

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-100-RS&SW	COOK	30	1
		ALUMOS	CONTRACT NO. 62J86	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT IS LOCATED IN THE CITY OF ARLINGTON HEIGHTS

PROPOSED HIGHWAY PLANS

ROUTE FAP 305 (PALATINE RD-SOUTH FRONTAGE ROAD)
N. VERDE AVENUE TO PALATINE ROAD
SECTION: 2019-100-RS&SW
PROJECT: STP-VNHN(793)
STANDARD OVERLAY, ADA IMPROVEMENTS
COOK COUNTY

TRAFFIC DATA:

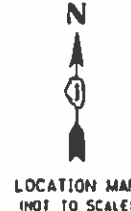
2018 ADT = 3800 VPD
SPEED LIMIT = 30 MPH

C-91-046-20

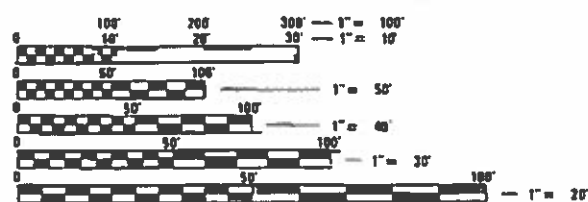
D-91-265-20



PROJECT BEGINS
STA 4+2.9



PROJECT ENDS
STA 43+52.9



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER VESELIN VELICHKOV (847) 705-4432
PROJECT MANAGER FAWAD AQUEEL (847) 705-4247

GROSS LENGTH = 3696.0 FT. = 0.70 MILE
NET LENGTH = 3696.0 FT. = 0.70 MILE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED DECEMBER 6 2019

Anthony A. Wright / **AAW**
REGIONAL ENGINEER

Jan 31 2020
cc *E.A. Elk*
ENGINEER OF DESIGN AND ENVIRONMENT

Jan 31 2020
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

CONTRACT NO. 62J86

INDEX OF SHEETS

STATE STANDARDS

GENERAL NOTES

SHEET NO.	DESCRIPTION	STANDARD NO.	DESCRIPTION
1	COVER SHEET	000001-07	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
2-3	INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES	442201-03	CLASS C AND D PATCHES
4-6	SUMMARY OF QUANTITIES	482011-03	HMA SHLD. STRIPS/SHLDS. WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
7-9	TYPICAL SECTIONS	642006	SHOULDER RUMBLE STRIPS, 8 IN.
10-11	ROADWAY AND PAVEMENT MARKING PLANS	701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' (4.5m) TO 24" (600mm) FROM PAVEMENT EDGE
12-13	CURB RAMPS DETAILS (ADA IMPROVEMENTS)	701011-04	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
14	BD-01: DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB OR EDGE OF SHOULDER GREATER THAN OR EQUAL TO 15' (4.5 M)	701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
15	BD-02: DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB IS LESS THAN 15' (4.5 M)	701311-03	LANE CLOSURE 2L, 2W MOVING OPERATIONS - DAY ONLY
16	BD-08: DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING	701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
17	BD-22: PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
18	BD-24: CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	701901-08	TRAFFIC CONTROL DEVICES
19	BD-32: BUTT JOINT AND HMA TAPER DETAILS	814001-03	HANDHOLES
20	BD-33: HMA TAPER AT EDGE OF P.C.C PAVEMENT		
21	TC-10: TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS		
22	TC-11: TYPICAL APPLICATIONS: RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)		
23	TC-13: DISTRICT ONE TYPICAL PAVEMENT MARKINGS		
24	TC-14: TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)		
25	TC-16: PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING		
26	TC-22: ARTERIAL ROAD INFORMATION SIGN		
27	TC-26: DRIVEWAY ENTRANCE SIGNING		
28	PD-01: PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS		
29	PD-04: PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/TURNING SPACE		
30	PD-05: PROJECT DETAIL FOR DEPRESSED CORNER CURB RAMPS		

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, CITY OF ARLINGTON HEIGHTS.
- FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
- THE CONTRACTOR SHALL CONTACT DISTRICT ONE ARTERIAL TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.
- UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE RESIDENT ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1½ INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1V:3H.
- ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF CURB OR DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER, SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY.
- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS, UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS THAT WILL NOT BE REMOVED INCLUDING PREVIOUSLY SEEDED AREAS. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE TO THE SATISFACTION OF THE ENGINEER.
- LOCATIONS OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT [OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)], WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER / TECHNICIAN.
- CATCH BASINS, MANHOLES, INLETS, DRAINAGE STRUCTURES AND VALVE VAULTS ADJUSTMENT AND/OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER / TECHNICIAN.
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- THE RESIDENT ENGINEER SHALL CONTACT WALTER CZARNY, AREA TRAFFIC FIELD ENGINEER, VIA E-MAIL AT WALTER.CZARNY@ILLINOIS.GOV, A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

SEE SHEET 3 FOR CONTINUATION

MODEL: Default
 FILE: \\blm1c1-pub\blm1room\ada\illinois.gov\PWIDOT\Documents\DOT_Offices\District_1\Projects\ID1265270\CaddData\Design\ID1265270-sh-agent\state.dgn

USER NAME = lababidim	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PALATINE RD-S. FRONTAGE RD (N. VERDE AVE TO PALATINE RD) INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DRAWN -	REVISIED -	305			2019-100-R5&SW	COOK	30	2	
PLOT SCALE = 100,0000 ' / in.	CHECKED -	REVISED -			CONTRACT NO. 62J86				
PLOT DATE = 12/13/2019	DATE -	REVISED -			ILLINOIS	FED. AID PROJECT			
				SCALE:	SHEET	OF	SHEETS	STA.	TO STA.

GENERAL NOTES (CONTINUED)

16. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
17. ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
18. THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENT FIRM TO CONTINUOUSLY MONITOR FOR WORKER SAFETY AND SOIL CONTAMINATION AT SEVERAL LOCATIONS. SEE SPECIAL PROVISION AND SUPPLEMENTAL SPECIFICATIONS FOR DETAILS.
19. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
20. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
21. ALL CAST OPEN LIDS FOR FRAMES, TYPE 1, WITHIN CURB RAMPS FOR SIDEWALK, SHALL BE "ADA COMPLIANT" CAST OPEN LIDS PER HIGHWAY STANDARD 604001.
22. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPLACEMENT OF ANY DETECTOR LOOPS DAMAGED DURING CONSTRUCTION.
23. PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER / TECHNICIAN.
24. WHEN EXISTING SIDEWALK IS TO BE REMOVED WITHOUT PROPOSED SIDEWALK REPLACEMENT, IT SHALL BE REPLACED WITH TOPSOIL AND SOD.
25. PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
26. THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION.
27. GAPS BETWEEN SUCCESSIVE LANE CLOSURES SHALL NOT BE LESS THAN 2 MILES (3 KM) IN LENGHT ACCORDING TO ARTICLE 701.05 MAXIMIM LENGTH OF LANE CLOSURE
28. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
29. THE CONTRACTOR SHALL USE CARE IN CONSTRUCTION OPERATIONS NEAR ANY EXISTING ITEMS THAT WILL NOT BE REMOVED. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE TO THE SATISFACTION OF THE ENGINEER
30. WHEN SOD IS PLACED IN A LOCATION THAT ABUTS TO EXISTING TREES OR PLANTING BEDS, THE CONTRACTOR SHALL PROVIDE A SPADED EDGE

MODEL: Default
 FILE: \\blm1c1-pub\blm1room\data\illinois.gov\PWIDOT\Documents\DOT_Offices\District_1\Projects\12165270\CaddData\Design\12165270-shl-generat.dgn

USER NAME = lababidim	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 12/13/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PALATINE RD-S. FRONTAGE RD (N. VERDE AVE TO PALATINE RD)
INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES**

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
--------	-------	----	--------	------	----	------

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-100-R5&SW	COOK	30	3
			CONTRACT NO. 62J86	
		ILLINOIS	FED. AID PROJECT	

URBAN

URBAN

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 80% FED 20% STATE	0005 100% STATE			
20200100	EARTH EXCAVATION	CU YD	42	42				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	777	777				
25200110	SODDING, SALT TOLERANT	SO YD	777	777				
25200200	SUPPLEMENTAL WATERING	UNIT	9	9				
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	16	16				
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	6810	6810				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	15	15				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	232	232				
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SO YD	370	370				
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	60	60				
40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	424	424				
40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	989	989				
42001300	PROTECTIVE COAT	SO YD	1316	1316				

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 80% FED 20% STATE	0005 100% STATE			
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SO YD	10	10				
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	2655	2655				
42400300	PORTLAND CEMENT CONCRETE SIDEWALK 6 INCH	SO FT	50	50				
42400800	DETECTABLE WARNINGS	SO FT	200	200				
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	7345	7345				
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	10	10				
44000600	SIDEWALK REMOVAL	SO FT	2655	2655				
44002210	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 2 1/2"	SO YD	422	422				
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SO YD	160	160				
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SO YD	16	16				
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SO YD	230	230				
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	74	74				
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	2	2				
60250200	CATCH BASINS TO BE ADJUSTED	EACH	1	1				

FILE NAME =	USER NAME = labatdim	DESIGNED -	REVISED -
pw:\planroom\dtd\illinois\gov\PI\DOT\Documents\DOT\Office\District\Projects\DI26520\CADD\Design\DI26520-4\DRAWN		REVISION	REVISED -
	PLOT SCALE = 100,0000' / 1in	CHECKED -	REVISED -
	PLOT DATE = 12/13/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PALATINE S. FRONTAGE RD (N. VERDE AVE TO PALATINE RD)
SUMMARY OF QUANTITIES**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-100-R5&SW	COOK	30	4
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62J86	

SCALE: SHEET NO. OF SHEETS STA. TO STA.

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 80% FED 20% STATE	0005 100% STATE			
60260100	INLETS TO BE ADJUSTED	EACH	2	2				
60262700	INLETS TO BE RECONSTRUCTED	EACH	2	2				
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	2	2				
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	5	5				
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	42	42				
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	4	4				
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	LSUM	1	1				
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	LSUM	1	1				
* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	2	2				
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6				
67100100	MOBILIZATION	L SUM	1	1				
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1				
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1				
70300100	SHORT TERM PAVEMENT MARKING	FOOT	1490	1490				
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	500	500				

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 80% FED 20% STATE	0005 100% STATE			
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	5150	5150				
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	20	20				
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	371	371				
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	5150	5150				
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	20	20				
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	75	75				
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	75	75				
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1				
X2020110	GRADING AND SHAPING SHOULDERS	UNIT	37	37				
X4405030	LONGITUDINAL PARTIAL DEPTH REMOVAL 3"	FOOT	909.3	909.3				
X440A300	PORTLAND CEMENT CONCRETE SURFACE REMOVAL (COLD MILLING) VARIABLE DEPTH	SO YD	606	606				
X4420900	LONGITUDINAL PARTIAL DEPTH PATCHING	TON	72	72				
△ X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	100		100			
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SO FT	1840	1840				

* = SPECIALTY ITEMS
 △ = NON-PARTICIPATING WORK (100% STATE)

FILE NAME =	USER NAME = ltab01djm	DESIGNED -	REVISED -
pw:\planroom\dtd\111\mof\gov\PI\DOT\Documents\DOT Offices\District\Projects\DI26520\CADD\Site\Design\DI26520-4\DRAWN		CHECKED -	REVISED -
		DATE -	REVISED -

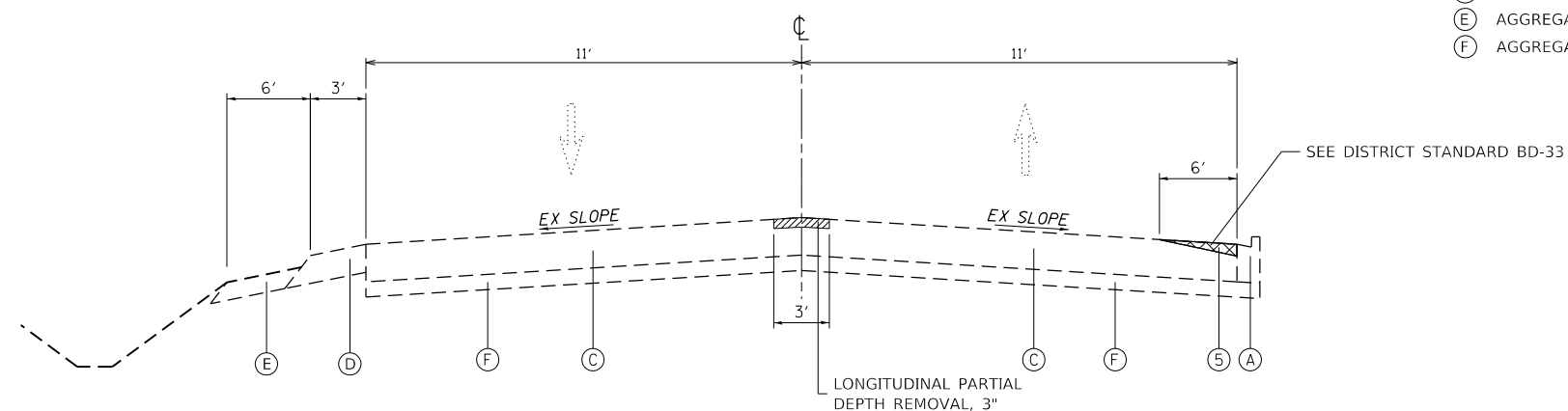
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PALATINE S. FRONTAGE RD (N. VERDE AVE TO PALATINE RD)
 SUMMARY OF QUANTITIES**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-100-R5&SW	COOK	30	5
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT			CONTRACT NO. 62J86	

SCALE: SHEET NO. OF SHEETS STA. TO STA.

PALATINE RD - SOUTH FRONTAGE ROAD



EXISTING TYPICAL SECTION

STA 4+02.9 TO STA 13+12.2

LEGEND - EXISTING:

- (A) EXISTING COMB. CURB AND GUTTER, (VARIES)
- (B) EXISTING H.M.A. VARIES 2"-2½" (I)
- (C) EXISTING PCC, VARIES 9¼"- 11" (I)
- (D) HMA SHOULLDER
- (E) AGGREGATE SHOULDER
- (F) AGGREGATE BASE

LEGEND - PROPOSED

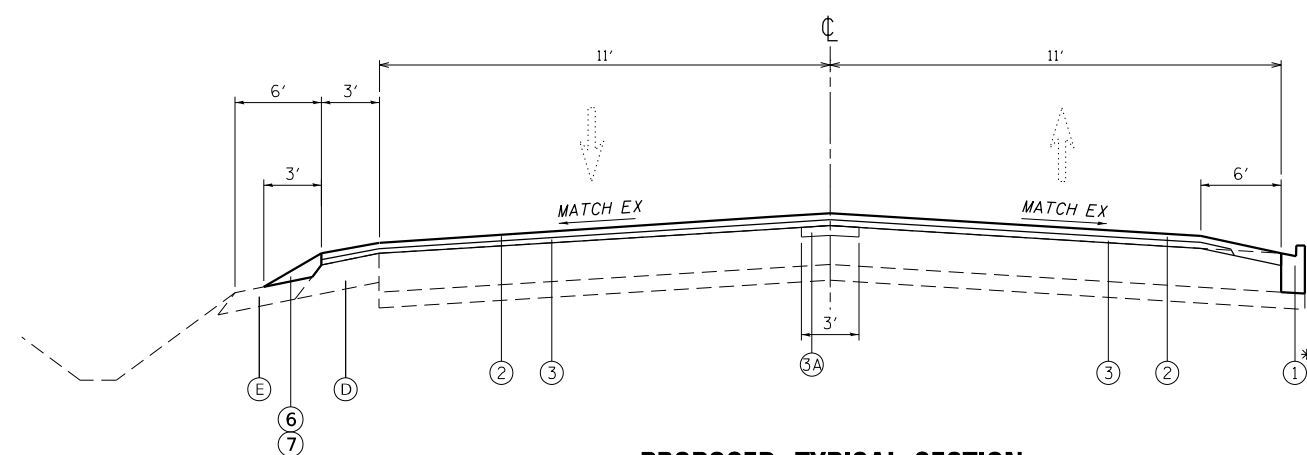
- (1) PROPOSED COMB. CURB AND GUTTER REMOVAL & REPLACMNT *
- (2) PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D" 9.5, N50, 1¾"
- (3) PROPOSED POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL - 4.75, N50., ¾"
- (3A) PROPOSED LONGITUDINAL PARTIAL DEPTH PATCHING, 3"
- (4) PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
- (5) PROPOSED PCC SURFACE REMOVAL, VAR DEPTH (SEE BD-33)
- (6) GRADING AND SHAPING SHOULDERS
- (7) AGGREGATE WEDGE SHOULDER, TYPE B

* = LOCATIONS OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT [OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)], WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER / TECHNICIAN.

NOTES:

- 1) THE CONTRACTOR SHALL PATCH FIRST BEFORE MILLING.
- 2) THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL - 4.75, N50.

PALATINE RD - SOUTH FRONTAGE ROAD



PROPOSED TYPICAL SECTION

STA 4+02.9 TO STA 13+12.2

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE TYPE	AIR Voids @ Ndes	
PAVEMENT RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D" IL 9.5, N50, 1 3/4"	4.0% AT 50 GYR	QC/QA
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, (IL-4.75), N50., 3/4"	3.5% AT 50 GYR.	QC/QA
PATCHING		
CLASS D PATCHES (HMA BINDER IL-19 mm)	4.0% AT 70 GYR	QC/QA
HOT-MIX ASPHALT REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm)	4.0% AT 70 GYR.	QC/QA
LONGITUDINAL PARTIAL DEPTH PATCHING		
HOT - MIX ASPHALT BINDER COURSE, IL - 9.5, N70; 3"	4.0% AT 70 GYR	QC/QA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP)		

- NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
- NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.
- NOTE 3: QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

MODEL: Default
 FILE: \\hpc101\pub\hpc\room\dat\illinois.gov\pww\DOT\Documents\DOT - Offices\District 1\Projects\1126520\CaddData\Design\1126520-sh-detailed.dgn

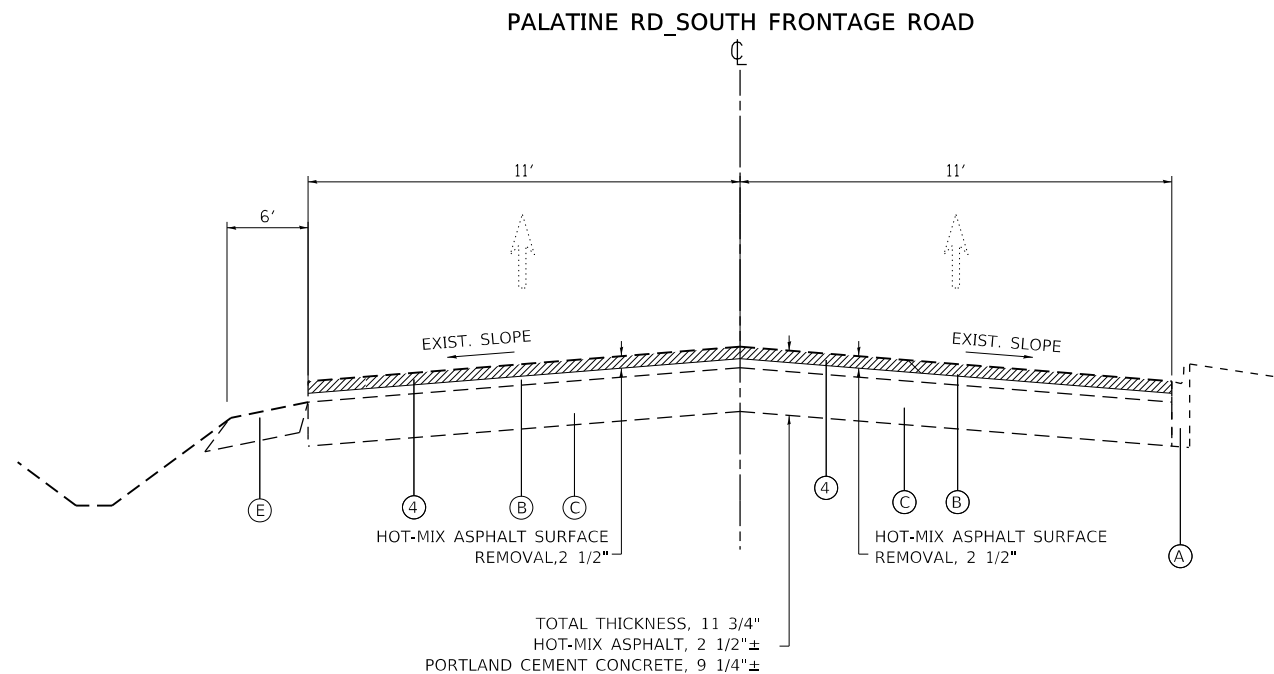
USER NAME = Velichkovv	DESIGNED -	REVISED -
PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 1/17/2020	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PALATINE RAOD - S. FRONTAGE RD (N. VERDE AVE TO PALATINE ROAD)
TYPICAL SECTIONS

