

| F.A.U. RTE. | SECTION        | COUNTY   | TOTAL SHEETS       | SHEET NO. |
|-------------|----------------|----------|--------------------|-----------|
| 1648        | 23-00118-00-RS | KANE     | 29                 | 1         |
|             |                | ILLINOIS | CONTRACT NO. 61L26 |           |

03-07-2025 LETTING ITEM 014

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**PROPOSED  
HIGHWAY PLANS**

FAU ROUTE 1648 (PRAIRIE STREET)  
RANDALL ROAD (FAP 0336) TO SOUTH 13TH STREET  
SECTION 23-00118-00-RS  
PROJECT C2FM(338)  
RESURFACING  
KANE COUNTY  
CITY OF ST. CHARLES

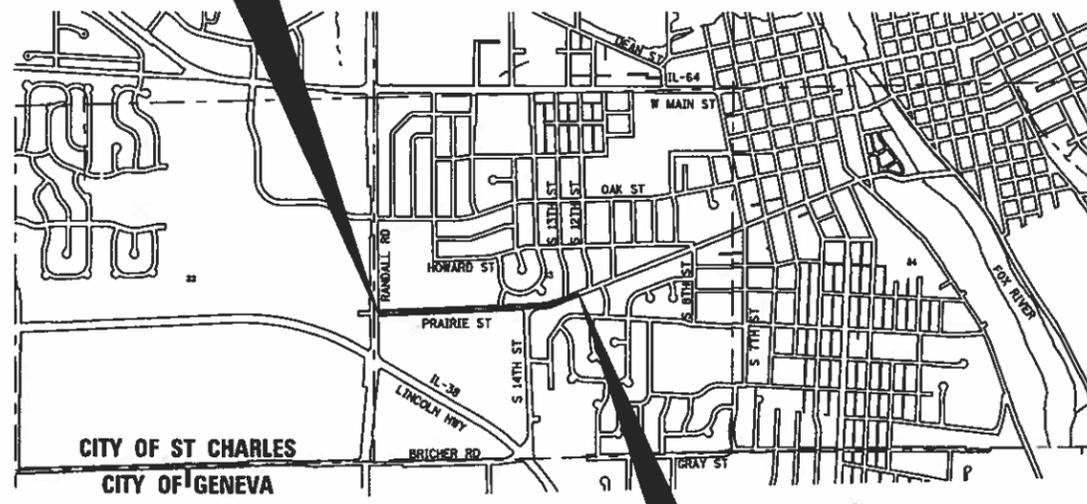
FOR INDEX OF SHEETS, SEE SHEET NO. 2



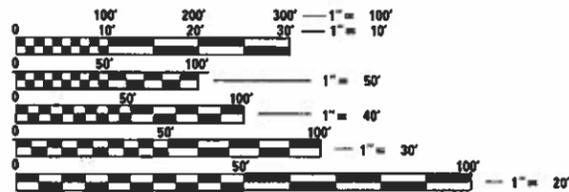
**TRAFFIC DATA**  
FUNCTIONAL CLASSIFICATION  
MAJOR COLLECTOR  
2018 ADT = 7,900  
P.V.=00.0% S.U.=0.0% M.U.=0.0%  
POSTED SPEED: 30 MPH  
DESIGN SPEED: 30 MPH

C-91-149-24

BEGIN IMPROVEMENT  
STA 1+89



END IMPROVEMENT  
STA 31+18



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS  
1-800-892-0123  
OR 811

LOCATION MAP  
1" = 1/4 MILE

GROSS LENGTH = 2,929 FT. = 0.55 MILE  
NET LENGTH = 2,929 FT. = 0.55 MILE

PLANS PREPARED BY:  
CITY OF ST. CHARLES

FEDERAL AID PROGRAM ENGINEER: CARMEN E. RAMOS, P.E. SCHAUMBURG, IL

PROJECT ENGINEER: ZLATKO NIKOLOV, P.E.  
PROJECT MANAGER: CHRIS GOTTLIEB, P.E.

CONTRACT NO. 61L26

|   |  |
|---|--|
| AGENCY RESPONSIBLE FOR LETTING            |  |
| Approved                                  | 12/20/2024<br><i>Zlatko Nikolov</i> C.E. 2<br>Local Agency, Position           |
| Passed                                    | 1-2-2025<br><i>C. Gottlieb</i><br>District 1 Engineer of Local Roads & Streets |
| Releasing for Bid Based on Limited Review | 1.6.2025<br><i>Jose R. O'Shea</i><br>Regional Engineer                         |

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# GENERAL NOTES

## SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS

- ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2022 (HEREINAFTER REFERRED TO AS THE STANDARD SPECIFICATIONS); THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2025; THE LATEST EDITIONS OF THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER CONSTRUCTION IN ILLINOIS"; THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS"; THE "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS"; THE CITY OF ST. CHARLES SPECIFICATIONS; THE "DETAILS" IN THE PLANS AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.
- PRIOR TO COMMENCEMENT OF CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS AFFECTING THEIR WORK WITH THE ACTUAL CONDITIONS AT THE JOB SITE. IN ADDITION, THE CONTRACTOR MUST VERIFY THE ENGINEER'S LINE AND GRADE STAKES. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONSTRUCTION PLANS, HE MUST IMMEDIATELY REPORT SAME TO THE ENGINEER BEFORE DOING ANY WORK, OTHERWISE THE CONTRACTOR ASSUMES FULL RESPONSIBILITY. IN THE EVENT OF DISAGREEMENT BETWEEN THE CONSTRUCTION PLANS, STANDARD SPECIFICATIONS AND/OR SPECIAL DETAILS, THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTIONS FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF THE WORK AFFECTED BY OMISSIONS OR DISCREPANCIES. FAILING TO SECURE SUCH INSTRUCTIONS, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEED AT HIS OWN RISK AND EXPENSE. IN THE EVENT OF ANY DOUBT OR QUESTION ARISING WITH RESPECT TO THE TRUE MEANING OF THE CONSTRUCTION PLANS OR SPECIFICATIONS, THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE OR CITY PROPERTY WITHOUT WRITTEN PERMISSION FROM THE CITY.
- OFFSET LOCATIONS GIVEN IN THE PLANS FOR STRUCTURES, EDGE OF PAVEMENT, ETC. ARE FROM THE ROADWAY CENTERLINE.
- THE CONTRACTOR SHALL NOTIFY THE CITY OF ST CHARLES ENGINEER 48 HOURS PRIOR TO COMMENCEMENT OF ALL ITEMS OF WORK.
- ALL ENGINES AND ENGINE DRIVEN EQUIPMENT USED FOR HAULING OR CONSTRUCTION SHALL BE EQUIPPED WITH AN ADEQUATE MUFFLER IN CONSTANT OPERATION AND PROPERLY MAINTAINED TO PREVENT EXCESSIVE UNUSUAL NOISES. ANY MACHINE OR DEVICE OR PART THEREOF WHICH IS REGULATED BY OR BECOMES REGULATED BY FEDERAL OR STATE OF ILLINOIS NOISE STANDARDS SHALL CONFORM TO THOSE STANDARDS.
- THE CONTRACTOR SHALL SCHEDULE AND CONDUCT HIS OPERATIONS SO THAT THE CLOSURE TIME OF EXISTING DRIVEWAYS ALONG THE ROUTE OF IMPROVEMENTS IS KEPT TO A MINIMUM. ALL HOMEOWNERS SHALL BE GIVEN A MINIMUM 48-HOUR NOTICE PRIOR TO INITIAL REMOVAL OF THEIR DRIVEWAY APRON. THE CONTRACTOR SHALL MAKE EVERY EFFORT TO KEEP DRIVEWAYS OPEN INCLUDING TEMPORARY GRADING AND PLACEMENT OF AGGREGATE.
- THE CONTRACTOR AND HIS SUBCONTRACTORS SHALL REMOVE FROM THE JOBSITE ANY RUBBISH AND WASTE MATERIAL BY THE END OF EACH WORKING DAY. THE CONTRACTOR SHALL KEEP THE PREMISES CLEAN DURING CONSTRUCTION TO THE SATISFACTION OF THE ENGINEER, INCLUDING UTILIZATION OF A STREET SWEEPER OR HAND CLEANING AS NECESSARY.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR POSTING SUITABLE ADVANCE NOTICE ON STREETS SCHEDULED TO BE RESURFACED AT LEAST 24 HOURS, BUT NOT MORE THAN 48 HOURS, PRIOR TO COMMENCEMENT OF WORK. "NO PARKING" SIGNS DISPLAYING CONSTRUCTION TIMES SHALL BE PLACED ON ROADWAYS 48 HOURS PRIOR TO PAVEMENT REMOVAL, TACK COAT, AND HMA PLACEMENT. ALL SUCH NOTICES SHALL BE REMOVED BY THE CONTRACTOR IMMEDIATELY UPON THE COMPLETION OF WORK IN EACH BLOCK.
- CONTRACTOR SHALL PROVIDE THE ENGINEER NOTIFICATION TEN WORKING DAYS PRIOR TO THE REMOVAL OF ANY TREE OR SHRUB.
- THE CONTRACTOR SHALL TAKE PRECAUTION BY PRESERVING EXISTING TREES WITHIN THE RIGHT OF WAY. IF ANY DAMAGE OCCURS, TREES SHALL BE REPLACED IN KIND PER ARTICLE 201.07. REPAIR OR REPLACEMENT OF EXISTING PLANT MATERIAL REQUIREMENTS STATED HEREIN.

## STORM SEWERS, WATER MAINS, AND UTILITIES

- THE CONTRACTOR MUST EXERCISE EXTREME CAUTION WHILE WORKING AROUND EXISTING UTILITIES. THE CONTRACTOR SHALL NOTIFY JULIE (1-800-892-0123) A MINIMUM OF 48 HOURS BEFORE COMMENCING CONSTRUCTION FOR UTILITY LOCATIONS WITHIN THE SCOPE OF THE PROJECT. IT IS RECOMMENDED THAT THE CONTRACTOR CONDUCT A JOINT UTILITY MEET PRIOR TO COMMENCING WORK. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO CONTACT AGENCIES WHO MAY OR MAY NOT BE PART OF THE JULIE SYSTEM IN ORDER TO OBTAIN THE HORIZONTAL AND VERTICAL FIELD LOCATIONS OF THEIR FACILITIES WITHIN THE LIMITS OF THE PROPOSED IMPROVEMENTS.
- ALL UTILITY COMPANIES SHALL BE NOTIFIED AT LEAST 3 DAYS PRIOR TO THE START OF CONSTRUCTION.
- CONSTRUCTION STAKING AND IDENTIFICATION OF UTILITY REPAIR LOCATIONS MUST BE COMPLETED BY THE CONTRACTOR BEFORE COMMENCEMENT OF ALL OTHER CONTRACT WORK.
- THE CONTRACTOR SHALL USE ALL NECESSARY PRECAUTIONS AND PROTECTION MEASURES REQUIRED TO MAINTAIN EXISTING UTILITIES, SEWERS, AND APPURTENANCES THAT MUST BE KEPT IN OPERATION.
- THE CONTRACTOR SHALL ENSURE THAT ALL WATER SYSTEM VALVES, VALVE VAULTS, AND SANITARY SEWER MANHOLES REMAIN READILY ACCESSIBLE TO THE CITY FOR EMERGENCY OPERATIONS. THE LOCATIONS OF ALL WATER AND SANITARY FACILITIES SHALL BE MARKED AND BE READILY VISIBLE AT ALL TIMES.
- THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY COMPANIES INVOLVED WITH THE REMOVAL, TEMPORARY RELOCATION, SUPPORT, PROTECTION, RECONSTRUCTION, OR ABANDONMENT BY THESE COMPANIES OF ANY AND ALL SERVICES OR FACILITIES OWNED OR OPERATED BY THEM WITHIN THE PROJECT LIMITS.

## TRAFFIC CONTROL

- THE CONTRACTOR SHALL AT ALL TIMES PROVIDE PROTECTION FOR TRAFFIC AS CALLED FOR IN THE SPECIAL PROVISIONS & PLANS.
- THE CONTRACTOR SHALL MAINTAIN EXISTING SIDE STREET ACCESS AND EXISTING DRIVEWAY ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING CONSTRUCTION OF THIS PROJECT.
- ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 107.14 OF THE STANDARD SPECIFICATIONS.
- PAVEMENT MARKING TAPE, TYPE IV, SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
- CARE IS TO BE TAKEN AS NOT TO DAMAGE ANY OF THE EXISTING TRAFFIC SIGNAL CONDUITS AND EQUIPMENT. IF ANY OF THE TRAFFIC SIGNAL CONDUIT AND/OR EQUIPMENT IS DAMAGED, THE CONTRACTOR SHALL REPAIR AND/OR REPLACE THE CONDUIT AND/OR EQUIPMENT AT NO COST TO THE COUNTY.

## EROSION CONTROL

- ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE KANE COUNTY AND THE CITY OF ST. CHARLES STORMWATER ORDINANCES, ALL CONSTRUCTION ACTIVITIES WILL BE IN ACCORDANCE WITH THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM STORM WATER PERMIT ILR10 AND THE SWPPP DOCUMENT. THE CONTRACTOR SHALL ALSO FOLLOW THE STANDARDS AND SPECIFICATIONS IN ACCORDANCE WITH THE ILLINOIS URBAN MANUAL.
- SEDIMENT AND EROSION CONTROL DEVICES SHALL BE FUNCTIONAL BEFORE THE PROJECT SITE IS OTHERWISE DISTURBED.
- CLEANING OF VEHICLES AND EQUIPMENT, INCLUDING CONCRETE MIXERS, SHALL BE PERFORMED IN A MANNER TO REDUCE THE AMOUNT OF POLLUTANTS TRIBUTARY TO STORM SEWERS AND OPEN WATERS TO THE MAXIMUM EXTENT PRACTICAL.
- ALL EROSION CONTROL MEASURES SHALL BE KEPT OPERATIONAL AND MAINTAINED CONTINUOUSLY THROUGHOUT THE PERIOD OF LAND DISTURBANCE UNTIL PERMANENT SEDIMENT AND EROSION CONTROL MEASURES ARE OPERATIONAL.
- THE CONTRACTOR SHALL USE CONCRETE WASHOUT BOXES.

## COMMITMENTS

- ANY SOIL EXCAVATION WILL BE LEFT ON, OR INCORPORATED WITHIN, THE LPA ROW.

## INDEX OF SHEETS

| SHEET NO. | DESCRIPTION  |
|-----------|--|
| 1         | COVER SHEET  |
| 2         | GENERAL NOTES, INDEX OF SHEETS & HIGHWAY STANDARDS |
| 3 - 6     | SUMMARY OF QUANTITIES                              |
| 7 - 8     | TYPICAL SECTIONS                                   |
| 9         | SCHEDULE OF QUANTITIES                             |
| 10 - 12   | ROADWAY AND UTILITY PLANS                          |
| 13 - 15   | PAVEMENT MARKING PLANS                             |
| 16        | DETECTOR LOOP PLAN                                 |
| 17 - 18   | KDOT TRAFFIC SIGNAL DRAWINGS                       |
| 19 - 20   | UTILITY TYPICAL DETAILS                            |
| 21 - 29   | IDOT STANDARD DRAWINGS                             |

## HIGHWAY STANDARDS

|           |  |
|-----------|--|
| 000001-08 | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS         |
| 424001-12 | PERPENDICULAR CURB RAMPS FOR SIDEWALKS               |
| 606001-08 | CONC. CURB TYPE B AND COMB. CURB AND GUTTER          |
| 701006-05 | OFF-RD OPERATIONS, 2L, 2W, 15' TO 24" FROM PVT. EDGE |
| 701301-04 | LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS          |
| 701311-03 | LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY   |
| 701501-06 | URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED                |
| 701701-10 | URBAN LANE CLOSURE, MULTILANE INTERSECTION           |
| 701801-06 | SIDEWALK, CORNER OR CROSSWALK CLOSURE                |
| 701901-10 | TRAFFIC CONTROL DEVICES                              |
| 780001-05 | TYPICAL PAVEMENT MARKINGS                            |

## DISTRICT 1 DETAILS

|       |   |
|-------|---|
| TC-10 | TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS |
| TC-13 | DISTRICT ONE TYPICAL PAVEMENT MARKINGS                                      |
| TC-16 | PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING                    |
| TC-22 | ARTERIAL ROAD INFORMATION SIGN  |
| BD-22 | PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT                                 |
| BD-24 | CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT                             |
| BD-32 | BUTT JOINT AND HMA TAPER DETAILS  |
| TS-05 | STANDARD TRAFFIC SIGNAL DESIGN DETAILS                                      |
| TS-07 | DETAILS FOR ROADWAY RESURFACING   |

| FILE NAME | USER NAME =  | DESIGNED - Z.N. | REVISED - | STATE OF ILLINOIS<br>DEPARTMENT OF TRANSPORTATION |  |  |  | PRAIRIE STREET<br>GENERAL NOTES, INDEX OF SHEETS & HIGHWAY STANDARDS |  |  |  | F.A.U<br>RTE. | SECTION        | COUNTY | TOTAL<br>SHEETS | SHEET<br>NO.                     |  |  |  |   |  |  |  |
|-----------|--------------|-----------------|-----------|---|--|--|--|--|--|--|--|---------------|----------------|--------|-----------------|----------------------------------|--|--|--|---|--|--|--|
|           |              | DRAWN - Z.N.    | REVISED - |   |  |  |  |  |  |  |  | 1648          | 23-00118-00-RS | KANE   | 29              | 2                                |  |  |  |   |  |  |  |
|           | PLOT SCALE = | CHECKED -       | REVISED - |   |  |  |  |  |  |  |  | SCALE:        |                |        |                 | SHEET NO. OF SHEETS STA. TO STA. |  |  |  | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |  |  |  |
|           | PLOT DATE =  | DATE -          | REVISED - |   |  |  |  |  |  |  |  |               |                |        |                 |                                  |  |  |  |   |  |  |  |

| CODE NO. | ITEM   | UNIT  | TOTAL QUANTITY | CONSTRUCTION CODE        |                              |                         |                            |                            |
|----------|--|-------|----------------|--------------------------|------------------------------|-------------------------|----------------------------|----------------------------|
|          |  |       |                | STU FUNDS                |                              |                         |                            | CITY FUNDS                 |
|          |  |       |                | 75% FED<br>25% CITY      | 75% FED<br>25% CITY          | 75% FED<br>25% CITY     | 75% FED<br>25% CITY        | 100% CITY                  |
|          |  |       |                | ROADWAY<br>0005<br>URBAN | ENVIRONMENT<br>0020<br>URBAN | SAFETY<br>0021<br>URBAN | LANDSCAPE<br>0031<br>URBAN | UTILITIES<br>0043<br>URBAN |
| 20201200 | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL          | CU YD | 116.5          |                          |                              |                         | 116.5                      |                            |
| 21001000 | GEO TECHNICAL FABRIC FOR GROUND STABILIZATION        | SQ YD | 350.0          | 350.0                    |                              |                         |                            |                            |
| 21101615 | TOPSOIL FURNISH AND PLACE, 4"                        | SQ YD | 300.3          |                          |                              |                         | 300.3                      |                            |
| 25000100 | SEEDING, CLASS 1                                     | ACRE  | 0.1            |                          |                              |                         | 0.1                        |                            |
| 25100630 | EROSION CONTROL BLANKET                              | SQ YD | 300.3          |                          | 300.3                        |                         |                            |                            |
| 28000250 | TEMPORARY EROSION CONTROL SEEDING                    | POUND | 10.0           |                          | 10.0                         |                         |                            |                            |
| 28000510 | INLET FILTERS  | EACH  | 10.0           |                          | 10.0                         |                         |                            |                            |
| 30300001 | AGGREGATE SUBGRADE IMPROVEMENT                       | CU YD | 115.5          | 115.5                    |                              |                         |                            |                            |
| 40600275 | BITUMINOUS MATERIALS (PRIME COAT)                    | POUND | 241.0          | 241.0                    |                              |                         |                            |                            |
| 40600290 | BITUMINOUS MATERIALS (TACK COAT)                     | POUND | 9409.0         | 9409.0                   |                              |                         |                            |                            |
| 40600370 | LONGITUDINAL JOINT SEALANT                           | FOOT  | 7500.0         | 7500.0                   |                              |                         |                            |                            |
| 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT         | SQ YD | 186.0          | 186.0                    |                              |                         |                            |                            |
| 40603080 | HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50          | TON   | 2147.0         | 2147.0                   |                              |                         |                            |                            |
| 40604060 | HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50 | TON   | 1171.0         | 1171.0                   |                              |                         |                            |                            |

\* SPECIALTY ITEM

|           |             |                 |           |   |   |   |                |        |              |           |  |
|-----------|-------------|-----------------|-----------|---|---|---|----------------|--------|--------------|-----------|--|
| FILE NAME | USER NAME = | DESIGNED - Z.N. | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>PRAIRIE STREET<br/>SUMMARY OF QUANTITIES</b> | F.A.U. RTE.   | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |  |
|           |             | DRAWN - Z.N.    | REVISED - |   |   | 1648  | 23-00118-00-R5 | KANE   | 29           | 3         |  |
|           |             | CHECKED -       | REVISED - |   |   | CONTRACT NO. 61L26                                  |                |        |              |           |  |
|           |             | DATE -          | REVISED - |   |   | FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT |                |        |              |           |  |
|           |             |                 |           | SCALE:  | SHEET NO. OF SHEETS                             | STA.  | TO STA.        |        |              |           |  |

| CODE NO. | ITEM  | UNIT  | TOTAL QUANTITY | CONSTRUCTION CODE  |                        |                   |                      |                      |  |
|----------|---|-------|----------------|--------------------|------------------------|-------------------|----------------------|----------------------|--|
|          |   |       |                | STU FUNDS          |                        |                   |                      | CITY FUNDS           |  |
|          |   |       |                | 75% FED 25% CITY   | 75% FED 25% CITY       | 75% FED 25% CITY  | 75% FED 25% CITY     | 100% CITY            |  |
|          |   |       |                | ROADWAY 0005 URBAN | ENVIRONMENT 0020 URBAN | SAFETY 0021 URBAN | LANDSCAPE 0031 URBAN | UTILITIES 0043 URBAN |  |
| 42400200 | PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH        | SQ FT | 1971.0         | 1971.0             |                        |                   |                      |                      |  |
| 42400300 | PORTLAND CEMENT CONCRETE SIDEWALK 6 INCH        | SQ FT | 316.0          | 316.0              |                        |                   |                      |                      |  |
| 42400800 | DETECTABLE WARNINGS                             | SQ FT | 136.0          |                    |                        | 136.0             |                      |                      |  |
| 44000165 | HOT-MIX ASPHALT SURFACE REMOVAL, 4"             | SQ YD | 13753.0        | 13753.0            |                        |                   |                      |                      |  |
| 44000600 | SIDEWALK REMOVAL                                | SQ FT | 2413.0         | 2413.0             |                        |                   |                      |                      |  |
| 44201725 | CLASS D PATCHES, TYPE I, 7 INCH                 | SQ YD | 25.0           | 25.0               |                        |                   |                      |                      |  |
| 44201729 | CLASS D PATCHES, TYPE II, 7 INCH                | SQ YD | 25.0           | 25.0               |                        |                   |                      |                      |  |
| 44201733 | CLASS D PATCHES, TYPE III, 7 INCH               | SQ YD | 75.0           | 75.0               |                        |                   |                      |                      |  |
| 44201735 | CLASS D PATCHES, TYPE IV, 7 INCH                | SQ YD | 150.0          | 150.0              |                        |                   |                      |                      |  |
| 60300105 | FRAMES AND GRATES TO BE ADJUSTED                | EACH  | 5.0            | 5.0                |                        |                   |                      |                      |  |
| 60300305 | FRAMES AND LIDS TO BE ADJUSTED                  | EACH  | 29.0           | 29.0               |                        |                   |                      |                      |  |
| 67100100 | MOBILIZATION                                    | L SUM | 1.0            | 1.0                |                        |                   |                      |                      |  |
| 70102620 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701501 | L SUM | 1.0            | 1.0                |                        |                   |                      |                      |  |
| 70102635 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701701 | L SUM | 1.0            | 1.0                |                        |                   |                      |                      |  |

