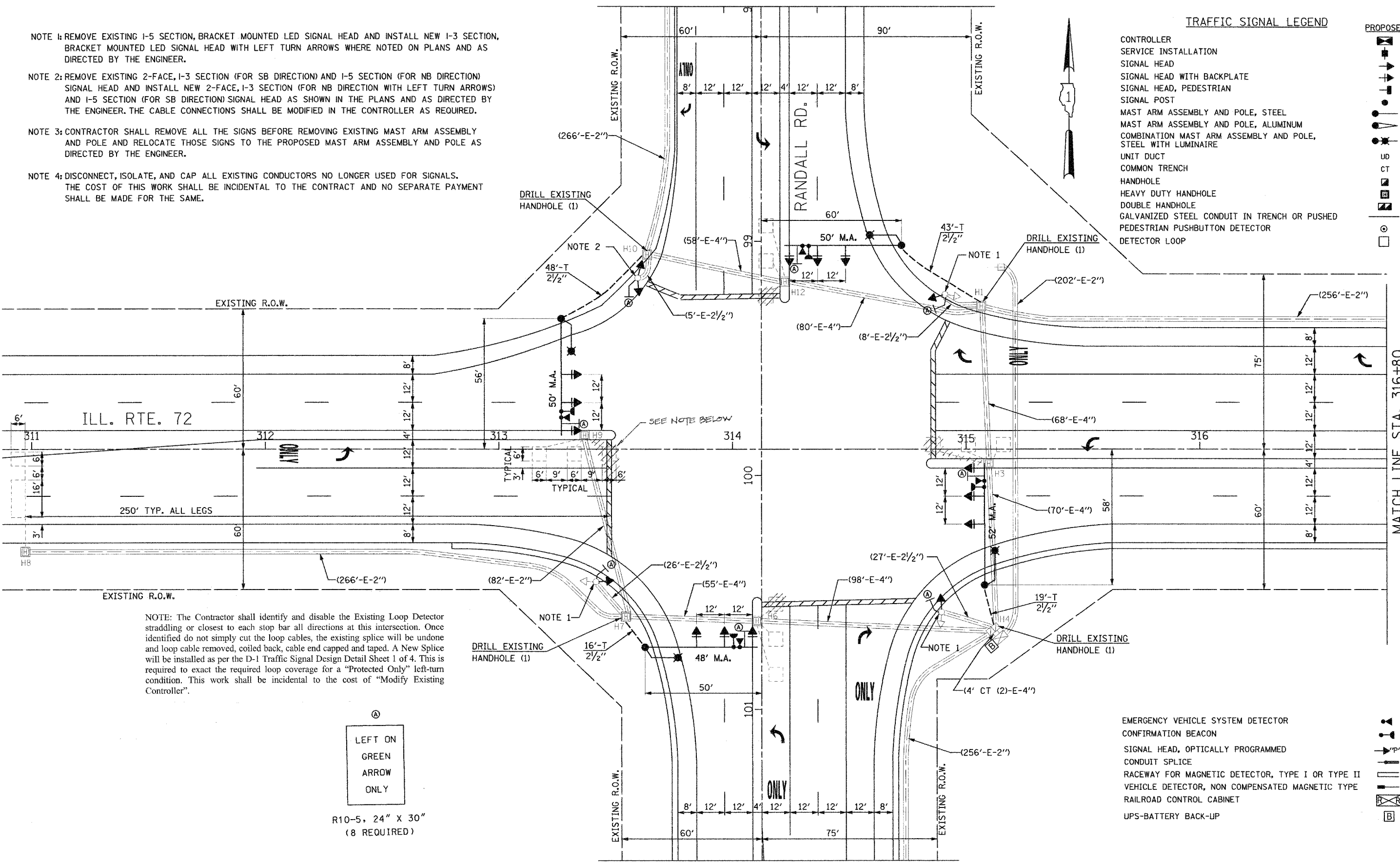


MATCH LINE STA. 98+00
(SEE SHEET 2 OF 2)

