m
 E = 4.054 m
 SE = 5.8%
 226 PC STA = 0+275.572
 228 PT STA = 0+354.413

PR CURVE RAMP B-1
 212 PI STA = 0+298.776
 $\Delta = 24^\circ 38' 29''$ (RT)
 R = 255.000 m
 T = 55.696 m
 L = 109.669 m
 E = 6.012 m
 SE = 6.0%
 211 PC STA = 0+243.080
 213 PT STA = 0+352.749

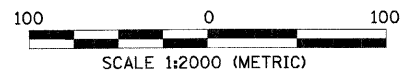
PR CURVE RAMP B-2
 215 PI STA = 0+466.190
 $\Delta = 26^\circ 13' 43''$ (LT)
 R = 165.000 m
 T = 38.440 m
 L = 75.533 m
 E = 4.419 m
 SE = 6.0%
 214 PC STA = 0+427.749
 216 PT STA = 0+503.283

PR CURVE RAMP C-1
 219 PI STA = 0+204.522
 $\Delta = 30^\circ 54' 17''$ (RT)
 R = 255.000 m
 T = 70.489 m
 L = 137.544 m
 E = 9.563 m
 SE = 6.0%
 218 PC STA = 0+134.033
 220 PT STA = 0+271.577

PR CURVE RAMP C-2
 221 PI STA = 0+312.778
 $\Delta = 8^\circ 58' 29''$ (RT)
 R = 525.000 m
 T = 41.201 m
 L = 82.234 m
 E = 1.614 m
 SE = 5.5% (MATCH FAP 310)
 220 PCC STA = 0+271.577
 222 PCC STA = 0+353.811

PR CURVE RAMP C-3
 223 PI STA = 0+431.510
 $\Delta = 11^\circ 40' 29''$ (RT)
 R = 760.000 m
 T = 77.699 m
 L = 154.860 m
 E = 3.962 m
 SE = 5.5% (MATCH FAP 310)
 222 PCC STA = 0+353.811
 224 PT STA = 0+508.671

| BENCHMARKS | | |
|------------|-----------|--|
| MARK | ELEVATION | DESCRIPTION |
| BM# 3005 | 189.690 | CUT "+" IN SOUTHEAST FLANGE BOLT ON FIRE HYDRANT LOCATED ON WEST SIDE OF HUMBERT ROAD AND IN FRONT OF LEWIS AND CLARK SCHOOL - HUMBERT STA = 20+343.6, 16.2 m LT |
| BM# 3010 | 187.953 | CUT "+" IN WEST FLANGE BOLT OF FIRE HYDRANT LOCATED ON EAST SIDE HUMBERT ROAD AND NORTH OF MAILBOX 6415 AT HUMBERT RD (ACROSS FROM CHURCH) - HUMBERT STA = 19+822.2, 11.2 m RT |
| BM# 3017 | 188.966 | RAILROAD SPIKE IN 12" DIA WALNUT - FAP 310 STA = 38+592.0, 8.7 m RT |
| BM# 3018 | 185.947 | RAILROAD SPIKE IN 8" DIA TREE - FAP 310 = STA 38+295.5, 0.8 m RT |
| BM# 3019 | 175.948 | RAILROAD SPIKE IN 12" DIA HICKORY - FAP 310 STA = 37+997.0, 1.0 m LT |
| BM# 3022 | 188.010 | RAILROAD SPIKE IN POWER POLE LOCATED ON WEST SIDE OF RAILROAD TRACKS BETWEEN RAMP D & FAP 310 - FAP 310 STA = 39+173.5, 27.0 m LT |



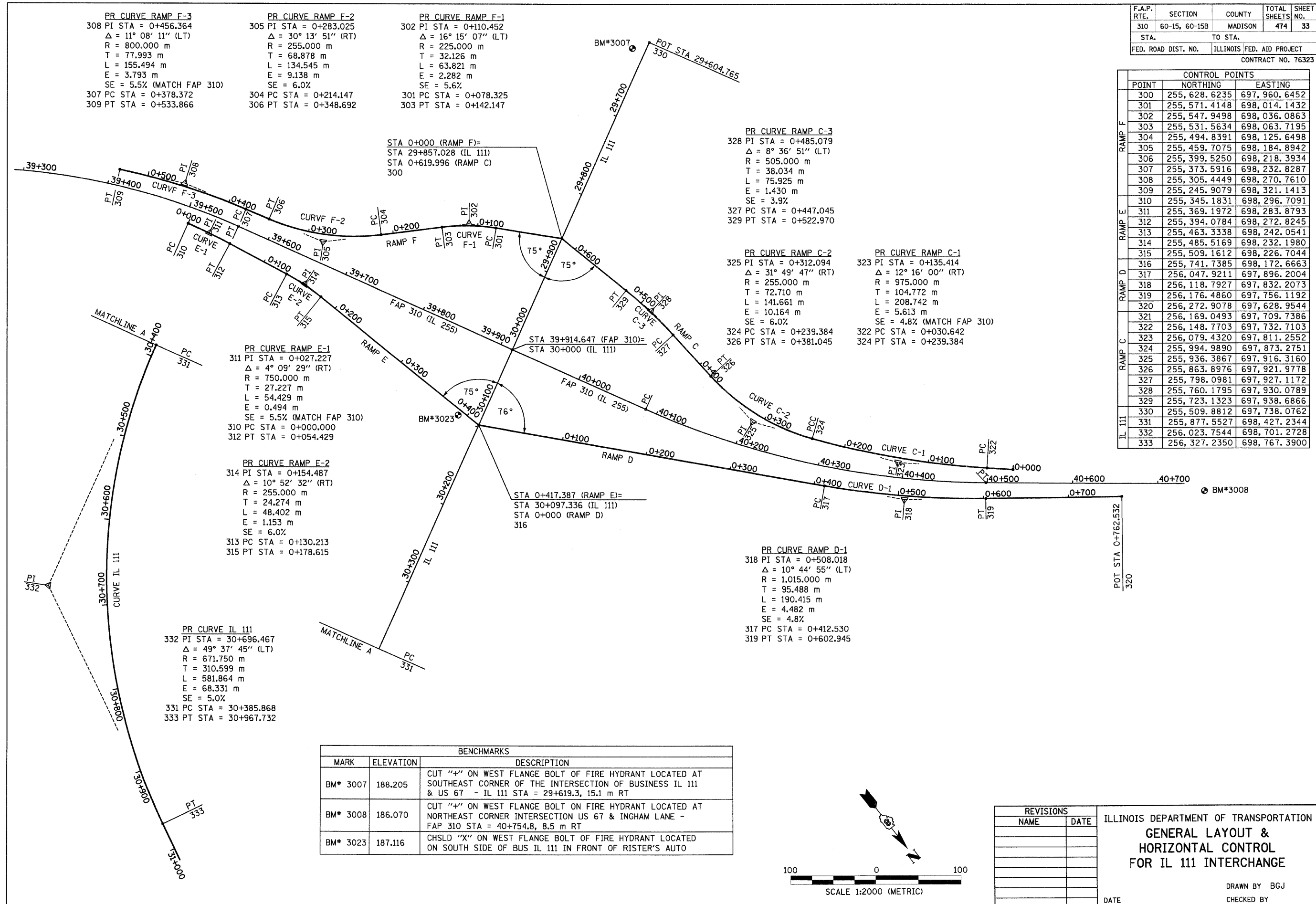
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ILLINOIS DEPARTMENT OF TRANSPORTATION
**GENERAL LAYOUT &
 HORIZONTAL CONTROL
 FOR HUMBERT ROAD INTERCHANGE**

DATE _____ DRAWN BY BGJ
 CHECKED BY _____

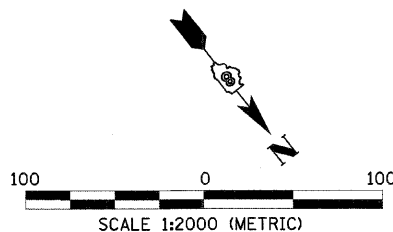
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| CONTROL POINTS | | | |
|----------------|--------------|--------------|--|
| POINT | NORTHING | EASTING | |
| 300 | 255,628.6235 | 697,960.6452 | |
| 301 | 255,571.4148 | 698,014.1432 | |
| 302 | 255,547.9498 | 698,036.0863 | |
| 303 | 255,531.5634 | 698,063.7195 | |
| 304 | 255,494.8391 | 698,125.6498 | |
| 305 | 255,459.7075 | 698,184.8942 | |
| 306 | 255,399.5250 | 698,218.3934 | |
| 307 | 255,373.5916 | 698,232.8287 | |
| 308 | 255,305.4449 | 698,270.7610 | |
| 309 | 255,245.9079 | 698,321.1413 | |
| 310 | 255,345.1831 | 698,296.7091 | |
| 311 | 255,369.1972 | 698,283.8793 | |
| 312 | 255,394.0784 | 698,272.8245 | |
| 313 | 255,463.3338 | 698,242.0541 | |
| 314 | 255,485.5169 | 698,232.1980 | |
| 315 | 255,509.1612 | 698,226.7044 | |
| 316 | 255,741.7385 | 698,172.6663 | |
| 317 | 256,047.9211 | 697,896.2004 | |
| 318 | 256,118.7927 | 697,832.2073 | |
| 319 | 256,176.4860 | 697,756.1192 | |
| 320 | 256,272.9078 | 697,628.9544 | |
| 321 | 256,169.0493 | 697,709.7386 | |
| 322 | 256,148.7703 | 697,732.7103 | |
| 323 | 256,079.4320 | 697,811.2552 | |
| 324 | 255,994.9890 | 697,873.2751 | |
| 325 | 255,936.3867 | 697,916.3160 | |
| 326 | 255,863.8976 | 697,921.9778 | |
| 327 | 255,798.0981 | 697,927.1172 | |
| 328 | 255,760.1795 | 697,930.0789 | |
| 329 | 255,723.1323 | 697,938.6866 | |
| 330 | 255,509.8812 | 697,738.0762 | |
| 331 | 255,877.5527 | 698,427.2344 | |
| 332 | 256,023.7544 | 698,701.2728 | |
| 333 | 256,327.2350 | 698,767.3900 | |

| BENCHMARKS | | |
|------------|-----------|---|
| MARK | ELEVATION | DESCRIPTION |
| BM# 3007 | 188.205 | CUT "+" ON WEST FLANGE BOLT OF FIRE HYDRANT LOCATED AT SOUTHEAST CORNER OF THE INTERSECTION OF BUSINESS IL 111 & US 67 - IL 111 STA = 29+619.3, 15.1 m RT |
| BM# 3008 | 186.070 | CUT "+" ON WEST FLANGE BOLT ON FIRE HYDRANT LOCATED AT NORTHEAST CORNER INTERSECTION US 67 & INGHAM LANE - FAP 310 STA = 40+754.8, 8.5 m RT |
| BM# 3023 | 187.116 | CHSLD "X" ON WEST FLANGE BOLT OF FIRE HYDRANT LOCATED ON SOUTH SIDE OF BUS IL 111 IN FRONT OF RISTER'S AUTO |

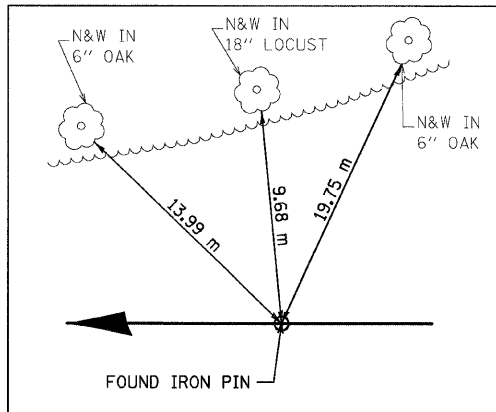


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ILLINOIS DEPARTMENT OF TRANSPORTATION
**GENERAL LAYOUT &
 HORIZONTAL CONTROL
 FOR IL 111 INTERCHANGE**

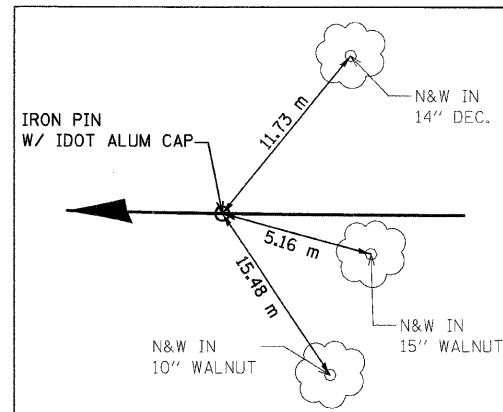
DATE _____ DRAWN BY B.G.J.
 CHECKED BY _____

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|---------------------|---------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 34 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |



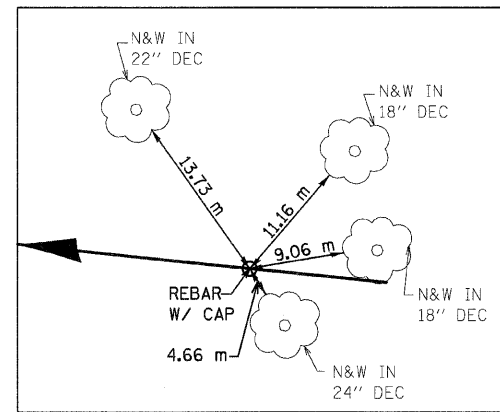
IL 255 MAINLINE POT
STA 35+150.000

N 253,215.1896 E 701,640.4235



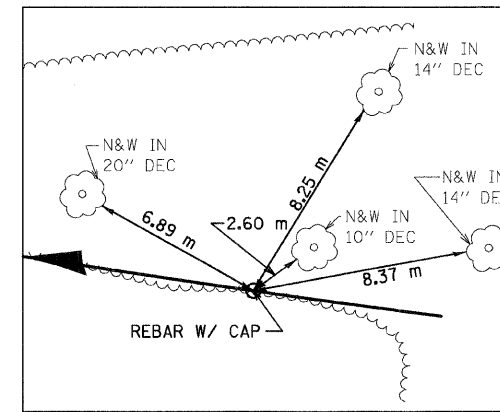
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STA 35+297.954

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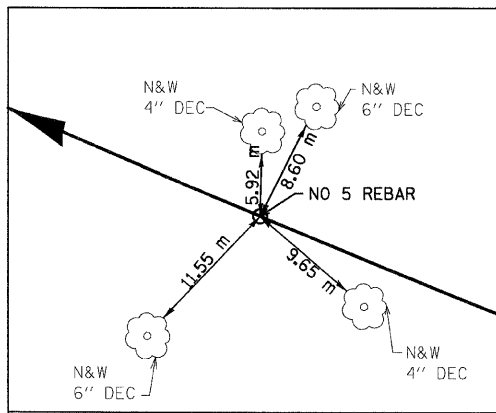
IL 255 MAINLINE POT
STA 35+425.000

N 253,221.5414 E 701,365.6354



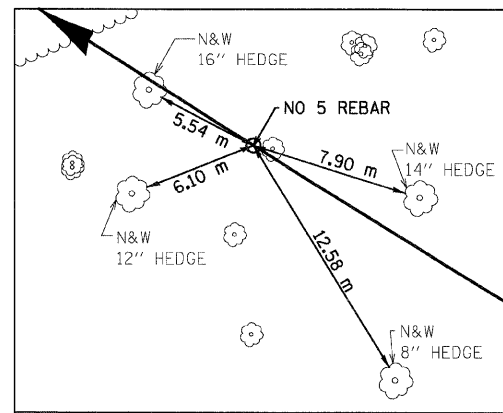
IL 255 MAINLINE POT
STA 35+475.000

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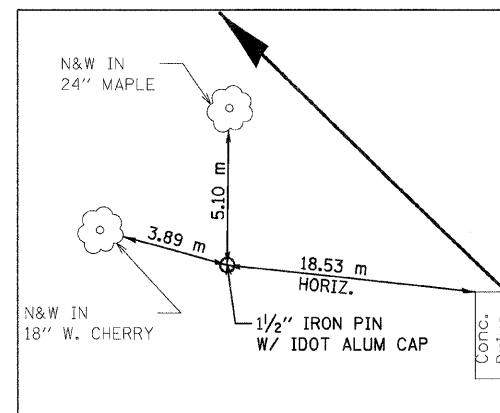
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STA 35+800.000

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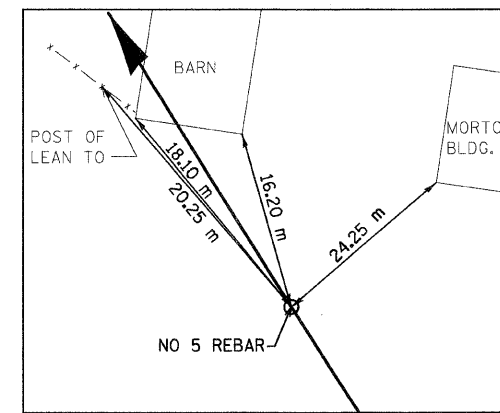
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STA 36+010.000

N 253,409.6328 E 700,817.1482



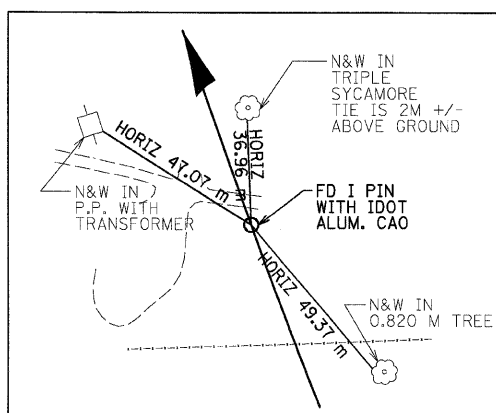
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STA 36+194.086

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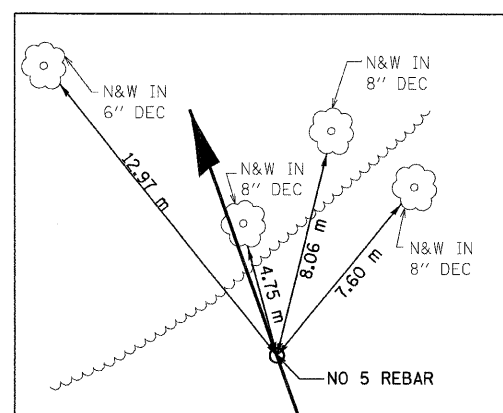
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STA 36+575.000

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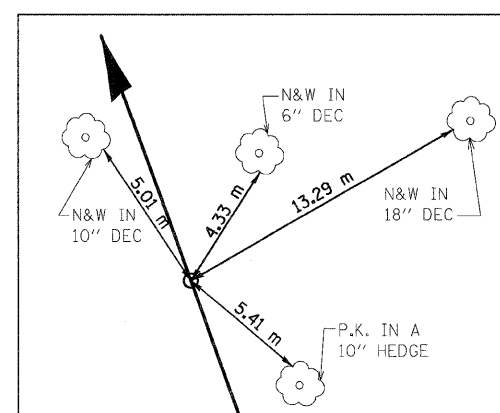
IL 255 MAINLINE PT
STA 36+858.745

N 254,059.4870 E 700,295.9546



IL 255 MAINLINE POT
STA 37+210.000

N 254,390.4205 E 700,178.2141



IL 255 MAINLINE PC
STA 37+279.595

N 254,455.9892 E 700,154.8859

NOTES:

- 1) DIMENSIONS LABELED "HORIZ" ARE HORIZONTAL PULLS.
- 2) DIMENSIONS NOT LABELED "HORIZ" ARE DIRECT PULLS.

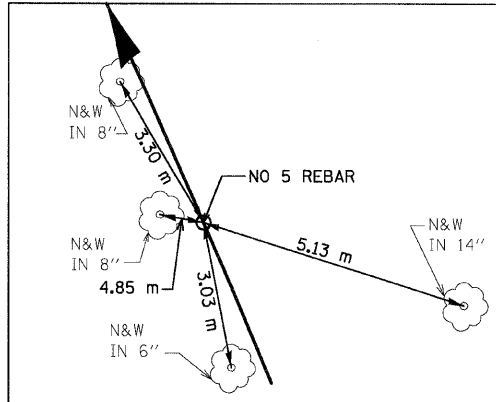


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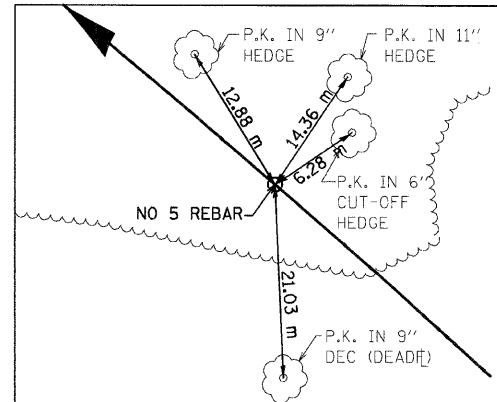
ILLINOIS DEPARTMENT OF TRANSPORTATION
HORIZONTAL CONTROL TIES
FAP 310 (IL 255)
SECTION 60-15
MADISON COUNTY
DRAWN BY CES
CHECKED BY
DATE

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------------|---------------------------|--------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 35 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |



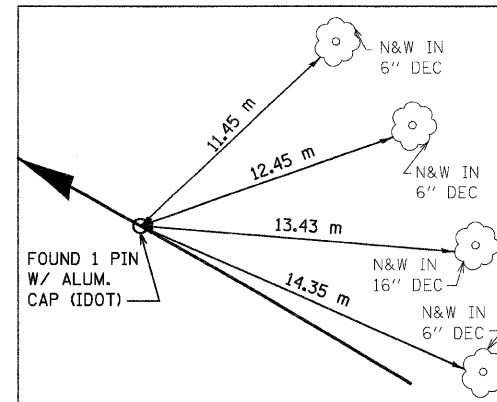
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STA 37+330.000

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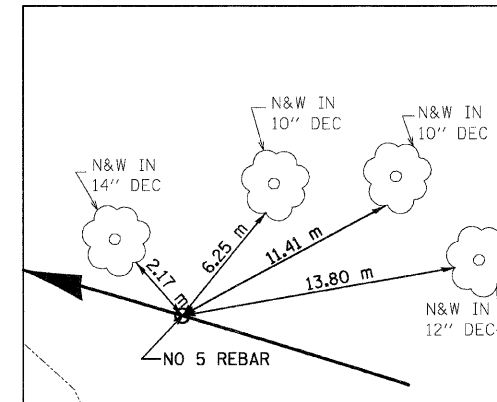
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STA 37+680.000

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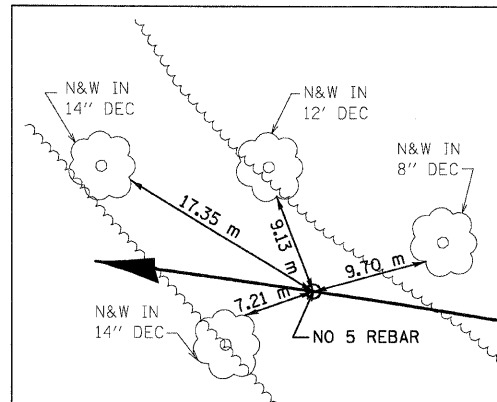
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STA 37+835.958

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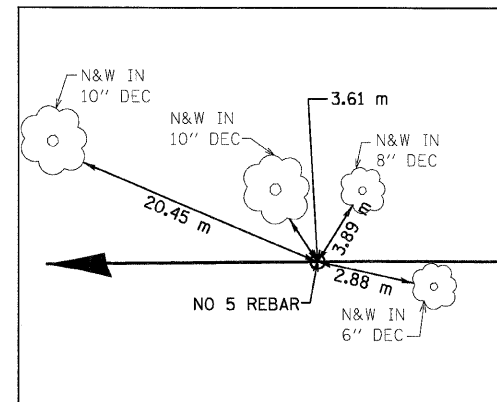
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STA 38+015.000

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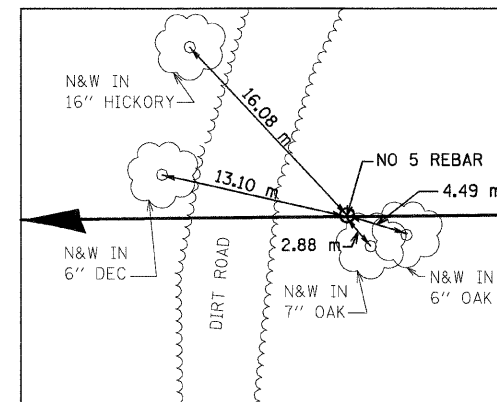
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STA 38+125.000

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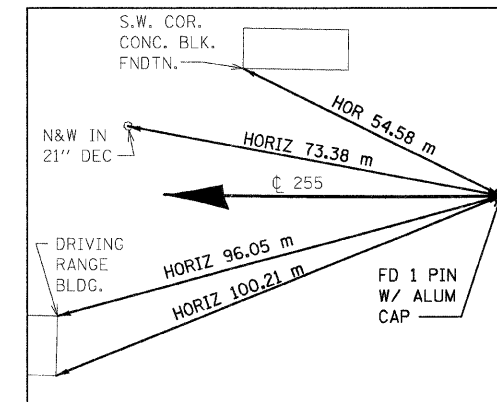
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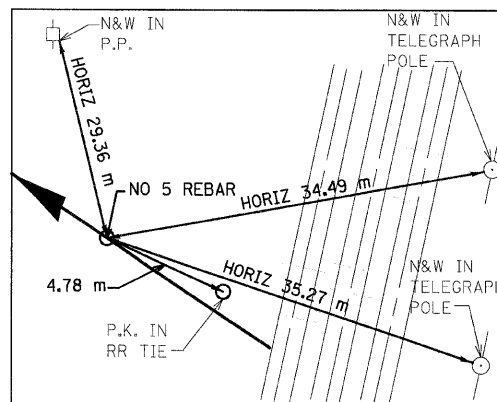
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STA 38+350.000

N 254,973.4340 E 699,308.2153



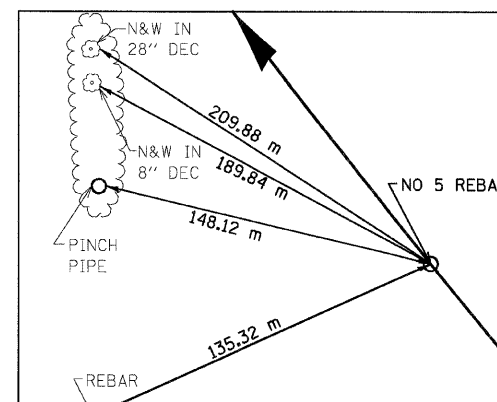
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STA 38+709.010

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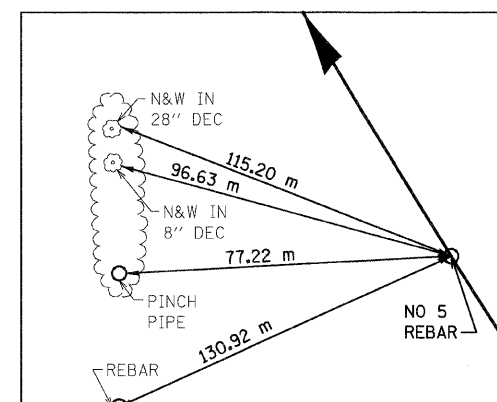
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STA 39+183.523

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IL 255 MAINLINE POT
STA 39+410.000

N 255,257.8887 E 698,335.8878



IL 255 MAINLINE POT
STA 39+505.000

N 255,335.0827 E 698,280.6163

NOTES:

- 1) DIMENSIONS LABELED "HORIZ" ARE HORIZONTAL PULLS.
- 2) DIMENSIONS NOT LABELED "HORIZ" ARE DIRECT PULLS.

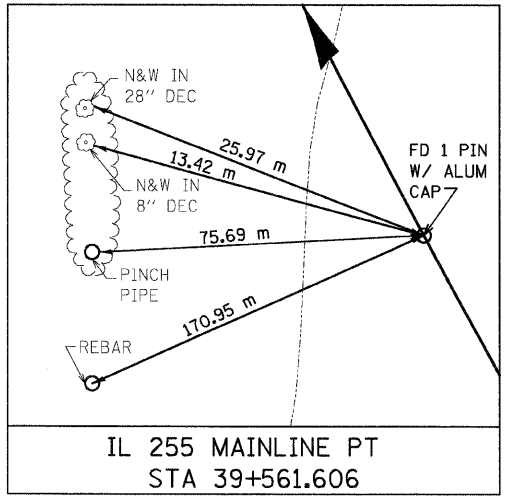


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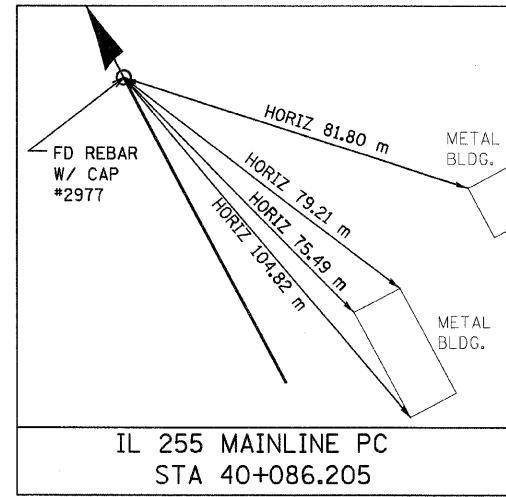
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ILLINOIS DEPARTMENT OF TRANSPORTATION
HORIZONTAL CONTROL TIES
FAP 310 (IL 255)
SECTION 60-15
MADISON COUNTY
DRAWN BY CES
CHECKED BY
DATE

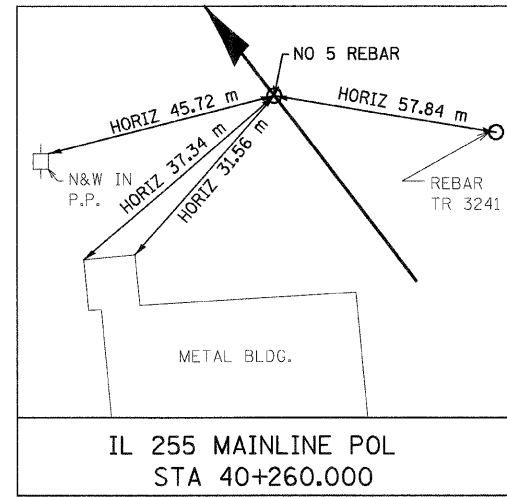
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 36 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | | |
| CONTRACT NO. 76323 | | | | |



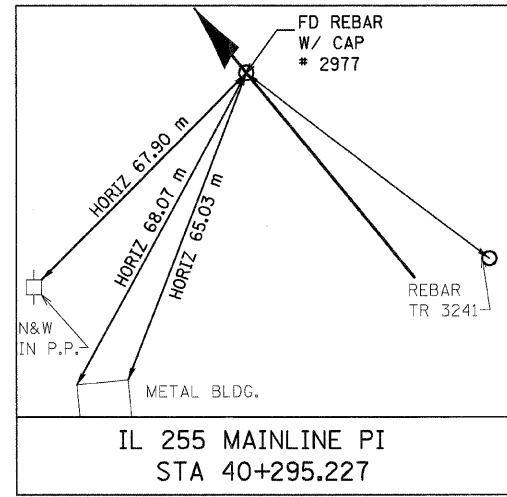
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N 255,384.0772 E 698,252.2896



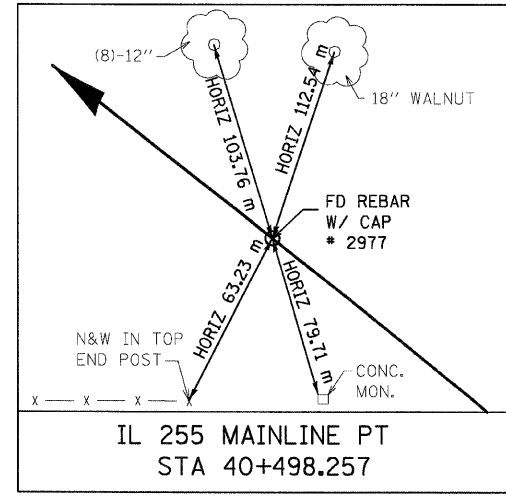
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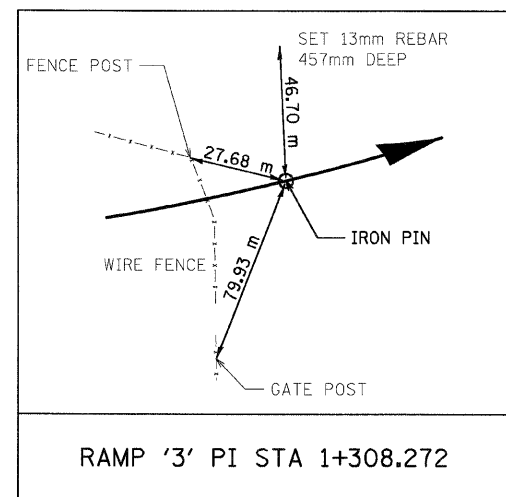
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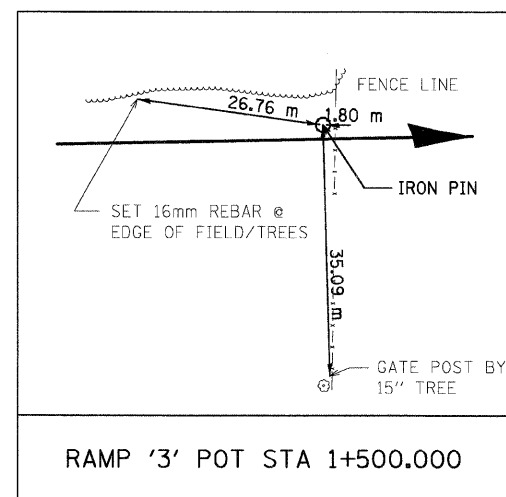
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STA 40+295.227
N 256,032.0913 E 697,908.3758



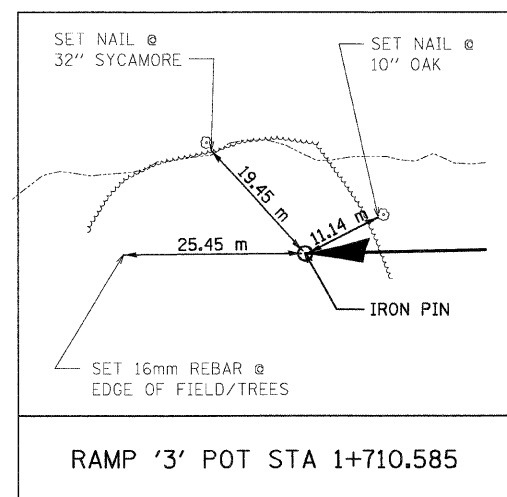
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STA 40+498.257
N 256,161.6866 E 697,744.3781



RAMP '3' PI STA 1+308.272
N 253,238.9613 E 701,949.4152



RAMP '3' POT STA 1+500.000
N 253,253.0994 E 701,756.2340



RAMP '3' POT STA 1+710.585
N 253,230.8904 E 701,545.6908

NOTES:

- 1) DIMENSIONS LABELED "HORIZ" ARE HORIZONTAL PULLS.
- 2) DIMENSIONS NOT LABELED "HORIZ" ARE DIRECT PULLS.

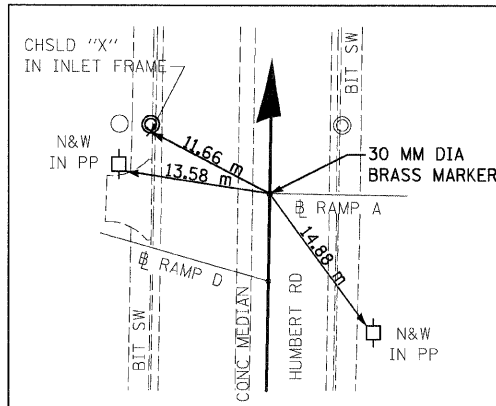


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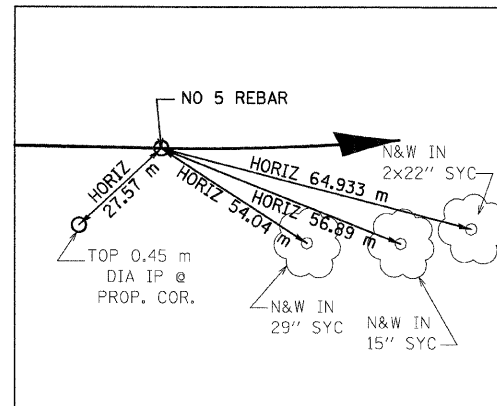
ILLINOIS DEPARTMENT OF TRANSPORTATION
HORIZONTAL CONTROL TIES
FAP 310 (IL 255)
SECTION 60-15
MADISON COUNTY
DRAWN BY CES
CHECKED BY
DATE

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|---------------------|---------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 37 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |



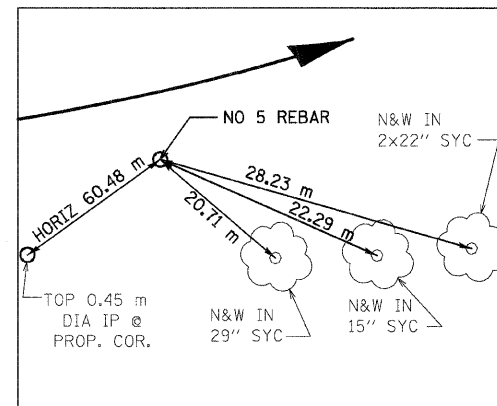
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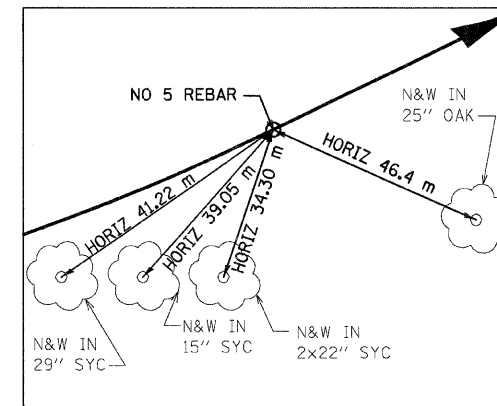
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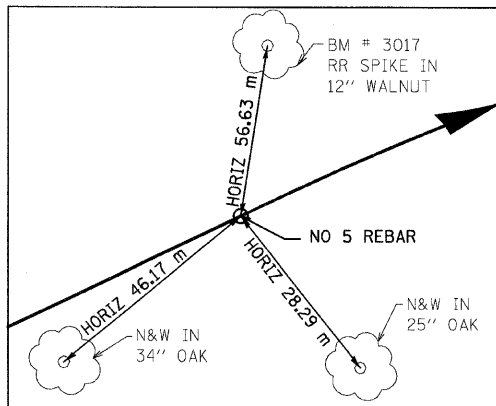
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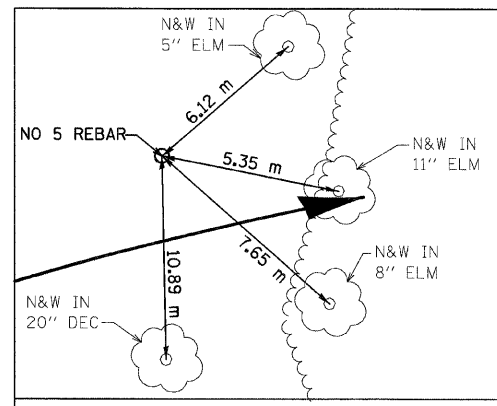
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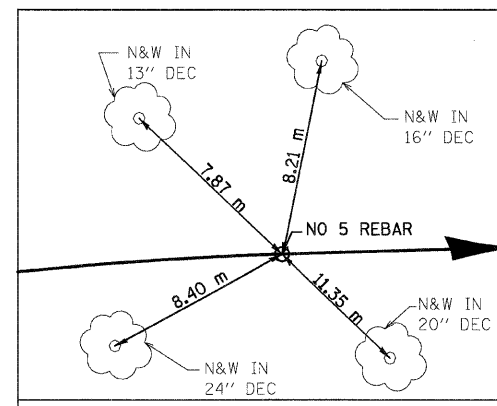
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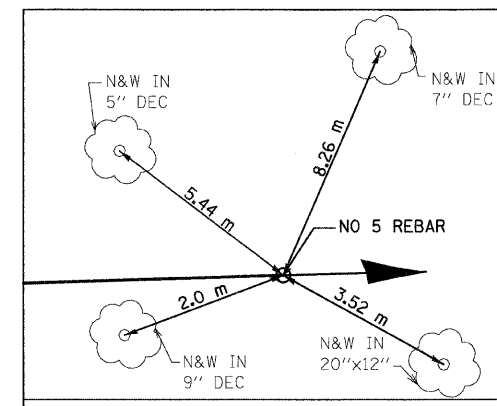
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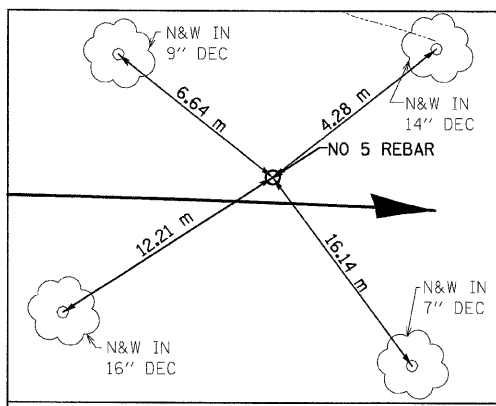
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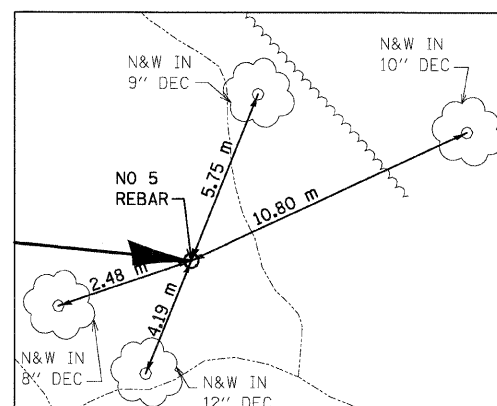
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NOTES:

- 1) DIMENSIONS LABELED "HORIZ" ARE HORIZONTAL PULLS.
- 2) DIMENSIONS NOT LABELED "HORIZ" ARE DIRECT PULLS.

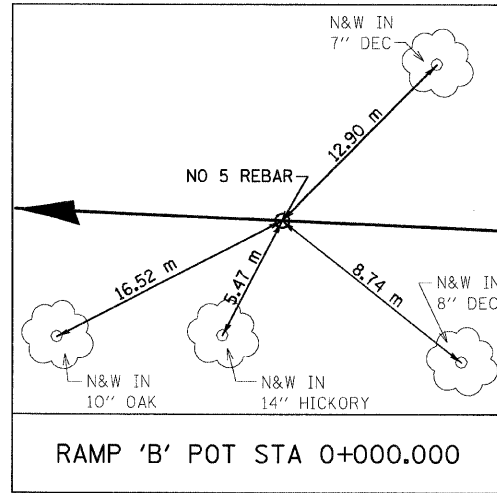


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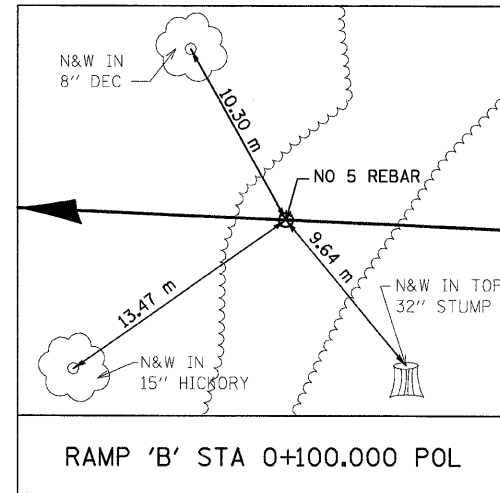
ILLINOIS DEPARTMENT OF TRANSPORTATION
HORIZONTAL CONTROL TIES
 FAP 310 (IL 255)
 SECTION 60-15
 MADISON COUNTY
 DRAWN BY CES
 CHECKED BY
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|---------------------|---------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 38 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |



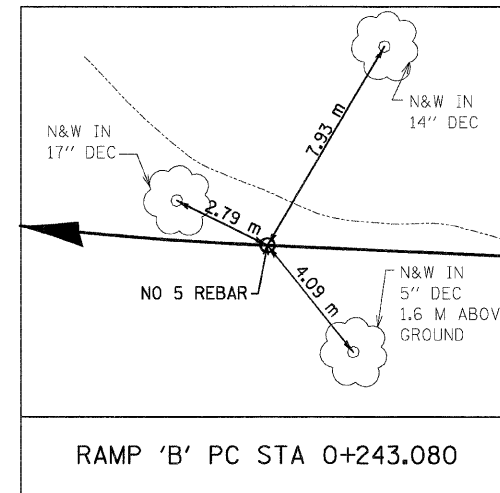
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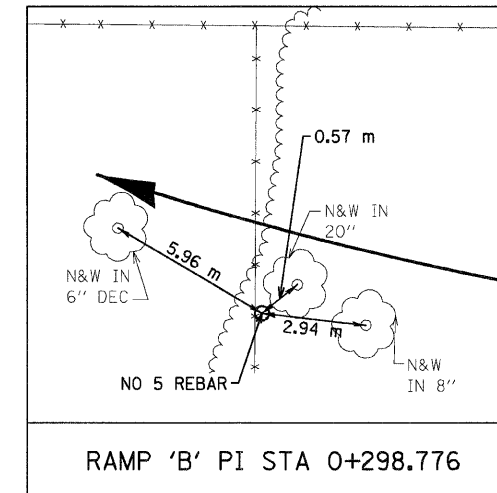
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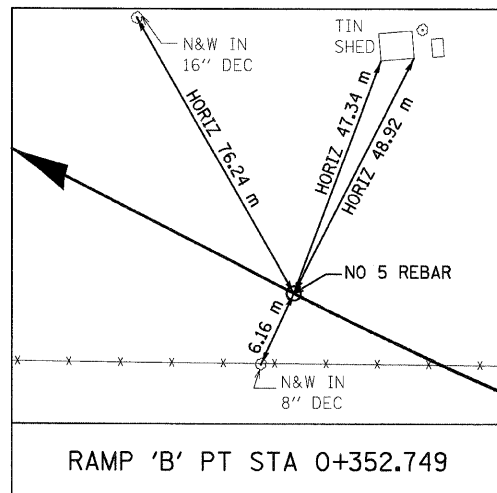
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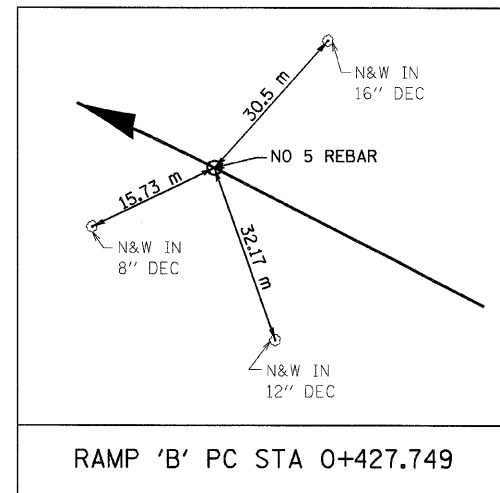
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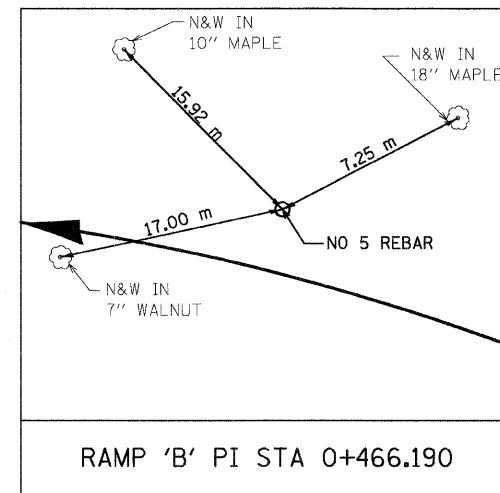
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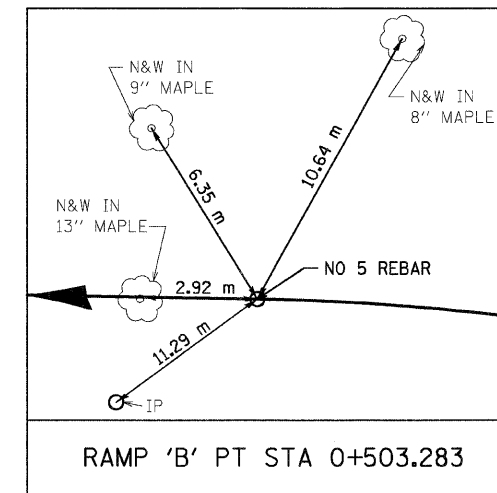
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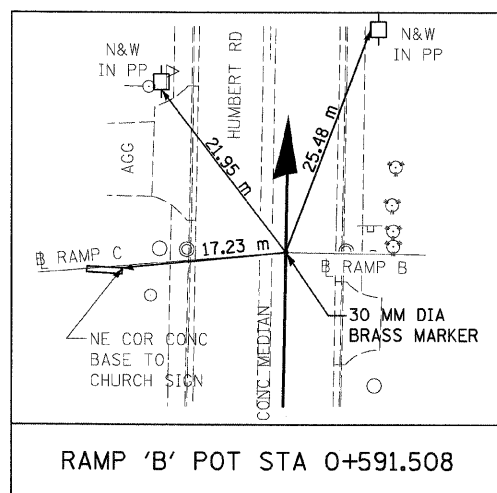
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RAMP 'B' POT STA 0+591.508

N 255,082.7092 E 698,830.4566

NOTES:

- 1) DIMENSIONS LABELED "HORIZ" ARE HORIZONTAL PULLS.
- 2) DIMENSIONS NOT LABELED "HORIZ" ARE DIRECT PULLS.

