

LEGEND

	EXISTING ROADWAY LIGHTPOLE AND LUMINAIRE TO BE REMOVED, SALVAGE		PROPOSED UTILITY POLE (BY OTHERS)
	PROPOSED DECORATIVE LIGHT POLE AND LUMINAIRE ALUMINUM, 40 FT. M.H., PULSE START METAL HALIDE, HORIZONTAL MOUNT 400 WATT (SEE NOTE 13 ON SHEET 487)		PROPOSED LIGHTING CONTROLLER, 100 AMP, 480V
	PROPOSED UNDERPASS LUMINAIRE, METAL HALIDE, VERTICAL MOUNT, 100 WATT		HH = PROPOSED HANDHOLE PER IDOT STD 814001 CHH = PROPOSED HANDHOLE, COMPOSITE CONCRETE TYPE AS INDICATED
			PROPOSED JUNCTION BOX
			PROPOSED CONDUIT ATTACHED TO STRUCTURE, SIZE AND CONDUCTORS AS INDICATED

	PROPOSED UNDERGROUND CONDUIT, GALVANIZED STEEL, BORED AND PULLED, SIZE AND CONDUCTORS AS INDICATED.
	PROPOSED UNIT DUCT, 600V, 2-1/C NO.6, 1/C NO.6 GROUND, (XLP-TYPE USE), 1 1/4" DIA. POLYETHYLENE
	PROPOSED UNDERGROUND CONDUIT, COILABLE NONMETALLIC CONDUIT, 2" DIA. WITH PULL STRING AND TRACER CABLE
	PROPOSED UNDERGROUND CONDUIT, GALVANIZED STEEL, SIZE AND CONDUCTORS AS INDICATED

NOTES:
1. REFER TO ROADWAY LIGHTING NOTES ON SHEET 487.

DIRECTORY = I:\PROJECTS\112804\Draw\CAD\Draw_Sheets\DR4821.DWG
 USER NAME = Jensen Dove



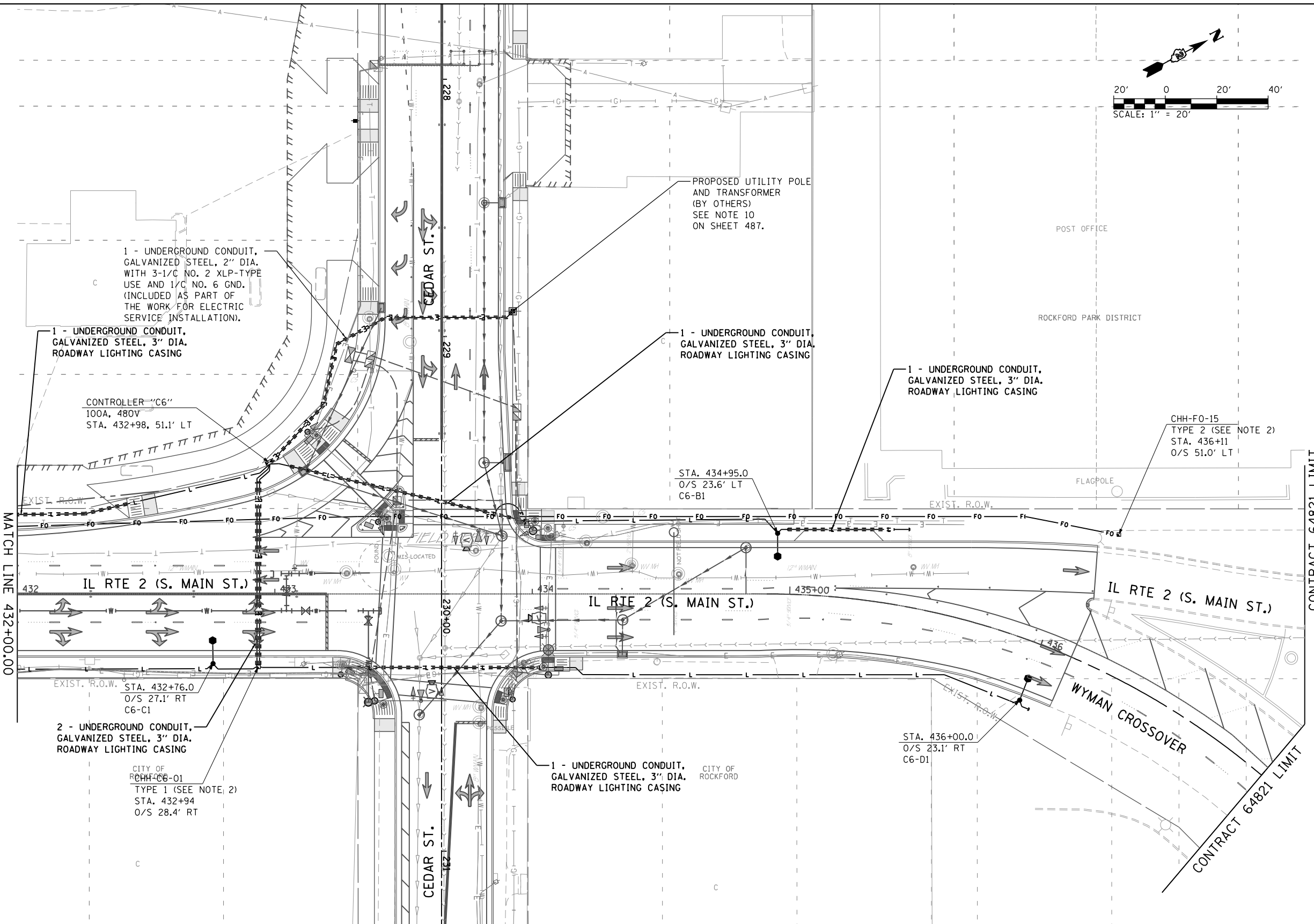
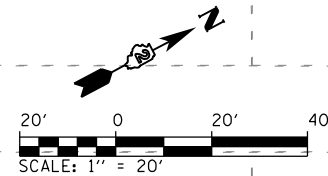
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY LIGHTING PLAN
IL RTE 2 (SOUTH MAIN STREET)**

SCALE: 1" = 20' SHEET 15 OF 32 SHEETS STA. 422+00 TO STA. 428+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32, 33) R-1	WINNEBAGO	705	501
CONTRACT NO. 64821				
ILLINOIS FED. AID PROJECT				



MATCH LINE 432+00.00

CONTRACT 64821 LIMIT

CONTRACT 64821 LIMIT

LEGEND

	EXISTING ROADWAY LIGHTPOLE AND LUMINAIRE TO BE REMOVED, SALVAGE		PROPOSED UTILITY POLE (BY OTHERS)
	PROPOSED DECORATIVE LIGHT POLE AND LUMINAIRE ALUMINUM, 40 FT. M.H., PULSE START METAL HALIDE, HORIZONTAL MOUNT 400 WATT (SEE NOTE 13 ON SHEET 487)		PROPOSED LIGHTING CONTROLLER, 100 AMP, 480V
	PROPOSED UNDERPASS LUMINAIRE, METAL HALIDE, VERTICAL MOUNT, 100 WATT		HH = PROPOSED HANDHOLE PER IDOT STD 814001 CHH = PROPOSED HANDHOLE, COMPOSITE CONCRETE TYPE AS INDICATED
			PROPOSED JUNCTION BOX
			PROPOSED CONDUIT ATTACHED TO STRUCTURE, SIZE AND CONDUCTORS AS INDICATED

	PROPOSED UNDERGROUND CONDUIT, GALVANIZED STEEL, BORED AND PULLED, SIZE AND CONDUCTORS AS INDICATED.
	PROPOSED UNIT DUCT, 600V, 2-1/2 NO.6, 1/2 NO.6 GROUND, (XLP-TYPE USE), 1/4" DIA. POLYETHYLENE
	PROPOSED UNDERGROUND CONDUIT, COILABLE NONMETALLIC CONDUIT, 2" DIA. WITH PULL STRING AND TRACER CABLE
	PROPOSED UNDERGROUND CONDUIT, GALVANIZED STEEL, SIZE AND CONDUCTORS AS INDICATED

NOTES:

- REFER TO ROADWAY LIGHTING NOTES ON SHEET 487.
- SEE "HANDHOLE, COMPOSITE CONCRETE" SPECIAL PROVISION FOR HANDHOLE SIZES.

