

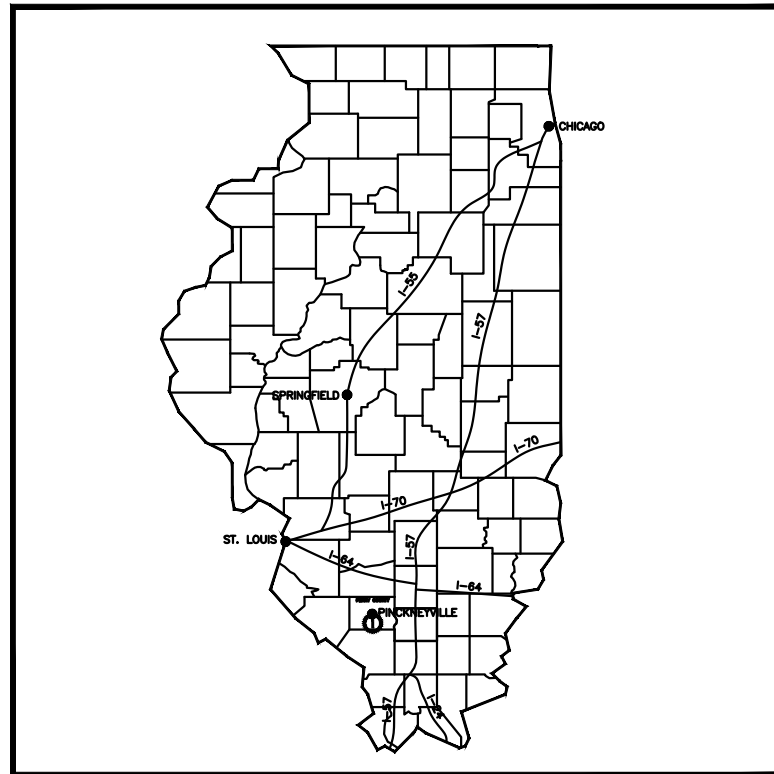
LETTING ITEM NO. 08A
MARCH 6, 2020 LETTING

CONSTRUCTION PLANS

PL017
TOTAL SHEETS: 15

FOR PINCKNEYVILLE-DU QUOIN AIRPORT

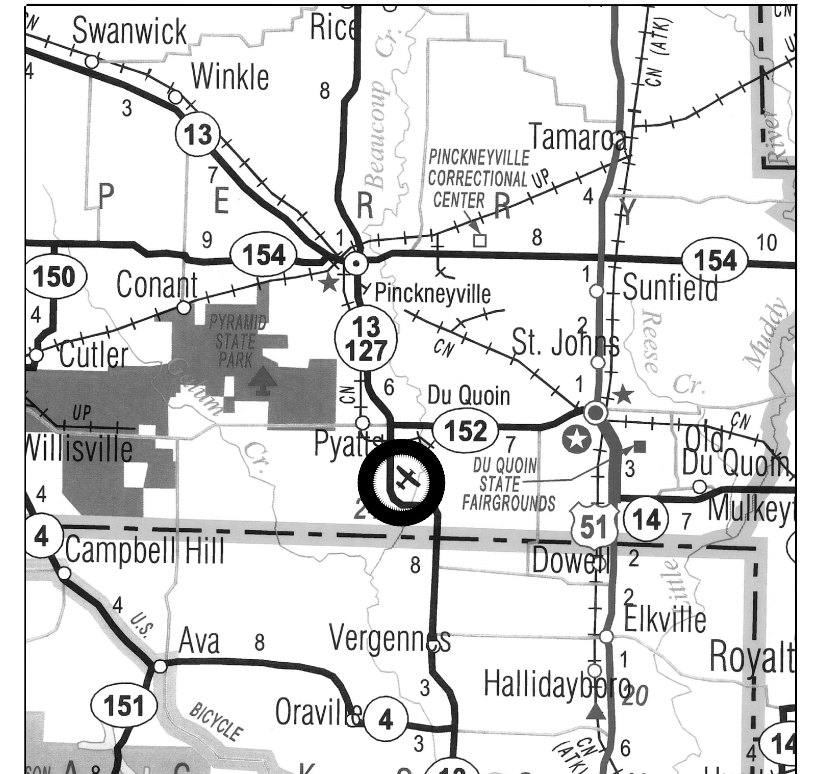
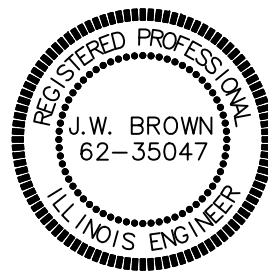
REHABILITATION OF MAIN AIRCRAFT PARKING APRON AREAS DESIGNATED AS R/1, R/2, R/3 AND TH/1



LOCATION MAP

FINAL SUBMITTAL JANUARY 10, 2020
ILLINOIS PROJECT NUMBER: PJY-4732
SBG PROJECT NUMBER: 3-17-SBGP-144/156

PINCKNEYVILLE, ILLINOIS



VICINITY MAP

DESIGN INFORMATION

- CRITICAL AIRCRAFT = BEECH KING AIR B100
- AIRCRAFT APPROACH CATEGORY (AAC) = B
- AIRPLANE DESIGN GROUP (ADG) = I
- TAXIWAY DESIGN GROUP (TDG) = IA
- DEPARTURE WEIGHT = 11,800 LBS.

BROWN AND ROBERTS, INC.
CONSULTING ENGINEER
PRESIDENT

SUBMITTED BY: *Jim W. Brown*
JIM W. BROWN, PRESIDENT
DATE SUBMITTED: 1/10/2020
LICENSE NUMBER: 062-035047
LICENSE EXPIRATION DATE: NOVEMBER 30, 2021

PLANS PREPARED BY:



BROWN AND ROBERTS, INC.
1 WESTRIDGE ROAD
HARRISBURG, IL. 62946
(618) 252-8111

PINCKNEYVILLE-DU QUOIN AIRPORT

ACTING CHAIRMAN
APPROVED BY: *John Hartsock* 01/08/2020
JOHN HARTSOCK DATE
SECRETARY
ATTESTED BY: *Stephen Moss* 01/08/2020
STEPHEN MOSS DATE

GENERAL NOTES

1. THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIAL, EQUIPMENT, AND TRANSPORTATION NECESSARY TO CONSTRUCT ALL ELEMENTS OF THE PROJECT AS DESCRIBED IN THE CONSTRUCTION PLANS AND SPECIFICATIONS.
2. THE RULES, REGULATIONS, AND SPECIFICATIONS ENUMERATED HEREIN SHALL BE CONSIDERED AS MINIMUM REQUIREMENTS. THEY SHALL NOT PROHIBIT THE CONTRACTOR FROM FURNISHING AND INSTALLING HIGHER GRADES OF MATERIAL THAN ARE SPECIFIED HEREIN.
3. ACCESS TO THE PROJECT FOR ALL HAULING OF MATERIALS AND EQUIPMENT SHALL BE RESTRICTED TO THE DESIGNATED CONSTRUCTION ENTRANCE AND HAUL ROUTE. ACCESS TO THE WORK AREAS FROM THE STAGING AREA SHALL BE COORDINATED WITH THE RESIDENT ENGINEER AND AIRPORT MANAGEMENT.
4. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT, PRESERVE, AND REPAIR THE EXISTING AIRFIELD AND ROADWAY PAVEMENTS AT ALL TIMES. THE CONTRACTOR SHALL REPAIR ANY DAMAGE TO EXISTING ELECTRICAL, DRAINAGE, AND PAVEMENT STRUCTURES AT NO ADDITIONAL COST TO THE CONTRACT.
5. NO EQUIPMENT SHALL BE PERMITTED TO CROSS OR USE ANY EXISTING PAVEMENT OUTSIDE THE CONSTRUCTION LIMITS, GENERAL PROJECT AREA OR ACCESS ROUTE.
6. CONTRACTOR IS REQUIRED TO PROVIDE THEIR OWN RESTROOM FACILITIES.
7. UNLESS OTHERWISE NOTED, ALL DISTURBED AREAS OUTSIDE OF THE PROPOSED CONSTRUCTION LIMITS SHALL BE GRADED, SEEDED AND/OR HYDROMULCH SEEDED AT NO ADDITIONAL COST TO THE CONTRACT.
8. ALL WASTE MATERIAL SHALL BE HAULED FROM THE AIRPORT AND PROPERLY DISPOSED OF UNLESS OTHERWISE SPECIFIED HEREIN.
9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING PERMITS FOR HAULING ON PUBLIC ROADS, AS APPLICABLE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING ANY DAMAGES TO ANY PAVEMENTS (PUBLIC OR PRIVATE) CAUSED BY HIS/HER CONSTRUCTION EQUIPMENT OR PERSONNEL.
10. THE OWNER SHALL HAVE THE RIGHT OF FIRST REFUSAL FOR ALL SALVAGEABLE MATERIAL REMOVED ON THE PROJECT.
11. CONTRACTOR SHALL RESTORE TO ORIGINAL CONDITION ALL GRASS, STONE, OR PAVEMENT DISTURBED BY CONTRACTOR'S CONSTRUCTION OPERATIONS, STAGING, AND CONSTRUCTION ACCESS ROUTES. DISTURBED AREAS TO BE REPAIRED, GRADED, AND MULCHED & SEEDED UNLESS OTHERWISE NOTED. STAGING AREA AND SITE ACCESS RESTORATION WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
12. THE PROJECT PAY ITEMS ARE INTENDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THESE PLANS. ALL INCIDENTAL WORK REQUIRED TO COMPLETE THE PROJECT TO THE SATISFACTION OF THE RESIDENT ENGINEER IS TO BE INCLUDED IN THE COSTS OF PERFORMING THESE ITEMS.
13. APPROXIMATE LOCATIONS OF UNDERGROUND UTILITIES ARE SHOWN THROUGHOUT THESE PLANS. THE CONTRACTOR SHALL DETERMINE EXACT LOCATIONS AND PROTECT THESE UTILITIES DURING CONSTRUCTION. ANY UTILITIES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. CONTRACTOR SHALL COORDINATE WITH THE PROPER PERSONS FOR THE PURPOSE OF LOCATING AND PROTECTING EXISTING UNDERGROUND UTILITIES.
14. THE CONTRACTOR MUST AT ALL TIMES MAINTAIN PROPER DRAINAGE FOR ALL AREAS AFFECTED BY HIS WORK, TO INCLUDE PIPE CULVERT(S) FOR HAUL/ACCESS ROUTE IF NECESSARY.
15. ANY FENCING OR FENCE POST REMOVAL ON ACCESS ROUTE NECESSARY FOR EQUIPMENT TO ACCESS THE PROJECT AREA SHALL BE REMOVED AND REPLACED AT THE EXPENSE OF THE CONTRACTOR.

