

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR LIST OF STATE AND LOCAL STANDARDS, SEE SHEET NO. 2

PROJECT LOCATED WITHIN THE VILLAGE OF BURR RIDGE

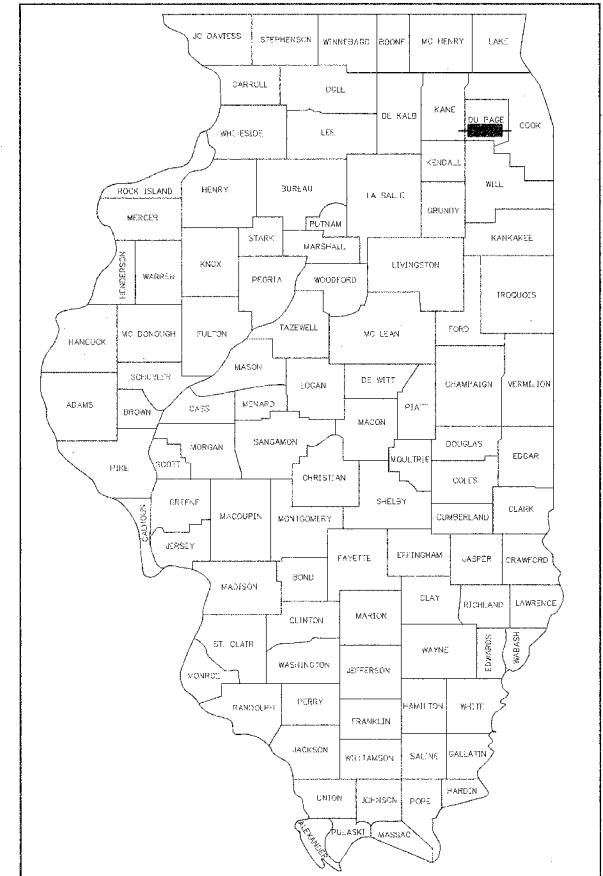
TRAFFIC DATA - 83RD STREET
POSTED SPEED: 30MPH
DESIGN SPEED: 30MPH
CURRENT ADT (2005): 4,420 VPD

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1552	07-00037-00-SW	DUPAGE	17	1
FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT NO. 83996	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED
FEDERAL AID HIGHWAY

FAU RTE. 1552 (83rd STREET)
SECTION NO.: 07-00037-00-SW
SOUTH FRONTAGE ROAD TO COUNTY LINE ROAD (FAU 2684)
SIDEWALK CONSTRUCTION
PROJECT NO.: M-8003 (930)
DUPAGE COUNTY
C-91-112-08



LOCATION OF SECTION INDICATED THUS: - [black box] -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

Approved DEC 19 20 07

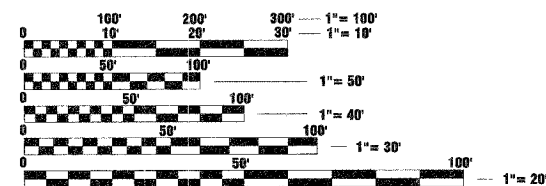
PAUL D. MAY, P.E.
VILLAGE OF BURR RIDGE, VILLAGE ENGINEER

Passed DECEMBER 21 2007

CHRISTOPHER HOLT
DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

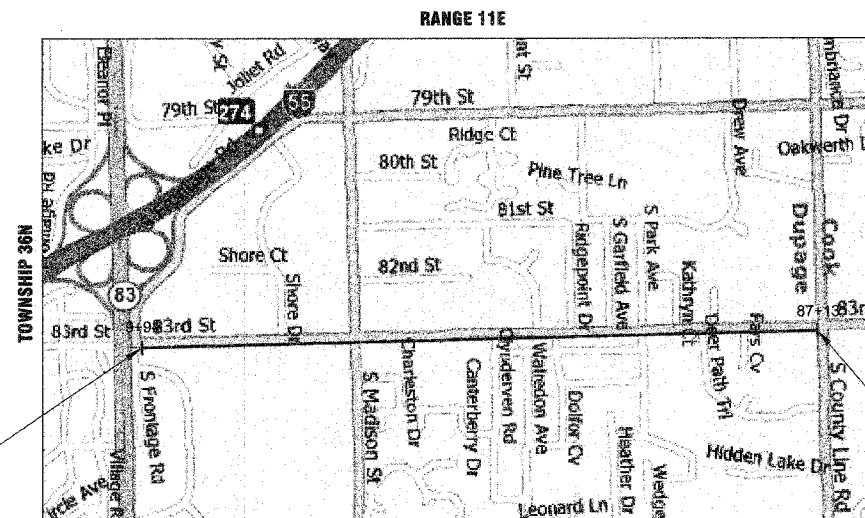
Relinquishing for Bid Based on Limited Review Dec 24 20 07

DONE OLL
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811



PROJECT BEGINS STATION 9+94

PROJECT ENDS STATION 87+13

LOCATION MAP

SCALE: 1" = 1000 MILES

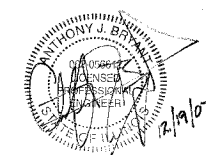
TOWNSHIP: 36 NORTH
RANGE: 11 EAST

GROSS LENGTH OF PROJECT = 7,736 LINEAL FEET (1.47 MILES)
NET LENGTH OF PROJECT = 7,736 LINEAL FEET (1.47 MILES)

IL DESIGN FIRM
NO. 184-001310



EXP. 04/30/09



ANTHONY J. BRYANT, P.E. ENGINEER
ILLINOIS REGISTRATION No. 062-056612
EXPIRATION DATE: 11/30/2009
APPLIES TO SHEETS 1-17

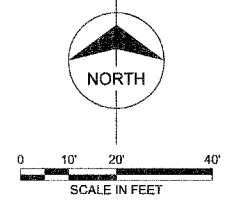
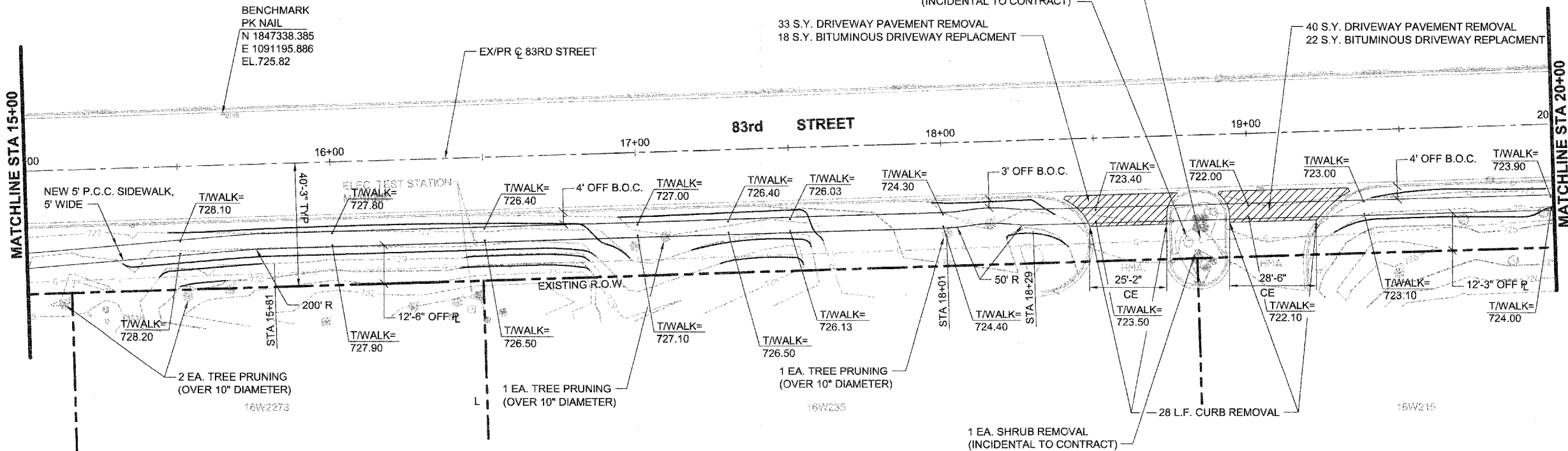
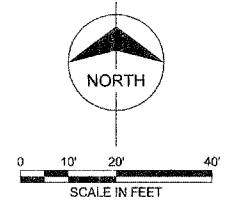
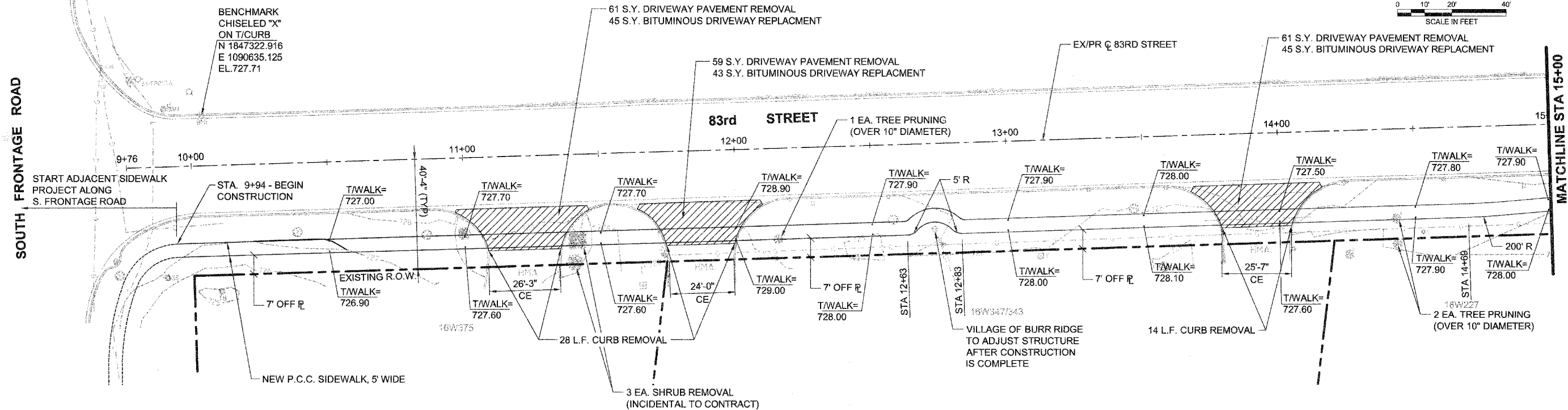
PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

