

BENCHMARK

Brass washer in N.E. wingwall of S.N. 100-0028
Elev. 559.402

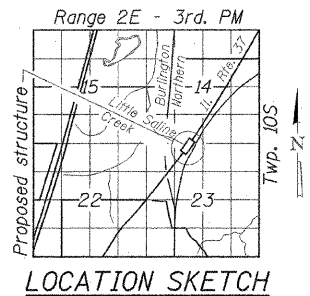
EXISTING STRUCTURE

S.N. 100-0028. Built in 1931 as S.B.I. Rte. 147, Section 113B at Station 531+81 as a 1-span reinforced concrete beam 58'-8 1/2" Bk. to Bk. abutments, supported on spread footings. Bridge widening in 1953 to 30 ft. bridge clear width.

PROPOSED STRUCTURE

Existing bridge to be removed and replaced with a simple span steel plate girder and concrete deck bridge on integral abutments. Traffic to be maintained utilizing stage construction.

No salvage.



INDEX OF SHEETS

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| 2. | Notes and Bill of Material |
| 3. | Stage Construction Details |
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| 5. | Temporary Bridge Rail Details |
| 6. | Deck Elevations 1 |
| 7. | Deck Elevations 2 |
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| 11. | Abutment Diaphragm Details |
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| 13. | Structural Steel Details |
| 14. | North Abutment |
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| 16. | Bar Splicer Assembly Details |
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| 18. | Boring Logs |

STATION 531+81.00
BUILT 200_ BY
STATE OF ILLINOIS
F.A.P. RT 726 SEC. 113B-2
LOADING HL-93
STR. NO. 100-0091

NAME PLATE

See Std. 515001

| Design Scour Elevation (feet) | N. Abutment | S. Abutment |
|-------------------------------|-------------|-------------|
| | 549.39 | 549.49 |

LOADING HL-93

Allow 50 psf for future wearing surface

DESIGN SPECIFICATIONS

2004 AASHTO LRFD Bridge Design Specifications with 2005 Interims

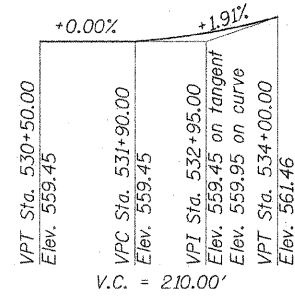
DESIGN STRESSES

FIELD UNITS

- $f'_c = 3,500$ psi
- $f_y = 60,000$ psi (reinforcement)
- $f_y = 50,000$ psi (structural steel)
- M270 Grade 50W

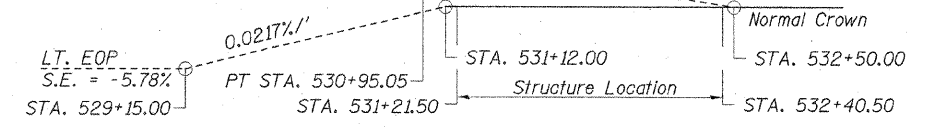
SEISMIC DATA

- Seismic Performance Zone (SPZ) = 2
- Bedrock Acceleration Coefficient (A) = 0.13%g
- Site Coefficient (S) = 1.5



PROFILE GRADE

RT. EOP S.E. = 1.5%
@ Profile - IL. 37



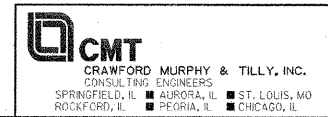
SUPERELEVATION TRANSITION SKETCH

NOTES:

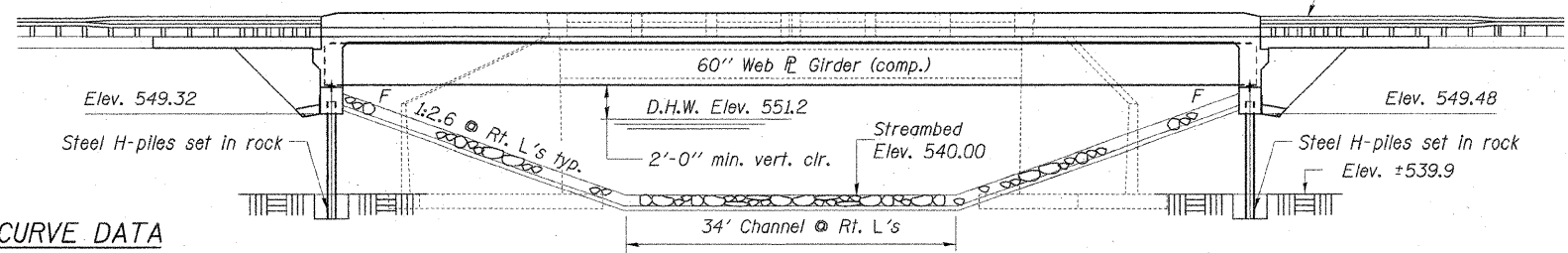
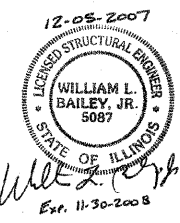
- 1. See Sheet 2 of 18 for riprap details and Section A-A.