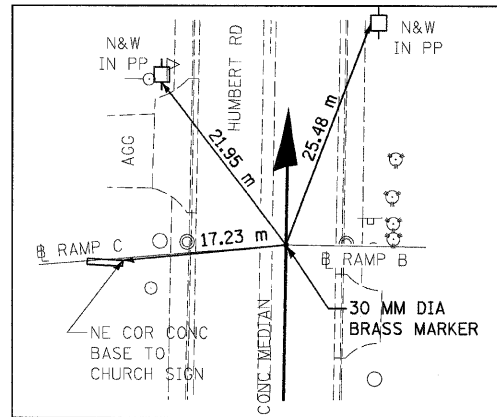


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| REVISIONS | |
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| NAME | DATE |
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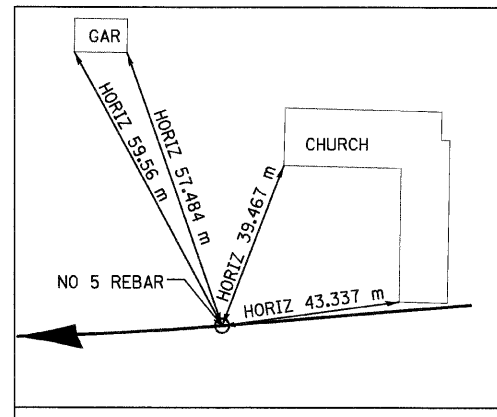
ILLINOIS DEPARTMENT OF TRANSPORTATION
 HORIZONTAL CONTROL TIES
 FAP 310 (IL 255)
 SECTION 60-15
 MADISON COUNTY
 DRAWN BY CES
 CHECKED BY
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|---------------------|---------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 39 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |



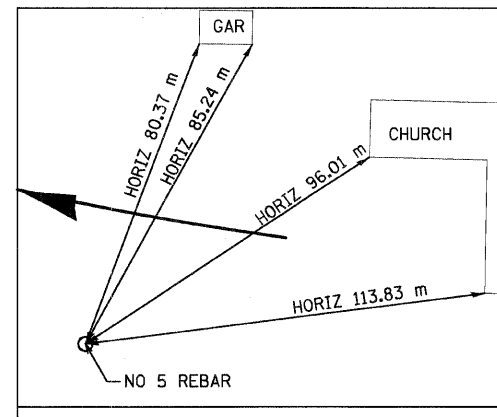
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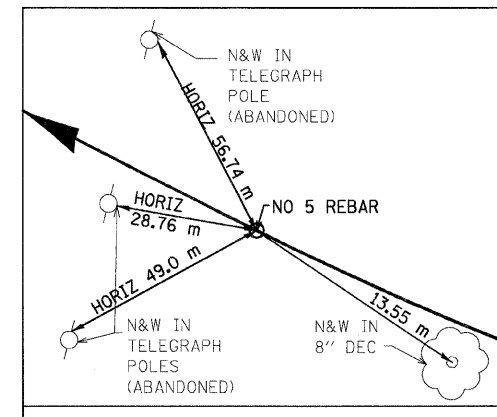
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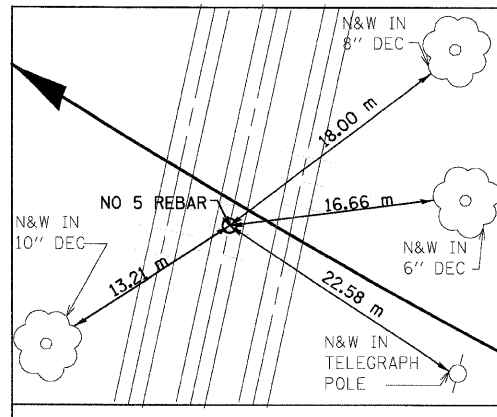
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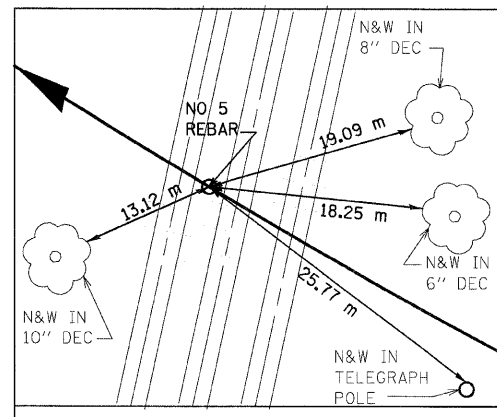
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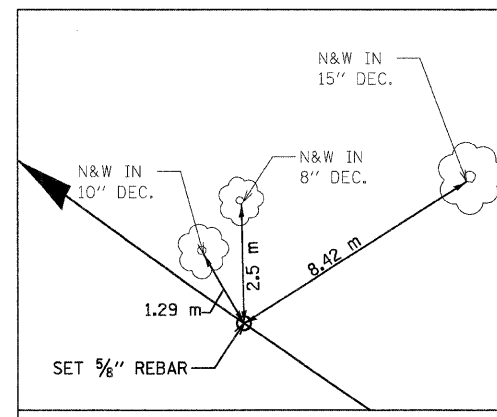
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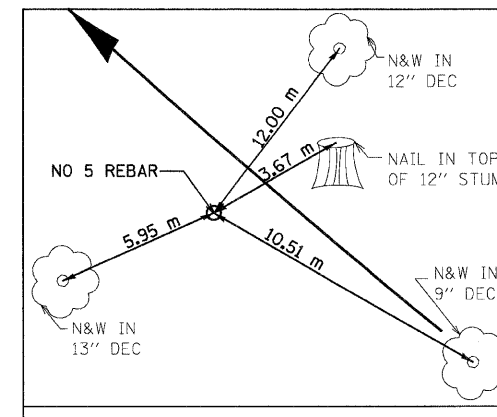
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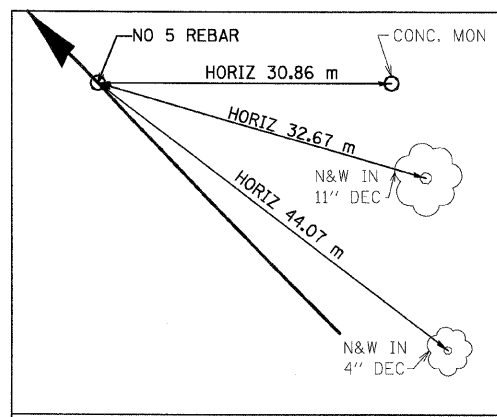
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PI STA 0+431.510

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HUMBERT RAMP 'C'
PT STA 0+508.671

N 255,240.2246 E 698,376.0084

NOTES:

- 1) DIMENSIONS LABELED "HORIZ" ARE HORIZONTAL PULLS.
- 2) DIMENSIONS NOT LABELED "HORIZ" ARE DIRECT PULLS.

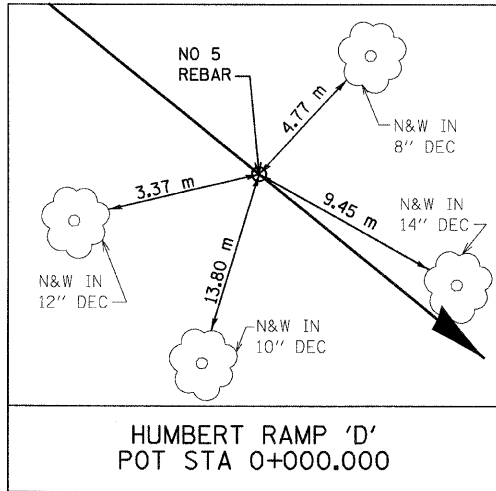


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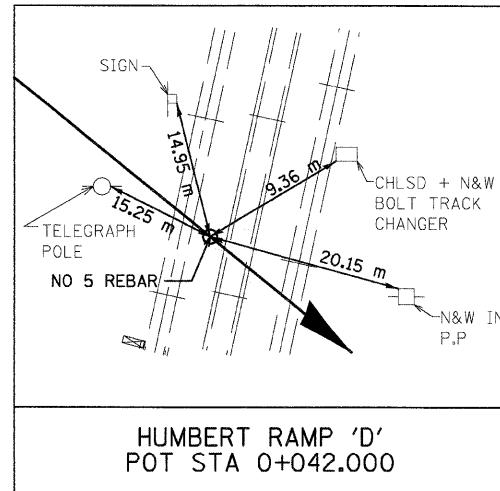
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ILLINOIS DEPARTMENT OF TRANSPORTATION
HORIZONTAL CONTROL TIES
 FAP 310 (IL 255)
 SECTION 60-15
 MADISON COUNTY
 DRAWN BY CES
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 DATE

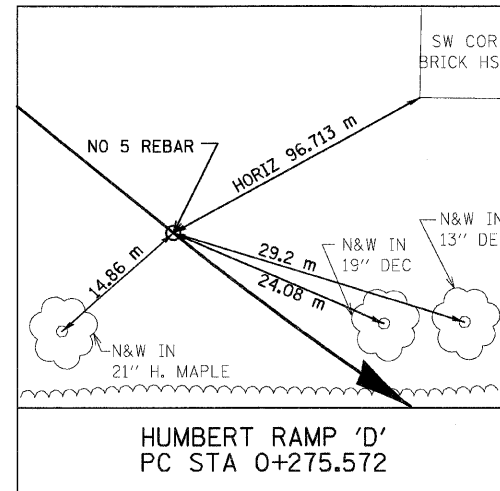
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 40 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |



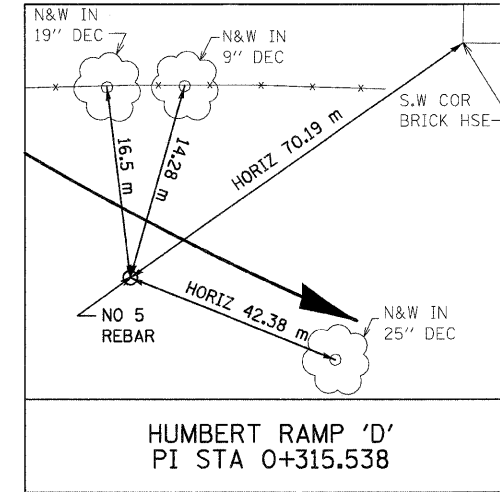
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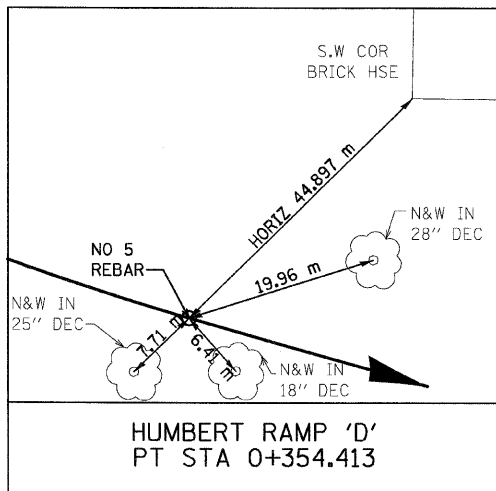
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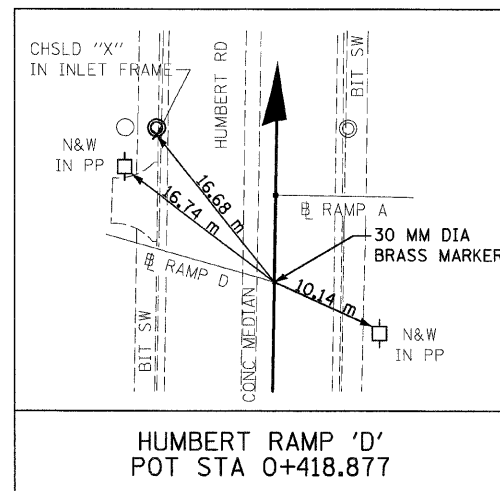
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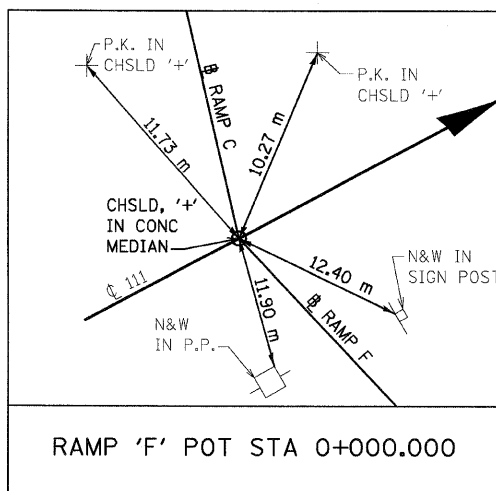
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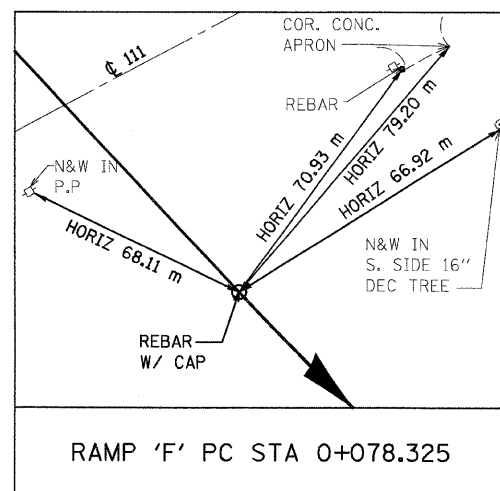
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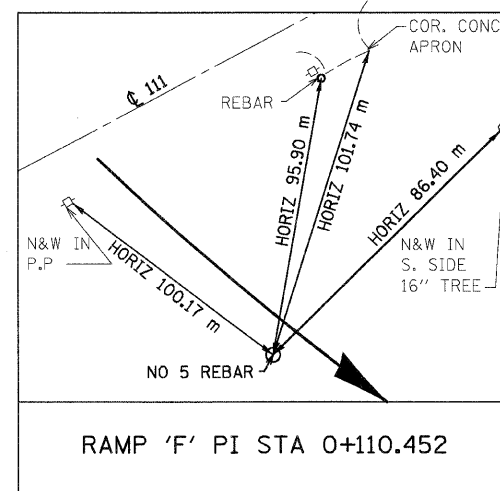
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NOTES:

- 1) DIMENSIONS LABELED "HORIZ" ARE HORIZONTAL PULLS.
- 2) DIMENSIONS NOT LABELED "HORIZ" ARE DIRECT PULLS.

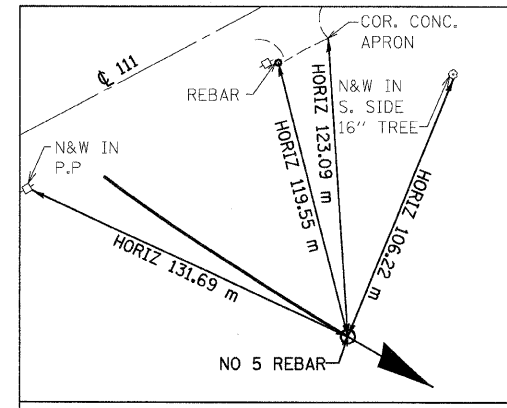


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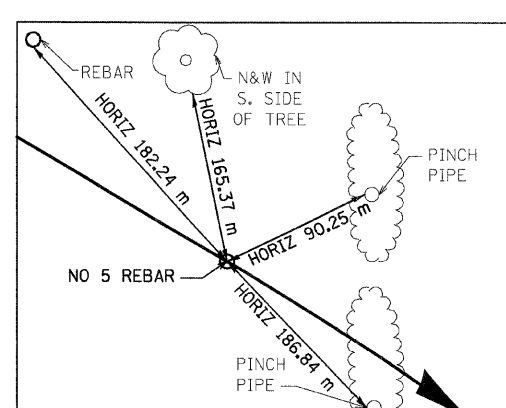
ILLINOIS DEPARTMENT OF TRANSPORTATION
HORIZONTAL CONTROL TIES
 FAP 310 (IL 255)
 SECTION 60-15
 MADISON COUNTY
 DRAWN BY CES
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|---------------------|---------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 41 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |



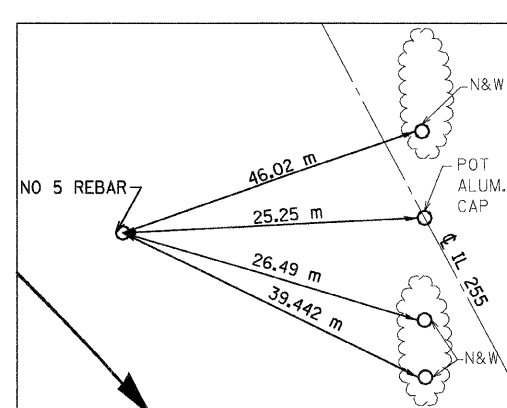
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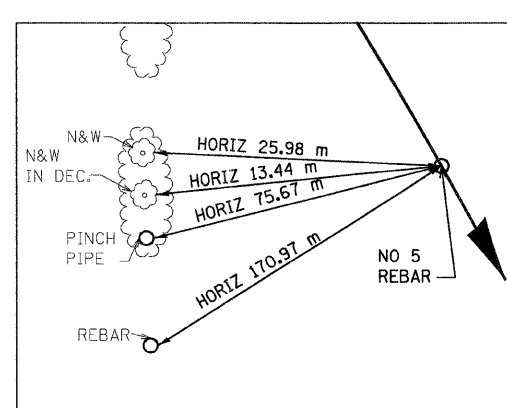
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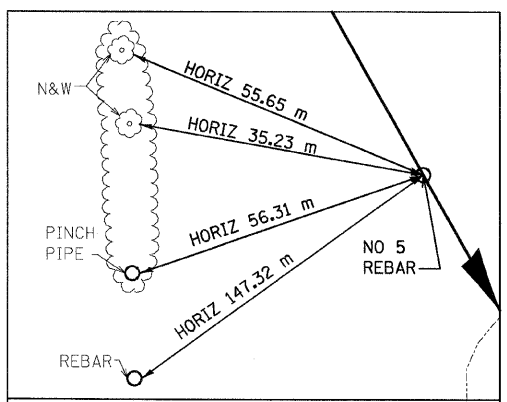
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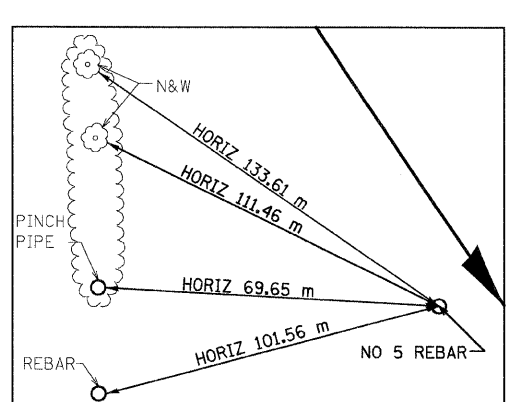
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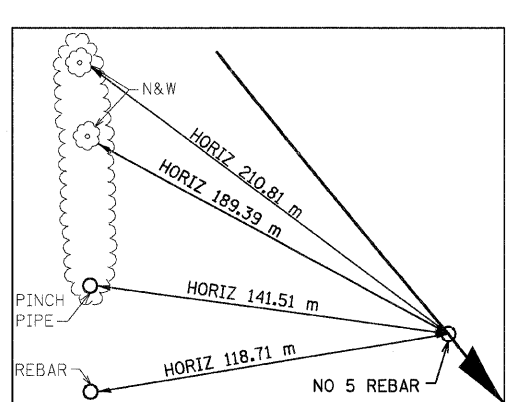
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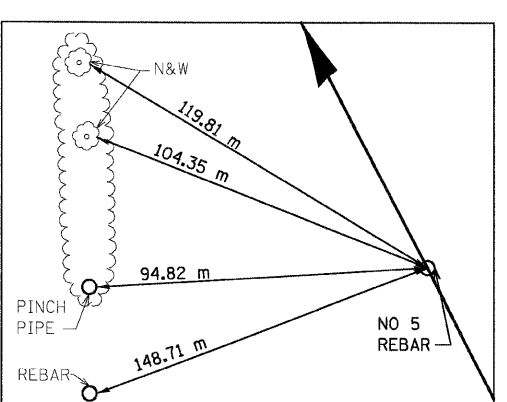
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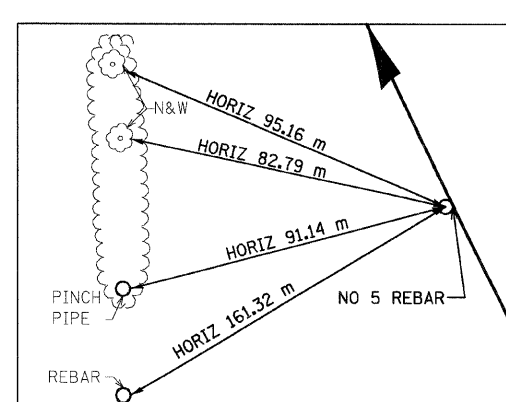
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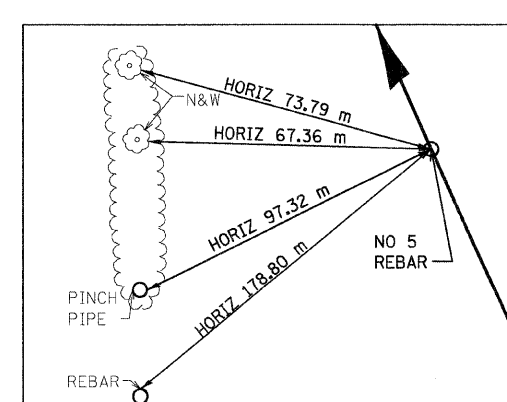
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NOTES:

- 1) DIMENSIONS LABELED "HORIZ" ARE HORIZONTAL PULLS.
- 2) DIMENSIONS NOT LABELED "HORIZ" ARE DIRECT PULLS.

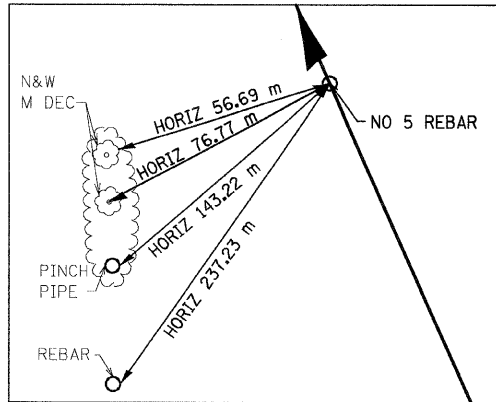


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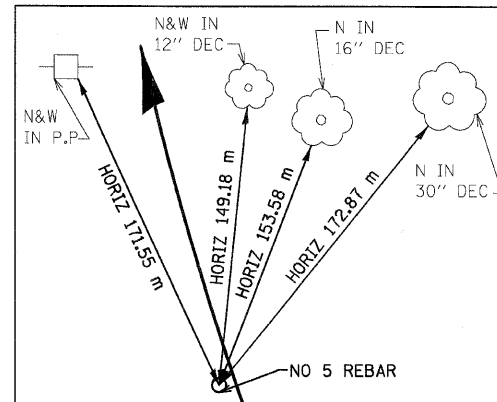
ILLINOIS DEPARTMENT OF TRANSPORTATION
HORIZONTAL CONTROL TIES
 FAP 310 (IL 255)
 SECTION 60-15
 MADISON COUNTY
 DRAWN BY CES
 CHECKED BY
 DATE

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|---------------------|---------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 42 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |



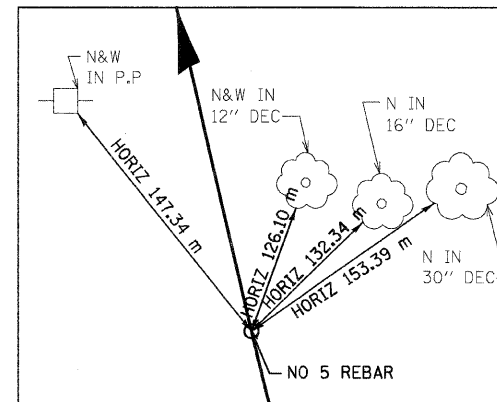
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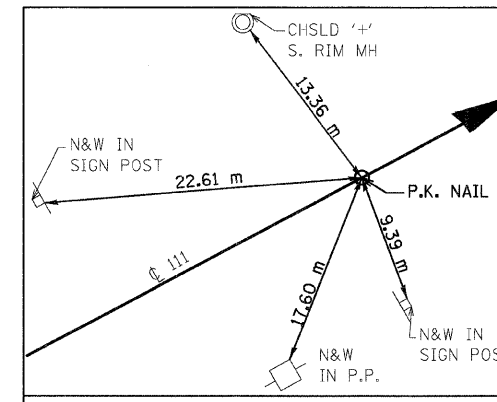
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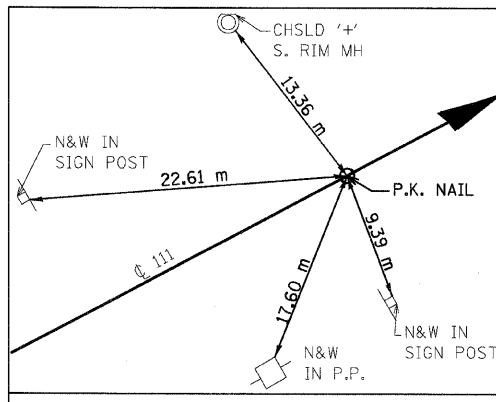
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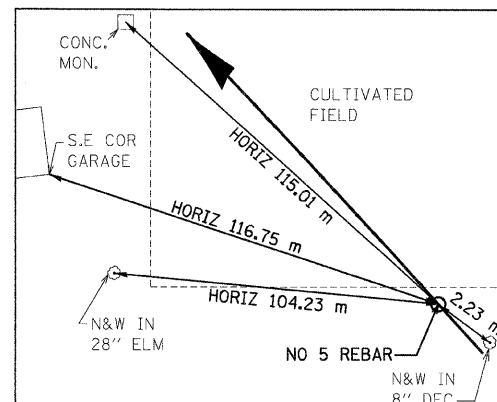
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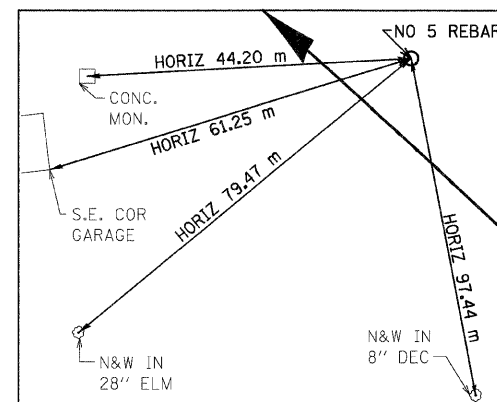
IL 111 RAMP 'D' POT STA 0+000.000

N 255,741.7385 E 698,172.6663



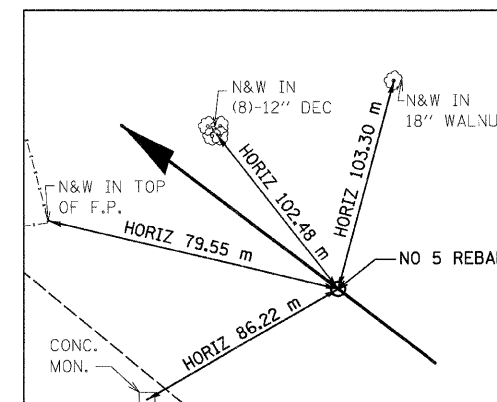
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N 256,047.9211 E 697,896.2004



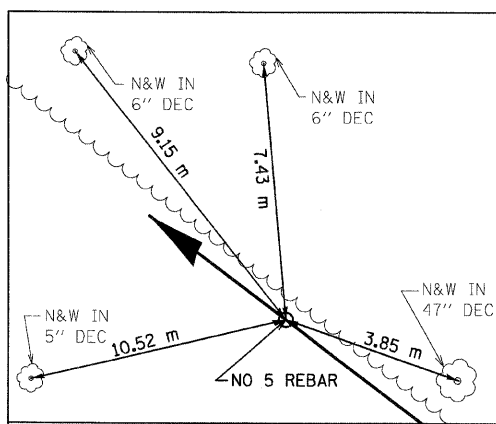
IL 111 RAMP 'D' PI STA 0+508.018

N 256,118.7927 E 697,832.2073



IL 111 RAMP 'D' PT STA 0+602.945

N 256,176.4860 E 697,756.1192



IL 111 RAMP 'D' POT STA 0+762.532

N 256,272.9078 E 697,628.9544

NOTES:

- 1) DIMENSIONS LABELED "HORIZ" ARE HORIZONTAL PULLS.
- 2) DIMENSIONS NOT LABELED "HORIZ" ARE DIRECT PULLS.

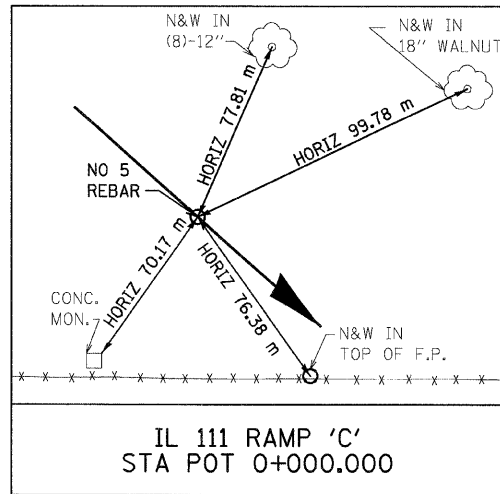


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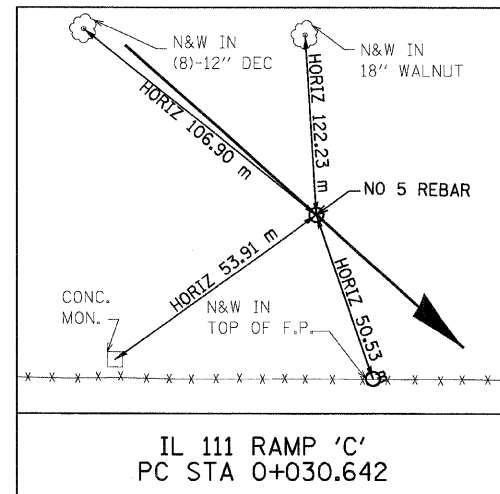
ILLINOIS DEPARTMENT OF TRANSPORTATION
 HORIZONTAL CONTROL TIES
 FAP 310 (IL 255)
 SECTION 60-15
 MADISON COUNTY
 DRAWN BY CES
 CHECKED BY
 DATE

| | | | | |
|---------------------|---------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 43 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |



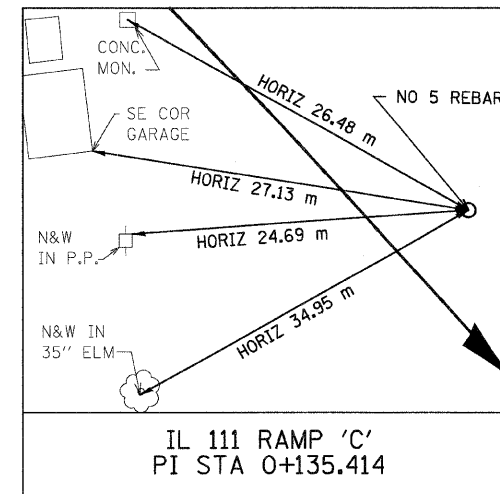
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STA POT 0+000.000

N 256,169.0493 E 697,709.7386



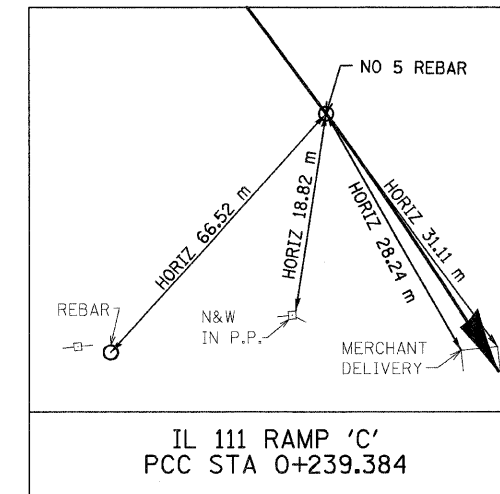
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PC STA 0+030.642

N 256,148.7703 E 697,732.7103



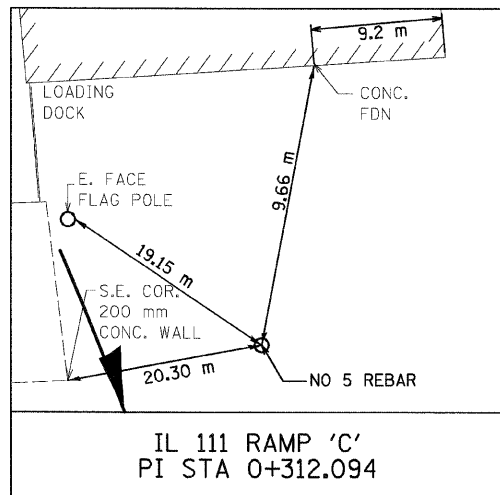
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PI STA 0+135.414

N 256,079.4320 E 697,811.2552



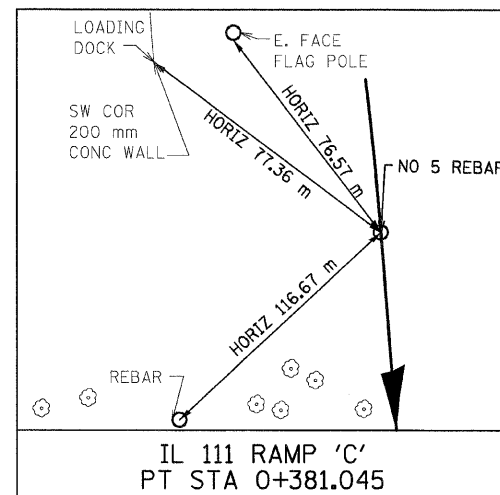
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PCC STA 0+239.384

N 255,994.9890 E 697,873.2751



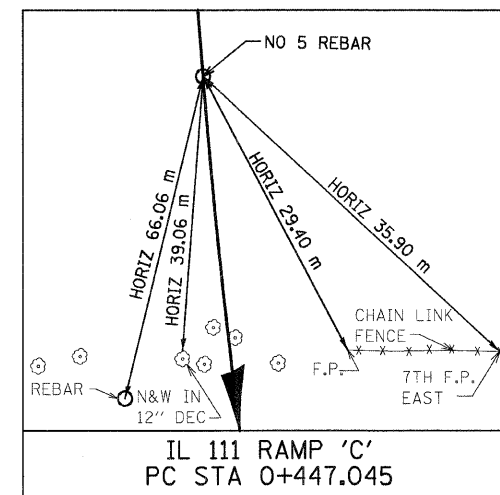
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PI STA 0+312.094

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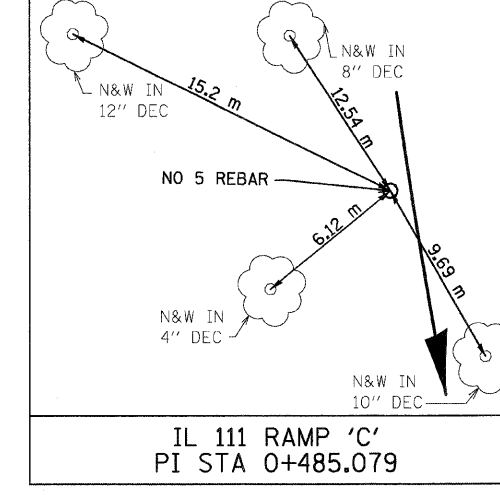
IL 111 RAMP 'C'
PT STA 0+381.045

N 255,863.8976 E 697,921.9778



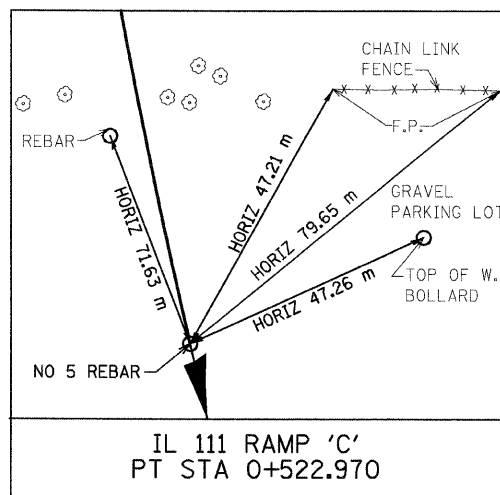
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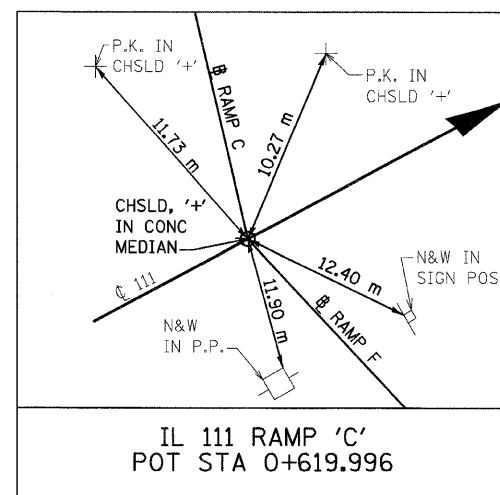
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PI STA 0+485.079

N 255,760.1795 E 697,930.0789



IL 111 RAMP 'C'
PT STA 0+522.970

N 255,723.1323 E 697,938.6866



IL 111 RAMP 'C'
POT STA 0+619.996

N 255,628.6235 E 697,960.6452

NOTES:

- 1) DIMENSIONS LABELED "HORIZ" ARE HORIZONTAL PULLS.
- 2) DIMENSIONS NOT LABELED "HORIZ" ARE DIRECT PULLS.

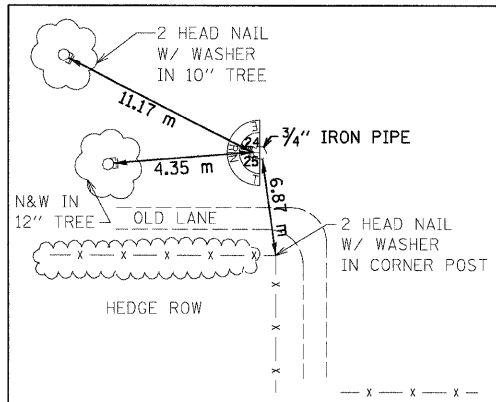


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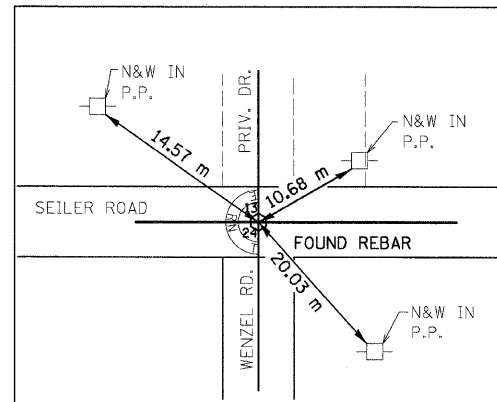
ILLINOIS DEPARTMENT OF TRANSPORTATION
HORIZONTAL CONTROL TIES
FAP 310 (IL 255)
SECTION 60-15
MADISON COUNTY
DRAWN BY CES
CHECKED BY
DATE

| | | | | |
|---------------------|---------------------------|---------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 44 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | | |
| CONTRACT NO. 76323 | | | | |



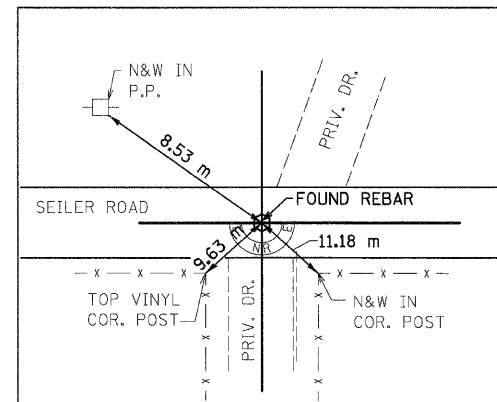
SE CORNER SECTION 24
NE CORNER SECTION 25

N 253,640.337 E 700,918.1282



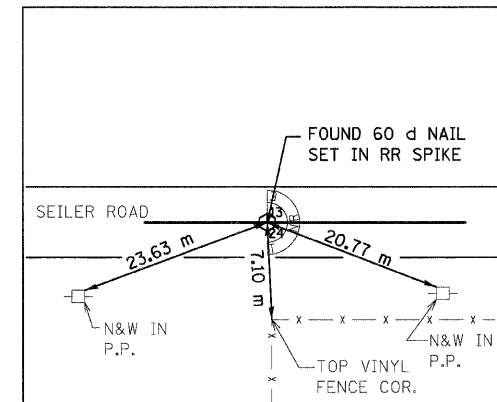
NE CORNER SECTION 24

N 255,262.2919 E 700,933.7386



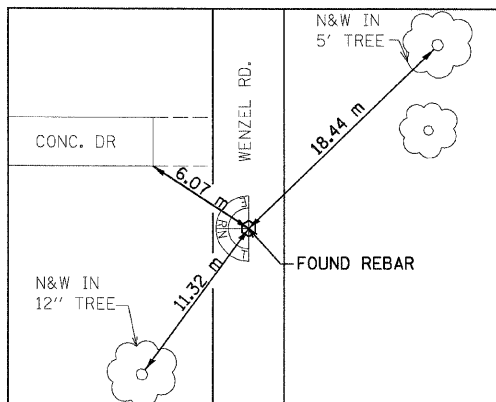
NORTH 1/4 CORNER SECTION 24

N 255,238.7902 E 700,091.0307



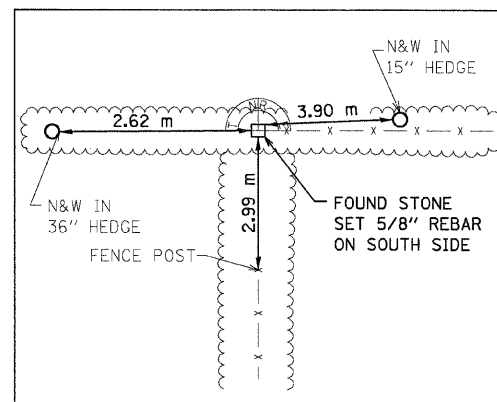
NW CORNER SECTION 24

N 255,222.6102 E 699,249.1922



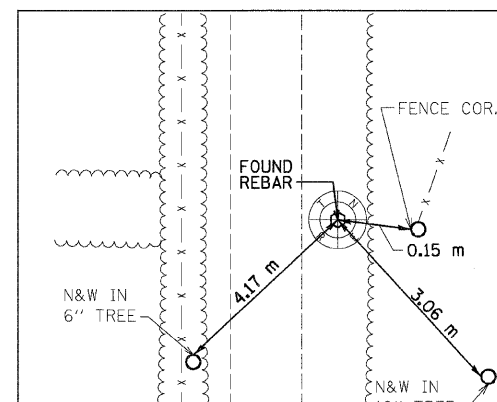
EAST 1/4 CORNER SECTION 24

N 254,452.7363 E 700,925.1417



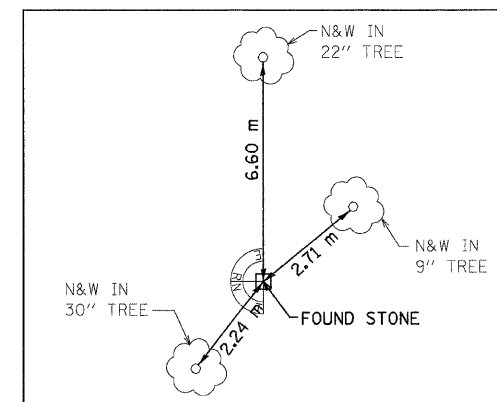
SOUTH 1/4 CORNER SECTION 24

N 253,619.5160 E 700,075.2311



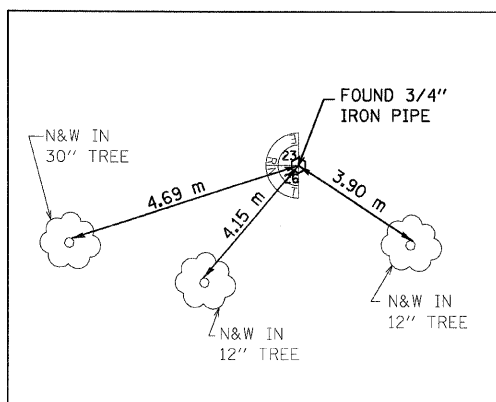
CENTER SECTION 24

N 254,429.0932 E 700,081.3198



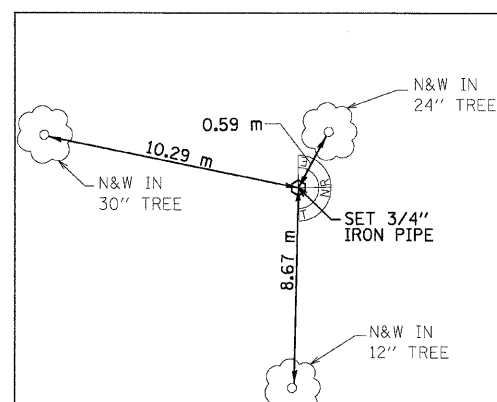
EAST 1/4 CORNER SECTION 25

N 253,157.4632 E 700,913.4030



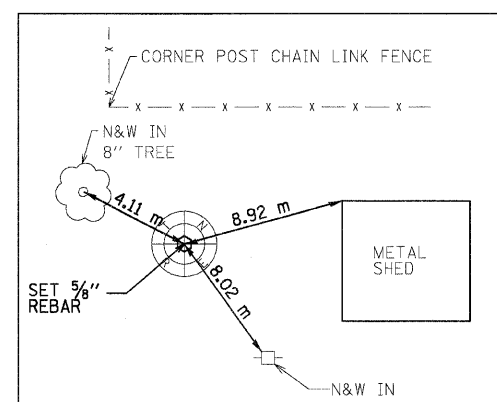
SE CORNER SECTION 23

N 253,599.3315 E 699,229.1951



WEST 1/4 CORNER SECTION 24

N 254,410.9679 E 699,239.4365



CENTER SECTION 23

N 254,420.6868 E 698,394.6957

NOTES:

- 1) DIMENSIONS LABELED "HORIZ" ARE HORIZONTAL PULLS.
- 2) DIMENSIONS NOT LABELED "HORIZ" ARE DIRECT PULLS.



(TYP ALL TIES THIS DWG)

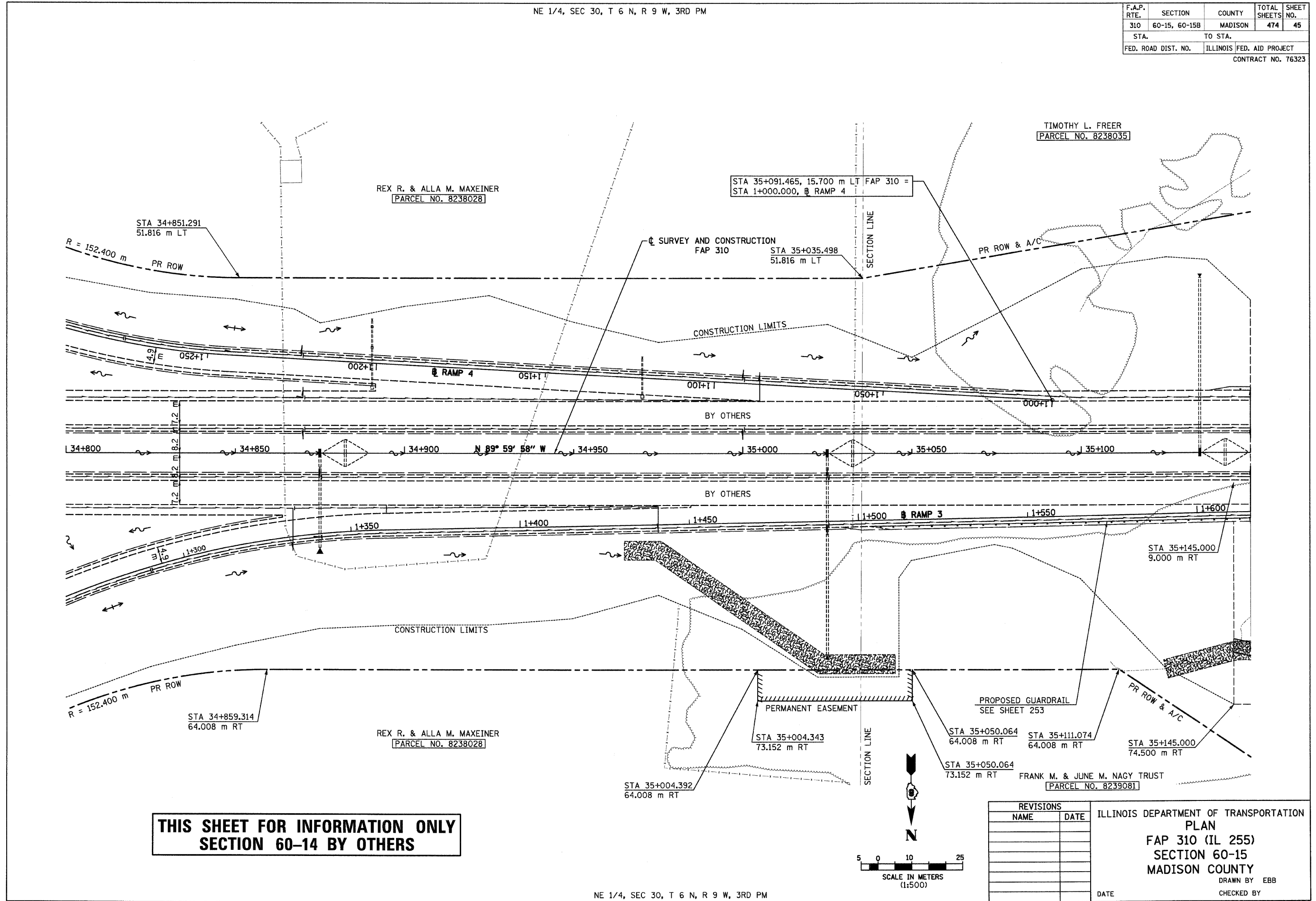
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ILLINOIS DEPARTMENT OF TRANSPORTATION
HORIZONTAL CONTROL TIES
FAP 310 (IL 255)
SECTION 60-15
MADISON COUNTY
DRAWN BY CES
CHECKED BY
DATE

6/28/2007

NE 1/4, SEC 30, T 6 N, R 9 W, 3RD PM

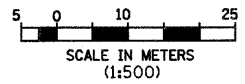
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------------|---------------------------|--------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 45 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |



**THIS SHEET FOR INFORMATION ONLY
SECTION 60-14 BY OTHERS**

| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAN
FAP 310 (IL 255)
SECTION 60-15
MADISON COUNTY
 DRAWN BY EBB
 CHECKED BY
 DATE



NE 1/4, SEC 30, T 6 N, R 9 W, 3RD PM

IL 255 (FAP 310), STA 34+800 TO STA 35+150

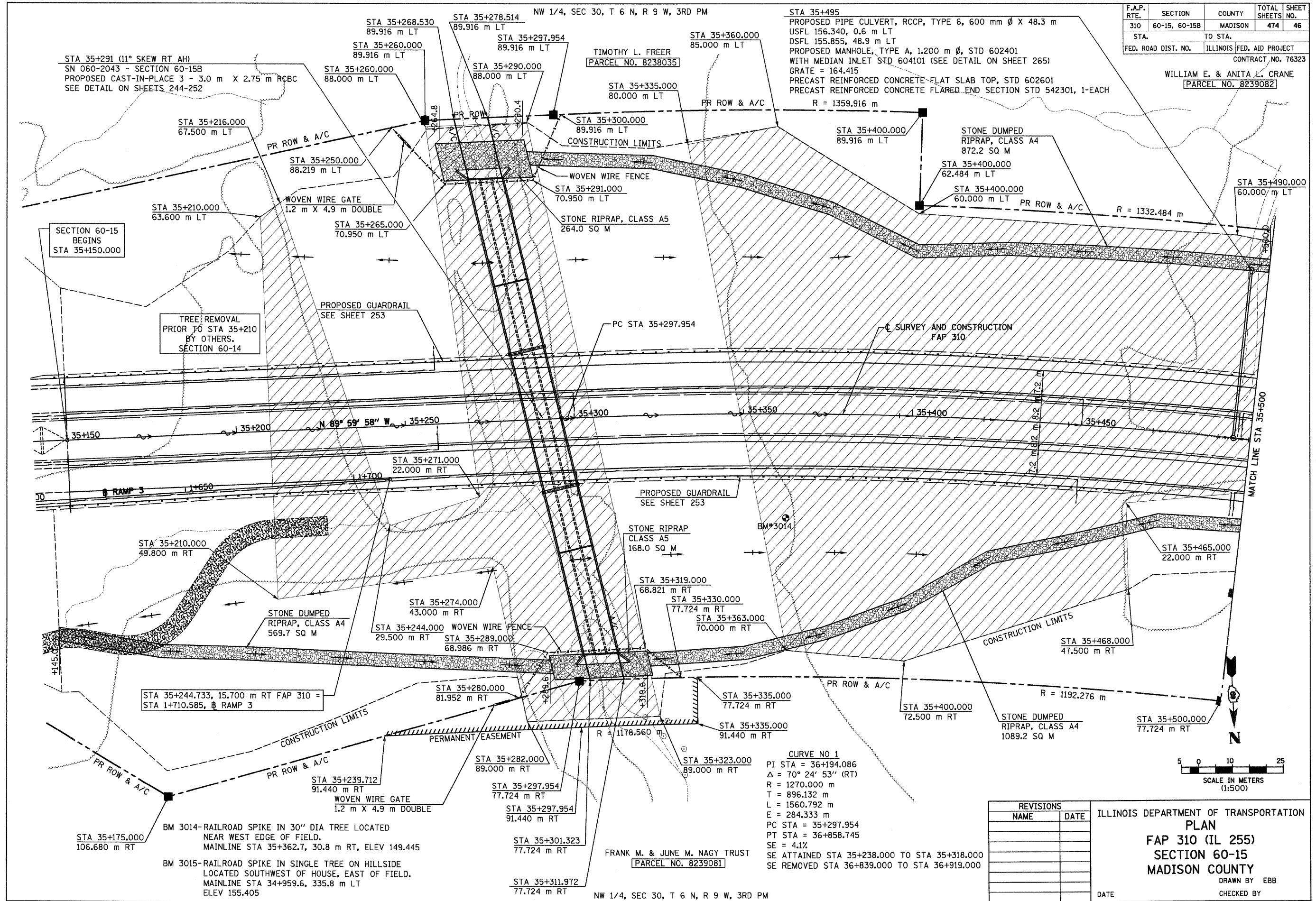
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6/28/2007

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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|---------------|---------|--------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 46 |