SCALE: SHEET 7 OF 30 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-100-R5&SW	COOK	30	7
CONTRACT NO. 62J86				
ILLINOIS FED. AID PROJECT				



LEGEND - EXISTING:

- (A) EXISTING COMB. CURB AND GUTTER, (VARIES)
- (B) EXISTING H.M.A. VARIES 2"-2 1/2" (±)
- (C) EXISTING PCC BASE COURSE, VARIES 9 1/4"- 11" (±)
- (D) HMA SHOULDER
- (E) AGGREGATE SHOULDER

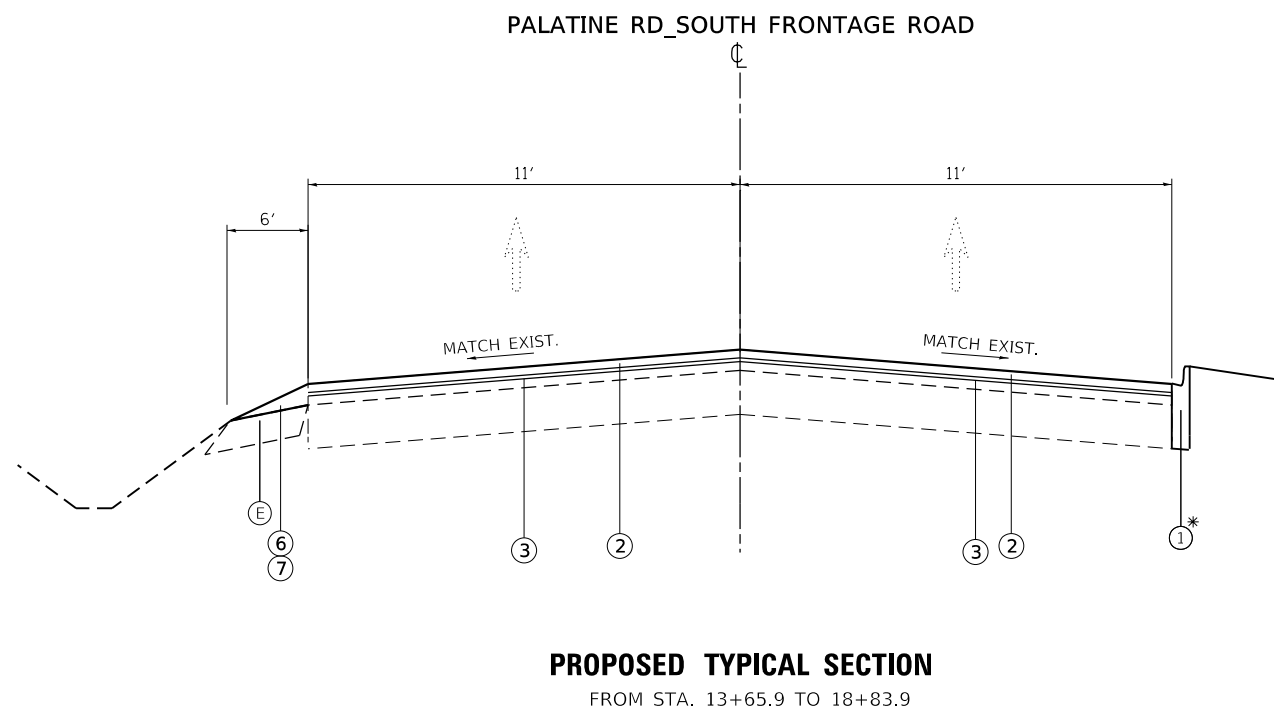
NOTES:

- 1.) THE CONTRACTOR SHALL PATCH FIRST BEFORE MILLING.
- 2.) THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL - 4.75, N50.

LEGEND - PROPOSED

- ① PROPOSED COMB. CURB AND GUTTER REMOVAL & REPLACEMENT
- ② PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D" 9.5, N50, 1 3/4"
- ③ PROPOSED POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL - 4.75, N50, 3/4"
- ④ PROPOSED LONGITUDINAL PARTIAL DEPTH PATCHING, 3"
- ⑤ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- ⑥ PROPOSED PCC SURFACE REMOVAL, VAR DEPTH (SEE BD-33)
- ⑦ GRADING AND SHAPING SHOULDERS
- ⑧ AGGREGATE WEDGE SHOULDER, TYPE B

* PROPOSED COMB. CURB AND GUTTER, (VARIES) SHALL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.



MODEL: Default
 FILE: \\blm1c:\pub\blmroom\data\illinois.gov\PWIDOT\Documents\DOT_Offices\Drawings\1\Projects\1216520\CaddData\Design\1216520-sh-detailed.dgn

USER NAME = lababidim	DESIGNED -	REVISED -
PLOT SCALE = 100,0000 ' / in.	DRAWN -	REVISED -
PLOT DATE = 12/13/2019	CHECKED -	REVISED -
	DATE -	REVISED -

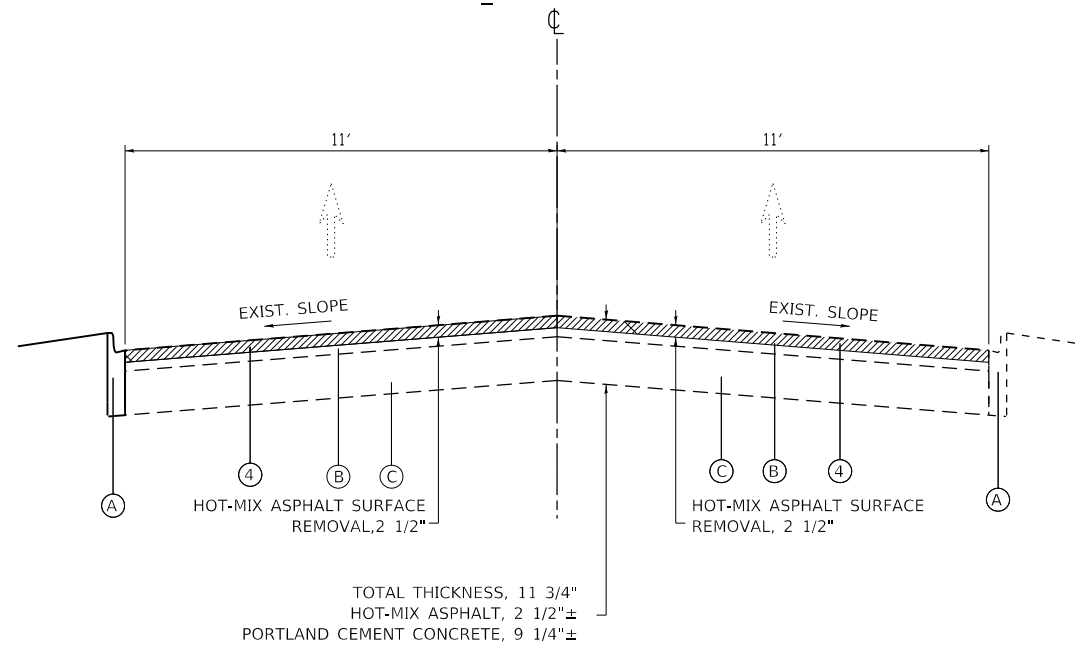
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PALATINE ROAD - S. FRONTAGE RD (N. VERDE AVE TO PALATINE ROAD)
TYPICAL SECTIONS**

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
--------	-------	----	--------	------	----	------

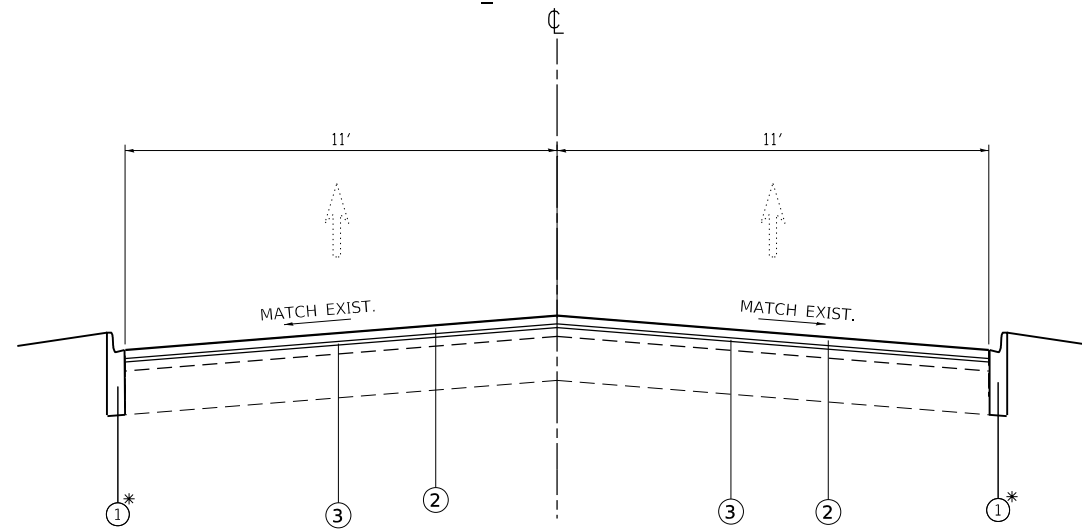
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-100-R5&SW	COOK	30	8
			CONTRACT NO. 62J86	
		ILLINOIS	FED. AID PROJECT	

PALATINE RD_SOUTH FRONTAGE ROAD



EXISTING TYPICAL SECTION
FROM STA. 18+83.90 TO 43+53

PALATINE RD_SOUTH FRONTAGE ROAD



PROPOSED TYPICAL SECTION
FROM STA. 18+83.90 TO 43+53

1 PAVEMENT WIDTH VARIES BETWEEN 22' TO 16' IN THE LAST 200 FT OF THE PROJECT LIMIT

LEGEND - EXISTING:

- (A) EXISTING COMB. CURB AND GUTTER, (VARIES)
- (B) EXISTING H.M.A. VARIES 2"-2 1/2" (±)
- (C) EXISTING PCC BASE COURSE, VARIES 9/4"- 11" (±)
- (D) HMA SHOULDER
- (E) AGGREGATE SHOULDER

NOTES:

- 1.) THE CONTRACTOR SHALL PATCH FIRST BEFORE MILLING.
- 2.) THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL - 4.75, N50.

LEGEND - PROPOSED

- ① PROPOSED COMB. CURB AND GUTTER REMOVAL & REPLACEMNT
- ② PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D" 9.5, N50, 1 3/4"
- ③ PROPOSED POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL - 4.75, N50, 3/4"
- ④A PROPOSED LONGITUDINAL PARTIAL DEPTH PATCHING, 3"
- ④ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- ⑤ PROPOSED PCC SURFACE REMOVAL, VAR DEPTH (SEE BD-33)
- ⑥ GRADING AND SHAPING SHOULDERS
- ⑦ AGGREGATE WEDGE SHOULDER, TYPE B

* PROPOSED COMB. CURB AND GUTTER, (VARIES) SHALL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.

MODEL: Default
FILE: h:\mhc\p\palatineroad\palatineroad\p\DOT\Documents\DOT_Offices\Drawings\1\Projects\1826520\CADD\Drawings\1826520-sh-detailed.dgn

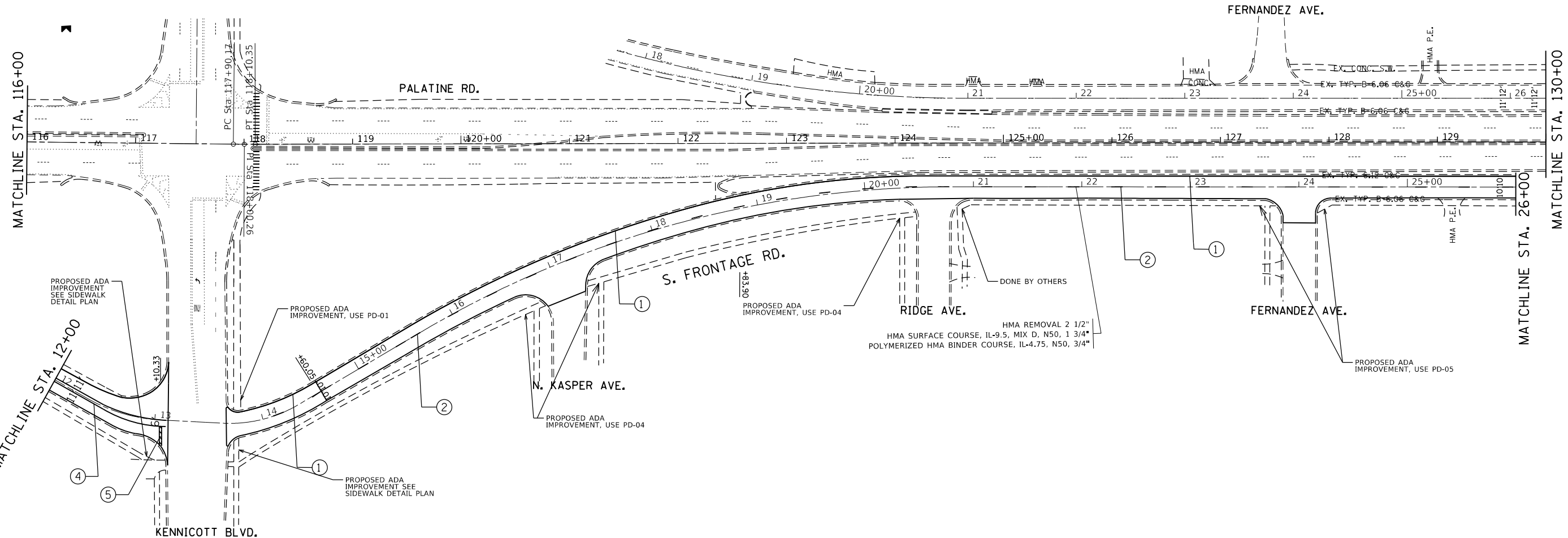
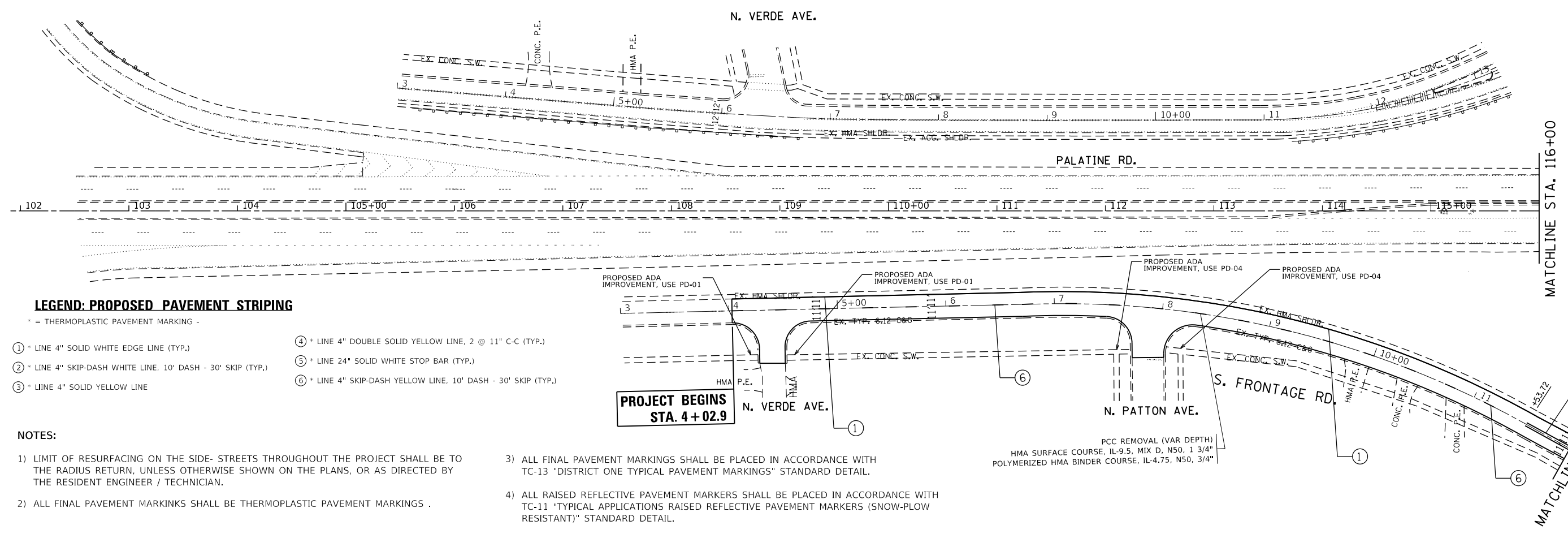
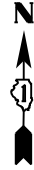
USER NAME = lababidim	DESIGNED -	REVISED -
PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/13/2019	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PALATINE ROAD - S. FRONTAGE RD (N. VERDE AVE TO PALATINE ROAD)
TYPICAL SECTIONS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-100-R5&SW	COOK	30	9
CONTRACT NO. 62J86				
		ILLINOIS	FED. AID PROJECT	



MODEL: Default
 FILE: \\nrcs\p\pub\barroom.dwg
 PROJECT: \\nrcs\p\pub\barroom\DOT Documents\DOT Office\Drawings\1\Projects\1265270\CaddData\Design\1265270-sh-1-bar.dwg

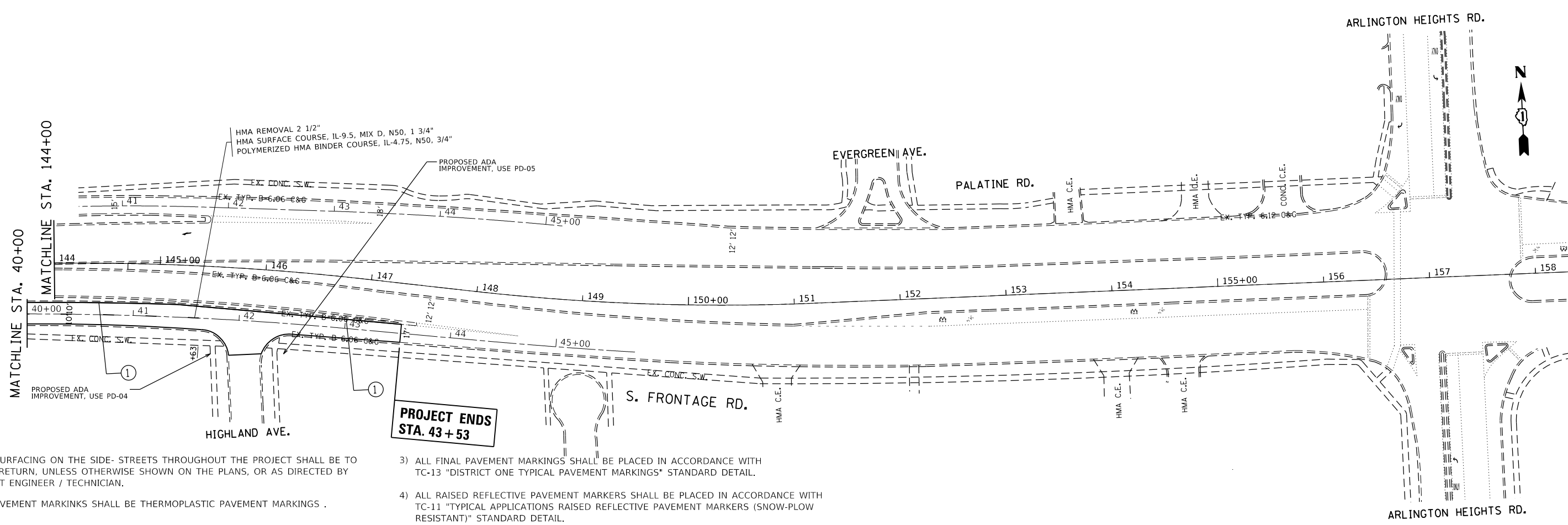
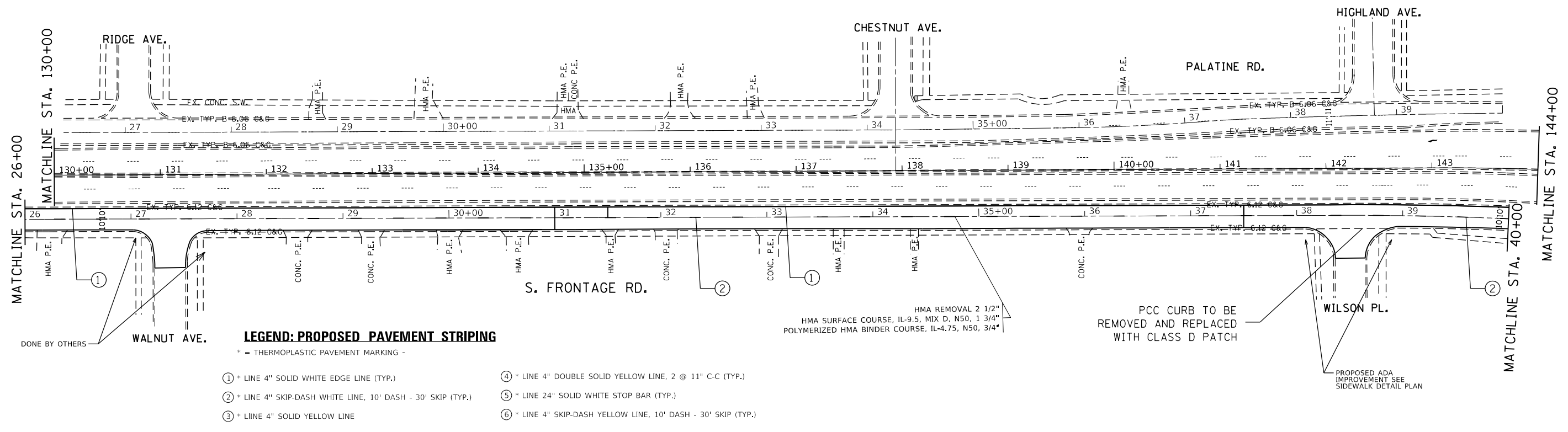
USER NAME = lababidim	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/13/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY PLAN FRONTAGE RD. NORTH VERDE AVE TO PALATINE RD.			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-100-R5&SW	COOK	30	10
CONTRACT NO. 62J86				
ILLINOIS		FED. AID PROJECT		

MODEL: Default
FILE: Northc_001.dwg
PROJECT: D:\PROJECTS\265270\CADDData\Design\1265270-sh-c-lb-r.dwg
DATE: 12/13/2019



NOTES:

- LIMIT OF RESURFACING ON THE SIDE- STREETS THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE SHOWN ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER / TECHNICIAN.
- ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS.
- ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH TC-13 "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" STANDARD DETAIL.
- ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH TC-11 "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" STANDARD DETAIL.

