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TRAFFIC DATA

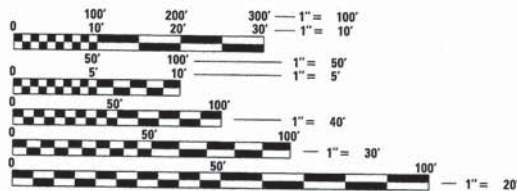
ADT:
KUHN ROAD 5,500 VPD (2012)

POSTED SPEED **DESIGN SPEED**
35 MPH (EXISTING) 40 MPH (EXISTING)
35 MPH (PROPOSED) 40 MPH (PROPOSED)

FUNCTIONAL CLASSIFICATION

MAJOR COLLECTOR

PROJECT IS LOCATED IN THE VILLAGE OF CAROL STREAM



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

CONTRACT NO. 63876

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

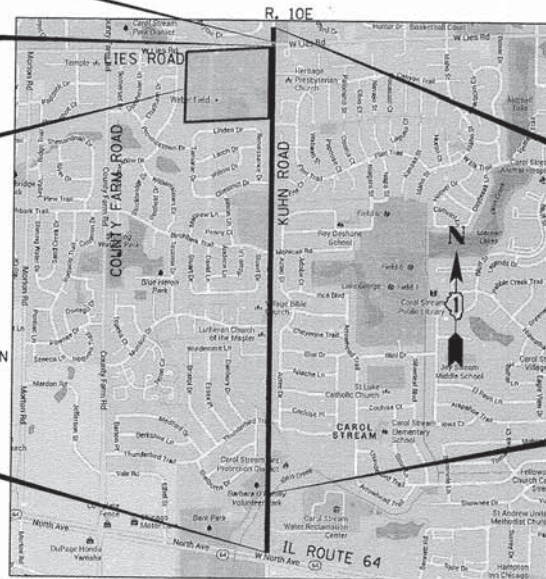
PLANS FOR PROPOSED
FEDERAL AID HIGHWAY
FAU 2554 (KUHN ROAD)
FAP 0307 (IL ROUTE 64) TO FAU 1375 (LIES ROAD)
RESURFACING
SECTION 13-00059-00-RS
PROJECT M-4003(208)
VILLAGE OF CAROL STREAM
DUPAGE COUNTY
JOB NO. C-91-386-13

END OMISSION
STATION 185+69

BEGIN OMISSION
STATION 184+28

GLENBARD NORTH
HIGH SCHOOL

PROJECT BEGINS
STATION 100+00



BLOOMINGDALE TOWNSHIP SECTIONS 19, 30, AND 31 VILLAGE OF CAROL STREAM 3RD P.M.

LOCATION MAP

NOT TO SCALE

PROJECT LENGTH (GROSS / NET)
9,026 FT (1.71 MILES) (GROSS)
OMISSION = 141 FT (0.03 MILES)
TOTAL PROJECT LENGTH = 8,885 FT (1.68 MILES) (NET)

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|----------------|--------|--------------|-----------|
| 2554 | 13-00059-00-RS | DUPAGE | 26 | 1 |

CONTRACT NO. 63876



PROJECT ENDS
STATION 190+26

EXISTING BRIDGE
SN 022-6206

1475 EAST WOODFIELD ROAD, SUITE 600
SCHAMBERG, ILLINOIS 60173
(847) 605-9800



FEDERAL AID PROGRAM ENGINEER: FAWAD AQUEEL, P.E., P.T.O.E., (847) 705-4021, SCHAMBERG, IL



DAVID W. BLOCK, P.E.
NO. 062-050966
EXP. DATE 11/30/15

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED December 18, 2014
[Signature]
VILLAGE OF CAROL STREAM, VILLAGE ENGINEER

PASSED December 29, 2014
[Signature]
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID
BASED ON LIMITED
REVIEW December 30, 2014
[Signature]
DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

| SUMMARY OF QUANTITIES | | | | 0005 ROADWAY 70% STP 30% LA |
|-----------------------|--|--------|----------------|--------------------------------------|
| CODE NO | ITEM | UNIT | TOTAL QUANTITY | |
| 20201200 | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | CU YD | 99 | 99 |
| 21101615 | TOPSOIL FURNISH AND PLACE, 4" | SO YD | 380 | 380 |
| 25000210 | SEEDING, CLASS 2A | ACRE | 1 | 1 |
| 25000400 | NITROGEN FERTILIZER NUTRIENT | POUND | 8 | 8 |
| 25000500 | PHOSPHORUS FERTILIZER NUTRIENT | POUND | 8 | 8 |
| 25000600 | POTASSIUM FERTILIZER NUTRIENT | POUND | 8 | 8 |
| 25100630 | EROSION CONTROL BLANKET | SO YD | 380 | 380 |
| 25200200 | SUPPLEMENTAL WATERING | UNIT | 10 | 10 |
| 28000510 | INLET FILTERS | EACH | 32 | 32 |
| 30300001 | AGGREGATE SUBGRADE IMPROVEMENT | CU YD | 56 | 56 |
| 31101200 | SUBBASE GRANULAR MATERIAL, TYPE B 4" | SO YD | 719 | 719 |
| 35800100 | PREPARATION OF BASE | SO YD | 33,220 | 33,220 |
| 40600275 | BITUMINOUS MATERIALS (PRIME COAT) | POUND | 22,424 | 22,424 |
| 40600400 | MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS | TON | 50 | 50 |
| 40600827 | POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 | TON | 1,828 | 1,828 |
| 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | SO YD | 213 | 213 |
| 40600990 | TEMPORARY RAMP | SO YD | 213 | 213 |
| 40603340 | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 | TON | 3,721 | 3,721 |
| 42001300 | PROTECTIVE COAT | SO YD | 3,763 | 3,763 |
| 42400200 | PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH | SO FT | 3,555 | 3,555 |
| 42400800 | DETECTABLE WARNINGS | SO FT | 247 | 247 |
| 44000161 | HOT-MIX ASPHALT SURFACE REMOVAL, 3" | SO YD | 33,007 | 33,007 |
| 44000500 | COMBINATION CURB AND GUTTER REMOVAL | FOOT | 1,140 | 1,140 |
| 44000600 | SIDEWALK REMOVAL | SO FT | 3,285 | 3,285 |
| 44201725 | CLASS D PATCHES, TYPE I, 7 INCH | SO YD | 416 | 416 |
| 44201729 | CLASS D PATCHES, TYPE II, 7 INCH | SO YD | 416 | 416 |
| 44201733 | CLASS D PATCHES, TYPE III, 7 INCH | SO YD | 416 | 416 |
| 44201735 | CLASS D PATCHES, TYPE IV, 7 INCH | SO YD | 416 | 416 |
| 60221000 | MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, OPEN LID | EACH | 1 | 1 |
| 60404800 | FRAMES AND GRATES, TYPE II | EACH | 6 | 6 |
| 60404805 | FRAMES AND GRATES, TYPE IIV | EACH | 21 | 21 |
| 60406100 | FRAMES AND LIDS, TYPE I, CLOSED LID | EACH | 5 | 5 |
| 60500040 | REMOVING MANHOLES | EACH | 1 | 1 |
| 60603800 | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 | FOOT | 1,080 | 1,080 |
| 60605000 | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 | FOOT | 60 | 60 |
| 67100100 | MOBILIZATION | L SUM | 1 | 1 |
| 70102620 | TRAFFIC CONTROL AND PROTECTION, STANDARD T01501 | L SUM | 1 | 1 |
| 70102622 | TRAFFIC CONTROL AND PROTECTION, STANDARD T01502 | L SUM | 1 | 1 |
| 70102635 | TRAFFIC CONTROL AND PROTECTION, STANDARD T01701 | L SUM | 1 | 1 |
| 70102640 | TRAFFIC CONTROL AND PROTECTION, STANDARD T01801 | L SUM | 1 | 1 |
| 70106800 | CHANGEABLE MESSAGE SIGN | CAL MO | 3 | 3 |
| 70300100 | SHORT TERM PAVEMENT MARKING | FOOT | 2,557 | 2,557 |
| 70300210 | TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS | SO FT | 354 | 354 |
| 70300220 | TEMPORARY PAVEMENT MARKING - LINE 4" | FOOT | 21,972 | 21,972 |

| SUMMARY OF QUANTITIES | | | | 0005 ROADWAY 70% STP 30% LA |
|-----------------------|--|-------|----------------|--------------------------------------|
| CODE NO | ITEM | UNIT | TOTAL QUANTITY | |
| 70300240 | TEMPORARY PAVEMENT MARKING - LINE 6" | FOOT | 1,881 | 1,881 |
| 70300260 | TEMPORARY PAVEMENT MARKING - LINE 12" | FOOT | 1,496 | 1,496 |
| 70300280 | TEMPORARY PAVEMENT MARKING - LINE 24" | FOOT | 221 | 221 |
| 70301000 | WORK ZONE PAVEMENT MARKING REMOVAL | SO FT | 1,056 | 1,056 |
| 72000100 | SIGN PANEL - TYPE 1 | SO FT | 75 | 75 |
| 72900100 | METAL POST - TYPE A | FOOT | 65 | 65 |
| 78000100 | THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS | SO FT | 354 | 354 |
| 78000200 | THERMOPLASTIC PAVEMENT MARKING - LINE 4" | FOOT | 21,972 | 21,972 |
| 78000400 | THERMOPLASTIC PAVEMENT MARKING - LINE 6" | FOOT | 1,881 | 1,881 |
| 78000600 | THERMOPLASTIC PAVEMENT MARKING - LINE 12" | FOOT | 1,496 | 1,496 |
| 78000650 | THERMOPLASTIC PAVEMENT MARKING - LINE 24" | FOOT | 221 | 221 |
| 88600600 | DETECTOR LOOP REPLACEMENT | FOOT | 164 | 164 |
| X0327036 | BIKE PATH REMOVAL | SO YD | 35 | 35 |
| X6030310 | FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) | EACH | 18 | 18 |
| Z0013798 | CONSTRUCTION LAYOUT | L SUM | 1 | 1 |
| Z0018400 | DRAINAGE STRUCTURES TO BE ADJUSTED | EACH | 45 | 45 |
| Z0018600 | DRAINAGE STRUCTURES TO BE RECONSTRUCTED | EACH | 3 | 3 |
| Z0030850 | TEMPORARY INFORMATION SIGNING | SO FT | 214 | 214 |

* SPECIALTY ITEM

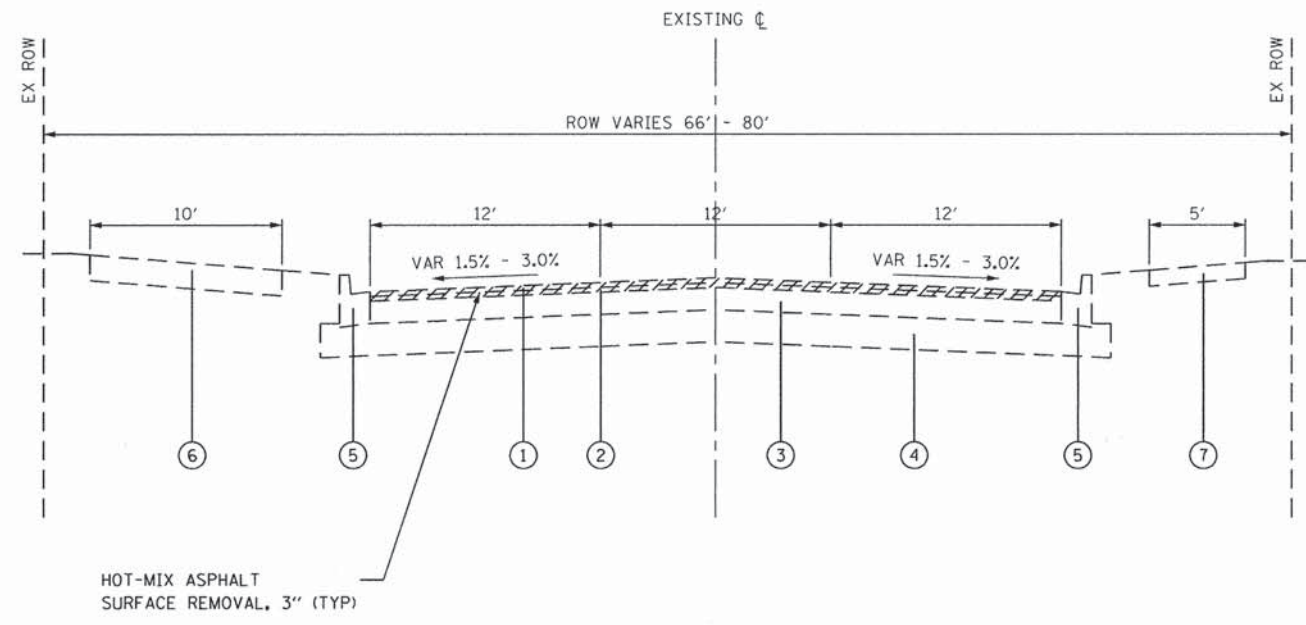
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| | PLOT SCALE = 50.000' / 1" | CHECKED - DWB | REVISED - |
| #MODELNAME# | PLOT DATE = 12/17/2014 | DATE - 10/17/2014 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

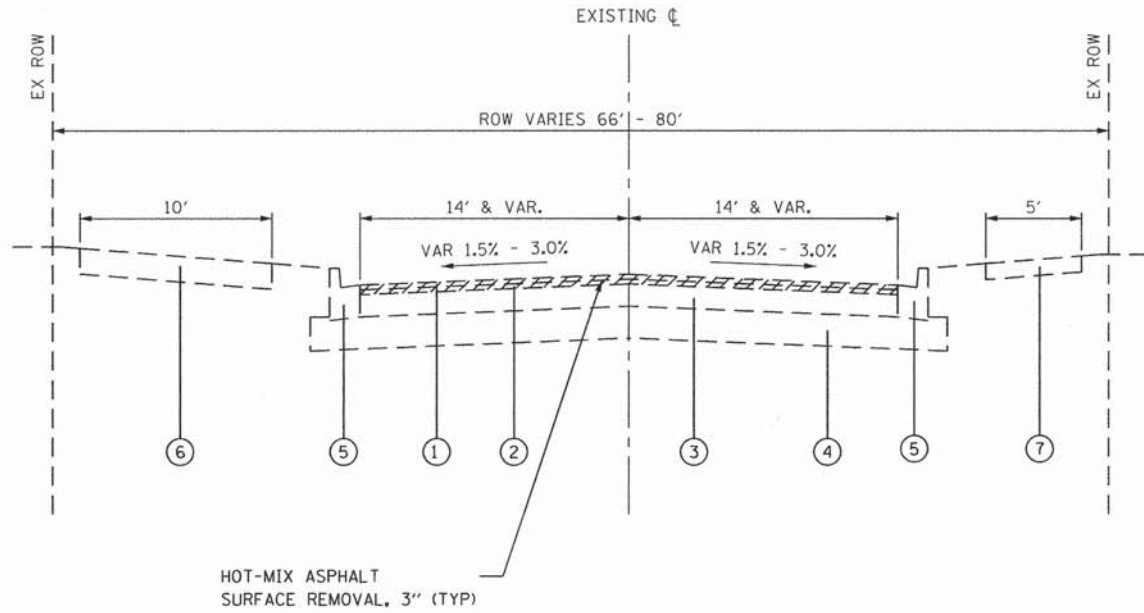
KUHN ROAD RESURFACING
SUMMARY OF QUANTITIES

SCALE: NTS SHEET 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|---------------------------|----------------|--------|--------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2554 | 13-00059-00-RS | | 26 | 3 |
| DUPAGE | | | CONTRACT NO. 63876 | |
| ILLINOIS FED. AID PROJECT | | | | |



EXISTING TYPICAL SECTION
 STA. 100+00 TO STA. 123+97
 KUHN ROAD

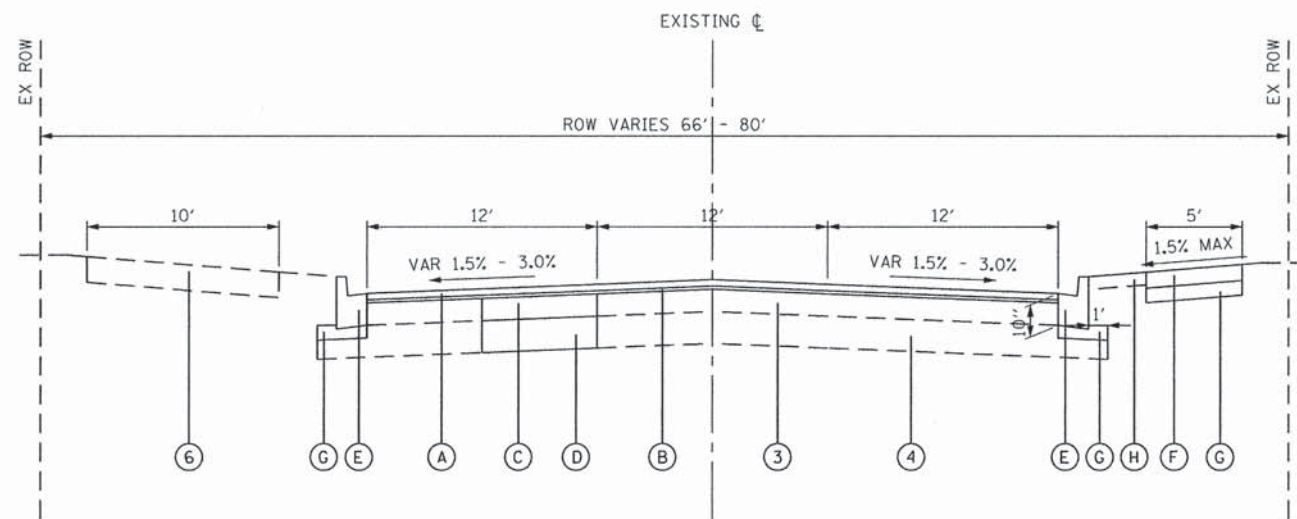


EXISTING TYPICAL SECTION
 STA. 123+97 TO STA. 184+28
 OMISSION STA. 184+28 TO STA. 185+69
 STA. 185+69 TO STA. 190+25
 KUHN ROAD