FIELD ENGINEER: MARLIN SOLOMON 847-705-4407

CONTRACT NO. 83996

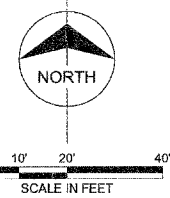
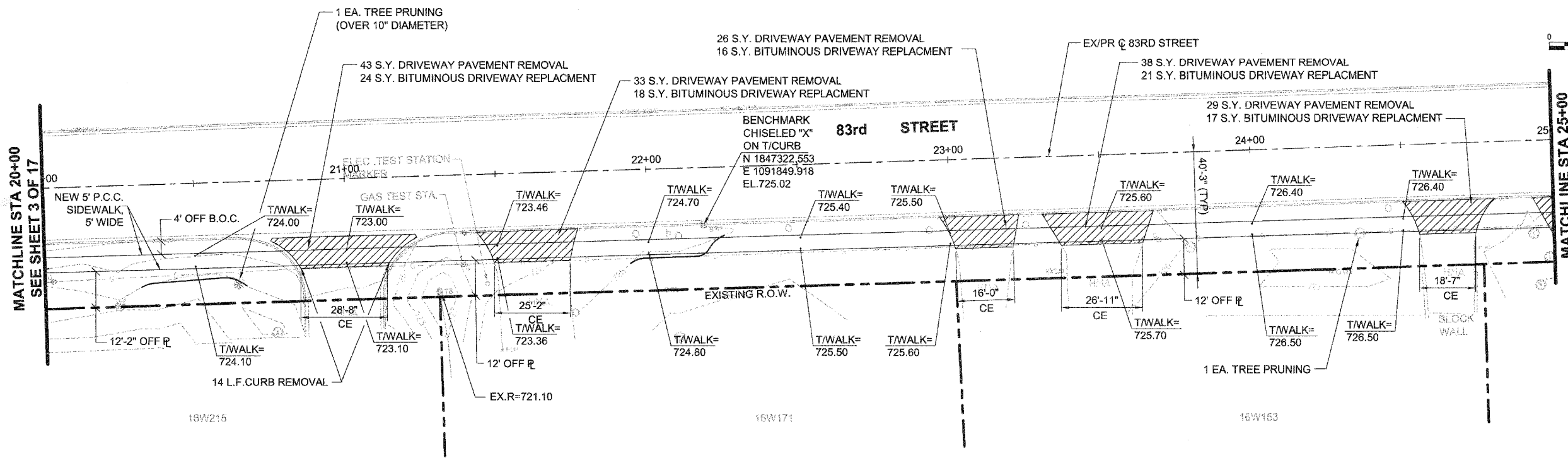
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NO. L. BOOK	
NO. OF SHEETS	
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STRUCTURE NOTATION'S OF NO.	

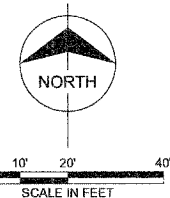
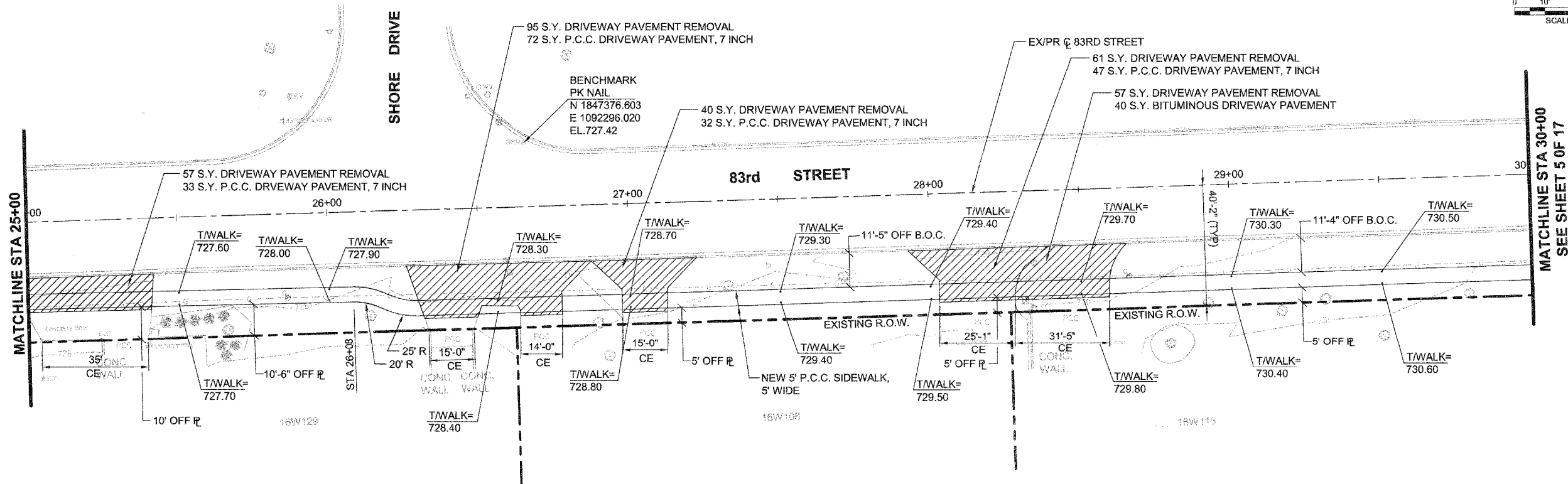


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		DATE --	REVISED --		STA. 9+76.00 TO STA. 20+00.00	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	

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PROFILE	DATE
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FILE NAME	



FILE NAME = 83RD115

USER NAME = JUSER

PLOT SCALE = 1"=20'

PLOT DATE = 11/11/15

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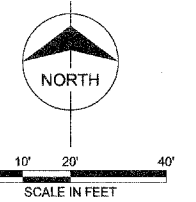
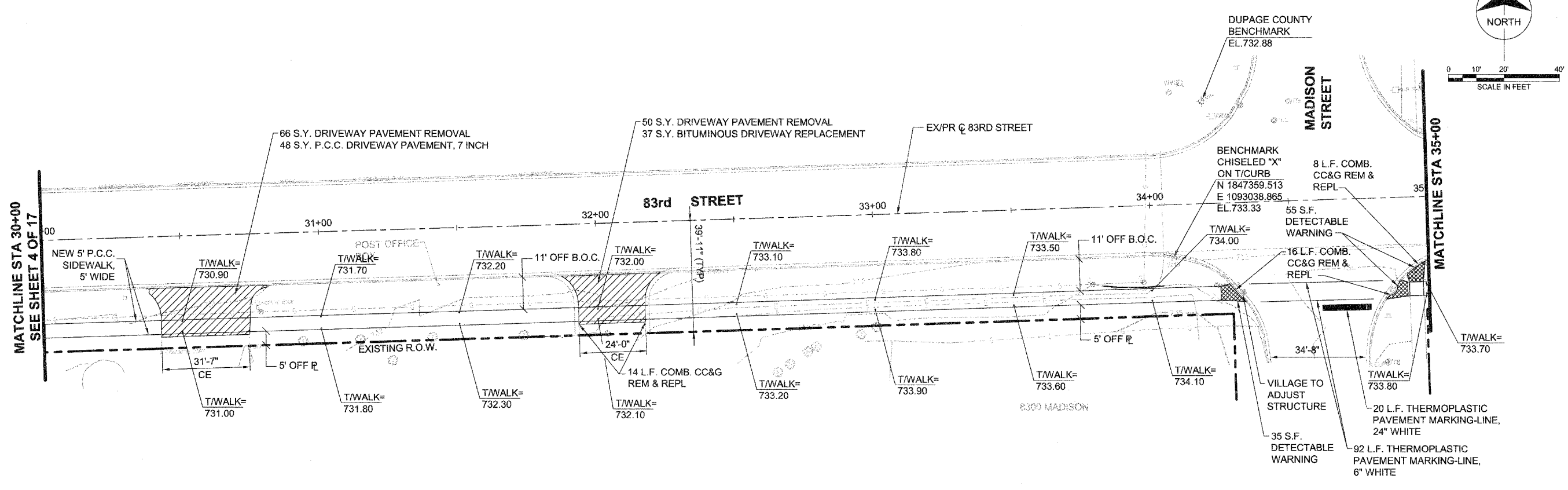
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

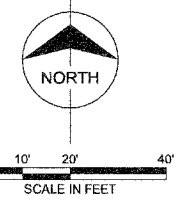
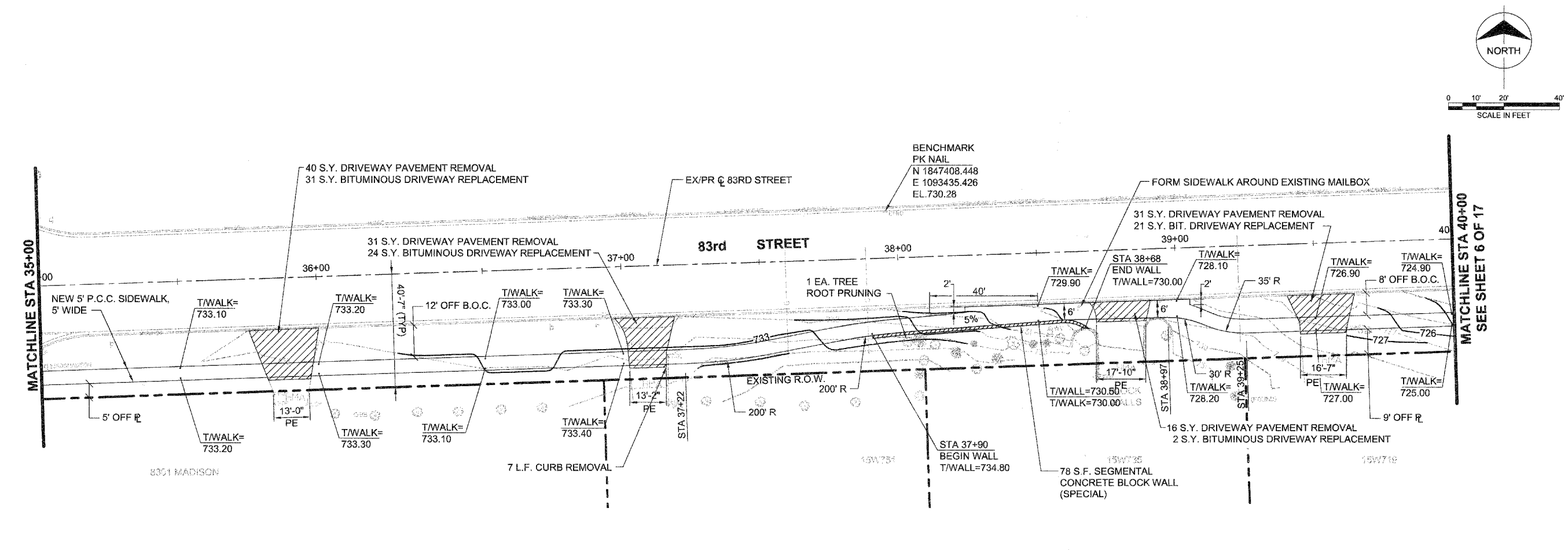
SCALE: 1"=20' SHEET NO. 4 OF 13 SHEETS STA. 20+00.00 TO STA. 30+00.00