CONTRACT NO. 76323
 WILLIAM E. & ANITA L. CRANE
 [PARCEL NO. 8239082]



| REVISIONS | |
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| NAME | DATE |
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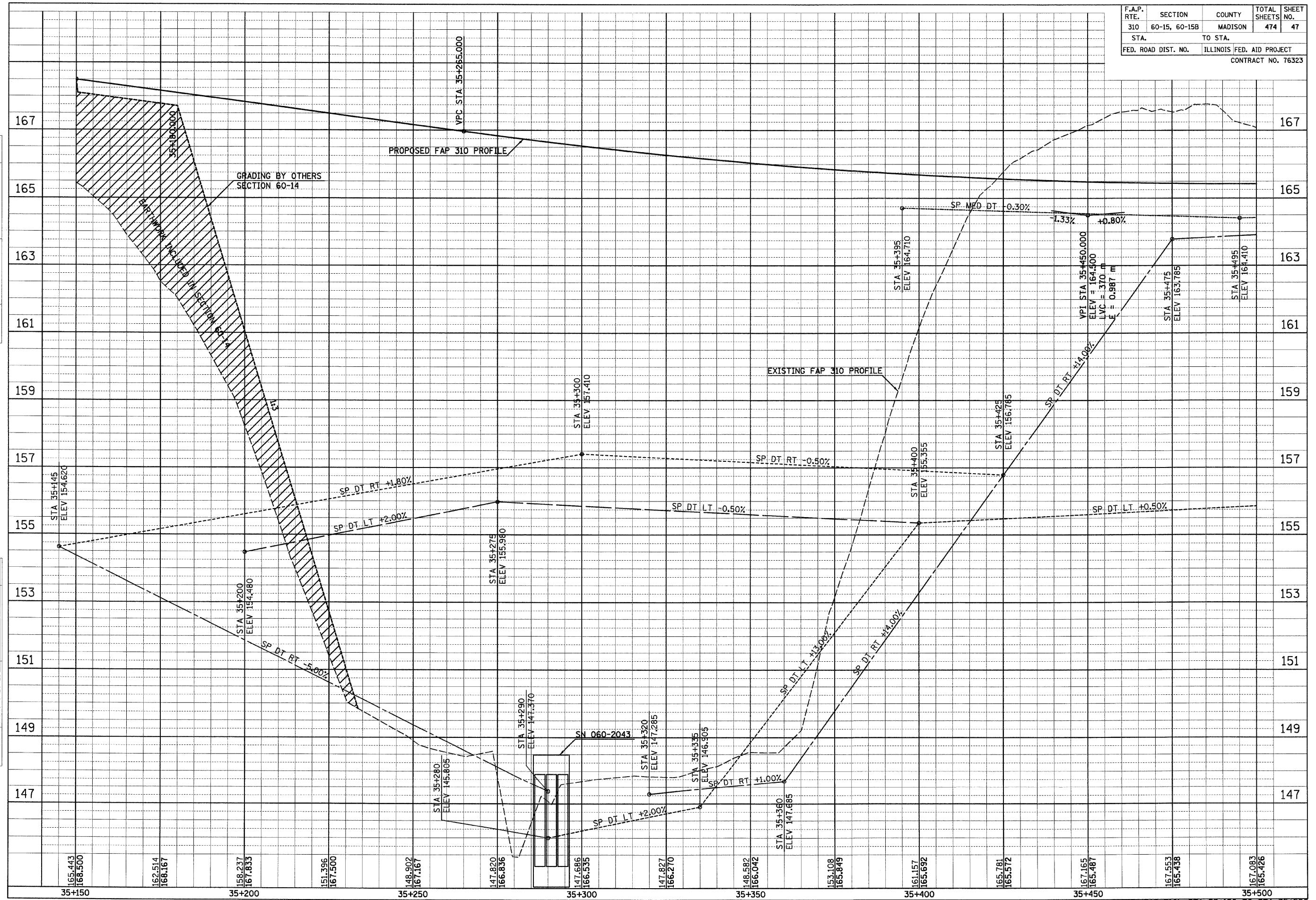
ILLINOIS DEPARTMENT OF TRANSPORTATION
 PLAN
 FAP 310 (IL 255)
 SECTION 60-15
 MADISON COUNTY
 DRAWN BY EBB
 CHECKED BY
 DATE

IL 255 (FAP 310), STA 35+150 TO STA 35+500

| ORIGINAL SURVEY | SURVEYED | BY | DATE |
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| BOOK | PLotted | | |
| NO. | DATE | | |
| | AREAS CHECKED | | |

| FINAL SURVEY | SURVEYED | BY | DATE |
|--------------|---------------|----|------|
| BOOK | PLotted | | |
| NO. | DATE | | |
| | AREAS CHECKED | | |

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------------|---------------------------|--------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 47 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |

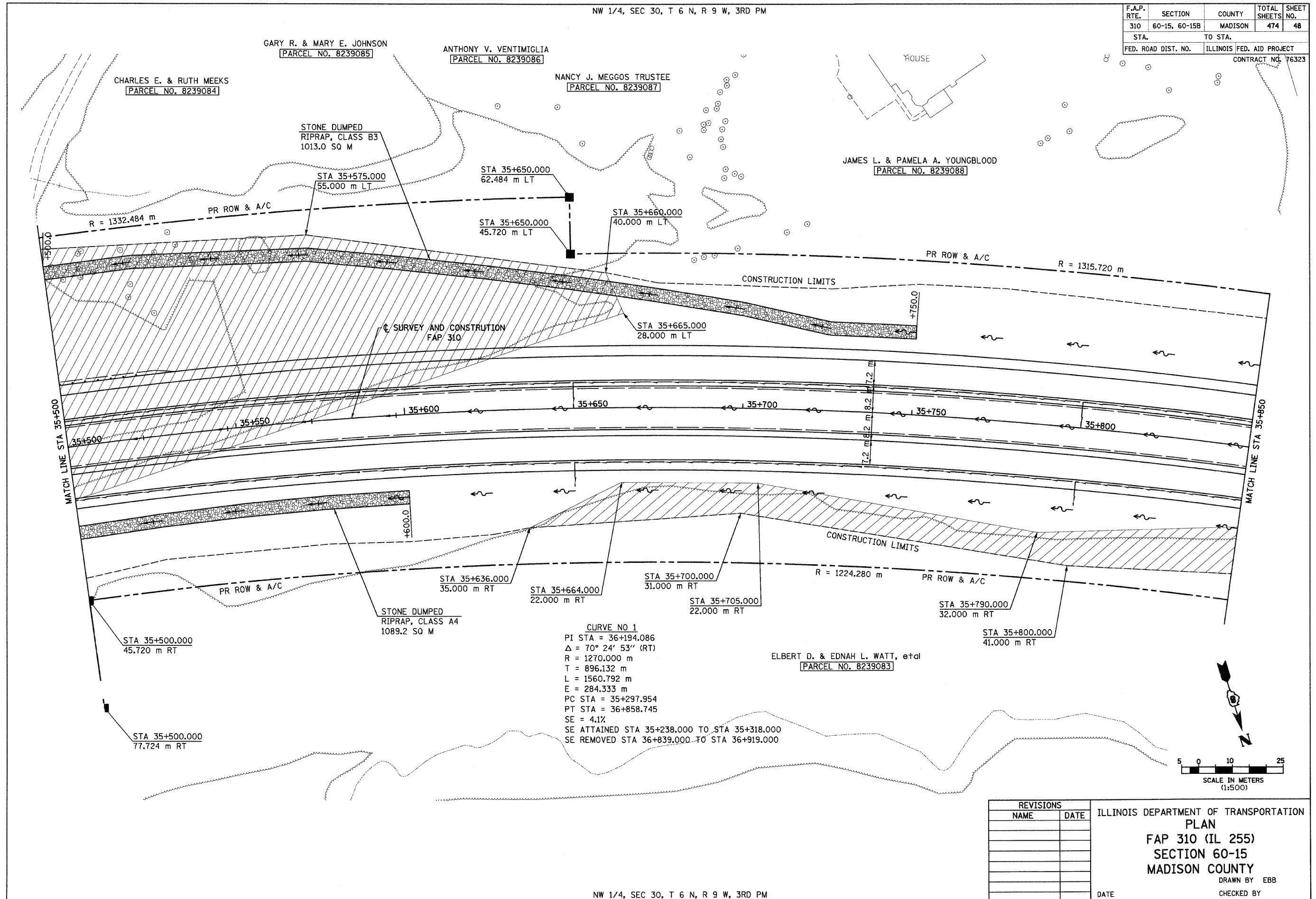


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NW 1/4, SEC 30, T 6 N, R 9 W, 3RD PM

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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 48 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | | |
| CONTRACT NO. 76323 | | | | |



NW 1/4, SEC 30, T 6 N, R 9 W, 3RD PM

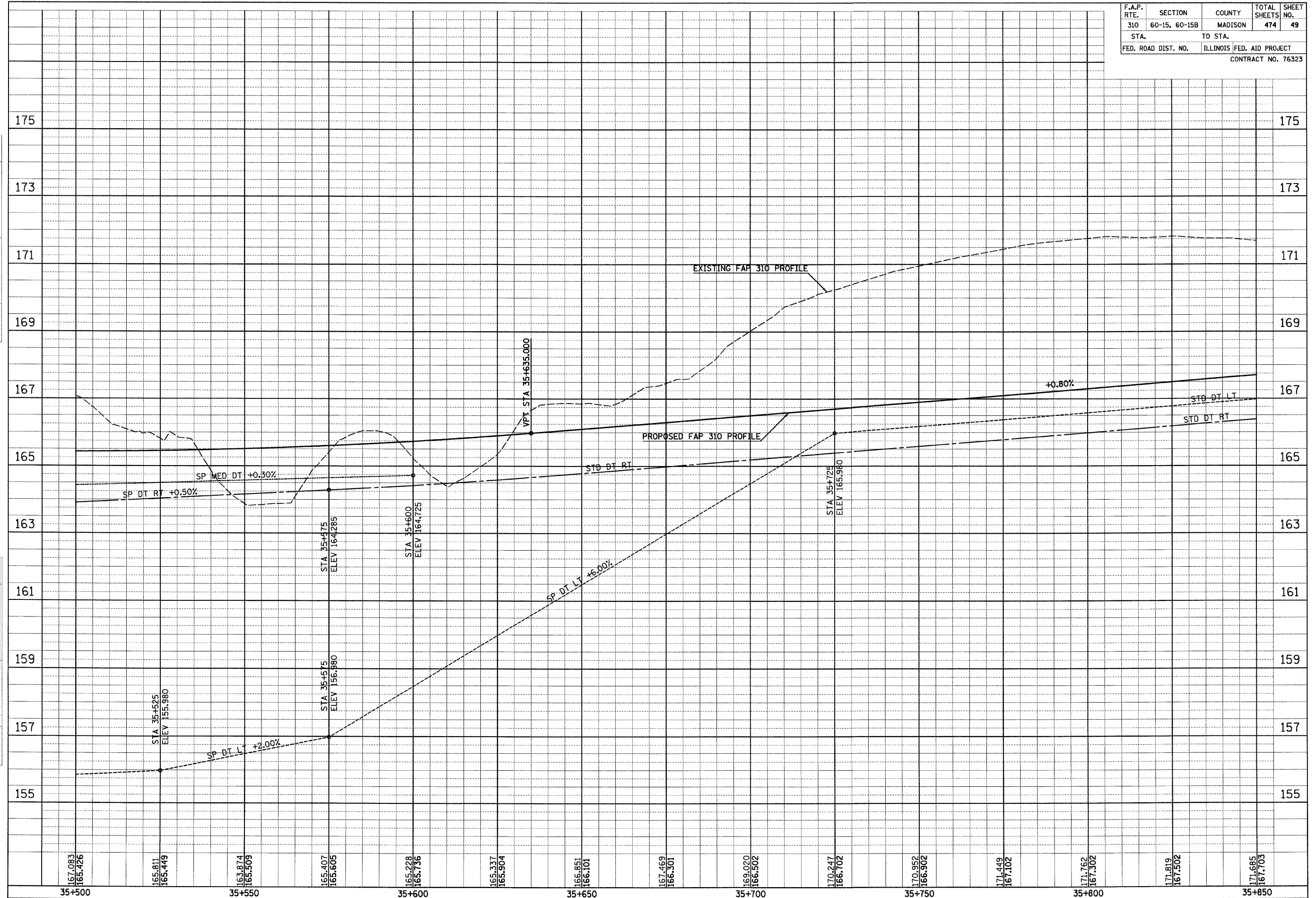
| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION PLAN FAP 310 (IL 255) SECTION 60-15 MADISON COUNTY |
|-----------|------|--|
| NAME | DATE | |
| | | DRAWN BY EBB CHECKED BY DATE |
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IL 255 (FAP 310), STA 35+500 TO STA 35+850

| ORIGINAL SURVEY | SURVEYED | BY | DATE |
|-----------------|---------------|----|------|
| NO. | NO. | | |
| AREAS CHECKED | AREAS CHECKED | | |
| TEMPLATE | TEMPLATE | | |
| NOTE BOOK | NOTE BOOK | | |

| FINAL SURVEY | SURVEYED | BY | DATE |
|---------------|---------------|----|------|
| NO. | NO. | | |
| AREAS CHECKED | AREAS CHECKED | | |
| TEMPLATE | TEMPLATE | | |
| NOTE BOOK | NOTE BOOK | | |

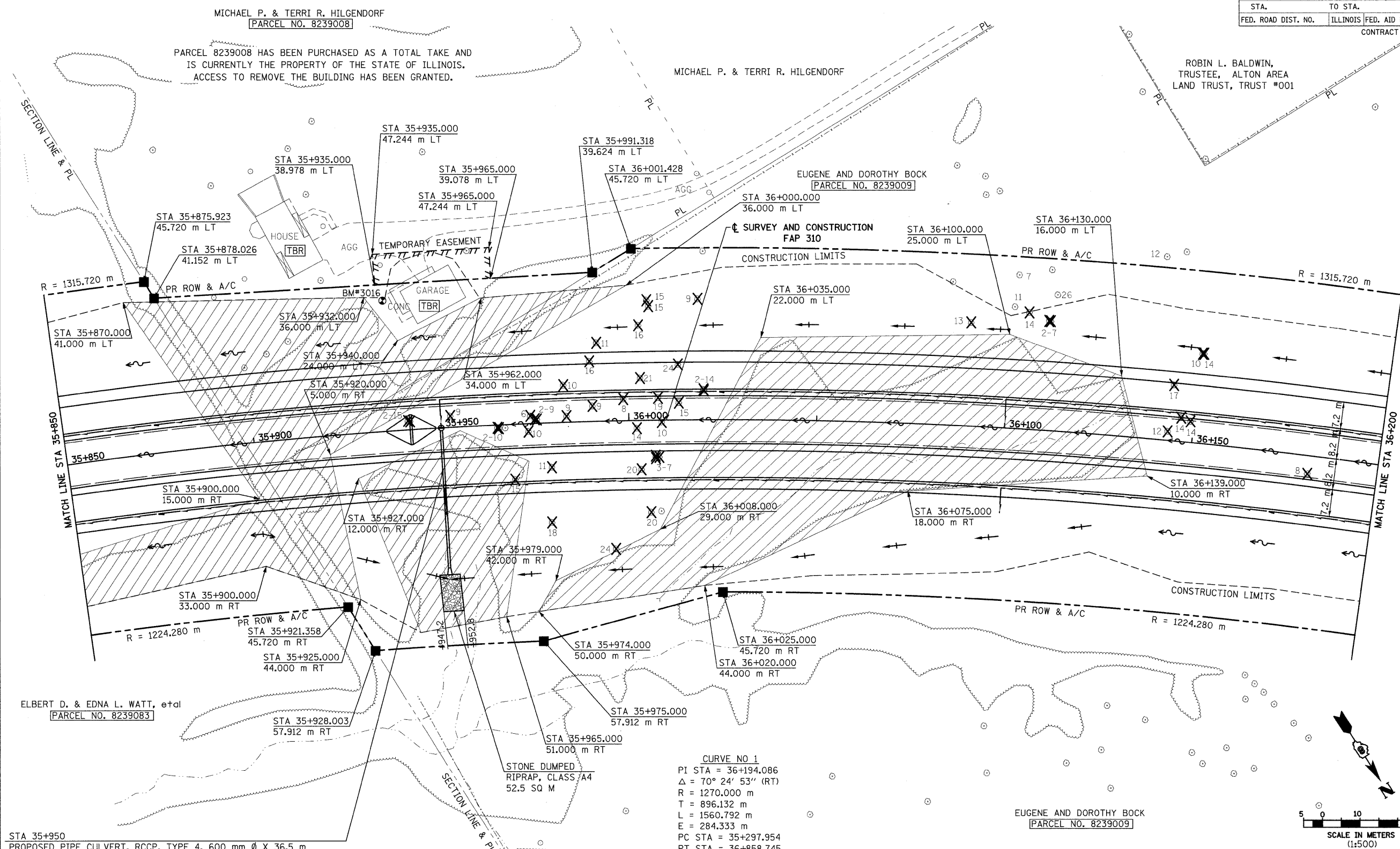
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 49 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |



11/30/2009

NE 1/4, SEC 25, T 6 N, R 10 W, 3RD PM

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|---------------------|---------------|----------|--------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 50 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |
| | | | CONTRACT NO. 76323 | |



MICHAEL P. & TERRI R. HILGENDORF
 [PARCEL NO. 8239008]

PARCEL 8239008 HAS BEEN PURCHASED AS A TOTAL TAKE AND IS CURRENTLY THE PROPERTY OF THE STATE OF ILLINOIS. ACCESS TO REMOVE THE BUILDING HAS BEEN GRANTED.

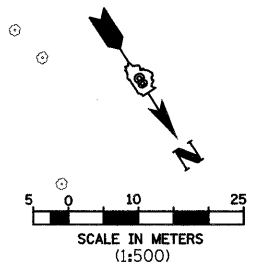
ROBIN L. BALDWIN,
 TRUSTEE, ALTON AREA
 LAND TRUST, TRUST #001

ELBERT D. & EDNA L. WATT, et al
 [PARCEL NO. 8239083]

CURVE NO 1
 PI STA = 36+194.086
 $\Delta = 70^\circ 24' 53''$ (RT)
 R = 1270.000 m
 T = 896.132 m
 L = 1560.792 m
 E = 284.333 m
 PC STA = 35+297.954
 PT STA = 36+858.745
 SE = 4.1%
 SE ATTAINED STA 35+238.000 TO STA 35+318.000
 SE REMOVED STA 36+839.000 TO STA 36+919.000

BM 3016-40d NAIL IN LIGHT POLE LOCATED NORTH OF HILGENDORF HOUSE AND EAST OF METAL GARAGE. MAINLINE STA 35+936.7, 34.8 m LT ELEV 171.485

STA 35+950
 PROPOSED PIPE CULVERT, RCCP, TYPE 4, 600 mm ϕ X 36.5 m
 USFL 162.625, 0.6 m RT
 DSFL 162.260, 37.1 m RT
 PROPOSED MANHOLE, TYPE A, 1.200 m ϕ , STD 602401
 WITH MEDIAN INLET STD 604101 (SEE DETAIL ON SHEET 265)
 GRATE = 167.495
 PRECAST REINFORCED CONCRETE FLAT SLAB TOP, STD 602601
 PRECAST REINFORCED CONCRETE FLARED END SECTION STD 542301, 1-EACH



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ILLINOIS DEPARTMENT OF TRANSPORTATION
 PLAN
 FAP 310 (IL 255)
 SECTION 60-15
 MADISON COUNTY
 DRAWN BY EBB
 CHECKED BY
 DATE

NW 1/4, SEC 30, T 6 N, R 9 W, 3RD PM

NE 1/4, SEC 25, T 6 N, R 10 W, 3RD PM

IL 255 (FAP 310), STA 35+850 TO STA 36+200

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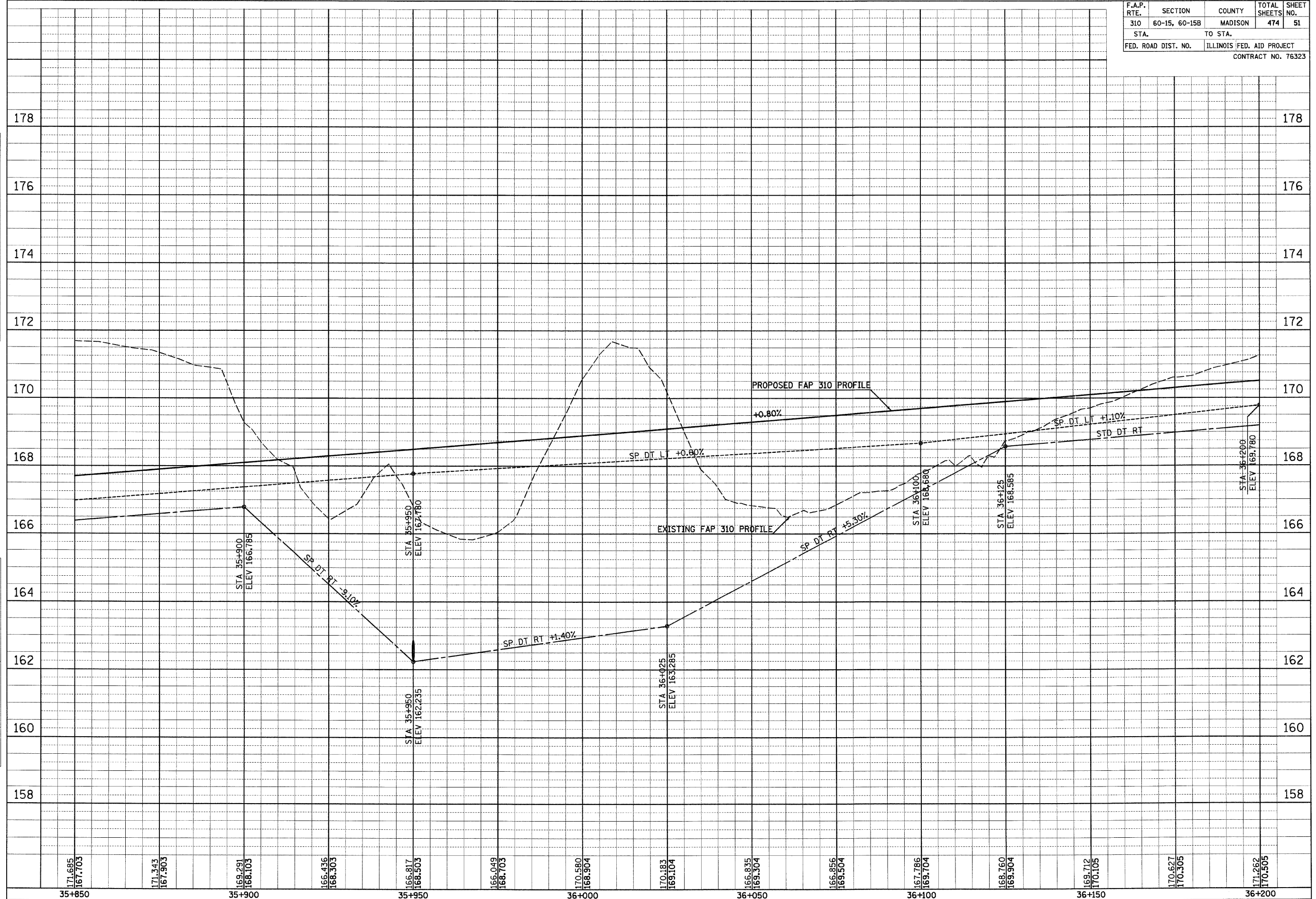
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| FINAL SURVEY | SURVEYED | BY | DATE |
| NOTE BOOK | PLOTTED | | |
| NO. | TEMPLATE | | |
| | AREAS | | |
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| ORIGINAL SURVEY | SURVEYED | BY | DATE |
| NOTE BOOK | PLOTTED | | |
| NO. | TEMPLATE | | |
| | AREAS | | |
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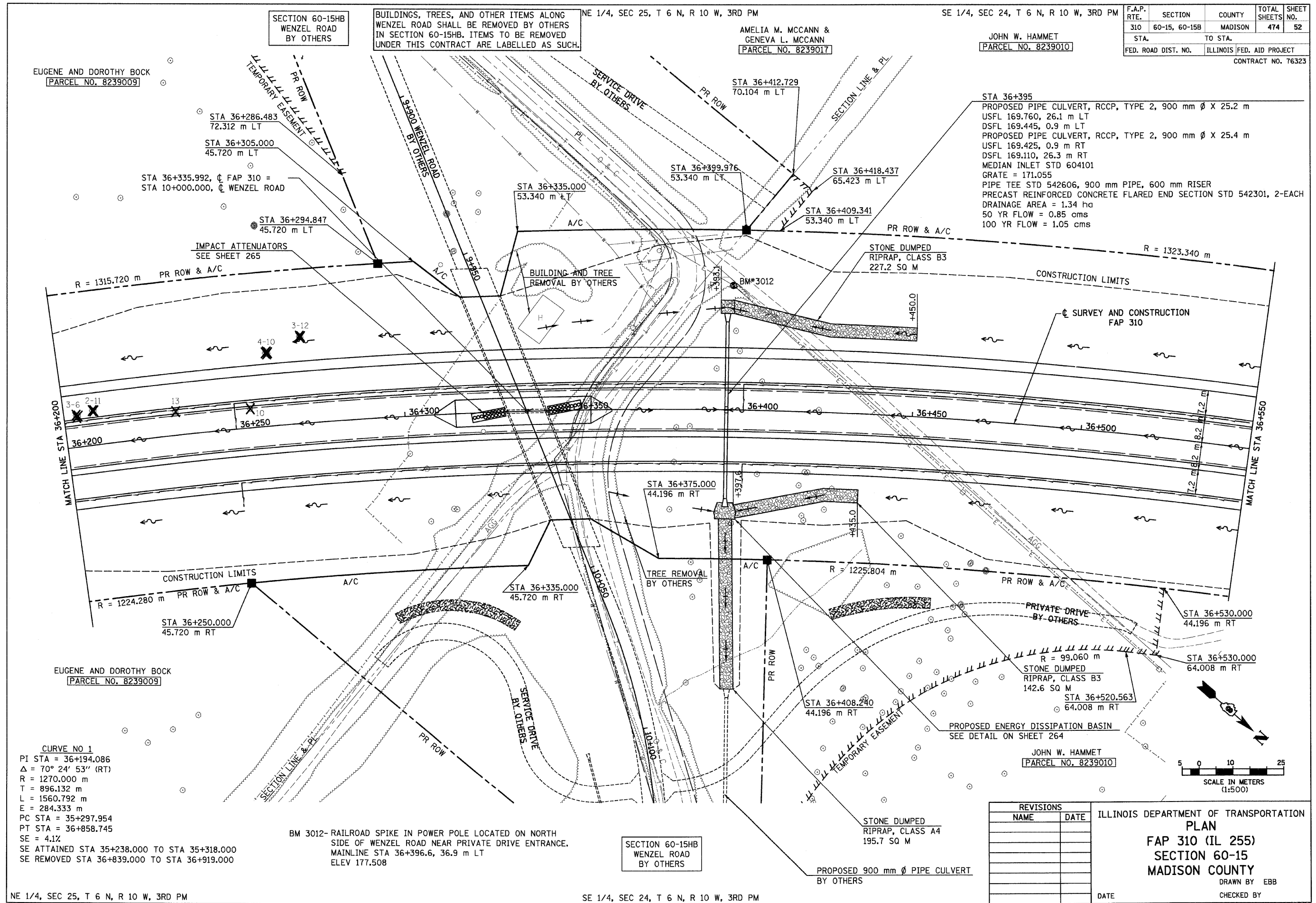
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|---------------------|---------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 51 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |



IL 255 (FAP 310), STA 35+850 TO STA 36+200

6/28/2007

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SECTION 60-15HB
WENZEL ROAD
BY OTHERS

BUILDINGS, TREES, AND OTHER ITEMS ALONG
WENZEL ROAD SHALL BE REMOVED BY OTHERS
IN SECTION 60-15HB. ITEMS TO BE REMOVED
UNDER THIS CONTRACT ARE LABELLED AS SUCH.

NE 1/4, SEC 25, T 6 N, R 10 W, 3RD PM

AMELIA M. MCCANN &
GENEVA L. MCCANN
[PARCEL NO. 8239017]

SE 1/4, SEC 24, T 6 N, R 10 W, 3RD PM

JOHN W. HAMMET
[PARCEL NO. 8239010]

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------------|---------------------------|--------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 52 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |

EUGENE AND DOROTHY BOCK
[PARCEL NO. 8239009]

STA 36+286.483
72.312 m LT

STA 36+305.000
45.720 m LT

STA 36+335.992, C FAP 310 =
STA 10+000.000, C WENZEL ROAD

STA 36+294.847
45.720 m LT

IMPACT ATTENUATORS
SEE SHEET 265

R = 1315.720 m
PR ROW & A/C

STA 36+335.000
53.340 m LT

STA 36+399.976
53.340 m LT

STA 36+418.437
65.423 m LT

STA 36+409.341
53.340 m LT

PR ROW & A/C

STONE DUMPED
RIPRAP, CLASS B3
227.2 SQ M

R = 1323.340 m

CONSTRUCTION LIMITS

SURVEY AND CONSTRUCTION
FAP 310

MATCH LINE STA 36+200

MATCH LINE STA 36+550

CONSTRUCTION LIMITS
R = 1224.280 m
PR ROW & A/C

STA 36+250.000
45.720 m RT

STA 36+375.000
44.196 m RT

STA 36+335.000
45.720 m RT

STA 36+375.000
44.196 m RT

PR ROW & A/C

PRIVATE DRIVE
BY OTHERS

STA 36+530.000
44.196 m RT

STA 36+530.000
64.008 m RT

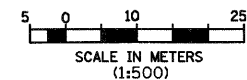
R = 99.060 m

STONE DUMPED
RIPRAP, CLASS B3
142.6 SQ M

STA 36+520.563
64.008 m RT

PROPOSED ENERGY DISSIPATION BASIN
SEE DETAIL ON SHEET 264

JOHN W. HAMMET
[PARCEL NO. 8239010]



CURVE NO 1
 PI STA = 36+194.086
 $\Delta = 70^\circ 24' 53''$ (RT)
 R = 1270.000 m
 T = 896.132 m
 L = 1560.792 m
 E = 284.333 m
 PC STA = 35+297.954
 PT STA = 36+858.745
 SE = 4.1%
 SE ATTAINED STA 35+238.000 TO STA 35+318.000
 SE REMOVED STA 36+839.000 TO STA 36+919.000

BM 3012- RAILROAD SPIKE IN POWER POLE LOCATED ON NORTH
SIDE OF WENZEL ROAD NEAR PRIVATE DRIVE ENTRANCE.
MAINLINE STA 36+396.6, 36.9 m LT
ELEV 177.508

SECTION 60-15HB
WENZEL ROAD
BY OTHERS

STONE DUMPED
RIPRAP, CLASS A4
195.7 SQ M

PROPOSED 900 mm ϕ PIPE CULVERT
BY OTHERS

| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAN
FAP 310 (IL 255)
SECTION 60-15
MADISON COUNTY

DRAWN BY EBB
CHECKED BY

DATE
IL 255 (FAP 310), STA 36+200 TO STA 36+550

NE 1/4, SEC 25, T 6 N, R 10 W, 3RD PM

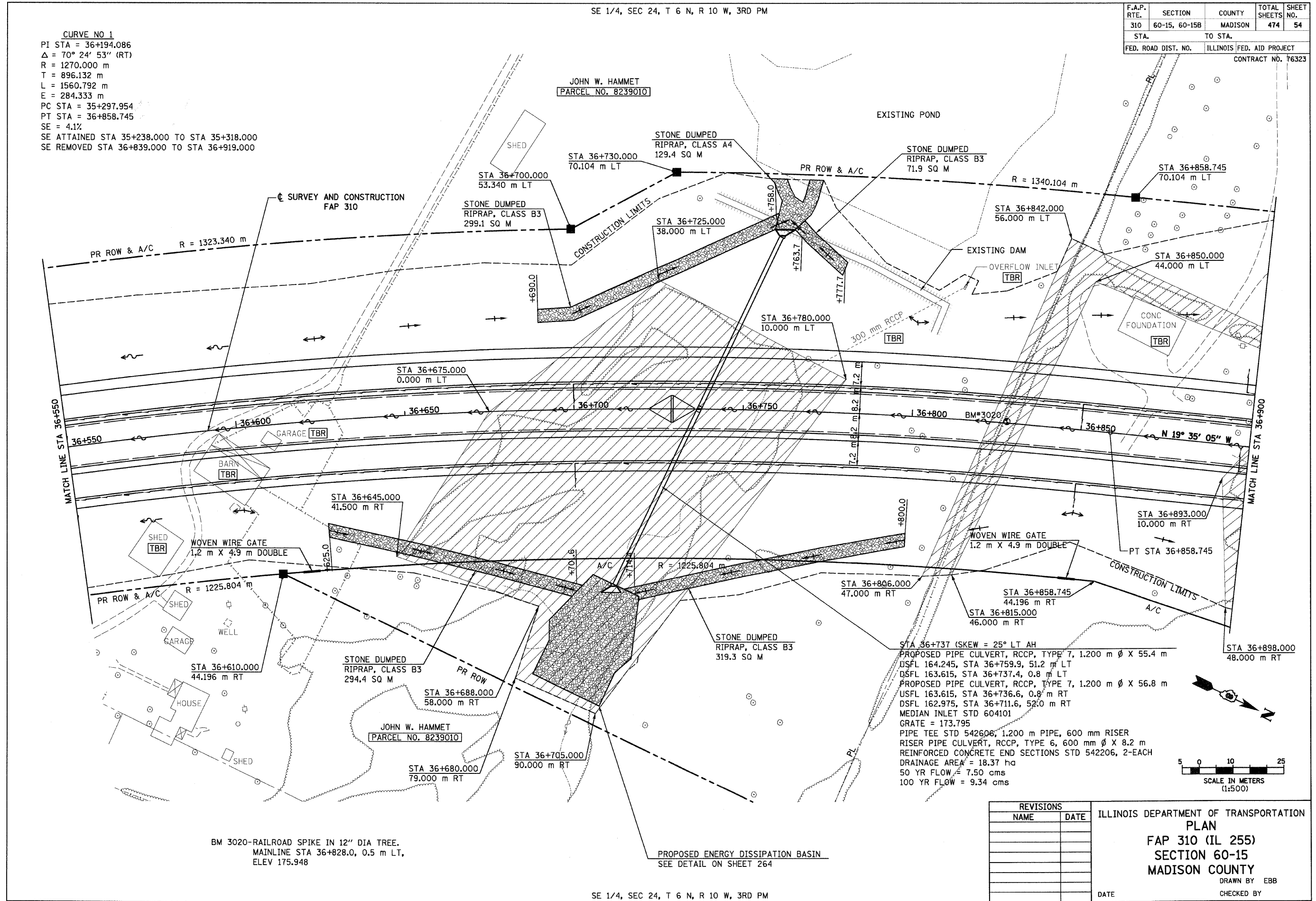
SE 1/4, SEC 24, T 6 N, R 10 W, 3RD PM

6/28/2007

SE 1/4, SEC 24, T 6 N, R 10 W, 3RD PM

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------------|---------------------------|--------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 54 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |

CURVE NO 1
 PI STA = 36+194.086
 $\Delta = 70^\circ 24' 53''$ (RT)
 R = 1270.000 m
 T = 896.132 m
 L = 1560.792 m
 E = 284.333 m
 PC STA = 35+297.954
 PT STA = 36+858.745
 SE = 4.1%
 SE ATTAINED STA 35+238.000 TO STA 35+318.000
 SE REMOVED STA 36+839.000 TO STA 36+919.000



STA 36+737 (SKEW = 25° LT AH)
 PROPOSED PIPE CULVERT, RCCP, TYPE 7, 1,200 m ϕ X 55.4 m
 USFL 164.245, STA 36+759.9, 51.2 m LT
 DSFL 163.615, STA 36+737.4, 0.8 m LT
 PROPOSED PIPE CULVERT, RCCP, TYPE 7, 1,200 m ϕ X 56.8 m
 USFL 163.615, STA 36+736.6, 0.8 m RT
 DSFL 162.975, STA 36+711.6, 52.0 m RT
 MEDIAN INLET STD 604101
 GRATE = 173.795
 PIPE TEE STD 542606, 1,200 m PIPE, 600 mm RISER
 RISER PIPE CULVERT, RCCP, TYPE 6, 600 m ϕ X 8.2 m
 REINFORCED CONCRETE END SECTIONS STD 542206, 2-EACH
 DRAINAGE AREA = 18.37 ha
 50 YR FLOW = 7.50 cms
 100 YR FLOW = 9.34 cms

BM 3020-RAILROAD SPIKE IN 12" DIA TREE.
 MAINLINE STA 36+828.0, 0.5 m LT,
 ELEV 175.948

PROPOSED ENERGY DISSIPATION BASIN
 SEE DETAIL ON SHEET 264

| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAN
FAP 310 (IL 255)
SECTION 60-15
MADISON COUNTY
 DRAWN BY EBB
 CHECKED BY
 DATE

SE 1/4, SEC 24, T 6 N, R 10 W, 3RD PM

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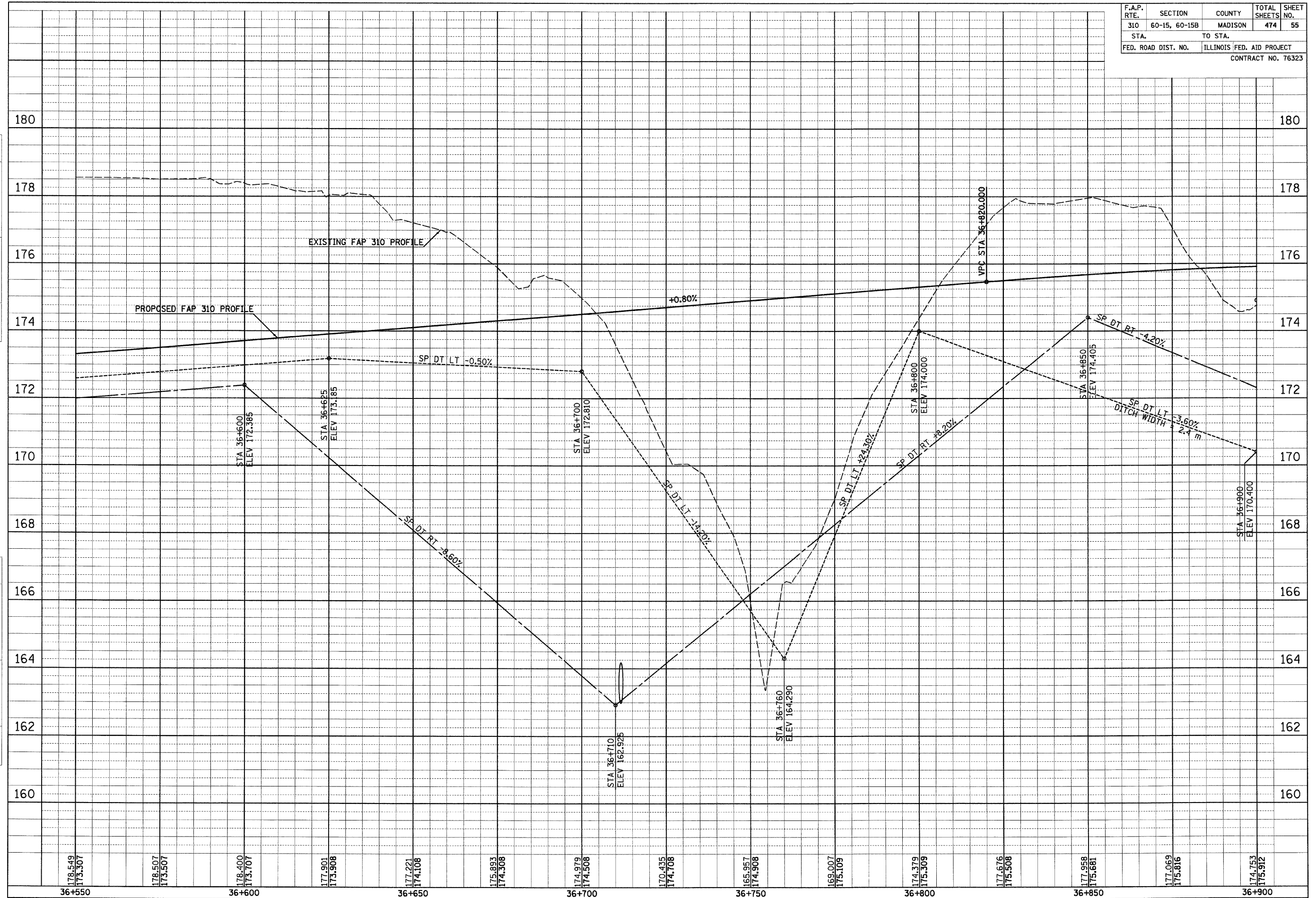
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| FINAL SURVEY PLOTTED | BY | DATE |
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 55 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |

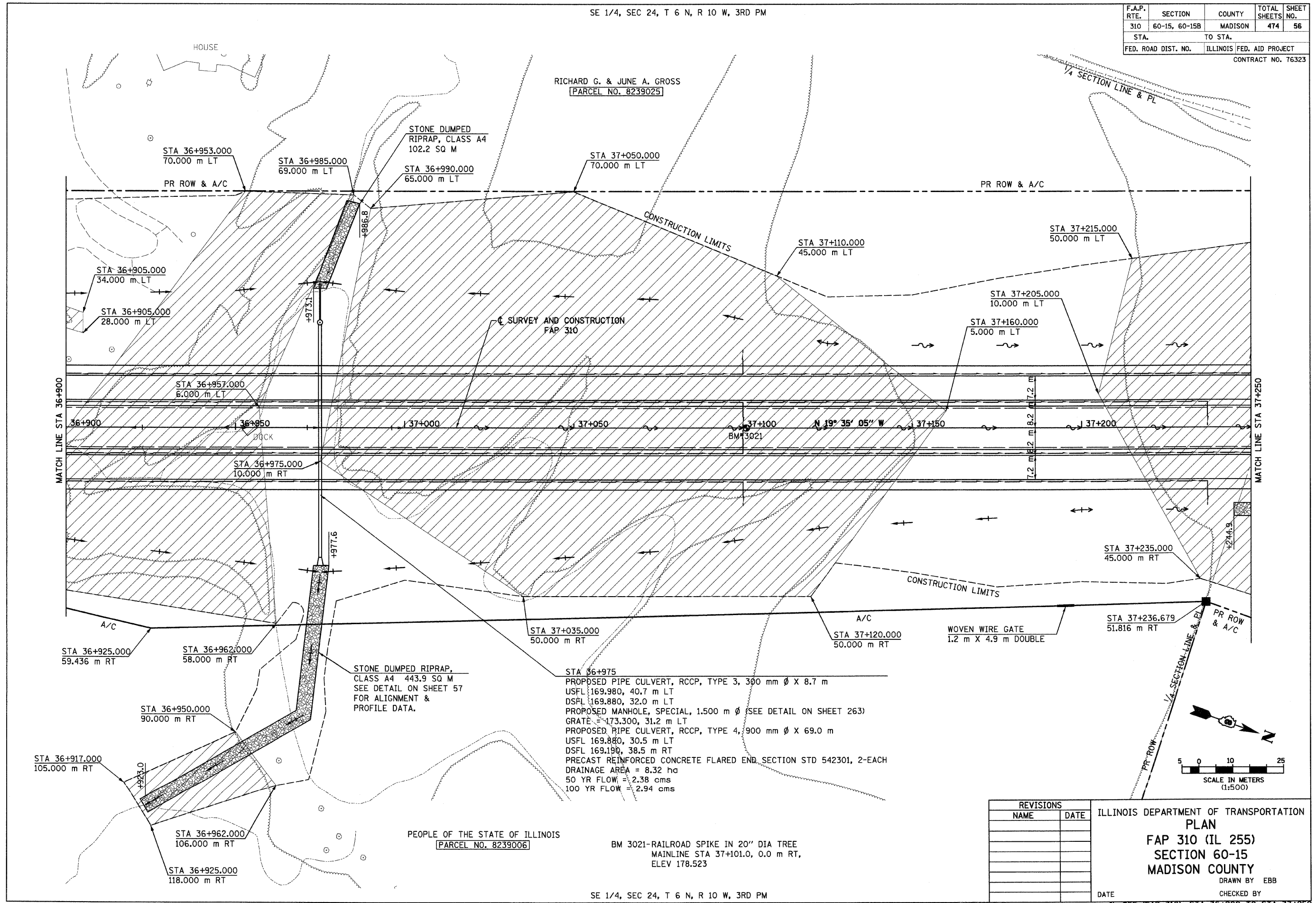


IL 255 (FAP 310), STA 36+550 TO STA 36+900

6/28/2007

SE 1/4, SEC 24, T 6 N, R 10 W, 3RD PM

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|---------------------|---------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 56 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |



HOUSE

RICHARD G. & JUNE A. GROSS
[PARCEL NO. 8239025]

STONE DUMPED RIPRAP, CLASS A4 102.2 SQ M

STONE DUMPED RIPRAP, CLASS A4 443.9 SQ M
SEE DETAIL ON SHEET 57 FOR ALIGNMENT & PROFILE DATA.

PROPOSED PIPE CULVERT, RCCP, TYPE 3, 300 mm Ø X 8.7 m
USFL 169.980, 40.7 m LT
DSFL 169.880, 32.0 m LT

PROPOSED MANHOLE, SPECIAL, 1,500 mm Ø (SEE DETAIL ON SHEET 263)
GRATE = 173.300, 31.2 m LT

PROPOSED PIPE CULVERT, RCCP, TYPE 4, 900 mm Ø X 69.0 m
USFL 169.880, 30.5 m LT
DSFL 169.190, 38.5 m RT

PRECAST REINFORCED CONCRETE FLARED END SECTION STD 542301, 2-EACH
DRAINAGE AREA = 8.32 ha
50 YR FLOW = 2.38 cms
100 YR FLOW = 2.94 cms

PEOPLE OF THE STATE OF ILLINOIS
[PARCEL NO. 8239006]

BM 3021-RAILROAD SPIKE IN 20" DIA TREE
MAINLINE STA 37+101.0, 0.0 m RT,
ELEV 178.523

SE 1/4, SEC 24, T 6 N, R 10 W, 3RD PM

| REVISIONS | |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAN
FAP 310 (IL 255)
SECTION 60-15
MADISON COUNTY

DRAWN BY EBB
CHECKED BY
DATE

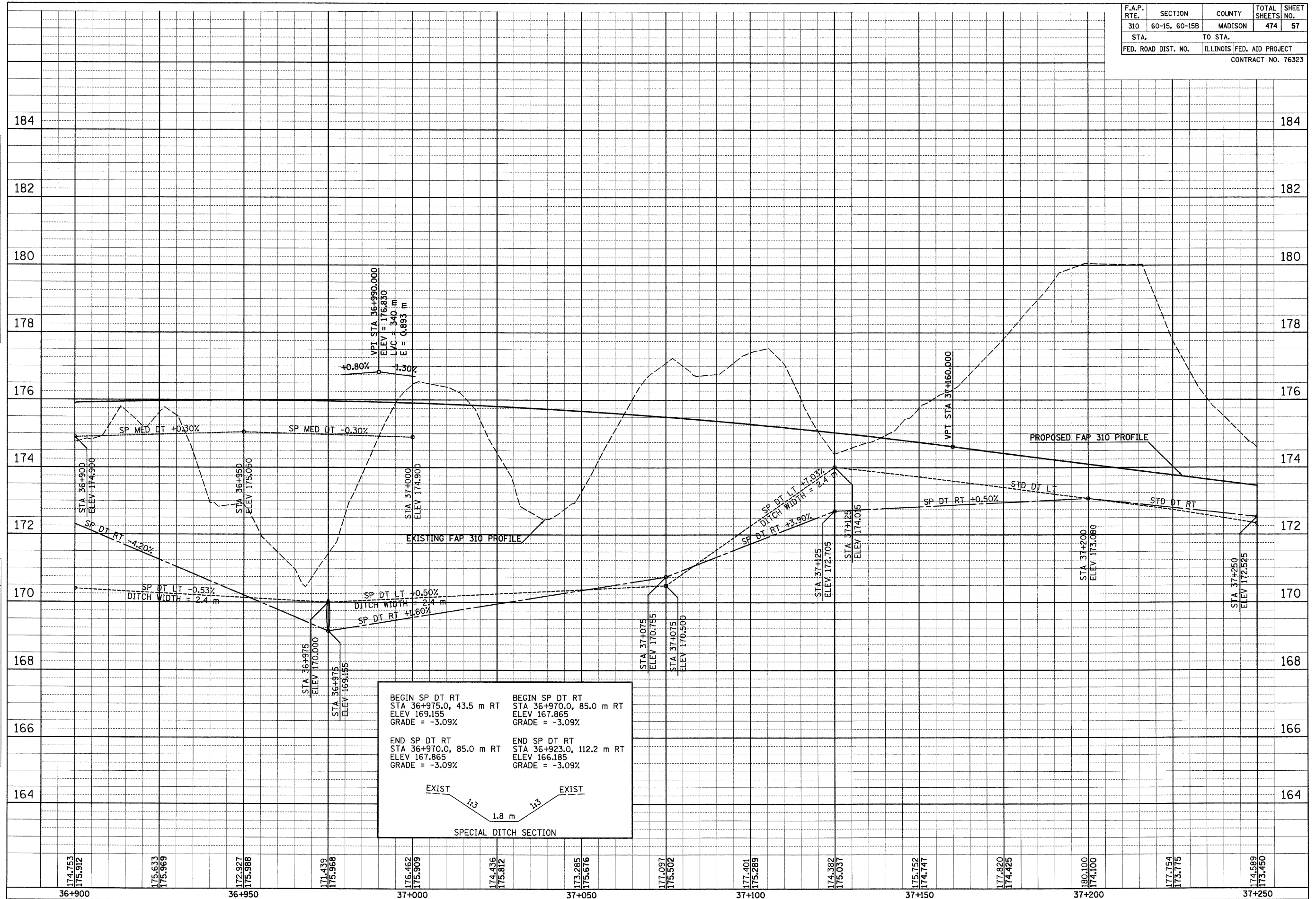
IL 255 (FAP 310), STA 36+900 TO STA 37+250

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| NO. | TEMP. DATE | | |
| | AREAS | CHECKED | |

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| ORIGINAL SURVEY | SURVEYED | BY | DATE |
| NOTE BOOK | PLOTTED | | |
| NO. | TEMP. DATE | | |
| | AREAS | CHECKED | |

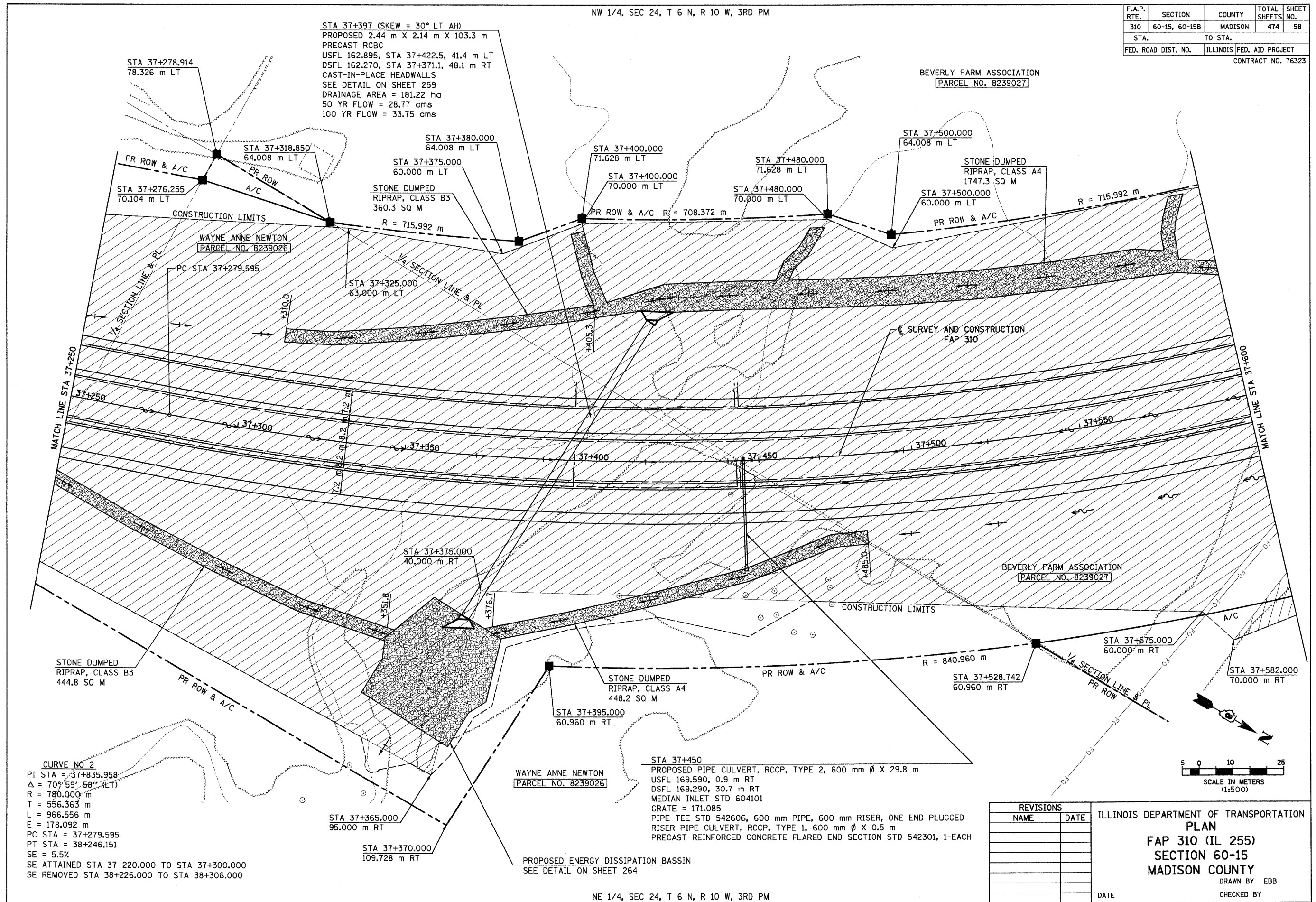
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 57 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |



6/28/2007

NW 1/4, SEC 24, T 6 N, R 10 W, 3RD PM

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|---------------------|---------------------------|---------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 58 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | | |
| CONTRACT NO. 76323 | | | | |



STA 37+397 (SKEW = 30° LT AH)
 PROPOSED 2.44 m X 2.14 m X 103.3 m
 PRECAST RCBC
 USFL 162.895, STA 37+422.5, 41.4 m LT
 DSFL 162.270, STA 37+371.1, 48.1 m RT
 CAST-IN-PLACE HEADWALLS
 SEE DETAIL ON SHEET 259
 DRAINAGE AREA = 181.22 ha
 50 YR FLOW = 28.77 cms
 100 YR FLOW = 33.75 cms

BEVERLY FARM ASSOCIATION
 [PARCEL NO. 8239027]

WAYNE ANNE NEWTON
 [PARCEL NO. 8239026]

BEVERLY FARM ASSOCIATION
 [PARCEL NO. 8239027]

WAYNE ANNE NEWTON
 [PARCEL NO. 8239026]

CURVE NO 2
 PI STA = 37+835.958
 $\Delta = 70^\circ 59' 58''$ (LT)
 R = 790.000 m
 T = 556.363 m
 L = 966.556 m
 E = 178.092 m
 PC STA = 37+279.595
 PT STA = 38+246.151
 SE = 5.5%
 SE ATTAINED STA 37+220.000 TO STA 37+300.000
 SE REMOVED STA 38+226.000 TO STA 38+306.000

STA 37+450
 PROPOSED PIPE CULVERT, RCCP, TYPE 2, 600 mm ϕ X 29.8 m
 USFL 169.590, 0.9 m RT
 DSFL 169.290, 30.7 m RT
 MEDIAN INLET STD 604101
 GRATE = 171.085
 PIPE TEE STD 542606, 600 mm PIPE, 600 mm RISER, ONE END PLUGGED
 RISER PIPE CULVERT, RCCP, TYPE 1, 600 mm ϕ X 0.5 m
 PRECAST REINFORCED CONCRETE FLARED END SECTION STD 542301, 1-EACH

PROPOSED ENERGY DISSIPATION BASSIN
 SEE DETAIL ON SHEET 264

| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
 PLAN
 FAP 310 (IL 255)
 SECTION 60-15
 MADISON COUNTY
 DRAWN BY EBB
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 DATE

NE 1/4, SEC 24, T 6 N, R 10 W, 3RD PM

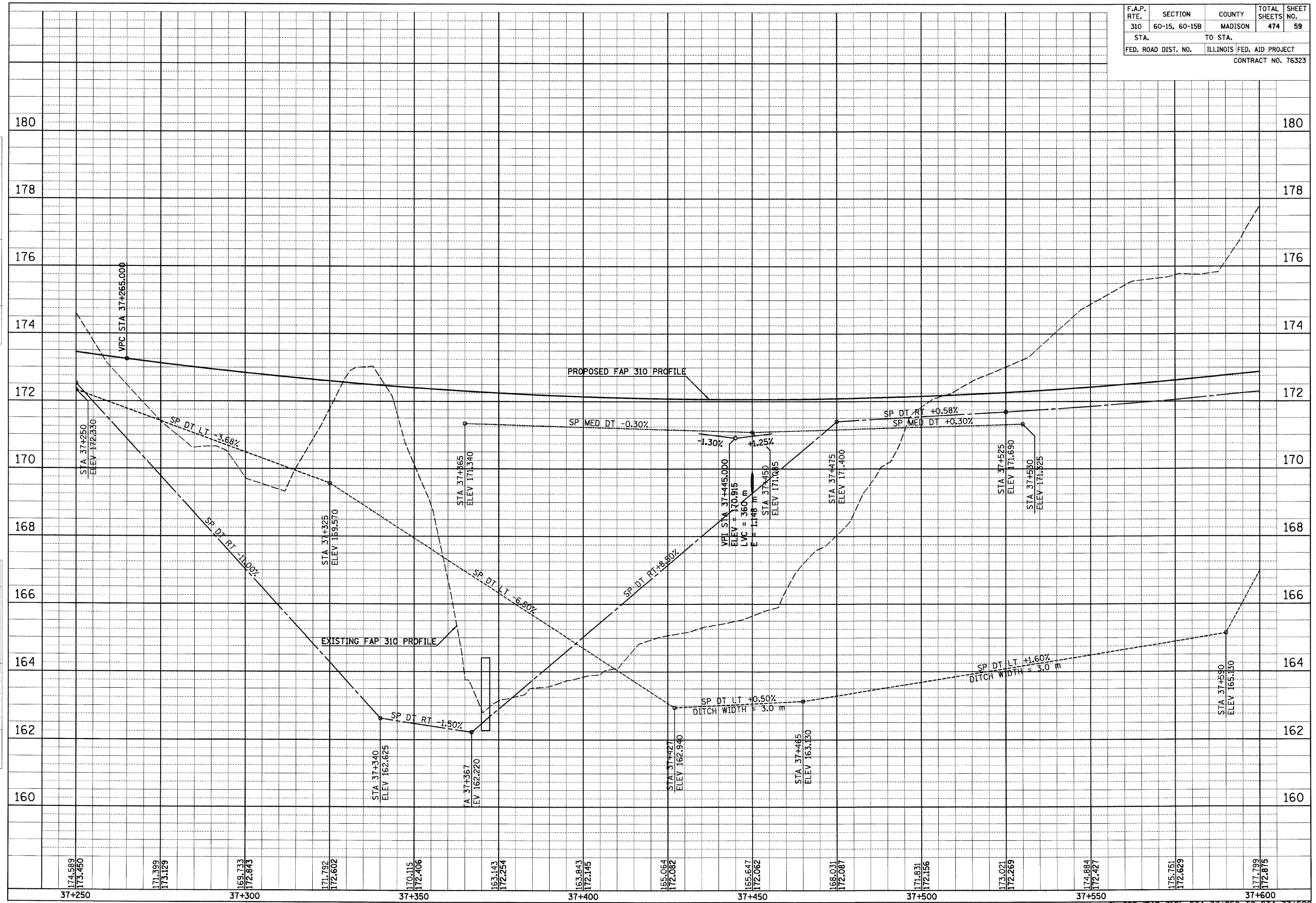
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IL 255 (FAP 310), STA 37+250 TO STA 37+600

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| FINAL SURVEY | SURVEYED | BY | DATE |
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------------|---------------------------|--------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 59 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |

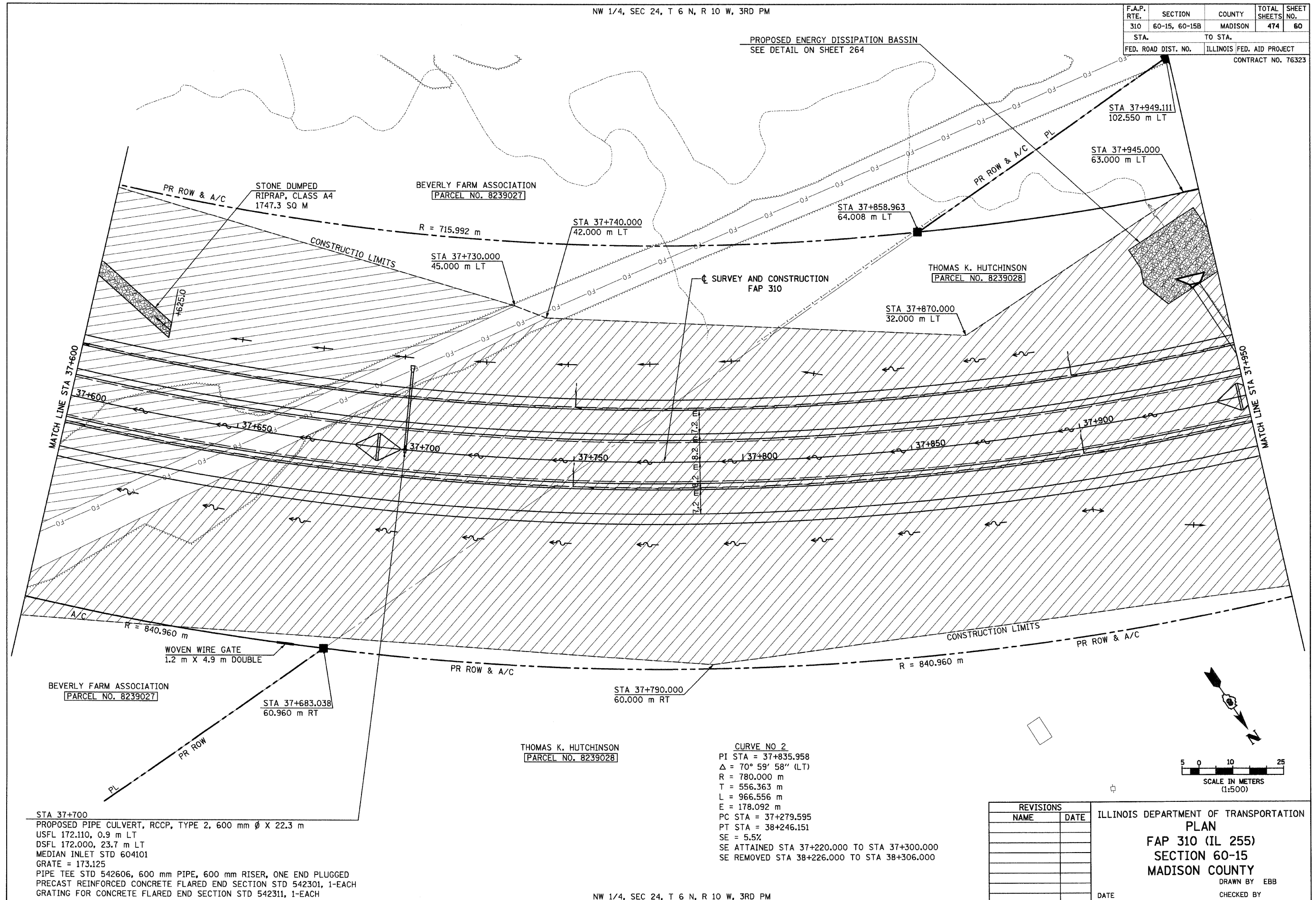


6/28/2007

NW 1/4, SEC 24, T 6 N, R 10 W, 3RD PM

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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 60 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | | |
| CONTRACT NO. 76323 | | | | |

PROPOSED ENERGY DISSIPATION BASSIN
SEE DETAIL ON SHEET 264



STA 37+700
 PROPOSED PIPE CULVERT, RCCP, TYPE 2, 600 mm Ø X 22.3 m
 USFL 172.110, 0.9 m LT
 DSFL 172.000, 23.7 m LT
 MEDIAN INLET STD 604101
 GRATE = 173.125
 PIPE TEE STD 542606, 600 mm PIPE, 600 mm RISER, ONE END PLUGGED
 PRECAST REINFORCED CONCRETE FLARED END SECTION STD 542301, 1-EACH
 GRATING FOR CONCRETE FLARED END SECTION STD 542311, 1-EACH

CURVE NO 2
 PI STA = 37+835.958
 $\Delta = 70^\circ 59' 58''$ (LT)
 R = 780.000 m
 T = 556.363 m
 L = 966.556 m
 E = 178.092 m
 PC STA = 37+279.595
 PT STA = 38+246.151
 SE = 5.5%
 SE ATTAINED STA 37+220.000 TO STA 37+300.000
 SE REMOVED STA 38+226.000 TO STA 38+306.000

| REVISIONS | |
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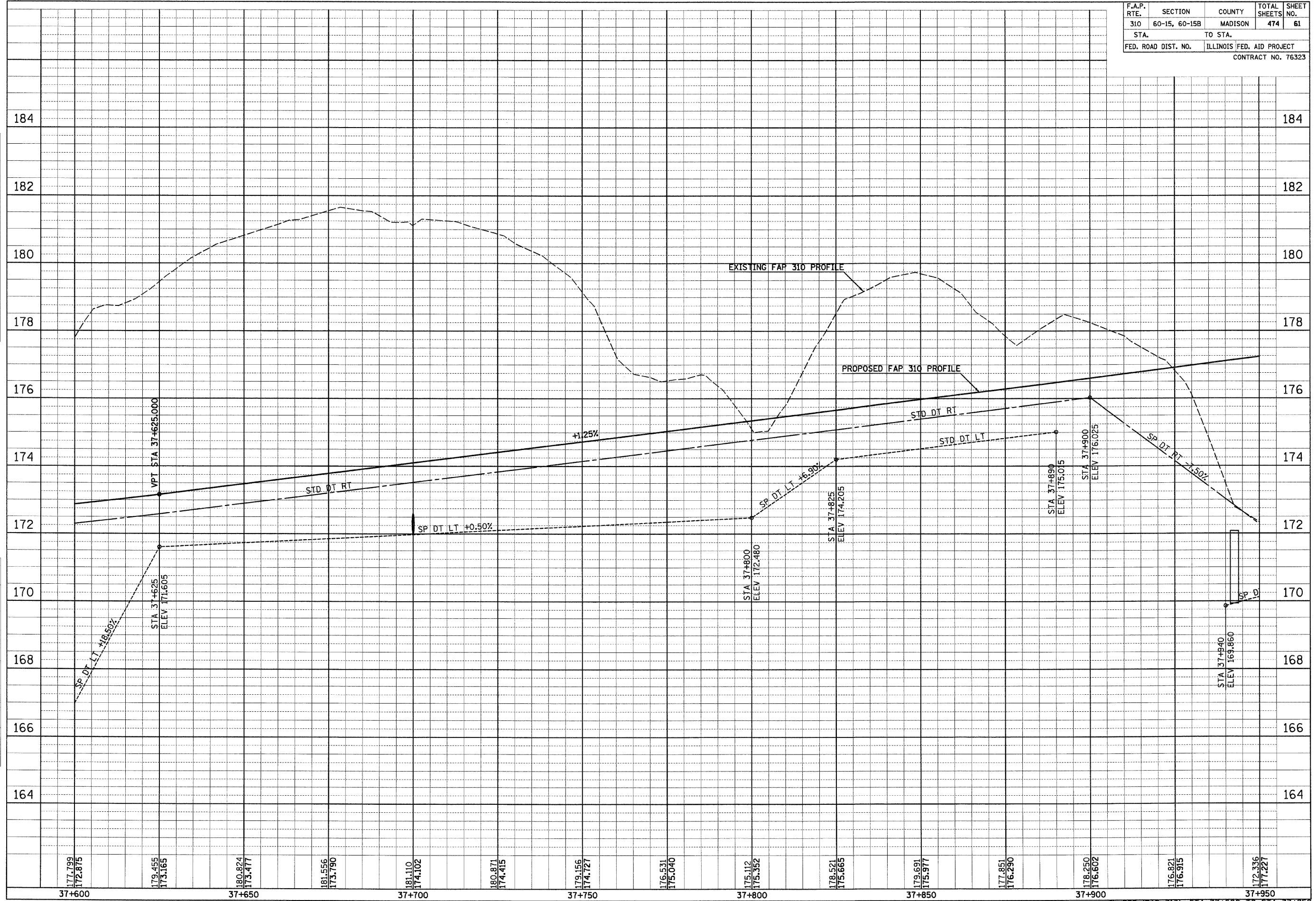
ILLINOIS DEPARTMENT OF TRANSPORTATION
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FAP 310 (IL 255)
SECTION 60-15
MADISON COUNTY
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 IL 255 (FAP 310), STA 37+600 TO STA 37+950

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| FINAL SURVEY | SURVEYED | BY | DATE |
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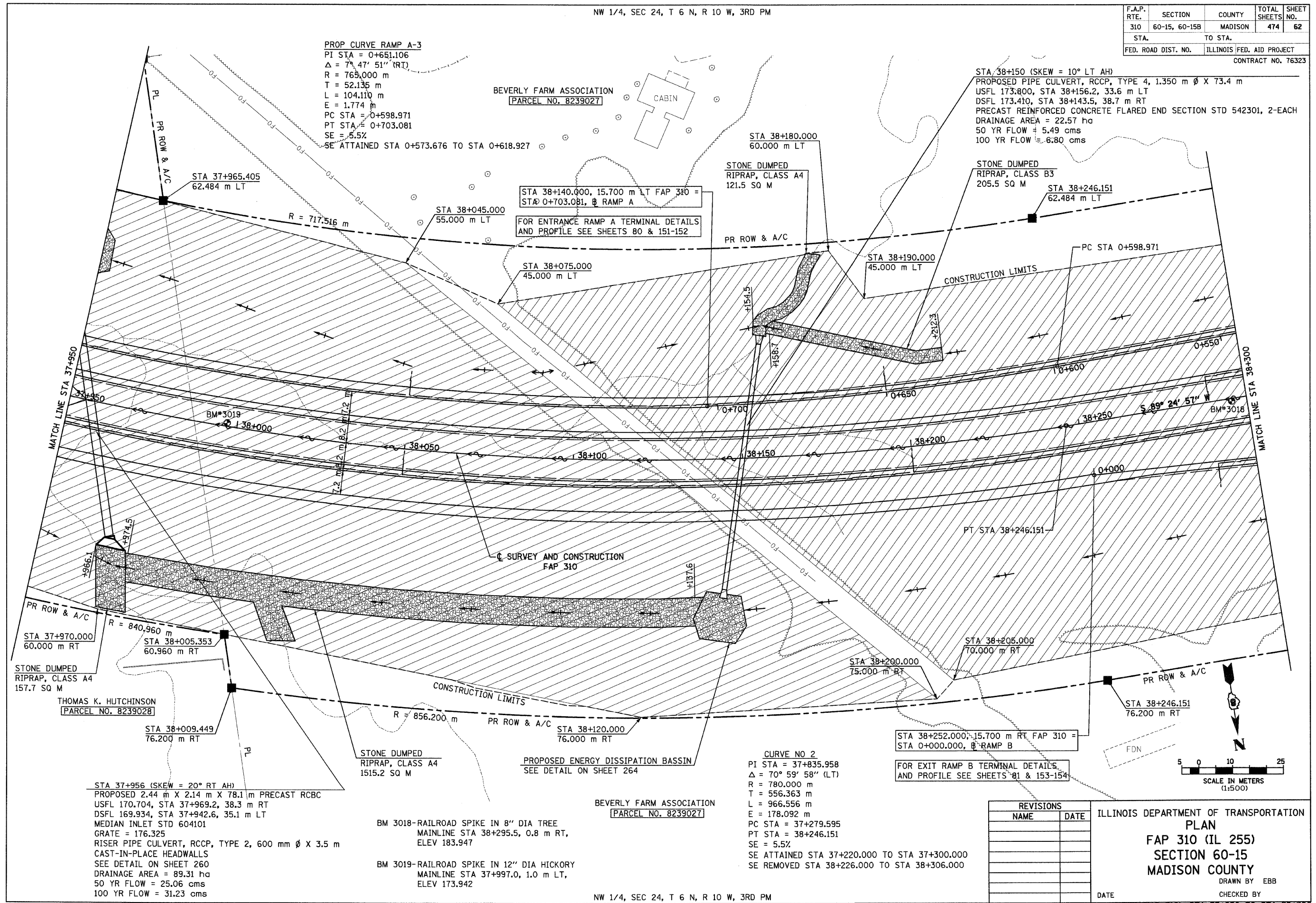
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 61 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | | |
| CONTRACT NO. 76323 | | | | |



6/28/2007

NW 1/4, SEC 24, T 6 N, R 10 W, 3RD PM

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------------|---------------------------|--------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 62 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |



PROP CURVE RAMP A-3
 PI STA = 0+651.106
 $\Delta = 7^\circ 47' 51''$ (RT)
 R = 765.000 m
 T = 52.135 m
 L = 104.110 m
 E = 1.774 m
 PC STA = 0+598.971
 PT STA = 0+703.081
 SE = 5.5%
 SE ATTAINED STA 0+573.676 TO STA 0+618.927

STA 38+150 (SKEW = 10° LT AH)
 PROPOSED PIPE CULVERT, RCCP, TYPE 4, 1.350 m ϕ X 73.4 m
 USFL 173.800, STA 38+156.2, 33.6 m LT
 DSFL 173.410, STA 38+143.5, 38.7 m RT
 PRECAST REINFORCED CONCRETE FLARED END SECTION STD 542301, 2-EACH
 DRAINAGE AREA = 22.57 ha
 50 YR FLOW = 5.49 cms
 100 YR FLOW = 6.80 cms

STA 38+140.000, 15.700 m LT FAP 310 = STA 0+703.081, B RAMP A
 FOR ENTRANCE RAMP A TERMINAL DETAILS AND PROFILE SEE SHEETS 80 & 151-152

STONE DUMPED RIPRAP, CLASS A4 121.5 SQ M

STONE DUMPED RIPRAP, CLASS B3 205.5 SQ M

STONE DUMPED RIPRAP, CLASS A4 157.7 SQ M

STONE DUMPED RIPRAP, CLASS A4 1515.2 SQ M

PROPOSED ENERGY DISSIPATION BASSIN SEE DETAIL ON SHEET 264

STA 38+252.000, 15.700 m RT FAP 310 = STA 0+000.000, B RAMP B
 FOR EXIT RAMP B TERMINAL DETAILS AND PROFILE SEE SHEETS 81 & 153-154

CURVE NO 2
 PI STA = 37+835.958
 $\Delta = 70^\circ 59' 58''$ (LT)
 R = 780.000 m
 T = 556.363 m
 L = 966.556 m
 E = 178.092 m
 PC STA = 37+279.595
 PT STA = 38+246.151
 SE = 5.5%
 SE ATTAINED STA 37+220.000 TO STA 37+300.000
 SE REMOVED STA 38+226.000 TO STA 38+306.000

STA 37+956 (SKEW = 20° RT AH)
 PROPOSED 2.44 m X 2.14 m X 78.1 m PRECAST RCBC
 USFL 170.704, STA 37+969.2, 38.3 m RT
 DSFL 169.934, STA 37+942.6, 35.1 m LT
 MEDIAN INLET STD 604101
 GRATE = 176.325
 RISER PIPE CULVERT, RCCP, TYPE 2, 600 mm ϕ X 3.5 m
 CAST-IN-PLACE HEADWALLS
 SEE DETAIL ON SHEET 260
 DRAINAGE AREA = 89.31 ha
 50 YR FLOW = 25.06 cms
 100 YR FLOW = 31.23 cms

BM 3018-RAILROAD SPIKE IN 8" DIA TREE
 MAINLINE STA 38+295.5, 0.8 m RT,
 ELEV 183.947

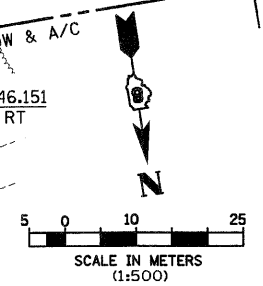
BM 3019-RAILROAD SPIKE IN 12" DIA HICKORY
 MAINLINE STA 37+997.0, 1.0 m LT,
 ELEV 173.942

BEVERLY FARM ASSOCIATION
 [PARCEL NO. 8239027]

NW 1/4, SEC 24, T 6 N, R 10 W, 3RD PM

| REVISIONS | |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAN
FAP 310 (IL 255)
SECTION 60-15
MADISON COUNTY
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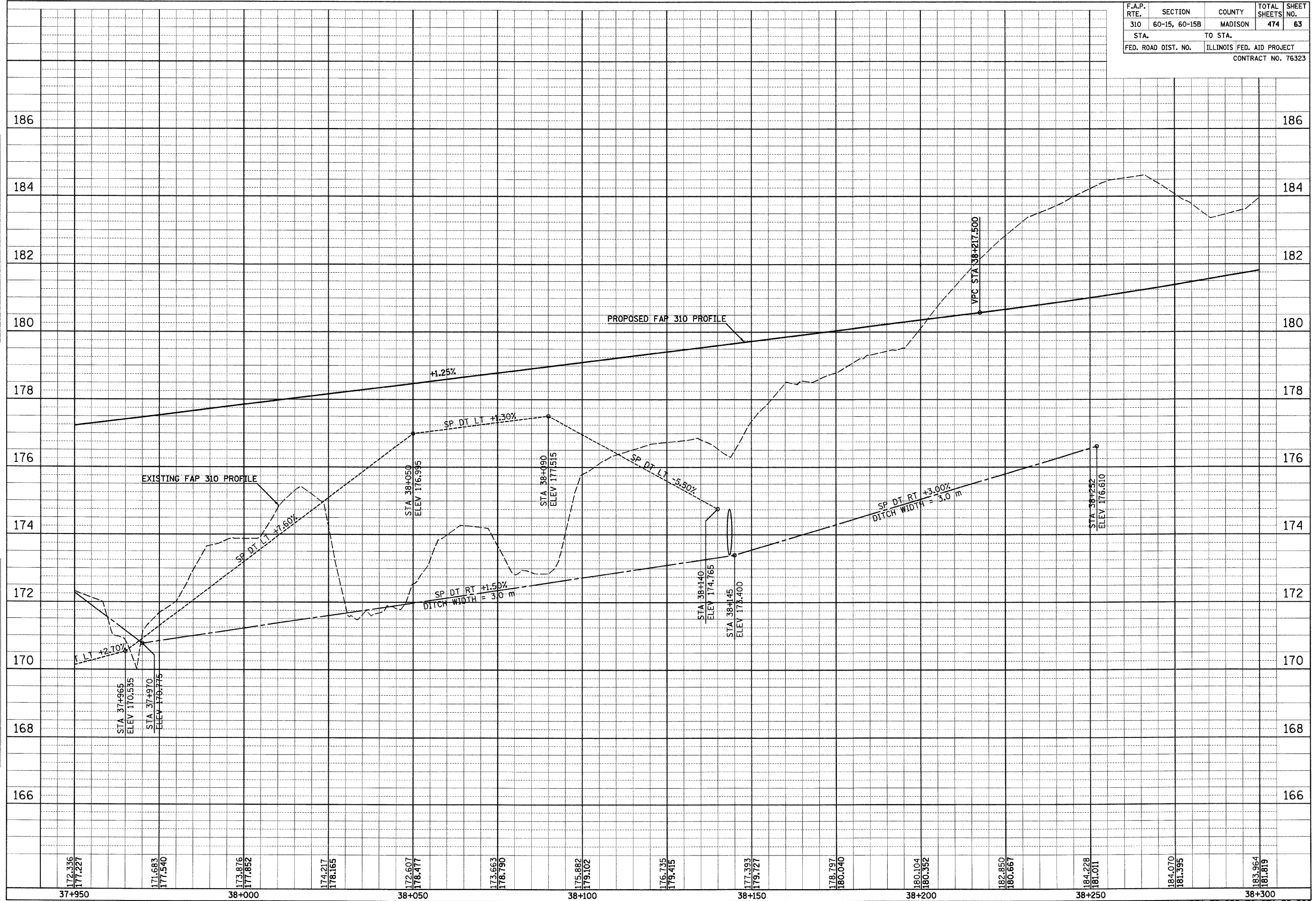
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 63 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |



IL 255 (FAP 310), STA 37+950 TO STA 38+300

12/10/2007

NW 1/4, SEC 24, R 6 N, R 10 W, 3RD PM

NE 1/4, SEC 23, R 6 N, R 10 W, 3RD PM

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------------|---------------------------|--------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 64 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| | | CONTRACT NO. 76323 | | |

CURVE RAMP A-2
 PI STA = 0+298.731
 $\Delta = 24^\circ 03' 05''$ (RT)
 R = 260.000 m
 T = 55.387 m
 L = 109.142 m
 E = 5.834 m
 PC STA = 0+243.344
 PT STA = 0+352.486
 SE = 6.0%
 SE ATTAINED STA 0+203.000 TO STA 0+263.000
 SE REMOVED STA 0+312.486 TO STA 0+464.273

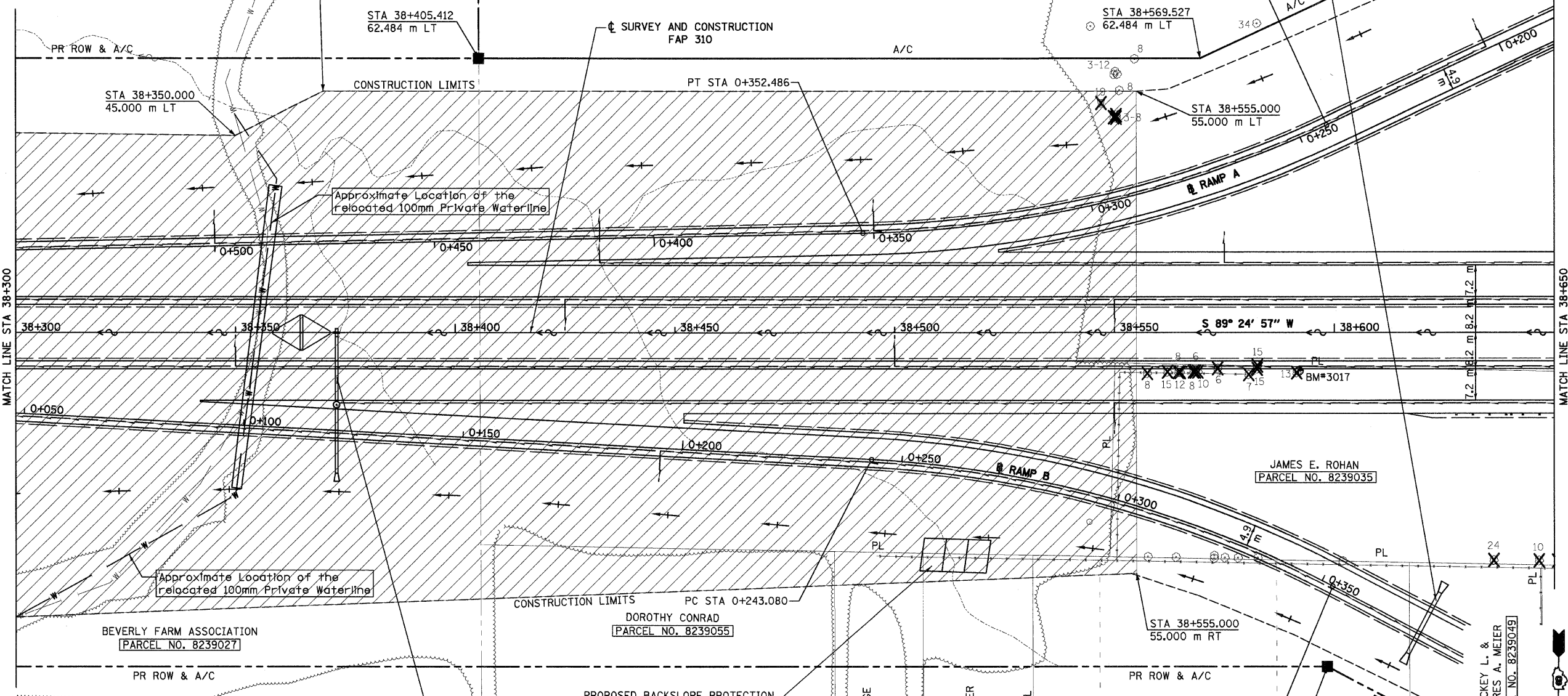
STA 0+375 (RAMP B)
 PROPOSED PIPE CULVERT, RCCP, TYPE 1, 900 mm ϕ X 15.6 m
 USFL 185.650, 10.0 m LT
 DSFL 185.570, 5.6 m RT
 PRECAST REINFORCED CONCRETE FLARED END SECTION STD 542301, 2-EACH
 GRATING LT FOR CONCRETE FLARED END SECTION STD 542311, 1-EACH
 DRAINAGE AREA = 2.24 ha
 50 YR FLOW = 0.82 cms
 100 YR FLOW = 0.99 cms

STONE DUMPED
 RIPRAP, CLASS B3
 103.8 SQ M

Approximate Location of the existing 100mm Private Waterline

BEVERLY FARM ASSOCIATION
 [PARCEL NO. 8239027]

FOR ENTRANCE RAMP A TERMINAL DETAILS AND PROFILE SEE SHEETS 80 & 151-152



STA 38+373
 PROPOSED PIPE CULVERT, RCCP, TYPE 2, 600 mm ϕ X 14.3 m
 USFL 180.605, 0.9 m RT
 DSFL 180.460, 15.2 m RT
 MEDIAN INLET STD 604101
 GRATE = 182.270
 PIPE TEE STD 542606, 600 mm PIPE, 600 mm RISER, ONE END PLUGGED
 PROPOSED MANHOLE, TYPE A, 1,500 m ϕ , STD 602401
 TYPE 1 FRAME AND OPEN LID STD 604001
 TOP LID = 183.270, 16.4 m RT
 PROPOSED PIPE CULVERT, RCCP, TYPE 2, 600 mm ϕ X 15.4 m
 USFL 180.445, 16.7 m RT
 DSFL 180.295, 32.1 m RT
 PRECAST REINFORCED CONCRETE FLARED END SECTION STD 542301, 1-EACH

CURVE RAMP B-1
 PI STA = 0+298.776
 $\Delta = 24^\circ 38' 29''$ (RT)
 R = 255.000 m
 T = 55.696 m
 L = 109.669 m
 E = 6.012 m
 PC STA = 0+243.080
 PT STA = 0+352.749
 SE = 6.0%
 SE ATTAINED STA 0+200.080 TO STA 0+264.080
 SE REMOVED STA 0+333.000 TO STA 0+393.000

BM 3017-RAILROAD SPIKE IN 12" DIA WALNUT
 MAINLINE STA 38+592.0, 8.7 m RT,
 ELEV 186.966

FOR EXIT RAMP B TERMINAL DETAILS AND PROFILE SEE SHEETS 81 & 153-154

| REVISIONS | |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAN
FAP 310 (IL 255)
SECTION 60-15
MADISON COUNTY
 DRAWN BY EBB
 CHECKED BY
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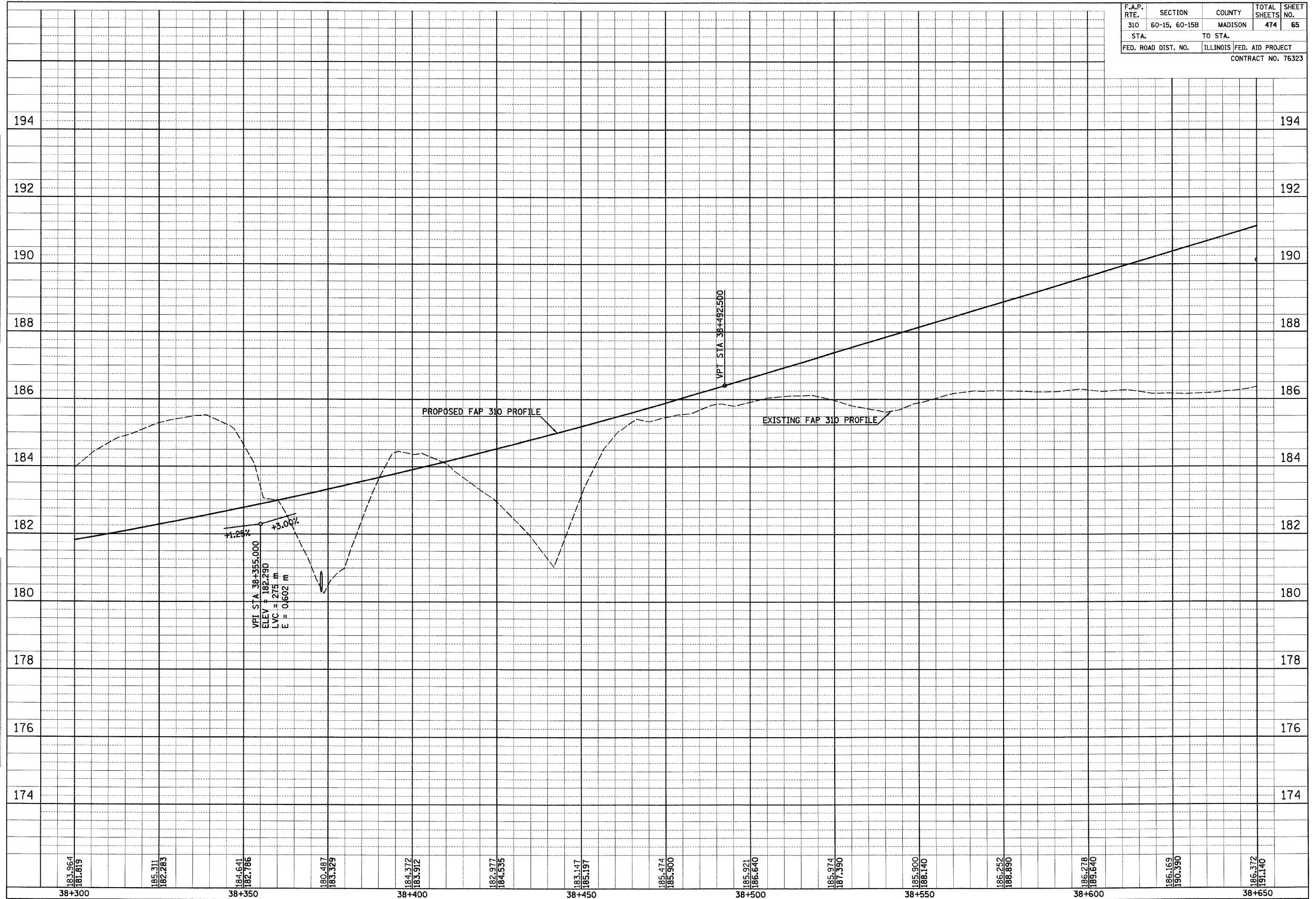
IL 255 (FAP 310), STA 38+300 TO STA 38+650

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| FINAL SURVEY | SURVEYED | BY | DATE |
|--------------|----------|----|------|
| NO. | NO. | | |
| AREAS | AREAS | | |
| CHECKED | CHECKED | | |

| ORIGINAL SURVEY | SURVEYED | BY | DATE |
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| NO. | NO. | | |
| AREAS | AREAS | | |
| CHECKED | CHECKED | | |

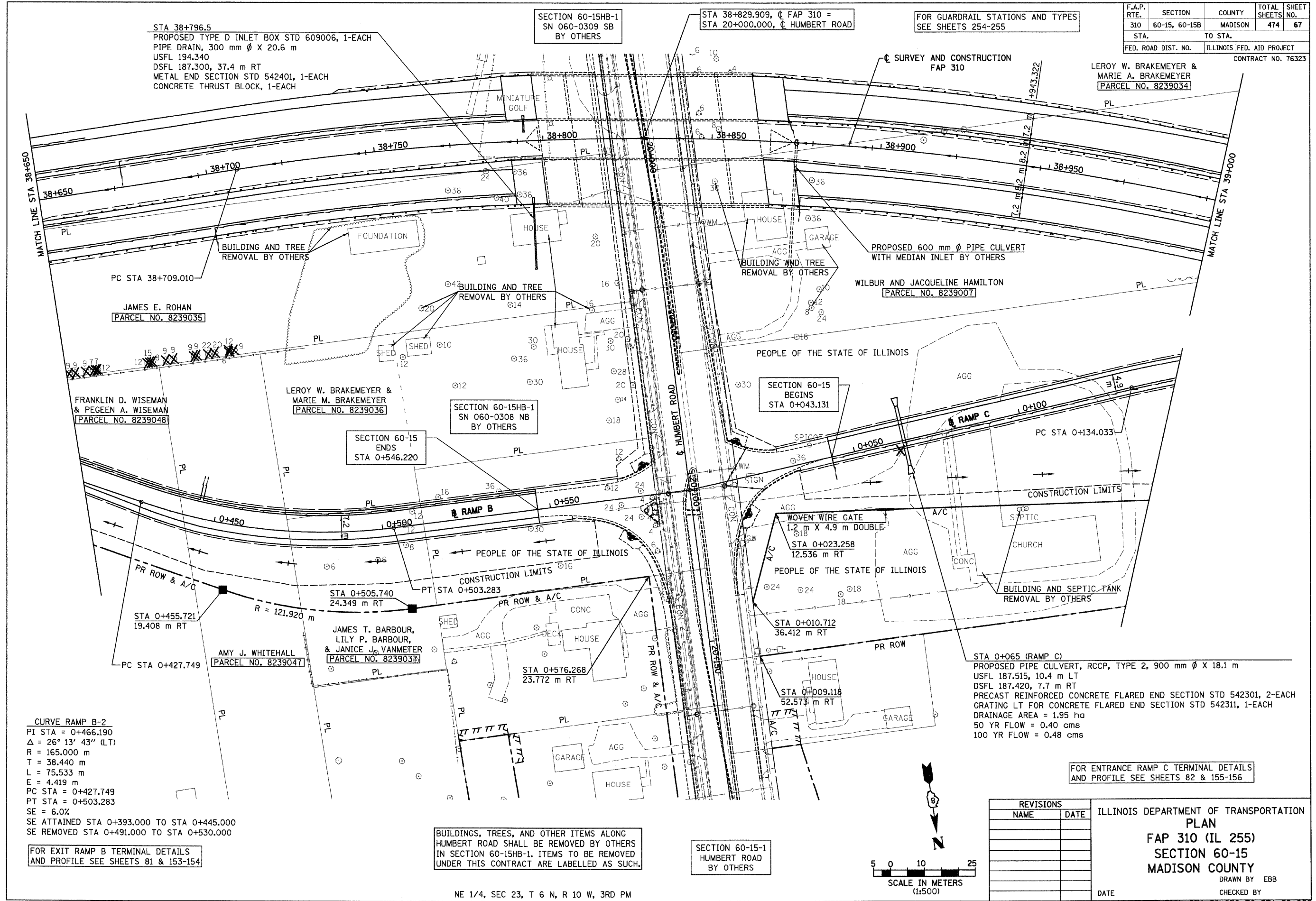
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 65 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |



6/28/2007

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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 67 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |



STA 38+796.5
 PROPOSED TYPE D INLET BOX STD 609006, 1-EACH
 PIPE DRAIN, 300 mm Ø X 20.6 m
 USFL 194.340
 DSFL 187.300, 37.4 m RT
 METAL END SECTION STD 542401, 1-EACH
 CONCRETE THRUST BLOCK, 1-EACH

SECTION 60-15HB-1
 SN 060-0309 SB
 BY OTHERS

STA 38+829.909, C FAP 310 =
 STA 20+000.000, C HUMBERT ROAD

FOR GUARDRAIL STATIONS AND TYPES
 SEE SHEETS 254-255

LEROY W. BRAKEMEYER &
 MARIE A. BRAKEMEYER
 [PARCEL NO. 8239034]

JAMES E. ROHAN
 [PARCEL NO. 8239035]

FRANKLIN D. WISEMAN
 & PEGEEN A. WISEMAN
 [PARCEL NO. 8239048]

LEROY W. BRAKEMEYER &
 MARIE M. BRAKEMEYER
 [PARCEL NO. 8239036]

SECTION 60-15HB-1
 SN 060-0308 NB
 BY OTHERS

SECTION 60-15
 ENDS
 STA 0+546.220

SECTION 60-15
 BEGINS
 STA 0+043.131

WILBUR AND JACQUELINE HAMILTON
 [PARCEL NO. 8239007]

WOVEN WIRE GATE
 1.2 m X 4.9 m DOUBLE
 STA 0+023.258
 12.536 m RT

STA 0+505.740
 24.349 m RT

AMY J. WHITEHALL
 [PARCEL NO. 8239047]

JAMES T. BARBOUR,
 LILY P. BARBOUR,
 & JANICE J. VANMETER
 [PARCEL NO. 8239037]

STA 0+576.268
 23.772 m RT

STA 0+010.712
 36.412 m RT

STA 0+009.118
 52.573 m RT

STA 0+065 (RAMP C)
 PROPOSED PIPE CULVERT, RCCP, TYPE 2, 900 mm Ø X 18.1 m
 USFL 187.515, 10.4 m LT
 DSFL 187.420, 7.7 m RT
 PRECAST REINFORCED CONCRETE FLARED END SECTION STD 542301, 2-EACH
 GRATING LT FOR CONCRETE FLARED END SECTION STD 542311, 1-EACH
 DRAINAGE AREA = 1.95 ha
 50 YR FLOW = 0.40 cms
 100 YR FLOW = 0.48 cms

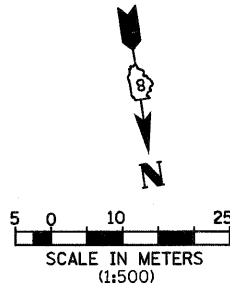
FOR ENTRANCE RAMP C TERMINAL DETAILS
 AND PROFILE SEE SHEETS 82 & 155-156

CURVE RAMP B-2
 PI STA = 0+466.190
 Δ = 26° 13' 43" (LT)
 R = 165.000 m
 T = 38.440 m
 L = 75.533 m
 E = 4.419 m
 PC STA = 0+427.749
 PT STA = 0+503.283
 SE = 6.0%
 SE ATTAINED STA 0+393.000 TO STA 0+445.000
 SE REMOVED STA 0+491.000 TO STA 0+530.000

FOR EXIT RAMP B TERMINAL DETAILS
 AND PROFILE SEE SHEETS 81 & 153-154

BUILDINGS, TREES, AND OTHER ITEMS ALONG
 HUMBERT ROAD SHALL BE REMOVED BY OTHERS
 IN SECTION 60-15HB-1. ITEMS TO BE REMOVED
 UNDER THIS CONTRACT ARE LABELLED AS SUCH.