DIRECTORY : I:\ROCKFORD\112804\Draw\CADD_Sheets\64821_DLA...
 USER NAME : Jason Dove



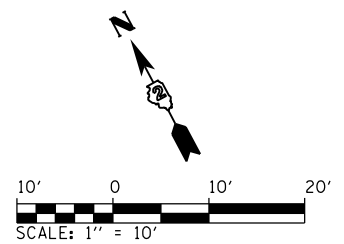
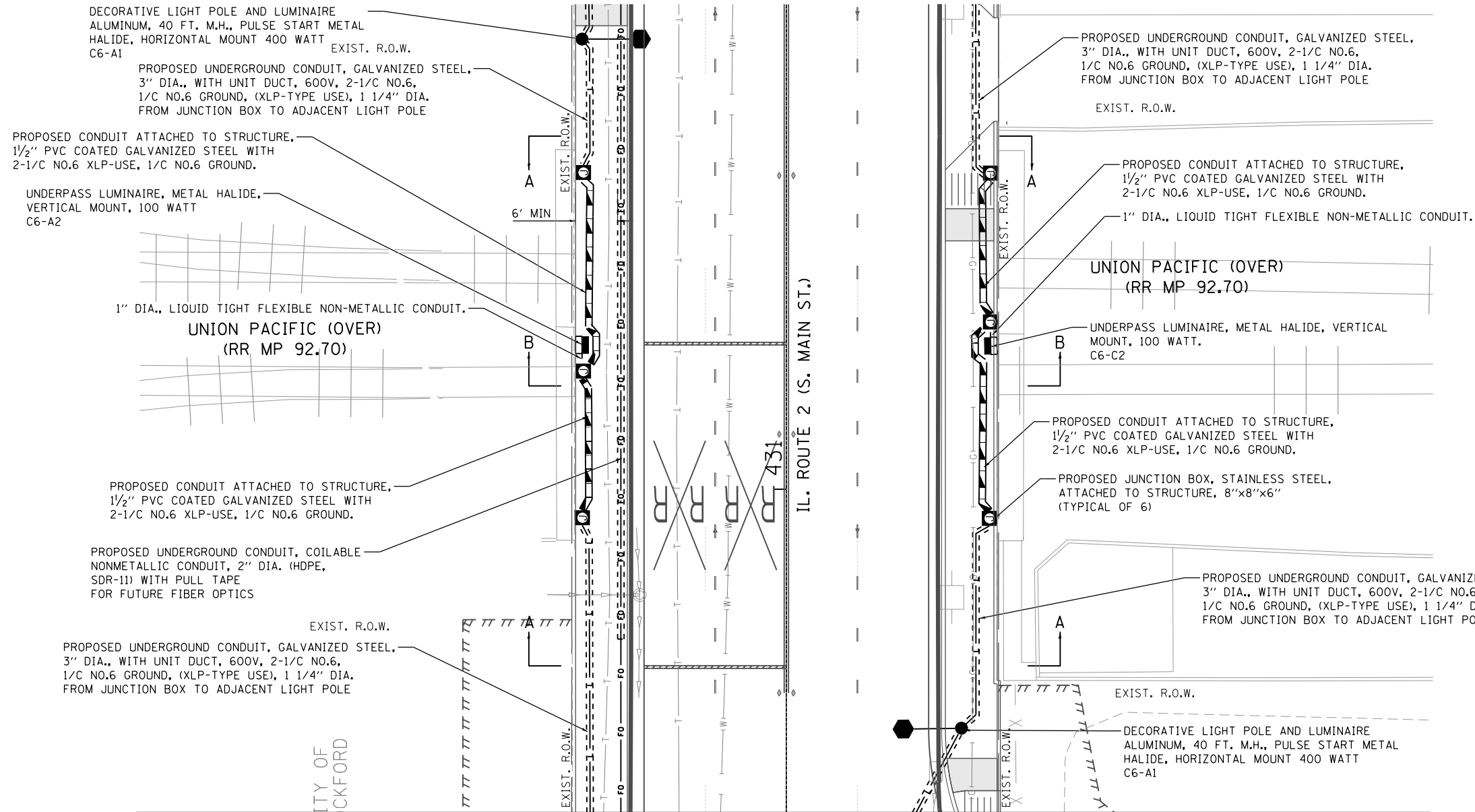
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

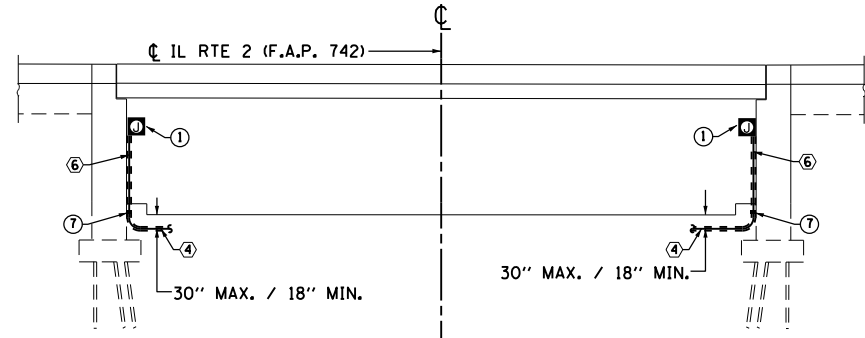
**ROADWAY LIGHTING PLAN
IL RTE 2 (SOUTH MAIN STREET)**

SCALE: 1" = 20' SHEET 17 OF 32 SHEETS STA. 432+00 TO STA. 438+00

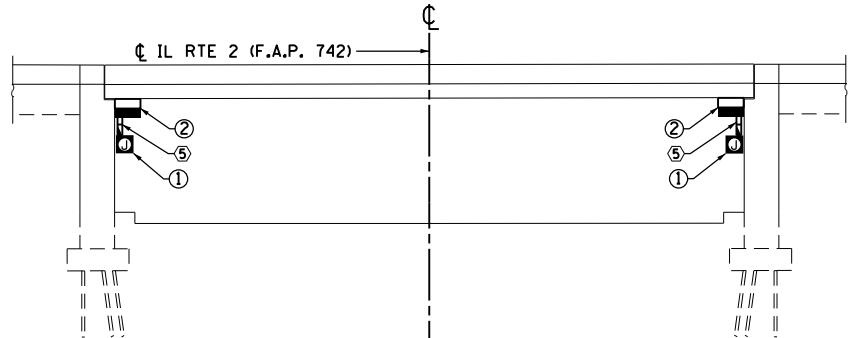
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32, 33) R-1	WINNEBAGO	705	503
CONTRACT NO. 64821			ILLINOIS FED. AID PROJECT	



IL ROUTE 2 PLAN



IL ROUTE 2 ELEVATION
(VIEW FROM NORTH/SOUTH)
SECTION A-A



IL ROUTE 2 ELEVATION
(VIEW AT CENTER)
SECTION B-B

- NOMENCLATURE:**
- ① JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 8"x8"x6" SHALL BE MOUNTED TO ABUTMENT WALL. TERMINATE UNIT DUCT AT JUNCTION BOX. (FOR MOUNTING HEIGHT RESTRICTIONS, SEE NOTE 2)
 - ② UNDERPASS LUMINAIRE, 100 WATT, METAL HALIDE, VERTICAL MOUNT.
 - ③ CONDUIT ATTACHED TO STRUCTURE, 1/2" DIA., PVC COATED, GALVANIZED STEEL.
 - ④ UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA., WITH UNIT DUCT, 600V, 2-1/C NO.6, 1/C NO.6 GROUND, (XLP-TYPE USE), 1 1/4" DIA. FROM JUNCTION BOX TO ADJACENT LIGHT POLE
 - ⑤ 1" DIA., LIQUID TIGHT FLEXIBLE NON-METALLIC CONDUIT. (INCIDENTAL TO UNDERPASS LUMINAIRE PAY ITEM)
 - ⑥ CONDUIT ATTACHED TO STRUCTURE, 3" DIA., PVC COATED, GALVANIZED STEEL.
 - ⑦ CONDUIT TRANSITION FROM GALVANIZED STEEL TO PVC COATED, GALVANIZED STEEL (INCIDENTAL TO CONDUIT ATTACHED TO STRUCTURE PAY ITEM)