\* SPECIALTY ITEM

|           |              |                 |           |   |   |  |                     |             |                |                    |              |           |
|-----------|--------------|-----------------|-----------|---|---|--|---------------------|-------------|----------------|--------------------|--------------|-----------|
| FILE NAME | USER NAME =  | DESIGNED - Z.N. | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>PRAIRIE STREET<br/>SUMMARY OF QUANTITIES</b> |  |                     | F.A.U. RTE. | SECTION        | COUNTY             | TOTAL SHEETS | SHEET NO. |
|           |              | DRAWN - Z.N.    | REVISED - |   |   |  |                     | 1548        | 23-00118-00-R5 | KANE               | 29           | 4         |
|           | PLOT SCALE = | CHECKED -       | REVISED - |   | SCALE:  |  | SHEET NO. OF SHEETS | STA.        | TO STA.        | CONTRACT NO. 61L26 |              |           |
|           | PLOT DATE =  | DATE -          | REVISED - |   | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |  |                     |             |                |                    |              |           |

| CODE NO.   | ITEM  | UNIT  | TOTAL QUANTITY | CONSTRUCTION CODE        |                              |                         |                            |                            |  |
|------------|---|-------|----------------|--------------------------|------------------------------|-------------------------|----------------------------|----------------------------|--|
|            |   |       |                | STU FUNDS                |                              |                         |                            | CITY FUNDS                 |  |
|            |   |       |                | 75% FED<br>25% CITY      | 75% FED<br>25% CITY          | 75% FED<br>25% CITY     | 75% FED<br>25% CITY        | 100% CITY                  |  |
|            |   |       |                | ROADWAY<br>0005<br>URBAN | ENVIRONMENT<br>0020<br>URBAN | SAFETY<br>0021<br>URBAN | LANDSCAPE<br>0031<br>URBAN | UTILITIES<br>0043<br>URBAN |  |
| 70102640   | TRAFFIC CONTROL AND PROTECTION, STANDARD 701801             | L SUM | 1.0            | 1.0                      |                              |                         |                            |                            |  |
| 70300100   | SHORT TERM PAVEMENT MARKING                                 | FOOT  | 13158.0        |                          |                              | 13158.0                 |                            |                            |  |
| 70300150   | SHORT TERM PAVEMENT MARKING REMOVAL                         | SQ FT | 2193.0         |                          |                              | 2193.0                  |                            |                            |  |
| * 78009000 | MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS    | SQ FT | 275.6          |                          |                              | 275.6                   |                            |                            |  |
| * 78009004 | MODIFIED URETHANE PAVEMENT MARKING - LINE 4"                | FOOT  | 5233.0         |                          |                              | 5233.0                  |                            |                            |  |
| * 78009006 | MODIFIED URETHANE PAVEMENT MARKING - LINE 6"                | FOOT  | 995.0          |                          |                              | 995.0                   |                            |                            |  |
| * 78009012 | MODIFIED URETHANE PAVEMENT MARKING - LINE 12"               | FOOT  | 431.0          |                          |                              | 431.0                   |                            |                            |  |
| * 78009024 | MODIFIED URETHANE PAVEMENT MARKING - LINE 24"               | FOOT  | 117.0          |                          |                              | 117.0                   |                            |                            |  |
| * 78011000 | GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS AND SYMBOLS | SQFT  | 275.6          | 275.6                    |                              |                         |                            |                            |  |
| * 78011025 | GROOVING FOR RECESSED PAVEMENT MARKING 5"                   | FOOT  | 5233.0         | 5233.0                   |                              |                         |                            |                            |  |
| * 78011035 | GROOVING FOR RECESSED PAVEMENT MARKING 7"                   | FOOT  | 995.0          | 995.0                    |                              |                         |                            |                            |  |
| * 78011065 | GROOVING FOR RECESSED PAVEMENT MARKING 13"                  | FOOT  | 431.0          | 431.0                    |                              |                         |                            |                            |  |
| * 78011125 | GROOVING FOR RECESSED PAVEMENT MARKING 25"                  | FOOT  | 117.0          | 117.0                    |                              |                         |                            |                            |  |
| * 85000200 | MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION         | EACH  | 1.0            | 1.0                      |                              |                         |                            |                            |  |

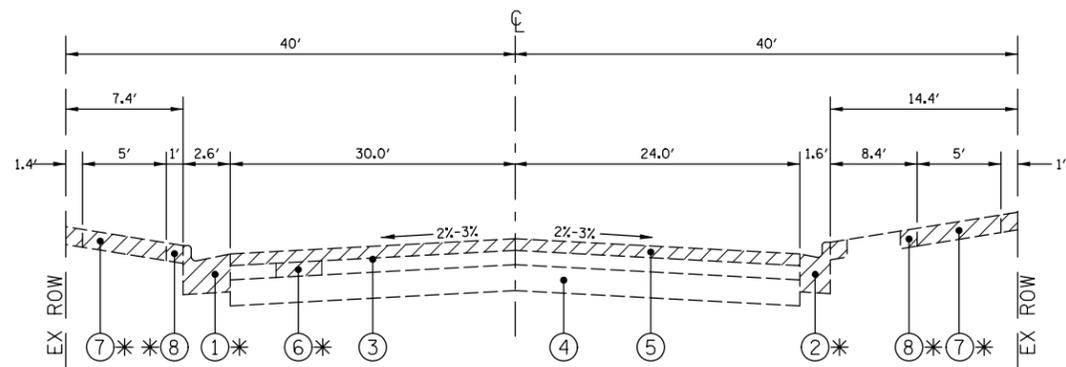
\* SPECIALTY ITEM

|           |             |                 |           |   |   |           |    |               |         |         |                 |                |      |    |   |
|-----------|-------------|-----------------|-----------|---|---|-----------|----|---------------|---------|---------|-----------------|----------------|------|----|---|
| FILE NAME | USER NAME = | DESIGNED - Z.N. | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>PRAIRIE STREET<br/>SUMMARY OF QUANTITIES</b>   |           |    | F.A.U<br>RTE. | SECTION | COUNTY  | TOTAL<br>SHEETS | SHEET<br>NO.   |      |    |   |
|           |             | DRAWN - Z.N.    | REVISED - |   | SCALE:  | SHEET NO. | OF | SHEETS        | STA.    | TO STA. | 1648            | 23-00118-00-RS | KANE | 29 | 5 |
|           |             | CHECKED -       | REVISED - |   | CONTRACT NO. 61L26                                |           |    |               |         |         |                 |                |      |    |   |
|           |             | DATE -          | REVISED - |   | FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT |           |    |               |         |         |                 |                |      |    |   |

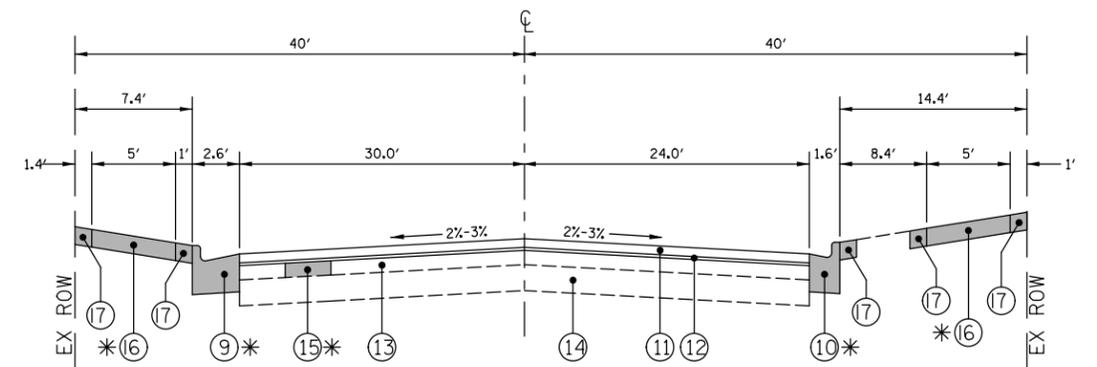
| CODE NO.   | ITEM  | UNIT  | TOTAL QUANTITY | CONSTRUCTION CODE |                  |                  |                  |                |      |
|------------|---|-------|----------------|-------------------|------------------|------------------|------------------|----------------|------|
|            |   |       |                | STU FUNDS         |                  |                  |                  | CITY FUNDS     |      |
|            |   |       |                | 75% FED 25% CITY  | 75% FED 25% CITY | 75% FED 25% CITY | 75% FED 25% CITY | 100% CITY      |      |
|            |   |       |                | ROADWAY 0005      | ENVIRONMENT 0020 | SAFETY 0021      | LANDSCAPE 0031   | UTILITIES 0043 |      |
|            |   |       |                |                   |                  |                  |                  |                |      |
| X0326806   | WASHOUT BASIN   | L SUM | 1.0            | 1.0               |                  |                  |                  |                |      |
| X4400082   | HOT-MIX ASPHALT DRIVEWAY PAVEMENT REMOVAL   | SQ YD | 106.8          | 106.8             |                  |                  |                  |                |      |
| X4400501   | COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT LESS THAN OR EQUAL TO 10 FEET | FOOT  | 104.0          | 104.0             |                  |                  |                  |                |      |
| X4400503   | COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT GREATER THAN 10 FEET          | FOOT  | 457.0          | 457.0             |                  |                  |                  |                |      |
| X6026052   | SANITARY MANHOLES FRAMES AND ADJUSTMENT SEALING                                   | EACH  | 10.0           |                   |                  |                  |                  |                | 10.0 |
| * X8860105 | DETECTOR LOOP REPLACEMENT   | FOOT  | 163.2          | 163.2             |                  |                  |                  |                |      |
| XX003668   | PRECONSTRUCTION VIDEO TAPING  | L SUM | 1.0            |                   |                  |                  |                  |                | 1.0  |
| Z0004510   | HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3"   | SQ YD | 106.8          | 106.8             |                  |                  |                  |                |      |
| Z0030850   | TEMPORARY INFORMATION SIGNING   | SQFT  | 128.5          | 128.5             |                  |                  |                  |                |      |
| Z0073510   | TEMPORARY TRAFFIC SIGNAL TIMING   | EACH  | 1.0            | 1.0               |                  |                  |                  |                |      |

\* SPECIALTY ITEM

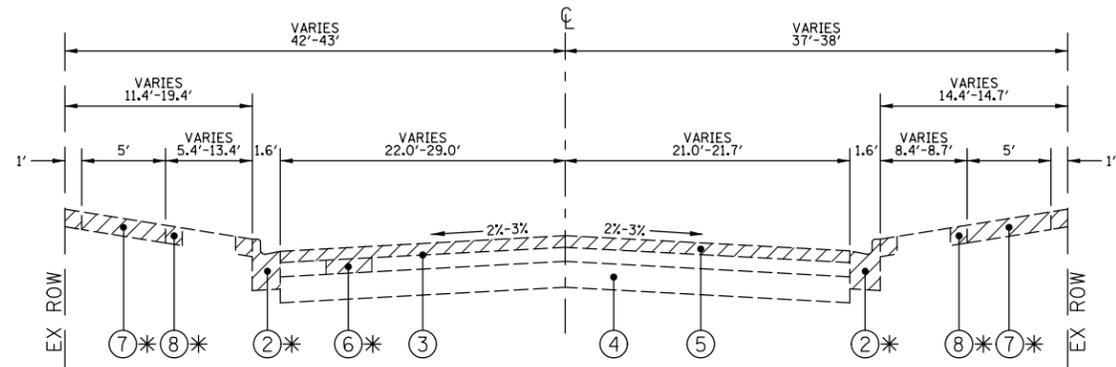
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|-----------|-------------|-----------------|-----------|---|---|--------------------|---------------------|--------------|---|-----------|--|
| FILE NAME | USER NAME = | DESIGNED - Z.N. | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>PRAIRIE STREET<br/>SUMMARY OF QUANTITIES</b> | F.A.U. RTE.        | SECTION             | COUNTY       | TOTAL SHEETS                                    | SHEET NO. |  |
|           |             | DRAWN - Z.N.    | REVISED - |   |   | 1648               | 23-00118-00-R5      | KANE         | 29  | 6         |  |
|           |             | CHECKED -       | REVISED - |   |   | CONTRACT NO. 61L26 |                     |              |   |           |  |
|           |             | DATE -          | REVISED - |   |   | SCALE:             | SHEET NO. OF SHEETS | STA. TO STA. | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |           |  |



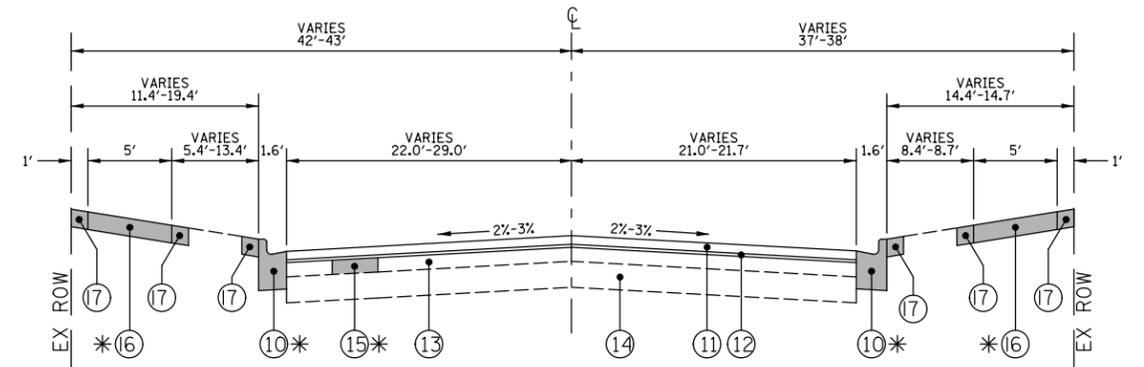
EXISTING TYPICAL CROSS SECTION  
PRAIRIE STREET, STA. 1+89 (RANDALL RD) TO STA. 3+98



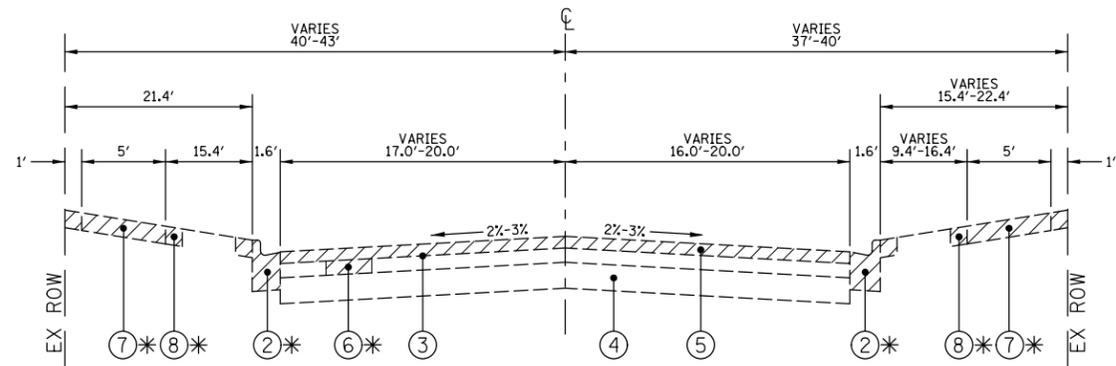
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PRAIRIE STREET, STA. 1+73 (RANDALL RD) TO STA. 3+98



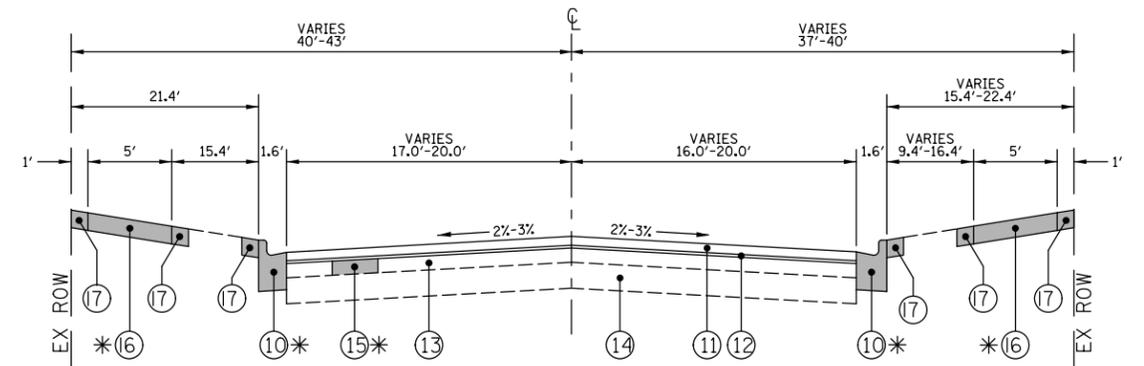
EXISTING TYPICAL CROSS SECTION  
PRAIRIE STREET, STA. 3+98 TO STA. 6+27



PROPOSED TYPICAL CROSS SECTION  
PRAIRIE STREET, STA. 3+98 TO STA. 6+27



EXISTING TYPICAL CROSS SECTION  
PRAIRIE STREET, STA. 6+27 TO STA. 13+00



PROPOSED TYPICAL CROSS SECTION  
PRAIRIE STREET, STA. 6+27 TO STA. 13+00

- ① EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B6.24, REMOVAL VARIES
- ② EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B6.12, REMOVAL VARIES
- ③ EXISTING HOT-MIX ASPHALT SURFACE AND BINDER, 11"
- ④ EXISTING PCC PAVEMENT, 8"
- ⑤ HOT-MIX ASPHALT SURFACE REMOVAL, 4"
- ⑥ REMOVAL FOR PAVEMENT PATCHING
- ⑦ EXISTING PCC SIDEWALK, REMOVAL VARIES
- ⑧ EXISTING PARKWAY, REMOVAL VARIES

LEGEND

REMOVAL ITEM

\* LOCATIONS AS SHOWN ON PLANS OR AS DETERMINED BY FIELD ENGINEER

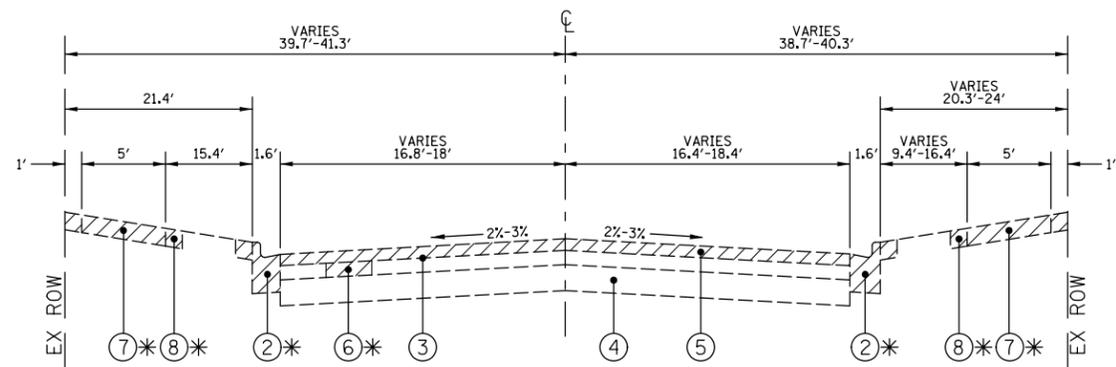
- ⑨ PROP. COMB. CURB & GUTTER TYPE B6.24
  - ⑩ PROP. COMB. CURB & GUTTER TYPE B6.12
  - ⑪ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50, 1-1/2"
  - ⑫ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2-3/4"
  - ⑬ EXISTING HOT-MIX ASPHALT SURFACE & BINDER COURSE, TO REMAIN
  - ⑭ EXISTING PCC PAVEMENT, TO REMAIN
  - ⑮ PROPOSED CLASS D PATCHING, 7"
  - ⑯ PROPOSED 5" PCC SIDEWALK, 6" THRU DRIVEWAYS
  - ⑰ TOPSOIL F&P 4, SEEDING CL 1, TEMP EROS CONTR SEED, EROSION CONTR BLANKET
- NOTE: THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED ON THE HMA BC IL-19.0 N50

|           |              |                   |           |
|-----------|--------------|-------------------|-----------|
| FILE NAME | USER NAME =  | DESIGNED - Z.N.   | REVISED - |
|           |              | DRAWN - Z.N.      | REVISED - |
|           | PLOT SCALE = | CHECKED -         | REVISED - |
|           | PLOT DATE =  | DATE - 11-13-2024 | REVISED - |

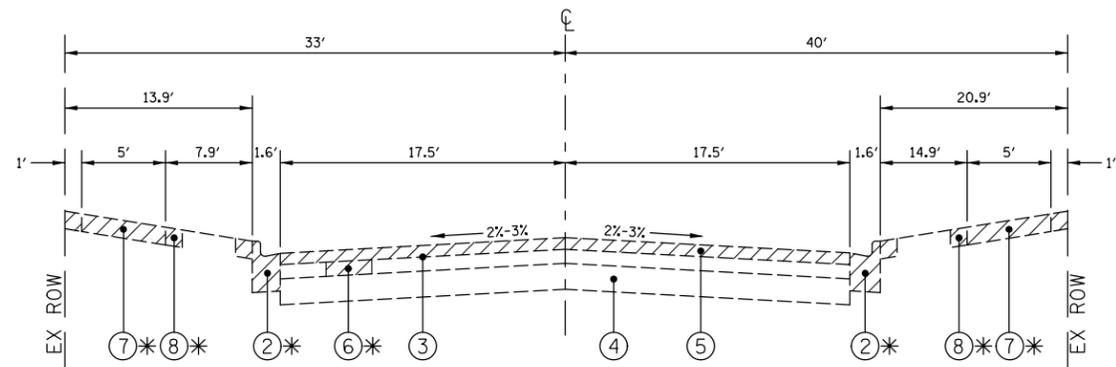
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|   |           |           |              |
|---|-----------|-----------|--------------|
| <b>PRAIRIE STREET RESURFACING<br/>TYPICAL STREET SECTIONS</b> |           |           |              |
| SCALE:  | SHEET NO. | OF SHEETS | STA. TO STA. |