INDEX OF SHEETS	
SHEET NUMBER	DESCRIPTION
1	COVER SHEET
2	GENERAL NOTES, INDEX OF SHEETS AND SUMMARY OF QUANTITIES
3	CONSTRUCTION SAFETY & PHASING PLAN
4	TYPICAL SECTIONS
5	STAGING DETAIL
6	REMOVAL PLAN
7	APRON DETAIL 1
8	APRON DETAIL 2
9	APRON DETAIL 3
10	JOINT PLAN
11	JOINT DETAILS
12	MARKING AND TIE DOWN PLAN
13	TIE DOWN DETAILS
14	FENCE DETAILS 1
15	FENCE DETAILS 2

SUMMARY OF QUANTITIES			
ITEM NUMBER	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY
AR150520	MOBILIZATION	L. S.	1
AR152511	SUBGRADE REPAIR	S.Y.	2000
AR162504	CLASS E FENCE 4'	L.F.	120
AR162900	REMOVE CLASS E FENCE	L.F.	120
AR209510	CRUSHED AGGREGATE BASE COURSE	TON	3000
AR401900	REMOVE BITUMINOUS PAVEMENT	S.Y.	7492
AR501506	6" PCC PAVEMENT	S.Y.	7931
AR501530	PCC TEST BATCH	EACH	1
AR501900	REMOVE PCC PAVEMENT	S.Y.	298
AR510510	TIE DOWN	EACH	36
AR510900	REMOVE TIE DOWN	EACH	36
AR620520	PAVEMENT MARKING - WATERBORNE	S.F.	740
AR620525	PAVEMENT MARKING - BLACK BORDER	S.F.	1080
AR751940	ADJUST INLET	EACH	1

SCOPE OF WORK

THE PROJECT SCOPE CONSISTS OF THE REMOVAL OF THE EXISTING BITUMINOUS PAVEMENT STRUCTURE OF THE AIRCRAFT PARKING AREAS R/1, R/2, R/3 AND TH/1. APRON AREAS TO BE RECONSTRUCTED WITH CRUSHED AGGREGATE BASE COURSE AND PCC PAVEMENT ALONG WITH OTHER NECESSARY AND RELATED WORK.

PROPOSED SAFETY PLAN

GENERAL – THE PINCKNEYVILLE-DU QUOIN AIRPORT CURRENTLY HAS A PAVED NORTH-SOUTH RUNWAY (4000-FT BY 60-FT).

IT IS ANTICIPATED THAT RUNWAY 18/36 WILL REMAIN OPEN FOR THE DURATION OF THIS PROJECT, AS NO CONSTRUCTION ACTIVITIES ASSOCIATED WITH THIS PROJECT WILL BE WITHIN 125 FT OF THE RUNWAY 18/36 C. ANY WORK WITHIN 125 FT OF THE C WILL REQUIRE CLOSURE OF THE RUNWAY.

THE CONTRACTOR SHALL SUBMIT A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) CONFIRMING COMPLIANCE WITH THE CONSTRUCTION SAFETY & PHASING PLAN (CSPP) PRIOR TO THE ISSUANCE OF THE NOTICE TO PROCEED AS SPECIFIED IN FAA AC 150/5370-2G.

TAXIWAY / TAXILANE CLOSURES

NO CONTRACTOR EQUIPMENT OR PERSONNEL WILL BE ALLOWED WITHIN 66' OF AN ACTIVE TAXIWAY CENTERLINE OR 58' OF AN ACTIVE TAXILANE CENTERLINE WITHOUT CLOSING THE TAXIWAY OR TAXILANE WITH THE USE OF BARRICADES.

CONTRACTOR'S RESPONSIBILITIES

IDENTIFICATION – THE CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE PROPERLY MARKED WITH 3-FOOT SQUARE INTERNATIONAL ORANGE AND WHITE CHECKERED FLAGS ANYTIME THEY ARE ON AIRPORT PROPERTY.

THE CONTRACTOR AND HIS EMPLOYEES SHALL BE RESTRICTED TO THE WORK AREA.

EQUIPMENT PARKING AND STORAGE – THE CONTRACTOR'S EQUIPMENT PARKING, STORAGE, AND EMPLOYEE PARKING WILL BE AT THE LOCATION SHOWN ON THIS SHEET. ONLY CONTRACTOR VEHICLES AND EQUIPMENT REQUIRED FOR CONSTRUCTION WILL BE ALLOWED OUTSIDE THIS AREA.

CONTRACTOR'S RESPONSIBILITIES (CONTINUED)

BARRICADES AND TRAFFIC CONES – IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN BARRICADES AND TRAFFIC CONES AS REQUIRED AND AS DIRECTED BY THE RESIDENT ENGINEER. BARRICADES, THEIR MAINTENANCE, PLACEMENT, AND REMOVAL WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE CONTRACTOR WILL BE RESPONSIBLE FOR REPAIRING ANY DAMAGE TO ADJACENT EXISTING PAVEMENTS CAUSED BY HIS PERSONNEL OR EQUIPMENT.

HAUL ROUTE AND EQUIPMENT PARKING

THE CONTRACTOR WILL USE THE DESIGNATED HAUL ROUTE AND EQUIPMENT PARKING AREA SHOWN ON THIS SAFETY PLAN. THE PROPOSED EQUIPMENT PARKING AREA WILL BE TWO AREAS APPROXIMATELY 70-FT BY 50-FT. THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN THE PROPOSED HAUL ROUTE AND PARKING AREA THROUGHOUT THE COURSE OF THE PROJECT. AT THE CONCLUSION OF THE PROJECT, ALL AREAS DISTURBED WILL BE RESTORED AS NEEDED TO ITS ORIGINAL STATE. RESTORATION OF THE HAUL ROUTE AND EQUIPMENT PARKING AREA WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

UTILITY NOTE

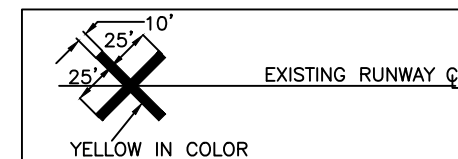
THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING ALL UTILITY COMPANIES AND ORGANIZATIONS THAT HAVE LINES OR CONDUITS IN THE PROPOSED WORK AREA. ALL LINES AND CONDUITS SHALL BE LOCATED AND IDENTIFIED FOR DEPTH BEFORE ANY EXCAVATION BEGINS. THE CONTRACTOR SHALL CALL JULIE (1-800-892-0123) TO ACCOMPLISH THESE REQUIREMENTS. THE CONTRACTOR IS RESPONSIBLE FOR IDENTIFYING ALL NON-JULIE UTILITIES LOCATED WITHIN THE PROPOSED CONSTRUCTION LIMITS. THESE UTILITIES ARE TO BE LOCATED PRIOR TO THE START OF CONSTRUCTION.

AIRCRAFT ACCESS

T-HANGAR TENANTS WILL NOT HAVE ACCESS TO THE RUNWAY DURING STAGE 1 CONSTRUCTION. THE AIRPORT MANAGER WILL COORDINATE WITH TENANTS REGARDING AIRCRAFT ACCESS TO THE RUNWAY DURING THIS STAGE.

AIRPORT SECURITY

AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. THE PROPOSED HAUL ROUTE SHOWN ON THIS SAFETY PLAN IS THE ONLY ACCESS. CONTRACTOR EQUIPMENT AND PERSONNEL WILL BE ALLOWED TO USE. THE CONTRACTOR SHALL PROVIDE BARRICADES AT THIS ACCESS AND ENSURE THE BARRICADES ARE IN PLACE AT THE END OF EACH WORKING DAY.



DETAIL OF CROSS FOR CLOSED RUNWAY
"NOT TO SCALE"

J.U.L.I.E. INFORMATION

COUNTY.....PERRY
CITY.....PINCKNEYVILLE
TWNHP/RNG.....6S/2W
SECTION NO.....30
NEAREST MAJOR ROAD INTERSECTION...ILLINOIS ROUTE 13/127 AND ILLINOIS ROUTE 152
AIRPORT ADDRESS....PINCKNEYVILLE-DU QUOIN AIRPORT
1352 STATE ROUTE 13/127
DU QUOIN, IL 62274

NOTE:

THE COST OF CONSTRUCTING, PLACING, MAINTAINING, AND REMOVING CROSSES WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. THE CROSSES WILL BE YELLOW IN COLOR AND SHALL BE MADE OF A SUITABLE MATERIAL AS APPROVED BY THE RESIDENT ENGINEER. THE CROSSES WILL BE PLACED AT THE ENDS OF THE RUNWAY AND SECURED IN A MANNER APPROVED BY THE RESIDENT ENGINEER. THE PROPOSED CROSSES WILL BE PLACED WHEN THE RUNWAY IS CLOSED AND REMOVED WHEN THE RUNWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PLACEMENT AND REMOVAL OF THE CROSSES AT NO ADDITIONAL COST TO THE CONTRACT.

RUNWAY CLOSURE PROCEDURES:

- * CONTACT THE AIRPORT MANAGER OR HIS ASSIGNED REPRESENTATIVE.
- * ISSUANCE OF NOTAM BY THE AIRPORT MANAGER OR HIS ASSIGNED REPRESENTATIVE.
- * PLACEMENT OF CROSSES (SEE DETAIL THIS SHEET).
- * PLACEMENT OF LIGHTED BARRICADES. ONLY AT THE TIME THAT ALL OF THE ABOVE ARE COMPLETED MAY ANY CONSTRUCTION OPERATIONS WITHIN 125-FT OF THE AFFECTED RUNWAY CENTERLINE AND WITHIN 600 FT OF THE RUNWAY END BEGIN.
- * RUNWAY LIGHTS SHALL BE DISABLED

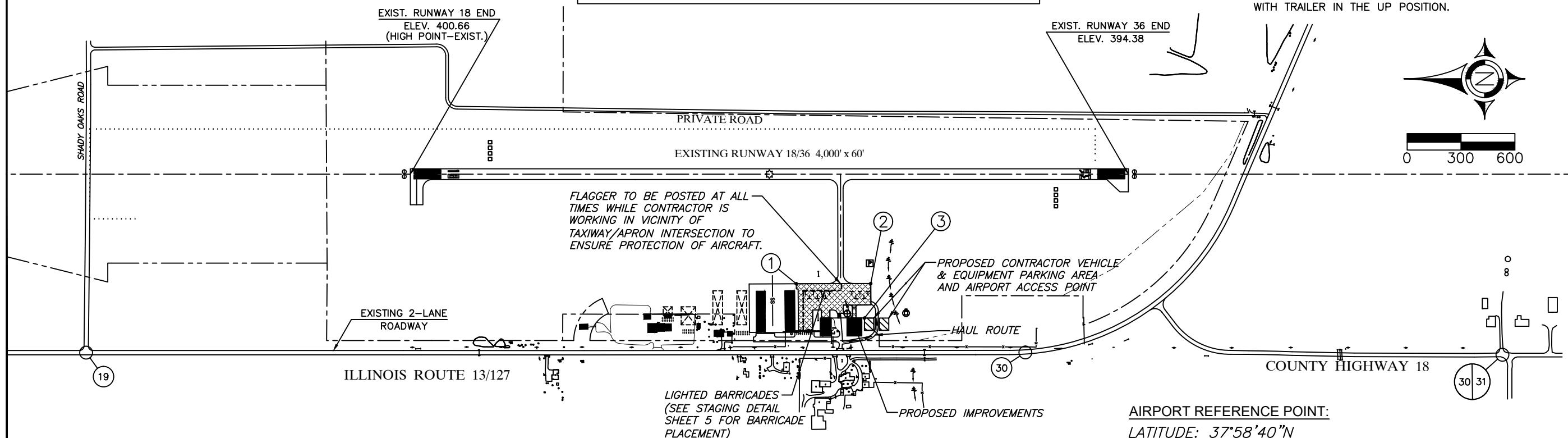
RUNWAY RE-OPENING PROCEDURES:

- * REMOVE CROSSES.
- * REMOVE LIGHTED BARRICADES.
- * NOTIFY THE AIRPORT MANAGER OR HIS REPRESENTATIVE TO CANCEL THE NOTAM.
- * CANCELLATION OF THE NOTAM. A CLOSED RUNWAY WILL NOT BE RE-OPENED UNTIL ALL EQUIPMENT AND WORK ARE FURTHER THAN 200 FT. FROM THE AFFECTED RUNWAY CENTERLINE
- * RUNWAY LIGHTS SHALL BE REACTIVATED.