USER NAME = lababitim	DESIGNED -	REVISD -
	DRAWN -	REVISD -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISD -
PLOT DATE = 12/13/2019	DATE -	REVISD -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

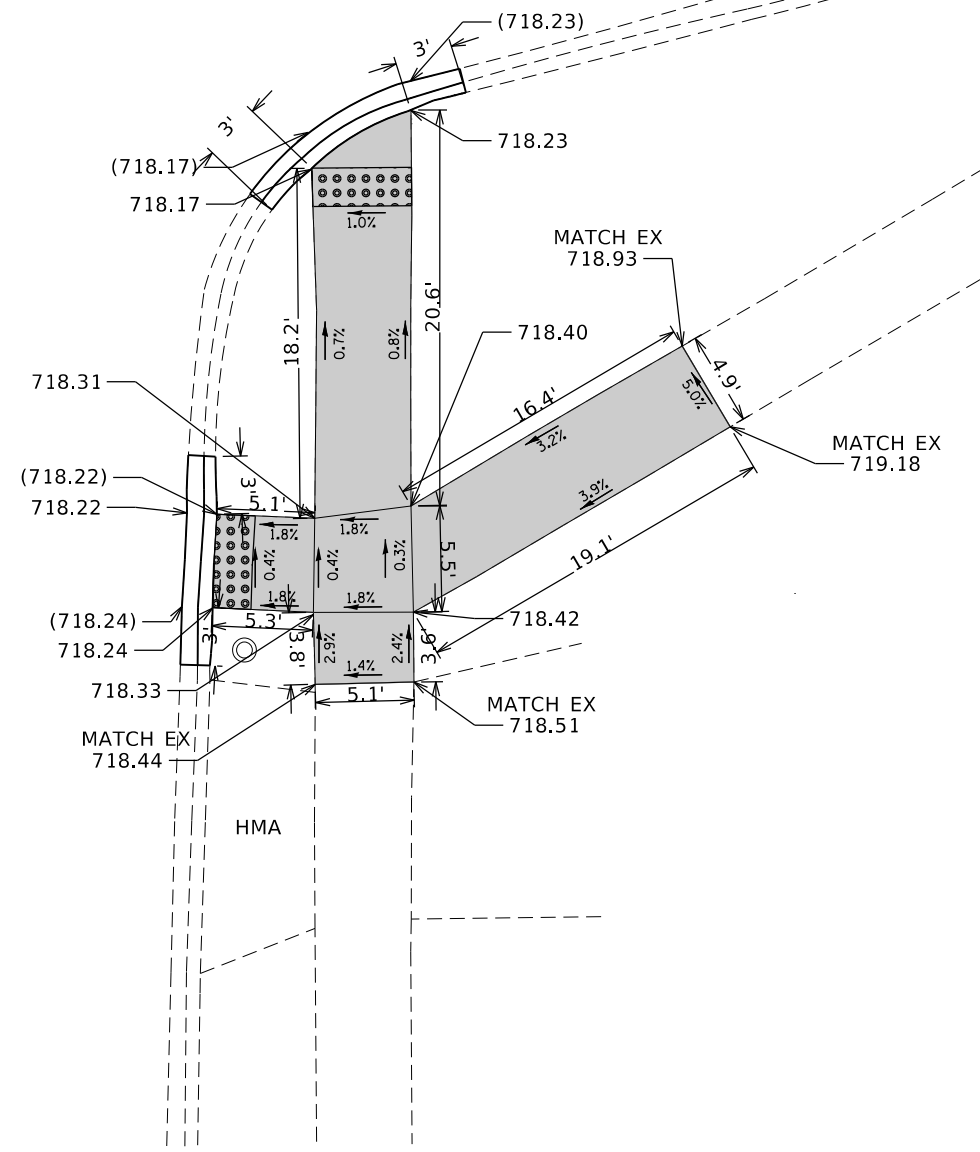
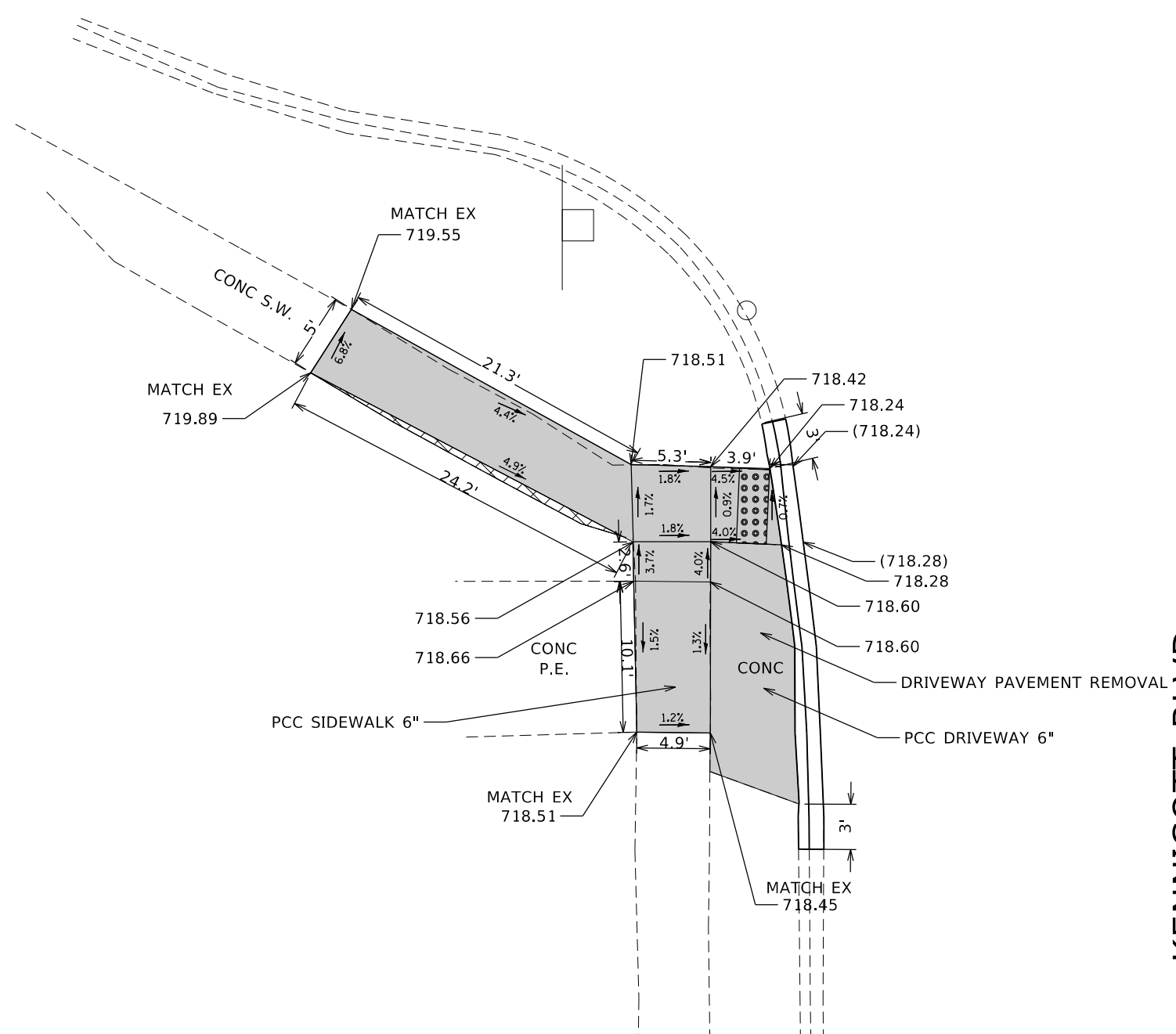
**ROADWAY PLAN FRONTAGE RD.
NORTH VERDE AVE. TO PALATINE RD.**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-100-R5&SW	COOK	30	11
CONTRACT NO. 62J86				
ILLINOIS FED. AID PROJECT				

S. FRONTAGE RD.

KENNICOTT BLVD.



LEGEND

- xx.xx' EXISTING LENGTH
- PROPOSED SIDE CURB
- () EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 718.905
 BENCHMARK : CUT @ WEST AREA POWER LIGHT POLE BASE
 LOCATION : NORTH EAST CORNER W. PALATINE (FRONTAGE RD & KENNICOTT)

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

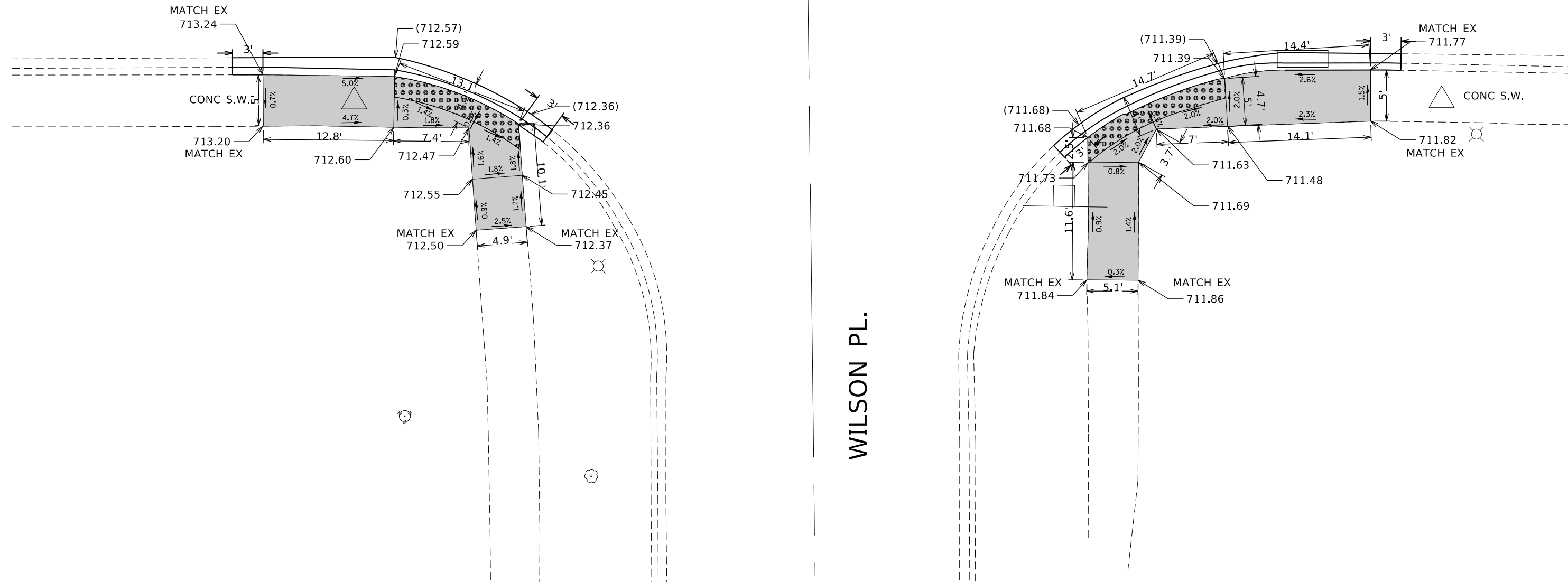
PALATINE ROAD – SOUTH FRONTAGE ROAD AT KENNICOTT BLVD
 CURB RAMP DETAIL (SE & SW CORNER)

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-100-R5&SW	COOK	30	12
CONTRACT NO. 62J86				
SCALE:		SHEET	OF	SHEETS
		STA.	TO	STA.
ILLINOIS FED. AID PROJECT				

MODEL: Default
 FILE: \\blm1c1-prod\pub\baroncom\ada\illinois\pcp\RWIDOT\Documents\DOT_Offices\Draftet_1\Projects\12165270\CaddData\Design\12165270-sh-rsch\sheet\k.blg.dgn



S. FRONTAGE RD.



REFERENCE BENCHMARK ELEV 711.895
 BENCHMARK : SQUARE ON NORTH FACE OF CONCRETE L.P. BASE
 LOCATION : SOUTH EAST CORNER OF PALATINE RD / S. FRONTAGE RD & WILSON PL.

LEGEND

- xx.xx' EXISTING LENGTH
- PROPOSED SIDE CURB
- () EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

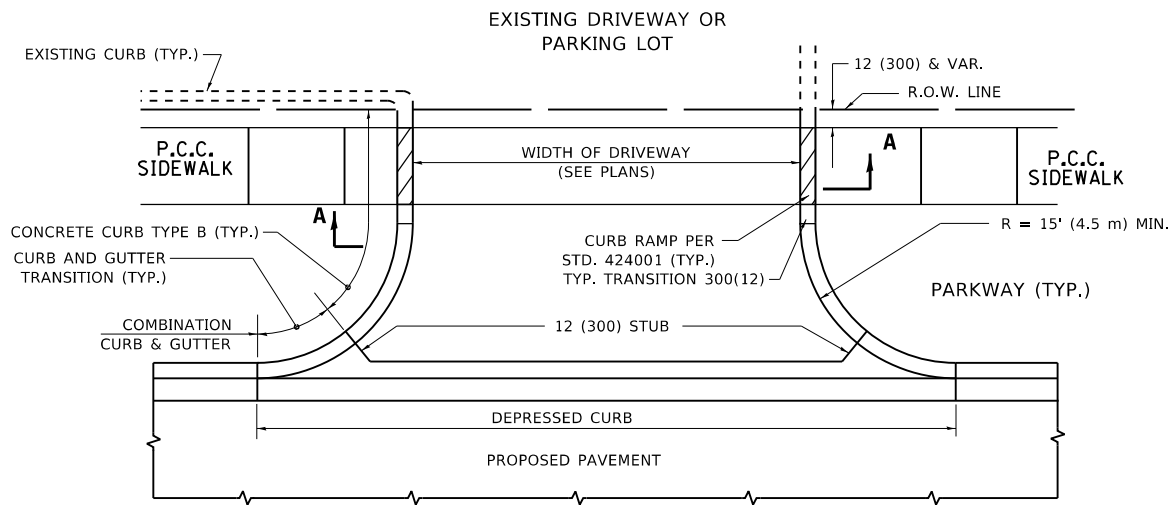
**PALATINE RD – SOUTH FRONTAGE ROAD AT WILSON PL.
CURB RAMP DETAIL (SE & SW CORNER)**

USER NAME = lababidim	DESIGNED -	REVISED -	
	DRAWN -	REVISED -	
PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -	
PLOT DATE = 12/13/2019	DATE -	REVISED -	

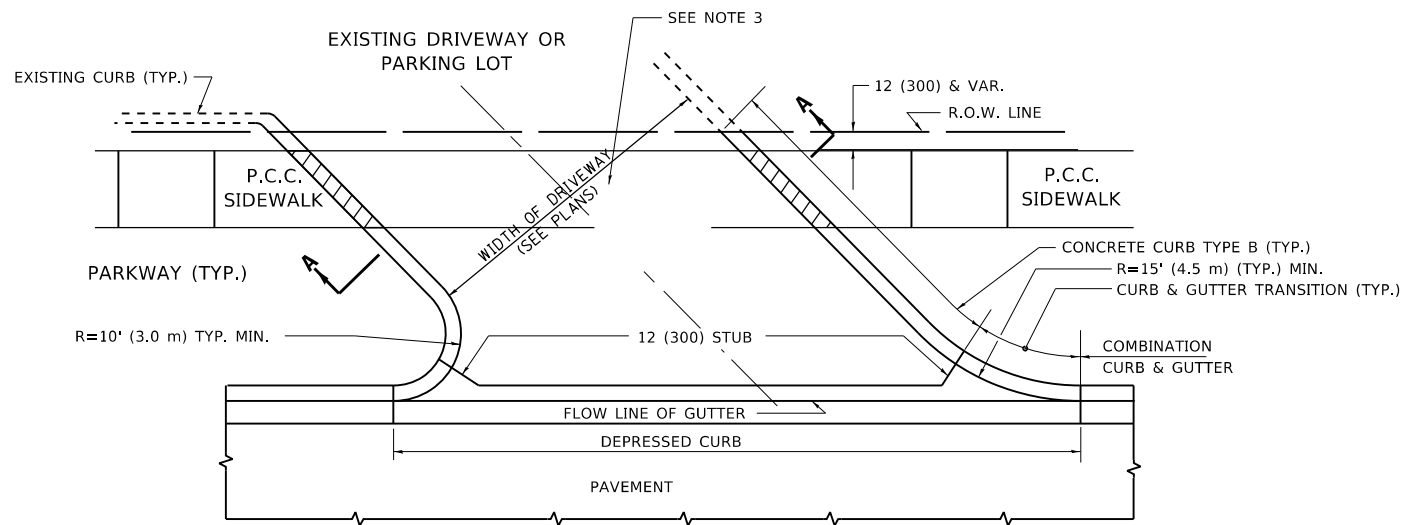
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-100-R5&SW	COOK	30	13
CONTRACT NO. 62J86				
ILLINOIS FED. AID PROJECT				

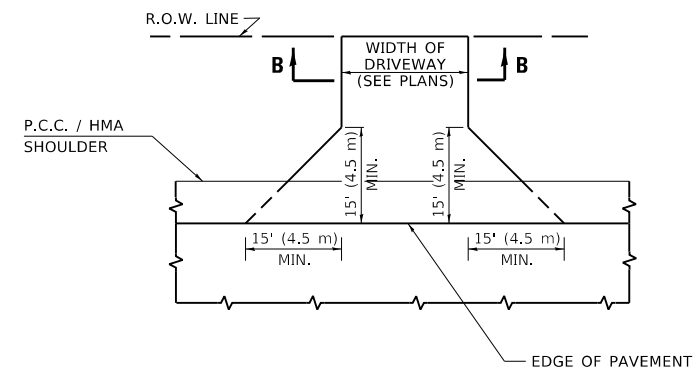
MODEL: Default
 FILE: \\blm1c1-p01\pub\arcom\ada\illinois\p01\DOT\Documents\DOT_Offices\Draft\1 - Projects\12165270\CaddData\Design\12165270-sh-sched\ile.dgn