| REVISIONS | NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
GENERAL PLAN
F.A.P. ROUTE 726 (IL. RTE. 37)
ILLINOIS ROUTE 37 OVER
LITTLE SALINE CREEK
SECTION 113B-2 STA. 531+81.00
STR. NO. 100-0091 - WILLIAMSON COUNTY
SCALE: NONE DRAWN BY: GLD
DATE: 12/14/07 CHECKED BY: WLW



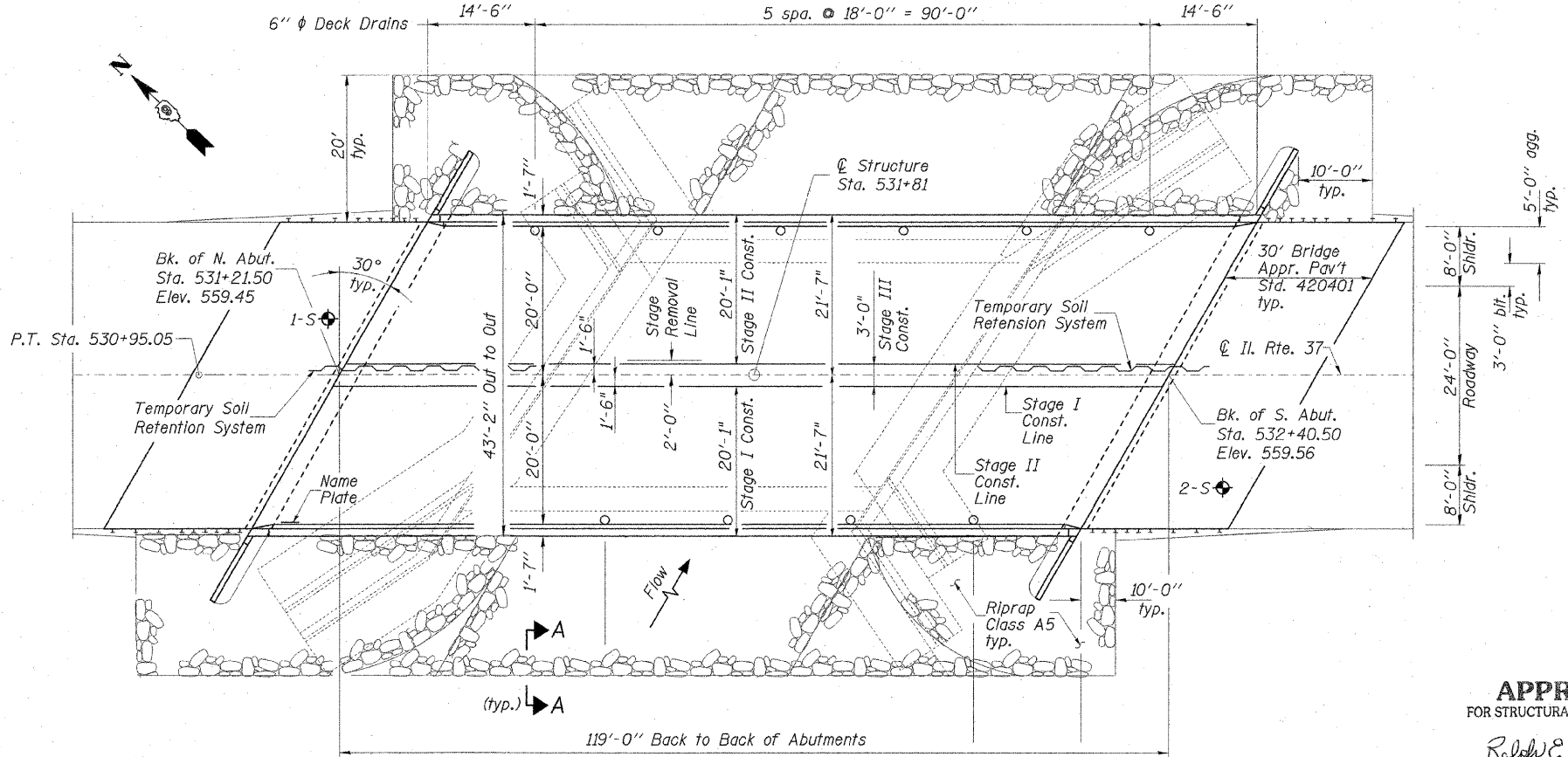
APPROVED
FOR STRUCTURAL ADEQUACY ONLY
Relph E. Anderson
ENGINEER OF BRIDGES AND STRUCTURES



ELEVATION

CURVE DATA

- PI Sta. = 525+17.88
- $\Delta = 35^\circ-31'-46''$ (LT)
- $D = 2^\circ-58'-31''$
- $R = 1,925.69'$
- $T = 616.97'$
- $L = 1,194.14'$
- $E = 96.42'$
- $e = \text{variable (see sketch)}$
- P.C. Sta. = 519+00.91
- P.T. Sta. = 530+95.05



PLAN

BORING DATA

| Boring No. | Station | Offset |
|------------|---------|---------|
| 1-S | 531+20 | 8' Lt. |
| 2-S | 532+47 | 14' Rt. |

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