HEIGHT OF CONSTRUCTION EQUIPMENT

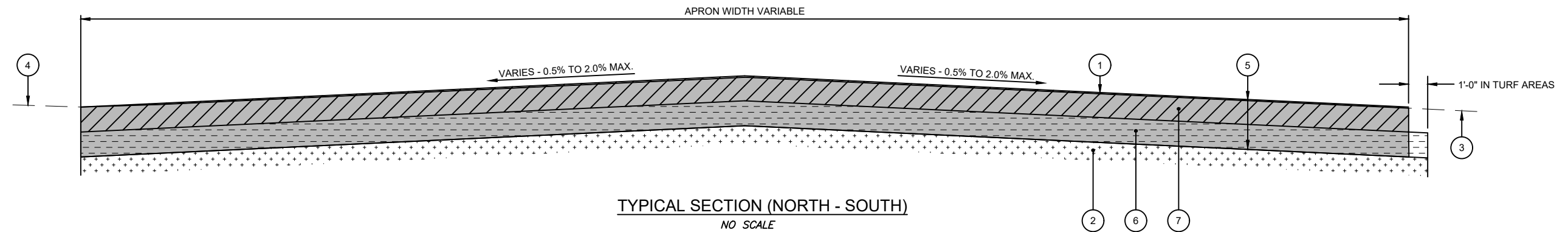
THE MAXIMUM ANTICIPATED HEIGHT OF THE CONSTRUCTION EQUIPMENT IS 20 FEET. THE TALLEST EQUIPMENT IS EXPECTED TO BE A SEMI-TRUCK WITH TRAILER IN THE UP POSITION.

CRITICAL POINTS			
CRITICAL POINT NO.	LATITUDE	LONGITUDE	GROUND ELEVATION (MSL)
1	37°58'39.08"N	89°21'45.26"W	392.9
2	37°58'34.95"N	89°21'45.36"W	390.2
3	37°58'35.36"N	89°21'47.74"W	392.0



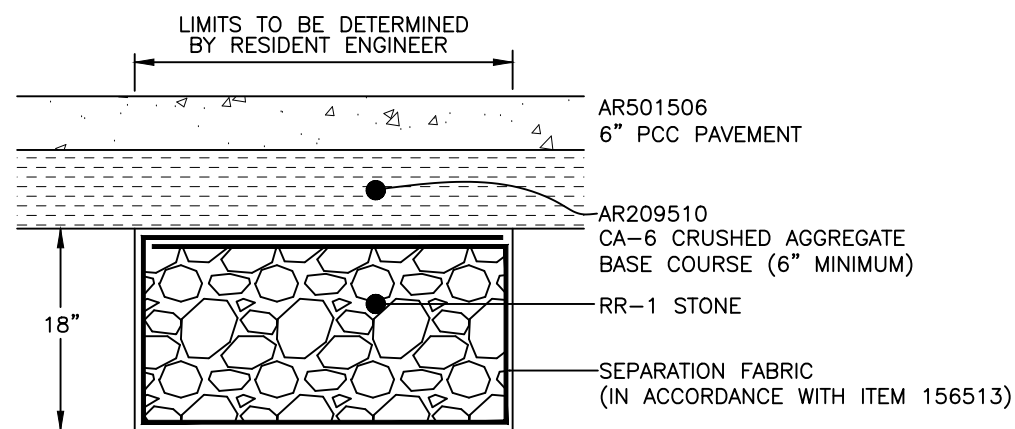
AIRPORT REFERENCE POINT:

LATITUDE: 37°58'40"N
LONGITUDE: 89°21'38"W
ELEVATION: 401 MSL

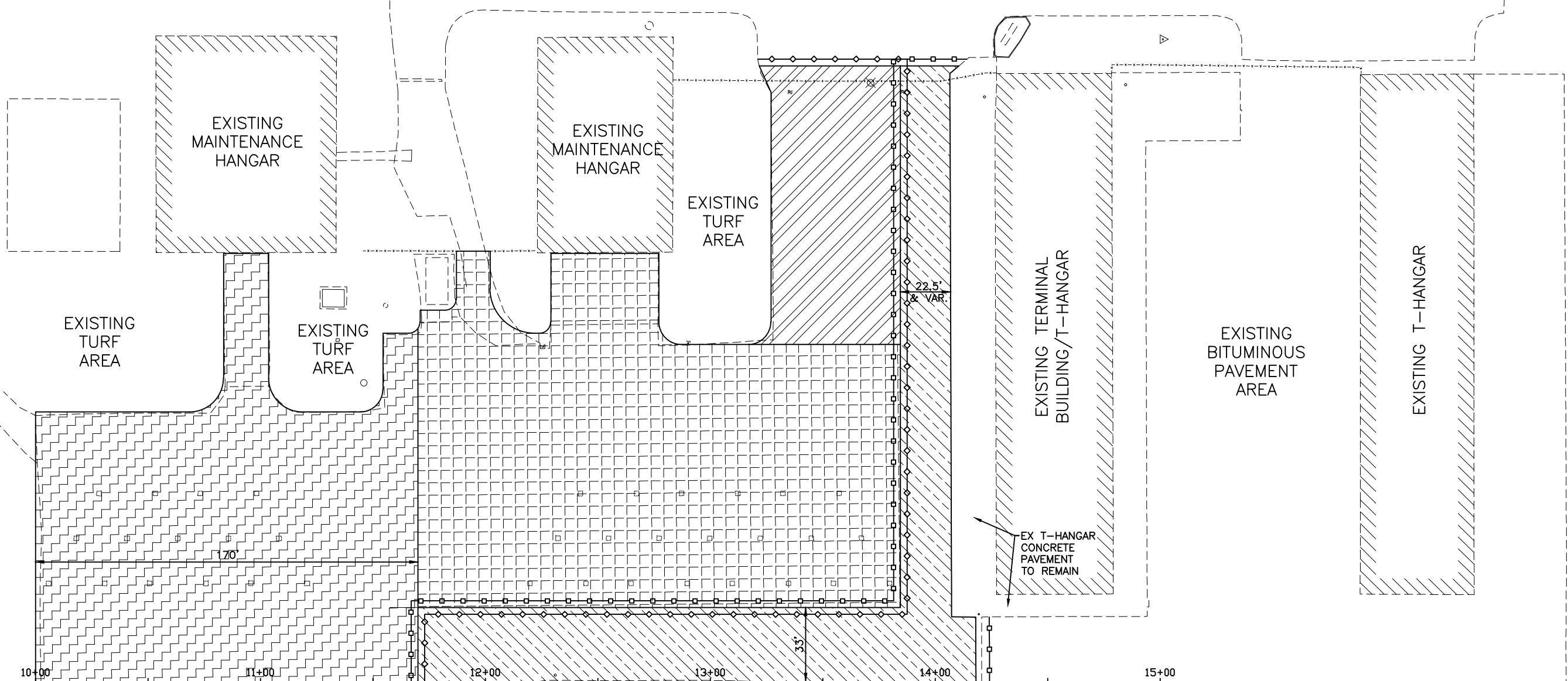


LEGEND

- | | |
|-------------------------------------|---|
| ① EXISTING BITUMINOUS SURFACE | ⑤ AR401900 - REMOVE BITUMINOUS PAVEMENT |
| ② EXISTING AGGREGATE BASE TO REMAIN | ⑥ AR209510 - CRUSHED AGGREGATE BASE COURSE (6" DEPTH) |
| ③ EXISTING TURF SHOULDER | ⑦ AR501506 - 6" PCC PAVEMENT |
| ④ EXISTING PCC PAVEMENT TO REMAIN | |



SUBGRADE REPAIR DETAIL
NO SCALE



AIRCRAFT ACCESS NOTE:
 T-HANGAR TENANTS WILL NOT HAVE ACCESS TO THE RUNWAY DURING STAGE 1 CONSTRUCTION. THE AIRPORT MANAGER WILL COORDINATE WITH TENANTS REGARDING AIRCRAFT ACCESS TO THE RUNWAY DURING THIS STAGE.

NOTE:
 FLAGGER TO BE POSTED AT ALL TIMES WHILE CONTRACTOR IS WORKING IN VICINITY OF TAXIWAY/APRON INTERSECTION TO ENSURE PROTECTION OF AIRCRAFT.

LEGEND	
	STAGE 1
	STAGE 2A
	STAGE 2B
	STAGE 2C
	STAGE 1 BARRICADE LINE
	STAGE 2A, 2B & 2C BARRICADE LINE

STAGE 1:

1. ERECT BARRICADES AT THE LOCATIONS SHOWN.
2. REMOVE EXISTING BITUMINOUS PAVEMENT.
3. PLACE CRUSHED AGGREGATE BASE COURSE.
4. CONSTRUCT 6" PCC PAVEMENT.

STAGE 2A:

1. ERECT BARRICADES AT THE LOCATIONS SHOWN.
2. REMOVE EXISTING BITUMINOUS PAVEMENT.
3. PLACE CRUSHED AGGREGATE BASE COURSE.
4. CONSTRUCT 6" PCC PAVEMENT.

STAGE 2B:

1. REMOVE EXISTING BITUMINOUS AND PCC PAVEMENT.
2. REMOVE EXISTING TIE DOWNS.
3. PLACE CRUSHED AGGREGATE BASE COURSE.
4. CONSTRUCT 6" PCC PAVEMENT AND TIE DOWNS

STAGE 2C:


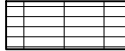
1. REMOVE EXISTING BITUMINOUS AND PCC PAVEMENT.
2. REMOVE EXISTING TIE DOWNS.
3. PLACE CRUSHED AGGREGATE BASE COURSE.
4. CONSTRUCT 6" PCC PAVEMENT AND TIE DOWNS.
5. PLACE PAVEMENT MARKINGS.
6. SEEDING AND MULCHING.