F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1552	07-00037-00-SW	DUPAGE	17	4
83rd STREET CONSTRUCTION			CONTRACT NO. 83996	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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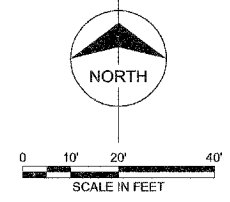
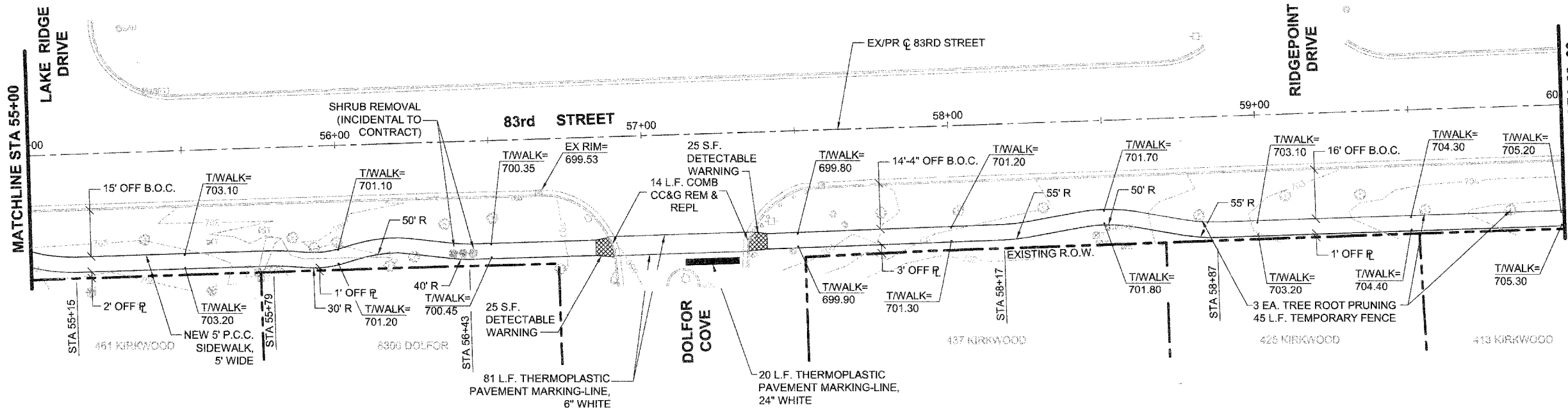
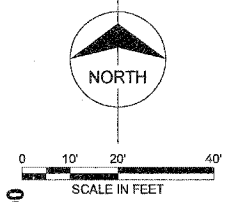
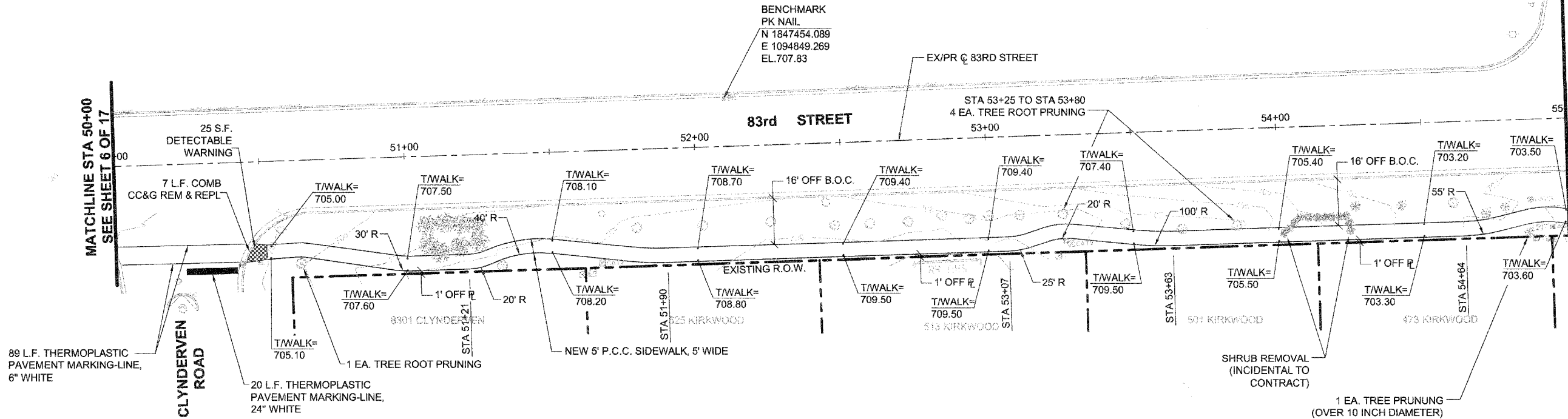
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		DATE --	REVISED --		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
					SCALE: 1"=20'	SHEET NO. 5 OF 13 SHEETS		STA. 30+00.00 TO STA. 40+00.00	

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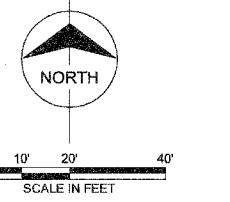
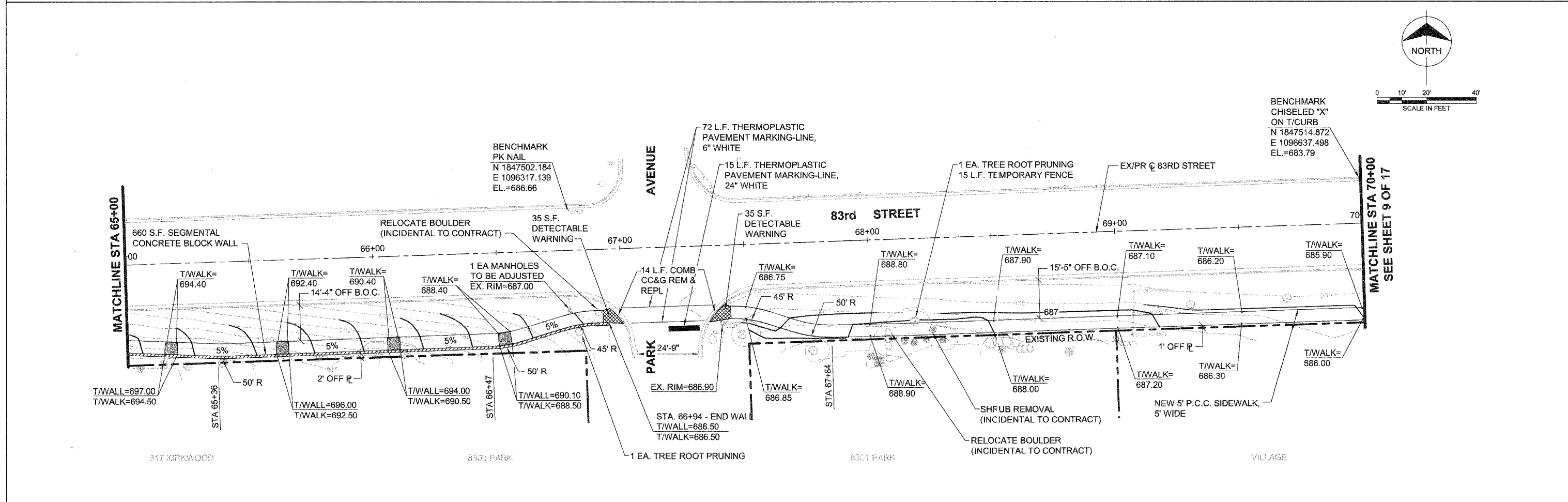
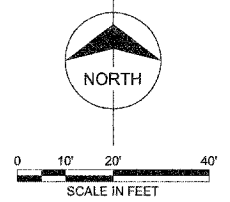
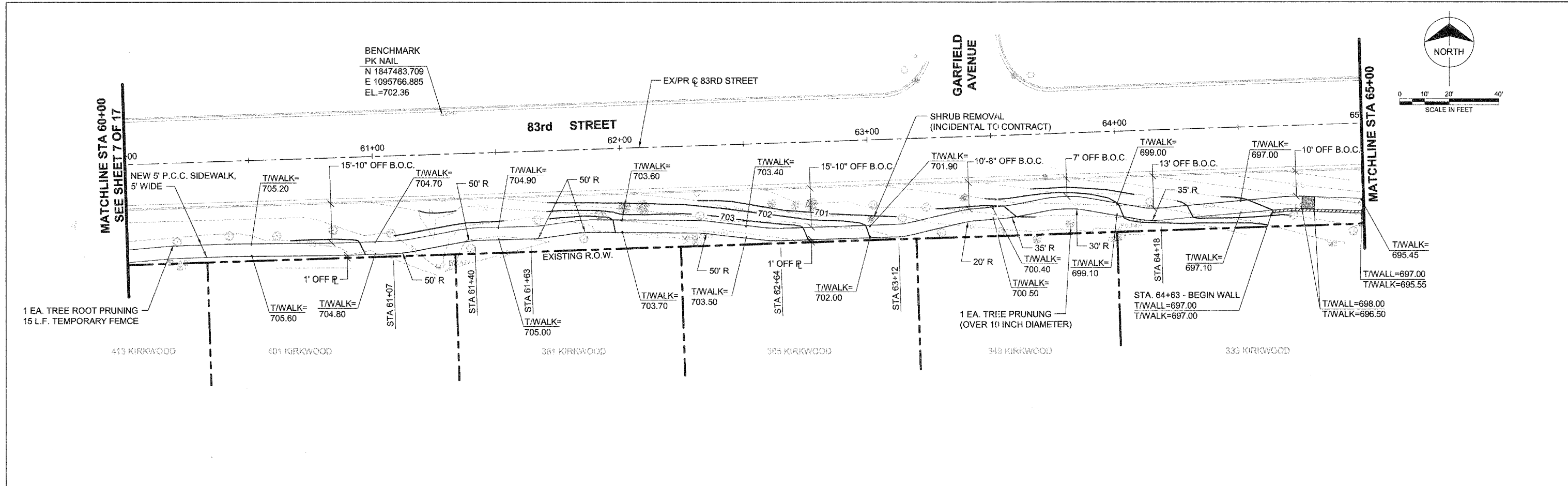
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		DATE --	REVISED --		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
SCALE: 1"=20'					SHEET NO. 7 OF 13 SHEETS		STA. 50+00.00 TO STA. 60+00.00		