LEGEND		PROPOSED UTILITY POLE (BY OTHERS)		PROPOSED UNDERGROUND CONDUIT, GALVANIZED STEEL, BORED AND PULLED, SIZE AND CONDUCTORS AS INDICATED.	
	EXISTING ROADWAY LIGHTPOLE AND LUMINAIRE TO BE REMOVED, SALVAGE		PROPOSED LIGHTING CONTROLLER, 100 AMP, 480V		HH = PROPOSED HANDHOLE PER IDOT STD 814001 CHH = PROPOSED HANDHOLE, COMPOSITE CONCRETE TYPE AS INDICATED
	PROPOSED DECORATIVE LIGHT POLE AND LUMINAIRE ALUMINUM, 40 FT. M.H., PULSE START METAL HALIDE, HORIZONTAL MOUNT 400 WATT (SEE NOTE 13 ON SHEET 487)		PROPOSED JUNCTION BOX		PROPOSED CONDUIT ATTACHED TO STRUCTURE, SIZE AND CONDUCTORS AS INDICATED
	PROPOSED UNDERPASS LUMINAIRE, METAL HALIDE, VERTICAL MOUNT, 100 WATT		PROPOSED UTILITY POLE (BY OTHERS)		PROPOSED UNIT DUCT, 600V, 2-1/C NO.6, 1/C NO.6 GROUND, (XLP-TYPE USE), 1 1/4" DIA. POLYETHYLENE
	PROPOSED CONDUIT ATTACHED TO STRUCTURE, 1/2" PVC COATED GALVANIZED STEEL WITH 2-1/C NO.6 XLP-USE, 1/C NO.6 GROUND.		PROPOSED UNDERGROUND CONDUIT, COILABLE NONMETALLIC CONDUIT, 2" DIA. WITH PULL STRING AND TRACER CABLE		PROPOSED UNDERGROUND CONDUIT, GALVANIZED STEEL, SIZE AND CONDUCTORS AS INDICATED

NOTES:

- REFER TO ROADWAY LIGHTING NOTES ON SHEET 487.
- REFER TO WALL MOUNT UNDERPASS LUMINAIRE INSTALLATION DETAILS ON SHEET 505.
- POSITION JUNCTION BOXES AT LEAST 10 FEET ABOVE THE GROUND TO PREVENT VANDALISM.

DIRECTORY : L:\ROCKFORD\118004\Draw\CADD_Sheets\DR4821_DLA...
 USER NAME : Jaseen Dove



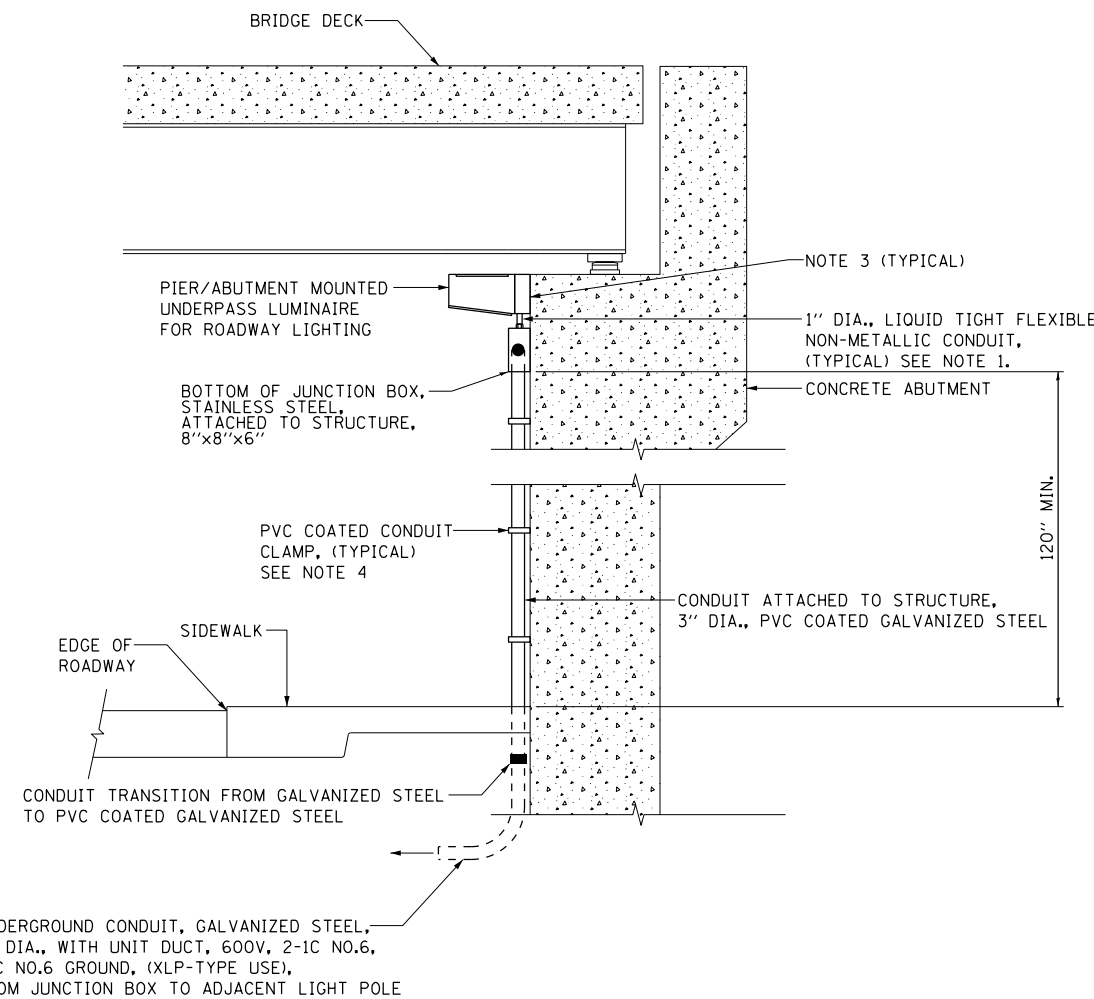
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY LIGHTING PLAN AND ELEVATION
IL RTE 2 (SOUTH MAIN STREET)**

SCALE: 1" = 10' SHEET 18 OF 32 SHEETS STA. 430+50 TO STA. 431+75

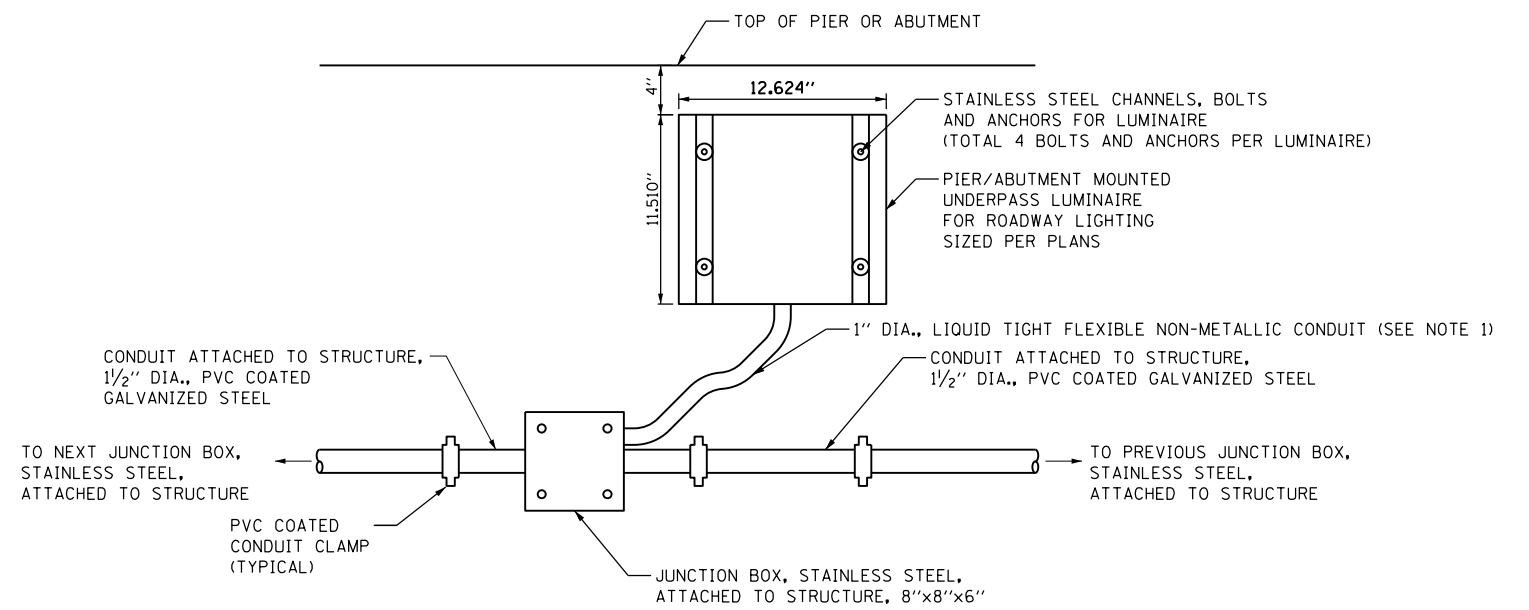
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32, 33) R-1	WINNEBAGO	705	504
CONTRACT NO. 64821			ILLINOIS FED. AID PROJECT	