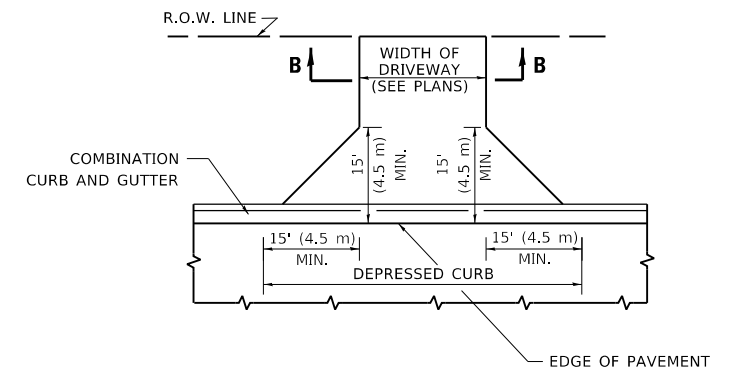
WITH CONCRETE CURB, TYPE B



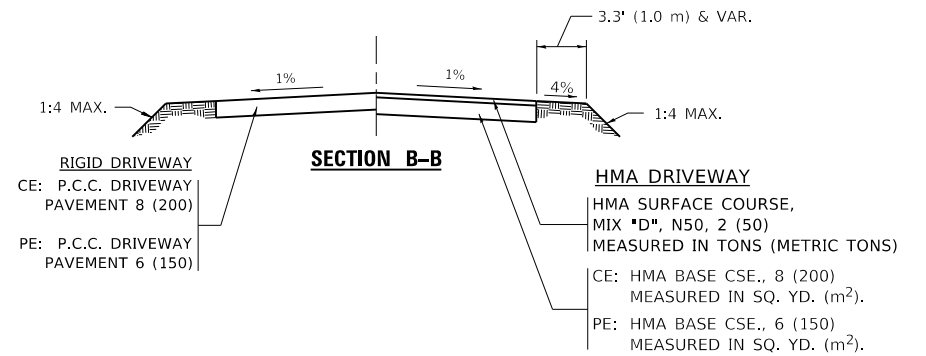
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C./HMA SHOULDER



ADJACENT TO CURB AND GUTTER



GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

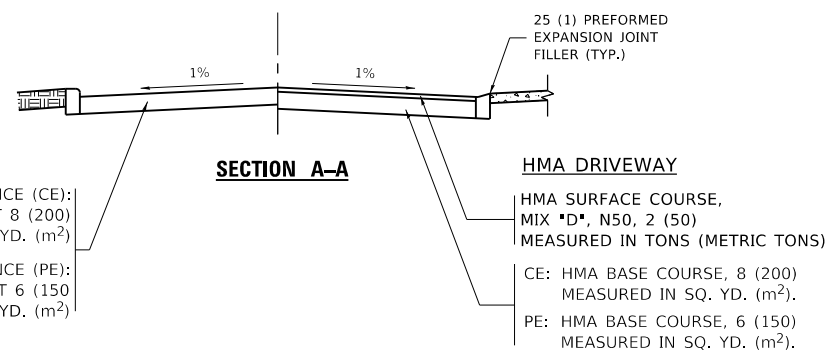
COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.



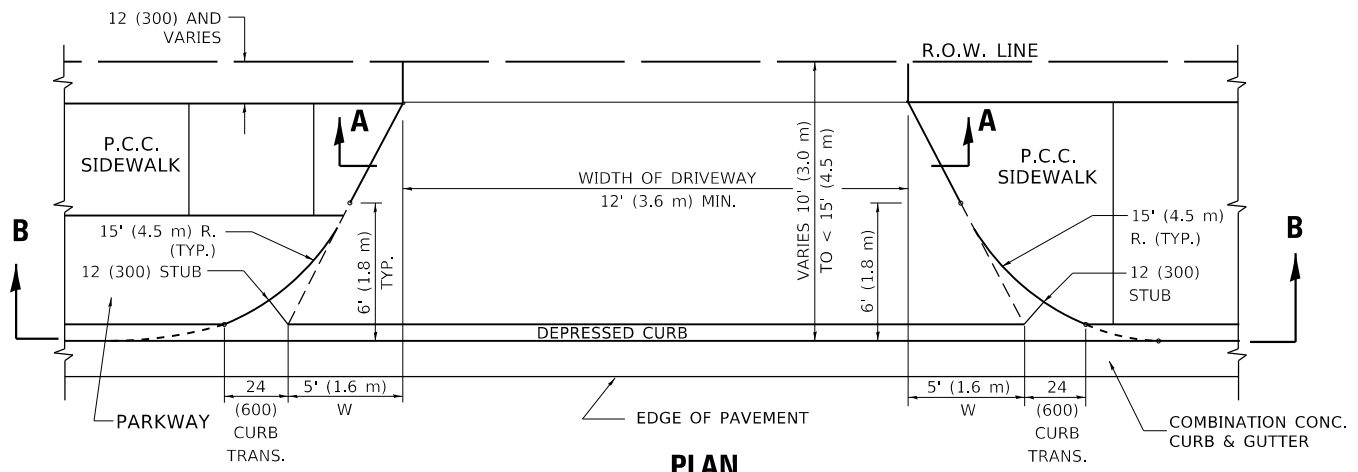
MODEL: Default
FILE: \\ns13\pcc\pub\pccroom\dta\illinois.gov\PROJECTS\DOT\Documents\DOT - Offices\District 1\Projects\ID 126520\CaddData\Design\DRS55.dgn

USER NAME = lababidim	DESIGNED - R. SHAH	REVISED - P. LoFLUER 04-15-03
	DRAWN -	REVISED - R. BORO 01-01-07
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED - R. BORO 06-11-08
PLOT DATE = 12/13/2019	DATE - 11-04-95	REVISED - R. BORO 09-06-11

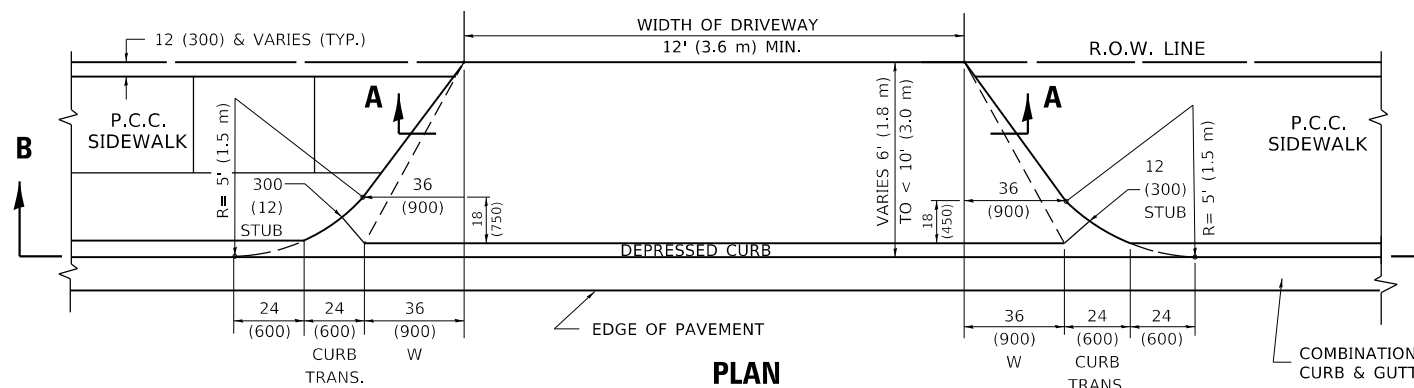
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DRIVEWAY DETAILS – DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER ≥ 15'(4.5m)			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

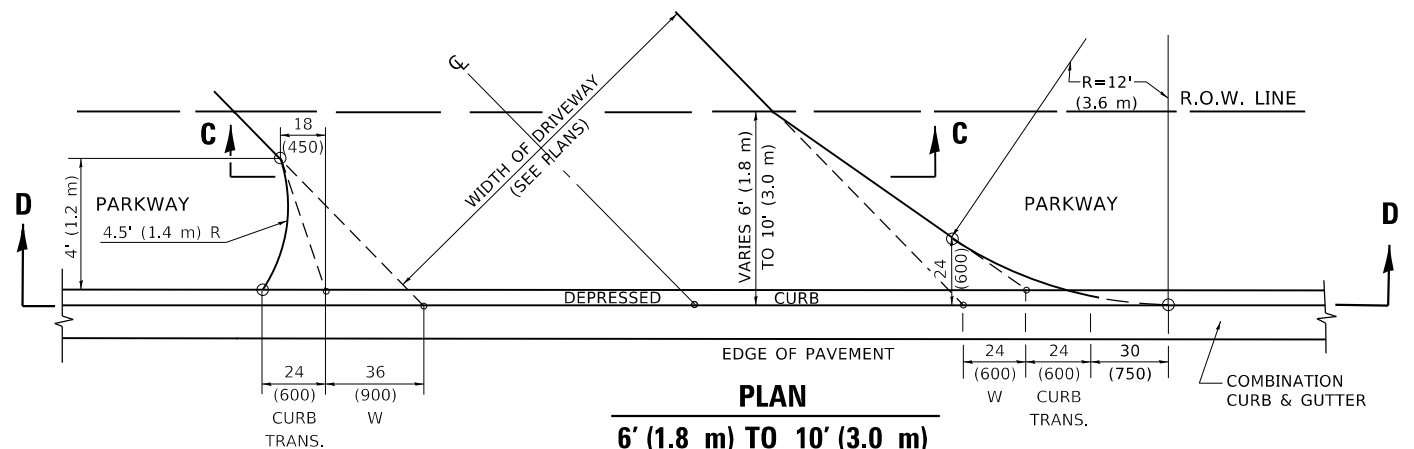
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-100-R5&5W	COOK	30	14
BD400-01 (BD-01)		CONTRACT NO. 62J86		
ILLINOIS		FED. AID PROJECT		



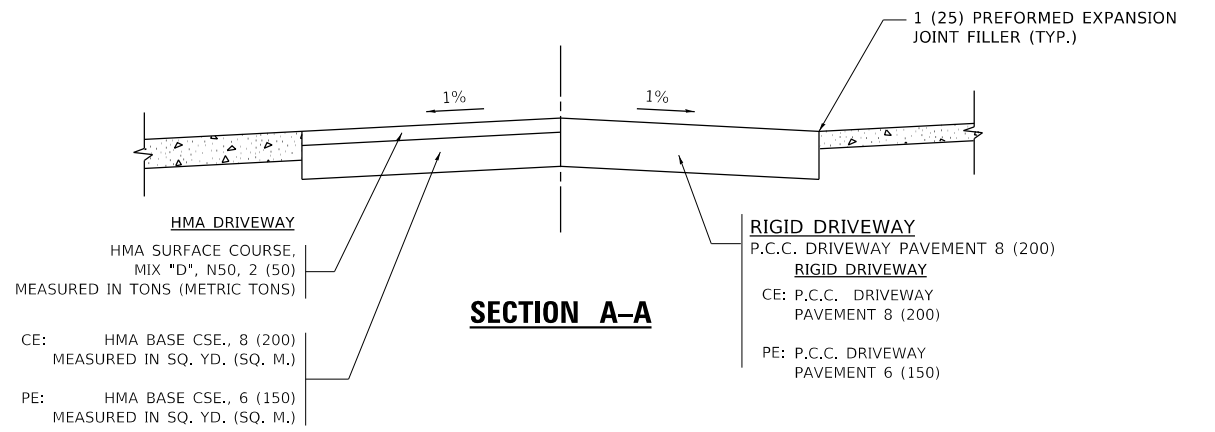
PLAN
10' (3.0 m) TO < 15' (4.5 m)



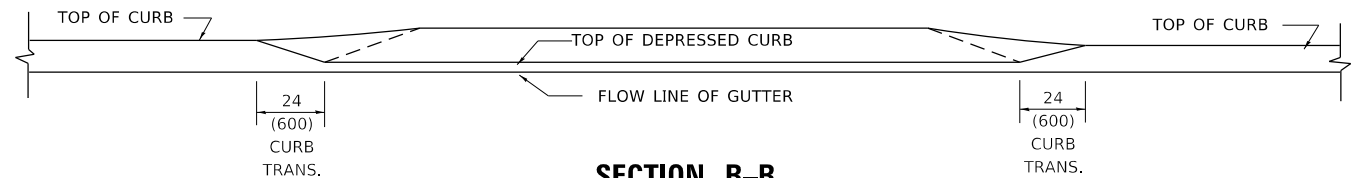
PLAN
6' (1.8 m) TO < 10' (3.0 m)



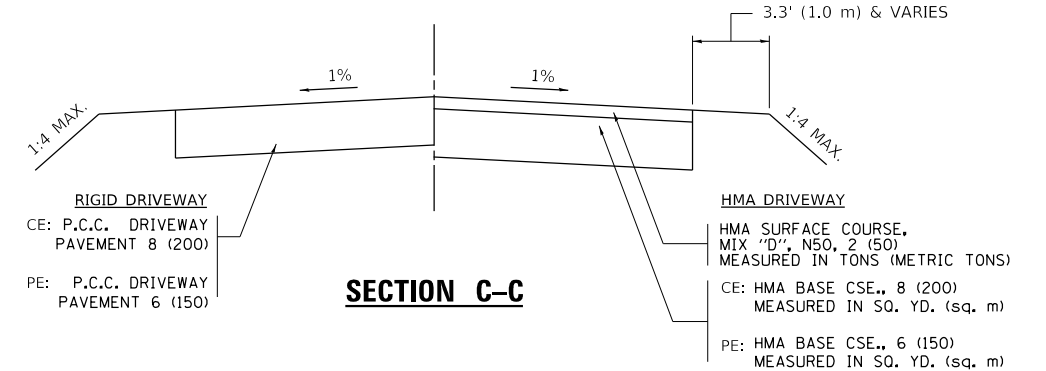
PLAN
6' (1.8 m) TO < 10' (3.0 m)



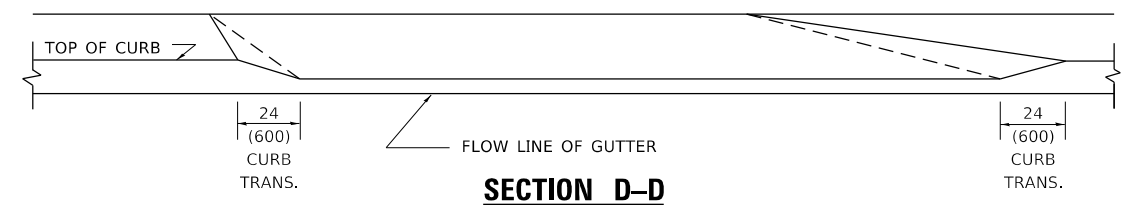
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

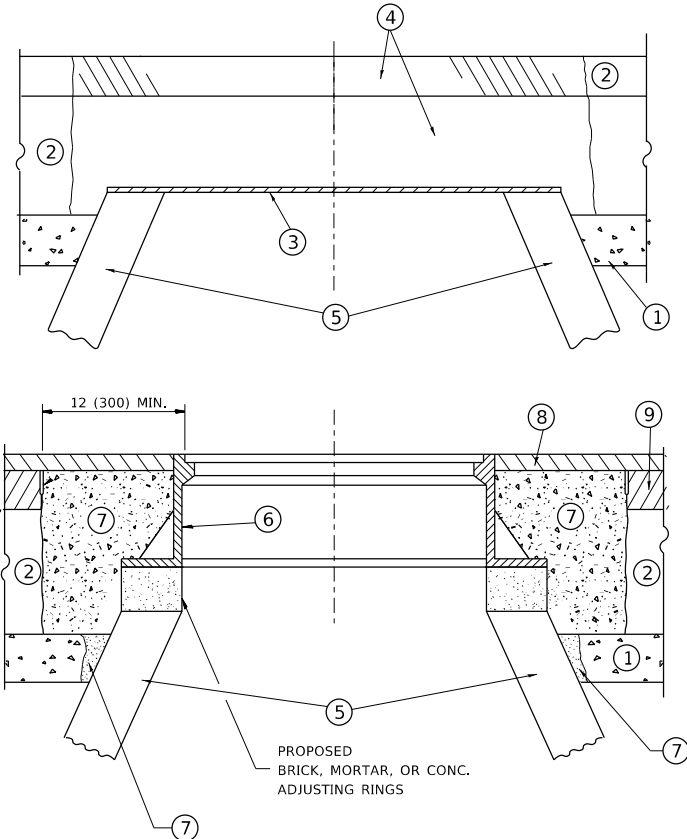
USER NAME = lababidi	DESIGNED - R. SHAH	REVISED - M. GOMEZ 04-06-01
PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED - P. LofLUER 04-15-03
PLOT DATE = 12/13/2019	CHECKED -	REVISED - R. BORO 01-01-07
	DATE - 11-06-95	REVISED - R. BORO 09-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DRIVEWAY DETAILS
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5m)**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-100-R5&5W	COOK	30	15
BD400-02 (BD-02)		CONTRACT NO. 62J86		
ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1 * CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT *THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER.*

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1 *CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

NOTES

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR *FRAMES AND LIDS TO BE ADJUSTED (SPECIAL).*

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

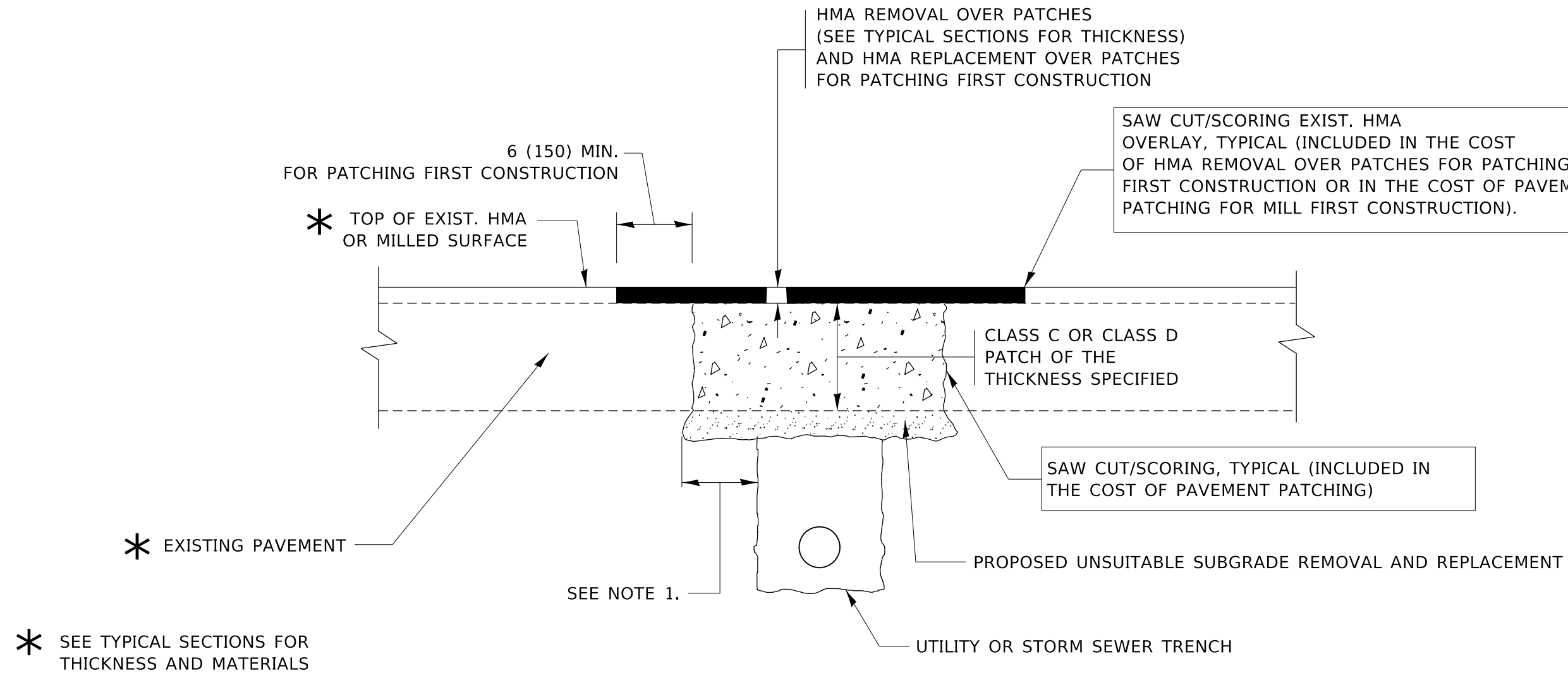
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-100-R5&SW	COOK	30	16
BD600-03 (BD-8)		CONTRACT NO. 62J86		
ILLINOIS FED. AID PROJECT				