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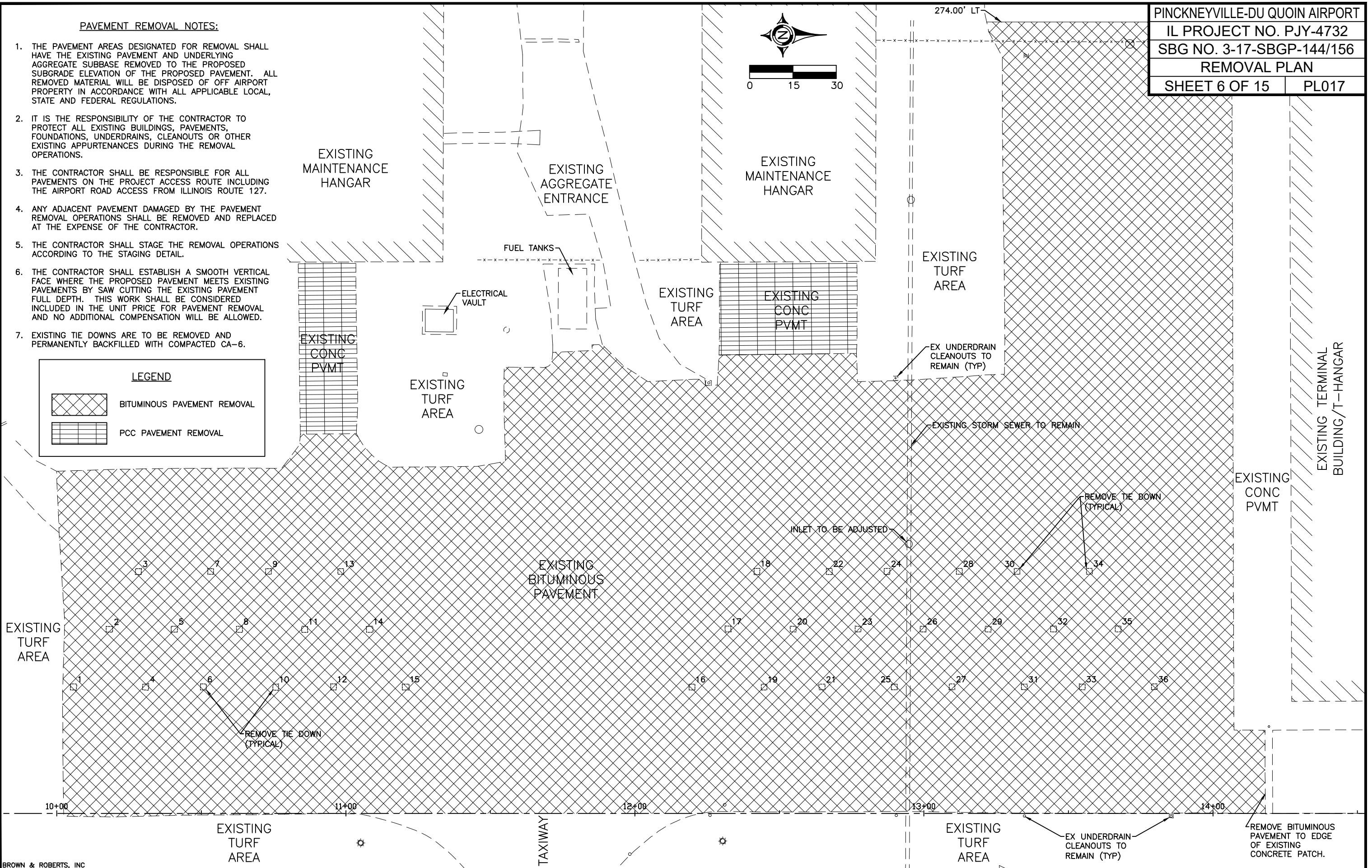
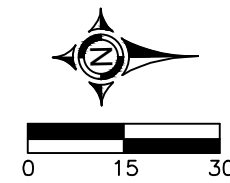
PAVEMENT REMOVAL NOTES:

1. THE PAVEMENT AREAS DESIGNATED FOR REMOVAL SHALL HAVE THE EXISTING PAVEMENT AND UNDERLYING AGGREGATE SUBBASE REMOVED TO THE PROPOSED SUBGRADE ELEVATION OF THE PROPOSED PAVEMENT. ALL REMOVED MATERIAL WILL BE DISPOSED OF OFF AIRPORT PROPERTY IN ACCORDANCE WITH ALL APPLICABLE LOCAL, STATE AND FEDERAL REGULATIONS.
2. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT ALL EXISTING BUILDINGS, PAVEMENTS, FOUNDATIONS, UNDERDRAINS, CLEANOUTS OR OTHER EXISTING APPURTENANCES DURING THE REMOVAL OPERATIONS.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL PAVEMENTS ON THE PROJECT ACCESS ROUTE INCLUDING THE AIRPORT ROAD ACCESS FROM ILLINOIS ROUTE 127.
4. ANY ADJACENT PAVEMENT DAMAGED BY THE PAVEMENT REMOVAL OPERATIONS SHALL BE REMOVED AND REPLACED AT THE EXPENSE OF THE CONTRACTOR.
5. THE CONTRACTOR SHALL STAGE THE REMOVAL OPERATIONS ACCORDING TO THE STAGING DETAIL.
6. THE CONTRACTOR SHALL ESTABLISH A SMOOTH VERTICAL FACE WHERE THE PROPOSED PAVEMENT MEETS EXISTING PAVEMENTS BY SAW CUTTING THE EXISTING PAVEMENT FULL DEPTH. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE UNIT PRICE FOR PAVEMENT REMOVAL AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
7. EXISTING TIE DOWNS ARE TO BE REMOVED AND PERMANENTLY BACKFILLED WITH COMPACTED CA-6.

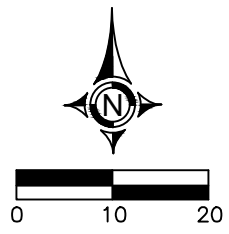
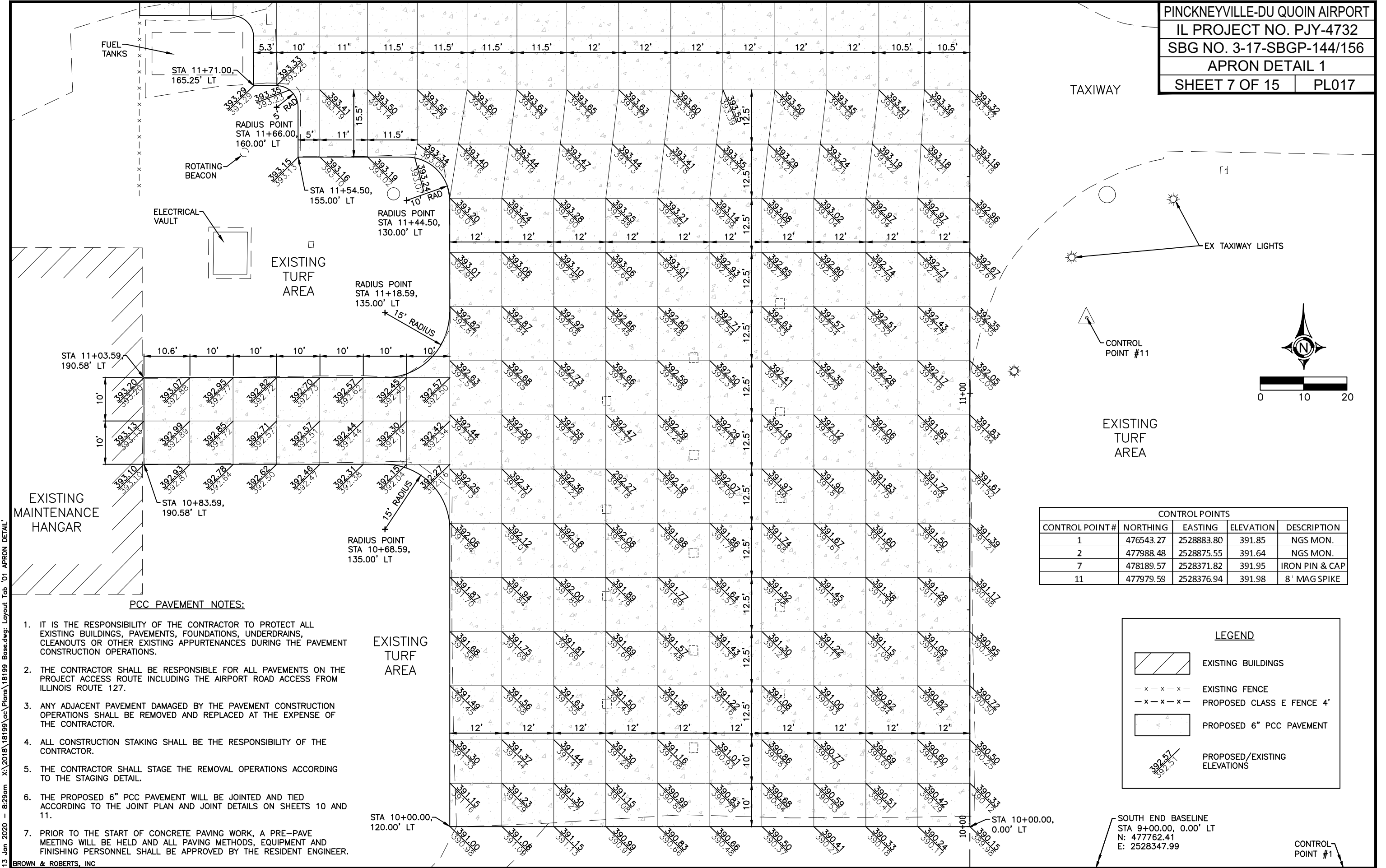
LEGEND

	BITUMINOUS PAVEMENT REMOVAL
	PCC PAVEMENT REMOVAL

PINCKNEYVILLE-DU QUOIN AIRPORT	
IL PROJECT NO. PJY-4732	
SBG NO. 3-17-SBGP-144/156	
REMOVAL PLAN	
SHEET 6 OF 15	PL017



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CONTROL POINTS				
CONTROL POINT #	NORTHING	EASTING	ELEVATION	DESCRIPTION
1	476543.27	2528883.80	391.85	NGS MON.
2	477988.48	2528875.55	391.64	NGS MON.
7	478189.57	2528371.82	391.95	IRON PIN & CAP
11	477979.59	2528376.94	391.98	8" MAG SPIKE