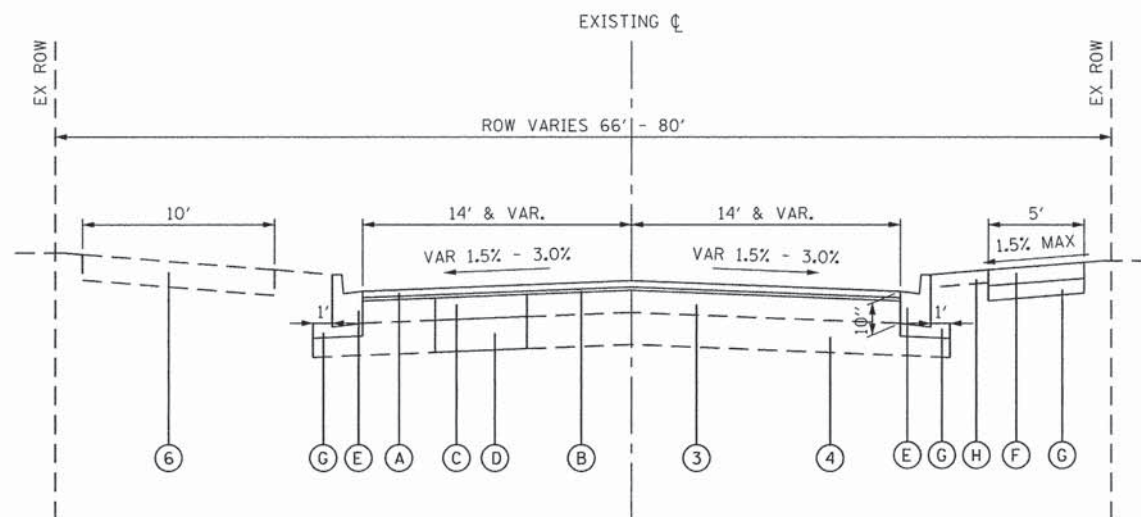
LEGEND

- ① EXISTING HMA SURFACE COURSE, 1.5"
- ② EXISTING HMA BINDER COURSE, 1.5"
- ③ EXISTING HMA SUBBASE, 7"
- ④ EXISTING AGGREGATE SUBGRADE, 8"
- ⑤ EXISTING COMBINATION CONCRETE CURB & GUTTER TYPE B-6.12
- ⑥ EXISTING MULTI-USE PATH, 3" HMA OVER 9" AGGREGATE BASE COURSE
- ⑦ EXISTING PCC SIDEWALK, 5"
- Ⓐ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- Ⓑ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- Ⓒ CLASS D PATCHES, 7" (AS DIRECTED BY ENGINEER)
- Ⓓ REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
AGGREGATE SUBGRADE IMPROVEMENT (AT PATCHING LOCATIONS AS DIRECTED BY ENGINEER)
- Ⓔ COMBINATION CURB AND GUTTER REMOVAL
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 OR TYPE B-6.24
(AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
- Ⓕ SIDEWALK REMOVAL
PCC SIDEWALK, 5"
(AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
- Ⓖ SUBBASE GRANULAR MATERIAL, TYPE B 4"
(AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
- Ⓗ SEEDING, CLASS 2A
EROSION CONTROL BLANKET
TOPSOIL FURNISH AND PLACE, 4"
(AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)

| | | | | | | | | | | | |
|---|------------------------|-------------------|------------|---|---|---------------------|--------------|---------------------------|--------|--------------|-----------|
| FILE NAME = | USER NAME = .USER. | DESIGNED - BSH | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | KUHN ROAD RESURFACING TYPICAL SECTIONS | | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| Go\CH09\0109\Road\Sheets\0109-TYPSECT10 | NS-Ldgn | DRAWN - BSH | REVISED - | | | | 2554 | 13-00059-00-RS | DUPAGE | 26 | 4 |
| PLOT SCALE = 1/8" = 1' / in. | CHECKED - DWB | REVISIED - | REVISIED - | | CONTRACT NO. 63876 | | | | | | |
| MODELNAME | PLOT DATE = 12/22/2014 | DATE - 10/17/2014 | REVISED - | | SCALE: NTS | SHEET 1 OF 2 SHEETS | STA. TO STA. | ILLINOIS FED. AID PROJECT | | | |



PROPOSED TYPICAL SECTION
 STA. 100+00 TO STA. 123+97
 KUHN ROAD



PROPOSED TYPICAL SECTION
 STA. 123+97 TO STA. 184+28
 OMISSION STA. 184+28 TO STA. 185+69
 STA. 185+69 TO STA. 190+25
 KUHN ROAD

LEGEND

- ① EXISTING HMA SURFACE COURSE, 1.5"
- ② EXISTING HMA BINDER COURSE, 1.5"
- ③ EXISTING HMA SUBBASE, 7"
- ④ EXISTING AGGREGATE SUBGRADE, 8"
- ⑤ EXISTING COMBINATION CONCRETE CURB & GUTTER TYPE B-6.12
- ⑥ EXISTING MULTI-USE PATH, 3" HMA OVER 9" AGGREGATE BASE COURSE
- ⑦ EXISTING PCC SIDEWALK, 5"
- (A) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- (B) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- (C) CLASS D PATCHES, 7" (AS DIRECTED BY ENGINEER)
- (D) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
 AGGREGATE SUBGRADE IMPROVEMENT (AT PATCHING LOCATIONS AS DIRECTED BY ENGINEER)
- (E) COMBINATION CURB AND GUTTER REMOVAL
 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 OR TYPE B-6.24
 (AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
- (F) SIDEWALK REMOVAL
 PCC SIDEWALK, 5"
 (AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
- (G) SUBBASE GRANULAR MATERIAL, TYPE B 4"
 (AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)
- (H) SEEDING, CLASS 2A
 EROSION CONTROL BLANKET
 TOPSOIL FURNISH AND PLACE, 4"
 (AS SHOWN ON PLANS AND AS DIRECTED BY ENGINEER)

NOTES

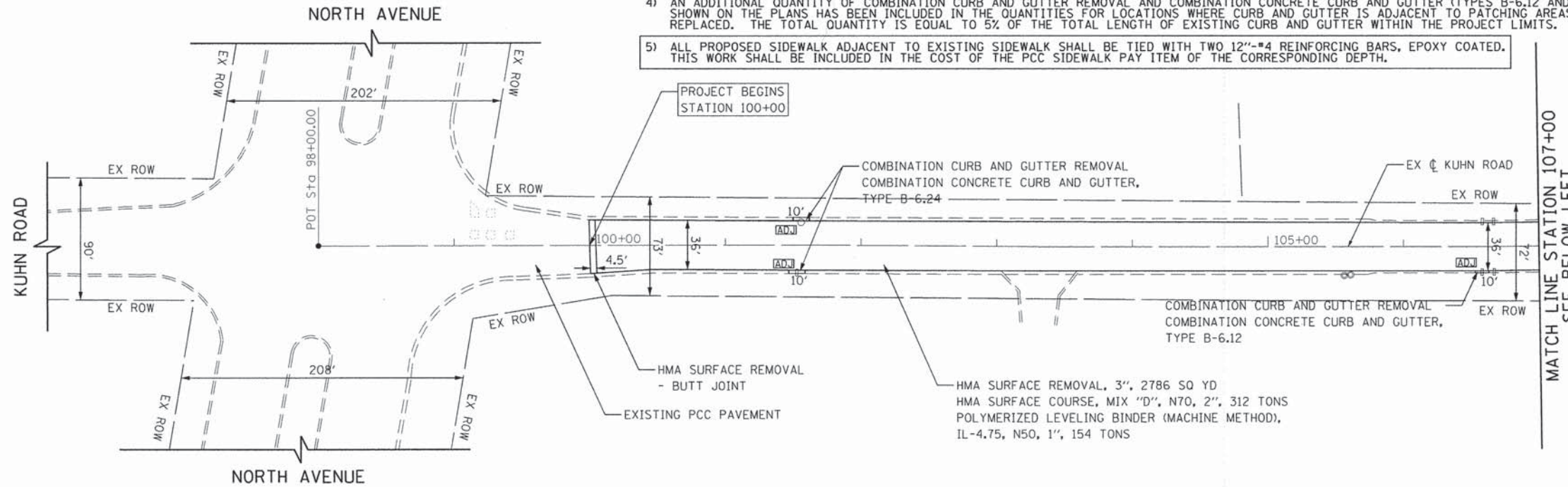
1. PROPOSED ROADWAY PROFILE AND CROSS-SLOPES AT PEDESTRIAN CROSSINGS SHALL BE ADA COMPLIANT.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

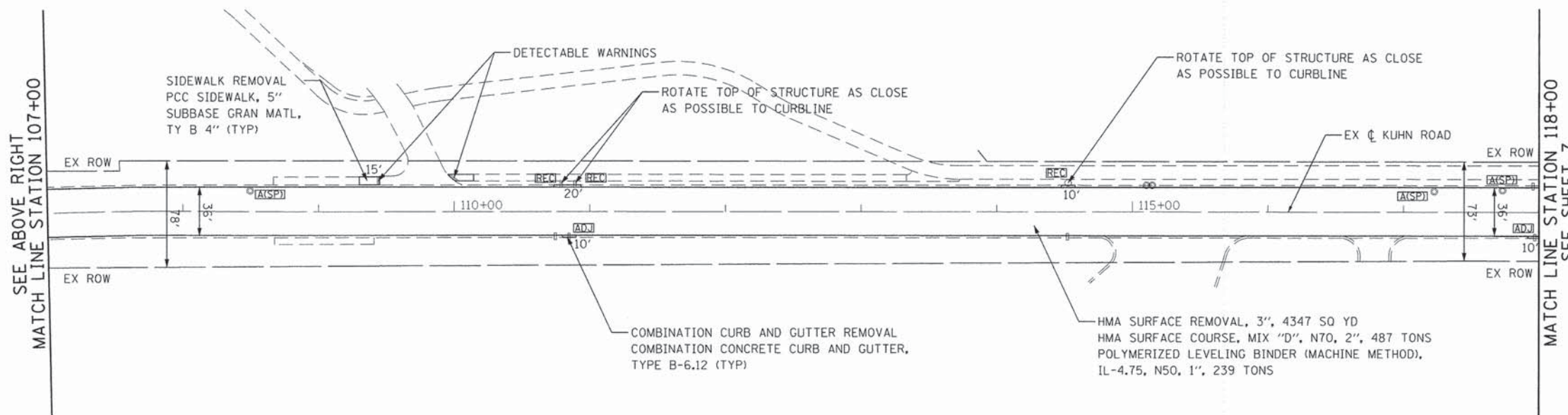
| MIXTURE TYPE | AIR VOIDS @ Ndes |
|--|---------------------|
| HMA SURFACE COURSE, MIX "D", N70 (IL 9.5mm), 2" | 4% @ 70 GYRATIONS |
| POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1" | 3.5% @ 50 GYRATIONS |
| CLASS D PATCHES (HMA BINDER IL-19 MM), N70, 7" (IN 2 LIFTS) | 4% @ 70 GYRATIONS |

- NOTES: 1) THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/50 YD/IN.
 2) THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE THE SPECIAL PROVISIONS.
 3) THE CONTRACTOR SHALL MILL BEFORE PATCHING.

- NOTES: 1) CLASS D PATCHES, 7" AS DIRECTED BY ENGINEER.
 2) THE ADJUSTMENTS OF DRAINAGE STRUCTURES IN THE CURB AND GUTTER LINE SHALL BE PAID FOR AS DRAINAGE STRUCTURES TO BE ADJUSTED.
 3) REMOVAL AND DISPOSAL OF THE MATERIAL UNDER THE PROPOSED CURB AND GUTTER AND PROPOSED SIDEWALK LOCATIONS IN ORDER TO INSTALL THE PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4" SHALL BE CONSIDERED INCLUDED IN THE COSTS OF COMBINATION CURB AND GUTTER REMOVAL AND SIDEWALK REMOVAL.
 4) AN ADDITIONAL QUANTITY OF COMBINATION CURB AND GUTTER REMOVAL AND COMBINATION CONCRETE CURB AND GUTTER (TYPES B-6.12 AND B-6.24) WHICH IS NOT SHOWN ON THE PLANS HAS BEEN INCLUDED IN THE QUANTITIES FOR LOCATIONS WHERE CURB AND GUTTER IS ADJACENT TO PATCHING AREAS AND WILL NEED TO BE REPLACED. THE TOTAL QUANTITY IS EQUAL TO 5% OF THE TOTAL LENGTH OF EXISTING CURB AND GUTTER WITHIN THE PROJECT LIMITS.
 5) ALL PROPOSED SIDEWALK ADJACENT TO EXISTING SIDEWALK SHALL BE TIED WITH TWO 12"-#4 REINFORCING BARS, EPOXY COATED. THIS WORK SHALL BE INCLUDED IN THE COST OF THE PCC SIDEWALK PAY ITEM OF THE CORRESPONDING DEPTH.



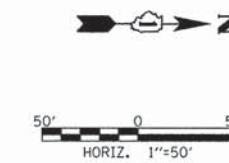
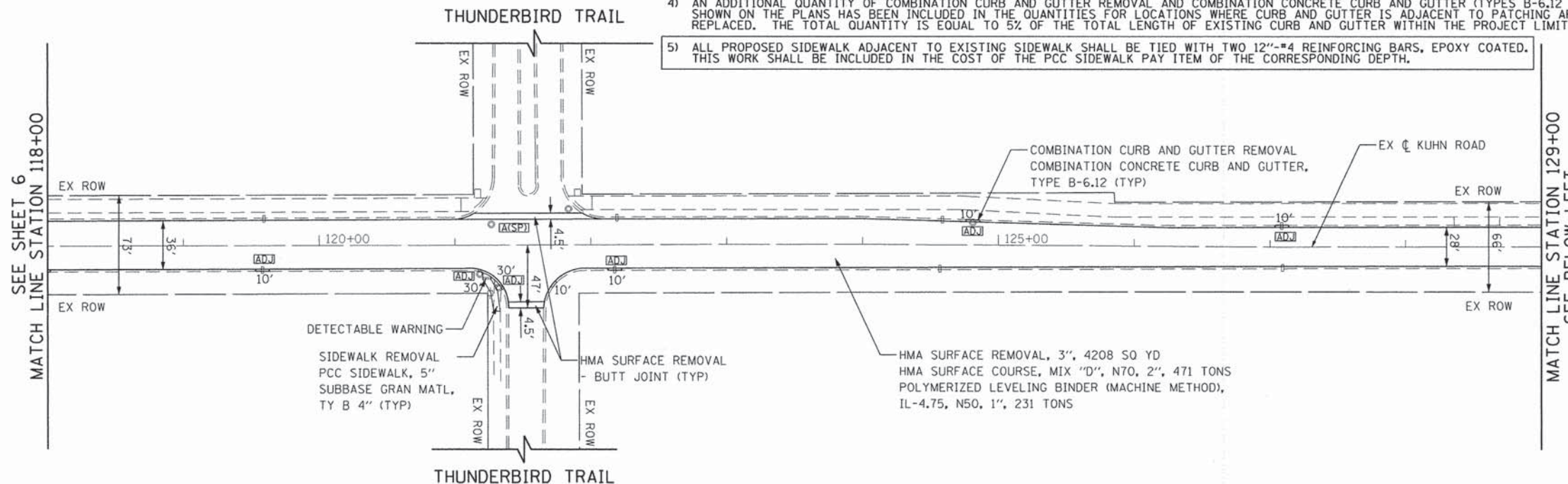
- LEGEND
- [ADJ] DRAINAGE STRUCTURES TO BE ADJUSTED
 - [REC] DRAINAGE STRUCTURES TO BE RECONSTRUCTED
 - [A(SP)] FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
 - [I1] TYPE I1 FRAME & GRATE
 - [I1V] TYPE I1V FRAME & GRATE



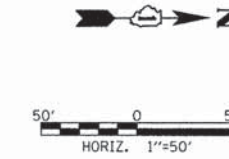
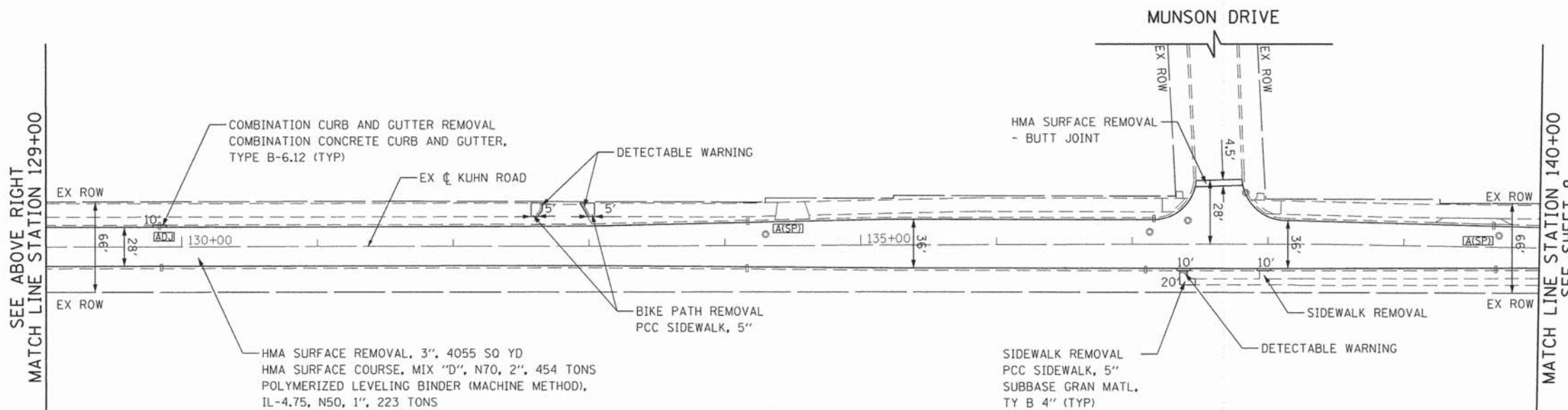
- LEGEND
- [ADJ] DRAINAGE STRUCTURES TO BE ADJUSTED
 - [REC] DRAINAGE STRUCTURES TO BE RECONSTRUCTED
 - [A(SP)] FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
 - [I1] TYPE I1 FRAME & GRATE
 - [I1V] TYPE I1V FRAME & GRATE

| | | | | | | | | | | | | |
|--|-------------------------------|-------------------|-----------|---|---|---------------------------|------------------------|----------------------------|-----------------|--------------------|--|--|
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| MODELNAME# | PLOT SCALE = 100.0000' / 1in. | CHECKED - DWB | REVISED - | | | SCALE: 1"=50' | SHEET 1 OF 5 SHEETS | STA. 100+00 TO STA. 118+00 | | CONTRACT NO. 63876 | | |
| | PLOT DATE = 12/22/2014 | DATE - 10/17/2014 | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | | | |
| | | | | | | | | | | | | |

- NOTES: 1) CLASS D PATCHES, 7" AS DIRECTED BY ENGINEER.
 2) THE ADJUSTMENTS OF DRAINAGE STRUCTURES IN THE CURB AND GUTTER LINE SHALL BE PAID FOR AS DRAINAGE STRUCTURES TO BE ADJUSTED.
 3) REMOVAL AND DISPOSAL OF THE MATERIAL UNDER THE PROPOSED CURB AND GUTTER AND PROPOSED SIDEWALK LOCATIONS IN ORDER TO INSTALL THE PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4" SHALL BE CONSIDERED INCLUDED IN THE COSTS OF COMBINATION CURB AND GUTTER REMOVAL AND SIDEWALK REMOVAL.
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 5) ALL PROPOSED SIDEWALK ADJACENT TO EXISTING SIDEWALK SHALL BE TIED WITH TWO 12"-#4 REINFORCING BARS, EPOXY COATED. THIS WORK SHALL BE INCLUDED IN THE COST OF THE PCC SIDEWALK PAY ITEM OF THE CORRESPONDING DEPTH.