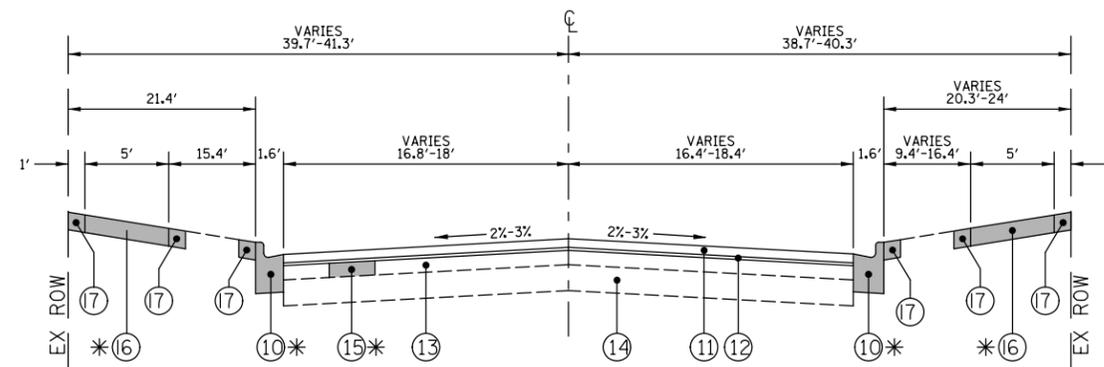
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|---|----------------|--------|--------------|-----------|
| F.A.U. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1648  | 23-00118-00-RS | KANE   | 29           | 7         |
| CONTRACT NO.                                    |                |        | 61L26        |           |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |                |        |              |           |



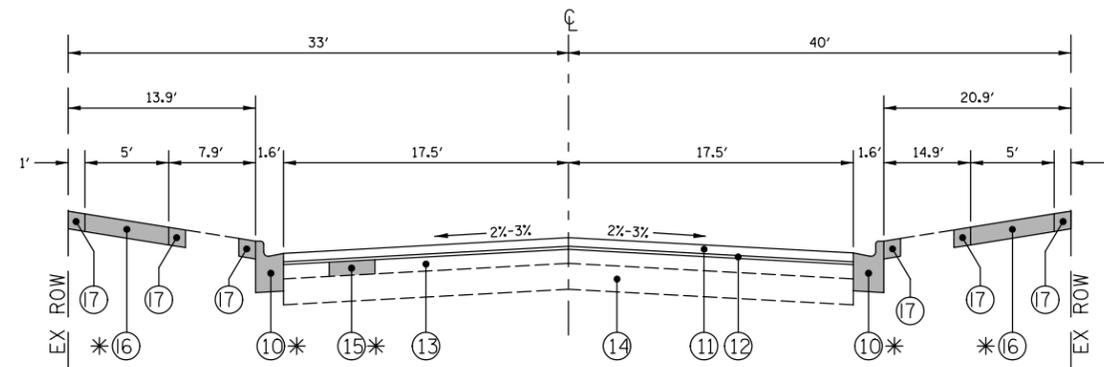
EXISTING TYPICAL CROSS SECTION  
PRAIRIE STREET, STA. 13+00 TO STA. 27+42



EXISTING TYPICAL CROSS SECTION  
PRAIRIE STREET, STA. 27+42 TO STA. 31+18



PROPOSED TYPICAL CROSS SECTION  
PRAIRIE STREET, STA. 13+00 TO STA. 27+42



PROPOSED TYPICAL CROSS SECTION  
PRAIRIE STREET, STA. 27+42 TO STA. 31+18

| HOT-MIX ASPHALT MIXTURE REQUIREMENTS                                    |                  | QMP      |
|---|------------------|----------|
| MIXTURE TYPE  | AIR VOIDS @ Ndes |          |
| PAVEMENT RESURFACING  |                  |          |
| HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50 1-1/2"             | 4% @ 50 Gyr.     | LR1030-2 |
| HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50 2-3/4"                      | 4% @ 50 Gyr.     | LR1030-2 |
| HMA DRIVEWAY PAVEMENT 3"  |                  |          |
| HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50, 3" (IN 2 LIFTS)   | 4% @ 50 Gyr.     | LR1030-2 |
| PATCHING  |                  |          |
| CLASS D PATCHES (HMA BINDER IL-19.0)                                    | 4% @ 50 Gyr.     | LR1030-2 |
| QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA) PER LR1030-2 |                  |          |

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.  
THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.

- ① EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B6.24, REMOVAL VARIES
- ② EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B6.12, REMOVAL VARIES
- ③ EXISTING HOT-MIX ASPHALT SURFACE AND BINDER, 11"
- ④ EXISTING PCC PAVEMENT, 8"
- ⑤ HOT-MIX ASPHALT SURFACE REMOVAL, 4"
- ⑥ REMOVAL FOR PAVEMENT PATCHING
- ⑦ EXISTING PCC SIDEWALK, REMOVAL VARIES
- ⑧ EXISTING PARKWAY, REMOVAL VARIES

LEGEND

REMOVAL ITEM

\* LOCATIONS AS SHOWN ON PLANS OR AS DETERMINED BY FIELD ENGINEER

- ⑨ PROP. COMB. CURB & GUTTER TYPE B6.24
  - ⑩ PROP. COMB. CURB & GUTTER TYPE B6.12
  - ⑪ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50, 1-1/2"
  - ⑫ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2-3/4"
  - ⑬ EXISTING HOT-MIX ASPHALT SURFACE & BINDER COURSE, TO REMAIN
  - ⑭ EXISTING PCC PAVEMENT, TO REMAIN
  - ⑮ PROPOSED CLASS D PATCHING, 7"
  - ⑯ PROPOSED 5" PCC SIDEWALK, 6" THRU DRIVEWAYS
  - ⑰ TOPSOIL F&P 4, SEEDING CL 1, TEMP EROS CONTR SEED, EROSION CONTR BLANKET
- NOTE: THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED ON THE HMA BC IL-19.0 N50

| STRUCTURE ADJUSTMENTS |      |        |        |         |
|-----------------------|------|--------|--------|---------|
| LOCATION              |      | ADJUST | ADJUST | CHIMNEY |
| STA                   | SIDE | F&L    | F&G    | SEAL    |
| 1+99                  | R    | 1      |        | 1       |
| 2+70                  | L    | 1      |        |         |
| 2+78                  | L    | 1      |        |         |
| 3+00                  | L    |        | 1      |         |
| 4+09                  | L    |        | 1      |         |
| 5+08                  | L    | 1      |        |         |
| 6+00                  | L    | 1      |        |         |
| 6+03                  | R    | 1      |        | 1       |
| 6+15                  | L    | 1      |        |         |
| 8+49                  | L    | 1      |        |         |
| 9+30                  | R    | 1      |        | 1       |
| 11+68                 | L    | 1      |        |         |
| 11+91                 | R    | 1      |        | 1       |
| 13+27                 | R    | 1      |        | 1       |
| 13+33                 | L    | 1      |        |         |
| 14+06                 | L    | 1      |        |         |
| 15+35                 | R    | 1      |        | 1       |
| 16+26                 | R    | 1      |        | 1       |
| 16+34                 | L    | 1      |        |         |
| 17+26                 | R    |        | 1      |         |
| 17+37                 | R    | 1      |        | 1       |
| 17+73                 | L    | 1      |        |         |
| 17+80                 | L    | 1      |        |         |
| 19+06                 | L    |        | 1      |         |
| 19+51                 | L    | 1      |        |         |
| 20+26                 | R    |        | 1      | 1       |
| 20+54                 | L    | 1      |        |         |
| 22+42                 | R    | 1      |        | 1       |
| 23+39                 | L    | 1      |        |         |
| 27+36                 | R    | 1      |        |         |
| 29+21                 | L    | 1      |        |         |
| 29+56                 | L    | 1      |        |         |
| 29+87                 | R    | 1      |        |         |
| 30+00                 | L    | 1      |        |         |
| TOTALS=               |      | 29     | 5      | 10      |

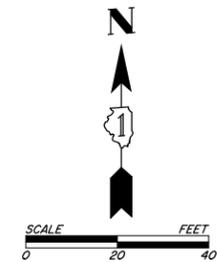
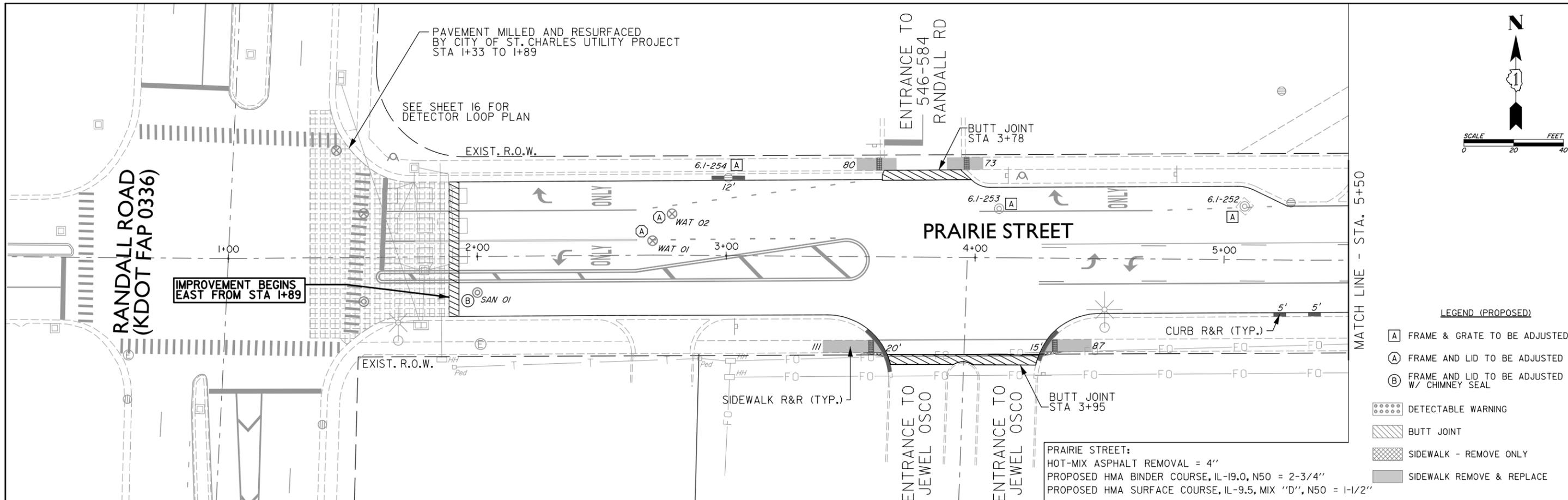
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|-----------|--------------|-----------------|-----------|
| FILE NAME | USER NAME =  | DESIGNED - Z.N. | REVISED - |
|           |              | DRAWN - Z.N.    | REVISED - |
|           | PLOT SCALE = | CHECKED -       | REVISED - |
|           | PLOT DATE =  | DATE -          | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

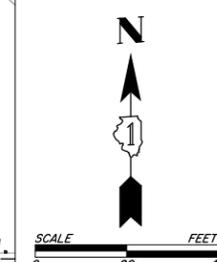
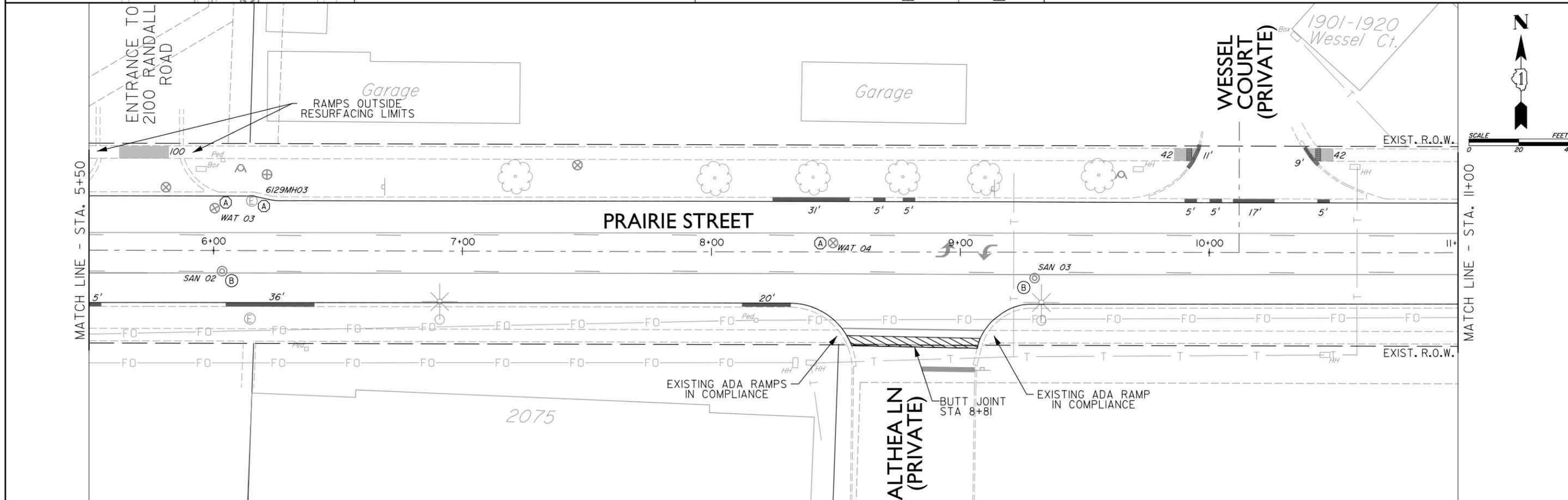
**PRAIRIE STREET  
SCHEDULE OF QUANTITIES**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

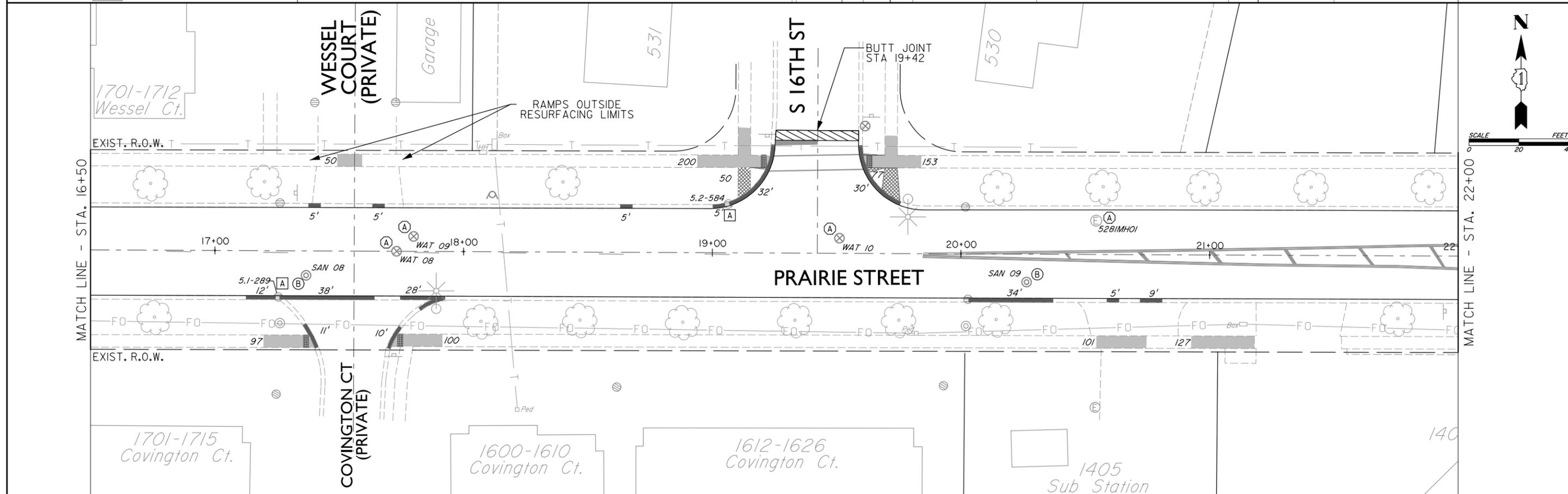
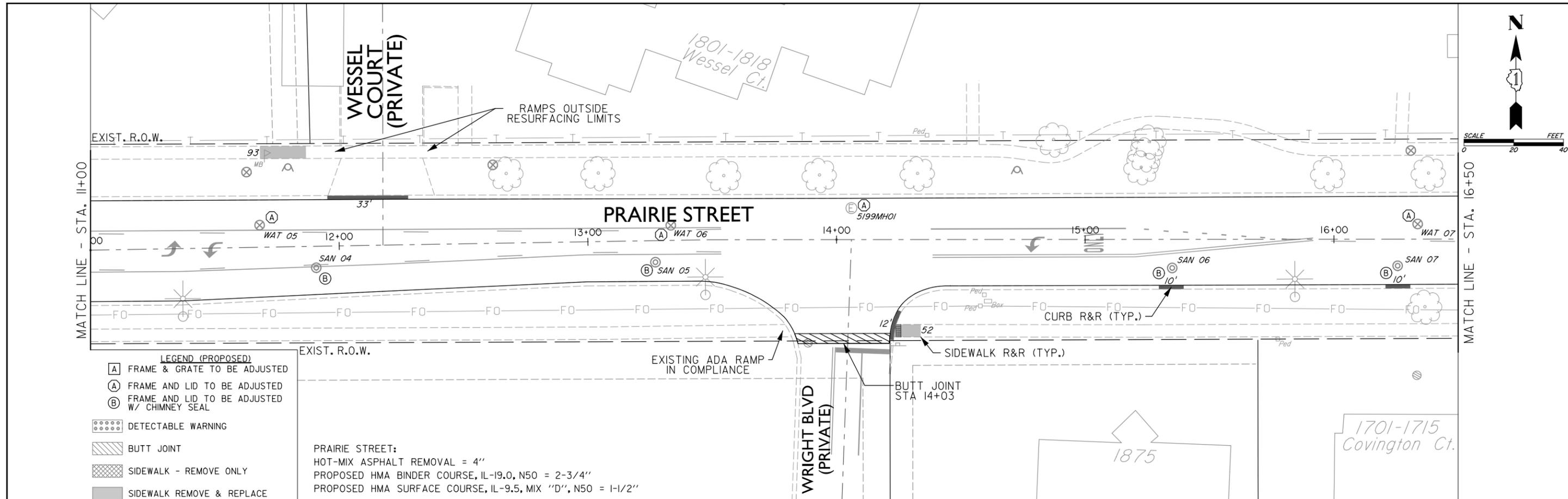
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|---|----------------|--------|--------------------|--------------|
| F.A.U<br>RTE.                                   | SECTION        | COUNTY | TOTAL<br>SHEETS    | SHEET<br>NO. |
| 1648  | 23-00118-00-RS | KANE   | 29                 | 9            |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |                |        | CONTRACT NO. 61L26 |              |



- LEGEND (PROPOSED)**
- [A] FRAME & GRATE TO BE ADJUSTED
  - [A] FRAME AND LID TO BE ADJUSTED
  - [B] FRAME AND LID TO BE ADJUSTED W/ CHIMNEY SEAL
  - [Dotted Box] DETECTABLE WARNING
  - [Hatched Box] BUTT JOINT
  - [Cross-hatched Box] SIDEWALK - REMOVE ONLY
  - [Solid Grey Box] SIDEWALK REMOVE & REPLACE



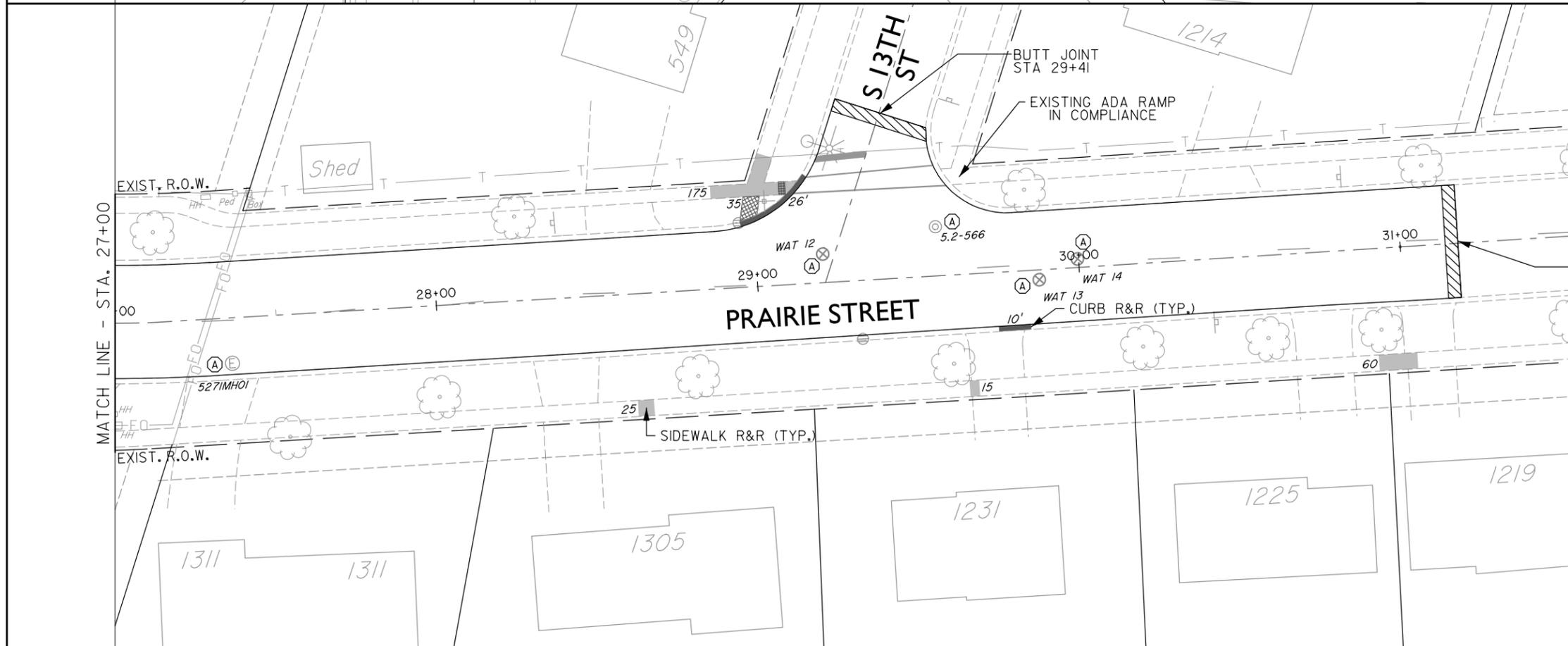
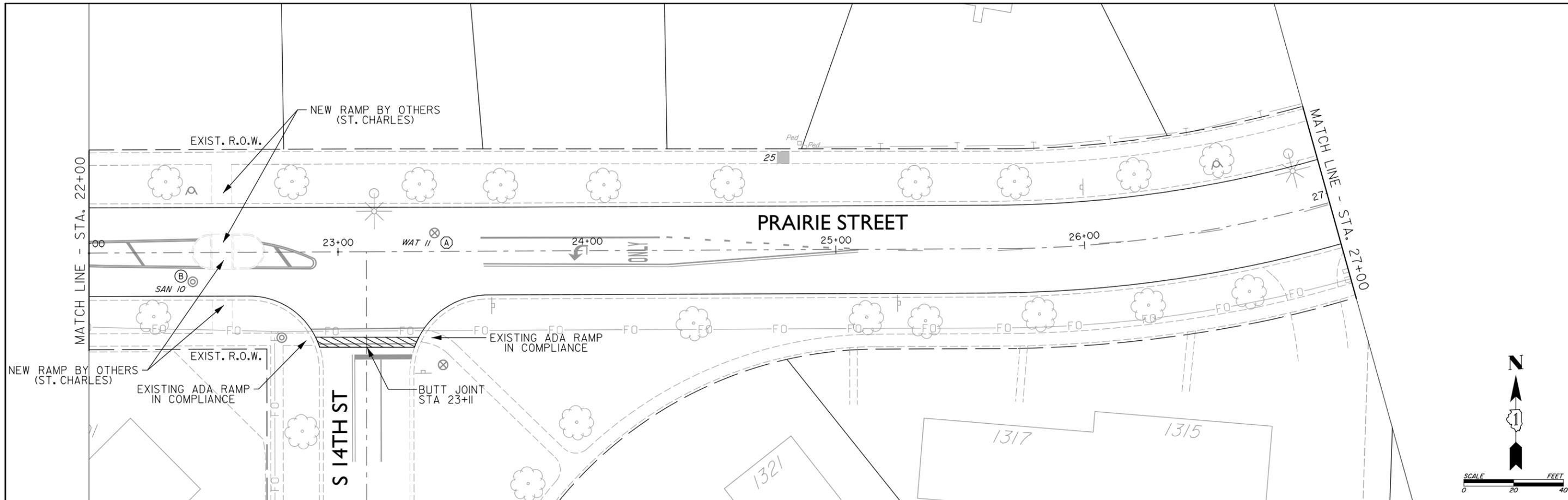
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|-----------|--------------|-------------------|-----------|---|--|-------------------------|-------------------------|-----------------------|-------------|------------------|--------------|
| FILE NAME | USER NAME =  | DESIGNED - Z.N.   | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>PRairie STREET<br/>ROADWAY AND UTILITY PLAN</b> |                         | F.A.U. RTE. 1648        | SECTION 23-0018-00-R5 | COUNTY KANE | TOTAL SHEETS 29  | SHEET NO. 10 |
|           | PLOT SCALE = | CHECKED -         | REVISED - |   | SCALE: 1" = 40'                                    | SHEET NO. 1 OF 3 SHEETS | STA. 1+89 TO STA. 11+00 | CONTRACT NO. 61L26    |             | FED. AID PROJECT |              |
|           | PLOT DATE =  | DATE - 11-13-2024 | REVISED - |   | FED. ROAD DIST. NO. 1                              | ILLINOIS                |                         |                       |             |                  |              |



