

Bench Mark: Cut "□" at southeast approach parapet 17.72 ft. left of Sta. 652+15.82, Elevation 541.87.

Existing Structure: The structure was originally built in 1922. In 1960, the superstructure was removed and replaced with precast prestressed concrete deck beams. The substructure was widened and approach spans were added. A concrete overlay was added in 1996. The structure consists of a single span PPC deck beam superstructure on timber pile supported closed abutments. The abutments support an approach span at each end consisting of 12½" thick simply supported reinforced concrete slab. The back to back of approach bents measures ±97'-9" (field measurements), while the out to out width measures 36'-4". Bridge mainspan superstructure shall be removed and replaced with new beams and reinforced concrete wearing surface. Partial road closure with a signalized one lane crossing will be used during construction.

No salvage of existing deck beams.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 1
FAP 310	57BR-1	JERSEY	25	10	14 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

Contract #76B20

INDEX OF SHEETS

Sheet No.	Description
1	General Plan
2	Gen. Notes, Details & Total Bill of Mat'l
3	Stage Construction Details
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5	Superstructure
6-10	Superstructure Details
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12	South Abutment Joint Details
13	Abutment Repairs
14	Bar Splicer Assembly Details

LOADING HS20-44

No allowance for future wearing surface.

DESIGN SPECIFICATIONS

2002 AASHTO

DESIGN STRESSES

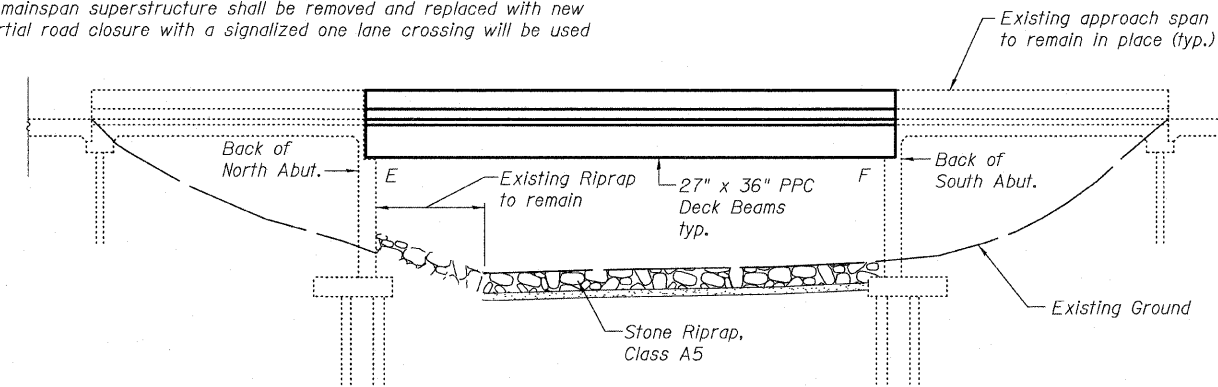
FIELD UNITS

$f'_c = 3,500$ psi
 $f_y = 60,000$ psi (reinforcement)