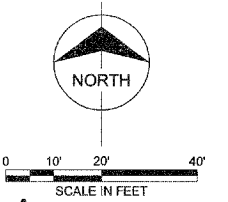
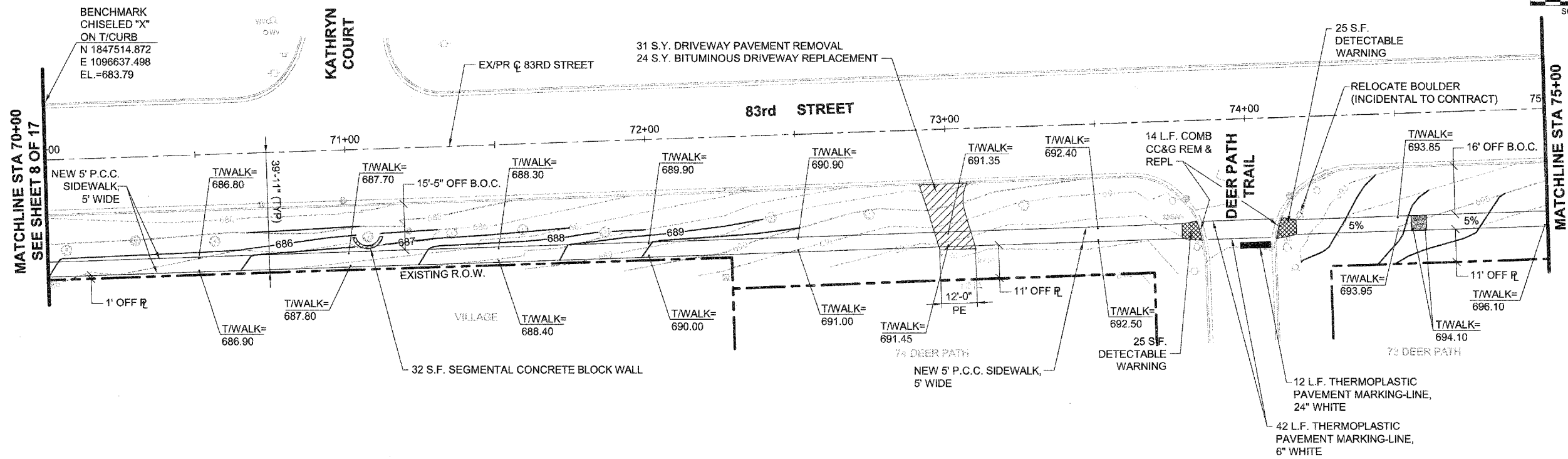
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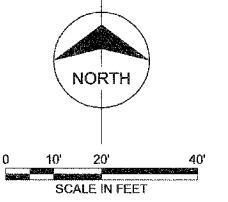
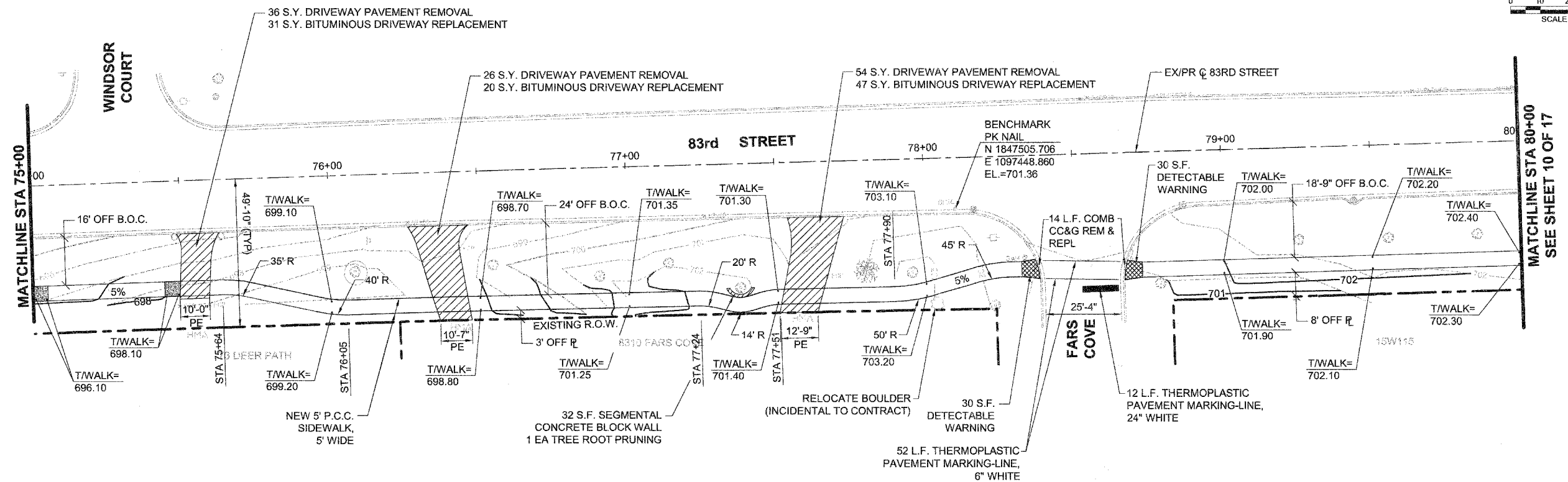


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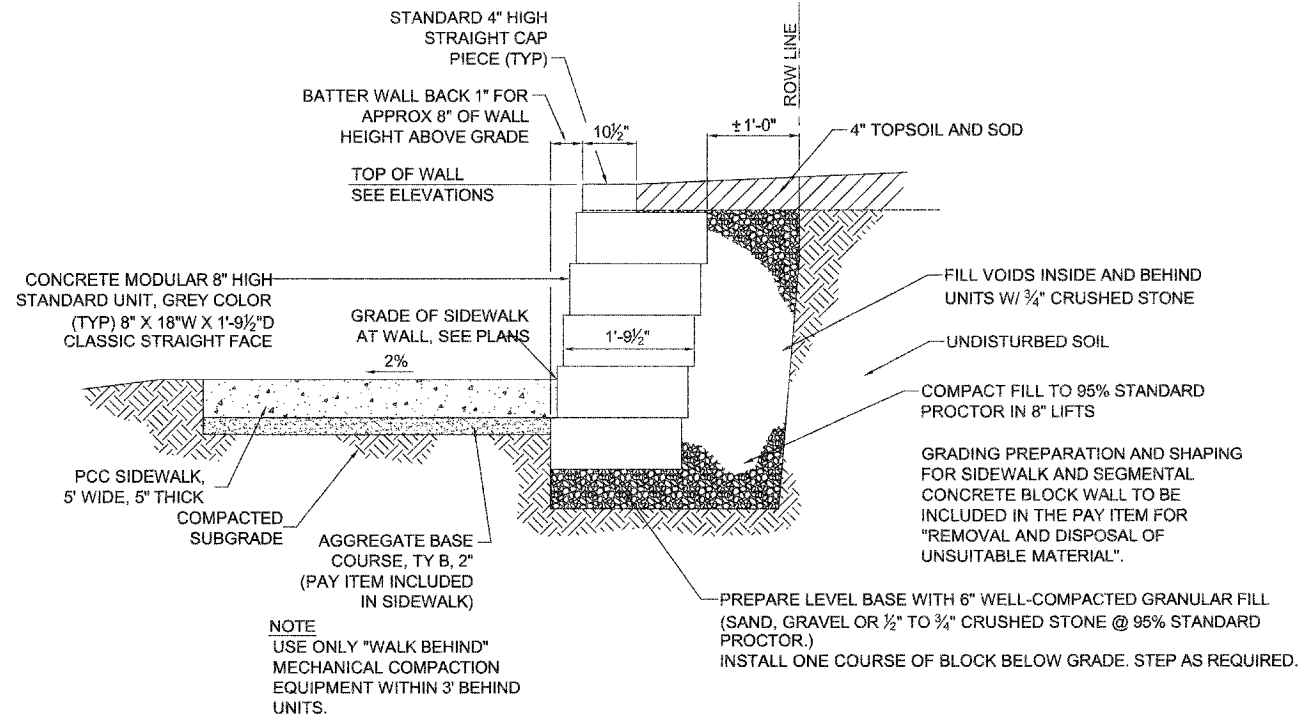
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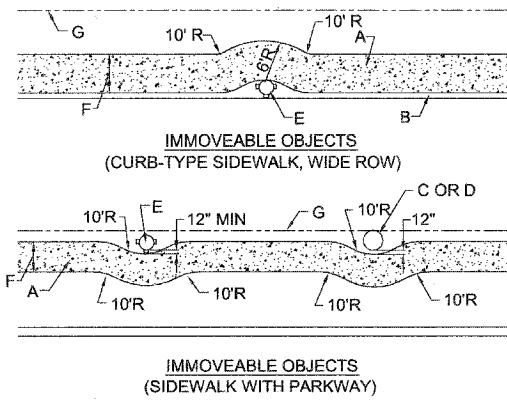
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SCALE: 1"=20'					SHEET NO. 9 OF 13 SHEETS. STA. 70+00.00 TO STA. 80+00.00				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT									

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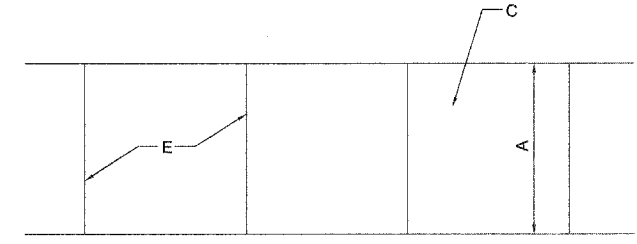
SEGMENTAL CONCRETE BLOCK WALL
TYPICAL SECTION
NOT TO SCALE



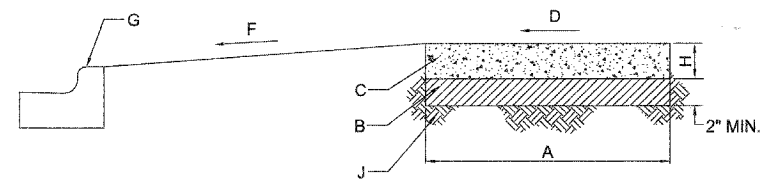
SIDEWALK OBSTRUCTIONS
NOT TO SCALE

- CONSTRUCTION NOTE:**
- A. SIDEWALK
 - B. CURB
 - C. SERVICE POLE, SEE NOTES 1 AND 2.
 - D. TREE TRUNK, SEE NOTE 1
 - E. FIRE HYDRANT, SEE NOTES 1 AND 2.
 - F. SIDEWALK WIDTH.
 - G. RIGHT-OF-WAY (ROW)

