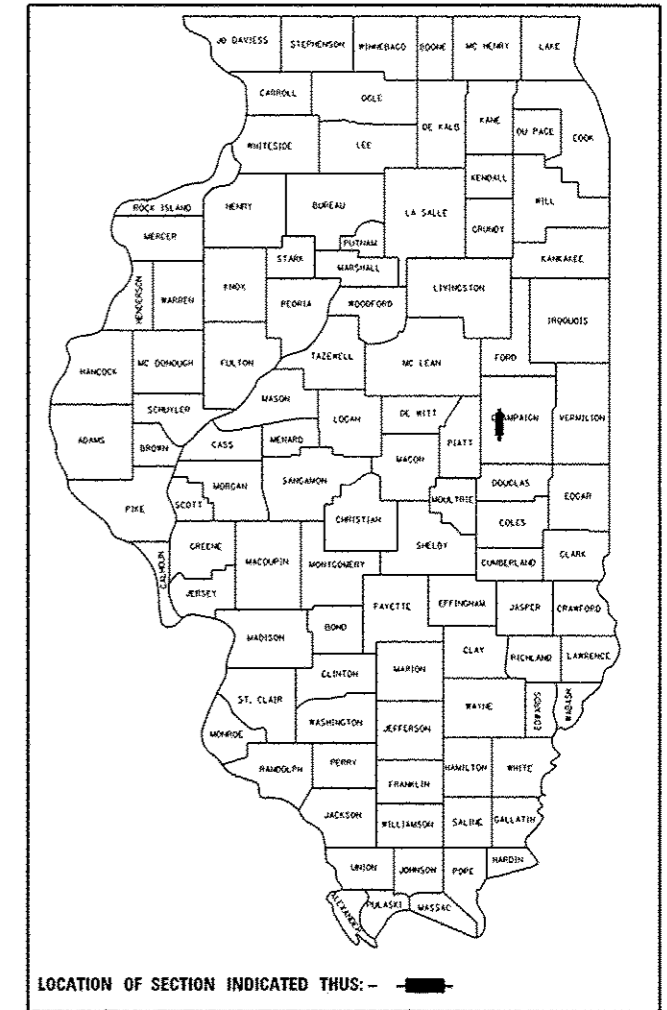


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	1
ILLINOIS			CONTRACT NO. 70844	

D95-013-10

* 474 + 1 = 475 TOTAL SHEETS



CURRENT ADT:
 LEG "A" - NORTH OF US 136 TO TR 95 (CR 2500N) = 3,450 (2014)
 LEG "B" - FROM TR 95 (CR 2500N) TO I-74 = 6,450 (2014)
 LEG "C" - SOUTH OF I-74 = 10,400 (2014)
 OTHER PRINCIPAL ARTERIAL

DESIGN DESIGNATION:
 N/A

FOR INDEX OF SHEETS, SEE SHEET NO. 2
 FOR STRUCTURE INFORMATION, SEE SHEET 6

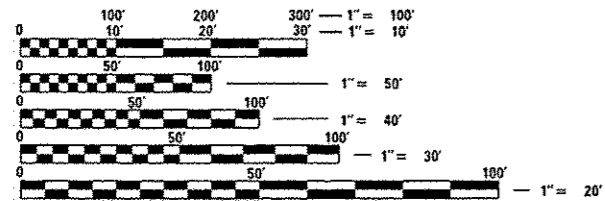
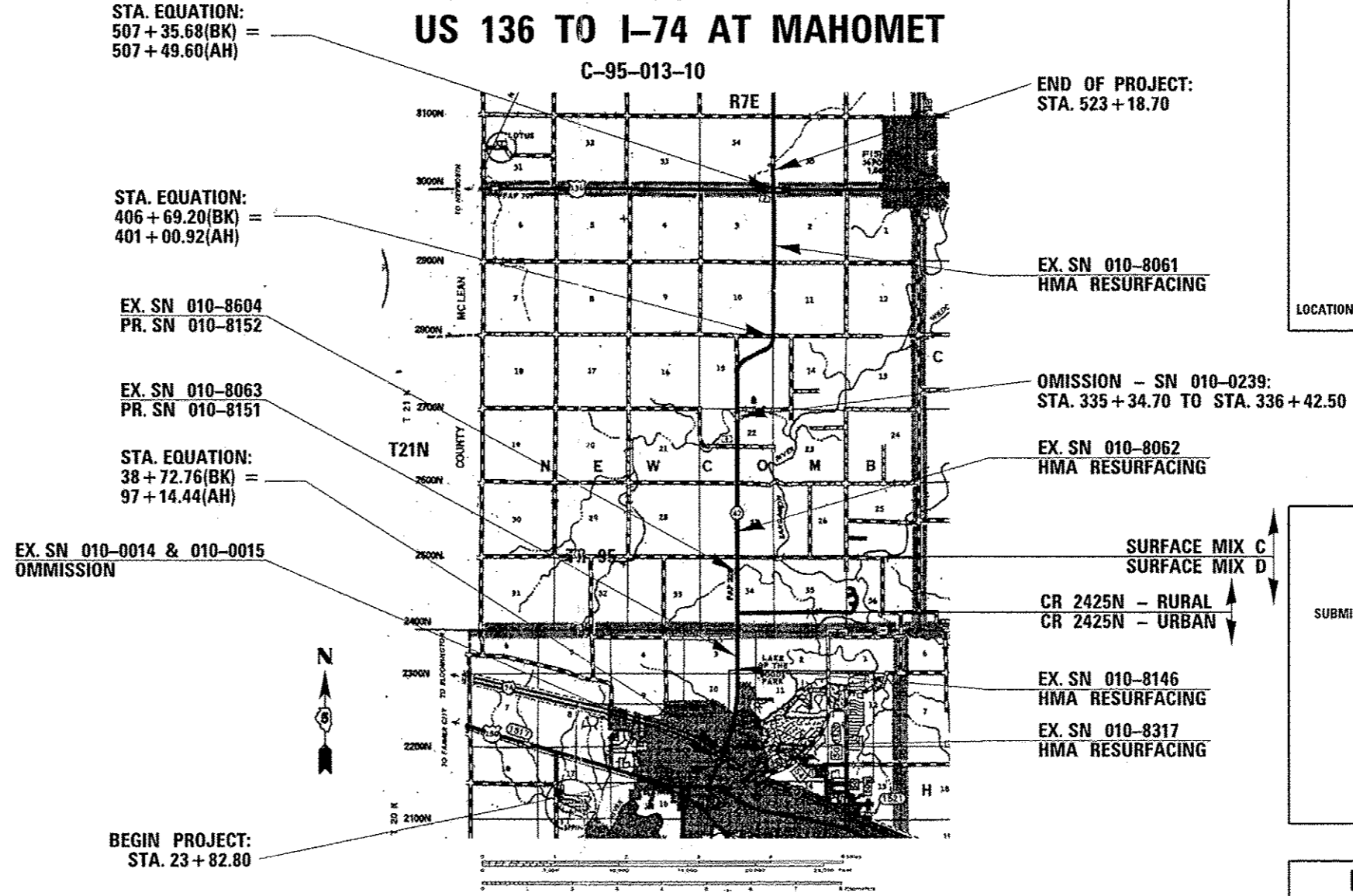
GROSS LENGTH = 44,678.78 FT. = 8.462 MILE
 NET LENGTH = 44,570.98 FT. = 8.441 MILE

DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

**PROPOSED
 HIGHWAY PLANS**

FAP 326 (IL 47)
 SECTION 136RS-3, CR & (10-4)RS-1
 PROJECT ACNHPP-0326 (094)
 RESURFACING (3R)
 CHAMPAIGN COUNTY
 US 136 TO I-74 AT MAHOMET

C-95-013-10



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123
 OR 811
 TOWNSHIPS: MAHOMET, NEWCOMB, & BROWN

PROJECT ENGINEER: NANCY FASIG
 PROJECT MANAGER: BRENT CEARLOCK
 PHONE: (217)465-4181
 CONTRACT NO. 70844

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

SUBMITTED October 16, 2014
Pamela Sanders
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

Jan 30, 2015
John D. Baranzelli, P.E.
 acting ENGINEER OF DESIGN AND ENVIRONMENT

Jan 30, 2015
Omer Osman, P.E.
 DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
 OF THE STATE OF ILLINOIS

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LIST OF STANDARDS

STANDARD NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
280001-07	EROSION CONTROL SYSTEMS, TEMPORARY
285001-02	FABRIC FORMED CONCRETE REVETMENT MATS
353001-04	PCC BASE COURSE WITH HMA BINDER AND SURFACE COURSES
406201-01	MAILBOX TURNOUTS
420001-08	PAVEMENT JOINTS
424021-03	DEPRESSED CORNER FOR SIDEWALKS
424026-01	ENTRANCE/ALLEY PEDESTRIAN CROSSING
442201-03	CLASS C AND D PATCHES
482011-03	HMA SHOULDER STRIPS (SHLDS) WITH RESURFACING OR WIDENING FOR WIDENING & RESURFACING PROJECTS
515001-03	NAMEPLATE FOR BRIDGES
542301-03	END SECTION, FLARED, PRECAST REINFORCED CONCRETE, ROUND
542306-02	END SECTION, FLARED, PRECAST REINFORCED CONCRETE, ELLIPTICAL
542401-01	END SECTION, METAL, FOR PIPE CULVERT
542406-01	END SECTION, METAL, FOR PIPE ARCH
542546-01	FLUSH INLET BOX FOR MEDIAN
602301-04	INLET TYPE A
602401-03	MANHOLE, TYPE A
602406-06	MANHOLE, TYPE A, 6' DIAMETER
602601-03	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
604001-04	FRAME AND LIDS, TYPE 1
604036-03	GRATE, TYPE 8
606001-06	CURB TYPE B AND COMBINATION CURB AND GUTTER, CONCRETE
606201-02	TYPE B GUTTER (INLET, OUTLET & ENTRANCE)
606301-04	PC CONCRETE ISLANDS AND MEDIANS
630001-10	STEEL PLATE BEAM GUARDRAIL
630101-09	GUARDRAIL MOUNTED ON EXISTING CULVERTS
630301-06	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
631031-13	TRAFFIC BARRIER TERMINAL, TYPE 6
635001-01	DELINEATORS
635006-03	REFLECTOR FOR TERMINAL MARKER PLACEMENT
635011-02	REFLECTOR MARKER AND MOUNTING DETAILS
642006	SHOULDER RUMBLE STRIPS, 8 IN.
666001-01	ROW MARKERS
667101-02	PERMANENT SURVEY MARKERS
701001-02	OFF-RD OPERATIONS, 2L, 2W, MORE THAN 15' (4.5 m) AWAY
701006-05	OFF RD OPERATIONS 2L, 2W, 15' TO 24' FROM PAVEMENT EDGE
701011-04	OFF RD MOVING OPERATIONS 2L, 2W, DAY ONLY
701201-04	LANE CLOSURE, 2L 2W, DAY ONLY FOR SPEEDS 45 MPH
701206-03	LANE CLOSURE, 2L 2W, NIGHT ONLY FOR SPEEDS 45 MPH
701301-04	LANE CLOSURE, 2L 2W, SHORT TIME OPERATIONS
701306-03	LANE CLOSURE, 2L 2W, SLOW MOVING OPERATIONS 45 MPH
701311-03	LANE CLOSURE, 2L 2W, MOVING OPERATIONS, DAY ONLY
701326-04	LANE CLOSURE, 2L 2W PAVEMENT WIDENING 45 MPH
701502-06	URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
701701-09	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-04	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
720011-01	METAL POSTS FOR SIGNS, MARKERS & DELINEATORS
728001-01	TELESCOPING STEEL SIGN SUPPORT
729001-01	APPLICATIONS OF TYPES A & B METAL POSTS (FOR SIGNS & MARKERS)
731001-01	BASE FOR TELESCOPING STEEL SIGN SUPPORT
780001-05	TYPICAL PAVEMENT MARKINGS
781001-03	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
805001-01	ELECTRIC SERVICE INSTALLATION DETAILS
814001-03	HANDHOLES
873001-02	TRAFFIC SIGNAL GROUNDING & BONDING
878001-10	CONCRETE FOUNDATION DETAILS
880001-01	SPAN WIRE MOUNTED SIGNALS AND FLASHING BEACON INSTALLATION
880006-01	TRAFFIC SIGNAL MOUNTING DETAILS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	DETECTION LOOPS, TYPICAL INSTALLATIONS

GENERAL NOTES

G. N. -100

ENGLISH UNITS OF MEASUREMENT SHALL GOVERN OVER AND SUPERSEDE ANY METRIC UNITS SHOWN IN THIS CONTRACT. WHERE INCLUDED, METRIC UNITS ARE FOR INFORMATION ONLY.

G. N. -100B

MICROSTATION AND GEOPAK FILES OF THIS PROJECT WILL BE MADE AVAILABLE TO THE CONTRACTOR AFTER CONTRACT AWARD. IF THERE IS A CONFLICT BETWEEN THE ELECTRONIC FILES AND THE PRINTED CONTRACT PLANS AND DOCUMENTS, THE PRINTED CONTRACT PLANS AND DOCUMENTS SHALL TAKE PRECEDENCE OVER THE ELECTRONIC FILES. THE CONTRACTOR SHALL ACCEPT ALL RISK ASSOCIATED WITH USING THE ELECTRONIC FILES AND SHALL HOLD THE DEPARTMENT HARMLESS FOR ANY ERRORS OR OMISSIONS IN THE ELECTRONIC FILES AND THE DATA CONTAINED THEREIN. ERRORS OR DELAYS RESULTING FROM THE USE OF THE ELECTRONIC FILES BY THE CONTRACTOR SHALL NOT RESULT IN AN EXTENSION OF TIME FOR ANY INTERIM OR FINAL COMPLETION DATE OR SHALL NOT BE CONSIDERED CAUSE FOR ADDITIONAL COMPENSATION. THE CONTRACTOR SHALL NOT USE, SHARE, OR DISTRIBUTE THESE ELECTRONIC FILES EXCEPT FOR THE PURPOSE OF CONSTRUCTING THIS CONTRACT. ANY CLAIMS BY THIRD PARTIES DUE TO USE OR ERRORS SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL INCLUDE THIS DISCLAIMER WITH THE TRANSFER OF THESE ELECTRONIC FILES TO ANY OTHER PARTIES AND SHALL INCLUDE APPROPRIATE LANGUAGE BINDING THEM TO SIMILAR RESPONSIBILITIES.

G. N. -105.07

EXISTING STATE-OWNED AND MAINTAINED UTILITY LINES ARE SHOWN ON THE PLANS TO INDICATE THEIR PRESENCE AND APPROXIMATE LOCATION. THE CONTRACTOR SHALL NOTIFY THE DISTRICT OPERATIONS ENGINEER TWO WEEKS PRIOR TO COMMENCING ANY EXCAVATION IN THE VICINITY OF THESE LINES. THE STATE WILL THEN LOCATE AND MARK THE HORIZONTAL LOCATIONS OF THE LINES AND PROVIDE ANY AVAILABLE INFORMATION AS TO THEIR DEPTH. SHOULD ANY OF THE LINES BE DAMAGED BY THE CONTRACTOR'S OPERATION, THE CONTRACTOR SHALL REPAIR THEM TO THE SATISFACTION OF THE ENGINEER AND AT NO COST TO THE STATE.

ALSO THERE MAY BE UTILITIES PRESENT WHICH WERE INSTALLED BY THE STATE BUT ARE MAINTAINED BY OTHERS (CITY, TOWN, ETC.) THE APPROXIMATE LOCATIONS OF THESE LINES ARE ALSO SHOWN ON THE PLANS ALONG WITH THE NAME OF THE MAINTAINING AGENCY. THE CONTRACTOR SHALL COORDINATE THE LOCATING OF THESE LINES WITH THE LOCAL AGENCY PRIOR TO COMMENCING ANY EXCAVATION OR BORING IN THEIR VICINITY. SHOULD THESE LINES BE DAMAGED BY THE CONTRACTOR'S OPERATIONS, THE CONTRACTOR SHALL REPAIR THEM TO THE SATISFACTION OF, AND AT NO COST TO, THE LOCAL AGENCY AND THE STATE.

G. N. -105.09A

ALL ELEVATIONS SHOWN IN THE PLANS ARE BASED ON NORTH AMERICAN VERTICAL DATUM OF 1988. (NAVD 88)

G. N. -107.37

UTILITY LINES WERE PLOTTED FROM INFORMATION FURNISHED BY THE VARIOUS UTILITY COMPANIES INVOLVED (QUALITY LEVEL C &/OR QUALITY LEVEL D) AND THE ACCURACY SHOULD BE CONSIDERED APPROXIMATE ONLY.

UTILITY COMPANIES MAY BE ADJUSTING THEIR FACILITIES DURING CONSTRUCTION. THE CONTRACTOR SHALL COOPERATE WITH THESE ORGANIZATIONS WHILE THESE ADJUSTMENTS ARE BEING PERFORMED. J. U. L. I. E. - JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS SYSTEM (800) 892-0123 OR 811.

G. N. -201

TREES THAT INTERFERE WITH THE CONSTRUCTION OPERATIONS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER. ANY TREE DUE TO ITS LOCATION AND DEEMED SUITABLE FOR SAVING BY THE ENGINEER SHALL BE PROTECTED DURING CLEARING AND SUBSEQUENT CONSTRUCTION OPERATIONS.

G. N. -202

GRADING SHALL BE DONE BY HAND AROUND LIGHT POLES, UTILITY POLES, THAT THE LIMITS OF CONSTRUCTION BE SUCH AS TO PRESERVE IN THE ORIGINAL STATE AS MUCH AREA OF TEMPORARY EASEMENTS AS POSSIBLE. THE DECISION AS TO ITEMS TO REMAIN IN PLACE SHALL BE AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE CONTRACT UNIT PRICE PER CUBIC YARD FOR EARTH EXCAVATION AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

G. N. -250C

SEEDING, CLASS 7 AND MULCH, METHOD 2 IS INCLUDED IN THIS CONTRACT TO SEED NEW EARTH SHOULDERS DURING TIME PERIODS WHEN PERMANENT SEEDING IS NOT ALLOWED. SOME OR ALL OF THE CLASS 7 SEEDING AND MULCH WILL BE DELETED IF IT IS POSSIBLE TO PLACE PERMANENT SEEDING ON EARTH SHOULDERS AT THE TIME OF THEIR COMPLETION.

G. N. -280

TEMPORARY EROSION CONTROL SEEDING IS INCLUDED IN THIS CONTRACT TO SEED DISTURBED EARTH DURING TIME PERIODS WHEN PERMANENT SEEDING IS NOT ALLOWED. SOME OR ALL OF THE TEMPORARY EROSION CONTROL SEEDING WILL BE DELETED IF IT IS POSSIBLE TO PLACE PERMANENT SEEDING ON EARTH AT THE TIME OF THEIR COMPLETION.

FILE NAME *	USER NAME * ceerlookbn	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwwork\ceerlookbn\01876121\070844-shl-gemote.dgn	PLOT SCALE * 40.0000 / in.	DRAWN -	REVISED -			326	136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	3
Default	PLOT DATE * 10/8/2014	CHECKED -	REVISED -		SCALE: N/A SHEET 1 OF 3 SHEETS STA. ---- TO STA. ----					CONTRACT NO. 70844
ILLINOIS FED. AID PROJECT										

GENERAL NOTES

G. N. -280A

THE VARIOUS MULCH PAY ITEMS IN THE PLANS INCLUDE QUANTITIES FOR TEMPORARY MULCH FOR EROSION CONTROL. THE TEMPORARY MULCH INCLUDES MAINTENANCE AND REMOVAL IF NECESSARY, PER THE REQUIREMENTS OF ARTICLE 280 OF THE STANDARD SPECIFICATIONS, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. SOME OR ALL OF THE MULCH USED AS TEMPORARY EROSION CONTROL WILL BE DELETED IF IT IS NOT NECESSARY DUE TO ESTABLISHMENT OF PERMANENT SEEDING.

G. N. -353

THE SHADED AREAS OF P. C. CONCRETE BASE COURSE CONSTRUCTED ADJACENT TO COMBINATION CONCRETE CURB AND GUTTER AS SHOWN IN THE PLANS SHALL BE POURED MONOLITHIC WITH THE COMBINATION CONCRETE CURB AND GUTTER. THIS WORK WILL BE MEASURED AND INCLUDED IN THE CONTRACT UNIT PRICE PER SQUARE YARD FOR PORTLAND CEMENT CONCRETE BASE COURSE OF THE THICKNESS BE ALLOWED.

G. N. -406

THE QUANTITIES INCLUDED IN THE PLANS FOR HOT-MIX ASPHALT RESURFACING ARE INTENDED TO GIVE THE COVERAGE SHOWN ON THE TYPICAL CROSS SECTIONS. IT IS NOT INTENDED TO INCREASE THE THICKNESS OF THE HOT-MIX ASPHALT MIXTURE IN ORDER TO USE ALL OF THE QUANTITIES INCLUDED IN THE CONTRACT.

G. N. -440L (SPL)

THE HMA BEING MILLED FROM THIS PROJECT CONTAINS STEEL SLAG COARSE AGGREGATE. THIS RECYCLED ASPHALT PAVEMENT (RAP) MATERIAL WILL BE REQUIRED TO BE STOCKPILED SEPARATELY FROM OTHER RAP STOCKPILES. TESTING PER THE CURRENT REQUIREMENTS IN THE MANUAL OF TEST PROCEDURES FOR MATERIALS FOR DETERMINATION OF RAP AGGREGATE BULK SPECIFIC GRAVITY WILL BE REQUIRED PRIOR TO INCORPORATING THIS MATERIAL INTO ANY FUTURE HMA PROJECTS.

STEEL SLAG COARSE AGGREGATE MIX CAN BE FOUND FROM:
STA. 23+52.70 TO STA. 105+10.00

G. N. -442B -- PATCHING SCHEDULES

THE PATCHING SCHEDULES INCLUDED IN THE PLANS REPRESENT THE BEST INFORMATION AVAILABLE AT THE TIME OF COMPLETION OF THE PLANS FOR LETTING. VARIATIONS IN LOCATION AND SIZES OF BOTH FULL-DEPTH AND PARTIAL-DEPTH PATCHES MAY OCCUR.

G. N. -482

ALL LOW ESAL MIXTURE PLACED AS HOT-MIX ASPHALT SHOULDERS SHALL BE COMPACTED TO 94.0-98.4 PERCENT OF THE MAXIMUM THEORETICAL DENSITY. THIS REQUIREMENT SHALL APPLY TO IL 9.5L GRADATION MIXES. THIS MAXIMUM DENSITY SHALL BE DETERMINED FROM THE MOVING AVERAGE OF FOUR TESTS AS IN OTHER QC/QA TESTING. A NUCLEAR GAUGE DENSITY/CORE CORRELATION SHALL BE PERFORMED FOR THE IL 9.5L USING STANDARD CORRELATION PROCEDURES WHEN MORE THAN 3,000 TONS ARE TO BE PLACED.

GN 406H MIXTURE REQUIREMENTS

Location	IL 47	IL 47 & US 136	IL 47 & US 136	IL 47
Mixture Use	Class D Patch	Level Binder	Surface (north of 236+00)	Surface (south of 236+00)
AC/PG	PG 64-22	PG 64-22	PG 64-22	PG 64-22
Design Air Voids	4.0% @ Ndes=70	4.0% @ Ndes=70	4.0% @ Ndes=70	4.0% @ Ndes=70
Mix Comp(Gradation)	IL 19.0 FG	IL 9.5 FG	IL 9.5	IL 9.5
Friction Aggregate	N. A.	Mix C	Mix C	Mix D
Mixture Weight	112	112	112	112
Quality Mngmnt Program	QC/QA	OCP	OCP	OCP
Sublot Size	N. A.	1000 Ton	1000 Ton	1000 Ton

Location	IL 47	IL 47
Mixture Use	Incidental	Bottom 6 1/2" of 8" HMA Shoulder
AC/PG	PG 64-22	PG 64-22
Design Air Voids	4.0% @ Ndes=70	2.0% @ Ndes=30
Mix Comp(Gradation)	IL 9.5	Other
Friction Aggregate	Mix C	N. A.
Mixture Weight	112	112
Quality Mngmnt Program	QC/QA	QC/QA
Sublot Size	N. A.	N. A.

G. N. -540

THE CONTRACTOR SHALL ASSEMBLE AND MATCH-MARK THE PRECAST BOX CULVERT SECTIONS AND END SECTIONS PRIOR TO SHIPMENT OF THESE COMPONENTS FROM THE MANUFACTURER, AND AS DIRECTED BY THE ENGINEER IN ORDER TO SECURE A PROPER FIT ON EACH JOINT. ANY SECTIONS OR END SECTIONS WHICH DO NOT PROVIDE A PROPER FIT AT THE JOINT SHALL BE REJECTED BY THE ENGINEER AND REPLACED BY THE CONTRACTOR WITH NO ADDITIONAL COMPENSATION BEING ALLOWED.

THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER FOOT FOR PRECAST CONCRETE BOX CULVERTS OF THE SIZE SPECIFIED.

FILE NAME :	USER NAME : ceerlockm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
Default	ce:\p\work\p\1dot\ceerlockm\08107612\08107612-44-44t-gc.mxd	DRAWN -	REVISED -		SCALE: N/A	SHEET 2	OF 3	SHEETS	STA. ----	TO STA. ----	326	136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	4
	PLOT SCALE = 10.0000 1/16 in.	CHECKED -	REVISED -												
	PLOT DATE = 10/9/2014	DATE -	REVISED -												
											CONTRACT NO. 70844		ILLINOIS FED. AID PROJECT		

GENERAL NOTES

G. N. -542

BEFORE ORDERING PIPE CULVERTS, THE CONTRACTOR SHALL CONSULT THE ENGINEER FOR THE EXACT LENGTHS.

G. N. -542B

ALL THE ENTRANCE CULVERTS LENGTHS SHOWN IN THE PLANS WERE CALCULATED WITH THE ASSUMPTION THAT METAL PIPES AND METAL END SECTION WOULD BE USED.

G. N. -550

BEFORE ORDERING STORM SEWERS, THE CONTRACTOR SHALL CONSULT THE ENGINEER FOR THE EXACT LENGTHS.

G. N. -631

IF THE CONTRACTOR ELECTS TO USE THE ALTERNATE MOUNTING METHOD OF THRU DRILLING THE MOUNTING HOLES FOR THE TRAFFIC BARRIER TERMINALS, TYPE 6, THE HOLES SHALL BE DRILLED USING A CORE DRILL. A HAMMER DRILL WILL NOT BE ALLOWED.

G. N. -667

THE RESIDENT ENGINEER SHALL CONTACT THE PROGRAM DEVELOPMENT CHIEF OF SURVEYS PRIOR TO THE PRE-CONSTRUCTION CONFERENCE FOR INSTRUCTION AS TO SETTING OF TEMPORARY OR PERMANENT TIES FOR CENTERLINE ALIGNMENT CONTROL SURVEY MARKERS (PC*S, PT*S, AND PI*S). PROJECT IMPLEMENTATION PERSONNEL WILL BE RESPONSIBLE FOR LAYOUT OF THESE MARKERS.

G. N. -703A

SHORT TERM PAVEMENT MARKING SHALL BE APPLIED TO THE PAVEMENT AFTER ANY OF THE FOLLOWING: COLD MILLING AND/OR PLACING BITUMINOUS MATERIALS (PRIME COAT), LEVELING BINDER (MACHINE METHOD), BINDER AND SURFACE COURSES. SHORT TERM PAVEMENT MARKING PLACED ON THE SURFACE, SHALL COINCIDE WITH THE FINAL PAVEMENT STRIPING. SHORT TERM PAVEMENT MARKING PLACED PRIOR TO THE SURFACE SHALL COINCIDE WITH THE EXISTING PAVEMENT MARKINGS. USE 4 FEET PER 40 FEET (OR 10% PER STATION).

G. N. -781

RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH STANDARD 781001, AND THE DETAILS SHOWN IN THE PLANS. IF THERE IS ANY DISCREPANCY BETWEEN THE STANDARD AND THE DETAILS IN THE PLANS, THE DETAILS IN THE PLANS SHALL GOVERN. THE FINAL PAVEMENT MARKINGS SHALL BE IN PLACE PRIOR TO PLACING THE RAISED REFLECTIVE PAVEMENT MARKERS AND THE RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED MIDWAY IN THE 30 FOOT (9 m) SPACE BETWEEN THE DASHED CENTERLINE STRIPES (WHEN APPLICABLE).

G. N. -1004.01

COARSE AGGREGATE GRADATION CA-10 MAY BE USED WHENEVER COARSE AGGREGATE CA-6 IS SPECIFIED IN THE STANDARD SPECIFICATIONS.

G. N. -Z0038

AN ALUMINUM TABLET OF THE TYPE SHOWN ON STANDARD 667101 SHALL BE PLACED ON THE PROPOSED STRUCTURE AS DIRECTED BY THE ENGINEER. THE BENCH MARK ELEVATION WILL BE ESTABLISHED AND MARKED BY THE DEPARTMENT. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR PERMANENT BENCH MARKS.

COMMITMENTS:

PARCEL - 5770006

AS A RESULT OF ROW NEGOTIATIONS, PLEASE BE ADVISED OF THE FOLLOWING CHANGES:

DALE AND CONNIE HABECK HAVE REQUESTED TWO (2) EUROPEAN BEECH (*Fagus sylvatica*) TREES AS REPLACEMENT TREES. THE HABECK'S CAN BE REACHED AT (217) 586-3588 FOR PLACEMENT OF THE NEW TREES.

PARCEL - 5770019

AS A RESULT OF ROW NEGOTIATIONS, PLEASE BE ADVISED THAT THE PROPERTY OWNER, KEVIN HUSINGER, HAS REQUESTED THE FOLLOWING TREES FOR REPLACEMENT TREES:

- 3 RED MAPLE (*acer rubrum*) 2" DBH
- 3 SAWTOOTH OAK (*quercus acutissima*) 2" DBH
- 20 WHITE PINE (*pinus strobus*) 5 to 6 foot

MR. HUSINGER CAN BE REACHED AT 217-841-2721 FOR PLACEMENT OF TREES.

PARCEL - 5770023

AS A RESULT OF RIGHT OF WAY NEGOTIATION, PLEASE BE ADVISED THAT THE PROPERTY OWNER, KEN KEEFE, HAS REQUESTED THIRTY (30) 4 - 6' IN HEIGHT COLORADO BLUE SPRUCE TREES (*pungens*) AS REPLACEMENT TREES. MR. KEEFE CAN BE REACHED AT 217-246-0888 FOR PLACEMENT OF THE TREES.

PARCEL - 5770026

AS PART OF LAND ACQUISITION NEGOTIATIONS ON THIS PARCEL, WE HAVE AGREED TO PROVIDE TWO REPLACEMENT TREES FOR THE LOSS OF ONE TREE AT LEFT OF APPROXIMATE STATION 324+34.

THE OWNER HAS REQUESTED ONE SUGAR MAPLE AND EITHER A FALL FIESTA MAPLE OR A MAJESTY MAPLE. WE HAVE SETTLED THE PARCEL ON THE PREMISE THAT WE WILL PROVIDE SUCH TREES AS ARE REASONABLY AVAILABLE. THEY WILL BE PLANTED ON THIS PROPERTY AT A LOCATION AT THE CHOICE OF THE OWNERS FLYNN C. AND CARLEEN McCORMICK. THEY MAY BE REACHED BY TELEPHONE AT 217-722-5751.

PARCEL - 5770041 & TEMPORARY EASEMENT

AS A RESULT OF RIGHT OF WAY NEGOTIATIONS, PLEASE BE ADVISED THAT THE PROPERTY OWNER, LOUIS WOZNIAK, HAS REQUESTED A DRAINAGE STRUCTURE TO BE ADDED TO THE TEMPORARY EASEMENT ON THE ABOVE MENTIONED PARCEL NUMBER. THE STRUCTURE IS SHOWN IN THE PLANS AT APPROXIMATE STATION 466+20. THIS ADDITION IS INTENDED FOR THE BENEFIT OF THE PROPERTY OWNER AND ALL FUTURE MAINTENANCE WILL BE THE RESPONSIBILITY OF THE PROPERTY OWNER.

MR. WOZNIAK CAN BE REACHED AT 217-586-4289 IF NECESSARY.

FILE NAME :	USER NAME : ceeerlockbm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ci:\pwwork\p1dot\c\eeerlockbm\20187612\072044\shtrgenote.dgn	DRAWN -	REVISED -	326			136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	5	
PLOT SCALE = 48.0000' / 1" =	CHECKED -	REVISED -	CONTRACT NO. 70844							
PLOT DATE = 12/2/2014	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							
#MODELNAME#				SCALE: N/A		SHEET 3 OF 3 SHEETS		STA. ---- TO STA. ----		

INVENTORIED STRUCTURE INFORMATION

ROUTE	STATION	EXISTING S.N.	EXISTING STRUCTURE DESCRIPTION	CONTRACT 70844 - PROPOSED WORK
FAI-74	1518+75.19	010-0014	TWO-SPAN STRUCTURE CARRYING FAI-74 OVER IL 47	OMMISION - NO WORK TO THE OVERHEAD STRUCTURE
FAI-74	1518+75.19	010-0015	TWO-SPAN STRUCTURE CARRYING FAI-74 OVER IL 47	OMMISION - NO WORK TO THE OVERHEAD STRUCTURE
FAP 326 (IL 47)	36+35.15	010-8317	DOUBLE 8' X 7' RC BOX CULVERT (NO SKEW)	REMAIN IN PLACE AND RECEIVE HMA RESURFACING
FAP 326 (IL 47)	175+61.00	010-8146	SINGLE 6' X 4' PRECAST BOX CULVERT (NO SKEW)	REMAIN IN PLACE AND RECEIVE HMA RESURFACING
FAP 326 (IL 47)	181+31.00	010-8063	SINGLE 7' X 3' RC BOX CULVERT (SKEW = 30 DEGREES LEFT FORWARD)	REPLACE WITH PR SN 010-8151, 8' X 5' PRECAST BOX CULVERT, CLASS D PATCH & HMA RESURFACING
FAP 326 (IL 47)	224+48.00	010-8604	SINGLE 12' X 4' RC BOX CULVERT (NO SKEW)	REPLACE WITH PR SN 010-8152, DOUBLE 8' X 5' PRECAST BOX CULVERT, CLASS D PATCH & HMA RESURFACING
FAP 326 (IL 47)	250+32.00	010-8062	SINGLE 8' X 2.5' RC BOX CULVERT (NO SKEW)	REMAIN IN PLACE AND RECEIVE HMA RESURFACING
FAP 326 (IL 47)	335+87.80	010-0239	THREE SPAN P.P.C. DECK BEAM STRUCTURE (NO SKEW)	OMMISION FROM HMA RESURFACING, GUARDRAIL UPDATES
FAP 326 (IL 47)	466+18.00	010-8061	SINGLE 8' X 2.5' RC BOX CULVERT (NO SKEW)	REMAIN IN PLACE AND RECEIVE HMA RESURFACING

FILE NAME :	USER NAME : ceer-lockm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INVENTORIED STRUCTURE INFORMATION	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cr:\pwork\p101dot\ceer-lockm\0107612\070844-shi-gemote.dgn	DRAWN -	REVISED -	326			136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	6	
PLOT SCALE = 40.0000 1 / in.	CHECKED -	REVISED -	CONTRACT NO. 70844							
Default	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							
				SCALE: N/A SHEET 1 OF 1 SHEETS STA. ---- TO STA. ----						

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	STA. 23+82.80	193+75.00	STA. 107+53.91	STA. 16+78.43
				TO	TO	TO	TO
				STA. 193+75.00	STA. 523+18.70	STA. 126+36.70	STA. 25+54.63
				FAP 326 ROADWAY	FAP 326 ROADWAY	FAP 326 MULTI-USE TRAIL - C	FAP 326 MULTI-USE TRAIL - I
				80% FED, 20% STATE	80% FED, 20% STATE	60% FED, 20% STATE, 20% LOCAL	80% FED, 20% LOCAL
				0004 URBAN <i>MOBILE</i>	0004 RURAL <i>MOBILE</i>	0028 (01) URBAN <i>MOBILE</i>	0028 (02) URBAN <i>MOBILE</i>
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	123.0	-	108.0	15.0	-
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	245.0	22.0	184.0	39.0	-
20100500	TREE REMOVAL, ACRES	ACRE	0.3	-	0.2	0.1	-
20101200	TREE ROOT PRUNING	EACH	12.0	-	12.0	-	-
20101700	SUPPLEMENTAL WATERING	UNIT	6.0	-	6.0	-	-
20200100	EARTH EXCAVATION	CU YD	27,219.0	4,616.0	22,496.0	-	107.0
20700220	POROUS GRANULAR EMBANKMENT	CU YD	430.0	264.0	166.0	-	-
20800150	TRENCH BACKFILL	CU YD	165.0	-	2.0	163.0	-
20900110	POROUS GRANULAR BACKFILL	CU YD	159.0	75.0	65.0	19.0	-
21301052	EXPLORATION TRENCH 52" DEPTH	FOOT	42,800.0	8,400.0	34,400.0	-	-
* 25000210	SEEDING, CLASS 2A	ACRE	35.0	8.5	26.5	-	-
* 25000312	SEEDING, CLASS 4A	ACRE	4.0	1.0	3.0	-	-
* 25000350	SEEDING, CLASS 7	ACRE	107.0	26.50	80.50	-	-
* 25000400	NITROGEN FERTILIZER NUTRIENT	POUND	3,453.0	851.0	2,587.0	-	15.0

* DENOTES SPECIALTY ITEM

FILE NAME =	USER NAME = gberlookm	DESIGNED - BMC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pvc\work\p1001\gberlookm\2010761210	770844-sht-500.dgn	DRAWN - BMC	REVISED -		326	136RS-3, CR & (10-4RS-1)	CHAMPAIGN	474	7				
#MODELNAME#	PLOT SCALE = 40,0000 1/ in.	CHECKED -	REVISED -		SCALE: N/A SHEET 1 OF 19 SHEETS STA. ---- TO STA. ----				CONTRACT NO. 70844				
	PLOT DATE = 11/18/2014	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	STA. 23+82.80	193+75.00	STA. 107+53.91	STA. 16+78.43
				TO	TO	TO	TO
				STA. 193+75.00	STA. 523+18.70	STA. 126+36.70	STA. 25+54.63
				FAP 326 ROADWAY	FAP 326 ROADWAY	FAP 326 MULTI-USE TRAIL - C	FAP 326 MULTI-USE TRAIL - I
				80% FED, 20% STATE	80% FED, 20% STATE	60% FED, 20% STATE, 20% LOCAL	80% FED, 20% LOCAL
				0004 URBAN	0004 RURAL	0028 URBAN	0028 URBAN
▪ 25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	3,453.0	851.0	2,587.0	-	15.0
▪ 25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	3,453.0	851.0	2,587.0	-	15.0
25100105	MULCH, METHOD 1	ACRE	107.00	26.50	80.50	-	-
▪ 25100115	MULCH, METHOD 2	ACRE	39.00	9.50	29.50	-	-
▪ 25200100	SODDING	SQ YD	1,210.0	-	-	-	1,210.0
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	10,895.0	2,645.0	8,175.0	-	75.0
28000305	TEMPORARY DITCH CHECKS	FOOT	7,990.0	1,570.0	6,390.0	-	30.0
28000400	PERIMETER EROSION BARRIER	FOOT	8,392.0	1,998.0	6,394.0	-	-
28000500	INLET AND PIPE PROTECTION	EACH	130.0	26.0	98.0	6.0	-
28100105	STONE RIPRAP, CLASS A3	SQ YD	674.0	433.0	241.0	-	-
28100107	STONE RIPRAP, CLASS A4	SQ YD	1,018.0	432.0	542.0	44.0	-
28100201	STONE RIPRAP, CLASS A1	TON	514.0	220.0	280.0	14.0	-
28200200	FILTER FABRIC	SQ YD	1,018.0	432.0	542.0	44.0	-
31101200	SUBBASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	4,541.0	58.0	4,483.0	-	-

▪ DENOTES SPECIALTY ITEM

FILE NAME :	USER NAME : ceerlockbn	DESIGNED - BMC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\p\work\pav\dat\ceerlockbn\0197612\0778044.sht-900.dgn	DRAWN - BMC	REVISED -	326						136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	8	
MODELNAME#	PLOT SCALE = 48,0000' / in.	CHECKED -	REVISED -		SCALE: N/A	SHEET 2	OF 19 SHEETS	STA. ----	TO STA. ----	CONTRACT NO. 70844			
	PLOT DATE = 11/10/2014	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	STA. 23+82.80	193+75.00	STA. 107+53.91	STA. 16+78.43
				TO	TO	TO	TO
				STA. 193+75.00	STA. 523+18.70	STA. 126+36.70	STA. 25+54.63
				FAP 326 ROADWAY	FAP 326 ROADWAY	FAP 326 MULTI-USE TRAIL - C	FAP 326 MULTI-USE TRAIL - I
				80% FED, 20% STATE	80% FED, 20% STATE	60% FED, 20% STATE, 20% LOCAL	80% FED, 20% LOCAL
				0004 URBAN	0004 RURAL	0028 URBAN	0028 URBAN
31102100	SUBBASE GRANULAR MATERIAL, TYPE C 4"	SQ YD	19.0	-	-	-	19.0
35101800	AGGREGATE BASE COURSE, TYPE B 6"	SQ YD	2,613.0	503.0	2,110.0	-	-
35300310	PORTLAND CEMENT CONCRETE BASE COURSE 8 1/2"	SQ YD	4,541.0	58.0	4,483.0	-	-
35800100	PREPARATION OF BASE	SQ YD	510.0	318.0	192.0	-	-
40200800	AGGREGATE SURFACE COURSE, TYPE B	TON	1,757.0	430.0	1,327.0	-	-
40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	104,623.0	29,077.0	75,546.0	-	-
40600637	LEVELING BINDER (MACHINE METHOD). IL-9.5FG, N70	TON	6,075.0	1,416.0	4,659.0	-	-
40600982	HOT-MIX ASPHALT SURFACE REMOVAL-BUTT JOINT	SQ YD	3,216.0	949.0	2,267.0	-	-
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL-BUTT JOINT	SQ YD	294.0	294.0	-	-	-
40600990	TEMPORARY RAMP	SQ YD	130.0	25.0	105.0	-	-
40603315	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70	TON	8,293.0	-	8,293.0	-	-
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	3,858.0	2,832.0	1,026.0	-	-
40800025	BITUMINOUS MATERIALS (PRIME COAT)	POUND	2,378.0	764.0	1,614.0	-	-
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	666.0	214.0	452.0	-	-

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	STA. 23+82.80	193+75.00	STA. 107+53.91	STA. 16+78.43
				TO	TO	TO	TO
				STA. 193+75.00	STA. 523+18.70	STA. 126+36.70	STA. 25+54.63
				FAP 326 ROADWAY	FAP 326 ROADWAY	FAP 326 MULTI-USE TRAIL - C	FAP 326 MULTI-USE TRAIL - I
				80% FED, 20% STATE	80% FED, 20% STATE	60% FED, 20% STATE, 20% LOCAL	80% FED, 20% LOCAL
				0004 URBAN	0004 RURAL	0028 URBAN	0028 URBAN
42300200	PCC DRIVEWAY PAVEMENT, 6 INCH	SQ YD	107.0	-	107.0	-	-
42300300	PCC DRIVEWAY PAVEMENT, 7 INCH	SQ YD	186.0	186.0	-	-	-
42300400	PCC DRIVEWAY PAVEMENT, 8 INCH	SQ YD	197.0	-	-	-	197.0
42400200	PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH	SQ FT	25,398.0	-	-	18,249.0	7,149.0
42400800	DETECTABLE WARNINGS	SQ FT	256.0	-	-	120.0	136.0
44000100	PAVEMENT REMOVAL	SQ YD	449.0	116.0	69.0	245.0	19.0
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	143,324.0	29,587.0	113,737.0	-	-
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	4,522.0	4,522.0	-	-	-
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	3,499.0	622.0	2,569.0	-	308.0
44000400	GUTTER REMOVAL	FOOT	60.0	-	60.0	-	-
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	115.0	-	25.0	-	90.0
44000600	SIDEWALK REMOVAL	SQ FT	770.0	-	-	-	770.0
44003510	MEDIAN REMOVAL PARTIAL DEPTH	SQ FT	949.0	-	949.0	-	-
44004000	PAVED DITCH REMOVAL	FOOT	38.0	38.0	-	-	-

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	STA. 23+82.80	193+75.00	STA. 107+53.91	STA. 16+78.43
				TO	TO	TO	TO
				STA. 193+75.00	STA. 523+18.70	STA. 126+36.70	STA. 25+54.63
				FAP 326 ROADWAY	FAP 326 ROADWAY	FAP 326 MULTI-USE TRAIL - C	FAP 326 MULTI-USE TRAIL - I
				80% FED, 20% STATE	80% FED, 20% STATE	60% FED, 20% STATE, 20% LOCAL	80% FED, 20% LOCAL
				0004 URBAN	0004 RURAL	0028 URBAN	0028 URBAN
44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	378.0	159.0	219.0	-	-
44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SQ YD	251.0	110.0	141.0	-	-
44201777	CLASS D PATCHES, TYPE II, 11 INCH	SQ YD	78.0	78.0	-	-	-
44201781	CLASS D PATCHES, TYPE III, 11 INCH	SQ YD	18.0	18.0	-	-	-
44201783	CLASS D PATCHES, TYPE IV, 11 INCH	SQ YD	152.0	152.0	-	-	-
44201803	CLASS D PATCHES, TYPE II, 13 INCH	SQ YD	350.0	142.0	208.0	-	-
44201807	CLASS D PATCHES, TYPE III, 13 INCH	SQ YD	122.0	39.0	83.0	-	-
44201809	CLASS D PATCHES, TYPE IV, 13 INCH	SQ YD	74.0	37.0	37.0	-	-
44201827	CLASS D PATCHES, TYPE II, 15 INCH	SQ YD	57.0	57.0	-	-	-
44213204	TIE BAR 3/4"	EACH	166.0	166.0	-	-	-
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	79,725.0	17,938.0	61,787.0	-	-
45100200	CRACK FILLING	POUND	53,579.0	13,381.0	40,198.0	-	-
48101200	AGGREGATE SHOULDER, TYPE B	TON	369.0	-	369.0	-	-
48101500	AGGREGATE SHOULDERS, TYPE B 6"	SQ YD	26,872.0	7,001.0	19,871.0	-	-

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	STA. 23+82.80	193+75.00	STA. 107+53.91	STA. 16+78.43
				TO	TO	TO	TO
				STA. 193+75.00	STA. 523+18.70	STA. 126+36.70	STA. 25+54.63
				FAP 326 ROADWAY	FAP 326 ROADWAY	FAP 326 MULTI-USE TRAIL - C	FAP 326 MULTI-USE TRAIL - I
				80% FED, 20% STATE	80% FED, 20% STATE	60% FED, 20% STATE, 20% LOCAL	80% FED, 20% LOCAL
				0004 URBAN	0004 RURAL	0028 URBAN	0028 URBAN
48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SQ YD	18,333.0	5,022.0	13,311.0	-	-
48203100	HOT-MIX ASPHALT SHOULDERS	TON	1,307.0	1,181.0	126.0	-	-
48300100	PORTLAND CEMENT CONCRETE SHOULDER, 6 INCH	SQ YD	18.0	-	-	-	18.0
50100300	REMOVAL OF EXISTING STRUCTURES NO. 1	EACH	1.0	1.0	-	-	-
50100400	REMOVAL OF EXISTING STRUCTURES NO. 2	EACH	1.0	1.0	-	-	-
50100500	REMOVAL OF EXISTING STRUCTURES NO. 3	EACH	1.0	-	1.0	-	-
50100600	REMOVAL OF EXISTING STRUCTURES NO. 4	EACH	1.0	-	1.0	-	-
50100700	REMOVAL OF EXISTING STRUCTURES NO. 5	EACH	1.0	-	1.0	-	-
50104400	CONCRETE HEADWALL REMOVAL	EACH	8.0	5.0	3.0	-	-
50105220	PIPE CULVERT REMOVAL	FOOT	4,029.0	1,166.0	2,863.0	-	-
50300225	CONCRETE STRUCTURES	CU YD	7.0	-	-	7.0	-
50800105	REINFORCEMENT BARS	LBS	186.0	-	186.0	-	-
51500100	NAME PLATES	EACH	2.0	1.0	1.0	-	-
54001001	BOX CULVERT END SECTIONS, CULVERT NO. 1	EACH	2.0	2.0	-	-	-

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	STA. 23+82.80	193+75.00	STA. 107+53.91	STA. 16+78.43
				TO	TO	TO	TO
				STA. 193+75.00	STA. 523+18.70	STA. 126+36.70	STA. 25+54.63
				FAP 326 ROADWAY	FAP 326 ROADWAY	FAP 326 MULTI-USE TRAIL - C	FAP 326 MULTI-USE TRAIL - I
				80% FED, 20% STATE	80% FED, 20% STATE	60% FED, 20% STATE, 20% LOCAL	80% FED, 20% LOCAL
				0004 URBAN	0004 RURAL	0028 URBAN	0028 URBAN
54001002	BOX CULVERT END SECTIONS, CULVERT NO. 2	EACH	2.0	2.0	-	-	-
54001003	BOX CULVERT END SECTIONS, CULVERT NO. 3	EACH	2.0	-	2.0	-	-
54001004	BOX CULVERT END SECTIONS, CULVERT NO. 4	EACH	2.0	-	2.0	-	-
54001005	BOX CULVERT END SECTIONS, CULVERT NO. 5	EACH	2.0	-	2.0	-	-
54001006	BOX CULVERT END SECTIONS, CULVERT NO. 6	EACH	1.0	-	-	1.0	-
54002060	EXPANSION BOLTS, 3/4 INCH X 12 INCH	EACH	15.0	-	15.0	-	-
54003000	CONCRETE BOX CULVERTS	CU YD	2.0	-	2.0	-	-
54010303	PRECAST CONCRETE BOX CULVERTS 3' X 3'	FOOT	45.0	-	45.0	-	-
54010402	PRECAST CONCRETE BOX CULVERTS 4' X 2'	FOOT	49.0	-	39.0	10.0	-
54010403	PRECAST CONCRETE BOX CULVERTS 4' X 3'	FOOT	45.5	45.5	-	-	-
54010805	PRECAST CONCRETE BOX CULVERTS 8' X 5'	FOOT	165.0	72.0	93.0	-	-
54213450	END SECTIONS 15"	EACH	44.0	6.0	38.0	-	-
54213453	END SECTIONS 18"	EACH	36.0	13.0	22.0	1.0	-
54213465	END SECTIONS 30"	EACH	2.0	-	2.0	-	-

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	STA. 23+82.80	193+75.00	STA. 107+53.91	STA. 16+78.43
				TO	TO	TO	TO
				STA. 193+75.00	STA. 523+18.70	STA. 126+36.70	STA. 25+54.63
				FAP 326 ROADWAY	FAP 326 ROADWAY	FAP 326 MULTI-USE TRAIL - C	FAP 326 MULTI-USE TRAIL - I
				80% FED, 20% STATE	80% FED, 20% STATE	60% FED, 20% STATE, 20% LOCAL	80% FED, 20% LOCAL
				0004 URBAN	0004 RURAL	0028 URBAN	0028 URBAN
54213660	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 15"	EACH	3.0	-	3.0	-	-
54213663	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 18"	EACH	13.0	2.0	10.0	1.0	-
54213666	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 21"	EACH	3.0	1.0	2.0	-	-
54213669	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 24"	EACH	2.0	2.0	-	-	-
54214290	END SECTIONS, EQUIVALENT ROUND-SIZE 15"	EACH	46.0	10.0	36.0	-	-
54214293	END SECTIONS, EQUIVALENT ROUND-SIZE 18"	EACH	4.0	-	4.0	-	-
54214296	END SECTIONS, EQUIVALENT ROUND-SIZE 21"	EACH	2.0	-	2.0	-	-
54214299	END SECTIONS, EQUIVALENT ROUND-SIZE 24"	EACH	2.0	2.0	-	-	-
54214305	END SECTIONS, EQUIVALENT ROUND-SIZE 30"	EACH	2.0	-	2.0	-	-
54214500	PRECAST REINFORCED CONCRETE FLARED END SECTIONS, EQUIVALENT ROUND-SIZE 15"	EACH	2.0	2.0	-	-	-
54214509	PRECAST REINFORCED CONCRETE FLARED END SECTIONS, EQUIVALENT ROUND-SIZE 24"	EACH	20.0	2.0	18.0	-	-
54214521	PRECAST REINFORCED CONCRETE FLARED END SECTIONS, EQUIVALENT ROUND-SIZE 36"	EACH	2.0	-	2.0	-	-
54244405	FLUSH INLET BOX FOR MEDIAN, STANDARD 542546	EACH	1.0	-	1.0	-	-
54248510	CONCRETE COLLAR	CU YD	28.0	4.0	24.0	-	-

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	STA. 23+82.80	193+75.00	STA. 107+53.91	STA. 16+78.43
				TO	TO	TO	TO
				STA. 193+75.00	STA. 523+18.70	STA. 126+36.70	STA. 25+54.63
				FAP 326 ROADWAY	FAP 326 ROADWAY	FAP 326 MULTI-USE TRAIL - C	FAP 326 MULTI-USE TRAIL - I
				80% FED, 20% STATE	80% FED, 20% STATE	60% FED, 20% STATE, 20% LOCAL	80% FED, 20% LOCAL
				0004 URBAN	0004 RURAL	0028 URBAN	0028 URBAN
542A0220	PIPE CULVERTS, CLASS A, TYPE 1 15"	FOOT	90.0	-	90.0	-	-
542A0223	PIPE CULVERTS, CLASS A, TYPE 1 18"	FOOT	413.0	100.0	313.0	-	-
542A0226	PIPE CULVERTS, CLASS A, TYPE 1 21"	FOOT	132.0	70.0	62.0	-	-
542A0229	PIPE CULVERTS, CLASS A, TYPE 1 24"	FOOT	150.0	150.0	-	-	-
542A5470	PIPE CULVERTS, CLASS A, TYPE 1 EQUIVALENT ROUND-SIZE 15"	FOOT	105.0	105.0	-	-	-
542A5479	PIPE CULVERTS, CLASS A, TYPE 1 EQUIVALENT ROUND-SIZE 24"	FOOT	128.0	12.0	116.0	-	-
542A5491	PIPE CULVERTS, CLASS A, TYPE 1, EQUIVALENT ROUND-SIZE 36"	FOOT	8.0	-	8.0	-	-
542D0211	PIPE CULVERTS, CLASS D, TYPE 1 6"	FOOT	8.0	8.0	-	-	-
542D0217	PIPE CULVERTS, CLASS D, TYPE 1 12"	FOOT	8.0	8.0	-	-	-
542D0220	PIPE CULVERTS, CLASS D, TYPE 1 15"	FOOT	985.0	120.0	865.0	-	-
542D0223	PIPE CULVERTS, CLASS D, TYPE 1 18"	FOOT	856.0	331.0	450.0	75.0	-
542D0229	PIPE CULVERTS, CLASS D, TYPE 1 24"	FOOT	10.0	10.0	-	-	-
542D0235	PIPE CULVERTS, CLASS D, TYPE 1 30"	FOOT	58.0	-	58.0	-	-
542D5470	PIPE CULVERTS, CLASS D, TYPE 1 EQUIVALENT ROUND-SIZE 15"	FOOT	885.0	185.0	700.0	-	-