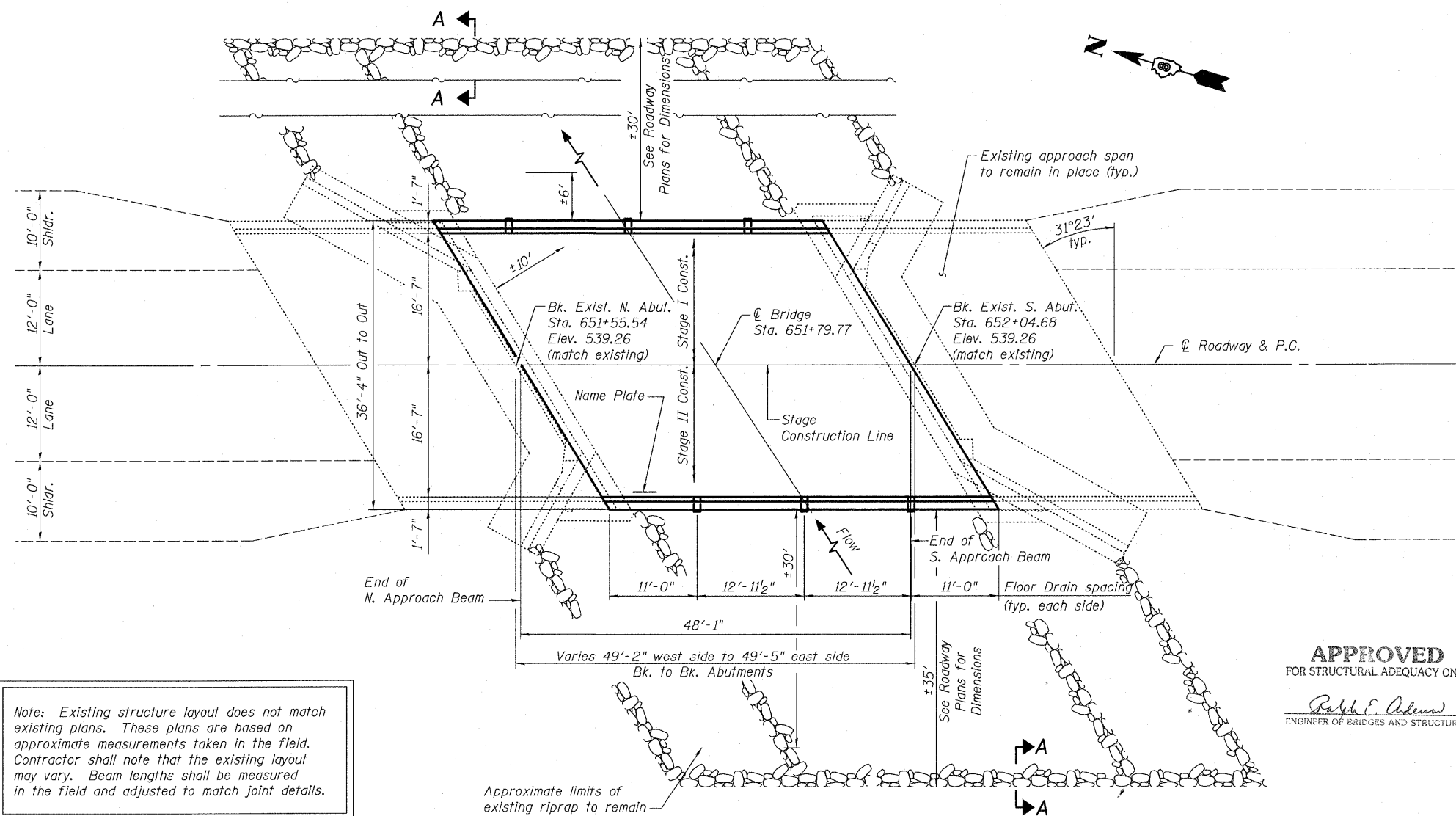
PRECAST PRESTRESSED UNITS

$f'_c = 5,000$ psi
 $f'_a = 4,000$ psi
 $f'_s = 270,000$ psi (½" low lax. strands)
 $f'_{sl} = 201,960$ psi (½" low lax. strands)

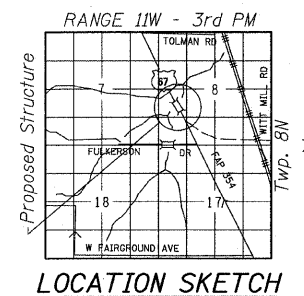
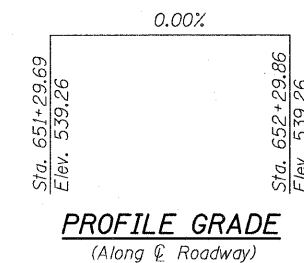
Notes:
① See sheet 2 of 14 for Section A-A.



ELEVATION



PLAN



STATION 651+79.77
RE-BUILT 200 BY
STATE OF ILLINOIS
FAP ROUTE 310 - SEC 57BR-1
LOADING HS20
STRUCTURE NO. 042-0001

NAME PLATE

See Std. 515001
Existing Name Plate shall be cleaned and relocated adjacent to new Name Plate.

APPROVED
FOR STRUCTURAL ADEQUACY ONLY
Ralph E. Adams
ENGINEER OF BRIDGES AND STRUCTURES

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11/15/08
EXPIRES 11/30/08

GENERAL PLAN
US ROUTE 67 OVER
DEARCY CREEK
FAP ROUTE 310 SECTION 57BR-1
JERSEY COUNTY
STATION 651+79.77
STRUCTURE NO. 042-0001

Note: Existing structure layout does not match existing plans. These plans are based on approximate measurements taken in the field. Contractor shall note that the existing layout may vary. Beam lengths shall be measured in the field and adjusted to match joint details.