LEGEND

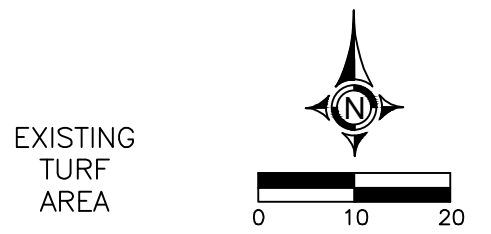
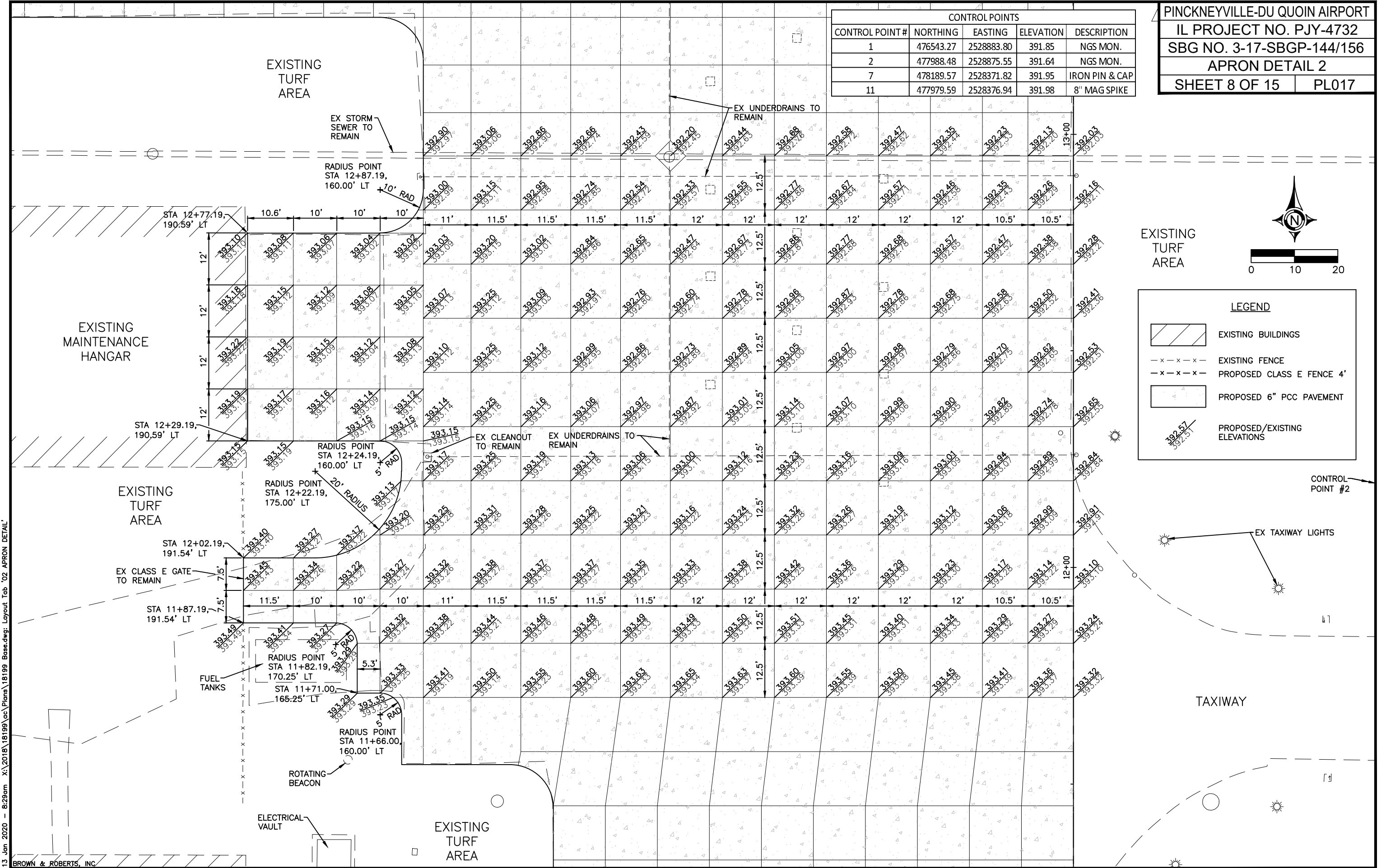
- EXISTING BUILDINGS
- EXISTING FENCE
- PROPOSED CLASS E FENCE 4'
- PROPOSED 6" PCC PAVEMENT
- PROPOSED/EXISTING ELEVATIONS

- PCC PAVEMENT NOTES:**
- IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT ALL EXISTING BUILDINGS, PAVEMENTS, FOUNDATIONS, UNDERDRAINS, CLEANOUTS OR OTHER EXISTING APPURTENANCES DURING THE PAVEMENT CONSTRUCTION OPERATIONS.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL PAVEMENTS ON THE PROJECT ACCESS ROUTE INCLUDING THE AIRPORT ROAD ACCESS FROM ILLINOIS ROUTE 127.
 - ANY ADJACENT PAVEMENT DAMAGED BY THE PAVEMENT CONSTRUCTION OPERATIONS SHALL BE REMOVED AND REPLACED AT THE EXPENSE OF THE CONTRACTOR.
 - ALL CONSTRUCTION STAKING SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - THE CONTRACTOR SHALL STAGE THE REMOVAL OPERATIONS ACCORDING TO THE STAGING DETAIL.
 - THE PROPOSED 6" PCC PAVEMENT WILL BE JOINTED AND TIED ACCORDING TO THE JOINT PLAN AND JOINT DETAILS ON SHEETS 10 AND 11.
 - PRIOR TO THE START OF CONCRETE PAVING WORK, A PRE-PAVE MEETING WILL BE HELD AND ALL PAVING METHODS, EQUIPMENT AND FINISHING PERSONNEL SHALL BE APPROVED BY THE RESIDENT ENGINEER.

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PINCKNEYVILLE-DU QUOIN AIRPORT
IL PROJECT NO. PJY-4732
SBG NO. 3-17-SBGP-144/156
APRON DETAIL 2
SHEET 8 OF 15 | **PL017**

CONTROL POINTS				
CONTROL POINT #	NORTHING	EASTING	ELEVATION	DESCRIPTION
1	476543.27	2528883.80	391.85	NGS MON.
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LEGEND

- EXISTING BUILDINGS
- EXISTING FENCE
- PROPOSED CLASS E FENCE 4'
- PROPOSED 6" PCC PAVEMENT
- PROPOSED/EXISTING ELEVATIONS

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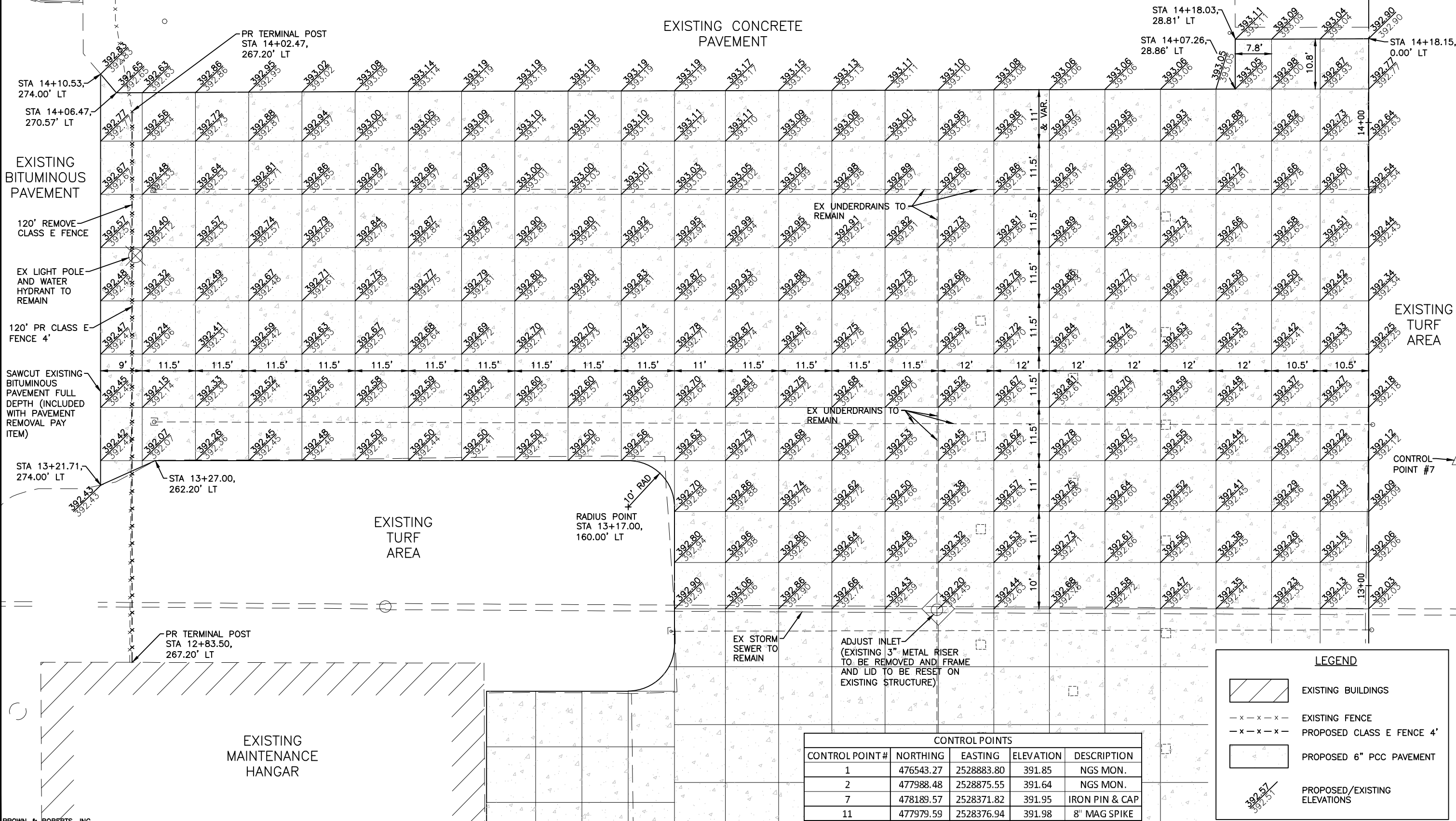
EXISTING TERMINAL BUILDING/T-HANGAR



