

DUPAGE AIRPORT AUTHORITY WEST CHICAGO, ILLINOIS

CONSTRUCTION PLANS FOR DUPAGE AIRPORT

INSTALL RUNWAY 28 PAPI AND RELOCATE REIL LIGHTS

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A.I.P. PROJECT: 3-17-0017-B22
ILLINOIS PROJECT: DPA-3769

MARCH 7, 2008

ITEM NO.	DESCRIPTION	UNIT	QUANTITY	RECORD QUANTITY
AR108404	1/C # 4 600V UG CABLE	LF	3,723	
AR108756	1/C # 6 GROUND	LF	2,034	
AR110314	4" STEEL DUCT, JACKED	LF	250	
AR125615	PAPI (L-880 SYSTEM)	EACH	1	
AR125909	REMOVE VASI	EACH	1	
AR125967	RELOCATE REILS	PAIR	1	
AR156510	SILT FENCE	LF	200	
AR156520	INLET PROTECTION	EACH	3	
AR800053	SOIL GUARD	SY	2,885	
AR800093	1/C #1 600V UG CABLE	LF	2,379	
AR800118	REMOVE GRAVEL	CY	200	
AR800177	1/C #1/0 COUNTERPOISE	LF	2,034	
AR901510	SEEDING	ACRE	0.6	

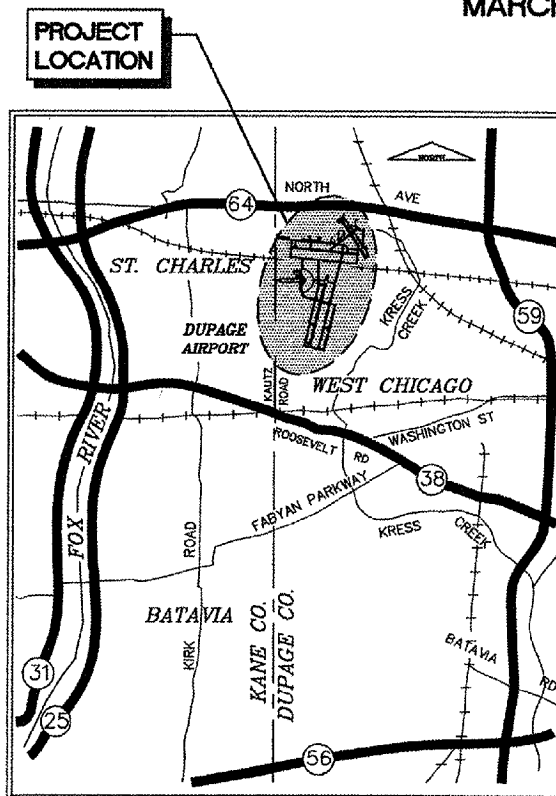
DUPAGE AIRPORT

TOWNSHIP: 40 NORTH WAYNE TOWNSHIP
RANGE: 9 EAST (SECTIONS: 31)
DUPAGE COUNTY

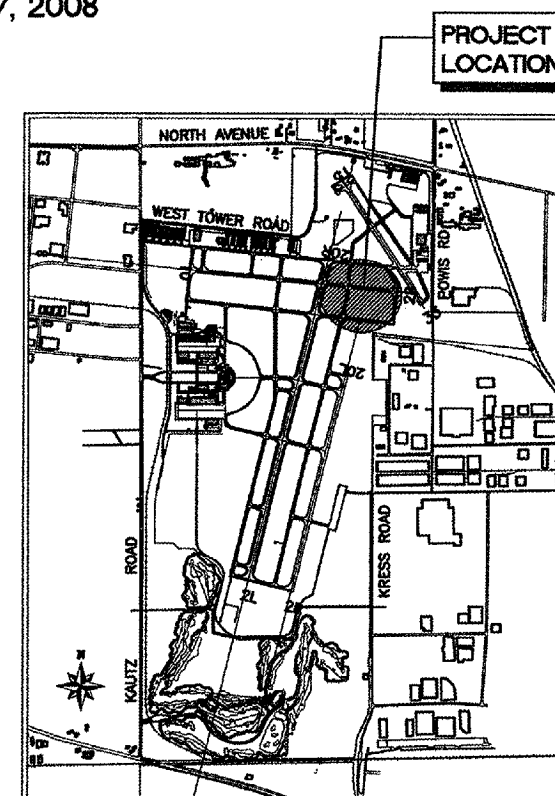
**INSTALL RUNWAY 28 PAPI AND
RELOCATE REIL LIGHTS**

DESIGN AIRCRAFT APPROACH CATEGORY B
DESIGN AIRCRAFT GROUP II

**CALL JULIE
BEFORE EXCAVATING
1-800-892-0123**



LOCATION MAP



SITE PLAN

CMT
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
600 N. COMMONS DRIVE
SUITE 107
AURORA, IL 60504
PHONE (830) 820-1022
FAX (830) 820-0350

07257-04
DANIEL L. PAPE, P.E.
052-047269
STATE OF ILLINOIS
EXPIRES 11/30/09

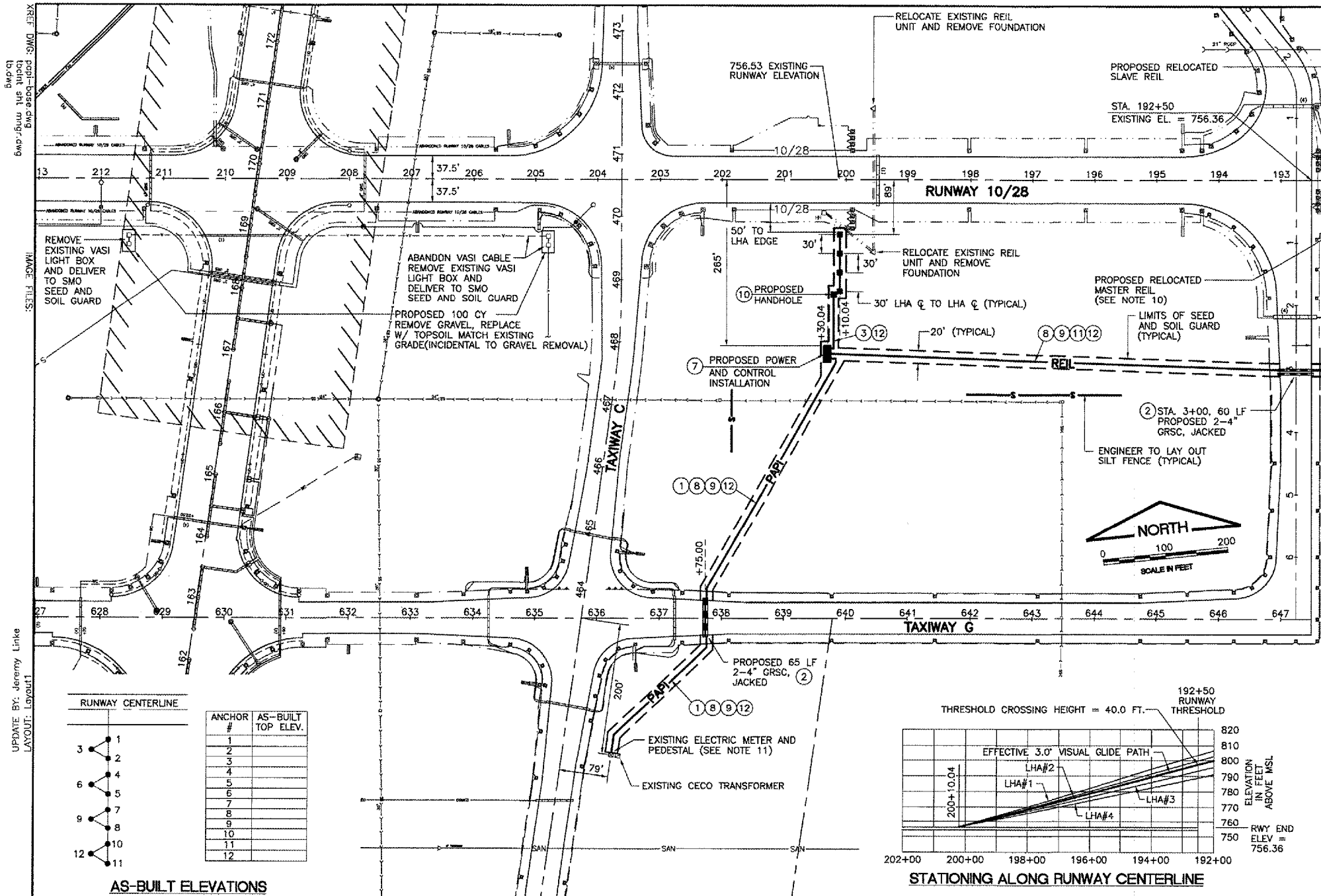
SUBMITTED BY *[Signature]*
DATE *1/8/08*

DDA
DuPage Airport

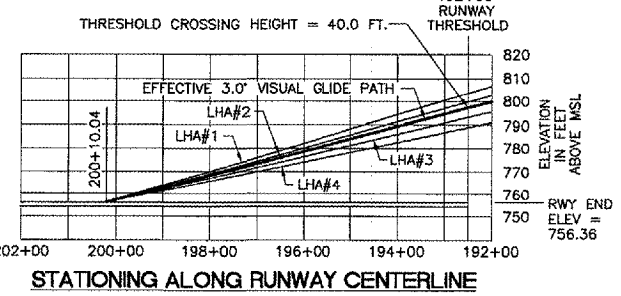
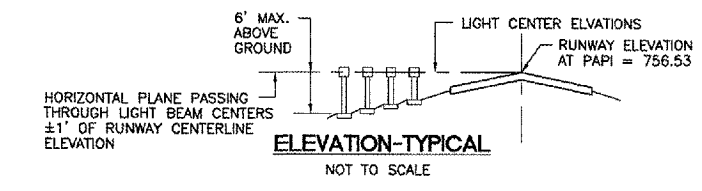
2700 INTERNATIONAL DRIVE
SUITE 200
WEST CHICAGO, IL 60185