TYPICAL ABUTMENT MOUNTED UNDERPASS LUMINAIRE INSTALLATION DETAILS
NOT TO SCALE



PVC COATED CONDUIT CLAMP
NOT TO SCALE



TYPICAL UNDERPASS LUMINAIRE AND JUNCTION BOX MOUNTING DETAIL
NOT TO SCALE

NOTES:

1. LIQUID TIGHT FLEXIBLE NON-METALLIC CONDUIT, MAXIMUM LENGTH 6'-0", TYPICAL FOR EACH INSTANCE AS SHOWN. PROVIDE PVC COATED RIGID GALVANIZED STEEL CONDUIT AS REQUIRED NOT TO EXCEED 6'-0" OF FLEXIBLE LIQUID TIGHT NON-METALLIC CONDUIT. LIQUID TIGHT FLEXIBLE NON-METALLIC CONDUIT WILL BE INCLUDED IN THE COST OF THE "CONDUIT ATTACHED TO STRUCTURE, OF THE CORRESPONDING DIA., PVC COATED GALVANIZED STEEL" PAY ITEM EXCEPT THAT 1" DIA. CONDUIT AND 1" DIA. FLEXIBLE CONDUIT SHALL BE INCLUDED IN THE COST OF UNDERPASS LUMINAIRE INSTALLATION.
2. UNDERPASS LUMINAIRE MOUNTED TO FACE OF PIER OR ABUTMENT WALL. MOUNTING HEIGHT OF 4" BELOW THE TOP OF PIER OR ABUTMENT WALL TYPICAL FOR ALL PIER/ABUTMENT MOUNTED UNDERPASS LUMINAIRES UNLESS OTHERWISE NOTED.
3. EXPANSION ANCHOR, POWER ACTUATED FASTENER WILL NOT BE ALLOWED. EXPANSION ANCHOR MUST BE SIZED IN ACCORDANCE WITH MANUFACTURER REQUIREMENTS.
4. SECURE THE CONDUIT WITH PVC COATED CONDUIT CLAMPS OR CONDUIT BEAM CLAMPS AS SHOWN AT 5'-0" INTERVALS FOR LATERALS AND WITHIN 2'-0" MAXIMUM FROM ANY JUNCTION BOX, FLEXIBLE CONDUIT, OR CHANGE IN DIRECTION. ALL PVC COATED CONDUIT CLAMPS OR BEAM CLAMPS SHALL BE INCLUDED WITH THE COST OF THE "CONDUIT ATTACHED TO STRUCTURE, OF THE CORRESPONDING DIA., PVC COATED GALVANIZED STEEL" PAY ITEM.
5. ATTACHING THE UNDERGROUND CONDUIT, GALVANIZED STEEL TO THE STRUCTURE SHALL BE INCLUDED IN THE COST OF THE UNDERGROUND CONDUIT, GALVANIZED STEEL PAY ITEMS.
6. ALL CONDUIT ATTACHED TO STRUCTURE SHALL BE PVC COATED RIGID STEEL CONDUIT (PVCC RGC) TYPICAL.
7. ALL CONDUIT TRANSITIONS SHALL BE INCLUDED WITH THE COST OF THE "CONDUIT ATTACHED TO STRUCTURE, OF THE CORRESPONDING DIA., PVC COATED GALVANIZED STEEL" PAY ITEM.

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 USER NAME = Jensen Dove


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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

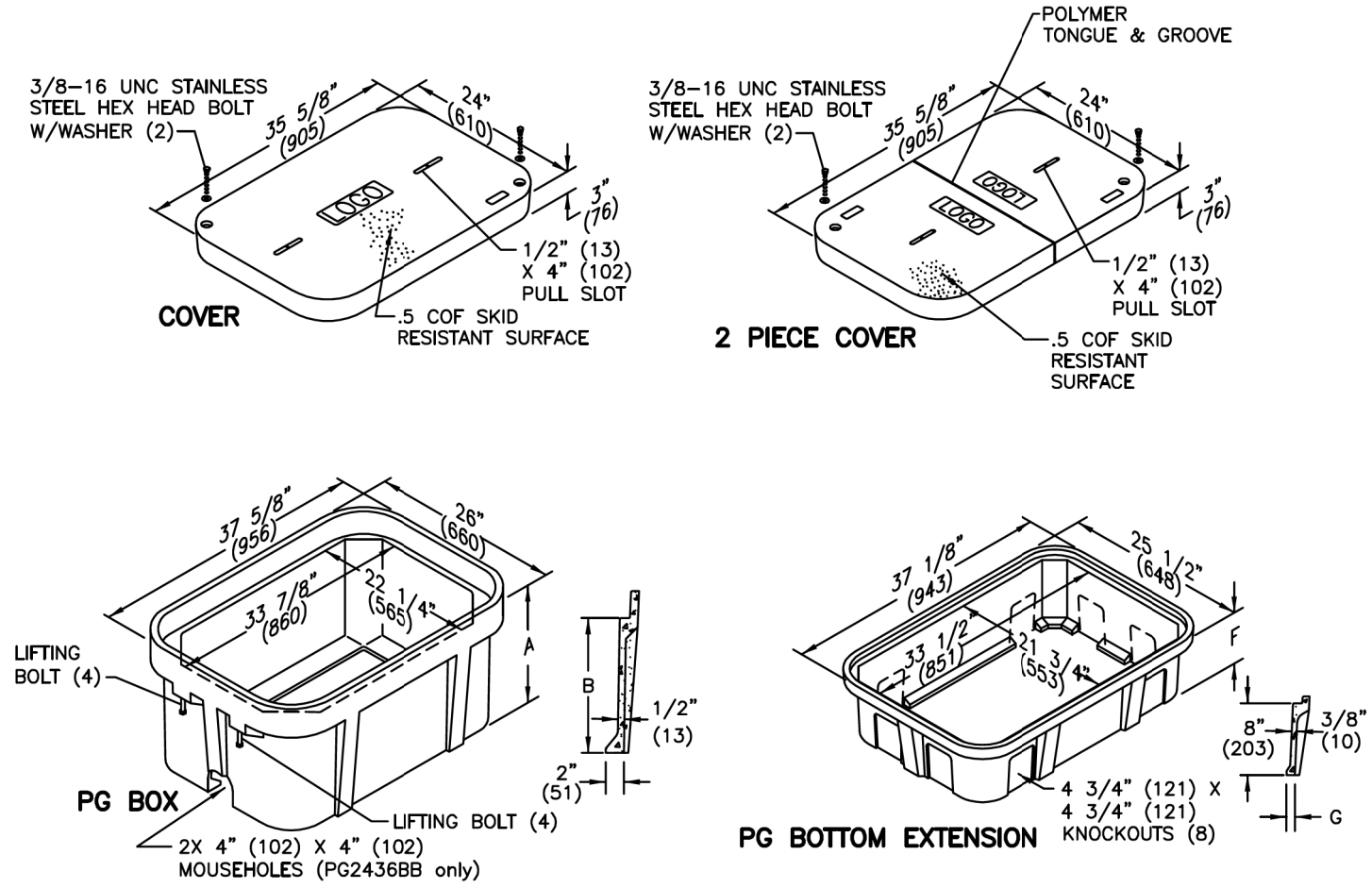
ROADWAY LIGHTING DETAIL
WALL MOUNT UNDERPASS LUMINAIRE INSTALLATION DETAILS

SCALE: N/A SHEET 19 OF 32 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32, 33) R-1	WINNEBAGO	705	505
CONTRACT NO. 64821			ILLINOIS FED. AID PROJECT	

SPECIFICATIONS/DATA

24" x 36" PG Style (Stackable) Assembly



SPECIFICATIONS/DATA

Covers (Blank unless logo is specified)

DESCRIPTION	PART NO.	WEIGHT #	DESIGN/TEST LOAD #	ANSI TIER*
W/2 Bolts	PG2436CA00	100 (45 kg)	8,000 / 12,000	8
Gasketed w/2 Bolts	PG2436CG00	100 (45 kg)	8,000 / 12,000	8
2-Piece w/2 Bolts	PG2436CS00	122 (55 kg)	8,000 / 12,000	8
No Bolts	PG2436WA00	100 (45 kg)	8,000 / 12,000	8
Heavy Duty w/2 Bolts	PG2436HA00	115 (52 kg)	15,000 / 22,500	15
Gasketed Heavy Duty w/2 Bolts	PG2436HG00	115 (52 kg)	15,000 / 22,500	15
Heavy Duty 2-Piece w/2 Bolts	PG2436HS00	122 (55 kg)	15,000 / 22,500	15
Heavy Duty w/2 Bolts	PG2436HH00	122 (55 kg)	22,500 / 33,750	22

SEE NOTE 1
 Gasketed covers and bolt grommets must be used with a gasketed box. Gaskets reduce the inflow of fluids but do not make the enclosure water tight.