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	STA. 23+82.80	193+75.00	STA. 107+53.91	STA. 16+78.43
				TO	TO	TO	TO
				STA. 193+75.00	STA. 523+18.70	STA. 126+36.70	STA. 25+54.63
				FAP 326 ROADWAY	FAP 326 ROADWAY	FAP 326 MULTI-USE TRAIL - C	FAP 326 MULTI-USE TRAIL - I
				80% FED, 20% STATE	80% FED, 20% STATE	60% FED, 20% STATE, 20% LOCAL	80% FED, 20% LOCAL
				0004 URBAN	0004 RURAL	0028 URBAN	0028 URBAN
542D5473	PIPE CULVERTS, CLASS D, TYPE 1 EQUIVALENT ROUND-SIZE 18"	FOOT	120.0	-	120.0	-	-
542D5476	PIPE CULVERTS, CLASS D, TYPE 1 EQUIVALENT ROUND-SIZE 21"	FOOT	40.0	-	40.0	-	-
542D5479	PIPE CULVERTS, CLASS D, TYPE 1 EQUIVALENT ROUND-SIZE 24"	FOOT	40.0	40.0	-	-	-
542D5485	PIPE CULVERTS, CLASS D, TYPE 1 EQUIVALENT ROUND-SIZE 30"	FOOT	40.0	-	40.0	-	-
550A0090	STORM SEWERS, CLASS A, TYPE 1 18"	FOOT	473.0	-	-	473.0	-
55200400	STORM SEWERS JACKED IN PLACE, 15"	FOOT	178.0	-	178.0	-	-
56109210	WATER VALVES TO BE ADJUSTED	EACH	1.0	-	-	-	1.0
60100060	CONCRETE HEADWALL FOR PIPE DRAIN	EACH	1.0	-	1.0	-	-
60100085	GEOTECHNICAL FABRIC FOR FRENCH DRAINS	SQ YD	56.0	-	-	56.0	-
60107700	PIPE UNDERDRAINS 6 INCH	FOOT	50.0	-	-	50.0	-
60108200	PIPE UNDERDRAINS (SPECIAL) 6 INCH	FOOT	75.0	-	-	75.0	-
60218400	MANHOLE, TYPE A, 4' DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1.0	1.0	-	-	-
60221100	MANHOLE, TYPE A, 5' DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1.0	-	1.0	-	-
60223800	MANHOLE, TYPE A, 6' DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1.0	1.0	-	-	-

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	STA. 23+82.80	193+75.00	STA. 107+53.91	STA. 16+78.43
				TO	TO	TO	TO
				STA. 193+75.00	STA. 523+18.70	STA. 126+36.70	STA. 25+54.63
				FAP 326	FAP 326	FAP 326	FAP 326
				ROADWAY	ROADWAY	MULTI-USE TRAIL - C	MULTI-USE TRAIL - I
				80% FED, 20% STATE	80% FED, 20% STATE	60% FED, 20% STATE, 20% LOCAL	80% FED, 20% LOCAL
				0004	0004	0028	0028
				URBAN	RURAL	URBAN	URBAN
60234200	INLETS, TYPE A, TYPE 1 FRAME, OPEN LID	EACH	1.0	-	1.0	-	-
60235700	INLETS, TYPE A, TYPE 3 FRAME AND GRATE	EACH	1.0	-	1.0	-	-
60236200	INLETS, TYPE A, TYPE 8 GRATE	EACH	1.0	-	1.0	-	-
60240210	INLETS, TYPE B, TYPE 1 OPEN LID	EACH	5.0	-	-	5.0	-
60240215	INLETS, TYPE B, TYPE 1 FRAME CLOSED LID	EACH	1.0	-	1.0	-	-
60255410	CATCH BASIN TO BE CLEANED	EACH	11.0	-	11.0	-	-
60260500	INLETS TO BE ADJUSTED WITH NEW TYPE 3 FRAME AND GRATE	EACH	2.0	-	-	-	2.0
60500050	REMOVING CATCH BASINS	EACH	2.0	-	2.0	-	-
60602800	CONCRETE GUTTER TYPE B	FOOT	60.0	-	60.0	-	-
60604400	COMBINATION CONCRETE CURB AND GUTTER TYPE B-6.18	FOOT	24.0	-	-	-	24.0
60605000	COMBINATION CONCRETE CURB AND GUTTER TYPE B-6.24	FOOT	726.0	73.0	586.0	-	67.0
60620400	CONCRETE MEDIAN, TYPE SB-9.06	SO FT	515.0	515.0	-	-	-
61100605	MISCELLANEOUS CONCRETE	CU YD	3.0	3.0	-	-	-
61101017	STORM SEWER PROTECTED, CLASS A 15"	FOOT	147.0	-	147.0	-	-

* DENOTES SPECIALTY ITEM

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	STA. 23+82.80	193+75.00	STA. 107+53.91	STA. 16+78.43
				TO	TO	TO	TO
				STA. 193+75.00	STA. 523+18.70	STA. 126+36.70	STA. 25+54.63
				FAP 326 ROADWAY	FAP 326 ROADWAY	FAP 326 MULTI-USE TRAIL - C	FAP 326 MULTI-USE TRAIL - I
				80% FED, 20% STATE	80% FED, 20% STATE	60% FED, 20% STATE, 20% LOCAL	80% FED, 20% LOCAL
				0004	0004	0028	0028
				URBAN	RURAL	URBAN	URBAN
61101026	STORM SEWER PROTECTED, CLASS A 24"	FOOT	45.0	-	45.0	-	-
* 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	1,250.0	250.0	1,000.0	-	-
* 63000025	STEEL PLATE BEAM GUARDRAIL, ATTACHED TO STRUCTURES	FOOT	125.0	-	125.0	-	-
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	4.0	-	4.0	-	-
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	26.0	6.0	20.0	-	-
63200310	GUARDRAIL REMOVAL	FOOT	1,805.0	355.0	1,450.0	-	-
63500105	DELINEATORS	EACH	100.0	26.0	72.0	2.0	-
64200108	SHOULDER RUMBLE STRIPS, 8 INCH	FOOT	72,766.0	15,833.0	56,933.0	-	-
66600105	FURNISHING AND ERECTING RIGHT OF WAY MARKERS	EACH	197.0	61.0	136.0	-	-
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	1,300.0	292.0	878.0	-	130.0
* 66900450	SPECIAL WASTE PLANS AND REPORTS	L SUM	1.0	0.2	0.7	-	0.1
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	1.0	0.2	0.7	-	0.1
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	12.0	3.0	9.0	-	-
67100100	MOBILIZATION	L SUM	1.0	0.2	0.8	-	-

* DENOTES SPECIALTY ITEM

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	STA. 23+82.80	193+75.00	STA. 107+53.91	STA. 16+78.43
				TO	TO	TO	TO
				STA. 193+75.00	STA. 523+18.70	STA. 126+36.70	STA. 25+54.63
				FAP 326 ROADWAY	FAP 326 ROADWAY	FAP 326 MULTI-USE TRAIL - C	FAP 326 MULTI-USE TRAIL - I
				80% FED, 20% STATE	80% FED, 20% STATE	60% FED, 20% STATE, 20% LOCAL	80% FED, 20% LOCAL
				0004	0004	0028	0028
				URBAN	RURAL	URBAN	URBAN
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1.0	0.2	0.8	-	-
70100455	TRAFFIC CONTROL AND PROTECTION, STANDARD 701206	L SUM	1.0	0.2	0.8	-	-
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1.0	0.2	0.8	-	-
70100500	TRAFFIC CONTROL AND PROTECTION, STANDARD 701326	L SUM	1.0	0.2	0.8	-	-
70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	L SUM	1.0	-	-	-	1.0
70102635	TRAFFIC CONTROL AND PROTECTION STANDARD 701701	L SUM	1.0	-	-	-	1.0
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1.0	-	-	-	1.0
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	25.0	8.0	17.0	-	-
70200100	NIGHT TIME WORK ZONE LIGHTING	LSUM	1.0	0.5	0.5	-	-
70300100	SHORT TERM PAVEMENT MARKING	FOOT	4,355.0	850.0	3,505.0	-	-
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	141,180.0	33,311.0	107,869.0	-	-
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1,049.0	259.0	790.0	-	-
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	2,177.0	128.0	2,049.0	-	-
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	190.0	22.0	168.0	-	-

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	STA. 23+82.80	193+75.00	STA. 107+53.91	STA. 16+78.43
				TO	TO	TO	TO
				STA. 193+75.00	STA. 523+18.70	STA. 126+36.70	STA. 25+54.63
				FAP 326 ROADWAY	FAP 326 ROADWAY	FAP 326 MULTI-USE TRAIL - C	FAP 326 MULTI-USE TRAIL - I
				80% FED, 20% STATE	80% FED, 20% STATE	60% FED, 20% STATE, 20% LOCAL	80% FED, 20% LOCAL
				0004 URBAN	0004 RURAL	0028 URBAN	0028 URBAN
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1,438.0	281.0	1,157.0	-	-
* 72000100	SIGN PANEL TYPE 1	SQ FT	19.0	-	-	9.0	10.0
* 72000200	SIGN PANEL TYPE 2	SQ FT	56.0	-	-	19.0	37.0
* 72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	99.0	-	-	33.0	66.0
* 72900100	METAL POST TYPE A	FOOT	180.0	-	-	72.0	108.0
* 73100100	BASE FOR TELESCOPING SIGN SUPPORT	EACH	6.0	-	-	2.0	4.0
* 78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	134,055.0	26,186.0	107,869.0	-	-
* 78009000	MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	344.0	94.0	250.0	-	-
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	7,125.0	7,125.0	-	-	-
* 78009006	MODIFIED URETHANE PAVEMENT MARKING - LINE 6"	FOOT	1,235.0	259.0	790.0	122.0	64.0
* 78009012	MODIFIED URETHANE PAVEMENT MARKING - LINE 12"	FOOT	2,177.0	128.0	2,049.0	-	-
* 78009024	MODIFIED URETHANE PAVEMENT MARKING - LINE 24"	FOOT	204.0	-	168.0	22.0	14.0
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	820.0	136.0	684.0	-	-
* 78200410	GUARDRAIL MARKERS, TYPE A	EACH	51.0	9.0	42.0	-	-

* DENOTES SPECIALTY ITEM

FILE NAME :	USER NAME : ceerlockm	DESIGNED - BMC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
oi:\work\work\dat\ceerlockm\00187612\070844\sh-500.dgn	DRAWN - BMC	REVISED -	326						136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	20	
PLOT SCALE = 48.0000' / 1"	CHECKED -	REVISED -	SCALE: N/A SHEET 14 OF 19 SHEETS STA. ---- TO STA. ----				CONTRACT NO. 70844						
MODELNAME:	DATE -	REVISED -	ILLINOIS FED. AID PROJECT										

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	STA. 23+82.80	193+75.00	STA. 107+53.91	STA. 16+78.43
				TO	TO	TO	TO
				STA. 193+75.00	STA. 523+18.70	STA. 126+36.70	STA. 25+54.63
				FAP 326 ROADWAY	FAP 326 ROADWAY	FAP 326 MULTI-USE TRAIL - C	FAP 326 MULTI-USE TRAIL - I
				80% FED, 20% STATE	80% FED, 20% STATE	60% FED, 20% STATE, 20% LOCAL	80% FED, 20% LOCAL
				0004 URBAN	0004 RURAL	0028 URBAN	0028 URBAN
* 78200520	BARRIER WALL MARKER, TYPE B	EACH	6.0	-	6.0	-	-
* 78200530	BARRIER WALL MARKER, TYPE C	EACH	6.0	-	6.0	-	-
* 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	26.0	6.0	20.0	-	-
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	530.0	124.0	406.0	-	-
* 80500100	SERVICE INSTALLATION, TYPE A	EACH	1.0	-	-	-	1.0
* 81028340	UNDERGROUND CONDUIT, PVC, 1 1/2" DIA.	FOOT	253.0	-	-	-	253.0
* 81028360	UNDERGROUND CONDUIT, PVC, 2 1/2" DIA.	FOOT	7.0	-	-	-	7.0
* 81100500	CONDUIT ATTACHED TO STRUCTURE, 1 1/2" DIA., GALVANIZED STEEL	FOOT	24.0	-	-	-	24.0
* 81400100	HANDHOLE	EACH	1.0	-	-	-	1.0
* 85800100	FLASHER CONTROLLER	EACH	1.0	-	-	-	1.0
* 87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2 C	FOOT	247.0	-	-	-	247.0
* 87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	206.0	-	-	-	206.0
* 87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	158.0	-	-	-	158.0
* 87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7 C	FOOT	315.0	-	-	-	315.0

* DENOTES SPECIALTY ITEM

FILE NAME =	USER NAME = oearlockbn	DESIGNED - BMC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES				F.A.P. RT.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
at:\pwwork\pwwork\oearlockbn\20187612\078844-sht-500.dgn	DRAWN - BMC	REVISED -	326						136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	21	
MODELNAME*	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -		SCALE: N/A	SHEET 15	OF 19 SHEETS	STA. ----	TO STA. ----	CONTRACT NO. 70844			
	PLOT DATE = 11/10/2014	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	STA. 23+82.80	193+75.00	STA. 107+53.91	STA. 16+78.43
				TO	TO	TO	TO
				STA. 193+75.00	STA. 523+18.70	STA. 126+36.70	STA. 25+54.63
				FAP 326 ROADWAY	FAP 326 ROADWAY	FAP 326 MULTI-USE TRAIL - C	FAP 326 MULTI-USE TRAIL - I
				80% FED, 20% STATE	80% FED, 20% STATE	60% FED, 20% STATE, 20% LOCAL	80% FED, 20% LOCAL
				0004 URBAN	0004 RURAL	0028 URBAN	0028 URBAN
* 87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	148.0	-	-	-	148.0
* 87800100	CONCRETE FOUNDATION, TYPE A	FOOT	3.0	-	-	-	3.0
* 88040030	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 1-SECTION, POST MOUNTED	EACH	2.0	-	-	-	2.0
* 88600100	DETECTOR LOOP, TYPE I	FOOT	229.0	229.0	-	-	-
* 88800100	PEDESTRIAN PUSH-BUTTON	EACH	2.0	-	-	-	2.0
* 89501250	RELOCATE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1.0	-	-	-	1.0
* 89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	793.0	-	-	-	793.0
* 89502385	REMOVE EXISTING CONCRETE FOUNDATION	EACH	1.0	-	-	-	1.0
* A2001016	TREE, ACER RUBRUM (RED MAPLE), 2" CALIPER, BALLED AND BURLAPPED	EACH	3.0	-	3.0	-	-
* A2001724	TREE, ACER SACCHARUM (SUGAR MAPLE), 3" CALIPER, BALLED AND BURLAPPED	EACH	1.0	-	1.0	-	-
* A2006517	TREE, QUERCUS ACUTISSIMA (SAWTOOTH OAK), 2" CALIPER, BALLED AND BURLAPPED	EACH	3.0	-	3.0	-	-
* A2015990	TREE, FAGUS SYLVATICA, EUROPEAN BEECH, 1-1/2" BALLED AND BURLAPPED	EACH	2.0	2.0	-	-	-
* D2002272	EVERGREEN, PICEA PUNGENS GLAUCA (COLORADO BLUE SPRUCE), 6' HEIGHT, BALLED AND BURLAPPED	EACH	30.0	-	30.0	-	-
* D2002972	EVERGREEN, PINUS STROBUS (EASTERN WHITE PINE), 6' HEIGHT, BALLED AND BURLAPPED	EACH	20.0	-	20.0	-	-

* DENOTES SPECIALTY ITEM

FILE NAME =	USER NAME = oee-lockm	DESIGNED - BMC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwwork\oee\lockm\oee\107612\070844\sh-500.dgn	70844\sh-500.dgn	DRAWN - BMC	REVISED -						326	136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	22
MODELNAME =	PLOT SCALE = 48,0000' / in.	CHECKED -	REVISED -		SCALE: N/A	SHEET 16	OF 19 SHEETS	STA. ----	TO STA. ----	CONTRACT NO. 70844			
	PLOT DATE = 12/2/2014	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	STA. 23+82.80	193+75.00	STA. 107+53.91	STA. 16+78.43
				TO	TO	TO	TO
				STA. 193+75.00	STA. 523+18.70	STA. 126+36.70	STA. 25+54.63
				FAP 326 ROADWAY	FAP 326 ROADWAY	FAP 326 MULTI-USE TRAIL - C	FAP 326 MULTI-USE TRAIL - I
				80% FED, 20% STATE	80% FED, 20% STATE	60% FED, 20% STATE, 20% LOCAL	80% FED, 20% LOCAL
				0004	0004	0028	0028
				URBAN	RURAL	URBAN	URBAN
X0321461	PILLAR REMOVAL	EACH	2.0	-	-	2.0	-
X0326696	SIGN AND POST	EACH	17.0	3.0	14.0	-	-
X0327746	PIPE CULVERTS, CLASS D, TYPE 1, 30" FITTING W/24" TEE	EACH	1.0	-	1.0	-	-
X0549200	CLEAN PAVED DITCH	FOOT	1,282.0	1,282.0	-	-	-
X2011000	TEMPORARY FENCE (SPECIAL)	FOOT	1,400.0	1,400.0	-	-	-
X4400196	HOT-MIX ASPHALT SURFACE REMOVAL (SPECIAL)	SO YD	159.0	159.0	-	-	-
X4403300	CONCRETE MEDIAN REMOVAL	SO FT	29,405.0	515.0	28,890.0	-	-
X4406260	PARTIAL DEPTH REMOVAL, TYPE II, 6"	SO YD	116.0	116.0	-	-	-
X4406290	PARTIAL DEPTH REMOVAL, TYPE II, 9"	SO YD	80.0	-	80.0	-	-
X4406300	PARTIAL DEPTH REMOVAL, TYPE II, 10"	SO YD	658.0	136.0	522.0	-	-
X4406490	PARTIAL DEPTH REMOVAL, TYPE III, 9"	SO YD	39.0	39.0	-	-	-
X4406500	PARTIAL DEPTH REMOVAL, TYPE III, 10"	SO YD	300.0	22.0	278.0	-	-
X4406700	PARTIAL DEPTH REMOVAL, TYPE IV, 10"	SO YD	810.0	405.0	405.0	-	-
X4421000	PARTIAL DEPTH PATCHING	TON	1,040.0	338.0	702.0	-	-

Δ 100% STATE

FILE NAME:	USER NAME: deerlockm	DESIGNED - BMC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
en\p\work\p\edit\deerlockm\201076121077044\sh-500.dgn	DRAWN - BMC	REVISED -	326		136RS-3, CR & 110-4RS-1	CHAMPAIGN	474	23					
PLOT SCALE = 40.0000 1/ in.	CHECKED -	REVISED -	SCALE: N/A SHEET 17 OF 19 SHEETS STA. ---- TO STA. ----										
PLOT DATE = 12/2/2014	DATE -	REVISED -	ILLINOIS FED. AID PROJECT CONTRACT NO. 70844										

STA. 23+82.80	193+75.00	STA. 107+53.91	STA. 16+78.43
TO	TO	TO	TO
STA. 193+75.00	STA. 523+18.70	STA. 126+36.70	STA. 25+54.63
FAP 326	FAP 326	FAP 326	FAP 326
ROADWAY	ROADWAY	MULTI-USE TRAIL - C	MULTI-USE TRAIL - I
80% FED, 20% STATE	80% FED, 20% STATE	60% FED, 20% STATE, 20% LOCAL	80% FED, 20% LOCAL
0004	0004	0028	0028
URBAN	RURAL	URBAN	URBAN

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	0004 URBAN	0004 RURAL	0028 URBAN	0028 URBAN
X5015225	PIPE CULVERT REMOVAL (SPECIAL)	FOOT	151.0	19.0	132.0	-	-
X6061702	CONCRETE MEDIAN, TYPE SM (DOWELLED)	SQ FT	1,417.0	-	1,417.0	-	-
* X6330725	STEEL PLATE BEAM GUARDRAIL (SHORT RADIUS)	FOOT	47.0	47.0	-	-	-
X6660410	REMOVE RIGHT-OF-WAY MARKER	EACH	23.0	6.0	17.0	-	-
X7015005	CHANGEABLE MESSAGE SIGN	CAL DA	18.0	11.0	7.0	-	-
* X7830070	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT	7,125.0	7,125.0	-	-	-
* X7830074	GROOVING FOR RECESSED PAVEMENT MARKING 7"	FOOT	1,235.0	259.0	790.0	122.0	64.0
* X7830078	GROOVING FOR RECESSED PAVEMENT MARKING 13"	FOOT	2,177.0	128.0	2,049.0	-	-
* X7830090	GROOVING FOR RECESSED PAVEMENT MARKING 25"	FOOT	204.0	-	168.0	22.0	14.0
* X8730250	ELECTRIC CABLE IN CONDUIT NO. 20 3/C, TWISTED, SHIELDED	FOOT	161.0	-	-	-	161.0
XZ193300	SURVEY MARKER, TYPE 1 (SPECIAL)	EACH	15.0	4.0	11.0	-	-
Z0013798	CONSTRUCTION LAYOUT	L SUM	1.0	0.2	0.8	-	-
Z0022800	FENCE REMOVAL	FOOT	195.0	26.0	125.0	44.0	-
Z0030900	INSPECTION WELLS	EACH	10.0	2.0	8.0	-	-

* DENOTES SPECIALTY ITEM

20100110 TREE REMOVAL (6 TO 15 UNITS DIAMETER) - IL 47 MAINLINE

STATION	OFFSET	UNITS
<u>TRAIL - C</u> URBAN		
RT 110+09.00	51.4	15.0
TRAIL - C TOTAL =		15.0
RURAL		
<u>IL 47</u> RT 294+93.00 *	41.0	12.0
RT 294+95.00 *	41.0	12.0
RT 294+97.00 *	41.0	12.0
RT 295+00.00 *	41.0	12.0
RT 295+05.00 *	41.0	12.0
RT 295+10.00 *	41.0	12.0
RT 295+15.00 *	41.0	12.0
RT 295+20.00 *	41.0	12.0
RT 295+25.00 *	41.0	12.0
IL 47 RURAL TOTAL =		108.0
PROJECT TOTAL =		123.0

20100210 TREE REMOVAL (OVER 15 UNITS DIAMETER) - IL 47 MAINLINE

STATION	OFFSET	UNITS
<u>TRAIL - C</u> URBAN		
RT 114+06.00	49.5	21.0
RT 117+74.50	61.9	18.0
TRAIL - C TOTAL =		39.0
<u>IL 47</u> RT 125+20.90	50.0	22.0
IL 47 URBAN TOTAL =		22.0
RURAL		
LT 224+52.72	51.0	17.0
LT 283+42.24	40.0	16.0
LT 286+02.82	40.0	61.0
RT 295+20.00 *	41.0	18.0
RT 295+30.00 *	41.0	18.0
RT 295+35.00 *	41.0	18.0
LT 320+59.67	33.0	36.0
IL 47 RURAL TOTAL =		184.0
PROJECT TOTAL =		245.0

* NO EXISTING TREE TOPO AVAILABLE AT THESE LOCATIONS.
QUANTITIES PROVIDED ARE ESTIMATES AND NEED FIELD VERIFIED.

20100500 TREE REMOVAL, ACRES - IL 47 MAINLINE

STATION	TO	STATION	ACRES
<u>TRAIL - C</u> URBAN			
RT 118+90.00		120+90.00	0.1
TRAIL - C TOTAL =			0.1
RURAL			
<u>IL 47</u> LT 273+67.34		279+51.54	0.17
RT 314+50.00		316+00.00	0.03
IL 47 RURAL TOTAL =			0.2
PROJECT TOTAL =			0.30

20101200 TREE ROOT PRUNING

STATION	TO	STATION	EACH
<u>IL 47</u> RURAL			
RT 314+57.00		RT 316+00.00	12.0
IL 47 RURAL TOTAL =			12.0
PROJECT TOTAL =			12.0

20101700 SUPPLEMENTAL WATERING

STATION	TO	STATION	UNIT
<u>IL 47</u> RURAL			
RT 314+57.00		RT 316+00.00	6.0
IL 47 RURAL TOTAL =			6.0
PROJECT TOTAL =			6.0

40600982 HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

21301052 EXPLORATION TRENCH 52" DEPTH

STATION	TO	STATION	FOOT
IL 47			
URBAN			
RT 104+00.00		RT 118+00.00	1,400.0
RT 119+00.00		RT 125+50.00	650.0
RT 127+00.00		RT 138+00.00	1,100.0
RT 139+00.00		RT 158+00.00	1,900.0
RT 159+00.00		RT 166+50.00	750.0
RT 168+00.00		RT 193+00.00	2,500.0
LT 181+10.00		LT 182+10.00	100.0
IL 47 URBAN TOTAL =			8,400.0
RURAL			
RT 194+50.00		RT 233+50.00	3,900.0
RT 234+50.00		RT 260+00.00	2,550.0
RT 261+50.00		RT 286+50.00	2,500.0
RT 287+50.00		RT 313+00.00	2,550.0
RT 314+00.00		RT 335+00.00	2,100.0
RT 336+50.00		RT 339+50.00	300.0
RT 340+50.00		RT 364+50.00	2,400.0
RT 367+50.00		RT 406+00.00	3,850.0
STA. EQ. 406+69.20 (BK) = 401+00.92 (AH)			
RT 401+50.00		RT 427+00.00	2,550.0
RT 428+00.00		RT 453+50.00	2,550.0
RT 454+50.00		RT 507+00.00	5,250.0
LT 465+80.00		LT 466+30.00	50.0
RT 508+00.00		RT 523+00.00	1,500.0
IL 47 RURAL SUB-TOTAL =			32,050.0
US 136			
RURAL			
RT 1648+00.00		RT 1660+00.00	1,200.0
RT 1662+50.00		RT 1674+00.00	1,150.0
IL 47 RURAL SUB-TOTAL =			2,350.0
IL 47 RURAL TOTAL =			34,400.0
PROJECT TOTAL = 42,800.0			

ROADWAY	STATION TO	STATION	LENGTH	WIDTH	AREA SQ YD
RURAL					
IL 47	334+83.40	335+13.40	30.0	30.0	100.0
IL 47	336+42.40	336+72.40	30.0	30.0	100.0
IL 47	522+88.80	523+18.80	30.0	30.0	100.0
US 136	1651+00.00	1651+30.00	30.0	26.0	86.7
US 136	1673+70.00	1674+00.00	30.0	26.0	86.7
IL 47 RURAL SUB-TOTAL =					473.3

SIDEROADS		STATION	DESCRIPTION	AREA SQ YD
URBAN				
RT	118+53.87		SIDEROAD - L.O.W. PARK RD.	360.6
RT	125+99.42		SIDEROAD - BRIARCLIFF DR.	273.6
LT	176+00.21		SIDEROAD - CR 2400N	146.3
RT	193+54.96		SIDEROAD - CR 2425N	168.5
IL 47 URBAN TOTAL =				949.0
RURAL				
LT	233+77.62		SIDEROAD - CR 2500N	133.9
RT	233+78.49		SIDEROAD - CR 2500N	133.6
RT	286+95.05		SIDEROAD - CR 2600N	142.9
LT	286+96.75		SIDEROAD - CR 2600N	141.0
LT	313+46.08		SIDEROAD - CR 2650N	123.6
RT	313+46.41		SIDEROAD - CR 2650N	152.9
RT	340+16.15		SIDEROAD - CR 2700N	59.8
LT	370+03.00		SIDEROAD - CR 350E	332.0
LT	406+35.14		SIDEROAD - CR 2800N	166.4
RT	406+37.00		SIDEROAD - CR 2800N	114.8
LT	453+94.74		SIDEROAD - CR 2900N	145.9
RT	453+98.42		SIDEROAD - CR 2900N	146.4
IL 47 RURAL SUB-TOTAL =				1793.2
IL 47 RURAL TOTAL =				2266.5
IL 47 RURAL USE =				2267.0
PROJECT TOTAL = 3216.0				

40600985 PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT

STATION		SQ YD
URBAN		
RT 138+40.67	CE	144.8
RT 167+08.74	SIDE ROAD (RESERVE COURT)	149.1
IL 47 URBAN TOTAL =		293.9
PROJECT TOTAL =		293.9
USE =		294.0

44000155 HMA SURFACE REMOVAL, 1-1/2"

40600990 TEMPORARY RAMP

STATION	LENGTH	WIDTH	SQ YD
<u>IL 47</u>			
URBAN			
LT 23+52.70	7.5	15.0	12.5
RT 23+52.70	7.5	15.0	12.5
IL 47 URBAN TOTAL =			25.0
RURAL			
335+13.40	7.5	26.0	21.7
336+42.40	7.5	26.0	21.7
523+18.80	7.5	26.0	21.7
IL 47 RURAL SUB-TOTAL =			65.0
<u>US 136</u>			
RURAL			
1651+00.00	7.5	24.0	20.0
1674+00.00	7.5	24.0	20.0
IL 47 RURAL SUB-TOTAL =			40.0
IL 47 RURAL TOTAL =			105.0
PROJECT TOTAL =			130.0

STATION	TO	STATION	LENGTH	WIDTH	SQ YD
<u>IL 47</u>					
URBAN					
104+00.00		193+75.00	8,975.0	26.0	25,927.8
IL 47 URBAN SUB-TOTAL =					25,927.8
RURAL					
193+75.00		335+34.70	14,159.7	26.0	40,905.8
336+42.50		406+69.2 (BK)	7,026.7	26.0	20,299.4
401+00.92 (AH)		499+00.00	9,799.1	26.0	28,308.5
499+00.00		507+35.6 (BK)	835.7	VAR.	4,537.0
507+49.60 (AH)		522+88.70	1,539.1	VAR.	8,138.0
IL 47 RURAL SUB-TOTAL =					102,188.6
<u>US 136</u>					
RURAL					
1651+30.00		1673+70.00	2,240.0	VAR.	9,834.0
IL 47 RURAL SUB-TOTAL =					9,834.0

STATION	TO	STATION	LENGTH	WIDTH	SQ YD
<u>IL 47 SHOULDERS</u>					
URBAN					
LT 25+00.00		LT 38+72.76 (BK)	1372.8	8.0	1220.2
LT 97+14.44		LT 104+00.00	685.6	8.0	609.4
RT 25+00.00		RT 38+72.76 (BK)	1372.8	8.0	1220.2
RT 97+14.44		RT 104+00.00	685.6	8.0	609.4
IL 47 URBAN SUB-TOTAL =					3659.2
RURAL					
RT 326+26.00		RT 332+67.50	641.5	8.0	570.2
LT 326+45.34		LT 332+88.50	643.2	8.0	571.7
LT 336+42.50		LT 336+64.56	22.1	5.0	12.3
RT 336+42.50		RT 336+61.17	18.7	5.0	10.4
RT 338+66.22		RT 342+63.65	397.4	8.0	353.3
LT 339+60.69		LT 342+12.81	252.1	7.0	196.1
IL 47 RURAL SUB-TOTAL =					1713.9
IL 47 URBAN TOTAL =					29587.0
IL 47 RURAL TOTAL =					113736.5
IL 47 RURAL USE =					113737.0
PROJECT TOTAL =					143324.0

44000100 PAVEMENT REMOVAL

STATION	DESCRIPTION	SQ YD
URBAN		
<u>TRAIL - I</u>		
RT 25+44.00	EXISTING SHOULDER	9.4
LT 25+44.00	EXISTING SHOULDER	9.4
TRAIL - I TOTAL =		18.8
TRAIL - I USE =		19.0
<u>IL 47</u>		
LT 108+00.00	EXISTING TRAIL	33.9
RT 108+00.00	EXISTING TRAIL	20.5
RT 132+82.00	MB TURNOUT	34.3
RT 156+70.00	MB TURNOUT	26.6
IL 47 URBAN TOTAL =		115.3
IL 47 URBAN USE =		116.0
RURAL		
RT 227+86.00	MB TURNOUT	40.9
RT 443+95.00	MB TURNOUT	28.0
IL 47 RURAL TOTAL =		68.9
IL 47 RURAL USE =		69.0

TRAIL - C

STATION	TO	STATION	DESCRIPTION	SQ YD
URBAN				
RT. 115+83.27		RT. 117+43.47	EXIST. PARKING LOT	244.2
TRAIL - C TOTAL =				244.2
TRAIL - C USE =				245.0
PROJECT TOTAL =				449.0
USE =				449.0

44000158 HMA SURFACE REMOVAL, 2 1/4"

STATION	TO	STATION	LENGTH	WIDTH	SQ YD
<u>IL 47</u>					
URBAN					
23+82.80		102+00.00	1,852.5	VAR	3,944.0
102+00.00		104+00.00	200.0	26.0	577.8
IL 47 URBAN TOTAL =					4,521.8
IL 47 US					4,522.0
PROJECT TOTAL =					4522.0

*NOTE: AREAS FOR VARIABLE WITH LOCATIONS WERE OBTAINED USING CADD.

44000200 DRIVEWAY PAVEMENT REMOVAL

STATION	DESCRIPTION	SQ YD
URBAN		
TRAIL - I		
RT 17+38.94	CE	37.3
RT 18+53.17	CE	49.5
RT 19+06.78	CE	104.4
RT 19+97.34	CE	116.1
	TRAIL - I TOTAL =	307.3
	TRAIL - I USE =	308.0

STATION	DESCRIPTION	SQ YD
IL 47		
LT 107+44.06	CE	112.1
RT 114+70.55	CE	39.9
LT 127+48.00	FE	16.4
LT 132+00.63	PE	78.2
LT 132+82.86	PE	42.4
LT 134+55.23	PE	11.7
RT 135+48.00	FE	18.0
RT 139+43.00	FE	16.8
RT 141+97.19	PE	44.1
LT 142+42.00	FE	17.1
RT 149+18.00	FE	21.0
RT 151+93.00	FE - OLD NO LONGER IN USE	63.4
LT 153+26.00	FE	21.8
LT 156+82.00	FE	42.4
LT 162+58.69	FE	17.7
RT 177+51.00	FE	19.7
RT 184+15.00	FE	21.4
LT 184+56.00	FE	17.9
	IL 47 URBAN TOTAL =	622.0

STATION	DESCRIPTION	SQ YD
RURAL		
RT 194+11.00	FE	11.3
LT 198+64.04	PE	15.1
RT 207+20.00	FE	20.0
RT 213+89.00	FE	13.4
RT 220+36.00	FE	29.4
LT 220+40.00	FE	24.1
LT 226+33.00	FE	19.9
RT 227+03.00	FE	18.9
LT 227+94.50	PE	61.9
LT 228+87.78	PE	65.8
LT 229+88.00	FE	49.3
LT 236+30.87	PE	40.6
LT 248+73.00	FE	18.3
RT 260+29.00	FE	20.5
LT 260+46.41	CE	46.3
LT 261+19.00	FE	21.2
LT 273+45.11	PE	42.8
RT 273+48.47	PE	90.9
RT 280+40.00	FE	19.2
LT 280+46.00	FE	31.4
LT 300+04.00	FE	22.1
RT 300+05.00	FE	19.9
RT 311+86.78	PE/MB TURNOUT	63.9
LT 311+87.00	FE	14.3
LT 316+90.00	FE	20.3
LT 320+04.51	PE/MB TURNOUT	34.3
LT 322+05.61	PE/MB TURNOUT	90.8
	IL 47 RURAL SUB-TOTAL =	925.9

44000200 DRIVEWAY PAVEMENT REMOVAL

STATION	DESCRIPTION	SQ YD
IL 47		
RURAL		
RT 326+04.15	PE	80.8
LT 342+93.84	PE/MB TURNOUT	52.2
RT 345+03.78	CE	93.3
LT 345+43.48	PE	26.8
LT 346+14.37	CE	50.7
RT 353+86.00	FE	20.1
LT 357+83.86	PE	63.0
RT 359+09.00	FE	14.2
LT 359+10.48	PE	42.9
LT 359+73.77	PE	45.6
RT 365+38.00	FE	17.4
RT 366+16.49	PE	113.2
LT 366+44.17	CE	54.5
RT 366+84.16	PE	45.7
RT 385+14.83	CE	61.9
RT 397+79.00	FE	17.3
LT 403+57.35	PE	43.0
LT 427+23.00	FE	14.5
RT 427+32.00	FE	21.4
LT 428+68.29	CE	114.3
RT 433+00.00	FE	18.4
RT 435+67.41	PE	45.2
LT 440+53.00	FE	16.1
LT 444+41.76	PE	44.7
RT 480+41.11	PE	87.6
LT 480+83.24	CE	44.4
LT 494+54.00	FE	41.7
LT 495+87.13	PE	48.6
RT 496+61.00	FE	16.3
LT 504+92.40	CE	46.7
RT 518+89.93	PE	47.7
	IL 47 RURAL SUB-TOTAL =	1450.2

STATION	DESCRIPTION	SQ YD
US 136		
RURAL		
RT 1654+91.33	PE	42.0
RT 1659+93.99	CE	31.8
RT 1670+01.47	PE	39.9
RT 1671+10.60	PE	78.6
	IL 47 RURAL SUB-TOTAL =	192.3
	TRAIL - I TOTAL =	308.0
	IL 47 URBAN TOTAL =	622.0
	IL 47 RURAL TOTAL =	2568.4
	IL 47 RURAL USE =	2569.0
	PROJECT TOTAL =	3499.0

44000400 GUTTER REMOVAL

IL 47

STATION	TO	STATION	DESCRIPTION	FOOT
RURAL				
RT 339+87.00		RT 340+46.50	TYPE B GUTTER AT CR 2700N	59.5
			IL 47 RURAL TOTAL =	59.5
			IL 47 RURAL USE =	60.0
			PROJECT TOTAL =	60.0

44003510 MEDIAN REMOVAL PARTIAL DEPTH

IL 47

STATION	TO	STATION	FOOT
RURAL			
500+82.00		503+19.50	237.5
516+77.80		519+13.30	235.5
		IL 47 RURAL SUB-TOTAL =	473.0
US 136			
1654+76.00		1657+12.70	236.7
1669+65.60		1672+04.90	239.3
		IL 47 RURAL SUB-TOTAL =	476.0
		IL 47 RURAL TOTAL =	949.0
		PROJECT TOTAL =	949.0

44000500 COMBINATION CURB AND GUTTER REMOVAL

STATION	TO	STATION	DESCRIPTION	FOOT
URBAN				
TRAIL - I				
RT 16+80.84		17+03.75	NE QUADRANT OF FRANKLIN ST.	42.0
RT 20+41.64		20+57.86	SE QUADRANT OF EASTWOOD DR.	24.0
RT 24+24.00		24+40.00	SOUTH SIDE I-74 E.B. ON RAMP	24.0
			TRAIL - I TOTAL =	90.0
RURAL				
IL 47				
LT 406+14.00		406+16.30	SW QUADRANT OF CR 2800N	5.0
LT 406+51.40		406+61.90	NW QUADRANT OF CR 2800N	20.0
			IL 47 RURAL TOTAL =	25.0
			PROJECT TOTAL =	115.0

44004000 PAVED DITCH REMOVAL

IL 47

STATION	TO	STATION	FOOT
URBAN			
RT 99+41.70		RT 99+79.75	38.0
		IL 47 URBAN TOTAL =	38.0
		PROJECT TOTAL =	38.0

44000600 SIDEWALK REMOVAL

STATION	TO	STATION	AREA (SQ. FT.)
URBAN			
TRAIL - I			
RT 16+82.50		RT 18+41.65	742.0
RT 21+47.60		RT 21+59.80	28.0
		TRAIL - I TOTAL =	770.0
		PROJECT TOTAL =	770.0

REMOVAL OF EXISTING STRUCTURES

ITEM NO	CULVERT NO.	STATION	EACH
IL 47			
URBAN			
50100300	1	135+81.00	1.0
50100400	2	181+27.00	1.0
		IL 47 URBAN TOTAL =	2.0
RURAL			
50100500	3	224+48.00	1.0
50100600	4	352+60.00	1.0
50100700	5	438+01.00	1.0
		IL 47 RURAL TOTAL =	3.0
		PROJECT TOTAL =	5.0

NOTE: AREA OBTAINED USING CADD.

50104400 CONCRETE HEADWALL REMOVAL

IL 47	STATION	EACH
	URBAN	
	RT 99+17.94	2.0
	RT 99+90.22	2.0
	RT 102+82.22	1.0
	IL 47 URBAN TOTAL =	5.0
	RURAL	
	LT 405+00.00	1.0
	LT 403+57.35	2.0
	IL 47 RURAL TOTAL =	3.0
	PROJECT TOTAL =	8.0

50105220 PIPE CULVERT REMOVAL

IL 47	STATION	FOOT
	URBAN	
	RT 99+17.94	49.0
	RT 99+90.22	21.0
	RT 102+82.22	36.0
	RT 108+00.00	41.0
	LT 108+06.00	21.0
	RT 118+53.87	99.0
	RT 125+99.42	81.0
	LT 127+47.25	33.0
	LT 132+00.63	33.0
	LT 132+82.86	31.0
	LT 134+55.23	35.0
	RT 135+47.76	28.0
	RT 139+43.90	29.0
	RT 141+97.19	23.0
	LT 142+44.98	32.0
	RT 149+15.80	34.0
	LT 153+26.82	28.0
	LT 156+82.29	25.0
	RT 158+55.12	93.0
	LT 162+58.69	32.0
	LT 175+69.00	8.0
	LT 176+00.21	67.0
	RT 177+50.39	35.0
	181+26.00	144.0
	RT 184+13.89	31.0
	LT 181+58.90	10.0
	LT 181+60.00	10.0
	LT 181+61.00	10.0
	LT 181+74.50	10.0
	LT 181+78.40	10.0
	LT 184+55.18	27.0
	IL 47 URBAN TOTAL =	1166.0
	RURAL	
	RT 194+11.10	28.0
	RT 207+20.14	32.0
	RT 213+89.05	30.0
	RT 220+36.33	62.0
	LT 220+40.29	57.0
	LT 226+33.00	35.0
	RT 227+00.51	32.0
	LT 227+94.50	31.0
	LT 228+87.78	31.0
	LT 229+89.44	34.0
	LT 233+77.62	70.0
	RT 233+78.49	56.0
	LT 236+30.87	29.0
	LT 248+72.96	30.0
	RT 260+26.38	33.0
	LT 260+46.41	30.0
	LT 261+18.90	34.0
	LT 273+45.11	34.0
	RT 273+48.47	34.0
	IL 47 RURAL SUB-TOTAL =	722.0

50105220 PIPE CULVERT REMOVAL

IL 47	STATION	FOOT
	RURAL	
	RT 280+40.75	29.0
	LT 280+46.23	29.0
	RT 286+95.05	55.0
	LT 286+96.75	55.0
	RT 295+11.90	20.0
	RT 300+04.30	32.0
	LT 300+04.92	35.0
	LT 311+87.29	31.0
	LT 313+46.08	51.0
	RT 313+46.41	73.0
	LT 316+88.79	34.0
	LT 320+04.51	45.0
	LT 322+05.61	45.0
	RT 322+06.03	41.0
	RT 326+04.15	31.0
	LT 332+55.26	51.0
	LT 342+93.84	33.0
	RT 345+03.78	93.0
	LT 345+43.48	44.0
	LT 346+14.37	33.0
	RT 352+63.90	12.0
	RT 353+84.89	32.0
	LT 357+83.86	27.0
	LT 359+73.77	26.0
	RT 365+37.84	31.0
	RT 366+16.49	31.0
	LT 366+44.17	28.0
	RT 366+84.16	41.0
	RT 385+14.83	33.0
	LT 397+77.70	34.0
	RT 397+77.50	34.0
	LT 403+57.35	25.0
	LT 405+00.00	166.0
	LT 427+22.63	32.0
	RT 427+32.09	29.0
	LT 428+68.29	38.0
	RT 432+99.39	33.0
	RT 435+67.41	24.0
	LT 440+53.84	32.0
	RT 453+98.42	55.0
	RT 466+18.00	45.0
	RT 480+41.11	32.0
	LT 480+83.24	35.0
	RT 481+89.53	24.0
	LT 494+54.54	24.0
	LT 495+87.13	26.0
	RT 496+62.06	30.0
	LT 504+92.40	30.0
	RT 518+89.93	32.0
	IL 47 RURAL SUB-TOTAL =	1901.0
	US 136	
	RURAL	
	LT 1654+90.78	28.0
	RT 1654+91.33	26.0
	RT 1659+93.99	155.0
	RT 1670+01.47	31.0
	IL 47 RURAL SUB-TOTAL =	240.0
	IL 47 RURAL TOTAL =	2863.0
	PROJECT TOTAL =	4029.0

60255410 CATCH BASINS TO BE CLEANED

STATION	EACH
RURAL	
LT 268+87.70	1.0
LT 342+32.30	1.0
RT 342+95.70	1.0
RT 363+38.64	1.0
LT 422+43.61	1.0
LT 482+58.00	1.0
RT 482+58.00	1.0
RT 490+56.80	1.0
LT 490+69.10	1.0
RT 506+64.10	1.0
RT 506+67.80	1.0
IL 47 RURAL TOTAL =	11.0
PRO	11.0

60500050 REMOVING CATCH BASINS

STATION	EACH
RURAL	
RT 295+11.85	1.0
LT 406+58.00	1.0
IL 47 RURAL TOTAL =	2.0
PROJECT TOTAL =	2.0

63200310 GUARDRAIL REMOVAL

STATION	TO	STATION	FOOT
URBAN			
LT 36+40.40		LT 97+21.96	240.0
LT 174+91.09		LT 175+85.97	114.1
		IL 47 URBAN TOTAL =	354.1
		IL 47 URBAN USE =	355.0
RURAL			
RT 249+36.66		RT 250+87.33	150.7
LT 249+50.74		LT 251+26.99	176.3
RT 293+88.00		RT 295+76.30	188.3
RT 333+71.90		RT 335+49.44	177.5
LT 334+34.75		LT 335+48.71	114.0
RT 336+28.66		RT 337+42.37	113.7
LT 336+28.86		LT 338+06.22	177.4
RT 465+27.64		RT 467+03.49	175.8
LT 465+30.61		LT 467+06.67	176.1
		IL 47 RURAL TOTAL =	1449.7
		IL 47 RURAL USE =	1450.0
		PROJECT TOTAL =	1805.0

78300200 RAISED REFLECTIVE PAVEMENT MARKER REMOVAL

IL 47	STATION	TO	STATION	LENGTH	EACH
URBAN					
	25+35.00		27+35.00	200.0	3.0
	32+65.00		34+65.00	200.0	3.0
	99+62.00		193+75.00	9413.0	118.0
			IL 47 URBAN TOTAL =		124.0
RURAL					
	193+75.00		406+69.20 (BK)	21294.2	267.0
	401+00.92 (AH)		500+82.00	9981.1	125.0
	519+60.00		523+18.70	358.7	5.0
			IL 47 RURAL SUB-TOTAL =		397.0
US 136	STATION	TO	STATION	LENGTH	EACH
RURAL					
	1651+00.00		1654+75.00	375.0	5.0
	1671+55.00		1674+00.00	245.0	4.0
			IL 47 RURAL SUB-TOTAL =		9.0
			IL 47 RURAL TOTAL =		406.0
			PROJECT TOTAL =		530.0

X0321461 PILLAR REMOVAL

STATION	O/S	EACH
URBAN		
112+10.00	45' RT	1.0
112+35.00	45' RT	1.0
	TRAIL -	2.0
	PROJECT TOTAL =	2.0

X0549200 CLEANING PAVED DITCH

STATION	TO	STATION	FOOT
URBAN			
RT 36+69.50		RT 38+72.76 (BK)	203.3
RT 97+14.44 (AH)		RT 98+90.00	175.6
RT 100+05.00		RT 102+55.00	250.0
LT 38+68.00		LT 38+72.76 (BK)	4.8
LT 97+14.44 (AH)		LT 103+62.00	647.6
		IL 47 URBAN TOTAL =	1281.1
		IL 47 URBAN USE =	1282.0
		PROJECT TOTAL =	1282.0

X4400196 HOT MIX ASPHALT SURFACE REMOVAL (SPECIAL)

STATION	DESCRIPTION	SQ YD
URBAN		
RT 99+17.94	CE	74.6
RT 99+90.22	PE	35.6
RT 102+82.22	CE	48.8
IL 47 URBAN TOTAL =		159.0
PROJECT TOTAL =		159.0

X5015225 PIPE CULVERT REMOVAL (SPECIAL)

STATION	FOOT
URBAN	
LT 155+60.00	10.0
RT 155+60.00	9.0
IL 47 URBAN TOTAL = 19.0	
RURAL	
LT 233+48.00	9.0
RT 233+48.00	9.0
LT 243+15.00	8.0
RT 243+15.00	10.0
LT 278+50.00	9.0
RT 278+50.00	11.0
LT 295+10.00	10.0
LT 310+49.00	10.0
LT 319+00.00	12.0
LT 324+34.00	7.0
RT 324+34.00	9.0
RT 363+36.00	4.0
LT 451+00.00	10.0
RT 451+00.00	9.0
LT 454+41.00	5.0
IL 47 RURAL TOTAL = 132.0	
PROJECT TOTAL = 151.0	

X6660410 REMOVE RIGHT-OF-WAY MARKERS

STATION	EACH
IL 47 URBAN	
RT 135+95.99	1.0
RT 136+01.71	1.0
RT 140+83.50	1.0
RT 141+57.65	1.0
RT 143+28.04	1.0
RT 156+06.23	1.0
IL 47 URBAN TOTAL = 6.0	
RURAL	
RT 233+38.42	1.0
RT 233+97.30	1.0
LT 243+19.05	1.0
LT 278+85.69	1.0
RT 313+63.31	1.0
RT 324+37.23	1.0
LT 334+00.00	1.0
LT 338+00.00	1.0
RT 339+81.45	1.0
LT 352+38.22	1.0
RT 363+29.52	1.0
RT 377+72.94	1.0
LT 382+47.56	1.0
RT 413+80.54	1.0
RT 413+87.90	1.0
RT 413+92.65	1.0
LT 482+90.48	1.0
IL 47 RURAL TOTAL = 17.0	
PROJECT TOTAL = 23.0	

X4403300 CONCRETE MEDIAN REMOVAL

IL 47	STATION	TO	STATION	LENGTH	WIDTH	SQ FT
URBAN						
	25+34.80	CENTER MEDIAN	25+79.70	44.9	VAR.	282.0
	34+19.40	CENTER MEDIAN	34+58.70	39.3	VAR.	233.0
						IL 47 URBAN TOTAL = 515.0
RURAL						
	502+31.00	CENTER MEDIAN	506+88.40	457.4	VAR.	3,744.6
	507+95.10	CENTER MEDIAN	517+64.73	969.6	VAR.	10,631.8
	506+85.00	RT. ISLAND MEDIAN	507+05.00 RT.	VAR.	VAR.	299.6
	506+89.00	LT. ISLAND MEDIAN	507+09.00 LT.	VAR.	VAR.	166.2
	507+76.00	RT. ISLAND MEDIAN	507+97.00 RT.	VAR.	VAR.	170.2
	507+81.00	LT. ISLAND MEDIAN	508+05.00 LT.	VAR.	VAR.	344.5
						IL 47 RURAL SUB-TOTAL = 15,356.9
US 136						
RURAL						
	1656+25.00	CENTER MEDIAN	1661+01.00	476.0	VAR.	4,001.8
	1661+82.00	CENTER MEDIAN	1670+54.00	872.0	VAR.	9,530.6
						IL 47 RURAL SUB-TOTAL = 13,532.4
						IL 47 RURAL TOTAL = 28,889.3
						IL 47 RURAL USE = 28,890.0
						PROJECT TOTAL = 29,405.0

*NOTE: AREAS OBTAINED USING CADD.

Z0022800 FENCE REMOVAL

STATION	TO	STATION	FOOT
URBAN			
TRAIL - C			
RT 115+06.00		RT 115+50.00	44.0
			TRAIL - C TOTAL = 44.0
IL 47			
LT 136+11.28		LT 136+11.28	16.0
LT 158+44.27		LT 158+44.54	10.0
			IL 47 URBAN TOTAL = 26.0
RURAL			
LT 278+86.14		LT 279+51.50	65.4
RT 294+93.68		RT 295+45.09	52.0
LT 346+64.94		LT 346+65.62	7.0
			IL 47 RURAL TOTAL = 124.4
			IL 47 RURAL USE = 125.0
			PROJEC 195.0

Z0042200 PORTLAND CEMENT CONCRETE PAVEMENT SURFACE REMOVAL

STATION	SIDEROAD	THICKNESS (IN.)	SQ YD
URBAN			
158+55.12	EAST OAK VALLEY RD.	6.5	255.5
		IL 47 URBAN TOTAL =	255.5
		IL 47 URBAN USE =	256.0
		PROJECT TOTAL =	256.0

FILE NAME =	USER NAME = ceerlockbm	DESIGNED - BMC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES REMOVAL ITEMS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\pwork\ceerlockbm\d0187612\0970844-sht-schedules of removal items.dgn	DRAWN - BMC	REVISED -	326			136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	33	
#MODELNAME#	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 70844				
	PLOT DATE = 10/10/2014	DATE -	REVISED -			SCALE: N/A	SHEET 9 OF 9 SHEETS	STA. ----- TO STA. -----	ILLINOIS FED. AID PROJECT	

EARTHWORK SCHEDULE

STATION	TO	STATION	20200100 EARTH EXCAVATION CU YD	ADJUSTED FOR SHRINKAGE CU YD	EMBANKMENT CU YD	EARTHWORK BALANCE WASTE (-) SHORTAGE (+) CU YD
TRAIL - I						
URBAN						
RT 17+00.00		RT 26+00.00	107.0	80.3	93.0	12.8
TRAIL - I TOTAL =			107.0	80.3	93.0	12.8
IL 47						
URBAN						
LT/RT 31+00.00		LT/RT 193+75.00	4615.3	3461.5	1394.0	-2067.5
IL 47 URBAN TOTAL =			4615.3	3461.5	1394.0	-2067.5
IL 47 URBAN USE =			4616.0			
RURAL						
LT/RT 193+75.00		LT/RT 523+00.00	21017.5	15763.1	7916.4	-7846.7
IL 47 RURAL SUB-TOTAL =			21017.5	15763.1	7916.4	-7846.7
US 136						
RURAL						
LT/RT 1648+00.00		LT/RT 1674+00.00	1478.0	1108.5	206.0	-902.5
IL 47 RURAL SUB-TOTAL =			1478.0	1108.5	206.0	-902.5
IL 47 RURAL TOTAL =			22495.5	16871.6	8122.4	-8749.2
IL 47 RURAL USE =			22496.0	16872.0	8123.0	-8750.0
PROJECT TOTAL =			27219.0	16952.3	8216.0	-8737.3

NOTE: EXCAVATION QTY. WAS CALCULATED USING AVERAGE END AREAS FROM CUT/FILL DATA OBTAINED USING GEOPAK.

20700220 POUROUS GRANULAR EMBANKMENT

IL 47

* NOTE: SEE AR BOX CULVERTS TABLE FOR QUANTITY.