APPROVED BY *[Signature]*
DAVID BIRD - EXECUTIVE DIRECTOR

DATE *1/9/08*



- ### NUMBERED LEGEND
- 3-1/2 #1 TYPE U.S.E. POWER CABLES AND 1/2 #6 BARE COPPER GROUND WIRE. SEE DWG. GL-D-2795-7-4.
 - JACKING OPERATION: JACK 4" GRS A MINIMUM OF 4 FEET UNDER TAXIWAYS AND SHOULDERS. EXTEND A MINIMUM OF 5 FEET BEYOND CONCRETE EDGE. RESTORE PIT AREAS TO PREVIOUS CONDITIONS. MARK CASING PIPE ENDS WITH ABOVE GROUND MARKERS.
 - 8-1/2 #8, 4-#6 GROUND, TYPE U.S.E. POWER CABLES, FOUR 6 PR #19 SHIELDED CONTROL CABLES AND 1/2 #6 BARE COPPER BURIED GUARD WIRE, PER DWG. GL-D-2795-7-4.
 - PAPI LAMP HOUSING ASSEMBLY (LHA) PER DWG. GL-D-2795-7-2.
 - CRUSHED ROCK AREA, SEE NOTE 7.
 - 3/4" x 10' COPPER CLAD GROUND ROD W/ EXOTHERMIC WELD CONNECTION SEE DETAIL "1" DWG. GL-D-2795-7-2.
 - POWER AND CONTROL INSTALL. SEE DETAIL "1", DWG. GL-D-2795-7-3.
 - CONTRACTOR TO INSTALL GROUND RODS ALONG CABLE RUN AT 90' MAXIMUM INTERVALS AND CONNECT TO #1/0 BARE COPPER GROUND WIRE PER SECTION 16A.4E OF SPECIFICATION FAA-GL-918C.
 - CONTRACTOR TO INSTALL CABLE MARKERS FOR BURIED CABLE PER SECT. 16F-3G OF SPECIFICATIONS FAA-GL-918C, AND SHEET 8 OF 10.
 - ELECTRIC HANDHOLE, SEE DETAIL 6, DWG. D-2795-7-12.
 - 3-1/2 #4, TYPE U.S.E. POWER CABLES AND 1/2 #6 BARE COPPER GROUND WIRE TO REIL MASTER UNIT.
 - TRENCH SECTIONS, SEE DWG. GL-D-2795-7-6. THE NORMAL TRENCH DEPTH FOR THIS PROJECT IS 30".



AS-BUILT ELEVATIONS
NOT TO SCALE

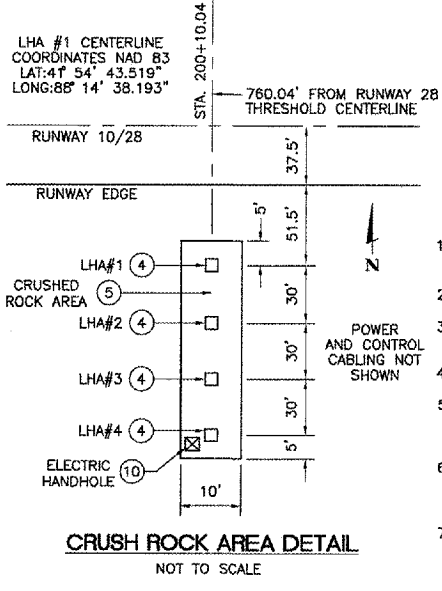
ANCHOR #	AS-BUILT TOP ELEV.
1	
2	
3	
4	
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9	
10	
11	
12	

	PAPI LHA #1	PAPI LHA #2	PAPI LHA #3	PAPI LHA #4
DISTANCE FROM RUNWAY CENTERLINE (NOTE 1)	89'	119'	149'	179'
AIMING ANGLE	3'30"	3'10"	2'50"	2'30"
APPROXIMATE GROUND ELEVATION (NOTE 2)	753.9	753.2	752.4	751.4
PAPI LHA (NOTE 3) APERTURE CENTERLINE ELEVATION	756.5	756.5	756.5	756.5

- ### GENERAL NOTES
- #### FOUNDATIONS:
- FOUNDATIONS FOR MOUNTING LIGHT BOXES SHALL BE MADE OF ITEM 610 CONCRETE. ALL LIGHT BOXES SHALL BE FRANGIBLY MOUNTED TO THE FOUNDATION.
- #### AZIMUTHAL AIMING:
- EACH LIGHT UNIT SHALL BE AIMED OUTWARD INTO THE APPROACH ZONE ON A LINE PARALLEL TO THE RUNWAY CENTERLINE WITHIN A TOLERANCE OF ±1/2 DEGREE.
- #### MOUNTING HEIGHT TOLERANCES:
- THE BEAM CENTERS OF ALL LIGHT UNITS SHALL BE WITHIN ±1 INCH OF A HORIZONTAL PLANE AT THE ELEVATION GIVEN IN THE TABLE.
- #### TOLERANCE ALONG LINE PERPENDICULAR TO RUNWAY:
- THE FRONT FACE OF EACH LIGHT UNIT IN A BAR SHALL BE LOCATED ON A LINE PERPENDICULAR TO THE RUNWAY CENTERLINE WITHIN ±6 INCHES.
 - PROVIDE FRANGIBLE MOUNTS FOR ALL LEGS OF LIGHT UNITS AND POWER ADAPTERS.
 - NUMBER AND CONFIGURATION OF LEGS PER MANUFACTURER. (3 LEGS MINIMUM)
 - GROUND EACH LAMP HOUSING AND POWER ADAPTER PER MANUFACTURER
 - THE POWER AND CONTROL UNIT SHALL BE STYLE A, CLASS II.
 - THE DIFFERENCE IN LATERAL SPACING BETWEEN THE LIGHT UNITS SHALL NOT EXCEED ONE FOOT.
- EACH LIGHT UNIT SHALL HAVE TWO LAMPS AND SHALL PROVIDE A BEAM OF LIGHT SPLIT HORIZONTALLY TO PRODUCE WHITE LIGHT IN THE TOP SECTOR AND RED LIGHT ON THE BOTTOM SECTOR. THEY SHALL HAVE A MINIMUM OF THREE MOUNTING LEGS WHICH SHALL BE ADJUSTABLE TO PERMIT LEVELING WHERE ONE SIDE OF THE UNIT IS INSTALLED UP TO 1 INCH LOWER THAN THE OPPOSITE SIDE.
 - APPLY "NEVER SEEZ" OR APPROVED EQUAL TO ALL THREADED BOLTS AND CONNECTIONS.

- ### LIGHTING NOTES
- THE REIL LIGHT BEAM CENTERLINE OF EACH LIGHT UNIT SHALL BE AIMED 15 DEGREES OUTWARD FROM A LINE PARALLEL TO RUNWAY CENTERLINE AND INCLINED AT AN ANGLE 10 DEGREES ABOVE THE HORIZONTAL. IF THIS ANGLE IS OPERATIONALLY OBJECTIONABLE, CONTRACTOR SHALL PROVIDE AN OPTICAL Baffle AND ORIENT THE BEAM AXIS OF UNIT 10 DEGREES OUTWARD FROM A LINE PARALLEL TO THE RUNWAY CENTERLINE AND INCLINED AT AN ANGLE OF 3 DEGREES ABOVE THE HORIZONTAL AT NO ADDITIONAL COST.
 - THE ROUTING OF PROPOSED AND EXISTING CABLE SHOWN IS FOR INFORMATION ONLY. THE EXACT ROUTING SHALL BE FIELD VERIFIED.
 - THE CONTRACTOR SHALL VERIFY THAT THE EXISTING RUNWAY LIGHTING CIRCUIT IS OPERATIONAL AT THE END OF EACH WORKING DAY.
 - ANY EXISTING CABLE MARKERS THAT ARE DISTURBED SHOULD BE REMOVED AND REINSTALLED AT THE SAME LOCATION. COST SHALL BE INCIDENTAL TO GRADING.
 - 1/2 #1/0 BARE COPPER COUNTERPOISE COMPLETE WITH 3/4" x 10' GROUND RODS SHALL BE INSTALLED EVERY 90' AT 1' ABOVE ALL NON-ARMORED CABLE. CONTRACTOR SHALL REQUEST FAA PERSONNEL TO INSPECT GROUNDING BEFORE BACKFILLING.
 - THE EXISTING RUNWAY LIGHTING CIRCUIT, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS DIRECTED BY THE ENGINEER. ALL TEMPORARY CABLING AND SPLICING REQUIRED SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.
 - AT ANY LOCATION WHERE THE PROPOSED DUCT OR CABLE ROUTE CROSSES AN EXISTING UTILITY, THE CONTRACTOR SHALL HAND DIG AND LOCATE THE EXISTING UTILITY PRIOR TO TRENCHING. COST OF LOCATING ALL EXISTING UTILITIES SHALL BE INCIDENTAL TO THE CONTRACT.
 - CONTRACTOR SHALL PROVIDE PULL WIRE FOR ALL DUCT BANKS AND CAP THE UNUSED DUCT BANKS FOR FUTURE USE.
 - ALL SPLICES OF PROPOSED AND EXISTING CABLES IN ELECTRICAL HANDHOLES AND LIGHT BASES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
 - CONTRACTOR TO FURNISH AND INSTALL CONTROL WIRES BETWEEN REIL MASTER AND SLAVE UNITS, ISOLATION TRANSFORMER AND CURRENT SENSING UNIT TO MAKE REIL SYSTEM COMPLETE AND OPERATIONAL.
 - CONTRACTOR TO REMOVE EXISTING ELECTRIC SERVICE CONDUIT AND INSTALL NEW 2" CONDUIT WITH "LB" FOR NEW ELECTRIC SERVICE. COORDINATE NEW SERVICE CONNECTION WITH COMED. NEW ELECTRIC SERVICE SHALL BE 80A, 120/240V, SINGLE PHASE, 3-WIRE.
 - THE EXISTING RUNWAY 10/28 EDGE LIGHTING AND PAVEMENT MARKING SHALL BE CHANGED BY OTHERS.
 - INSTALL CABLE MARKERS AT 200' SPACING ON RUNS OVER 200' IN LENGTH AND AT LOCATION OF CHANGE OF DIRECTION ON CABLE RUN AND AT SPLICES AS REQUIRED BY SPECIFICATION FAA-C-1391.