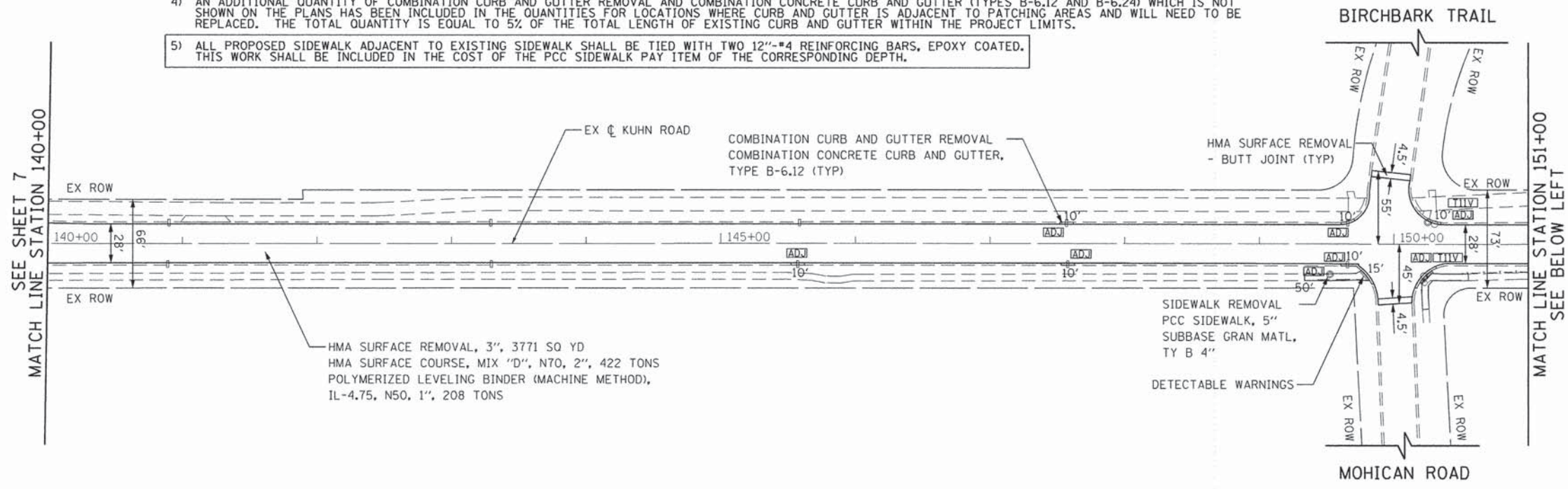
- LEGEND
- [ADJ] DRAINAGE STRUCTURES TO BE ADJUSTED
 - [REC] DRAINAGE STRUCTURES TO BE RECONSTRUCTED
 - [A(SP)] FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
 - [I] TYPE II FRAME & GRATE
 - [IIV] TYPE IIV FRAME & GRATE



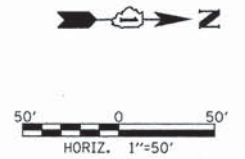
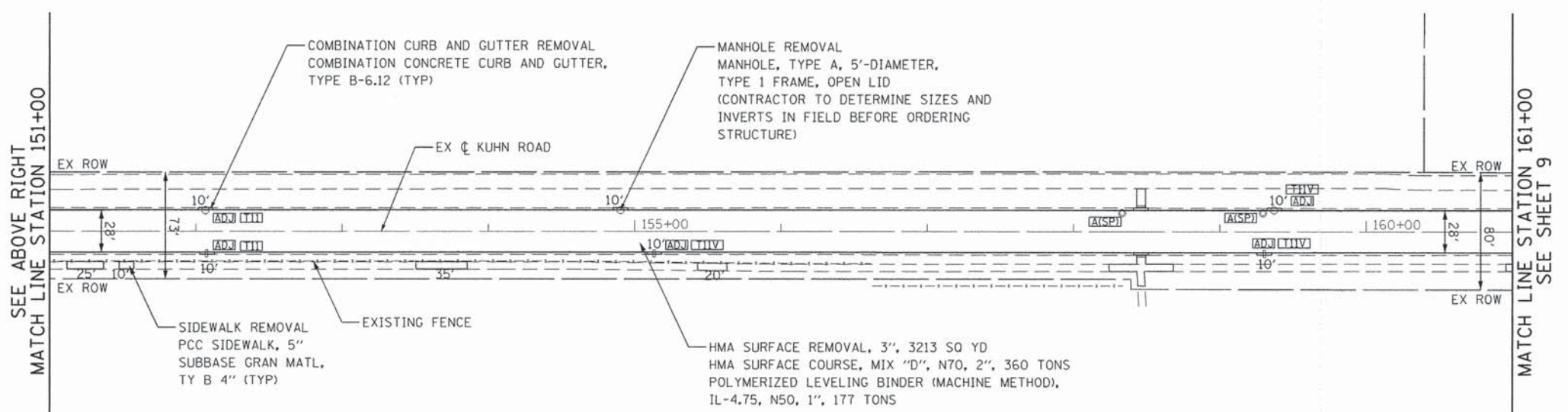
- LEGEND
- [ADJ] DRAINAGE STRUCTURES TO BE ADJUSTED
 - [REC] DRAINAGE STRUCTURES TO BE RECONSTRUCTED
 - [A(SP)] FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
 - [I] TYPE II FRAME & GRATE
 - [IIV] TYPE IIV FRAME & GRATE

| | | | | | | | | | | | |
|--|-----------------------------|-------------------|-----------|---|---|----------------------------|------------------------|--------------------|-----------------|---------------------------|--|
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| MODELNAME# | PLOT SCALE = 100.000' / in. | CHECKED - DWB | REVISED - | | | SCALE: 1"=50' | SHEET 2 OF 5 SHEETS | CONTRACT NO. 63876 | | ILLINOIS FED. AID PROJECT | |
| | PLOT DATE = 12/22/2014 | DATE - 10/17/2014 | REVISED - | | | STA. 118+00 TO STA. 140+00 | | | | | |
| | | | | | | | | | | | |

- NOTES: 1) CLASS D PATCHES, 7" AS DIRECTED BY ENGINEER.
 2) THE ADJUSTMENTS OF DRAINAGE STRUCTURES IN THE CURB AND GUTTER LINE SHALL BE PAID FOR AS DRAINAGE STRUCTURES TO BE ADJUSTED.
 3) REMOVAL AND DISPOSAL OF THE MATERIAL UNDER THE PROPOSED CURB AND GUTTER AND PROPOSED SIDEWALK LOCATIONS IN ORDER TO INSTALL THE PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B 4" SHALL BE CONSIDERED INCLUDED IN THE COSTS OF COMBINATION CURB AND GUTTER REMOVAL AND SIDEWALK REMOVAL.
 4) AN ADDITIONAL QUANTITY OF COMBINATION CURB AND GUTTER REMOVAL AND COMBINATION CONCRETE CURB AND GUTTER (TYPES B-6.12 AND B-6.24) WHICH IS NOT SHOWN ON THE PLANS HAS BEEN INCLUDED IN THE QUANTITIES FOR LOCATIONS WHERE CURB AND GUTTER IS ADJACENT TO PATCHING AREAS AND WILL NEED TO BE REPLACED. THE TOTAL QUANTITY IS EQUAL TO 5% OF THE TOTAL LENGTH OF EXISTING CURB AND GUTTER WITHIN THE PROJECT LIMITS.
 5) ALL PROPOSED SIDEWALK ADJACENT TO EXISTING SIDEWALK SHALL BE TIED WITH TWO 12"-#4 REINFORCING BARS, EPOXY COATED. THIS WORK SHALL BE INCLUDED IN THE COST OF THE PCC SIDEWALK PAY ITEM OF THE CORRESPONDING DEPTH.



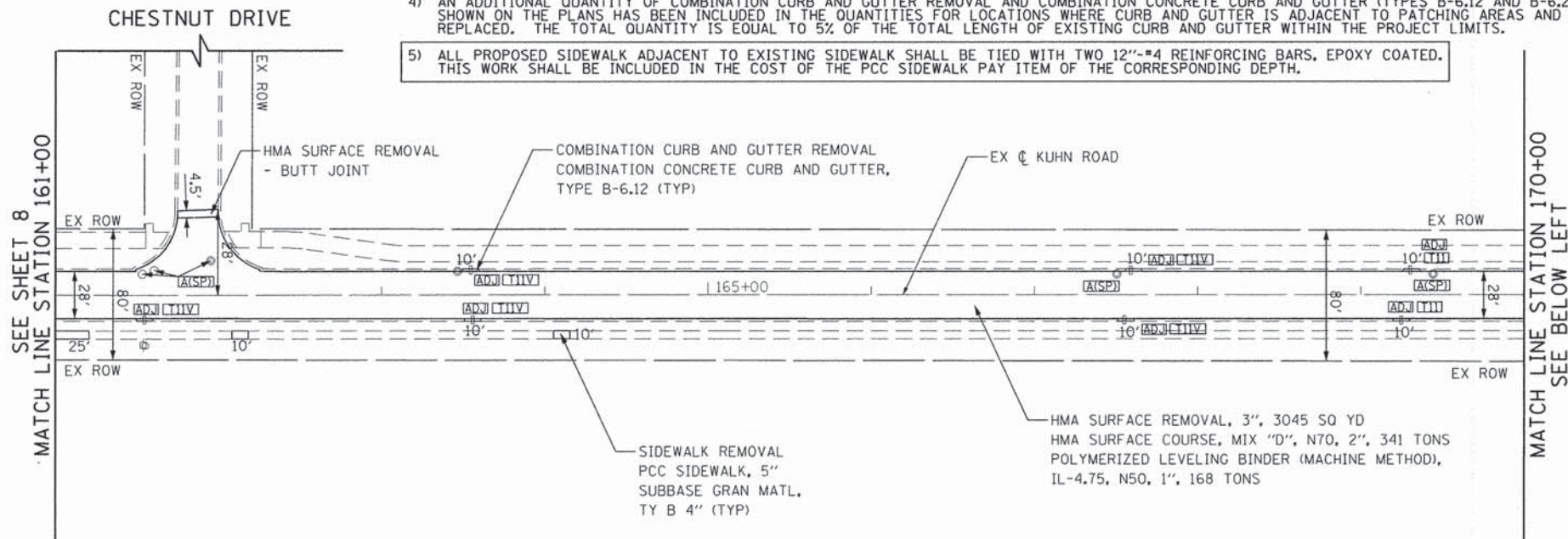
- LEGEND
- [ADJ] DRAINAGE STRUCTURES TO BE ADJUSTED
 - [REC] DRAINAGE STRUCTURES TO BE RECONSTRUCTED
 - [A(SP)] FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
 - [TII] TYPE II FRAME & GRATE
 - [TIV] TYPE IIV FRAME & GRATE



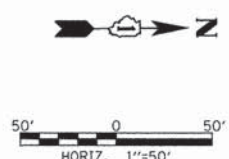
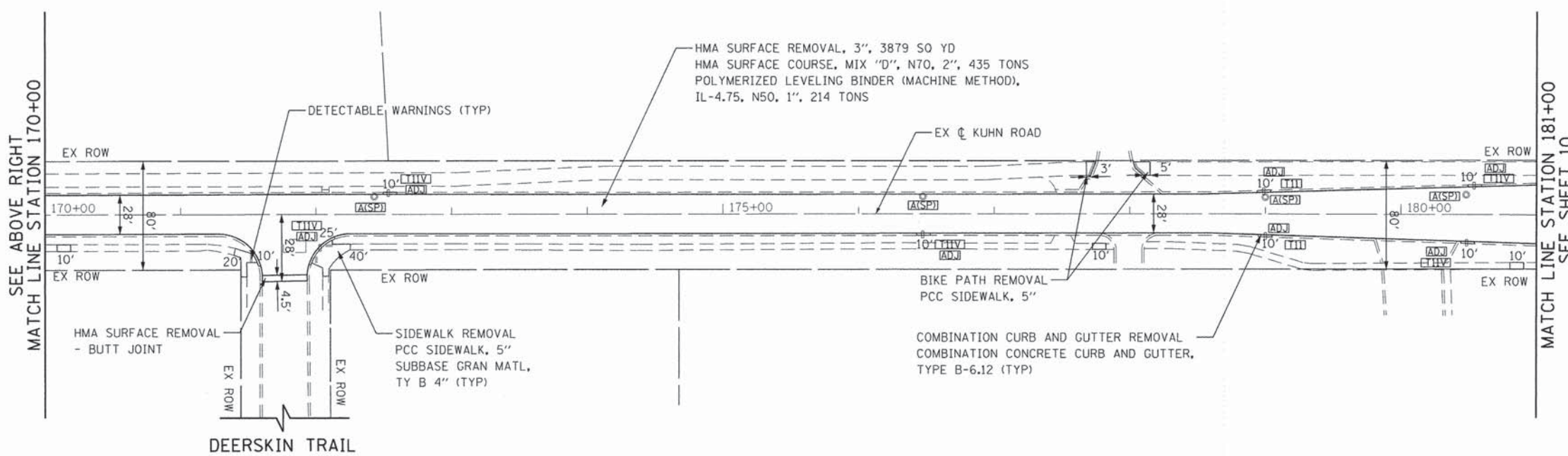
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 - [A(SP)] FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
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 - [TIV] TYPE IIV FRAME & GRATE

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| PLOT SCALE = 100,000 / 1in. | CHECKED - DWB | REVISED - | SCALE: 1"=50' | | | SHEET 3 OF 5 SHEETS | STA. 140+00 TO STA. 161+00 | CONTRACT NO. 63876 | | ILLINOIS FED. AID PROJECT | | |
| PLOT DATE = 12/22/2014 | DATE - 10/17/2014 | REVISED - | | | | | | | | | | |
| #MODELNAME# | | | | | | | | | | | | |

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- LEGEND
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 - [REC] DRAINAGE STRUCTURES TO BE RECONSTRUCTED
 - [A[SP]] FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
 - [I] TYPE I1 FRAME & GRATE
 - [IIV] TYPE I1V FRAME & GRATE



- LEGEND
- [ADJ] DRAINAGE STRUCTURES TO BE ADJUSTED
 - [REC] DRAINAGE STRUCTURES TO BE RECONSTRUCTED
 - [A[SP]] FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
 - [I] TYPE I1 FRAME & GRATE
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| #MODELNAME# | PLOT SCALE = 100,000' / 1" = | CHECKED - DWB | REVISED - | | | SCALE: 1"=50' | SHEET 4 OF 5 SHEETS | STA. 161+00 TO STA. 181+00 | | CONTRACT NO. 63876 | | |
| | PLOT DATE = 12/22/2014 | DATE - 10/17/2014 | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | | | |
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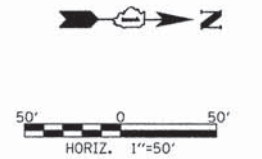
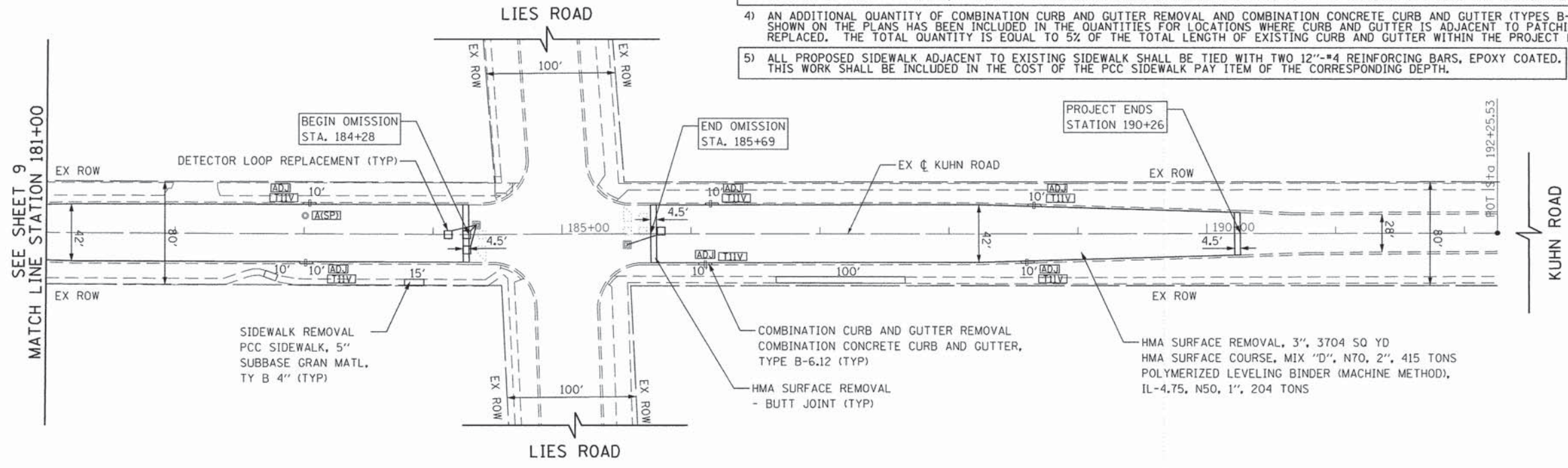
NOTES: 1) CLASS D PATCHES, 7" AS DIRECTED BY ENGINEER.