USER NAME = lababidim	DESIGNED - R. SHAH	REVISED - R. WEDEMAN 05-14-04
	DRAWN -	REVISED - R. BORO 01-01-07
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - R. BORO 03-09-11
PLOT DATE = 12/13/2019	DATE - 10-25-94	REVISED - R. BORO 12-06-11

MODEL: Default
FILE: h:\miller\proj\lhamroom.dwg
PROJECT: R:\Projects\2019\DOT\DOT\Office\Drawings\1\Project\1\265270\CaddData\Design\BDS&S.dgn



NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: Default
FILE: hma19c_patchpavement.dwg
PROJECT: I:\Projects\126520\CADD\Design\Design\126520.dwg

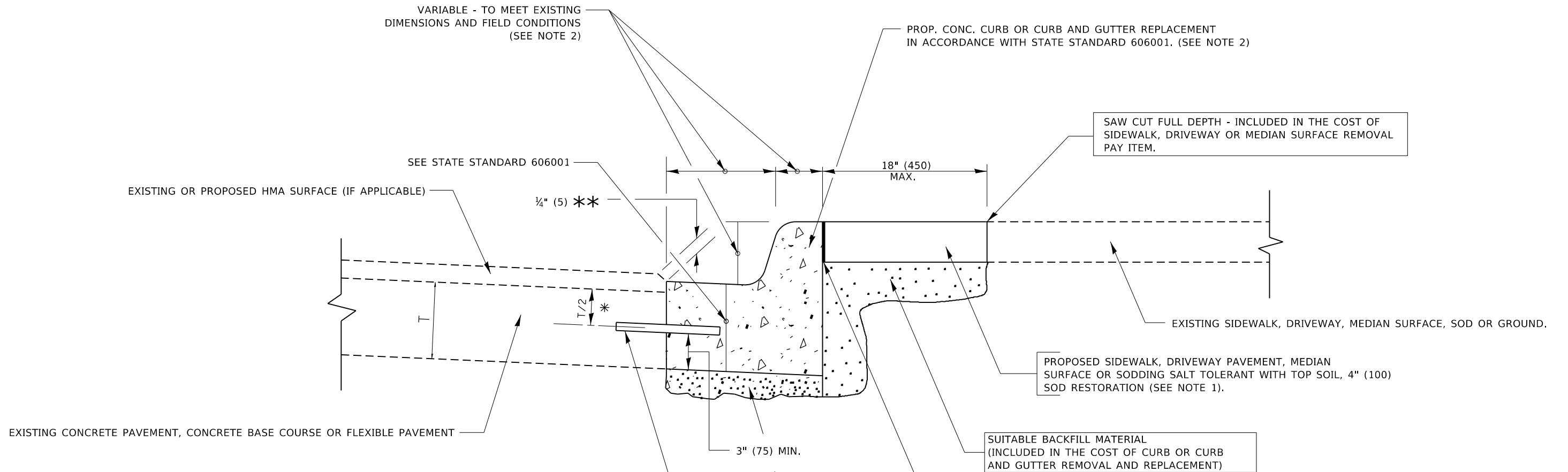
USER NAME = lababidim	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98
	DRAWN -	REVISED - R. BORO 01-01-07
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 09-04-07
PLOT DATE = 12/13/2019	DATE - 10-25-94	REVISED - K. ENG 10-27-08

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-100-R5&SW	COOK	30	17
BD400-04 (BD-22)		CONTRACT NO. 62J86		
ILLINOIS FED. AID PROJECT				



* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE:

1. SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,
2. FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED.
3. CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
4. FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
5. LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
6. THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
7. THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
8. THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE 3).

BASIS OF PAYMENT

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: Default FILE: A:\mhc\proj\pub\barroom.dwg\illinois.gov\pww\DOT\Documents\DOT_Offices\District 1\Projects\1265270\CADD\data\Design\DES24.dgn

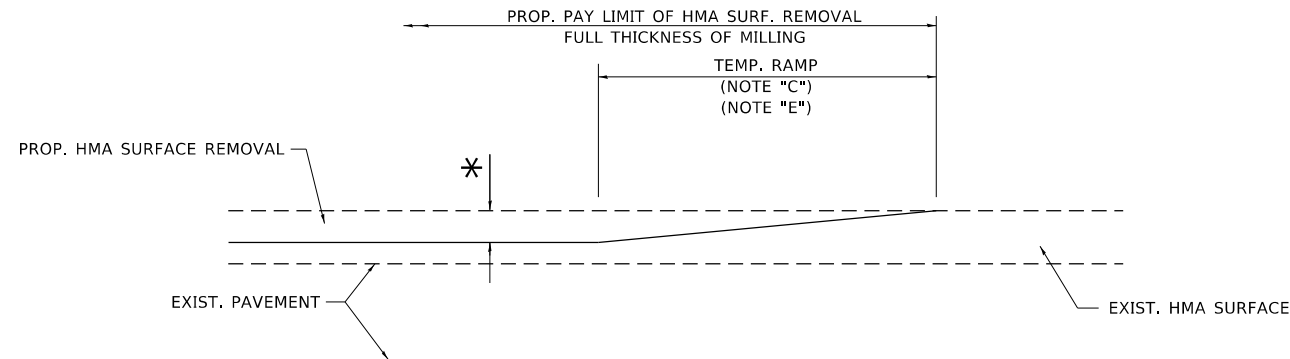
USER NAME = lababidim	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96
	DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - M. GOMEZ 01-22-01
PLOT DATE = 12/13/2019	DATE - 03-11-94	REVISED - R. BORO 12-15-09

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CURB OR CURB AND GUTTER
REMOVAL AND REPLACEMENT**

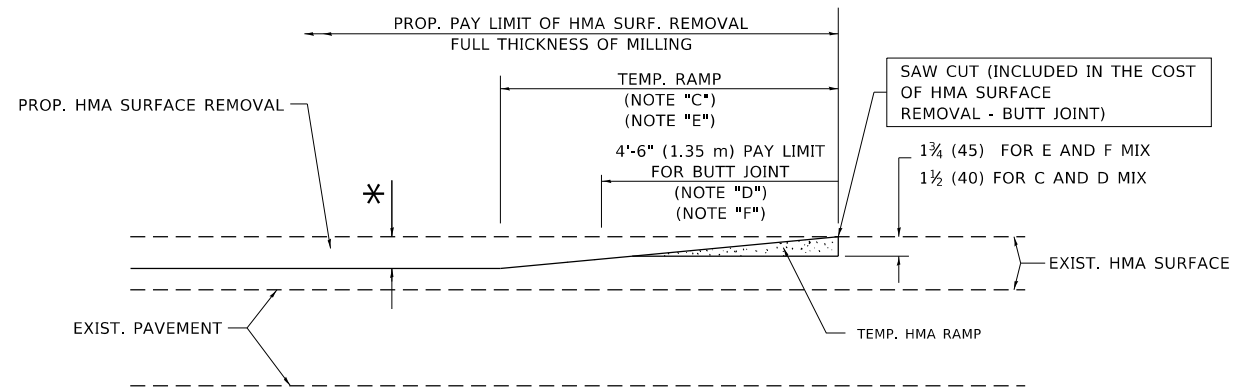
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-100-R5&5W	COOK	30	18
BD600-06 (BD-24)		CONTRACT NO. 62J86		
ILLINOIS FED. AID PROJECT				



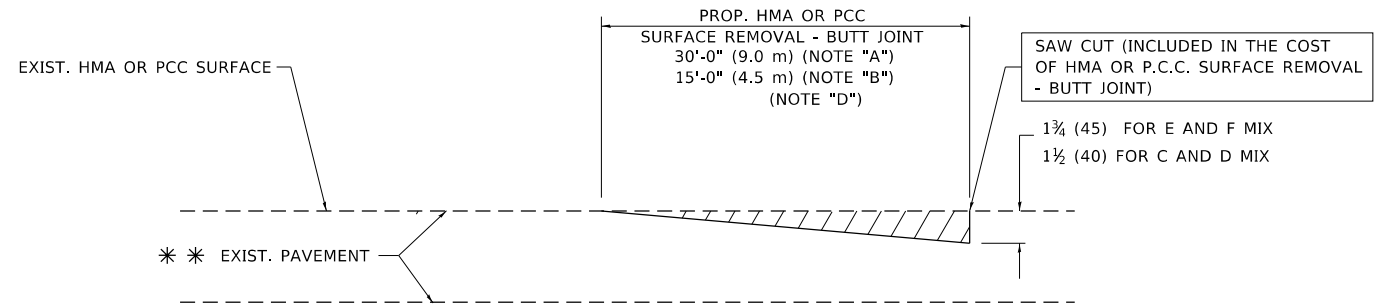
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

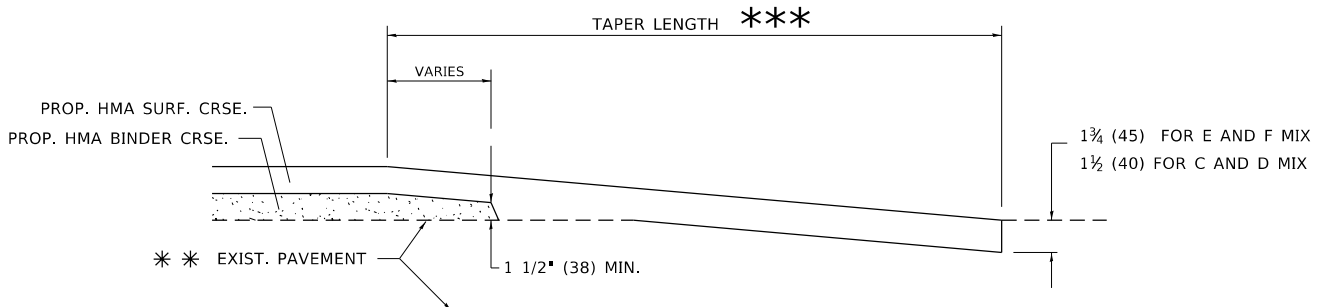


HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2
TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

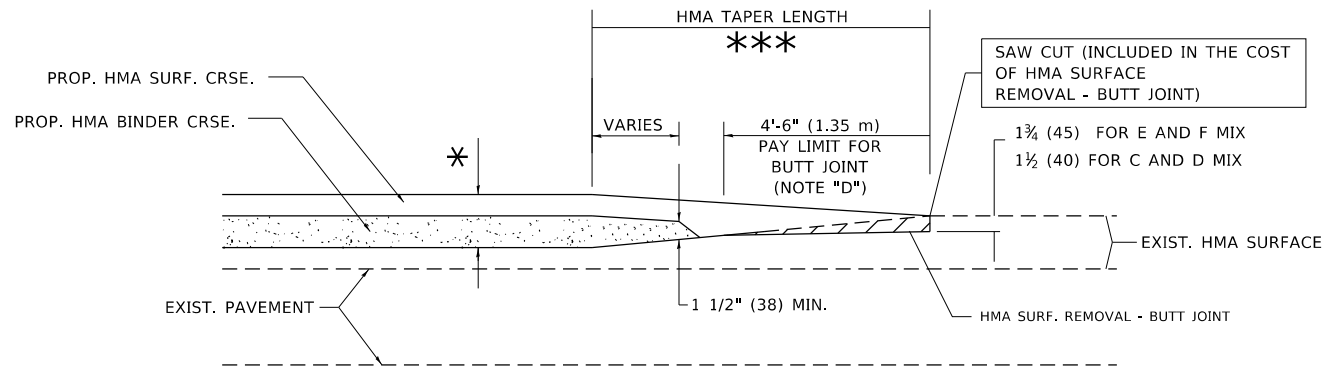
NOTES

- A. MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F. INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- G. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

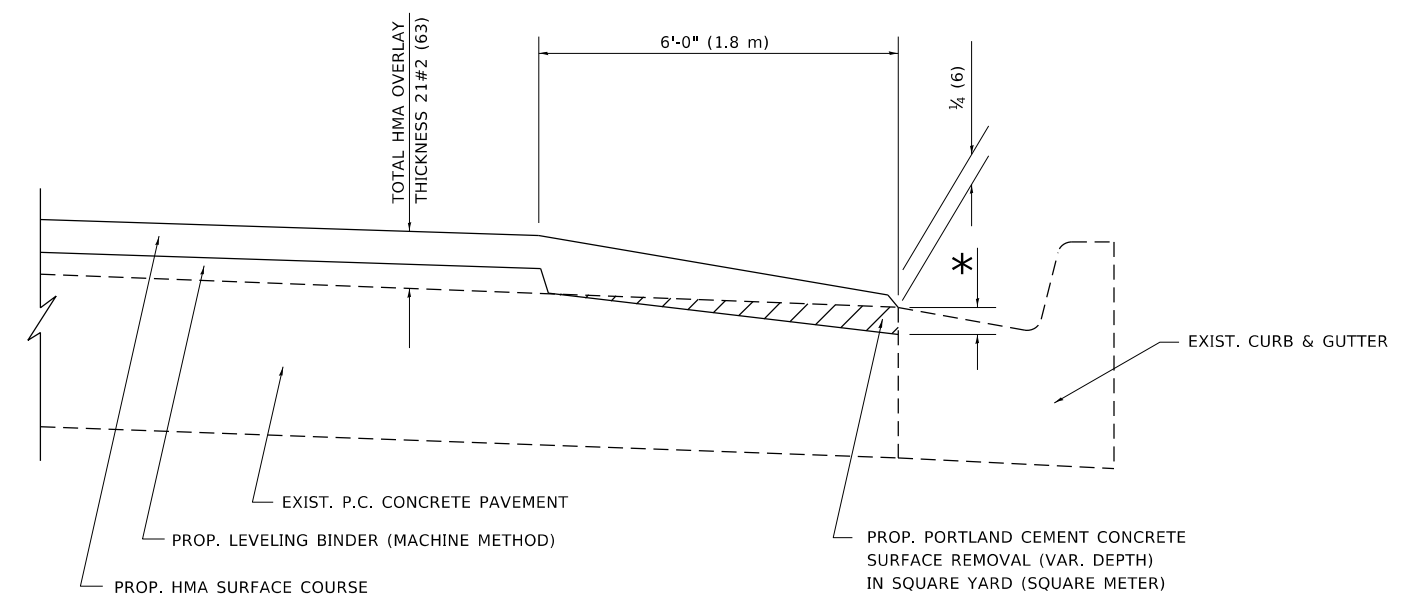
MODEL: Default
FILE: \\hpc\p\pub\hpc\room\dat\illinois\pcc\p\WIDOT\Documents\DOT_Offices\Illinet.1\Projects\126520\CaddData\Design\DES.dgn

USER NAME = lababidim	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
	DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
PLOT DATE = 12/13/2019	DATE - 06-13-90	REVISED - R.BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-100-R565W	COOK	30	19
BD400-05 BD32		CONTRACT NO. 62186		
ILLINOIS FED. AID PROJECT				



HMA SURFACE	LEVELING BINDER	MILLING AT GUTTER FLAG
MIX	THICKNESS	THICKNESS
C OR D	1 1/2 (38)	1 (25)
E	1 3/4 (44)	3/4 (19)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

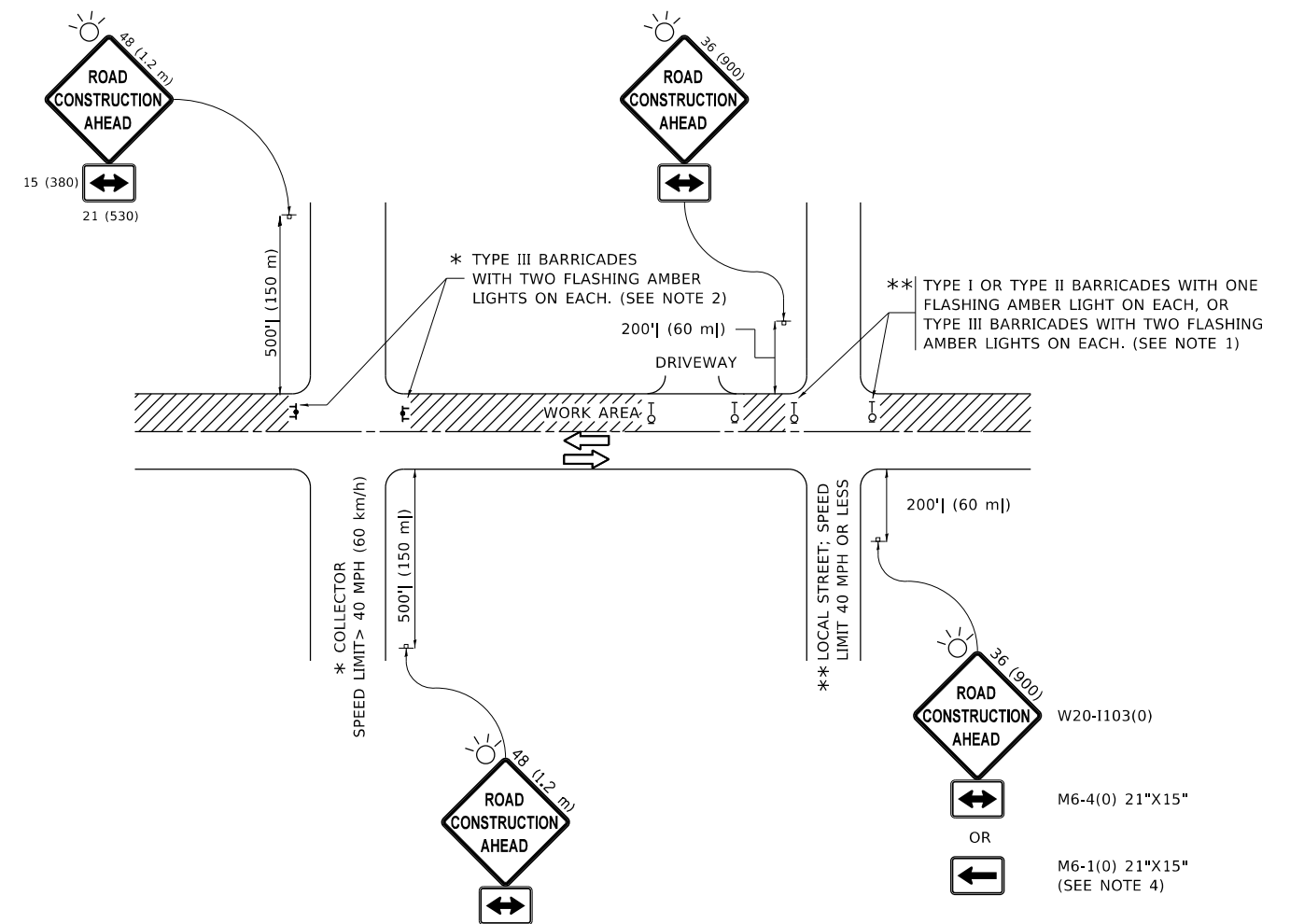
MODEL: Default
 FILE: hma_taper.dwg
 PROJECT: 2019-100-R5&SW
 SHEET: 30 OF 30

USER NAME = lababidim	DESIGNED - R. SHAH	REVISED - A. ABBAS 05-05-9
	DRAWN - JIS	REVISED - E. GOMEZ 12-21-00
PLOT SCALE = 100.0000' / in.	CHECKED - A. ABBAS	REVISED - R. BORO 01-01-07
PLOT DATE = 12/13/2019	DATE - 09-10-94	REVISED - JP CHANG 07-08-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

HMA TAPER AT EDGE OF P.C.C. PAVEMENT			
SCALE: NONE	SHEET 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-100-R5&SW	COOK	30	20
BD400-06 (BD33)		CONTRACT NO. 62J86		
ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default
 FILE: \\nashville-prod\planroom\ada\illinois.gov\PWIDOT\Documents\DOT_Offices\Director_1\Projects\ID1765270\CADD\Drawings\Design\DESIGN.dgn

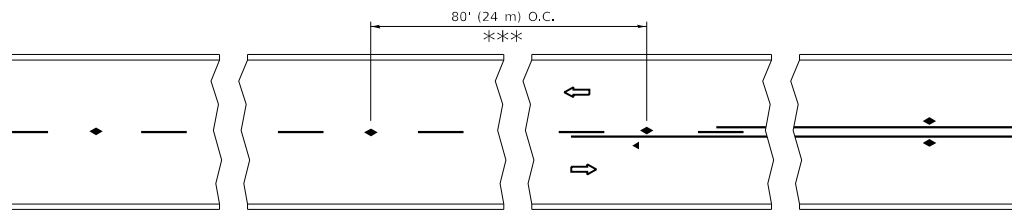
USER NAME = lababidim	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
	DRAWN -	REVISED - T. RAMMACHER 01-06-00
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
PLOT DATE = 12/13/2019	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