- ### NOTES
- DIMENSION LINES ARE REFERENCED TO THE CENTERLINE OF EACH PAPI LHA (MIDWAY BETWEEN THE FRONT TWO LEGS).
 - THIS ELEVATION IS NOT TO BE USED TO ESTABLISH LAMP HOUSING ASSEMBLY ELEVATIONS.
 - THESE ELEVATIONS ARE REFERENCED TO THE ADJACENT RUNWAY CENTERLINE ELEVATION WHICH IS 756.53.
 - CONTRACTOR TO SHOW ELEVATION OF ANCHOR TOPS ON AS BUILT DRAWINGS.
 - PRIOR TO BEGINNING ANY EXCAVATION, THE CONTRACTOR SHALL CONTACT THE AIRPORT MANAGEMENT, LOCAL FAA PERSONNEL, AND UTILITY COMPANY TO HAVE UNDERGROUND CABLING/UTILITIES LOCATED AND MARKED. ALSO SEE NOTE 6.
 - PRIOR TO BEGINNING ANY EXCAVATION, THE CONTRACTOR SHALL CONTACT THE AIRPORT MANAGEMENT, LOCAL FAA PERSONNEL, UTILITY COMPANY TO COORDINATE CONSTRUCTION ACTIVITIES AND TRENCHING OF UNDERGROUND CABLING.
 - CONTRACTOR SHALL REMOVE 6" OF TOPSOIL. COMPACT SUBGRADE AND PLACE GEOTEXTILE FABRIC CONFORMING TO SECTION 2B.3 OF THE SPECIFICATION FAA-GL-918C OVER SUBGRADE PRIOR TO PLACING 6" COMPACTED DEPTH OF 3/4" CRUSHED AGGREGATE (209) OVER EXCAVATED AREA.



IL. CONTRACT: **DU077**
 IL. LETTING ITEM: **3A**
 IL. PROJECT: **DPA-3769**
 A.L.P. PROJECT: **3-17-0017-B22**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34x22).

DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS
INSTALL RUNWAY 28 PAPI AND RELOCATE REIL LIGHTS
SITE PLAN - PROJECT LAYOUT

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DDM
 DuPage Airport

DESIGN BY:	DLP
DRAWN BY:	JRO
CHECKED BY:	JRL
APPROVED BY:	DLP
DATE:	MARCH 7, 2008
JOB No:	07257-04

SHEET 2 OF 10 SHEETS

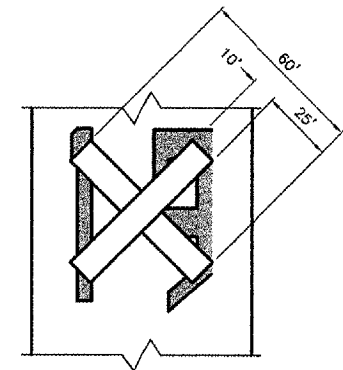
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 LAYOUT: Layout1

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 LAYOUT: Layout1
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 SURVEY BOOK # BOOK #
 REVISIONS
 NUMBER BY DATE
 0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).
 DUPAGE AIRPORT WEST CHICAGO, ILLINOIS
 INSTALL RUNWAY 28 PAPI AND RELOCATE REIL LIGHTS
 SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS
 CMT CRAWFORD, MARRY & TILLY, INC. CONSULTING ENGINEERS License No. 184-000613
 DPAI DuPage Airport
 DESIGN BY: DLP
 DRAWN BY: JRO
 CHECKED BY: JRL
 APPROVED BY: DLP
 DATE: MARCH 7, 2008
 JOB No: 07257-04
 SHEET 4 OF 10 SHEETS

GENERAL NOTES:

- ALL CONSTRUCTION SEQUENCING AND OPERATIONS SHALL CONFORM TO THE APPLICABLE PROVISIONS OF AC 150/5370-2E OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION (LATEST EDITION). **PAYMENT FOR MAINTENANCE OF TRAFFIC AND REQUIREMENTS OF THE SEQUENCE OF CONSTRUCTION SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.**
- THE CONTRACTOR MUST STAGE CONSTRUCTION AROUND AIRPORT OPERATIONS. STAGING SHOWN IS SUGGESTED AND IS INTENDED TO PROVIDE THE CONTRACTOR WITH MAJOR WORK AREAS WHILE MINIMIZING DISRUPTIONS TO AIRPORT OPERATIONS. THE CONTRACTOR MAY USE ALTERNATE STAGING PLANS; HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT DIRECTOR AND RESIDENT ENGINEER AND BE APPROVED BY THE DIVISION OF AERONAUTICS AND FEDERAL AVIATION ADMINISTRATION.
- THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER TWO WORKING DAYS IN ADVANCE OF ANY STAGING CHANGES WHICH WILL REQUIRE CHANGES IN AIRCRAFT MOVEMENT. THE RESIDENT ENGINEER SHALL THEN NOTIFY THE AIRPORT WHO WILL ISSUE APPROPRIATE NOTAMS. THE MOVING AND MAINTENANCE OF ALL TRAFFIC CONTROL SIGNS, BARRICADES, LATHE/RIBBON AT A.O.A. LIMITS ETC. SHALL BE THE CONTRACTOR'S RESPONSIBILITY, AND SHALL BE INCIDENTAL TO THE CONTRACT.
- BARRICADES AT 10' CENTERS SHALL BE PLACED AT THE LOCATIONS SHOWN OR AS DIRECTED BY THE ENGINEER. BARRICADES SHALL BE WEIGHED TO PREVENT BLOWING OVER, HAVE A FLASHING RED LIGHT AND CONFORM TO IDOT STANDARD 702001, TYPE II. ROPE WITH HIGH VISIBILITY ORANGE FLAGGING SHALL BE INSTALLED BETWEEN ALL BARRICADES.
- THE AIRPORT DIRECTOR IN CONSULTATION WITH THE RESIDENT ENGINEER SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING PAVEMENTS, EXCEPT AS SHOWN OR WITH THE PRIOR APPROVAL OF THE AIRPORT DIRECTOR.
- EXISTING AREAS BEYOND PROJECT LIMITS INCLUDING THE HAUL ROAD(S) DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY THE CONTRACTOR TO PRE-CONSTRUCTION CONDITION AT NO ADDITIONAL COST TO THE OWNER AND TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT DIRECTOR.
- EACH DAY AT THE COMPLETION OF WORK, OR MORE FREQUENTLY AS DETERMINED BY THE AIRPORT DIRECTOR, THE CONTRACTOR SHALL BE REQUIRED TO USE A PICKUP TYPE SWEEPER IN ALL ACTIVE CONSTRUCTION AIRFIELD PAVEMENT AREAS AND HAUL ROADS. THE COST OF SWEEPING SHALL BE INCIDENTAL TO THE CONTRACT.
- CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE AREA WHEN CONSTRUCTION IS NOT IN PROGRESS.
- CONTRACTOR'S WORK WITHIN AIRCRAFT OPERATIONS AREA (A.O.A.) SHALL BE EXPEDITED TO MINIMIZE ANY TEMPORARY CLOSURE TIME.
- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE RESIDENT ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY.
- ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A FULL TIME CROSSING GUARD IN RADIO CONTACT WITH THE AIR TRAFFIC CONTROL TOWER SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLES OR EQUIPMENT TO AND FROM THE WORK ZONE. THE RADIO OPERATOR SHALL BE FAMILIAR WITH AIRPORT GROUND CONTROL PROCEDURES. THE CONTRACTOR SHALL PROVIDE THEIR OWN RADIOS. ANY PAVEMENT DAMAGED BY CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND DIRECTOR OF OPERATIONS AT NO ADDITIONAL COST TO THE OWNER. PAVEMENT SHALL BE CONTINUALLY SWEEPED TO PROVIDE A DEBRIS FREE SURFACE DURING ALL HAUL ROAD OPERATIONS. THIS COST OF CROSSING GUARDS, PAVEMENT REPAIR OR ANY NECESSARY ITEMS ASSOCIATED WITH THE SEQUENCE OR STAGING SHALL NOT BE PAID SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. WHEN ACTIVE AIRFIELD PAVEMENTS ARE UTILIZED AS HAUL ROADS BY THE CONTRACTOR, MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- IF REQUESTED BY THE AIRPORT, FAA, OR ENGINEER, REMOVAL ITEMS/MATERIALS SHALL BE DELIVERED TO A DESIGNATED LOCATION AT THE AIRPORT MAINTENANCE BUILDING. IF NOT REQUESTED, MATERIALS REMOVED FROM THE PROJECT SHALL BE DISPOSED OF OFF OF AIRPORT PROPERTY (INCIDENTAL).
- PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO, BARRICADES, SIGNING, RUNWAY CLOSE MARKERS, AIR OPERATIONS AREA (A.O.A.) LATHE AND RIBBON, ETC. SHALL NOT BE PAID FOR SEPERATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR WILL BE REQUIRED TO PLACE A LINE OF LATHE AND RIBBON ALONG THE AIR OPERATIONS AREA (A.O.A.) LIMIT AND ALONG DESIGNATED LIMITS OF CONSTRUCTION AT LOCATIONS AS SHOWN ON THE PLANS.