- LEGEND (PROPOSED)**
- [A] FRAME & GRATE TO BE ADJUSTED
  - [A] FRAME AND LID TO BE ADJUSTED
  - [B] FRAME AND LID TO BE ADJUSTED W/ CHIMNEY SEAL
  - [.....] DETECTABLE WARNING
  - [//] BUTT JOINT
  - [X] SIDEWALK - REMOVE ONLY
  - [■] SIDEWALK REMOVE & REPLACE

PRAIRIE STREET:  
 HOT-MIX ASPHALT REMOVAL = 4"  
 PROPOSED HMA BINDER COURSE, IL-19.0, N50 = 2-3/4"  
 PROPOSED HMA SURFACE COURSE, IL-9.5, MIX "D", N50 = 1-1/2"

|           |              |                   |           |   |  |                         |                          |                       |             |                    |                  |
|-----------|--------------|-------------------|-----------|---|--|-------------------------|--------------------------|-----------------------|-------------|--------------------|------------------|
| FILE NAME | USER NAME =  | DESIGNED - Z.N.   | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>PRAIRIE STREET<br/>ROADWAY AND UTILITY PLAN</b> |                         | F.A.U. RTE. 1648         | SECTION 23-0018-00-R5 | COUNTY KANE | TOTAL SHEETS 27    | SHEET NO. II     |
|           | PLOT SCALE = | DRAWN - Z.N.      | REVISED - |   | SCALE: 1" = 40'                                    | SHEET NO. 2 OF 3 SHEETS | STA. 11+00 TO STA. 22+00 | FED. ROAD DIST. NO. I | ILLINOIS    | CONTRACT NO. 61L26 | FED. AID PROJECT |
|           | PLOT DATE =  | CHECKED -         | REVISED - |   |  |                         |                          |                       |             |                    |                  |
|           |              | DATE - 11-13-2024 | REVISED - |   |  |                         |                          |                       |             |                    |                  |



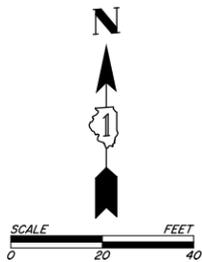
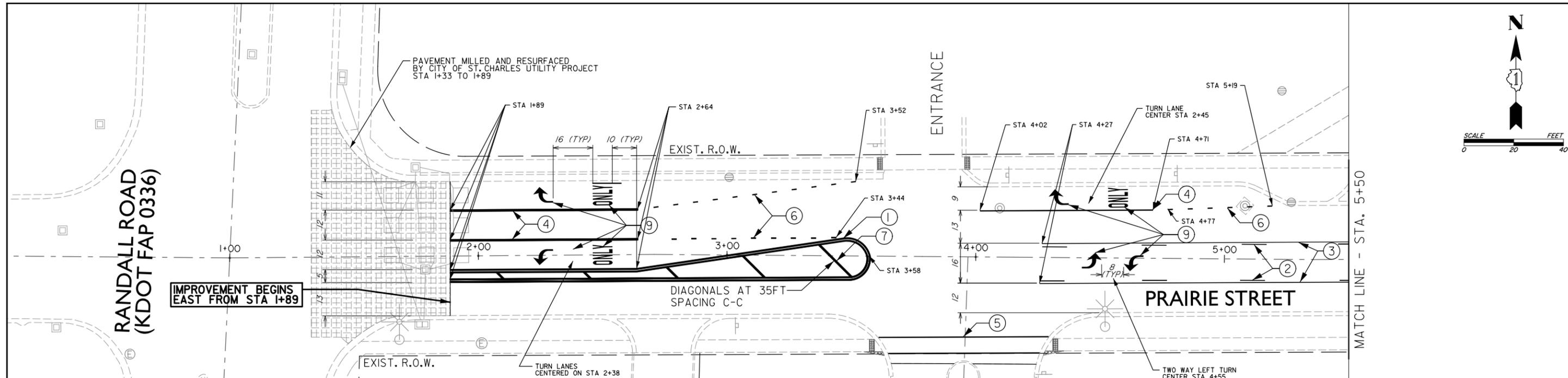
**LEGEND (PROPOSED)**

- [A] FRAME & GRATE TO BE ADJUSTED
- [A] FRAME AND LID TO BE ADJUSTED
- [B] FRAME AND LID TO BE ADJUSTED W/ CHIMNEY SEAL
- [••••] DETECTABLE WARNING
- [//] BUTT JOINT
- [X] SIDEWALK - REMOVE ONLY
- [■] SIDEWALK REMOVE & REPLACE

**END IMPROVEMENT STA 31+18**  
**UTILITY PROJECT BY CITY OF ST. CHARLES**

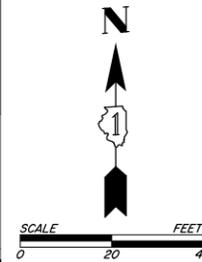
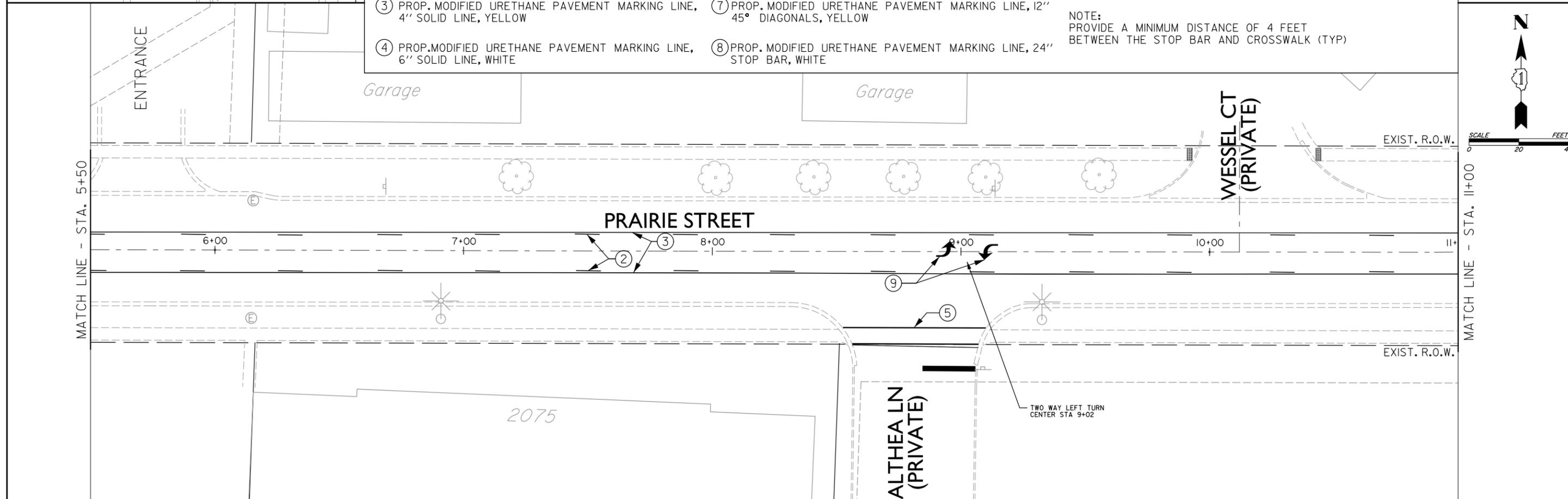
PRAIRIE STREET:  
 HOT-MIX ASPHALT REMOVAL = 4"  
 PROPOSED HMA BINDER COURSE, IL-19.0, N50 = 2-3/4"  
 PROPOSED HMA SURFACE COURSE, IL-9.5, MIX "D", N50 = 1-1/2"

|           |              |                   |           |   |  |                       |                         |          |                          |                  |                   |
|-----------|--------------|-------------------|-----------|---|--|-----------------------|-------------------------|----------|--------------------------|------------------|-------------------|
| FILE NAME | USER NAME =  | DESIGNED - Z.N.   | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>PRAIRIE STREET<br/>ROADWAY AND UTILITY PLAN</b> |                       | F.A.U. RTE.             | SECTION  | COUNTY                   | TOTAL SHEETS     | SHEET NO.         |
|           | DRAWN - Z.N. | CHECKED -         | REVISED - |   | 1648   | 23-0018-00-R5         | KANE                    | 29       | 12                       |                  |                   |
|           | PLOT SCALE = | DATE - 11-13-2024 | REVISED - |   | SCALE: 1" = 40'                                    |                       | SHEET NO. 3 OF 3 SHEETS |          | STA. 22+00 TO STA. 31+18 |                  | CONTRACT NO. 6L26 |
|           | PLOT DATE =  |                   |           |   |  | FED. ROAD DIST. NO. 1 |                         | ILLINOIS |                          | FED. AID PROJECT |                   |

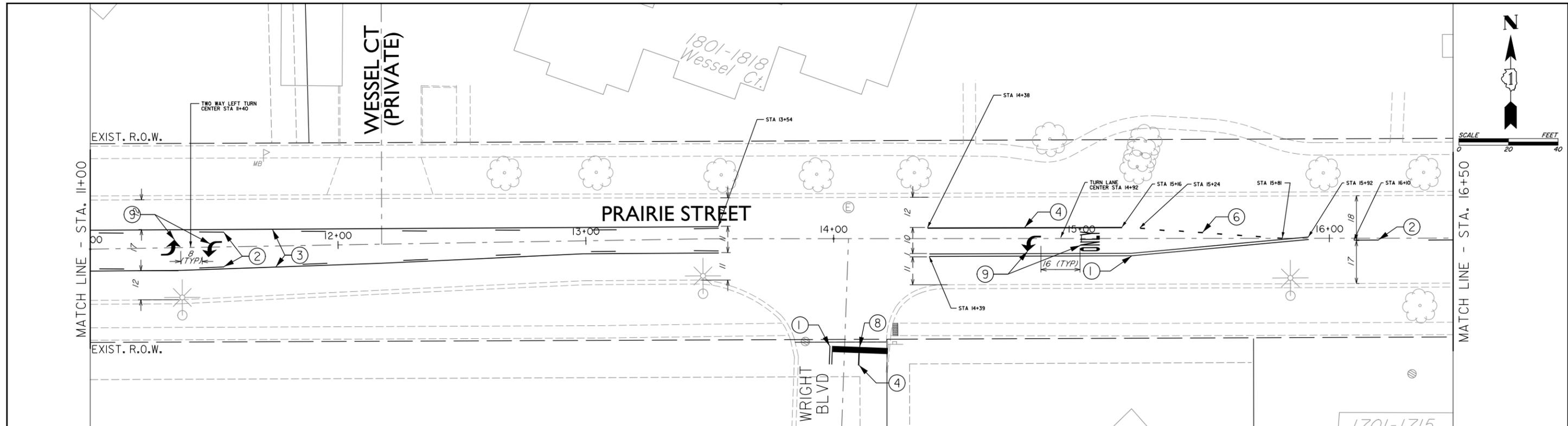


| PAVEMENT MARKING LEGEND (PROPOSED)   |   |   |
|--|---|---|
| ① PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 4" DOUBLE LINE @ 11" C-C, YELLOW                | ⑤ PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 6" CROSSWALK LINES SPACED 6' APART, WHITE    | ⑨ PROP. MODIFIED URETHANE PAVEMENT MARKING LETTERS & SYMBOLS, WHITE                             |
| ② PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 4" SKIP DASH LINE @ 30' SKIP & 10' DASH, YELLOW | ⑥ PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 6" SKIP DASH LINE @ 6' SKIP & 2' DASH, WHITE | ⑩ PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 12" 6' CROSSWALK LINES SPACED AT 3' C-C, WHITE |
| ③ PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 4" SOLID LINE, YELLOW                           | ⑦ PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 12" 45° DIAGONALS, YELLOW                    |   |
| ④ PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 6" SOLID LINE, WHITE                            | ⑧ PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 24" STOP BAR, WHITE                          |   |

NOTE:  
PROVIDE A MINIMUM DISTANCE OF 4 FEET BETWEEN THE STOP BAR AND CROSSWALK (TYP)

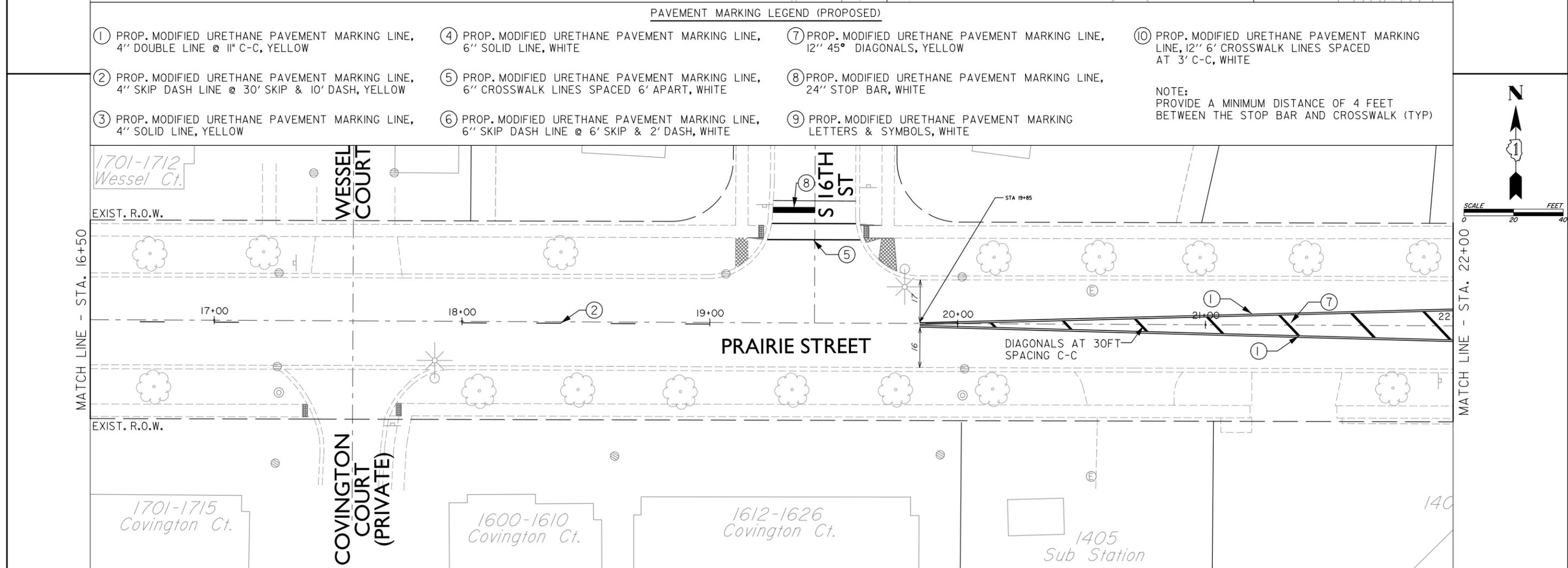


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| FILE NAME | USER NAME =  | DESIGNED - Z.N.   | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>PRAIRIE STREET<br/>PAVEMENT MARKING PLAN</b> |                         | F.A.U. RTE. 1648        | SECTION 23-0018-00-R5 | COUNTY KANE | TOTAL SHEETS 29    | SHEET NO. 13     |
|           | PLOT SCALE = | CHECKED -         | REVISED - |   | SCALE: 1" = 40'                                 | SHEET NO. 1 OF 3 SHEETS | STA. 1+89 TO STA. 11+00 | FED. ROAD DIST. NO. 1 | ILLINOIS    | CONTRACT NO. 61L26 | FED. AID PROJECT |
|           | PLOT DATE =  | DATE - 11-13-2024 | REVISED - |   |   |                         |                         |                       |             |                    |                  |

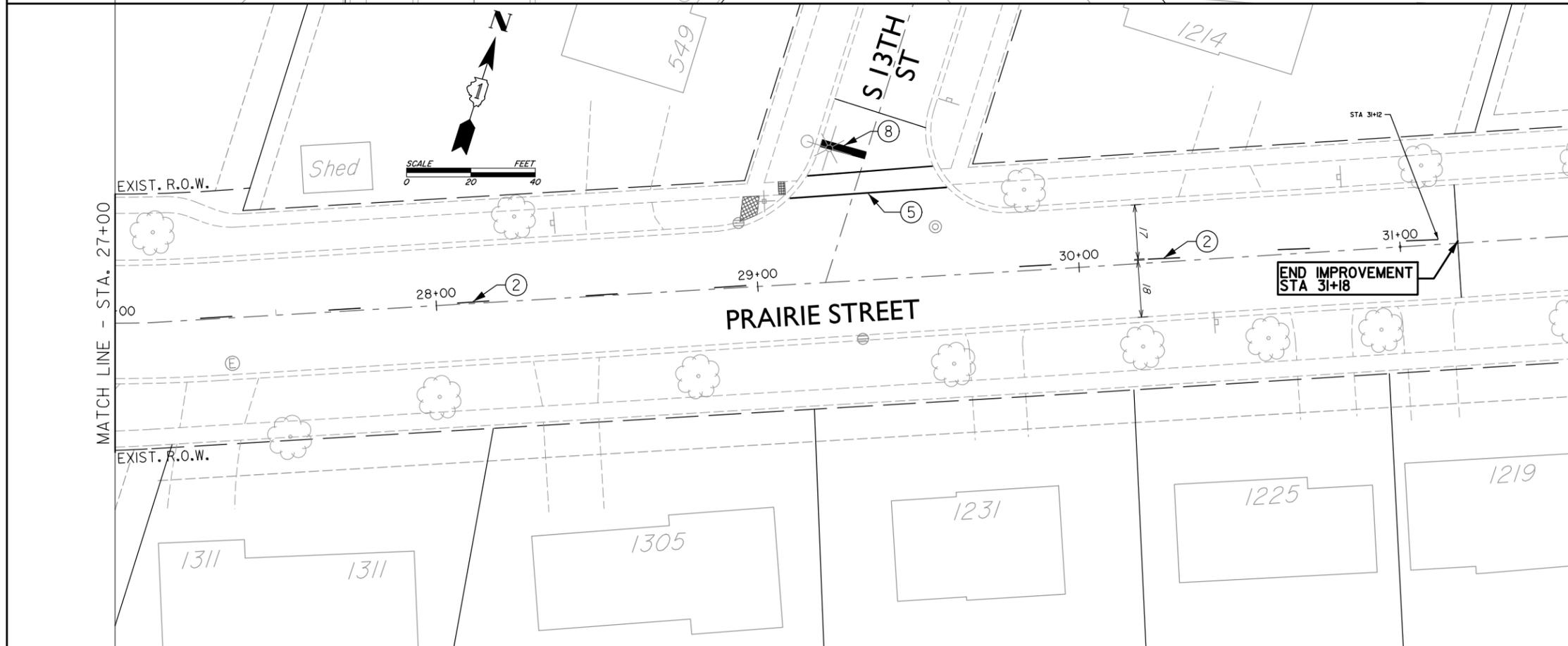
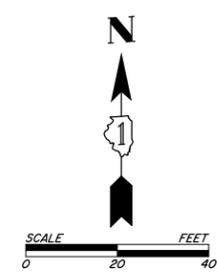
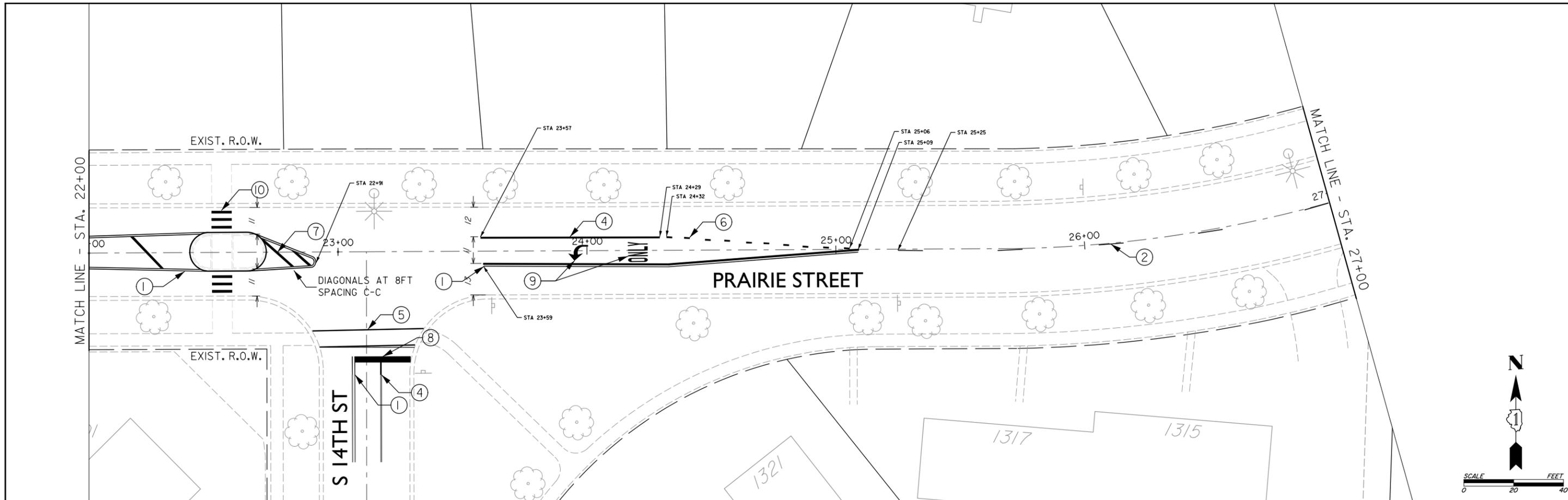