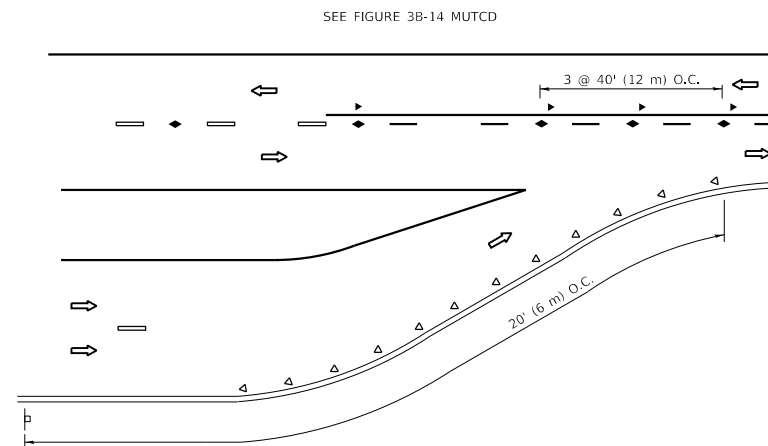
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-100-R5&SW	COOK	30	21
TC-10			CONTRACT NO. 62186	
ILLINOIS FED. AID PROJECT				

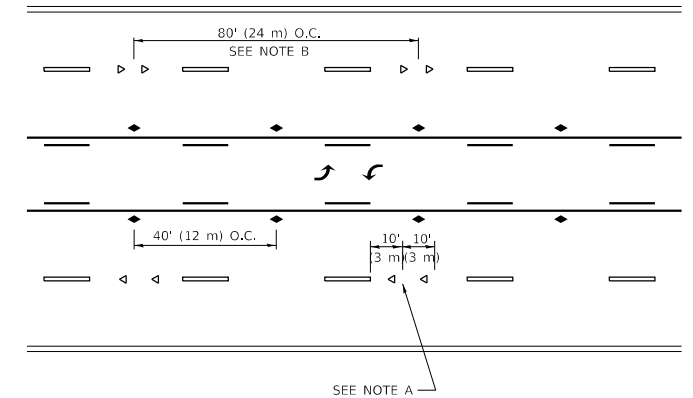


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

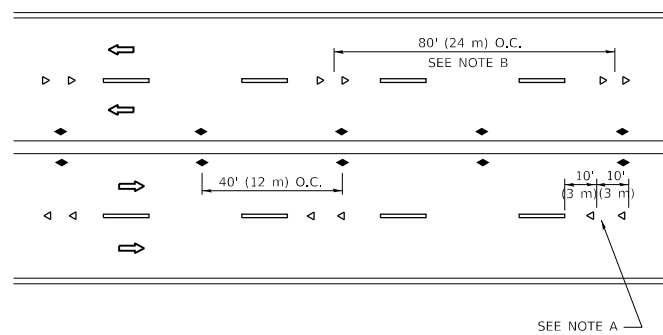
TWO-LANE/TWO-WAY



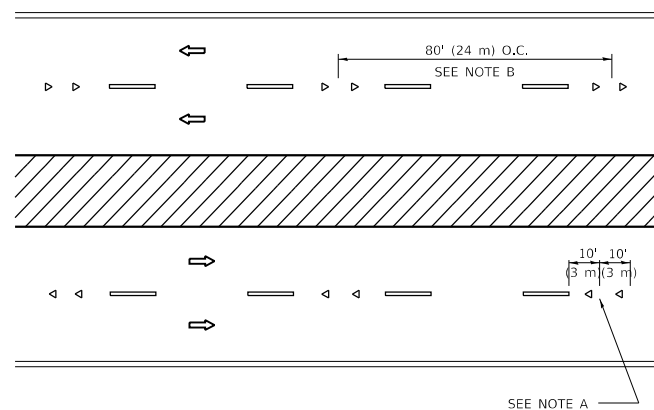
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

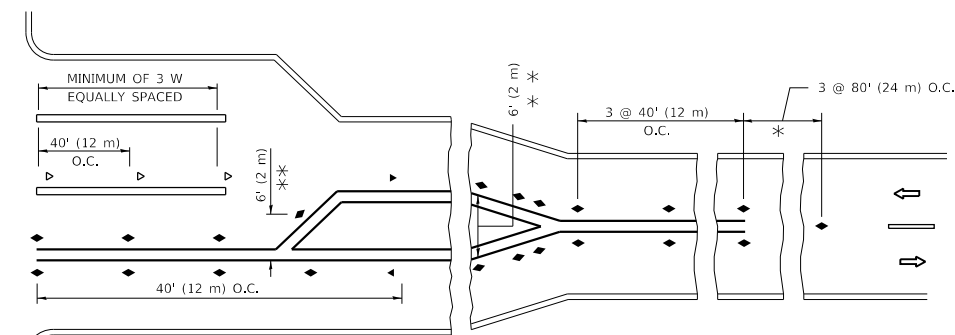
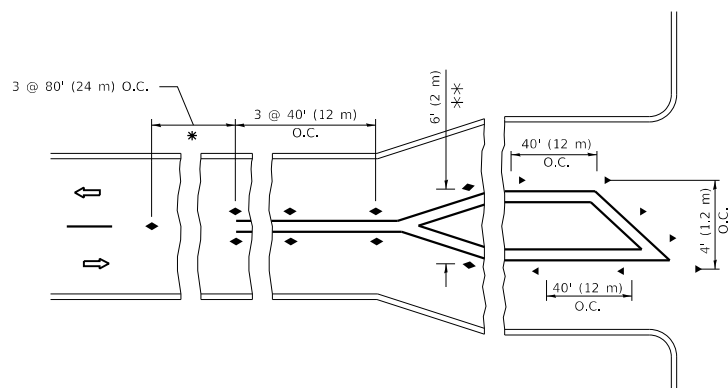
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

TURN LANES

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default
 FILE: \\blm01c-pub\pub\aroom\dat\illinois.gov\PI\DOT\Documents\DOT_Offices\Illindec_1\Projects\12165270\Cadd\Drawings\Design\DESIGN.dgn

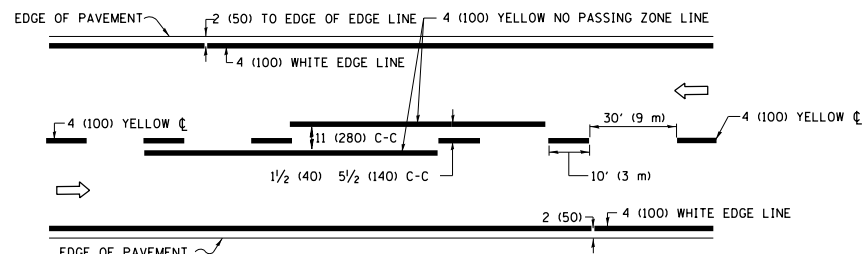
USER NAME = lababidim	DESIGNED -	REVISED - T. RAMMACHER 03-12-99
	DRAWN -	REVISED - T. RAMMACHER 01-06-00
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - C. JUCIUS 09-09-09
PLOT DATE = 12/13/2019	DATE -	REVISED - C. JUCIUS 07-01-13

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

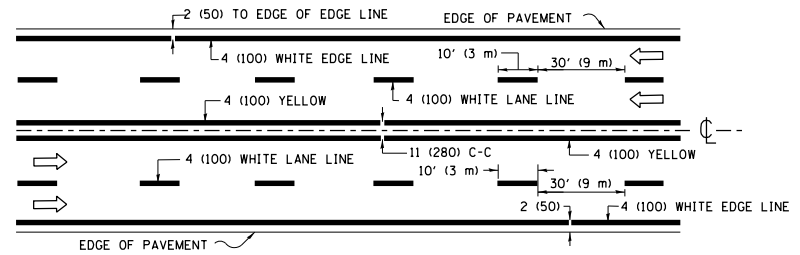
TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

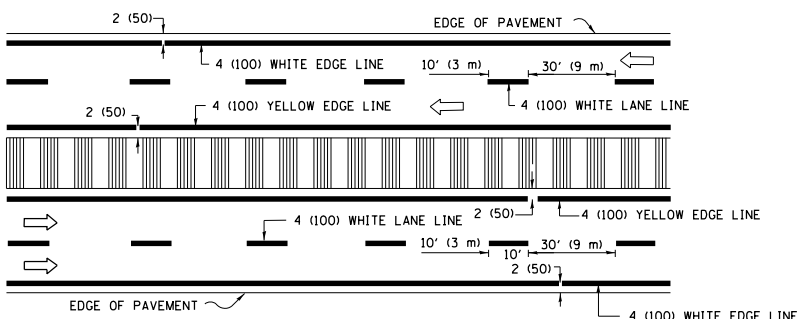
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-100-R5&SW	COOK	30	22
TC-11		CONTRACT NO. 62J86		
ILLINOIS		FED. AID PROJECT		



2-LANE ROADWAY

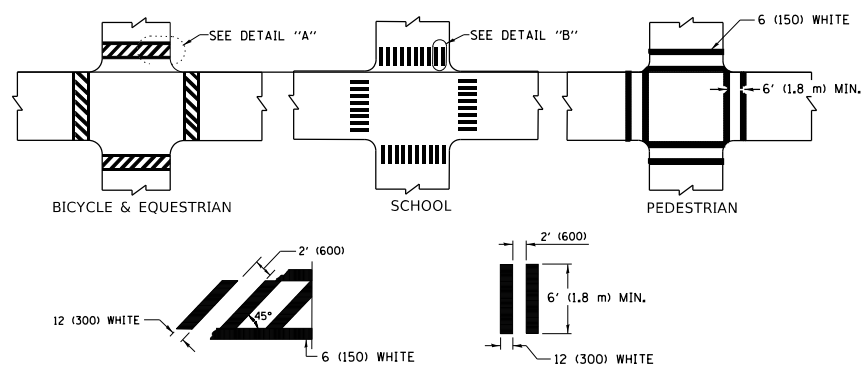


MULTI-LANE UNDIVIDED



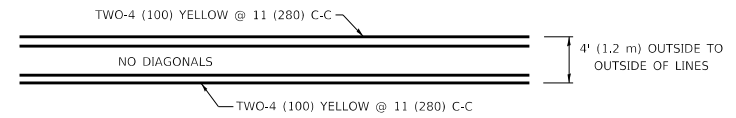
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

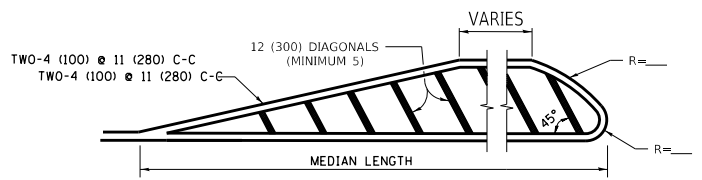


TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



4' (1.2 m) WIDE MEDIANS ONLY

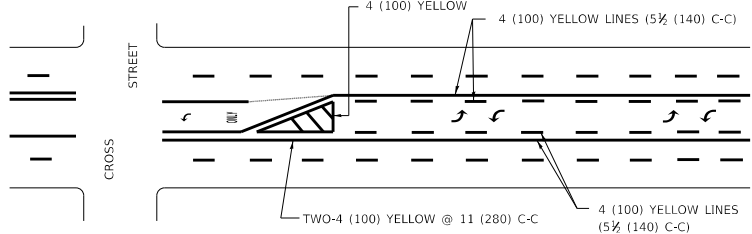


MEDIANS OVER 4' (1.2 m) WIDE

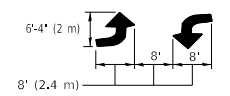
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

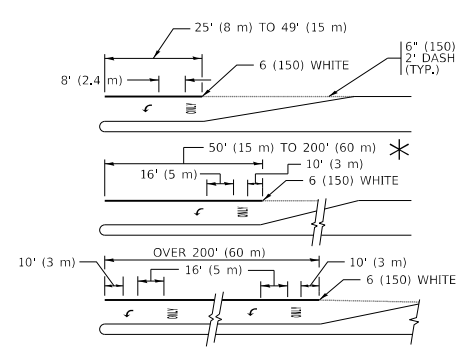
TYPICAL PAINTED MEDIAN MARKING



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



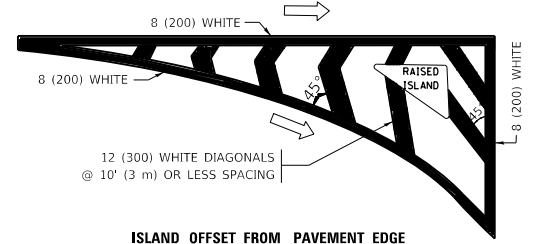
MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING



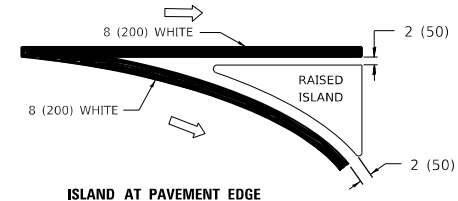
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

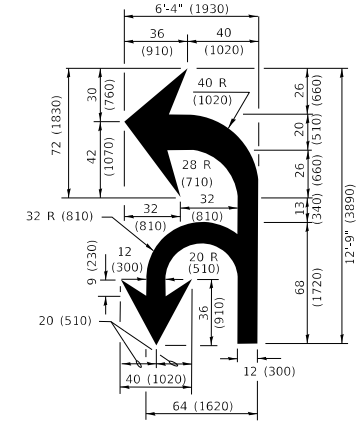
TYPICAL LEFT (OR RIGHT) TURN LANE TYPICAL TURN LANE MARKING



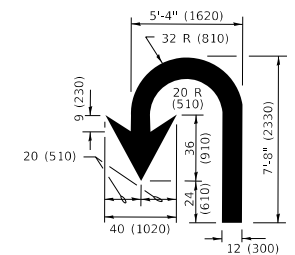
ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES "RR" IS 6' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default
FILE: h:\bldg\pavement\room\ad\illinois.gov\pavement\Documents\DOT Office\District 1\Projects\1216520\CADD\Drawings\Design\Dist1\54.dgn

USER NAME	DESIGNED	REVISION	DATE
= lababidim	- EVERS	- C. JUCIUS 09-09-09	
	-	- C. JUCIUS 07-01-13	
	-	- C. JUCIUS 12-21-15	
	-	- C. JUCIUS 04-12-16	

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET 1	OF 2 SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-100-R5&SW	COOK	30	23
TC-13		CONTRACT NO. 62J86		
ILLINOIS FED. AID PROJECT				

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

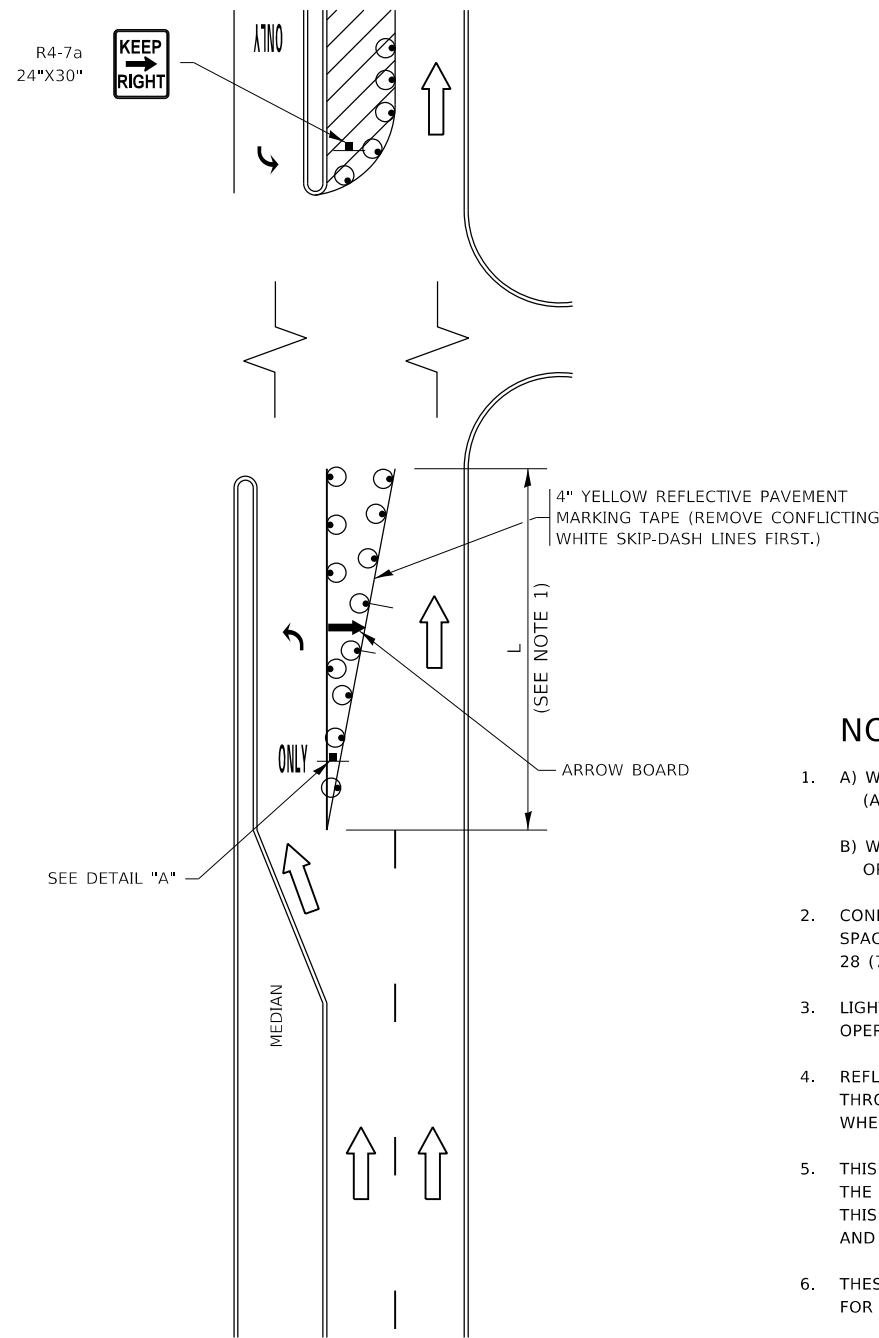


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

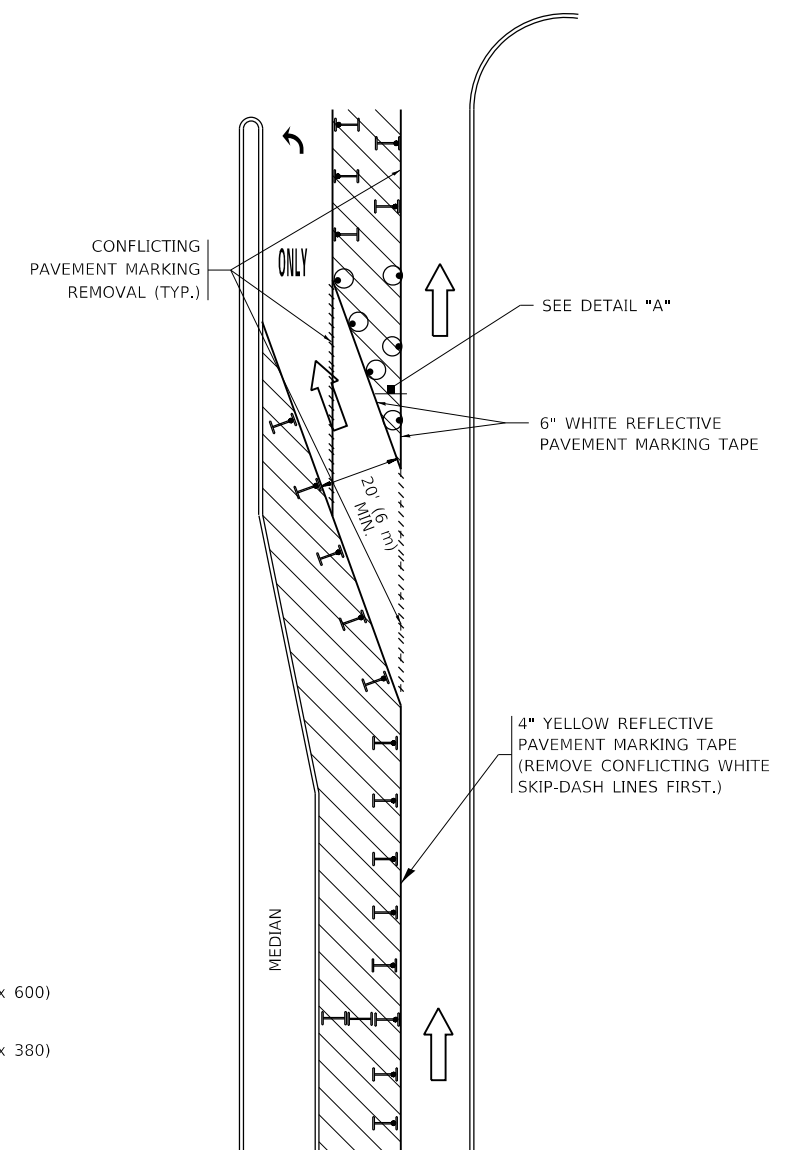
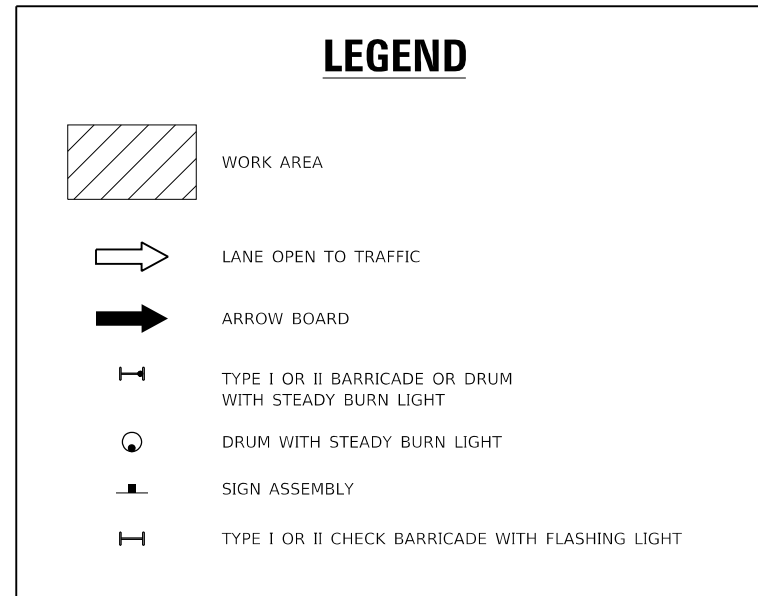
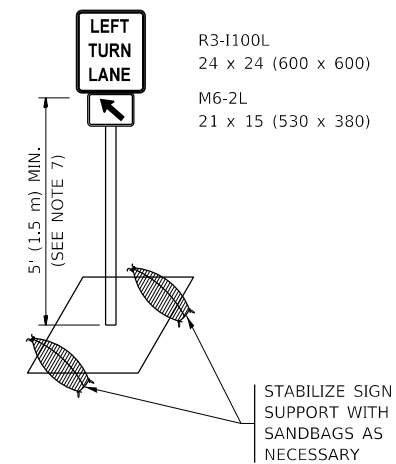