20800150 TRENCH BACKFILL

STATION	TO	STATION	PAY LENGTH (FOOT)	VOLUME (CU YD)
URBAN				
TRAIL - C				
RT 107+92.00		108+06.0	14.0	1.5
LT 107+98.00		108+14.0	16.0	1.7
RT 126+19.00		126+33.0	14.0	1.5
RT 120+25.00		120+72.7	47.8	5.0
TRAIL - C SUB-TOTAL =				9.6
STR 1 - STR 2	120+77.25	121+97.7	120.5	33.7
STR 2 - STR 3	122+02.25	122+97.7	95.5	31.7
STR 3 - STR 4	123+02.25	123+97.7	95.5	39.0
STR 4 - STR 5	124+02.25	124+72.7	70.5	48.2
TRAIL - C SUB-TOTAL =				152.6
TRAIL - C TOTAL =				162.3
TRAIL - C USE =				163.0
IL 47				
RURAL				
LT 228+68.00		229+38.5	16.0	1.7
IL 47 RURAL TOTAL =				1.7
IL 47 RURAL USE =				2.0
PROJECT TOTAL =				165.0

NOTE: VOLUME MULTIPLIERS WERE OBTAINED FROM CONSTRUCTION MANUAL TABLES FOR TRENCH BACKFILL.

20900110 POROUS GRANULAR BACKFILL

IL 47

* NOTE: SEE PIPE CULVERT TABLES FOR QUANTITY.

FILE NAME =	USER NAME = ceorlockbm	DESIGNED - BMC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES PLAN PAY ITEMS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\dot\ceorlockbm\d0187612\0970844-sht-schedules of plan pay items.dgn	DRAWN - BMC	REVISED -	326					136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	34	
MODELNAME	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -		SCALE: N/A SHEET 1 OF 47 SHEETS STA. ----- TO STA. -----			CONTRACT NO. 70844				
	PLOT DATE = 10/10/2014	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

LANDSCAPING TABLE

	STATION	TO	STATION	AREA (SQ FT)	AREA (ACRE)	25000210 SEEDING CLASS 2A (ACRE)	25000350 SEEDING CLASS 7 (ACRE)	25000400 NITROGEN FERT. (POUNDS)	25000500 PHOS FERT. (POUNDS)	25000600 POTAS. FERT. (POUNDS)	25100105 MULCH METHOD 1 (ACRE)	25100115 MULCH METHOD 2 (ACRE)	25200100 SODDING (SQ YD)	28000250 TEMPORARY EROSION SEEDING (POUND)
TRAIL - I	URBAN													
	RT 17+00.00		RT 26+00.00	167586.0	0.25	-	-	15.0	15.0	15.0	-	-	1210.0	75.0
	TRAIL - I TOTAL =							15.0	15.0	15.0	-	-	1210.0	75.0
IL 47	LT 19+50.00		LT 193+75.00	167586.0	4.00	4.00	12.0	360.0	360.0	360.0	12.0	4.0	-	1200.0
	RT 19+50.00		RT 193+75.00	190910.0	4.50	4.50	13.5	405.0	405.0	405.0	13.5	4.5	-	1350.0
	LT 181+00.00	**	LT 182+00.00	2726.0	0.25	-	-	-	-	-	-	-	-	-
	IL 47 URBAN TOTAL =					8.5	25.5	765.0	765.0	765.0	25.5	8.5	-	2550.0
	RURAL													
	LT 193+75.00		LT 524+00.00	560837.0	13.00	13.00	39.0	1170.0	1170.0	1170.0	39.0	13.0	-	3900.0
	RT 193+75.00		RT 524+00.00	535524.0	12.50	12.50	37.5	1125.0	1125.0	1125.0	37.5	12.5	-	3750.0
	IL 47 RURAL SUB-TOTAL =					25.5	76.5	2295.0	2295.0	2295.0	76.5	25.5	-	7650.0
US 136	RURAL													
	LT 1647+00.00		LT 1661+00.00	22861.5	0.50	0.50	0.5	45.0	45.0	45.0	0.5	0.5	-	150.0
	LT 1663+00.00		LT 1674+00.00	14138.0	0.50	0.50	0.5	45.0	45.0	45.0	0.5	0.5	-	150.0
	IL 47 RURAL SUB-TOTAL =					1.0	1.0	90.0	90.0	90.0	1.0	1.0	-	300.0
	IL 47 RURAL TOTAL =					26.5	77.5	2385.0	2385.0	2385.0	77.5	26.5	-	7950.0
	PROJECT TOTAL =					35.00	*	*	*	*	*	*	1210.0	*
	USE =					35.00	*	*	*	*	*	*	1210.0	*

SCHEDULE NOTE: IT IS THE INTENT THAT CLASS 7 SEEDING BE PLACED ALONG WITH THE CLASS 2A SEEDING AS A SUPPLEMENTAL SEEDING TO PROMOTE ACCELERATED VEGETATION GROWTH.

* ADDITIONAL PAY ITEM QUANTITY CAN BE FOUND IN THE CLASS 4A SEEDING SCHEDULE TABLE ON THE FOLLOWING SHEET.
 ** QUANTITY REQUIRED FOR TEMPORARY AGGREGATE RUNAROUND TO BE CONSTRUCTED FOR AR CULVERT REPLACEMENT AT THIS LOCATION.

CLASS 4A SEEDING SCHEDULE (IL 47 MAINLINE)

	STATION	TO	STATION	AREA (SQ FT)	AREA (ACRE)	28000250 TEMP. EROSION SEEDING (POUNDS)	25000312 SEEDING CLASS 4A (ACRE)	25000350 SEEDING CLASS 7 (ACRE)	25000400 NITROGEN FERT. (POUNDS)	25000500 PHOS FERT. (POUNDS)	25000600 POTAS. FERT. (POUNDS)	25100105 MULCH METHOD 1 (ACRE)	25100115 MULCH METHOD 2 (ACRE)
	URBAN												
LT	125+35.00		126+35.00	3200.0	0.07	7.3	0.07	0.07	6.6	6.6	6.6	0.07	0.07
LT	135+31.00		136+31.00	3500.0	0.08	8.0	0.08	0.08	7.2	7.2	7.2	0.08	0.08
LT	141+70.00		142+70.00	3200.0	0.07	7.3	0.07	0.07	6.6	6.6	6.6	0.07	0.07
LT	155+15.00		156+15.00	2200.0	0.05	5.1	0.05	0.05	4.5	4.5	4.5	0.05	0.05
LT	161+00.00		162+00.00	2200.0	0.05	5.1	0.05	0.05	4.5	4.5	4.5	0.05	0.05
LT	175+12.00		175+88.00	1672.0	0.04	3.8	0.04	0.04	3.5	3.5	3.5	0.04	0.04
LT	181+00.00		182+00.00	5700.0	0.13	13.1	0.13	0.13	11.8	11.8	11.8	0.13	0.13
LT	191+85.00		192+85.00	2200.0	0.05	5.1	0.05	0.05	4.5	4.5	4.5	0.05	0.05
IL 47 URBAN SUB-TOTAL =						54.8	0.5	0.5	49.3	49.3	49.3	0.5	0.5

	STATION	TO	STATION	AREA (SQ FT)	AREA (ACRE)	28000250 TEMP. EROSION SEEDING (POUNDS)	25000312 SEEDING CLASS 4A (ACRE)	25000350 SEEDING CLASS 7 (ACRE)	25000400 NITROGEN FERT. (POUNDS)	25000500 PHOS FERT. (POUNDS)	25000600 POTAS. FERT. (POUNDS)	25100105 MULCH METHOD 1 (ACRE)	25100115 MULCH METHOD 2 (ACRE)
RURAL													
LT	223+50.00		225+00.00	4800.0	0.11	11.0	0.11	0.11	9.9	9.9	9.9	0.11	0.11
LT	233+00.00		233+66.00	1452.0	0.03	3.3	0.03	0.03	3.0	3.0	3.0	0.03	0.03
LT	242+65.00		243+65.00	3700.0	0.08	8.5	0.08	0.08	7.6	7.6	7.6	0.08	0.08
LT	249+81.00		250+81.00	2200.0	0.05	5.1	0.05	0.05	4.5	4.5	4.5	0.05	0.05
LT	278+00.00		279+00.00	3200.0	0.07	7.3	0.07	0.07	6.6	6.6	6.6	0.07	0.07
LT	294+60.00		295+60.00	3200.0	0.07	7.3	0.07	0.07	6.6	6.6	6.6	0.07	0.07
LT	310+00.00		311+00.00	2200.0	0.05	5.1	0.05	0.05	4.5	4.5	4.5	0.05	0.05
LT	318+50.00		319+50.00	2200.0	0.05	5.1	0.05	0.05	4.5	4.5	4.5	0.05	0.05
LT	323+85.00		324+85.00	3700.0	0.08	8.5	0.08	0.08	7.6	7.6	7.6	0.08	0.08
LT	352+10.00		353+10.00	2700.0	0.06	6.2	0.06	0.06	5.6	5.6	5.6	0.06	0.06
LT	362+85.00		363+85.00	2200.0	0.05	5.1	0.05	0.05	4.5	4.5	4.5	0.05	0.05
LT	382+57.00		383+57.00	3200.0	0.07	7.3	0.07	0.07	6.6	6.6	6.6	0.07	0.07
LT	413+50.00		414+50.00	3200.0	0.07	7.3	0.07	0.07	6.6	6.6	6.6	0.07	0.07
LT	437+50.00		438+50.00	2200.0	0.05	5.1	0.05	0.05	4.5	4.5	4.5	0.05	0.05
LT	450+50.00		451+50.00	2200.0	0.05	5.1	0.05	0.05	4.5	4.5	4.5	0.05	0.05
LT	454+10.00		454+91.00	2688.0	0.06	6.2	0.06	0.06	5.6	5.6	5.6	0.06	0.06
LT	465+65.00		466+65.00	3200.0	0.07	7.3	0.07	0.07	6.6	6.6	6.6	0.07	0.07
IL 47 RURAL SUB-TOTAL =						110.7	1.1	1.1	99.7	99.7	99.7	1.1	1.1

	STATION	TO	STATION	AREA (SQ FT)	AREA (ACRE)	28000250 TEMP. EROSION SEEDING (POUNDS)	25000312 SEEDING CLASS 4A (ACRE)	25000350 SEEDING CLASS 7 (ACRE)	25000400 NITROGEN FERT. (POUNDS)	25000500 PHOS FERT. (POUNDS)	25000600 POTAS. FERT. (POUNDS)	25100105 MULCH METHOD 1 (ACRE)	25100115 MULCH METHOD 2 (ACRE)
URBAN													
RT	135+31.00		136+31.00	2500.0	0.06	5.7	0.06	0.06	5.2	5.2	5.2	0.06	0.06
RT	141+18.00		142+18.00	3200.0	0.07	7.3	0.07	0.07	6.6	6.6	6.6	0.07	0.07
RT	175+12.00		176+12.00	4200.0	0.10	9.6	0.10	0.10	8.7	8.7	8.7	0.10	0.10
RT	180+60.00		181+60.00	5200.0	0.12	11.9	0.12	0.12	10.7	10.7	10.7	0.12	0.12
RT	191+85.00		192+85.00	2200.0	0.05	5.1	0.05	0.05	4.5	4.5	4.5	0.05	0.05
IL 47 URBAN SUB-TOTAL =						39.7	0.4	0.4	35.7	35.7	35.7	0.4	0.4

	STATION	TO	STATION	AREA (SQ FT)	AREA (ACRE)	28000250 TEMP. EROSION SEEDING (POUNDS)	25000312 SEEDING CLASS 4A (ACRE)	25000350 SEEDING CLASS 7 (ACRE)	25000400 NITROGEN FERT. (POUNDS)	25000500 PHOS FERT. (POUNDS)	25000600 POTAS. FERT. (POUNDS)	25100105 MULCH METHOD 1 (ACRE)	25100115 MULCH METHOD 2 (ACRE)
RURAL													
RT	223+50.00		225+00.00	5500.0	0.13	12.6	0.13	0.13	11.4	11.4	11.4	0.13	0.13
RT	233+00.00		233+65.00	1430.0	0.03	3.3	0.03	0.03	3.0	3.0	3.0	0.03	0.03
RT	242+65.00		243+65.00	2700.0	0.06	6.2	0.06	0.06	5.6	5.6	5.6	0.06	0.06
RT	249+81.00		250+81.00	2200.0	0.05	5.1	0.05	0.05	4.5	4.5	4.5	0.05	0.05
RT	278+00.00		279+00.00	2200.0	0.05	5.1	0.05	0.05	4.5	4.5	4.5	0.05	0.05
RT	294+60.00		295+60.00	3700.0	0.08	8.5	0.08	0.08	7.6	7.6	7.6	0.08	0.08
RT	310+00.00		311+00.00	2200.0	0.05	5.1	0.05	0.05	4.5	4.5	4.5	0.05	0.05
RT	318+50.00		319+50.00	2200.0	0.05	5.1	0.05	0.05	4.5	4.5	4.5	0.05	0.05
RT	323+85.00		324+85.00	3200.0	0.07	7.3	0.07	0.07	6.6	6.6	6.6	0.07	0.07
RT	352+10.00		353+10.00	3700.0	0.08	8.5	0.08	0.08	7.6	7.6	7.6	0.08	0.08
RT	362+85.00		363+85.00	3500.0	0.08	8.0	0.08	0.08	7.2	7.2	7.2	0.08	0.08
RT	382+57.00		383+57.00	2200.0	0.05	5.1	0.05	0.05	4.5	4.5	4.5	0.05	0.05
RT	413+50.00		414+50.00	3200.0	0.07	7.3	0.07	0.07	6.6	6.6	6.6	0.07	0.07
RT	437+50.00		438+50.00	3200.0	0.07	7.3	0.07	0.07	6.6	6.6	6.6	0.07	0.07
RT	450+50.00		451+50.00	2200.0	0.05	5.1	0.05	0.05	4.5	4.5	4.5	0.05	0.05
RT	454+10.00		454+91.00	2835.0	0.07	6.5	0.07	0.07	5.9	5.9	5.9	0.07	0.07
RT	465+65.00		466+65.00	3200.0	0.07	7.3	0.07	0.07	6.6	6.6	6.6	0.07	0.07
IL 47 RURAL SUB-TOTAL =						113.3	1.1	1.1	102.0	102.0	102.0	1.1	1.1

IL 47 URBAN TOTAL =	94.5	0.9	0.9	85.1	85.1	85.1	0.9	0.9
IL 47 URBAN USE =	95.0	1.0	1.0	86.0	86.0	86.0	1.0	1.0
IL 47 RURAL TOTAL =	224.1	2.2	2.2	201.7	201.7	201.7	2.2	2.2
IL 47 RURAL USE =	225.0	3.0	3.0	202.0	202.0	202.0	3.0	3.0
PROJECT TOTAL =	*	4.0	*	*	*	*	*	*

SCHEDULE NOTE: IT IS THE INTENT THAT CLASS 7 SEEDING BE PLACED ALONG WITH THE CLASS 4A SEEDING AS A SUPPLEMENTAL SEEDING TO PROMOTE ACCELERATED VEGETATION GROWTH.

* ADDITIONAL PAY ITEM QUANTITY CAN BE FOUND IN LANDSCAPING SCHEDULE TABLE LOCATED ON THE PREVIOUS SHEET.

FILE NAME =	USER NAME = ceerlockbm	DESIGNED - BMC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES PLAN PAY ITEMS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw_work\pwork\ceerlockbm\d0187612\d0870844-sht-schedules of plan pay items.dgn	DRAWN - BMC	REVISED -	326					136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	36	
PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -	CONTRACT NO. 70844									
#MODELNAME#	DATE -	REVISED -	ILLINOIS FED. AID PROJECT									
				SCALE: N/A	SHEET 3	OF 47 SHEETS	STA. -----	TO STA. -----				

28000305 TEMPORARY DITCH CHECKS

STATION	FOOT	STATION	FOOT
URBAN		URBAN	
-	-	RT 22+50.00	15.0
-	-	RT 23+50.00	15.0
TRAIL - I TOTAL =		TRAIL - I TOTAL = 30.0	

IL 47

LT 104+00.00	10.0	RT 104+00.00	10.0
LT 105+00.00	10.0	RT 105+00.00	10.0
LT 106+00.00	10.0	RT 106+00.00	10.0
LT -	-	RT 107+00.00	10.0
LT 109+00.00	10.0	RT 109+00.00	10.0
LT 110+00.00	10.0	RT 110+00.00	10.0
LT 111+00.00	10.0	RT 111+00.00	10.0
LT 112+00.00	10.0	RT 112+00.00	10.0
LT 113+00.00	10.0	RT 113+00.00	10.0
LT 114+00.00	10.0	RT -	-
LT 115+00.00	10.0	RT -	-
LT 116+00.00	10.0	RT 116+00.00	10.0
LT 117+00.00	10.0	RT 117+00.00	10.0
LT 118+00.00	10.0	RT -	-
LT 119+00.00	10.0	RT 119+00.00	10.0
LT 120+00.00	10.0	RT -	-
LT 121+00.00	10.0	RT -	-
LT 122+00.00	10.0	RT -	-
LT 123+00.00	10.0	RT -	-
LT 124+00.00	10.0	RT -	-
LT 125+00.00	10.0	RT -	-
LT 126+00.00	10.0	RT -	-
LT 127+00.00	10.0	RT 127+00.00	10.0
LT 128+00.00	10.0	RT 128+00.00	10.0
LT 129+00.00	10.0	RT 129+00.00	10.0
LT 130+00.00	10.0	RT 130+00.00	10.0
LT 131+00.00	10.0	RT 131+00.00	10.0
LT -	-	RT 132+00.00	10.0
LT -	-	RT 133+00.00	10.0
LT 134+00.00	10.0	RT -	-
LT -	-	RT 135+00.00	10.0
LT 135+65.00	10.0	RT 135+65.00	10.0
LT 136+00.00	10.0	RT 136+00.00	10.0
LT 137+00.00	10.0	RT 137+00.00	10.0
LT 138+00.00	10.0	RT -	-
LT 139+00.00	10.0	RT -	-
LT 140+00.00	10.0	RT -	-
LT 141+00.00	10.0	RT 141+00.00	10.0
LT -	-	RT 141+75.00	10.0
LT 142+00.00	10.0	RT -	-
LT 142+35.00	10.0	RT -	-
LT -	-	RT 143+00.00	10.0
LT 144+00.00	10.0	RT 144+00.00	10.0
LT 145+00.00	10.0	RT 145+00.00	10.0
LT 146+00.00	10.0	RT 146+00.00	10.0
LT 147+00.00	10.0	RT 147+00.00	10.0
LT 148+00.00	10.0	RT 148+00.00	10.0
LT 149+00.00	10.0	RT -	-
LT 150+00.00	10.0	RT -	-
LT 151+00.00	10.0	RT 151+00.00	10.0

IL 47 URBAN SUB-TOTAL = 440.0 320.0

28000305 TEMPORARY DITCH CHECKS

STATION	FOOT	STATION	FOOT
URBAN		URBAN	
LT 152+00.00	10.0	RT 152+00.00	10.0
LT -	-	RT 153+00.00	10.0
LT 154+00.00	10.0	RT 154+00.00	10.0
LT 155+00.00	10.0	RT 155+00.00	10.0
LT 156+00.00	10.0	RT 156+00.00	10.0
LT -	-	RT 157+00.00	10.0
LT 158+00.00	10.0	RT -	-
LT 159+00.00	10.0	RT -	-
LT 160+00.00	10.0	RT 160+00.00	10.0
LT 161+00.00	10.0	RT 161+00.00	10.0
LT 162+00.00	10.0	RT 162+00.00	10.0
LT 163+00.00	10.0	RT 163+00.00	10.0
LT 164+00.00	10.0	RT 164+00.00	10.0
LT 165+00.00	10.0	RT 165+00.00	10.0
LT 166+00.00	10.0	RT 166+00.00	10.0
LT 167+00.00	10.0	RT -	-
LT 168+00.00	10.0	RT -	-
LT 169+00.00	10.0	RT 169+00.00	10.0
LT 170+00.00	10.0	RT 170+00.00	10.0
LT 171+00.00	10.0	RT 171+00.00	10.0
LT 172+00.00	10.0	RT 172+00.00	10.0
LT 173+00.00	10.0	RT 173+00.00	10.0
LT 174+00.00	10.0	RT 174+00.00	10.0
LT 175+00.00	10.0	RT 175+00.00	10.0
LT 175+35.00	10.0	RT -	-
LT -	-	RT 175+50.00	10.0
LT -	10.0	RT 176+00.00	10.0
LT 177+00.00	10.0	RT 177+00.00	10.0
LT 178+00.00	10.0	RT 178+00.00	10.0
LT 179+00.00	10.0	RT 179+00.00	10.0
LT 180+00.00	10.0	RT 180+00.00	10.0
LT 180+50.00	10.0	RT 180+50.00	10.0
LT 181+00.00	10.0	RT -	-
LT -	-	RT 181+50.00	10.0
LT 182+00.00	10.0	RT 182+00.00	10.0
LT 183+00.00	10.0	RT 183+00.00	10.0
LT 184+00.00	10.0	RT -	-
LT 185+00.00	10.0	RT 185+00.00	10.0
LT 186+00.00	10.0	RT 186+00.00	10.0
LT 187+00.00	10.0	RT 187+00.00	10.0
LT 188+00.00	10.0	RT 188+00.00	10.0
LT 189+00.00	10.0	RT 189+00.00	10.0
LT 190+00.00	10.0	RT 190+00.00	10.0
LT 191+00.00	10.0	RT 191+00.00	10.0
LT 192+00.00	10.0	RT 192+00.00	10.0
LT 193+00.00	10.0	RT 193+00.00	10.0

IL 47 URBAN SUB-TOTAL = 420.0 390.0

RURAL		RURAL	
LT 194+00.00	10.0	RT -	-
LT 195+00.00	10.0	RT 195+00.00	10.0
LT 196+00.00	10.0	RT 196+00.00	10.0
LT 197+00.00	10.0	RT 197+00.00	10.0

IL 47 RURAL SUB-TOTAL = 40.0 30.0

28000305 TEMPORARY DITCH CHECKS

	STATION	FOOT		STATION	FOOT
	RURAL			RURAL	
LT	198+00.00	10.0	RT	198+00.00	10.0
LT	-	-	RT	199+00.00	10.0
LT	200+00.00	10.0	RT	200+00.00	10.0
LT	201+00.00	10.0	RT	201+00.00	10.0
LT	202+00.00	10.0	RT	202+00.00	10.0
LT	203+00.00	10.0	RT	203+00.00	10.0
LT	204+00.00	10.0	RT	204+00.00	10.0
LT	205+00.00	10.0	RT	205+00.00	10.0
LT	206+00.00	10.0	RT	206+00.00	10.0
LT	207+00.00	10.0	RT	-	-
LT	208+00.00	10.0	RT	208+00.00	10.0
LT	209+00.00	10.0	RT	209+00.00	10.0
LT	210+00.00	10.0	RT	210+00.00	10.0
LT	211+00.00	10.0	RT	211+00.00	10.0
LT	212+00.00	10.0	RT	212+00.00	10.0
LT	213+00.00	10.0	RT	213+00.00	10.0
LT	214+00.00	10.0	RT	-	-
LT	215+00.00	10.0	RT	215+00.00	10.0
LT	216+00.00	10.0	RT	216+00.00	10.0
LT	217+00.00	10.0	RT	217+00.00	10.0
LT	218+00.00	10.0	RT	218+00.00	10.0
LT	219+00.00	10.0	RT	219+00.00	10.0
LT	221+00.00	10.0	RT	221+00.00	10.0
LT	222+00.00	10.0	RT	222+00.00	10.0
LT	223+00.00	10.0	RT	223+00.00	10.0
LT	224+00.00	10.0	RT	224+00.00	10.0
LT	225+00.00	10.0	RT	225+00.00	10.0
LT	226+00.00	10.0	RT	226+00.00	10.0
LT	227+00.00	10.0	RT	-	-
LT	-	-	RT	228+00.00	10.0
LT	-	-	RT	229+00.00	10.0
LT	-	-	RT	230+00.00	10.0
LT	231+00.00	10.0	RT	231+00.00	10.0
LT	232+00.00	10.0	RT	232+00.00	10.0
LT	233+00.00	10.0	RT	233+00.00	10.0
LT	235+00.00	10.0	RT	235+00.00	10.0
LT	-	-	RT	236+00.00	10.0
LT	237+00.00	10.0	RT	237+00.00	10.0
LT	238+00.00	10.0	RT	238+00.00	10.0
LT	239+00.00	10.0	RT	239+00.00	10.0
LT	240+00.00	10.0	RT	240+00.00	10.0
LT	241+00.00	10.0	RT	241+00.00	10.0
LT	242+00.00	10.0	RT	242+00.00	10.0
LT	243+00.00	10.0	RT	243+00.00	10.0
LT	244+00.00	10.0	RT	244+00.00	10.0
LT	245+00.00	10.0	RT	245+00.00	10.0
LT	246+00.00	10.0	RT	246+00.00	10.0
LT	247+00.00	10.0	RT	247+00.00	10.0
LT	248+00.00	10.0	RT	248+00.00	10.0
LT	-	-	RT	249+00.00	10.0

IL 47 RURAL SUB-TOTAL = 440.0

28000305 TEMPORARY DITCH CHECKS

	STATION	FOOT		STATION	FOOT
	RURAL			RURAL	
LT	250+00.00	10.0	RT	250+00.00	10.0
LT	251+00.00	10.0	RT	251+00.00	10.0
LT	252+00.00	10.0	RT	252+00.00	10.0
LT	253+00.00	10.0	RT	253+00.00	10.0
LT	254+00.00	10.0	RT	254+00.00	10.0
LT	255+00.00	10.0	RT	255+00.00	10.0
LT	256+00.00	10.0	RT	256+00.00	10.0
LT	257+00.00	10.0	RT	257+00.00	10.0
LT	258+00.00	10.0	RT	258+00.00	10.0
LT	259+00.00	10.0	RT	259+00.00	10.0
LT	-	-	RT	261+00.00	10.0
LT	262+00.00	10.0	RT	262+00.00	10.0
LT	263+00.00	10.0	RT	263+00.00	10.0
LT	264+00.00	10.0	RT	264+00.00	10.0
LT	265+00.00	10.0	RT	265+00.00	10.0
LT	266+00.00	10.0	RT	266+00.00	10.0
LT	267+00.00	10.0	RT	267+00.00	10.0
LT	268+00.00	10.0	RT	268+00.00	10.0
LT	269+00.00	10.0	RT	269+00.00	10.0
LT	270+00.00	10.0	RT	270+00.00	10.0
LT	271+00.00	10.0	RT	271+00.00	10.0
LT	272+00.00	10.0	RT	272+00.00	10.0
LT	273+00.00	10.0	RT	273+00.00	10.0
LT	274+00.00	10.0	RT	274+00.00	10.0
LT	275+00.00	10.0	RT	275+00.00	10.0
LT	276+00.00	10.0	RT	276+00.00	10.0
LT	277+00.00	10.0	RT	277+00.00	10.0
LT	278+00.00	10.0	RT	278+00.00	10.0
LT	279+00.00	10.0	RT	279+00.00	10.0
LT	282+00.00	10.0	RT	282+00.00	10.0
LT	283+00.00	10.0	RT	283+00.00	10.0
LT	284+00.00	10.0	RT	284+00.00	10.0
LT	285+00.00	10.0	RT	285+00.00	10.0
LT	286+00.00	10.0	RT	286+00.00	10.0
LT	287+00.00	10.0	RT	287+00.00	10.0
LT	288+00.00	10.0	RT	288+00.00	10.0
LT	289+00.00	10.0	RT	289+00.00	10.0
LT	290+00.00	10.0	RT	290+00.00	10.0
LT	291+00.00	10.0	RT	291+00.00	10.0
LT	292+00.00	10.0	RT	292+00.00	10.0
LT	293+00.00	10.0	RT	293+00.00	10.0
LT	294+00.00	10.0	RT	294+00.00	10.0
LT	295+00.00	10.0	RT	295+00.00	10.0
LT	296+00.00	10.0	RT	296+00.00	10.0
LT	297+00.00	10.0	RT	297+00.00	10.0
LT	298+00.00	10.0	RT	298+00.00	10.0
LT	299+00.00	10.0	RT	299+00.00	10.0
LT	301+00.00	10.0	RT	301+00.00	10.0
LT	302+00.00	10.0	RT	302+00.00	10.0
LT	303+00.00	10.0	RT	303+00.00	10.0

470.0

IL 47 RURAL SUB-TOTAL = 490.0

500.0

FILE NAME =	USER NAME = ceorlockbm	DESIGNED - BMC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES PLAN PAY ITEMS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ce:\pw_work\p\midot\ceorlockbm\d0187612\0970844-sht-schedules of plan pay items.dgn		DRAWN - BMC	REVISED -					326	136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	38
\$MODELNAME\$	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -		SCALE: N/A SHEET 5 OF 47 SHEETS STA. ----- TO STA. -----			CONTRACT NO. 70844				
	PLOT DATE = 10/10/2014	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

28000305 TEMPORARY DITCH CHECKS

	STATION	FOOT			STATION	FOOT
	RURAL				RURAL	
LT	304+00.00	10.0		RT	304+00.00	10.0
LT	305+00.00	10.0		RT	305+00.00	10.0
LT	306+00.00	10.0		RT	306+00.00	10.0
LT	307+00.00	10.0		RT	307+00.00	10.0
LT	308+00.00	10.0		RT	308+00.00	10.0
LT	309+00.00	10.0		RT	309+00.00	10.0
LT	310+00.00	10.0		RT	310+00.00	10.0
LT	311+00.00	10.0		RT	311+00.00	10.0
LT	312+50.00	10.0		RT	312+50.00	10.0
LT	313+00.00	10.0		RT	313+00.00	10.0
LT	314+00.00	10.0		RT	314+00.00	10.0
LT	315+00.00	10.0		RT	315+00.00	10.0
LT	316+00.00	10.0		RT	316+00.00	10.0
LT	-	-		RT	317+00.00	10.0
LT	318+00.00	10.0		RT	318+00.00	10.0
LT	318+90.00	10.0		RT	318+90.00	10.0
LT	319+10.00	10.0		RT	319+10.00	10.0
LT	-	-		RT	320+00.00	10.0
LT	321+00.00	10.0		RT	321+00.00	10.0
LT	323+00.00	10.0		RT	323+00.00	10.0
LT	324+00.00	10.0		RT	324+00.00	10.0
LT	325+00.00	10.0		RT	325+00.00	10.0
LT	326+00.00	10.0		RT	-	-
LT	333+00.00	10.0		RT	333+00.00	10.0
LT	334+00.00	10.0		RT	334+00.00	10.0
LT	335+00.00	10.0		RT	335+00.00	10.0
LT	337+00.00	10.0		RT	337+00.00	10.0
LT	338+00.00	10.0		RT	338+00.00	10.0
LT	339+00.00	10.0		RT	-	-
LT	344+00.00	10.0		RT	344+00.00	10.0
LT	345+00.00	10.0		RT	-	-
LT	-	-		RT	346+00.00	10.0
LT	347+00.00	10.0		RT	347+00.00	10.0
LT	348+00.00	10.0		RT	348+00.00	10.0
LT	349+00.00	10.0		RT	349+00.00	10.0
LT	350+00.00	10.0		RT	350+00.00	10.0
LT	351+00.00	10.0		RT	351+00.00	10.0
LT	352+00.00	10.0		RT	352+00.00	10.0
LT	352+40.00	10.0		RT	352+40.00	10.0
LT	353+00.00	10.0		RT	353+00.00	10.0
LT	354+00.00	10.0		RT	-	-
LT	355+00.00	10.0		RT	355+00.00	10.0
LT	356+00.00	10.0		RT	356+00.00	10.0
LT	357+00.00	10.0		RT	357+00.00	10.0
LT	-	-		RT	358+00.00	10.0
LT	-	-		RT	360+00.00	10.0
LT	361+00.00	10.0		RT	361+00.00	10.0
LT	362+00.00	10.0		RT	362+00.00	10.0
LT	363+00.00	10.0		RT	363+00.00	10.0
LT	364+00.00	10.0		RT	364+00.00	10.0

IL 47 RURAL SUB-TOTAL = 450.0

460.0

28000305 TEMPORARY DITCH CHECKS

	STATION	FOOT			STATION	FOOT
	RURAL				RURAL	
LT	365+00.00	10.0		RT	365+00.00	10.0
LT	366+00.00	10.0		RT	-	-
LT	367+00.00	10.0		RT	-	-
LT	368+00.00	10.0		RT	368+00.00	10.0
LT	369+00.00	10.0		RT	369+00.00	10.0
LT	-	-		RT	370+00.00	10.0
LT	-	-		RT	371+00.00	10.0
LT	372+00.00	10.0		RT	372+00.00	10.0
LT	373+00.00	10.0		RT	373+00.00	10.0
LT	374+00.00	10.0		RT	374+00.00	10.0
LT	375+00.00	10.0		RT	375+00.00	10.0
LT	376+00.00	10.0		RT	376+00.00	10.0
LT	377+00.00	10.0		RT	377+00.00	10.0
LT	378+00.00	10.0		RT	378+00.00	10.0
LT	379+00.00	10.0		RT	379+00.00	10.0
LT	380+00.00	10.0		RT	380+00.00	10.0
LT	381+00.00	10.0		RT	381+00.00	10.0
LT	382+00.00	10.0		RT	382+00.00	10.0
LT	382+90.00	10.0		RT	382+90.00	10.0
LT	383+20.00	10.0		RT	383+20.00	10.0
LT	384+00.00	10.0		RT	-	-
LT	385+00.00	10.0		RT	385+00.00	10.0
LT	386+00.00	10.0		RT	386+00.00	10.0
LT	387+00.00	10.0		RT	387+00.00	10.0
LT	388+00.00	10.0		RT	388+00.00	10.0
LT	389+00.00	10.0		RT	389+00.00	10.0
LT	390+00.00	10.0		RT	390+00.00	10.0
LT	391+00.00	10.0		RT	391+00.00	10.0
LT	392+00.00	10.0		RT	392+00.00	10.0
LT	393+00.00	10.0		RT	393+00.00	10.0
LT	394+00.00	10.0		RT	394+00.00	10.0
LT	395+00.00	10.0		RT	395+00.00	10.0
LT	396+00.00	10.0		RT	396+00.00	10.0
LT	397+00.00	10.0		RT	397+00.00	10.0
LT	399+00.00	10.0		RT	399+00.00	10.0
LT	401+00.00	10.0		RT	401+00.00	10.0
LT	403+00.00	10.0		RT	403+00.00	10.0
LT	405+00.00	10.0		RT	405+00.00	10.0
LT	-	-		RT	406+00.00	10.0
STA. EQUATION: 406+69.20 (BK) = 401+00.92 (AH)						
LT	402+00.00	10.0		RT	402+00.00	10.0
LT	403+00.00	10.0		RT	403+00.00	10.0
LT	404+00.00	10.0		RT	404+00.00	10.0
LT	405+00.00	10.0		RT	405+00.00	10.0
LT	406+00.00	10.0		RT	406+00.00	10.0
LT	407+00.00	10.0		RT	407+00.00	10.0
LT	408+00.00	10.0		RT	408+00.00	10.0
LT	409+00.00	10.0		RT	409+00.00	10.0
LT	410+00.00	10.0		RT	410+00.00	10.0
LT	411+00.00	10.0		RT	411+00.00	10.0

IL 47 RURAL SUB-TOTAL = 460.0

460.0

28000305 TEMPORARY DITCH CHECKS

	STATION	FOOT		STATION	FOOT
	RURAL			RURAL	
LT	412+00.00	10.0	RT	412+00.00	10.0
LT	413+00.00	10.0	RT	413+00.00	10.0
LT	413+85.00	10.0	RT	413+85.00	10.0
LT	414+15.00	10.0	RT	414+15.00	10.0
LT	415+00.00	10.0	RT	415+00.00	10.0
LT	416+00.00	10.0	RT	416+00.00	10.0
LT	417+00.00	10.0	RT	417+00.00	10.0
LT	418+00.00	10.0	RT	418+00.00	10.0
LT	419+00.00	10.0	RT	419+00.00	10.0
LT	420+00.00	10.0	RT	420+00.00	10.0
LT	421+00.00	10.0	RT	421+00.00	10.0
LT	422+00.00	10.0	RT	422+00.00	10.0
LT	423+00.00	10.0	RT	423+00.00	10.0
LT	424+00.00	10.0	RT	424+00.00	10.0
LT	425+00.00	10.0	RT	425+00.00	10.0
LT	426+00.00	10.0	RT	426+00.00	10.0
LT	428+00.00	10.0	RT	428+00.00	10.0
LT	-	-	RT	429+00.00	10.0
LT	430+00.00	10.0	RT	430+00.00	10.0
LT	431+00.00	10.0	RT	431+00.00	10.0
LT	432+00.00	10.0	RT	432+00.00	10.0
LT	433+00.00	10.0	RT	-	-
LT	434+00.00	10.0	RT	434+00.00	10.0
LT	435+00.00	10.0	RT	435+00.00	10.0
LT	436+00.00	10.0	RT	436+00.00	10.0
LT	437+00.00	10.0	RT	437+00.00	10.0
LT	437+80.00	10.0	RT	437+80.00	10.0
LT	438+20.00	10.0	RT	438+20.00	10.0
LT	439+00.00	10.0	RT	439+00.00	10.0
LT	440+00.00	10.0	RT	440+00.00	10.0
LT	441+00.00	10.0	RT	441+00.00	10.0
LT	442+00.00	10.0	RT	442+00.00	10.0
LT	443+00.00	10.0	RT	443+00.00	10.0
LT	-	-	RT	444+00.00	10.0
LT	445+00.00	10.0	RT	445+00.00	10.0
LT	446+00.00	10.0	RT	446+00.00	10.0
LT	447+00.00	10.0	RT	447+00.00	10.0
LT	448+00.00	10.0	RT	448+00.00	10.0
LT	449+00.00	10.0	RT	449+00.00	10.0
LT	450+00.00	10.0	RT	450+00.00	10.0
LT	450+85.00	10.0	RT	450+85.00	10.0
LT	451+15.00	10.0	RT	451+15.00	10.0
LT	452+00.00	10.0	RT	452+00.00	10.0
LT	453+00.00	10.0	RT	453+00.00	10.0
LT	455+00.00	10.0	RT	455+00.00	10.0
LT	456+00.00	10.0	RT	456+00.00	10.0
LT	457+00.00	10.0	RT	457+00.00	10.0
LT	458+00.00	10.0	RT	458+00.00	10.0
LT	459+00.00	10.0	RT	459+00.00	10.0
LT	460+00.00	10.0	RT	460+00.00	10.0

IL 47 RURAL SUB-TOTAL = 480.0

28000305 TEMPORARY DITCH CHECKS

	STATION	FOOT		STATION	FOOT
	RURAL			RURAL	
LT	461+00.00	10.0	RT	461+00.00	10.0
LT	462+00.00	10.0	RT	462+00.00	10.0
LT	463+00.00	10.0	RT	463+00.00	10.0
LT	464+00.00	10.0	RT	464+00.00	10.0
LT	465+00.00	10.0	RT	465+00.00	10.0
LT	465+75.00	10.0	RT	465+75.00	10.0
LT	466+50.00	10.0	RT	466+50.00	10.0
LT	467+00.00	10.0	RT	467+00.00	10.0
LT	468+00.00	10.0	RT	468+00.00	10.0
LT	469+00.00	10.0	RT	469+00.00	10.0
LT	470+00.00	10.0	RT	470+00.00	10.0
LT	471+00.00	10.0	RT	471+00.00	10.0
LT	472+00.00	10.0	RT	472+00.00	10.0
LT	473+00.00	10.0	RT	473+00.00	10.0
LT	474+00.00	10.0	RT	474+00.00	10.0
LT	475+00.00	10.0	RT	475+00.00	10.0
LT	476+00.00	10.0	RT	476+00.00	10.0
LT	477+00.00	10.0	RT	477+00.00	10.0
LT	478+00.00	10.0	RT	478+00.00	10.0
LT	479+00.00	10.0	RT	479+00.00	10.0
LT	480+00.00	10.0	RT	480+00.00	10.0
LT	-	-	RT	481+00.00	10.0
LT	482+00.00	10.0	RT	-	-
LT	483+00.00	10.0	RT	483+00.00	10.0
LT	484+00.00	10.0	RT	484+00.00	10.0
LT	485+00.00	10.0	RT	485+00.00	10.0
LT	486+00.00	10.0	RT	486+00.00	10.0
LT	487+00.00	10.0	RT	487+00.00	10.0
LT	488+00.00	10.0	RT	488+00.00	10.0
LT	489+00.00	10.0	RT	489+00.00	10.0
LT	490+00.00	10.0	RT	490+00.00	10.0
LT	491+00.00	10.0	RT	491+00.00	10.0
LT	492+00.00	10.0	RT	492+00.00	10.0
LT	493+00.00	10.0	RT	493+00.00	10.0
LT	494+00.00	10.0	RT	494+00.00	10.0
LT	495+00.00	10.0	RT	495+00.00	10.0
LT	-	-	RT	496+00.00	10.0
LT	497+00.00	10.0	RT	497+00.00	10.0
LT	498+00.00	10.0	RT	498+00.00	10.0
LT	499+00.00	10.0	RT	499+00.00	10.0
LT	500+00.00	10.0	RT	500+00.00	10.0
LT	501+00.00	10.0	RT	501+00.00	10.0
LT	502+00.00	10.0	RT	502+00.00	10.0
LT	503+00.00	10.0	RT	503+00.00	10.0
LT	504+00.00	10.0	RT	504+00.00	10.0
LT	-	-	RT	505+00.00	10.0
LT	506+00.00	10.0	RT	506+00.00	10.0
LT	506+55.00	10.0	RT	506+55.00	10.0
LT	509+00.00	10.0	RT	509+00.00	10.0
LT	510+00.00	10.0	RT	510+00.00	10.0

IL 47 RURAL SUB-TOTAL = 470.0

28000305 TEMPORARY DITCH CHECKS							
	STATION		FOOT		STATION		FOOT
	RURAL				RURAL		
LT	511+00.00		10.0	RT	511+00.00		10.0
LT	512+00.00		10.0	RT	512+00.00		10.0
LT	513+00.00		10.0	RT	513+00.00		10.0
LT	514+00.00		10.0	RT	514+00.00		10.0
LT	515+00.00		10.0	RT	515+00.00		10.0
LT	516+00.00		10.0	RT	516+00.00		10.0
LT	517+00.00		10.0	RT	517+00.00		10.0
LT	518+00.00		10.0	RT	518+00.00		10.0
LT	519+00.00		10.0	RT	-		-
LT	520+00.00		10.0	RT	520+00.00		10.0
LT	521+00.00		10.0	RT	521+00.00		10.0
LT	522+00.00		10.0	RT	522+00.00		10.0
LT	523+00.00		10.0	RT	523+00.00		10.0
US 136							
RURAL							
RURAL							
LT	1649+00.00		10.0	RT	1649+00.00		10.0
LT	1650+00.00		10.0	RT	1650+00.00		10.0
LT	1651+00.00		10.0	RT	1651+00.00		10.0
LT	1652+00.00		10.0	RT	1652+00.00		10.0
LT	1653+00.00		10.0	RT	1653+00.00		10.0
LT	1654+00.00		10.0	RT	1654+00.00		10.0
LT	1656+00.00		10.0	RT	1656+00.00		10.0
LT	1657+00.00		10.0	RT	1657+00.00		10.0
LT	1658+00.00		10.0	RT	1658+00.00		10.0
LT	1659+00.00		10.0	RT	-		-
LT	1660+00.00		10.0	RT	-		-
LT	1662+10.00		10.0	RT	-		-
LT	1663+00.00		10.0	RT	1663+00.00		10.0
LT	1664+00.00		10.0	RT	1664+00.00		10.0
LT	1665+00.00		10.0	RT	1665+00.00		10.0
LT	1666+00.00		10.0	RT	1666+00.00		10.0
LT	1667+00.00		10.0	RT	1667+00.00		10.0
LT	1668+00.00		10.0	RT	1668+00.00		10.0
LT	1669+00.00		10.0	RT	1669+00.00		10.0
LT	1670+00.00		10.0	RT	-		-
LT	1671+00.00		10.0	RT	-		-
LT	1672+00.00		10.0	RT	-		-
LT	1673+00.00		10.0	RT	-		-
LT	1674+00.00		10.0	RT	1674+00.00		10.0

IL 47 RURAL SUB-TOTAL = 370.0 290.0

TRAIL - I TOTAL = 30.0
 IL 47 URBAN TOTAL = 1570.0
 IL 47 RURAL TOTAL = 6390.0
 PROJECT TOTAL = 7990.0

28000400 PERIMETER EROSION BARRIER

	STATION	TO	STATION	FOOT
URBAN				
LT	36+50.00		LT 96+50.00	200.0
RT	107+50.00		RT 109+50.00	200.0
RT	111+50.00		RT 113+50.00	200.0
RT	121+75.00		RT 125+25.00	351.0
RT	135+00.00		RT 136+50.00	150.0
RT	139+75.00		RT 141+75.00	200.0
RT	171+50.00		RT 175+00.00	350.0
RT	181+75.00		RT 184+00.00	222.0
RT	192+00.00		RT 193+25.00	125.0

IL 47 URBAN TOTAL = 1,998.0

	STATION	TO	STATION	FOOT
RURAL				
LT	310+00.00		LT 311+50.00	150.0
LT	318+00.00		LT 319+75.00	175.0
LT	324+00.00		LT 326+00.00	201.0
LT	333+00.00		LT 335+25.00	250.0
LT	336+50.00		LT 340+00.00	350.0
LT	351+00.00		LT 354+00.00	300.0
LT	360+50.00		LT 364+00.00	350.0
LT	437+00.00		LT 439+00.00	200.0
LT	447+00.00		LT 452+00.00	500.0
LT	460+00.00		LT 463+00.00	300.0
LT	520+00.00		LT 523+00.00	300.0

RT	233+00.00		RT 233+50.00	71.0
RT	242+00.00		RT 252+00.00	1000.0
RT	267+00.00		RT 270+00.00	300.0
RT	276+50.00		RT 279+50.00	300.0
RT	290+50.00		RT 296+50.00	552.0
RT	332+50.00		RT 335+50.00	320.0
RT	381+00.00		RT 384+00.00	300.0
RT	466+25.00		RT 469+00.00	275.0
RT	521+00.00		RT 523+00.00	200.0

IL 47 RURAL TOTAL = 6394.0

PROJECT TOTAL = 8,392.0

28000500 INLET & PIPE PROTECTION

STATION	O/S	EACH
URBAN		
TRAIL - C		
RT 120+00.00	26.5	1.0
RT 120+75.00	20.5	1.0
RT 122+00.00	20.5	1.0
RT 123+00.00	20.5	1.0
RT 124+00.00	20.5	1.0
RT 124+75.00	20.5	1.0
TRAIL - C TOTAL =		6.0

URBAN

IL 47

RT 99+45.40	40.6	1.0
RT 103+00.00	31.4	1.0
LT 108+19.37	30.6	1.0
RT 108+22.50	31.6	1.0
RT 118+00.60	31.3	1.0
RT 120+00.00	26.6	1.0
RT 126+43.42	49.6	1.0
LT 127+67.25	30.0	1.0
LT 132+20.63	30.0	1.0
LT 133+02.86	30.0	1.0
RT 142+12.19	35.3	1.0
LT 143+20.01	32.5	1.0
RT 149+35.00	30.0	1.0
LT 153+06.82	31.0	1.0
LT 155+64.00	32.0	1.0
LT 156+62.29	31.3	1.0
RT 157+99.84	35.0	1.0
LT 162+78.74	30.7	1.0
LT 176+40.07	35.0	1.0
RT 177+70.39	29.9	1.0
RT 180+89.10	68.1	1.0
LT 181+59.10	61.4	1.0
LT 181+71.96	66.6	1.0
LT 181+78.41	63.7	1.0
RT 184+75.18	33.4	1.0
LT 184+75.17	32.4	1.0
IL 47 URBAN TOTAL =		26.0

RURAL

IL 47

RT 194+31.10	33.1	1.0
RT 207+00.14	30.0	1.0
RT 213+69.05	30.0	1.0
RT 220+06.33	29.0	1.0
LT 220+10.29	28.9	1.0
LT 226+65.40	29.5	1.0
RT 227+20.48	31.1	1.0
LT 227+79.00	30.0	1.0
IL 47 RURAL SUB-TOTAL =		8.0

28000500 INLET & PIPE PROTECTION

STATION	O/S	EACH
RURAL		
LT 229+39.94	30.0	1.0
LT 230+18.00	30.0	1.0
LT 233+47.50	34.2	1.0
LT 233+95.00	53.7	1.0
LT 234+13.81	33.9	1.0
RT 234+17.20	34.9	1.0
LT 243+15.00	32.0	1.0
LT 248+52.98	31.1	1.0
RT 260+46.38	28.0	1.0
LT 260+61.41	29.0	1.0
LT 261+38.90	29.0	1.0
LT 268+88.00	32.4	1.0
RT 273+68.47	31.0	1.0
LT 278+51.00	34.0	1.0
RT 286+65.05	29.7	1.0
LT 286+66.75	30.0	1.0
RT 295+11.00	52.5	1.0
LT 295+11.50	31.0	1.0
LT 299+84.92	29.0	1.0
RT 310+50.00	29.4	1.0
LT 312+07.28	32.0	1.0
RT 313+10.00	30.0	1.0
LT 313+18.00	30.7	1.0
LT 316+68.79	31.9	1.0
LT 318+99.00	34.0	1.0
LT 320+24.48	31.0	1.0
LT 321+85.62	30.5	1.0
RT 324+34.31	33.3	1.0
RT 326+24.14	29.3	1.0
LT 332+30.30	30.1	1.0
LT 342+32.00	29.0	1.0
RT 342+96.00	29.5	1.0
LT 343+22.94	29.2	1.0
RT 345+28.78	31.0	1.0
LT 345+63.47	29.0	1.0
LT 346+34.37	30.0	1.0
RT 352+74.40	54.0	1.0
RT 354+04.89	35.8	1.0
LT 358+03.83	29.8	1.0
LT 359+53.77	32.0	1.0
LT 363+35.90	29.0	1.0
RT 363+39.00	43.5	1.0
RT 365+57.80	35.4	1.0
RT 366+36.47	31.5	1.0
LT 366+64.17	30.0	1.0
IL 47 RURAL SUB-TOTAL =		45.0

28000500 INLET & PIPE PROTECTION

STATION	O/S	EACH
RURAL		
RT 367+04.16	33.0	1.0
RT 385+34.82	32.6	1.0
LT 397+97.00	35.5	1.0
RT 397+97.00	38.0	1.0
LT 403+42.35	31.0	1.0
LT 406+58.00	25.0	1.0
LT 406+60.33	35.0	1.0
LT 414+48.00	49.5	1.0
RT 414+48.00	49.5	1.0
LT 422+43.50	32.0	1.0
LT 427+42.63	32.0	1.0
RT 427+52.00	31.8	1.0
RT 432+79.39	30.0	1.0
RT 435+55.00	32.0	1.0
LT 440+73.84	31.6	1.0
LT 450+99.00	33.4	1.0
LT 451+25.00	39.5	1.0
RT 451+25.00	39.5	1.0
RT 453+70.00	29.8	1.0
RT 454+41.00	25.5	1.0
LT 466+07.00	50.0	1.0
RT 466+22.00	50.0	1.0
RT 466+24.00	60.0	1.0
RT 480+21.11	30.6	1.0
LT 480+63.24	35.0	1.0
RT 481+69.53	32.0	1.0
RT 482+06.00	39.7	1.0
LT 482+57.50	32.0	1.0
RT 482+57.50	31.0	1.0
RT 490+57.00	34.0	1.0
LT 490+69.00	33.0	1.0
LT 494+74.53	30.7	1.0
LT 496+07.12	30.9	1.0
RT 496+82.06	31.0	1.0
LT 504+77.40	41.8	1.0
RT 506+63.00	60.0	1.0
RT 506+64.00	67.0	1.0
RT 506+68.00	109.0	1.0
RT 508+09.00	61.0	1.0
RT 518+70.00	27.6	1.0
IL 47 RURAL SUB-TOTAL =		40.0

US 136

RURAL

LT 1654+71.68	36.6	1.0
RT 1654+72.00	39.0	1.0
RT 1660+10.00	35.3	1.0
LT 1663+37.00	80.0	1.0
RT 1670+21.47	29.6	1.0
IL 47 RURAL SUB-TOTAL =		5.0

TRAIL - C TOTAL = 6.0
 IL 47 URBAN TOTAL = 26.0
 IL 47 RURAL TOTAL = 98.0

PROJECT TOTAL = 130.0

FILE NAME =	USER NAME = ceerlockbm	DESIGNED - BMC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES PLAN PAY ITEMS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ci:\pw\work\p\dot\ceerlockbm\d0187612\0870844-sht-schedules of plan pay items.dgn	DRAWN - BMC	REVISED -	326					136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	42	
MODELNAME	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -		SCALE: N/A SHEET 9 OF 47 SHEETS STA. ----- TO STA. -----			CONTRACT NO. 70844				
	PLOT DATE = 11/18/2014	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

28100105 STONE RIPRAP, CLASS A3

28100107 STONE RIPRAP, CLASS A4

28100201 28200200
STONE RIPRAP FILTER
CLASS A1 FABRIC
TON SQ YD

STATION TO	STATION	DESCRIPTION	WIDTH	SQ YD
URBAN				
IL 47				
RT 99+41.70	RT 99+79.70	RIPRAP LINED DITCH	8.0	33.8
LT 135+74.00	LT 135+88.00	AR CULVERT	12.0	18.7
RT 135+74.00	RT 135+88.00	AR CULVERT	17.0	26.4
RT 141+58.80	RT 141+73.80	AR CULVERT	15.0	25.0
LT 142+13.50	LT 142+28.50	AR CULVERT	15.0	25.0
RT 155+50.00	RT 158+00.00	RIPRAP LINED DITCH	8.0	222.2
RT 159+47.33	RT 159+67.33	RIPRAP LINED DITCH	8.0	17.8
RT 192+26.00	RT 192+41.00	AR CULVERT	19.0	31.7
LT 192+26.00	LT 192+41.00	AR CULVERT	19.0	31.7
IL 47 URBAN TOTAL =				432.2
IL 47 URBAN USE =				433.0
RURAL				
RT 352+52.00	RT 352+68.00	AR CUVLERT	15.0	26.7
LT 352+51.00	LT 352+68.00	AR CUVLERT	17.0	32.1
RT 383+00.00	RT 383+15.00	AR CULVERT	12.0	20.0
LT 383+00.00	LT 383+15.00	AR CULVERT	15.0	25.0
RT 413+94.00	RT 414+09.00	AR CUVLERT	15.0	25.0
LT 413+94.00	LT 414+09.00	AR CUVLERT	15.0	25.0
RT 437+92.00	RT 438+10.00	AR CUVLERT	15.0	30.0
LT 437+92.00	LT 438+10.00	AR CUVLERT	15.0	30.0
LT 506+72.00	LT -	CURB CUT OUTLET	6.0	6.7
RT 506+72.00	RT -	CURB CUT OUTLET	6.0	6.7
LT 508+10.00	LT -	CURB CUT OUTLET	6.0	6.7
RT 508+14.00	RT -	CURB CUT OUTLET	6.0	6.7
IL 47 RURAL TOTAL =				240.4
IL 47 RURAL USE =				241.0
PROJECT TOTAL =				674.0

STATION TO	STATION	DESCRIPTION	WIDTH	28100107 SQ YD	28100201 CLASS A1 TON	28200200 FILTER FABRIC SQ YD
URBAN						
TRAIL - C						
RT 126+68.00	-	AR CULVERT	VAR.	44.0	13.2	44.0
TRAIL - C TOTAL =				44.0	13.2	44.0
TRAIL - C USE =				44.0	14.0	44.0
IL 47						
135+81.00	-	AR CULVERT BEDDING	VAR.	-	36.0	-
LT 175+43.00	LT 175+81.00	AR CULVERT	VAR.	62.9	18.9	62.9
RT 175+50.00	RT 175+75.00	AR CULVERT	13.0	36.1	10.8	36.1
RT 180+75.00	RT 181+20.00	AR CULVERT	20.0	100.0	30.0	100.0
181+27.00	-	AR CULVERT BEDDING	VAR.	-	54.0	-
RT 181+20.00	RT 182+00.00	RIPRAP LINED DITCH	8.0	71.1	21.3	71.1
LT 181+28.00	LT 181+80.00	AR CULVERT	28.0	161.8	48.5	161.8
IL 47 URBAN TOTAL =				431.9	219.6	431.9
IL 47 URBAN USE =				432.0	220.0	432.0
RURAL						
RT 224+30.00	RT 224+70.00	AR CULVERT	20.0	88.9	26.7	88.9
LT 224+30.00	LT 224+70.00	AR CULVERT	24.0	106.7	32.0	106.7
224+48.00	-	AR CULVERT BEDDING	VAR.	-	47.0	-
LT 250+13.00	LT 250+50.00	AR CULVERT	14.0	57.6	17.3	57.6
RT 250+22.00	RT 250+42.00	AR CULVERT	14.0	31.1	9.3	31.1
352+60.00	-	AR CULVERT BEDDING	VAR.	-	35.0	-
438+01.00	-	AR CULVERT BEDDING	VAR.	-	35.0	-
LT 465+97.00	LT 466+37.00	AR CULVERT	29.0	128.9	38.7	128.9
RT 465+97.00	RT 466+37.00	AR CULVERT	29.0	128.9	38.7	128.9
IL 47 RURAL TOTAL =				542.0	279.6	542.0
IL 47 RURAL USE =				542.0	280.0	542.0
PROJECT TOTAL =				1018.0	514.0	1018.0

31101200 SUB-BASE GRANULAR MATERIAL, TYPE B 4"

STATION	TO	STATION	DESCRIPTION	LENGTH	WIDTH	SQ YD
<u>IL 47 WIDENING</u>						
RURAL						
RT 497+00.00		RT 504+63.43	WIDENING	763.43	VAR.	328.3
LT 497+00.00		LT 504+37.89	WIDENING	737.89	VAR.	238.3
LT 505+90.00		LT 507+21.32	WIDENING	RADIUS	VAR.	89.2
RT 506+07.69		RT 507+02.84	WIDENING	RADIUS	VAR.	91.7
RT 507+71.67		RT 508+90.78	WIDENING	RADIUS	VAR.	67.4
LT 507+82.41		LT 508+60.05	WIDENING	RADIUS	VAR.	64.8
507+95.10		517+64.73	MEDIAN AREA	969.63	VAR.	1181.3
IL 47 RURAL SUB-TOTAL = 2061.0						

IL 47 MEDIAN / ISLAND AREAS

URBAN

25+34.80		25+79.70	MEDIAN AREA	44.90	VAR.	31.3
34+19.40		34+58.70	MEDIAN AREA	39.30	VAR.	25.9
IL 47 URBAN TOTAL = 57.2						
IL 47 URBAN USE = 58.0						

RURAL

502+31.00		506+88.40	MEDIAN AREA	457.40	VAR.	416.1
RT 506+85.00		RT 507+05.00	RT. ISLAND MEDIAN	VAR.	VAR.	33.3
LT 506+89.00		LT 507+09.00	LT. ISLAND MEDIAN	VAR.	VAR.	18.5
RT 507+76.00		RT 507+97.00	RT. ISLAND MEDIAN	VAR.	VAR.	18.9
LT 507+81.00		LT 508+05.00	LT. ISLAND MEDIAN	VAR.	VAR.	38.3
IL 47 RURAL SUB-TOTAL = 525.1						

US 136 WIDENING

RURAL

RT 1651+00.00		RT 1658+41.83	WIDENING	741.83	VAR.	39.1
LT 1651+00.00		LT 1654+07.80	WIDENING	307.80	VAR.	353.5
IL 47 RURAL SUB-TOTAL = 392.6						

US 136 MEDIAN AREAS

RURAL

1656+25.00		1661+01.00	MEDIAN AREA	476.00	VAR.	444.6
1661+82.00		1670+54.00	MEDIAN AREA	872.00	VAR.	1059.0
IL 47 RURAL SUB-TOTAL = 1503.6						
IL 47 RURAL TOTAL = 4482.3						
IL 47 RURAL USE = 4483.0						

NOTE: AREAS WERE OBTAINED USING CADD.