- THE CONTRACTOR SHALL CONTACT THE RESIDENT ENGINEER AND THE AIRPORT DIRECTOR A MINIMUM OF FIVE (5) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
- COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. SEE SECTION 50-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS.
- ORANGE CONES SHALL BE PLACED AT 25' CENTERS ALONG THE PAVEMENT EDGE DURING PAVING OPERATIONS OF THE CLOSURE LANES TO PREVENT VEHICLES FROM ENTERING.
- VEHICLES AND EQUIPMENT OPERATING ON THE AIRPORT SHALL DISPLAY ORANGE AND WHITE FLAGS (SEE DETAIL THIS SHEET) AND HAVE BEACON LIGHTS.
- ANY PAVEMENT DAMAGED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT DIRECTOR AT NO ADDITIONAL COST TO THE OWNER. THE PAVEMENT SHALL BE CONTINUALLY SWEEPED TO PROVIDE A DEBRIS-FREE SURFACE DURING HAUL ROAD OPERATIONS. THIS COST SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED AT ALL TIMES UNLESS THE GATE IS MANNED WITH A GUARD (COST INCIDENTAL). THE CONTRACTOR SHALL PROVIDE AN "AUTHORIZED PERSONNEL ONLY" SIGN AT THE ACCESS GATE. THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE UPON LEAVING THE SITE. THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGES TO THE ACCESS ROAD, ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COSTS RELATING TO THE CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- WORK WITHIN THE A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BECOME NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, HE SHALL DO SO AT NO EXTRA COST TO THE CONTRACT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND THE AIRPORT DIRECTOR TWO (2) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.
- IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL NOTIFY THE AIRPORT DIRECTOR AND THE RESIDENT ENGINEER IMMEDIATELY.
- DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO WORK.
- THE TALLEST PIECE OF CONSTRUCTION EQUIPMENT IS ANTICIPATED TO BE A STONE/ASPHALT TRUCK, WHICH HAS A MAXIMUM HEIGHT OF 20 FEET IN A FULL DUMP POSITION.
- IF RUNWAY NUMERALS ARE PRESENT DURING CONSTRUCTION, THE CONTRACTOR SHALL PLACE "RUNWAY CLOSED" MARKERS OVER THE NUMERALS AS DETAILED IN THE PLANS. OTHERWISE, "RUNWAY CLOSED" MARKERS SHALL BE PLACED IN THE TURF AT THE ENDS OF THE RUNWAY AS DETAILED IN THE PLANS.
- RUNWAYS SHALL BE CLOSED DURING ANY TIME THE CONTRACTOR IS WORKING WITHIN THE RUNWAY A.O.A. AND/OR OPEN EXCAVATIONS NOT MEETING THE RUNWAY GRADING CRITERIA EXIST WITHIN 200' OF THE AFFECTED RUNWAY CENTERLINE.
- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.



- CLOSED RUNWAY MARKERS SHALL BE YELLOW.
- MARKERS SHALL BE A MATERIAL APPROVED BY THE ENGINEER.
- CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.
- MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.
- COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS AS COORDINATED WITH THE AIRPORT AND TOWER PERSONNEL. THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING A YELLOW CROSS AT THE LOCATION AND DIMENSIONS DETAILED ON THIS SHEET. THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE SUGGESTED SEQUENCE OF CONSTRUCTION.

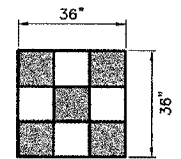
CLOSED RUNWAY MARKER DETAIL
ON PAVEMENT - NO SCALE

DESIGN AIRCRAFT APPROACH CATEGORY: B DESIGN AIRPORT GROUP: II	
MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: 20' POINT "A" - @ VASI REMOVAL CLOSEST POINT OF CONSTRUCTION TO RUNWAY 2L/20R (10/28 CLOSED) STA. 204+89.88, 82' LT. RUNWAY 10/28 CENTERLINE GEOGRAPHIC NAD 83: LATITUDE: 41°54'44.08" LONGITUDE: 88°14'44.38" POTENTIAL OBJECT ELEVATION: 774.8 = (GROUND ELEVATION 754.8 + TRUCK 20')	MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: 20' POINT "B" - @ CABLE INSTALLATION CLOSEST POINT OF CONSTRUCTION TO RUNWAY 10/28 STA. 200+19.82, 200' LT. RUNWAY 10/28 CENTERLINE GEOGRAPHIC NAD 83: LATITUDE: 41°54'42.43" LONGITUDE: 88°14'38.48" POTENTIAL OBJECT ELEVATION: 771.4 = (GROUND ELEVATION 751.4 + TRUCK 20')
MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: 20' POINT "C" - @ CABLE INSTALLATION CLOSEST POINT OF CONSTRUCTION TO RUNWAY 10/28 STA. 192+09.96, 200' LT. RUNWAY 10/28 CENTERLINE GEOGRAPHIC NAD 83: LATITUDE: 41°54'41.60" LONGITUDE: 88°14'27.82" POTENTIAL OBJECT ELEVATION: 773 = (GROUND ELEVATION 753 + TRUCK 20')	MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: 20' POINT "D" - @ RELOCATED REIL UNIT CLOSEST POINT OF CONSTRUCTION TO RUNWAY 15/33 (10/28 CLOSED) STA. 192+06.47, 84' RT. RUNWAY 10/28 CENTERLINE GEOGRAPHIC NAD 83: LATITUDE: 41°54'44.38" LONGITUDE: 88°14'27.38" POTENTIAL OBJECT ELEVATION: 774.5 = (GROUND ELEVATION 754.5 + TRUCK 20')
CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE SPECIAL PROVISIONS SECTION 30-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT: <ul style="list-style-type: none"> RE-MARK AND RE-LIGHT RUNWAY 10/28 REHABILITATION OF RUNWAY 2L/20R - PHASE 1 AIRFIELD SIGNAGE UPDATE - PHASE 1 	

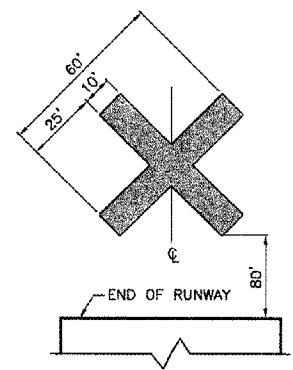
LIMITATIONS ON CONSTRUCTION WITHIN AIRPORT OPERATIONS AREA (AOA)

RUNWAYS:
THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND DIRECTOR OF OPERATIONS TWO (2) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION PHASE CHANGES. WORK SHALL BE EXPEDITED IN RUNWAY 10/28 AND RUNWAY 2L/20R A.O.A. AND NEAR OPEN TAXIWAYS. THE CONTRACTOR SHALL BE GIVEN 10 CALENDAR DAYS TO COMPLETE WORK WITHIN RUNWAY 10/28 AND RUNWAY 2L/20R A.O.A. IF DURING RUNWAY CLOSURE AN EMERGENCY IS DECLARED, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE RUNWAY OF ALL VEHICLES, MEN AND EQUIPMENT.

TAXIWAYS:
CONSTRUCTION WILL BE ALLOWED UP TO THE EDGE OF THE TAXIWAY PAVEMENTS WITHOUT CLOSURE ON A LIMITED BASIS. WORK WITHIN THE TAXIWAY A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT DIRECTOR TWO (2) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.



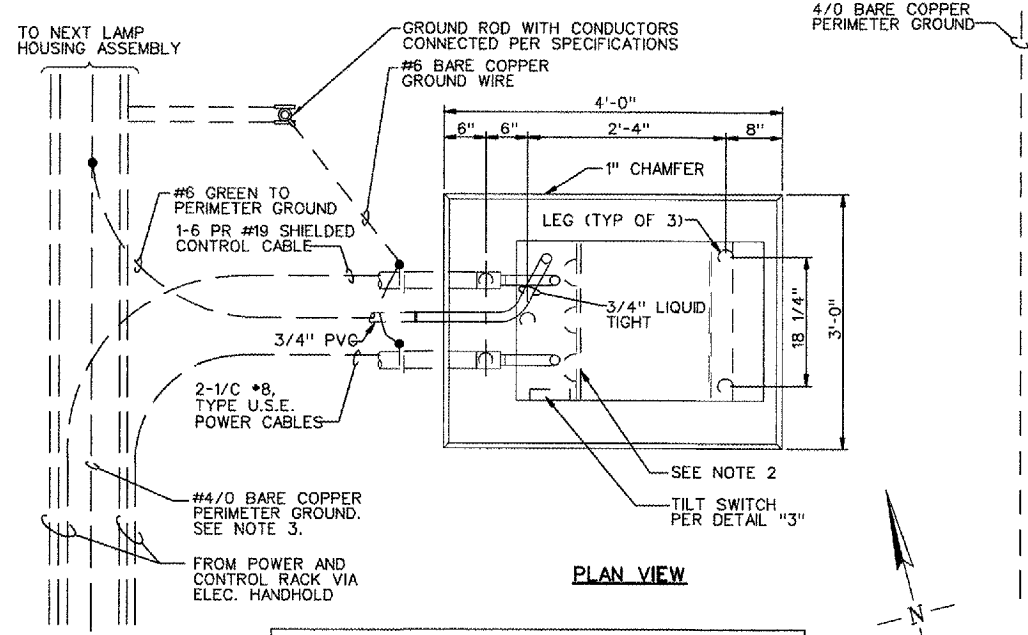
CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG
ORANGE/WHITE