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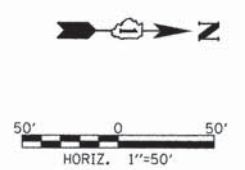
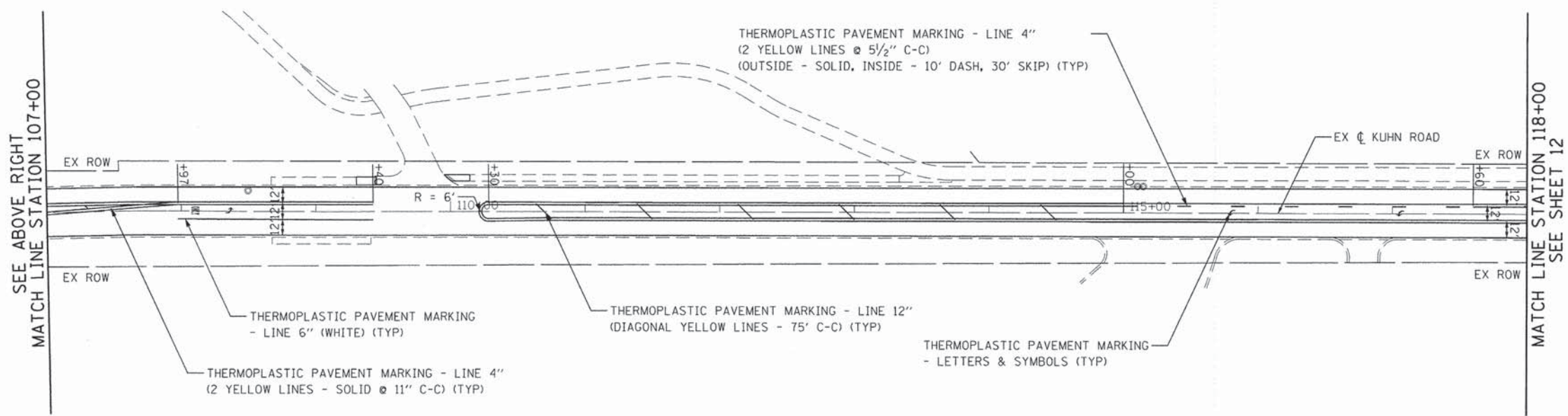
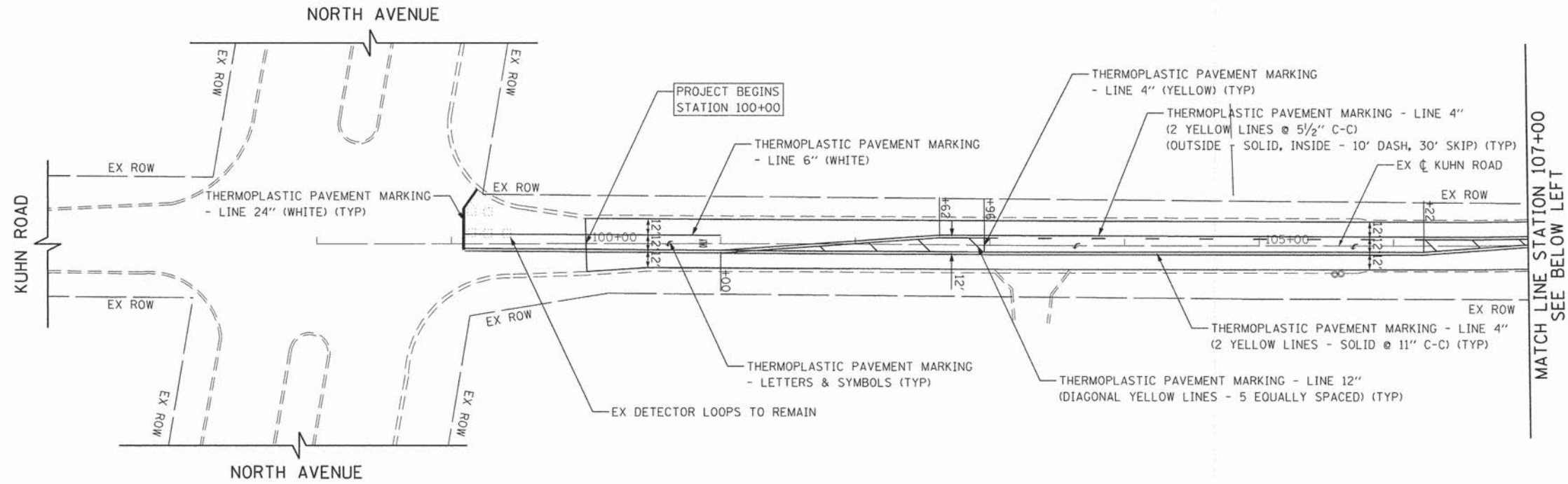


LEGEND

| | |
|---------|--|
| [ADJ] | DRAINAGE STRUCTURES TO BE ADJUSTED |
| [REC] | DRAINAGE STRUCTURES TO BE RECONSTRUCTED |
| [A(SP)] | FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) |
| [TII] | TYPE II FRAME & GRATE |
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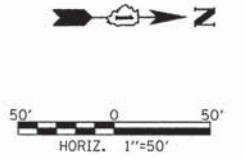
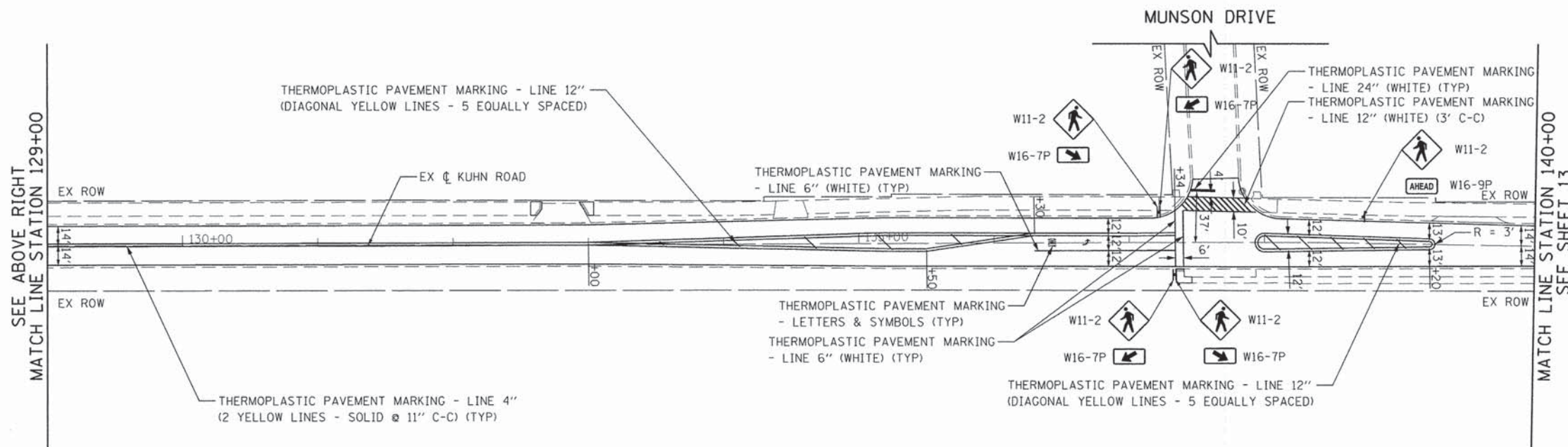
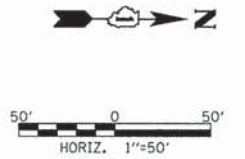
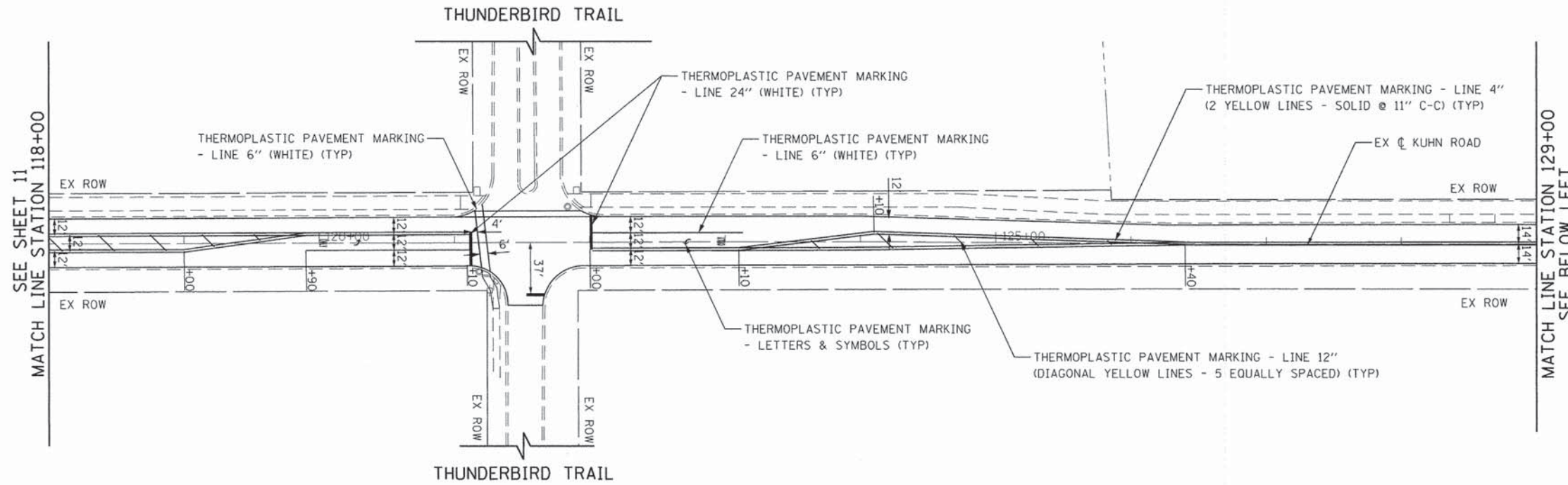
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| MODELNAME# | PLOT SCALE = 100.000' / 1" | CHECKED - DWB | REVISED - | | | SCALE: 1"=50' | SHEET 5 OF 5 SHEETS | CONTRACT NO. 63876 | | ILLINOIS FED. AID PROJECT | |
| | PLOT DATE = 12/22/2014 | DATE - 10/17/2014 | REVISED - | | | STA. 181+00 TO STA. 190+26 | | | | | |
| | | | | | | | | | | | |

NOTES: 1) SEE DISTRICT ONE DETAIL TC-13 TYPICAL PAVEMENT MARKINGS FOR GUIDANCE.



| | | | | | | | | | | |
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| | PLOT DATE = 12/22/2014 | DATE - 10/17/2014 | REVISED - | | | CONTRACT NO. 63876 | | | | |
| | | | | | | ILLINOIS FED. AID PROJECT | | | | |
| #MODELNAME# | | | | SCALE: 1"=50' | | SHEET 1 OF 5 SHEETS | | STA. 100+00 TO STA. 118+00 | | |

NOTES: 1) SEE DISTRICT ONE DETAIL TC-13 TYPICAL PAVEMENT MARKINGS FOR GUIDANCE.



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 DATE - 10/17/2014

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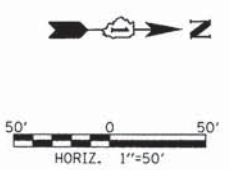
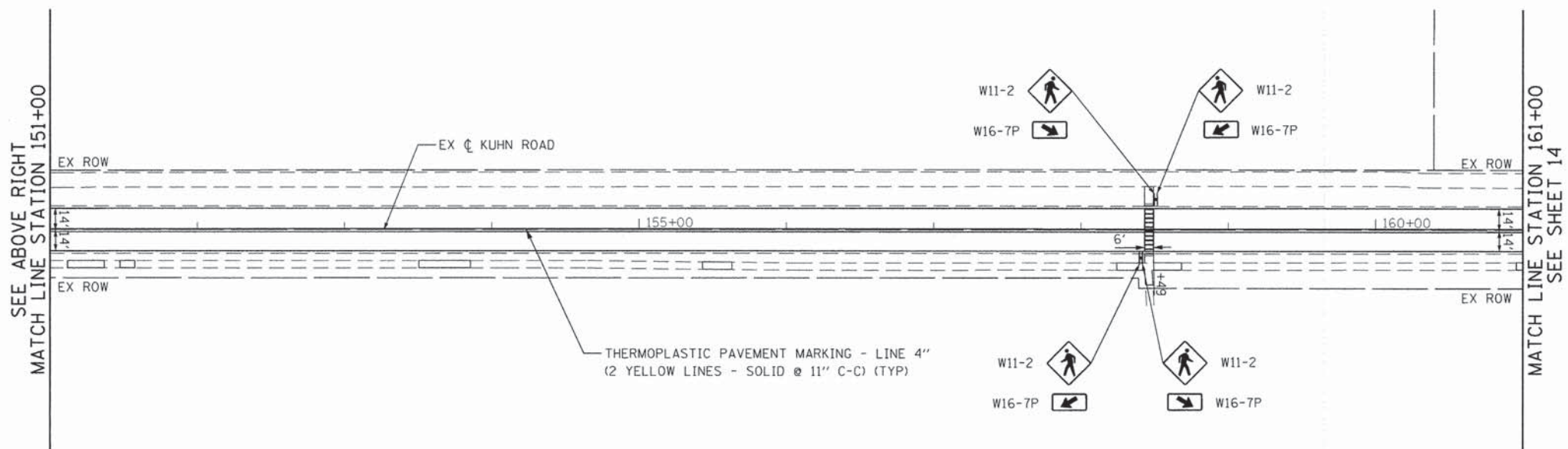
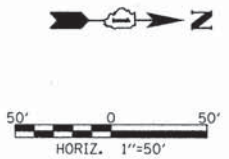
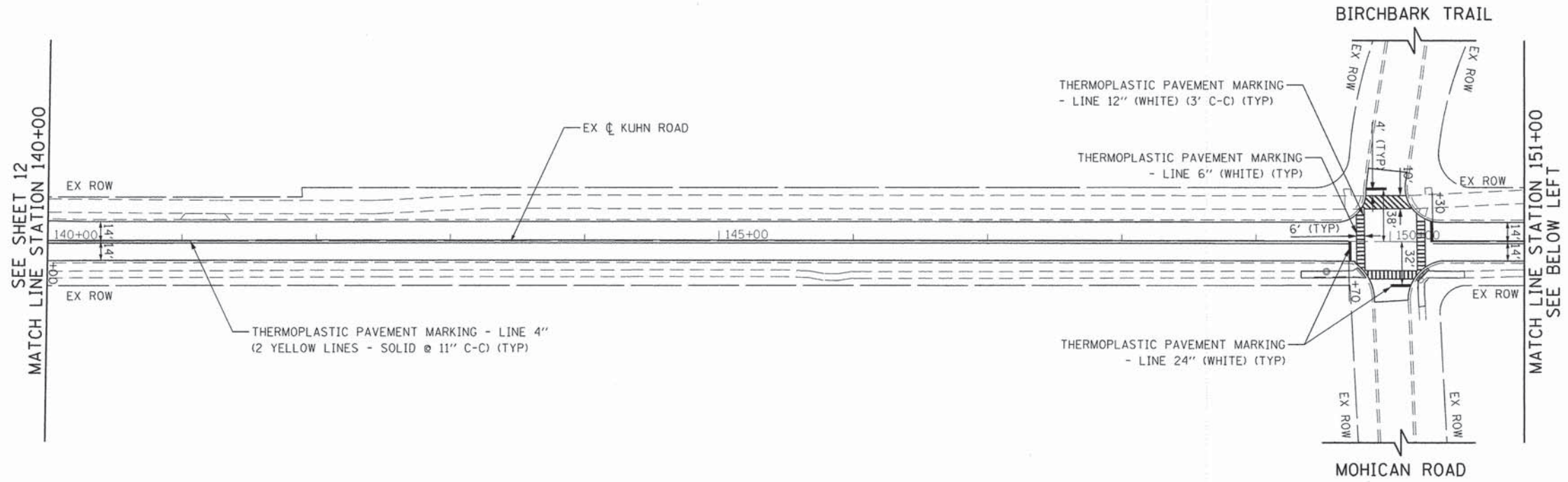
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**KUHN ROAD RESURFACING
 PAVEMENT MARKING PLAN**