PAVEMENT MARKING LEGEND (PROPOSED)

- |  |   |  |   |
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| ① PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 4" DOUBLE LINE @ 11" C-C, YELLOW                | ④ PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 6" SOLID LINE, WHITE                         | ⑦ PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 12" 45° DIAGONALS, YELLOW   | ⑩ PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 12" 6' CROSSWALK LINES SPACED AT 3' C-C, WHITE |
| ② PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 4" SKIP DASH LINE @ 30' SKIP & 10' DASH, YELLOW | ⑤ PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 6" CROSSWALK LINES SPACED 6' APART, WHITE    | ⑧ PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 24" STOP BAR, WHITE         | NOTE:<br>PROVIDE A MINIMUM DISTANCE OF 4 FEET BETWEEN THE STOP BAR AND CROSSWALK (TYP)          |
| ③ PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 4" SOLID LINE, YELLOW                           | ⑥ PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 6" SKIP DASH LINE @ 6' SKIP & 2' DASH, WHITE | ⑨ PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 6" LETTERS & SYMBOLS, WHITE |   |



|           |              |                   |           |   |   |                         |                          |                       |             |                    |                  |
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| FILE NAME | USER NAME =  | DESIGNED - Z.N.   | REVISED - | STATE OF ILLINOIS<br>DEPARTMENT OF TRANSPORTATION | PRAIRIE STREET<br>PAVEMENT MARKING PLAN |                         | F.A.U. RTE. 1648         | SECTION 23-0018-00-R5 | COUNTY KANE | TOTAL SHEETS 29    | SHEET NO. 14     |
|           | PLOT SCALE = | CHECKED -         | REVISED - |   | SCALE: 1" = 40'                         | SHEET NO. 2 OF 3 SHEETS | STA. 11+00 TO STA. 22+00 | FED. ROAD DIST. NO. 1 | ILLINOIS    | CONTRACT NO. 61L26 | FED. AID PROJECT |
|           | PLOT DATE =  | DATE - 11-13-2024 | REVISED - |   |   |                         |                          |                       |             |                    |                  |



- PAVEMENT MARKING LEGEND (PROPOSED)**
- ① PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 4" DOUBLE LINE @ 11" C-C, YELLOW
  - ② PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 4" SKIP DASH LINE @ 30' SKIP & 10' DASH, YELLOW
  - ③ PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 4" SOLID LINE, YELLOW
  - ④ PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 6" SOLID LINE, WHITE
  - ⑤ PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 6" CROSSWALK LINES SPACED 6' APART, WHITE
  - ⑥ PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 6" SKIP DASH LINE @ 6' SKIP & 2' DASH, WHITE
  - ⑦ PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 12" 45° DIAGONALS, YELLOW
  - ⑧ PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 24" STOP BAR, WHITE
  - ⑨ PROP. MODIFIED URETHANE PAVEMENT MARKING LETTERS & SYMBOLS, WHITE
  - ⑩ PROP. MODIFIED URETHANE PAVEMENT MARKING LINE, 12" 6' CROSSWALK LINES SPACED AT 3' C-C, WHITE
- NOTE:  
PROVIDE A MINIMUM DISTANCE OF 4 FEET BETWEEN THE STOP BAR AND CROSSWALK (TYP)

|           |              |                   |           |   |   |                         |                          |                          |                |                    |                 |
|-----------|--------------|-------------------|-----------|---|---|-------------------------|--------------------------|--------------------------|----------------|--------------------|-----------------|
| FILE NAME | USER NAME =  | DESIGNED - Z.N.   | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>PRAIRIE STREET<br/>PAVEMENT MARKING PLAN</b> |                         | F.A.U. RTE.<br>1648      | SECTION<br>23-0018-00-R5 | COUNTY<br>KANE | TOTAL SHEETS<br>29 | SHEET NO.<br>15 |
|           | PLOT SCALE = | CHECKED -         | REVISED - |   | SCALE: 1" = 40'                                 | SHEET NO. 3 OF 3 SHEETS | STA. 22+00 TO STA. 31+18 | CONTRACT NO. 6L26        |                | FED. AID PROJECT   |                 |
|           | PLOT DATE =  | DATE - 11-13-2024 | REVISED - |   |   |                         |                          |                          |                |                    |                 |

**TRAFFIC SIGNAL NOTES**

1. THE TRAFFIC SIGNAL WORK SHALL BE INSTALLED BY AN ELECTRICAL CONTRACTOR PREQUALIFIED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION
2. THE CONTRACTOR SHALL INFORM THE KDOT ENGINEER AT (630) 584-1170 PRIOR TO THE START OF ANY WORK ON THE CONTRACT. A MINIMUM OF FIVE (5) WORKING DAYS ADVANCED NOTICE IS REQUIRED.
3. VEHICLE LOOP DETECTOR SHALL BE COMPATIBLE WITH THE CONTROL EQUIPMENT. ALL TRAFFIC SIGNAL EQUIPMENT SHALL BE OF THE TYPE AND BRAND THAT IS ACCEPTABLE TO THE KANE COUNTY DIVISION OF TRANSPORTATION. ONE (1) COPY OF ALL APPROVED CATALOG CUTS TO BE FURNISHED TO THE COUNTY PRIOR TO INSTALLATION.
4. THE CORNERS OF ALL NEW DETECTOR LOOPS SHALL BE CORE DRILLED INCLUDING PAVEMENT JOINTS AND CRACK.
5. IT IS THE CONTRACTOR'S RESPONSIBILITY TO LOCATE EXISTING TRAFFIC SIGNAL CABLES AND CONDUITS.
6. CARE IS TO BE TAKEN AS NOT TO DAMAGE ANY OF THE EXISTING TRAFFIC SIGNAL CONDUITS AND EQUIPMENT. IF ANY OF THE TRAFFIC SIGNAL CONDUIT AND/OR EQUIPMENT IS DAMAGED, THE CONTRACTOR SHALL REPAIR AND/OR REPLACE THE CONDUIT AND/OR THE EQUIPMENT AT NO COST TO THE COUNTY.
7. FOLLOW IDOT DISTRICT I SPECIAL PROVISIONS, BUT CONTACT KDOT INSTEAD.

| CODE     | ITEM  | UNIT | TOTAL QUANTITY |
|----------|---|------|----------------|
| 85000200 | MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION | EACH | 1.0            |
| X8860105 | DETECTOR LOOP REPLACEMENT                           | FOOT | 163.2          |
| Z0073510 | TEMPORARY TRAFFIC SIGNAL TIMING                     | EACH | 1.0            |

EX HANDHOLE (TYP)

PAVEMENT MILLED AND RESURFACED BY CITY OF ST. CHARLES UTILITY PROJECT STA 1+33 TO STA 1+89

**RANDALL ROAD  
(KDOT FAP 0336)**

0+00

1+00

LIMIT OF RESURFACING STA 1+89

**PRAIRIE STREET**

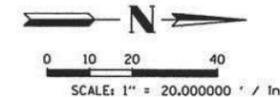
DETECTOR LOOPS TO BE REPLACED 6'X6'



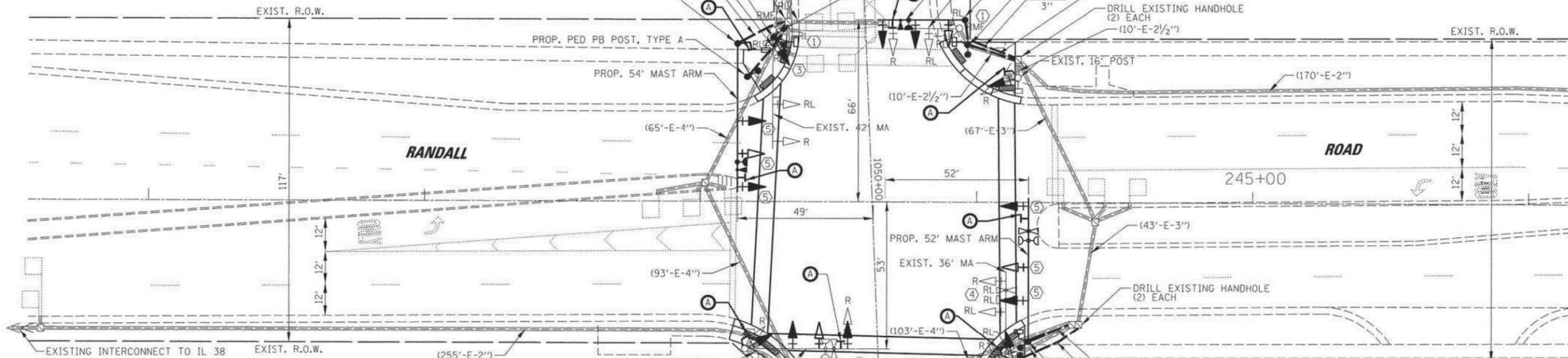
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| FILE NAME | USER NAME =  | DESIGNED - Z.N.   | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>PRAIRIE STREET AT RANDALL ROAD<br/>DETECTOR LOOP PLAN</b> |                         | F.A.U. RTE.<br>1648 | SECTION<br>23-0018-00-R5 | COUNTY<br>KANE | TOTAL SHEETS<br>29   | SHEET NO.<br>16  |
|           | PLOT SCALE = | DRAWN - Z.N.      | REVISED - |   | SCALE: 1" = 20'  | SHEET NO. 1 OF 1 SHEETS | STA. TO STA.        | FED. ROAD DIST. NO. 1    | ILLINOIS       | CONTRACT NO.<br>6126 | FED. AID PROJECT |
|           | PLOT DATE =  | DATE - 11-13-2024 | REVISED - |   |  |                         |                     |                          |                |                      |                  |

**GENERAL NOTES:**

- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARMS LENGTHS.
- THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES. THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811. IN THE CITY OF CHICAGO CONTACT DIGGER AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION REQUIRED).
- THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, LOCAL GOVERNMENT AGENCIES AND IDOT.



**RESTORATION OF WORK AREA.**  
 RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE COST FOR THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



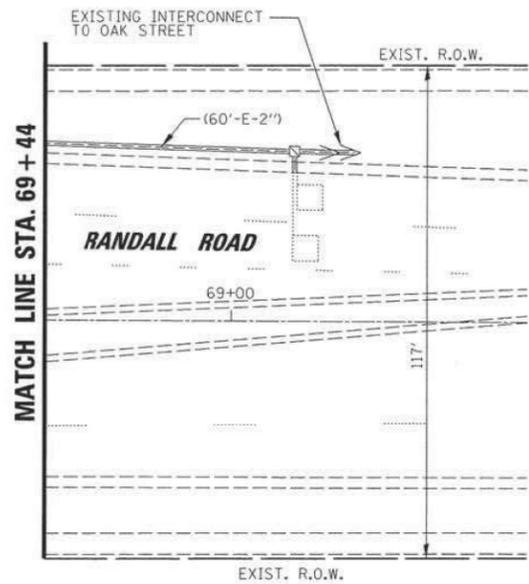
**CONSTRUCTION NOTES:**

- PEDESTRIAN SIGNAL HEADS AND PUSHBUTTON SHALL BE RELOCATED AS SHOWN ON THE PLANS.
- THE CONTROLLER SHALL BE MODIFIED AS NECESSARY TO IMPLEMENT THE PROPOSED SEQUENCE AS SHOWN ON THE PLAN, INCLUDING THE FLASHING YELLOW ARROW PHASE. ALL NECESSARY LABOR, MATERIALS, LOAD SWITCHES, SOFTWARE, MMUs, CABINET WIRING, HARDWARE, AND CONTROLLER PROGRAMMING SHALL BE INCLUDED IN THE COST OF MODIFY EXISTING CONTROLLER CABINET.
- EACH PEDESTRIAN PUSH-BUTTON SHALL CALL ONLY ONE PHASE, ALL WIRING COSTS NECESSARY TO ELIMINATE DUAL CALL PUSHBUTTON OPERATION SHALL BE INCIDENTAL TO THE PAY ITEM PEDESTRIAN PUSH-BUTTON.
- THE CONTRACTOR SHALL RELOCATE THE EXISTING LIGHT DETECTOR EQUIPMENT AND IMPLEMENT THE PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE AS SPECIFIED ON THE PLANS AND PROJECT SPECIAL PROVISIONS. ALL COSTS PERTAINING TO ACQUIRING NECESSARY EQUIPMENT, FIELD WIRING, OR PERIPHERALS, LABOR, AND MATERIALS TO PERFORM THIS WORK SHALL BE INCLUDED IN THE COST OF RELOCATE LIGHT DETECTOR.
- TRAFFIC SIGNAL BACKPLATE, RETROREFLECTIVE SHALL BE INSTALLED ON NORTHBOUND AND SOUTHBOUND MAST ARM MOUNTED TRAFFIC SIGNAL HEADS ONLY. ALL OTHER TRAFFIC SIGNAL HEADS SHALL BE FITTED WITH TRAFFIC SIGNAL BACKPLATE. REMOVAL OF EXISTING BACKPLATES SHALL BE INCLUDED IN THE COST OF TRAFFIC SIGNAL BACKPLATE AND TRAFFIC SIGNAL BACKPLATE, RETROREFLECTIVE.
- THE CONTRACTOR SHALL REMOVE ALL EXISTING SIGNAGE FROM THE EXISTING MAST ARM ASSEMBLIES OR TRAFFIC SIGNAL POSTS THAT WILL BE REMOVED AS PART OF THE PROPOSED PROJECT SCOPE. THE EXISTING SIGN PANELS SHALL BE REINSTALLED ON THE PROPOSED MAST ARM ASSEMBLIES USING NEW MOUNTING HARDWARE ACCORDING TO THE DETAILS SHOWN IN THE PLANS OR AS DIRECTED BY THE ENGINEER. ANY NEW SIGN SUPPORT BRACKETS OR REDRILLING OF EXISTING BRACKETS SHALL BE PROVIDED WHEN NECESSARY. ALL COSTS RELATED TO RELOCATING THE EXISTING SIGNAGE SHALL BE INCLUDED IN THE UNIT COST OF THE PROPOSED MAST ARM AND POLE ASSEMBLY OR TRAFFIC SIGNAL POST.

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 3 EACH SIGNAL HEAD, 1 FACE, 3 SECTION
- 7 EACH SIGNAL HEAD, 1 FACE, 5 SECTION
- 1 EACH SIGNAL HEAD, 2 FACE, 1-3 SECTION, 1-5 SECTION
- 1 EACH SIGNAL HEAD, 2 FACE, 2-5 SECTION
- 1 EACH TRAFFIC SIGNAL POST
- 3 EACH STEEL MAST ARM AND POLE

FOR REFERENCE ONLY



|  |                                    |                 |           |   |  |  |  |   |  |  |  |   |                |         |              |              |
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| FILE NAME =  | USER NAME = ghickey                | DESIGNED - EAJ  | REVISED - | STATE OF ILLINOIS<br>DEPARTMENT OF TRANSPORTATION |  |  |  | TRAFFIC SIGNAL MODIFICATION PLAN<br>RANDALL ROAD AND PRAIRIE STREET   |  |  |  | F.A.U. RTE.                                     | SECTION        | COUNTY  | TOTAL SHEETS | SHEET NO.    |
| K:\Projects\Kane County\2012 HSIP\CADD\Drawings\Sheets\Traffic Signal Plans\38 MOD_PRAIRIE.dgn | DRAWN - FPB                        | CHECKED - WCE   | REVISED - |   |  |  |  | SCALE: 1" = 20'   |  |  |  | SHEET NO.                                       | OF SHEETS      | STA. -- | TO STA. --   | CONTRACT NO. |
|  | PLOT SCALE = 1" = 20.000000' / in. | DATE - FEB 2014 | REVISED - | STATE OF ILLINOIS<br>DEPARTMENT OF TRANSPORTATION |  |  |  | PRAIRIE STREET<br>KDOT TRAFFIC SIGNAL DRAWINGS - FOR INFORMATION ONLY |  |  |  | 1648  | 23-00118-00-RS | KANE    | 29           | 17           |
|  | PLOT DATE = 5/12/2014              |                 |           |   |  |  |  | SCALE: SHEET NO. OF SHEETS STA. TO STA.                               |  |  |  | CONTRACT NO. 61L26                              |                |         |              |              |
|  |                                    |                 |           |   |  |  |  |   |  |  |  | ILLINOIS FED. AID PROJECT                       |                |         |              |              |
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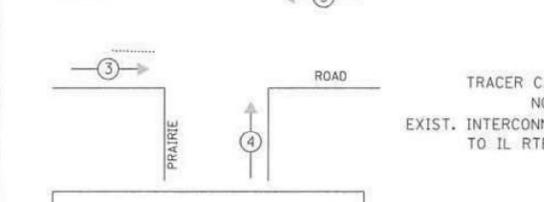
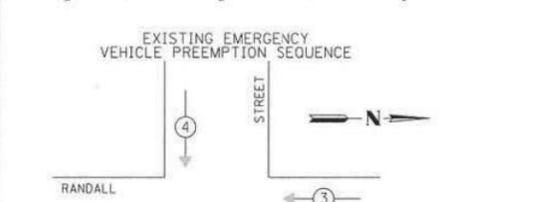
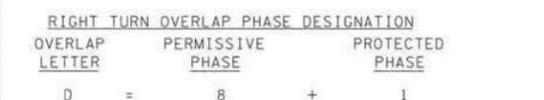
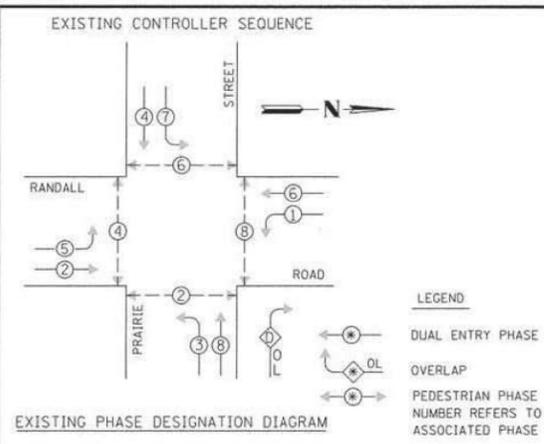
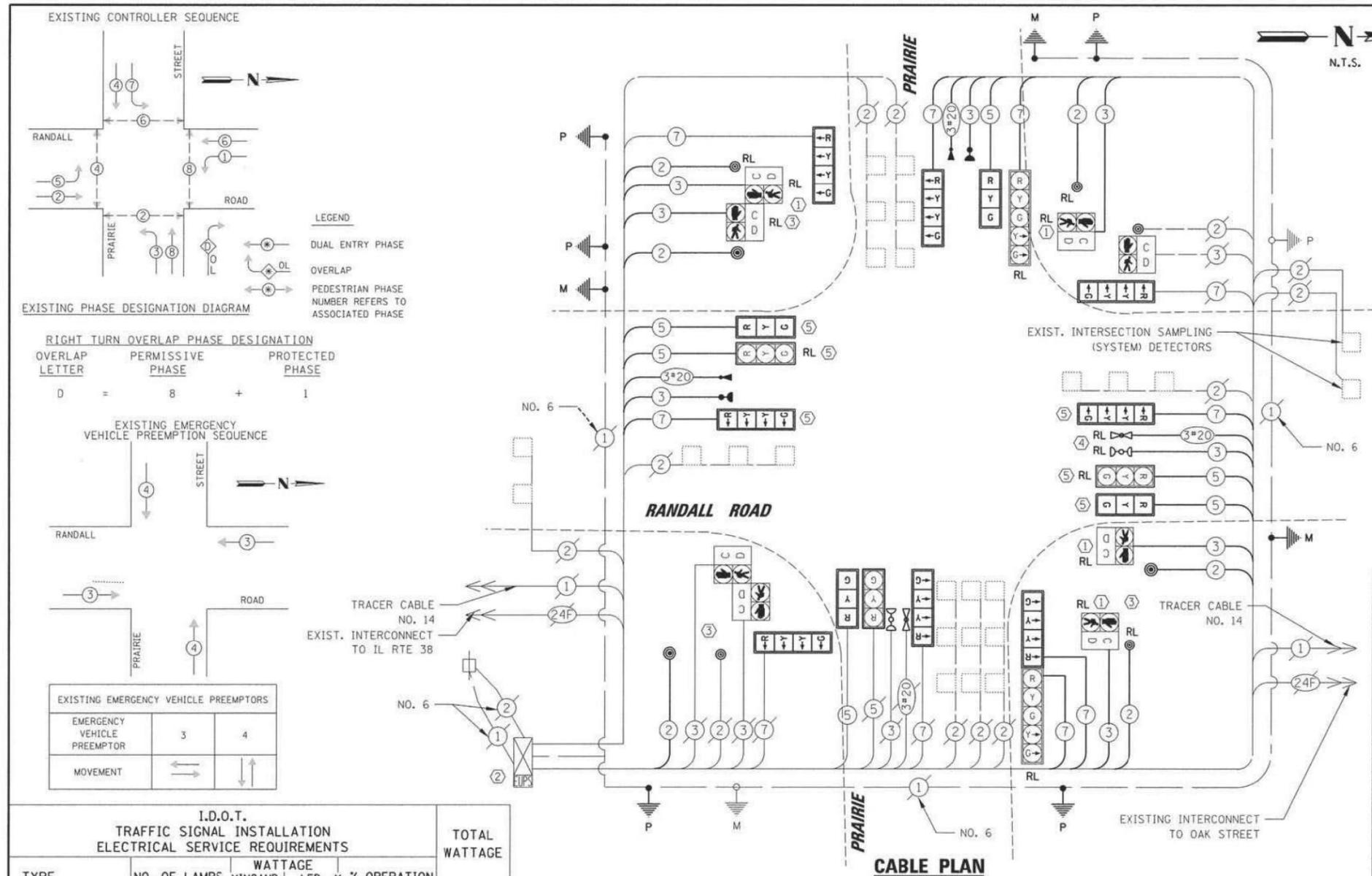
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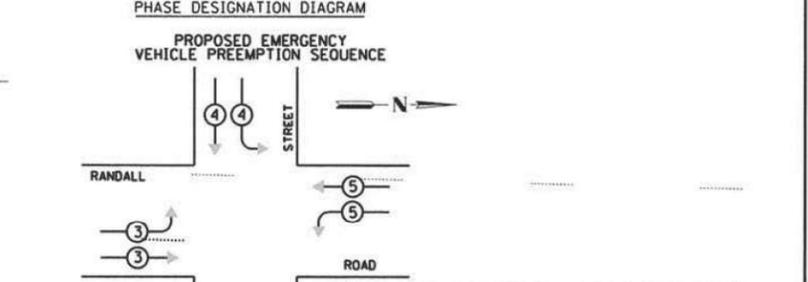
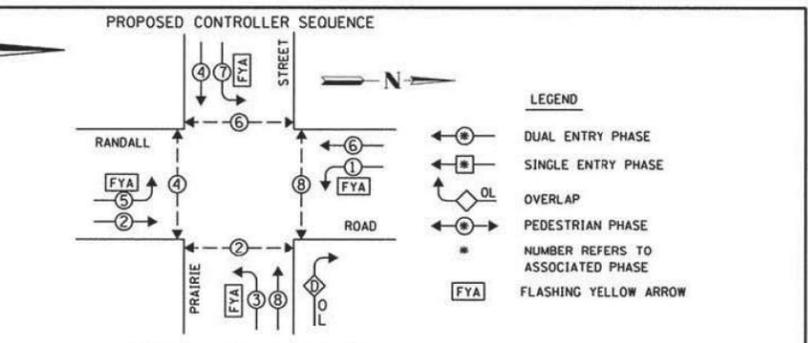
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| EMERGENCY VEHICLE PREEMPTOR | 3 | 4 |
|-----------------------------|---|---|
| MOVEMENT                    | ← | ↑ |



| EMERGENCY VEHICLE PREEMPTOR | 3 | 4 | 5 | 6 |
|-----------------------------|---|---|---|---|
| MOVEMENT                    | ← | ↑ | ← | ↑ |

| I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS |              |                                     |      | TOTAL WATTAGE |
|--|--------------|-------------------------------------|------|---------------|
| TYPE   | NO. OF LAMPS | WATTAGE X INCAND. LED X % OPERATION |      |               |
| SIGNAL (RED)   | 17           | 17                                  | 0.50 | 144.5         |
| (YELLOW)   | 17           | 25                                  | 0.25 | 106.25        |
| (GREEN)  | 17           | 15                                  | 0.25 | 63.75         |
| ARROW  | 4            | 12                                  | 0.10 | 4.8           |
| FYA SECTION  | 8            | 12                                  | 0.30 | 28.80         |
| PED. SIGNAL  | 8            | 25                                  | 1.00 | 200.00        |
| CONTROLLER   | 1            | 100                                 | 1.00 | 100.00        |
| ILLUM SIGN   | -            | 25                                  | 0.05 | -             |
| VIDEO SYSTEM   | -            | 150                                 | 1.00 | -             |
| UPS  | 1            | 25                                  | 1.00 | 25.00         |
| FLASHER  | -            | -                                   | -    | 0.50          |
| ENERGY COSTS TO:   |              |                                     |      | TOTAL = 673.1 |