IL 47 URBAN TOTAL = 58.0
IL 47 RURAL TOTAL = 4483.0

PROJECT TOTAL = 4541.0

31102100 SUBBASE GRANULAR MATERIAL, TYPE C 4"

STATION	TO	STATION	LEN	WIDTH	SQ YD
<u>TRAIL - I</u>					
URBAN					
RT 25+44.50		RT 25+54.50	8.5	10.00	9.4
LT 25+44.50		LT 25+54.50	8.5	10.00	9.4
TRAIL - I TOTAL = 18.9					
PROJECT TOTAL = 19.0					

35101800 AGGREGATE BASE COURSE, TYPE B 6"

IL 47

* NOTE: QUANTITY FOR THIS ITEM IS LOCATED WITHIN THE INCIDENTAL HMA SURFACE TABLE

FILE NAME =	USER NAME = ceerlockbm	DESIGNED - BMC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES PLAN PAY ITEMS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\dot\ceerlockbm\d0187612\0870844-sht-schedules of plan pay items.dgn	DRAWN - BMC	REVISED -	326					136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	44	
MODELNAME	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -		SCALE: N/A SHEET 11 OF 47 SHEETS STA. ----- TO STA. -----			CONTRACT NO. 70844				
	PLOT DATE = 10/10/2014	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

INCIDENTAL HMA SURFACE TABLE

STATION	INCIDENTAL AREA DESCRIPTION				THICKNESS INCHES	35101800	35800100	ADDITIONAL INFORMATION	40800025	40800050
	PE SQ YD	MBTO SQ YD	CE SQ YD	SIDEROAD SQ YD		AGG BASE CRSE TYPE B, 6" SQ YD	PREPARATION OF BASE SQ YD		BIT MATLS PR CT POUND	INCIDENTAL HMA SURFACING TONS
IL 47 URBAN										
RT 99+17.94	-	-	74.6	-	2.25	-	-	-	33.6	9.4
RT 99+90.22	35.6	-	-	-	2.25	-	-	-	16.0	4.5
RT 102+82.22	-	-	48.8	-	2.25	-	-	-	22.0	6.1
LT 107+44.06	-	-	105.1	-	2.25	116.4	-	-	47.3	13.2
RT 114+70.55	-	-	47.5	-	2.25	41.9	-	-	21.4	6.0
RT 118+53.87 (LAKE OF WOODS)	-	-	-	360.6	2.25	-	-	-	162.3	45.4
RT 125+99.42 (BRIARCLIFF DR.)	-	-	-	273.6	2.25	-	273.6	(PREP. OF BASE, ENTIRE AREA)	123.1	34.5
LT 132+00.63	84.0	-	-	-	2.25	94.6	-	-	37.8	10.6
RT 132+82.00	-	26.4	-	-	2.25	34.2	-	-	11.9	3.3
LT 132+82.86	27.0	-	-	-	2.25	32.5	-	-	12.2	3.4
LT 134+55.23	24.4	-	-	-	2.25	29.4	-	-	11.0	3.1
RT 141+97.19	24.4	18.9	-	-	2.25	53.8	-	-	19.5	5.5
RT 149+68.67	-	-	56.3	-	2.25	66.2	-	-	25.3	7.1
RT 156+70.00	-	26.3	-	-	2.25	33.4	-	-	11.8	3.3
RT 167+08.78 (RESERVE COURT)	-	-	-	149.1	2.25	-	-	-	67.1	18.8
LT 176+00.21 (CR 2400N)	-	-	-	146.3	2.25	-	-	-	65.8	18.4
RT 193+54.96 (CR 2425N)	-	-	-	168.5	2.25	-	44.0	(PREP. OF BASE, 8' WIDE THRU RAD. - SE QUAD)	75.8	21.2
IL 47 URBAN TOTAL =						502.4	317.6	-	763.8	213.9
IL 47 URBAN USE =						503.0	318.0	-	764.0	214.0

RURAL										
STATION	PE SQ YD	MBTO SQ YD	CE SQ YD	SIDEROAD SQ YD	THICKNESS INCHES	35101800 AGG BASE CRSE TYPE B, 6" SQ YD	35800100 PREPARATION OF BASE SQ YD	ADDITIONAL INFORMATION	40800025 BIT MATLS PR CT POUND	40800050 INCIDENTAL HMA SURFACING TONS
LT 198+64.04	26.2	-	-	-	2.25	31.4	-	-	11.8	3.3
RT 227+86.00	-	26.3	-	-	2.25	34.0	-	-	11.8	3.3
LT 227+94.50	55.0	-	-	-	2.25	64.4	-	-	24.8	6.9
LT 228+87.78	54.9	-	-	-	2.25	64.0	-	-	24.7	6.9
LT 233+77.62 (CR 2500N)	-	-	-	133.9	2.25	-	-	-	60.3	16.9
RT 233+78.49 (CR 2500N)	-	-	-	133.6	2.25	-	35.6	(PREP. OF BASE, 8' WIDE THRU RAD. - SE QUAD)	60.1	16.8
LT 236+30.87	30.3	-	-	-	2.25	36.3	-	-	13.6	3.8
LT 260+46.41	-	-	31.7	-	2.25	37.7	-	-	14.3	4.0
LT 273+45.11	31.1	-	-	-	2.25	37.4	-	-	14.0	3.9
RT 273+48.47	74.8	-	-	-	2.25	85.0	-	-	33.7	9.4
RT 286+95.05 (CR 2600N)	-	-	-	142.9	2.25	-	-	-	64.3	18.0
LT 286+97.16 (CR 2600N)	-	-	-	141.0	2.25	-	-	-	63.5	17.8
RT 311+86.78	30.3	13.5	-	-	2.25	52.5	-	-	19.7	5.5
LT 313+46.08 (CR 2650N)	-	-	-	123.6	2.25	-	-	-	55.6	15.6
RT 313+46.41 (CR 2650N)	-	-	-	152.9	2.25	-	-	-	68.8	19.3
LT 320+04.51	25.3	18.9	-	-	2.25	53.2	-	-	19.9	5.6
LT 322+05.61	54.9	16.5	-	-	2.25	84.1	-	-	32.1	9.0
RT 322+06.03	32.8	20.2	-	-	2.25	63.2	-	-	23.9	6.7
RT 326+04.15	63.5	-	-	-	2.25	72.9	-	-	28.6	8.0
LT 326+50.00	-	21.2	-	-	2.25	26.4	-	-	9.5	2.7
RT 340+16.15 (CR 2700N)	-	-	-	59.8	2.25	-	-	-	26.9	7.5
LT 342+93.84	27.5	16.0	-	-	2.25	52.3	-	-	19.6	5.5
RT 345+03.78	-	-	49.1	-	2.25	58.0	-	-	22.1	6.2
LT 345+43.48	24.4	15.9	-	-	2.25	48.5	-	-	18.1	5.1
LT 346+14.37	-	-	31.7	-	2.25	37.8	-	-	14.3	4.0
LT 357+83.86	30.3	13.5	-	-	2.25	52.5	-	-	19.7	5.5
LT 359+10.48	30.3	-	-	-	2.25	36.4	-	-	13.6	3.8
LT 359+73.77	30.3	-	-	-	2.25	36.4	-	-	13.6	3.8
RT 366+16.49	84.6	-	-	-	2.25	97.5	-	-	38.1	10.7
LT 366+44.17	-	15.0	25.9	-	2.25	49.6	-	-	18.4	5.2
RT 366+84.16	33.3	-	-	-	2.25	40.1	-	-	15.0	4.2
LT 370+03.00 (CR 350E)	-	-	-	332.0	2.25	-	-	-	149.4	41.8
RT 385+14.83	-	-	37.9	-	2.25	44.5	-	-	17.1	4.8
LT 403+57.35	29.9	-	-	-	2.25	35.9	-	-	13.5	3.8
LT 406+35.14 (CR 2800N)	-	-	-	166.4	2.25	-	-	-	74.9	21.0
RT 406+37.00 (CR 2800N)	-	-	-	114.8	2.25	-	114.8	(PREP. OF BASE, ENTIRE AREA)	51.7	14.5
LT 428+68.29	-	-	100.6	-	2.25	111.9	-	-	45.3	12.7
RT 435+67.41	30.3	-	-	-	2.25	36.5	-	-	13.6	3.8
RT 443+95.00	-	26.3	-	-	2.25	33.7	-	-	11.8	3.3
LT 444+41.76	31.1	-	-	-	2.25	37.2	-	-	14.0	3.9
LT 453+94.74 (CR 2900N)	-	-	-	145.9	2.25	-	-	-	65.7	18.4
RT 453+98.42 (CR 2900N)	-	-	-	146.4	2.25	-	41.0	(PREP. OF BASE, 8' WIDE THRU RAD. - SE QUAD)	65.9	18.4
RT 480+41.00	90.5	-	-	-	2.25	103.0	-	-	40.7	11.4
LT 480+83.24	-	-	31.7	-	2.25	37.9	-	-	14.3	4.0
RT 481+89.53	-	-	31.7	-	2.25	37.9	-	-	14.3	4.0
LT 495+87.13	33.0	-	-	-	2.25	39.2	-	-	14.9	4.2
LT 504+92.40	-	-	33.8	-	2.25	40.0	-	-	15.2	4.3
RT 518+89.93	32.2	-	-	-	2.25	37.7	-	-	14.5	4.1
RT 1654+91.33	49.5	-	-	-	2.25	57.3	-	-	22.3	6.2
RT 1659+93.99	-	-	45.0	-	2.25	52.0	-	-	20.3	5.7
RT 1670+01.47	52.8	-	-	-	2.25	60.9	-	-	23.8	6.7
RT 1671+10.60	50.9	30.2	-	-	2.25	92.5	-	-	36.5	10.2
IL 47 RURAL TOTAL =						2109.7	191.4	-	1613.6	451.8
IL 47 RURAL USE =						2110.0	192.0	-	1614.0	452.0
PROJECT TOTAL =						2613.0	510.0	-	2378.0	666.0

FILE NAME =	USER NAME = cealockbm	DESIGNED - BMC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES PLAN PAY ITEMS				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ci:\pw\work\p\d01\cealockbm\d0187612\0870844-sht-schedules of plan pay items.dgn	DRAWN - BMC	REVISED -	326						136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	45	
\$MODELNAME\$	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -		SCALE: N/A SHEET 12 OF 47 SHEETS STA. ----- TO STA. -----				CONTRACT NO. 70844				
	PLOT DATE = 10/10/2014	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								

35300310 PORTLAND CEMENT CONCRETE BASE COURSE 8 1/2"

STATION	TO	STATION	DESCRIPTION	LENGTH	WIDTH	SQ YD
<u>IL 47 WIDENING</u>						
RURAL						
RT 497+00.00		RT 504+63.43	WIDENING	763.43	VAR.	328.3
LT 497+00.00		LT 504+37.89	WIDENING	737.89	VAR.	238.3
LT 505+90.00		LT 507+21.32	WIDENING	RADIUS	VAR.	89.2
RT 506+07.69		RT 507+02.84	WIDENING	RADIUS	VAR.	91.7
RT 507+71.67		RT 508+90.78	WIDENING	RADIUS	VAR.	67.4
LT 507+82.41		LT 508+60.05	WIDENING	RADIUS	VAR.	64.8
507+95.10		517+64.73	MEDIAN AREA	969.63	VAR.	1181.3
				IL 47 RURAL SUB-TOTAL = 2061.0		
<u>IL 47 MEDIAN / ISLAND AREAS</u>						
URBAN						
25+34.80		25+79.70	MEDIAN AREA	44.90	VAR.	31.3
34+19.40		34+58.70	MEDIAN AREA	39.30	VAR.	25.9
				IL 47 URBAN TOTAL = 57.2		
				IL 47 URBAN USE = 58.0		
RURAL						
502+31.00		506+88.40	MEDIAN AREA	457.40	VAR.	416.1
RT 506+85.00		RT 507+05.00	RT. ISLAND MEDIAN	VAR.	VAR.	33.3
LT 506+89.00		LT 507+09.00	LT. ISLAND MEDIAN	VAR.	VAR.	18.5
RT 507+76.00		RT 507+97.00	RT. ISLAND MEDIAN	VAR.	VAR.	18.9
LT 507+81.00		LT 508+05.00	LT. ISLAND MEDIAN	VAR.	VAR.	38.3
				IL 47 RURAL SUB-TOTAL = 525.1		
<u>US 136 WIDENING</u>						
RURAL						
RT 1651+00.00		RT 1658+41.83	WIDENING	741.83	VAR.	39.1
LT 1651+00.00		LT 1654+07.80	WIDENING	307.80	VAR.	353.5
				IL 47 RURAL SUB-TOTAL = 392.6		
<u>US 136 MEDIAN AREAS</u>						
RURAL						
1656+25.00		1661+01.00	MEDIAN AREA	476.00	VAR.	444.6
1661+82.00		1670+54.00	MEDIAN AREA	872.00	VAR.	1059.0
				IL 47 RURAL SUB-TOTAL = 1503.6		
				IL 47 RURAL TOTAL = 4482.3		
				IL 47 RURAL USE = 4483.0		
NOTE: AREAS WERE OBTAINED USING CADD.						
				IL 47 URBAN TOTAL = 58.0		
				IL 47 RURAL TOTAL = 4483.0		
				PROJECT TOTAL = 4541.0		

FILE NAME =	USER NAME = ceorlockbm	DESIGNED - BMC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES PLAN PAY ITEMS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pw\work\p\dot\ceorlockbm\d0187612\0970844-sht-schedules of plan pay items.dgn	DRAWN - BMC	REVISED -	326					136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	46	
\$MODELNAME\$	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -		SCALE: N/A SHEET 13 OF 47 SHEETS STA. ----- TO STA. -----			CONTRACT NO. 70844				
	PLOT DATE = 10/10/2014	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

40200800 AGGREGATE SURFACE COURSE, TYPE B

STATION	FE SQ YD	PE SQ YD	CE SQ YD	THICKNESS INCHES	TONS
URBAN					
IL 47					
RT 114+70.55	-	-	40.1	6.0	12.0
LT 127+47.25	99.6	-	-	6.0	29.9
LT 132+82.86	-	29.2	-	6.0	8.8
RT 133+95.20	123.8	-	-	6.0	37.1
LT 134+55.23	-	42.2	-	6.0	12.7
RT 139+43.90	99.4	-	-	6.0	29.8
RT 141+97.19	-	36.3	-	6.0	10.9
LT 143+00.00	137.7	-	-	6.0	41.3
RT 149+15.80	121.4	-	-	6.0	36.4
RT 149+68.67	-	-	61.8	6.0	18.5
LT 153+26.82	99.4	-	-	6.0	29.8
LT 156+82.29	126.1	-	-	6.0	37.8
LT 162+58.69	99.4	-	-	6.0	29.8
RT 177+50.39	99.4	-	-	6.0	29.8
RT 184+55.18	99.4	-	-	6.0	29.8
LT 184+55.18	117.3	-	-	6.0	35.2
IL 47 URBAN TOTAL =					429.8
IL 47 URBAN USE =					430.0

STATION	FE SQ YD	PE SQ YD	CE SQ YD	THICKNESS INCHES	TONS
RURAL					
IL 47					
RT 194+11.10	96.0	-	-	6.0	28.8
LT 198+64.04	-	26.8	-	6.0	8.0
RT 207+20.14	99.4	-	-	6.0	29.8
RT 213+89.05	99.4	-	-	6.0	29.8
RT 220+36.33	116.1	-	-	6.0	34.8
LT 220+40.29	126.9	-	-	6.0	38.1
LT 226+45.40	99.6	-	-	6.0	29.9
RT 227+00.15	99.4	-	-	6.0	29.8
LT 229+89.44	99.6	-	-	6.0	29.9
LT 236+30.87	-	24.7	-	6.0	7.4
LT 248+72.96	99.4	-	-	6.0	29.8
RT 260+26.38	139.5	-	-	6.0	41.9
LT 260+46.41	-	-	28.4	6.0	8.5
LT 261+18.90	99.4	-	-	6.0	29.8
LT 273+45.11	-	26.6	-	6.0	8.0
RT 280+40.75	99.4	-	-	6.0	29.8
LT 280+46.23	99.4	-	-	6.0	29.8
RT 300+04.30	99.4	-	-	6.0	29.8
LT 300+04.92	99.4	-	-	6.0	29.8
RT 311+86.78	-	26.4	-	6.0	7.9
LT 311+87.29	99.4	-	-	6.0	29.8
LT 316+88.79	126.1	-	-	6.0	37.8
LT 320+04.51	-	24.9	-	6.0	7.5
RT 322+06.03	-	40.0	-	6.0	12.0
IL 47 RURAL SUB-TOTAL =					598.7

40200800 AGGREGATE SURFACE COURSE, TYPE B (CONTINUED)

STATION	FE SQ YD	PE SQ YD	CE SQ YD	THICKNESS INCHES	TONS
RURAL					
IL 47					
LT 342+93.84	-	26.1	-	6.0	7.8
RT 345+03.78	-	-	54.7	6.0	16.4
LT 345+43.48	-	23.0	-	6.0	6.9
LT 346+14.37	-	-	43.9	6.0	13.2
RT 353+84.89	139.5	-	-	6.0	41.9
LT 357+83.86	-	24.6	-	6.0	7.4
RT 359+09.71	123.4	24.6	-	6.0	44.4
LT 359+10.48	-	27.0	-	6.0	8.1
LT 359+73.77	-	24.5	-	6.0	7.4
RT 365+37.48	153.2	-	-	6.0	46.0
LT 366+44.17	-	-	28.3	6.0	8.5
RT 366+84.16	-	56.9	-	6.0	17.1
RT 385+14.83	-	-	41.6	6.0	12.5
RT 397+77.50	130.7	-	-	6.0	39.2
LT 397+77.70	163.4	-	-	6.0	49.0
LT 403+57.35	-	24.6	-	6.0	7.4
LT 427+22.63	126.0	-	-	6.0	37.8
RT 427+32.09	99.4	-	-	6.0	29.8
RT 432+99.39	99.4	-	-	6.0	29.8
RT 435+67.41	-	24.7	-	6.0	7.4
LT 440+53.84	99.4	-	-	6.0	29.8
LT 444+41.76	-	26.5	-	6.0	8.0
LT 480+83.24	-	-	43.2	6.0	13.0
RT 481+89.53	-	-	28.4	6.0	8.5
LT 494+54.54	124.0	-	-	6.0	37.2
LT 495+87.13	-	53.0	-	6.0	15.9
RT 496+62.06	130.6	-	-	6.0	39.2
LT 504+92.40	-	-	29.0	6.0	8.7
RT 518+89.93	-	35.0	-	6.0	10.5
LT 1654+90.78	165.0	-	-	6.0	49.5
RT 1654+91.33	-	57.9	-	6.0	17.4
RT 1659+93.99	-	-	12.3	6.0	3.7
RT 1670+01.47	-	70.8	-	6.0	21.2
RT 1671+10.60	-	91.8	-	6.0	27.5
IL 47 RURAL SUB-TOTAL =					727.9
IL 47 RURAL TOTAL =					1326.6
IL 47 RURAL USE =					1327.0
PROJECT TOTAL =					1757.0

* NOTE: A CONVERSION FACTOR OF 1.8 TON/CU YD WAS UTILIZED TO CALCULATE QTY.

SCHEDULE OF MAINLINE RESURFACING (IL 47)			LENGTH (FOOT)	WIDTH (FOOT)	SQ YD	LEVEL BINDER THICKNESS INCHES	SURFACE COURSE THICKNESS INCHES	40600275 BIT MATLS PR CT POUND	40600637 LEVEL BINDER (MACHINE METHOD) IL-9.5FG, N70 TON	40603315 HMA SC "C", N70 TON	40603340 HMA SC "D", N70 TON	
STATION	TO	STATION										
URBAN												
IL 47	24+12.80	102+00.00 **	1,945.5	VAR	7,201.5	0.75	1.5	4861.0	302.5	-	604.9	
	102+00.00	193+75.00	9175.0	26.0	26,505.6	0.75	1.5	17891.3	1113.2	-	2226.5	
								IL 47 URBAN TOTAL =	22752.3	1415.7	-	2831.4
								IL 47 URBAN USE =	22753.0	1416.0	-	2832.0
RURAL												
	193+75.00	236+00.00	4225.0	26.0	12,205.6	0.75	1.5	8238.8	512.6	-	1025.3	
	236+00.00	335+34.70	9934.7	26.0	28,700.2	0.75	1.5	19372.7	1205.4	2410.8	-	
	336+42.50	406+69.20 (BK)	7026.7	26.0	20,299.4	0.75	1.5	13702.1	852.6	1705.1	-	
	401+00.92 (AH)	497+00.00	9599.1	26.0	27,730.7	0.75	1.5	18718.2	1164.7	2329.4	-	
	497+00.00	507+35.68 (BK)	1035.7	VAR.	5,998.0	0.75	1.5	4048.7	251.9	503.8	-	
	507+49.60 (AH)	522+88.69	1539.1	VAR.	7,446.1	0.75	1.5	5026.1	312.7	625.5	-	
								IL 47 RURAL SUB-TOTAL =	69,106.5	4,300.0	7,574.6	1,025.3

SCHEDULE OF SIDEROAD RESURFACING (US 136)			LENGTH (FOOT)	WIDTH (FOOT)	SQ YD	LEVEL BINDER THICKNESS INCHES	SURFACE COURSE THICKNESS INCHES	40600275 BIT MATLS PR CT POUND	40600637 LEVEL BINDER (MACHINE METHOD) IL-9.5FG, N70 TON	40603315 HMA SC "C", N70 TON	40603340 HMA SC "D", N70 TON	
STATION	TO	STATION										
RURAL												
	1651+30.00	1660+00.00	870.0	VAR.	3,753.9	0.75	1.5	2533.9	157.7	315.3	-	
US 136	1663+00.00	1673+70.00	1070.0	VAR.	4,786.3	0.75	1.5	3230.8	201.0	402.0	-	
								IL 47 RURAL SUB-TOTAL =	5,764.6	358.7	717.4	-
								IL 47 RURAL TOTAL =	74,871.1	4,658.6	8,292.0	1,025.3
								IL 47 RURAL USE =	74,872.0	4,659.0	8,293.0	1,026.0
								PROJECT TOTAL =	*	6,075.0	8,293.0	3,858.0

NOTES:
 * ADDITIONAL QUANTITIES FOR THIS PAY ITEM CAN BE FOUND IN HOT-MIX ASPHALT SHOULDERS (INLAY AREAS) TABLE LOCATED WITHIN THE SCHEDULE OF QUANTITIES PLAN PAY ITEMS (PROJECT TOTAL = 104623.0 POUND)
 ** STA. EQ. FALLS WITHIN THIS STATION RANGE. STA. 38+72.76(BK) = STA. 97+14.44(AH)
 ***AREAS WITH VARIABLE WIDTHS WERE OBTAINED USING CADD.

PCC DRIVEWAY PAVEMENT QUANTITIES

STATION	42300200 PCC DRVWY PVMNT 6 INCH (SQ YD)	42300300 PCC DRVWY PVMNT 7 INCH (SQ YD)	42300400 PCC DRVWY PVMNT 8 INCH (SQ YD)
URBAN			
TRAIL - I			
RT 17+38.94	-	-	28.9
RT 18+53.17	-	-	29.2
RT 19+06.78	-	-	65.6
RT 19+97.34	-	-	72.8
TRAIL - I TOTAL =	-	-	196.5
TRAIL - I USE =	-	-	197.0
IL 47 URBAN			
RT 158+55.12 (EAST OAK VALLEY RD.)	-	186.0	-
IL 47 URBAN TOTAL =	-	186.0	-
RURAL			
LT 332+55.26 (PE)	107.0	-	-
IL 47 RURAL TOTAL =	107.0	-	-
PROJECT TOTAL =	107.0	186.0	197.0

42400200 PCC SIDEWALK, 5"

TRAIL - I	STA.	TO	STA.	DESCRIPTION	LENGTH (FOOT)	WIDTH (FOOT)	AREA (SQ FT)
RT	16+81.32	RT	16+94.25	ADA RAMP (FRANKLIN ST.)	12.9	11.0	142.2
RT	16+93.24	RT	16+99.75	ADA RAMP (IL 47)	12.9	6.0	77.4
	3000+00.00		3003+51.73	MULTI-USE TRAIL - I	351.7	10.0	3517.3
	3004+66.13		3007+40.60	MULTI-USE TRAIL - I	274.5	10.0	2744.7
	3007+96.34		3008+63.02	MULTI-USE TRAIL - I	66.7	10.0	666.8
				TRAIL - I TOTAL =			7148.4
				TRAIL - I USE =			7149.0
TRAIL - C							
	2000+00.00		2010+89.25	MULTI-USE TRAIL - C	1089.3	10.0	10892.5
	2011+25.74		2018+20.00	MULTI-USE TRAIL - C	694.3	10.0	6942.6
	2018+82.76		2019+00.76	MULTI-USE TRAIL - C	18.0	10.0	180.0
	2019+00.76		TIE IN LOCATION	MULTI-USE TRAIL - C	23.3	10.0	233.0
				TRAIL - C TOTAL =			18248.1
				TRAIL - C USE =			18249.0
				PROJECT TOTAL =			25398.0

42400800 DETECTABLE WARNINGS

STATION	O/S	ROADWAY CROSSING	LENGTH (FOOT)	WIDTH (FOOT)	AREA (SQ FT)
<u>TRAIL - I</u>					
URBAN					
16+83.00	RT 47.5'	FRANKLIN ST.	2.0	10.0	20.0
16+96.00	RT 32.0'	IL 47	2.0	5.0	10.0
20+49.00	RT 43.0'	EASTWOOD DR.	2.0	13.0	26.0
24+30.00	RT 47.9'	I-74 RAMP	2.0	10.0	20.0
24+86.00	RT 46.4'	I-74 RAMP	2.0	10.0	20.0
25+50.00	RT 25.0'	IL 47	2.0	10.0	20.0
25+50.00	LT 24.0'	IL 47	2.0	10.0	20.0
TRAIL - I TOTAL =					136.0
<u>TRAIL - C</u>					
108+00.00	15' RT	IL 47	2.0	10.0	20.0
108+00.00	15' LT	IL 47	2.0	10.0	20.0
118+32.00	71' RT	PARK ENTRANCE	2.0	10.0	20.0
118+72.00	71' RT	PARK ENTRANCE	2.0	10.0	20.0
125+52.00	23' RT	BRIARCLIFF DR	2.0	10.0	20.0
126+25.00	30' RT	BRIARCLIFF DR	2.0	10.0	20.0
TRAIL - C TOTAL =					120.0
PROJECT TOTAL =					256.0

CLASS D PATCHING, 11 INCH

ROUTE	STATION	DIRECTION	LANE	LENGTH (FOOT)	WIDTH (FOOT)	44201777	44201781	44201783
						TYPE II 11 INCH (SQ YD)	TYPE III 11 INCH (SQ YD)	TYPE IV 11 INCH (SQ YD)
URBAN								
IL 47	23+69.00	NB	DL	6.0	13.0	8.7	-	-
IL 47	23+69.00	SB	DL	6.0	18.0	12.0	-	-
IL 47	34+90.00	NB	PL	12.0	13.0	-	17.3	-
IL 47	34+90.00	NB	DL	6.0	13.0	8.7	-	-
IL 47	35+30.00	SB	I-74 RAMP	12.0	8.0	10.7	-	-
IL 47	97+35.00	SB	DL	6.0	13.0	8.7	-	-
IL 47	97+68.00	SB	DL	6.0	13.0	8.7	-	-
IL 47	98+39.00	SB	DL	6.0	13.0	8.7	-	-
IL 47	101+82.00	SB	DL	8.0	13.0	11.6	-	-
IL 47	102+30.00	SB	DL	25.0	13.0	-	-	36.1
IL 47	103+43.00	SB	DL	80.0	13.0	-	-	115.6
IL 47 URBAN TOTAL =						77.6	17.3	151.7
IL 47 URBAN USE =						78.0	18.0	152.0
PROJECT TOTAL =						78.0	18.0	152.0

CLASS D PATCHING, 13 INCH

ROUTE	STATION	DIRECTION	LANE	LENGTH (FOOT)	WIDTH (FOOT)	44201803	44201807	44201809
						TYPE II 13 INCH (SQ YD)	TYPE III 13 INCH (SQ YD)	TYPE IV 13 INCH (SQ YD)
URBAN								
IL 47	112+82.00	SB	DL	10.0	13.0	14.4	-	--
IL 47	113+25.00	SB	DL	6.0	13.0	8.7	-	-
IL 47	117+65.00	SB	DL	8.0	13.0	11.6	-	-
IL 47	132+85.00	NB	DL	6.0	13.0	8.7	-	-
IL 47	132+85.00	SB	DL	6.0	13.0	8.7	-	-
IL 47	135+61.00	NB	DL	6.0	13.0	8.7	-	-
IL 47	136+80.00	NB	DL	6.0	13.0	8.7	-	-
IL 47	136+80.00	SB	DL	6.0	13.0	8.7	-	-
IL 47	141+88.00	NB	DL	10.0	13.0	14.4	-	-
IL 47	145+67.00	NB	DL	6.0	13.0	8.7	-	-
IL 47	147+40.00	NB	DL	25.0	13.0	-	-	36.1
IL 47	147+70.00	NB	DL	8.0	13.0	11.6	-	-
IL 47	147+70.00	SB	DL	8.0	13.0	11.6	-	-
IL 47	149+41.00	SB	DL	15.0	13.0	-	21.7	-
IL 47	149+98.00	NB	DL	6.0	13.0	8.7	-	-
IL 47	149+98.00	SB	DL	6.0	13.0	8.7	-	-
IL 47	174+65.00	NB	DL	12.0	13.0	-	17.3	-
IL 47 URBAN TOTAL =						141.6	39.0	36.1
IL 47 URBAN USE =						142.0	39.0	37.0

CLASS D PATCHING, 15 INCH

ROUTE	STATION	DIRECTION	LANE	LENGTH (FOOT)	WIDTH (FOOT)	44201803	44201807	44201809
						TYPE II 13 INCH (SQ YD)	TYPE III 13 INCH (SQ YD)	TYPE IV 13 INCH (SQ YD)
RURAL								
IL 47	195+53.00	NB	DL	8.0	13.0	11.6	-	-
IL 47	195+53.00	SB	DL	8.0	13.0	11.6	-	-
IL 47	214+71.00	NB	DL	10.0	13.0	14.4	-	-
IL 47	214+71.00	SB	DL	10.0	13.0	14.4	-	-
IL 47	223+52.00	NB	DL	6.0	13.0	8.7	-	-
IL 47	223+52.00	SB	DL	6.0	13.0	8.7	-	-
IL 47	260+00.00	SB	DL	15.0	13.0	-	21.7	-
IL 47	299+89.00	NB	DL	6.0	13.0	8.7	-	-
IL 47	299+89.00	SB	DL	6.0	13.0	8.7	-	-
IL 47	395+60.00	SB	DL	6.0	13.0	8.7	-	-
IL 47	407+97.00	NB	DL	15.0	13.0	-	21.7	-
IL 47	407+97.00	SB	DL	15.0	13.0	-	21.7	-
IL 47	414+12.00	SB	DL	25.0	13.0	-	-	36.1
IL 47	419+56.00	SB	DL	8.0	13.0	11.6	-	-
IL 47 RURAL SUB-TOTAL =						106.9	65.0	36.1

ROUTE	STATION	DIRECTION	LANE	LENGTH (FOOT)	WIDTH (FOOT)	44201827
						TYPE II 15 INCH (SQ YD)
RURAL						
IL 47	506+91.00	SB	PL	10.0	13.0	14.4
IL 47	506+91.00	SB	DL	10.0	13.0	14.4
IL 47	517+28.00	NB	DL	6.0	15.0	10.0
IL 47	520+93.00	NB	DL	6.0	13.0	8.7
IL 47	520+93.00	SB	DL	6.0	13.0	8.7
IL 47 RURAL TOTAL =						56.2
IL 47 RURAL USE =						57.0
PROJECT TOTAL =						57.0

CLASS D PATCHING, 13 INCH

ROUTE	STATION	DIRECTION	LANE	LENGTH (FOOT)	WIDTH (FOOT)	44201803	44201807	44201809
						TYPE II 13 INCH (SQ YD)	TYPE III 13 INCH (SQ YD)	TYPE IV 13 INCH (SQ YD)
RURAL								
US 136	1652+84.00	EB	DL	6.0	13.0	8.7	-	-
US 136	1652+84.00	WB	DL	6.0	13.0	8.7	-	-
US 136	1652+99.00	EB	DL	6.0	13.0	8.7	-	-
US 136	1652+99.00	WB	DL	6.0	13.0	8.7	-	-
US 136	1653+69.00	WB	DL	12.0	13.0	-	17.3	-
US 136	1653+94.00	EB	DL	10.0	13.0	14.4	-	-
US 136	1653+94.00	WB	DL	10.0	13.0	14.4	-	-
US 136	1657+59.00	WB	DL	6.0	13.0	8.7	-	-
US 136	1671+14.00	WB	DL	6.0	13.0	8.7	-	-
US 136	1672+03.00	EB	DL	6.0	15.0	10.0	-	-
US 136	1672+03.00	WB	DL	6.0	15.0	10.0	-	-
IL 47 RURAL SUB-TOTAL =						100.9	17.3	-
IL 47 RURAL TOTAL =						207.8	82.3	36.1
IL 47 RURAL USE =						208.0	83.0	37.0
PROJECT TOTAL =						350.0	122.0	74.0

PARTIAL DEPTH PATCHING, 6 INCH

ROUTE	STATION	DIRECTION	LANE	LENGTH (FOOT)	WIDTH (FOOT)	EX. HMA OVERLAY THICKNESS 3" OR MORE (INCH)	X4406260 PARTIAL DEPTH REMOVAL TYPE II, 6 INCH (SQ YD)	X4421000 PARTIAL DEPTH PATCHING (TON)
URBAN								
IL 47	24+86.00	NB	DL	4.0	13.0	3.0	5.8	1.9
IL 47	26+32.00	SB	PL	6.0	13.0	3.0	8.7	2.9
IL 47	28+38.00	SB	DL	6.0	13.0	3.0	8.7	2.9
IL 47	28+79.00	SB	DL	4.0	13.0	3.0	5.8	1.9
IL 47	28+94.00	NB	DL	4.0	13.0	3.0	5.8	1.9
IL 47	30+95.00	NB	DL	4.0	13.0	3.0	5.8	1.9
IL 47	31+45.00	SB	DL	4.0	13.0	3.0	5.8	1.9
IL 47	32+44.00	SB	DL	4.0	13.0	3.0	5.8	1.9
IL 47	32+96.00	NB	PL	4.0	13.0	3.0	5.8	1.9
IL 47	32+96.00	NB	DL	4.0	13.0	3.0	5.8	1.9
IL 47	33+45.00	SB	DL	4.0	13.0	3.0	5.8	1.9
IL 47	33+93.00	NB	PL	4.0	13.0	3.0	5.8	1.9
IL 47	33+93.00	NB	DL	4.0	13.0	3.0	5.8	1.9
IL 47	36+93.00	SB	DL	4.0	13.0	3.0	5.8	1.9
IL 47	97+28.00	SB	DL	4.0	13.0	3.0	5.8	1.9
IL 47	99+41.00	SB	DL	6.0	13.0	3.0	8.7	2.9
IL 47	100+41.00	SB	DL	6.0	13.0	3.0	8.7	2.9
IL 47	101+42.00	SB	DL	4.0	13.0	3.0	5.8	1.9
IL 47 URBAN TOTAL =							115.6	38.8
IL 47 URBAN USE =							116.0	39.0
PROJECT TOTAL =							116.0	-

PARTIAL DEPTH PATCHING, 9 INCH

ROUTE	STATION	DIRECTION	LANE	LENGTH (FOOT)	WIDTH (FOOT)	EX. HMA OVERLAY THICKNESS 3" OR MORE (INCH)	X4406290 PARTIAL DEPTH REMOVAL TYPE II, 9 INCH (SQ YD)	X4406490 PARTIAL DEPTH REMOVAL TYPE III, 9 INCH (SQ YD)	X4421000 PARTIAL DEPTH PATCHING (TON)
RURAL									
IL 47	509+23.00	SB	DL	6.0	13.0	6.0	8.7	-	4.4
IL 47	510+63.00	SB	PL	6.0	13.0	6.0	8.7	-	4.4
IL 47	510+63.00	SB	DL	6.0	13.0	6.0	8.7	-	4.4
IL 47	517+20.00	SB	DL	6.0	13.0	6.0	8.7	-	4.4
IL 47	517+66.00	SB	DL	8.0	13.0	6.0	11.6	-	5.8
IL 47	517+95.00	NB	DL	6.0	15.0	6.0	10.0	-	5.0
IL 47	518+29.00	SB	DL	12.0	13.0	6.0	-	17.3	8.7
IL 47	522+42.00	NB	DL	15.0	13.0	6.0	-	21.7	10.9
IL 47	522+42.00	SB	DL	6.0	13.0	6.0	8.7	-	4.4
IL 47	522+76.00	NB	DL	10.0	13.0	6.0	14.4	-	7.3
IL 47 RURAL TOTAL =							79.3	39.0	59.6
IL 47 RURAL USE =							80.0	39.0	60.0
PROJECT TOTAL =							80.0	39.0	-

PARTIAL DEPTH PATCHING, 10 INCH										
ROUTE	STATION	DIRECTION	LANE	LENGTH (FOOT)	WIDTH (FOOT)	EX. HMA OVERLAY THICKNESS 3" OR MORE (INCH)	X4406300 PARTIAL DEPTH REMOVAL TYPE II, 10 INCH (SQ YD)	X4406500 PARTIAL DEPTH REMOVAL TYPE III, 10 INCH (SQ YD)	X4406700 PARTIAL DEPTH REMOVAL TYPE IV, 10 INCH (SQ YD)	X4421000 PARTIAL DEPTH PATCHING (TON)
URBAN										
IL 47	106+07.00	SB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	106+82.00	SB	DL	4.0	13.0	6.5	5.8	-	-	3.1
IL 47	116+28.00	SB	DL	8.0	13.0	6.5	11.6	-	-	6.1
IL 47	121+11.00	SB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	124+42.00	SB	DL	4.0	13.0	6.5	5.8	-	-	3.1
IL 47	128+00.00	SB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	130+18.00	SB	DL	50.0	13.0	6.5	-	-	72.2	38.4
IL 47	132+11.00	NB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	132+16.00	NB	DL	4.0	13.0	6.5	5.8	-	-	3.1
IL 47	138+76.00	SB	DL	4.0	13.0	6.5	5.8	-	-	3.1
IL 47	140+19.00	NB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	140+19.00	SB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	141+88.00	SB	DL	4.0	13.0	6.5	5.8	-	-	3.1
IL 47	146+10.00	NB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	153+29.00	NB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	153+29.00	SB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	153+45.00	NB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	153+45.00	SB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	156+68.00	NB	DL	40.0	13.0	6.5	-	-	57.8	30.7
IL 47	156+15.00	SB	DL	40.0	13.0	6.5	-	-	57.8	30.7
IL 47	161+68.00	NB	DL	50.0	13.0	6.5	-	-	72.2	38.4
IL 47	161+68.00	SB	DL	50.0	13.0	6.5	-	-	72.2	38.4
IL 47	174+65.00	SB	DL	15.0	13.0	6.5	-	21.7	-	11.5
IL 47	192+35.00	SB	DL	50.0	13.0	6.5	-	-	72.2	38.4
IL 47 URBAN TOTAL =							135.8	21.7	404.4	298.9
IL 47 URBAN USE =							136.0	22.0	405.0	299.0
RURAL										
IL 47	216+09.00	NB	DL	8.0	13.0	6.5	11.6	-	-	6.1
IL 47	216+09.00	SB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	219+12.00	NB	DL	15.0	13.0	6.5	-	21.7	-	11.5
IL 47	219+92.00	NB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	219+92.00	SB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	223+32.00	NB	DL	10.0	13.0	6.5	14.4	-	-	7.7
IL 47	223+32.00	SB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	232+26.00	NB	DL	4.0	13.0	6.5	5.8	-	-	3.1
IL 47	232+26.00	SB	DL	4.0	13.0	6.5	5.8	-	-	3.1
IL 47	237+27.00	NB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	237+27.00	SB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	238+00.00	SB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	240+74.00	NB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	240+74.00	SB	DL	12.0	13.0	6.5	-	17.3	-	9.2
IL 47	241+14.00	NB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	241+14.00	SB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	242+41.00	NB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	242+41.00	SB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	254+27.00	NB	DL	20.0	13.0	6.5	-	-	28.9	15.4
IL 47	262+50.00	NB	DL	20.0	13.0	6.5	-	-	28.9	15.4
IL 47	262+50.00	SB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47 RURAL SUB-TOTAL =							150.2	39.0	57.8	131.4

PARTIAL DEPTH PATCHING, 10 INCH										
ROUTE	STATION	DIRECTION	LANE	LENGTH (FOOT)	WIDTH (FOOT)	EX. HMA OVERLAY THICKNESS 3" OR MORE	X4406300 PARTIAL DEPTH REMOVAL TYPE II, 10 INCH (SQ YD)	X4406500 PARTIAL DEPTH REMOVAL TYPE III, 10 INCH (SQ YD)	X4406700 PARTIAL DEPTH REMOVAL TYPE IV, 10 INCH (SQ YD)	X4421000 PARTIAL DEPTH PATCHING (TON)
						(INCH)	(SQ YD)	(SQ YD)	(SQ YD)	(TON)
RURAL										
IL 47	264+86.00	NB	DL	8.0	13.0	6.5	11.6	-	-	6.1
IL 47	269+86.00	NB	DL	20.0	13.0	6.5	-	-	28.9	15.4
IL 47	269+86.00	SB	DL	20.0	13.0	6.5	-	-	28.9	15.4
IL 47	281+10.00	NB	DL	15.0	13.0	6.5	-	21.7	-	11.5
IL 47	281+10.00	SB	DL	15.0	13.0	6.5	-	21.7	-	11.5
IL 47	281+56.00	NB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	281+56.00	SB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	282+81.00	NB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	282+81.00	SB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	292+50.00	NB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	292+50.00	SB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	300+15.00	NB	DL	15.0	13.0	6.5	-	21.7	-	11.5
IL 47	300+15.00	SB	DL	15.0	13.0	6.5	-	21.7	-	11.5
IL 47	308+77.00	NB	DL	10.0	13.0	6.5	14.4	-	-	7.7
IL 47	308+77.00	SB	DL	10.0	13.0	6.5	14.4	-	-	7.7
IL 47	322+49.00	NB	DL	8.0	13.0	6.5	11.6	-	-	6.1
IL 47	322+49.00	SB	DL	8.0	13.0	6.5	11.6	-	-	6.1
IL 47	323+24.00	SB	DL	8.0	13.0	6.5	11.6	-	-	6.1
IL 47	334+12.00	NB	DL	15.0	13.0	6.5	-	21.7	-	11.5
IL 47	334+12.00	SB	DL	15.0	13.0	6.5	-	21.7	-	11.5
IL 47	336+71.00	NB	DL	10.0	13.0	6.5	14.4	-	-	7.7
IL 47	336+71.00	SB	DL	10.0	13.0	6.5	14.4	-	-	7.7
IL 47	337+69.00	SB	DL	10.0	13.0	6.5	14.4	-	-	7.7
IL 47	348+01.00	NB	DL	12.0	13.0	6.5	-	17.3	-	9.2
IL 47	348+01.00	SB	DL	12.0	13.0	6.5	-	17.3	-	9.2
IL 47	352+79.00	SB	DL	20.0	13.0	6.5	-	-	28.9	15.4
IL 47	360+74.00	SB	DL	25.0	13.0	6.5	-	-	36.1	19.2
IL 47	363+45.00	NB	DL	20.0	13.0	6.5	-	-	28.9	15.4
IL 47	363+45.00	SB	DL	15.0	13.0	6.5	-	21.7	-	11.5
IL 47	368+38.00	NB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	368+38.00	SB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	389+85.00	NB	DL	10.0	13.0	6.5	14.4	-	-	7.7
IL 47	389+85.00	SB	DL	10.0	13.0	6.5	14.4	-	-	7.7
IL 47	390+15.00	NB	DL	12.0	13.0	6.5	-	17.3	-	9.2
IL 47	390+15.00	SB	DL	12.0	13.0	6.5	-	17.3	-	9.2
IL 47	390+59.00	NB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	390+59.00	SB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	407+40.00	NB	DL	8.0	13.0	6.5	11.6	-	-	6.1
IL 47	407+40.00	SB	DL	8.0	13.0	6.5	11.6	-	-	6.1
IL 47	409+21.00	SB	DL	30.0	13.0	6.5	-	-	43.3	23.1
IL 47	419+87.00	SB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	420+83.00	SB	DL	20.0	13.0	6.5	-	-	28.9	15.4
IL 47	421+69.00	SB	DL	6.0	13.0	6.5	8.7	-	-	4.6
IL 47	423+37.00	SB	DL	12.0	13.0	6.5	-	17.3	-	9.2
IL 47	428+04.00	SB	DL	10.0	13.0	6.5	14.4	-	-	7.7
IL 47	440+38.00	SB	DL	25.0	13.0	6.5	-	-	36.1	19.2
IL 47	450+57.00	SB	DL	10.0	13.0	6.5	14.4	-	-	7.7
IL 47	459+96.00	SB	DL	10.0	13.0	6.5	14.4	-	-	7.7
IL 47	474+05.00	SB	DL	20.0	13.0	6.5	-	-	28.9	15.4
IL 47	848+22.00	SB	DL	20.0	13.0	6.5	-	-	28.9	15.4
IL 47	485+82.00	SB	DL	20.0	13.0	6.5	-	-	28.9	15.4
IL 47 RURAL SUB-TOTAL =							317.8	238.3	346.7	480.3

PARTIAL DEPTH PATCHING, 10 INCH										
ROUTE	STATION	DIRECTION	LANE	LENGTH (FOOT)	WIDTH (FOOT)	EX. HMA OVERLAY THICKNESS 3" OR MORE (INCH)	X4406300 PARTIAL DEPTH REMOVAL TYPE II, 10 INCH (SQ YD)	X4406500 PARTIAL DEPTH REMOVAL TYPE III, 10 INCH (SQ YD)	X4406700 PARTIAL DEPTH REMOVAL TYPE IV, 10 INCH (SQ YD)	X4421000 PARTIAL DEPTH PATCHING (TON)
RURAL										
US 136	1656+07.00	EB	DL	6.0	13.0	7.0	8.7	-	-	4.9
US 136	1656+07.00	WB	DL	6.0	13.0	7.0	8.7	-	-	4.9
US 136	1657+06.00	EB	DL	6.0	13.0	7.0	8.7	-	-	4.9
US 136	1658+53.00	WB	DL	6.0	13.0	7.0	8.7	-	-	4.9
US 136	1672+91.00	EB	DL	6.0	13.0	7.0	8.7	-	-	4.9
US 136	1672+91.00	WB	DL	6.0	15.0	7.0	10.0	-	-	5.6
IL 47 RURAL SUB-TOTAL =							53.3	-	-	29.9
IL 47 RURAL TOTAL =							521.3	277.3	404.4	641.5
IL 47 RURAL USE =							522.0	278.0	405.0	642.0
PROJECT TOTAL =							658.0	300.0	810.0	1040.0

44213204 TIF BAR 3/4"

44300200 STRIP REFLECTIVE CRACK CONTROL TREATMENT

IL 47	STATION	TO	STATION	EACH
URBAN				
	25+34.80		25+79.70	88.0
	34+19.40		34+58.70	78.0
IL 47 URBAN TOTAL =				166.0
PROJECT TOTAL =				166.0

IL 47	STATION	Q/S	TO	STATION	Q/S	FOOT
URBAN						
	104+06.45	RT 9.0'		193+75.00	RT 9.0'	8968.6
	104+06.45	LT 9.0'		193+75.00	LT 9.0'	8968.6
IL 47 URBAN TOTAL =						17937.1
IL 47 URBAN USE =						17938.0
RURAL						
	193+75.00	RT 9.0'		406+69.20 (BK)		21294.2
	401+00.92 (AH)	RT 9.0'		497+00.00		9599.1
	193+75.00	LT 9.0'		406+69.20 (BK)		21294.2
	401+00.92 (AH)	LT 9.0'		497+00.00		9599.1
IL 47 RURAL TOTAL =						61786.6
IL 47 RURAL USE =						61787.0
PROJECT TOTAL =						79725.0

45100200 CRACK FILLING

STATION	TO	STATION	ESTIMATED CRACKS LENGTH	POUND
URBAN				
<u>IL 47</u>				
23+82.80		38+72.76(BK)	4469.9	1788.0
97+14.44	(AH)	193+75.00	28981.7	<u>11592.7</u>
				IL 47 URBAN TOTAL = 13380.6
				IL 47 URBAN USE = 13381.0
RURAL				
193+75.00		406+69.20(BK)	63882.6	25553.0
401+00.92	(AH)	507+35.68(BK)	31904.3	12761.7
507+49.60	(AH)	523+18.80	4707.6	<u>1883.0</u>
				IL 47 RURAL TOTAL = 40197.8
				IL 47 RURAL USE = 40198.0
				PROJECT TOTAL = 53579.0

NOTE:
 CRACK FILLING SHALL BE USED AFTER THE COMPLETION OF THE HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2". THE INTENT IS TO FILL THE MISCELLANEOUS TRANSVERSE AND LONGITUDINAL CRACKS THAT ARE 1/2" WIDE AND GREATER IN SIZE. THE QUANTITY IN THE PLANS IS AN ESTIMATE ONLY. THE ACTUAL CRACK FILLING REQUIRED WILL BE DETERMINED BY THE ENGINEER AFTER THE MILLING OPERATIONS ARE COMPLETE.

(APPLICATION RATE = 0.40 LB/FT)

48101200 AGGREGATE SHOULDERS, TYPE B

STATION	TO	STATION	LENGTH (FT)	WIDTH (FT)	AREA (SQ FT)	AVG. THICKNESS INCHES	TONS
RURAL							
<u>IL 47</u>							
LT 499+91.00		LT 506+10.00	619.0	VAR.	6229.8	2.0	69.2
RT 500+15.00		RT 506+18.00	603.0	VAR.	5331.3	2.0	59.2
LT 508+66.00		LT 517+30.00	864.0	VAR.	9462.3	2.0	105.1
RT 508+75.00		RT 517+30.00	855.0	VAR.	12182.0	2.0	<u>135.4</u>
						IL 47 RURAL TOTAL =	368.9
						IL 47 RURAL USE =	369.0
						PROJECT TOTAL =	369.0

NOTES:

- AREAS WERE OBTAINED USING CADD.
- CONVERSION FACTOR OF 1.8 TONS/ CU YD WAS USED TO CALCULATE QUANTITY.
- AVG. THICKNESS OF 2" WAS DERIVED BY ASSUMING THICKNESS OF 3" REQUIRED AT PAVEMENT EDGE AND 1" THICKNESS AT OUTSIDE EDGE. (THIS WAS DONE TO ACCOMMODATE FOR LOW SHOULDERS, ACTUAL FIELD CONDITIONS MAY VARY).