SCALE: 1"=50' SHEET 2 OF 5 SHEETS STA. 118+00 TO STA. 140+00

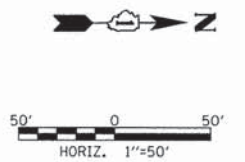
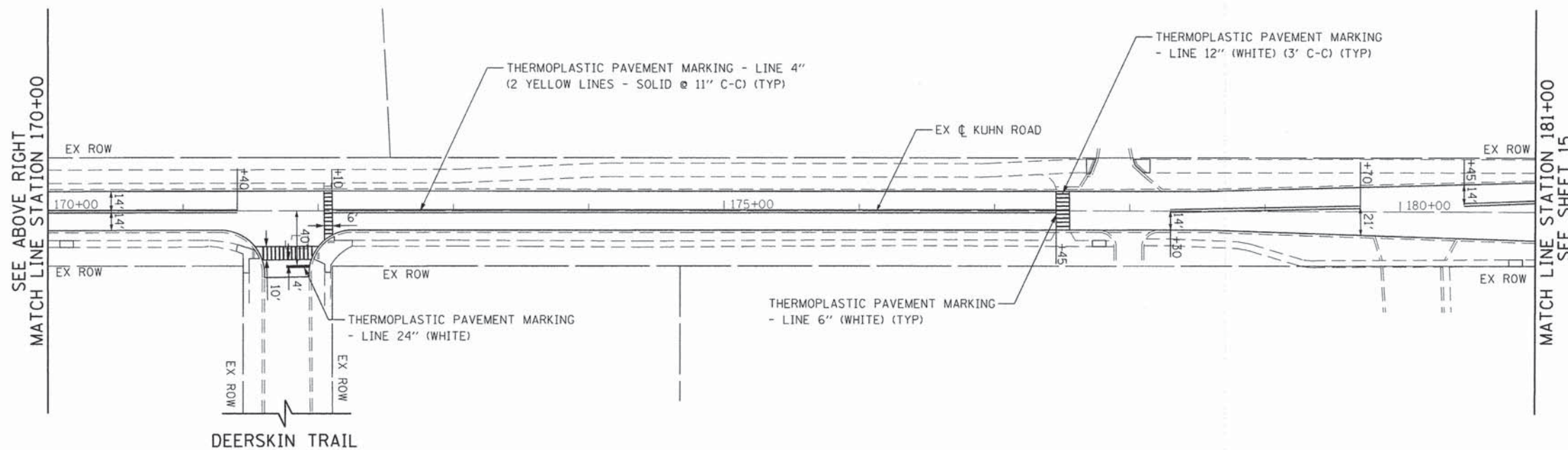
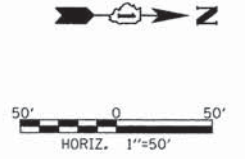
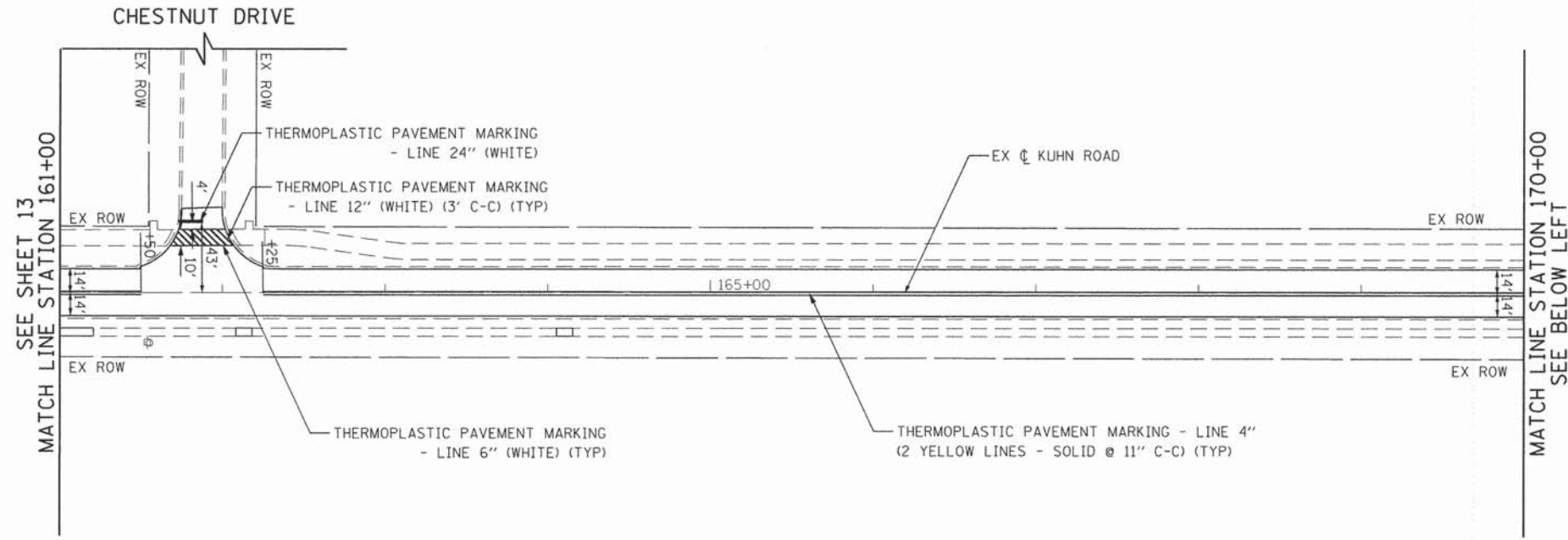
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------|--------|--------------|-----------|
| 2554 | 13-00059-00-RS | DUPAGE | 26 | 12 |
| CONTRACT NO. 63876 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

NOTES: 1) SEE DISTRICT ONE DETAIL TC-13 TYPICAL PAVEMENT MARKINGS FOR GUIDANCE.



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| | PLOT SCALE = 100,000 1/ in. | CHECKED - DWB | REVISED - | | | | | CONTRACT NO. 63876 | | | | |
| #MODELNAME# | PLOT DATE = 12/22/2014 | DATE = 10/17/2014 | REVISED - | SCALE: 1"=50' SHEET 3 OF 5 SHEETS STA. 140+00 TO STA. 161+00 | | | ILLINOIS FED. AID PROJECT | | | | | |

NOTES: 1) SEE DISTRICT ONE DETAIL TC-13 TYPICAL PAVEMENT MARKINGS FOR GUIDANCE.



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 DATE - 10/17/2014

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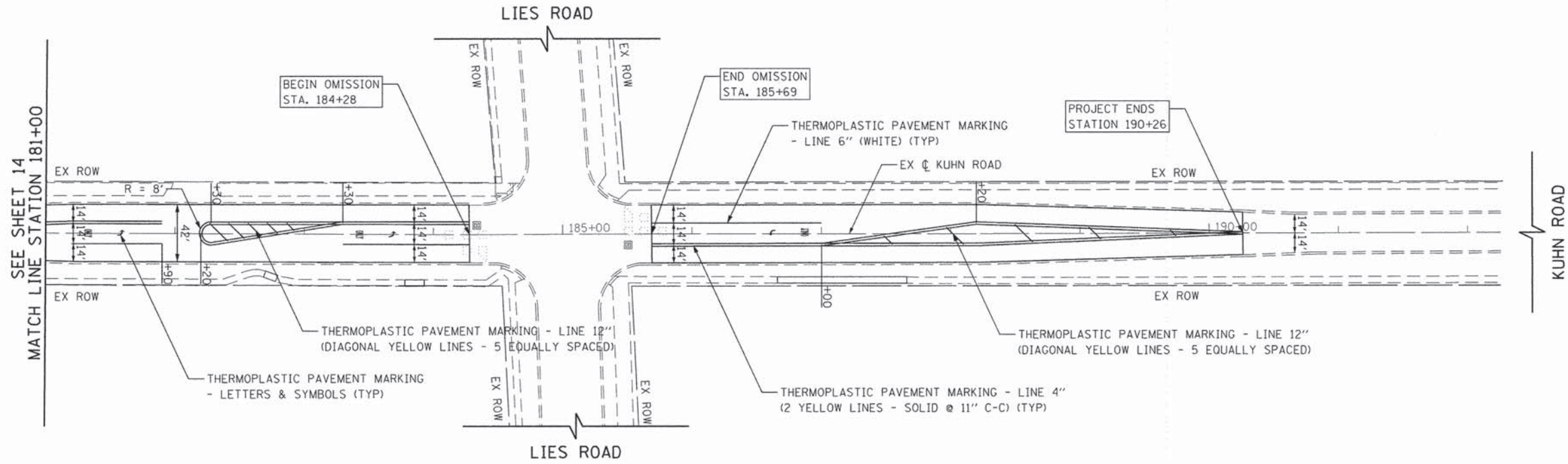
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**KUHN ROAD RESURFACING
 PAVEMENT MARKING PLAN**

SCALE: 1"=50' SHEET 4 OF 5 SHEETS STA. 161+00 TO STA. 181+00

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------|--------|--------------|-----------|
| 2554 | 13-00059-00-RS | DUPAGE | 26 | 14 |
| CONTRACT NO. 63876 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

- NOTES: 1) SEE DISTRICT ONE DETAIL TC-13 TYPICAL PAVEMENT MARKINGS FOR GUIDANCE.
 2) ALL DETECTOR LOOPS SHALL BE DIMENSIONED AS 6' BY 6' UNLESS OTHERWISE SHOWN. SEE DISTRICT ONE DETAIL TS-07 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING FOR GUIDANCE. FOR 14' LANES THE DETECTOR LOOPS SHALL BE CENTERED IN THE LANE.



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 PLOT DATE = 12/22/2014

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 DATE - 10/17/2014

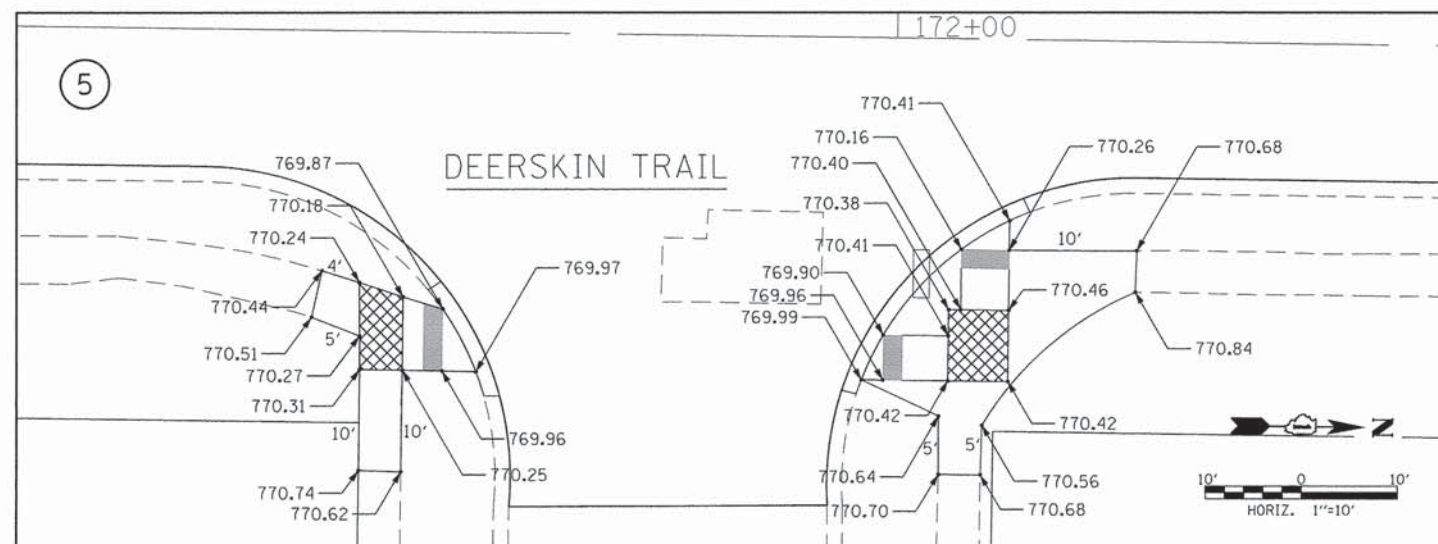
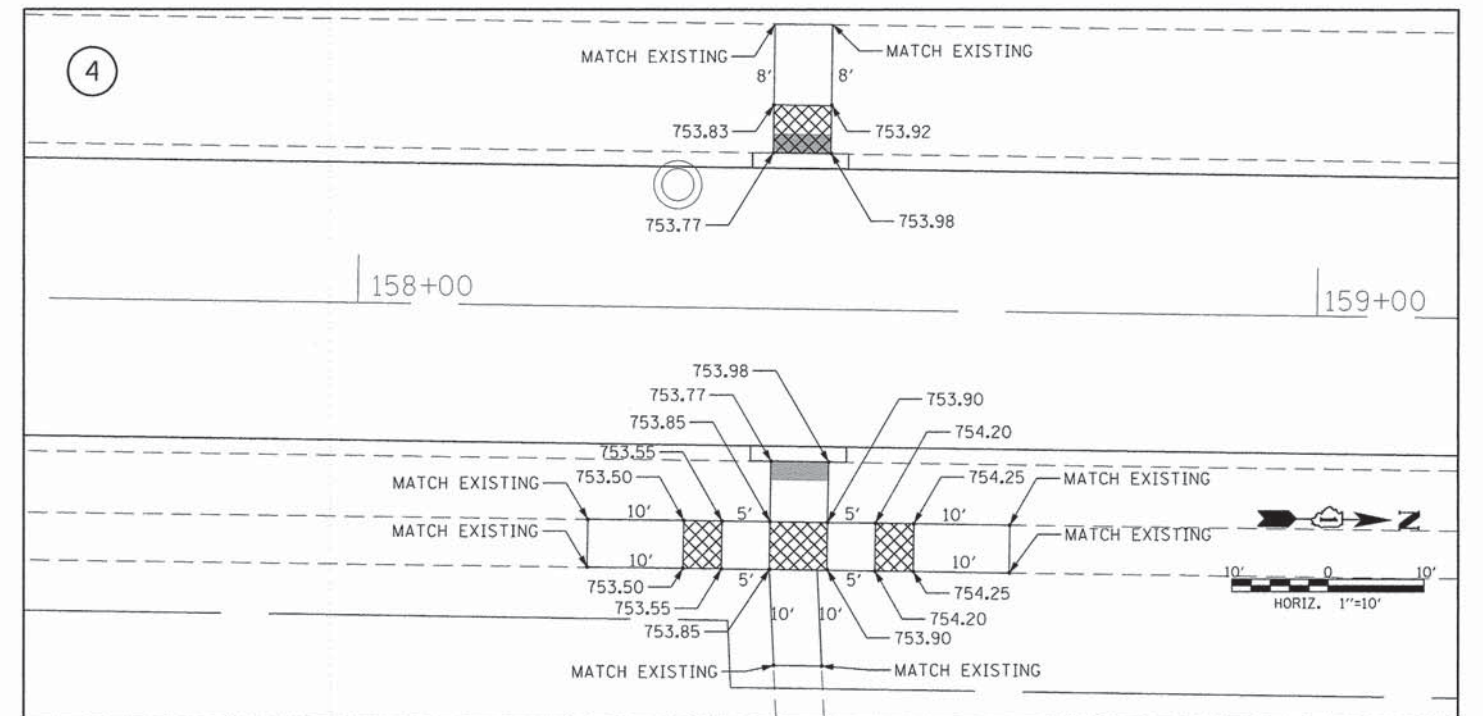
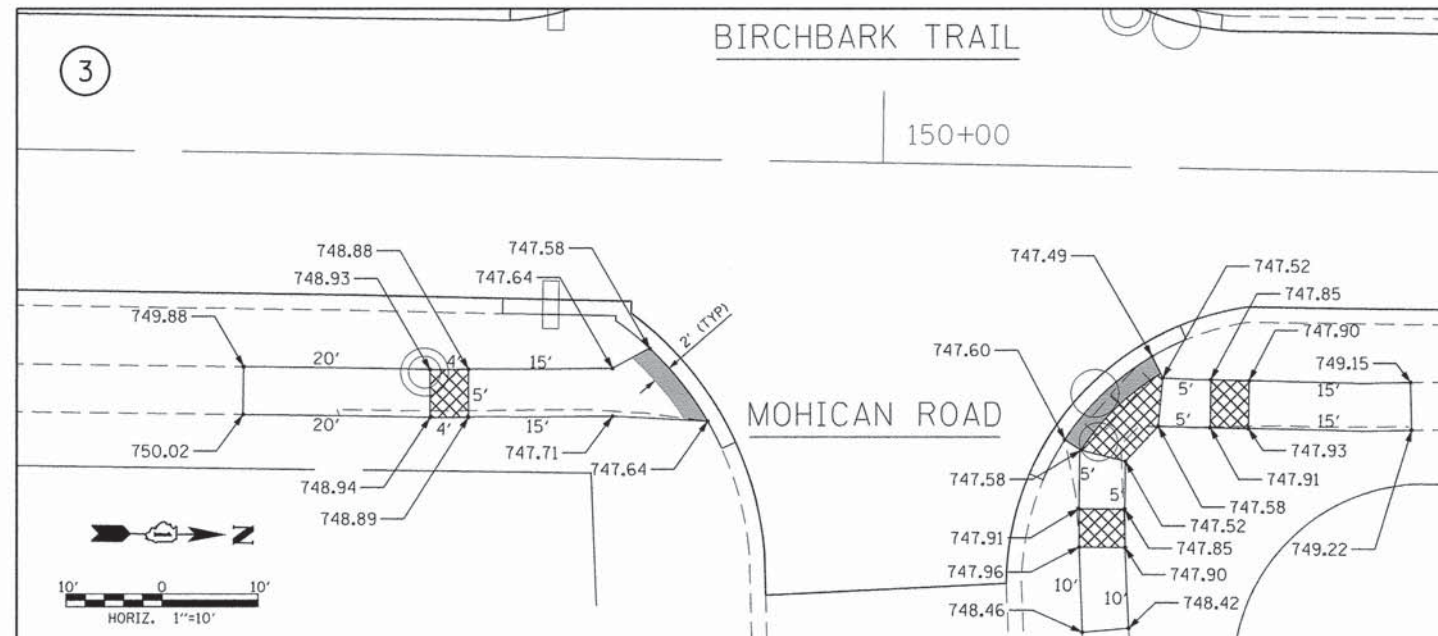
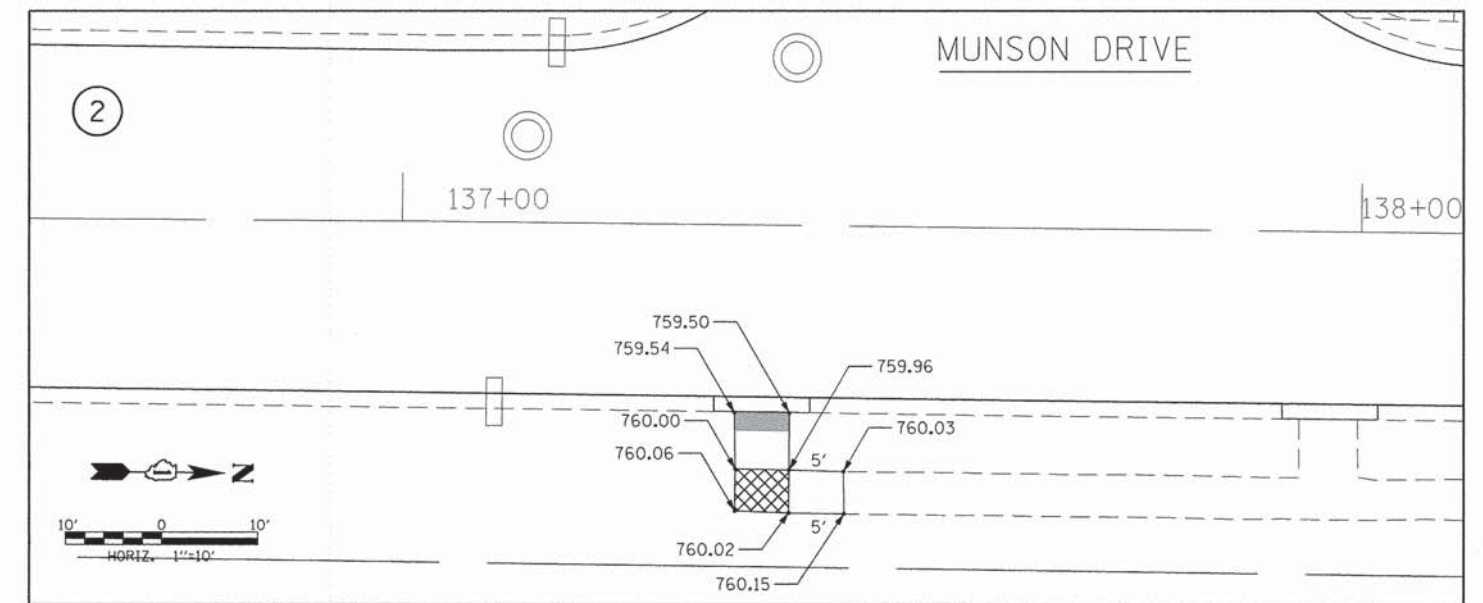
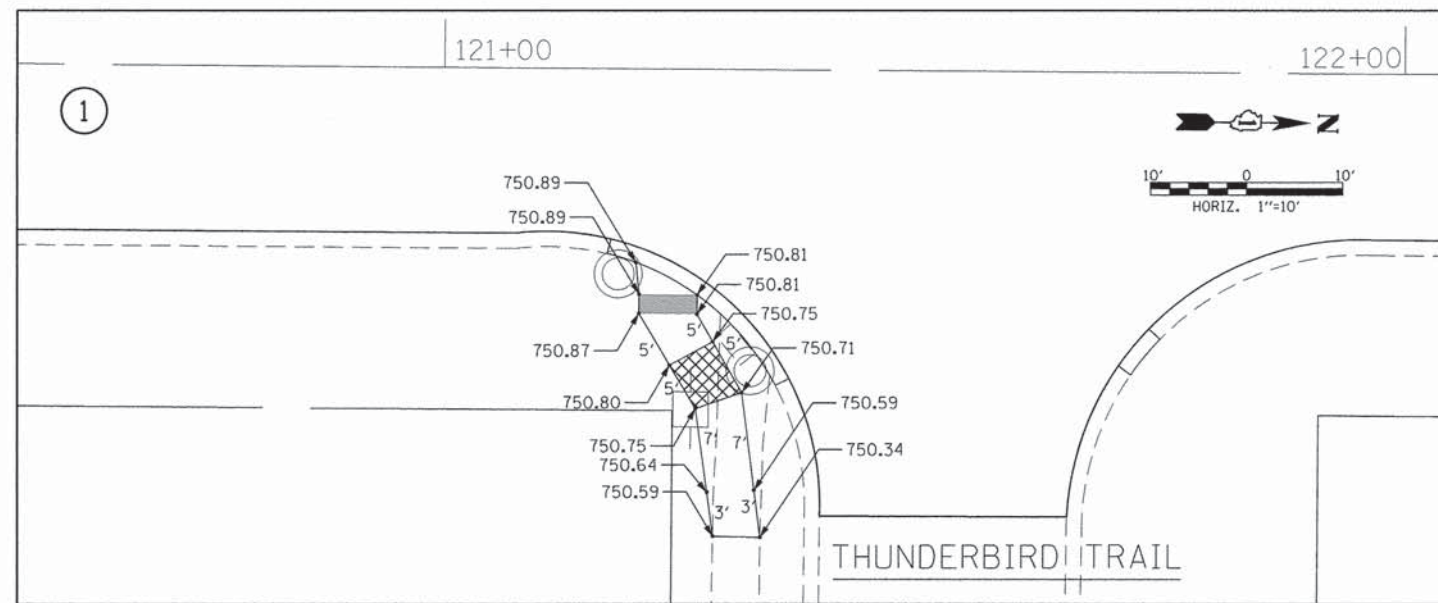
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**KUHN ROAD RESURFACING
 PAVEMENT MARKING PLAN**