**KANE COUNTY DIVISION OF TRANSPORTATION**  
 41W011 BURLINGTON ROAD  
 ST. CHARLES, IL 60175  
 ENERGY SUPPLY: CONTACT: MIKE WIDHALM  
 PHONE: (847) 608-2376  
 COMPANY: COMED

- CONSTRUCTION NOTES:**
- PEDESTRIAN SIGNAL HEADS AND PUSHBUTTON SHALL BE RELOCATED AS SHOWN ON THE PLANS.
  - THE CONTROLLER SHALL BE MODIFIED AS NECESSARY TO IMPLEMENT THE PROPOSED SEQUENCE AS SHOWN ON THE PLAN, INCLUDING THE FLASHING YELLOW ARROW PHASE. ALL NECESSARY LABOR, MATERIALS, LOAD SWITCHES, SOFTWARE, MMUs, CABINET WIRING, HARDWARE, AND CONTROLLER PROGRAMMING SHALL BE INCLUDED IN THE COST OF MODIFY EXISTING CONTROLLER CABINET.
  - EACH PEDESTRIAN PUSH-BUTTON SHALL CALL ONLY ONE PHASE, ALL WIRING COSTS NECESSARY TO ELIMINATE DUAL CALL PUSHBUTTON OPERATION SHALL BE INCIDENTAL TO THE PAY ITEM PEDESTRIAN PUSH-BUTTON.
  - THE CONTRACTOR SHALL RELOCATE THE EXISTING LIGHT DETECTOR EQUIPMENT AND IMPLEMENT THE PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE AS SPECIFIED ON THE PLANS AND PROJECT SPECIAL PROVISIONS. ALL COSTS PERTAINING TO ACQUIRING NECESSARY EQUIPMENT, FIELD WIRING, OR PERIPHERALS, LABOR, AND MATERIALS TO PERFORM THIS WORK SHALL BE INCLUDED IN THE COST OF RELOCATE LIGHT DETECTOR.
  - TRAFFIC SIGNAL BACKPLATE, RETROREFLECTIVE SHALL BE INSTALLED ON NORTHBOUND AND SOUTHBOUND MAST ARM MOUNTED TRAFFIC SIGNAL HEADS ONLY. ALL OTHER TRAFFIC SIGNAL HEADS SHALL BE FITTED WITH TRAFFIC SIGNAL BACKPLATE. REMOVAL OF EXISTING BACKPLATES SHALL BE INCLUDED IN THE COST OF TRAFFIC SIGNAL BACKPLATE AND TRAFFIC SIGNAL BACKPLATE, RETROREFLECTIVE.

FOR REFERENCE ONLY

|   |                       |                 |           |
|---|-----------------------|-----------------|-----------|
| FILE NAME =   | USER NAME = jgurtovec | DESIGNED - EAJ  | REVISED - |
| K:\Projects\Kane County\2812 HSP\CADD\Drawings\Sheets\Traffic Signal Plans\31 CAB_PRAIRIE.dgn |                       | DRAWN - FPB     | REVISED - |
| PLOT SCALE = 1" = 20.0000' / 1"   |                       | CHECKED - WCE   | REVISED - |
| PLOT DATE = 5/15/2014   |                       | DATE - FEB 2014 | REVISED - |

**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

| SCHEDULE OF QUANTITIES, CABLE PLAN, PHASE DESIGNATION DIAGRAM AND EMERGENCY VEHICLE PREEMPTION SEQUENCE |                     |         |            | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------------------|---------|------------|-----------|---------|--------|--------------|-----------|
| SCALE: 1" = 20'   | SHEET NO. OF SHEETS | STA. -- | TO STA. -- |           |         |        |              |           |

|              |                           |
|--------------|---------------------------|
| CONTRACT NO. | ILLINOIS FED. AID PROJECT |
|--------------|---------------------------|

|              |             |                 |           |
|--------------|-------------|-----------------|-----------|
| FILE NAME =  | USER NAME = | DESIGNED - Z.N. | REVISED - |
|              |             | DRAWN - Z.N.    | REVISED - |
| PLOT SCALE = |             | CHECKED -       | REVISED - |
| PLOT DATE =  |             | DATE -          | REVISED - |

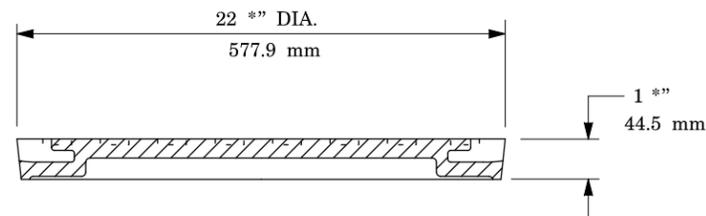
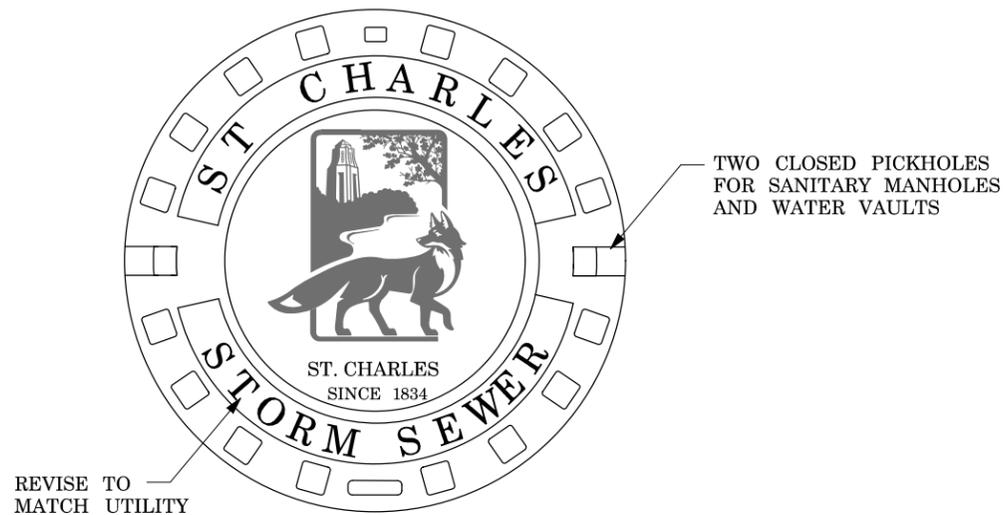
**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

| PRAIRIE STREET KDOT TRAFFIC SIGNAL DRAWINGS - FOR INFORMATION ONLY |                     |         |            | F.A.U RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--|---------------------|---------|------------|------------|---------|--------|--------------|-----------|
| SCALE:   | SHEET NO. OF SHEETS | STA. -- | TO STA. -- |            |         |        |              |           |

|              |                           |
|--------------|---------------------------|
| CONTRACT NO. | ILLINOIS FED. AID PROJECT |
|--------------|---------------------------|

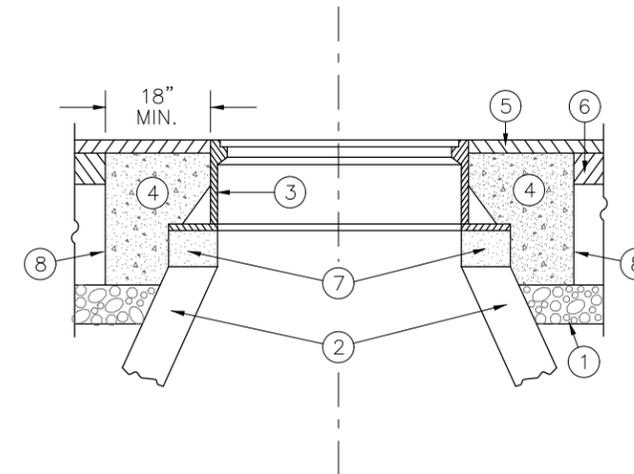
# SPECIAL LETTERED WATER VAULT, SANITARY SEWER AND STORM SEWER MANHOLE COVER

1 \*'' (38.1 mm) LETTERS  
(RECESSED FLUSH)



HEAVY DUTY  
MATERIAL ASTM A48 CL 35  
MACHINED BEARING SURFACE  
COVER WT: 125 LBS ( 56.7 kg)

NOTE:  
GASKET REQUIRED FOR  
SANITARY MANHOLES  
AND WATER VAULTS



## LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING STRUCTURE
- ③ FRAME AND LID (SEE NOTE)
- ④ CLASS SI CONCRETE
- ⑤ PROPOSED HMA SURFACE COURSE
- ⑥ PROPOSED HMA BINDER COURSE
- ⑦ 2 MAX. PRECAST CONCRETE ADJUSTING RINGS, MORTARED WITH STEEL SHIMS.
- ⑧ FULL DEPTH SAW CUT

## NOTE:

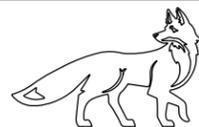
EXISTING FRAMES AND LIDS ARE THE PROPERTY OF THE CITY OF ST. CHARLES AND SHALL BE REMOVED AND RETURNED TO THE CITY OF ST. CHARLES.

## CONSTRUCTION PROCEDURES

AFTER PAVEMENT MILLING AND PLACEMENT OF HMA BINDER:

- A) FULL DEPTH SAW CUT 5' X 5' SQUARE OR DIAMOND AROUND CENTER OF FRAME TO BE ADJUSTED.
- B) REMOVE FRAME, EXISTING PAVEMENT AND AGGREGATE MATERIAL TO A DEPTH OF 2" BELOW TOP OF MANHOLE STRUCTURE & 2" BELOW BOTTOM OF LOWEST ADJUSTMENT RING.
- C) INSTALL FRAME AND LID TO MATCH PROPOSED HMA PAVEMENT SURFACE GRADE USING MORTAR, STEEL SHIMS EMBEDDED IN MORTAR, AND PRECAST CONCRETE ADJUSTING RINGS.
- D) COMPACT EXISTING AGGREGATE STONE BASE.
- E) FILL 5'X5' SURROUNDING SPACE WITH IDOT APPROVED CLASS SI CONCRETE TO THE SAME ELEVATION OF THE TOP OF THE BINDER COURSE.
- F) PAVE SURFACE COURSE OVER 5' X 5' CONCRETE COLLAR.
- G) STRUCTURE SHALL BE CLEANED OF DEBRIS AFTER FRAME ADJUSTMENT IS COMPLETED.

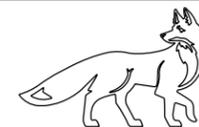
THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE IDOT STANDARD SPECIFICATIONS.



**CITY OF ST. CHARLES**  
ILLINOIS • SINCE 1834  
*Pride of the Fox*

### STANDARD COVER

|                 |                  |
|-----------------|------------------|
| DATE: 5-12-2021 | NOT TO SCALE     |
| REVISED:        | DRAWING NO. D-01 |



**CITY OF ST. CHARLES**  
ILLINOIS • SINCE 1834  
*Pride of the Fox*

### FRAME & LID ADJUSTMENT WITH CONCRETE COLLAR

|                 |                  |
|-----------------|------------------|
| DATE: 9-10-2021 | NOT TO SCALE     |
| REVISED:        | DRAWING NO. D-02 |

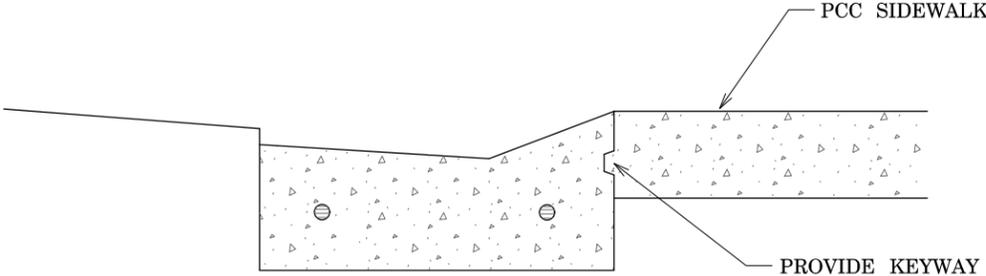
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|           |              | DRAWN - Z.N.    | REVISED -            |
|           | PLOT SCALE = | CHECKED -       | REVISED -            |
|           | PLOT DATE =  | DATE -          | REVISED -            |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

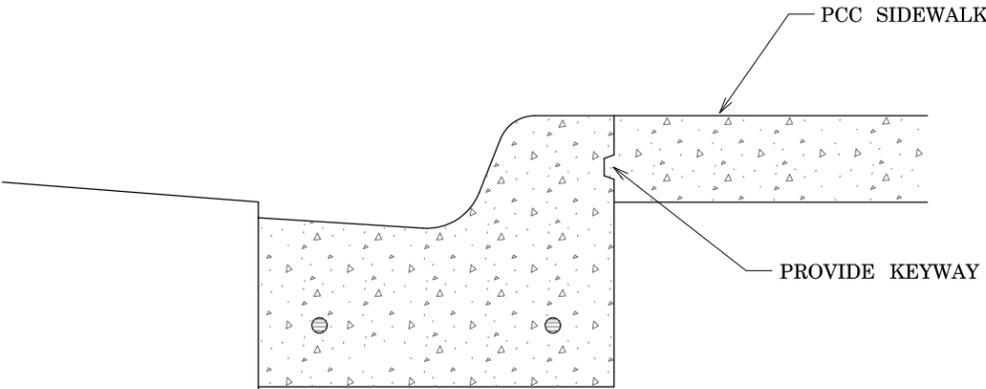
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|---|-----------|-----------|--------------|
| <b>PRAIRIE STREET<br/>UTILITY TYPICAL DETAILS</b> |           |           |              |
| SCALE:  | SHEET NO. | OF SHEETS | STA. TO STA. |

| F.A.U. RTE.                                       | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|----------------|--------|--------------|-----------|
| 1648  | 23-00118-00-RS | KANE   | 29           | 19        |
| CONTRACT NO. 61L26                                |                |        |              |           |
| FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT |                |        |              |           |

DEPRESSED CURB



CURB & GUTTER



NOTES:

1. SEE TYPICAL DETAIL FOR CURB AND GUTTER FOR CURB DIMENSIONS AND SPECIFICATIONS.

NOT TO SCALE

**SIDEWALK AT CURB AND GUTTER**

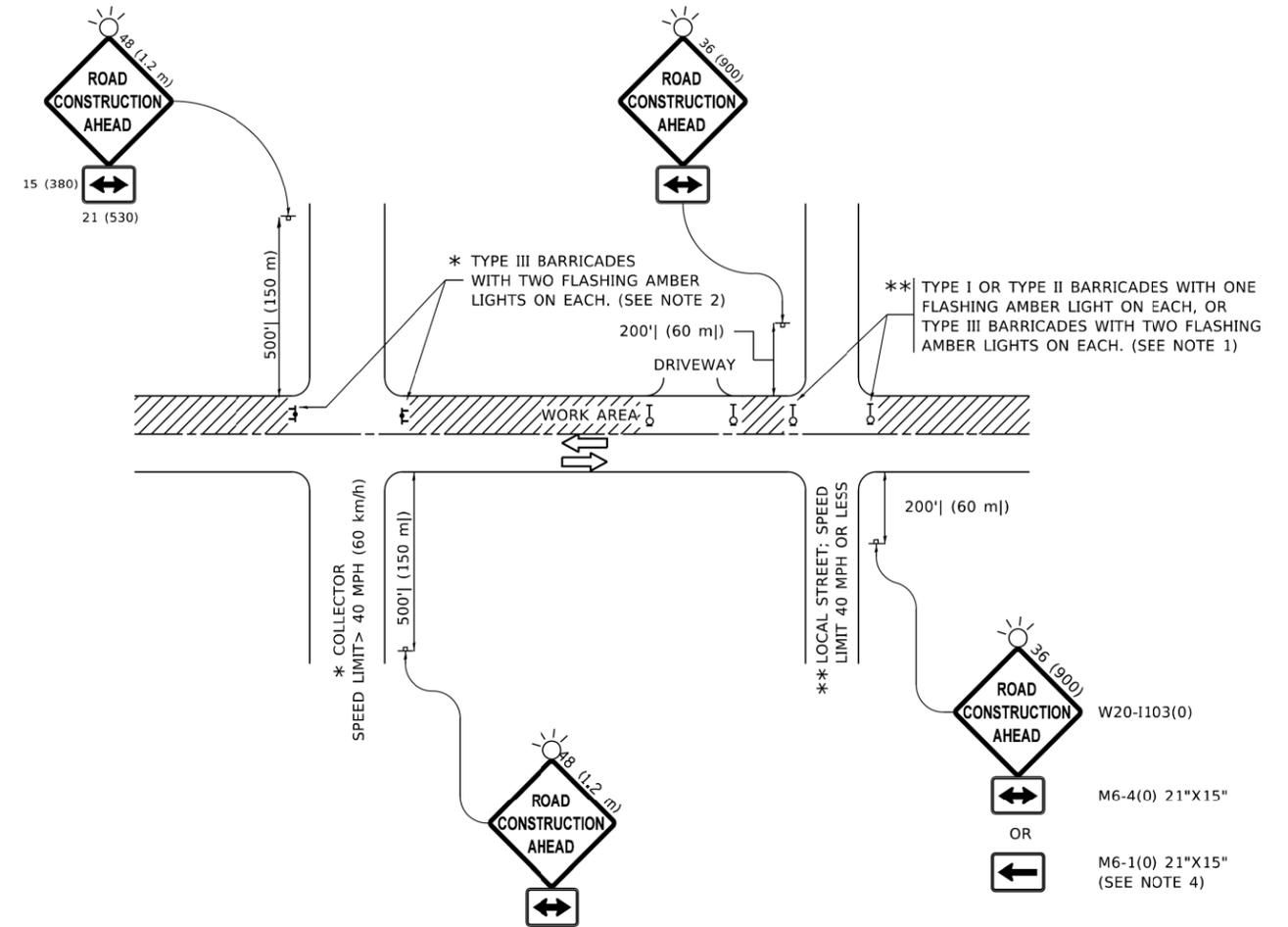
DATE: 3-31-09

|           |              |                 |                      |
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|           | PLOT SCALE = | CHECKED -       | REVISED -            |
|           | PLOT DATE =  | DATE -          | REVISED -            |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|   |           |           |              |
|---|-----------|-----------|--------------|
| <b>PRAIRIE STREET<br/>UTILITY TYPICAL DETAILS</b> |           |           |              |
| SCALE:  | SHEET NO. | OF SHEETS | STA. TO STA. |

|   |                |        |                    |           |
|---|----------------|--------|--------------------|-----------|
| F.A.U. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS       | SHEET NO. |
| 1648  | 23-00118-00-RS | KANE   | 29                 | 20        |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |                |        | CONTRACT NO. 61L26 |           |



**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.  
THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
  - b) CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
4. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default  
FILE: \\nwrts-pw-bentley.com\PI\DOT\Documents\DOT\_Offices\Di\hfrt\_1\Projects\Di\hfrt\_1\CAD\Drawn\CAD\hfrt\_1.dgn

|                               |                   |                                 |
|-------------------------------|-------------------|---------------------------------|
| USER NAME = Lawrence.DeManche | DESIGNED - L.H.A. | REVISED - T. RAMMACHER 01-06-00 |
|                               | DRAWN -           | REVISED - A. SCHUETZE 07-01-13  |
| PLOT SCALE = 100.0000' / in.  | CHECKED -         | REVISED - A. SCHUETZE 09-15-16  |
| PLOT DATE = 5/3/2024          | DATE - 06-89      | REVISED - D. SENDERAK 05-03-24  |