FILE NAME =	USER NAME = ceorlockbm	DESIGNED - BMC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES PLAN PAY ITEMS				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\pwork\ceorlockbm\d0187612\0870844-sht-schedules of plan pay items.dgn	DRAWN - BMC	REVISED -	326						136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	56	
MODELNAME	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 70844								
	PLOT DATE = 10/10/2014	DATE -	REVISED -		SCALE: N/A	SHEET 23	OF 47 SHEETS	STA. -----	TO STA. -----	ILLINOIS FED. AID PROJECT			

48101500 AGGREGATE SHOULDERS, TYPE B, 6"

STATION	TO	STATION	LENGTH	WIDTH	SQ_YD
IL 47 URBAN					
RT 104+00.00		RT 118+05.41	1405.4	3.0	468.5
LT 108+05.00		LT 175+64.00	6759.0	3.0	2253.0
RT 119+03.00		RT 125+53.00	650.0	3.0	216.7
RT 126+30.00		RT 158+08.00	3178.0	3.0	1059.3
RT 159+03.00		RT 166+52.00	749.0	3.0	249.7
RT 167+85.00		RT 193+22.00	2537.0	3.0	845.7
LT 176+30.00		LT 233+52.00	5722.0	3.0	1907.3
			IL 47 URBAN TOTAL =		7000.1
			IL 47 URBAN USE =		7001.0
RURAL					
RT 193+87.00		RT 233+48.00	3961.0	3.0	1320.3
RT 234+03.00		RT 286+63.00	5260.0	3.0	1753.3
LT 234+07.00		LT 286+67.00	5260.0	3.0	1753.3
RT 287+23.00		RT 313+14.00	2591.0	3.0	863.7
LT 287+23.00		LT 313+18.00	2595.0	3.0	865.0
RT 313+71.00		RT 325+82.00	1211.0	3.0	403.7
LT 313+74.00		LT 326+31.00	1257.0	3.0	419.0
RT 332+67.00		RT 335+14.00	247.0	3.0	82.3
LT 332+89.00		LT 335+14.00	225.0	3.0	75.0
RT 336+42.00		RT 338+67.00	225.0	3.0	75.0
LT 336+42.00		LT 339+61.00	319.0	3.0	106.3
LT 342+12.00		LT 369+02.00	2690.0	3.0	896.7
RT 342+63.00		RT 406+12.00	6349.0	3.0	2116.3
LT 370+42.00		LT 406+01.00	3559.0	3.0	1186.3
RT 406+64.00		RT 453+68.00	4704.0	3.0	1568.0
LT 406+64.00		LT 453+69.00	4705.0	3.0	1568.3
RT 454+25.00		RT 500+15.00	4590.0	3.0	1530.0
LT 454+25.00		LT 499+91.00	4566.0	3.0	1522.0
RT 517+30.00		RT 523+19.00	589.0	3.0	196.3
LT 517+30.00		LT 523+19.00	589.0	3.0	196.3
			IL 47 RURAL SUB-TOTAL =		18,497.3

STATION	TO	STATION	LENGTH	WIDTH	SQ_YD
US 136 RURAL					
RT 1651+00.00		RT 1660+32.00	932.0	3.0	310.7
LT 1651+00.00		LT 1660+05.00	905.0	3.0	301.7
LT 1662+45.00		LT 1674+00.00	1155.0	3.0	385.0
RT 1662+72.00		RT 1674+00.00	1128.0	3.0	376.0
			IL 47 RURAL SUB-TOTAL =		1373.3
			IL 47 RURAL TOTAL =		19870.7
			IL 47 RURAL USE =		19871.0
			PROJECT TOTAL =		26872.0

NOTES:

1. AT ALL SIDEROADS THE AGGREGATE SHOULDER SHALL TAPER FROM 3 FT ALONG THE MAINLINE DOWN TO 2 FT AT THE BACK OF RADIUS AS SHOWN ON THE SIDEROAD DETAILS.
2. NO DEDUCTIONS IN QUANTITY WERE MADE FOR THE PRIVATE OR FIELD ENTRANCES. THE STATIONS LISTED ABOVE DO NOT CONTINUE THROUGH THE SIDEROADS OR HMA SHOULDER INLAY AREAS. THE QUANTITY OF AGGREGATE CALCULATED THROUGH THE ENTRANCES WILL BE SUFFICIENT TO COVER THE QUANTITY REQUIRED FOR THE SIDEROAD RETURNS.

HOT-MIX ASPHALT SHOULDERS (INLAY AREAS)

STATION	TO	STATION	LENGTH	WIDTH	SQ_YDS	BIT	MATLS	PR	CT	HMA	SHLD
IL 47 URBAN											
LT 24+99.00		LT 104+00.00	7901.0	8.0	7023.1					3160.4	589.9
RT 24+92.00		RT 104+00.00	7908.0	8.0	7029.3					3163.2	590.5
			IL 47 URBAN TOTAL =							6323.6	1180.4
			IL 47 URBAN USE =							6324.0	1181.0
RURAL											
RT 326+26.00		RT 332+68.00	642.0	7.0	499.3					224.7	41.9
LT 326+58.00		LT 332+89.00	631.0	7.0	490.8					220.9	41.2
RT 338+66.00		RT 342+64.00	398.0	7.0	309.6					139.3	26.0
LT 339+61.00		LT 342+13.00	252.0	7.0	196.0					88.2	16.5
			IL 47 RURAL TOTAL =							673.1	125.6
			IL 47 RURAL USE =							674.0	126.0

PROJECT TOTAL = * 1,307.0

NOTE: * ADDITIONAL QUANTITIES FOR THIS PAY ITEM CAN BE FOUND IN THE HMA RESURFACING TABLE THAT IS LOCATED WITHIN THE SCHEDULE OF QUANTITIES PLAN PAY ITEMS (PROJECT TOTAL = 104623.0 POUND).

48203029 HOT-MIX ASPHALT SHOULDERS, 8"

STATION	TO	STATION	LENGTH	WIDTH	SQ_YD
IL 47 URBAN					
RT 104+00.00		RT 118+05.41	1405.4	2.0	312.3
LT 104+00.00		LT 107+99.00	399.0	8.0	354.7
LT 108+05.00		LT 175+64.00	6759.0	2.0	1502.0
RT 119+03.00		RT 125+53.00	650.0	2.0	144.4
RT 126+30.00		RT 158+08.00	3178.0	2.0	706.2
RT 159+03.00		RT 166+52.00	749.0	2.0	166.4
RT 167+85.00		RT 193+22.00	2537.0	2.0	563.8
LT 176+30.00		LT 233+52.00	5722.0	2.0	1271.6
			IL 47 URBAN TOTAL =		5021.4
			IL 47 URBAN USE =		5022.0
RURAL					
RT 193+87.00		RT 233+48.00	3961.0	2.0	880.2
RT 234+03.00		RT 286+63.00	5260.0	2.0	1168.9
LT 234+07.00		LT 286+67.00	5260.0	2.0	1168.9
RT 287+23.00		RT 313+14.00	2591.0	2.0	575.8
LT 287+23.00		LT 313+18.00	2595.0	2.0	576.7
RT 313+71.00		RT 325+82.00	1211.0	2.0	269.1
LT 313+74.00		LT 326+31.00	1257.0	2.0	279.3
RT 332+67.00		RT 335+14.00	247.0	2.0	54.9
LT 332+89.00		LT 335+14.00	225.0	2.0	50.0
RT 336+42.00		RT 338+67.00	225.0	2.0	50.0
LT 336+42.00		LT 339+61.00	319.0	2.0	70.9
LT 342+12.00		LT 364+00.00	2188.0	2.0	486.2
RT 342+63.00		RT 364+00.00	2137.0	2.0	474.9
LT 364+00.00		LT 369+02.00	502.0	3.0	167.3
RT 364+00.00		RT 382+00.00	1800.0	3.0	600.0
LT 370+42.00		LT 382+00.00	1158.0	3.0	386.0
LT 382+00.00		LT 391+00.00	900.0	2.0	200.0
RT 382+00.00		RT 391+00.00	900.0	2.0	200.0
LT 391+00.00		LT 406+01.00	1501.0	3.0	500.3
RT 391+00.00		RT 406+12.00	1512.0	3.0	504.0
RT 401+01.00		RT 453+68.00	5267.0	2.0	1170.4
LT 401+01.00		LT 453+69.00	5268.0	2.0	1170.7
RT 454+25.00		RT 506+18.00	5193.0	2.0	1154.0
LT 454+25.00		LT 506+10.00	5185.0	2.0	1152.2
			IL 47 RURAL TOTAL =		13310.8
			IL 47 RURAL USE =		13,311.0

PROJECT TOTAL = 18,333.0

FILE NAME =	USER NAME = ceerlockbm	DESIGNED - BMC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES PLAN PAY ITEMS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ce:\pw\work\p\dot\ceerlockbm\d0187612\0870844-sht-schedules of plan pay items.dgn	DRAWN - BMC	REVISED -	326					136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	57	
#MODELNAME#	PLOT SCALE = 48.0000' / in.	CHECKED -	REVISED -		SCALE: N/A SHEET 24 OF 47 SHEETS STA. ----- TO STA. -----			CONTRACT NO. 70844				
	PLOT DATE = 10/10/2014	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

48300100 PORTLAND CEMENT CONCRETE SHOULDERS, 6 INCH

STATION	TO	STATION	LENGTH (FT)	WIDTH (FT)	AREA (SQ. YD.)
TRAIL - I URBAN					
RT 25+44.50		RT 25+54.50	8.0	10.0	8.9
LT 25+44.50		LT 25+54.50	8.0	10.0	8.9
TRAIL - I TOTAL =					17.8
TRAIL - I USE =					18.0
PROJECT TOTAL =					18.0

54002060 EXPANSION BOLTS, 3/4 INCH X 12 INCH
IL 47

NOTE: SEE TRANSITION FROM PIPE CULVERTS TO EXISTING PAVED DITCH DETAILS FOR QUANTITY.

50300225 CONCRETE STRUCTURES

TRAIL - C

NOTE: SEE CONCRETE RETAINING WALL DETAILS FOR QUANTITY.

54003000 CONCRETE BOX CULVERTS

IL 47

* NOTE: SEE MISCELANEOUS BOX CULVERT ITEMS TABLE FOR QUANTITY.

MISCELANEOUS BOX CULVERT ITEMS

STATION	EXIST STRUCT NUMBER	50800105 REINFORCEMENT BARS LBS	54003000 CONCRETE BOX CULVERTS CU YD	542D0217 PIPE CULV CLASS D TYPE 1, 12" FOOT	61101026 STORM SEWER PROTECTED CLASS A, 24" FOOT
URBAN IL 47					
175+62.00	010-8146	-	-	8.0	-
192+34.00	-	-	-	-	-
IL 47 URBAN TOTAL =		-	-	8.0	-

RURAL					
STATION	EXIST STRUCT NUMBER	50800105 REINFORCEMENT BARS LBS	54003000 CONCRETE BOX CULVERTS CU YD	542D0217 PIPE CULV CLASS D TYPE 1, 12" FOOT	61101026 STORM SEWER PROTECTED CLASS A, 24" FOOT
250+31.00	010-8062	186.0	2.0	-	-
414+00.00	-	-	-	-	-
466+17.00	010-8061	-	-	-	45.0
IL 47 RURAL TOTAL =		186.0	2.0	-	45.0
PROJECT TOTAL =		186.0	2.0	8.0	45.0

SCHEDULE OF A. R. BOX CULVERTS

STATION	CULV. NO.	EXIST STRUCT NUMBER	PROP STRUCT NUMBER	20700220 POROUS GRAN EMB CU YD	44201759 CLASS D PATCHES TYPE IV 9 INCH SQ YD	51500100 NAME PLATES EACH	BOX CULVERT END SECTION EACH	54010303 PCC BOX CULVERT 3x3 FOOT	54010402 PCC BOX CULVERT 4x2 FOOT	54010403 PCC BOX CULVERT 4x3 FOOT	54010805 PCC BOX CULVERT 8x5 FOOT	Z0038700 PERM BENCH MARKS EACH
126+68.00	N/A	N/A	N/A	-	-	-	1.0	-	10.0	-	-	-
TRAIL - C TOTAL =				-	-	-	1.0	-	10.0	-	-	-
IL 47												
135+81.00	1	SLD 13.39	SLD 13.39	41.0	26.0	-	2.0	-	-	45.5	-	-
181+27.00	2	010-8063	010-8151	223.0	84.0	1.0	2.0	-	-	-	72.0	1.0
IL 47 URBAN TOTAL =				264.0	110.0	1.0	4.0	-	-	45.5	72.0	1.0
RURAL												
224+48.00	3	010-8604	010-8152	106.0	92.0	1.0	2.0	-	-	-	93.0	1.0
352+60.00	4	SLD 9.30	SLD 9.30	37.0	23.0	-	2.0	45.0	-	-	-	-
438+01.00	5	SLD 7.57	SLD 7.57	23.0	26.0	-	2.0	-	39.0	-	-	-
IL 47 RURAL TOTAL =				166.0	141.0	1.0	6.0	45.0	39.0	0.0	93.0	1.0
PROJECT TOTAL =				430.0	251.0	2.0	11.0	45.0	49.0	45.5	165.0	2.0

SCHEDULE OF PIPE CULVERTS

STATION	54213450 END SEC 15"	54213453 END SEC 18"	54214290 END SEC ERS 15"	54214293 END SEC ERS 18"	54214296 END SEC ERS 21"	54214299 END SEC ERS 24"	54214305 END SEC ERS 30"	542D0211 PIPE CULV CLASS D TYPE 1 6" FOOT	542D0220 PIPE CULV CLASS D TYPE 1 15" FOOT	542D0223 PIPE CULV CLASS D TYPE 1 18" FOOT	542D0229 PIPE CULV. CLASS D TYPE 1, 24" FOOT	542D5470 PIPE CULV CLASS D TYPE 1 ERS 15" FOOT	542D5473 PIPE CULV CLASS D TYPE 1 ERS 18" FOOT	542D5476 PIPE CULV CLASS D TYPE 1 ERS 21" FOOT	542D5479 PIPE CULV CLASS D TYPE 1 ERS 24" FOOT	542D5485 PIPE CULV CLASS D TYPE 1 ERS 30" FOOT
URBAN																
TRAIL - C RT	120+00.00		1.0									75.0				
TRAIL - C TOTAL =			1.0									75.0				

IL 47		54213450	54213453	54214290	54214293	54214296	54214299	54214305	542D0211	542D0220	542D0223	542D0229	542D5470	542D5473	542D5476	542D5479	542D5485
RT	99+17.94		1.0										50.0				
RT	99+90.22		1.0										25.0				
RT	102+82.22		1.0										38.0				
RT	108+00.00		2.0										45.0				
LT	108+08.29			2.0											25.0		
LT	127+47.25		2.0										40.0				
LT	132+00.64	2.0											40.0				
LT	132+82.86	2.0															
RT	141+97.19		2.0														
LT	143+00.00						2.0										40.0
RT	149+15.80		2.0														
LT	153+26.82			2.0													
LT	156+82.29	2.0											40.0				
LT	162+58.69			2.0													40.0
RT	177+50.39		2.0										40.0				
LT	181+79.00								8.0			10.0					
RT	184+55.18			2.0									40.0				
LT	184+55.18			2.0									40.0				
IL 47 URBAN TOTAL =		6.0	13.0	10.0			2.0		8.0	120.0	331.0	10.0	185.0				40.0

RURAL		54213450	54213453	54214290	54214293	54214296	54214299	54214305	542D0211	542D0220	542D0223	542D0229	542D5470	542D5473	542D5476	542D5479	542D5485
IL 47																	
RT	194+11.10			2.0									40.0				
RT	207+20.14			2.0									40.0				
RT	213+89.05		2.0														
RT	220+36.33				2.0											60.0	
LT	220+40.29				2.0											60.0	
LT	226+32.00			2.0									40.0				
RT	227+00.51	2.0															
LT	227+94.50		2.0										30.0				
LT	248+72.94			2.0													40.0
RT	260+26.38		2.0										40.0				
LT	260+46.41			2.0													30.0
LT	261+18.90		2.0										40.0				
RT	273+48.47	2.0											40.0				
LT	300+04.92		2.0										40.0				
LT	311+87.34			2.0													40.0
LT	316+88.79			2.0													40.0
LT	320+04.51			2.0													40.0
LT	322+05.61			2.0													40.0
RT	326+04.15			2.0													40.0
LT	332+55.26		2.0										50.0				
LT	342+93.83	2.0											35.0				
RT	345+03.78		2.0										50.0				
LT	345+43.48	2.0											40.0				
LT	346+14.37	2.0											40.0				
RT	353+84.89	2.0											40.0				
LT	357+83.86	2.0											40.0				
LT	359+73.77			2.0													40.0
RT	365+37.84			2.0													40.0
RT	366+16.49	2.0											40.0				
LT	366+44.17	2.0											40.0				
RT	366+84.16	2.0											40.0				
RT	385+14.83					2.0											40.0
LT	397+77.70		2.0										40.0				
RT	397+77.51		2.0										40.0				
LT	403+57.35	2.0											30.0				
LT	427+22.63			2.0													40.0
RT	427+32.09			2.0													40.0
RT	432+99.39	2.0											40.0				
RT	435+67.41	2.0											40.0				
LT	440+53.84		2.0										40.0				
RT	480+41.11	2.0											40.0				
LT	480+83.24							2.0									40.0
RT	481+89.53	2.0											40.0				
LT	494+54.54			2.0													40.0
LT	495+87.13			2.0													40.0
RT	496+62.06			2.0													40.0
LT	504+92.40			2.0													30.0
RT	518+89.92		2.0										40.0				
LT	1654+90.78	2.0											40.0				
RT	1654+91.33	2.0											40.0				
RT	1659+93.99	2.0											160.0				
RT	1670+01.47	2.0											40.0				
IL 47 RURAL TOTAL =		38.0	22.0	36.0	4.0	2.0		2.0		865.0	450.0		700.0	120.0	40.0		40.0
PROJECT TOTAL =		44.0	36.0	46.0	4.0	2.0	2.0	2.0	8.0	985.0	856.0	10.0	885.0	120.0	40.0		40.0

SCHEDULE OF PIPE CULVERTS

STATION	ROADWAY/DESCRIPTION	20900110 POROUS GRAN. BACKFILL CU YD	44201747 CLASS D PATCHES TYPE IV, 8 INCH CU YD	54213465 END SEC 30" EACH	54213660 PC REINF CONC FLRD END SEC 15" EACH	54213663 PC REINF CONC FLRD END SEC 18" EACH	54213666 PC REINF CONC FLRD END SEC 21" EACH	54213669 PC REINF CONC FLRD END SEC 24" EACH	54214500 PC REINF CONC FLRD END SEC ERS 15" EACH	542A0220 PIPE CULV CLASS A TYPE 1 15" FOOT	542A0223 PIPE CULV CLASS A TYPE 1 18" FOOT	542A0226 PIPE CULV CLASS A TYPE 1 21" FOOT	542A0229 PIPE CULV CLASS A TYPE 1 24" FOOT	542A5470 PIPE CULV CLASS A TYPE 1 ERS 15" FOOT	542D0235 PIPE CULV CLASS D TYPE 1 30" FOOT	X0327746 PIPE CULV CLASS D TYPE 1, 30" FITTING W/ 24" TEE EACH
URBAN																
TRAIL - C																
RT	121+00.00	RETAINING WALL	19.0	-	-	-	-	-	-	-	-	-	-	-	-	-
RT	125+30.00	-	-	-	-	1.0	-	-	-	-	-	-	-	-	-	-
TRAIL - C TOTAL =		19.0	-	-	-	1.0	-	-	-	-	-	-	-	-	-	-
IL 47																
RT	118+53.87	L. O. W. PARK ENTRANCE	20.3	51.5	-	-	-	-	2.0	-	-	-	-	105.0	-	-
RT	125+87.50	BRIARCLIFF DR.	22.5	35.4	-	2.0	-	-	-	100.0	-	-	-	-	-	-
RT	158+55.12	EAST OAK VALLEY RD.	13.2	48.0	-	-	-	2.0	-	-	-	150.0	-	-	-	-
LT	176+00.21	CR 2400N	18.1	23.1	-	-	1.0	-	-	-	70.0	-	-	-	-	-
IL 47 URBAN TOTAL =		74.1	158.0	-	-	2.0	1.0	2.0	2.0	-	100.0	70.0	150.0	105.0	-	-
IL 47 URBAN USE =		75.0	159.0	-	-	2.0	1.0	2.0	2.0	-	100.0	70.0	150.0	105.0	-	-
RURAL																
IL 47																
RT	233+77.70	CR 2500N	10.8	23.1	-	-	2.0	-	-	-	-	62.0	-	-	-	-
LT	233+78.25	CR 2500N	10.3	29.0	2.0	-	-	-	-	-	-	-	-	-	58.0	1.0
RT	286+94.30	CR 2600N	7.1	29.7	-	2.0	-	-	-	-	60.0	-	-	-	-	-
LT	286+97.16	CR 2600N	7.4	28.8	-	2.0	-	-	-	-	60.0	-	-	-	-	-
LT	313+46.08	CR 2650N	8.3	26.4	-	2.0	-	-	-	-	55.0	-	-	-	-	-
RT	313+46.41	CR 2650N	6.5	28.0	-	2.0	-	-	-	-	78.0	-	-	-	-	-
LT	406+35.14	CR 2800N	7.0	25.0	-	1.0	-	-	-	90.0	-	-	-	-	-	-
RT	453+98.47	CR 2900N	7.1	28.0	-	2.0	-	-	-	-	60.0	-	-	-	-	-
IL 47 RURAL TOTAL =		64.5	218.1	2.0	1.0	10.0	2.0	-	-	90.0	313.0	62.0	-	-	58.0	1.0
IL 47 RURAL USE =		65.0	219.0	2.0	1.0	10.0	2.0	-	-	90.0	313.0	62.0	-	-	58.0	1.0
PROJECT TOTAL =		159.0	378.0	2.0	1.0	13.0	3.0	2.0	2.0	90.0	413.0	132.0	150.0	105.0	58.0	1.0

* NOTE: ADDITIONAL QUANTITY FOR ITEM 54213660 PC REINFORCED CONCRETE FLARED END SECTION 15" CAN BE FOUND IN THE STORM SEWERS TABLE (PROJECT TOTAL = 3.0 EACH)

54248510 CONCRETE COLLAR

STATION CU_YD

URBAN

IL 47

LT 155+64.20	1.0
RT 155+64.20	1.0
LT 175+62.00	1.0
RT 180+89.00	0.5
LT 181+72.00	0.5
IL 47 URBAN TOTAL =	4.0

RURAL

LT 233+48.00	1.0
RT 233+48.00	1.0
LT 243+15.00	1.0
RT 243+15.00	1.0
LT 278+50.00	1.0
RT 278+50.00	1.0
LT 295+10.00	1.0
RT 295+10.00	1.0
LT 310+49.00	1.0
RT 310+49.00	1.0
LT 319+00.00	-
RT 319+00.00	1.0
LT 324+34.00	1.0
RT 324+34.00	1.0
RT 352+74.00	0.5
LT 363+36.00	1.0
RT 363+36.00	1.0
LT 414+48.00	0.5
RT 414+48.00	0.5
LT 451+00.00	1.0
RT 451+00.00	1.0
LT 451+25.00	0.5
RT 451+25.00	0.5
LT 454+41.00	1.0
RT 454+10.00	1.0
LT 466+07.00	0.5
RT 466+18.00	0.5
RT 466+22.00	0.5
RT 466+25.00	0.5
RT 482+06.00	0.5
IL 47 RURAL TOTAL =	24.0
IL 47 RURAL USE =	24.0
PROJECT TOTAL =	28.0

AR CULVERT EXTENSION SCHEDULES

STATION	REMOVED FOOT	54214509 PC REINF CONC FLRD END SEC ERS 24" EACH	542A5479 PIPE CULV CLASS A TYPE 1, ERS 24" FOOT	542A5491 PIPE CULV CLASS A TYPE 1, ERS 36" FOOT	54214521 PC REINF CONC FLRD END SEC ERS 36" EACH
URBAN					
IL 47 LT 155+64.20	10.0	1.0	6.0	-	-
RT 155+64.20	9.0	1.0	6.0	-	-
IL 47 URBAN TOTAL =		2.0	12.0	-	-

RURAL

LT 233+48.00	9.0	-	-	4.0	1.0
RT 233+48.00	9.0	-	-	4.0	1.0
LT 243+15.00	8.0	1.0	6.0	-	-
RT 243+15.00	10.0	1.0	8.0	-	-
LT 278+50.00	9.0	1.0	8.0	-	-
RT 278+50.00	11.0	1.0	10.0	-	-
LT 295+10.00	10.0	1.0	8.0	-	-
RT 295+10.00	-	1.0	13.0	-	-
LT 310+49.00	10.0	1.0	8.0	-	-
RT 310+49.00	-	1.0	4.0	-	-
LT 319+00.00	12.0	1.0	7.0	-	-
RT 319+00.00	-	1.0	-	-	-
LT 324+34.00	7.0	1.0	11.0	-	-
RT 324+34.00	9.0	1.0	8.0	-	-
LT 363+36.00	-	1.0	5.0	-	-
RT 363+36.00	4.0	1.0	4.0	-	-
LT 451+00.00	10.0	1.0	8.0	-	-
RT 451+00.00	9.0	1.0	4.0	-	-
LT 454+41.00	5.0	1.0	4.0	-	-
RT 454+10.00	-	1.0	-	-	-
IL 47 RURAL TOTAL =		18.0	116.0	8.0	2.0
PROJECT TOTAL =		20.0	128.0	8.0	2.0

STORM SEWERS

	STATION	TO	STATION	54213660 PC REINF CONC FLRD END SEC 15" EACH	550A0090 STORM SEWER CLASS A, TYPE 1 18" FOOT	55200400 STORM SEWERS JACKED IN PLACE 15" FOOT	61101017 STORM SEWER PROTECTED CLASS A 15" FOOT
<u>TRAIL - C</u>	URBAN						
	120+76.50		121+98.50	-	122.0	-	-
	122+01.50		122+98.50	-	97.0	-	-
	123+01.50		123+98.50	-	97.0	-	-
	124+01.50		124+98.50	-	97.0	-	-
	124+73.50		125+30.00	-	60.0	-	-
	TRAIL - C TOTAL =			-	473.0	-	-

	STATION	TO	STATION	54213660 PC REINF CONC FLRD END SEC 15" EACH	550A0090 STORM SEWER CLASS A, TYPE 1 18" FOOT	55200400 STORM SEWERS JACKED IN PLACE 15" FOOT	61101017 STORM SEWER PROTECTED CLASS A 15" FOOT
<u>IL 47</u>	RURAL						
	LT 228+68.00		229+38.50	1.0	-	-	70.5
	LT 229+41.50		230+18.00	1.0	-	-	76.5
	414+48.00		414+48.00	-	-	99.0	-
	451+25.00		451+25.00	-	-	79.0	-
	IL 47 RURAL TOTAL =			2.0	-	178.0	147.0
	PROJECT TOTAL =			*	473.0	178.0	147.0

* NOTE: ADDITIONAL QUANTITY FOR ITEM 54213660 CAN BE FOUND IN THE PIPE CULVERT TABLES (PROJECT TOTAL = 3.0 EACH)

60100060 CONCRETE HEADWALL FOR PIPE DRAIN

RURAL		
<u>IL 47</u>		
STATION	OFFSET	EACH
352+69.00	RT X2.5'	1.0
IL 47 RURAL TOTAL =		1.0
PROJECT TOTAL =		1.0

60100085 GEOTECHNICAL FABRIC FOR FRENCH DRAINS

TRAIL - C

NOTE: SEE CONCRETE RETAINING WALL DETAILS FOR QUANTITY.

54244405 FLUSH INLET BOX FOR MEDIAN, STANDARD 542546

STATION	Q/S	EACH
<u>IL 47 RURAL</u>		
RT466+24.00	60' RT	1.0
IL 47 RURAL TOTAL =		1.0
PROJECT TOTAL =		1.0

60107700 PIPE UNDERDRAINS 6 INCH

TRAIL - C

NOTE: SEE CONCRETE RETAINING WALL DETAILS FOR QUANTITY.

60108200 PIPE UNDERDRAINS (SPECIAL) 6 INCH

TRAIL - C

NOTE: SEE CONCRETE RETAINING WALL DETAILS FOR QUANTITY.

56109210 WATER VALVES TO BE ADJUSTED

STATION	OFFSET (FT)	EACH
<u>TRAIL - I</u>		
URBAN		
16+86.00	RT 47.3'	1.0
TRAIL - I TOTAL =		1.0
PROJECT TOTAL =		1.0

60218400 MANHOLES, TYPE A, 4' -DIAMETER, TYPE 1 FRAME, CLOSED LID

STATION	OFFSET	EACH
IL 47 URBAN		
181+78.00	LT 64.0'	<u>1.0</u>
	IL 47 URBAN TOTAL =	1.0
	PROJECT TOTAL =	1.0

60221100 MANHOLES, TYPE A, 5' -DIAMETER, TYPE 1 FRAME, CLOSED LID

STATION	OFFSET	EACH
IL 47 RURAL		
319+00.00	LT 20.6'	<u>1.0</u>
	IL 47 RURAL TOTAL =	1.0
	PROJECT TOTAL =	1.0

60223800 MANHOLES, TYPE A, 6' DIAMETER, TYPE 1 FRAME, CLOSED LID

STATION	OFFSET	EACH
IL 47 URBAN		
181+59.00	LT 61.0'	<u>1.0</u>
	IL 47 URBAN TOTAL =	1.0

60234200 INLET, TYPE A, TYPE 1 FRAME OPEN LID

STATION	EACH
47 RURAL	
RT 295+10.00	<u>1.0</u>
IL 47 RURAL TOTAL =	1.0
PROJECT TOTAL =	1.0

60235700 INLETS, TYPE A, TYPE 3 FRAME AND GRATE

STATION	EACH
47 RURAL	
LT 406+60.33	<u>1.0</u>
IL 47 RURAL TOTAL =	1.0
PROJECT TOTAL =	1.0

60236200 INLETS TYPE A, TYPE 8 GRATE

STATION	EACH
47 RURAL	
LT 229+40.00	<u>1.0</u>
IL 47 RURAL TOTAL =	1.0
PROJECT TOTAL =	1.0

60240210 INLETS TYPE B, TYPE 1 OPEN LID

TRAIL - C STATION	STRUCTURE NO.	60240210 (EACH)
RT 120+75.00	1	1.0
RT 122+00.00	2	1.0
RT 123+00.00	3	1.0
RT 124+00.00	4	1.0
RT 124+75.00	5	<u>1.0</u>
TRAIL - C TOTAL =		5.0
PROJECT TOTAL =		5.0

60240215 INLETS, TYPE B, TYPE 1 FRAME, CLOSED LID

STATION	EACH
47 RURAL	
LT 406+60.33	<u>1.0</u>
IL 47 RURAL TOTAL =	1.0
PROJECT TOTAL =	1.0

60260500 INLETS TO BE ADJUSTED WITH NEW TYPE 3 FRAME AND GRATE

TRAIL - I STATION	OFFSET	EACH
URBAN		
RT 16+86.30	37.5' RT	1.0
RT 16+88.50	35.5' RT	<u>1.0</u>
TRAIL - I TOTAL =		2.0
PROJECT TOTAL =		2.0

60602800 CONCRETE GUTTER TYPE B

STATION	TO	STATION	DESCRIPTION	(FOOT)
IL 47 RURAL				
RT 339+87.00		RT 340+47.00	CR 2700N	60.0
IL 47 RURAL TOTAL =				60.0
PROJECT TOTAL =				60.0

60620400 CONCRETE MEDIAN, TYPE SB 9.06

STATION	TO	STATION	LENGTH	WIDTH	SQ. FT.
IL 47 URBAN					
25+34.80		25+79.70	44.9	VAR.	282.0
34+19.40		34+58.70	39.3	VAR.	233.0
IL 47 URBAN TOTAL =					515.0
PROJECT TOTAL =					515.0

60604400 COMBINATION CONCRETE CURB & GUTTER TYPE B-6.18

STATION	TO	STATION	DESCRIPTION	(FOOT)
URBAN TRAIL - I				
RT 20+41.64		RT 20+57.86	SE QUADRANT OF EASTWOOD DR.	24.0
TRAIL - I TOTAL =				24.0
PROJECT TOTAL =				24.0

61100605 MISCELLANEOUS CONCRETE

IL 47

NOTE: SEE TRANSITION FROM PIPE CULVERTS TO EXISTING PAVED DITCH DETAILS FOR QUANTITY.

60605000 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24

STATION	TO	STATION	DESCRIPTION	(FOOT)
URBAN TRAIL - I				
RT 16+80.84		RT 17+03.75	NE QUADRANT OF FRANKLIN ST.	42.0
RT 24+24.00		RT 24+40.00	SOUTH SIDE I-74 E.B. ON RAMP	25.0
TRAIL - I TOTAL =				67.0
IL 47 URBAN TOTAL = 73.0				
RURAL				
LT 406+14.00		LT 406+16.30	SW QUAD. AT CR 2800N	5.0
LT 406+51.40		LT 406+61.90	NW QUAD. AT CR 2800N	20.0
LT 506+09.90		LT 507+11.90	SW QUAD. RADIUS AT IL 47/US 136	140.0
RT 506+18.30		RT 506+95.80	SE QUAD. RADIUS AT IL 47/US 136	140.5
RT 507+73.40		RT 508+75.50	NE QUAD. RADIUS AT IL 47/US 136	140.0
LT 507+89.40		LT 508+67.00	NW QUAD. RADIUS AT IL 47/US 136	140.5
IL 47 RURAL TOTAL =				586.0
PROJECT TOTAL =				726.0

GUARDRAIL, TRAFFIC BARRIER & MARKERS SCHEDULES

IL 47	STATION	TO	STATION	63000001	63000025	63100085	63100167	78200410	78200520	78200530	78201000	X6330725
				SPB GUARDRAIL TY A 6 FT POSTS (FOOT)	SPB GUARDRAIL ATTACH TO STR (FOOT)	TRAFFIC BARRIER TERM., TYPE 6 (EACH)	TRAFFIC BARRIER TERM., TYPE 1 (SPL) TAN (EACH)	GRDRL MRK, A (EACH)	BARR. WALL MRK B (EACH)	BARR. WALL MRK C (EACH)	TERM MRK DA (SHORT RAD.) (EACH)	SPBGR (FOOT)
	URBAN											
RT	134+30.38		136+30.38	112.5	-	-	2.0	4.0	-	-	2.0	-
LT	135+31.61		137+31.61	112.5	-	-	2.0	4.0	-	-	2.0	-
LT	174+82.09		175+84.50	25.0	-	-	2.0	1.0	-	-	2.0	47.0
IL 47 URBAN TOTAL =				250.0	-	-	6.0	9.0	-	-	6.0	47.0

IL 47	RURAL		STATION	TO	STATION	63000001	63000025	63100085	63100167	78200410	78200520	78200530	78201000	X6330725
	STATION	TO				SPB GUARDRAIL TY A 6 FT POSTS (FOOT)	SPB GUARDRAIL ATTACH TO STR (FOOT)	TRAFFIC BARRIER TERM., TYPE 6 (EACH)	TRAFFIC BARRIER TERM., TYPE 1 (SPL) TAN (EACH)	GRDRL MRK, A (EACH)	BARR. WALL MRK B (EACH)	BARR. WALL MRK C (EACH)	TERM MRK DA (SHORT RAD.) (EACH)	SPBGR (FOOT)
RT	222+83.38		225+20.88			112.5	25.00	-	2.0	4.0	-	-	2.0	-
LT	223+78.92		226+16.42			112.5	25.00	-	2.0	4.0	-	-	2.0	-
RT	249+15.03		251+15.03			87.5	12.5	-	2.0	4.0	-	-	2.0	-
LT	249+43.94		251+43.94			87.5	12.5	-	2.0	4.0	-	-	2.0	-
RT	333+41.55		337+60.65			125.0	-	2.0	2.0	5.0	3.0	3.0	2.0	-
LT	334+16.55		338+35.65			125.0	-	2.0	2.0	5.0	3.0	3.0	2.0	-
RT	351+34.06		353+34.06			100.0	-	-	2.0	4.0	-	-	2.0	-
LT	351+85.94		353+85.94			100.0	-	-	2.0	4.0	-	-	2.0	-
RT	465+06.47		467+06.47			75.0	25.0	-	2.0	4.0	-	-	2.0	-
LT	465+27.87		467+27.87			75.0	25.0	-	2.0	4.0	-	-	2.0	-
IL 47 RURAL TOTAL =						1000.0	125.0	4.0	20.0	42.0	6.0	6.0	20.0	-
PROJECT TOTAL =						1250.0	125.0	4.0	26.0	51.0	6.0	6.0	26.0	47.0

S. F. = SINGLE FACE
 D. F. = DOUBLE FACE
 T. A. = TRIPLE AMBER

O. T. P. = ORANGE TOP POST

63500105 DELINEATORS

63500105 DELINEATORS (CONT.)

STATION (+/-)	OFFSET (FT)	TYPE	EACH
URBAN			
TRAIL - C			
120+00.00	21.0RT	D. F./T. A	1.0
125+30.00	50.0RT	D. F./T. A	1.0
TRAIL - C TOTAL =			2.0
IL 47			
36+78.00	47.5LT	O. T. P.	1.0
98+94.00	37.0RT	D. F./T. A	1.0
99+75.00	37.0RT	D. F./T. A	1.0
102+61.00	30.0RT	D. F./T. A	1.0
107+77.50	31.0RT	D. F./T. A	1.0
108+20.00	29.0LT	D. F./T. A	1.0
118+00.00	30.0RT	D. F./T. A	1.0
120+00.00	25.5RT	S. F./T. A	1.0
125+33.50	42.0RT	O. T. P.	1.0
125+88.00	23.0RT	S. F./T. A	1.0
127+67.00	28.0LT	S. F./T. A	1.0
132+20.63	28.5LT	D. F./T. A	1.0
133+02.86	28.5LT	D. F./T. A	1.0
141+62.00	26.0RT	S. F./T. A	1.0
142+24.00	23.0LT	S. F./T. A	1.0
155+63.00	24.0RT	S. F./T. A	1.0
155+67.00	27.5LT	S. F./T. A	1.0
158+00.00	33.0RT	D. F./T. A	1.0
161+40.50	27.0RT	S. F./T. A	1.0
161+48.00	23.5LT	S. F./T. A	1.0
175+57.00	31.0RT	O. T. P.	1.0
176+40.00	33.0LT	D. F./T. A	1.0
181+00.00	31.0RT	O. T. P.	1.0
181+56.00	37.5LT	O. T. P.	1.0
192+29.00	20.0RT	S. F./T. A	1.0
192+38.00	20.0LT	S. F./T. A	1.0
IL 47 URBAN TOTAL =			26.0
RURAL			
220+06.00	27.5RT	S. F./T. A	1.0
220+71.00	28.0LT	S. F./T. A	1.0
226+52.00	28.0LT	D. F./T. A	1.0
230+18.00	28.0LT	D. F./T. A	1.0
233+45.00	28.5RT	S. F./T. A	1.0
233+50.00	27.0LT	S. F./T. A	1.0
234+13.00	32.0LT	D. F./T. A	1.0
234+16.00	33.0RT	D. F./T. A	1.0
234+16.00	34.0LT	D. F./T. A	1.0
243+14.00	29.0RT	S. F./T. A	1.0
243+17.00	27.0LT	S. F./T. A	1.0
260+06.00	26.0RT	S. F./T. A	1.0
260+61.41	28.0LT	D. F./T. A	1.0
261+39.00	27.5LT	S. F./T. A	1.0
273+28.00	30.0RT	D. F./T. A	1.0
278+48.00	31.0RT	O. T. P.	1.0
278+52.00	28.0LT	O. T. P.	1.0
286+65.00	28.0RT	D. F./T. A	1.0
287+27.00	28.0LT	D. F./T. A	1.0
295+09.00	40.0RT	O. T. P.	1.0
295+13.00	26.0LT	S. F./T. A	1.0
IL 47 RURAL SUB-TOTAL =			21.0

STATION (+/-)	OFFSET (FT)	TYPE	EACH	
RURAL				
IL 47	300+25.00	28.0LT	S. F./T.	1.0
	310+47.00	25.0RT	S. F./T.	1.0
	310+51.00	28.0LT	S. F./T.	1.0
	313+10.00	28.0RT	D. F./T.	1.0
	313+73.00	30.0LT	D. F./T.	1.0
	318+97.00	20.0RT	S. F./T.	1.0
	319+01.00	29.0LT	S. F./T.	1.0
	320+25.00	30.0LT	D. F./T.	1.0
	322+26.00	30.0LT	D. F./T.	1.0
	324+32.00	28.0RT	O. T. P.	1.0
	324+36.00	37.0LT	O. T. P.	1.0
	326+25.00	28.0RT	D. F./T.	1.0
	332+81.00	32.0LT	D. F./T.	1.0
	343+23.00	28.0LT	D. F./T.	1.0
	344+78.00	30.0RT	D. F./T.	1.0
	345+64.00	28.0LT	D. F./T.	1.0
	346+35.00	29.0LT	D. F./T.	1.0
	358+04.00	28.0LT	D. F./T.	1.0
	359+94.00	31.0LT	D. F./T.	1.0
	363+34.00	25.0RT	S. F./T.	1.0
	363+37.00	24.0LT	S. F./T.	1.0
	365+96.00	31.5RT	D. F./T.	1.0
	366+65.00	29.0LT	D. F./T.	1.0
	366+64.00	32.0RT	D. F./T.	1.0
	383+04.00	27.0RT	S. F./T.	1.0
	383+11.00	24.0LT	S. F./T.	1.0
	385+35.00	31.5RT	D. F./T.	1.0
	403+73.00	30.0LT	D. F./T.	1.0
	413+97.00	24.0RT	S. F./T.	1.0
	414+05.00	23.0LT	S. F./T.	1.0
	435+55.00	31.0RT	D. F./T.	1.0
	437+96.00	20.0RT	S. F./T.	1.0
	438+05.00	20.0LT	S. F./T.	1.0
	450+97.00	25.0RT	S. F./T.	1.0
	451+01.00	28.0LT	S. F./T.	1.0
	453+70.00	28.0RT	D. F./T.	1.0
	454+39.00	21.0RT	S. F./T.	1.0
	454+43.00	24.0LT	S. F./T.	1.0
	480+21.00	30.0RT	D. F./T.	1.0
	481+04.00	34.0LT	D. F./T.	1.0
	481+69.00	31.0RT	D. F./T.	1.0
	496+08.00	30.0LT	D. F./T.	1.0
	505+08.00	41.0LT	D. F./T.	1.0
	506+61.00	59.0RT	S. F./T.	1.0
	506+69.00	54.0LT	S. F./T.	1.0
	508+06.00	59.0RT	S. F./T.	1.0
	508+14.00	68.0LT	S. F./T.	1.0
	518+69.00	26.5RT	D. F./T.	1.0
IL 47 RURAL SUB-TOTAL =			48.0	
RURAL				
US 136	1654+72.00	38.0RT	D. F./T.	1.0
	1658+50.00	34.5RT	D. F./T.	1.0
	1669+81.00	28.5RT	D. F./T.	1.0
IL 47 RURAL SUB-TOTAL =			3.0	
IL 47 RURAL TOTAL =			72.0	
PROJECT TOTAL = 100.0				

64200108 SHOULDER RUMBLE STRIPS, 8 INCH

64200108 SHOULDER RUMBLE STRIPS, 8 INCH

STATION	TO	STATION	FOOT
URBAN			
IL 47			
LT 104+00.00		LT 107+08.00	308.0
LT 108+05.00		LT 127+15.00	1910.0
LT 127+80.00		LT 131+66.00	386.0
LT 133+06.00		LT 134+34.00	128.0
LT 134+77.00		LT 142+57.00	780.0
LT 143+43.00		LT 152+95.00	952.0
LT 153+59.00		LT 156+51.00	292.0
LT 157+14.00		LT 162+27.00	513.0
LT 162+90.00		LT 175+63.00	1273.0
LT 176+40.00		LT 184+23.00	783.0
LT 184+87.00		LT 193+75.00	888.0
IL 47 URBAN SUB-TOTAL =			8213.0
RURAL			
LT 193+75.00		LT 198+42.00	467.0
LT 198+86.00		LT 219+95.00	2109.0
LT 220+86.00		LT 226+14.00	528.0
LT 226+77.00		LT 227+68.00	91.0
LT 228+21.00		LT 228+61.00	40.0
LT 229+14.00		LT 229+58.00	44.0
LT 230+21.00		LT 233+46.00	325.0
LT 234+13.00		LT 236+04.00	191.0
LT 236+57.00		LT 248+41.00	1184.0
LT 249+05.00		LT 260+19.00	1114.0
LT 261+50.00		LT 273+18.00	1168.0
LT 273+72.00		LT 280+15.00	643.0
LT 280+78.00		LT 286+61.00	583.0
LT 287+29.00		LT 299+73.00	1244.0
LT 300+36.00		LT 311+56.00	1120.0
LT 312+18.00		LT 313+11.00	93.0
LT 313+80.00		LT 316+57.00	277.0
LT 317+20.00		LT 319+83.00	263.0
LT 320+69.00		LT 321+79.00	110.0
LT 322+69.00		LT 326+18.00	349.0
LT 327+10.00		LT 332+00.00	490.0
LT 333+00.00		LT 335+13.00	213.0
LT 336+43.00		LT 342+52.00	609.0
LT 343+28.00		LT 344+92.00	164.0
LT 346+42.00		LT 347+32.00	90.0
LT 358+10.00		LT 358+84.00	74.0
LT 360+00.00		LT 366+17.00	617.0
LT 367+10.00		LT 368+87.00	177.0
LT 370+48.00		LT 397+33.00	2685.0
LT 398+22.00		LT 405+92.00	770.0
LT 401+03.00		LT 403+31.00	228.0
LT 403+84.00		LT 426+91.00	2307.0
LT 427+54.00		LT 428+30.00	76.0
LT 429+07.00		LT 440+22.00	1115.0
LT 440+85.00		LT 444+15.00	330.0
LT 444+69.00		LT 453+63.00	894.0
LT 454+30.00		LT 481+56.00	2726.0
LT 481+11.00		LT 494+23.00	1312.0
LT 494+86.00		LT 495+59.00	73.0
LT 496+15.00		LT 504+67.00	852.0
IL 47 RURAL SUB-TOTAL =			27745.0

STATION	TO	STATION	FOOT
URBAN			
IL 47			
RT 104+00.00		RT 107+94.00	394.0
RT 108+05.00		RT 114+41.00	636.0
RT 115+00.00		RT 118+00.00	300.0
RT 119+13.00		RT 125+47.00	634.0
RT 126+36.00		RT 132+33.00	597.0
RT 133+06.00		RT 133+64.00	58.0
RT 134+27.00		RT 137+97.00	370.0
RT 139+75.00		RT 141+33.00	158.0
RT 142+18.00		RT 148+84.00	666.0
RT 150+12.00		RT 156+29.00	617.0
RT 157+01.00		RT 157+89.00	88.0
RT 159+22.00		RT 166+34.00	712.0
RT 167+96.00		RT 177+19.00	923.0
RT 177+82.00		RT 183+82.00	600.0
RT 184+45.00		RT 193+12.00	867.0
IL 47 URBAN SUB-TOTAL =			7620.0
RURAL			
RT 194+43.00		RT 206+88.00	1245.0
RT 207+52.00		RT 213+57.00	605.0
RT 214+21.00		RT 220+00.00	579.0
RT 220+71.00		RT 226+69.00	598.0
RT 228+15.00		RT 233+41.00	526.0
RT 234+09.00		RT 259+95.00	2586.0
RT 260+58.00		RT 273+18.00	1260.0
RT 273+78.00		RT 280+09.00	631.0
RT 280+72.00		RT 286+56.00	584.0
RT 287+30.00		RT 299+72.00	1242.0
RT 300+35.00		RT 311+60.00	1125.0
RT 312+38.00		RT 313+06.00	68.0
RT 313+78.00		RT 321+36.00	758.0
RT 322+33.00		RT 325+73.00	340.0
RT 326+80.00		RT 335+13.00	833.0
RT 336+43.00		RT 339+87.00	344.0
RT 340+47.00		RT 344+65.00	418.0
RT 345+42.00		RT 353+53.00	811.0
RT 354+17.00		RT 358+78.00	461.0
RT 359+41.00		RT 365+05.00	564.0
RT 367+20.00		RT 384+84.00	1764.0
RT 385+46.00		RT 397+37.00	1191.0
RT 398+18.00		RT 406+07.00	789.0
RT 401+02.00		RT 427+00.00	2598.0
RT 427+64.00		RT 432+68.00	504.0
RT 433+31.00		RT 435+41.00	210.0
RT 435+93.00		RT 443+52.00	759.0
RT 444+25.00		RT 453+61.00	936.0
RT 454+31.00		RT 480+12.00	2581.0
RT 480+71.00		RT 481+62.00	91.0
RT 482+17.00		RT 496+30.00	1413.0
RT 496+93.00		RT 504+67.00	774.0
IL 47 RURAL SUB-TOTAL =			29188.0
IL 47 URBAN TOTAL =			15833.0
IL 47 RURAL TOTAL =			56933.0
PROJECT TOTAL = 72766.0			

FILE NAME =	USER NAME = ceorlockbm	DESIGNED - BMC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES PLAN PAY ITEMS				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\pwork\ceorlockbm\d0187612\08	70844-sht-schedules of plan pay items.dgn	DRAWN - BMC	REVISED -		326	136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	68				
\$MODELNAME\$	PLOT SCALE = 40.0000 ' / in.	CHECKED -	REVISED -		CONTRACT NO. 70844								
	PLOT DATE = 10/10/2014	DATE -	REVISED -		SCALE: N/A	SHEET 35	OF 47 SHEETS	STA. -----	TO STA. -----	ILLINOIS FED. AID PROJECT			

66600105 FURNISHING AND ERECTING RIGHT OF WAY MARKERS

STATION	OFFSET (FT.)	EACH
URBAN		
IL 47		
LT 120+78.81	40	1.0
LT 120+84.53	50	1.0
LT 126+20.00	40/50	2.0
LT 134+32.62	40	1.0
LT 134+31.72	55	1.0
LT 137+00.00	40/55	2.0
LT 142+25.00	40/50	2.0
LT 143+29.50	40/50	2.0
LT 156+44.75	40	1.0
LT 156+44.65	50	1.0
LT 160+00.00	40/50	2.0
LT 165+00.00	40/50	2.0
LT 170+00.00	40/50	2.0
LT 175+29.44	40	1.0
LT 175+78.49	90	1.0
LT 180+67.21	50.64	1.0
LT 180+67.39	75	1.0
LT 182+50.00	48.79/75	2.0
LT 187+00.00	44.22/60	2.0
LT 190+00.00	41.18/60	2.0
IL 47 URBAN SUB-TOTAL =		30.0

STATION	OFFSET (FT.)	EACH
RURAL		
LT 226+00.00	40/50	2.0
LT 242+00.00	40/55	2.0
LT 246+00.00	40/55	2.0
LT 255+00.00	40/50	2.0
LT 259+00.00	40/50	2.0
LT 273+67.11	40	1.0
LT 273+67.34	50	1.0
LT 279+51.54	40	1.0
LT 279+51.78	50	1.0
LT 294+00.00	40/50	2.0
LT 295+00.00	40/50	2.0
LT 313+66.47	50	1.0
LT 318+48.61	40	1.0
LT 318+48.82	50	1.0
LT 323+00.00	40/50	2.0
LT 326+63.49	55	1.0
LT 326+63.53	40	1.0
LT 332+05.63	40	1.0
LT 332+05.85	50	1.0
LT 334+01.00	40/50	2.0
LT 338+01.00	50	1.0
LT 340+00.00	40/50	2.0
LT 345+82.79	40	1.0
LT 345+84.02	50	1.0
LT 349+00.00	40/50	2.0
LT 352+00.00	40/45	2.0
LT 353+00.00	40/45	2.0
LT 382+00.00	40/50	2.0
LT 386+50.00	50	1.0
LT 391+00.00	50/60	2.0
LT 396+00.00	60	1.0
LT 401+00.00	60/70	2.0
LT 406+06.85	70	1.0
LT 411+00.00	40/50	2.0
LT 415+00.00	50	1.0
LT 420+00.00	50	1.0
LT 425+00.00	50	1.0
LT 454+20.40	50	1.0
LT 459+00.00	50	1.0
LT 464+00.00	50	1.0
LT 469+00.00	50	1.0
LT 474+00.00	50	1.0
LT 479+00.00	50	1.0
LT 484+00.00	50	1.0
LT 489+00.00	50	1.0
LT 492+45.70	50	1.0
LT 493+96.74	46.93	1.0
IL 47 RURAL SUB-TOTAL =		64.0

STATION	OFFSET (FT.)	EACH
URBAN		
IL 47		
RT 134+77.02	45	1.0
RT 136+13.63	40	1.0
RT 136+13.83	45	1.0
RT 137+00.00	40/45	2.0
RT 140+21.39	40	1.0
RT 140+21.60	50	1.0
RT 143+29.50	50	1.0
RT 144+77.25	40	1.0
RT 144+77.27	50	1.0
RT 149+33.75	40	1.0
RT 149+33.77	50	1.0
RT 152+04.92	50	1.0
RT 152+04.97	40	1.0
RT 155+50.00	50	1.0
RT 158+24.85	50	1.0
RT 158+84.85	50	1.0
RT 162+62.80	50	1.0
RT 162+62.88	40	1.0
RT 174+00.00	40/60	2.0
RT 177+00.00	40/60	2.0
RT 180+00.00	40/70	2.0
RT 183+00.00	40/70	2.0
RT 187+00.00	40/50	2.0
RT 191+00.00	40/50	2.0
IL 47 URBAN SUB-TOTAL =		31.0

STATION	OFFSET (FT.)	EACH
RURAL		
RT 223+00.00	40/55	2.0
RT 226+00.00	40/55	2.0
RT 233+00.00	40	1.0
RT 233+29.87	50	1.0
RT 233+56.55	105	1.0
RT 233+96.55	105	1.0
RT 234+24.88	50	1.0
RT 240+00.00	40/50	2.0
RT 242+00.00	40/45	2.0
RT 244+00.00	40/45	2.0
RT 253+00.00	40/55	2.0
RT 257+50.00	55	1.0
RT 262+00.00	40/55	2.0
RT 294+00.00	40/55	2.0
RT 296+00.00	40/55	2.0
RT 313+65.12	50	1.0
RT 316+00.00	40/50	2.0
RT 323+00.79	50	1.0
RT 323+01.00	40	1.0
RT 325+69.66	50	1.0
RT 325+69.87	40	1.0
RT 332+00.00	40/50	2.0
RT 339+83.50	50	1.0
RT 347+00.00	40/55	2.0
RT 350+50.00	55	1.0
RT 366+57.92	40	1.0
RT 366+58.34	55	1.0
RT 371+00.00	55	1.0
RT 376+00.00	55	1.0
RT 380+00.00	40/55	2.0
RT 393+00.00	40/50	2.0
RT 396+50.00	50	1.0
RT 400+00.00	40/50	2.0
RT 411+00.00	40/50	2.0
RT 416+00.00	50	1.0
RT 421+00.00	50	1.0
RT 426+00.00	50	1.0
RT 437+00.00	40/50	2.0
RT 441+50.00	50	1.0
RT 446+00.00	40/50	2.0
RT 454+23.48	60	1.0
RT 455+00.00	55	1.0
RT 459+44.97	55	1.0
IL 47 RURAL SUB-TOTAL =		61.0

STATION	OFFSET (FT.)	EACH
RURAL		
IL 47		
RT 464+00.00	50	1.0
RT 469+00.00	50	1.0
RT 474+00.00	50	1.0
RT 479+00.00	50	1.0
RT 480+58.74	40	1.0
RT 480+58.83	50	1.0
RT 485+00.00	40/50	2.0
RT 489+00.00	50	1.0
RT 492+45.70	50	1.0
RT 494+00.00	43.15	1.0
IL 47 RURAL SUB-TOTAL =		11.0

IL 47 URBAN TOTAL = 61.0
 IL 47 RURAL TOTAL = 136.0
 PROJECT TOTAL = 197.0

70103815 TRAFFIC CONTROL SURVEILLANCE

STATION	CAL. DAYS
URBAN	
IL 47	
BEG. TO 193+75.00	8.0
IL 47 URBAN TOTAL =	8.0
RURAL	
193+75.00 TO END	17.0
IL 47 RURAL TOTAL =	17.0
PROJECT TOTAL = 25.0	

78001110 PAINT PAVEMENT MARKING - LINE 4" (WHITE)

EDGE LINES/TURN LANES SINGLE SOLID WHITE

70300220
TEMP PVT
MK - 4"
FOOT FOOT

IL 47

URBAN

STATION	TO	STATION	78001110 FOOT	70300220 FOOT
LT 101+00.00		LT 175+63.00	7,463.0	7,463.0
RT 101+00.00		RT 118+00.00	1,700.0	1,700.0
RT 119+12.50		RT 125+48.00	635.5	635.5
RT 126+36.00		RT 157+89.00	3,153.0	3,153.0
RT 159+22.00		RT 166+34.00	712.0	712.0
RT 167+96.00		RT 193+12.00	2,516.0	2,516.0
LT 176+40.00		LT 193+75.00	1,735.0	1,735.0

IL 47 URBAN SUB-TOTAL = 17,914.5 17,914.5

RURAL

LT 193+75.00		LT 233+46.00	3,971.0	3,971.0
RT 193+96.00		RT 233+41.00	3,945.0	3,945.0
RT 234+10.00		RT 286+56.00	5,246.0	5,246.0
LT 234+13.00		LT 286+61.00	5,248.0	5,248.0
LT 287+29.00		LT 313+11.00	2,582.0	2,582.0
RT 287+29.00		RT 313+06.00	2,577.0	2,577.0
RT 313+78.00		RT 339+87.00	2,609.0	2,609.0
LT 313+80.00		LT 368+87.00	5,507.0	5,507.0
RT 340+47.00		RT 406+07.00	6,560.0	6,560.0
LT 370+48.00		LT 405+92.00	3,544.0	3,544.0
LT 401+03.00		LT 408+63.00	760.0	760.0
RT 401+03.00		RT 408+61.00	758.0	758.0
LT 409+30.00		LT 504+00.00	9,470.0	9,470.0
RT 409+30.00		RT 504+00.00	9,470.0	9,470.0
RT 504+00.00 (IL 47) SE QUAD. RAD.		RT 1663+00.00 (US 136)	191.5	191.5
RT 504+00.00 (IL 47) SW QUAD. RAD.		RT 1660+00.00 (US 136)	184.8	184.8
LT 504+16.91 (TURN LANE)		LT 506+83.91	267.0	267.0
RT 1660+00.00 (US 136) NW QUAD. RAD.		RT 509+00.00	182.5	182.5
LT 1663+00.00 (US 136) NE QUAD. RAD.		LT 509+00.00	223.5	223.5
RT 507+98.71 (TURN LANE)		RT 510+65.71	267.0	267.0
RT 509+00.00		RT 523+18.70	1,418.7	1,418.7
LT 509+00.00		LT 523+18.70	1,418.7	1,418.7

IL 47 RURAL SUB-TOTAL = 66,400.7 66,400.7

US 136

RURAL

LT 1651+00.00		LT 1660+00.00	900.0	900.0
RT 1651+00.00		RT 1660+00.00	900.0	900.0
RT 1658+25.35 (TURN LANE)		RT 1661+04.36	279.0	279.0
LT 1660+75.76 (TURN LANE)		LT 1664+52.44	376.7	376.7
LT 1663+00.00		LT 1674+00.00	1,100.0	1,100.0
RT 1663+00.00		RT 1674+00.00	1,100.0	1,100.0

IL 47 RURAL SUB-TOTAL = 4,655.7 4,655.7

FILE NAME =	USER NAME = ceerlockbm	DESIGNED - BMC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES PLAN PAY ITEMS				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
c:\pwork\pwork\ceerlockbm\d0187612\08	70844-sht-schedules of plan pay items.dgn	DRAWN - BMC	REVISED -		SCALE: N/A	SHEET 37	OF 47	SHEETS	STA. -----	TO STA. -----	326	136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	70
\$MODELNAME\$	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -						CONTRACT NO. 70844						
	PLOT DATE = 10/10/2014	DATE -	REVISED -						ILLINOIS FED. AID PROJECT						

78001110 PAINT PAVEMENT MARKING - LINE 4" (YELLOW)

CENTERLINE SKIP-DASH

STATION	TO	STATION	78001110	70300220	70300100	70301000
			FOOT	TEMP PVT MK - 4" FOOT	SHRT TRM PVT MK FOOT	WRK ZONE PVT MK REM SQ FT
URBAN						
108+82.00		157+95.00	1,228.3	1,228.3	491.3	162.1
157+95.00		160+48.00	*	*	25.3	8.3
160+48.00		193+75.00	831.8	831.8	332.7	109.8
IL 47 URBAN SUB-TOTAL =			2,060.0	2,060.0	849.3	280.3
RURAL						
193+75.00		333+62.00	3,496.8	3,496.8	1398.7	461.6
333+62.00		337+43.00	*	*	38.1	12.6
337+43.00		350+25.00	320.5	320.5	128.2	42.3
350+25.00		353+14.00	*	*	28.9	9.5
353+14.00		360+87.00	193.3	193.3	77.3	25.5
360+87.00		377+77.00	*	*	169.0	55.8
377+77.00		384+79.00	175.5	175.5	70.2	23.2
384+79.00		385+80.00	*	*	10.1	3.3
385+80.00		395+93.00	253.3	253.3	101.3	33.4
395+93.00		403+20.00	*	*	72.7	24.0
403+20.00		497+01.91	2,345.5	2,345.5	938.2	309.6
497+01.91		507+00.00	*	*	99.8	32.9
508+00.00		517+80.70	*	*	98.1	32.4
517+80.70		523+18.70	134.5	134.5	53.8	17.8
1651+00.00		1661+00.00	*	*	100.0	33.0
1662+00.00		1671+67.24	*	*	96.7	31.9
1671+67.24 (US 136)		1674+00.00 (US 136)	58.2	58.2	23.3	7.7
IL 47 RURAL SUB-TOTAL =			6,977.4	6,977.4	3,504.4	1,156.4

* NO SKIP-DASH QUANTITY REQUIRED DUE TO DOUBLE NO PASSING ZONE. THESE STATION RANGES WERE SHOWN FOR THE SHORT TERM PAVEMENT MARKING QUANTITIES.