FIGURE 2



NOTES:

1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default
FILE: h:\mhc\p\pub\lanroom.d\at\illinois.gov\PIWDOT\Documents\DOT_Offices\Director_1\Projects\1265270\CaddData\Design\DF54.dgn

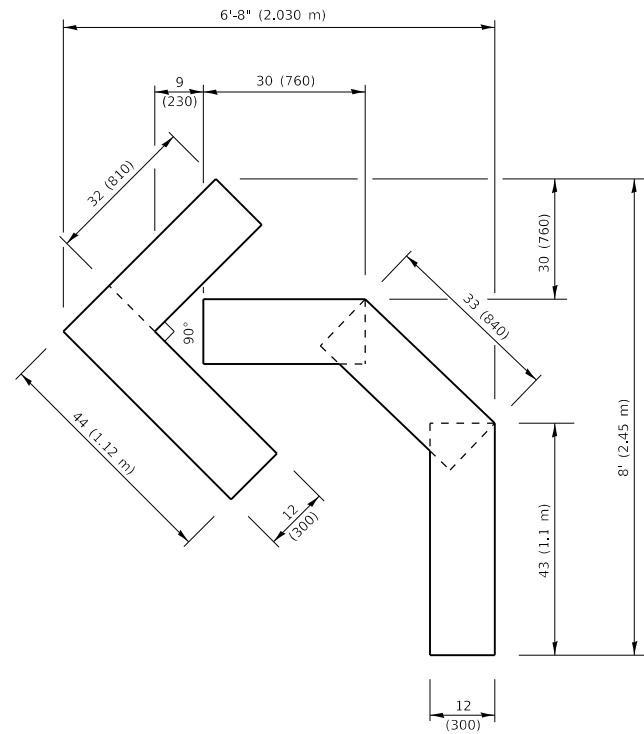
USER NAME = lababidim	DESIGNED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
	DRAWN - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13
PLOT SCALE = 100,0000' / in.	CHECKED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16
PLOT DATE = 12/13/2019	DATE - T. RAMMACHER 01-06-00	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)

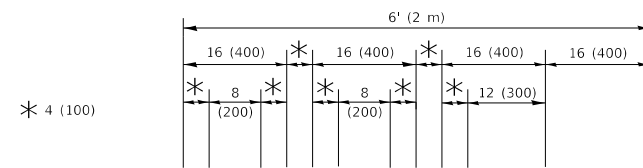
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-100-R5&SW	COOK	30	24
TC-14		CONTRACT NO. 62J86		
ILLINOIS FED. AID PROJECT				



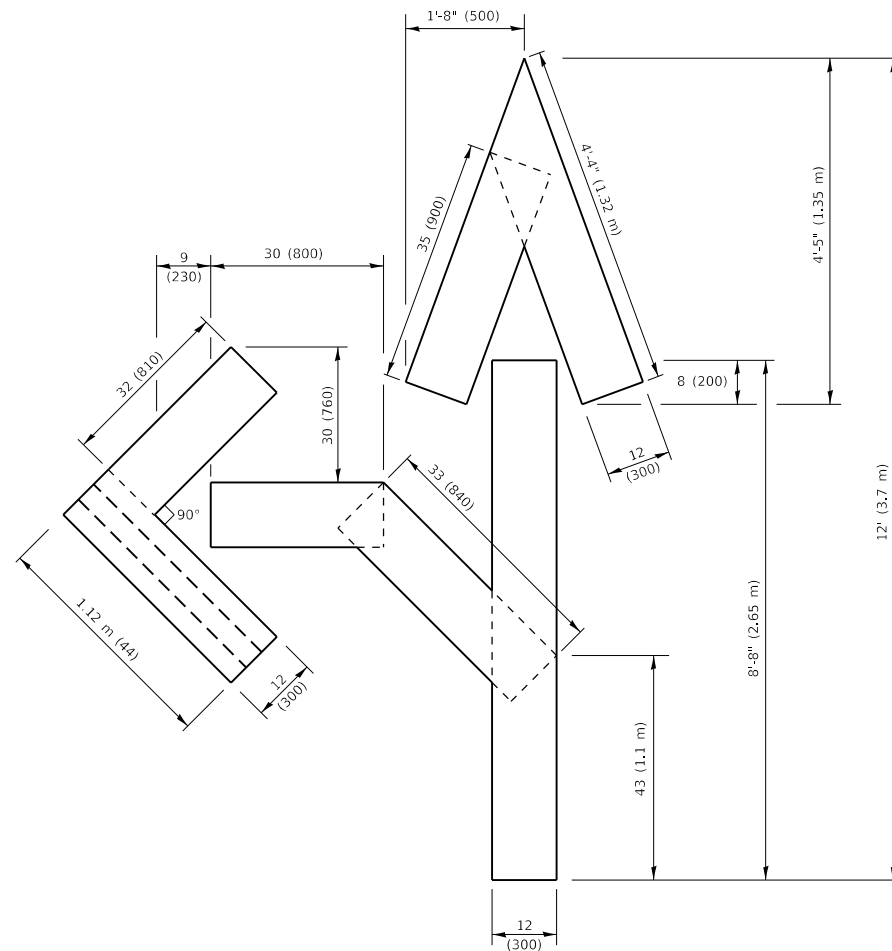
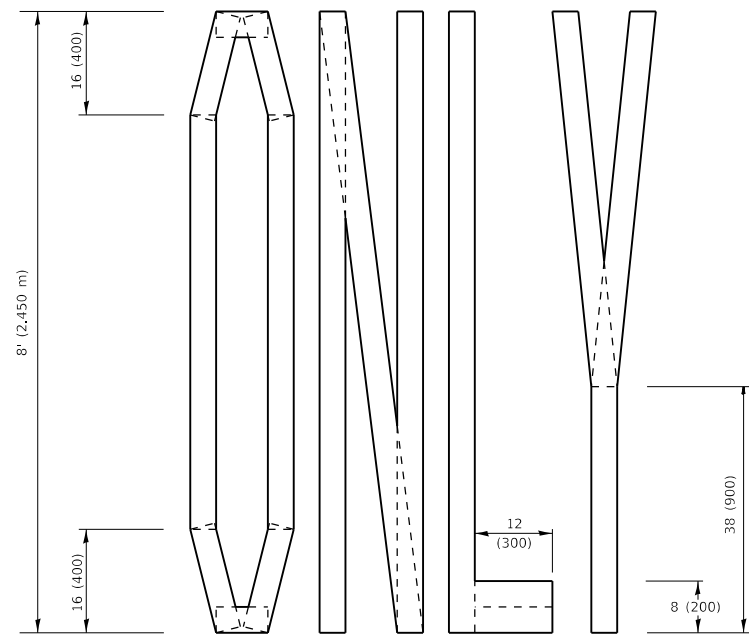
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

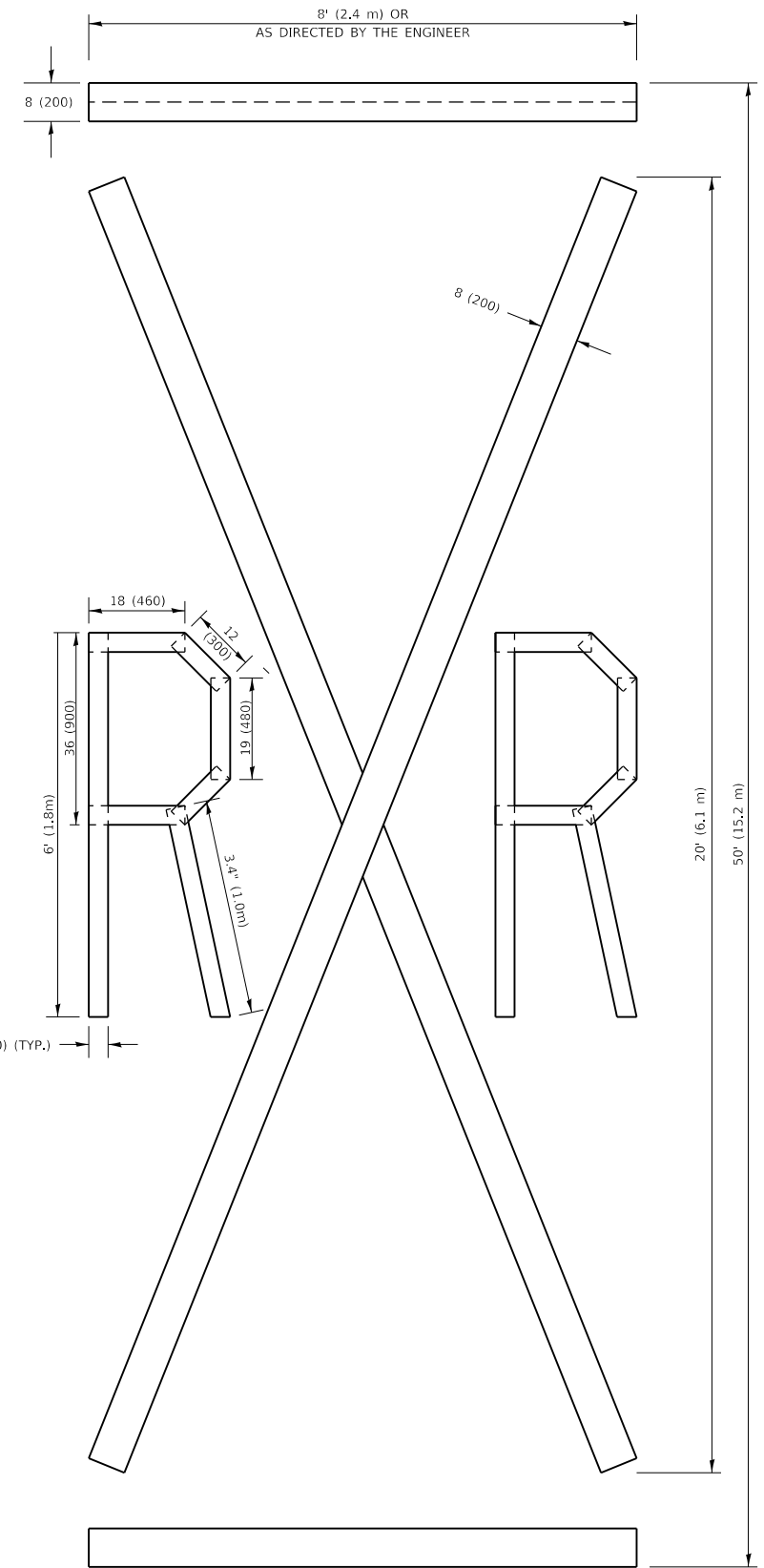


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default
FILE: Model: pavlpavmark.dwg, I:\projects\126520\CADD\pav\Design\Dist\Stc.dgn

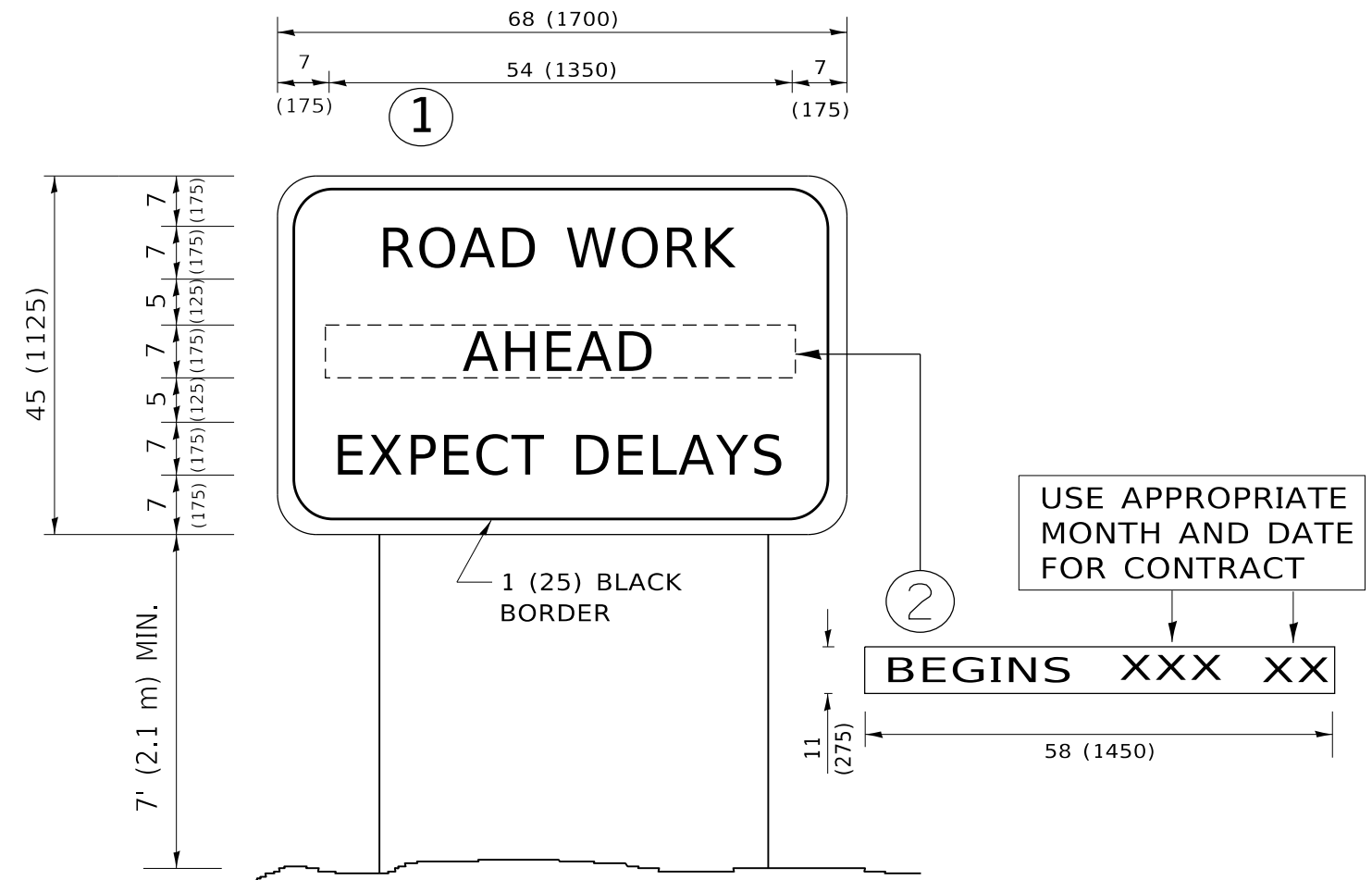
USER NAME = lababidim	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
	DRAWN -	REVISED - E. GOMEZ 08-28-00
PLOT SCALE = 100.0001' / in.	CHECKED -	REVISED - E. GOMEZ 08-28-00
PLOT DATE = 12/13/2019	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE. 305	SECTION 2019-100-RS&SW	COUNTY COOK	TOTAL SHEETS 30	SHEET NO. 25
TC-16		CONTRACT NO. 62J86		
ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: Default
 FILE: \\nas01c.psu.edu\pub\lanscom\data\illinois.gov\PWIDOT\Documents\DOT_Offices\District_1\Projects\1216520\CADD\Drawings\Design\DRS4.dgn

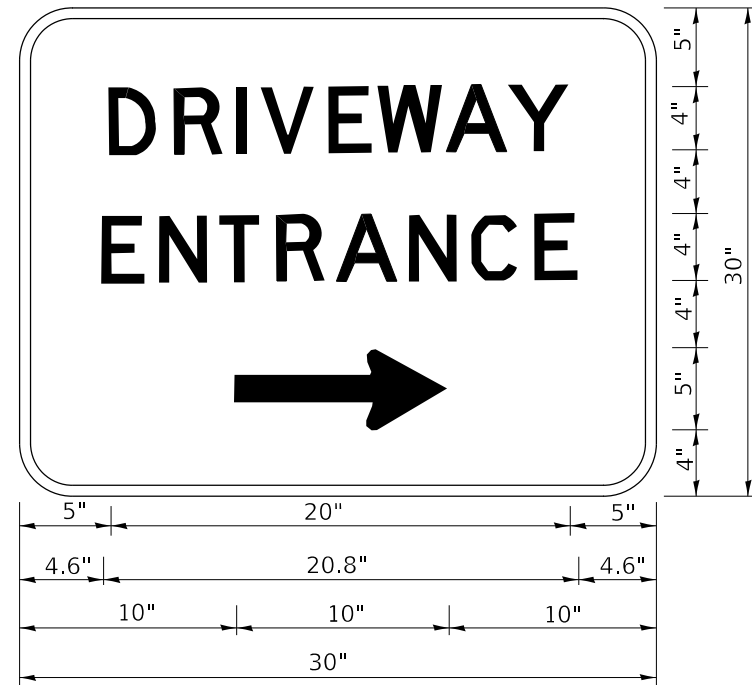
USER NAME = lababidim	DESIGNED -	REVISED - R. MIRS 09-15-97
	DRAWN -	REVISED - R. MIRS 12-11-97
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
PLOT DATE = 12/13/2019	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-100-R5&SW	COOK	30	26
TC-22			CONTRACT NO. 62186	
		ILLINOIS	FED. AID PROJECT	



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

MODEL: Default
 FILE: h:\mhc\p\pub\planroom\dat\illinois.gov\PWIDOT\Documents\DOT_Offices\DI\drctc_1\Project\ID 1265270\CADD\data\Design\DRS4.dgn

USER NAME = lababidim	DESIGNED -	REVISED - C. JUCIUS 02-15-07
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/13/2019	DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