- GENERAL NOTES:**
1. A FIRE HYDRANT, SERVICE POLE OR TREE TRUNK SHALL BE CONSIDERED IMMOVEABLE ONLY WHEN DIRECTED BY THE VILLAGE ENGINEER.
 2. WHEN A SERVICE POLE OR FIRE HYDRANT IS LOCATED WITHIN THE SIDEWALK AREA, AND IS CONSIDERED IMMOVEABLE, A 1/2\"/>



PLAN



PROFILE SIDEWALK DETAIL

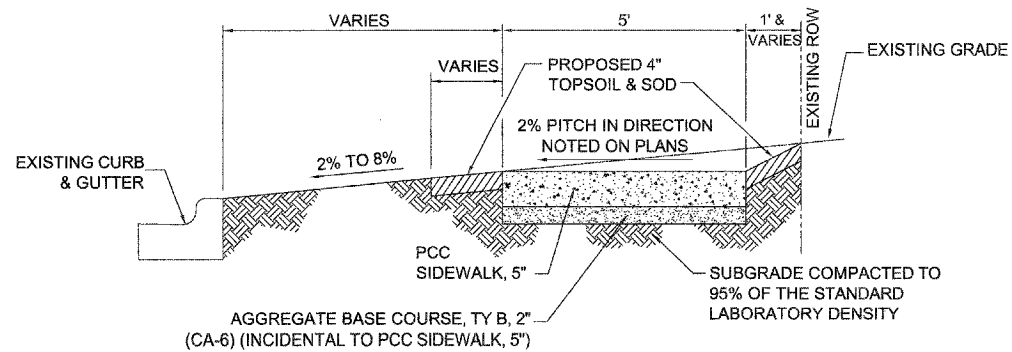
- CONSTRUCTION NOTES**
- A. 5' NORMAL WIDTH OR AS INDICATED ON PLANS
 - B. AGGREGATE BASE COURSE, TY. B, 2\"/>

- GENERAL NOTES**
1. STANDARD THICKNESS OF PCC SIDEWALK SHALL BE A MINIMUM OF 5\"/>

P.C.C. SIDEWALK 5\"/>

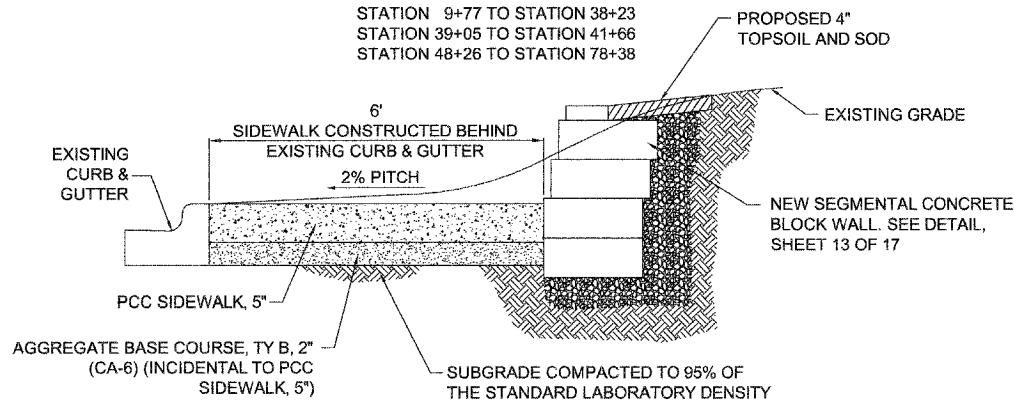
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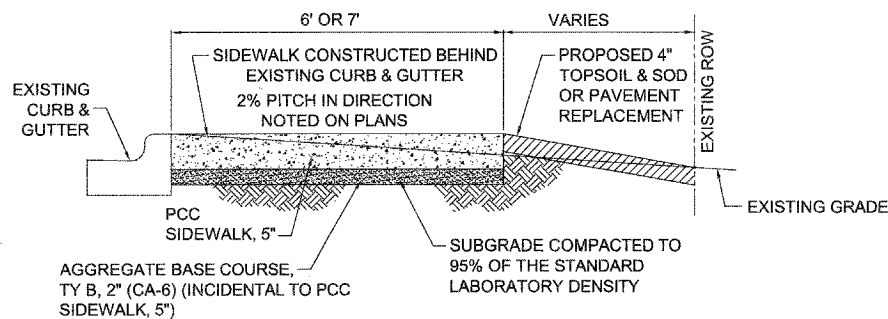
EXISTING & PROPOSED TYPICAL SECTION

NOT TO SCALE
SOUTH FRONTAGE ROAD
STATION 1+25 TO STATION 12+73
83rd STREET
STATION 9+77 TO STATION 38+23
STATION 39+05 TO STATION 41+66
STATION 48+26 TO STATION 78+38