NO PASSING ZONE SOLID YELLOW

STATION	DESCRIPTION	TO	STATION	DESCRIPTION	78001110	70300220	70300100	70301000
					FOOT	TEMP PVT MK - 4" FOOT	SHRT TRM PVT MK FOOT	WRK ZONE PVT MK REM SQ FT
URBAN								
101+00.00	BEG. N. B.		108+82.00	END N. B.	782.0	782.0	-	-
101+00.00	END S. B.		115+91.00	BEG. S. B.	1,491.0	1,491.0	-	-
140+74.00	BEG. N. B.		149+32.00	END N. B.	858.0	858.0	-	-
150+49.00	END S. B.		160+48.00	BEG. S. B.	999.0	999.0	-	-
157+95.00	BEG. N. B.		166+43.00	END N. B.	848.0	848.0	-	-
168+90.00	END S. B.		177+11.00	BEG. S. B.	821.0	821.0	-	-
189+63.00	BEG. N. B.		193+75.00		412.0	412.0	-	-
IL 47 URBAN SUB-TOTAL =					6,211.0	6,211.0	-	-
RURAL								
193+75.00			198+36.00	END N. B.	461.0	461.0	-	-
200+24.00	END S. B.		208+71.00	BEG. S. B.	847.0	847.0	-	-
316+68.00	BEG. N. B.		326+60.00	END N. B.	992.0	992.0	-	-
326+60.00	END S. B.		337+43.00	BEG. S. B.	1,083.0	1,083.0	-	-
333+62.00	BEG. N. B.		342+59.00	END N. B.	897.0	897.0	-	-
346+60.00	END S. B.		353+14.00	BEG. S. B.	654.0	654.0	-	-
350+25.00	BEG. N. B.		377+77.00	END N. B.	2,752.0	2,752.0	-	-
360+87.00	END S. B.		385+80.00	BEG. S. B.	2,493.0	2,493.0	-	-
384+79.00	BEG. N. B.		403+20.00	END N. B.	1,841.0	1,841.0	-	-
395+93.00	END S. B.		408+70.00	BEG. S. B.	1,277.0	1,277.0	-	-
IL 47 RURAL SUB-TOTAL =					13,297.0	13,297.0	-	-

MEDIAN STRIPING SINGLE SOLID YELLOW				70300220	70300100	70301000
				TEMP PVT	SHRT TRM	WRK ZONE
STATION	TO	STATION	78001110	MK - 4"	PVT MK	PVT MK REM
RURAL				FOOT	FOOT	FOOT
				FOOT	FOOT	SO FT
LT	497+01.91	NOTE: DOUBLE LINE	LT 506+86.00	1,968.2	1,968.2	-
RT	497+01.91	NOTE: DOUBLE LINE	RT 506+86.00	1,968.2	1,968.2	-
LT	507+97.00	NOTE: DOUBLE LINE	LT 517+81.00	1,968.0	1,968.0	-
RT	507+97.00	NOTE: DOUBLE LINE	RT 517+81.00	1,968.0	1,968.0	-
IL 47 & US 136 SE QUAD ISLAND				208.0	208.0	-
IL 47 & US 136 NE QUAD ISLAND				187.0	187.0	-
IL 47 & US 136 NW QUAD ISLAND				208.0	208.0	-
IL 47 & US 136 SW QUAD ISLAND				187.0	187.0	-
<u>US 136</u>						
RURAL						
LT	1651+10.00	NOTE: DOUBLE LINE	LT 1660+95.00	1,970.0	1,970.0	-
RT	1651+10.00	NOTE: DOUBLE LINE	RT 1660+95.00	1,970.0	1,970.0	-
LT	1661+83.30	NOTE: DOUBLE LINE	LT 1671+67.24	1,967.9	1,967.9	-
RT	1661+83.30	NOTE: DOUBLE LINE	RT 1671+67.24	1,967.9	1,967.9	-
IL 47 RURAL SUB-TOTAL =				16,538.1	16,538.1	-
IL 47 RURAL TOTAL =				107,868.9	107,868.9	3,504.4
IL 47 RURAL USE =				107,869.0	107,869.0	3,505.0
IL 47 URBAN TOTAL =				26,185.5	26,185.5	849.3
IL 47 URBAN USE =				26,186.0	26,186.0	850.0
PROJECT TOTAL =				134,055.0	-	4,355.0

78009004 MODIFIED URETHANE PAVEMENT MARKING LINE - 4"

EDGELINES/TURN LANES SINGLE SOLID WHITE				70300220	X7830070
				TEMP PVT	GRV RCSD PVT
STATION	TO	STATION	78009004	MK - 4"	MRKG 5"
URBAN				FOOT	FOOT
				FOOT	FOOT
RT	25+00.00		RT 35+00.00	1,000.0	1,000.0
LT	25+10.00		LT 34+94.00	984.0	984.0
LT	25+55.00 (TURN LANE)		LT 27+36.00	181.0	181.0
RT	31+65.00 (TURN LANE)		RT 34+39.00	274.0	274.0
LT	36+05.00		LT 38+72.76 (BK)	267.8	267.8
LT	97+14.44 (AH)		LT 101+00.00	385.6	385.6
RT	36+27.50		RT 38+72.76 (BK)	245.3	245.3
RT	97+14.44 (AH)		RT 101+00.00	385.6	385.6
IL 47 URBAN SUB-TOTAL =				3723.1	3723.1

MEDIAN STRIPING SINGLE SOLID YELLOW				70300220	X7830070
				TEMP PVT	GRV RCSD PVT
STATION	TO	STATION	78009004	MK - 4"	MRKG 5"
URBAN				FOOT	FOOT
				FOOT	FOOT
RT	23+82.80		RT 24+70.00	87.2	87.2
LT	23+82.80		RT 24+70.00	87.2	87.2
RT	25+55.00 (LT SIDE OF MEDIAN)		LT 34+40.00	885.0	885.0
RT	25+55.00 (RT SIDE OF MEDIAN)		LT 34+40.00	885.0	885.0
LT	35+30.00 (LT SIDE OF MEDIAN)		LT 38+72.76 (BK)	342.8	342.8
LT	35+30.00 (RT SIDE OF MEDIAN)		RT 38+72.76 (BK)	342.8	342.8
LT	97+14.44 (AH) (LT SIDE OF MEDIAN)		RT 101+00.00	385.6	385.6
RT	97+14.44 (AH) (RT SIDE OF MEDIAN)		RT 101+00.00	385.6	385.6
IL 47 URBAN SUB-TOTAL =				3,401.0	3,401.0
IL 47 URBAN TOTAL =				7,124.2	7,124.2
IL 47 URBAN USE =				7,125.0	7,125.0
PROJECT TOTAL =				7,125.0	-

TEMP. PVMNT MK 4" PROJECT TOTAL = 141180.00

FILE NAME =	USER NAME = ceerlockbm	DESIGNED - BMC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES PLAN PAY ITEMS				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
et:\pw\work\p\midot\ceerlockbm\d0187612\0970844-sht-schedules of plan pay items.dgn		DRAWN - BMC	REVISED -		SCALE: N/A	SHEET 39	OF 47 SHEETS	STA. -----	TO STA. -----	326	136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	72
\$MODELNAME\$		CHECKED -	REVISED -						CONTRACT NO. 70844					
		DATE -	REVISED -						ILLINOIS FED. AID PROJECT					

78009006 MODIFIED URETHANE PAVEMENT MARKING - LINE 6"

LOCATION DESCRIPTION	FOOT	70300240 TEMP PVT MK - 6"	X7830074 GRV RCSD PVT MRKG 7"
<u>TRAIL - I</u> URBAN (CROSS WALK AT FRANKLIN ST.)	64.0	-	64.0
	TRAIL - I TOTAL =		
	64.0	-	64.0
<u>TRAIL - C</u> (CROSSWALK AT BRIARCLIFF DR.)	122.0	-	122.0
	TRAIL - C TOTAL =		
	122.0	-	122.0
<u>IL 47</u> ISLAND STRIPING AT IL 47 & E. B. I-74 OFF RAMP	155.0	155.0	155.0
ISLAND STRIPING AT IL 47 & W. B. I-74 OFF RAMP	104.0	104.0	104.0
	IL 47 URBAN TOTAL =		
	259.0	259.0	259.0
RURAL			
ISLAND STRIPING AT IL 47 & US 136 SE QUAD	208.0	208.0	208.0
ISLAND STRIPING AT IL 47 & US 136 NE QUAD	187.0	187.0	187.0
ISLAND STRIPING AT IL 47 & US 136 NW QUAD	208.0	208.0	208.0
ISLAND STRIPING AT IL 47 & US 136 SW QUAD	187.0	187.0	187.0
	IL 47 RURAL TOTAL =		
	790.0	790.0	790.0
	PROJECT TOTAL =		
	1235.0	1049.0	1235.0

78009012 MODIFIED URETHANE PAVEMENT MARKING - LINE 12"

12" DIAGONAL (YELLOW)				70300260 TEMP PVT MK - 12"	X7830078 GRV RCSD PVT MRKG 13"
STATION	DESCRIPTION	IQ	STATION	FOOT	FOOT
<u>IL 47</u> RURAL					
497+00.00	STRIPED MEDIAN		506+86.00	411.3	411.3
507+97.00	STRIPED MEDIAN		517+81.00	402.3	402.3
					0.0
<u>US 136</u> 1651+00.00	STRIPED MEDIAN		1661+00.00	359.0	359.0
1662+00.00	STRIPED MEDIAN		1671+68.00	344.6	344.6
				IL 47 RURAL SUB-TOTAL =	
				1517.2	1517.2
					1517.2
12" DIAGONAL (WHITE)				70300260 TEMP PVT MK - 12"	X7830078 GRV RCSD PVT MRKG 13"
DESCRIPTION				FOOT	FOOT
URBAN					
ISLAND STRIPING AT IL 47 & E. B. I-74 OFF RAMP				77.7	77.7
ISLAND STRIPING AT IL 47 & W. B. I-74 OFF RAMP				50	50
				IL 47 URBAN TOTAL =	
				127.7	127.7
				IL 47 URBAN USE =	
				128.0	128.0
RURAL					
ISLAND STRIPING AT IL 47 & US 136 SE QUAD				147.7	147.7
ISLAND STRIPING AT IL 47 & US 136 NE QUAD				118.1	118.1
ISLAND STRIPING AT IL 47 & US 136 NW QUAD				147.7	147.7
ISLAND STRIPING AT IL 47 & US 136 SW QUAD				118.1	118.1
				IL 47 RURAL SUB-TOTAL =	
				531.6	531.6
				IL 47 RURAL TOTAL =	
				2048.8	2048.8
				IL 47 RURAL USE =	
				2049.0	2049.0
				PROJECT TOTAL =	
				2177.0	2177.0

78009024 MODIFIED URETHANE PAVEMENT MARKING - LINE 24"

70300280 X7830090
 TEMP PVT GRV RCSD PVT
 MRK - 24" MRKG 25"
 FOOT FOOT FOOT

LOCATION DESCRIPTION	STATION	FOOT	FOOT	FOOT
<u>TRAIL - I</u>				
STOP BAR AT FRANKLIN ST.	16+70.00	14.0	-	14.0
		TRAIL - I TOTAL =		14.0
<u>IL 47</u>				
STOP BAR AT BRIARCLIFF DR.	126+04.00	22.0	22.0	22.0
		IL 47 URBAN TOTAL =		22.0
	RURAL			
<u>IL 47</u>				
US 136 E. B.	1660+92.35	12.0	12.0	12.0
US 136 E. B.	1661+02.36	12.0	12.0	12.0
US 136 W. B.	1661+78.00	12.0	12.0	12.0
US 136 W. B.	1661+87.24	12.0	12.0	12.0
IL 47 N. B. TO US 136 E. B.	506+92.76	18.0	18.0	18.0
US 136 W. B. TO IL 47 N. B.	507+95.91	18.0	18.0	18.0
IL 47 S. B. TO US 136 W. B.	507+95.48	18.0	18.0	18.0
US 136 E. B. TO IL 47 S. B.	506+74.77	18.0	18.0	18.0
IL 47 N. B.	506+81.90	24.0	24.0	24.0
IL 47 S. B.	508+00.70	24.0	24.0	24.0
		IL 47 RURAL TOTAL =		168.0
		PROJECT TOTAL =		204.0

78009000 MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS

DESCRIPTION	STATION	SQ. FT.
	URBAN	
<u>IL 47</u>		
LEFT TURN ARROW	25+76.00	15.6
LEFT TURN ARROW	26+56.00	15.6
LEFT TURN ARROW	27+36.00	15.6
LEFT TURN ARROW	32+65.20	15.6
LEFT TURN ARROW	33+43.60	15.6
LEFT TURN ARROW	34+22.00	15.6
		IL 47 URBAN TOTAL = 93.6
		IL 47 URBAN USE = 94.0
	RURAL	
LEFT TURN ARROW	504+16.91	15.6
LEFT TURN ARROW	504+78.66	15.6
LEFT TURN ARROW	505+40.41	15.6
LEFT TURN ARROW	506+02.16	15.6
LEFT TURN ARROW	506+63.91	15.6
LEFT TURN ARROW	508+18.71	15.6
LEFT TURN ARROW	508+80.46	15.6
LEFT TURN ARROW	509+42.21	15.6
LEFT TURN ARROW	510+03.96	15.6
LEFT TURN ARROW	510+65.71	15.6
LEFT TURN ARROW	1658+25.35	15.6
LEFT TURN ARROW	1658+87.10	15.6
LEFT TURN ARROW	1659+48.85	15.6
LEFT TURN ARROW	1663+28.74	15.6
LEFT TURN ARROW	1663+90.49	15.6
LEFT TURN ARROW	1664+52.24	15.6
		IL 47 RURAL TOTAL = 249.6
		IL 47 RURAL USE = 250.0
		PROJECT TOTAL = 344.0

SIGN SCHEDULE

STATION	TYPE	DESCRIPTION	O/S	72000100	72000200	72800100	72900100	73100100
				SIGN PANEL TYPE SQ. FT.	SIGN PANEL TYPE 2 SQ. FT.	TELESCOPING STEEL SIGN SUPPORT FOOT	METAL POST TYPE A FOOT	BASE FOR TELESCOPING SIGN SUPPORT EACH
URBAN								
TRAIL - I								
16+85.37	W11-15/W11-15P	COMBO. BIKE/PED CROSSING (30" X 30" W/ 24"X18" SUPP.)	57.9' RT	-	9.25	16.5	-	1.0
17+02.59	R1-2	YIELD (18" x 18" x 18")	38.4' RT	0.98	-	-	12.0	-
20+53.14	R1-2	YIELD (18" x 18" x 18")	51.6' RT	0.98	-	-	12.0	-
21+23.63	W11-15/W11-15P	COMBO. BIKE/PED CROSSING (30" X 30" W/ 24"X18" SUPP.)	143.0' RT	-	9.25	16.5	12.0	1.0
21+68.50	R1-2	YIELD (18" x 18" x 18")	45.0' RT	0.98	-	-	12.0	-
22+50.00	W11-15/W16-2aP	COMBO. BIKE/PED CROSSING (30" X 30" W/ 24"X18" SUPP.)	30.7' RT	-	9.25	16.5	-	1.0
24+35.60	R1-2	YIELD (18" x 18" x 18")	57.1' RT	0.98	-	-	12.0	-
25+00.00	R1-2	YIELD (18" x 18" x 18")	36.3' RT	0.98	-	-	12.0	-
25+39.50	W11-15/W11-15P	COMBO. BIKE/PED CROSSING (30" X 30" W/ 24"X18" SUPP.)	36.1' RT	0.98	-	-	12.0	-
25+59.73	W11-15/W11-15P	COMBO. BIKE/PED CROSSING (30" X 30" W/ 24"X18" SUPP.)	35.7' LT	0.98	-	-	12.0	-
25+59.75	R1-1	STOP (18" X 18")	37.0' RT	2.25	-	-	12.0	-
28+50.00	W11-15/W16-2aP	COMBO. BIKE/PED CROSSING (30" X 30" W/ 24"X18" SUPP.)	38.3' LT	-	9.25	16.5	-	1.0
TRAIL - I TOTAL =				9.1	37.0	66.0	108.0	4.0
TRAIL - I USE =				10.0	37.0	66.0	108.0	4.0

STATION	TYPE	DESCRIPTION	O/S	72000100	72000200	72800100	72900100	73100100
				SIGN PANEL TYPE SQ. FT.	SIGN PANEL TYPE 2 SQ. FT.	TELESCOPING STEEL SIGN SUPPORT FOOT	METAL POST TYPE A FOOT	BASE FOR TELESCOPING SIGN SUPPORT EACH
TRAIL - C								
107+91.00	R1-1	STOP (18" x 18")	15' LT	2.25	-	-	12.0	-
108+07.00	R1-1	STOP (18" x 18")	18' RT	2.25	-	-	12.0	-
118+30.00	R1-2	YIELD (18" x 18" x 18")	80' RT	0.98	-	-	12.0	-
118+60.00	W11-15/W11-15P	COMBO. BIKE/PED CROSSING (30" X 30" W/ 24"X18" SUPP.)	108' RT	-	9.25	16.5	-	1.0
118+78.00	R1-2	YIELD (18" x 18" x 18")	61' RT	0.98	-	-	12.0	-
125+54.00	R1-2	YIELD (18" x 18" x 18")	33' RT	0.98	-	-	12.0	-
126+10.00	W11-15/W11-15P	COMBO. BIKE/PED CROSSING (30" X 30" W/ 24"X18" SUPP.)	69' RT	-	9.25	16.5	-	1.0
126+34.00	R1-2	YIELD (18" x 18" x 18")	22' RT	0.98	-	-	12.0	-
TRAIL - C TOTAL =				8.40	18.50	33.0	72.0	2.0
TRAIL - C USE =				9.0	19.0	33.0	72.0	2.0
PROJECT TOTAL =				19.0	56.0	99.0	180.0	6.0

78100100 RAISED REFLECTIVE PAVEMENT MARKERS

STATION	TO	STATION	DESCRIPTION	TWO WAY	ONE WAY	ONE WAY
				AMBER RRP	AMBER RRP	CRYSTAL RRP
				EACH	EACH	EACH
<u>IL 47</u>						
URBAN						
99+62.00 *		108+82.00	DOUBLE NO PASSING	24.0	-	-
108+82.00		157+95.00	CENTERLINE	62.0	-	-
157+95.00 *		160+48.00	DOUBLE NO PASSING	8.0	-	-
160+48.00		193+75.00	CENTERLINE	42.0	-	-
IL 47 URBAN TOTAL =				136.0	-	-
RURAL						
193+75.00		333+62.00	CENTERLINE	175.0	-	-
333+62.00 *		337+43.00	DOUBLE NO PASSING	10.0	-	-
337+43.00		350+25.00	CENTERLINE	17.0	-	-
350+25.00 *		353+14.00	DOUBLE NO PASSING	8.0	-	-
353+14.00		360+87.00	CENTERLINE	10.0	-	-
360+87.00 *		377+77.00	DOUBLE NO PASSING	44.0	-	-
377+77.00		384+79.00	CENTERLINE	9.0	-	-
384+79.00 *		385+80.00	DOUBLE NO PASSING	4.0	-	-
385+80.00		395+93.00	CENTERLINE	13.0	-	-
395+93.00 *		403+20.00	DOUBLE NO PASSING	20.0	-	-
403+20.00		406+69.20 (BK)	CENTERLINE	5.0	-	-
401+00.92 (AH)		497+01.91	CENTERLINE	121.0	-	-
497+01.91 *		506+86.00	STRIPED MEDIAN	-	50.0	-
504+16.91		507+83.91	TURN LANE	-	-	10.0
507+97.00 *		517+81.00	STRIPED MEDIAN	-	50.0	-
507+98.71		510+65.71	TURN LANE	-	-	7.0
517+81.00		523+18.70	CENTERLINE	7.0	-	-
IL 47 RURAL SUB-TOTAL =				443.0	100.0	17.0
<u>US 136</u>						
RURAL						
1651+10.00		1660+95.00	STRIPED MEDIAN	-	50.0	-
1655+60.35		1661+04.36	TURN LANE	-	-	14.0
1661+75.76		1664+52.44	TURN LANE	-	-	7.0
1661+83.30		1671+67.24	STRIPED MEDIAN	-	50.0	-
1671+67.24		1674+00.00	CENTERLINE	3.0	-	-
IL 47 RURAL SUB-TOTAL =				3.0	100.0	21.0
IL 47 RURAL TOTAL =				446.0	200.0	38.0
IL 47 URBAN TOTAL =				136.0	-	-
PROJECT TOTAL =				582.0	200.0	38.0

* NOTE: DOUBLE NO PASSING ZONE REQUIRES DOUBLE RRPMS.

NOTE: QUANTITIES FOR THE ITEMS LISTED BELOW ARE LOCATED IN THE BILL OF MATERIALS FOUND ON THE FOLLOWING DETAILS:

PLAN DETAILS:

- * FLASHING BEACON DETAILS MULTI-USE TRAIL - I
- * TRAFFIC SIGNAL MODERNIZATION DETAIL (FRANKLIN ST) MULTI-USE TRAIL - I

<u>TRAIL - I</u>	<u>PAY ITEM</u>	<u>DESCRIPTION</u>
	80500100	SERVICE INSTALLATION, TYPE A
	81028340	UNDERGROUND CONDUIT, PVC, 1 1/2" DIA.
	81028360	UNDERGROUND CONDUIT, PVC, 2 1/2" DIA.
	81100500	CONDUIT ATTACHED TO STRUCTURE, 1 1/2" DIA. GALVANIZED STEEL
	81400100	HANDHOLE
	85800100	FLASHER CONTROLLER
	87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2 C
	87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
	87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
	87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7 C
	87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C
	87800100	CONCRETE FOUNDATION, TYPE A
	88040030	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 1-SECTION, POST MOUNTED
	88800100	PEDESTRIAN PUSH-BUTTON
	89501250	RELOCATE EXISTING TRAFFIC SIGNAL EQUIPMENT
	89502300	REMOVE ELECTRIC CABLE FROM CONDUIT
	89502385	REMOVE EXISTING CONCRETE FOUNDATION
	X8730250	ELECTRIC CABLE IN CONDUIT NO. 20 3/C, TWISTED, SHIELDED

X0326696 SIGN AND POST

<u>STATION</u>	<u>OFFSET (FT)</u>	<u>EACH</u>
URBAN		
IL 47		
180+89.00	68.0 RT	1.0
181+59.00	61.0 LT	1.0
181+72.00	64.0 LT	1.0
		IL 47 URBAN TOTAL = 3.0
RURAL		
268+88.00	40.0 LT	1.0
295+11.00	55.0 RT	1.0
342+32.00	40.0 LT	1.0
342+96.00	40.0 RT	1.0
363+39.00	53.0 RT	1.0
422+43.50	40.0 LT	1.0
466+07.00	50.0 LT	1.0
466+22.00	50.0 RT	1.0
482+58.00	40.0 LT	1.0
482+58.00	40.0 RT	1.0
490+57.00	40.0 RT	1.0
490+69.00	40.0 LT	1.0
506+64.00	70.0 RT	1.0
506+68.00	112.0 RT	1.0
		IL 47 RURAL TOTAL = 14.0
PROJECT TOTAL = 17.0		

* NOTE: THESE ARE INTENDED TO BE INSTALLED FOR DRAINAGE STRUCTURE MARKERS. SEE DISTRICT SPECIAL PROVISIONS.

88600100 DETECTOR LOOP, TYPE I

<u>LOCATION</u>	<u>FOOT</u>
URBAN	
IL 47 NEAR NORTHERN I-74 RAMPS	229.00
IL 47 URBAN TOTAL = 229.00	
PROJECT TOTAL = 229.0	

X2011000 TEMPORARY FENCE (SPECIAL)

<u>STATION TO</u>	<u>STATION</u>	<u>FOOT</u>
URBAN		
IL 47		
LT 104+00.00	LT 106+00.00	200.0
LT 108+50.00	LT 120+50.00	1200.0
		IL 47 URBAN TOTAL = 1400.0
PROJECT TOTAL = 1400.0		

X6061702 CONCRETE MEDIAN, TYPE SM (DOWELED)

STATION	DESCRIPTION	SQ FT
RURAL		
IL 47		
RT 506+96.00	ISLAND, SE QUAD OF IL 47 & US 136	429.0
LT 507+02.00	ISLAND, SW QUAD OF IL 47 & US 136	271.0
RT 507+85.00	ISLAND, NE QUAD OF IL 47 & US 136	271.0
LT 507+90.00	ISLAND, NW QUAD OF IL 47 & US 136	446.0
IL 47 RURAL TOTAL =		1417.0
PROJECT TOTAL =		1417.0

XZ193300 SURVEY MARKER, TYPE 1 (SPECIAL)

STATION	OFFSET	DESCRIPTION	EACH
URBAN			
IL 47			
106+31.61	52.45' RT	P. I. (#133)	1.0
115+40.80	0.00'	P. C./P. T. (#112)	1.0
129+38.64	60.54' RT	P. I. (#113)	1.0
143+29.50	0.00'	P. T. (#108)	1.0
IL 47 URBAN TOTAL =			4.0
RURAL			
311+42.22	0.00'	P. I. (#9)	1.0
366+05.20	0.00'	P. C. (#65)	1.0
377+76.51	0.00'	P. T. (#66)	1.0
394+96.46	0.00'	P. C. (#67)	1.0
406+69.20(BK) =401+00.92(AH)	0.00'	P. T. (#57)	1.0
492+45.70	0.00'	P. C. (#63)	1.0
498+51.25	10.33' RT	P. I. (#58)	1.0
504+56.32	0.00'	P. T. (#73)	1.0
507+35.68(BK) =507+49.60(AH)	0.00'	INT. PT. (#61 & #49)	1.0
512+87.49	0.00'	P. C. (#62)	1.0
518+87.49	8.81' LT	P. C. (#75)	1.0
IL 47 RURAL TOTAL =			11.0
PROJECT TOTAL =			15.0

* NOTE: NUMBERS IN DESCRIPTION ARE SURVEY POINT NUMBERS. SEE ALIGNMENT, TIES, AND BENCHMARKS PLAN SHEETS FOR ADDITIONAL DATA.

X7015005 CHANGEABLE MESSAGE SIGN

CULVERT	STATION	S.N.	CAL. DAYS
URBAN			
IL 47			
NO. 2	181+27.00	010-8151	11.0
IL 47 URBAN TOTAL =			11.0
RURAL			
NO. 3	224+48.00	010-8152	7.0
IL 47 RURAL TOTAL =			7.0
PROJECT TOTAL =			18.0

Z0030900 INSPECTION WELLS

STATION	OFFSET	EACH
URBAN		
IL 47		
18089	RT 68.0'	1.0
181+72.00	LT 66.0'	1.0
IL 47 URBAN TOTAL =		2.0
RURAL		
352+74.00	RT 55.0'	1.0
414+48.00	LT 49.5'	1.0
414+48.00	RT 49.5'	1.0
451+25.00	LT 39.5'	1.0
451+25.00	RT 39.5'	1.0
466+07.00	LT 49.0'	1.0
466+22.00	RT 49.0'	1.0
482+06.00	RT 39.7'	1.0
IL 47 RURAL TOTAL =		8.0
PROJECT TOTAL =		10.0

Z0070100 SURVEY MONUMENT COVER ASSEMBLY

STATION	OFFSET	DESCRIPTION	EACH
URBAN			
IL 47 149+33.66	3.53' LT	EXISTING VAULT	1.0
IL 47 URBAN TOTAL =			1.0
RURAL			
233+78.15	2.08' LT	EXISTING VAULT	1.0
313+33.99	2.94' RT	EXISTING VAULT	1.0
326+61.09	4.15' RT	EXISTING VAULT	1.0
340+55.24	4.29' RT	EXISTING VAULT	1.0
406+39.92	14.22' RT	EXISTING VAULT	1.0
453+93.71	3.76' RT	EXISTING VAULT	1.0
480+58.36	4.46' LT	EXISTING VAULT	1.0
IL 47 RURAL TOTAL =			7.0
PROJECT TOTAL =			8.0

Z0073002 TEMPORARY SOIL RETENTION SYSTEM

STATION	STATION	HEIGH	SQ FT
IL 47 RURAL			
RT 180+77.00	RT 181+30.00	10.0	530.0
RT 181+12.00	RT 181+42.00	10.0	300.0
LT 181+28.00	LT 181+81.00	10.0	530.0
IL 47 RURAL TOTAL =			1360.0
PROJECT TOTAL =			1360.0

Z0070202 SURVEY MARKER VAULTS

STATION	OFFSET	EACH
URBAN		
IL 47 180+66.92	3.11' LT	1.0
IL 47 URBAN TOTAL =		1.0
RURAL		
260+29.17	12.17' LT	1.0
286+81.06	22.25' LT	1.0
427+39.11	8.78' RT	1.0
507+34.39	52.29' RT	1.0
507+56.50	70.60' LT	1.0
IL 47 RURAL TOTAL =		5.0
PROJECT TOTAL =		6.0

A2001016 TREE, ACER RUBRUM (RED MAPLE), 2" CALIPER, BALLED AND BURLAPPED

IL 47	STATION	TO	STATION	PARCEL NO.	EACH
	LT 273+67.34		LT 279+51.78	5770019	<u>3.0</u>
					PROJECT TOTAL = 3.0

A2001724 TREE, ACER SACCHARUM (SUGAR MAPLE), 3" CALIPER, BALLED AND BURLAPPED

IL 47	STATION	TO	STATION	PARCEL NO.	EACH
	LT 324+39.89		LT 326+63.53	5770026	<u>1.0</u>
					PROJECT TOTAL = 1.0

A2006517 TREE, QUERCUS ACUTISSIMA (SAWTOOTH OAK), 2" CALIPER, BALLED AND BURLAPPED

IL 47	STATION	TO	STATION	PARCEL NO.	EACH
	LT 273+67.34		LT 279+51.78	5770019	<u>3.0</u>
					PROJECT TOTAL = 3.0

A2015990 TREE, FAGUS SYLVATICA, EUROPEAN BEECH, 1-1/2" BALLED AND BURLAPPED

IL 47	STATION	TO	STATION	PARCEL NO.	EACH
	RT 140+21.60		RT 143+29.50	5770006	<u>2.0</u>
					PROJECT TOTAL = 2.0

D2002272 EVERGREEN, PICEA PUNGENS GLAUCA (COLORADO BLUE SPRUCE), 6' HEIGHT, BALLED AND BURLAPPED

IL 47	STATION	TO	STATION	PARCEL NO.	EACH
	RT 313+65.12		RT 316+00.00	5770023	<u>30.0</u>
					PROJECT TOTAL = 30.0

D2002972 EVERGREEN, PINUS STROBUS (EASTERN WHITE PINE), 6' HEIGHT, BALLED AND BURLAPPED

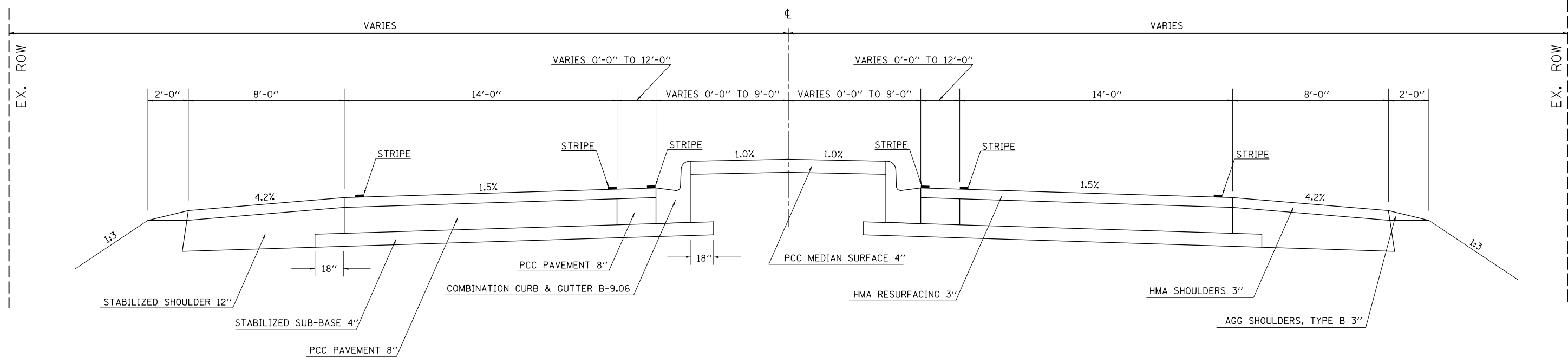
IL 47	STATION	TO	STATION	PARCEL NO.	EACH
	LT 273+67.34		LT 279+51.78	5770019	<u>20.0</u>
					PROJECT TOTAL = 20.0

TREE, ACER SACCHARUM BAILSTA (FALL FIESTA MAPLE), 3" CALIPER, BALLED AND BURLAPPED

IL 47	STATION	TO	STATION	PARCEL NO.	EACH
	LT 324+39.89		LT 326+63.53	5770026	<u>1.0</u>
					PROJECT TOTAL = 1.0

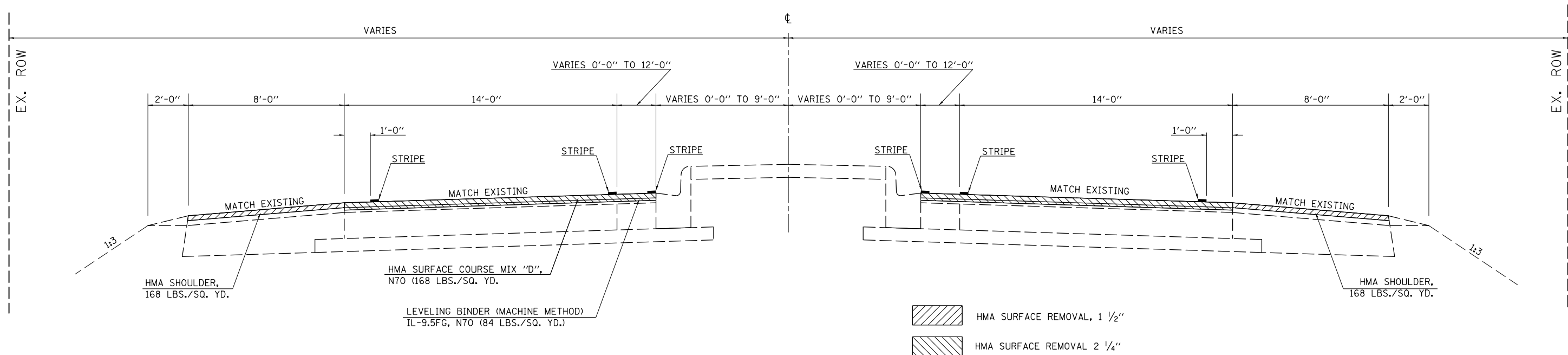
1 EXISTING TYPICAL CROSS SECTION

STATION TO STATION
 23+82.80 TO 38+72.76 (BK)
 97+14.44 (AH) TO 99+61.80 ②



1 PROPOSED TYPICAL CROSS SECTION

STATION TO STATION
 23+82.80 TO 38+72.76 (BK)
 97+14.44 (AH) TO 99+61.80 ②

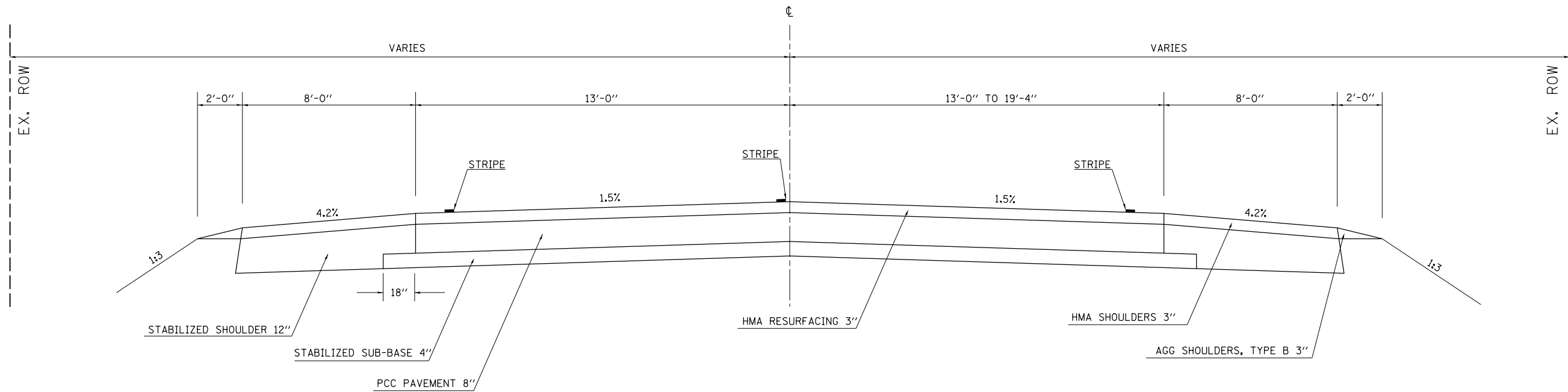


HMA SURFACE REMOVAL, 1 1/2"
 HMA SURFACE REMOVAL 2 1/4"

FILE NAME =	USER NAME = ceerlockbm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL CROSS SECTIONS IL 47			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ce:\pwork\work\pwork\ceerlockbm\d0187612\0870844-sht-typical.dgn		DRAWN -	REVISED -		SCALE: N/A	SHEET NO. 01 OF 07 SHEETS	STA. ---- TO STA. ----	326	136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	81
		CHECKED -	REVISED -					CONTRACT NO. 70844				
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

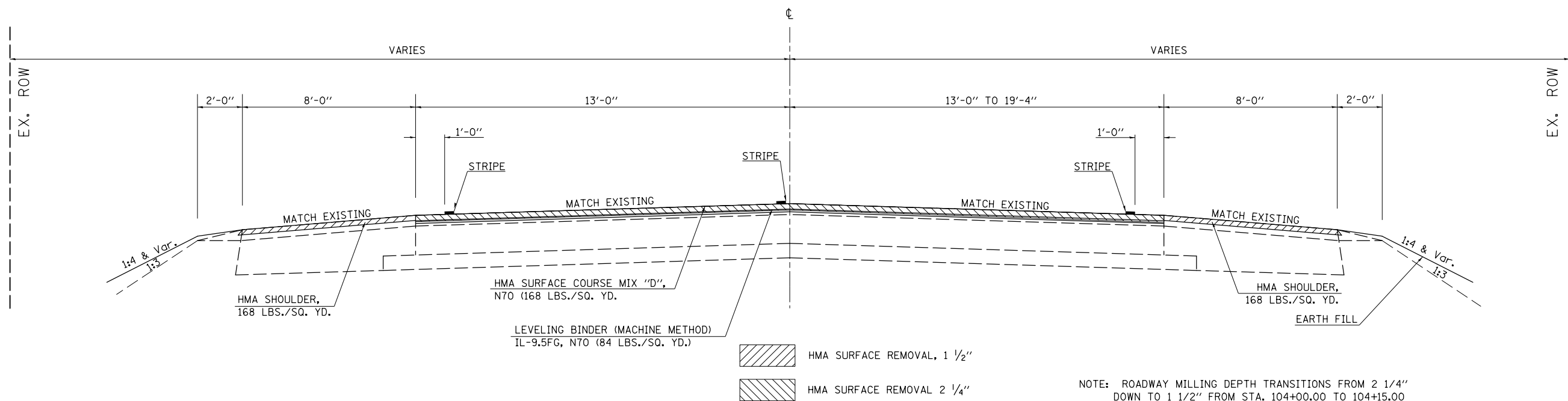
② EXISTING TYPICAL CROSS SECTION

STATION TO STATION
 ① 99+61.80 104+06.45 ③



② PROPOSED TYPICAL CROSS SECTION

STATION TO STATION
 ① 99+61.80 104+06.45 ③



NOTE: ROADWAY MILLING DEPTH TRANSITIONS FROM 2 1/4" DOWN TO 1 1/2" FROM STA. 104+00.00 TO 104+15.00

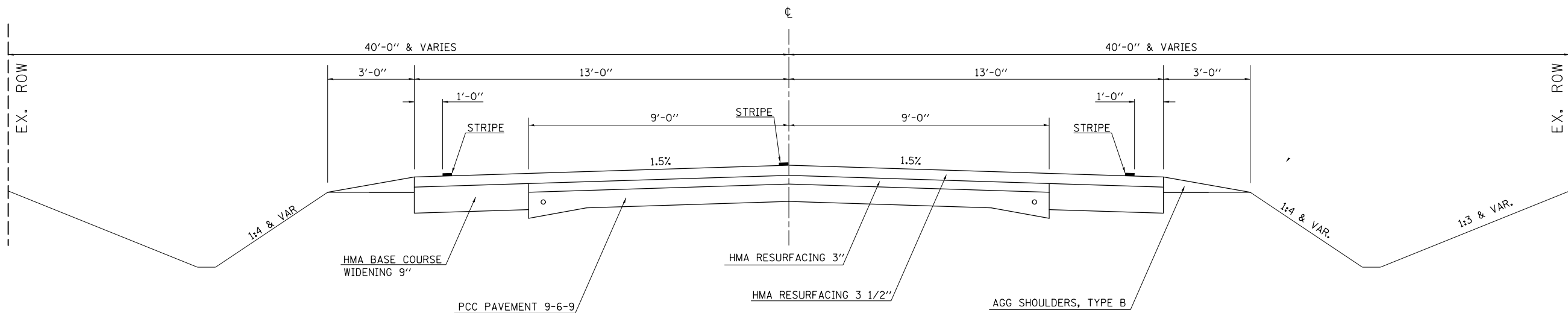
FILE NAME =	USER NAME = ceorlockbm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL CROSS SECTIONS IL 47	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\pwidot\ceorlockbm\d0187612\0870844-sht-typical.dgn		DRAWN -	REVISED -			326	136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	82
PLOT SCALE = 40.0000' / in.		CHECKED -	REVISED -			CONTRACT NO. 70844				
PLOT DATE = 10/10/2014		DATE -	REVISED -		SCALE: N/A	SHEET NO. 02 OF 07 SHEETS		STA. ---- TO STA. ----		ILLINOIS FED. AID PROJECT

3 EXISTING TYPICAL CROSS SECTION

STATION	TO	STATION
② 104+06.45		326+22.50 ④
④ 332+58.03		339+55.06 ④
④ 342+55.07		364+75.87 ⑤
⑤ 379+03.16		393+66.36 ⑤
⑤ 402+28.92		497+00.00 ⑥

STATION EQUATION:
STA. 406+69.20 (BK) = STA. 401+00.92 (AH)

BRIDGE OMISSION:
SN 010-0239
STA. 335+34.70 TO STA. 336+42.50



3 PROPOSED TYPICAL CROSS SECTION

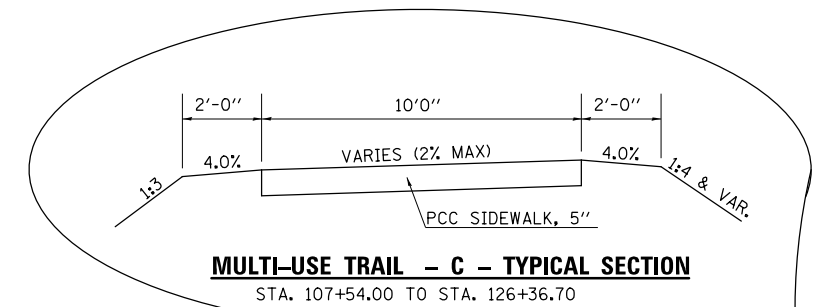
STATION	TO	STATION
② 104+06.45		326+22.50 ④
④ 332+58.03		340+15.16 ④
④ 342+55.07		364+75.87 ⑤
⑤ 379+03.16		393+66.36 ⑤
⑤ 402+28.92		497+00.00 ⑥

STATION EQUATION:
STA. 406+69.20 (BK) = STA. 401+00.92 (AH)

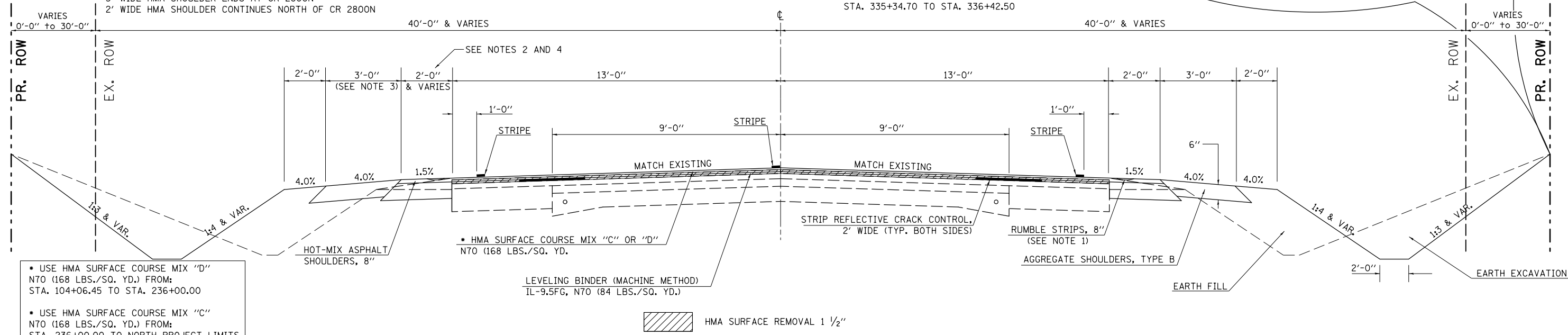
BRIDGE OMISSION:
SN 010-0239
STA. 335+34.70 TO STA. 336+42.50

NOTES:

- RUMBLE STRIPS BEG. AT STA. 104+00.00 AND END AT STA. 504+67.00. RUMBLE STRIPS ARE TO BE INSTALLED ON BOTH SIDES OF ROADWAY.
- 8' WIDE HMA SHOULDER SHALL BE CONSTRUCTED ALONG THE LEFT SIDE OF IL 47 UP TO LT. STA. 108+00.00.
- 3'-0" AGGREGATE SHOULDERS, TYPE B, 6" ARE NOT REQUIRED FROM LT. STA. 104+06.45 TO LT. STA. 108+00.00.
- HMA SHOULDER WIDTH TAPERS FROM 2'-0" TO 3'-0" FROM:
STA. 364+00.00 TO STA. 365+00.00
& STA. 391+00.00 TO STA. 392+00.00
HMA SHOULDER WIDTH TAPERS FROM 3'-0" TO 2'-0" FROM:
STA. 381+00.00 TO STA. 382+00.00
3' WIDE HMA SHOULDER ENDS AT CR 2800N
2' WIDE HMA SHOULDER CONTINUES NORTH OF CR 2800N