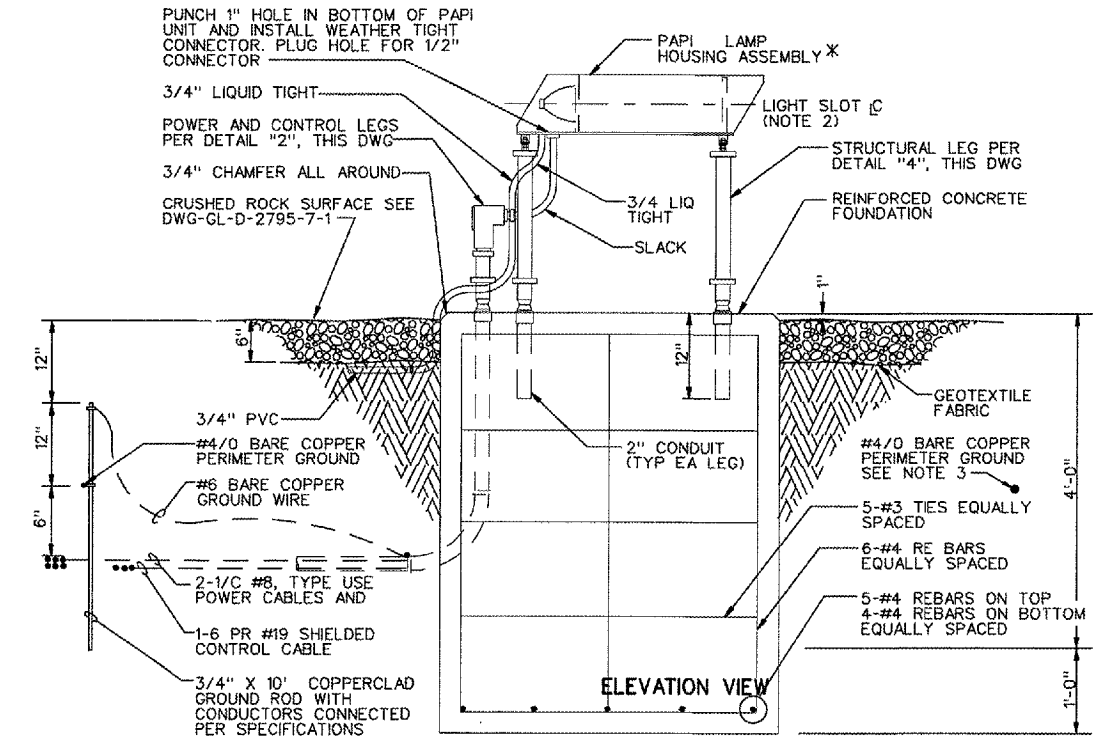
- CLOSED RUNWAY MARKERS SHALL BE YELLOW.
- MARKERS SHALL BE A MATERIAL APPROVED BY THE ENGINEER.
- CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.
- MARKERS SHALL BE PLACED 80- FEET OFF THE RUNWAY-END OR AS NOTED.
- COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

CLOSED RUNWAY MARKER DETAIL
OFF PAVEMENT - NO SCALE

DATE: Wednesday, January 09, 2008 4:56:05 PM
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 UPDATE BY: Jeremy Linke
 LAYOUT: Layout1
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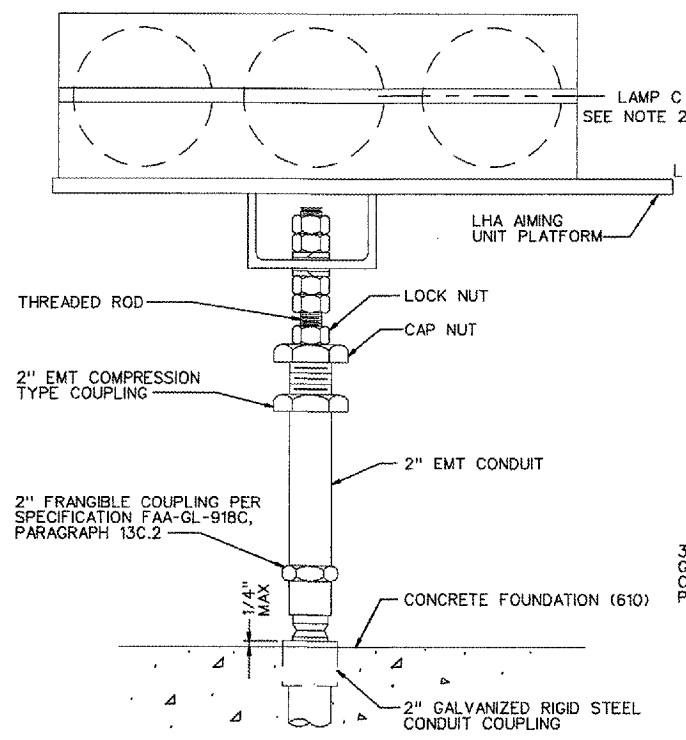


THE PAPI EQUIPMENT SHOWN IS BEDFORD PANORAMIC TYPE AS MANUFACTURED BY GODFREY

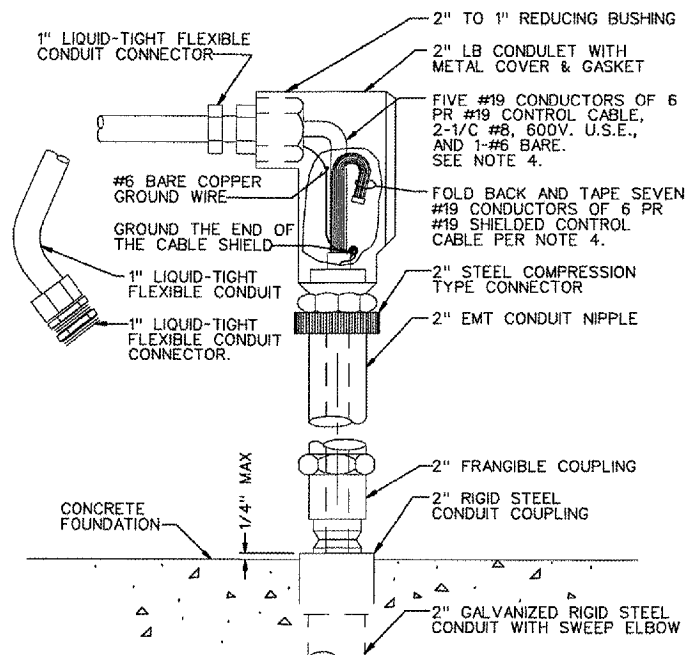


* FURNISHED BY FAA AND INSTALL BY THE CONTRACTOR. ALL OTHER ITEMS SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR.

