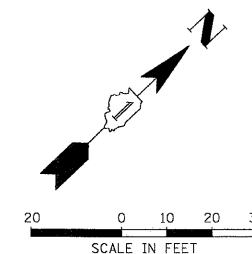


F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2320	113X-N	WILL	133	79
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

NOTES FOR TEMPORARY TRAFFIC SIGNALS

1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1. INSTALLED IN A NEMA TS1 OR TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE 12" (300mm). HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL. AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.



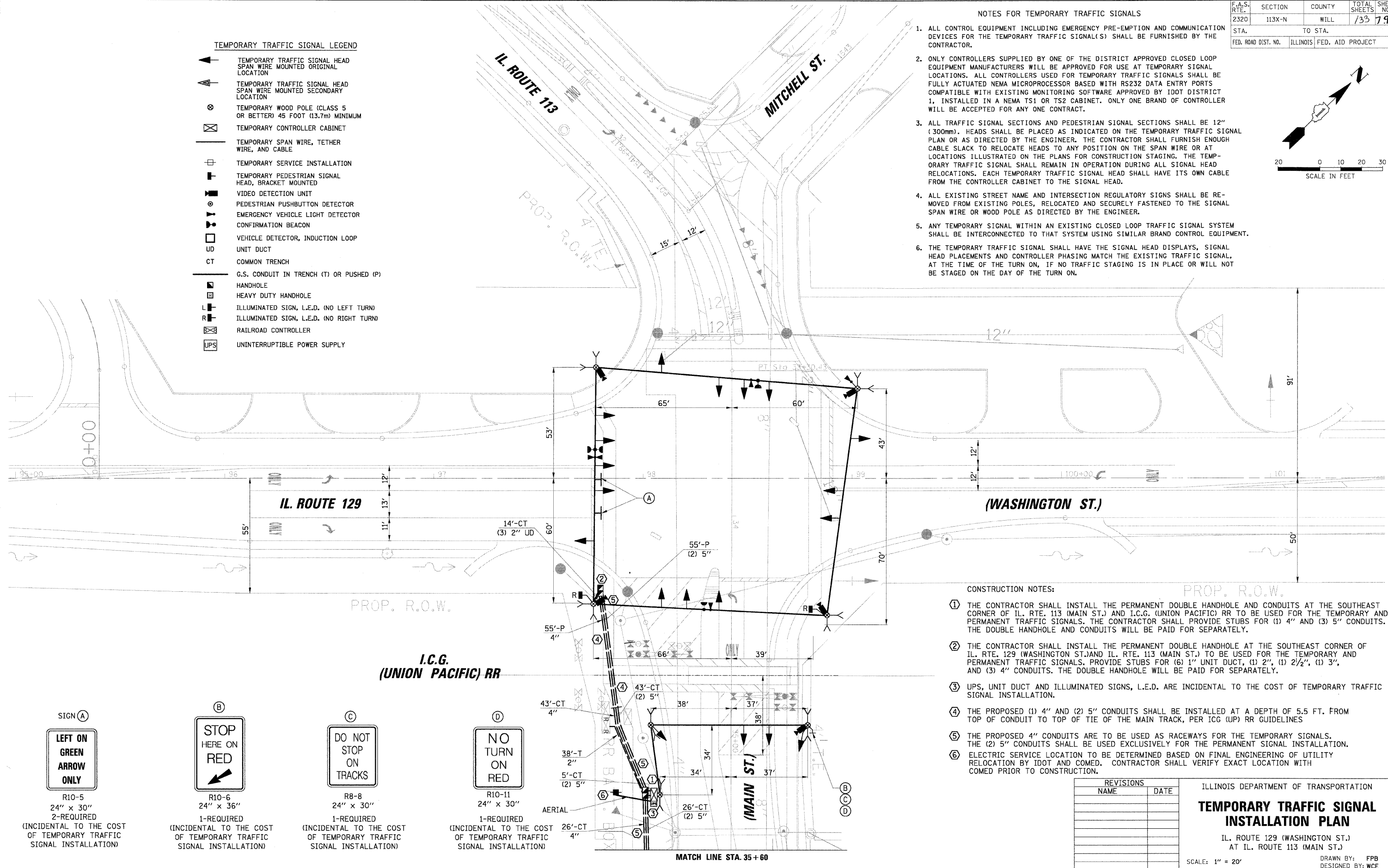
TEMPORARY TRAFFIC SIGNAL LEGEND

- ← TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED ORIGINAL LOCATION
- ▲ TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED SECONDARY LOCATION
- ⊙ TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM
- ⊠ TEMPORARY CONTROLLER CABINET
- TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE
- ⊕ TEMPORARY SERVICE INSTALLATION
- ▬ TEMPORARY PEDESTRIAN SIGNAL HEAD, BRACKET MOUNTED
- VIDEO DETECTION UNIT
- ⊙ PEDESTRIAN PUSHBUTTON DETECTOR
- ▼ EMERGENCY VEHICLE LIGHT DETECTOR
- CONFIRMATION BEACON
- ⊞ VEHICLE DETECTOR, INDUCTION LOOP
- UD UNIT DUCT
- CT COMMON TRENCH
- G.S. CONDUIT IN TRENCH (T) OR PUSHED (P)
- HANDHOLE
- ▣ HEAVY DUTY HANDHOLE
- L ILLUMINATED SIGN, L.E.D. (NO LEFT TURN)
- R ILLUMINATED SIGN, L.E.D. (NO RIGHT TURN)
- ⊠ RAILROAD CONTROLLER
- UPS UNINTERRUPTIBLE POWER SUPPLY

DATE	BY	REVISIONS
DATE	BY	REVISIONS

PROFILE
 DATE: _____ BY: _____
 NOTE BOOK NO. _____
 STRUCTURE NOTATION CHRG. _____
 CHANGES CHECKED _____
 STRUCTURE NOTATION CHRG. _____

CHRISTOPHER B. BURKE ENGINEERING LTD.
 100 West High Road, Suite 600
 Rosemont, Illinois 60018
 (847) 823-0500



CONSTRUCTION NOTES:

1. THE CONTRACTOR SHALL INSTALL THE PERMANENT DOUBLE HANDHOLE AND CONDUITS AT THE SOUTHEAST CORNER OF IL. RTE. 113 (MAIN ST.) AND I.C.G. (UNION PACIFIC) RR TO BE USED FOR THE TEMPORARY AND PERMANENT TRAFFIC SIGNALS. THE CONTRACTOR SHALL PROVIDE STUBS FOR (1) 4" AND (3) 5" CONDUITS. THE DOUBLE HANDHOLE AND CONDUITS WILL BE PAID FOR SEPARATELY.
2. THE CONTRACTOR SHALL INSTALL THE PERMANENT DOUBLE HANDHOLE AND CONDUITS AT THE SOUTHEAST CORNER OF IL. RTE. 129 (WASHINGTON ST.) AND IL. RTE. 113 (MAIN ST.) TO BE USED FOR THE TEMPORARY AND PERMANENT TRAFFIC SIGNALS. PROVIDE STUBS FOR (6) 1" UNIT DUCT, (1) 2", (1) 2 1/2", (1) 3", AND (3) 4" CONDUITS. THE DOUBLE HANDHOLE WILL BE PAID FOR SEPARATELY.
3. UPS, UNIT DUCT AND ILLUMINATED SIGNS, L.E.D. ARE INCIDENTAL TO THE COST OF TEMPORARY TRAFFIC SIGNAL INSTALLATION.
4. THE PROPOSED (1) 4" AND (2) 5" CONDUITS SHALL BE INSTALLED AT A DEPTH OF 5.5 FT. FROM TOP OF CONDUIT TO TOP OF TIE OF THE MAIN TRACK, PER ICG (UP) RR GUIDELINES
5. THE PROPOSED 4" CONDUITS ARE TO BE USED AS RACEWAYS FOR THE TEMPORARY SIGNALS. THE (2) 5" CONDUITS SHALL BE USED EXCLUSIVELY FOR THE PERMANENT SIGNAL INSTALLATION.
6. ELECTRIC SERVICE LOCATION TO BE DETERMINED BASED ON FINAL ENGINEERING OF UTILITY RELOCATION BY IDOT AND COMED. CONTRACTOR SHALL VERIFY EXACT LOCATION WITH COMED PRIOR TO CONSTRUCTION.



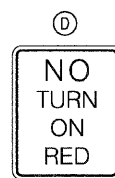
R10-5
24" x 30"
2-REQUIRED
(INCIDENTAL TO THE COST OF TEMPORARY TRAFFIC SIGNAL INSTALLATION)



R10-6
24" x 36"
1-REQUIRED
(INCIDENTAL TO THE COST OF TEMPORARY TRAFFIC SIGNAL INSTALLATION)



R8-8
24" x 30"
1-REQUIRED
(INCIDENTAL TO THE COST OF TEMPORARY TRAFFIC SIGNAL INSTALLATION)



R10-11
24" x 30"
1-REQUIRED
(INCIDENTAL TO THE COST OF TEMPORARY TRAFFIC SIGNAL INSTALLATION)

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN

IL. ROUTE 129 (WASHINGTON ST.)
AT IL. ROUTE 113 (MAIN ST.)

SCALE: 1" = 20'
DATE: 5/9/2007

DRAWN BY: FBP
DESIGNED BY: WCE
CHECKED BY: MJT/GMZ

MATCH LINE STA. 35 + 60