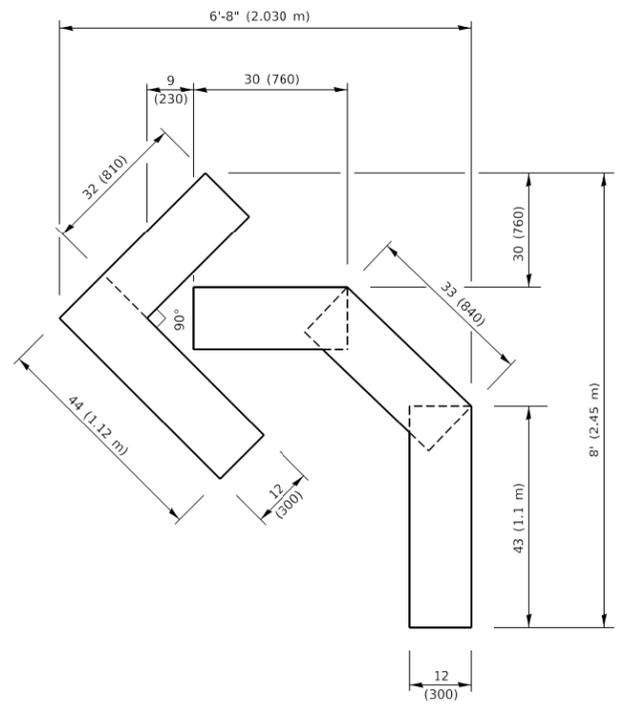
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

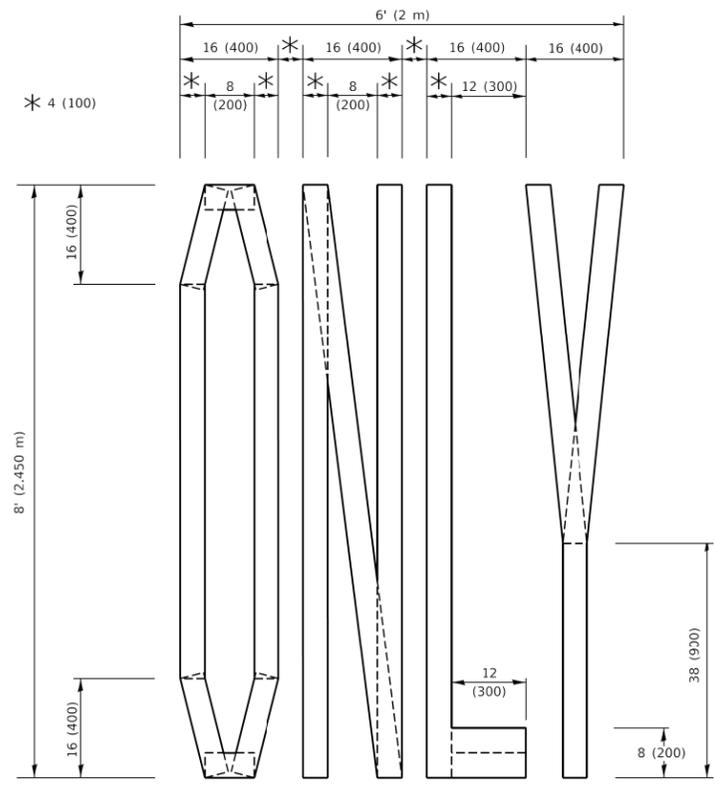
| F.A.U. RTE.               | SECTION       | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|---------------|--------|--------------|-----------|
| 1648                      | 23-0018-00-RS | KANE   | 29           | 21        |
| <b>TC-10</b>              |               |        | CONTRACT NO. | 6IL26     |
| ILLINOIS FED. AID PROJECT |               |        |              |           |





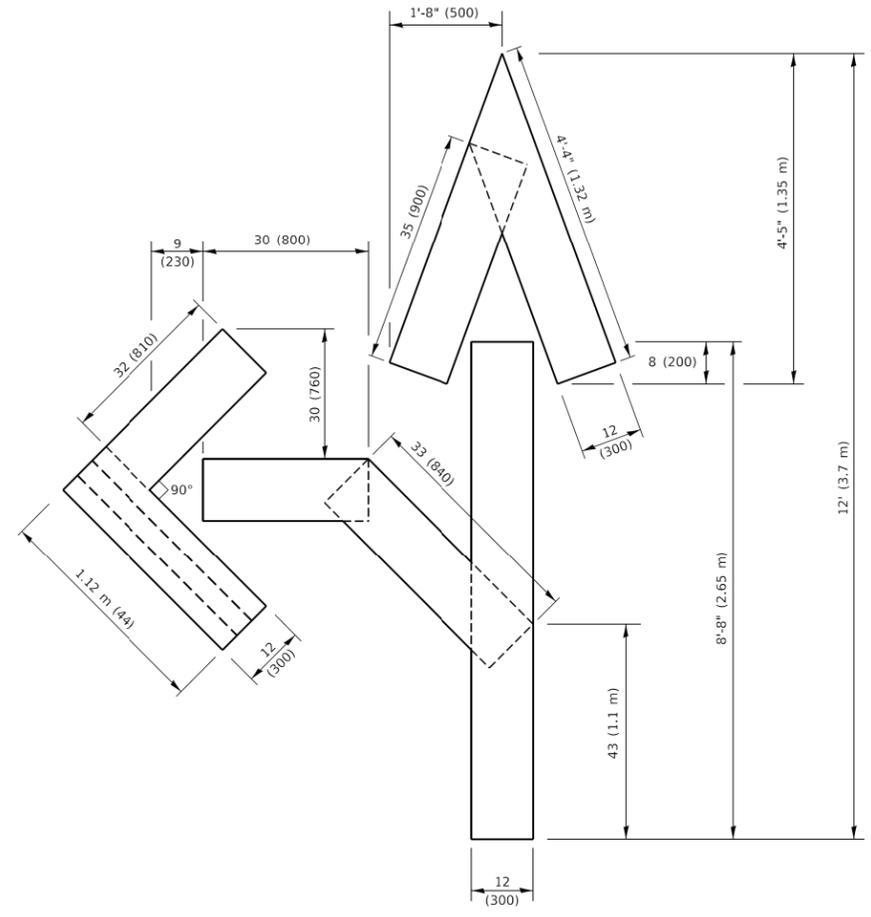
**QUANTITY**

4 (100) LINE = 45.5 ft. (13.9 m)  
15.2 sq. ft. (1.41 sq. m)



**QUANTITY**

4 (100) LINE = 64.1 ft. (19.5 m)  
21.4 sq. ft. (1.99 sq. m)

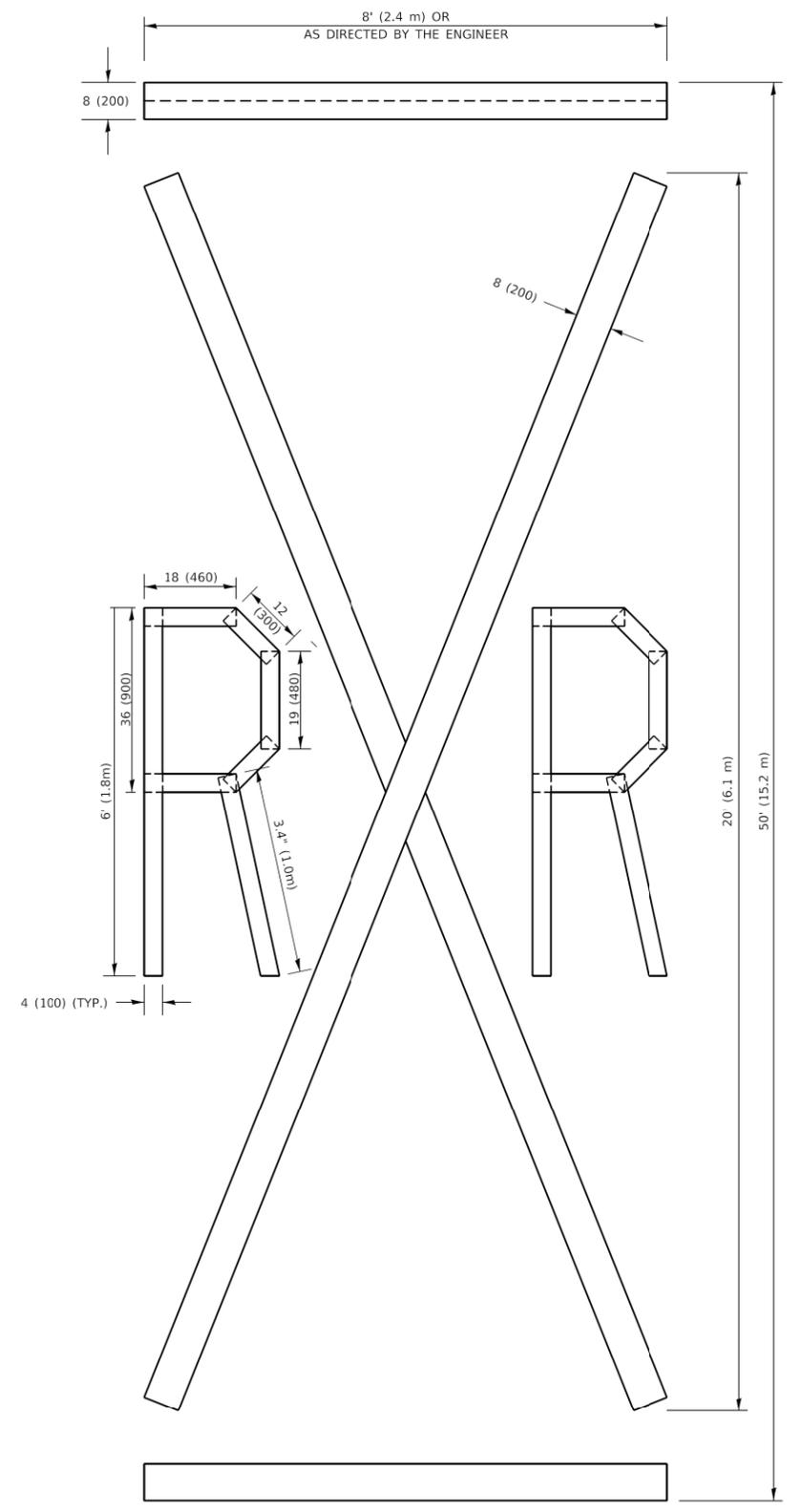


**QUANTITY**

4 (100) LINE = 82.5 ft. (25.1 m)  
27.5 sq. ft. (2.53 sq. m)

**NOTE:**

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**

4 (100) LINE = 225.9 ft. (68.9 m)  
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default  
FILE NAME: I:\110848\EBID\ITEC\Illinois.gov\PWD\DOT\Documents\DOT\_Offices\District\_1\Projects\Dist5\S22-24\CAD\Drawn\CAD\Sheets\16.dgn

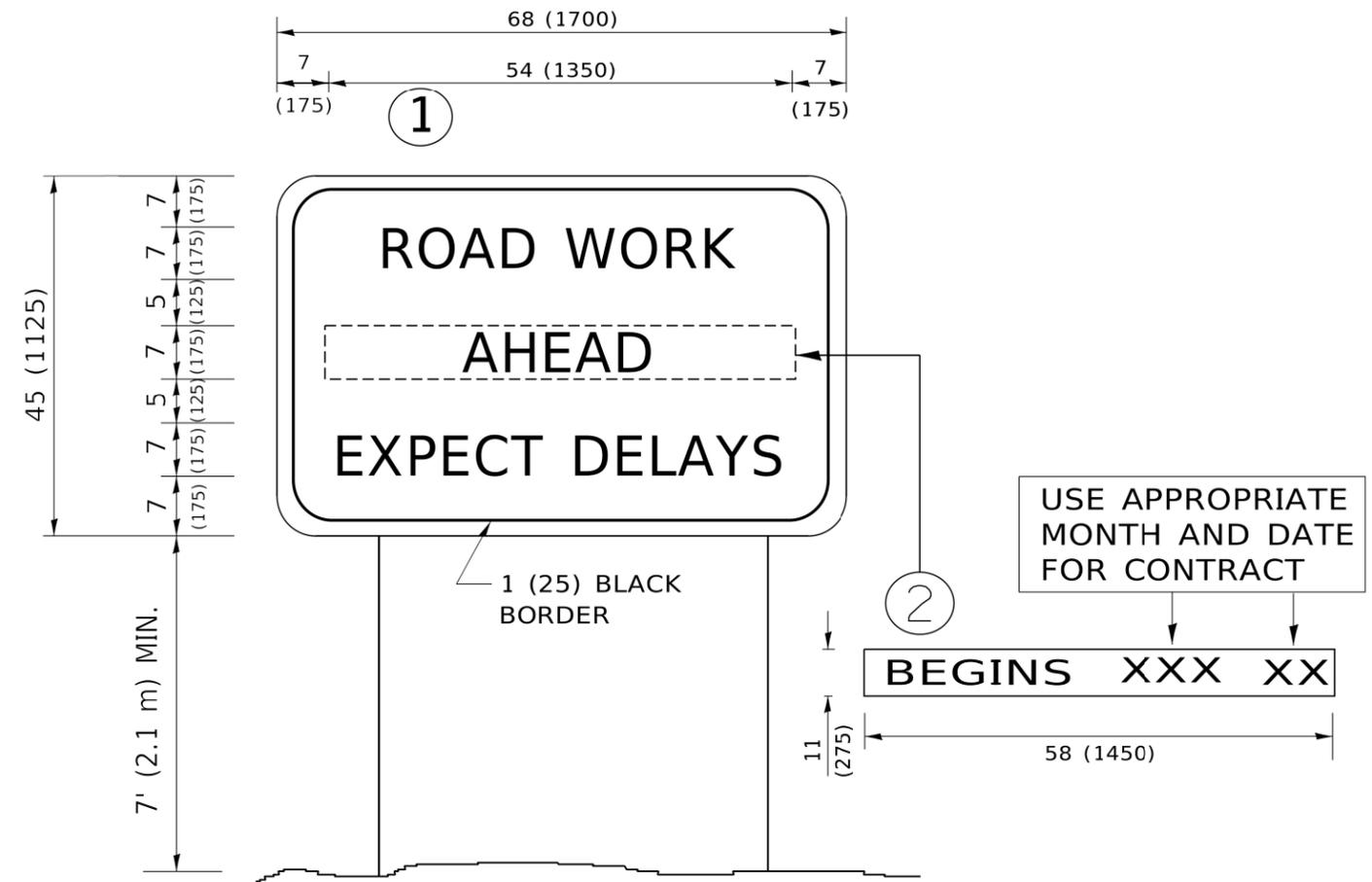
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| USER NAME = footemj         | DESIGNED -      | REVISED - T. RAMMACHER 03-02-98 |
| PLOT SCALE = 50.0068" / in. | DRAWN -         | REVISED - E. GOMEZ 08-28-00     |
| PLOT DATE = 3/4/2019        | CHECKED -       | REVISED - E. GOMEZ 08-28-00     |
|                             | DATE - 09-18-94 | REVISED - A. SCHUETZE 09-15-16  |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

| F.A.U. RTE.               | SECTION       | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|---------------|--------|--------------|-----------|
| 1648                      | 23-0018-00-RS | KANE   | 29           | 23        |
| <b>TC-16</b>              |               |        | CONTRACT NO. | 61L26     |
| ILLINOIS FED. AID PROJECT |               |        |              |           |



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: Default  
 FILE: Model\_Plan\_I11084EIBDINTEG\_ILlincois.gov.PWIDOTDocuments\DOT\_Offices\District\_1\Projects\DUH54272-21\CADD\BNA\CAD\sheet122.dgn

|                              |            |                                 |
|------------------------------|------------|---------------------------------|
| USER NAME = footemj          | DESIGNED - | REVISED - R. MIRS 09-15-97      |
| PLOT SCALE = 50.0000 ' / ft. | DRAWN -    | REVISED - R. MIRS 12-11-97      |
| PLOT DATE = 3/4/2019         | CHECKED -  | REVISED - T. RAMMACHER 02-02-99 |
|                              | DATE -     | REVISED - C. JUCIUS 01-31-07    |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

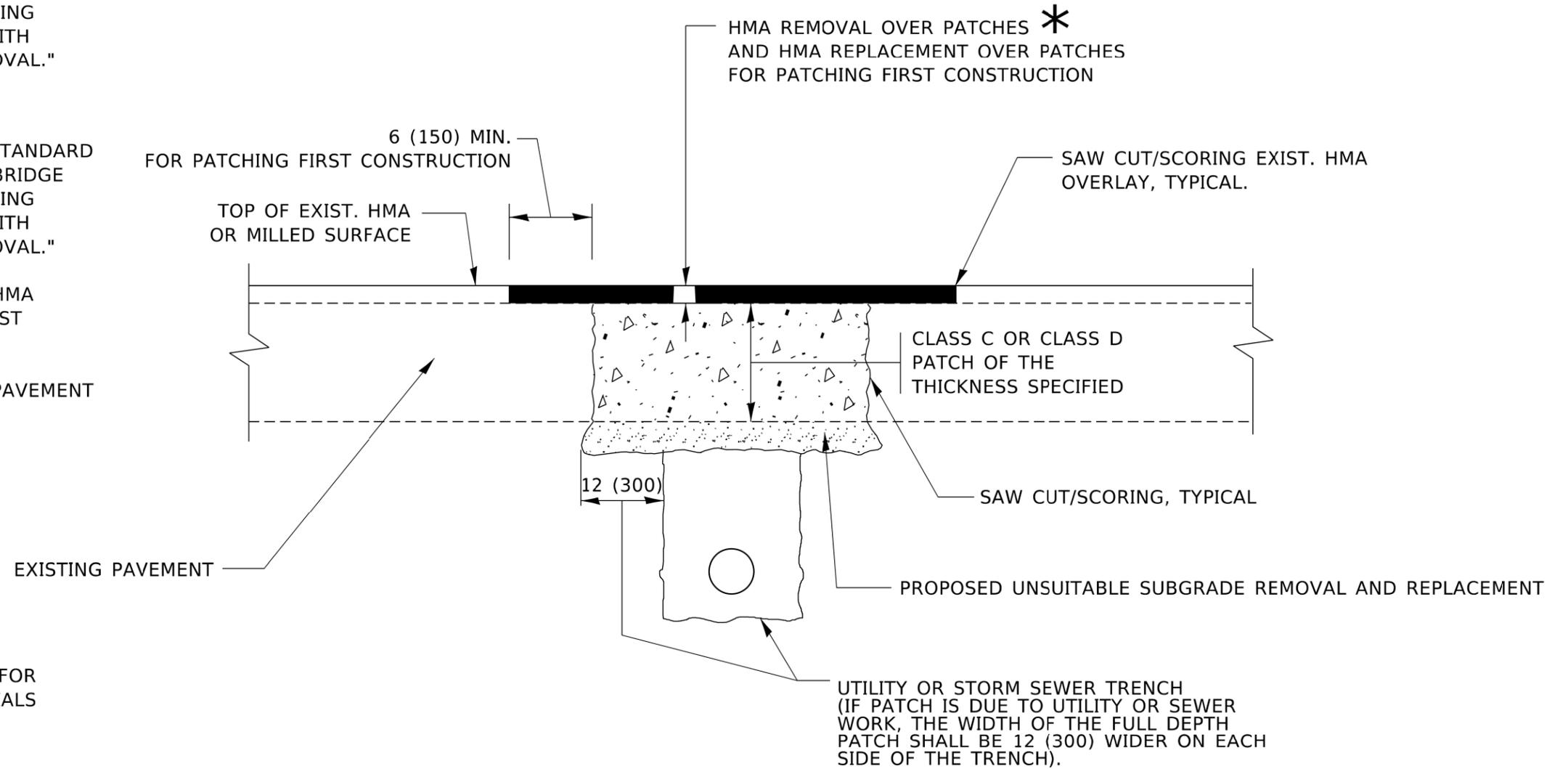
| F.A.U. RTE.               | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------|--------|--------------|-----------|
| 1648                      | 23-00118-00-RS | KANE   | 29           | 24        |
| <b>TC-22</b>              |                |        | CONTRACT NO. | 6IL26     |
| ILLINOIS FED. AID PROJECT |                |        |              |           |

**METHOD OF MEASUREMENT**

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

**BASIS OF PAYMENT**

1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**SEQUENCE OF CONSTRUCTION (PATCHING FIRST)**

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

**SEQUENCE OF CONSTRUCTION (MILLING FIRST)**

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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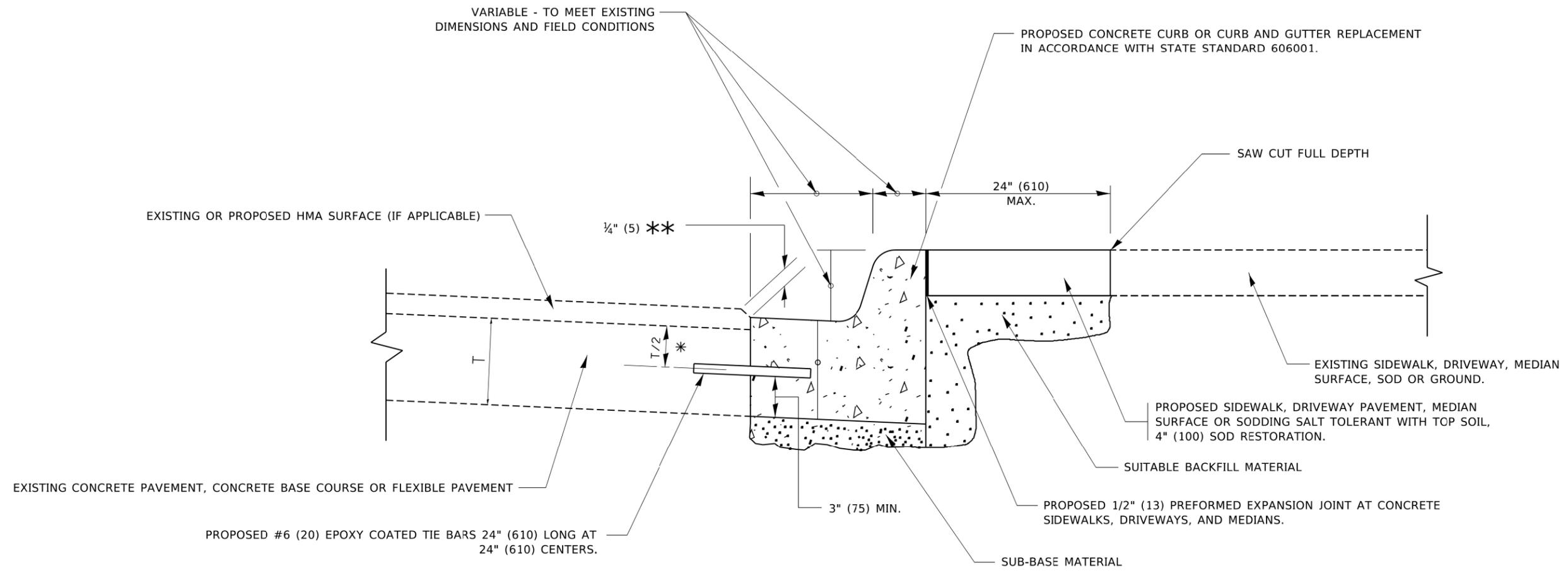
|                               |                    |                             |
|-------------------------------|--------------------|-----------------------------|
| USER NAME = Lawrence.DeManche | DESIGNED - R. SHAH | REVISED - R. BORO 01-01-07  |
|                               | DRAWN -            | REVISED - R. BORO 09-04-07  |
| PLOT SCALE = 100.0000' / in.  | CHECKED -          | REVISED - K. ENG 10-27-08   |
| PLOT DATE = 11/18/2022        | DATE - 10-25-94    | REVISED - K. SMITH 11-18-22 |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT PATCHING FOR  
HMA SURFACED PAVEMENT**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

|                           |               |              |              |           |
|---------------------------|---------------|--------------|--------------|-----------|
| F.A.U. RTE.               | SECTION       | COUNTY       | TOTAL SHEETS | SHEET NO. |
| 1648                      | 23-0018-00-RS | KANE         | 29           | 25        |
| <b>BD400-04 (BD-22)</b>   |               | CONTRACT NO. | 6IL26        |           |
| ILLINOIS FED. AID PROJECT |               |              |              |           |



- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- \*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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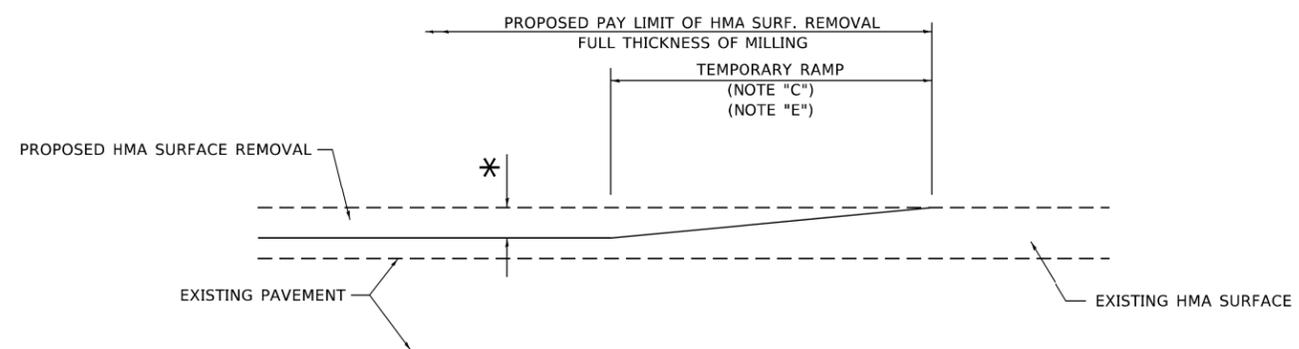
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|---|----------------------|-----------------------------|
| USER NAME = footemj   | DESIGNED - A. HOUSEH | REVISED - A. ABBAS 03-21-97 |
|   | DRAWN -              | REVISED - M. GOMEZ 01-22-01 |
| PLOT SCALE = 50.0000 "/td> <td>CHECKED -</td> <td>REVISED - R. BORO 12-15-09</td> | CHECKED -            | REVISED - R. BORO 12-15-09  |
| PLOT DATE = 7/11/2019   | DATE - 03-11-94      | REVISED - K. SMITH 07-11-19 |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CURB OR CURB AND GUTTER  
REMOVAL AND REPLACEMENT**

SCALE: NONE    SHEET 1 OF 1 SHEETS    STA.    TO STA.