1 PAPI LAMP HOUSING ASSEMBLY (LHA)
 SCALE IN FEET



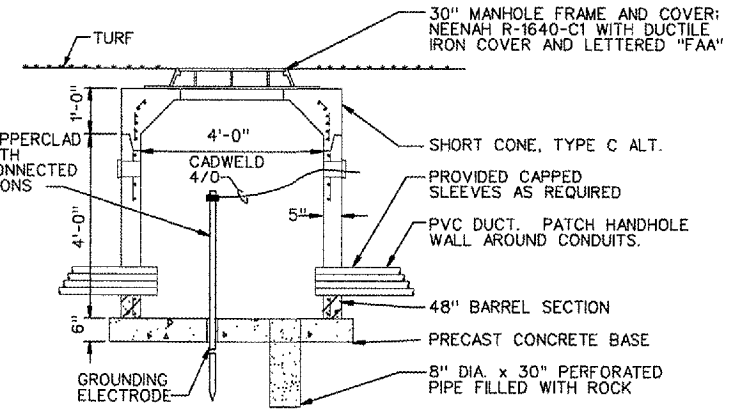
4 LAMP HOUSING ASSEMBLY STRUCTURAL LEG
 SCALE: NONE



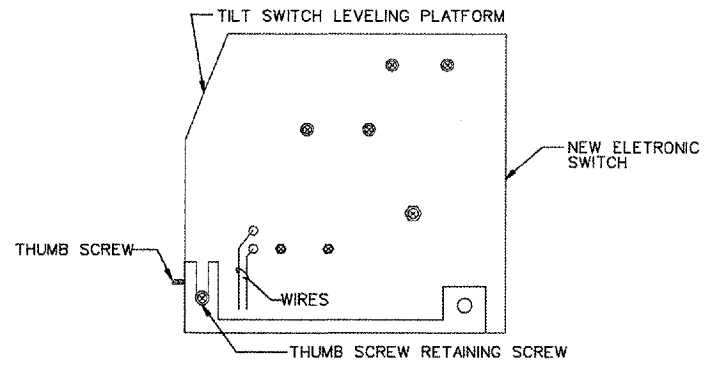
2 POWER & CONTROL LEG
 SCALE: NONE

NOTES

- REMOVE PENDULUM RETAINING BOLT PRIOR TO AMING THE PAPI UNIT AND COVERED HOLE WITH TAPE. AIM PAPI UNIT USING THE SUPPLIED AIMING INSTRUMENT AND TIGHTENED ADJUSTING NUTS. SET AIMING INSTRUMENT TO 0 00" AND SET ON TOP OF TILT SWITCH PLATFORM. LOOSEN THUMB SCREW RETAINING SCREW AND ADJUST TILT SWITCH UNTIL PLATFORM IS LEVEL. TIGHTEN THUMB SCREW RETAINING SCREW. TILT SWITCH IS NOW SET. AIMING TO BE COMPLETED BY FAA PERSONEL.
- FOR LAMP HOUSING ASSEMBLY LAMP CENTERLINE ELEVATION AND AIMING ANGLE, SEE TABLE "A" DWG. GL-D-2795-7-1. THE LAMP HOUSING UNITS ARE INSTALLED SUCH THAT THE BEAM CENTERS OF ALL THE UNITS ARE WITHIN 1 INCH (+/-) OF ELEVATION FROM UNIT TO UNIT.
- THE #4/0 AWG BARE COPPER PERIMETER GROUND INSTALLED IN A CLOSED LOOP ENCOMPASSING ALL FOUR LAMP HOUSING ASSEMBLIES. GROUNDING RODS INSTALLED AT FOUR CORNERS. THE #6 BURIED GUARD WIRE CAD WELDED WITH THE #4/0 TO A GROUND ROD. SEE SYSTEM WIRING DIAGRAM, DWG GL-D-2795-7-4.
- INSULATION AND SHIELD REMOVED FROM A LENGTH OF 6 PR #19 SH CONTROL CABLE SUFFICIENT TO PERMIT 5 CONDUCTORS TO REACH THEIR TERMINATIONS IN THE PAPI LAMP HOUSING UNIT WITHOUT STRAIN. INSIDE THE CONDUIT, THE END OF THE CABLE SHIELD IS GROUNDED TO THE #6 BARE COPPER GROUND WIRE AND FOLDED BACK WITHOUT CUTTING. THE ENDS OF THE UNUSED SEVEN #19 CONDUCTORS TAPED.



6 ELECTRICAL HANDHOLE
 SCALE: NONE

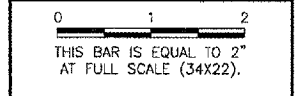


3 TILT SWITCH DETAILS
 SCALE: NONE

IL. CONTRACT: DU077
 IL. LETTING ITEM: 3A
 IL. PROJECT: DPA-3769
 A.I.P. PROJECT: 3-17-0017-B22

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



DUPAGE AIRPORT
 WEST CHICAGO, ILLINOIS
 INSTALL RUNWAY 28 PAPI AND RELOCATE REIL LIGHTS
 LAMP HOUSING DETAILS
 PAPI - RUNWAY 28

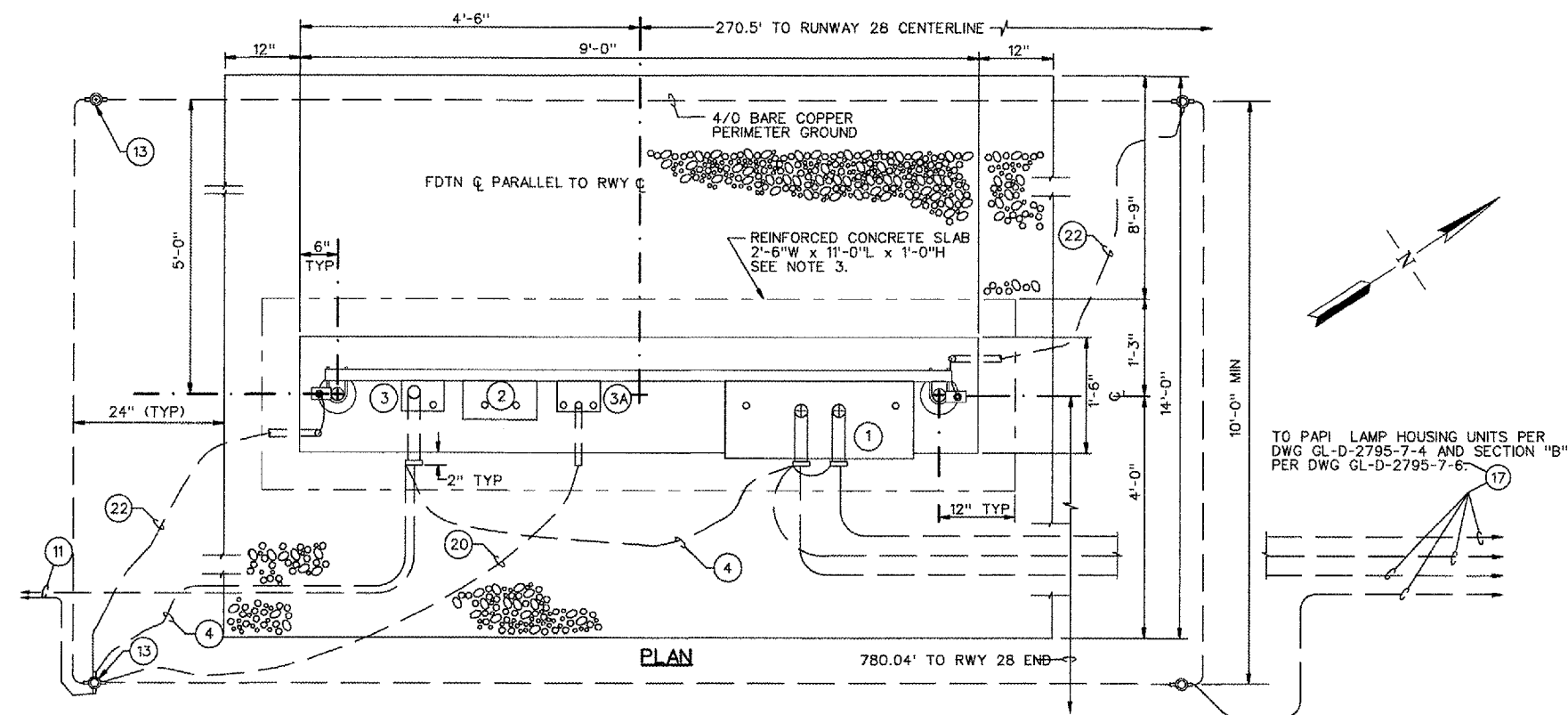
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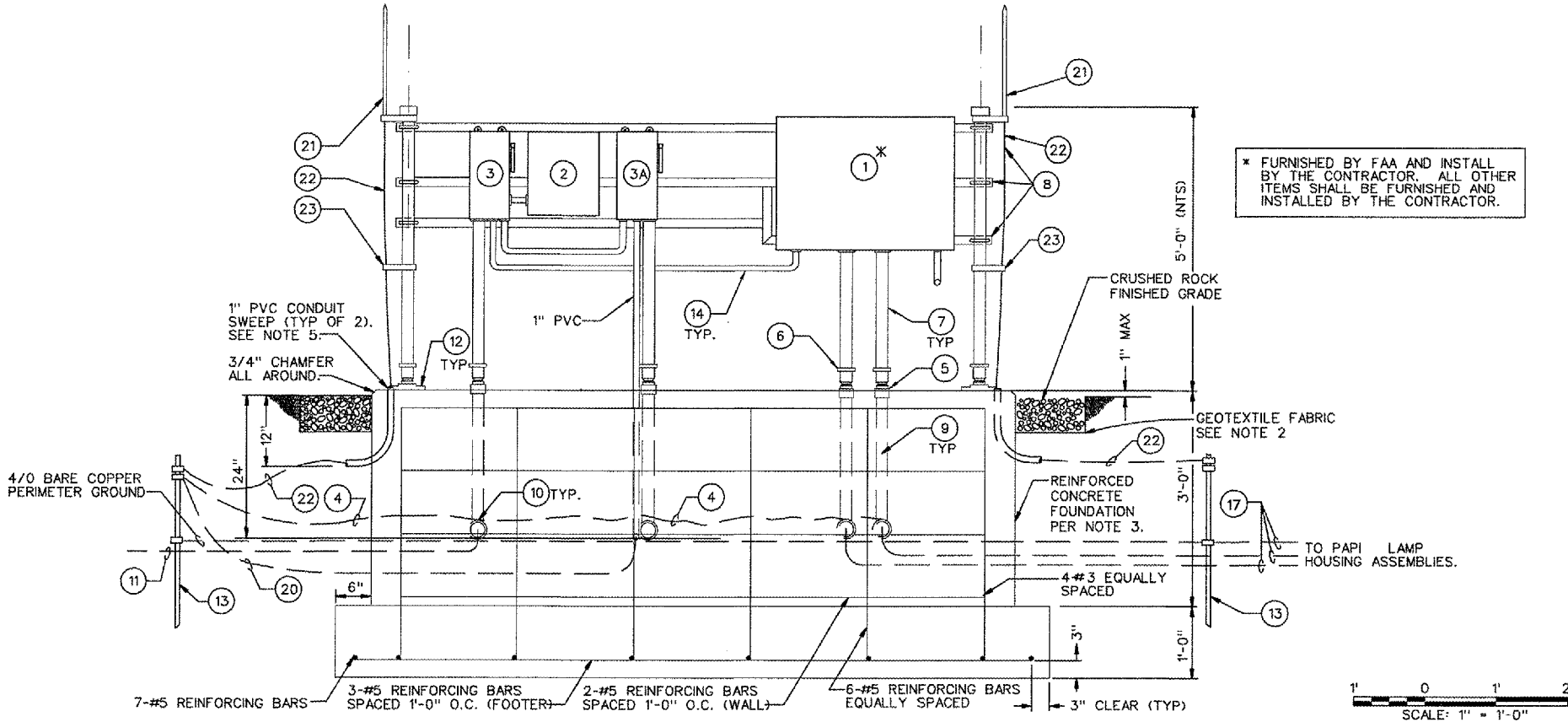
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DRAWN BY:	JRO
CHECKED BY:	JRL
APPROVED BY:	DLP
DATE:	MARCH 7, 2008
JOB No:	07257-04

DATE: Wednesday, January 09, 2008 4:58:10 PM
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 UPDATE BY: Jeremy Linke
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 EMPLOYER: JUS
 DWG: JUS
 SHEET: 4 OF 10