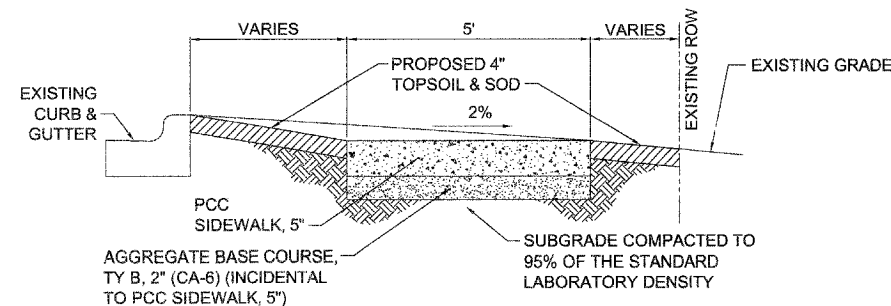
EXISTING & PROPOSED TYPICAL SECTION

NOT TO SCALE
83rd STREET
STATION 38+23 TO STATION 38+62



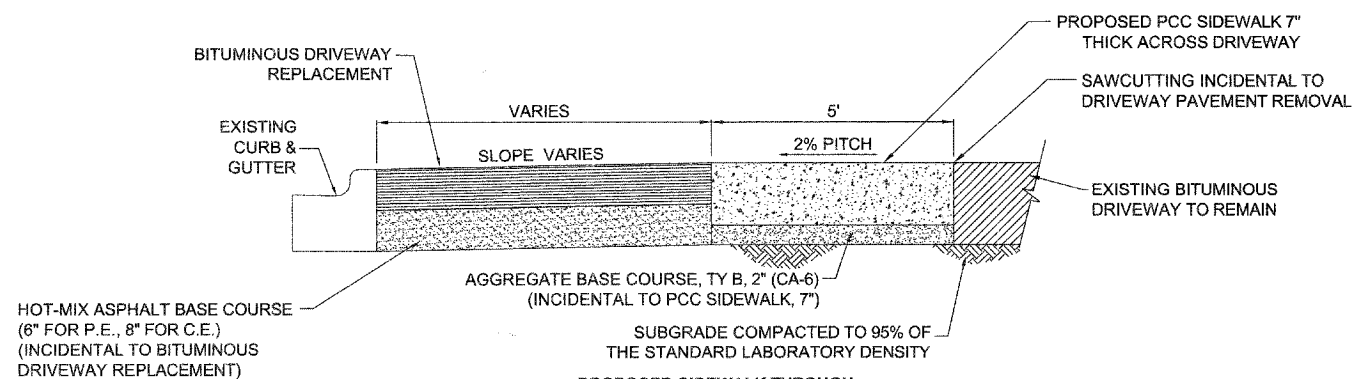
EXISTING & PROPOSED TYPICAL SECTION

NOT TO SCALE
SOUTH FRONTAGE ROAD
STATION 0+00 TO STATION 1+25
83rd STREET
STATION 38+62 TO STATION 39+05



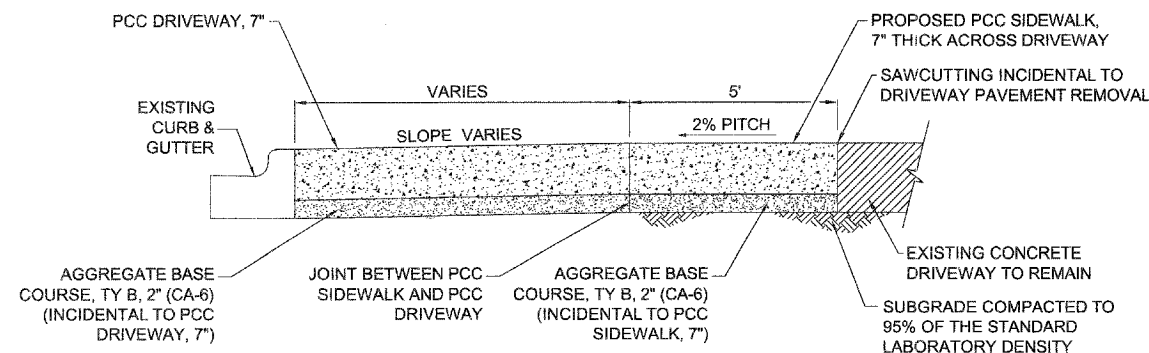
EXISTING & PROPOSED TYPICAL SECTION

NOT TO SCALE
83rd STREET
STATION 78+67 TO STATION 87+13



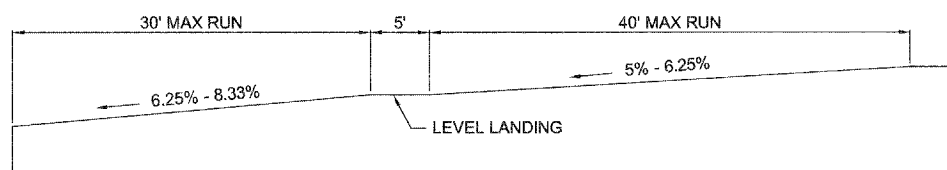
PROPOSED SIDEWALK THROUGH BITUMINOUS DRIVEWAY

NOT TO SCALE



PROPOSED SIDEWALK THROUGH PCC DRIVEWAY

NOT TO SCALE



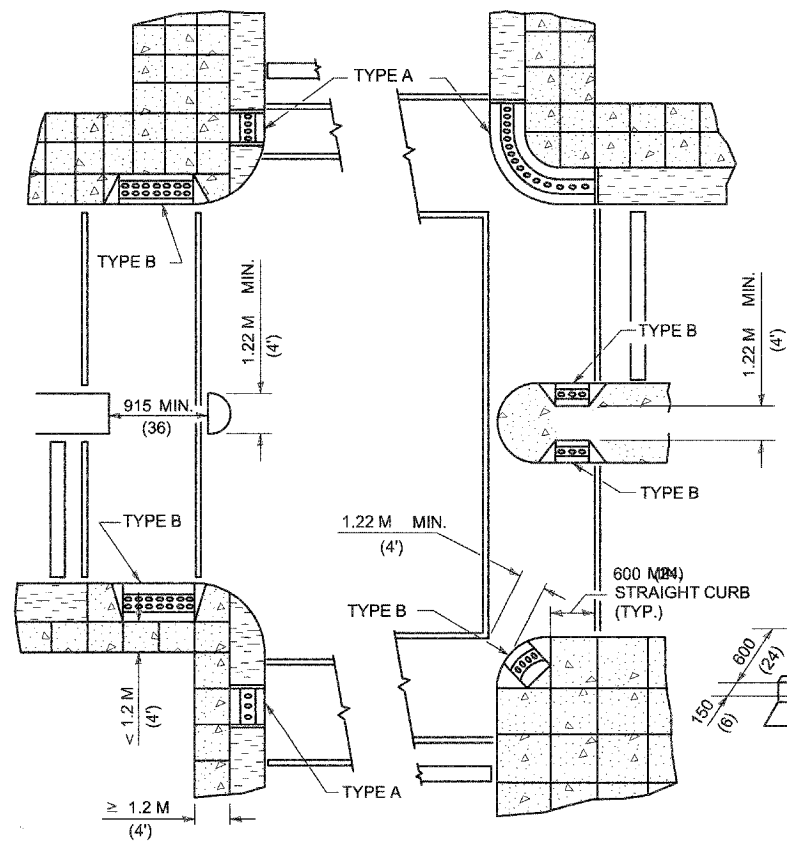
ADA SLOPE REQUIREMENTS

NOT TO SCALE

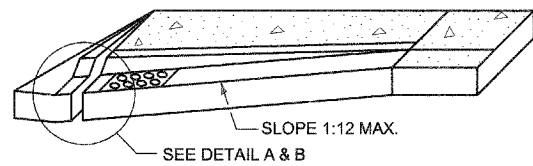
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		CHECKED -	REVISED -		83rd STREET CONSTRUCTION		CONTRACT NO. 83996		
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT				
				SCALE: 1"=20'	SHEET NO. 12 OF 13 SHEETS	STA.	TO STA.		

DATE	
BY	
PLAN	DESIGNED BY
	CHECKED BY
	NOTED BY
	DATE
	NO.

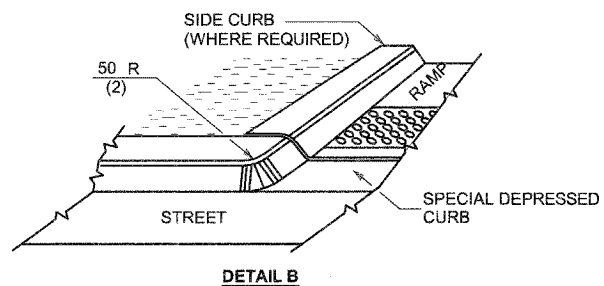
DATE	
BY	
PROFILE	DESIGNED BY
	CHECKED BY
	NOTED BY
	DATE
	NO.



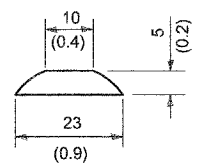
RECOMMENDED LOCATION OF RAMPS



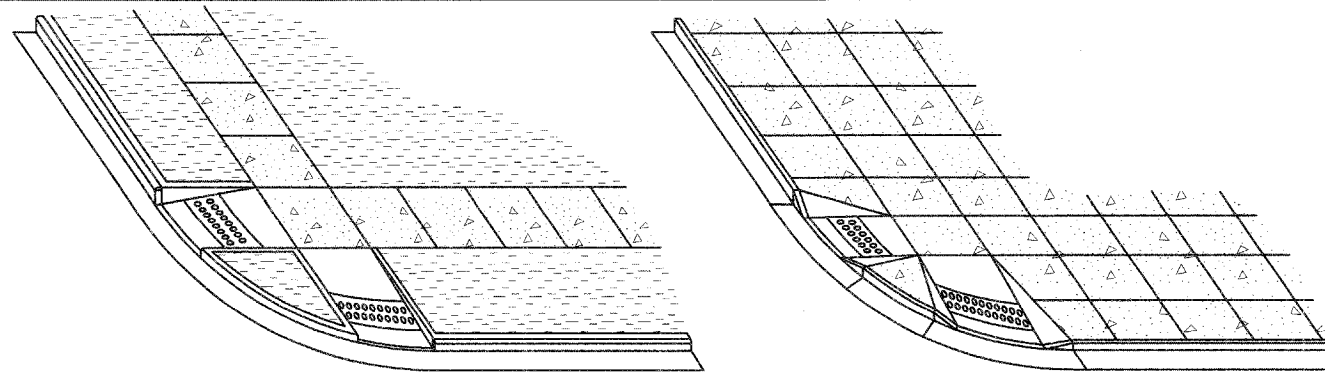
RAMP PROFILE



DETAIL B

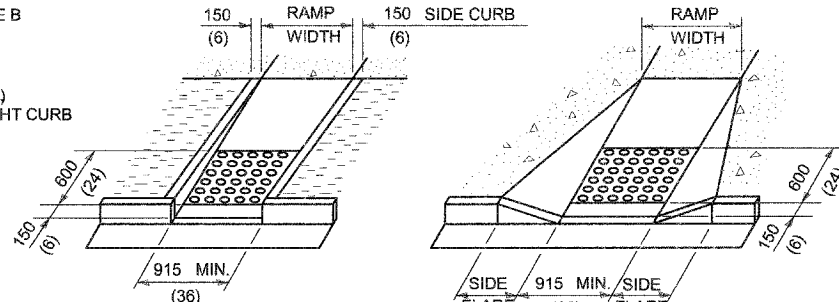


TRUNCATED DOME DETAIL



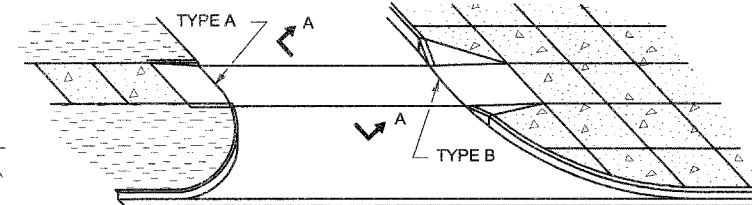
TYPE A RAMPS

TYPE B RAMPS



TYPE A

TYPE B



RAMPS AT ALLEY OR DRIVEWAY

VARIABLE 2% MAX. VARIABLE

SECTION A-A

LEGEND

- SIDEWALK
- RAMP
- DETECTABLE WARNINGS
- NON WALKING AREA

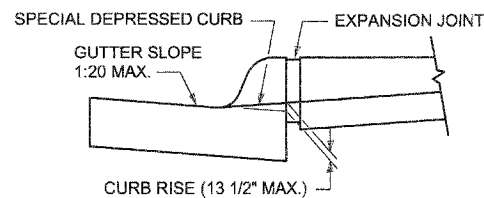
GENERAL NOTES

THE MAXIMUM SLOPE OF THE SIDE FLARE FOR TYPE B RAMPS SHALL BE 1:10; HOWEVER, IF THE WIDTH OF THE LANDING AREA BETWEEN THE TOP OF THE RAMP AND AN OBSTRUCTION IS LESS THAN 1.2 M (4'-0") THEN THE MAXIMUM SLOPE SHALL BE 1:12.

ALL SLOPE RATIOS ARE EXPRESSED AS UNITS OF VERTICAL DISPLACEMENT TO UNITS OF HORIZONTAL DISPLACEMENT (V:H).

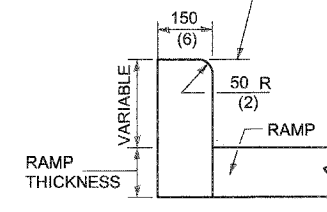
ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

DETAILS OF RAMPS



DETAIL A

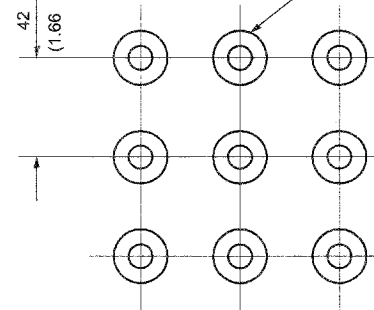
FLUSH WITH TOP OF ROADWAY CURB AND TOP OF SIDEWALK



DETAIL OF SIDE CURB

(SIDE CURB MAY BE CONSTRUCTED MONOLITHICALLY WITH RAMP)

42 MIN. - MAX. (1.66 MIN. - 1.68 MAX.)

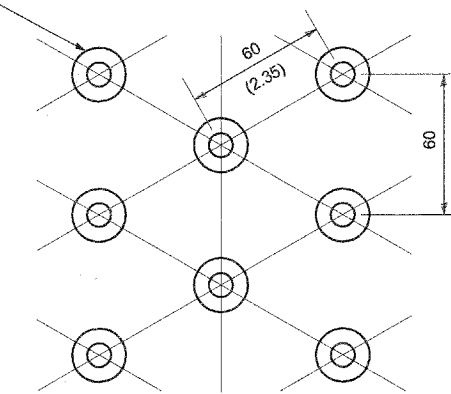


SQUARE PATTERN

(PARALLEL ALIGNMENT)