PG Boxes (Stackable with self-aligning, replaceable EZ Nut) **24" - 42" Deep boxes must be used as bottom of any stack.)

DESCRIPTION	PART NO.	WEIGHT #	DIMENSION A	DIMENSION B	DESIGN/TEST LOAD #	ANSI TIER*
Open Bottom	PG2436BA18	141 (64 kg)	18" (457 mm)	15" (381 mm)	22,500 / 33,750	22
	PG2436BA24	180 (81.6 kg)	24" (610 mm)	21" (533 mm)	22,500 / 33,750	22
	PG2436BA30	196 (88.9 kg)	30" (762 mm)	27" (686 mm)	22,500 / 33,750	22
	PG2436BA36	254 (115 kg)	36" (914 mm)	33" (838 mm)	22,500 / 33,750	22
Open Bottom w/2 Mouseholes	PG2436BA42	293 (133 kg)	42" (1067 mm)	39" (991 mm)	22,500 / 33,750	22
	PG2436BB18	139 (63.1 kg)	18" (457 mm)	15" (381 mm)	22,500 / 33,750	22
	PG2436BB24	178 (80.7 kg)	24" (610 mm)	21" (533 mm)	22,500 / 33,750	22
	PG2436BB30	194 (88.0 kg)	30" (762 mm)	27" (686 mm)	22,500 / 33,750	22
Solid Bottom	PG2436BB36	252 (114 kg)	36" (914 mm)	33" (838 mm)	22,500 / 33,750	22
	PG2436BB42	293 (133 kg)	42" (1067 mm)	39" (991 mm)	22,500 / 33,750	22
	PG2436DA18	171 (78 kg)	18 1/2" (470 mm)	15" (381 mm)	22,500 / 33,750	22
	PG2436DA24	228 (103.4 kg)	24 1/2" (622 mm)	21" (533 mm)	22,500 / 33,750	22
Solid Bottom	PG2436DA30	238 (107.0 kg)	30 1/2" (775 mm)	27" (686 mm)	22,500 / 33,750	22
	PG2436DA36	282 (128 kg)	36 1/2" (927 mm)	33" (838 mm)	22,500 / 33,750	22
	PG2436DA42	321 (146 kg)	42 1/2" (1080 mm)	39" (991 mm)	22,500 / 33,750	22

SEE NOTE 1

NOTE:

1. OR APPROVED EQUAL

Extensions (For use under 18" deep box only, one per box.)

DESCRIPTION	PART NO.	WEIGHT #	DIMENSION F	DIMENSION G	DESIGN/TEST LOAD #	ANSI TIER*
Open Bottom	PG2436EA08	81 (37 kg)	8 3/4" (222 mm)	1" (25 mm)	22,500 / 33,750	22
Solid Bottom	PG2436RA08	95 (43.1 kg)	9 1/4" (235 mm)	N/A	22,500 / 33,750	22

Dimensions & weights in parentheses are metric equivalent.

* Loadings comply with ANSI/SCTE 77

**HANDHOLES
 24"X36" PG STYLE (STACKABLE) ASSEMBLY**

DIRECTORY = L:\ROCKFORD\118004\Draw\CA00_Sheet\2012\12.dwg
 USER NAME = Jason Dove



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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

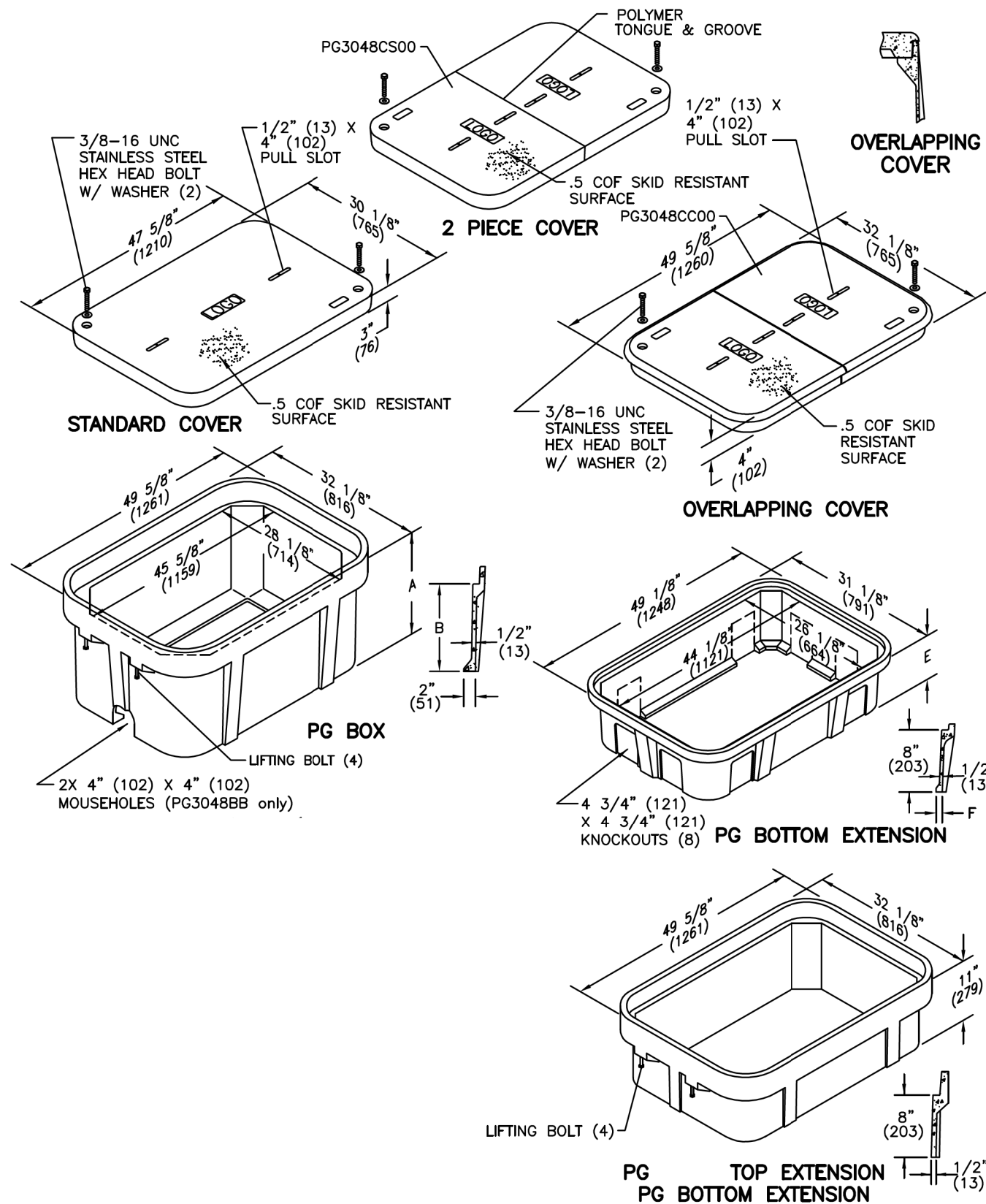
**ROADWAY LIGHTING DETAIL
 HANDHOLE, COMPOSITE CONCRETE, TYPE 1**

SCALE: N/A SHEET 20 OF 32 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32, 33) R-1	WINNEBAGO	705	506
CONTRACT NO. 64821			ILLINOIS FED. AID PROJECT	

SPECIFICATIONS/DATA

30" x 48" PG Style (Stackable) Assembly



SPECIFICATIONS/DATA

30" x 48" PG Style (Stackable) Assembly

Covers (Blank unless logo is specified)

DESCRIPTION	PART NO.	WEIGHT #	DESIGN/TEST LOAD	ANSI TIER*
W/2 Bolts	PG3048CA00	159 (72.1 kg)	8,000 / 12,000	8
Gasketed w/2 Bolts	PG3048CG00	159 (72.1 kg)	8,000 / 12,000	8
No Bolts	PG3048WA00	159 (72.1 kg)	8,000 / 12,000	8
Heavy Duty w/2 Bolts	PG3048HA00	206 (93.4 kg)	15,000 / 22,500	15
Gasketed Heavy Duty w/2 Bolts	PG3048HG00	206 (93.4 kg)	15,000 / 22,500	15
Extra Heavy Duty w/2 Bolts	PG3048HA00	220 (99.7 kg)	22,500 / 33,750	22
2-piece w/2 Bolts	PG3048CS00	181 (82.1 kg)	8,000 / 12,000	8
Heavy Duty 2-piece w/2 Bolts	PG3048HS00	206 (93.4 kg)	15,000 / 22,500	15
2-piece Overlapping	PG3048CC00	248 (112.0 kg)	8,000 / 12,000	8
Heavy Duty 2-piece Overlapping	PG3048HC00	248 (112.0 kg)	15,000 / 22,500	15

SEE NOTE 1

Gasketed covers and bolt grommets must be used with a gasketed box. Gaskets reduce the inflow of fluids but do not make the enclosure water tight.