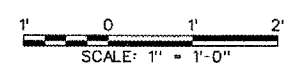


- NUMBERED LEGEND:**
- * (1) PAPI POWER AND CONTROL ASSEMBLY CABINET *
 - (2) SURGE ARRESTER: LPC 20206-7.
 - (3) DISCONNECT SWITCH, SQUARE D CAT. NO. H222NRB.
 - (3A) DISCONNECT SWITCH, SQUARE D CAT. NO. H221NRB.
 - (4) #6 BARE COPPER GROUND WIRE.
 - (5) 2" RIGID STEEL CONDUIT COUPLING. SEE NOTE 5.
 - (6) 2" FRANGIBLE COUPLING
 - (7) 2" EMT CONDUIT
 - (8) UNISTRUT 3000 SERIES CHANNEL
 - (9) 2" GALVANIZED RIGID STEEL CONDUIT, AFTER GROUNDING BUSHING TRANSITION TO PVC CONDUIT
 - (10) GROUNDING BUSHING
 - (11) THREE 1/C #1, 600V TYPE U.S.E. POWER CABLES AND 1-#6 BARE COPPER GUARD WIRE PER SHEET 2 OF 7.
 - (12) GALV. THREADED FLOOR FLANGE WITH MIN. 4 ANCHORS EACH (3 REOD.).
 - (13) 3/4" X 10' COPPERCLAD GROUND ROD WITH CONDUCTORS CONNECTED PER PARAGRAPH 16A.4C OF SPECIFICATION FAA-GL-918D. GROUND RODS ARE SPACED A MINIMUM OF 10' APART.
 - (14) 1" SEAL TIGHT
 - (15) NOT USED
 - (16) NOT USED
 - (17) FOUR 6 PR#19 SH CONTROL CABLES IN 2" C, 4-#6 GREEN GND AND 8-1/C #8 POWER CABLES IN 2" C, AND 1-#6 COPPER GUARD WIRE PER SECTION "B", DWG GL-D-2795-7-6.
 - (18) NOT USED
 - (19) NOT USED
 - (20) #2 GROUNDING ELECTRODE.
 - (21) THOMPSON NO. 660 AIR TERMINAL, THOMPSON NO 27 SWIVEL AND THOMPSON NO. 240XP-4 PIPE BRACKET, 2 PLACES
 - (22) THOMPSON NO. 32 DOWN CONDUCTOR
 - (23) THOMPSON NO. 238 PIPE CLAMP

- NOTES:**
- CONTRACTOR WAS RESPONSIBLE FOR LOCATING AND PROTECTING UTILITIES. THE CONTRACTOR HAND-DUG THE TRENCH IN THE VICINITY OF UNDERGROUND UTILITIES. ANY DAMAGE DONE TO UTILITIES WAS REPAIRED BY THE CONTRACTOR AT NO EXPENSE TO THE CONTRACT.
 - CONTRACTOR REMOVED 6" OF TOPSOIL, COMPACT THE SUBGRADE, AND PLACED GEOTEXTILE FABRIC CONFORMING TO SECTION 2B.3 OF SPECIFICATION FAA-GL-918C OVER SUBGRADE PRIOR TO PLACING 6" COMPACTED DEPTH OF 3/4" CRUSHED ROCK OVER EXCAVATED AREA.
 - CONCRETE WAS PLACED AND CURED IN ACCORDANCE WITH SECTION 3B.2 OF THE SPECIFICATION FAA-GL-918C (ITEM 610).
 - FOR WIRING DIAGRAM, SEE DWG GL-D-2795-7-4.
 - TOPS OF COUPLINGS (5) AND PVC CONDUITS FOR THOMPSON DOWN CONDUCTORS ARE NO MORE THAN 1/4" ABOVE FOUNDATION TOP.



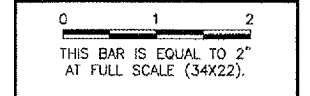
1 DETAIL: PAPI POWER AND CONTROL RACK



ILL. CONTRACT: **DU077**
 ILL. LETTING ITEM: **3A**
 ILL. PROJECT: **DPA-3769**
 A.I.P. PROJECT: **3-17-0017-B22**

SURVEY BOOK # BOOK #

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NUMBER	BY	DATE

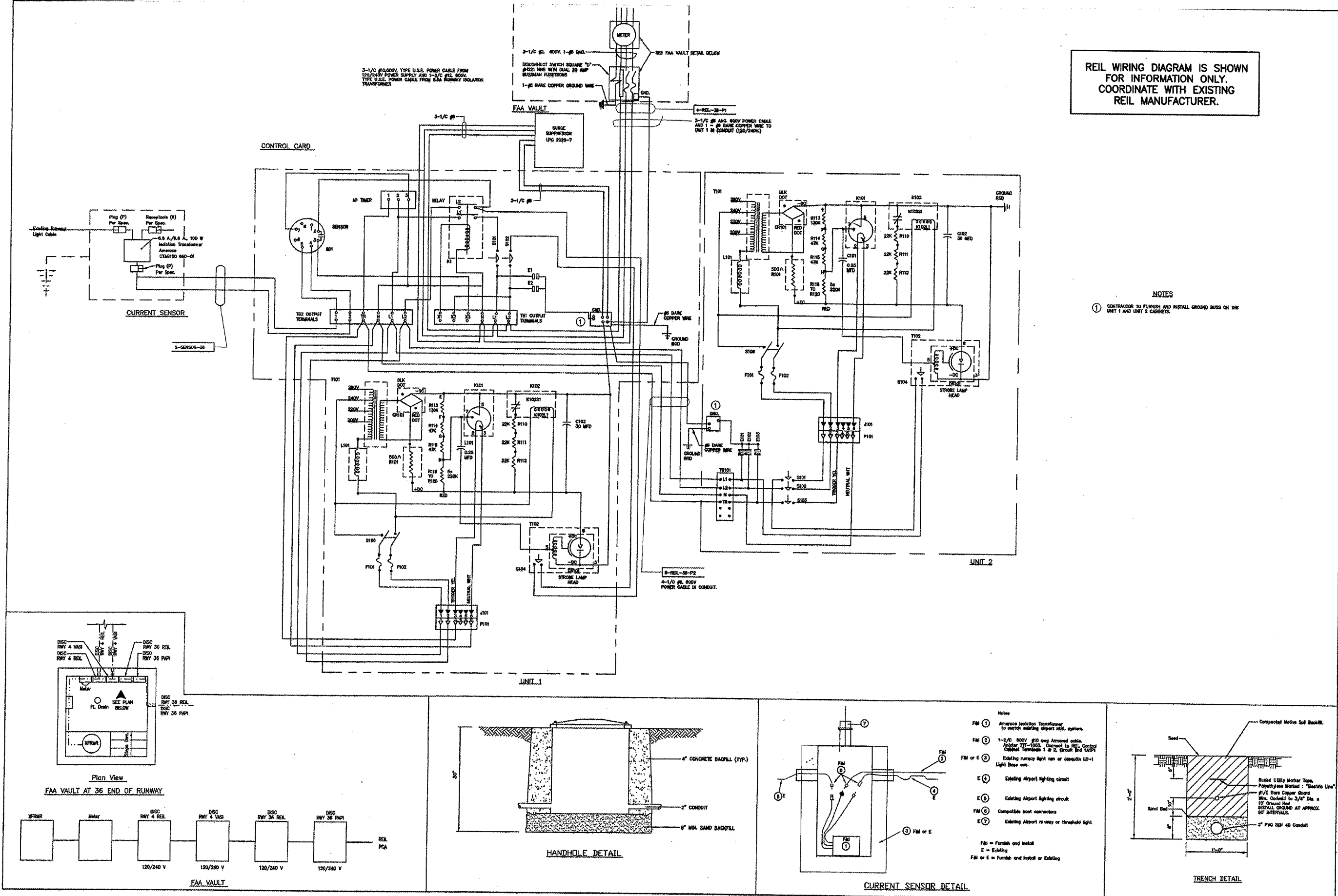


DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS
INSTALL RUNWAY 28 PAPI AND RELOCATE REIL LIGHTS
POWER AND CONTROL INSTALLATION
PAPI - RUNWAY 28

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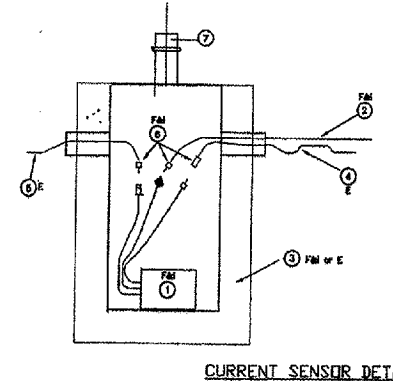
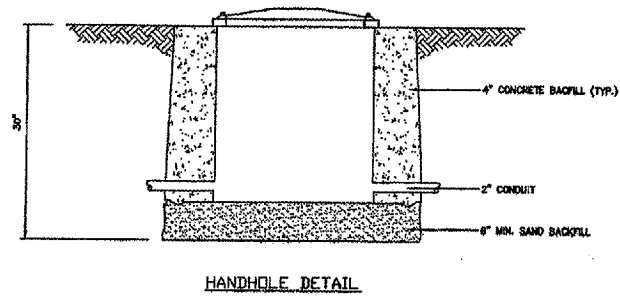
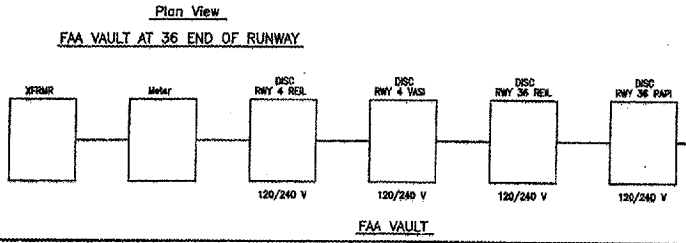
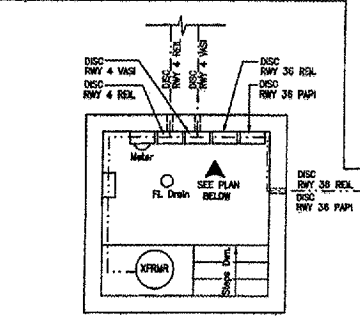
DESIGN BY:	DLP
DRAWN BY:	JRO
CHECKED BY:	JRL
APPROVED BY:	DLP
DATE:	MARCH 7, 2008
JOB No:	07257-04