MULTI-USE TRAIL - C - TYPICAL SECTION
STA. 107+54.00 TO STA. 126+36.70

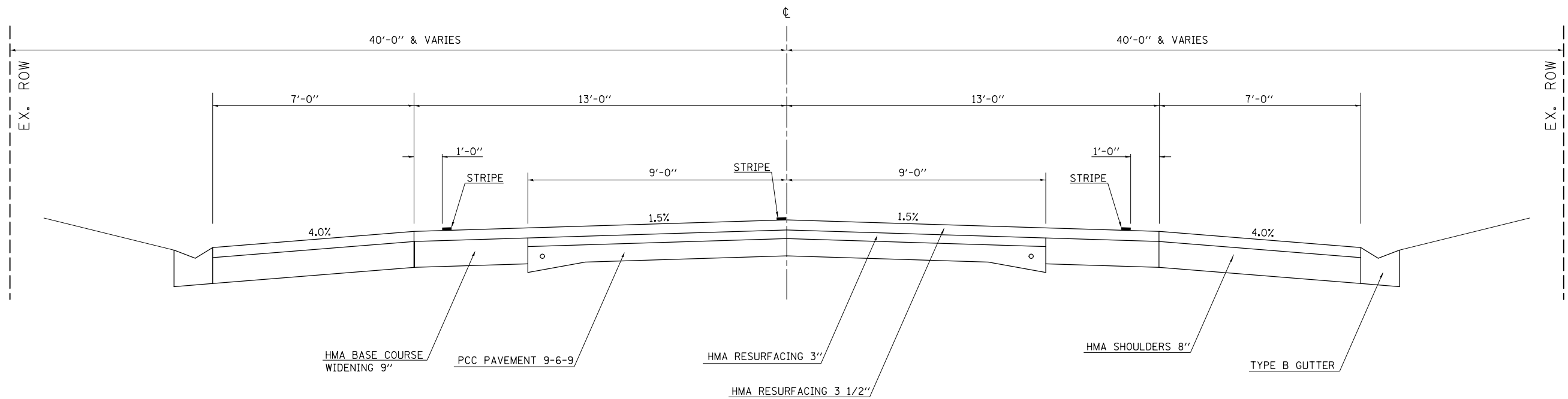


- USE HMA SURFACE COURSE MIX "D" N70 (168 LBS./SQ. YD.) FROM: STA. 104+06.45 TO STA. 236+00.00
- USE HMA SURFACE COURSE MIX "C" N70 (168 LBS./SQ. YD.) FROM: STA. 236+00.00 TO NORTH PROJECT LIMITS

FILE NAME =	USER NAME = cealockbm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL CROSS SECTIONS IL 47			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\dot\cealockbm\d0187612\0870844-sht-typical.dgn		DRAWN -	REVISED -		SCALE: N/A	SHEET NO. 03 OF 07 SHEETS	STA. ---- TO STA. ----	326	136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	83
		CHECKED -	REVISED -					CONTRACT NO. 70844				
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

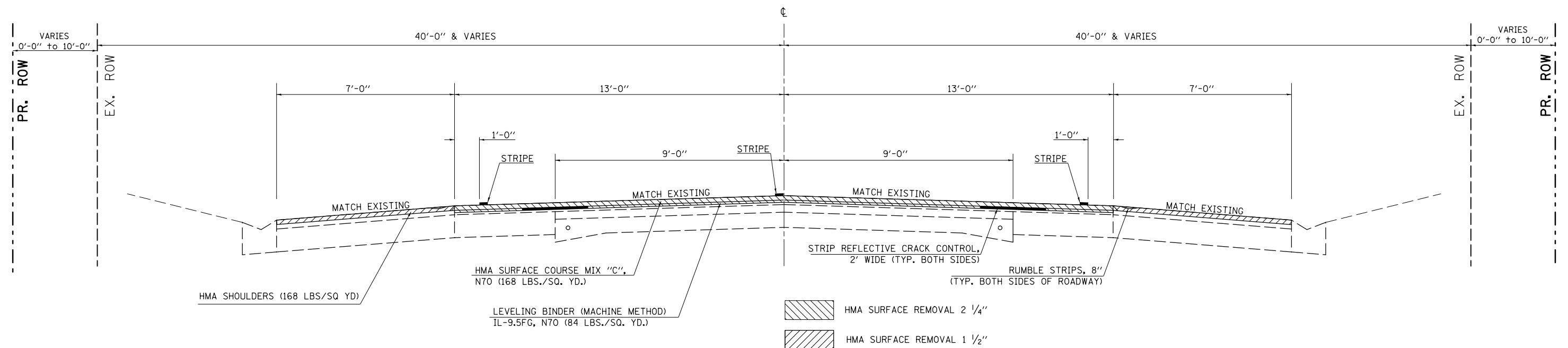
④ EXISTING TYPICAL CROSS SECTION

STATION	TO	STATION
③ 326+22.50		332+58.03③
④ 339+55.06		342+55.07③



④ PROPOSED TYPICAL CROSS SECTION

STATION	TO	STATION
③ 326+22.50		332+58.03③
③ 340+15.16		342+55.07③



FILE NAME =	USER NAME = ceerlockbm	DESIGNED -	REVISED -
ei:\pwwork\pwwid\ceerlockbm\d0187612\0870844-sht-typical.dgn		DRAWN -	REVISED -
PLOT SCALE = 40.0000' / in.		CHECKED -	REVISED -
PLOT DATE = 10/10/2014		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL CROSS SECTIONS
IL 47

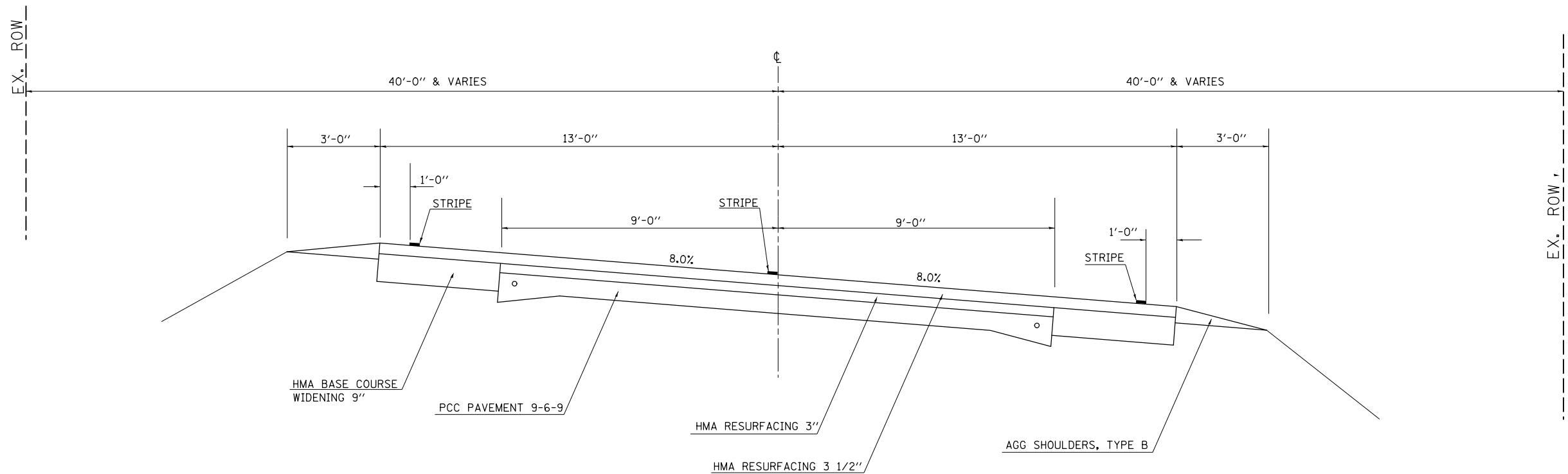
SCALE: N/A SHEET NO. 04 OF 07 SHEETS STA. ---- TO STA. ----

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	84
				CONTRACT NO. 70844

ILLINOIS FED. AID PROJECT

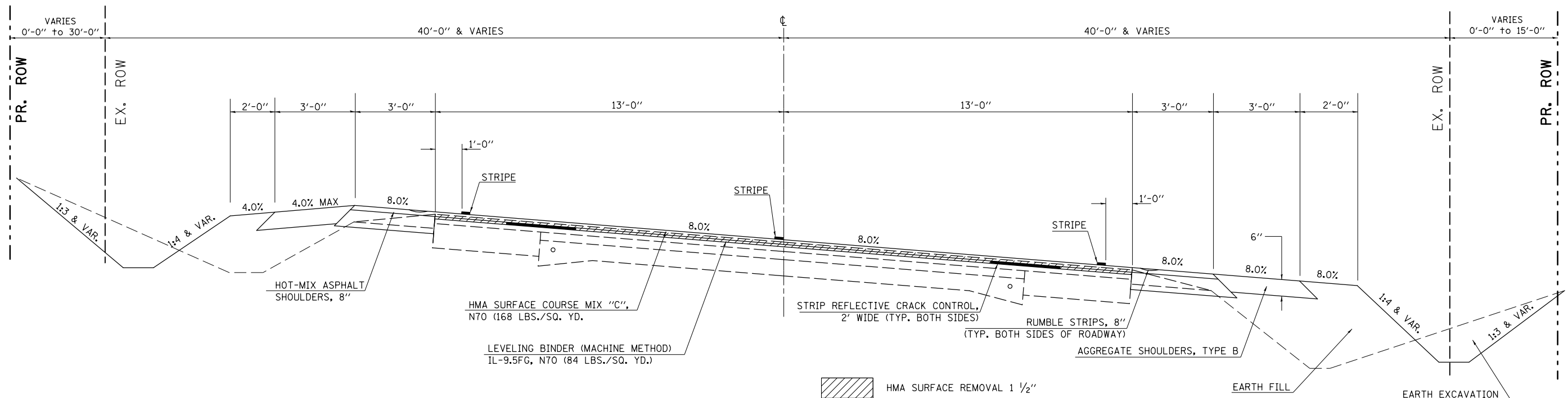
5 EXISTING TYPICAL CROSS SECTION

STATION TO STATION
 ③ 364+75.87 379+03.16 ③
 ③ 393+66.36 402+28.92 ③



5 PROPOSED TYPICAL CROSS SECTION

STATION TO STATION
 ③ 364+75.87 379+03.16 ③
 ③ 393+66.36 402+28.92 ③



FILE NAME =	USER NAME = ceerlockbm	DESIGNED -	REVISED -
et:\pw\work\p\dot\ceerlockbm\d0187612\0870844-sht-typical.dgn		DRAWN -	REVISED -
PLOT SCALE = 40.0000' / in.		CHECKED -	REVISED -
PLOT DATE = 10/10/2014		DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL CROSS SECTIONS
 IL 47

SCALE: N/A SHEET NO. 05 OF 07 SHEETS STA. ---- TO STA. ----

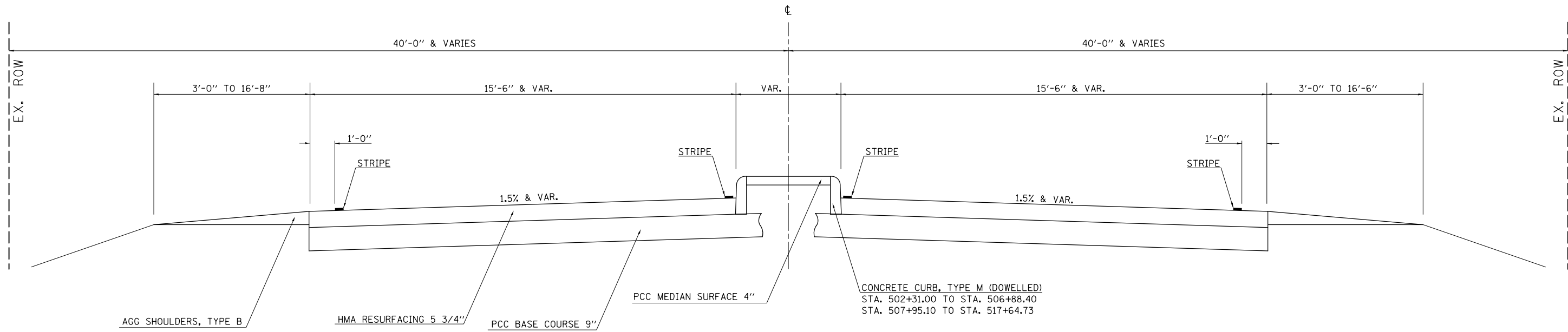
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	85
CONTRACT NO. 70844				
ILLINOIS FED. AID PROJECT				

⑥ EXISTING TYPICAL CROSS SECTION

STATION EQUATION:
STA. 507+35.68 (BK) = STA. 507+49.60 (AH)

STATION TO STATION
③ 497+00.00 TO 509+14.61 ⑦

NOTE:
NO MEDIAN SURFACE OR CURB FROM:
STA. 497+00.00 TO STA. 500+82.56
STA. 506+88.00 TO STA. 507+81.00

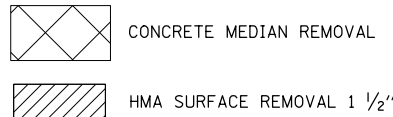
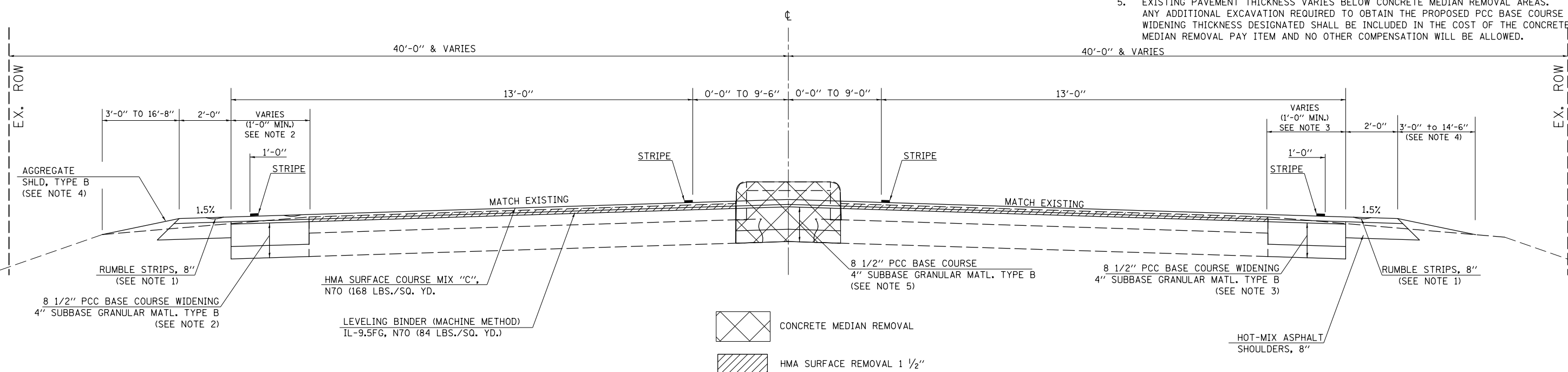


⑥ PROPOSED TYPICAL CROSS SECTION

STATION EQUATION:
STA. 507+35.68 (BK) = STA. 507+49.60 (AH)

STATION TO STATION
③ 497+00.00 TO 509+14.61 ⑦

- NOTES:
- RUMBLE STRIPS END AT STA. 504+67.00. RUMBLE STRIPS ARE TO BE INSTALLED ON BOTH SIDES OF ROADWAY.
 - PCC BASE COURSE WIDENING FROM LT. STA. 497+02.00 TO LT. STA. 504+37.90. PCC BASE COURSE WIDENING ALSO REQUIRED ALONG NEW COMBINATION CURB & GUTTER IN SW AND NW QUADRANTS OF INTERSECTION OF US 136 AND IL 47.
 - PCC BASE COURSE WIDENING FROM RT. STA. 497+02.00 TO RT. STA. 504+63.50. PCC BASE COURSE WIDENING ALSO REQUIRED ALONG NEW COMBINATION CURB & GUTTER LOCATED IN SE AND NE QUADRANTS OF INTERSECTION OF US 136 AND IL 47.
 - AGGREGATE SHOULDER, TYPE B BEGIN AT LT. STA. 499+91.00 & RT. STA. 500+15.00. THE AREA FROM STA. 497+00.00 TO LT. STA. 499+91.00 & RT. STA. 500+15.00 SHALL BE AGGREGATE SHOULDERS, TYPE B, 6".
 - EXISTING PAVEMENT THICKNESS VARIES BELOW CONCRETE MEDIAN REMOVAL AREAS. ANY ADDITIONAL EXCAVATION REQUIRED TO OBTAIN THE PROPOSED PCC BASE COURSE WIDENING THICKNESS DESIGNATED SHALL BE INCLUDED IN THE COST OF THE CONCRETE MEDIAN REMOVAL PAY ITEM AND NO OTHER COMPENSATION WILL BE ALLOWED.



FILE NAME =	USER NAME = ceerlockbm	DESIGNED -	REVISED -
ce:\pwwork\pwwork\ceerlockbm\d0187612\0870844-sht-typical.dgn		DRAWN -	REVISED -
PLOT SCALE = 40.0000' / in.		CHECKED -	REVISED -
PLOT DATE = 10/10/2014		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL CROSS SECTIONS
IL 47

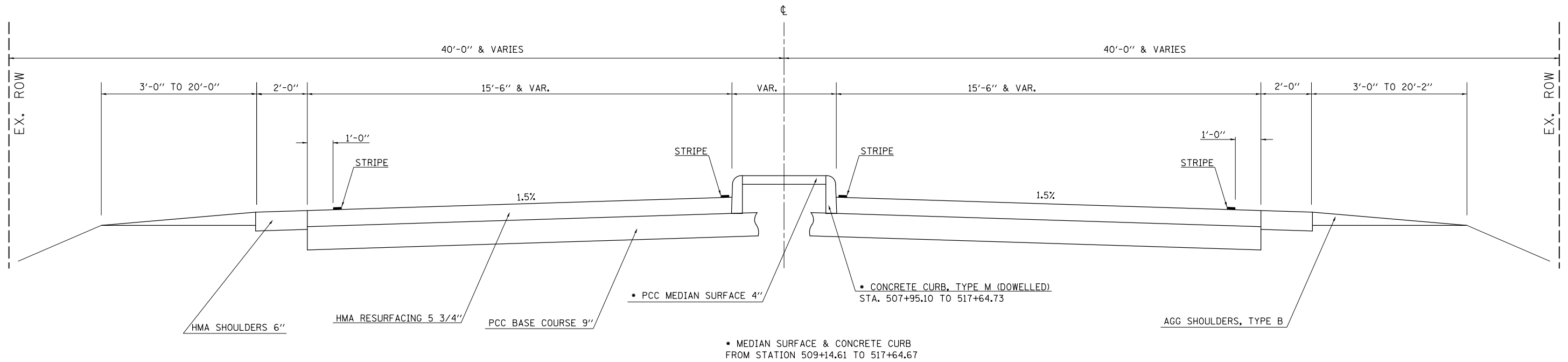
SCALE: N/A SHEET NO. 06 OF 07 SHEETS STA. ---- TO STA. ----

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	86
CONTRACT NO. 70844				

ILLINOIS FED. AID PROJECT

7 EXISTING TYPICAL CROSS SECTION

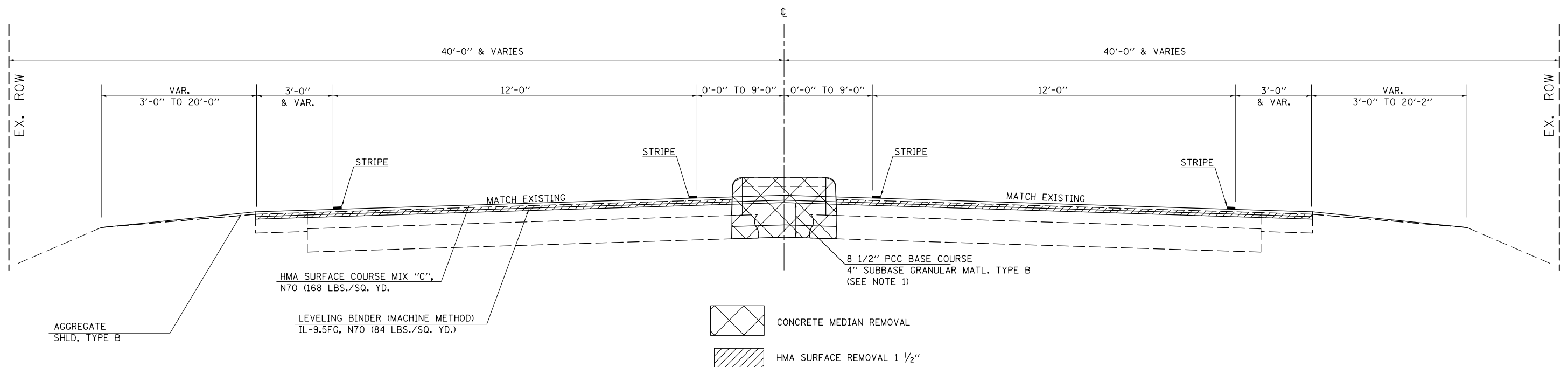
STATION TO STATION
 ⑥ 509+14.61 523+18.70



7 PROPOSED TYPICAL CROSS SECTION

STATION TO STATION
 ⑥ 509+14.61 523+18.70

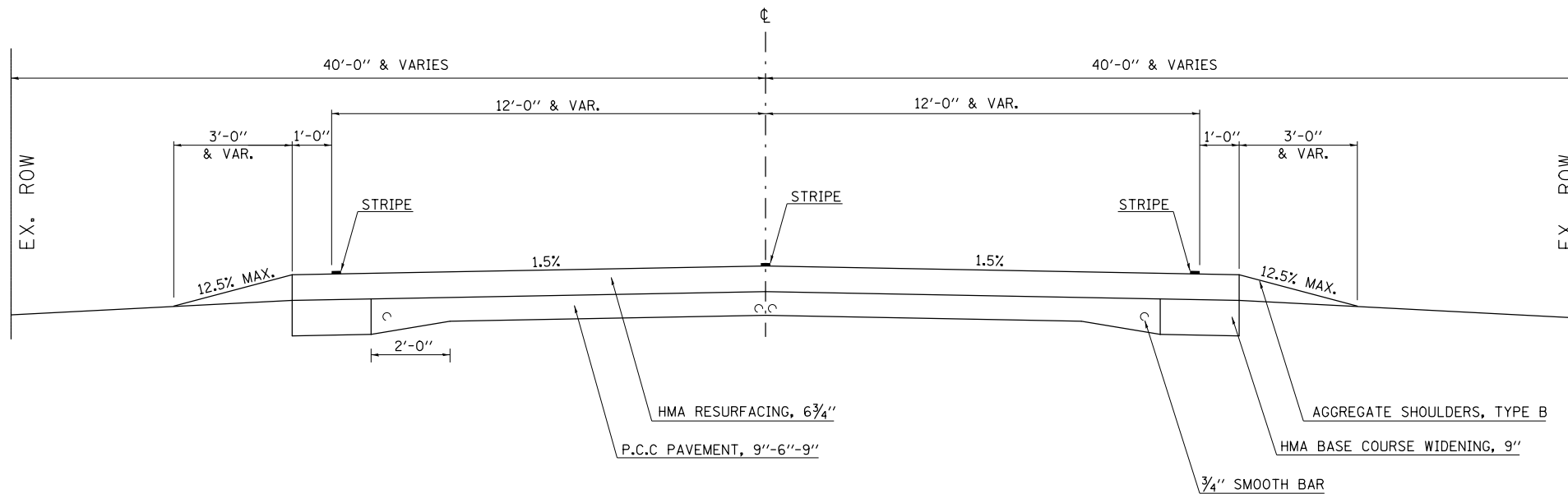
- NOTES:
- EXISTING PAVEMENT THICKNESS VARIES BELOW CONCRETE MEDIAN REMOVAL AREAS. ANY ADDITIONAL EXCAVATION REQUIRED TO OBTAIN THE PROPOSED PCC BASE COURSE WIDENING THICKNESS DESIGNATED SHALL BE INCLUDED IN THE COST OF THE CONCRETE MEDIAN REMOVAL PAY ITEM AND NO OTHER COMPENSATION WILL BE ALLOWED.



FILE NAME =	USER NAME = ceerlockbm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL CROSS SECTIONS IL 47			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ce:\pwork\pwork\pwork\ceerlockbm\d0187612\0870844-sht-typical.dgn		DRAWN -	REVISED -		SCALE: N/A	SHEET NO. 07 OF 07 SHEETS	STA. ---- TO STA. ----	326	136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	87
	PLOT SCALE = 48.0000' / in.	CHECKED -	REVISED -					CONTRACT NO. 70844				
	PLOT DATE = 10/10/2014	DATE -	REVISED -					ILLINOIS FED. AID PROJECT				

(A) EXISTING TYPICAL CROSS SECTION

STATION	TO	STATION
1650+00.00		1656+25.00 (B)
1670+54.00		1674+00.00

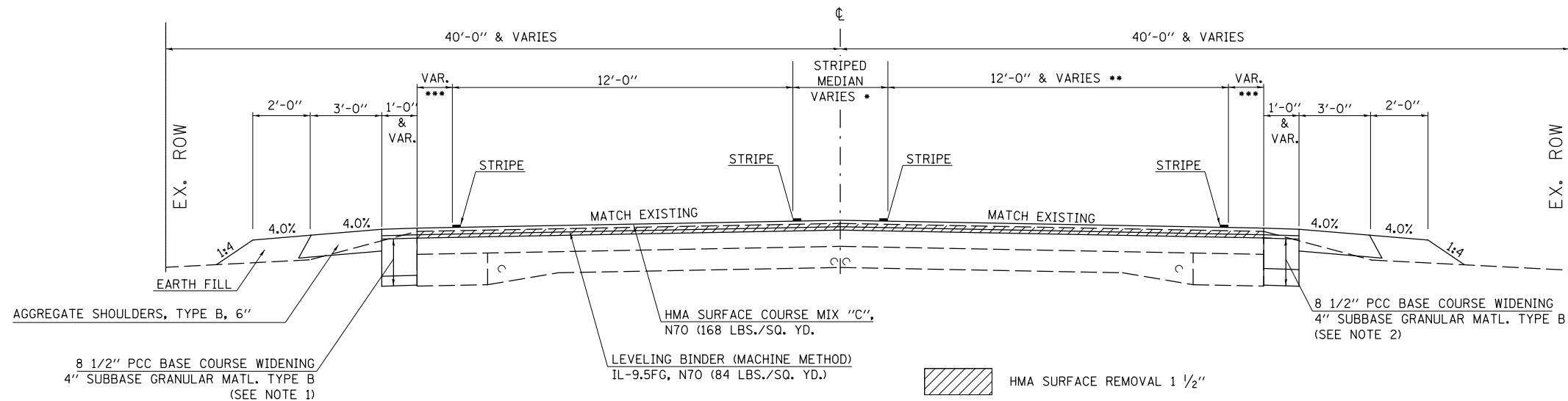


(A) PROPOSED TYPICAL CROSS SECTION

STATION	TO	STATION
1650+00.00		1656+25.00 (B)
1670+54.00		1674+00.00

- STRIPED MEDIAN VARIES 0'-0" UP TO 14'-8"
- ** TURN LANE BEGINS AT 1655+60.35 FOR APPROACH TO US 136
- *** HMA SHOULDER WIDTH VARIES BASED ON EXISTING LIMITS OF HMA ALONG US 136. THE OUTSIDE EDGE OF PROPOSED HMA SHALL FOLLOW THE EXISTING OUTSIDE EDGE OF HMA ALONG US 136 (EXCLUDING AREAS WHERE PCC BASE COURSE WIDENING IS REQUIRED).

- NOTES:
1. PCC BASE COURSE WIDENING FROM LT. STA. 1651+00 TO LT. STA. 1654+07.80
 2. PCC BASE COURSE WIDENING FROM RT. STA. 1651+00 TO RT. STA. 1658+41.83

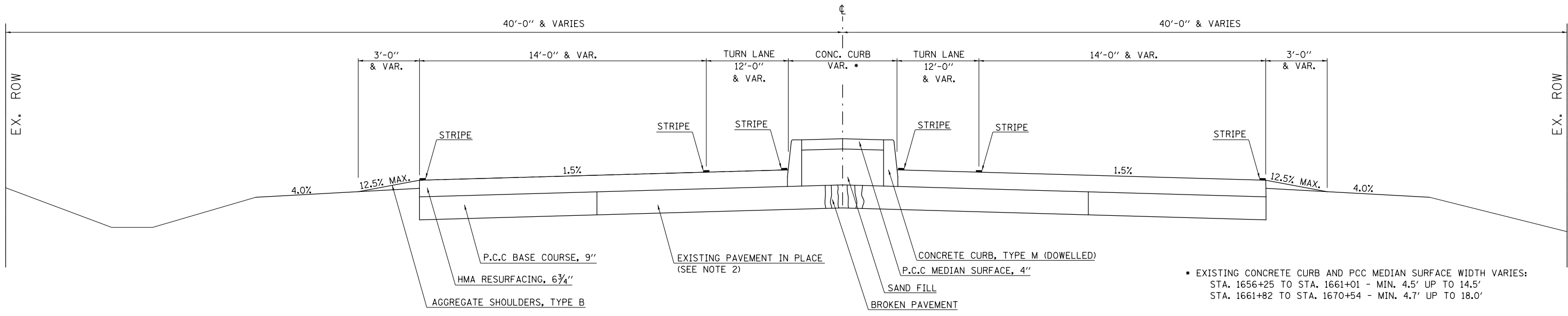


FILE NAME =	USER NAME = ceorlockbm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL CROSS SECTIONS US 136	SCALE: N/A	SHEET NO. 01 OF 02 SHEETS	STA. ---- TO STA. ----	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pwork\work\pwork\ceorlockbm\d0187612\0970844-sht-typical.dgn		DRAWN -	REVISED -						326	136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	88
PLOT SCALE = 40.0000' / in.		CHECKED -	REVISED -						CONTRACT NO. 70844				
PLOT DATE = 10/10/2014		DATE -	REVISED -						ILLINOIS FED. AID PROJECT				

(B) EXISTING TYPICAL CROSS SECTION

NOTE:
STA. 1661+01 TO 1661+82 FALLS WITHIN THE IL 47/US 136 INTERSECTION.
NO TYPICAL SECTION AVAILABLE.

STATION	TO	STATION
1656+25.00		1661+01.00
1661+82.00		1670+54.00 (A)



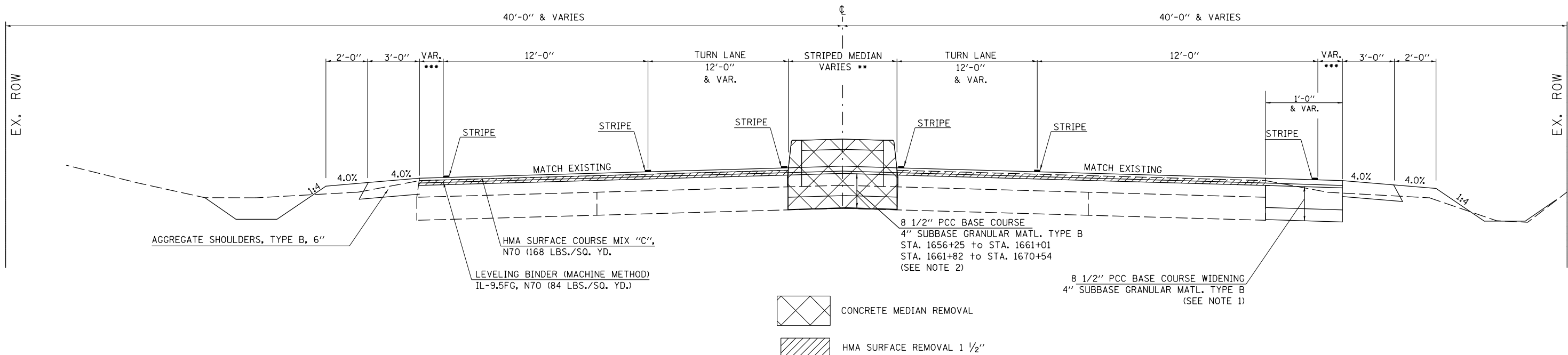
* EXISTING CONCRETE CURB AND PCC MEDIAN SURFACE WIDTH VARIES:
STA. 1656+25 TO STA. 1661+01 - MIN. 4.5' UP TO 14.5'
STA. 1661+82 TO STA. 1670+54 - MIN. 4.7' UP TO 18.0'

(B) PROPOSED TYPICAL CROSS SECTION

- ** STRIPED MEDIAN WIDTH VARIES
STA. 1656+35.00 TO STA. 1660+94.35 - BETWEEN 12'-2 1/2" & 4'-0"
STA. 1661+83.30 TO STA. 1670+45.00 - BETWEEN 4'-0" & 14'-8"
- *** HMA SHOULDER WIDTH VARIES BASED ON EXISTING LIMITS OF HMA ALONG US 136. THE OUTSIDE EDGE OF PROPOSED HMA SHALL FOLLOW THE EXISTING OUTSIDE EDGE OF HMA ALONG US 136 (EXCLUDING AREAS WHERE PCC BASE COURSE WIDENING IS REQUIRED).

STATION	TO	STATION
1656+25.00		1661+01.00
1661+82.00		1670+54.00 (A)

- NOTES:
- PCC BASE COURSE WIDENING FROM RT. STA. 1651+00 TO RT. STA. 1658+41.83
 - EXISTING PAVEMENT THICKNESS VARIES BELOW CONCRETE MEDIAN REMOVAL AREAS. ANY ADDITIONAL EXCAVATION REQUIRED TO OBTAIN THE PROPOSED PCC BASE COURSE WIDENING THICKNESS DESIGNATED SHALL BE INCLUDED IN THE COST OF THE CONCRETE MEDIAN REMOVAL PAY ITEM AND NO OTHER COMPENSATION WILL BE ALLOWED.

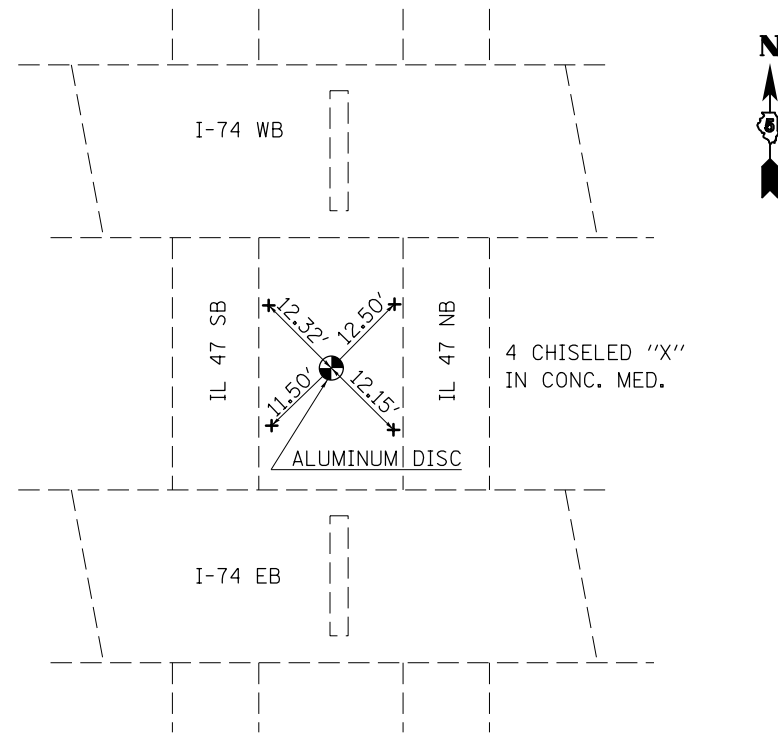


CONCRETE MEDIAN REMOVAL
HMA SURFACE REMOVAL 1 1/2"

FILE NAME =	USER NAME = ceerlockbm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL CROSS SECTIONS US 136	SCALE: N/A	SHEET NO. 02 OF 02 SHEETS	STA. ---- TO STA. ----	F.A.P. RTE. 326	SECTION 136RS-3, CR & (10-4)RS-1	COUNTY CHAMPAIGN	TOTAL SHEETS 474	SHEET NO. 89
et:\pwork\pwork\ceerlockbm\d0187612\0870844-sht-typical.dgn		DRAWN -	REVISED -										
PLOT SCALE = 40.0000' / in.		CHECKED -	REVISED -										
PLOT DATE = 10/10/2014		DATE -	REVISED -										

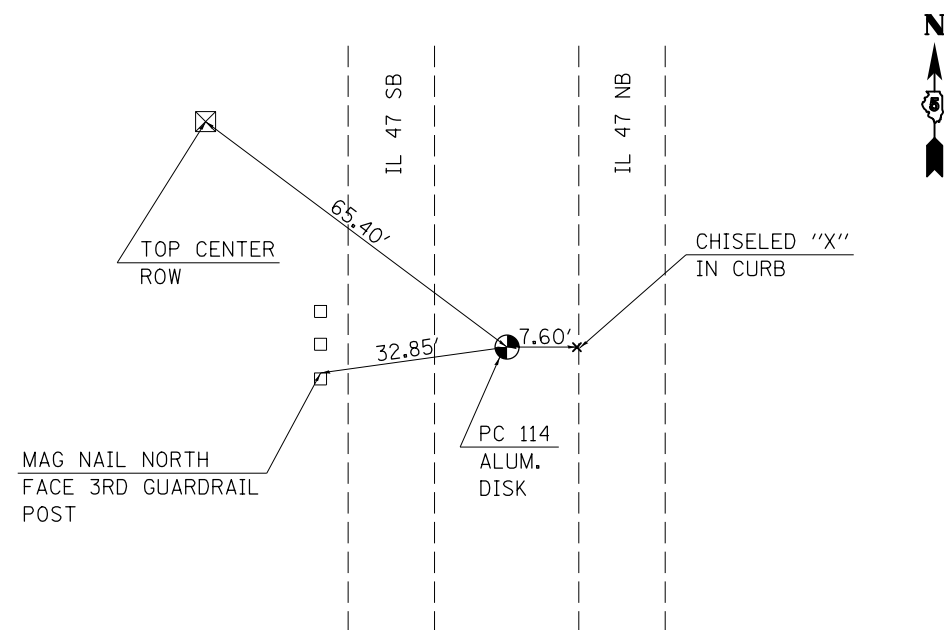
BASIS OF STATIONING

POT #125 30+00



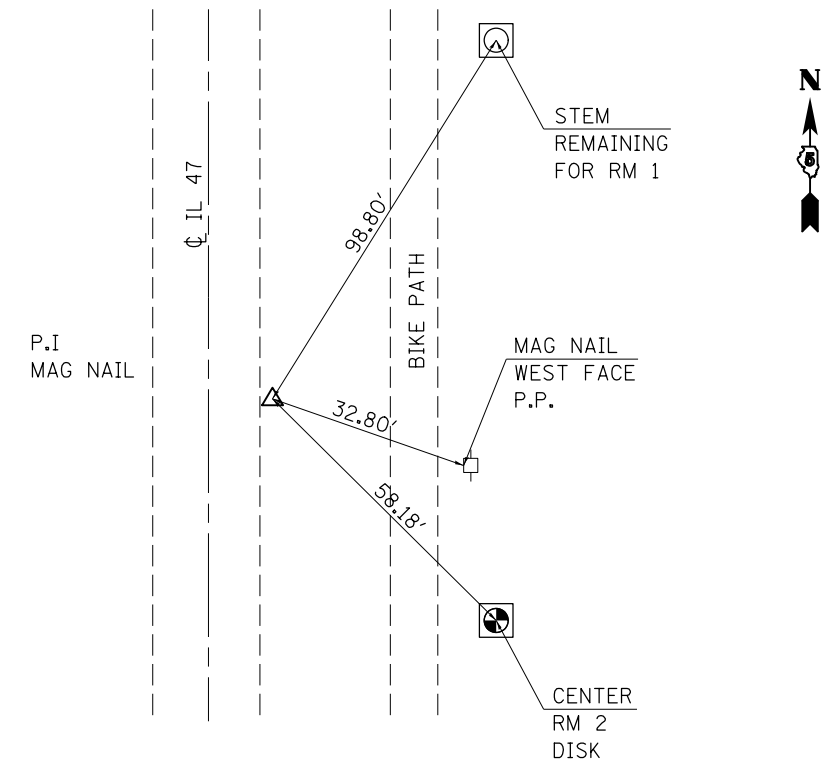
PC #114

38+72.76 BK = 97+14.44 AH



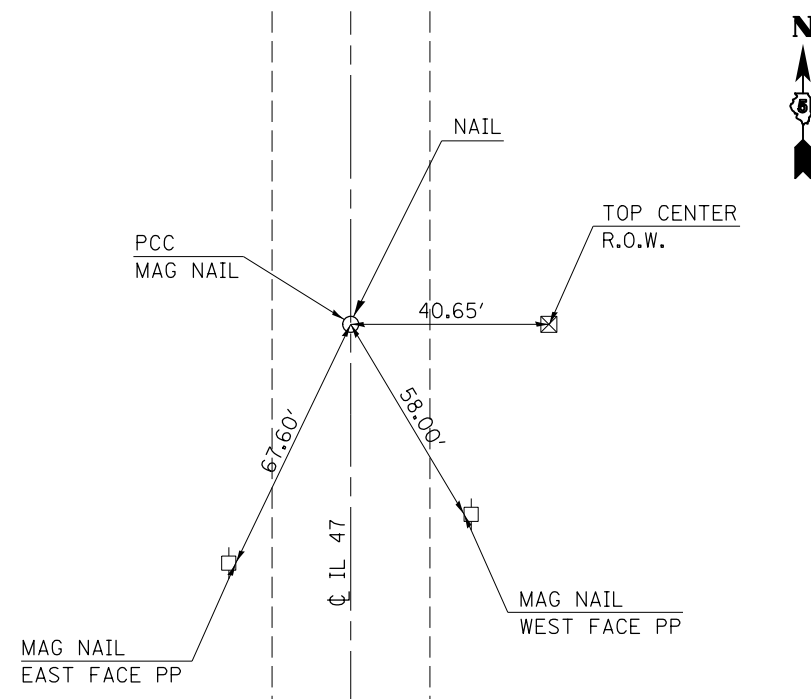
P.I. #133

106+31.61/52.45' RT.



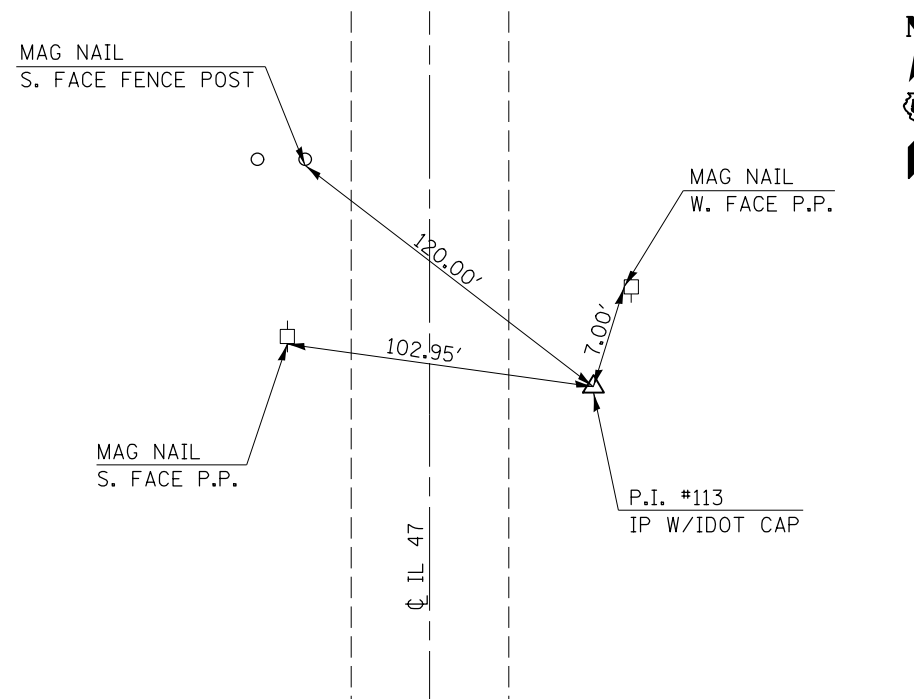
PCC #112

115+40.80



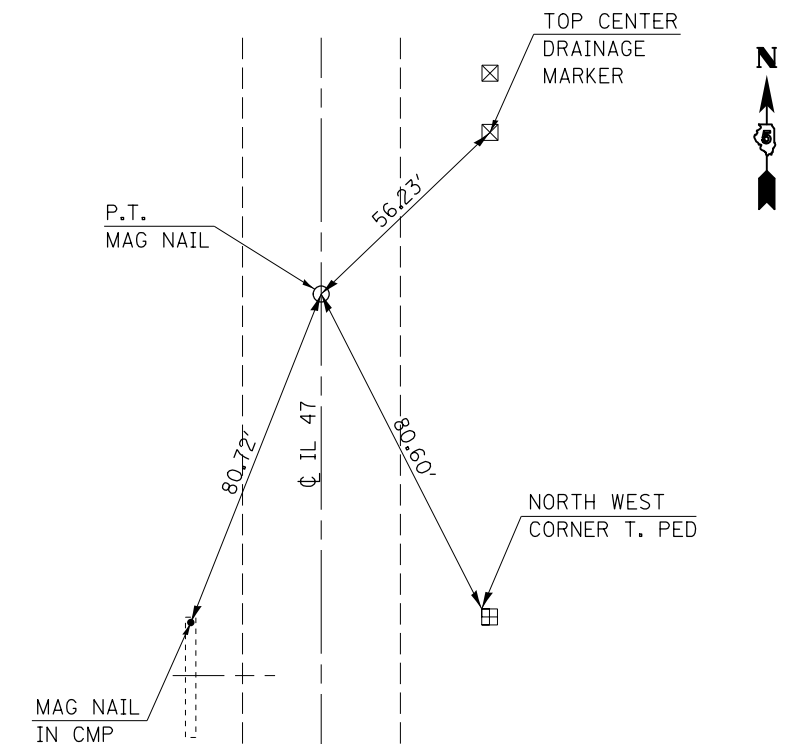
PI #113

129+38.64 /60.54' RT.



P.I. #108

143+29.50



FILE NAME =	USER NAME = ceerlockbm	DESIGNED -	REVISED -
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#MODELNAME#	PLOT DATE = 10/10/2014	DATE -	REVISED -

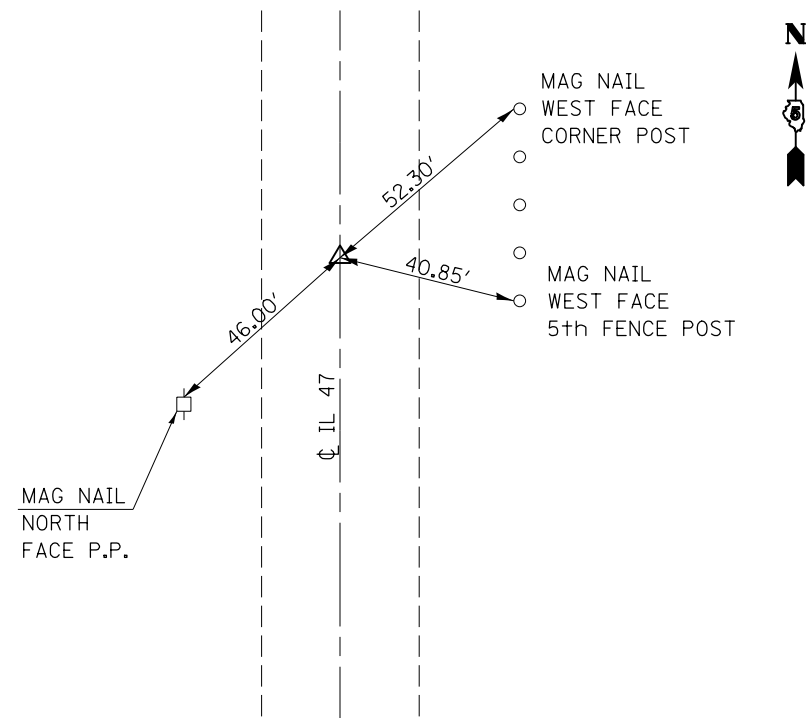
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TIE POINTS	
SCALE: NONE	SHEET 1 OF 3 SHEETS STA. ---- TO STA. ----

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	90
CONTRACT NO. 70844				
ILLINOIS FED. AID PROJECT				

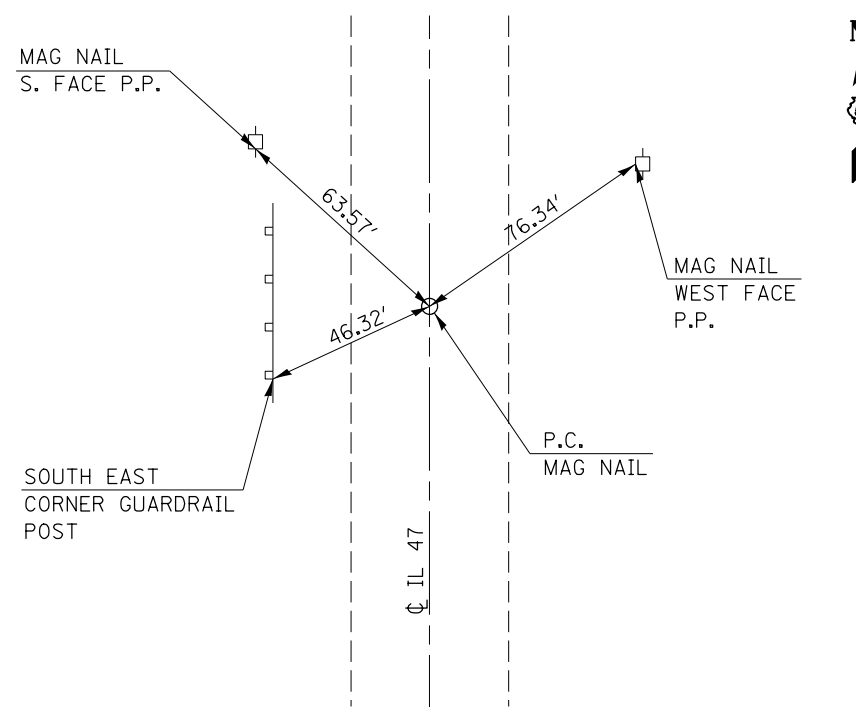
P.I. KINK #9

311 + 42.22



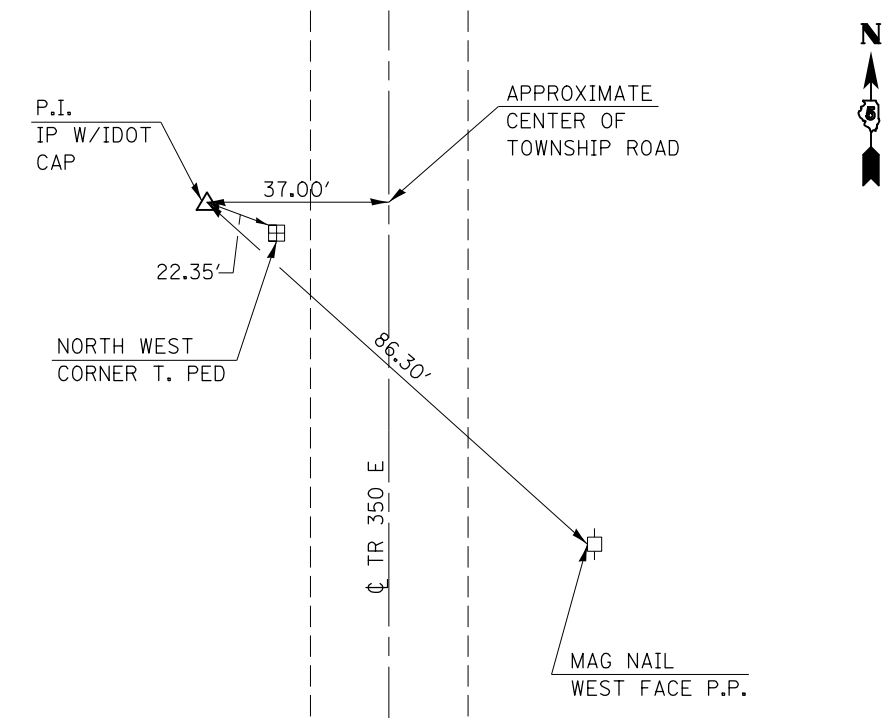
P.C. #65

366 + 05.20



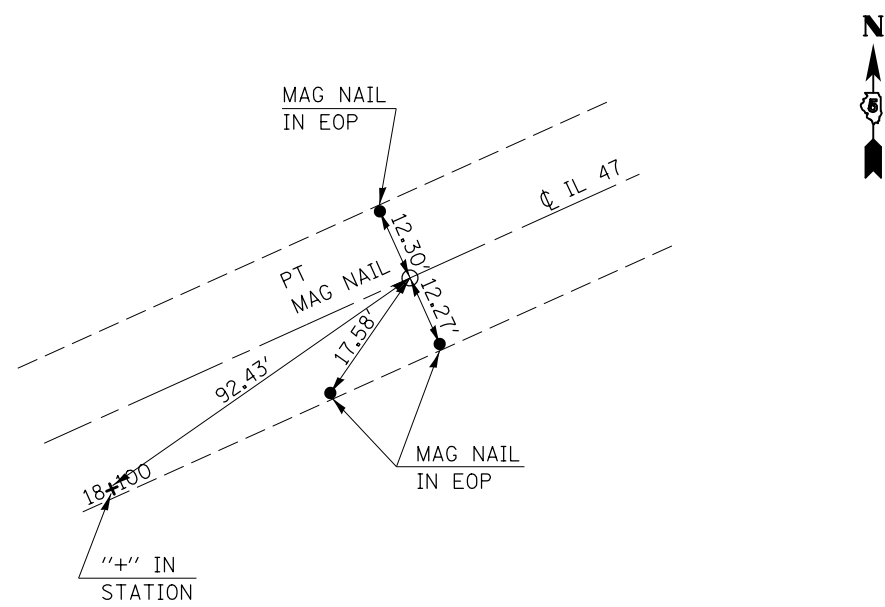
P.I. #51

372 + 58.00 / 184.12' LT.