PG BOXES (Stackable with self-aligning, replaceable EZ-Nut) *24" & 36" deep boxes must be used as bottom of any stack

DESCRIPTION	PART NO.	WEIGHT #	DIMENSION A	DIMENSION B	DESIGN/TEST LOAD #	ANSI TIER*
Open Bottom	PG3048BA18	185 (83.9 kg)	18" (457 mm)	15" (381 mm)	22,500 / 33,750	22
	PG3048BA24	236 (107.0 kg)	24" (609 mm)	21" (533 mm)	22,500 / 33,750	22
	PG3048BA36	343 (155.6 kg)	36" (914 mm)	33" (838 mm)	22,500 / 33,750	22
Open Bottom w/ Gasket	PG3048BG18	185 (83.9 kg)	18" (457 mm)	15" (381 mm)	22,500 / 33,750	22
	PG3048BG24	236 (107.0 kg)	24" (609 mm)	21" (533 mm)	22,500 / 33,750	22
	PG3048BG36	343 (155.6 kg)	36" (914 mm)	33" (838 mm)	22,500 / 33,750	22
Open Bottom w/ 2 Mouseholes	PG3048BB18	185 (83.9 kg)	18" (457 mm)	15" (381 mm)	22,500 / 33,750	22
	PG3048BB24	236 (107.0 kg)	24" (610 mm)	21" (533 mm)	22,500 / 33,750	22
	PG3048BB36	343 (155.6 kg)	36" (914 mm)	33" (838 mm)	22,500 / 33,750	22
Solid Bottom	PG3048DA18	220 (99.8 kg)	18 1/2" (470 mm)	15" (381 mm)	22,500 / 33,750	22
	PG3048DA24	287 (130.2 kg)	24 1/2" (622 mm)	21" (533 mm)	22,500 / 33,750	22
	PG3048DA36	394 (178.7 kg)	36 1/2" (927 mm)	33" (838 mm)	22,500 / 33,750	22
Solid Bottom w/ Gasket	PG3048DG18	220 (99.8 kg)	18 1/2" (470 mm)	15" (381 mm)	22,500 / 33,750	22
	PG3048DG24	287 (130.2 kg)	24 1/2" (622 mm)	21" (533 mm)	22,500 / 33,750	22
	PG3048DG36	394 (178.7 kg)	36 1/2" (927 mm)	33" (838 mm)	22,500 / 33,750	22

SEE NOTE 1

NOTE:

1. OR APPROVED EQUAL

Top Extension (For use on top of PG boxes of any depth)

DESCRIPTION	PART NO.	WEIGHT #	DESIGN/TEST LOAD #	ANSI TIER*
Open Bottom*	PG3048EA11	100 (45.4 kg)	22,500 / 33,750	22

* In addition, this extension can be used as a bottom extension for 18" deep PG boxes.

Bottom Extensions (For use under 18" deep PG style box only, one per box)

DESCRIPTION	PART NO.	WEIGHT #	DIMENSION E	DIMENSION F	DESIGN/TEST LOAD #	ANSI TIER*
Open Bottom	PG3048EA08	102 (46.3 kg)	8 3/4" (222 mm)	1" (25 mm)	22,500 / 33,750	22
Solid Bottom	PG3048RA08	151 (58.0 kg)	9 1/4" (235 mm)	N/A	22,500 / 33,750	22

Dimensions & weights in parentheses are metric equivalent.

* Loadings comply with ANSI/SCTE 77

HANDHOLES
30"X48" PG STYLE (STACKABLE) ASSEMBLY

DIRECTORY: L:\ROCKFORD\118004\Draw\CA00_Sheets\DR48Z_L.DWG
 USER NAME: J. Jensen Dove

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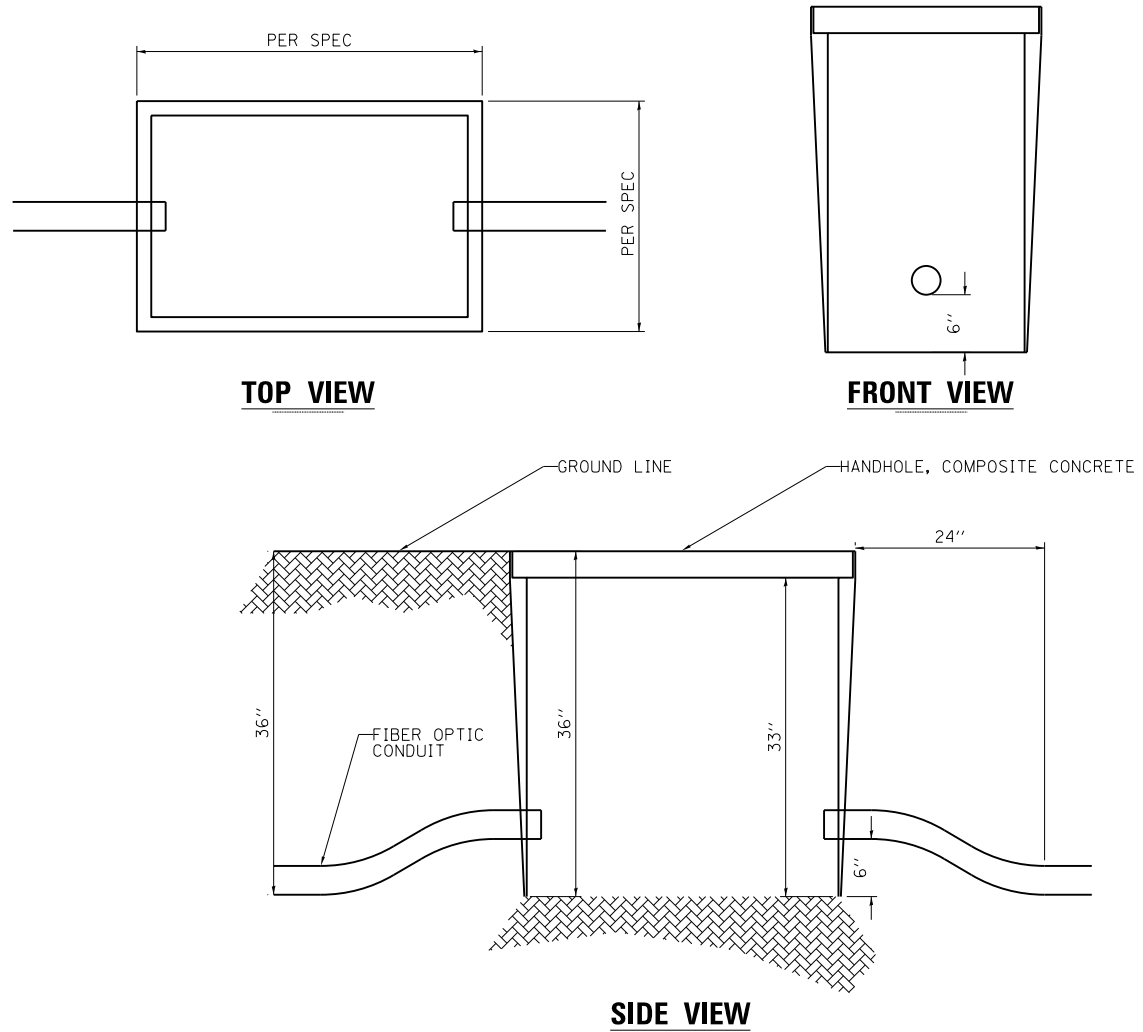
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY LIGHTING DETAIL
HANDHOLE, COMPOSITE CONCRETE, TYPE 2

SCALE: N/A SHEET 21 OF 32 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32, 33) R-1	WINNEBAGO	705	507
CONTRACT NO. 64821			ILLINOIS FED. AID PROJECT	