SECTION 60-15-1
 HUMBERT ROAD
 BY OTHERS



| REVISIONS | |
|-----------|------|
| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAN
FAP 310 (IL 255)
SECTION 60-15
MADISON COUNTY
 DRAWN BY EBB
 CHECKED BY
 DATE

NE 1/4, SEC 23, T 6 N, R 10 W, 3RD PM

IL 255 (FAP 310), STA 38+650 TO STA 39+000

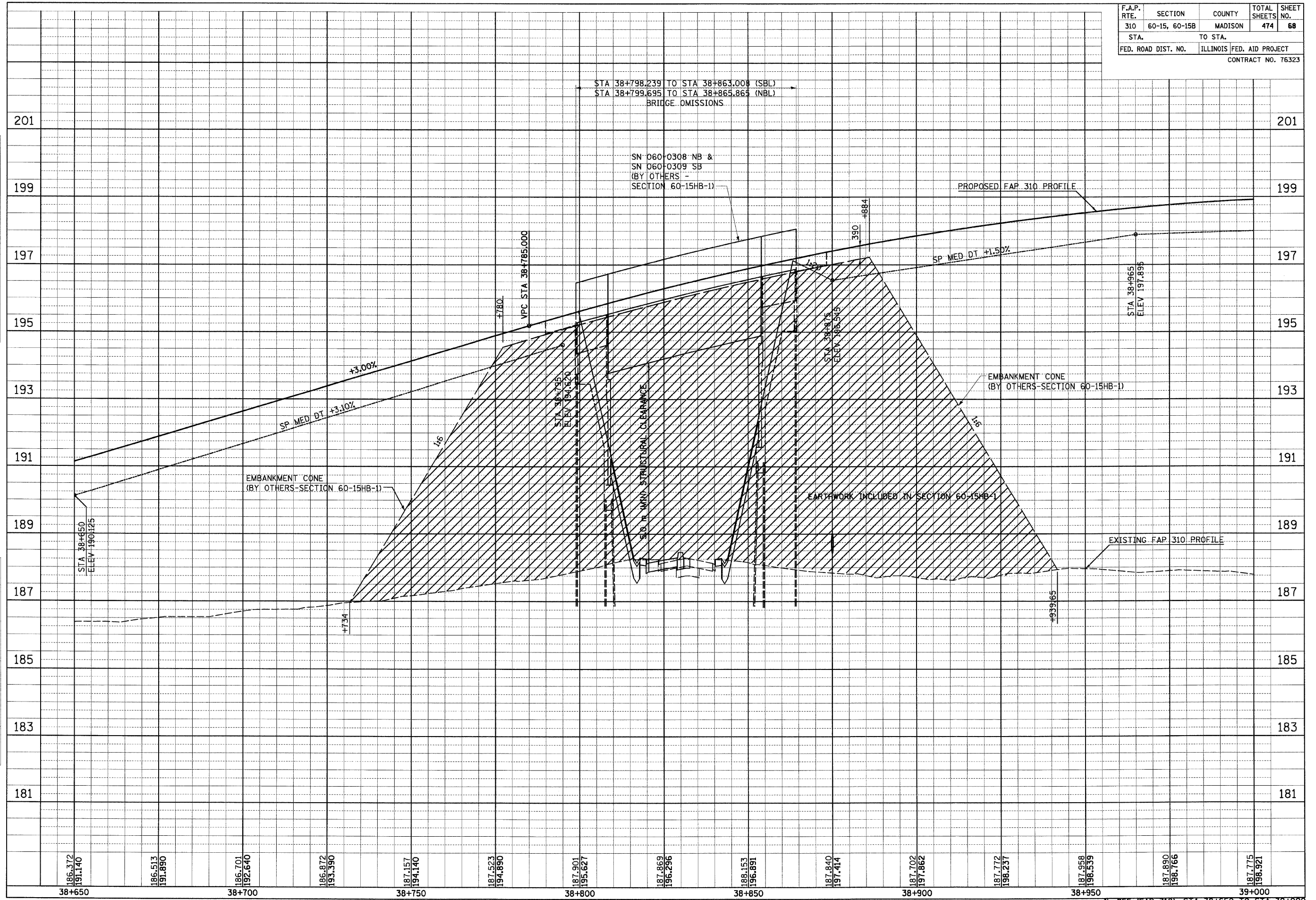
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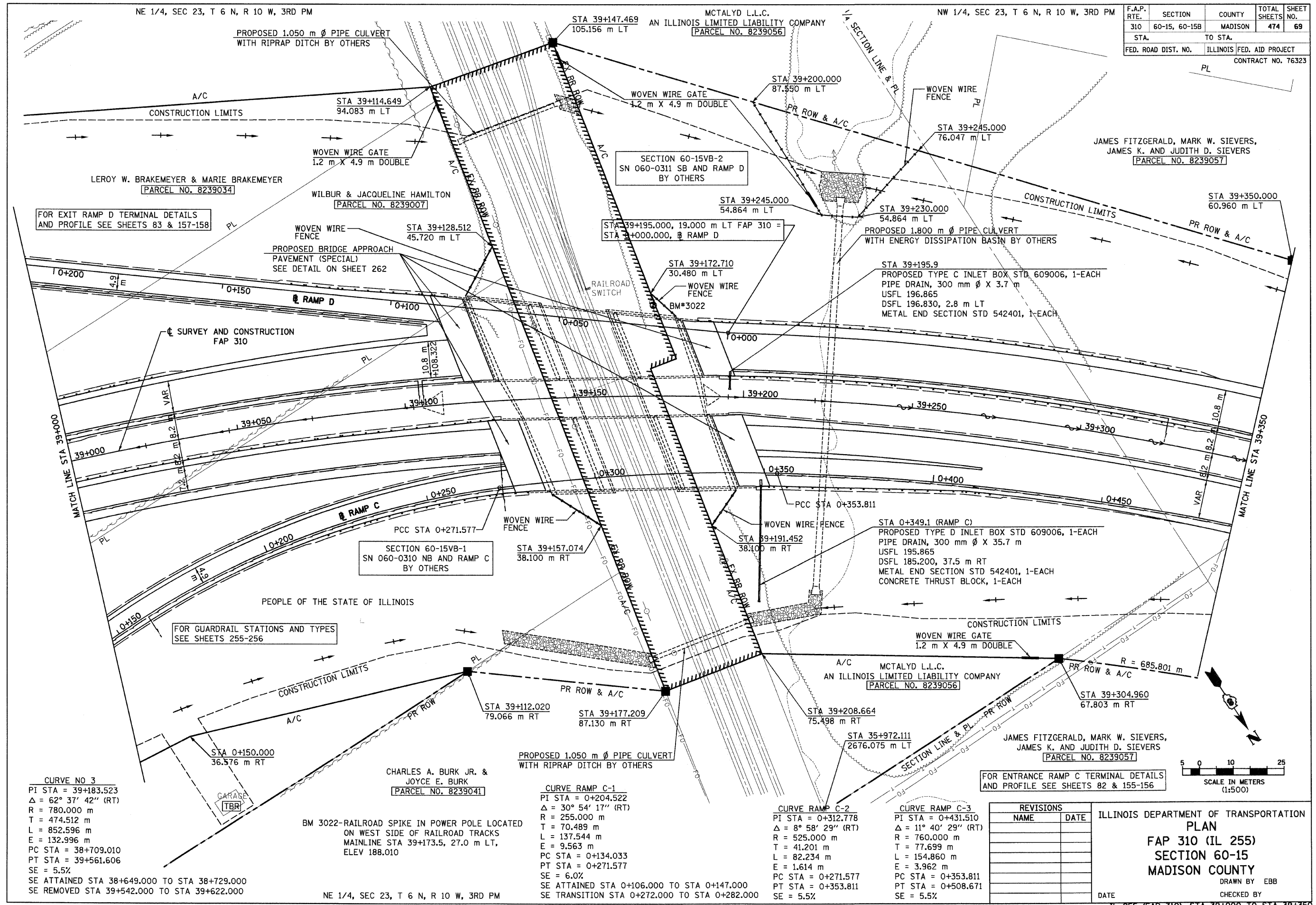
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| FINAL SURVEY | SURVEYED | BY | DATE |
| NOTE BOOK | PLOTTED | | |
| NO. | TEMPLATE | | |
| | AREAS | | |
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| ORIGINAL SURVEY | SURVEYED | BY | DATE |
| NOTE BOOK | PLOTTED | | |
| NO. | TEMPLATE | | |
| | AREAS | | |
| | CHECKED | | |

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|---------------------|---------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 68 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |



6/28/2007



| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------------|---------------------------|--------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 69 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |

FOR EXIT RAMP D TERMINAL DETAILS AND PROFILE SEE SHEETS 83 & 157-158

FOR GUARDRAIL STATIONS AND TYPES SEE SHEETS 255-256

FOR ENTRANCE RAMP C TERMINAL DETAILS AND PROFILE SEE SHEETS 82 & 155-156

CURVE NO 3
 PI STA = 39+183.523
 $\Delta = 62^\circ 37' 42''$ (RT)
 R = 780.000 m
 T = 474.512 m
 L = 852.596 m
 E = 132.996 m
 PC STA = 38+709.010
 PT STA = 39+561.606
 SE = 5.5%
 SE ATTAINED STA 38+649.000 TO STA 38+729.000
 SE REMOVED STA 39+542.000 TO STA 39+622.000

BM 3022-RAILROAD SPIKE IN POWER POLE LOCATED ON WEST SIDE OF RAILROAD TRACKS MAINLINE STA 39+173.5, 27.0 m LT, ELEV 188.010

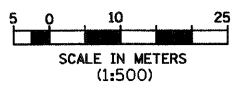
CURVE RAMP C-1
 PI STA = 0+204.522
 $\Delta = 30^\circ 54' 17''$ (RT)
 R = 255.000 m
 T = 70.489 m
 L = 137.544 m
 E = 9.563 m
 PC STA = 0+134.033
 PT STA = 0+271.577
 SE = 6.0%
 SE ATTAINED STA 0+106.000 TO STA 0+147.000
 SE TRANSITION STA 0+272.000 TO STA 0+282.000

CURVE RAMP C-2
 PI STA = 0+312.778
 $\Delta = 8^\circ 58' 29''$ (RT)
 R = 525.000 m
 T = 41.201 m
 L = 82.234 m
 E = 1.614 m
 PC STA = 0+271.577
 PT STA = 0+353.811
 SE = 5.5%

CURVE RAMP C-3
 PI STA = 0+431.510
 $\Delta = 11^\circ 40' 29''$ (RT)
 R = 760.000 m
 T = 77.699 m
 L = 154.860 m
 E = 3.962 m
 PC STA = 0+353.811
 PT STA = 0+508.671
 SE = 5.5%

| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAN
FAP 310 (IL 255)
SECTION 60-15
MADISON COUNTY
 DRAWN BY EBB
 CHECKED BY
 DATE
 IL 255 (FAP 310), STA 39+000 TO STA 39+350



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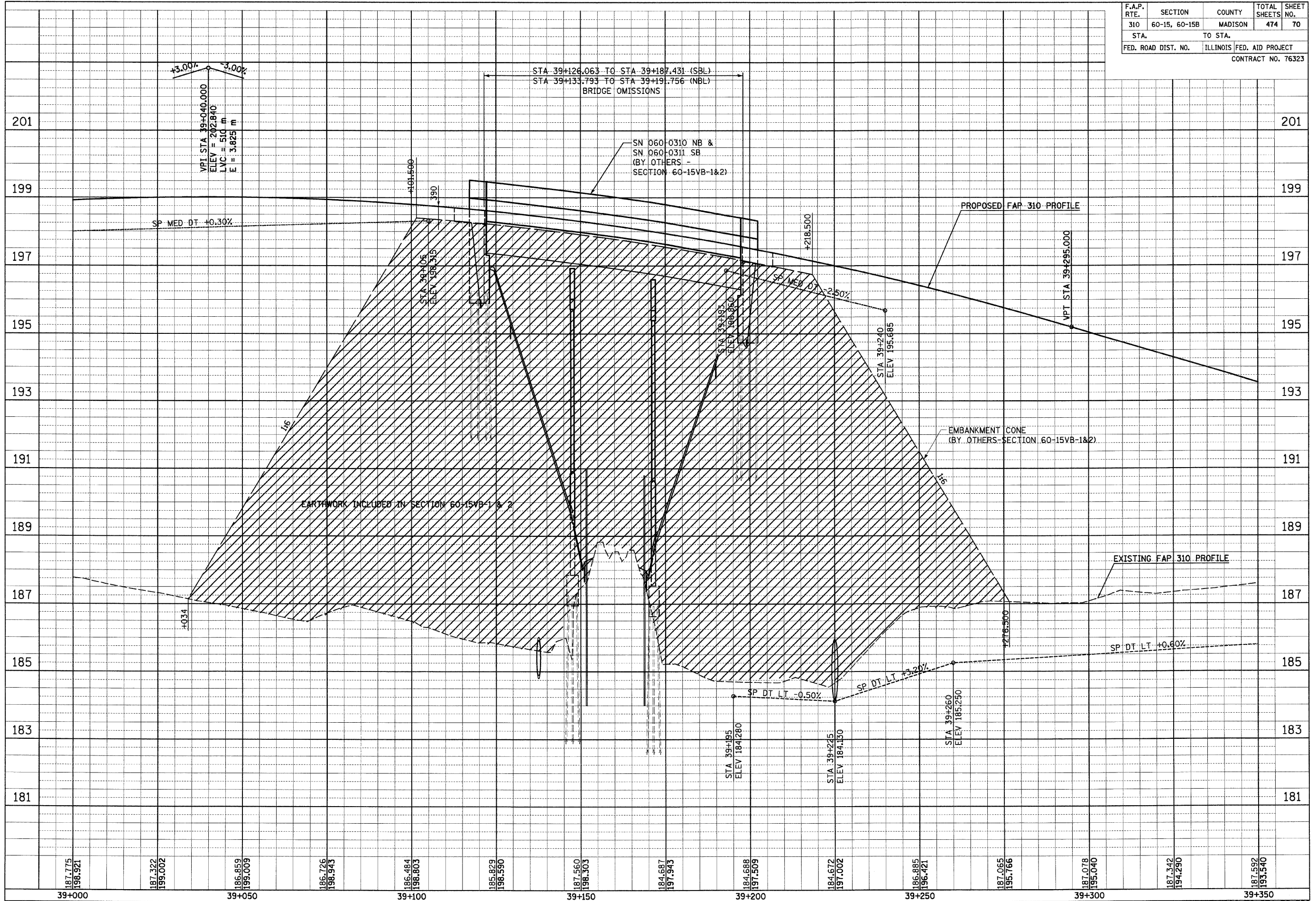
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 70 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | | |
| CONTRACT NO. 76323 | | | | |

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| FINAL SURVEYED SURVEY PLOTTED | BY | DATE |
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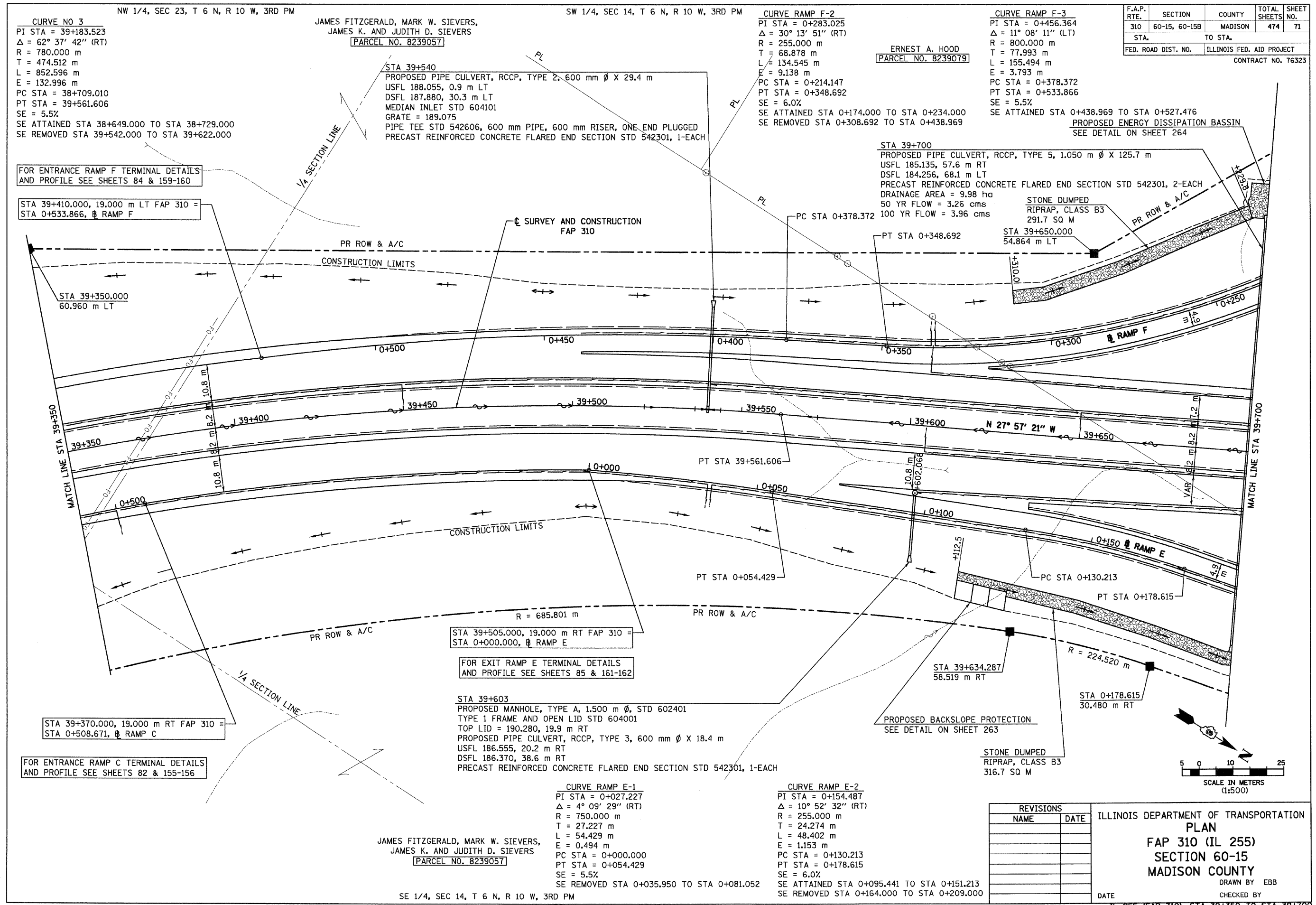
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| ORIGINAL SURVEYED SURVEY PLOTTED | BY | DATE |
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| AREAS CHECKED | | |
| NO. | | |



IL 255 (FAP 310), STA 39+000 TO STA 39+350

6/28/2007

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CURVE NO 3
 NW 1/4, SEC 23, T 6 N, R 10 W, 3RD PM
 PI STA = 39+183.523
 $\Delta = 62^\circ 37' 42''$ (RT)
 R = 780.000 m
 T = 474.512 m
 L = 852.596 m
 E = 132.996 m
 PC STA = 38+709.010
 PT STA = 39+561.606
 SE = 5.5%
 SE ATTAINED STA 38+649.000 TO STA 38+729.000
 SE REMOVED STA 39+542.000 TO STA 39+622.000

JAMES FITZGERALD, MARK W. SIEVERS,
 JAMES K. AND JUDITH D. SIEVERS
 [PARCEL NO. 8239057]
 SW 1/4, SEC 14, T 6 N, R 10 W, 3RD PM
 STA 39+540
 PROPOSED PIPE CULVERT, RCCP, TYPE 2, 600 mm ϕ X 29.4 m
 USFL 188.055, 0.9 m LT
 DSFL 187.880, 30.3 m LT
 MEDIAN INLET STD 604101
 GRATE = 189.075
 PIPE TEE STD 542606, 600 mm PIPE, 600 mm RISER, ONE END PLUGGED
 PRECAST REINFORCED CONCRETE FLARED END SECTION STD 542301, 1-EACH

CURVE RAMP F-2
 PI STA = 0+283.025
 $\Delta = 30^\circ 13' 51''$ (RT)
 R = 255.000 m
 T = 68.878 m
 L = 134.545 m
 E = 9.138 m
 PC STA = 0+214.147
 PT STA = 0+348.692
 SE = 6.0%
 SE ATTAINED STA 0+174.000 TO STA 0+234.000
 SE REMOVED STA 0+308.692 TO STA 0+438.969

CURVE RAMP F-3
 PI STA = 0+456.364
 $\Delta = 11^\circ 08' 11''$ (LT)
 R = 800.000 m
 T = 77.993 m
 L = 155.494 m
 E = 3.793 m
 PC STA = 0+378.372
 PT STA = 0+533.866
 SE = 5.5%
 SE ATTAINED STA 0+438.969 TO STA 0+527.476
 PROPOSED ENERGY DISSIPATION BASSIN
 SEE DETAIL ON SHEET 264

| | | | | |
|---------------------|---------------------------|--------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 71 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | CONTRACT NO. 76323 | | |

FOR ENTRANCE RAMP F TERMINAL DETAILS AND PROFILE SEE SHEETS 84 & 159-160

STA 39+410.000, 19.000 m LT FAP 310 = STA 0+533.866, RAMP F

STA 39+350.000, 60.960 m LT

STA 39+370.000, 19.000 m RT FAP 310 = STA 0+508.671, RAMP C

FOR ENTRANCE RAMP C TERMINAL DETAILS AND PROFILE SEE SHEETS 82 & 155-156

STA 39+505.000, 19.000 m RT FAP 310 = STA 0+000.000, RAMP E

FOR EXIT RAMP E TERMINAL DETAILS AND PROFILE SEE SHEETS 85 & 161-162

STA 39+603
 PROPOSED MANHOLE, TYPE A, 1.500 m ϕ , STD 602401
 TYPE 1 FRAME AND OPEN LID STD 604001
 TOP LID = 190.280, 19.9 m RT
 PROPOSED PIPE CULVERT, RCCP, TYPE 3, 600 mm ϕ X 18.4 m
 USFL 186.555, 20.2 m RT
 DSFL 186.370, 38.6 m RT
 PRECAST REINFORCED CONCRETE FLARED END SECTION STD 542301, 1-EACH

CURVE RAMP E-1
 PI STA = 0+027.227
 $\Delta = 4^\circ 09' 29''$ (RT)
 R = 750.000 m
 T = 27.227 m
 L = 54.429 m
 E = 0.494 m
 PC STA = 0+000.000
 PT STA = 0+054.429
 SE = 5.5%
 SE ATTAINED STA 0+035.950 TO STA 0+081.052

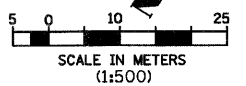
CURVE RAMP E-2
 PI STA = 0+154.487
 $\Delta = 10^\circ 52' 32''$ (RT)
 R = 255.000 m
 T = 24.274 m
 L = 48.402 m
 E = 1.153 m
 PC STA = 0+130.213
 PT STA = 0+178.615
 SE = 6.0%
 SE ATTAINED STA 0+095.441 TO STA 0+151.213
 SE REMOVED STA 0+164.000 TO STA 0+209.000

JAMES FITZGERALD, MARK W. SIEVERS,
 JAMES K. AND JUDITH D. SIEVERS
 [PARCEL NO. 8239057]

SE 1/4, SEC 14, T 6 N, R 10 W, 3RD PM

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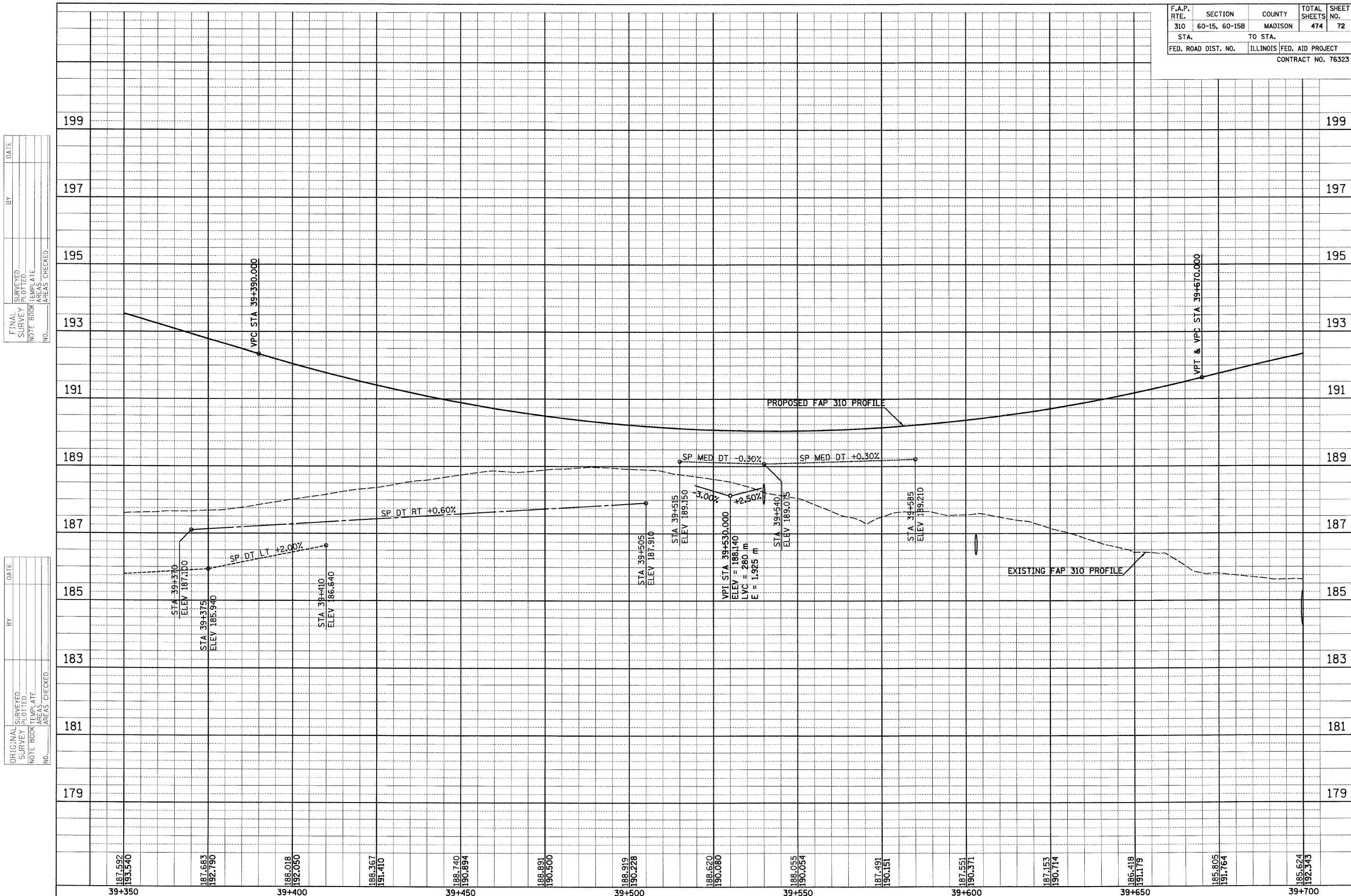
ILLINOIS DEPARTMENT OF TRANSPORTATION
FAP 310 (IL 255)
SECTION 60-15
MADISON COUNTY
 DRAWN BY EBB
 CHECKED BY
 DATE



IL 255 (FAP 310), STA 39+350 TO STA 39+700

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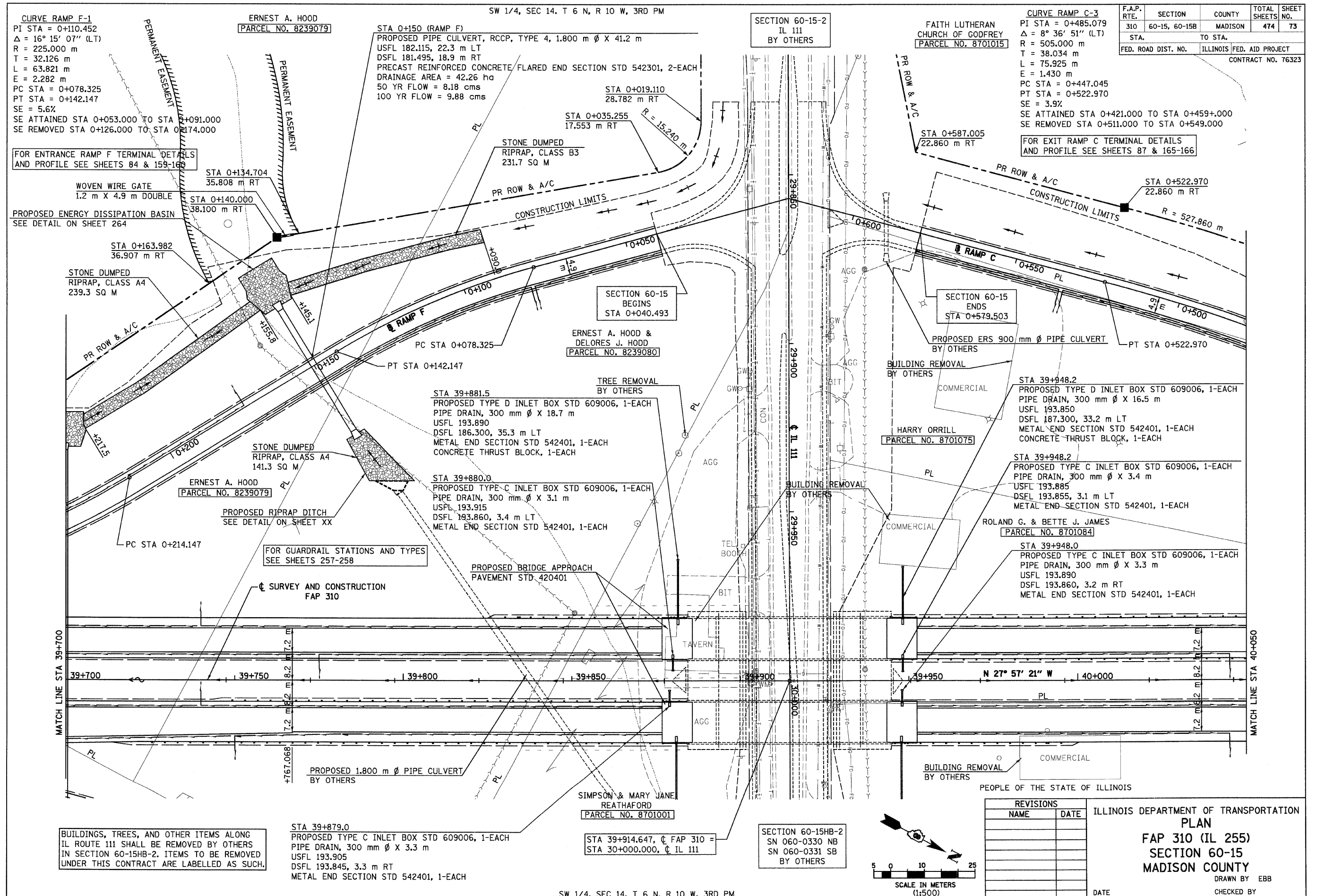
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| F.A.P. RTE. 310 | SECTION 60-15, 60-15B | COUNTY MADISON | TOTAL SHEETS 474 | SHEET NO. 72 |
| STA. TO STA. | | ILLINOIS FED. AID PROJECT | | |
| FED. ROAD DIST. NO. | | CONTRACT NO. 76323 | | |

6/28/2007

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CURVE RAMP F-1
 PI STA = 0+110.452
 $\Delta = 16^\circ 15' 07''$ (LT)
 R = 225.000 m
 T = 32.126 m
 L = 63.821 m
 E = 2.282 m
 PC STA = 0+078.325
 PT STA = 0+142.147
 SE = 5.6%
 SE ATTAINED STA 0+053.000 TO STA 0+091.000
 SE REMOVED STA 0+126.000 TO STA 0+174.000

FOR ENTRANCE RAMP F TERMINAL DETAILS AND PROFILE SEE SHEETS 84 & 159-160

WOVEN WIRE GATE
 1.2 m X 4.9 m DOUBLE
 PROPOSED ENERGY DISSIPATION BASIN
 SEE DETAIL ON SHEET 264

STONE DUMPED RIPRAP, CLASS A4
 239.3 SQ M

STONE DUMPED RIPRAP, CLASS B3
 231.7 SQ M

STONE DUMPED RIPRAP, CLASS A4
 141.3 SQ M

PROPOSED RIPRAP DITCH
 SEE DETAIL ON SHEET XX

FOR GUARDRAIL STATIONS AND TYPES
 SEE SHEETS 257-258

PROPOSED BRIDGE APPROACH
 PAVEMENT STD 420401

PROPOSED 1.800 m ϕ PIPE CULVERT
 BY OTHERS

PROPOSED TYPE C INLET BOX STD 609006, 1-EACH
 PIPE DRAIN, 300 mm ϕ X 3.3 m
 USFL 193.905
 DSFL 193.845, 3.3 m RT
 METAL END SECTION STD 542401, 1-EACH

BUILDINGS, TREES, AND OTHER ITEMS ALONG IL ROUTE 111 SHALL BE REMOVED BY OTHERS IN SECTION 60-15HB-2. ITEMS TO BE REMOVED UNDER THIS CONTRACT ARE LABELLED AS SUCH.

ERNEST A. HOOD
 (PARCEL NO. 8239079)

STA 0+150 (RAMP F)
 PROPOSED PIPE CULVERT, RCCP, TYPE 4, 1.800 m ϕ X 41.2 m
 USFL 182.115, 22.3 m LT
 DSFL 181.495, 18.9 m RT
 PRECAST REINFORCED CONCRETE FLARED END SECTION STD 542301, 2-EACH
 DRAINAGE AREA = 42.26 ha
 50 YR FLOW = 8.18 cms
 100 YR FLOW = 9.88 cms

STONE DUMPED RIPRAP, CLASS B3
 231.7 SQ M

ERNEST A. HOOD & DELORES J. HOOD
 (PARCEL NO. 8239080)

PROPOSED TYPE D INLET BOX STD 609006, 1-EACH
 PIPE DRAIN, 300 mm ϕ X 18.7 m
 USFL 193.890
 DSFL 186.300, 35.3 m LT
 METAL END SECTION STD 542401, 1-EACH
 CONCRETE THRUST BLOCK, 1-EACH

PROPOSED TYPE C INLET BOX STD 609006, 1-EACH
 PIPE DRAIN, 300 mm ϕ X 3.1 m
 USFL 193.915
 DSFL 193.860, 3.4 m LT
 METAL END SECTION STD 542401, 1-EACH

PROPOSED TYPE C INLET BOX STD 609006, 1-EACH
 PIPE DRAIN, 300 mm ϕ X 3.4 m
 USFL 193.885
 DSFL 193.855, 3.1 m LT
 METAL END SECTION STD 542401, 1-EACH

PROPOSED TYPE C INLET BOX STD 609006, 1-EACH
 PIPE DRAIN, 300 mm ϕ X 3.3 m
 USFL 193.890
 DSFL 193.860, 3.2 m RT
 METAL END SECTION STD 542401, 1-EACH

PROPOSED TYPE C INLET BOX STD 609006, 1-EACH
 PIPE DRAIN, 300 mm ϕ X 3.3 m
 USFL 193.905
 DSFL 193.845, 3.3 m RT
 METAL END SECTION STD 542401, 1-EACH

PROPOSED TYPE C INLET BOX STD 609006, 1-EACH
 PIPE DRAIN, 300 mm ϕ X 3.3 m
 USFL 193.905
 DSFL 193.845, 3.3 m RT
 METAL END SECTION STD 542401, 1-EACH

PROPOSED TYPE C INLET BOX STD 609006, 1-EACH
 PIPE DRAIN, 300 mm ϕ X 3.3 m
 USFL 193.905
 DSFL 193.845, 3.3 m RT
 METAL END SECTION STD 542401, 1-EACH

PROPOSED TYPE C INLET BOX STD 609006, 1-EACH
 PIPE DRAIN, 300 mm ϕ X 3.3 m
 USFL 193.905
 DSFL 193.845, 3.3 m RT
 METAL END SECTION STD 542401, 1-EACH

PROPOSED TYPE C INLET BOX STD 609006, 1-EACH
 PIPE DRAIN, 300 mm ϕ X 3.3 m
 USFL 193.905
 DSFL 193.845, 3.3 m RT
 METAL END SECTION STD 542401, 1-EACH

SECTION 60-15-2
 IL 111
 BY OTHERS

FAITH LUTHERAN CHURCH OF GODFREY
 (PARCEL NO. 8701015)

CURVE RAMP C-3
 PI STA = 0+485.079
 $\Delta = 8^\circ 36' 51''$ (LT)
 R = 505.000 m
 T = 38.034 m
 L = 75.925 m
 E = 1.430 m
 PC STA = 0+447.045
 PT STA = 0+522.970
 SE = 3.9%
 SE ATTAINED STA 0+421.000 TO STA 0+459.000
 SE REMOVED STA 0+511.000 TO STA 0+549.000

FOR EXIT RAMP C TERMINAL DETAILS AND PROFILE SEE SHEETS 87 & 165-166

STONE DUMPED RIPRAP, CLASS A4
 239.3 SQ M

STONE DUMPED RIPRAP, CLASS A4
 141.3 SQ M

STONE DUMPED RIPRAP, CLASS A4
 141.3 SQ M

STONE DUMPED RIPRAP, CLASS A4
 141.3 SQ M

STONE DUMPED RIPRAP, CLASS A4
 141.3 SQ M

STONE DUMPED RIPRAP, CLASS A4
 141.3 SQ M

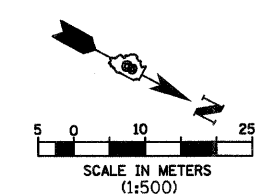
STONE DUMPED RIPRAP, CLASS A4
 141.3 SQ M

STONE DUMPED RIPRAP, CLASS A4
 141.3 SQ M

STONE DUMPED RIPRAP, CLASS A4
 141.3 SQ M

STONE DUMPED RIPRAP, CLASS A4
 141.3 SQ M

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------------|---------------------------|--------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 73 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |



| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAN
FAP 310 (IL 255)
SECTION 60-15
MADISON COUNTY
 DRAWN BY EBB
 CHECKED BY
 DATE

SW 1/4, SEC 14, T 6 N, R 10 W, 3RD PM

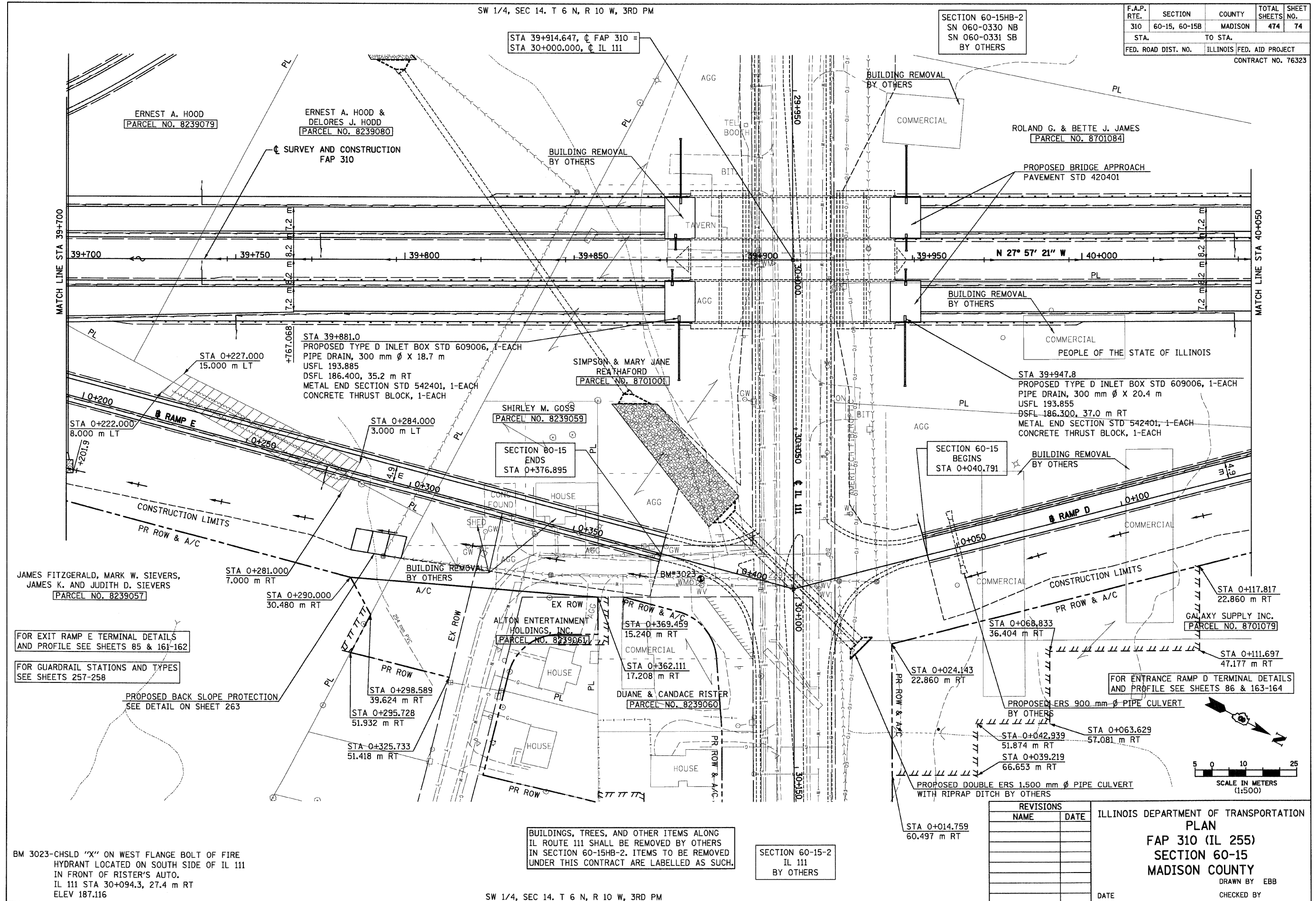
IL 255 (FAP 310), STA 39+700 TO STA 40+050

6/28/2007

SW 1/4, SEC 14. T 6 N, R 10 W, 3RD PM

SECTION 60-15B-2
SN 060-0330 NB
SN 060-0331 SB
BY OTHERS

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS NO. | SHEET NO. |
|---------------------|---------------|---------------------------|------------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 74 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |



STA 39+914.647, C FAP 310 =
STA 30+000.000, C IL 111

ERNEST A. HOOD
PARCEL NO. 8239079

ERNEST A. HOOD &
DELORES J. HOOD
PARCEL NO. 8239080

ROLAND G. & BETTE J. JAMES
PARCEL NO. 8701084

SIMPSON & MARY JANE
REATHAFORD
PARCEL NO. 8701001

SHIRLEY M. GOSS
PARCEL NO. 8239059

ALTON ENTERTAINMENT
HOLDINGS, INC.
PARCEL NO. 8239061

DUANE & CANDACE RISTER
PARCEL NO. 8239060

GALAXY SUPPLY INC.
PARCEL NO. 8701079

JAMES FITZGERALD, MARK W. SIEVERS,
JAMES K. AND JUDITH D. SIEVERS
PARCEL NO. 8239057

FOR EXIT RAMP E TERMINAL DETAILS
AND PROFILE SEE SHEETS 85 & 161-162

FOR GUARDRAIL STATIONS AND TYPES
SEE SHEETS 257-258

PROPOSED BACK SLOPE PROTECTION
SEE DETAIL ON SHEET 263

FOR ENTRANCE RAMP D TERMINAL DETAILS
AND PROFILE SEE SHEETS 86 & 163-164

BUILDINGS, TREES, AND OTHER ITEMS ALONG
IL ROUTE 111 SHALL BE REMOVED BY OTHERS
IN SECTION 60-15B-2. ITEMS TO BE REMOVED
UNDER THIS CONTRACT ARE LABELLED AS SUCH.

SECTION 60-15-2
IL 111
BY OTHERS

| REVISIONS | |
|-----------|------|
| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAN
FAP 310 (IL 255)
SECTION 60-15
MADISON COUNTY
DRAWN BY EBB

DATE _____ CHECKED BY _____

BM 3023-CHSLD "X" ON WEST FLANGE BOLT OF FIRE
HYDRANT LOCATED ON SOUTH SIDE OF IL 111
IN FRONT OF RISTER'S AUTO.
IL 111 STA 30+094.3, 27.4 m RT
ELEV 187.116

SW 1/4, SEC 14. T 6 N, R 10 W, 3RD PM

IL 255 (FAP 310), STA 39+700 TO STA 40+050

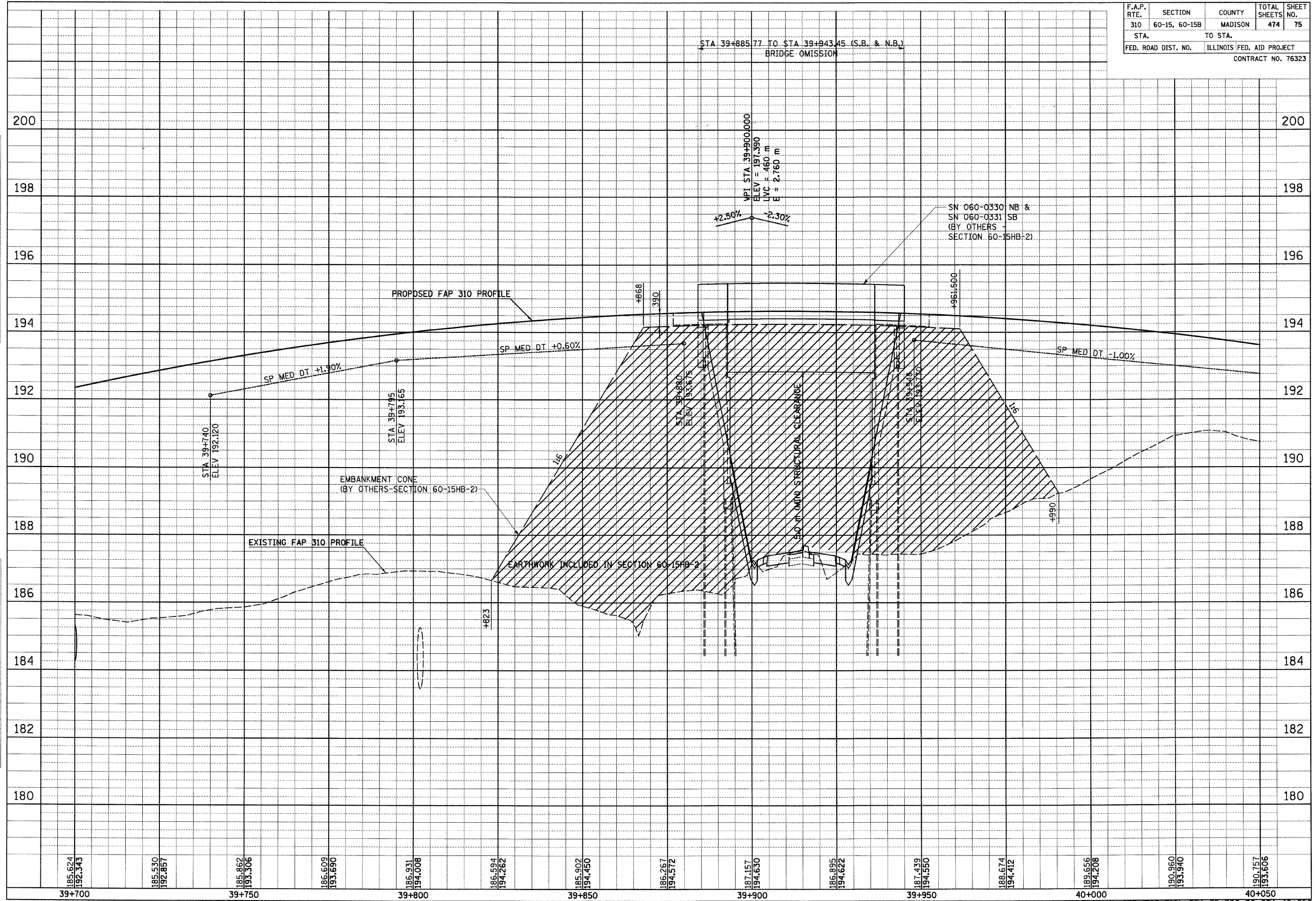
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| ORIGINAL SURVEY | SURVEYED | BY | DATE |
| SURVEY PLOTTED | PLOTTED | | |
| NOTE BOOK TEMPLATE | DATE | | |
| NO. | AREAS CHECKED | | |

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| FINAL SURVEY | SURVEYED | BY | DATE |
| NOTE BOOK | PLOTTED | | |
| NO. | AREAS CHECKED | | |

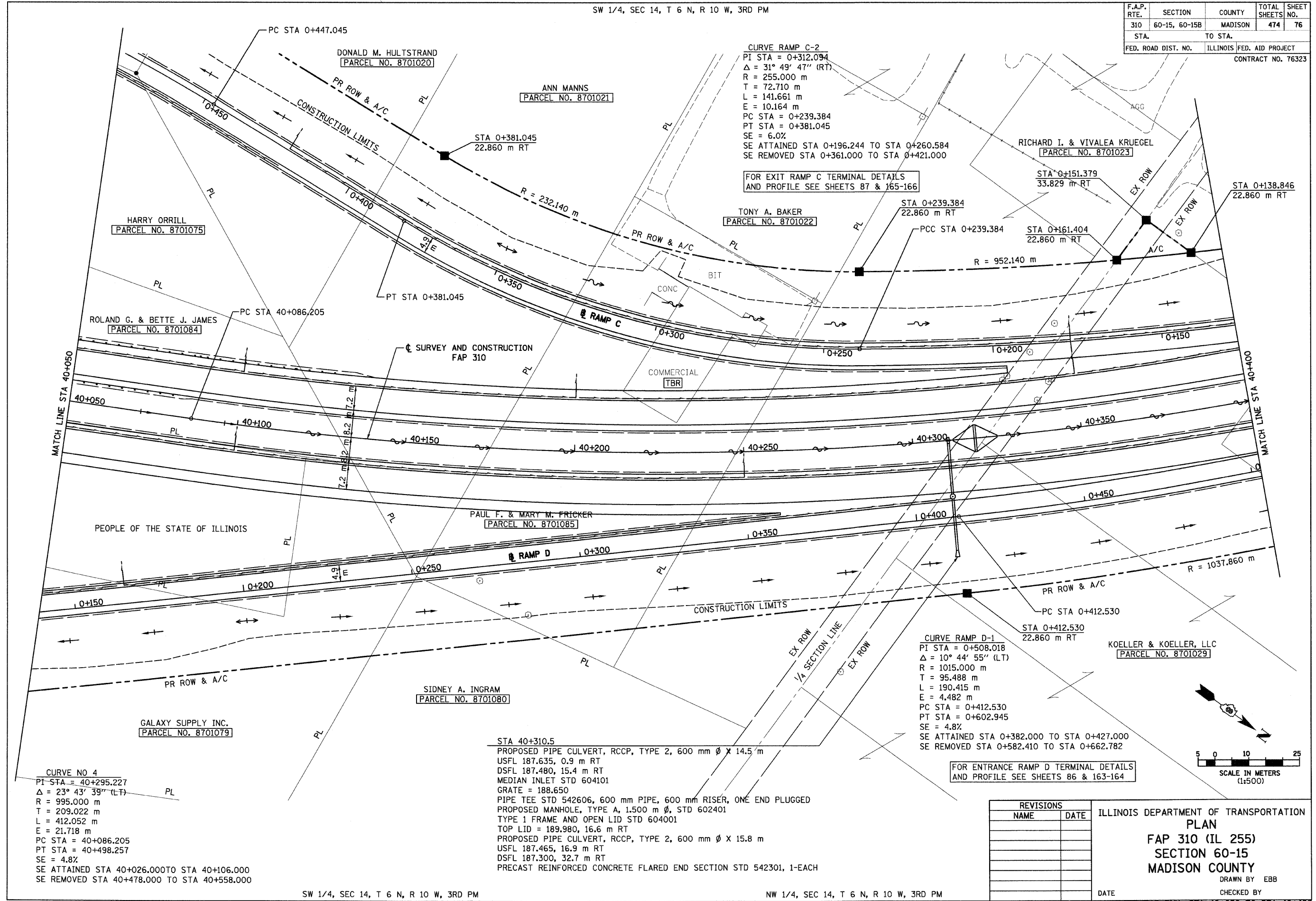


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|---------------------|---------------------------|---------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 75 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | | |
| | CONTRACT NO. 76323 | | | |

6/28/2007

SW 1/4, SEC 14, T 6 N, R 10 W, 3RD PM

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------------|---------------------------|--------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 76 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |



CURVE NO. 4
 PI STA = 40+295.227
 $\Delta = 23^\circ 43' 39''$ (LT)
 R = 995.000 m
 T = 209.022 m
 L = 412.052 m
 E = 21.718 m
 PC STA = 40+086.205
 PT STA = 40+498.257
 SE = 4.8%
 SE ATTAINED STA 40+026.000 TO STA 40+106.000
 SE REMOVED STA 40+478.000 TO STA 40+558.000

STA 40+310.5
 PROPOSED PIPE CULVERT, RCCP, TYPE 2, 600 mm ϕ X 14.5 m
 USFL 187.635, 0.9 m RT
 DSFL 187.480, 15.4 m RT
 MEDIAN INLET STD 604101
 GRATE = 188.650
 PIPE TEE STD 542606, 600 mm PIPE, 600 mm RISER, ONE END PLUGGED
 PROPOSED MANHOLE, TYPE A, 1,500 m ϕ , STD 602401
 TYPE 1 FRAME AND OPEN LID STD 604001
 TOP LID = 189.980, 16.6 m RT
 PROPOSED PIPE CULVERT, RCCP, TYPE 2, 600 mm ϕ X 15.8 m
 USFL 187.465, 16.9 m RT
 DSFL 187.300, 32.7 m RT
 PRECAST REINFORCED CONCRETE FLARED END SECTION STD 542301, 1-EACH

CURVE RAMP C-2
 PI STA = 0+312.094
 $\Delta = 31^\circ 49' 47''$ (RT)
 R = 255.000 m
 T = 72.710 m
 L = 141.661 m
 E = 10.164 m
 PC STA = 0+239.384
 PT STA = 0+381.045
 SE = 6.0%
 SE ATTAINED STA 0+196.244 TO STA 0+260.584
 SE REMOVED STA 0+361.000 TO STA 0+421.000

CURVE RAMP D-1
 PI STA = 0+508.018
 $\Delta = 10^\circ 44' 55''$ (LT)
 R = 1015.000 m
 T = 95.488 m
 L = 190.415 m
 E = 4.482 m
 PC STA = 0+412.530
 PT STA = 0+602.945
 SE = 4.8%
 SE ATTAINED STA 0+382.000 TO STA 0+427.000
 SE REMOVED STA 0+582.410 TO STA 0+662.782

FOR ENTRANCE RAMP D TERMINAL DETAILS AND PROFILE SEE SHEETS 86 & 163-164

FOR EXIT RAMP C TERMINAL DETAILS AND PROFILE SEE SHEETS 87 & 165-166

| REVISIONS | |
|-----------|------|
| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAN
FAP 310 (IL 255)
SECTION 60-15
MADISON COUNTY
 DRAWN BY EBB
 CHECKED BY
 DATE

SW 1/4, SEC 14, T 6 N, R 10 W, 3RD PM

NW 1/4, SEC 14, T 6 N, R 10 W, 3RD PM

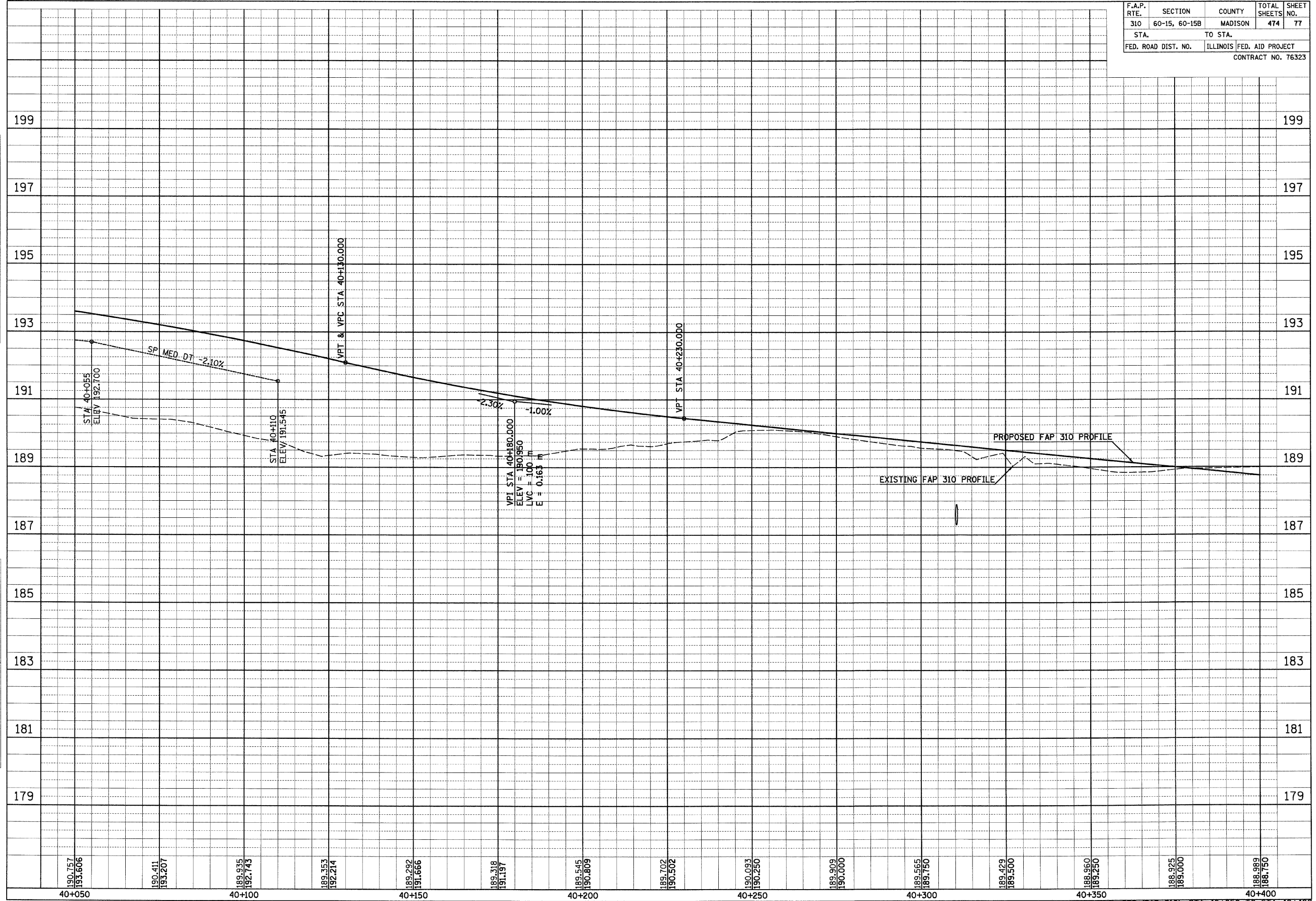
IL 255 (FAP 310), STA 40+050 TO STA 40+400

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| NOTE BOOK | PLOTTED | | |
| NO. | TEMPLATE | | |
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| FINAL SURVEY | SURVEYED | BY | DATE |
| NOTE BOOK | PLOTTED | | |
| NO. | TEMPLATE | | |
| | AREAS CHECKED | | |

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|---------------------|---------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 77 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |



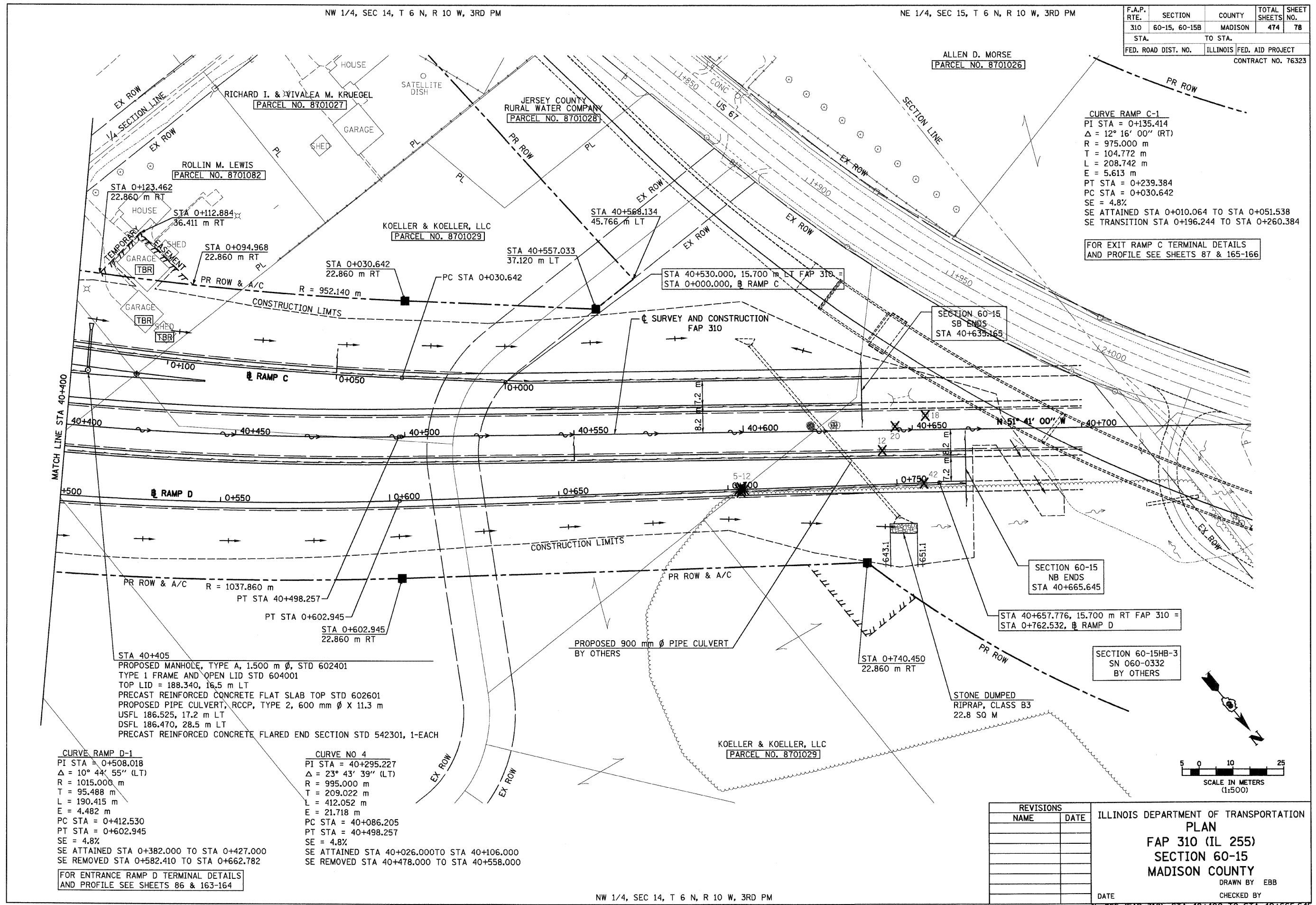
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NW 1/4, SEC 14, T 6 N, R 10 W, 3RD PM

NE 1/4, SEC 15, T 6 N, R 10 W, 3RD PM

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------------|---------------------------|--------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 78 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |



CURVE RAMP C-1
 PI STA = 0+135.414
 $\Delta = 12^\circ 16' 00''$ (RT)
 R = 975.000 m
 T = 104.772 m
 L = 208.742 m
 E = 5.613 m
 PT STA = 0+239.384
 PC STA = 0+030.642
 SE = 4.8%
 SE ATTAINED STA 0+010.064 TO STA 0+051.538
 SE TRANSITION STA 0+196.244 TO STA 0+260.384

FOR EXIT RAMP C TERMINAL DETAILS AND PROFILE SEE SHEETS 87 & 165-166

STA 40+405
 PROPOSED MANHOLE, TYPE A, 1.500 m ϕ , STD 602401
 TYPE I FRAME AND OPEN LID STD 604001
 TOP LID = 188.340, 16.5 m LT
 PRECAST REINFORCED CONCRETE FLAT SLAB TOP STD 602601
 PROPOSED PIPE CULVERT, RCCP, TYPE 2, 600 mm ϕ X 11.3 m
 USFL 186.525, 17.2 m LT
 DSFL 186.470, 28.5 m LT
 PRECAST REINFORCED CONCRETE FLARED END SECTION STD 542301, 1-EACH

CURVE RAMP D-1
 PI STA = 0+508.018
 $\Delta = 10^\circ 44' 55''$ (LT)
 R = 1015.000 m
 T = 95.488 m
 L = 190.415 m
 E = 4.482 m
 PC STA = 0+412.530
 PT STA = 0+602.945
 SE = 4.8%
 SE ATTAINED STA 0+382.000 TO STA 0+427.000
 SE REMOVED STA 0+582.410 TO STA 0+662.782

CURVE NO 4
 PI STA = 40+295.227
 $\Delta = 23^\circ 43' 39''$ (LT)
 R = 995.000 m
 T = 209.022 m
 L = 412.052 m
 E = 21.718 m
 PC STA = 40+086.205
 PT STA = 40+498.257
 SE = 4.8%
 SE ATTAINED STA 40+026.000 TO STA 40+106.000
 SE REMOVED STA 40+478.000 TO STA 40+558.000

FOR ENTRANCE RAMP D TERMINAL DETAILS AND PROFILE SEE SHEETS 86 & 163-164

| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAN
FAP 310 (IL 255)
SECTION 60-15
MADISON COUNTY
 DRAWN BY EBB
 CHECKED BY
 DATE

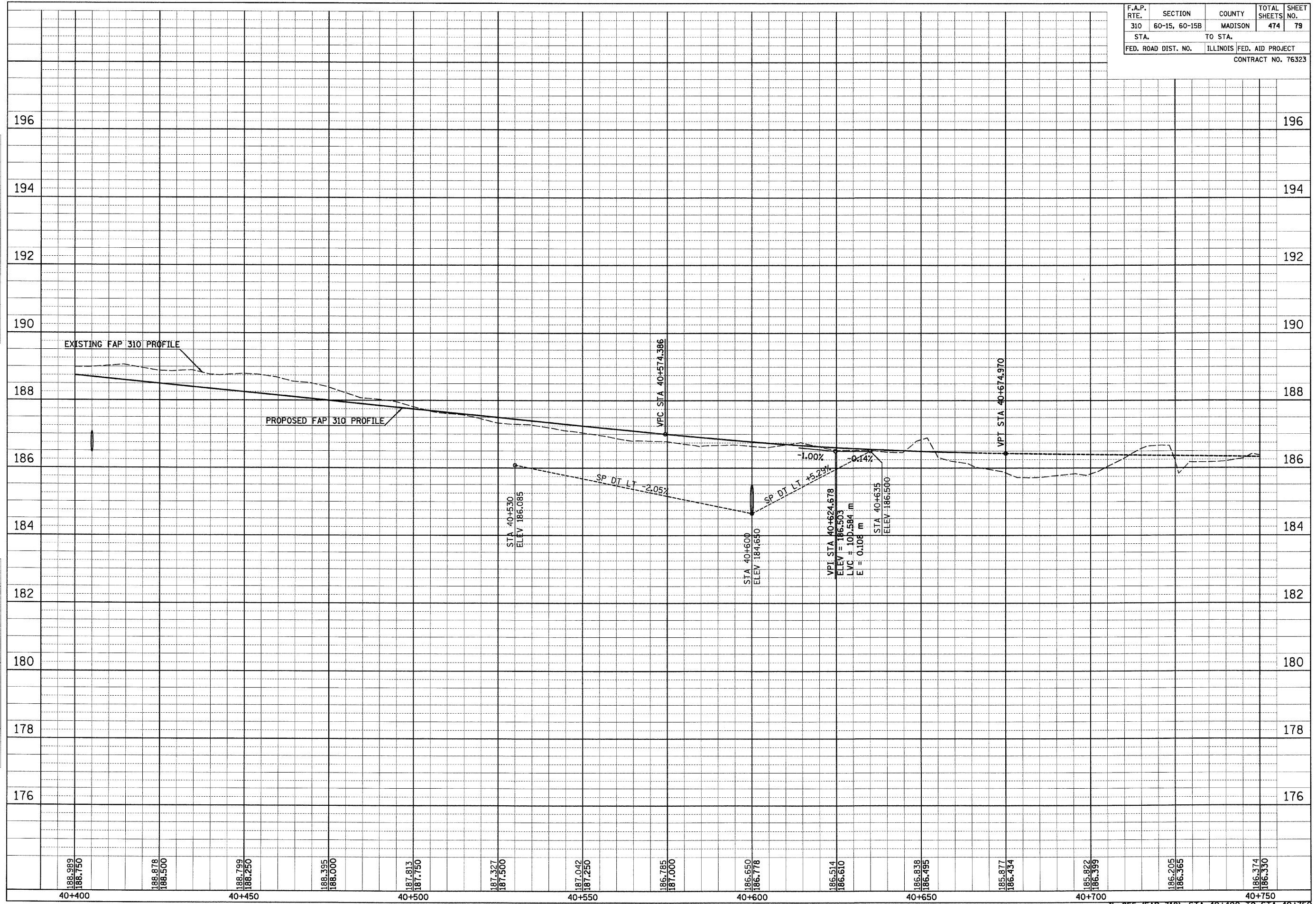
NW 1/4, SEC 14, T 6 N, R 10 W, 3RD PM

IL 255 (FAP 310), STA 40+400 TO STA 40+665.645

| ORIGINAL SURVEY | DATE |
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| SURVEY PLOTTED | |
| NOTE BOOK TEMPLATE | |
| NO. | |
| AREAS CHECKED | |

| FINAL SURVEY | DATE |
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| SURVEY PLOTTED | |
| NOTE BOOK TEMPLATE | |
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| AREAS CHECKED | |

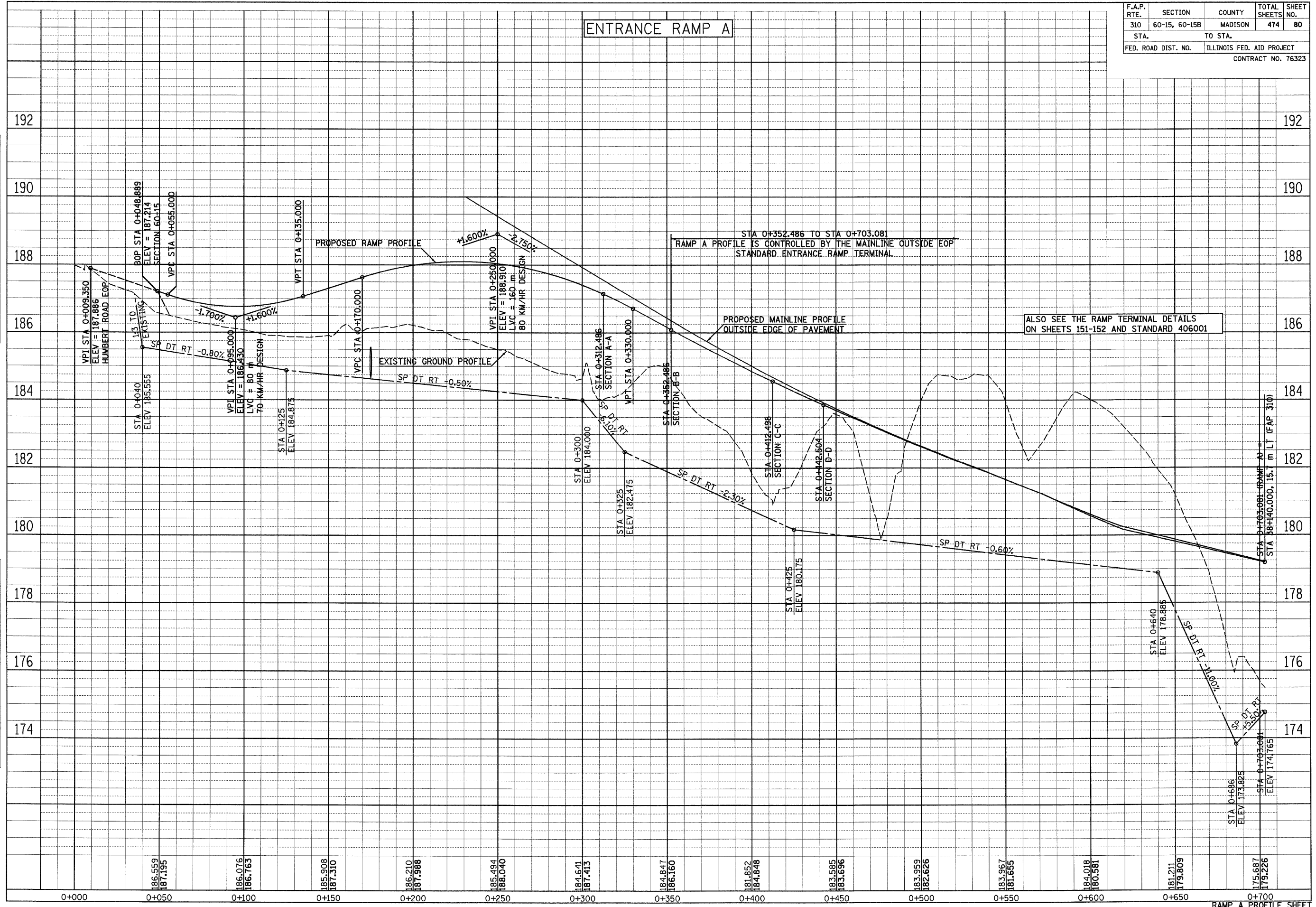
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------------|---------------------------|--------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 79 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |



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| ORIGINAL SURVEY | BY | DATE |
| SURVEY PLOTTED | | |
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| FINAL SURVEY | BY | DATE |
| SURVEY PLOTTED | | |
| NOTE BOOK TEMPLATE | | |
| NO. | AREAS CHECKED | |

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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 80 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |

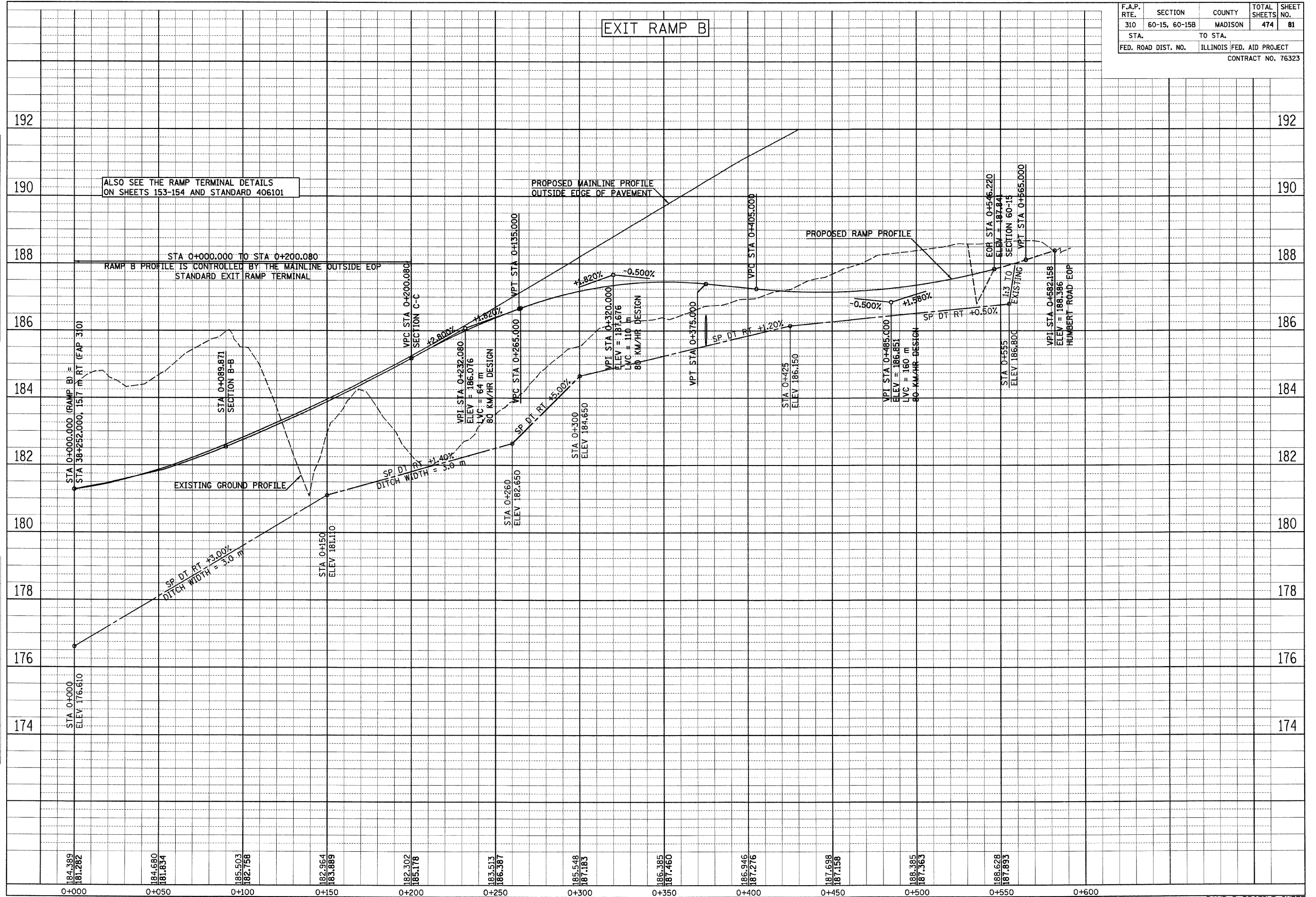


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| FINAL SURVEY | SURVEYED | BY | DATE |
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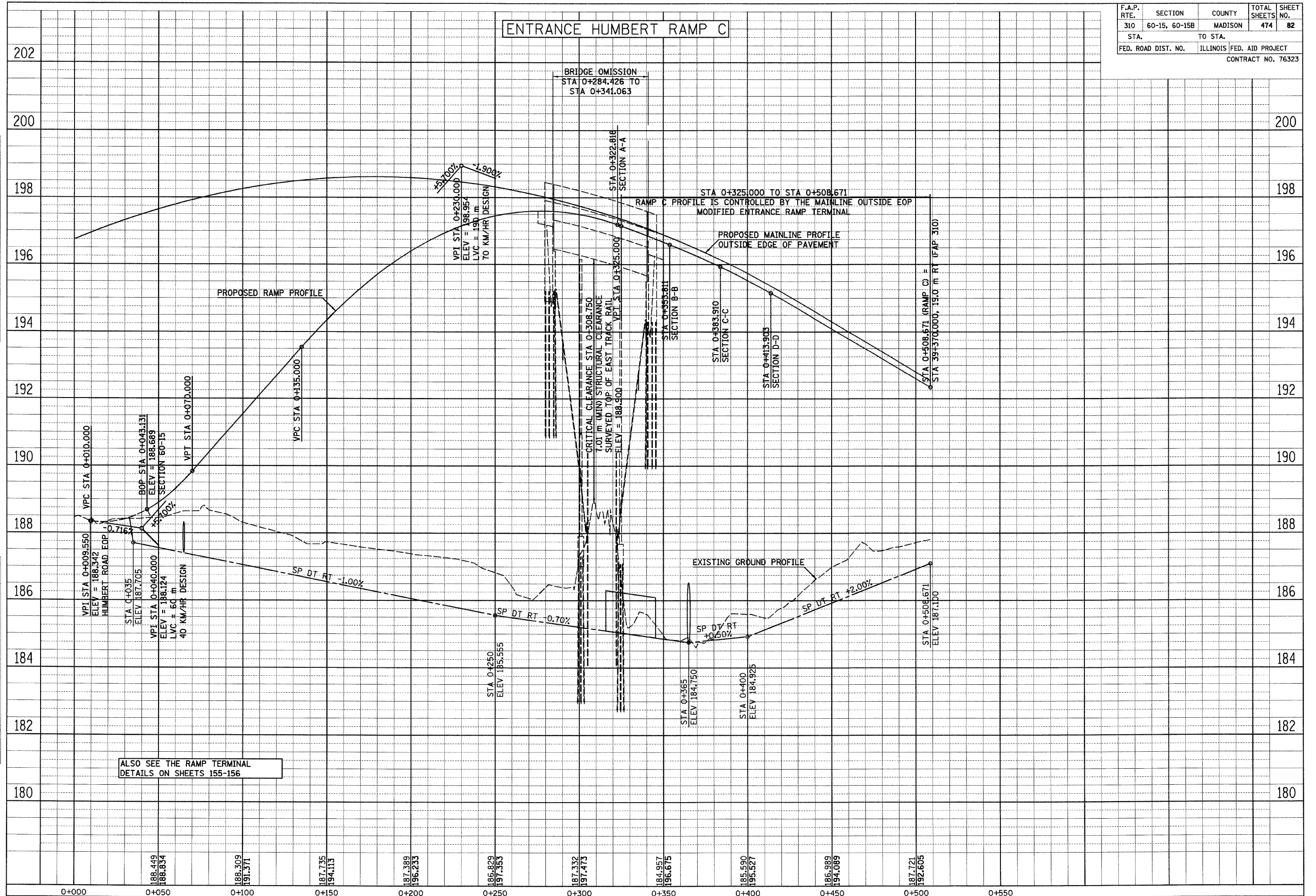
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 81 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | CONTRACT NO. 76323 | |

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| ORIGINAL SURVEY | BY | DATE |
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| FINAL SURVEY | BY | DATE |
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| NOTE BOOK TEMPLATE | | |
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| AREAS CHECKED | | |



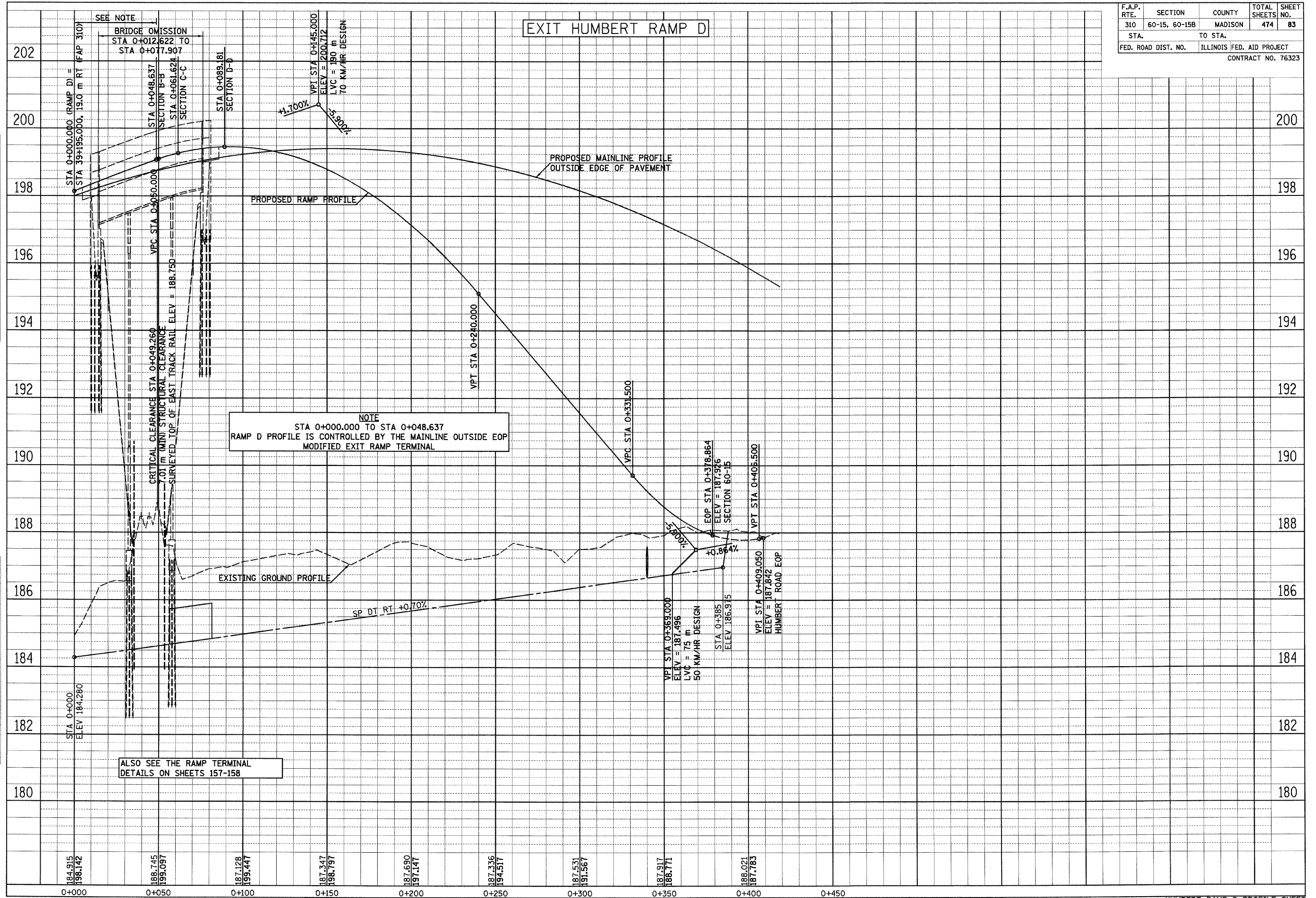
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------------|---------------------------|--------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 82 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |

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| ORIGINAL SURVEY | BY | DATE |
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| NOTE BOOK | | |
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| NO. | | |
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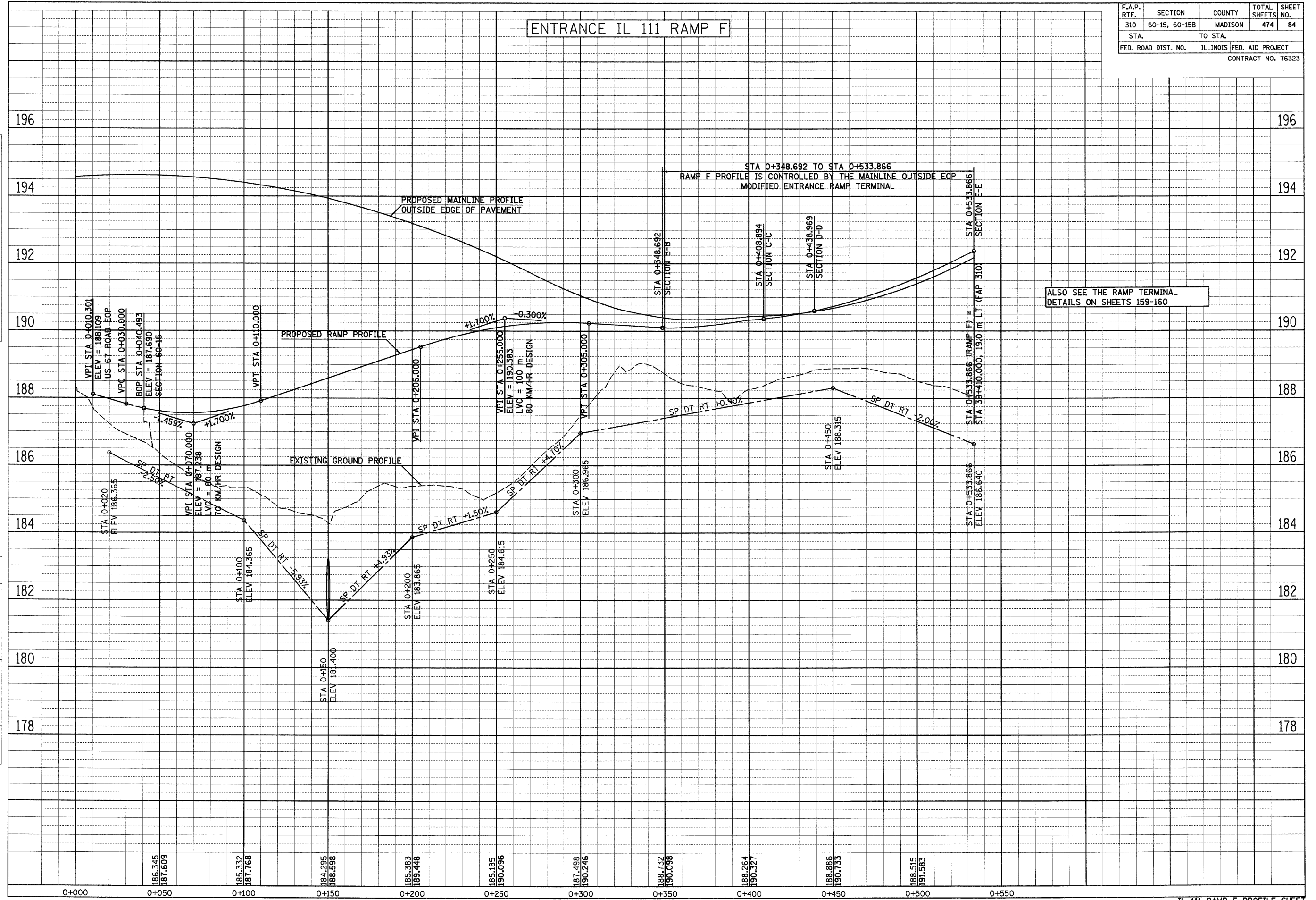
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| NOTE BOOK | | |
| AREAS | | |
| NO. | | |
| AREAS CHECKED | | |



| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------------|---------------------------|--------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 83 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |

| ORIGINAL SURVEY | DATE |
|-----------------|------|
| SURVEY PLOTTED | |
| NOTE BOOK | |
| NO. | |

| FINAL SURVEY | DATE |
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| SURVEY PLOTTED | |
| NOTE BOOK | |
| NO. | |



| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|---------------|---------|--------------|-----------|
| 310 | 60-15, 60-15B | MADISON | 474 | 84 |

| STA. | TO STA. |
|---------------------|---------------------------|
| FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT |

CONTRACT NO. 76323

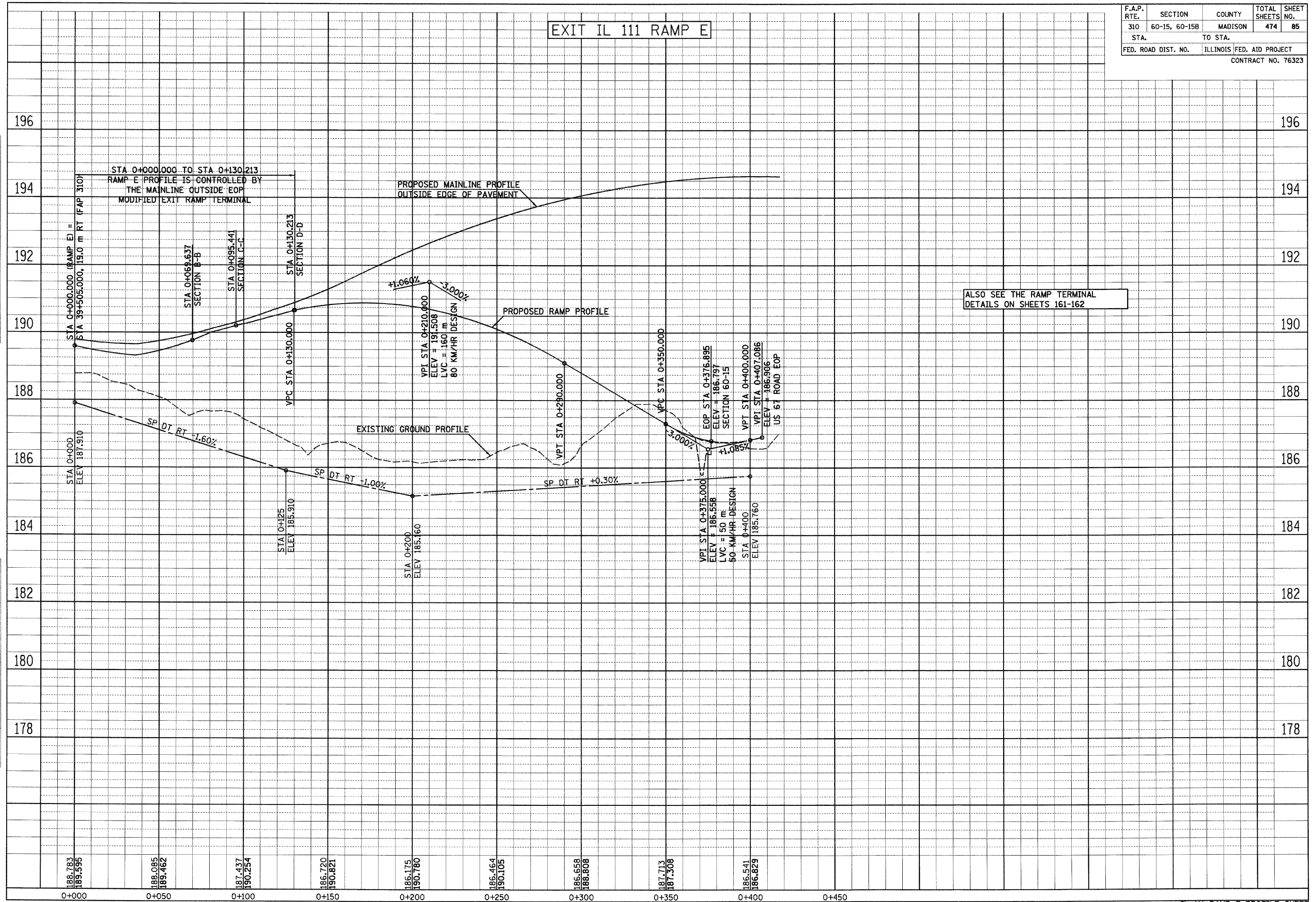
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| FINAL SURVEYED | BY | DATE |
| SURVEY PLOTTED | | |
| NOTE BOOK TEMPLATE | | |
| NO. | | |
| AREAS CHECKED | | |

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|---------------------|---------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 85 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |



ALSO SEE THE RAMP TERMINAL DETAILS ON SHEETS 161-162

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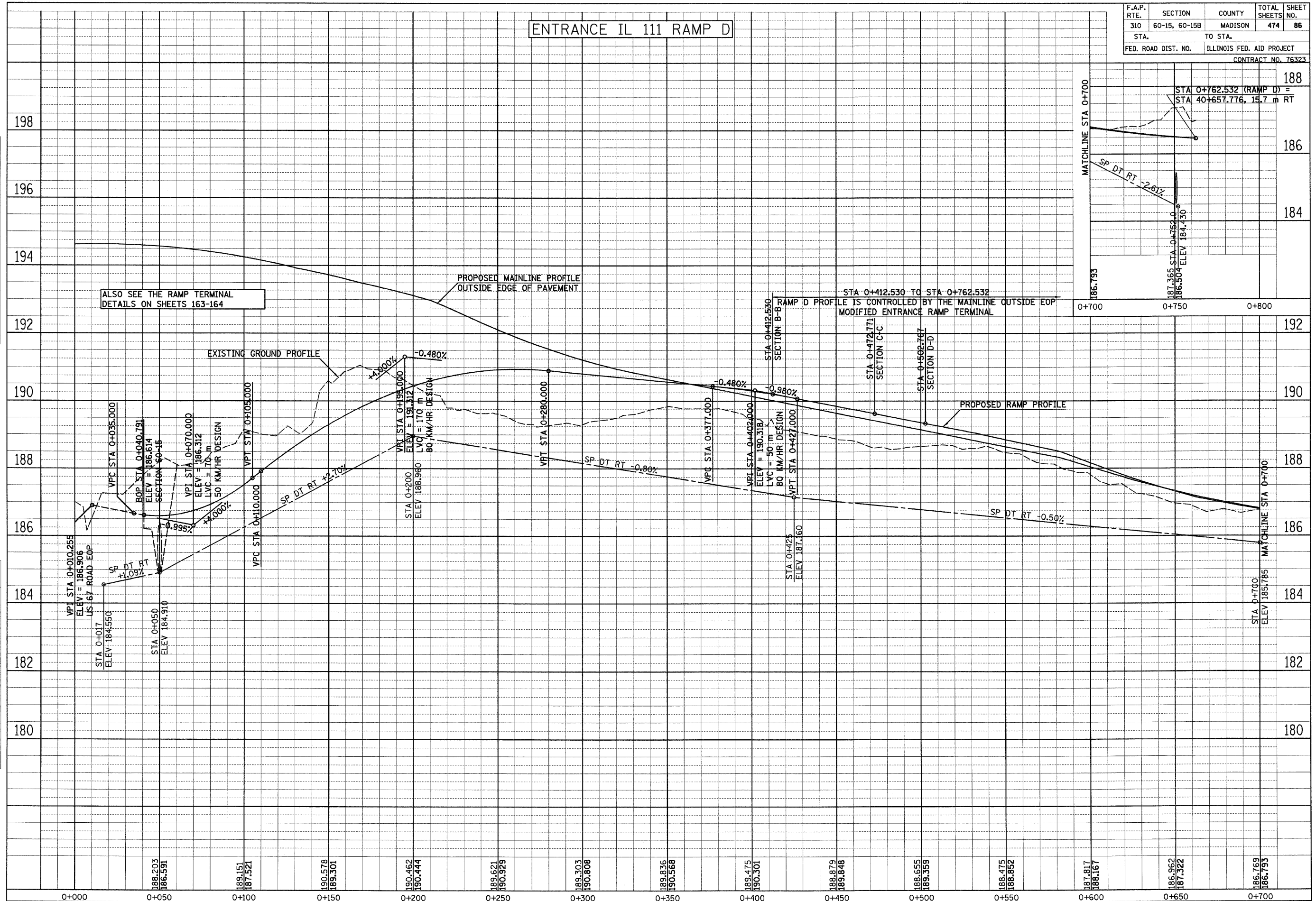
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| FINAL SURVEY | SURVEYED | BY | DATE |
| NO. | NO. | | |
| AREAS CHECKED | AREAS CHECKED | | |

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| ORIGINAL SURVEY | SURVEYED | BY | DATE |
| NO. | NO. | | |
| AREAS CHECKED | AREAS CHECKED | | |

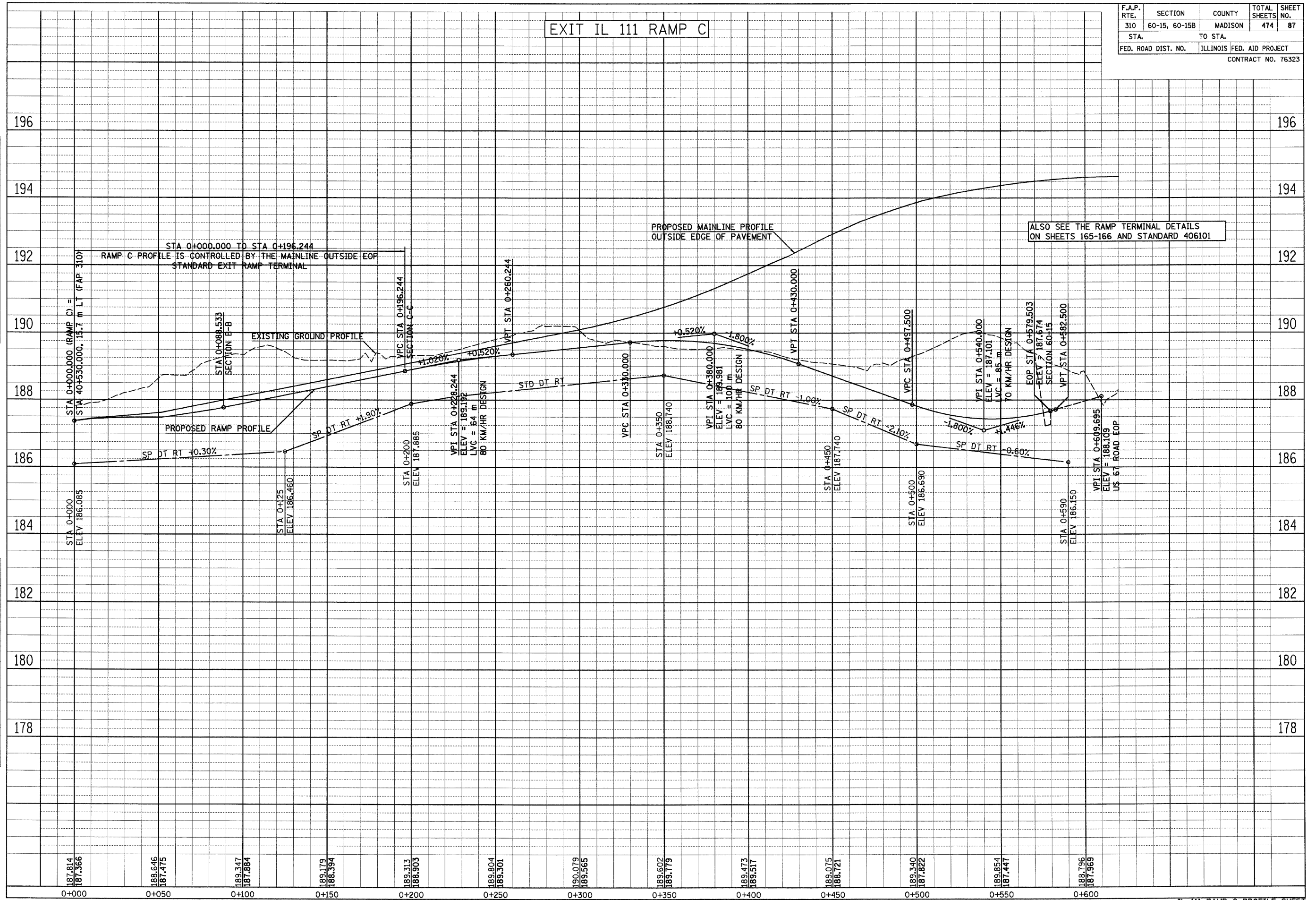
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 86 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |



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| NOTE BOOK | | |
| AREAS CHECKED | | |
| NO. | | |

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| FINAL SURVEY | BY | DATE |
| SURVEY PLOTTED | | |
| NOTE BOOK | | |
| AREAS CHECKED | | |
| NO. | | |



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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 87 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |

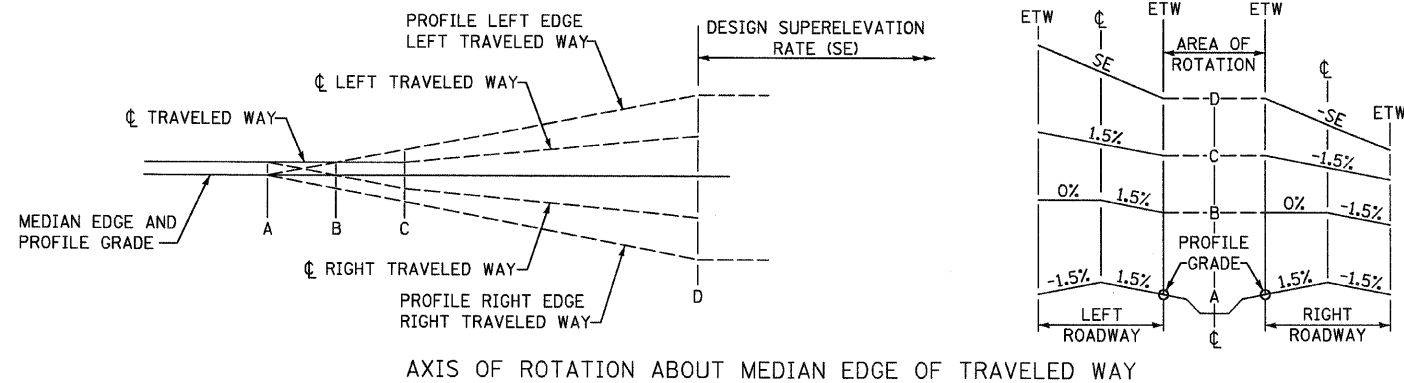
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 88 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |

| CURVE NO 1 | | | | | | | | | | | |
|------------|------------|--------------|---------|---------|---------|-------------|-------------|---------|---------|---------|--------------|
| SECTION | STATION | OUTSIDE EDGE | SLOPE % | CL | SLOPE % | MEDIAN EDGE | MEDIAN EDGE | SLOPE % | CL | SLOPE % | OUTSIDE EDGE |
| A | 35+238.000 | 167.327 | -1.50 | 167.381 | -1.50 | 167.327 | 167.327 | -1.50 | 167.381 | -1.50 | 167.327 |
| | 35+240.000 | 167.305 | -1.36 | 167.354 | -1.50 | 167.300 | 167.300 | -1.36 | 167.349 | -1.50 | 167.295 |
| | 35+250.000 | 167.197 | -0.66 | 167.221 | -1.50 | 167.167 | 167.167 | -0.66 | 167.190 | -1.50 | 167.136 |
| B | 35+259.429 | 167.095 | 0.00 | 167.095 | -1.50 | 167.041 | 167.041 | 0.00 | 167.041 | -1.50 | 166.987 |
| | 35+260.000 | 167.089 | 0.04 | 167.087 | -1.50 | 167.033 | 167.033 | 0.04 | 167.032 | -1.50 | 166.978 |
| | 35+270.000 | 166.981 | 0.74 | 166.955 | -1.50 | 166.901 | 166.901 | 0.74 | 166.874 | -1.50 | 166.820 |
| C | 35+280.000 | 166.879 | 1.44 | 166.827 | -1.50 | 166.773 | 166.773 | 1.44 | 166.721 | -1.50 | 166.667 |
| | 35+280.857 | 166.871 | 1.50 | 166.817 | -1.50 | 166.763 | 166.763 | 1.50 | 166.709 | -1.50 | 166.655 |
| | 35+290.000 | 166.805 | 2.14 | 166.728 | -2.14 | 166.651 | 166.651 | 2.14 | 166.574 | -2.14 | 166.497 |
| D | 35+300.000 | 166.740 | 2.84 | 166.638 | -2.84 | 166.535 | 166.535 | 2.84 | 166.433 | -2.84 | 166.331 |
| | 35+310.000 | 166.680 | 3.54 | 166.553 | -3.54 | 166.425 | 166.425 | 3.54 | 166.298 | -3.54 | 166.170 |
| | 35+318.000 | 166.636 | 4.10 | 166.489 | -4.10 | 166.341 | 166.341 | 4.10 | 166.193 | -4.10 | 166.046 |

| CURVE NO 2 | | | | | | | | | | | |
|------------|------------|--------------|---------|---------|---------|-------------|-------------|---------|---------|---------|--------------|
| SECTION | STATION | OUTSIDE EDGE | SLOPE % | CL | SLOPE % | MEDIAN EDGE | MEDIAN EDGE | SLOPE % | CL | SLOPE % | OUTSIDE EDGE |
| A | 37+220.000 | 173.840 | -1.50 | 173.894 | -1.50 | 173.840 | 173.840 | -1.50 | 173.894 | -1.50 | 173.840 |
| | 37+230.000 | 173.679 | -1.50 | 173.733 | -0.63 | 173.710 | 173.710 | -1.50 | 173.764 | -0.63 | 173.742 |
| | 37+237.143 | 173.563 | -1.50 | 173.617 | 0.00 | 173.617 | 173.617 | -1.50 | 173.671 | 0.00 | 173.671 |
| B | 37+240.000 | 173.517 | -1.50 | 173.571 | 0.25 | 173.580 | 173.580 | -1.50 | 173.634 | 0.25 | 173.643 |
| | 37+250.000 | 173.356 | -1.50 | 173.410 | 1.13 | 173.450 | 173.450 | -1.50 | 173.504 | 1.13 | 173.545 |
| | 37+254.286 | 173.286 | -1.50 | 173.340 | 1.50 | 173.394 | 173.394 | -1.50 | 173.448 | 1.50 | 173.502 |
| C | 37+260.000 | 173.176 | -2.00 | 173.248 | 2.00 | 173.320 | 173.320 | -2.00 | 173.392 | 2.00 | 173.464 |
| | 37+270.000 | 172.984 | -2.88 | 173.087 | 2.88 | 173.191 | 173.191 | -2.88 | 173.294 | 2.88 | 173.398 |
| | 37+280.000 | 172.798 | -3.75 | 172.933 | 3.75 | 173.068 | 173.068 | -3.75 | 173.203 | 3.75 | 173.338 |
| D | 37+290.000 | 172.619 | -4.63 | 172.786 | 4.63 | 172.952 | 172.952 | -4.63 | 173.119 | 4.63 | 173.285 |
| | 37+300.000 | 172.447 | -5.50 | 172.645 | 5.50 | 172.843 | 172.843 | -5.50 | 173.041 | 5.50 | 173.239 |

| CURVE NO 3 | | | | | | | | | | | |
|------------|------------|--------------|---------|---------|---------|-------------|-------------|---------|---------|---------|--------------|
| SECTION | STATION | OUTSIDE EDGE | SLOPE % | CL | SLOPE % | MEDIAN EDGE | MEDIAN EDGE | SLOPE % | CL | SLOPE % | OUTSIDE EDGE |
| A | 38+649.000 | 191.110 | -1.50 | 191.164 | -1.50 | 191.110 | 191.110 | -1.50 | 191.164 | -1.50 | 191.110 |
| | 38+650.000 | 191.143 | -1.41 | 191.194 | -1.50 | 191.140 | 191.140 | -1.41 | 191.191 | -1.50 | 191.137 |
| | 38+660.000 | 191.475 | -0.54 | 191.494 | -1.50 | 191.440 | 191.440 | -0.54 | 191.459 | -1.50 | 191.405 |
| B | 38+666.143 | 191.678 | 0.00 | 191.678 | -1.50 | 191.624 | 191.624 | 0.00 | 191.624 | -1.50 | 191.570 |
| | 38+670.000 | 191.806 | 0.34 | 191.794 | -1.50 | 191.740 | 191.740 | 0.34 | 191.728 | -1.50 | 191.674 |
| | 38+680.000 | 192.138 | 1.21 | 192.094 | -1.50 | 192.040 | 192.040 | 1.21 | 191.996 | -1.50 | 191.942 |
| C | 38+683.286 | 192.247 | 1.50 | 192.193 | -1.50 | 192.139 | 192.139 | 1.50 | 192.085 | -1.50 | 192.031 |
| | 38+690.000 | 192.490 | 2.09 | 192.415 | -2.09 | 192.340 | 192.340 | 2.09 | 192.265 | -2.09 | 192.190 |
| | 38+700.000 | 192.853 | 2.96 | 192.747 | -2.96 | 192.640 | 192.640 | 2.96 | 192.533 | -2.96 | 192.427 |
| D | 38+710.000 | 193.216 | 3.84 | 193.078 | -3.84 | 192.940 | 192.940 | 3.84 | 192.802 | -3.84 | 192.664 |
| | 38+720.000 | 193.579 | 4.71 | 193.410 | -4.71 | 193.240 | 193.240 | 4.71 | 193.070 | -4.71 | 192.901 |
| | 38+729.000 | 193.906 | 5.50 | 193.708 | -5.50 | 193.510 | 193.510 | 5.50 | 193.312 | -5.50 | 193.114 |

| CURVE NO 4 | | | | | | | | | | | |
|------------|------------|--------------|---------|---------|---------|-------------|-------------|---------|---------|---------|--------------|
| SECTION | STATION | OUTSIDE EDGE | SLOPE % | CL | SLOPE % | MEDIAN EDGE | MEDIAN EDGE | SLOPE % | CL | SLOPE % | OUTSIDE EDGE |
| A | 40+026.000 | 193.928 | -1.50 | 193.982 | -1.50 | 193.928 | 193.928 | -1.50 | 193.982 | -1.50 | 193.928 |
| | 40+030.000 | 193.867 | -1.50 | 193.921 | -1.19 | 193.878 | 193.878 | -1.50 | 193.932 | -1.19 | 193.889 |
| | 40+040.000 | 193.707 | -1.50 | 193.761 | -0.40 | 193.747 | 193.747 | -1.50 | 193.801 | -0.40 | 193.787 |
| B | 40+045.048 | 193.623 | -1.50 | 193.677 | 0.00 | 193.677 | 193.677 | -1.50 | 193.731 | 0.00 | 193.731 |
| | 40+050.000 | 193.538 | -1.50 | 193.592 | 0.39 | 193.606 | 193.606 | -1.50 | 193.660 | 0.39 | 193.674 |
| | 40+060.000 | 193.358 | -1.50 | 193.412 | 1.18 | 193.454 | 193.454 | -1.50 | 193.508 | 1.18 | 193.550 |
| C | 40+064.095 | 193.281 | -1.50 | 193.335 | 1.50 | 193.389 | 193.389 | -1.50 | 193.443 | 1.50 | 193.497 |
| | 40+070.000 | 193.151 | -1.97 | 193.221 | 1.97 | 193.292 | 193.292 | -1.97 | 193.363 | 1.97 | 193.433 |
| | 40+080.000 | 192.922 | -2.75 | 193.021 | 2.75 | 193.120 | 193.120 | -2.75 | 193.219 | 2.75 | 193.318 |
| D | 40+090.000 | 192.682 | -3.54 | 192.810 | 3.54 | 192.937 | 192.937 | -3.54 | 193.064 | 3.54 | 193.192 |
| | 40+100.000 | 192.431 | -4.33 | 192.587 | 4.33 | 192.743 | 192.743 | -4.33 | 192.899 | 4.33 | 193.055 |
| | 40+106.000 | 192.276 | -4.80 | 192.449 | 4.80 | 192.622 | 192.622 | -4.80 | 192.795 | 4.80 | 192.968 |



| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION SUPERELEVATION TABLES FAP 310 (IL 255) SECTION 60-15 MADISON COUNTY |
|-----------|------|---|
| NAME | DATE | |
| | | DRAWN BY EBB CHECKED BY DATE |
| | | |
| | | |
| | | |
| | | |

| | | | | |
|---------------------|---------------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 310 | 60-15, 60-15B | MADISON | 474 | 89 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 76323 | | | | |

| HUMBERT RAMP A | | | |
|------------------------|-----------|---------|------------|
| STATION | LEFT EDGE | SLOPE % | RIGHT EDGE |
| 0+065.000 | 186.887 | -1.50 | 186.961 |
| 0+070.000 | 186.800 | -2.08 | 186.901 |
| 0+080.000 | 186.656 | -3.23 | 186.814 |
| 0+090.000 | 186.553 | -4.38 | 186.768 |
| 0+100.000 | 186.491 | -5.54 | 186.763 |
| 0+104.000 | 186.478 | -6.00 | 186.772 |
| MAXIMUM SUPERELEVATION | | | |
| 0+151.000 | 187.032 | -6.00 | 187.326 |
| 0+160.000 | 187.227 | -4.96 | 187.470 |
| 0+170.000 | 187.443 | -3.81 | 187.630 |
| 0+180.000 | 187.646 | -2.65 | 187.776 |
| 0+190.000 | 187.822 | -1.50 | 187.896 |
| 0+200.000 | 187.971 | -0.35 | 187.988 |
| 0+203.000 | 188.010 | 0.00 | 188.010 |
| 0+210.000 | 188.087 | 0.70 | 188.053 |
| 0+220.000 | 188.174 | 1.70 | 188.090 |
| 0+230.000 | 188.233 | 2.70 | 188.101 |
| 0+240.000 | 188.265 | 3.70 | 188.084 |
| 0+250.000 | 188.270 | 4.70 | 188.040 |
| 0+260.000 | 188.248 | 5.70 | 187.969 |
| 0+263.000 | 188.236 | 6.00 | 187.942 |
| MAXIMUM SUPERELEVATION | | | |
| 0+312.486 | 187.444 | 6.00 | 187.150 |
| 0+320.000 | 187.264 | 5.96 | 186.971 |
| 0+330.000 | 187.000 | 5.92 | 186.710 |
| 0+340.000 | 186.723 | 5.87 | 186.435 |
| 0+350.000 | 186.445 | 5.82 | 186.160 |
| 0+352.486 | 186.377 | 5.81 | 186.092 |
| 0+360.000 | 186.157 | 5.49 | 185.888 |
| 0+370.000 | 185.868 | 5.07 | 185.620 |
| 0+380.000 | 185.586 | 4.64 | 185.359 |
| 0+390.000 | 185.311 | 4.22 | 185.104 |
| 0+400.000 | 185.042 | 3.79 | 184.856 |
| 0+410.000 | 184.780 | 3.37 | 184.615 |
| 0+412.498 | 184.715 | 3.26 | 184.555 |
| 0+420.000 | 184.523 | 3.00 | 184.376 |
| 0+430.000 | 184.274 | 2.67 | 184.143 |
| 0+440.000 | 184.030 | 2.33 | 183.916 |
| 0+442.504 | 183.970 | 2.24 | 183.860 |
| 0+450.000 | 183.793 | 1.99 | 183.696 |
| 0+460.000 | 183.562 | 1.65 | 183.482 |
| 0+464.273 | 183.465 | 1.50 | 183.392 |
| NORMAL CROSS SLOPE | | | |
| 0+573.676 | 181.234 | 1.50 | 181.191 |
| 0+580.000 | 181.095 | 2.05 | 181.039 |
| 0+590.000 | 180.880 | 2.93 | 180.805 |
| 0+600.000 | 180.670 | 3.81 | 180.580 |
| 0+610.000 | 180.463 | 4.70 | 180.362 |
| 0+618.927 | 180.284 | 5.50 | 180.175 |

| HUMBERT RAMP C | | | |
|------------------------|-----------|---------|------------|
| STATION | LEFT EDGE | SLOPE % | RIGHT EDGE |
| 0+106.000 | 191.960 | 1.50 | 191.886 |
| 0+110.000 | 192.209 | 1.94 | 192.114 |
| 0+120.000 | 192.833 | 3.04 | 192.684 |
| 0+130.000 | 193.457 | 4.13 | 193.254 |
| 0+140.000 | 194.075 | 5.23 | 193.819 |
| 0+147.000 | 194.488 | 6.00 | 194.194 |
| MAXIMUM SUPERELEVATION | | | |
| 0+272.000 | 197.888 | 6.00 | 197.594 |
| 0+280.000 | 197.873 | 5.60 | 197.599 |
| 0+282.000 | 197.866 | 5.50 | 197.596 |

| HUMBERT RAMP B | | | |
|------------------------|-----------|---------|------------|
| STATION | LEFT EDGE | SLOPE % | RIGHT EDGE |
| 0+000.000 | 181.272 | -3.23 | 181.282 |
| 0+010.000 | 181.360 | -2.35 | 181.380 |
| 0+020.000 | 181.455 | -1.47 | 181.475 |
| 0+030.000 | 181.586 | -0.59 | 181.598 |
| 0+040.000 | 181.724 | 0.28 | 181.717 |
| 0+050.000 | 181.869 | 1.16 | 181.834 |
| 0+053.904 | 181.927 | 1.50 | 181.878 |
| NORMAL CROSS SLOPE | | | |
| 0+200.080 | 185.254 | 1.50 | 185.180 |
| 0+210.000 | 185.558 | 2.20 | 185.450 |
| 0+220.000 | 185.850 | 2.90 | 185.707 |
| 0+230.000 | 186.126 | 3.60 | 185.949 |
| 0+240.000 | 186.387 | 4.31 | 186.176 |
| 0+250.000 | 186.632 | 5.01 | 186.387 |
| 0+260.000 | 186.863 | 5.71 | 186.583 |
| 0+264.080 | 186.952 | 6.00 | 186.658 |
| MAXIMUM SUPERELEVATION | | | |
| 0+333.000 | 187.719 | 6.00 | 187.425 |
| 0+340.000 | 187.707 | 5.30 | 187.447 |
| 0+350.000 | 187.671 | 4.30 | 187.460 |
| 0+360.000 | 187.614 | 3.30 | 187.452 |
| 0+370.000 | 187.536 | 2.30 | 187.423 |
| 0+380.000 | 187.440 | 1.30 | 187.376 |
| 0+390.000 | 187.341 | 0.30 | 187.326 |
| 0+393.000 | 187.311 | 0.00 | 187.311 |
| 0+400.000 | 187.236 | -0.81 | 187.276 |
| 0+410.000 | 187.131 | -1.96 | 187.228 |
| 0+420.000 | 187.038 | -3.12 | 187.191 |
| 0+430.000 | 186.957 | -4.27 | 187.167 |
| 0+440.000 | 186.890 | -5.42 | 187.156 |
| 0+445.000 | 186.861 | -6.00 | 187.155 |
| MAXIMUM SUPERELEVATION | | | |
| 0+491.000 | 187.008 | -6.00 | 187.302 |
| 0+500.000 | 187.119 | -4.96 | 187.363 |
| 0+510.000 | 187.256 | -3.81 | 187.443 |
| 0+520.000 | 187.406 | -2.65 | 187.536 |
| 0+530.000 | 187.568 | -1.50 | 187.642 |

| HUMBERT RAMP D | | | |
|------------------------|-----------|---------|------------|
| STATION | LEFT EDGE | SLOPE % | RIGHT EDGE |
| 0+048.637 | 198.878 | -4.00 | 199.074 |
| 0+050.000 | 198.905 | -3.92 | 199.097 |
| 0+060.000 | 199.085 | -3.30 | 199.247 |
| 0+070.000 | 199.226 | -2.68 | 199.357 |
| 0+080.000 | 199.326 | -2.07 | 199.427 |
| 0+089.181 | 199.383 | -1.50 | 199.456 |
| NORMAL CROSS SLOPE | | | |
| 0+250.000 | 194.444 | -1.50 | 194.517 |
| 0+260.000 | 193.798 | -2.63 | 193.927 |
| 0+270.000 | 193.153 | -3.76 | 193.337 |
| 0+280.000 | 192.507 | -4.89 | 192.747 |
| 0+288.000 | 191.991 | -5.80 | 192.275 |
| MAXIMUM SUPERELEVATION | | | |
| 0+342.000 | 188.855 | -5.80 | 189.139 |
| 0+350.000 | 188.531 | -4.89 | 188.771 |
| 0+360.000 | 188.209 | -3.76 | 188.393 |
| 0+370.000 | 187.976 | -2.63 | 188.105 |
| 0+380.000 | 187.834 | -1.50 | 187.908 |

| IL 111 RAMP F | | | |
|------------------------|-----------|---------|------------|
| STATION | LEFT EDGE | SLOPE % | RIGHT EDGE |
| 0+053.000 | 187.517 | -1.50 | 187.591 |
| 0+060.000 | 187.451 | -2.26 | 187.562 |
| 0+070.000 | 187.391 | -3.33 | 187.554 |
| 0+080.000 | 187.369 | -4.41 | 187.586 |
| 0+090.000 | 187.388 | -5.49 | 187.657 |
| 0+091.000 | 187.392 | -5.60 | 187.666 |
| MAXIMUM SUPERELEVATION | | | |
| 0+126.000 | 187.916 | -5.60 | 188.190 |
| 0+130.000 | 188.006 | -5.13 | 188.258 |
| 0+140.000 | 188.234 | -3.97 | 188.428 |
| 0+150.000 | 188.461 | -2.80 | 188.598 |
| 0+160.000 | 188.688 | -1.63 | 188.768 |
| 0+170.000 | 188.915 | -0.47 | 188.938 |
| 0+174.000 | 189.006 | 0.00 | 189.006 |
| 0+180.000 | 189.137 | 0.60 | 189.108 |
| 0+190.000 | 189.356 | 1.60 | 189.278 |
| 0+200.000 | 189.575 | 2.60 | 189.448 |
| 0+210.000 | 189.792 | 3.60 | 189.616 |
| 0+220.000 | 189.991 | 4.60 | 189.766 |
| 0+230.000 | 190.170 | 5.60 | 189.896 |
| 0+234.000 | 190.236 | 6.00 | 189.942 |
| MAXIMUM SUPERELEVATION | | | |
| 0+308.692 | 190.516 | 6.00 | 190.222 |
| 0+310.000 | 190.511 | 5.99 | 190.218 |
| 0+320.000 | 190.477 | 5.89 | 190.188 |
| 0+330.000 | 190.442 | 5.79 | 190.158 |
| 0+340.000 | 190.407 | 5.69 | 190.128 |
| 0+348.692 | 190.377 | 5.61 | 190.102 |
| 0+350.000 | 190.369 | 5.53 | 190.098 |
| 0+360.000 | 190.342 | 4.91 | 190.102 |
| 0+370.000 | 190.342 | 4.29 | 190.132 |
| 0+380.000 | 190.361 | 3.66 | 190.181 |
| 0+390.000 | 190.399 | 3.04 | 190.250 |
| 0+400.000 | 190.446 | 2.42 | 190.327 |
| 0+410.000 | 190.459 | 1.80 | 190.371 |
| 0+420.000 | 190.491 | 1.18 | 190.433 |
| 0+430.000 | 190.542 | 0.56 | 190.514 |
| 0+438.969 | 190.603 | 0.00 | 190.603 |
| 0+440.000 | 190.611 | -0.06 | 190.614 |
| 0+450.000 | 190.699 | -0.69 | 190.733 |
| 0+460.000 | 190.806 | -1.31 | 190.870 |
| 0+470.000 | 190.932 | -1.93 | 191.026 |
| 0+480.000 | 191.071 | -2.55 | 191.196 |
| 0+490.000 | 191.226 | -3.17 | 191.382 |
| 0+500.000 | 191.397 | -3.79 | 191.583 |
| 0+510.000 | 191.585 | -4.41 | 191.801 |
| 0+520.000 | 191.789 | -5.04 | 192.036 |
| 0+527.476 | 191.952 | -5.50 | 192.222 |

| IL 111 RAMP E | | | |
|------------------------|-----------|---------|------------|
| STATION | LEFT EDGE | SLOPE % | RIGHT EDGE |
| 0+035.950 | 189.455 | 5.50 | 189.316 |
| 0+040.000 | 189.496 | 5.13 | 189.352 |
| 0+050.000 | 189.611 | 4.23 | 189.462 |
| 0+060.000 | 189.745 | 3.34 | 189.603 |
| 0+070.000 | 189.896 | 2.46 | 189.775 |
| 0+080.000 | 190.067 | 1.59 | 189.989 |
| 0+081.052 | 190.140 | 1.50 | 190.067 |
| NORMAL CROSS SLOPE | | | |
| 0+095.441 | 190.278 | 1.50 | 190.205 |
| 0+100.000 | 190.349 | 1.87 | 190.258 |
| 0+110.000 | 190.518 | 2.67 | 190.387 |
| 0+120.000 | 190.692 | 3.48 | 190.522 |
| 0+130.000 | 190.870 | 4.29 | 190.660 |
| 0+140.000 | 191.003 | 5.10 | 190.753 |
| 0+150.000 | 191.111 | 5.90 | 190.821 |
| 0+151.213 | 191.122 | 6.00 | 190.828 |
| MAXIMUM SUPERELEVATION | | | |
| 0+164.000 | 191.168 | 6.00 | 190.874 |
| 0+170.000 | 191.146 | 5.40 | 190.881 |
| 0+180.000 | 191.088 | 4.40 | 190.873 |
| 0+190.000 | 191.006 | 3.40 | 190.839 |
| 0+200.000 | 190.898 | 2.40 | 190.780 |
| 0+209.000 | 190.779 | 1.50 | 190.706 |

| IL 111 RAMP D | | | |
|------------------------|-----------|---------|------------|
| STATION | LEFT EDGE | SLOPE % | RIGHT EDGE |
| 0+382.000 | 190.339 | -1.50 | 190.413 |
| 0+390.000 | 190.265 | -2.09 | 190.367 |
| 0+400.000 | 190.163 | -2.82 | 190.301 |
| 0+410.000 | 190.051 | -3.55 | 190.225 |
| 0+420.000 | 189.929 | -4.29 | 190.139 |
| 0+427.000 | 189.838 | -4.80 | 190.073 |
| MAXIMUM SUPERELEVATION | | | |
| 0+582.410 | 188.316 | -4.80 | 188.503 |
| 0+590.000 | 188.199 | -4.21 | 188.357 |
| 0+600.000 | 188.045 | -3.44 | 188.167 |
| 0+610.000 | 187.889 | -2.66 | 187.978 |
| 0+620.000 | 187.733 | -1.87 | 187.791 |
| 0+630.000 | 187.591 | -1.08 | 187.623 |
| 0+640.000 | 187.463 | -0.29 | 187.471 |
| 0+650.000 | 187.334 | 0.49 | 187.322 |
| 0+660.000 | 187.206 | 1.28 | 187.176 |
| 0+662.782 | 187.170 | 1.50 | 187.136 |

| IL 111 RAMP C | | | |
|------------------------|-----------|---------|------------|
| STATION | LEFT EDGE | SLOPE % | RIGHT EDGE |
| 0+010.064 | 187.443 | 1.50 | 187.430 |
| 0+020.000 | 187.486 | 2.28 | 187.454 |
| 0+030.000 | 187.529 | 3.07 | 187.470 |
| 0+040.000 | 187.573 | 3.87 | 187.477 |
| 0+050.000 | 187.618 | 4.68 | 187.475 |
| 0+051.538 | 187.624 | 4.80 | 187.474 |
| MAXIMUM SUPERELEVATION | | | |
| 0+196.244 | 189.101 | 4.80 | 188.866 |
| 0+200.000 | 189.142 | 4.87 | 188.903 |
| 0+210.000 | 189.246 | 5.06 | 188.999 |
| 0+220.000 | 189.343 | 5.24 | 189.086 |
| 0+230.000 | 189.432 | 5.43 | 189.165 |
| 0+240.000 | 189.512 | 5.62 | 189.237 |
| 0+250.000 | 189.585 | 5.81 | 189.301 |
| 0+260.000 | 189.651 | 5.99 | 189.357 |
| 0+260.384 | 189.653 | 6.00 | 189.359 |
| MAXIMUM SUPERELEVATION | | | |
| 0+361.000 | 190.065 | 6.00 | 189.771 |
| 0+370.000 | 189.993 | 5.10 | 189.743 |
| 0+380.000 | 189.892 | 4.10 | 189.691 |
| 0+390.000 | 189.767 | 3.10 | 189.615 |
| 0+400.000 | 189.620 | 2.10 | 189.517 |
| 0+410.000 | 189.449 | 1.10 | 189 |

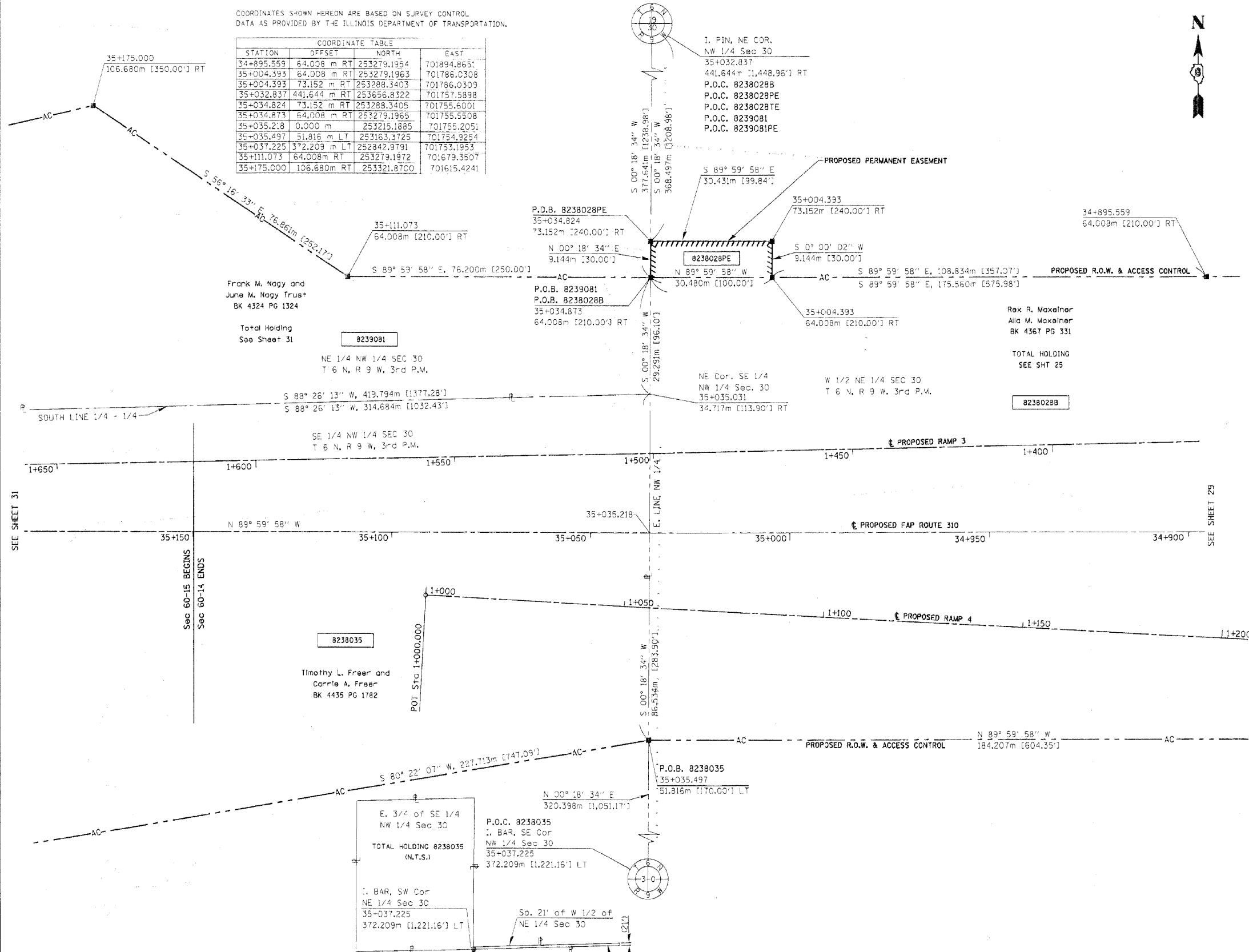
PART OF THE NE 1/4 & NW 1/4 OF SECTION 30, T6N, R9W, OF THE 3RD PM, MADISON COUNTY, ILLINOIS

BEARINGS SHOWN HEREON ARE BASED ON SURVEY CONTROL DATA AS PROVIDED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

| FAP ROUTE | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------|--------------|---------|--------------|-----------|
| 310 | 60-14, 60-15 | MADISON | 174 | 90 |

COORDINATES SHOWN HEREON ARE BASED ON SURVEY CONTROL DATA AS PROVIDED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

| STATION | OFFSET | NORTH | EAST |
|------------|--------------|-------------|-------------|
| 34+895.559 | 64.008 m RT | 253279.1954 | 701894.8651 |
| 35+004.393 | 64.008 m RT | 253279.1963 | 701786.0308 |
| 35+004.393 | 73.152 m RT | 253288.3403 | 701786.0309 |
| 35+032.837 | 441.644 m RT | 253656.8322 | 701757.5898 |
| 35+034.824 | 73.152 m RT | 253288.3405 | 701755.6001 |
| 35+034.873 | 64.008 m RT | 253279.1965 | 701755.5508 |
| 35+035.218 | 0.000 m | 253215.1885 | 701755.2051 |
| 35+035.497 | 51.816 m LT | 253163.3725 | 701754.9254 |
| 35+037.225 | 372.209 m LT | 252842.9791 | 701753.1953 |
| 35+111.073 | 64.008 m RT | 253279.1972 | 701679.3507 |
| 35+175.000 | 106.680 m RT | 253321.8700 | 701615.4241 |



LEGEND

- SECTION CORNER
- QUARTER SECTION CORNER
- ACCESS CONTROL LINE
- EXISTING CENTERLINE
- EXISTING RIGHT OF WAY LINE
- EXISTING EASEMENT LINE
- PROPOSED CENTERLINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED TEMPORARY EASEMENT LINE
- PROPOSED PERMANENT EASEMENT LINE
- SECTION LINE
- QUARTER SECTION LINE
- PROPERTY (DEED) LINE
- APL
- MEASURED DIMENSION
- COMPUTED DIMENSION
- ENGLISH EQUIVALENT
- RECORDED DIMENSION
- FOUND IRON PIPE OR IRON ROD SET 5/8 INCH IRON ROD PERMANENT SURVEY MONUMENT, I.D.O.T. STD. 2135 (TO BE SET BY OTHERS)
- CUT CROSS FOUND OR SET
- SAME OWNERSHIP

- STAKING OF PROPOSED RIGHT OF WAY. SET 5/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. SET 5/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER 20 INCHES BELOW GROUND SURFACE TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

STATE OF ILLINOIS)
 COUNTY OF)

THIS IS TO CERTIFY THAT I, THOMAS M. DRONE, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAS SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 30, TOWNSHIP 6 NORTH, RANGE 9 WEST, OF THE THIRD PRINCIPAL MERIDIAN, MADISON COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED _____
 THOMAS M. DRONE, PLS #2273



HURST-ROSCH ENGINEERS, INC.
 2657 W. LAWRENCE - SPRINGFIELD, ILLINOIS 62704
 217-787-1199

ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS
 FAP ROUTE 310 (IL 255)
 SECTION 60-14
 MADISON COUNTY
 JOB NO. R-98-038/039-92
 STATION 34+300 TO STATION 35+150

0 m 10 m 20 m 30 m
 SCALE 1:500

SHEET 30 OF 38

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS/DISTRICT 8
 1102 EASTPORT PLAZA DRIVE
 COLLINGSVILLE, ILLINOIS 62234-6138

| PARCEL NO. | OWNER | TOTAL HOLDING | R.O.W. REQUIRED | PREVIOUSLY DEDICATED | REMAINDER | EASEMENTS | | EASEMENT PURPOSE | PERMANENT TAX NUMBER | PROPERTY ACQUIRED BY |
|------------|--|---------------|-------------------------|----------------------|------------|-----------|------|------------------|--|----------------------|
| | | | | | | TEMP | PERM | | | |
| 8238035 | Timothy L. Freer and Carrie A. Freer Title Report MA-2674 | 32.0579 AC | 8.2921 AC 361,204 SF | | 23.7658 AC | | | | 20-1-02-30-00-000-006 20-1-02-30-00-000-006.R00 | C.C. 04-ED-7 |

SCALE 1:500

COORDINATES SHOWN HEREON ARE BASED ON SURVEY CONTROL DATA AS PROVIDED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

PART OF THE NW 1/4 OF SECTION 30, T6N, R9W, OF THE 3RD PM, MADISON COUNTY, ILLINOIS

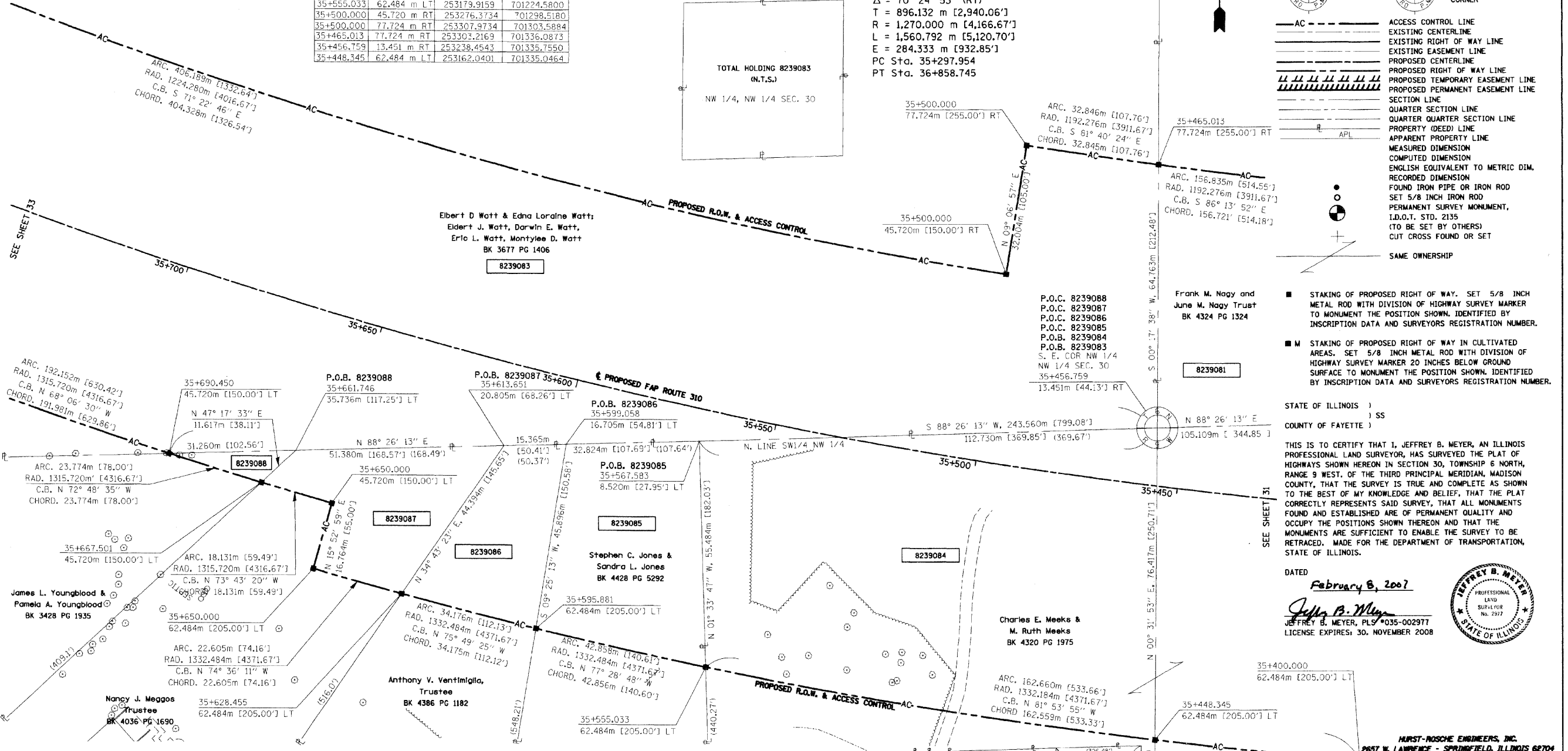
BEARINGS SHOWN HEREON ARE BASED ON SURVEY CONTROL DATA AS PROVIDED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

| FAP ROUTE | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------|---------|---------|--------------|-----------|
| 310 | 60-15 | MADISON | 474 | 92 |

| STATION | OFFSET | NORTH | EAST |
|------------|-------------|-------------|-------------|
| 35+690.450 | 45.720 m LT | 253231.8104 | 701092.2859 |
| 35+667.501 | 45.720 m LT | 253224.7840 | 701114.9981 |
| 35+661.746 | 35.736 m LT | 253232.6631 | 701123.5343 |
| 35+650.000 | 45.720 m LT | 253219.7019 | 701132.4025 |
| 35+650.000 | 62.484 m LT | 253203.5778 | 701127.8146 |
| 35+628.455 | 62.484 m LT | 253197.5762 | 701149.6079 |
| 35+613.651 | 20.805 m LT | 253234.0647 | 701174.8954 |

COORDINATES SHOWN HEREON ARE BASED ON SURVEY CONTROL DATA AS PROVIDED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

| STATION | OFFSET | NORTH | EAST |
|------------|-------------|-------------|-------------|
| 35+599.058 | 16.705 m LT | 253234.4838 | 701190.2547 |
| 35+595.881 | 62.484 m LT | 253189.2064 | 701182.7427 |
| 35+567.583 | 8.520 m LT | 253235.3792 | 701223.0665 |
| 35+555.033 | 62.484 m LT | 253179.9159 | 701224.5800 |
| 35+500.000 | 45.720 m RT | 253276.3734 | 701298.5180 |
| 35+500.000 | 77.724 m RT | 253307.9734 | 701303.5884 |
| 35+465.013 | 77.724 m RT | 253303.2169 | 701336.0873 |
| 35+456.759 | 13.451 m RT | 253238.4543 | 701335.7550 |
| 35+448.345 | 62.484 m LT | 253162.0401 | 701335.0464 |



LEGEND

- AC - ACCESS CONTROL LINE
- EXISTING CENTERLINE
- EXISTING RIGHT OF WAY LINE
- EXISTING EASEMENT LINE
- PROPOSED CENTERLINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED TEMPORARY EASEMENT LINE
- PROPOSED PERMANENT EASEMENT LINE
- SECTION LINE
- QUARTER SECTION LINE
- QUARTER QUARTER SECTION LINE
- PROPERTY (DEED) LINE
- APPARENT PROPERTY LINE
- MEASURED DIMENSION
- COMPUTED DIMENSION
- ENGLISH EQUIVALENT TO METRIC DIM.
- RECORDED DIMENSION
- FOUND IRON PIPE OR IRON ROD
- SET 5/8 INCH IRON ROD
- PERMANENT SURVEY MONUMENT, I.D.O.T. STD. 2135 (TO BE SET BY OTHERS)
- CUT CROSS FOUND OR SET
- SAME OWNERSHIP

■ STAKING OF PROPOSED RIGHT OF WAY. SET 5/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

■ M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. SET 5/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER 20 INCHES BELOW GROUND SURFACE TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

STATE OF ILLINOIS)
) SS
 COUNTY OF FAYETTE)

THIS IS TO CERTIFY THAT I, JEFFREY B. MEYER, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAS SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 30, TOWNSHIP 6 NORTH, RANGE 9 WEST, OF THE THIRD PRINCIPAL MERIDIAN, MADISON COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED
February 8, 2007

Jeffrey B. Meyer
 JEFFREY B. MEYER, PLS #035-002977
 LICENSE EXPIRES: 30, NOVEMBER 2008

JEFFREY B. MEYER
 PROFESSIONAL LAND SURVEYOR
 No. 2917
 STATE OF ILLINOIS

| PARCEL NO. | OWNER | TOTAL HOLDING | R.O.W. REQUIRED | PREVIOUSLY DEDICATED | REMAINDER | EASEMENTS | | EASEMENT PURPOSE | PERMANENT TAX NUMBER | PROPERTY ACQUIRED BY |
|------------|---|--------------------------|-------------------------|----------------------|---------------------------------|-----------|------|------------------|---|----------------------|
| | | | | | | TEMP | PERM | | | |
| 8239083 | A Life Estate in Elbert D. Watt & Edna Loraine Watt; remainder Interest in Elbert J. Watt, Darwin E. Watt, Eric L. Watt, Montylee D. Watt as Tenants in Common Title Report MA-2671 | 42.8404 AC 359,218 SF | 8.2465 AC 359,218 SF | | 33.0139 AC NO. 1,5800 AC SO. | | | | 20-1-02-30-00-000-002 | |
| 8239084 | Charles E. Meeks & M. Ruth Meeks, husband and wife as joint tenants Title Report MA-3884.0 & MA-3951.0 | 11.6546 AC | 4.3688 AC 190,305 SF | | 7.2858 AC | | | | 24-1-02-30-01-101-017 24-1-02-30-01-101-018 20-1-02-30-00-000-005 | |
| 8239085 | Stephen C. Jones & Sandra L. Jones, husband and wife, as joint tenants Title Report MA-3885.0 | 1.9134 AC | 0.4689 AC 20,425 SF | | 1.4445 AC | | | | 24-1-02-30-01-101-016 | |
| 8239086 | Anthony V. Ventimiglia, as Trustee under Self-Declaration of Trust dated the 6th day of August, 1997 Title Report MA-3886.0 | 1.8327 AC | 0.2617 AC 11,400 SF | | 1.5710 AC | | | | 24-1-02-30-01-101-015 | |
| 8239087 | Nancy J. Meggos, as Trustee under Self-Declaration of Trust dated March 30, 1996 Title Report MA-3887.0 | 1.8219 AC | 0.2999 AC 13,064 SF | | 1.5220 AC | | | | 24-1-02-30-01-101-014 | |

HURST-ROSCH ENGINEERS, INC.
 2857 W. LAWRENCE - SPRINGFIELD, ILLINOIS 62704
 217-781-1559

ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS
 FAP ROUTE 310 (IL 255)
 SECTION 60-15
 MADISON COUNTY
 JOB NO. R-98-039-92

STATION 35+400 TO STATION 35+700

0 m 10 m 20 m 30 m
 SCALE 1:500

SHEET 32 OF 38

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS/DISTRICT B
 1102 EASTPORT PLAZA DRIVE
 COLLINGSVILLE, ILLINOIS 62234-6198

SHEET 1 OF 1 IS A COVER SHEET AND IS NOT RECORDED

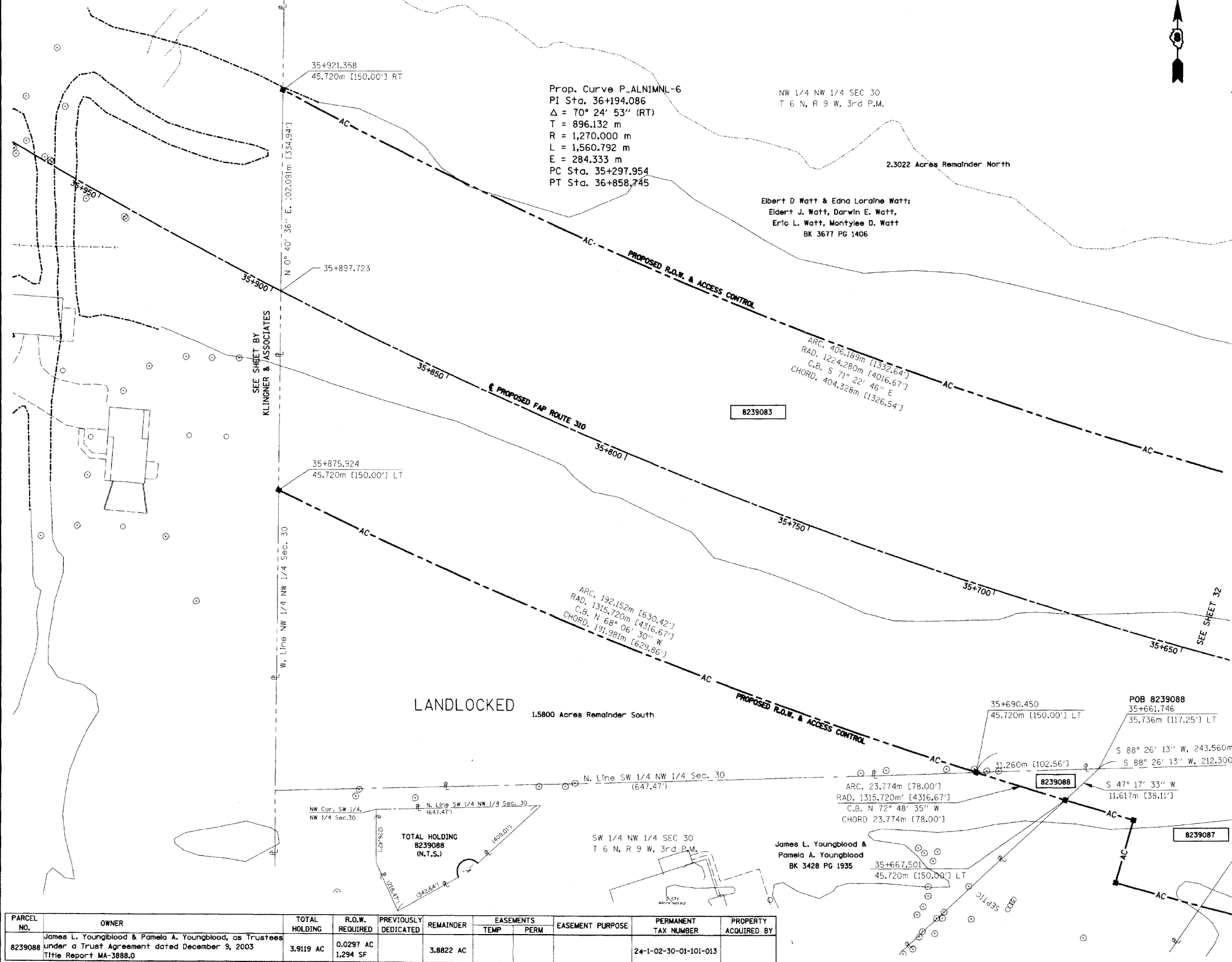
COORDINATES SHOWN HEREON ARE BASED ON SURVEY CONTROL DATA AS PROVIDED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

PART OF THE NW 1/4 OF SECTION 30, T7N, R9W, OF THE 3RD PM, MADISON COUNTY, ILLINOIS

BEARINGS SHOWN HEREON ARE BASED ON SURVEY CONTROL DATA AS PROVIDED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

| FAP ROUTE | SECTION | COUNTY | TOTAL SHEET NO. |
|-----------|---------|---------|-----------------|
| 310 | 60-14 | MADISON | 474 93 |

| COORDINATE TABLE | | | |
|------------------|-------------|-------------|-------------|
| STATION | OFFSET | NORTH | EAST |
| 35+921.358 | 45.720 m RT | 253405.4752 | 700915.3544 |
| 35+875.924 | 45.720 m LT | 253303.3912 | 700914.1487 |



LEGEND

SECTION CORNER: [Symbol]

QUARTER SECTION CORNER: [Symbol]

AC - ACCESS CONTROL LINE

--- EXISTING CENTERLINE

--- EXISTING RIGHT OF WAY LINE

--- EXISTING EASEMENT LINE

--- PROPOSED CENTERLINE

--- PROPOSED RIGHT OF WAY LINE

--- PROPOSED TEMPORARY EASEMENT LINE

--- PROPOSED PERMANENT EASEMENT LINE

--- SECTION LINE

--- QUARTER SECTION LINE

--- QUARTER QUARTER SECTION LINE

--- PROPERTY (DEED) LINE

--- APPARENT PROPERTY LINE

--- MEASURED DIMENSION

--- COMPUTED DIMENSION

--- ENGLISH EQUIVALENT TO METRIC DIM.

--- RECORDED DIMENSION

--- FOUND IRON PIPE OR IRON ROD

--- SET 5/8 INCH IRON ROD

--- PERMANENT SURVEY MONUMENT, I.D.O.T. STD. 2135 (TO BE SET BY OTHERS)

--- CUT CROSS FOUND OR SET

--- SAME OWNERSHIP

--- STAKING OF PROPOSED RIGHT OF WAY. SET 5/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

--- STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. SET 5/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER 20 INCHES BELOW GROUND SURFACE TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

STATE OF ILLINOIS)
) SS
 COUNTY OF FAYETTE)

THIS IS TO CERTIFY THAT I, JEFFREY B. MEYER, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAS SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 30, TOWNSHIP 6 NORTH, RANGE 9 WEST, OF THE THIRD PRINCIPAL MERIDIAN, MADISON COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED February 8, 2007

Jeffrey B. Meyer
 JEFFREY B. MEYER, PLS #035-002977
 LICENSE EXPIRES: 30. NOVEMBER 2008

JEFFREY B. MEYER
 PROFESSIONAL LAND SURVEYOR
 No. 2977
 STATE OF ILLINOIS

| PARCEL NO. | OWNER | TOTAL HOLDING | R.O.W. REQUIRED | PREVIOUSLY DEDICATED | REMAINDER | EASEMENTS | | EASEMENT PURPOSE | PERMANENT TAX NUMBER | PROPERTY ACQUIRED BY |
|------------|--|---------------|-----------------------|----------------------|-----------|-----------|------|------------------|-----------------------|----------------------|
| | | | | | | TEMP | PERM | | | |
| 8239088 | James L. Youngblood & Pamela A. Youngblood, as Trustees Under a Trust Agreement dated December 9, 2003 Title Report MA-3888.0 | 3.9119 AC | 0.0297 AC 1,294 SF | | 3.8822 AC | | | | 24-1-02-30-01-101-013 | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS
 FAP ROUTE 310 (IL 255)
 SECTION 60-14
 MADISON COUNTY
 JOB NO. R-98-039-92
 STATION 35+700 TO STATION 35+921.358

SCALE 1:500 SHEET 33 OF 38

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS/DISTRICT 8
 1102 EASTPORT PLAZA DRIVE
 COLLINGSVILLE, ILLINOIS 62234-6198

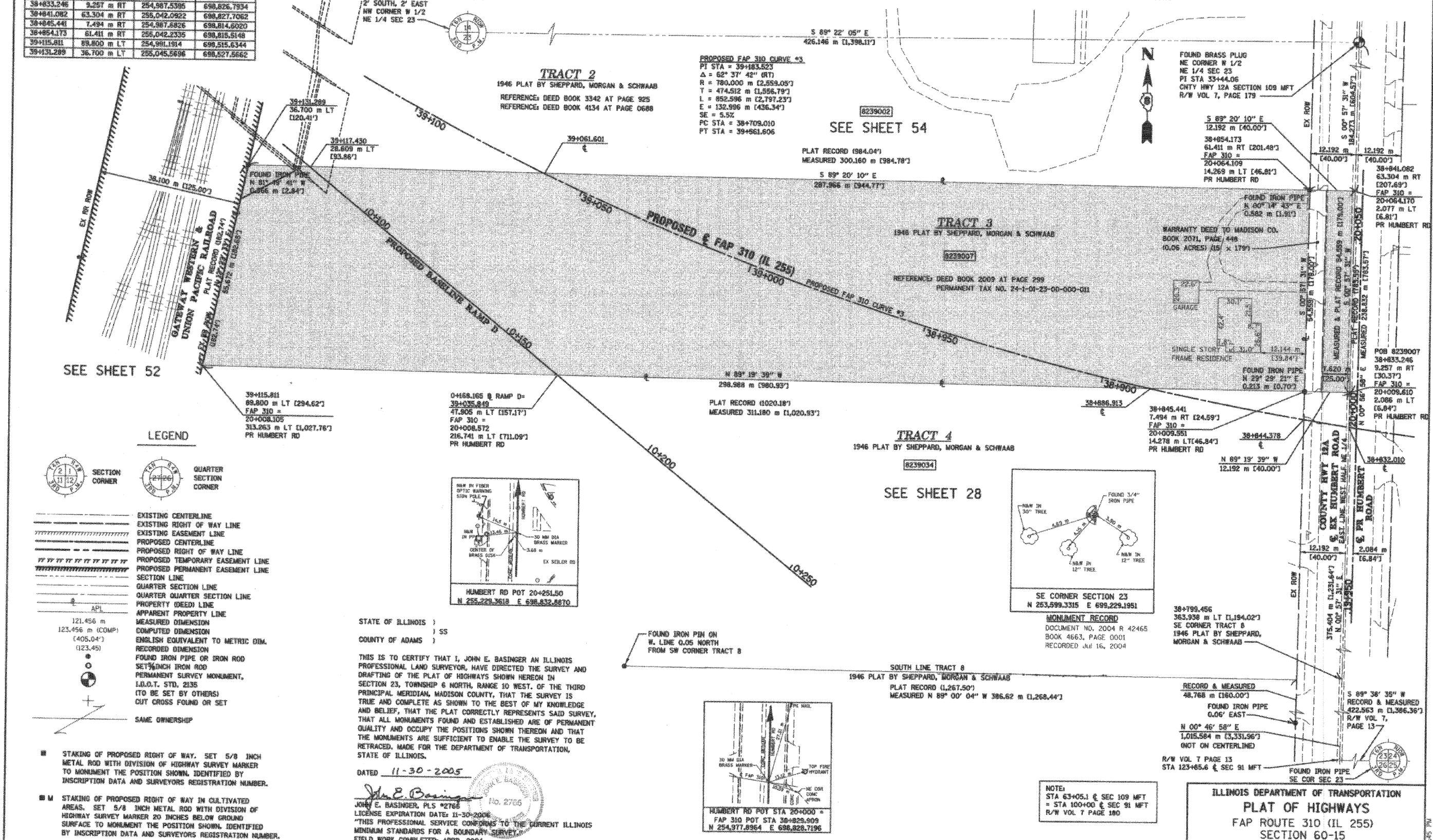
IS A COVER SHEET AND IS NOT RECORDED

COORDINATES SHOWN HEREON ARE BASED ON SURVEY CONTROL DATA AS PROVIDED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

| COORDINATE TABLE | | | |
|------------------|-------------|--------------|--------------|
| STATION | OFFSET | NORTH | EAST |
| 38+833.246 | 9.257 m RT | 254,987.5395 | 698,826.7934 |
| 38+841.082 | 63.304 m RT | 255,042.0922 | 698,827.7062 |
| 38+845.448 | 7.494 m RT | 254,987.6826 | 698,814.6020 |
| 38+854.173 | 61.411 m RT | 255,042.2335 | 698,815.5148 |
| 39+115.811 | 89.800 m LT | 254,981.1914 | 698,535.6344 |
| 39+131.289 | 36.700 m LT | 255,045.5696 | 698,527.8662 |

PART OF THE NE 1/4 OF SECTION 23, T6N, R10W, OF THE 3RD PM, MADISON COUNTY, ILLINOIS

BEARINGS SHOWN HEREON ARE BASED ON SURVEY CONTROL DATA AS PROVIDED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION.



LEGEND

- SECTION CORNER
- QUARTER SECTION CORNER
- EXISTING CENTERLINE
- EXISTING RIGHT OF WAY LINE
- EXISTING EASEMENT LINE
- PROPOSED CENTERLINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED TEMPORARY EASEMENT LINE
- PROPOSED PERMANENT EASEMENT LINE
- SECTION LINE
- QUARTER SECTION LINE
- QUARTER QUARTER SECTION LINE
- PROPERTY (DEED) LINE
- APPARENT PROPERTY LINE
- MEASURED DIMENSION
- COMPUTED DIMENSION
- ENGLISH EQUIVALENT TO METRIC DIM.
- RECORDED DIMENSION
- FOUND IRON PIPE OR IRON ROD
- SET 1/4 INCH IRON ROD
- PERMANENT SURVEY MONUMENT, I.B.O.T. STD. 2135 (TO BE SET BY OTHERS)
- CUT CROSS FOUND OR SET
- SAME OWNERSHIP

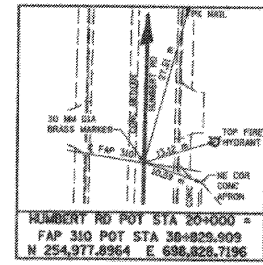
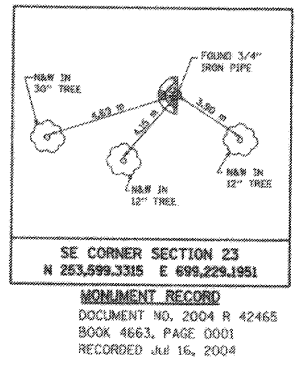
STATE OF ILLINOIS)
 COUNTY OF ADAMS) SS

THIS IS TO CERTIFY THAT I, JOHN E. BASINGER AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE DIRECTED THE SURVEY AND DRAFTING OF THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 23, TOWNSHIP 6 NORTH, RANGE 10 WEST, OF THE THIRD PRINCIPAL MERIDIAN, MADISON COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED 11-30-2005

John E. Basinger, PLS #2766
 LICENSE EXPIRATION DATE: 11-30-2006
 THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY. FIELD WORK COMPLETED APRIL 2004.

| PARCEL NO. | OWNER | TOTAL HOLDING ACRES | PREVIOUSLY DEDICATED | | NET | | REMAINDER ACRES | EASEMENTS | | PERMANENT TAX NUMBER | PROPERTY ACQUIRED BY |
|------------|--|---------------------|----------------------|---------|-------------|---------|-----------------|------------------|------------------|-----------------------|----------------------|
| | | | GROSS ACRES | SQ. FT. | GROSS ACRES | SQ. FT. | | PERM = PERMANENT | TEMP = TEMPORARY | | |
| 8239007 | WILBUR HAMILTON and JACQUELINE HAMILTON, HUSBAND AND WIFE, AS JOINT TENANTS TITLE REPORT NO. MA-2625 | 4.0576 | 4.0576 | | 0.1027 | 3.9549 | 0.0000 | | | 24-1-01-23-00-000-011 | |



NOTE:
 STA 43+05.1 & SEC 109 MFT
 = STA 100+00 & SEC 91 MFT
 R/W VOL 7 PAGE 180

ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS
 FAP ROUTE 310 (IL 255)
 SECTION 60-15
 MADISON COUNTY
 JOB NO. R-98-039-92

STATION 38+886.913 TO STATION 39+061.601

0 m 10 m 20 m 30 m
 SCALE 1:500

SHEET 2 OF 55

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS/DISTRICT II
 1102 EASTPORT PLAZA DRIVE
 COLLINGSVILLE, ILLINOIS 62254-6188

KLINGNER
 & ASSOCIATES, P.C.