NORTH END BASELINE
STA 15+00.00, 0.00' LT
N: 478362.38
E: 2528353.44

PINCKNEYVILLE-DU QUOIN AIRPORT
IL PROJECT NO. PJY-4732
SBG NO. 3-17-SBGP-144/156
APRON DETAIL 3
SHEET 9 OF 15 PL017

EXISTING CONCRETE PAVEMENT



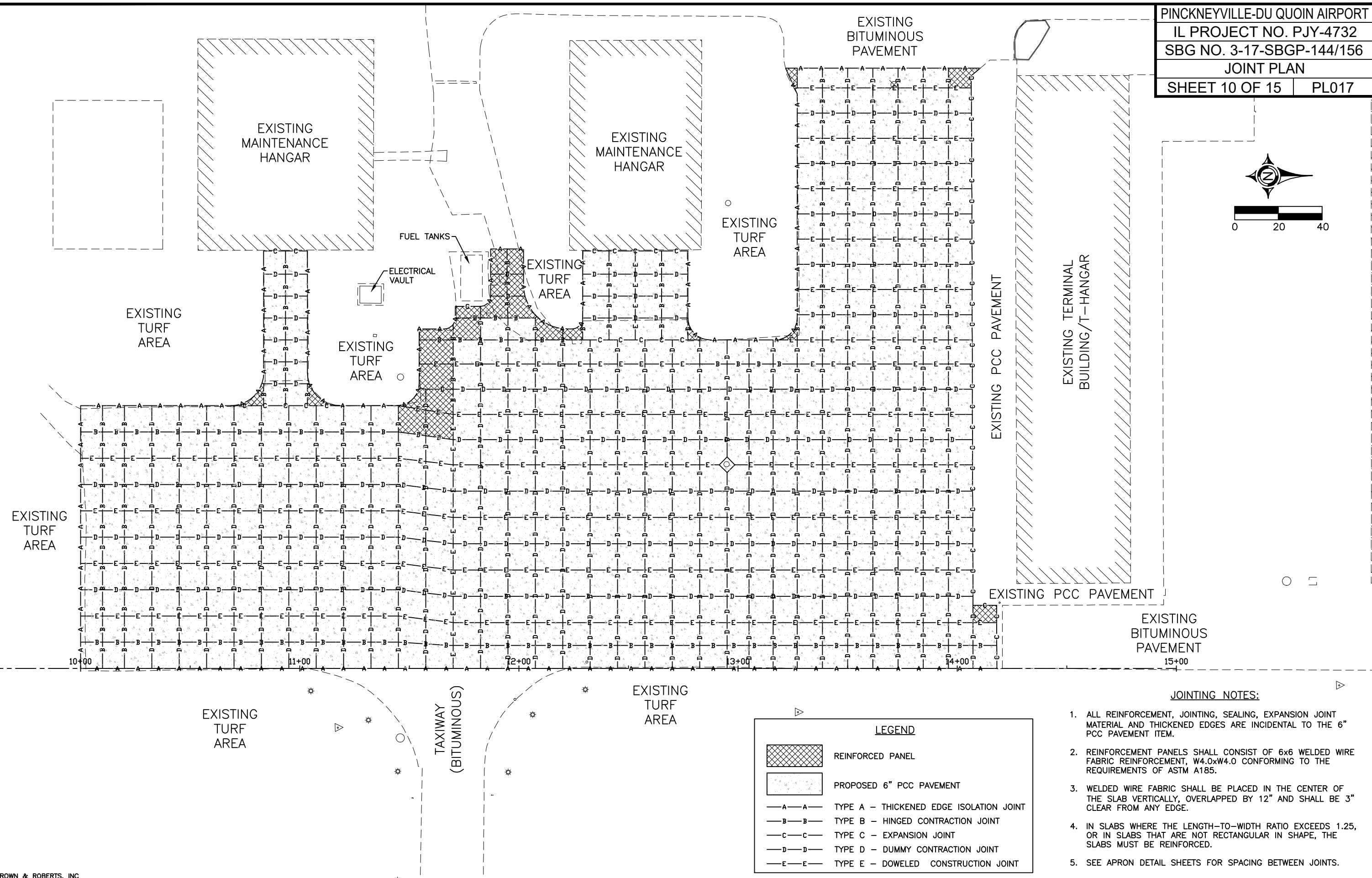
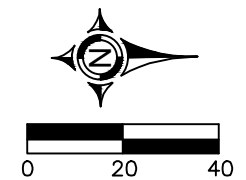
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LEGEND



- EXISTING BUILDINGS
- EXISTING FENCE
- PROPOSED CLASS E FENCE 4'
- PROPOSED 6" PCC PAVEMENT
- PROPOSED/EXISTING ELEVATIONS

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BROWN & ROBERTS, INC

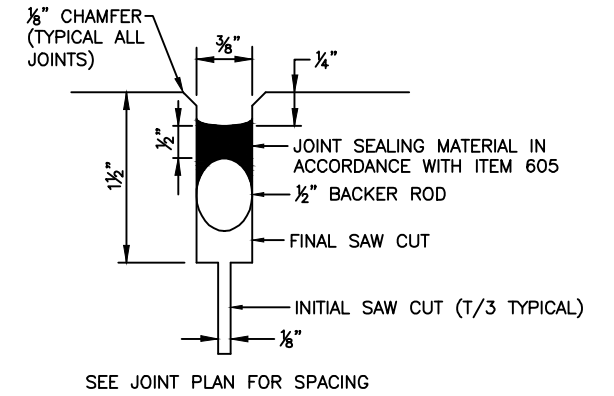
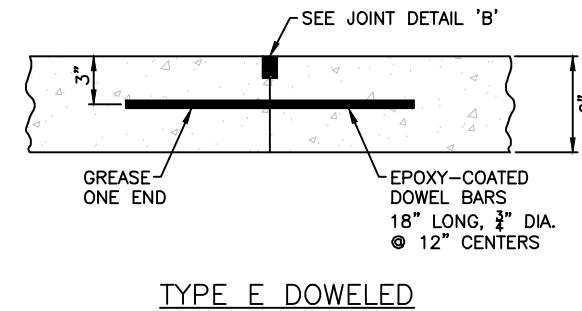
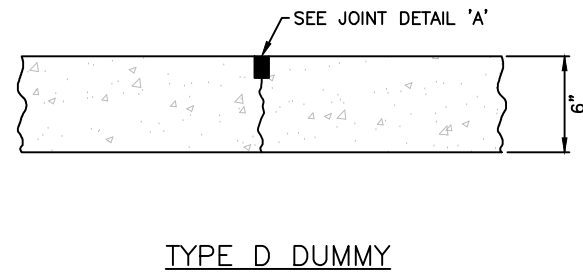
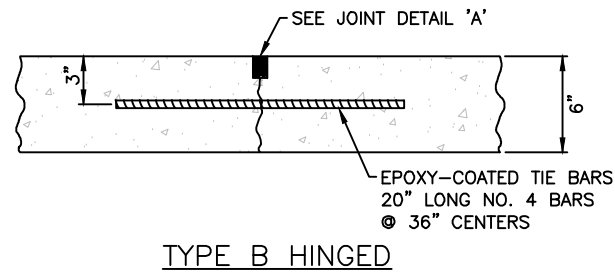


LEGEND

	REINFORCED PANEL
	PROPOSED 6" PCC PAVEMENT
—A—A—	TYPE A - THICKENED EDGE ISOLATION JOINT
—B—B—	TYPE B - HINGED CONTRACTION JOINT
—C—C—	TYPE C - EXPANSION JOINT
—D—D—	TYPE D - DUMMY CONTRACTION JOINT
—E—E—	TYPE E - DOWELED CONSTRUCTION JOINT

- JOINTING NOTES:**
1. ALL REINFORCEMENT, JOINTING, SEALING, EXPANSION JOINT MATERIAL AND THICKENED EDGES ARE INCIDENTAL TO THE 6" PCC PAVEMENT ITEM.
 2. REINFORCEMENT PANELS SHALL CONSIST OF 6x6 WELDED WIRE FABRIC REINFORCEMENT, W4.0xW4.0 CONFORMING TO THE REQUIREMENTS OF ASTM A185.
 3. WELDED WIRE FABRIC SHALL BE PLACED IN THE CENTER OF THE SLAB VERTICALLY, OVERLAPPED BY 12" AND SHALL BE 3" CLEAR FROM ANY EDGE.
 4. IN SLABS WHERE THE LENGTH-TO-WIDTH RATIO EXCEEDS 1.25, OR IN SLABS THAT ARE NOT RECTANGULAR IN SHAPE, THE SLABS MUST BE REINFORCED.
 5. SEE APRON DETAIL SHEETS FOR SPACING BETWEEN JOINTS.

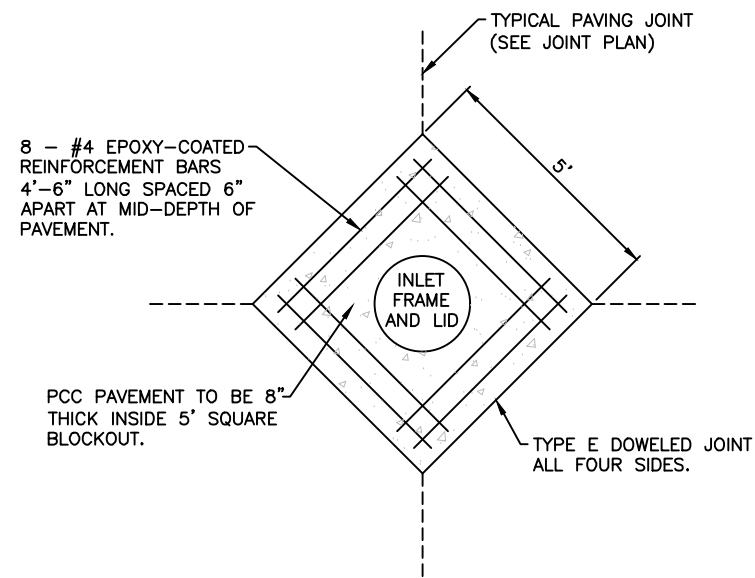
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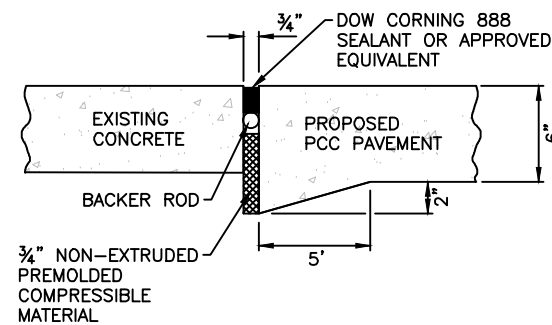
CONTRACTION JOINTS