SCALE: 1"=50' SHEET 5 OF 5 SHEETS STA. 181+00 TO STA. 192+26

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------|--------|--------------|--------------------|
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| ILLINOIS FED. AID PROJECT | | | | |

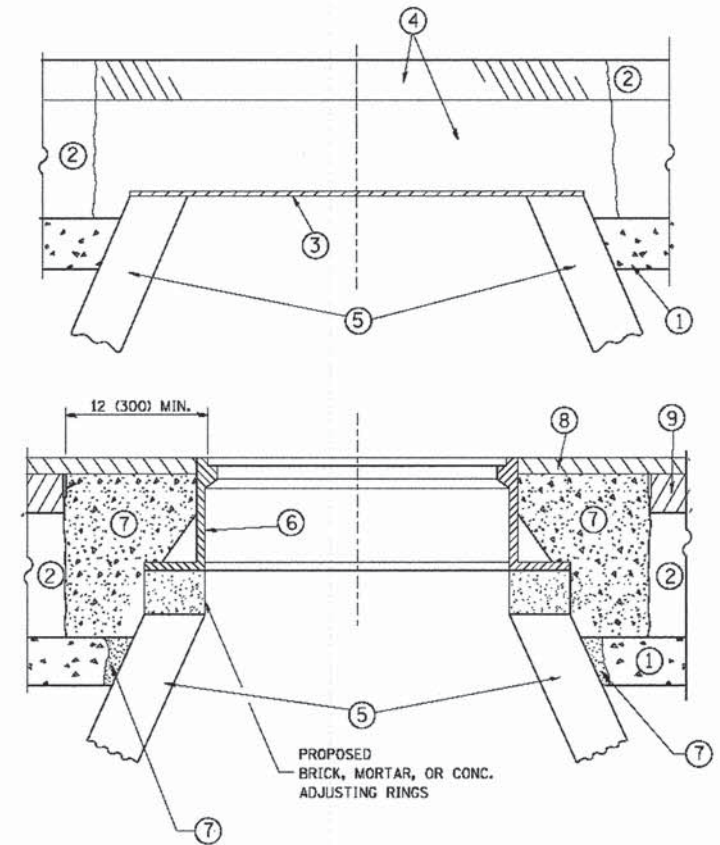


NOTES: 1) THE CONTRACTOR SHALL USE THE EXISTING SIDEWALK ELEVATIONS AT EACH INTERSECTION CORNER AS THE VERTICAL CONTROL FOR CONSTRUCTING THE PROPOSED ELEVATIONS AT THE RESPECTIVE LOCATION. NO OTHER TEMPORARY BENCHMARKS ARE PROVIDED.

LEGEND:  DETECTABLE WARNINGS
 ADA LANDING AREA

| | | | | | | | | | | |
|--|---|------------------------------------|--|---|--|---------------------------|---------------------------|------------------|-----------------------|--------------------|
| FILE NAME = G:\CH09\0109\Road\Sheets\0109-PRJDETAIL | USER NAME = .USER. -L.dgn | DESIGNED - BSH DRAWN - BSH | REVISED - REVISED - REVISED - REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | KUHN ROAD RESURFACING PROJECT DETAILS | F.A.U. RTE. 2554 | SECTION 13-00059-00-RS | COUNTY DUPAGE | TOTAL SHEETS 26 | SHEET NO. 16 |
| *MODELNAME* | PLOT SCALE = 100.0000' / 1" / PLOT DATE = 12/22/2014 | CHECKED - DWB DATE - 10/17/2014 | CONTRACT NO. 63876 | | | ILLINOIS FED. AID PROJECT | | | | |

SCALE: 1"=10' SHEET 1 OF 1 SHEETS STA. TO STA.



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

VILLAGE OF CAROL STREAM CASTINGS ARE THE PROPERTY OF THE VILLAGE AND THE CONTRACTOR SHALL NOTIFY THE VILLAGE FOR REMOVAL AND DISPOSITION OF THE CASTINGS

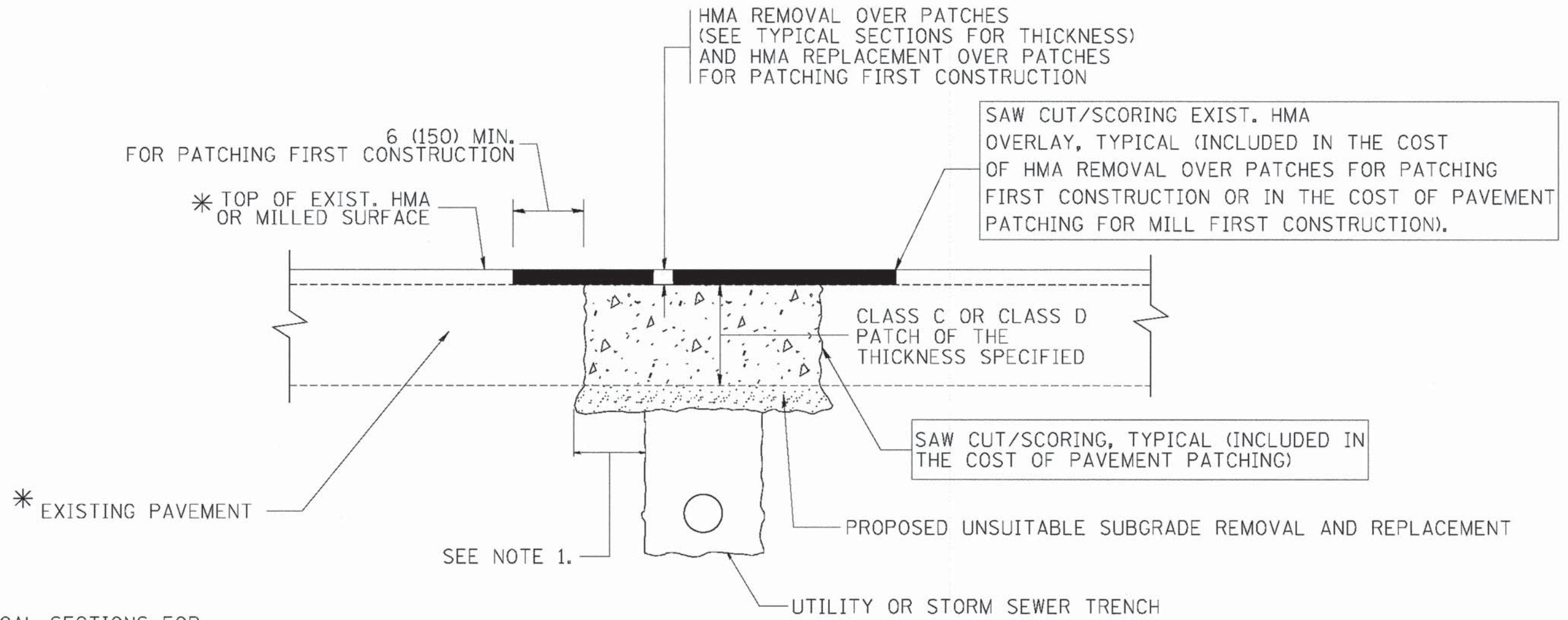
THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

| | | | | | | | | | | | | | |
|---|---------------------|---------------------------|--------------------------------|---|--|-------------------------|------|--------------------|---------|----------------|--------------|-----------|----|
| FILE NAME = | USER NAME = bauerdl | DESIGNED - R. SHAH | REVISED - R. WIEDEMAN 05-14-04 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING | | | F.A.J. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| as\pwork\psdot\bauerdl\0710315\bd08.dgn | | DRAWN - | REVISED - R. BORO 01-01-07 | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | 2554 | 13-00059-00-RS | DUPAGE | 26 | 17 |
| | | PLDT SCALE = 1/8" = 1'-0" | REVISED - R. BORO 03-09-11 | | BD600-03 (BD-8) | | | CONTRACT NO. 63876 | | | | | |
| | | PLDT DATE = 12/6/2011 | REVISED - R. BORO 12-06-11 | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | | | | | |



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

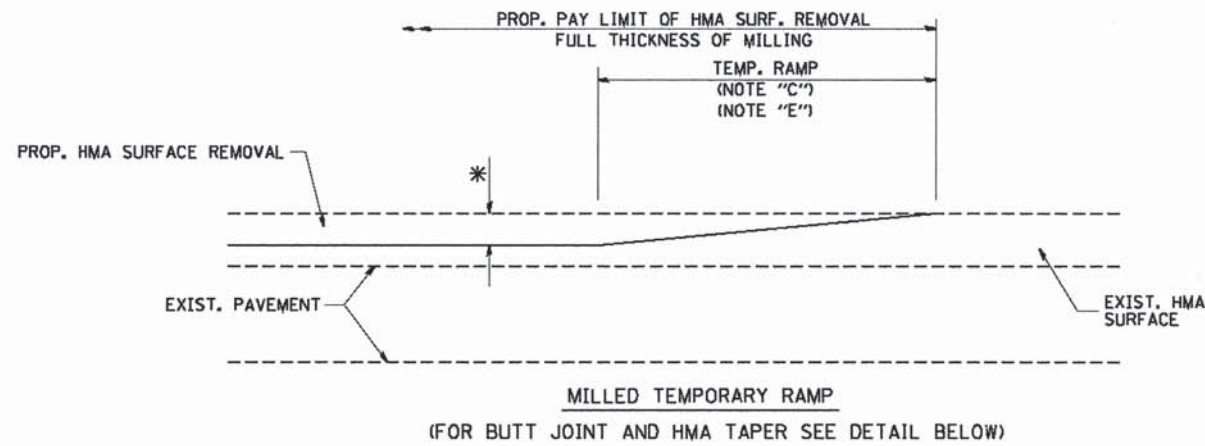
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

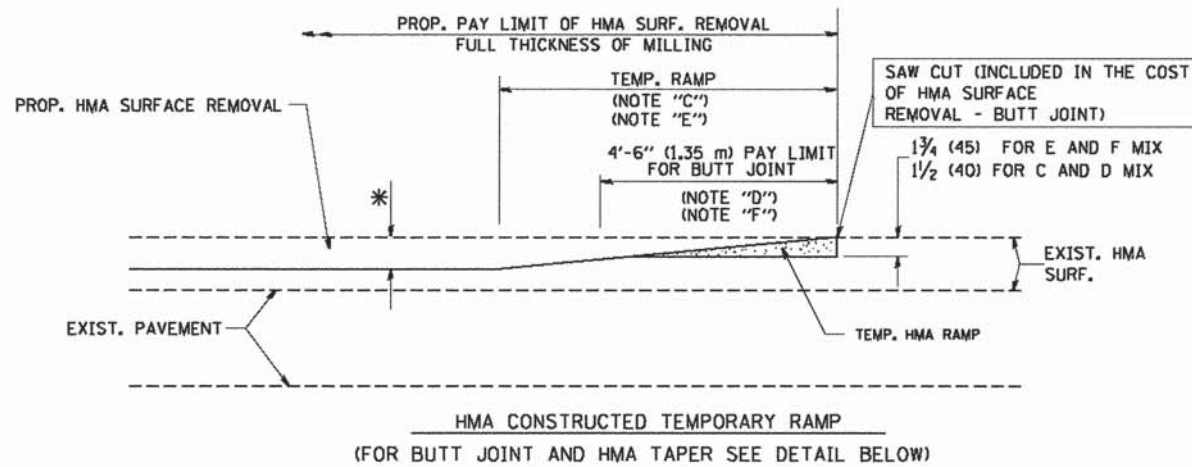
1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