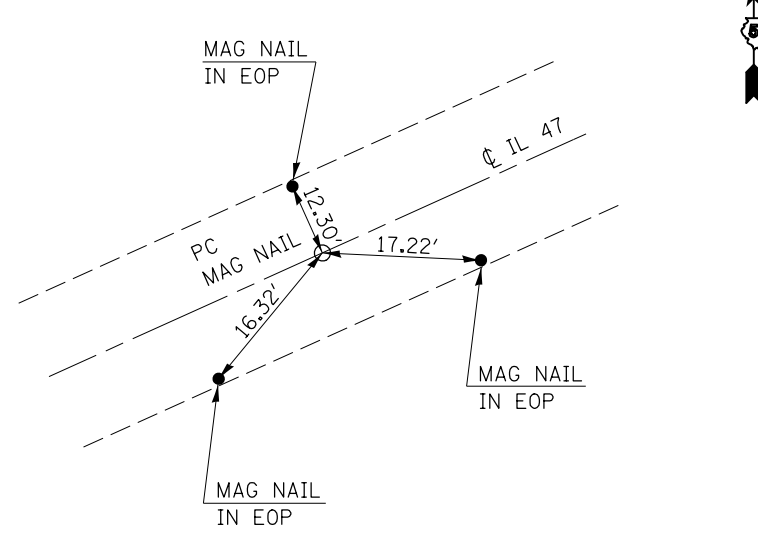
P.T. #66

377 + 76.51



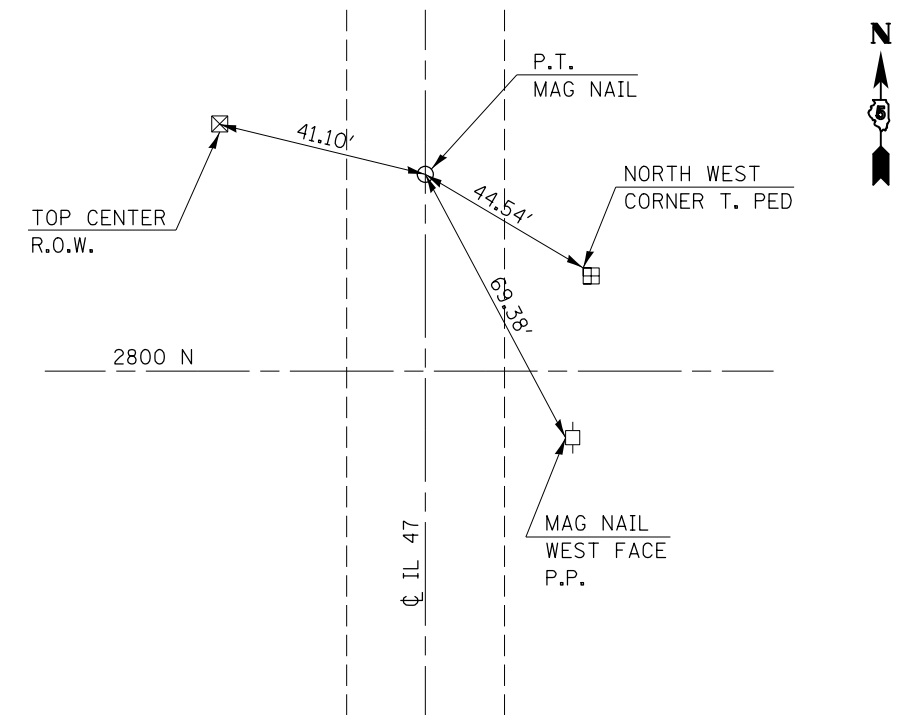
P.C. #67

394 + 96.46



P.T. #57

406 + 69.19 BK = 401 + 00.92 AH



FILE NAME =	USER NAME = ceerlockbm	DESIGNED -	REVISED -
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\$MODELNAME\$	PLOT DATE = 10/10/2014	DATE -	REVISED -

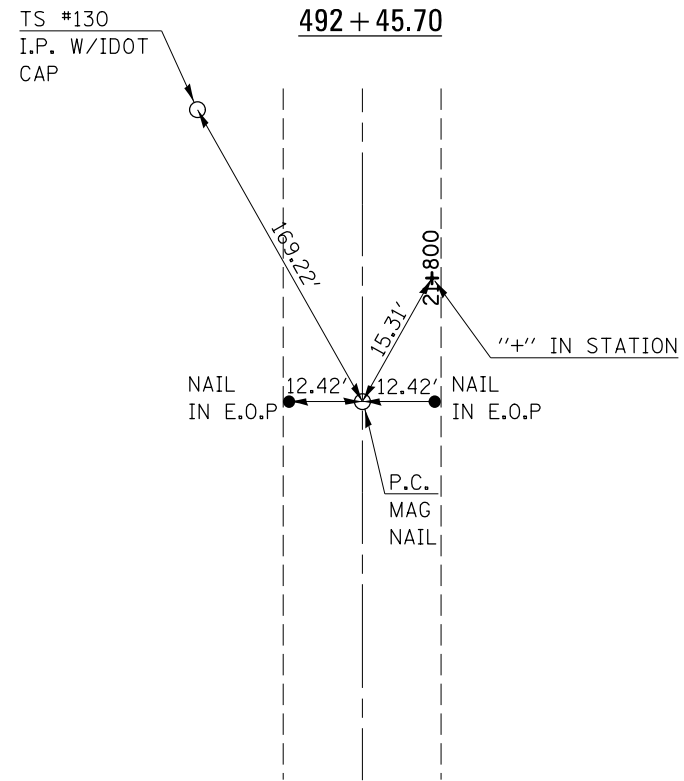
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCALE: NONE	SHEET 2 OF 3 SHEETS	STA. ---- TO STA. ----
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	91
CONTRACT NO. 70844			ILLINOIS FED. AID PROJECT	

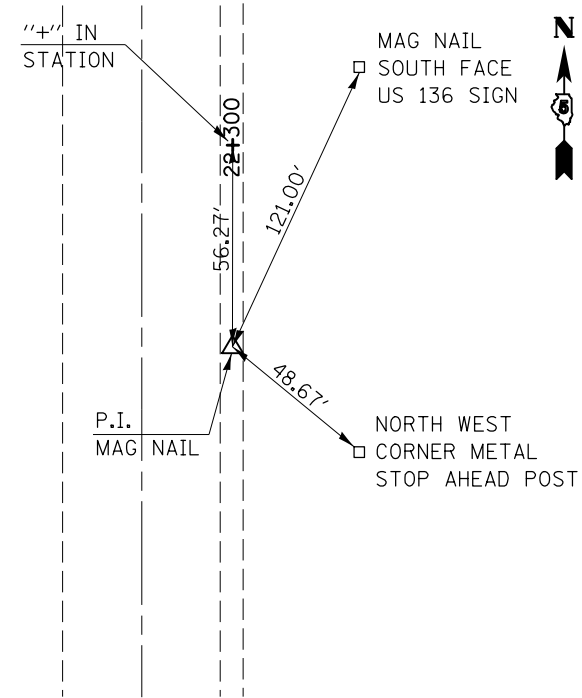
P.C. #63

492 + 45.70



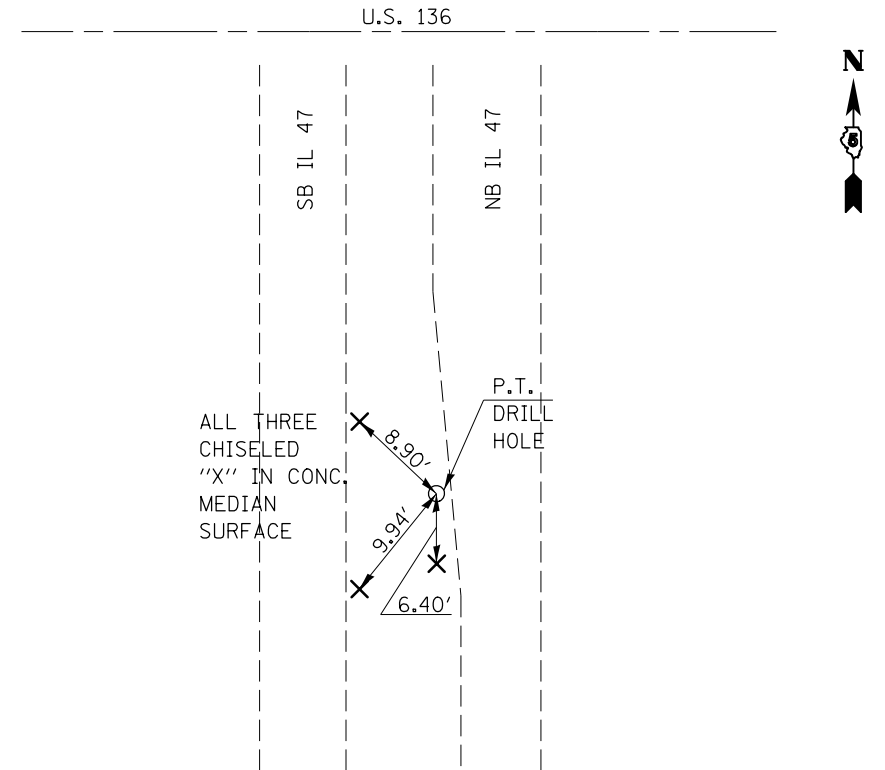
P.I. #58

498 + 51.25 /10.33' RT.

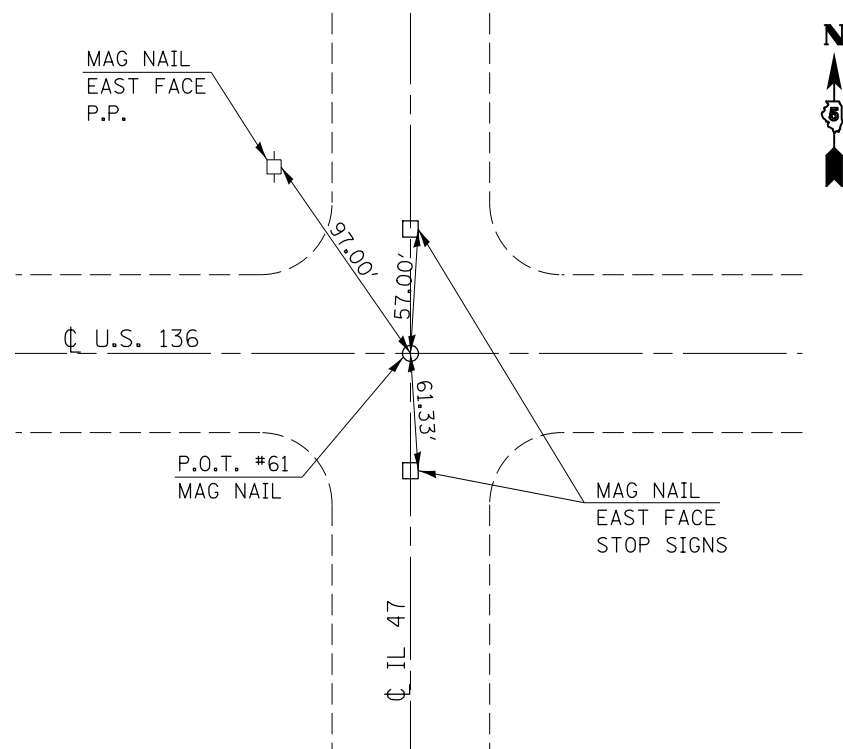


P.T. #73

504 + 56.32

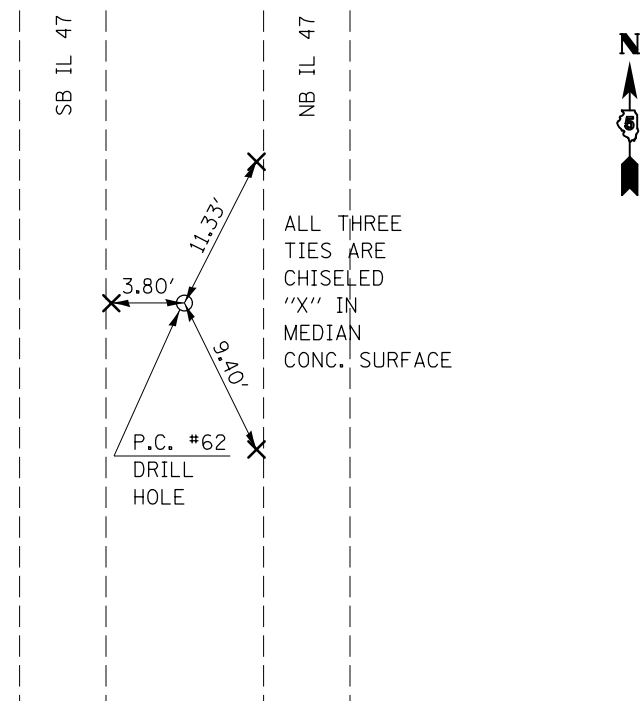


P.O.T. #61



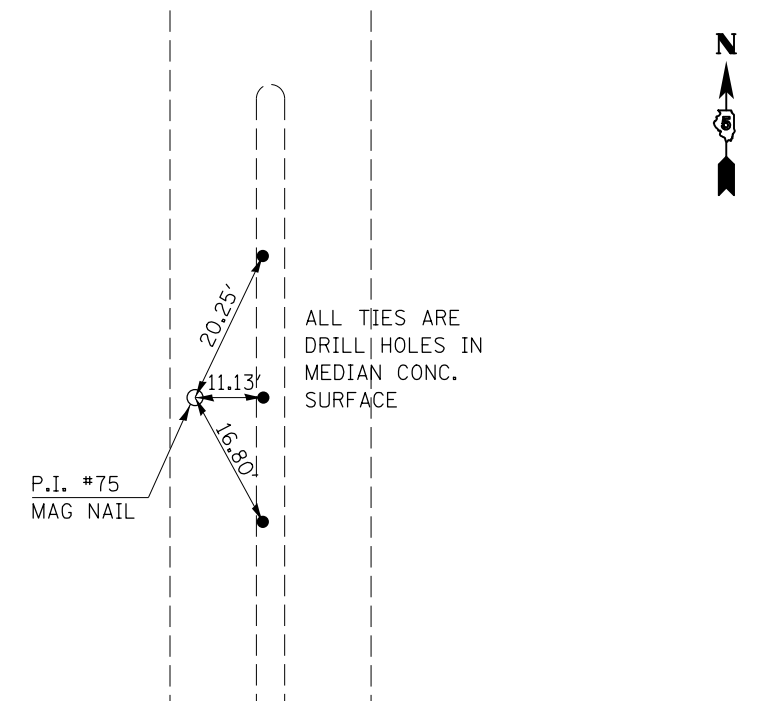
P.C. #62

512 + 87.49



P.I. #75

518 + 24.33 /8.81' LT.



FILE NAME =	USER NAME = cealockbm	DESIGNED -	REVISED -
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	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -
\$MODELNAME\$	PLOT DATE = 10/10/2014	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TIE POINTS			
SCALE: NONE	SHEET 3	OF 3 SHEETS	STA. ---- TO STA. ----

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	92
CONTRACT NO. 70844			ILLINOIS FED. AID PROJECT	

BENCHMARKS

B. M. 4875-3 STA. AND OFFSET NOT STATED
 CHAMPAIGN COUNTY
 CHISLED SQUARE
 TO REACH FROM THE INTERSECTION OF I-74 AND IL 47 GO NORTH ON IL 47 FOR 0.1 MILES TO MARK ON THE RIGHT. SAID MARK IS A CHISLED SQUARE LOCATED ON THE TOP OF THE NW CORNER OF THE STOP SIGN ISLAND AT THE INTERSECTION OF THE OFF RAMP OF I-74 AND IL47.
 ELEVATION = 692.168 (NAVD 88)

B. M. 4875-4 STA. AND OFFSET NOT STATED
 CHAMPAIGN COUNTY
 TOP OF ROW
 TO REACH FROM THE INTERSECTION OF I-74 AND IL 47 GO NORTH ON IL 47 FOR 0.5 MILES TO MARK ON THE RIGHT. SAID MARK IS LOCATED ON TOP OF A ROW MARKER LOCATED JUST WEST OF THE EARLY AMERICAN MUSEUM.
 ELEVATION = 723.466 (NAVD 88)

B. M. 4875-5 STA. AND OFFSET NOT STATED
 CHAMPAIGN COUNTY
 CHISLED SQUARE
 TO REACH FROM THE INTERSECTION OF I-74 AND IL 47 GO NORTH ON IL 47 FOR 0.7 MILES TO MARK ON THE LEFT. SAID MARK IS A CHISLED SQUARE LOCATED ON THE TOP OF THE SW CORNER OF AN AR BOX CULVERT. THE BOX IS WEST OF THE ENTRANCE TO BRIARCLIFF DRIVE.
 ELEVATION = 710.72 (NAVD 88)

B. M. 4875-6 STA. AND OFFSET NOT STATED
 CHAMPAIGN COUNTY
 CHISLED SQUARE
 TO REACH FROM THE INTERSECTION OF I-74 AND IL 47 GO NORTH ON IL 47 FOR 1.0 MILE TO MARK ON THE LEFT. SAID MARK IS LOCATED ON TOP OF THE SW CORNER OF AN AR BOX CULVERT. THE BOX CULVERT IS LOCATED ON THE EAST SIDE OF CONCRETE STRUCTURE.
 ELEVATION = 726.29 (NAVD 88)

B. M. 4875-7 STA. AND OFFSET NOT STATED
 CHAMPAIGN COUNTY
 CHISLED SQUARE
 TO REACH FROM THE INTERSECTION OF I-74 AND IL 47 GO NORTH ON IL 47 FOR 1.4 MILES TO MARK ON THE RIGHT. SAID MARK IS LOCATED ON TOP OF THE NW CORNER OF AN AR BOX CULVERT. THE BOX CULVERT IS LOCATED APPROXIMATELY 250 FT. NORTH OF ENTRANCE TO OAK VALLEY DR.
 ELEVATION = 727.51 (NAVD 88)

B. M. 4875-8 STA. AND OFFSET NOT STATED
 CHAMPAIGN COUNTY
 CHISLED SQUARE
 TO REACH FROM THE INTERSECTION OF I-74 AND IL 47 GO NORTH ON IL 47 FOR 1.65 MILES TO MARK ON THE LEFT. SAID MARK IS LOCATED ON TOP OF THE NW CORNER OF AN AR BOX CULVERT. THE BOX CULVERT IS LOCATED APPROXIMATELY 50 FT. SOUTH OF THE INTERSECTION OF IL 47 AND CR 2400N.
 ELEVATION = 723.02 (NAVD 88)

B. M. 4875-9 STA. AND OFFSET NOT STATED
 CHAMPAIGN COUNTY
 CHISLED SQUARE
 TO REACH FROM THE INTERSECTION OF I-74 AND IL 47 GO NORTH ON IL 47 FOR 2.0 MILES TO MARK ON THE RIGHT. SAID MARK IS LOCATED ON TOP OF THE SE CORNER OF AN AR BOX CULVERT. THE BOX CULVERT IS LOCATED APPROXIMATELY 100 FT. SOUTH OF THE INTERSECTION OF IL 47 AND CR 2425N.
 ELEVATION = 741.16 (NAVD 88)

B. M. 4875-10 STA. AND OFFSET NOT STATED
 CHAMPAIGN COUNTY
 CHISLED SQUARE
 TO REACH FROM THE INTERSECTION OF I-74 AND IL 47 GO NORTH ON IL 47 FOR 2.7 MILES TO MARK ON THE LEFT. SAID MARK IS LOCATED ON TOP OF THE SW CORNER OF AR BOX CULVERT STR. NO. 010-8604.
 ELEVATION = 727.22 (NAVD 88)

B. M. 4875-11 STA. AND OFFSET NOT STATED
 CHAMPAIGN COUNTY
 CHISLED SQUARE
 TO REACH FROM THE INTERSECTION OF I-74 AND IL 47 GO NORTH ON IL 47 FOR 3.0 MILES TO MARK ON THE LEFT. SAID MARK IS LOCATED ON TOP OF THE SW CORNER OF AR BOX CULVERT STR. NO. 010-8062.
 ELEVATION = 730.55 (NAVD 88)

B. M. 4875-12 STA. AND OFFSET NOT STATED
 CHAMPAIGN COUNTY
 WELL CAP LID
 TO REACH FROM THE INTERSECTION OF I-74 AND IL 47 GO NORTH ON IL 47 FOR 3.25 MILES TO MARK ON THE LEFT. SAID MARK IS LOCATED ON TOP OF THE NE CORNER OF AN INSPECTION WELL LID FOR NATURAL GAS. THE WELL IS LOCATED NEXT TO A YELLOW PUMP.
 ELEVATION = 742.62 (NAVD 88)

B. M. 4875-13 STA. AND OFFSET NOT STATED
 CHAMPAIGN COUNTY
 CHISLED SQUARE
 TO REACH FROM THE INTERSECTION OF I-74 AND IL 47 GO NORTH ON IL 47 FOR 3.9 MILES TO MARK ON THE RIGHT. SAID MARK IS LOCATED ON TOP OF THE NE CORNER OF AN AR BOX CULVERT. THE BOX CULVERT IS LOCATED APPROXIMATELY 1000' NORTH OF CR 2600N.
 ELEVATION = 729.68 (NAVD 88)

B. M. 4875-14 STA. AND OFFSET NOT STATED
 CHAMPAIGN COUNTY
 CHISLED SQUARE
 TO REACH FROM THE INTERSECTION OF I-74 AND IL 47 GO NORTH ON IL 47 FOR 4.35 MILES TO MARK ON THE RIGHT. SAID MARK IS LOCATED ON THE TOP CENTER OF AN AR BOX CULVERT. THE BOX CULVERT IS LOCATED APPROXIMATELY 650' NORTH OF CR 2650N.
 ELEVATION = 729.35 (NAVD 88)

FILE NAME =	USER NAME = ceorlockbm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BENCHMARKS				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ce:\pwork\pwork\ceorlockbm\d0187612\0870844-5ht-ATB.dgn	70844-5ht-ATB.dgn	DRAWN -	REVISED -		326	136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	93				
\$MODELNAME\$	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 70844								
	PLOT DATE = 10/10/2014	DATE -	REVISED -		SCALE: NONE	SHEET 1	OF 2 SHEETS	STA. -----	TO STA. -----	ILLINOIS FED. AID PROJECT			

BENCHMARKS

B. M. 4875-15 STA. AND OFFSET NOT STATED
 CHAMPAIGN COUNTY
 CHISLED SQUARE
 TO REACH FROM THE INTERSECTION OF I-74 AND IL 47 GO NORTH ON IL 47 FOR 4.7 MILES TO MARK ON THE RIGHT. SAID MARK IS LOCATED ON THE TOP THE NE WINGWALL OF STR. NO. 010-0239.
 ELEVATION = 709.16 (NAVD 88)

B. M. 4875-16 STA. AND OFFSET NOT STATED
 CHAMPAIGN COUNTY
 CHISLED SQUARE
 TO REACH FROM THE INTERSECTION OF I-74 AND IL 47 GO NORTH ON IL 47 FOR 5.0 MILES TO MARK ON THE LEFT. SAID MARK IS LOCATED ON THE TOP OF THE SW CORNER OF AN AR BOX CULVERT. THE BOX CULVERT IS LOCATED APPROXIMATELY 1300' NORTH OF CR 2700N.
 ELEVATION = 724.99 (NAVD 88)

B. M. 4875-17 STA. AND OFFSET NOT STATED
 CHAMPAIGN COUNTY
 CHISLED SQUARE
 TO REACH FROM THE INTERSECTION OF I-74 AND IL 47 GO NORTH ON IL 47 FOR 5.25 MILES TO MARK ON THE LEFT. SAID MARK IS LOCATED ON THE TOP OF THE SOUTHERLY MOSTE METAL GUARDPOST OF A SHORT PIECE OF YELLOW GUARDRAIL TO A NATRUAL GAS LIFT STATION.
 ELEVATION = 738.95 (NAVD 88)

B. M. 4875-18 STA. AND OFFSET NOT STATED
 CHAMPAIGN COUNTY
 CHISLED SQUARE
 TO REACH FROM THE INTERSECTION OF I-74 AND IL 47 GO NORTH ON IL 47 FOR 5.25 MILES TO MARK ON THE LEFT. SAID MARK IS LOCATED ON THE TOP OF THE NW CORNER OF AN AR BOX CULVERT APPROXIMATELY 300' WEST OF AN ENTRANCE TO A NATURAL GAS LIFT STATION THAT IS LOCATED IN THE TANGENT SECTION OF THE "S" CURVE.
 ELEVATION = 727.66 (NAVD 88)

B. M. 4875-19 STA. AND OFFSET NOT STATED
 CHAMPAIGN COUNTY
 TOP OF ROW
 TO REACH FROM THE INTERSECTION OF I-74 AND IL 47 GO NORTH ON IL 47 FOR 6.0 MILES TO MARK ON THE LEFT. SAID MARK IS LOCATED ON TOP OF A ROW MARKER LOCATED IN THE NW QUADRANT OF IL 47 AND CR 2800N JUST NORTH OF THE "S" CURVE.
 ELEVATION = 742.85 (NAVD 88)

B. M. 4875-20 STA. AND OFFSET NOT STATED
 CHAMPAIGN COUNTY
 CHISLED SQUARE
 TO REACH FROM THE INTERSECTION OF I-74 AND IL 47 GO NORTH ON IL 47 FOR 6.25 MILES TO MARK ON THE RIGHT. SAID MARK IS LOCATED ON THE TOP OF THE SE CORNER OF AN AR BOX CULVERT APPROXIMATELY 2600' NORTH OF CR 2800N.
 ELEVATION = 738.77 (NAVD 88)

B. M. 4875-21 STA. AND OFFSET NOT STATED
 CHAMPAIGN COUNTY
 CHISLED SQUARE
 TO REACH FROM THE INTERSECTION OF I-74 AND IL 47 GO NORTH ON IL 47 FOR 6.7 MILES TO MARK ON THE RIGHT. SAID MARK IS LOCATED ON THE TOP OF THE NE CORNER OF AN AR BOX CULVERT APPROXIMATELY 400' SOUTH OF A BRICK FARM HOUSE ON THE WEST SIDE OF IL 47 (2881).
 ELEVATION = 740.33 (NAVD 88)

B. M. 4875-22 STA. AND OFFSET NOT STATED
 CHAMPAIGN COUNTY
 CHISLED SQUARE
 TO REACH FROM THE INTERSECTION OF I-74 AND IL 47 GO NORTH ON IL 47 FOR 7.2 MILES TO MARK ON THE RIGHT. SAID MARK IS LOCATED ON THE TOP OF THE SE CORNER OF AR BOX CULVERT STR. NO. 010-8061.
 ELEVATION = 729.61 (NAVD 88)

B. M. 4875-23 STA. AND OFFSET NOT STATED
 CHAMPAIGN COUNTY
 TOP OF ROW
 TO REACH FROM THE INTERSECTION OF I-74 AND IL 47 GO NORTH ON IL 47 FOR 7.5 MILES TO MARK ON THE RIGHT. SAID MARK IS LOCATED ON TOP OF A ROW MARKER LOCATED JUST NORTH OF ENTRANCE TO CELLULAR TOWER AND APPROXIMATELY 1' NORTH OF A JUNCTION BOX.
 ELEVATION = 734.53 (NAVD 88)

B. M. 4875-24 STA. AND OFFSET NOT STATED
 CHAMPAIGN COUNTY
 CHISLED SQUARE
 MARK IS AT THE INTERSECTION OF US 136 AND IL 47.
 SAID MARK IS A CHISELED SQUARE LOCATED ON THE TOP OF THE NW CORNER OF THE STOP SIGN ISLAND IN THE SE QUADRANT OF THE INTERSECTION.
 ELEVATION = 735.45 (NAVD 88)

FILE NAME =	USER NAME = ceorlockbm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BENCHMARKS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ci:\pw\work\p1dot\ceorlockbm\d0187612\08	70844-5ht-ATB.dgn	DRAWN -	REVISED -			326	136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	94	
\$MODELNAME\$	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 70844					
	PLOT DATE = 10/10/2014	DATE -	REVISED -			SCALE: NONE	SHEET 2	OF 2 SHEETS	STA. -----	TO STA. -----	ILLINOIS FED. AID PROJECT

FAP 326 (IL 47):

Beginning chain IL47 description

Point 130 N 1,283,880.2329 E 965,351.9954 Sta 3+80.14

Course from 130 to 114 N 22° 42' 30.450" E Dist 3,492.6230

Equation: Sta 38+72.76 (BK) = Sta 97+14.44 (AH) -----
 End Region 1
 Begin Region 2

Point 114 N 1,287,102.1116 E 966,700.2953 Sta 97+14.44

Curve Data

Curve CURV133
 P.I. Station 106+31.61 N 1,287,948.1866 E 967,054.3629
 Delta = 13° 05' 28.472" (LT)
 Degree = 0° 43' 00.462"
 Tangent = 917.1725
 Length = 1,826.3578
 Radius = 7,993.3280
 External = 52.4472
 Long Chord = 1,822.3876
 Mid. Ord. = 52.1053
 P.C. Station 97+14.44 N 1,287,102.1123 E 966,700.2956
 P.T. Station 115+40.80 N 1,288,852.4690 E 967,207.5903
 C.C. N 1,290,187.8744 E 959,326.6015
 Back = N 22° 42' 30.450" E
 Ahead = N 9° 37' 01.977" E
 Chord Bear = N 16° 09' 46.213" E

Curve Data

Curve CURV113
 P.I. Station 129+38.64 N 1,290,230.5905 E 967,441.5771
 Delta = 9° 55' 12.099" (LT)
 Degree = 0° 21' 20.599"
 Tangent = 1,397.8450
 Length = 2,788.7028
 Radius = 16,106.9006
 External = 60.5425
 Long Chord = 2,785.2209
 Mid. Ord. = 60.3158
 P.C. Station 115+40.80 N 1,288,852.4683 E 967,207.5902
 P.T. Station 143+29.50 N 1,291,628.4183 E 967,434.6518
 C.C. N 1,291,548.6207 E 951,327.9489
 Back = N 9° 38' 10.208" E
 Ahead = N 0° 17' 01.891" W
 Chord Bear = N 4° 40' 34.158" E

Course from PT CURV113 to 9 N 0° 17' 01.891" W Dist 16,812.7204

Point 9 N 1,308,440.9324 E 967,351.3574 Sta 311+42.22

Course from 9 to PC CURV51 N 0° 16' 11.441" W Dist 5,462.9770

FAP 326 (IL 47) – CONTINUED:

Curve Data

Curve CURV51
 P.I. Station 372+58.00 N 1,314,556.6416 E 967,322.5542
 Delta = 63° 00' 11.300" (RT)
 Degree = 5° 22' 43.819"
 Tangent = 652.8000
 Length = 1,171.3149
 Radius = 1,065.2073
 External = 184.1183
 Long Chord = 1,113.1883
 Mid. Ord. = 156.9840
 P.C. Station 366+05.20 N 1,313,903.8488 E 967,325.6286
 P.T. Station 377+76.51 N 1,314,855.7109 E 967,902.8174
 C.C. N 1,313,908.8656 E 968,390.8241
 Back = N 0° 16' 11.441" W
 Ahead = N 62° 43' 59.859" E
 Chord Bear = N 31° 13' 54.209" E

Course from PT CURV51 to PC CURV52 N 62° 43' 59.859" E Dist 1,719.9501

Curve Data

Curve CURV52
 P.I. Station 401+48.76 N 1,315,942.5172 E 970,011.4716
 Delta = 62° 28' 15.307" (LT)
 Degree = 5° 19' 37.211"
 Tangent = 652.3000
 Length = 1,172.7217
 Radius = 1,075.5725
 External = 182.3430
 Long Chord = 1,115.4897
 Mid. Ord. = 155.9112
 P.C. Station 394+96.46 N 1,315,643.6770 E 969,431.6528
 P.T. Station 406+69.19 N 1,316,594.8104 E 970,014.4587
 C.C. N 1,316,599.7357 E 968,938.8975
 Back = N 62° 43' 59.859" E
 Ahead = N 0° 15' 44.552" E
 Chord Bear = N 31° 29' 52.205" E

Course from PT CURV52 to 57 N 0° 15' 44.551" E Dist 0.0138

Equation: Sta 406+69.20 (BK) = Sta 401+00.92 (AH) -----
 End Region 2
 Begin Region 3

Point 57 N 1,316,594.8241 E 970,014.4588 Sta 401+00.92

Course from 57 to PC CURV58 N 0° 15' 44.552" E Dist 9,144.7774

FILE NAME =	USER NAME = ceerlockbm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ALIGNMENT DATA IL 47 & US 136			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ce:\pw\work\pwidot\ceerlockbm\d0187612\0970844-5ht-ATB.dgn	DRAWN -	REVISED -	326					136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	95	
MODELNAME	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 70844			ILLINOIS FED. AID PROJECT				
	PLOT DATE = 10/10/2014	DATE -	REVISED -		SCALE: NONE	SHEET 1	OF 2 SHEETS	STA. ----	TO STA. ----			

FAP 326 (IL 47) – CONTINUED:

Curve Data

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Curve CURV58
P.I. Station      498+51.25  N      1,326,345.0443  E      970,059.1084
Delta            =      3° 54' 31.771" (LT)
Degree           =      0° 19' 22.360"
Tangent          =      605.5450
Length           =      1,210.6202
Radius           =      17,745.3389
External         =      10.3289
Long Chord       =      1,210.3855
Mid. Ord.        =      10.3228
P.C. Station     492+45.70  N      1,325,739.5057  E      970,056.3354
P.T. Station     504+56.32  N      1,326,949.3634  E      970,020.5960
C.C.             =      N      1,325,820.7669  E      952,311.1826
Back            = N      0° 15' 44.552" E
Ahead           = N      3° 38' 47.220" W
Chord Bear      = N      1° 41' 31.334" W
    
```

Course from PT CURV58 to 49 N 3° 38' 47.220" W Dist 279.3584

End Region 3

Equation: Sta 507+35.68 (BK) = Sta 507+49.60 (AH)

Begin Region 4

Point 49 N 1,327,228.1562 E 970,002.8289 Sta 507+49.60

Chain NIL47 found within chain IL47, contains:

61 CUR CURV75 25 CUR CURV56 CUR CURV33 35 38 53

Beginning subchain NIL47 description

Point 61 N 1,327,228.1562 E 970,002.8289 Sta 507+49.60

Course from 61 to PC CURV75 N 3° 38' 47.220" W Dist 537.8947

Curve Data

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Curve CURV75
P.I. Station      518+24.33  N      1,328,300.7152  E      969,934.4762
Delta            =      3° 45' 38.765" (RT)
Degree           =      0° 21' 01.421"
Tangent          =      536.8400
Length           =      1,073.2945
Radius           =      16,351.7785
External         =      8.8100
Long Chord       =      1,073.1018
Mid. Ord.        =      8.8053
P.C. Station     512+87.49  N      1,327,764.9620  E      969,968.6190
P.T. Station     523+60.79  N      1,328,837.5541  E      969,935.5473
C.C.             =      N      1,328,804.9286  E      986,287.2932
Back            = N      3° 38' 47.220" W
Ahead           = N      0° 06' 51.545" E
Chord Bear      = N      1° 45' 57.837" W
    
```

Course from PT CURV75 to 25 N 0° 06' 51.545" E Dist 6,555.8979

Point 25 N 1,335,393.4389 E 969,948.6278 Sta 589+16.69

FAP 709 (US 136):

Beginning chain US136 description

Point 140 N 1,327,232.2480 E 968,362.2130 Sta 1644+97.78

Course from 140 to 48 S 89° 51' 25.627" E Dist 567.4515

Point 48 N 1,327,230.8329 E 968,929.6627 Sta 1650+65.23

Course from 48 to 19 S 89° 51' 25.538" E Dist 2,504.8366

Point 19 N 1,327,224.5854 E 971,434.4915 Sta 1675+70.07

Course from 19 to 141 S 89° 51' 25.440" E Dist 356.5226

Point 141 N 1,327,223.6960 E 971,791.0130 Sta 1679+26.59

Ending chain US136 description

FILE NAME =	USER NAME = ceorlockbm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ALIGNMENT DATA IL 47 & US 136			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
ci:\pw\work\p1dot\ceorlockbm\d0187612\0870844-5ht-ATB.dgn		DRAWN -	REVISED -		SCALE: NONE	SHEET 2	OF 2 SHEETS	STA. ----	TO STA. ----	326	136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	96
MODELNAME		CHECKED -	REVISED -								CONTRACT NO. 70844			
		DATE -	REVISED -								ILLINOIS FED. AID PROJECT			

EAST SIDE TRAIL - I

<* 1 Describe Chain PRBIKE-OPTION-I

Chain PRBIKE-OPTION-I contains:
BC9 BC10 BC11 BC12 BC13 BC14 BC15 BC16 BC17 BC18 BC19 BC20 BC21 BC22 BC23 BC24

Beginning chain PRBIKE-OPTION-I description
=====

Point BC9 N 1,285,076.3237 E 965,897.8988 Sta 3000+00.00
Course from BC9 to BC10 N 19° 36' 27.844" E Dist 30.7327
Point BC10 N 1,285,105.2743 E 965,908.2121 Sta 3000+30.73
Course from BC10 to BC11 N 8° 30' 48.715" E Dist 7.7550
Point BC11 N 1,285,112.9439 E 965,909.3602 Sta 3000+38.49
Course from BC11 to BC12 N 20° 53' 06.683" E Dist 67.5782
Point BC12 N 1,285,176.0820 E 965,933.4516 Sta 3001+06.07
Course from BC12 to BC13 N 23° 08' 46.047" E Dist 245.6635
Point BC13 N 1,285,401.9709 E 966,030.0163 Sta 3003+51.73
Course from BC13 to BC14 N 25° 57' 17.058" E Dist 100.0783
Point BC14 N 1,285,491.9553 E 966,073.8167 Sta 3004+51.81
Course from BC14 to BC15 N 47° 49' 05.982" E Dist 14.3207
Point BC15 N 1,285,501.5714 E 966,084.4287 Sta 3004+66.13
Course from BC15 to BC16 N 26° 50' 11.887" E Dist 3.7784
Point BC16 N 1,285,504.9429 E 966,086.1344 Sta 3004+69.91

Describe Chain PRBIKE-OPTION-I (CONTINUED)

Course from BC16 to BC17 N 22° 01' 35.390" E Dist 32.6835
Point BC17 N 1,285,535.2408 E 966,098.3919 Sta 3005+02.59
Course from BC17 to BC18 N 30° 11' 53.347" E Dist 46.5457
Point BC18 N 1,285,575.4699 E 966,121.8040 Sta 3005+49.14
Course from BC18 to BC19 N 24° 36' 45.933" E Dist 64.2338
Point BC19 N 1,285,633.8676 E 966,148.5563 Sta 3006+13.37
Course from BC19 to BC20 N 22° 58' 14.296" E Dist 37.8012
Point BC20 N 1,285,668.6714 E 966,163.3086 Sta 3006+51.17
Course from BC20 to BC21 N 12° 50' 00.728" E Dist 89.4332
Point BC21 N 1,285,755.8706 E 966,183.1734 Sta 3007+40.60
Course from BC21 to BC22 N 21° 22' 26.993" E Dist 55.7315
Point BC22 N 1,285,807.7689 E 966,203.4852 Sta 3007+96.34
Course from BC22 to BC23 N 22° 28' 40.035" E Dist 57.6863
Point BC23 N 1,285,861.0727 E 966,225.5401 Sta 3008+54.02
Course from BC23 to BC24 N 67° 20' 52.034" W Dist 17.9875
Point BC24 N 1,285,868.0004 E 966,208.9401 Sta 3008+72.01
=====

Ending chain PRBIKE-OPTION-I description

FILE NAME =	USER NAME = ceorlockbm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ALIGNMENT DATA				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ce:\pwork\pwork\ceorlockbm\d0187612\0970844-5ht-ATB.dgn		DRAWN -	REVISED -		EAST SIDE TRAIL OPTION "I" - FRANKLIN ST. TO I-74 RAMP				326	136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	97
\$MODELNAME\$		CHECKED -	REVISED -		SCALE: NONE	SHEET 1	OF 1	SHEETS	STA. ----	TO STA. ----	ILLINOIS FED. AID PROJECT		
		DATE -	REVISED -		CONTRACT NO. 70844								

EAST SIDE TRAIL – C

Beginning chain PRBIKE-EAST description
 =====

Point BC4 N 1,288,073.8278 E 967,076.7735 Sta 2000+00.00

Course from BC4 to PC PRBIKE-EAST-1 N 14° 54' 13.116" E Dist 99.2598

Curve Data *-----*

Curve PRBIKE-EAST-1

P.I. Station 2004+23.51 N 1,288,483.5992 E 967,183.7602
 Delta = 4° 37' 21.905" (LT)
 Degree = 0° 42' 47.613"
 Tangent = 324.2493
 Length = 648.1468
 Radius = 8,033.3280
 External = 6.5412
 Long Chord = 647.9710
 Mid. Ord. = 6.5359
 P.C. Station 2000+99.26 N 1,288,169.7485 E 967,102.3025
 P.T. Station 2007+47.41 N 1,288,802.9940 E 967,239.6581
 C.C. N 1,290,187.8744 E 959,326.6015
 Back = N 14° 32' 58.803" E
 Ahead = N 9° 55' 36.899" E
 Chord Bear = N 12° 14' 17.851" E

Curve Data *-----*

Curve PRBIKE-EAST-2

P.I. Station 2007+59.02 N 1,288,814.4328 E 967,241.6601
 Delta = 13° 14' 52.005" (RT)
 Degree = 57° 17' 44.806"
 Tangent = 11.6127
 Length = 23.1217
 Radius = 100.0000
 External = 0.6720
 Long Chord = 23.0703
 Mid. Ord. = 0.6675
 P.C. Station 2007+47.41 N 1,288,802.9940 E 967,239.6581
 P.T. Station 2007+70.53 N 1,288,825.1084 E 967,246.2300
 C.C. N 1,288,785.7549 E 967,338.1610
 Back = N 9° 55' 36.899" E
 Ahead = N 23° 10' 28.904" E
 Chord Bear = N 16° 33' 02.901" E

Course from PT PRBIKE-EAST-2 to PC PRBIKE-EAST-3 N 23° 10' 28.904" E Dist 48.8709

Curve Data *-----*

Curve PRBIKE-EAST-3

P.I. Station 2008+31.92 N 1,288,881.5490 E 967,270.3910
 Delta = 14° 16' 36.243" (LT)
 Degree = 57° 17' 44.806"
 Tangent = 12.5237
 Length = 24.9176
 Radius = 100.0000
 External = 0.7812
 Long Chord = 24.8532
 Mid. Ord. = 0.7751
 P.C. Station 2008+19.40 N 1,288,870.0359 E 967,265.4625
 P.T. Station 2008+44.32 N 1,288,893.9219 E 967,272.3281
 C.C. N 1,288,909.3895 E 967,173.5316
 Back = N 23° 10' 28.904" E
 Ahead = N 8° 53' 52.661" E
 Chord Bear = N 16° 02' 10.782" E

EAST SIDE TRAIL – C (CONTINUED)

PRBIKE-EAST description (CONTINUED)
 =====

Course from PT PRBIKE-EAST-3 to PC PRBIKE-EAST-4 N 8° 53' 52.661" E Dist 146.4911

Curve Data *-----*

Curve PRBIKE-EAST-4

P.I. Station 2009+96.00 N 1,289,043.7750 E 967,295.7890
 Delta = 5° 56' 20.013" (RT)
 Degree = 57° 17' 44.806"
 Tangent = 5.1873
 Length = 10.3653
 Radius = 100.0000
 External = 0.1345
 Long Chord = 10.3607
 Mid. Ord. = 0.1343
 P.C. Station 2009+90.81 N 1,289,038.6501 E 967,294.9867
 P.T. Station 2010+01.17 N 1,289,048.7894 E 967,297.1173
 C.C. N 1,289,023.1826 E 967,393.7832
 Back = N 8° 53' 52.661" E
 Ahead = N 14° 50' 12.674" E
 Chord Bear = N 11° 52' 02.667" E

Course from PT PRBIKE-EAST-4 to PC PRBIKE-EAST-5 N 14° 50' 12.674" E Dist 77.1005

Curve Data *-----*

Curve PRBIKE-EAST-5

P.I. Station 2010+83.77 N 1,289,128.6368 E 967,318.2689
 Delta = 6° 17' 50.100" (LT)
 Degree = 57° 17' 44.806"
 Tangent = 5.5009
 Length = 10.9908
 Radius = 100.0000
 External = 0.1512
 Long Chord = 10.9852
 Mid. Ord. = 0.1510
 P.C. Station 2010+78.27 N 1,289,123.3192 E 967,316.8602
 P.T. Station 2010+89.26 N 1,289,134.0767 E 967,319.0857
 C.C. N 1,289,148.9260 E 967,220.1944
 Back = N 14° 50' 12.674" E
 Ahead = N 8° 32' 22.574" E
 Chord Bear = N 11° 41' 17.624" E

Course from PT PRBIKE-EAST-5 to PC PRBIKE-EAST-6 N 8° 32' 22.574" E Dist 36.4779

Curve Data *-----*

Curve PRBIKE-EAST-6

P.I. Station 2011+32.99 N 1,289,177.3139 E 967,325.5781
 Delta = 32° 19' 07.148" (LT)
 Degree = 229° 10' 59.225"
 Tangent = 7.2439
 Length = 14.1017
 Radius = 25.0000
 External = 1.0283
 Long Chord = 13.9155
 Mid. Ord. = 0.9877
 P.C. Station 2011+25.74 N 1,289,170.1502 E 967,324.5024
 P.T. Station 2011+39.84 N 1,289,183.9428 E 967,322.6573
 C.C. N 1,289,173.8626 E 967,299.7796
 Back = N 8° 32' 22.574" E
 Ahead = N 23° 46' 44.574" W
 Chord Bear = N 7° 37' 11.000" W

FILE NAME =	USER NAME = ceorlockbm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ALIGNMENT DATA EAST SIDE TRAIL OPTION "C"- EXIST. TRAILHEAD TO BRIARCLIFF DR.	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ci:\pwork\pwork\ceorlockbm\d0187612\0870844-5ht-ATB.dgn	DRAWN -	REVISED -	326			136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	98	
PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -	CONTRACT NO. 70844							
\$MODELNAME\$	DATE -	REVISED -	SCALE: NONE			SHEET 1 OF 2 SHEETS	STA. ---- TO STA. ----	ILLINOIS FED. AID PROJECT		

EAST SIDE TRAIL – C (CONTINUED)

PRBIKE-EAST description (CONTINUED)

Course from PT PRBIKE-EAST-6 to PC PRBIKE-EAST-7 N 23° 46' 44.575" W Dist 12.0828

Curve Data *-----*

Curve PRBIKE-EAST-7

P.I. Station	2011+80.50	N	1,289,221.1511	E	967,306.2627
Delta	=		31° 53' 48.776"	(RT)	
Degree	=		57° 17' 44.806"		
Tangent	=		28.5772		
Length	=		55.6706		
Radius	=		100.0000		
External	=		4.0031		
Long Chord	=		54.9544		
Mid. Ord.	=		3.8491		
P.C. Station	2011+51.93	N	1,289,194.9999	E	967,317.7853
P.T. Station	2012+07.60	N	1,289,249.4419	E	967,310.2981
C.C.		N	1,289,235.3210	E	967,409.2961
Back	= N		23° 46' 44.574"	W	
Ahead	= N		8° 07' 04.202"	E	
Chord Bear	= N		7° 49' 50.186"	W	

Course from PT PRBIKE-EAST-7 to PC PRBIKE-EAST-8 N 8° 07' 04.202" E Dist 35.1294

Curve Data *-----*

Curve PRBIKE-EAST-8

P.I. Station	2012+59.45	N	1,289,300.7783	E	967,317.6206
Delta	=		18° 59' 29.332"	(LT)	
Degree	=		57° 17' 44.806"		
Tangent	=		16.7266		
Length	=		33.1464		
Radius	=		100.0000		
External	=		1.3892		
Long Chord	=		32.9949		
Mid. Ord.	=		1.3702		
P.C. Station	2012+42.73	N	1,289,284.2193	E	967,315.2587
P.T. Station	2012+75.87	N	1,289,317.2046	E	967,314.4653
C.C.		N	1,289,298.3402	E	967,216.2607
Back	= N		8° 07' 04.202"	E	
Ahead	= N		10° 52' 25.130"	W	
Chord Bear	= N		1° 22' 40.464"	W	

Course from PT PRBIKE-EAST-8 to PC PRBIKE-EAST-9 N 10° 52' 25.130" W Dist 19.5943

EAST SIDE TRAIL – C (CONTINUED)

PRBIKE-EAST description (CONTINUED)

Curve Data *-----*

Curve PRBIKE-EAST-9

P.I. Station	2013+11.21	N	1,289,351.9098	E	967,307.7987
Delta	=		17° 53' 45.638"	(RT)	
Degree	=		57° 17' 44.806"		
Tangent	=		15.7454		
Length	=		31.2344		
Radius	=		100.0000		
External	=		1.2320		
Long Chord	=		31.1076		
Mid. Ord.	=		1.2170		
P.C. Station	2012+95.47	N	1,289,336.4471	E	967,310.7689
P.T. Station	2013+26.70	N	1,289,367.5371	E	967,309.7236
C.C.		N	1,289,355.3115	E	967,408.9735
Back	= N		10° 52' 25.130"	W	
Ahead	= N		7° 01' 20.508"	E	
Chord Bear	= N		1° 55' 32.311"	W	

Course from PT PRBIKE-EAST-9 to PC PRBIKE-EAST-10 N 7° 01' 20.508" E Dist 419.1345

Curve Data *-----*

Curve PRBIKE-EAST-10

P.I. Station	2017+51.57	N	1,289,789.2177	E	967,361.6666
Delta	=		6° 33' 45.360"	(LT)	
Degree	=		57° 17' 44.806"		
Tangent	=		5.7332		
Length	=		11.4539		
Radius	=		100.0000		
External	=		0.1642		
Long Chord	=		11.4476		
Mid. Ord.	=		0.1639		
P.C. Station	2017+45.84	N	1,289,783.5275	E	967,360.9657
P.T. Station	2017+57.29	N	1,289,794.9508	E	967,361.7126
C.C.		N	1,289,795.7532	E	967,261.7158
Back	= N		7° 01' 20.508"	E	
Ahead	= N		0° 27' 35.147"	E	
Chord Bear	= N		3° 44' 27.827"	E	

Course from PT PRBIKE-EAST-10 to BC5 N 0° 27' 35.147" E Dist 44.3953

Point BC5 N 1,289,839.3447 E 967,362.0688 Sta 2018+01.69

Course from BC5 to BC6 N 5° 54' 45.904" E Dist 60.2282

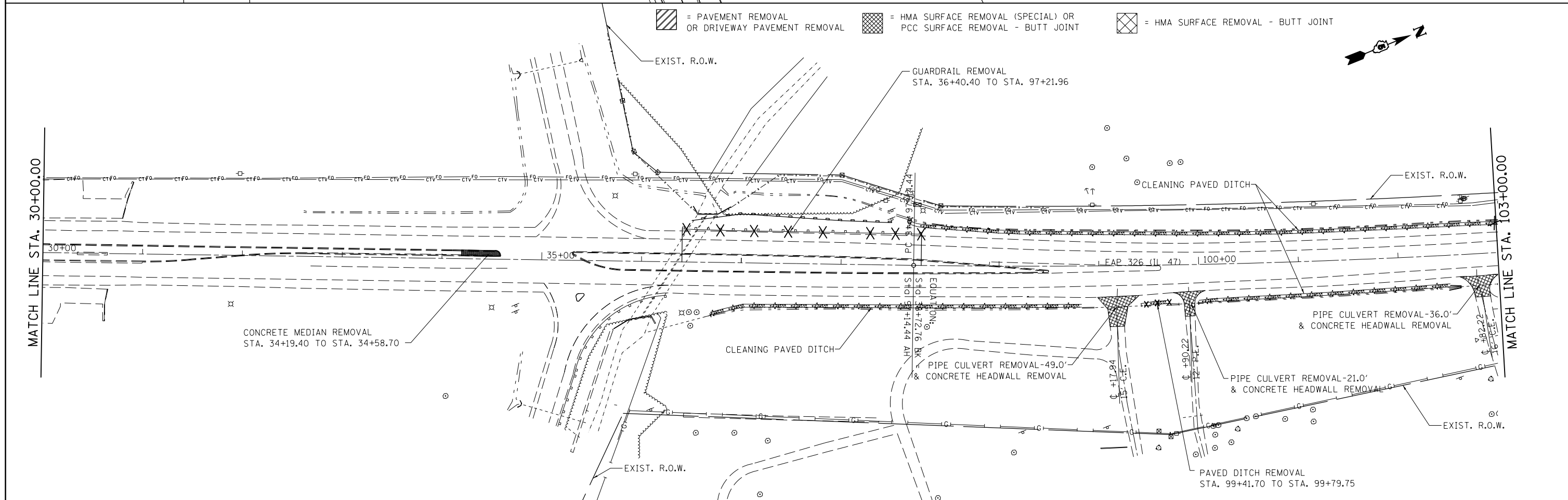
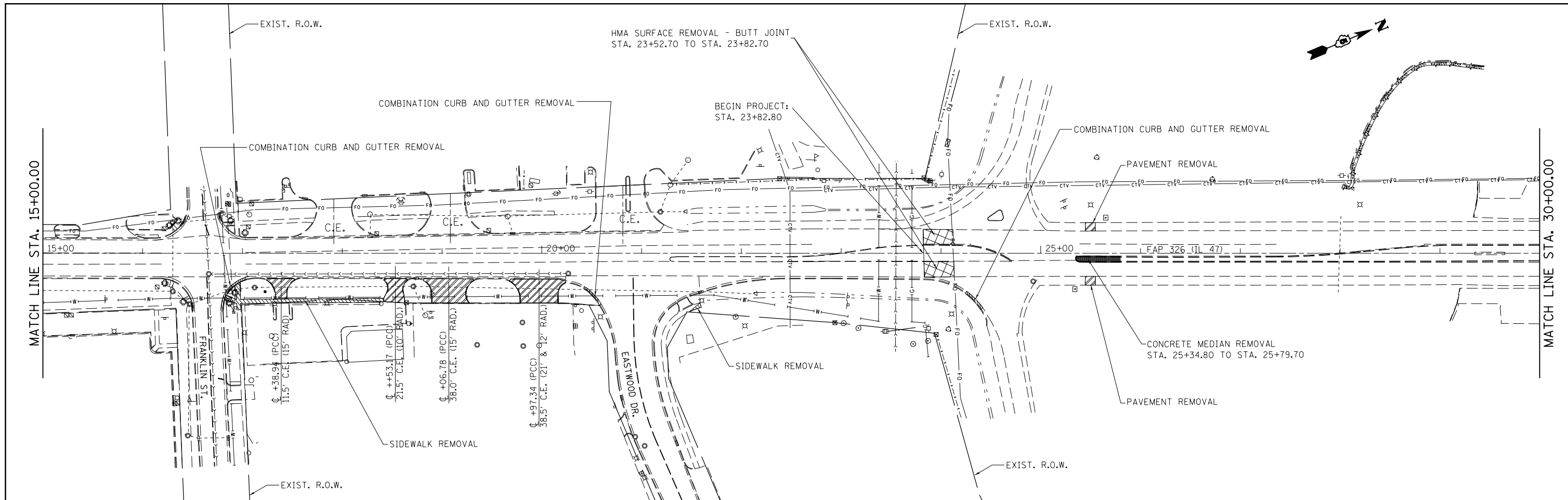
Point BC6 N 1,289,899.2524 E 967,368.2732 Sta 2018+61.91




Course from BC6 to BC7 N 21° 06' 01.008" E Dist 38.8428

Point BC7 N 1,289,935.4909 E 967,382.2566 Sta 2019+00.76

Ending chain PRBIKE-EAST description

FILE NAME =	USER NAME = ceerlockbm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ALIGNMENT DATA				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	ci:\pwork\pwork\ceerlockbm\d0187612\0870844-5ht-ATB.dgn	DRAWN -	REVISED -		EAST SIDE TRAIL OPTION "C" – EXIST. TRAILHEAD TO BRIARCLIFF DR.				326	136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	99
#MODELNAME#	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -	SCALE: NONE	SHEET 2	OF 2	SHEETS	STA. ----	TO STA. ----	ILLINOIS FED. AID PROJECT			
	PLOT DATE = 10/10/2014	DATE -	REVISED -	CONTRACT NO. 70844									



 = PAVEMENT REMOVAL OR DRIVEWAY PAVEMENT REMOVAL
  = HMA SURFACE REMOVAL (SPECIAL) OR PCC SURFACE REMOVAL - BUTT JOINT
  = HMA SURFACE REMOVAL - BUTT JOINT

FILE NAME =	USER NAME = ceorlockbm	DESIGNED -	REVISED -
ce:\pwork\pwork\ceorlockbm\d0187612\0870844-sht-Removal Details.50.dgn		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -
#MODELNAME#	PLOT DATE = 10/10/2014		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**REMOVAL DETAIL
F.A.P. 326 (IL 47)**

SCALE: 1"=50' SHEET 01 OF 16 SHEETS STA. 15+00.00 TO STA. 103+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	136RS-3, CR & (10-4)RS-1	CHAMPAIGN	474	100
CONTRACT NO. 70844				
ILLINOIS FED. AID PROJECT				