CONSTRUCTION JOINTS

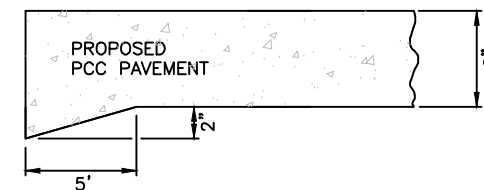
JOINT DETAIL 'A'
 NO SCALE



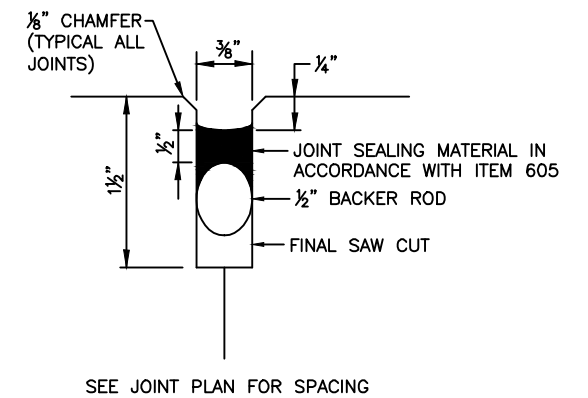
INLET BLOCKOUT DETAIL



TYPE C EXPANSION
 EXPANSION JOINTS



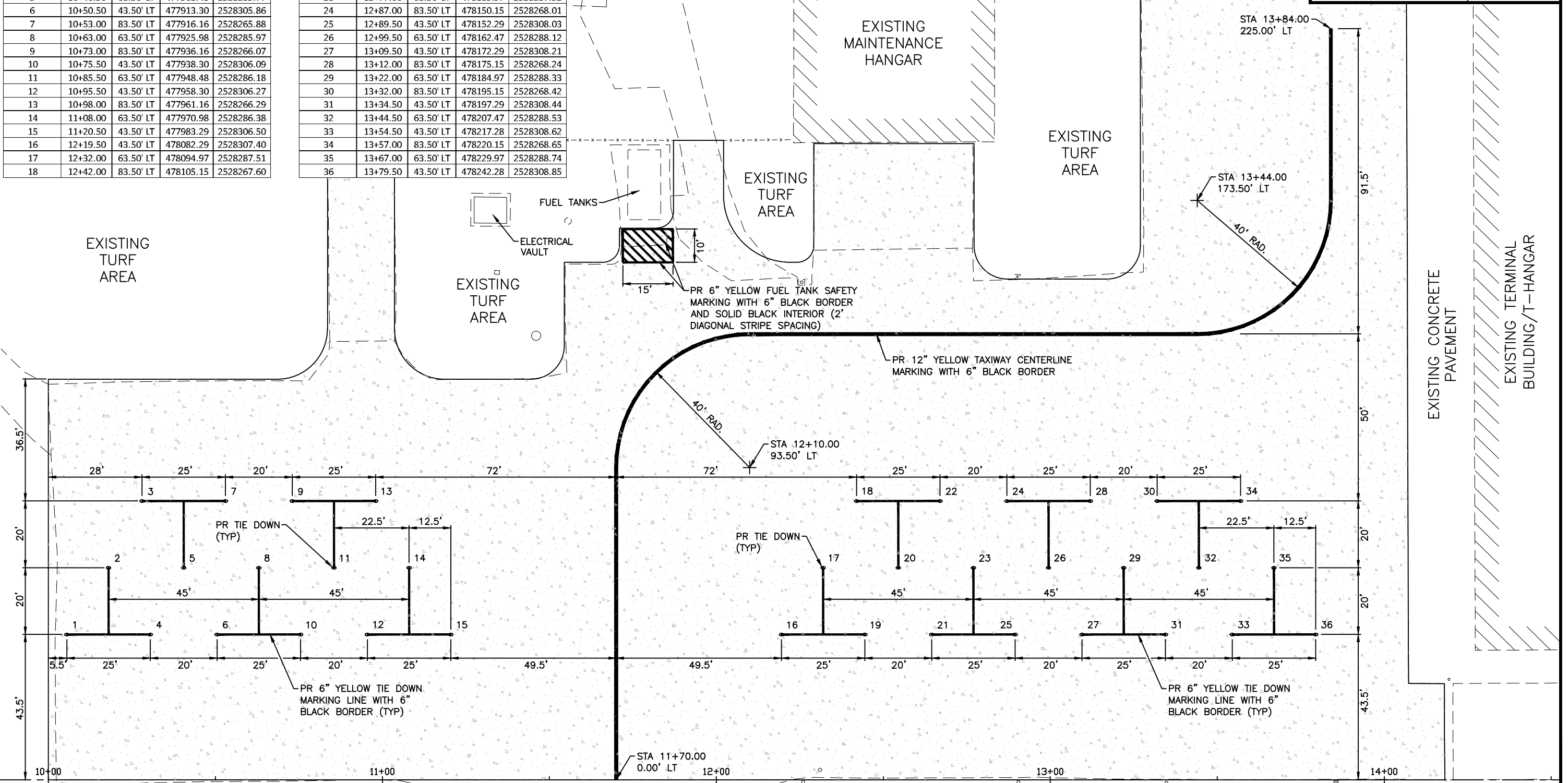
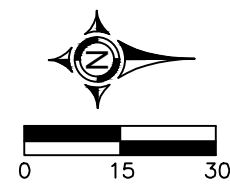
TYPE A THICKENED EDGE
 ISOLATION JOINTS



JOINT DETAIL 'B'
 NO SCALE

PROPOSED TIE DOWN SCHEDULE

TIE DOWN #	STATION	OFFSET	NORTHING	EASTING	TIE DOWN #	STATION	OFFSET	NORTHING	EASTING
1	10+05.50	43.50' LT	477868.30	2528305.45	19	12+44.50	43.50' LT	478107.29	2528307.62
2	10+18.00	63.50' LT	477880.98	2528285.57	20	12+54.50	63.50' LT	478117.47	2528287.72
3	10+28.00	83.50' LT	477891.16	2528265.66	21	12+64.50	43.50' LT	478127.29	2528307.81
4	10+30.50	43.50' LT	477893.30	2528305.68	22	12+67.00	83.50' LT	478130.15	2528267.83
5	10+40.50	63.50' LT	477903.48	2528285.77	23	12+77.00	63.50' LT	478139.97	2528287.92
6	10+50.50	43.50' LT	477913.30	2528305.86	24	12+87.00	83.50' LT	478150.15	2528268.01
7	10+53.00	83.50' LT	477916.16	2528265.88	25	12+89.50	43.50' LT	478152.29	2528308.03
8	10+63.00	63.50' LT	477925.98	2528285.97	26	12+99.50	63.50' LT	478162.47	2528288.12
9	10+73.00	83.50' LT	477936.16	2528266.07	27	13+09.50	43.50' LT	478172.29	2528308.21
10	10+75.50	43.50' LT	477938.30	2528306.09	28	13+12.00	83.50' LT	478175.15	2528268.24
11	10+85.50	63.50' LT	477948.48	2528286.18	29	13+22.00	63.50' LT	478184.97	2528288.33
12	10+95.50	43.50' LT	477958.30	2528306.27	30	13+32.00	83.50' LT	478195.15	2528268.42
13	10+98.00	83.50' LT	477961.16	2528266.29	31	13+34.50	43.50' LT	478197.29	2528308.44
14	11+08.00	63.50' LT	477970.98	2528286.38	32	13+44.50	63.50' LT	478207.47	2528288.53
15	11+20.50	43.50' LT	477983.29	2528306.50	33	13+54.50	43.50' LT	478217.28	2528308.62
16	12+19.50	43.50' LT	478082.29	2528307.40	34	13+57.00	83.50' LT	478220.15	2528268.65
17	12+32.00	63.50' LT	478094.97	2528287.51	35	13+67.00	63.50' LT	478229.97	2528288.74
18	12+42.00	83.50' LT	478105.15	2528267.60	36	13+79.50	43.50' LT	478242.28	2528308.85



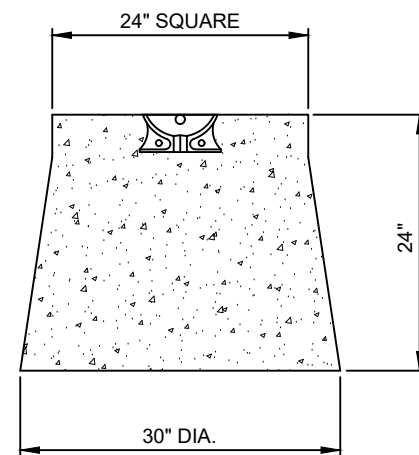
PAVEMENT MARKING - WATERBORNE SCHEDULE

DESCRIPTION	UNIT AREA (SQ FT)	NO. REQ'D	TOTAL (SQ FT)
AIRCRAFT TIEDOWN MARKING	22.5	12	270
FUEL TANK SAFETY MARKING	62	1	62
TAXIWAY CENTERLINE	405	1	405
TOTAL YELLOW			737
TOTAL WATERBORNE			737

PAVEMENT MARKING - BLACK BORDER SCHEDULE

DESCRIPTION	UNIT AREA (SQ FT)	NO. REQ'D	TOTAL (SQ FT)
AIRCRAFT TIEDOWN MARKING	45	12	540
FUEL TANK SAFETY MARKING	128	1	128
TAXIWAY CENTERLINE	405	1	405
TOTAL BLACK BORDER			1073

13 Jan 2020 - 8:42am X:\2018\18199\oc\Plans\18199 Base.dwg: Layout Tab TIEDOWN AND MARKINGS

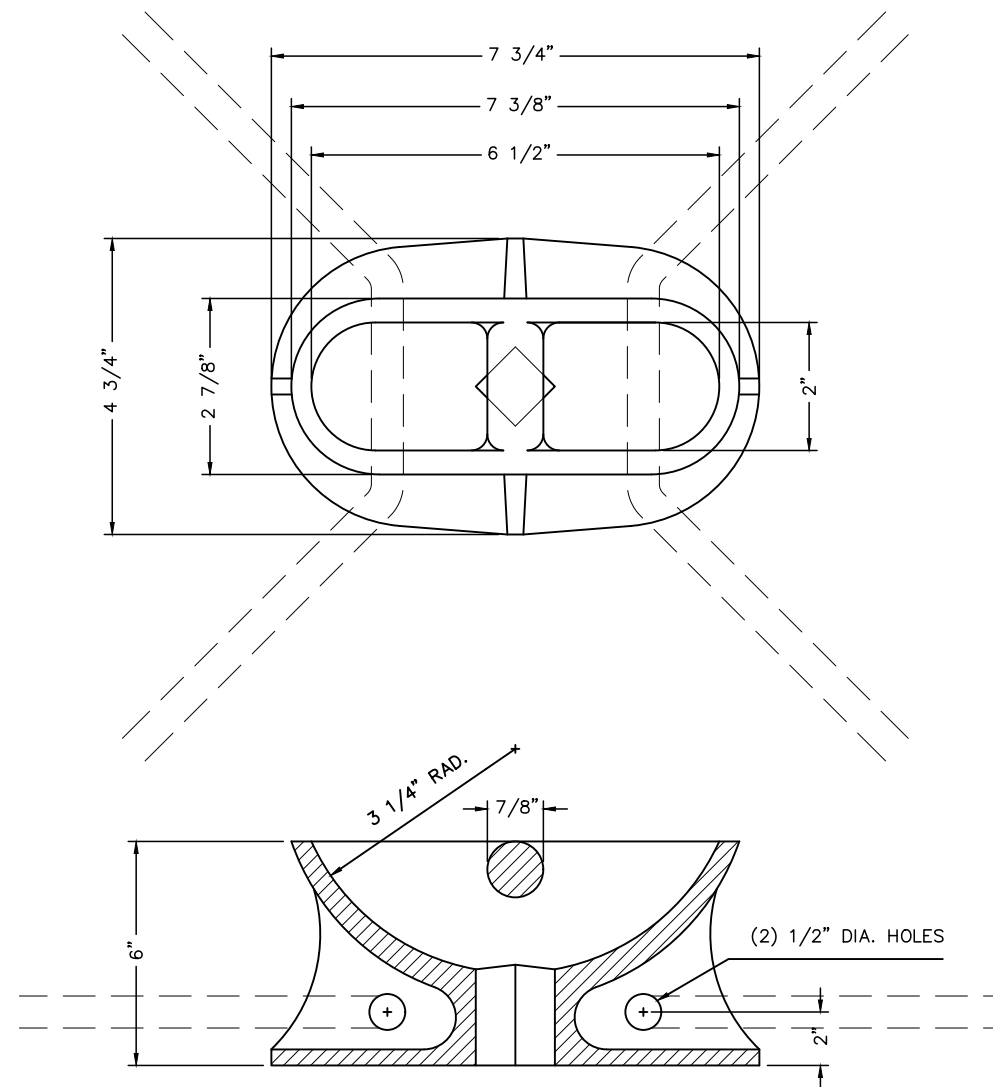


EXISTING TIE DOWN DETAIL

NO SCALE

TIE DOWN REMOVAL/BACKFILL NOTES:

1. EXCAVATION, DISPOSAL AND BACKFILL SHALL BE INCIDENTAL TO REMOVE TIE DOWN PAY ITEM.
2. ADJACENT PAVEMENT REMOVAL TO BE PAID FOR SEPARATELY.
3. TIE DOWN VOID SHALL BE BACKFILLED WITH COMPACTED CA-6 AGGREGATE MATERIAL.