Engineers / Architects
 616 North 24th Street Quincy, Illinois 62301
 616 North 24th Street Quincy, Illinois 62301
 Internet Address: www.klingner.com
 STATE OF ILLINOIS DESIGN FIRM # 1842738

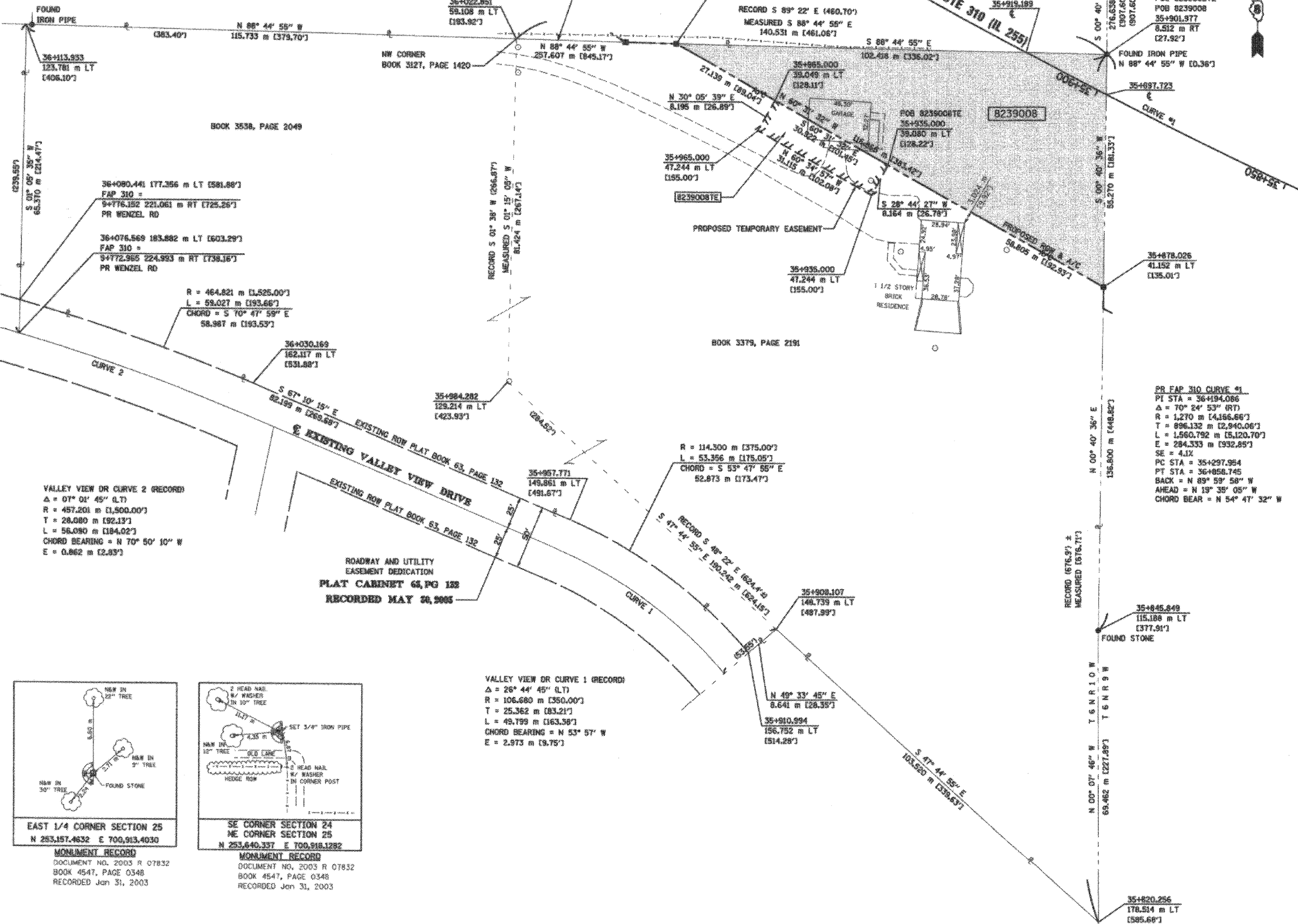
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 11/28/2005 3:07:45 PM
 11/28/2005 3:07:45 PM

COORDINATES SHOWN HEREON ARE BASED ON SURVEY CONTROL DATA AS PROVIDED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

PART OF THE NE 1/4 OF SECTION 25, T6N, R10W, OF THE 3RD PM, MADISON COUNTY, ILLINOIS

BEARINGS SHOWN HEREON ARE BASED ON SURVEY CONTROL DATA AS PROVIDED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

| COORDINATE TABLE | | | |
|------------------|-------------|--------------|--------------|
| STATION | OFFSET | NORTH | EAST |
| 35+878.026 | 41.152 m LT | 253,308.4491 | 700,914.2097 |
| 35+897.723 | 0.000 | 253,354.2086 | 700,914.7491 |
| 35+901.977 | 8.512 m RT | 253,363.7148 | 700,914.8614 |
| 35+919.189 | 0.000 | 253,364.133 | 700,895.7157 |
| 35+931.318 | 39.624 m LT | 253,365.9514 | 700,812.468 |



LEGEND

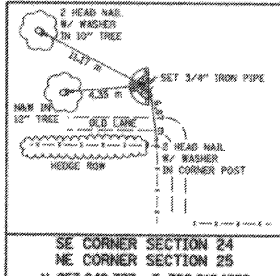
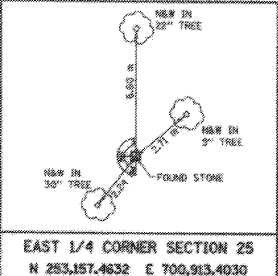
- EXISTING CENTERLINE
- EXISTING RIGHT OF WAY LINE
- EXISTING EASEMENT LINE
- PROPOSED CENTERLINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED TEMPORARY LINE
- PROPOSED TEMPORARY EASEMENT LINE
- PROPOSED PERMANENT EASEMENT LINE
- SECTION LINE
- QUARTER SECTION LINE
- QUARTER QUARTER SECTION LINE
- PROPERTY (DEED) LINE
- APPARENT PROPERTY LINE
- MEASURED DIMENSION
- COMPUTED DIMENSION
- ENGLISH EQUIVALENT TO METRIC DIM.
- RECORDED DIMENSION
- FOUND IRON PIPE OR IRON ROD
- SET 5/8 INCH IRON ROD
- PERMANENT SURVEY MONUMENT, I.D.O.T. STD. 2135 (TO BE SET BY OTHERS)
- CUT CROSS FOUND OR SET
- SAME OWNERSHIP

■ STAKING OF PROPOSED RIGHT OF WAY. SET 5/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
 ■ M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. SET 5/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER 20 INCHES BELOW GROUND SURFACE TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

STATE OF ILLINOIS)
 COUNTY OF ADAMS)

VALLEY VIEW DR CURVE 2 (RECORD)
 Δ = 07° 01' 45" (LT)
 R = 457.201 m [1,500.00']
 T = 28.080 m [92.13']
 L = 56.080 m [184.02']
 CHORD BEARING = N 70° 50' 10" W
 E = 0.862 m [2.83']

VALLEY VIEW DR CURVE 1 (RECORD)
 Δ = 26° 44' 45" (LT)
 R = 106.680 m [350.00']
 T = 25.362 m [83.21']
 L = 49.799 m [163.38']
 CHORD BEARING = N 53° 57' W
 E = 2.973 m [9.75']



MONUMENT RECORD
 DOCUMENT NO. 2003 R 07832
 BOOK 4547, PAGE 0348
 RECORDED Jan 31, 2003

MONUMENT RECORD
 DOCUMENT NO. 2003 R 07832
 BOOK 4547, PAGE 0348
 RECORDED Jan 31, 2003

| PARCEL NO. | OWNER | TOTAL HOLDING ACRES | FEE SIMPLE ACQUISITION | | | | EASEMENTS | | | | PERMANENT TAX NUMBER | PROPERTY ACQUIRED BY | |
|------------|--|---------------------|------------------------|----------------------------|-----------|-----------------|----------------------|----------------------|------------------|-----------|----------------------|----------------------|--|
| | | | GROSS ACRES | PREVIOUSLY DEDICATED ACRES | NET ACRES | REMAINDER ACRES | PE = PERMANENT ACRES | TE = TEMPORARY ACRES | EASEMENT PURPOSE | | | | |
| 8239008 | MICHAEL P. HILGENDORF AND TERRI R. HILGENDORF, HUSBAND AND WIFE, AS JOINT TENANTS TITLE REPORT NO. MA-2664, MA-2677 | 7.6989 | 0.6993 | 30,463 | N/A | N/A | 0.6993 | 30,463 | 6.9996 | TE 0.0627 | 2,731 | BLDG REMOVAL | 24-1-01-25-00-000-009.002 24-1-01-25-02-201-005.011 |

ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS
 FAP ROUTE 310 (IL 255)
 SECTION 60-15
 MADISON COUNTY
 JOB NO. R-98-039-92
 STATION 35+878.026 TO STATION 35+931.318

0 m 10 m 20 m 30 m
 SCALE 1:500

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS/DISTRICT B
 1302 EASTPORT PLAZA DRIVE
 COLLINSVILLE, ILLINOIS 62234-6198

KLINGNER ASSOCIATES, P.C.
 Engineers / Architects
 616 North 24th Street (217) 223-3670
 Quincy, Illinois 62501 FAX: 223-3603
 Internet Address: www.klingner.com
 STATE OF ILLINOIS DESIGN FIRM # 1842738

THIS IS TO CERTIFY THAT I, JOHN E. BASINGER AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE DIRECTED THE SURVEY AND DRAFTING OF THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 25, TOWNSHIP 6 NORTH, RANGE 10 WEST, OF THE THIRD PRINCIPAL MERIDIAN, MADISON COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED 11-30-2005

John E. Basinger
 JOHN E. BASINGER, PLS #2766
 LICENSE EXPIRATION DATE: 11-30-2008
 *THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.
 FIELD WORK COMPLETED: APRIL 2004

11/28/2005 3:07:54 PM
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COORDINATES SHOWN HEREON ARE BASED ON SURVEY CONTROL DATA AS PROVIDED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

PART OF THE NE 1/4 OF SECTION 25, T6N, R10W, OF THE 3RD PM, MADISON COUNTY, ILLINOIS

BEARINGS SHOWN HEREON ARE BASED ON SURVEY CONTROL DATA AS PROVIDED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

COORDINATE TABLE (FAP 310 - IL 255)

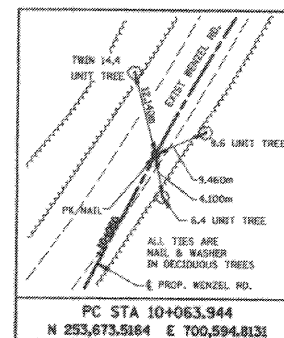
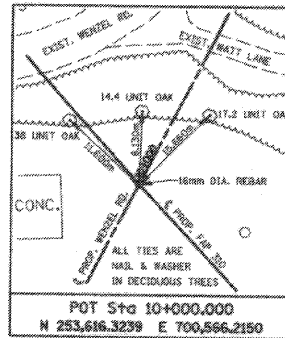
| STATION | OFFSET | NORTH | EAST |
|------------|-------------|--------------|--------------|
| 35+901.977 | 8.812 m RT | 253,363.7148 | 700,914.0614 |
| 35+921.358 | 45.720 m RT | 253,405.4752 | 700,915.3546 |
| 35+928.003 | 57.912 m RT | 253,419.2316 | 700,915.9170 |
| 35+975.990 | 57.912 m RT | 253,441.3075 | 700,976.4744 |
| 36+001.428 | 45.720 m LT | 253,366.2156 | 700,800.3733 |
| 36+025.000 | 45.720 m RT | 253,466.1143 | 700,829.2699 |
| 36+050.000 | 45.720 m RT | 253,509.1084 | 700,858.2786 |
| 36+083.357 | 77.331 m RT | 253,633.8886 | 700,857.1707 |
| 36+094.847 | 45.720 m LT | 253,594.4331 | 700,562.5301 |
| 36+134.791 | 45.720 m RT | 253,629.8142 | 700,813.8669 |
| 36+199.633 | 42.670 m LT | 253,629.9248 | 700,496.6796 |

COORDINATE TABLE (PR WENZEL RD)

| STATION | OFFSET | NORTH | EAST |
|-----------|--------------|--------------|--------------|
| 9+805.182 | 17.947 m RT | 253,644.0494 | 700,496.1370 |
| 9+806.065 | 19.8120 m RT | 253,644.0494 | 700,497.2003 |
| 9+820.000 | 19.8120 m RT | 253,646.4684 | 700,503.4327 |
| 9+840.000 | 24.3940 m RT | 253,662.3199 | 700,516.4667 |

SEE SHEET 5, 6, 10

SEE SHEET 5, 6



NOTE: ROAD RIGHT OF WAY ORIGINALLY CREATED AS DESCRIBED IN ROAD RECORD BOOK 3, PAGE 126 AND 132 VARIES FROM RIGHT OF WAY PRESENTLY OCCUPIED.

EXISTING RIGHT OF WAY IS SHOWN AS OCCUPIED RIGHT OF WAY.

NO RECORDS OR DOCUMENTS WHICH EXPLAIN VARIANCES BETWEEN ROAD RECORD BOOK 3, PAGE 132 RIGHT-OF-WAY AND OCCUPIED RIGHT OF WAY HAVE BEEN FOUND.



LEGEND



- EXISTING CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED ACCESS CONTROL LINE
- EXISTING EASEMENT LINE
- PROPOSED CENTERLINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY AND PROPOSED ACCESS CONTROL LINE
- PROPOSED TEMPORARY EASEMENT LINE
- PROPOSED PERMANENT EASEMENT LINE
- SECTION LINE
- QUARTER SECTION LINE
- QUARTER QUARTER SECTION LINE
- PROPERTY (DEED) LINE
- APPARENT PROPERTY LINE
- MEASURED DIMENSION
- COMPUTED DIMENSION
- ENGLISH EQUIVALENT TO METRIC DIM.
- RECORDED DIMENSION
- FOUND IRON PIPE OR IRON ROD
- SET 5/8 INCH IRON ROD
- PERMANENT SURVEY MONUMENT, I.D.O.T. STD. 2135 (TO BE SET BY OTHERS)
- CUT CROSS FOUND OR SET
- SAME OWNERSHIP

- STAKING OF PROPOSED RIGHT OF WAY. SET 5/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. SET 5/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER 20 INCHES BELOW GROUND SURFACE TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

STATE OF ILLINOIS)
COUNTY OF ADAMS)

THIS IS TO CERTIFY THAT I, JOHN E. BASINGER AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE DIRECTED THE SURVEY AND DRAFTING OF THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 25, TOWNSHIP 6 NORTH, RANGE 10 WEST, OF THE THIRD PRINCIPAL MERIDIAN, MADISON COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED 11-30-2005

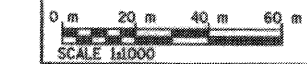
John E. Basinger
JOHN E. BASINGER, PLS #2766
LICENSE EXPIRATION DATE: 11-30-2006
"THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY."
FIELD WORK COMPLETED: APRIL 2004



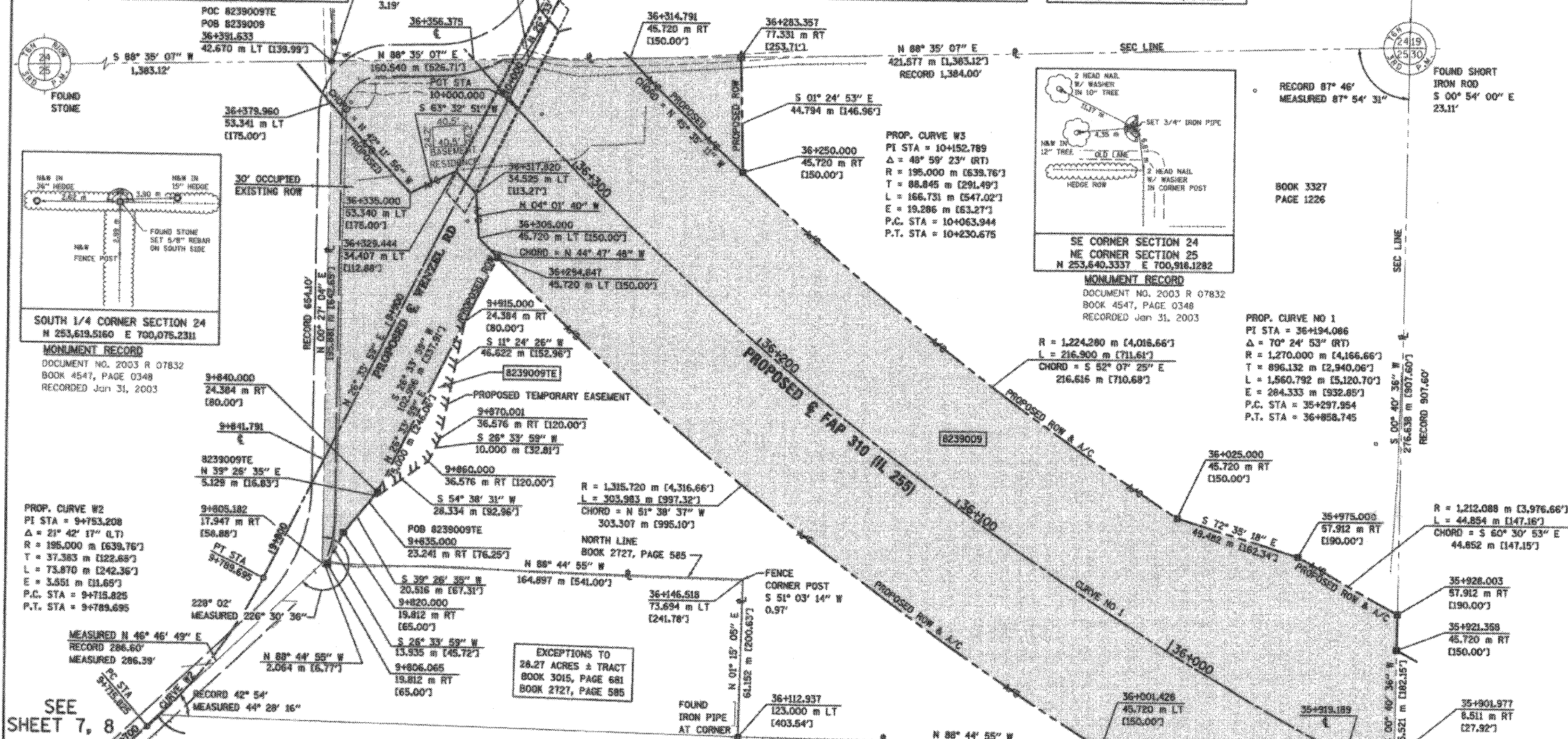
Engineers / Architects
616 North 24th Street (217) 223-3570
Quincy, Illinois 62301 FAX: 223-3603
Internet Address: www.klingner.com
STATE OF ILLINOIS DESIGN FIRM # 1842738

ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS
FAP ROUTE 310 (IL 255)
SECTION 60-15
MADISON COUNTY
JOB NO. R-98-039-92

STATION 35+919.189 TO STATION 36+356.375

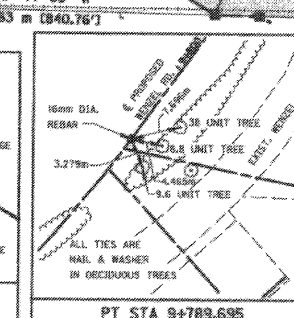
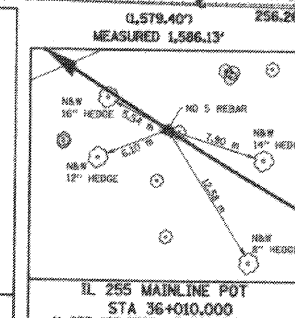
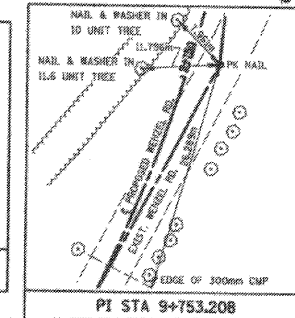
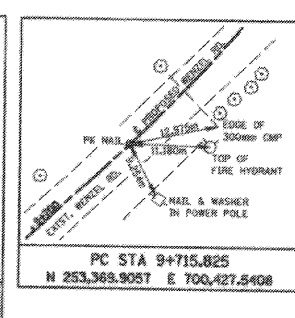
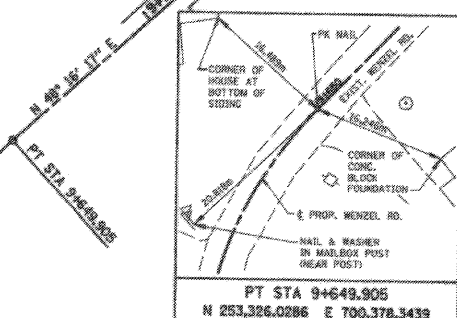


ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS/DISTRICT 8
1102 EASTPORT PLAZA DRIVE
COLLINGSVILLE, ILLINOIS 62234-8198



SEE SHEET 7, 8

SEE SHEET 3



| PARCEL NO. | OWNER | TOTAL HOLDINGS ACRES | FEE SIMPLE ACQUISITION | | | REMAINDER | | EASEMENTS | | PERMANENT TAX NUMBER | PROPERTY ACQUIRED BY | | | |
|------------|--|----------------------|------------------------|----------------------------|-----------|-----------------------------|-----------------------------|-------------------------------------|------------------|----------------------|----------------------|-------|----------------|---------------------------|
| | | | GROSS ACRES | PREVIOUSLY DEDICATED ACRES | NET ACRES | NORTH OF PROPOSED ROW ACRES | SOUTH OF PROPOSED ROW ACRES | PE - PERMANENT TE = TEMPORARY ACRES | EASEMENT PURPOSE | | | | | |
| 8239009 | EUGENE BOCK AND DOROTHY BOCK, AS JOINT TENANTS TITLE REPORT NO. MA-2670 | 25.2610 | 11.5705 | 504.011 | 0.3071 | 13.378 | 11.2634 | 490.633 | 9.2804 | 4.4101 | TE 0.1327 | 5,782 | FIELD ENTRANCE | 24-1-01-25-00-000-007.R00 |

11/30/2005 3:08:44 PM
D:\WORK\1100\110025\110025.DWG
PLS: JEB
REF: JEB
APP: JEB

PLOT DATE: 11/30/2005 3:08:44 PM

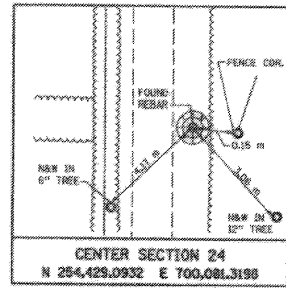
PART OF THE S 1/2 OF SECTION 24, T6N, R10W, OF THE 3RD PM, MADISON COUNTY, ILLINOIS

BEARINGS SHOWN HEREON ARE BASED ON SURVEY CONTROL DATA AS PROVIDED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

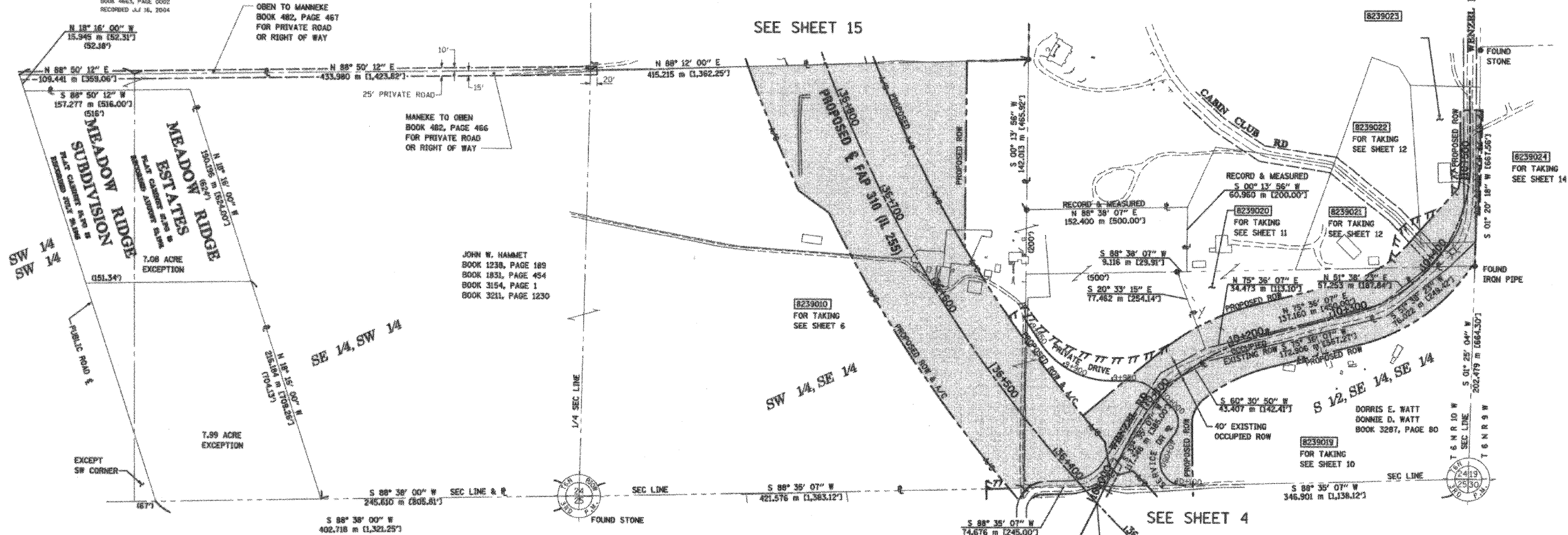
NOTE: ROAD RIGHT OF WAY ORIGINALLY CREATED AS DESCRIBED IN ROAD RECORD BOOK 3, PAGE 125 AND 132 VARIES FROM RIGHT OF WAY PRESENTLY OCCUPIED.

EXISTING RIGHT OF WAY IS SHOWN AS OCCUPIED RIGHT OF WAY.

NO RECORDS OR DOCUMENTS WHICH EXPLAIN VARIANCES BETWEEN ROAD RECORD BOOK 3, PAGE 132 RIGHT-OF-WAY AND OCCUPIED RIGHT OF WAY HAVE BEEN FOUND.



CENTER SECTION 24 N 254,429.0932 E 700,081,3198 MONUMENT RECORD DOCUMENT NO. 2004 R 42466



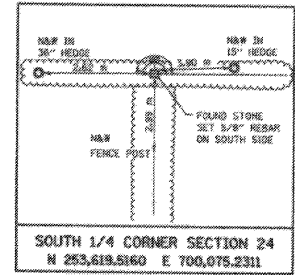
MEADOW RIDGE SUBDIVISION 7.08 ACRE EXCEPTION

MEADOW RIDGE ESTATES 7.08 ACRE EXCEPTION

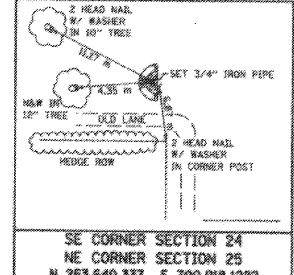
JOHN W. HAMMET BOOK 1238, PAGE 189 BOOK 1831, PAGE 454 BOOK 3194, PAGE 1 BOOK 3211, PAGE 1230

LEGEND

- SECTION CORNER, QUARTER SECTION CORNER, EXISTING CENTERLINE, EXISTING RIGHT OF WAY LINE, PROPOSED ACCESS CONTROL LINE, EXISTING EASEMENT LINE, PROPOSED CENTERLINE, PROPOSED RIGHT OF WAY LINE, PROPOSED RIGHT OF WAY AND PROPOSED ACCESS CONTROL LINE, PROPOSED TEMPORARY EASEMENT LINE, PROPOSED PERMANENT EASEMENT LINE, SECTION LINE, QUARTER SECTION LINE, QUARTER QUARTER SECTION LINE, PROPERTY (DEED) LINE, APPARENT PROPERTY LINE, MEASURED DIMENSION, COMPUTED DIMENSION, ENGLISH EQUIVALENT TO METRIC DIM., RECORDED DIMENSION, FOUND IRON PIPE OR IRON ROD, SET 5/8 INCH IRON ROD, PERMANENT SURVEY MONUMENT, I.D.O.T. STD. 2135, (TO BE SET BY OTHERS), CUT CROSS FOUND OR SET, SAME OWNERSHIP



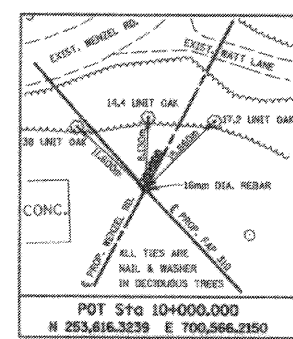
SOUTH 1/4 CORNER SECTION 24 N 253,619,5160 E 700,075,2311 MONUMENT RECORD DOCUMENT NO. 2003 R 07832



SE CORNER SECTION 24 NE CORNER SECTION 25 N 253,640,337 E 700,918,1282 MONUMENT RECORD DOCUMENT NO. 2003 R 07832

- STAKING OF PROPOSED RIGHT OF WAY. SET 5/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER. STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. SET 5/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER 20 INCHES BELOW GROUND SURFACE TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

NOTE: NO RECORDS OR DOCUMENTS WHICH EXPLAIN VARIANCES BETWEEN ROAD RECORD BOOK 3, PAGE 132 RIGHT-OF-WAY AND OCCUPIED RIGHT OF WAY HAVE BEEN FOUND.



POT Sta 10+000.000 N 253,616,3239 E 700,566,2150

STATE OF ILLINOIS) COUNTY OF ADAMS)

THIS IS TO CERTIFY THAT I, JOHN E. BASINGER AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE DIRECTED THE SURVEY AND DRAFTING OF THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 24, TOWNSHIP 6 NORTH, RANGE 10 WEST, OF THE THIRD PRINCIPAL MERIDIAN, MADISON COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED 11-30-2005

John E. Basinger, Professional Land Surveyor, No. 2766, License Expiration Date: 11-30-2006



"THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY." FIELD WORK COMPLETED: APRIL 2004



Engineers / Architects 616 North 24th Street (217) 223-3670 O'Leary, Illinois 62301 FAX: 223-3603 Internet Address: www.klingner.com STATE OF ILLINOIS DESIGN FIRM # 1842736

ILLINOIS DEPARTMENT OF TRANSPORTATION

PLAT OF HIGHWAYS FAP ROUTE 310 (IL 255) SECTION 60-15 MADISON COUNTY JOB NO. R-98-039-92

STATION 36+356.375 TO STATION 36+826.844



ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS/DISTRICT 8 1102 EASTPORT PLAZA DRIVE COLLINGSVILLE, ILLINOIS 62234-6196

11/30/2005 11:30:27 AM 3/8/2004 11:30:27 AM 4/27/2004 11:30:27 AM 4/27/2004 11:30:27 AM

COORDINATES SHOWN HEREON ARE BASED ON SURVEY CONTROL DATA AS PROVIDED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

PART OF THE NE 1/4 OF SECTION 25, T6N, R10W, OF THE 3RD PM, MADISON COUNTY, ILLINOIS

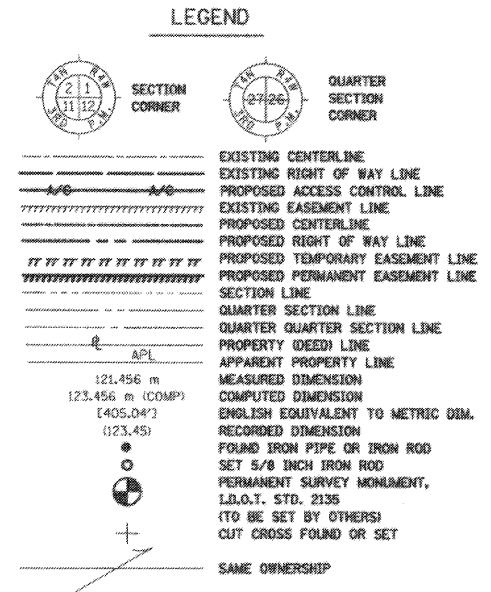
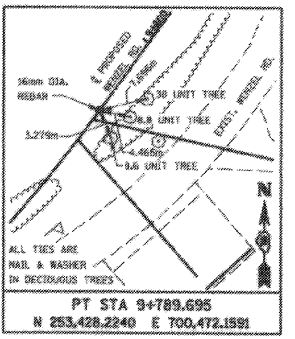
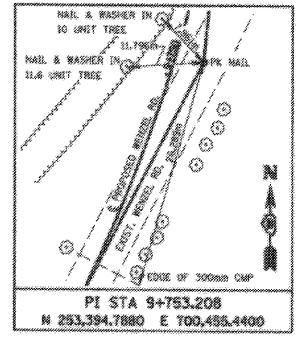
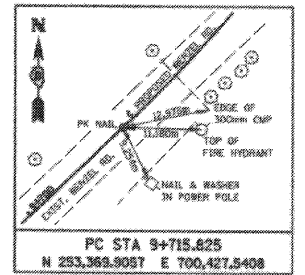
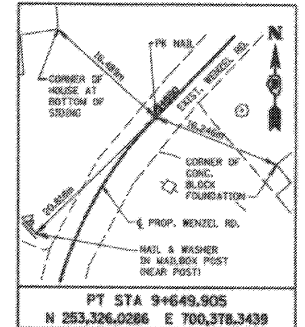
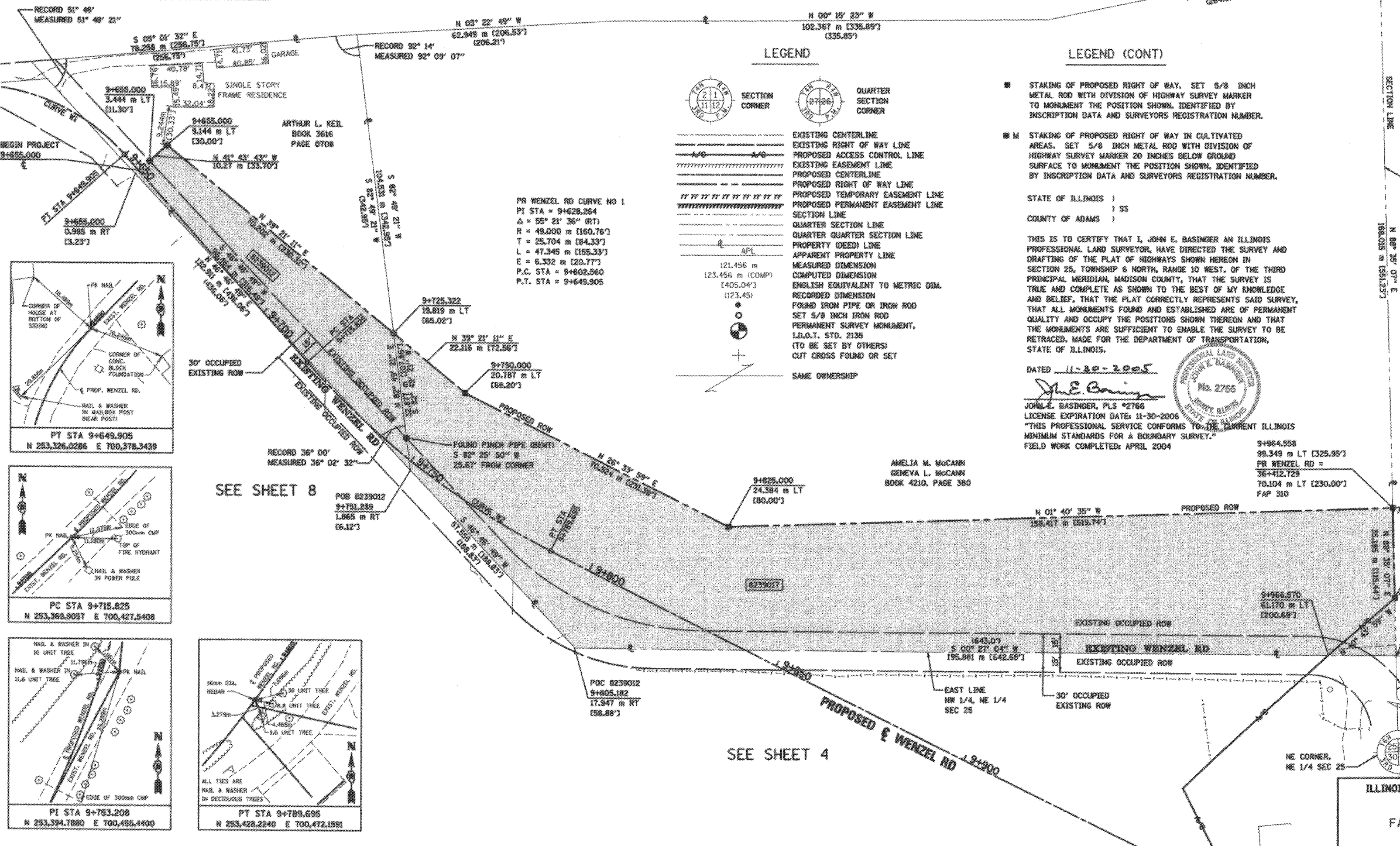
BEARINGS SHOWN HEREON ARE BASED ON SURVEY CONTROL DATA AS PROVIDED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

| STATION | OFFSET | NORTH | EAST |
|-----------|-------------|--------------|--------------|
| 9+655.000 | 1.129 m RT | 253,328.5776 | 700,382.8979 |
| 9+655.000 | 9.144 m LT | 253,336.2443 | 700,376.0603 |
| 9+655.000 | 3.444 m LT | 253,331.9305 | 700,379.8541 |
| 9+750.000 | 20.787 m LT | 253,407.6287 | 700,434.6982 |
| 9+805.182 | 17.947 m RT | 253,434.0494 | 700,495.1370 |
| 9+825.000 | 24.384 m LT | 253,470.7065 | 700,466.1392 |
| 9+964.558 | 99.349 m LT | 253,629.0561 | 700,461.8990 |
| 9+974.619 | 80.411 m LT | 253,629.5855 | 700,482.9408 |
| 9+981.066 | 68.276 m LT | 253,629.9246 | 700,496.6798 |

PROP. CURVE W1
 PI STA = 9+628.264
 $\Delta = 55^\circ 21' 36''$ (RT)
 R = 49.000 m (160.76')
 T = 25.704 m (84.33')
 L = 47.345 m (155.33')
 E = 6.332 m (20.77')
 P.C. STA = 9+602.560
 P.T. STA = 9+649.905

PROP. CURVE W2
 PI STA = 9+753.208
 $\Delta = 21^\circ 42' 17''$ (L)
 R = 195.000 m (639.76')
 T = 37.383 m (122.65')
 L = 73.870 m (242.36')
 E = 3.851 m (12.65')
 P.C. STA = 9+715.825
 P.T. STA = 9+789.695

NOTE:
 ROAD RIGHT OF WAY ORIGINALLY CREATED AS DESCRIBED IN ROAD RECORD BOOK 3, PAGE 126 AND 132 VARIES FROM RIGHT OF WAY PRESENTLY OCCUPIED.
 EXISTING RIGHT OF WAY IS SHOWN AS OCCUPIED RIGHT OF WAY.
 NO RECORDS OR DOCUMENTS WHICH EXPLAIN VARIANCES BETWEEN ROAD RECORD BOOK 3, PAGE 132 RIGHT-OF-WAY AND OCCUPIED RIGHT OF WAY HAVE BEEN FOUND.



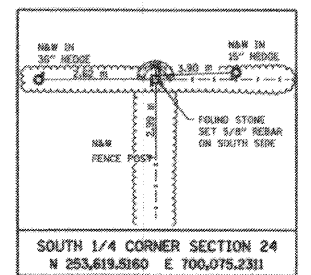
LEGEND (CONT)

- STAKING OF PROPOSED RIGHT OF WAY, SET 5/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS, SET 5/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER 20 INCHES BELOW GROUND SURFACE TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

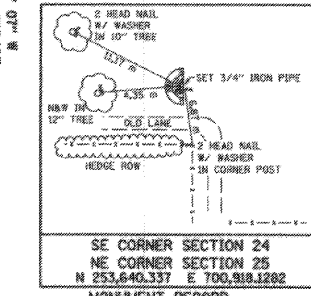
STATE OF ILLINOIS)
 COUNTY OF ADAMS) SS

THIS IS TO CERTIFY THAT I, JOHN E. BASINGER AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE DIRECTED THE SURVEY AND DRAFTING OF THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 25, TOWNSHIP 6 NORTH, RANGE 10 WEST, OF THE THIRD PRINCIPAL MERIDIAN, MADISON COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED 11-30-2005
 John E. Basinger
 JOHN E. BASINGER, PLS #2766
 LICENSE EXPIRATION DATE: 11-30-2006
 "THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY."
 FIELD WORK COMPLETED: APRIL 2004



SOUTH 1/4 CORNER SECTION 24
 N 253,619.5160 E 700,075.2311
 MONUMENT RECORD
 DOCUMENT NO. 2003 R 07832
 BOOK 4547, PAGE 0348
 RECORDED Jan 31, 2003



SE CORNER SECTION 24
 SE CORNER SECTION 25
 N 253,640.337 E 700,918.1282
 MONUMENT RECORD
 DOCUMENT NO. 2003 R 07832
 BOOK 4547, PAGE 0348
 RECORDED Jan 31, 2003

| PARCEL NO. | OWNER | TOTAL HOLDING ACRES | FEE SIMPLE ACQUISITION | | | | EASEMENTS | | | | PERMANENT TAX NUMBER | PROPERTY ACQUIRED BY | |
|------------|---|---------------------|------------------------|---------|----------------------------|-----------|-----------------|----------------------|----------------------|------------------|----------------------|-----------------------|--|
| | | | GROSS ACRES | SQ. FT. | PREVIOUSLY DEDICATED ACRES | NET ACRES | REMAINDER ACRES | PE = PERMANENT ACRES | TE = TEMPORARY ACRES | EASEMENT PURPOSE | | | |
| 8239012 | ARTHUR L. KEIL, A SINGLE PERSON TITLE REPORT NO. MA-2676 | 1.0100 | 0.3186 | 13,877 | 0.1446 | 6,297 | 0.1740 | 7,580 | 0.6914 | N/A | N/A | 24-1-01-25-02-201-001 | |
| 8239017 | AMELIA M. MCCANN AND HER DAUGHTER, GENEVA L. MCCANN BOTH UNMARRIED PERSONS AS JOINTS TENANTS TITLE REPORT NO. MA-2672 | 6.7761 | 1.9860 | 86,508 | 0.2965 | 12,917 | 1.6895 | 73,591 | 6.7901 | N/A | N/A | 24-1-01-25-00-000-006 | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS
 FAP ROUTE 310 (IL 255)
 SECTION 60-15
 MADISON COUNTY
 JOB NO. R-98-039-92
 WENZEL ROAD
 STATION 9+655.000 TO STATION 9+964.558

SCALE 1:500

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS/OBSOLETE
 102 EASTPORT PLAZA DRIVE
 COLLINGSVILLE, ILLINOIS 62234-6198

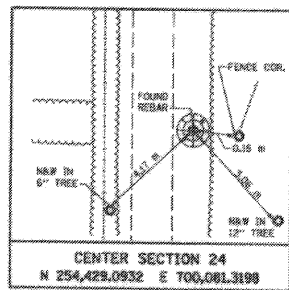
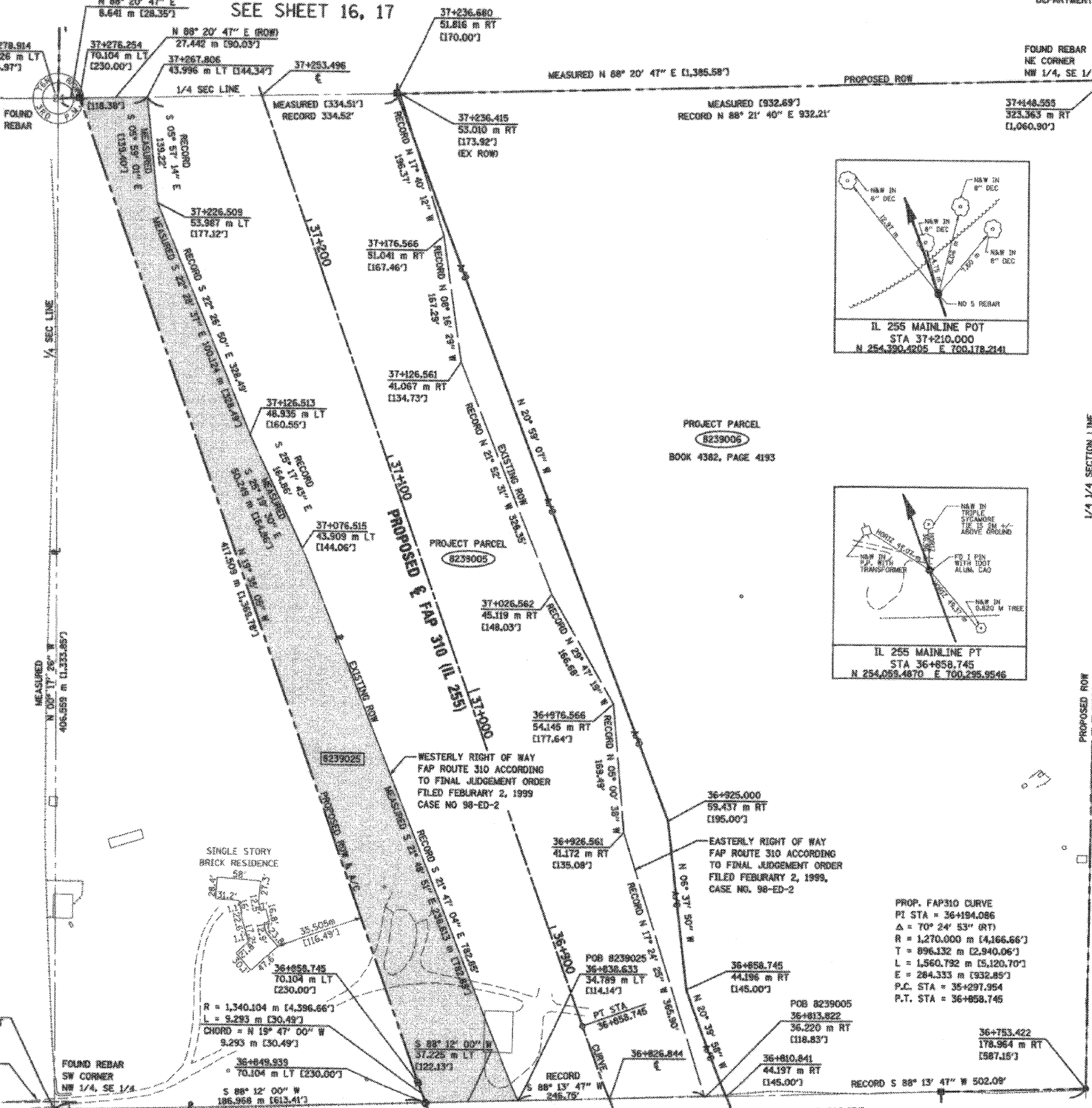
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 REF
 DEF

COORDINATES SHOWN HEREON ARE BASED ON SURVEY CONTROL DATA AS PROVIDED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

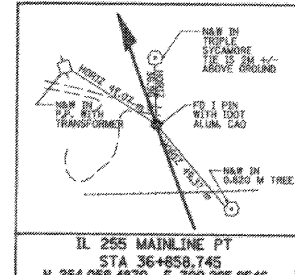
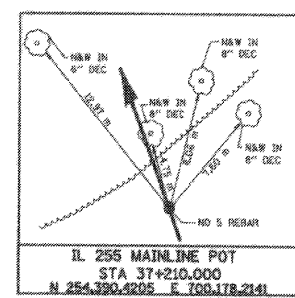
PART OF THE SE 1/4 OF SECTION 24, T6N, R10W, OF THE 3RD PM, MADISON COUNTY, ILLINOIS

BEARINGS SHOWN HEREON ARE BASED ON SURVEY CONTROL DATA AS PROVIDED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

| STATION | COORDINATE TABLE | | |
|------------|------------------|--------------|--------------|
| | OFFSET | NORTH | EAST |
| 36+753.422 | 178.964 m RT | 254,035.5828 | 700,486.3925 |
| 36+810.841 | 44.197 m RT | 254,031.0420 | 700,353.9109 |
| 36+838.633 | 34.789 m LT | 254,028.4130 | 700,270.2582 |
| 36+843.939 | 70.104 m LT | 254,027.2436 | 700,233.0519 |
| 36+858.745 | 44.196 m RT | 254,074.3010 | 700,337.5940 |
| 36+858.745 | 70.104 m LT | 254,035.9881 | 700,228.9062 |
| 36+895.190 | 212.669 m LT | 254,022.5398 | 700,083.3824 |
| 36+925.000 | 59.437 m RT | 254,141.8320 | 700,325.7440 |
| 37+076.515 | 43.909 m LT | 254,249.9398 | 700,181.5898 |
| 37+126.913 | 48.935 m LT | 254,295.3600 | 700,160.0954 |
| 37+148.955 | 323.363 m RT | 254,440.9213 | 700,803.4655 |
| 37+226.509 | 53.987 m LT | 254,387.8778 | 700,121.8166 |
| 37+236.580 | 51.816 m RT | 254,432.9254 | 700,218.0895 |
| 37+267.806 | 43.996 m LT | 254,430.1344 | 700,117.3874 |
| 37+276.254 | 70.104 m LT | 254,423.3426 | 700,069.9574 |
| 37+278.914 | 76.326 m LT | 254,423.0932 | 700,061.3198 |



MONUMENT RECORD
DOCUMENT NO. 2004 R 42466
BOOK 4663, PAGE 0002
RECORDED JUL 16, 2004



- LEGEND**
- EXISTING CENTERLINE
 - EXISTING RIGHT OF WAY LINE
 - PROPOSED ACCESS CONTROL LINE
 - EXISTING EASEMENT LINE
 - PROPOSED CENTERLINE
 - PROPOSED RIGHT OF WAY LINE
 - PROPOSED RIGHT OF WAY AND PROPOSED ACCESS CONTROL LINE
 - PROPOSED TEMPORARY EASEMENT LINE
 - PROPOSED PERMANENT EASEMENT LINE
 - SECTION LINE
 - QUARTER SECTION LINE
 - PROPERTY (DEED) LINE
 - APPARENT PROPERTY LINE
 - MEASURED DIMENSION
 - COMPUTED DIMENSION
 - ENGLISH EQUIVALENT TO METRIC DIM.
 - RECORDED DIMENSION
 - FOUND IRON PIPE OR IRON ROD
 - SET 5/8 INCH IRON ROD
 - PERMANENT SURVEY MONUMENT, I.D.O.T. STD. 2135 (TO BE SET BY OTHERS)
 - CUT CROSS FOUND OR SET
 - SAME OWNERSHIP

- STAKING OF PROPOSED RIGHT OF WAY. SET 5/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. SET 5/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER 20 INCHES BELOW GROUND SURFACE TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

STATE OF ILLINOIS)
COUNTY OF ADAMS)

THIS IS TO CERTIFY THAT I, JOHN E. BASINGER AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE DIRECTED THE SURVEY AND DRAFTING OF THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 24, TOWNSHIP 6 NORTH, RANGE 10 WEST, OF THE THIRD PRINCIPAL MERIDIAN, MADISON COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED 11-30-2005

John E. Basinger, PLS #2766
LICENSE EXPIRATION DATE: 11-30-2006
"THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY."
FIELD WORK COMPLETED: APRIL 2004



KUNGNER & ASSOCIATES, P.C.
Engineers / Architects
618 North 24th Street #217 223-3670
Quincy, Illinois 62301 FAX 223-3603
Internet Address: www.kungner.com
STATE OF ILLINOIS DESIGN FIRM # 1842739

ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS
FAP ROUTE 310 (IL 255)
SECTION 60-15
MADISON COUNTY
JOB NO. R-98-039-92

STATION 36+826.844 TO STATION 37+253.496

0 m 20 m 40 m 60 m
SCALE 1:1000 SHEET 15 OF 55

ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS/DISTRICT 8
1102 EASTPORT PLAZA DRIVE
COLLINGSVILLE, ILLINOIS 62234-6190

| PARCEL NO. | OWNER | TOTAL HOLDING | FEE SIMPLE ACQUISITION | | | | | | EASEMENTS | | | PERMANENT TAX NUMBER | PROPERTY ACQUIRED BY | |
|------------|--|---------------|------------------------|---------|----------------------|---------|--------|---------|-----------|----------------------------------|------------------|----------------------|-----------------------|-------|
| | | | GROSS | | PREVIOUSLY DEDICATED | | NET | | REMAINDER | PE = PERMANENT TE = TEMPORARY | EASEMENT PURPOSE | | | |
| | | | ACRES | SQ. FT. | ACRES | SQ. FT. | ACRES | SQ. FT. | | | | | | ACRES |
| 8239005 | RICHARD G. GROSS AND JUNE A. GROSS HUSBAND AND WIFE, AS JOINT TENANTS TITLE REPORT NO. MA-982 | 42.003 | 9.368 | 408,070 | N/A | N/A | 9.368 | 408,070 | 32.635 | N/A | N/A | N/A | | |
| 8239006 | RICHARD G. GROSS AND JUNE A. GROSS HUSBAND AND WIFE, AS JOINT TENANTS TITLE REPORT NO. MA-1363 | 32.635 | 21,913 | 954,530 | N/A | N/A | 21,913 | 954,530 | 10.722 | N/A | N/A | N/A | | |
| 8239025 | RICHARD G. GROSS AND JUNE A. GROSS HUSBAND AND WIFE, AS JOINT TENANTS TITLE REPORT NO. MA-3041.0 | 10.722 | 2,7627 | 120,342 | N/A | N/A | 2,7627 | 120,342 | 7.9693 | N/A | N/A | N/A | 24-1-01-24-00-000-005 | |