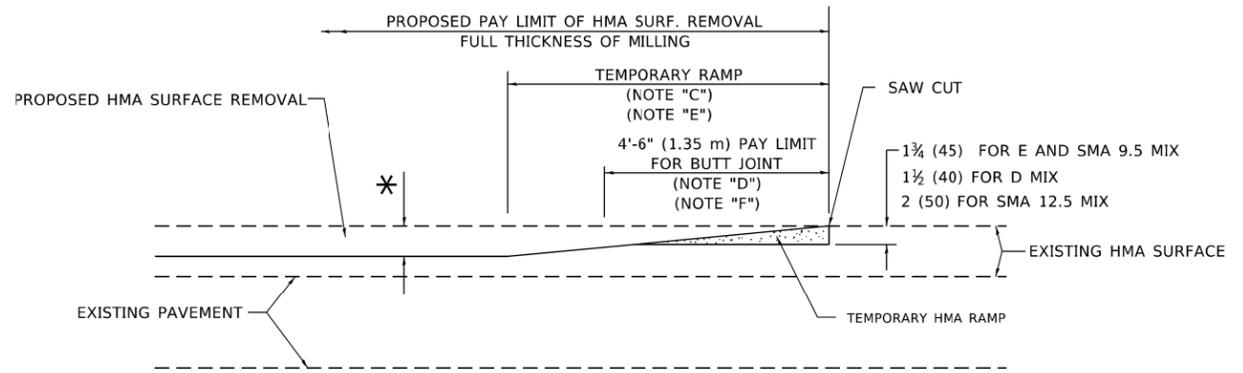
| F.A.U. RTE.               | SECTION        | COUNTY       | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------|--------------|--------------|-----------|
| 1648                      | 23-00118-00-RS | KANE         | 29           | 26        |
| <b>BD600-06 (BD-24)</b>   |                | CONTRACT NO. | 6IL26        |           |
| ILLINOIS FED. AID PROJECT |                |              |              |           |



**MILLED TEMPORARY RAMP**

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

**OPTION 1**

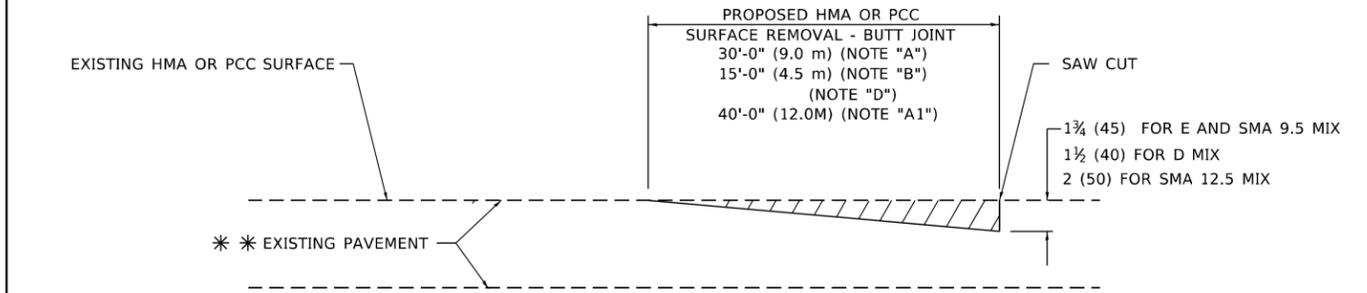


**HMA CONSTRUCTED TEMPORARY RAMP**

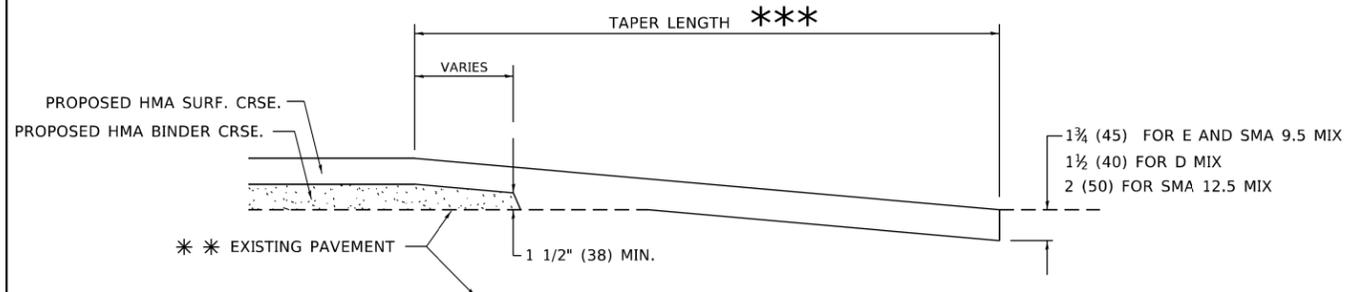
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

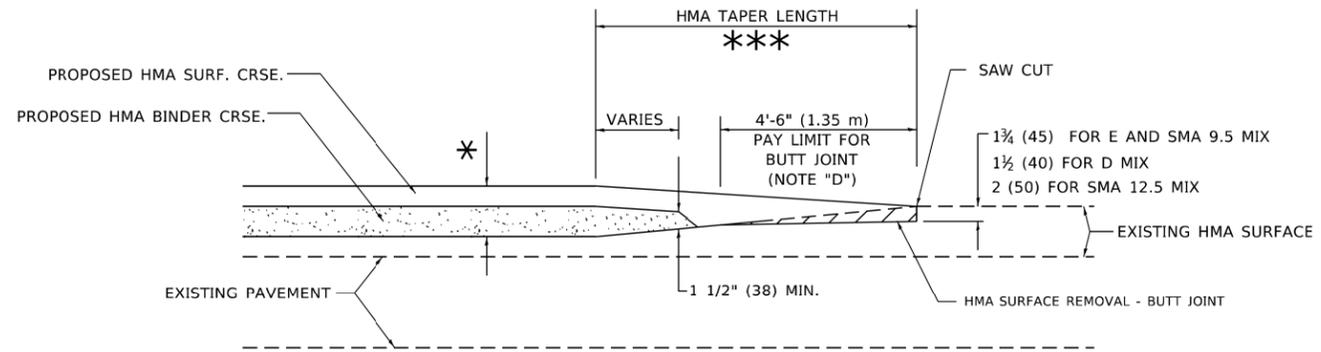
**GENERAL NOTES**

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.  
\* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".  
\*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT**

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



**BUTT JOINT AND HMA TAPER**

**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

MODEL: Default  
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|                               |                       |                             |
|-------------------------------|-----------------------|-----------------------------|
| USER NAME = Lawrence.DeManche | DESIGNED - M. DE YONG | REVISED - A. ABBAS 03-21-97 |
|                               | DRAWN -               | REVISED - M. GOMEZ 04-06-01 |
| PLOT SCALE = 100.0000' / in.  | CHECKED -             | REVISED - R. BORO 01-01-07  |
| PLOT DATE = 11/18/2022        | DATE - 06-13-90       | REVISED - K. SMITH 11-18-22 |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

|             |  |                     |  |              |  |
|-------------|--|---------------------|--|--------------|--|
| SCALE: NONE |  | SHEET 1 OF 1 SHEETS |  | STA. TO STA. |  |
|-------------|--|---------------------|--|--------------|--|

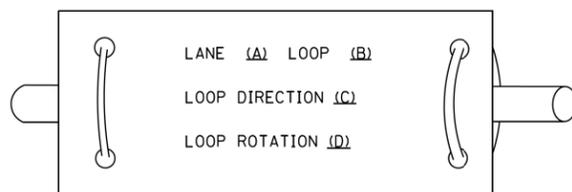
**BUTT JOINT AND HMA TAPER DETAILS**

|                           |               |              |              |           |
|---------------------------|---------------|--------------|--------------|-----------|
| F.A.U. RTE.               | SECTION       | COUNTY       | TOTAL SHEETS | SHEET NO. |
| 1648                      | 23-0018-00-RS | KANE         | 29           | 27        |
| BD400-05 BD-32            |               | CONTRACT NO. |              | 6IL26     |
| ILLINOIS FED. AID PROJECT |               |              |              |           |

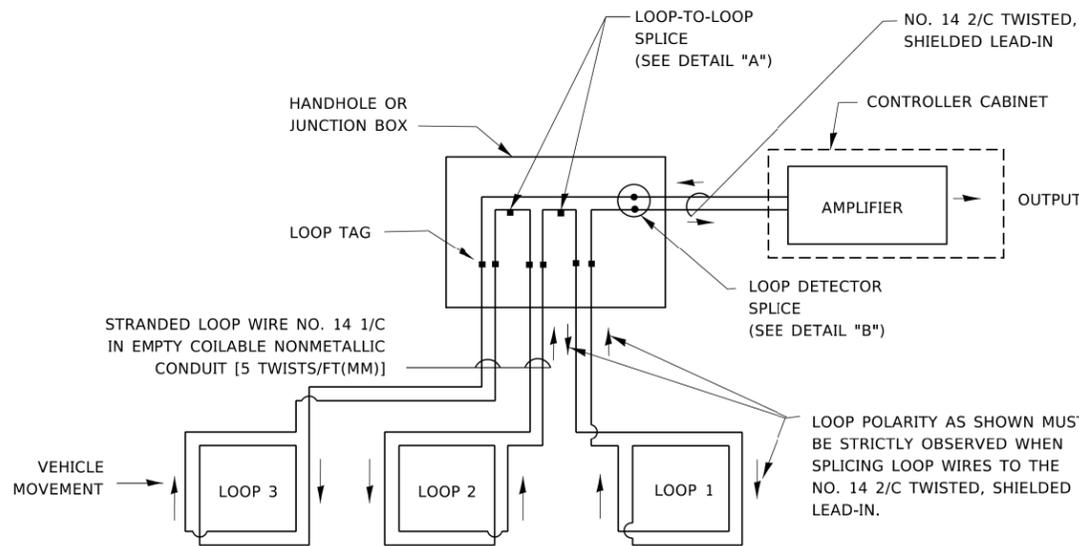
**LOOP DETECTOR NOTES**

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVESHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

**LOOP LEAD-IN CABLE TAG**

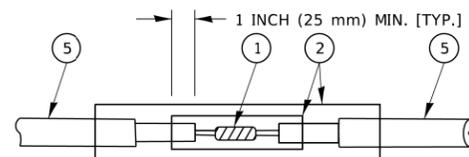


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

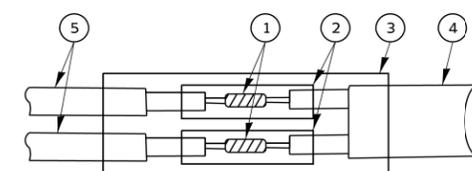


**DETECTOR LOOP WIRING SCHEMATIC**

- LOOPS SHALL BE SPLICED IN SERIES. SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE,
- THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

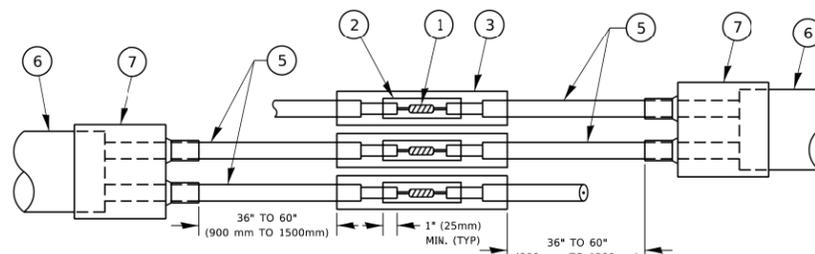


DETAIL "A"  
LOOP-TO-LOOP SPLICE

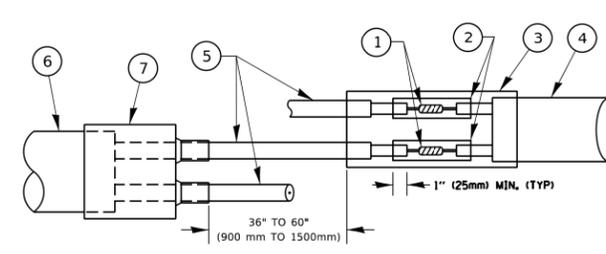


DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

**TYPE I LOOP**



DETAIL "A"  
LOOP-TO-LOOP SPLICE



DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

**PREFORMED LOOP**

**LOOP DETECTOR SPLICE**

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE. PREFORMED LOOP
- 6 XL POLYOLEFIN 2 CONDUCTOR
- 7 BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

MODEL: Default  
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|                            |            |           |
|----------------------------|------------|-----------|
| USER NAME = footemj        | DESIGNED - | REVISED - |
|                            | DRAWN -    | REVISED - |
| PLOT SCALE = 50.0000 "/ft. | CHECKED -  | REVISED - |
| PLOT DATE = 3/4/2019       | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

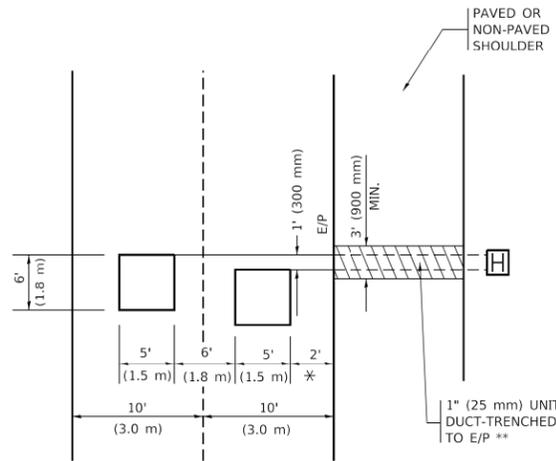
**DISTRICT ONE  
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET 2 OF 7 SHEETS STA. TO STA.

|                           |                       |                    |                 |              |
|---------------------------|-----------------------|--------------------|-----------------|--------------|
| F.A.U. RTE. 1648          | SECTION 23-0018-00-RS | COUNTY KANE        | TOTAL SHEETS 29 | SHEET NO. 28 |
| TS-05                     |                       | CONTRACT NO. 61L26 |                 |              |
| ILLINOIS FED. AID PROJECT |                       |                    |                 |              |

**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

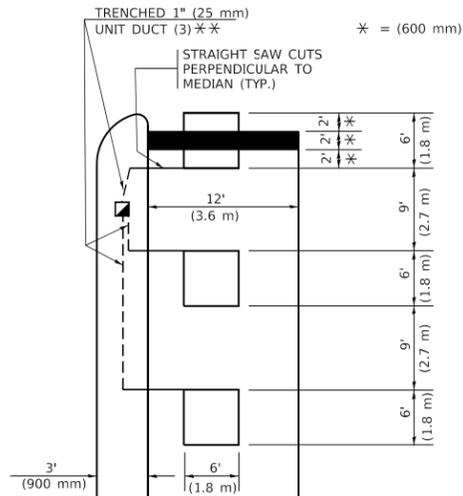


\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS**  
**VOLUME DENSITY ("FAR OUT" DETECTION)**  
**ON SAME APPROACH**  
**(PROTECTED / PERMITTED LEFT TURN PHASING)**

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

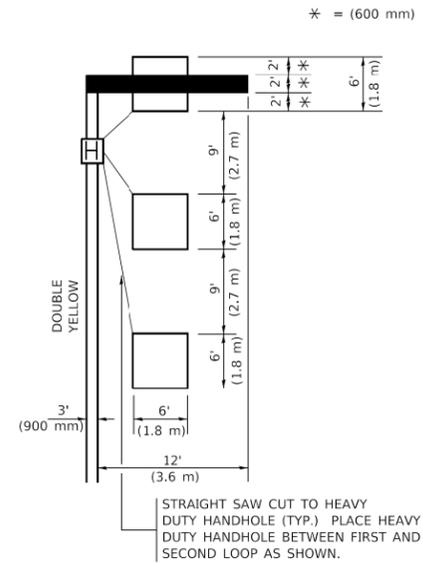


\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS**  
**VOLUME DENSITY ("FAR OUT" DETECTION)**  
**ON SAME APPROACH**  
**(PROTECTED / PERMITTED LEFT TURN PHASING)**



\* = (600 mm)

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**NOTES:**

**VEHICLES LOOP DETECTORS**

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

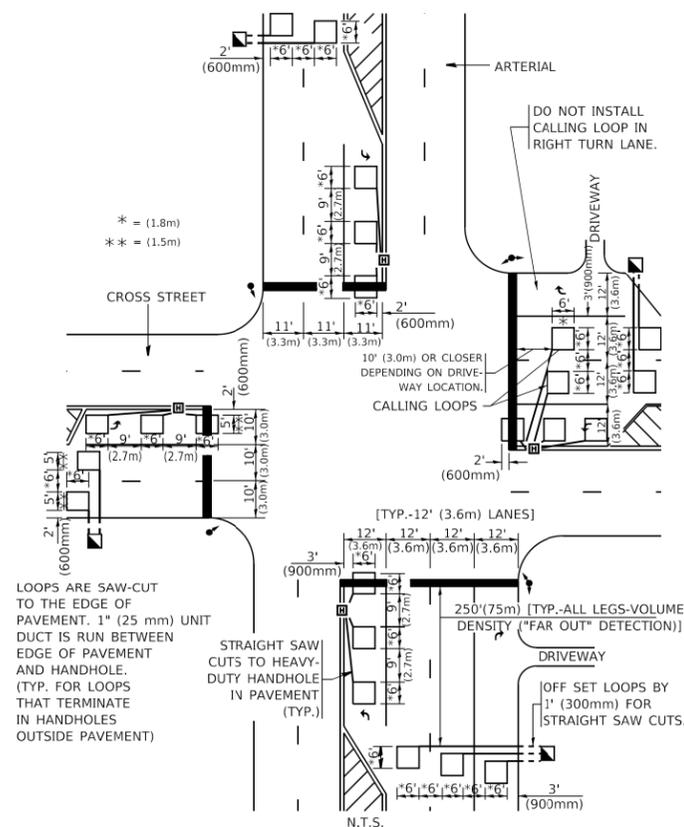
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

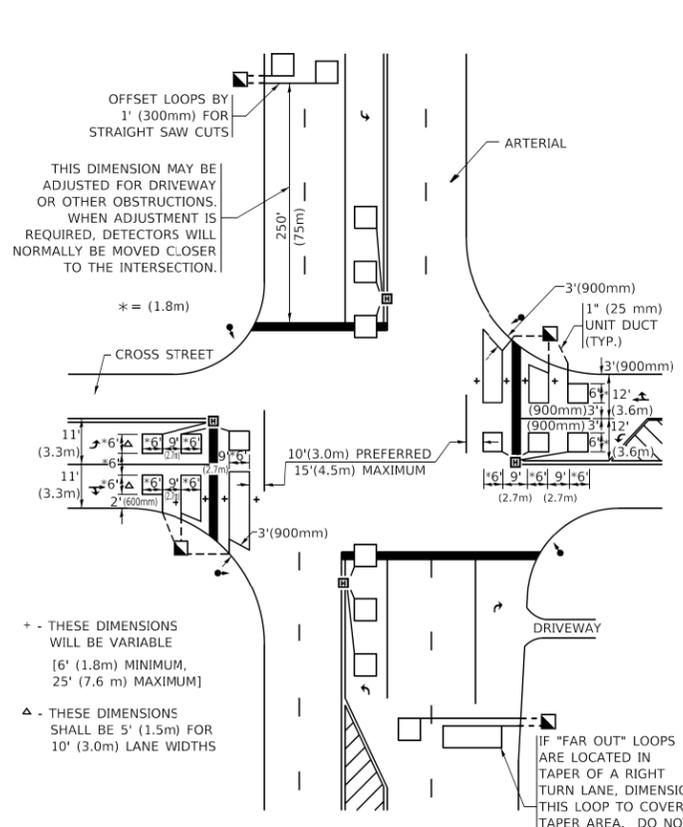
THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)**  
**CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1**  
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)**  
**CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2**  
N.T.S.

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION**  
**DETAILS FOR ROADWAY RESURFACING**

|                            |                  |           |
|----------------------------|------------------|-----------|
| USER NAME = footem         | DESIGNED -       | REVISED - |
| PLOT SCALE = 50.0000' / 1" | CHECKED - R.K.F. | REVISED - |
| PLOT DATE = 3/4/2019       | DATE -           | REVISED - |

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION       | COUNTY             | TOTAL SHEETS | SHEET NO. |
|-------------|---------------|--------------------|--------------|-----------|
| 1648        | 23-0018-00-RS | KANE               | 29           | 29        |
| TS-07       |               | CONTRACT NO. 6IL26 |              |           |