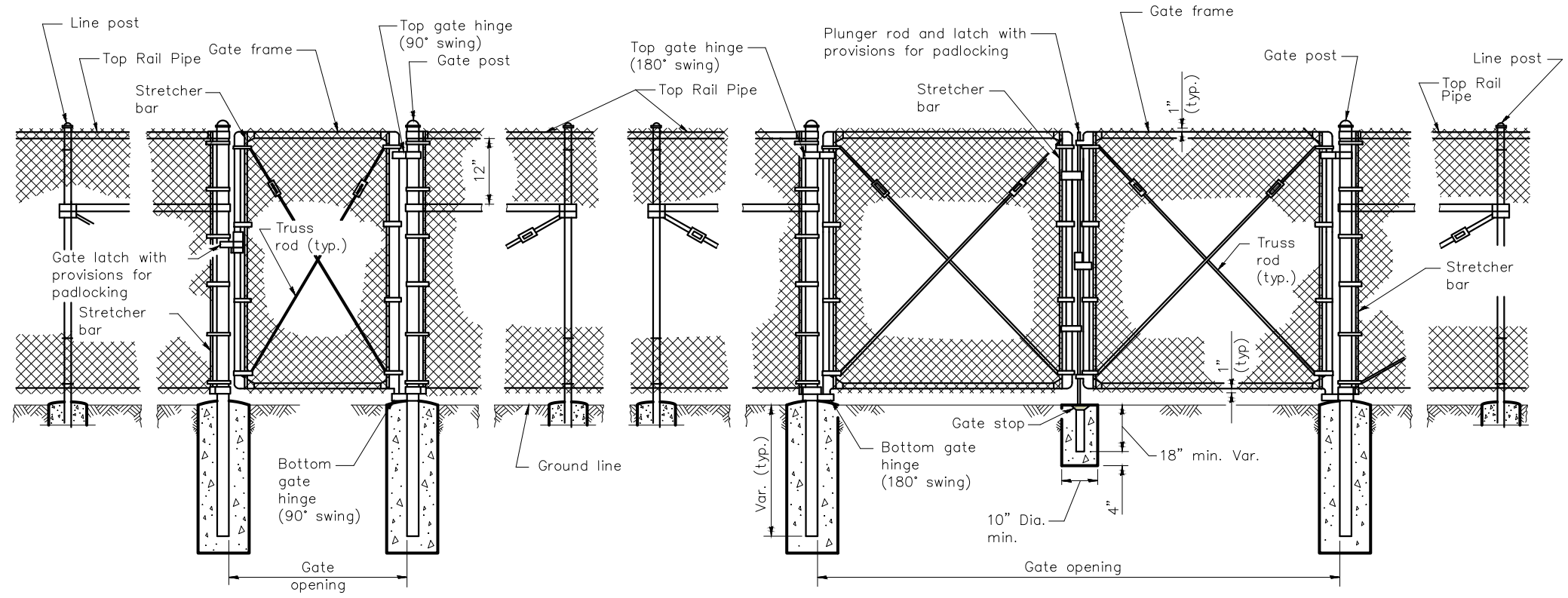


MOORING EYE DETAILS

NO SCALE

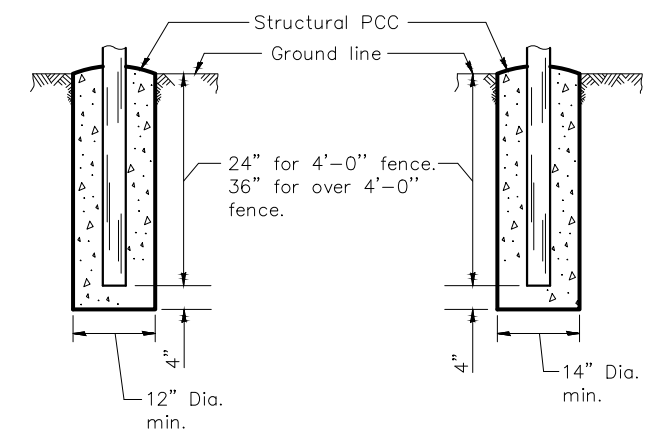
PROPOSED TIE DOWN/MOORING EYE NOTES:

1. MOORING EYE CASTINGS SHALL BE NEENAH R-3490-A, EJCO 00599600, OR APPROVED EQUIVALENT.
2. ANCHOR RODS SHALL BE #3 DEFORMED REBAR, 36" LONG AND SHALL BE BENT HORIZONTALLY AT 45° IN CENTER OF PAVEMENT SLAB.
3. METHOD OF PLACEMENT SHALL BE IN ACCORDANCE WITH THE TIE DOWN PLAN AND SHALL BE APPROVED BY THE RESIDENT ENGINEER.



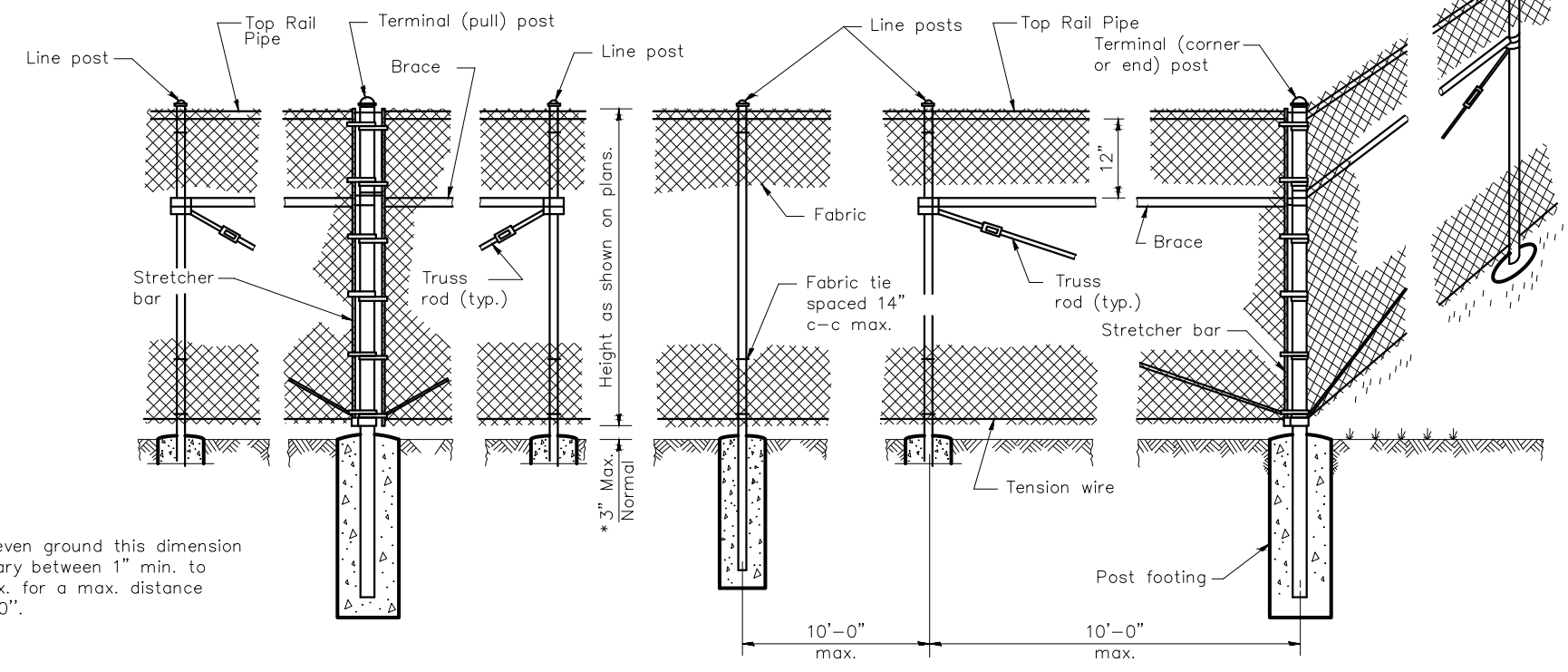
PEDESTRIAN GATE ARRANGEMENT

VEHICLE GATE ARRANGEMENT



FOOTING FOR LINE POST

FOOTING FOR GATE & TERMINAL POST



PULL POST ARRANGEMENT

LINE POST ARRANGEMENT

CORNER OR END POST ARRANGEMENT

* On uneven ground this dimension may vary between 1" min. to 3" max. for a max. distance of 8'-0".

NOTES

1. PULL POSTS SHALL BE PLACED AT LOCATIONS DETERMINED BY THE ENGINEER. THEY SHALL BE PLACED AT 660' INTERVALS BETWEEN POSTS TO WHICH THE ENDS OF THE FABRIC ARE CLAMPED OR MIDWAY BETWEEN SUCH POSTS WHEN THE DISTANCE IS LESS THAN 1320' AND GREATER THAN 660'.
2. CONTINUOUS FENCE SHALL BE GROUNDED AT INTERVALS NOT EXCEEDING 500 FT. EXCEPT THERE SHALL BE A GROUND NOT EXCEEDING 100 FT FROM GATE IN EACH SECTION OF THE FENCE ADJACENT TO THE GATE.
3. FENCE UNDER POWER LINES SHALL BE GROUNDED BY THREE GROUNDS, ONE DIRECTLY UNDER THE CROSSING AND ONE ON EACH SIDE 25 TO 50 FT AWAY. A SINGLE GROUND SHALL BE LOCATED DIRECTLY UNDER EACH TELEPHONE WIRE OR CABLE CROSSING.
4. THE COUNTERPOISE SHALL BE USED ONLY WHERE IT IS IMPOSSIBLE TO DRIVE A GROUND ROD BECAUSE OF IMPERVIOUS EARTH STRUCTURES.
5. THE GROUND WIRE SHALL BE CONNECTED TO THE FABRIC AND THE GROUND ROD BY A MECHANICAL CLAMP OF CAST BRONZE BODY AND BRONZE OR STAINLESS STEEL BOLTS AND WASHERS. WHEN A TENSION WIRE IS REQUIRED, THE BOTTOM CONNECTION OF THE GROUND WIRE SHALL BE MADE TO THE TENSION WIRE.
6. ALL PROPOSED CLASS E FENCE SHALL MEET THE REQUIREMENTS OF IDOT STANDARD 664001 LATEST REVISION, EXCEPT WHERE REVISED ON THESE DETAILS.