TRUNCATED DOME

DIRECTION OF TRAVEL

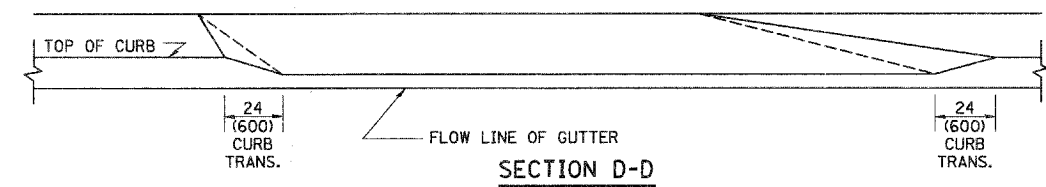
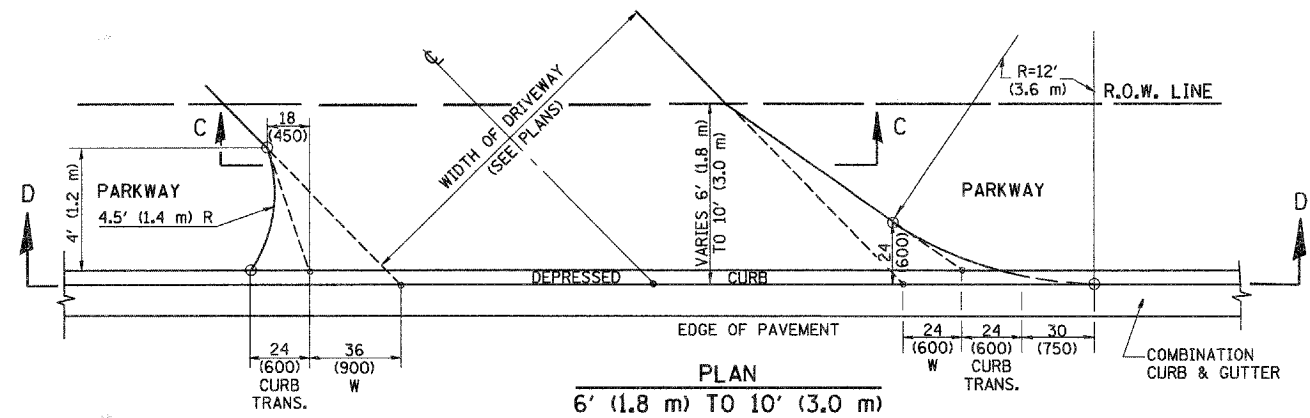
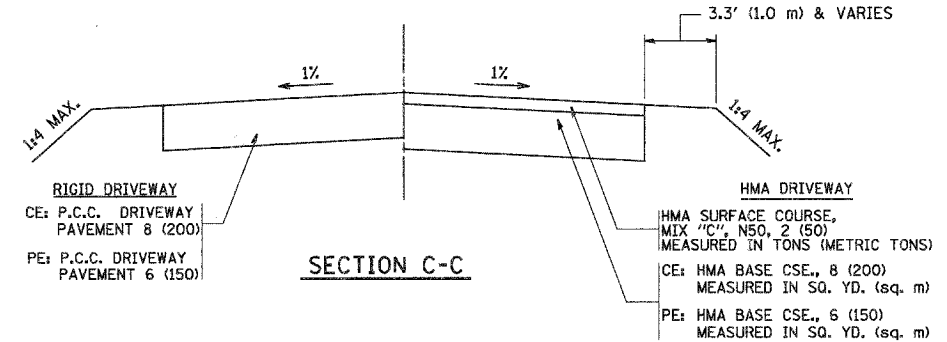
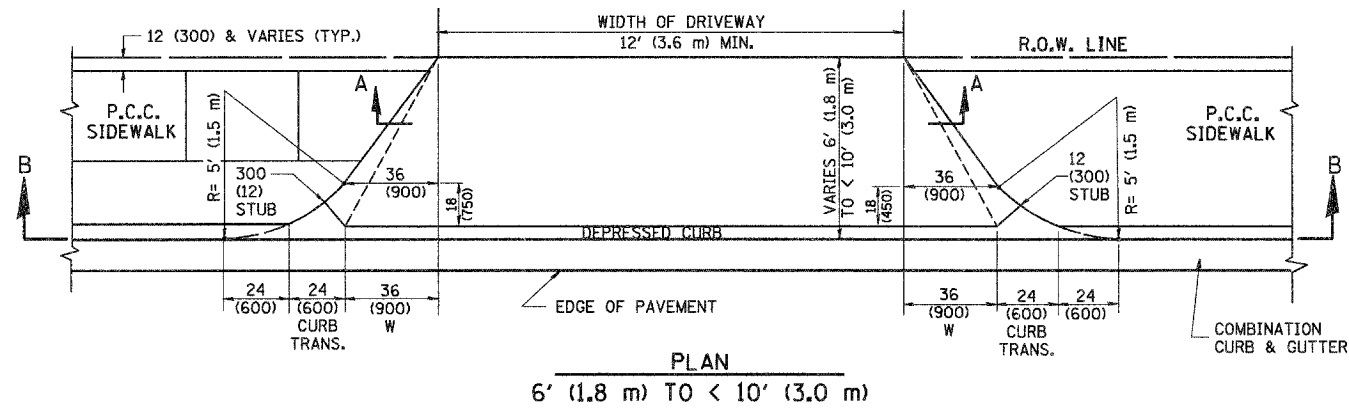
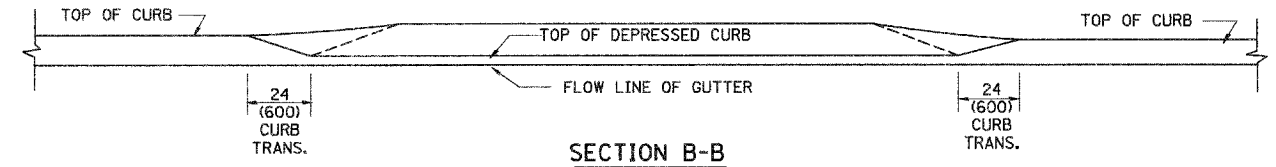
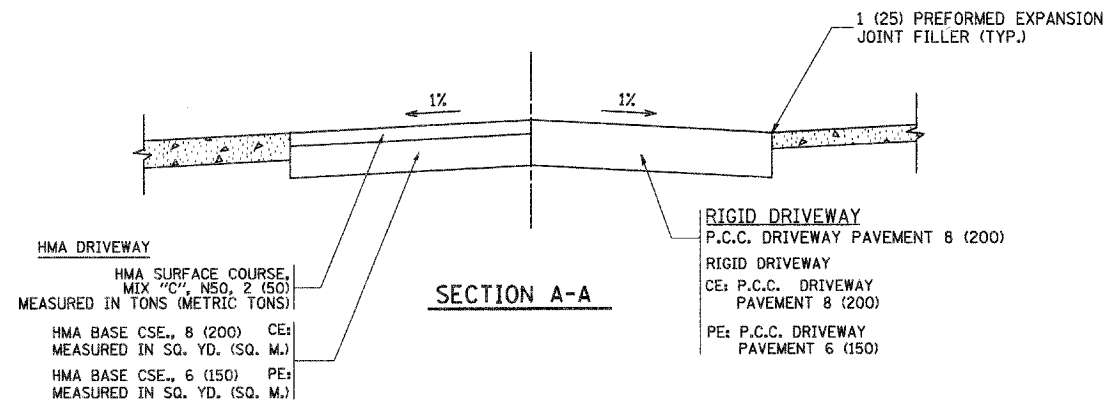
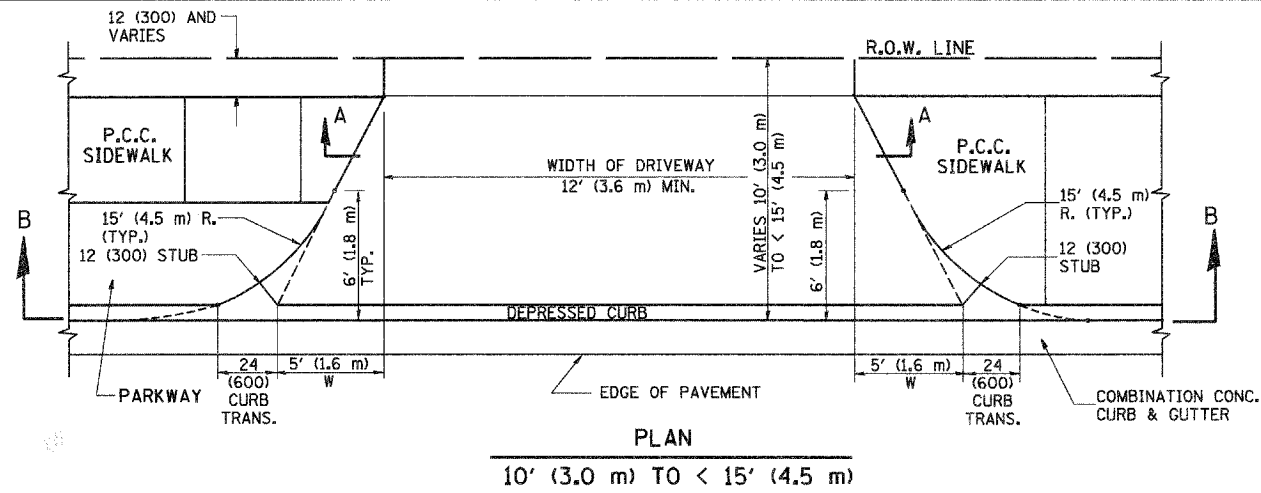


TRIANGULAR PATTERN

DETECTIBLE WARNINGS DETAIL

FILE NAME = \$FILE\$	USER NAME = \$USER\$	DESIGNED --	REVISED --	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCALE: 1"=20'	SHEET NO. 13 OF 13 SHEETS STA. TO STA.	F.A.U. RTE. 1552	SECTION 07-000.37-00-SW	COUNTY DUPAGE	TOTAL SHEETS 17	SHEET NO. 13
	PLOT SCALE = \$SCALE\$	DRAWN --	REVISED --				83rd STREET CONSTRUCTION	CONTRACT NO. 83996			
	PLOT DATE = \$DATE\$	CHECKED --	REVISED --				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
		DATE --	REVISED --								

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1512	05-00093-00-WR	DUPAGE	17	15
STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		
PROJECT NO. 1 R-8003 (699)				



GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

REVISIONS	
NAME	DATE
R. SHAH	11/06/95
J. POLLASTRINI	08/12/96
J. POLLASTRINI	12/14/96
A. ABBAS	03/21/97
T. HOLTZ	04/08/97
M. GOMEZ	04/06/01
P. LAFLEUR	04/15/03
R. BORO	01/01/07