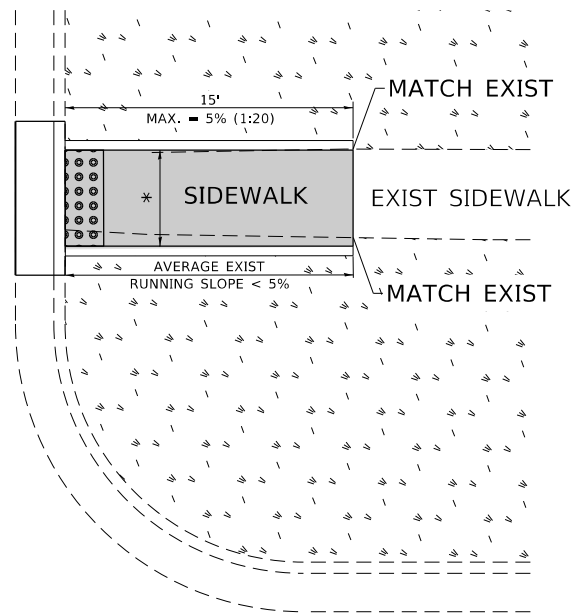
DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

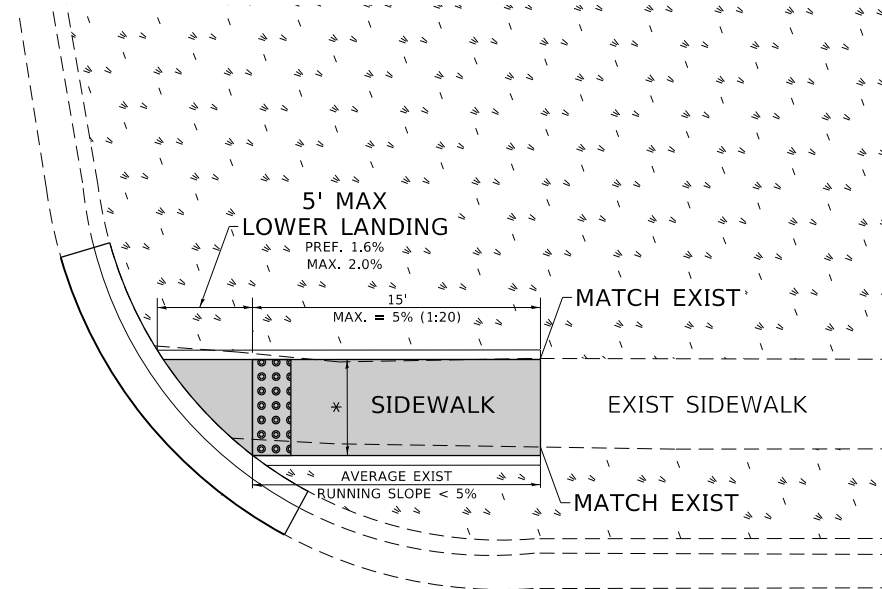
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-100-R5&5W	COOK	30	27
TC-26			CONTRACT NO. 62J86	
		ILLINOIS	FED. AID PROJECT	

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR LESS RUN. SLOPE

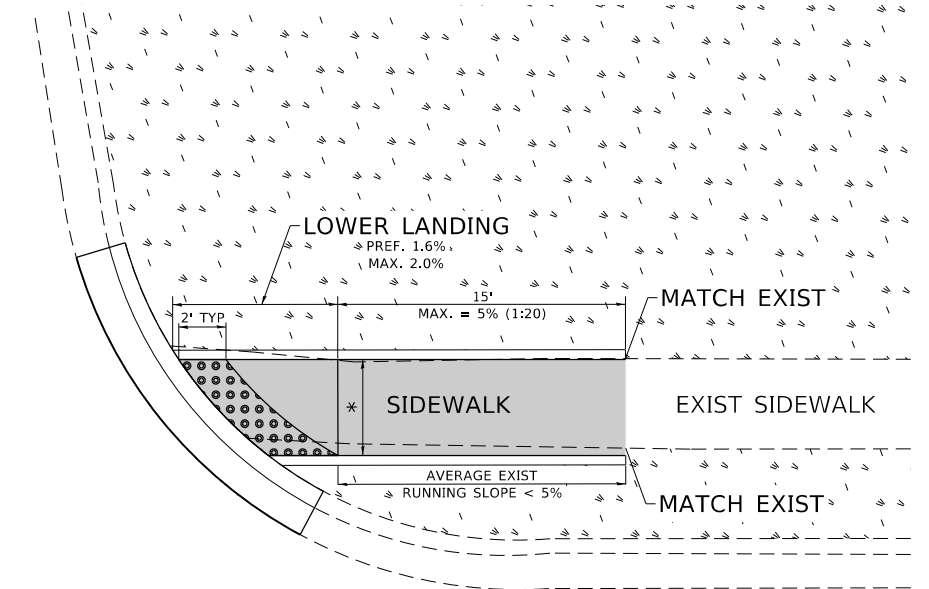
PD-01A



PD-01B



PD-01C



LEGEND

- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- PROPOSED SIDE CURB

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

MODEL: Default
 FILE: h:\m\c\p\pub\baronm\dat\illinois.gov\RW\DOT\Documents\DOT_Offices\Dir\ctc_1\Project\ID 1265270\Cadd\Drawn\Design\DES01.dgn

USER NAME = lababidim	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/13/2019	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

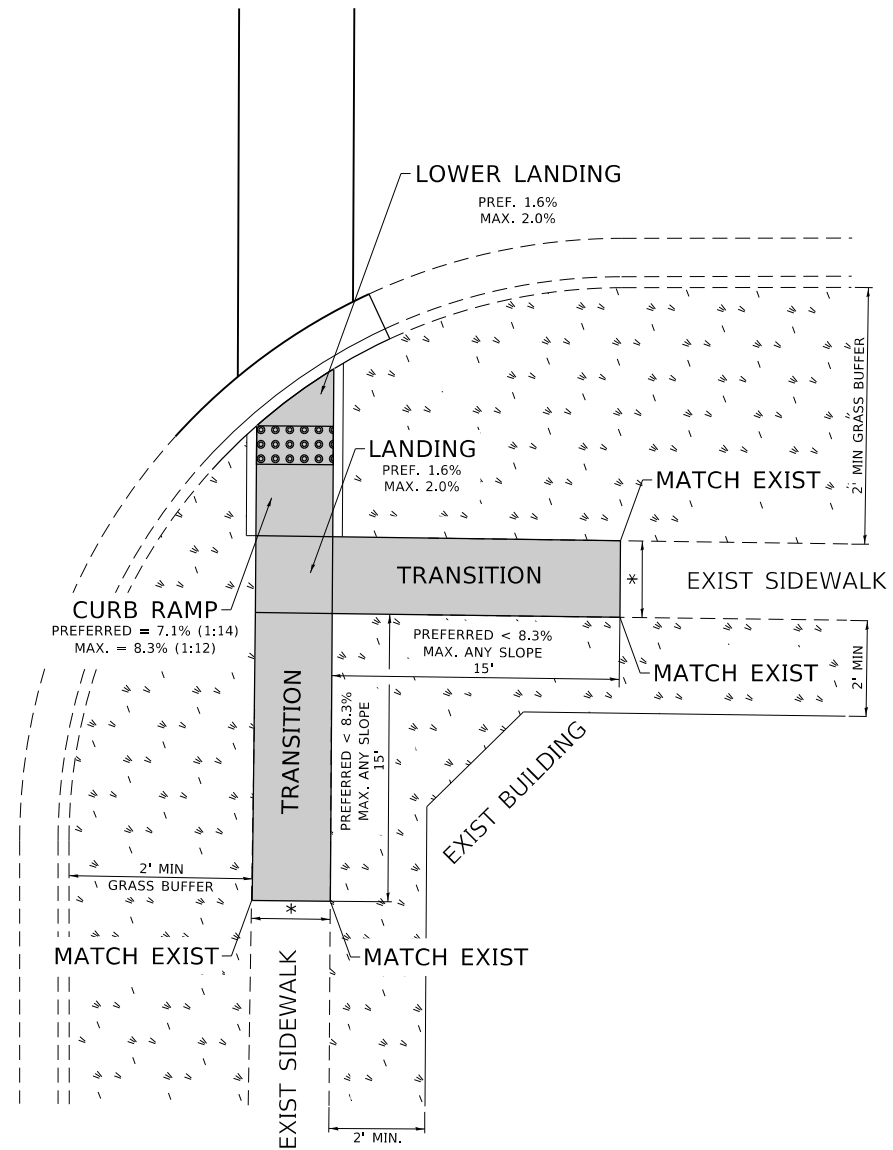
**PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS
(PD-01)**

SCALE: NONE SHEET OF SHEETS STA. TO STA.

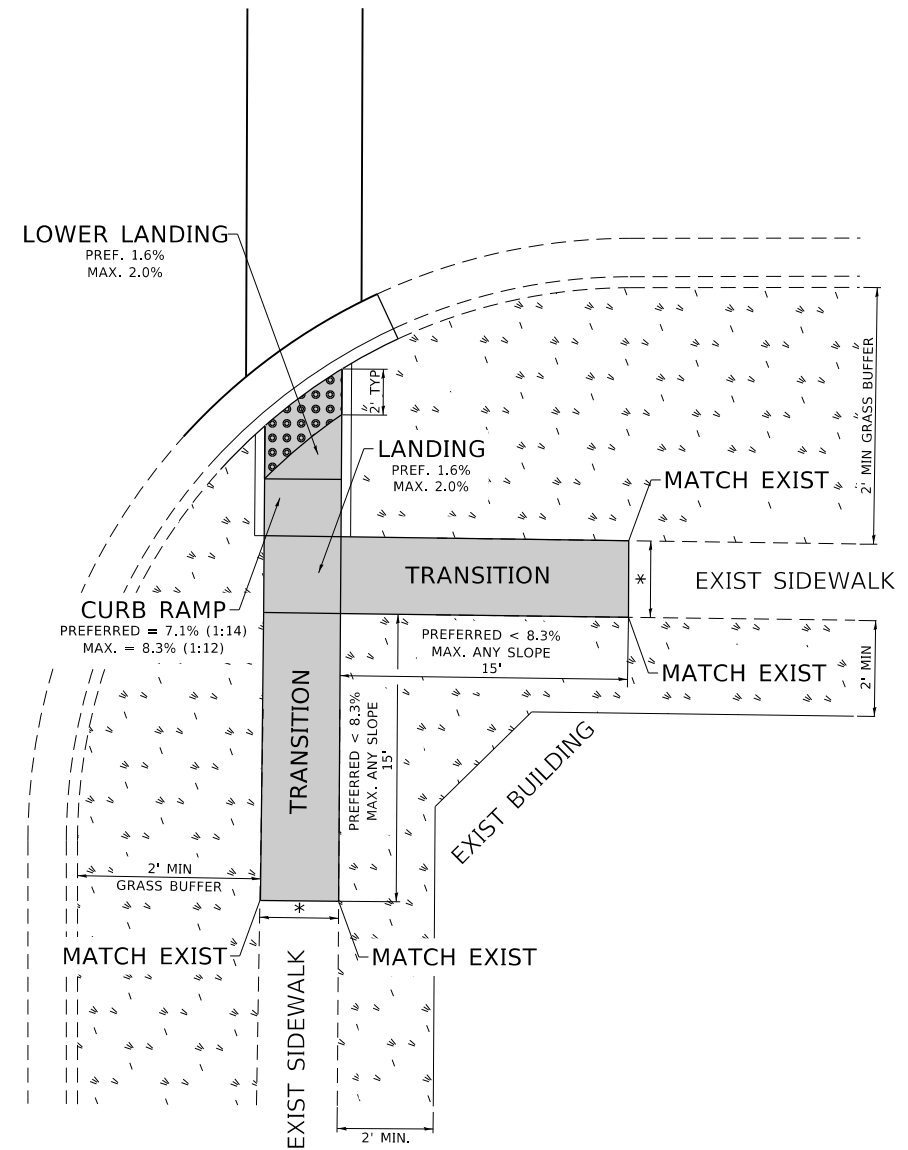
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-100-R5&SW	COOK	30	28
PD-01		CONTRACT NO. 62J86		
ILLINOIS		FED. AID PROJECT		

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ TURNING SPACE

PD-04A



PD-04B



LEGEND

- PROPOSED SIDE CURB
- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

MODEL: Default
FILE: \\harc...
PROJECT: ...

USER NAME = lababidim	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/13/2019	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

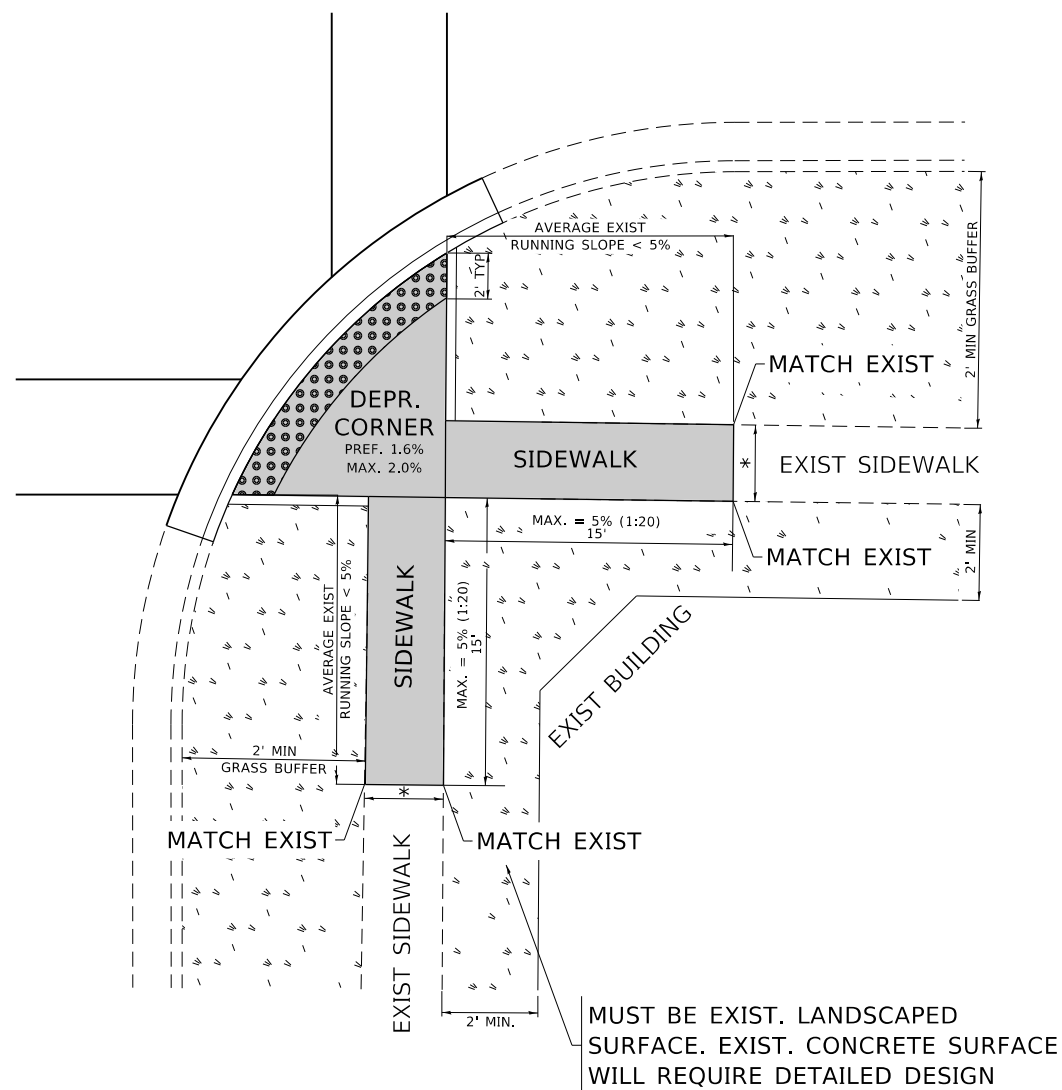
**PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS WITH
TURNING SPACE (PD-04)**

SCALE: NONE SHEET OF SHEETS STA. TO STA.

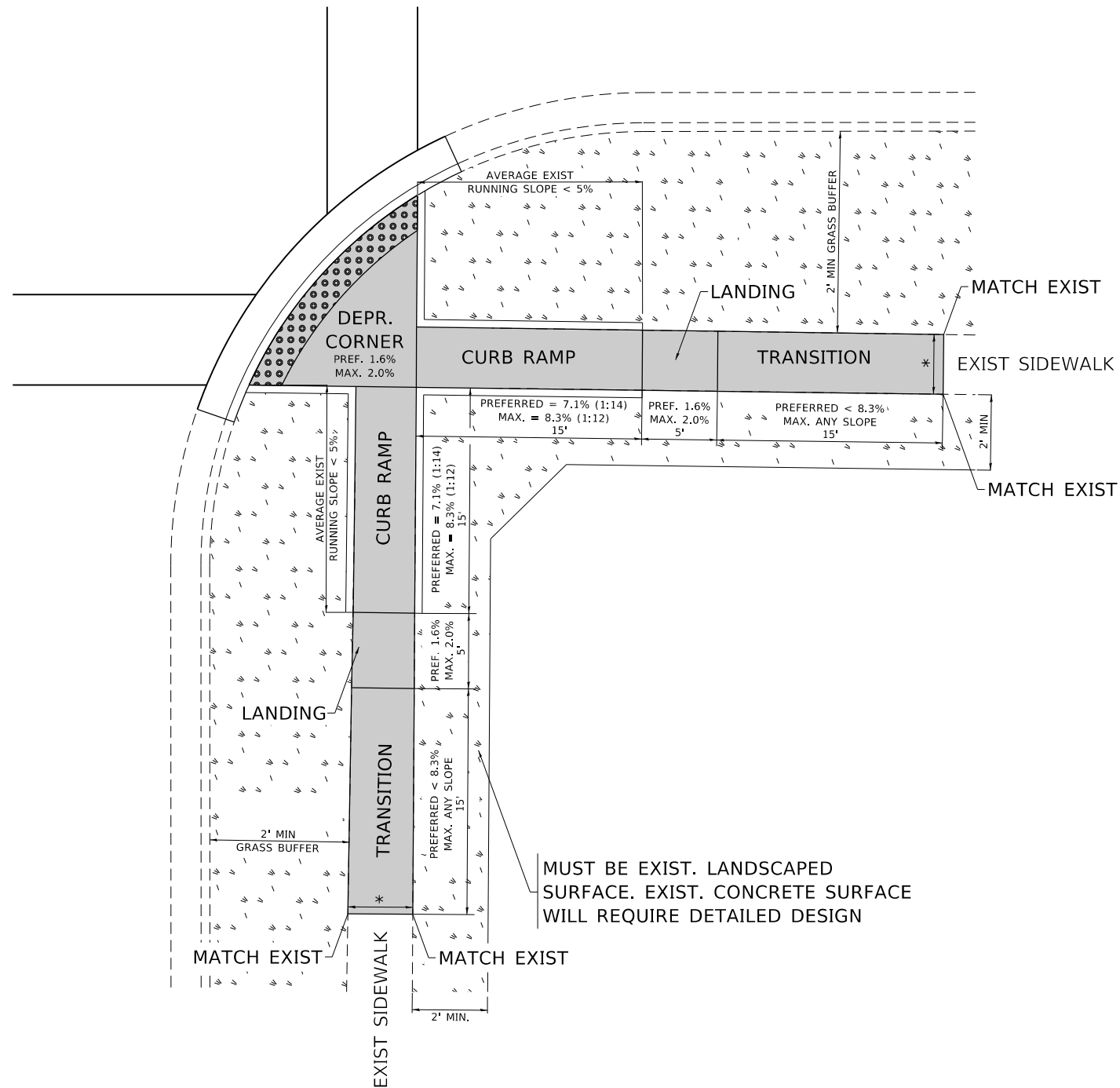
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-100-R5&SW	COOK	30	29
PD-04		CONTRACT NO. 62J86		
		ILLINOIS FED. AID PROJECT		

ADA DETAIL FOR DEPRESSED CORNER CURB RAMPS

PD-05A



PD-05B



LEGEND

- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- PROPOSED SIDE CURB

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

MODEL: Default
 FILE: h:\mfc_projects\p05\planroom.dwg
 PROJECT: PD-05A
 DATE: 12/13/2019

USER NAME = lababidim	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/13/2019	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROJECT DETAIL FOR DEPRESSED CORNER CURB RAMPS
(PD-05)**

SCALE: NONE SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2019-100-R5&SW	COOK	30	30
PD-05		CONTRACT NO. 62186		
ILLINOIS		FED. AID PROJECT		