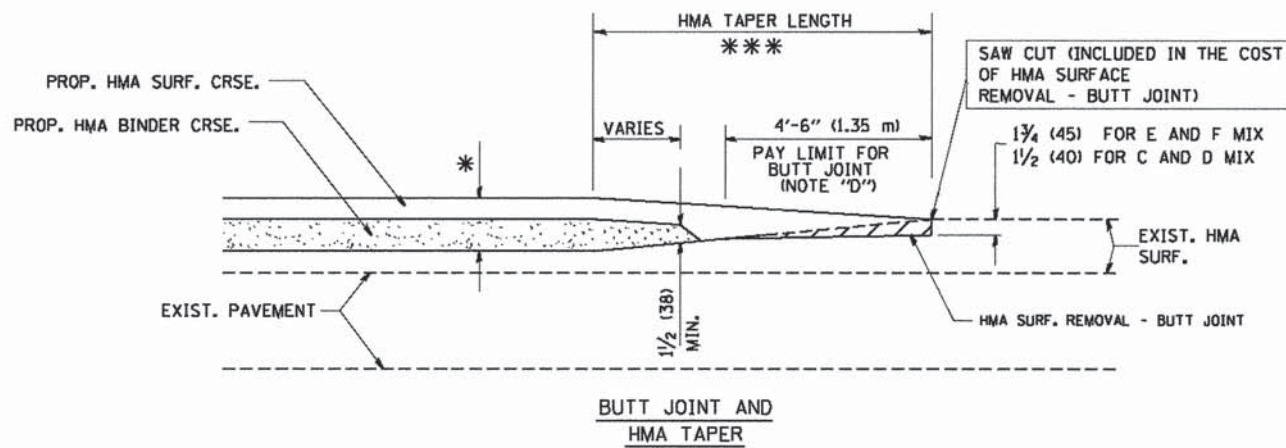
| | | | | | | | | | | | | |
|--|----------------------------|--------------------|-----------------------------|---|--|-------------------------|------|------------------------|---------------------------|------------------|-----------------------|--------------------|
| FILE NAME = c:\projects\diststd22x34\bd22.dgn | USER NAME = bouerd | DESIGNED - R. SHAH | REVISED - A. ABBAS 04-27-98 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT | | | F.A.U. RTE. 2554 | SECTION 13-00059-00-RS | COUNTY DUPAGE | TOTAL SHEETS 26 | SHEET NO. 18 |
| | PLOT SCALE = 50.000' / IN. | CHECKED - | REVISED - R. BORO 01-01-07 | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | BD400-04 (BD-22) | | CONTRACT NO. 63876 | |
| | PLOT DATE = 10/27/2008 | DATE - 10-25-94 | REVISED - K. ENG 10-27-08 | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | | | | |
| | | | | | | | | | | | | |



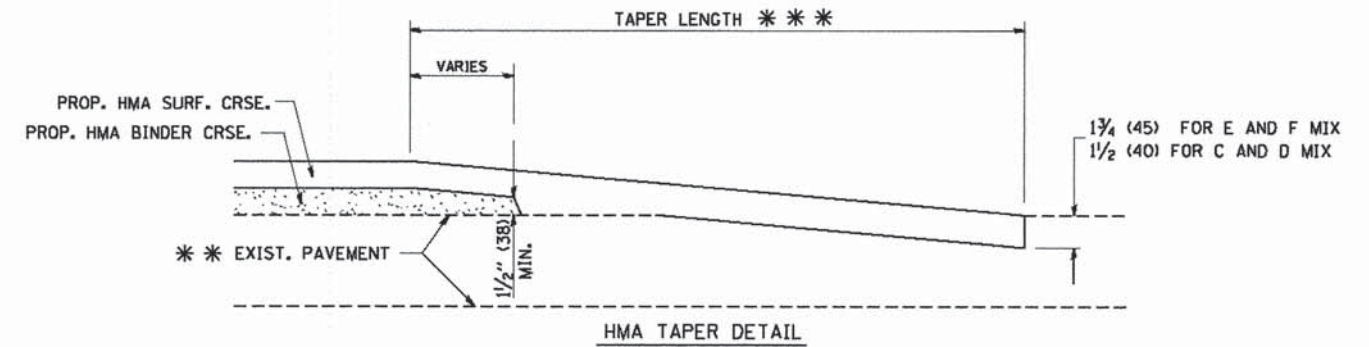
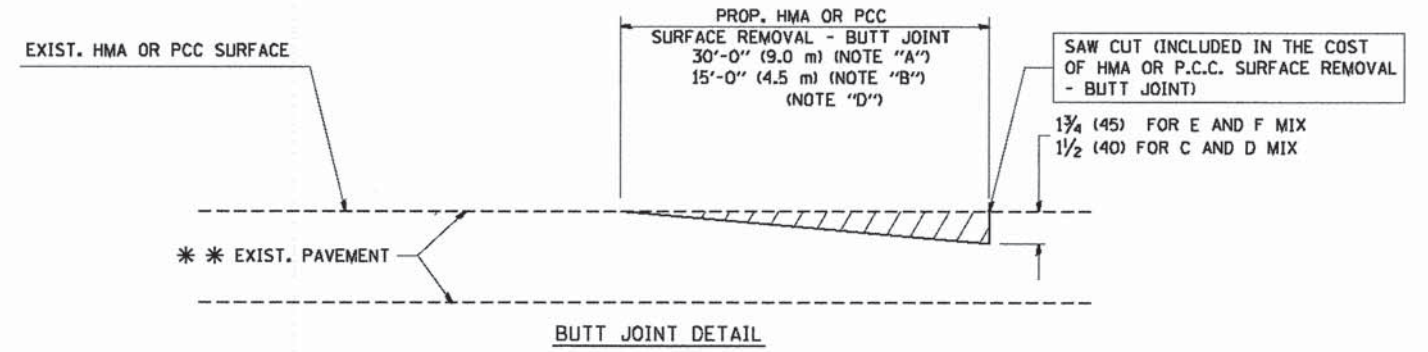
OPTION 1



**OPTION 2
TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

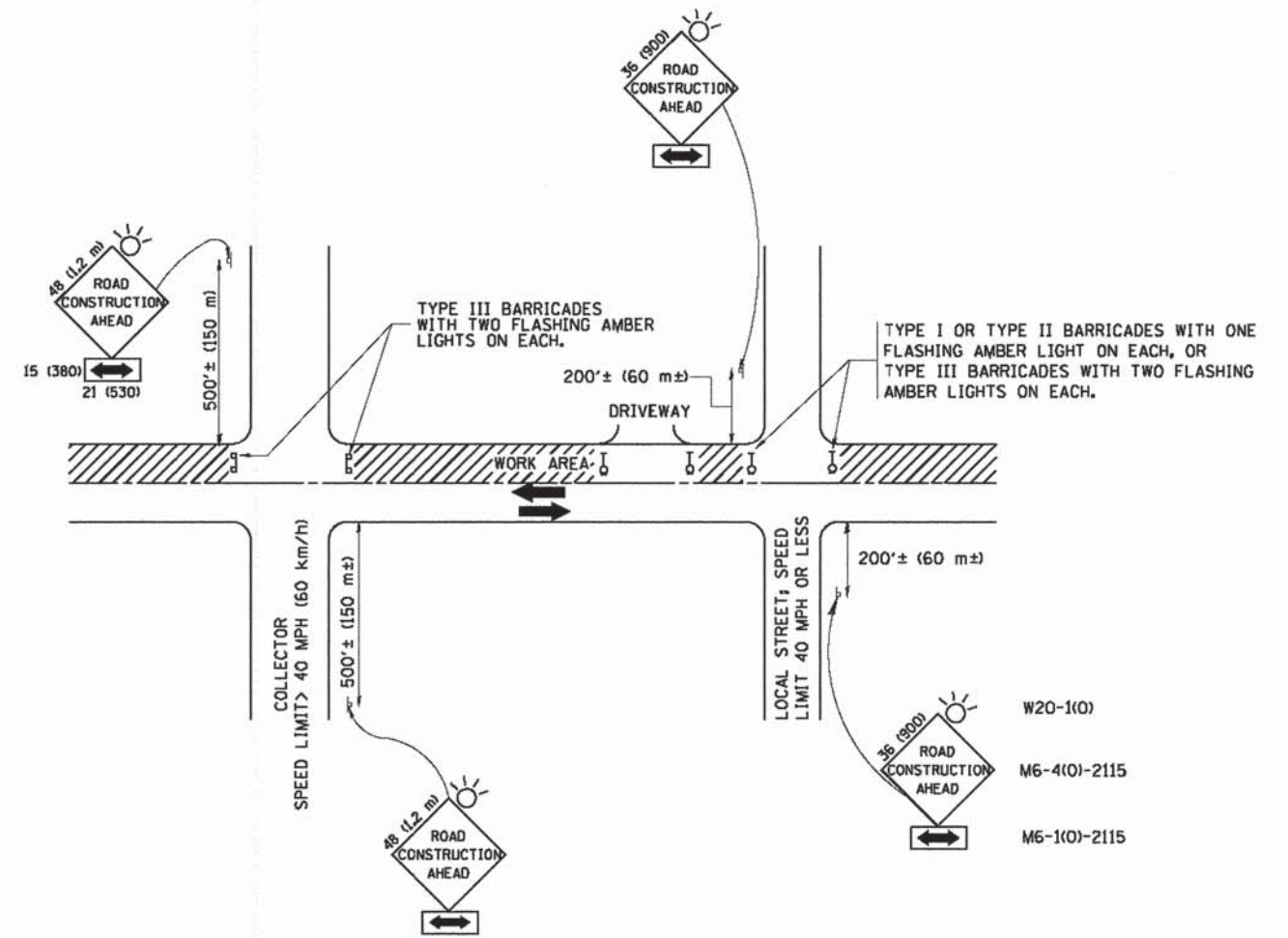
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| | | |
|-----------------------------|-----------------------|-----------------------------|
| USER NAME = geglisenbt | DESIGNED - M. DE YONG | REVISED - R. SHAH 10-25-94 |
| PLOT SCALE = 50.0000' / IN. | DRAWN - | REVISED - A. ABBAS 03-21-97 |
| PLOT DATE = 1/4/2008 | CHECKED - | REVISED - M. GOMEZ 04-06-01 |
| | DATE - 06-13-90 | REVISED - R. BORO 01-01-07 |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| | |
|---|-------------------------|
| BUTT JOINT AND HMA TAPER DETAILS | |
| SCALE: NONE | SHEET NO. 1 OF 1 SHEETS |
| STA. | TO STA. |

| | | | | |
|---|----------------|--------------------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2554 | 13-00059-00-RS | DUPAGE | 26 | 19 |
| BD400-05 BD32 | | CONTRACT NO. 63876 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

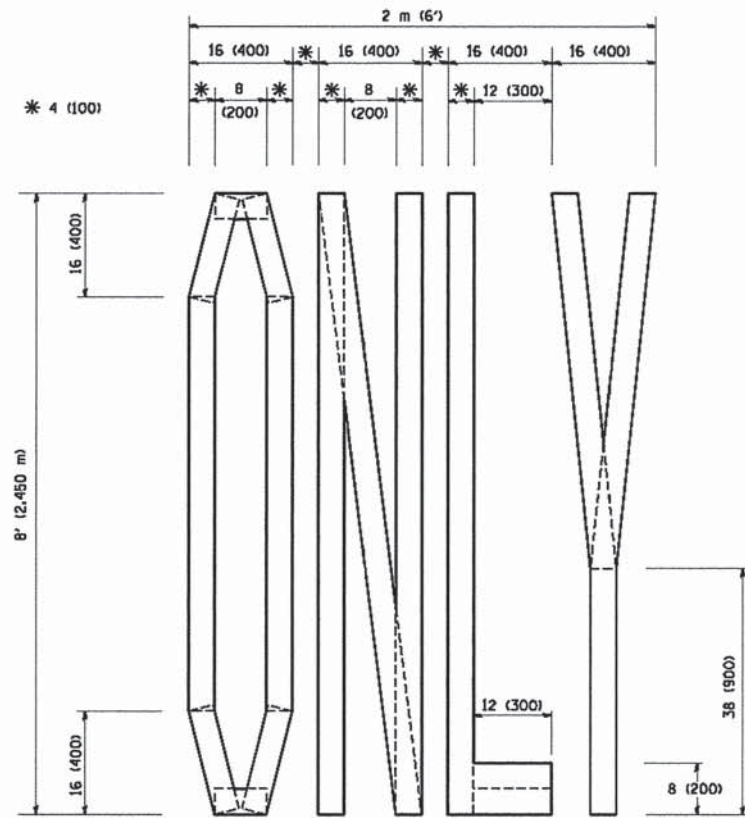
| | | | |
|---|----------------------------|----------------|---------------------------------|
| FILE NAME = W:\diststd\22x34\vol10.dgn | USER NAME = goglianobt | DESIGNED - LHA | REVISED - J. OBERLE 10-18-95 |
| | | DRAWN - | REVISED - A. HOUSEH 03-06-96 |
| | PLOT SCALE = 58.000' / IN. | CHECKED - | REVISED - A. HOUSEH 10-15-96 |
| | PLOT DATE = 1/4/2008 | DATE - 06-89 | REVISED - T. RAMMACHER 01-06-00 |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

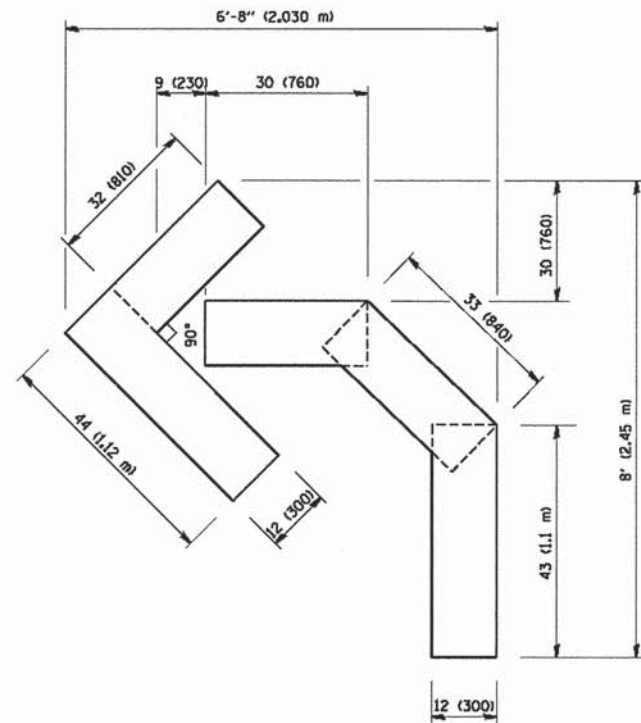
TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

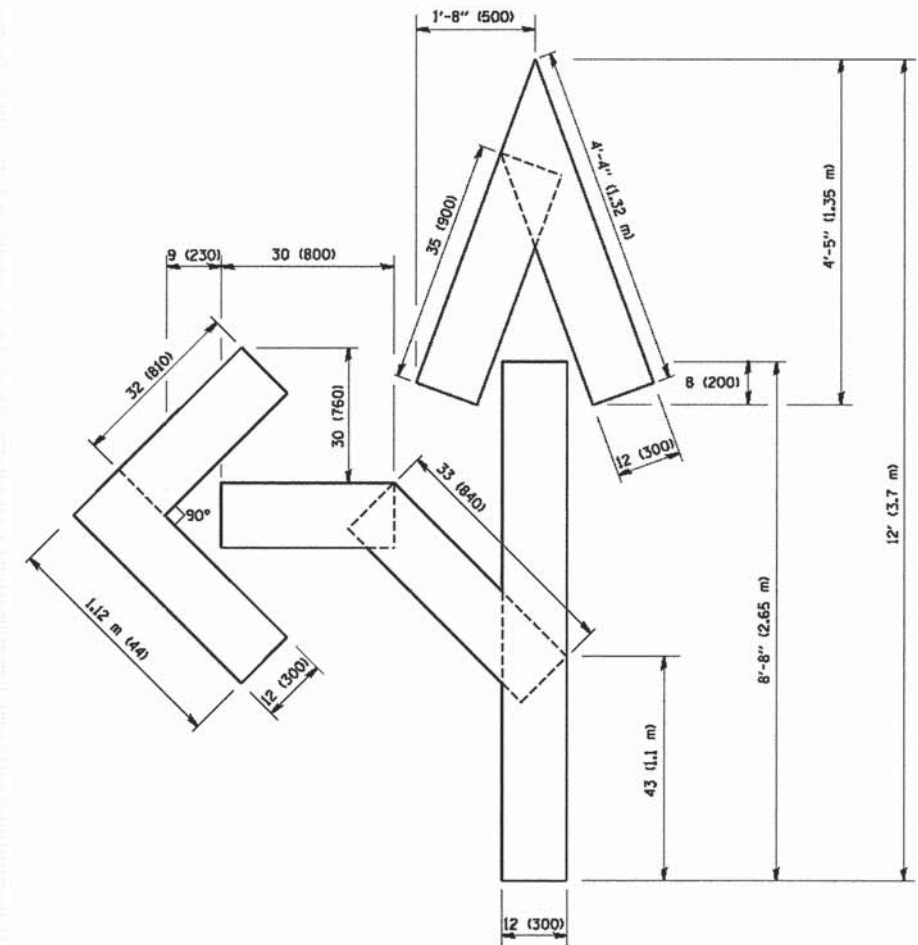
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|---|----------------|--------|--------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2554 | 13-00059-00-R5 | DUPAGE | 26 | 20 |
| TC-10 | | | CONTRACT NO. 63876 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)

All dimensions are in Inches (millimeters)
 unless otherwise shown.