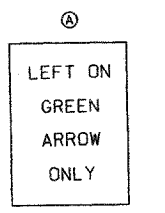
- NOTE 1: REMOVE EXISTING I-5 SECTION, BRACKET MOUNTED LED SIGNAL HEAD AND INSTALL NEW I-3 SECTION, BRACKET MOUNTED LED SIGNAL HEAD WITH LEFT TURN ARROWS WHERE NOTED ON PLANS AND AS DIRECTED BY THE ENGINEER.
- NOTE 2: REMOVE EXISTING 2-FACE, I-3 SECTION (FOR SB DIRECTION) AND I-5 SECTION (FOR NB DIRECTION) SIGNAL HEAD AND INSTALL NEW 2-FACE, I-3 SECTION (FOR NB DIRECTION WITH LEFT TURN ARROWS) AND I-5 SECTION (FOR SB DIRECTION) SIGNAL HEAD AS SHOWN IN THE PLANS AND AS DIRECTED BY THE ENGINEER. THE CABLE CONNECTIONS SHALL BE MODIFIED IN THE CONTROLLER AS REQUIRED.
- NOTE 3: CONTRACTOR SHALL REMOVE ALL THE SIGNS BEFORE REMOVING EXISTING MAST ARM ASSEMBLY AND POLE AND RELOCATE THOSE SIGNS TO THE PROPOSED MAST ARM ASSEMBLY AND POLE AS DIRECTED BY THE ENGINEER.
- NOTE 4: DISCONNECT, ISOLATE, AND CAP ALL EXISTING CONDUCTORS NO LONGER USED FOR SIGNALS. THE COST OF THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE FOR THE SAME.

TRAFFIC SIGNAL LEGEND

- | | | | |
|--|--|----------|--|
| CONTROLLER | | EXISTING | |
| SERVICE INSTALLATION | | EXISTING | |
| SIGNAL HEAD | | EXISTING | |
| SIGNAL HEAD WITH BACKPLATE | | EXISTING | |
| SIGNAL HEAD, PEDESTRIAN | | EXISTING | |
| SIGNAL POST | | EXISTING | |
| MAST ARM ASSEMBLY AND POLE, STEEL | | EXISTING | |
| MAST ARM ASSEMBLY AND POLE, ALUMINUM | | EXISTING | |
| COMBINATION MAST ARM ASSEMBLY AND POLE, STEEL WITH LUMINAIRE | | EXISTING | |
| UNIT DUCT | | EXISTING | |
| COMMON TRENCH | | EXISTING | |
| HANDHOLE | | EXISTING | |
| HEAVY DUTY HANDHOLE | | EXISTING | |
| DOUBLE HANDHOLE | | EXISTING | |
| GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED | | EXISTING | |
| PEDESTRIAN PUSHBUTTON DETECTOR | | EXISTING | |
| DETECTOR LOOP | | EXISTING | |



NOTE: The Contractor shall identify and disable the Existing Loop Detector straddling or closest to each stop bar all directions at this intersection. Once identified do not simply cut the loop cables, the existing splice will be undone and loop cable removed, coiled back, cable end capped and taped. A New Splice will be installed as per the D-1 Traffic Signal Design Detail Sheet 1 of 4. This is required to exact the required loop coverage for a "Protected Only" left-turn condition. This work shall be incidental to the cost of "Modify Existing Controller".



R10-5, 24" X 30"
(8 REQUIRED)

- | | | | |
|--|--|----------|--|
| EMERGENCY VEHICLE SYSTEM DETECTOR | | EXISTING | |
| CONFIRMATION BEACON | | EXISTING | |
| SIGNAL HEAD, OPTICALLY PROGRAMMED | | EXISTING | |
| CONDUIT SPLICE | | EXISTING | |
| RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II | | EXISTING | |
| VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE | | EXISTING | |
| RAILROAD CONTROL CABINET | | EXISTING | |
| UPS-BATTERY BACK-UP | | EXISTING | |

MATCH LINE STA. 101+65
(SEE SHEET 2 OF 2)

FILE NAME =	USER NAME = RDP	DESIGNED - PKG/RDP	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC SIGNAL MODIFICATION PLAN ILLINOIS ROUTE 72 AT RANDALL ROAD (SHEET 1 OF 2)				F.A. - RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
jt:\projects\projects 2007\ill_rte_72_randall_rd\08_Randall_road_ill_72_INT.dgn	DRAWN - MAA/RDP	REVISED -	REVISED -		SCALE: 1"=20'	SHEET NO.	OF	SHEETS	STA.	TO STA.	341	2007-059 L	KANE	21	08
PLOT SCALE = 20,0000' / IN.	CHECKED - PKG	REVISED -	REVISED -												
PLOT DATE = 12/11/2007	DATE - 12-06-2007	REVISED -	REVISED -												
										CONTRACT NO. 60098				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	