FIBER OPTIC HANDHOLE

N.T.S. 1

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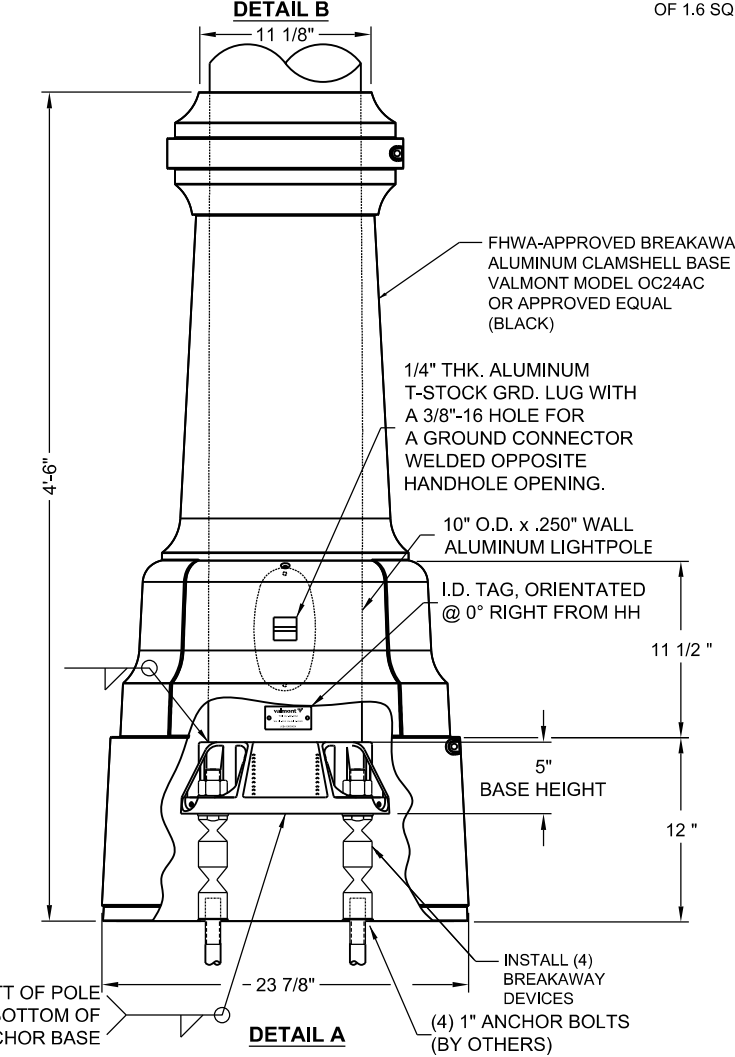
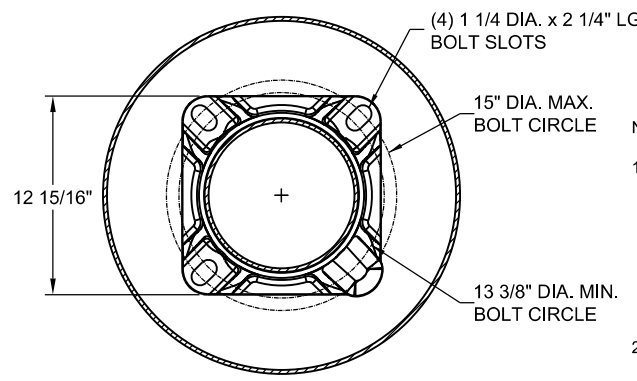
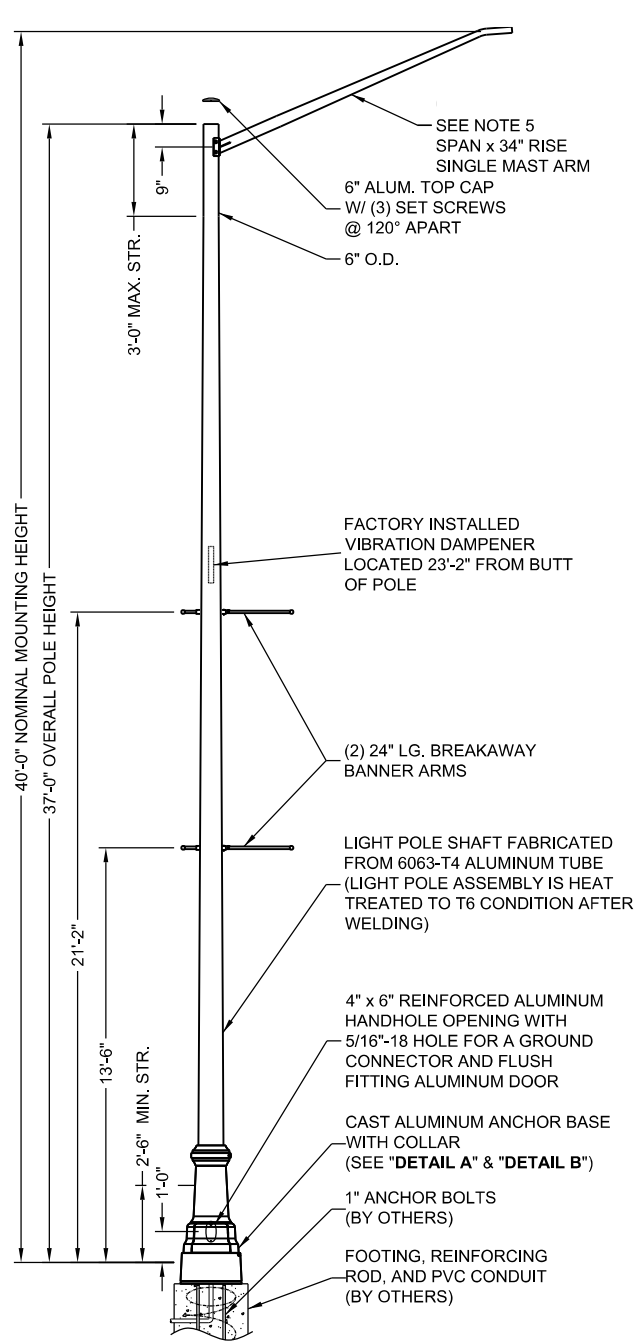
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY LIGHTING DETAIL
FIBER OPTIC HANDHOLE