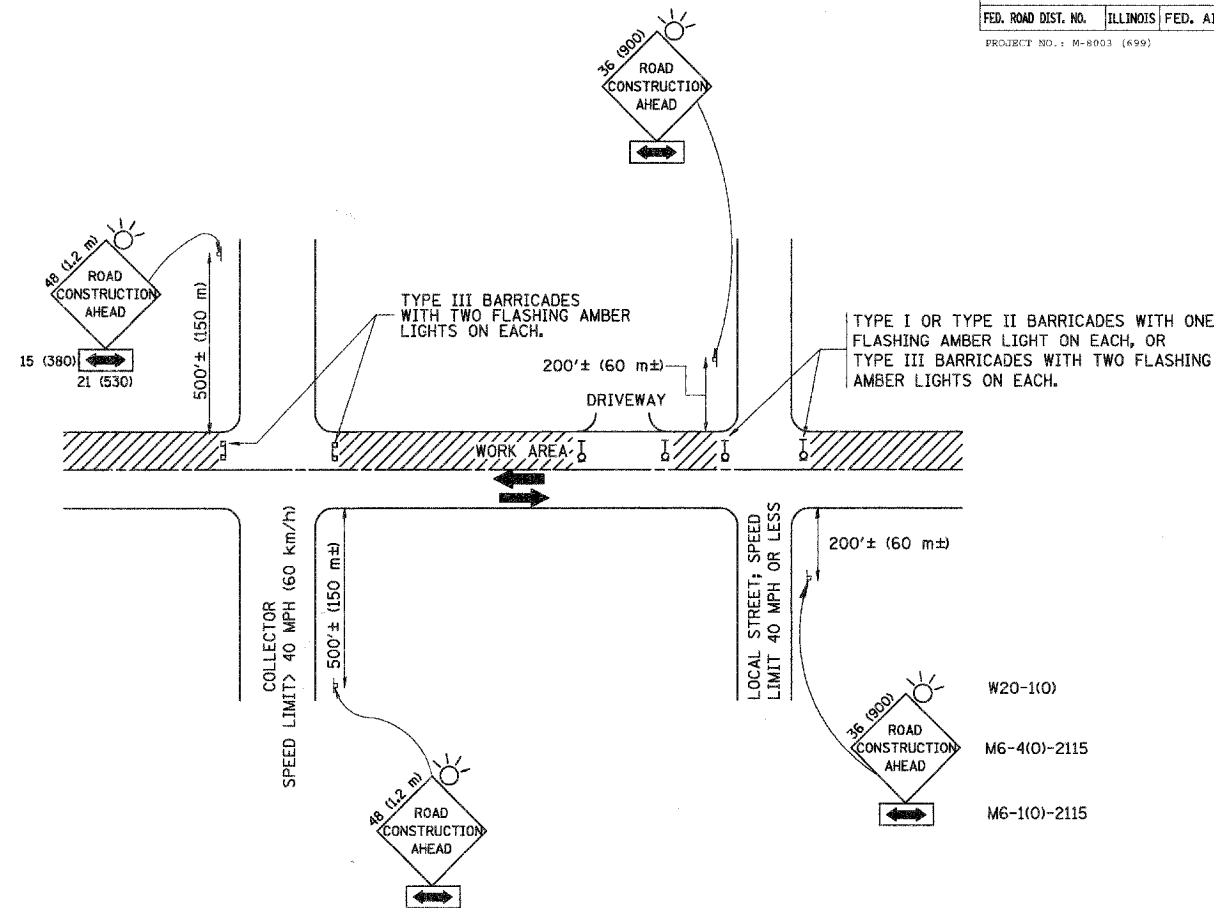
ILLINOIS DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)

SCALE: VERT. NONE
HORIZ.

DRAWN BY
CHECKED BY

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1512	05-00093-00-WR	DOPAGE	17	16
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
PROJECT NO.: M-8003 (699)				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

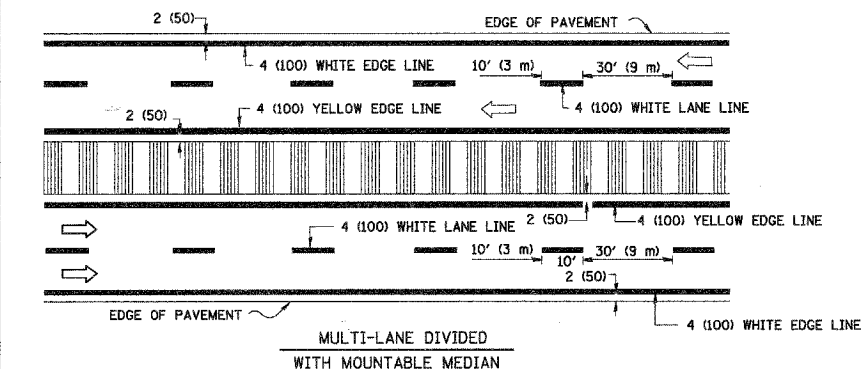
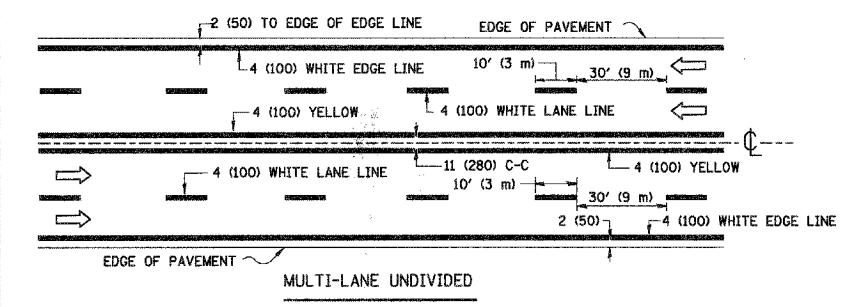
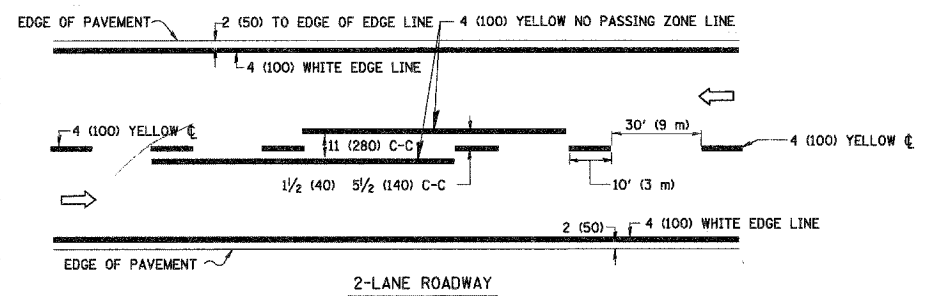
REVISIONS	
NAME	DATE
LHA	6/789
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TRAFFIC CONTROL AND PROTECTION
 FOR
 SIDE ROADS, INTERSECTIONS, AND
 DRIVEWAYS

SCALE: NONE

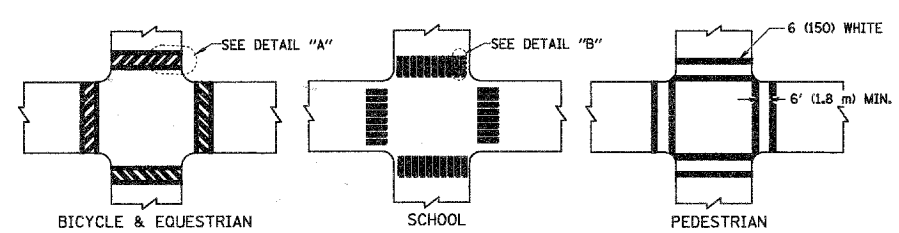
DRAWN BY
 CHECKED BY
 TC-10

F.A. DIST. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1512	05-00093-00-WR	DUPAGE	17	17
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS		FED. AID PROJECT
PROJECT NO. 1 M-8003 (699)				

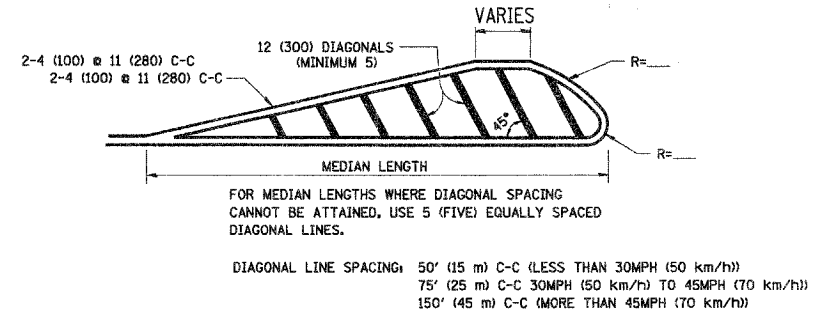
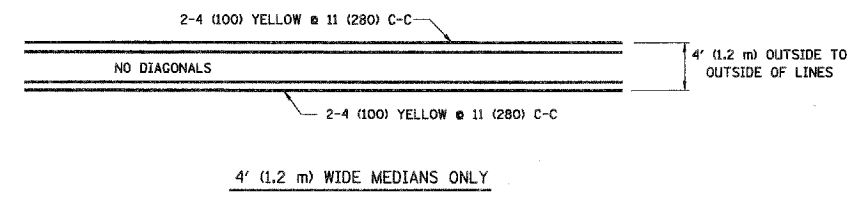


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

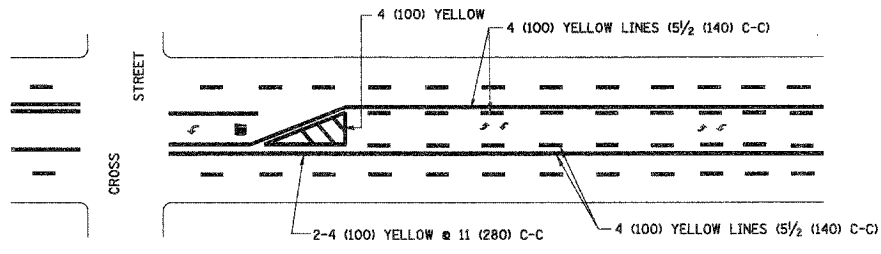


TYPICAL CROSSWALK MARKING

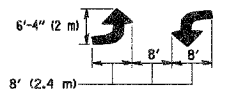


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

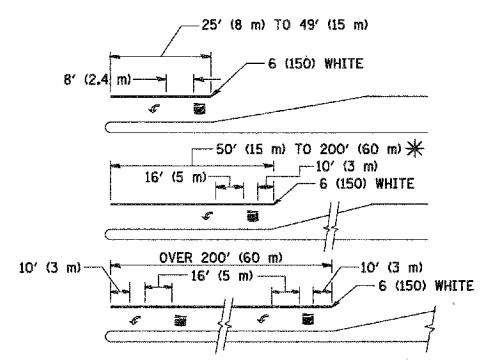


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

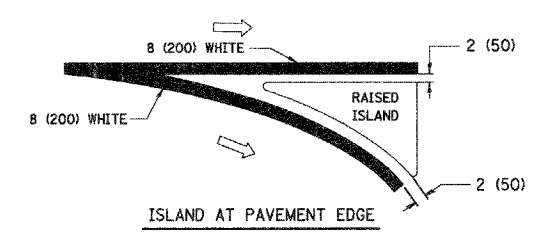
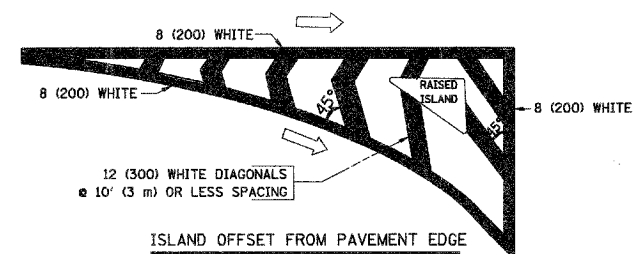
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
ALEX HOUSEH	10-09-96
ALEX HOUSEH	10-17-96
T. RAMMACHER	01-06-00

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE
DRAWN BY CADD
CHECKED BY
TC-13

PLT DATE = 3/6/2007
FILE NAME = K:\dists\td\1\03.dgn
PLT SCALE = 80.0000 / 1 in.
USER NAME = bauerd.