FILE NAME =
 M:\data\td\22x34\td16.dgn

USER NAME = geglert
 PLOT SCALE = 58.0000" / IN.
 PLOT DATE = 1/4/2008

DESIGNED -
 DRAWN -
 CHECKED -
 DATE - 09-18-94

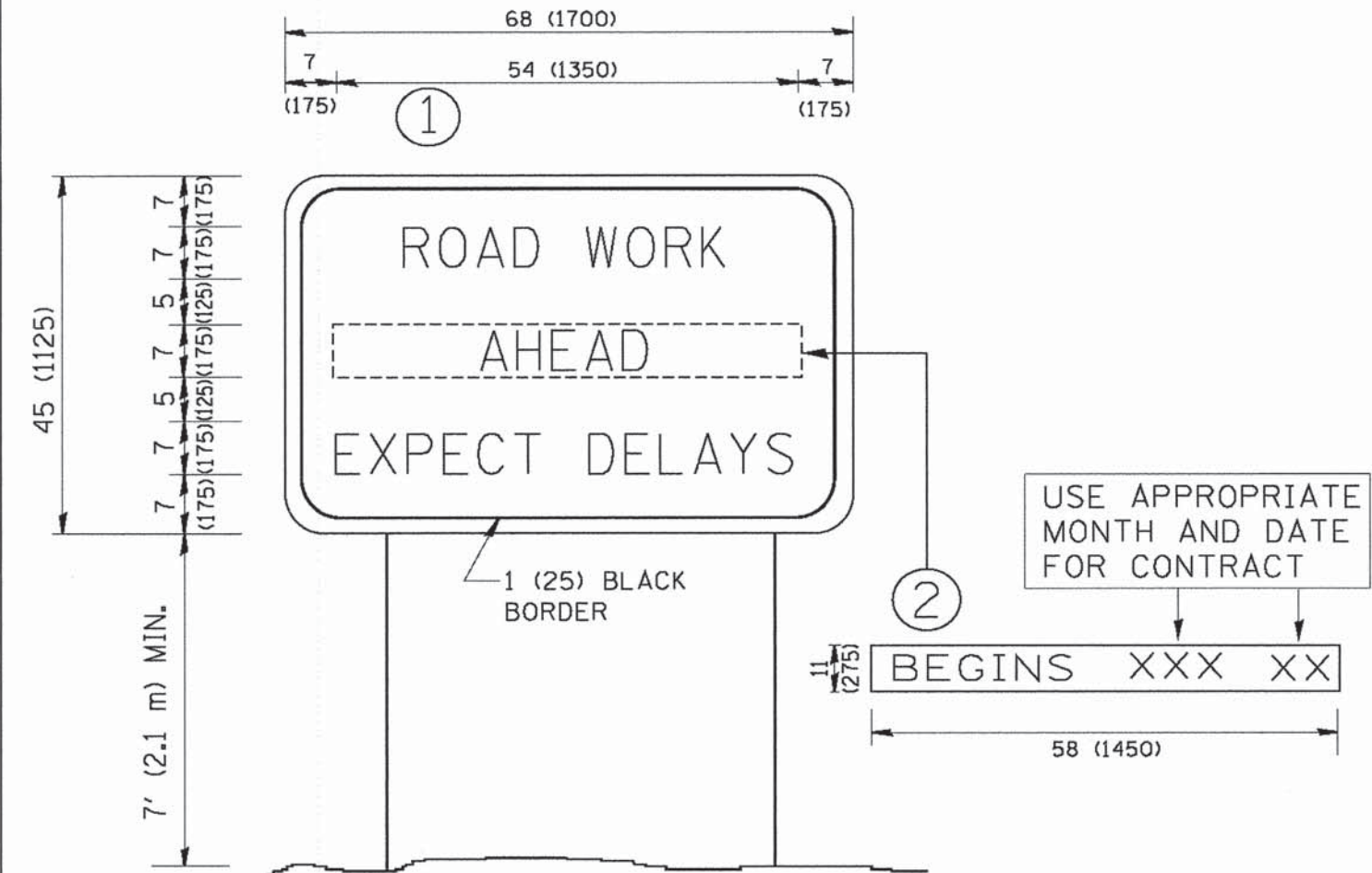
REVISED -T. RAMMACHER 06-05-96
 REVISED -T. RAMMACHER 11-04-97
 REVISED -T. RAMMACHER 03-02-98
 REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|----------------|--------|--------------------|-----------|
| 2554 | 13-00059-00-R5 | DUPAGE | 26 | 22 |
| TC-16 | | | CONTRACT NO. 63876 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| | | | | | | | | | | | | |
|--|----------------------------|------------|---------------------------------|---|---|-------------------------|------------------------|---------------------------|------------------|-----------------------|--------------------|--|
| FILE NAME = W:\disto\22x34\to22.dgn | USER NAME = geglionobt | DESIGNED - | REVISED - R. MIRS 09-15-97 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | ARTERIAL ROAD INFORMATION SIGN | | F.A.U. RTE. 2554 | SECTION 13-00059-00-RS | COUNTY DUPAGE | TOTAL SHEETS 26 | SHEET NO. 23 | |
| | PLOT SCALE = 50,000' / IN. | DRAWN - | REVISED - R. MIRS 12-11-97 | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | TC-22 | | CONTRACT NO. 63876 | |
| | PLOT DATE = 1/4/2008 | CHECKED - | REVISED - T. RAMMACHER 02-02-99 | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | | | | |
| | | DATE - | REVISED - C. JUCIUS 01-31-07 | | | | | | | | | |



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

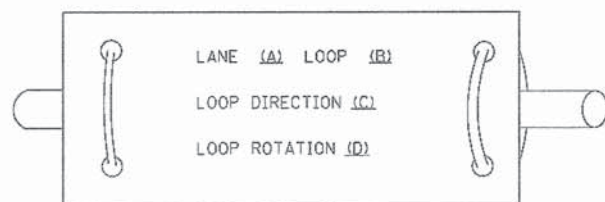
NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

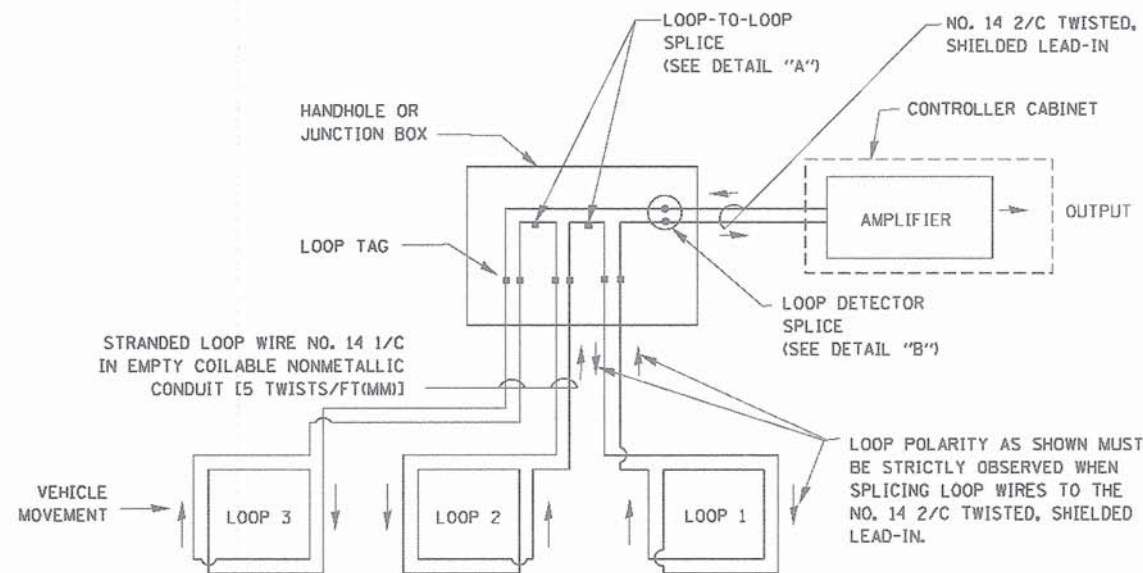
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

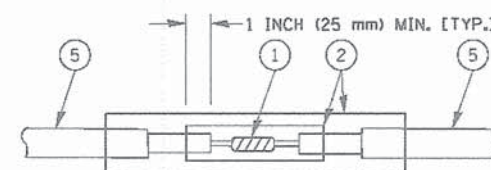


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

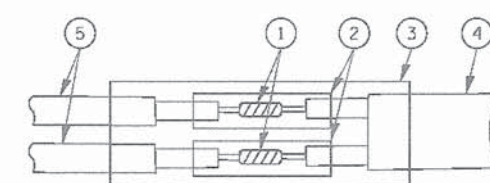


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

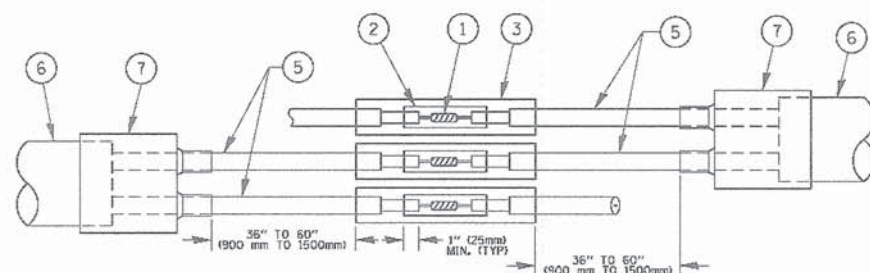


DETAIL "A"
LOOP-TO-LOOP SPLICE

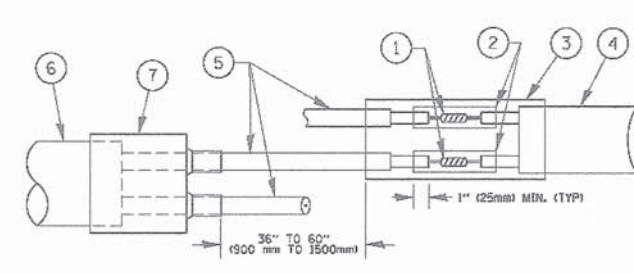


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PREFORMED LOOP

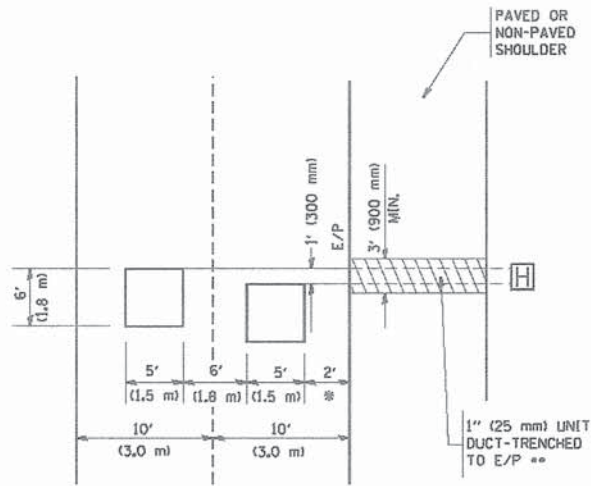
LOOP DETECTOR SPLICE

- ① WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- ② WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- ③ WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- ④ NO. 14 2/C TWISTED, SHIELDED CABLE.
- ⑤ LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- ⑥ PREFORMED LOOP
- ⑦ XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

| | | | | | | | | | | | | |
|---|---------------------|-------------------------|----------------------|---|--|-------------------------|------------------|------------------------|---------------|--------------------|--------------|--|
| FILE NAME = | USER NAME = footmjl | DESIGNED - DAD | REVISED - DAG 1-1-14 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS | | F.A.U. RTE. 2554 | SECTION 13-00059-00-RS | COUNTY DUPAGE | TOTAL SHEETS 26 | SHEET NO. 25 | |
| os:\pw_work\p\idat\footmjl\08310\ts05.dgn | | DRAWN - BCK | REVISED - | | SCALE: NONE | SHEET NO. 2 OF 7 SHEETS | STA. TO STA. | TS-05 | | CONTRACT NO. 63876 | | |
| | | CHECKED - DAD | REVISED - | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | | | | |
| | | PLOT SCALE = 5/8"=1'-0" | DATE - 10-28-09 | | | | | | | | | |

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



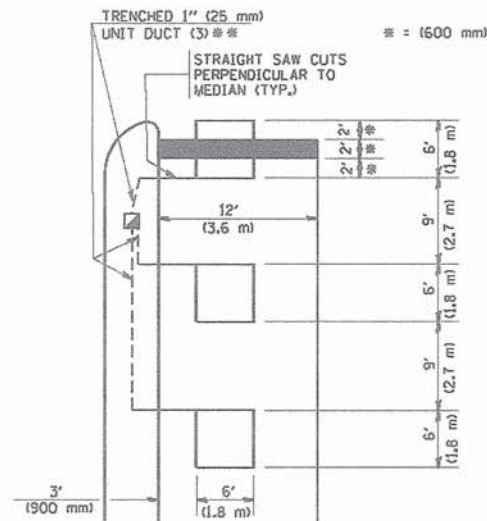
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

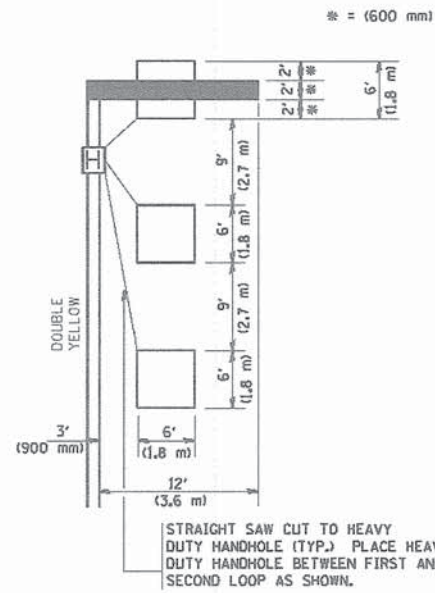


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

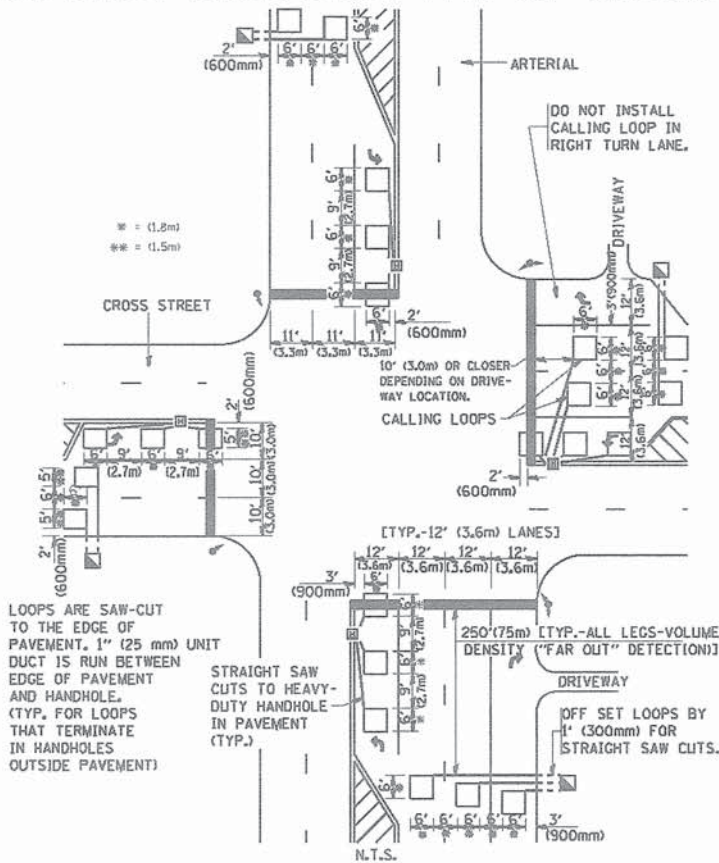
**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)



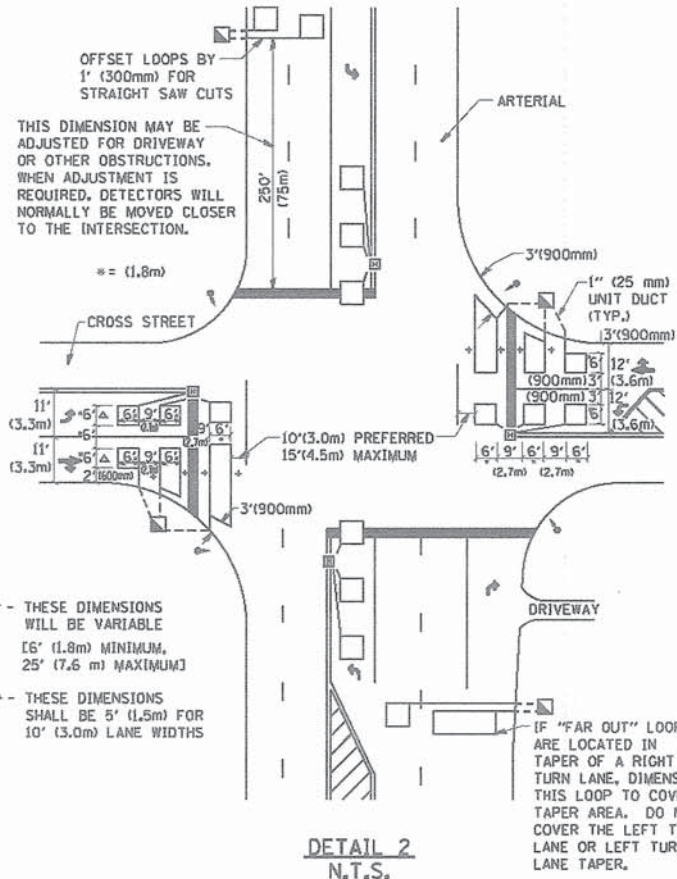
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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| FILE NAME = M:\data\td\22x34\td07.dgn | USER NAME = geglianobt | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING | | | F.A.U RTE. 2554 | SECTION 13-00059-00-R5 | COUNTY DUPAGE | TOTAL SHEETS 26 | SHEET NO. 26 |
| PLOT SCALE = 5/8" = 1" / IN. | CHECKED - R.K.F. | DATE - | REVISED - | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | TS-07 | | CONTRACT NO. 63876 | |
| PLOT DATE = 1/4/2008 | DATE - | REVISED - | REVISED - | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | | | | |
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