GL-D-2795-7-3
 SHEET 6 OF 10 SHEETS

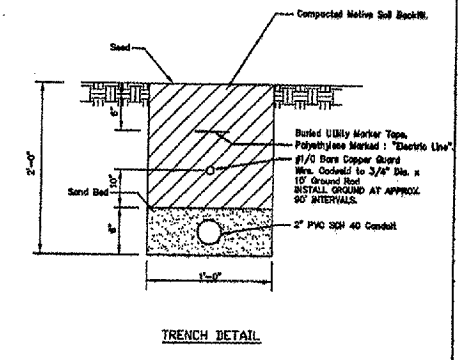


REIL WIRING DIAGRAM IS SHOWN FOR INFORMATION ONLY. COORDINATE WITH EXISTING REIL MANUFACTURER.

NOTES
 ① CONTRACTOR TO FURNISH AND INSTALL GROUND BASS ON THE UNIT 1 AND UNIT 2 CABINETS.



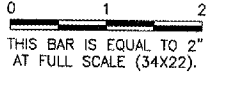
- Notes
- FM ① Avconce Isolation Transformer to match existing airport PSL system.
 - FM ② 1-3/4" BODY #10 AWG Armored cable. Addstar 777-1003. Connect to REL Control Cabinet Terminals 1 & 2. Circuit Break 1A/1P
 - FM or E ③ Existing runway light run or Jeppesen LD-1 Light Base run.
 - E ④ Existing Airport lighting circuit.
 - E ⑤ Existing Airport lighting circuit.
 - FM ⑥ Compatible boot connectors.
 - E ⑦ Existing Airport runway or threshold light.
- FM = Furnish and Install
 E = Existing
 FM or E = Furnish and Install or Existing



IL CONTRACT: DU077
 IL LETTING ITEM: 3A
 IL PROJECT: DPA-3769
 A.I.P. PROJECT: 3-17-0017-B22

SURVEY BOOK # BOOK #

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DUPAGE AIRPORT
 WEST CHICAGO, ILLINOIS

INSTALL RUNWAY 28 PAPI AND RELOCATE REIL LIGHTS

REIL WIRING DIAGRAM
 REIL - RUNWAY 28

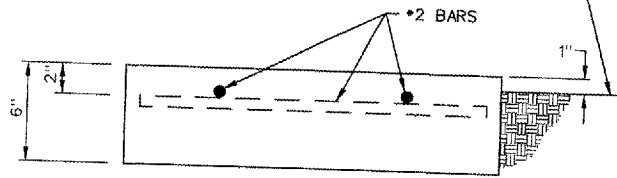
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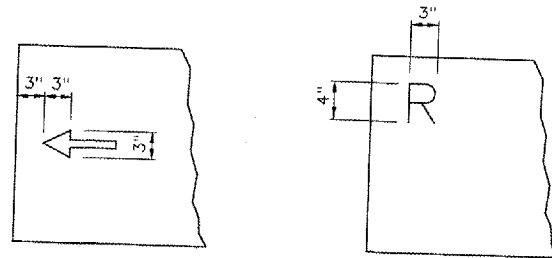
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 DRAWN BY: JRO
 CHECKED BY: JRL
 APPROVED BY: DLP
 DATE: MARCH 7, 2008
 JOB No: 07257-04

FINISHED GRADE HEIGHTS OF THE MARKER TOPS MORE THAN 1" ABOVE FINISHED GRADE WILL NOT BE ACCEPTED.



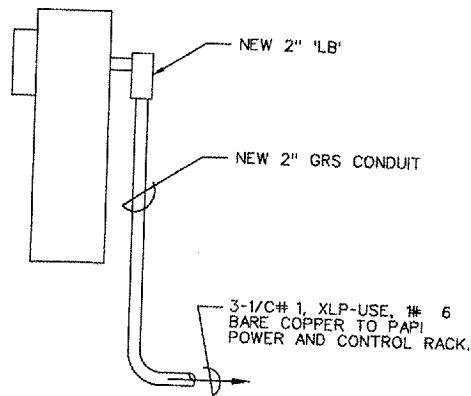
SECTION 'A-A'



DETAIL "A"

DETAIL "B"

CABLE MARKER DETAILS
SCALE: NONE



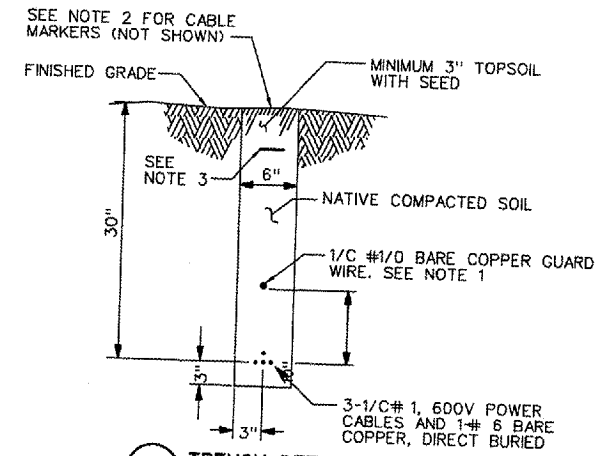
EXISTING ELECTRIC SERVICE PEDESTAL DETAIL
SCALE: NONE

NOTES:

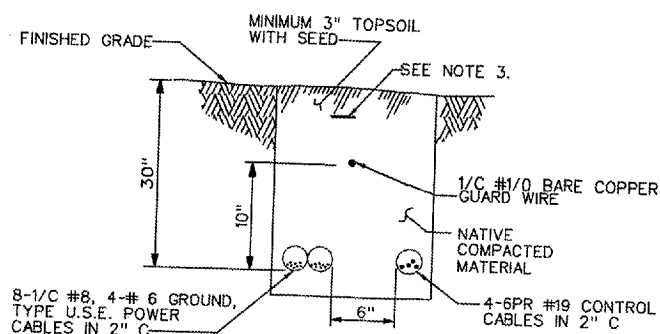
1. CONTRACTOR WAS RESPONSIBLE FOR LOCATING AND PROTECTING UTILITIES. THE CONTRACTOR HAND-DUG THE TRENCH IN THE VICINITY OF UNDERGROUND UTILITIES. ANY DAMAGE DONE TO UTILITIES WAS REPAIRED BY THE CONTRACTOR AT NO EXPENSE TO THE GOVERNMENT.
2. CONTRACTOR INSTALLED CABLE MARKERS FOR BURIED CABLING PER SECT. 16F.3g OF SPEC. FAA-GL-918C AND DWG GL-D-5388G.
3. CONTRACTOR INSTALLED WARNING TAPE "CAUTION-UNDERGROUND CABLE". ALLEN SYSTEMS P.N. 0761315 OR EQUAL, INSTALLED 6" BELOW GRADE.
4. CONTRACTOR REPAIRED ALL DAMAGED SURFACES IN ACCORDANCE WITH FAA-GL-918C-2A.9d.

NOTES:

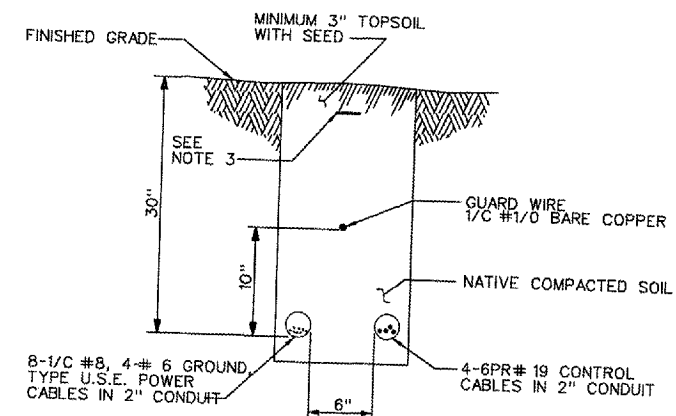
GROUND RODS NOT SHOWN IN TRENCH DETAILS.
SEE SITE PLAN FOR TRENCH TYPE LOCATION.



A TRENCH DETAIL
SCALE: NONE
BETWEEN SERVICE PEDESTAL AND PAPI POWER AND CONTROL RACK



C TRENCH DETAIL
SCALE: NONE
COMMON TRENCH AT POWER CONTROL RACK
(SEE SITE PLAN DWG GL-D-2795-7-1)



B SECTION
SCALE: NONE
TRENCH BETWEEN PAPI POWER AND CONTROL RACK AND ELEC. HANDHOLE
(SEE SITE PLAN DWG GL-D-2795-7-1)

IL CONTRACT: DU077
IL LETTING ITEM: 3A
IL PROJECT: DPA-3769
A.I.P. PROJECT: 3-17-0017-B22

SURVEY BOOK # BOOK #

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0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS

INSTALL RUNWAY 28 PAPI AND RELOCATE REIL LIGHTS
MISCELLANEOUS DETAILS

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GL-D-2795-7-6