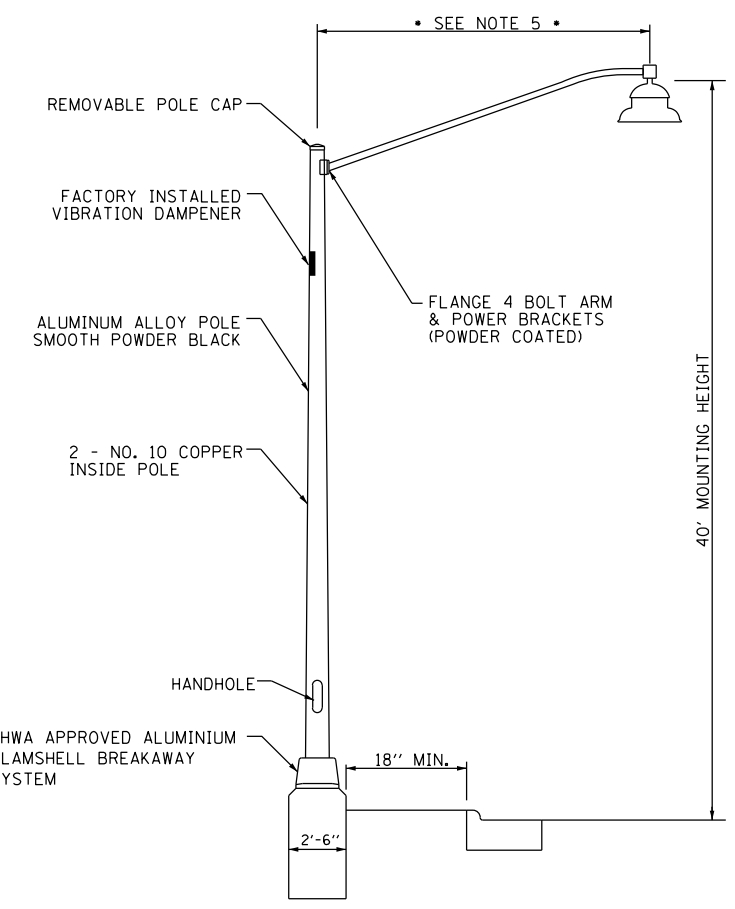
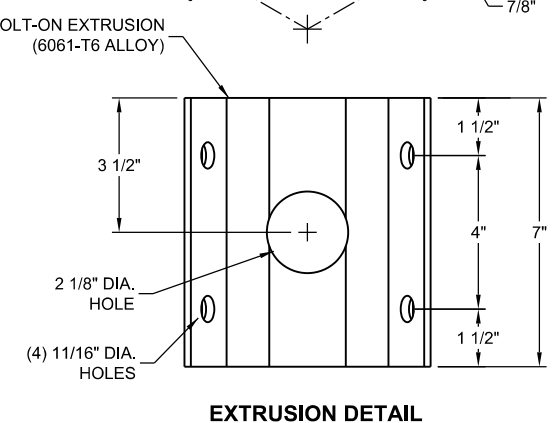
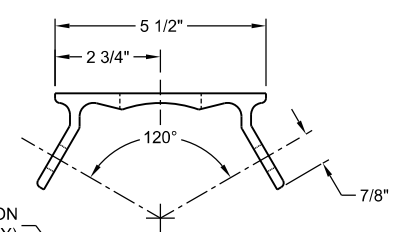
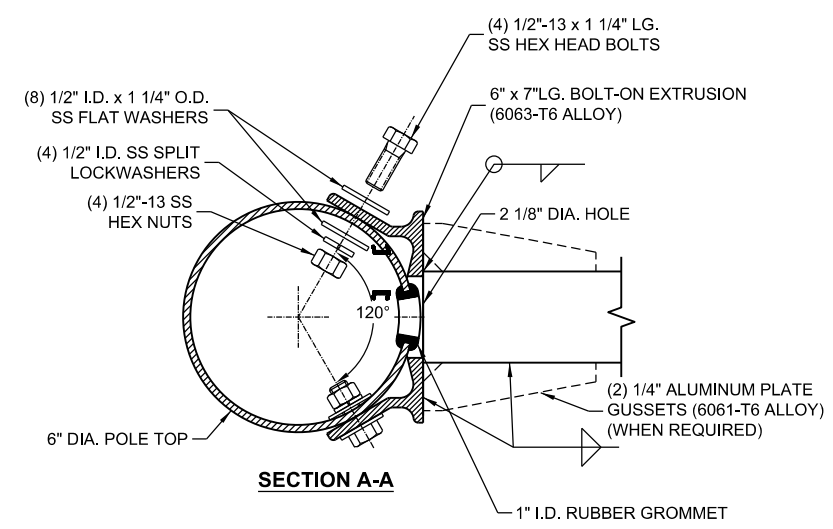
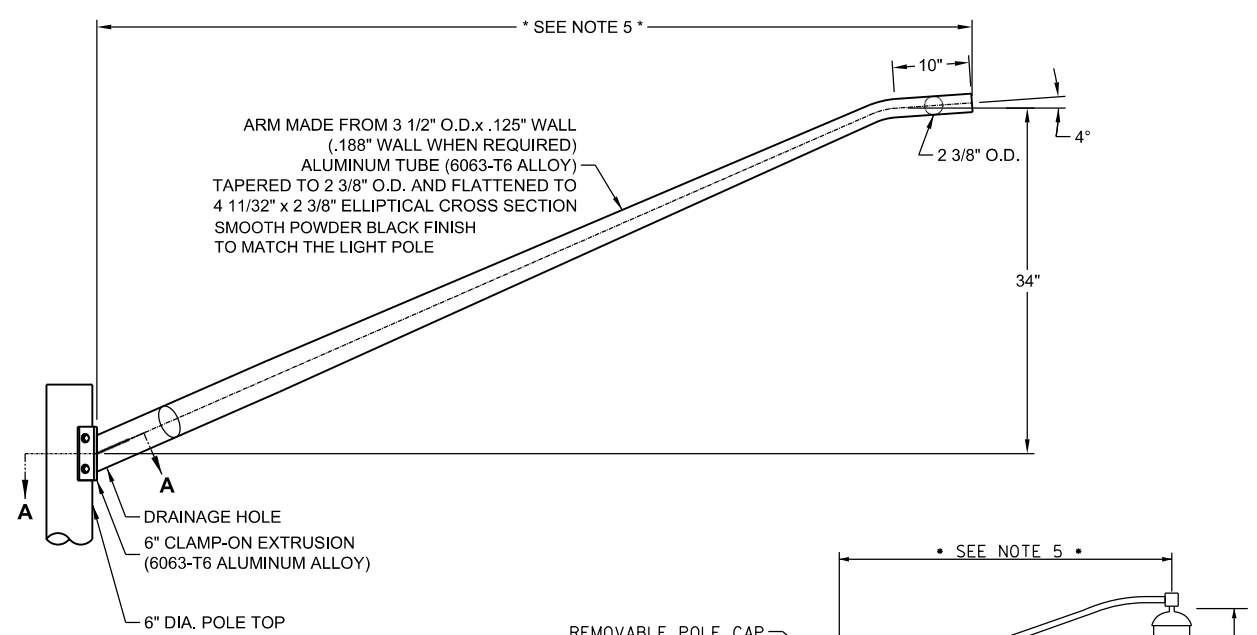
SCALE: N/A SHEET 22 OF 32 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32, 33) R-1	WINNEBAGO	705	508
ILLINOIS FED. AID PROJECT			CONTRACT NO. 64821	



NOTES:

1. UNITS SHALL BE MANUFACTURED ACCORDING TO AASHTO "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS" CURRENT AT THE TIME THE PROJECT IS ADVERTISED. LIGHT POLES SHALL BE DESIGNED FOR 90 MPH WIND VELOCITY AND A MINIMUM DESIGN LIFE OF 50 YEARS.
2. POLES SHALL BE DESIGNED TO WITHSTAND LOADING OF 75 LB. LUMINAIRE WITH AN EPA OF 1.6 SQ. FT. INCLUDING ALL ATTACHMENTS.



LIGHT POLE, SPECIAL
NOT TO SCALE

LIGHT POLE, SPECIAL, LUMINAIRE SCHEDULE					
TYPE	DESCRIPTION	MANUFACTURER	POLE	LAMP	VOLTAGE
LIGHT POLE, SPECIAL	DOMUS 55 SERIES, TYPE III CUTOFF CATALOG DMS55-400PSMH-SG3-480V	LUMEC	VALMONT • 40 FT M.H., WITH CLAMHELL BASE ON CONCRETE FOUNDATION MAST ARM LENGTH AS SPECIFIED	400W M.H. PULSE START	480 V

• OR APPROVED EQUAL

NOTE:

1. WELDING WIRE (ER4043).
2. MAST ARM HEAT TREATED AFTER FABRICATION TO T6 CONDITION.
3. ALL HARDWARE IS 300 SERIES STAINLESS STEEL, COATED WITH ANTI-SEIZE COMPOUND.

NOTES:

1. REFER TO ROADWAY LIGHTING DETAIL FOR LIGHT POLE FOUNDATION DETAIL (BE-301).
2. REFER TO ROADWAY LIGHTING DETAIL POLE WIRING DETAIL (BE-702)
3. REFER TO ROADWAY LIGHTING DETAIL CONDUIT TRENCH - PHOTOCELL MOUNTING
4. REFER TO DETAIL A ON THIS SHEET.
5. SPAN LENGTH SHALL BE 6', 8', 10' OR 12'. REFER TO NOTE 13, SHEET 487 FOR WHICH LENGTH.

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 USER NAME: Jansen Dove



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

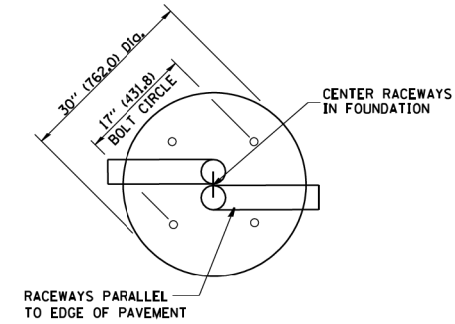
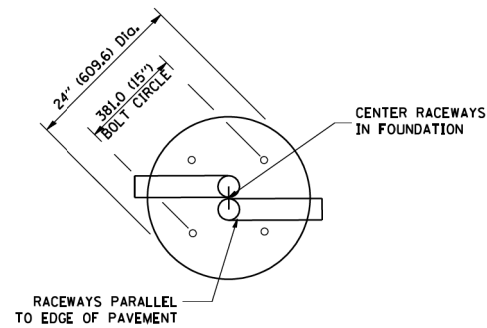
ROADWAY LIGHTING DETAIL
DECORATIVE LIGHT POLES - LUMINAIRE SCHEDULE

SCALE: N/A SHEET 23 OF 32 SHEETS STA. TO STA.

F.A.P. RTE. 742	SECTION (32, 33) R-1	COUNTY WINNEBAGO	TOTAL SHEETS 705	SHEET NO. 509
CONTRACT NO. 64821			ILLINOIS FED. AID PROJECT	

LIGHT POLE FOUNDATION DEPTH TABLE
40 FT. (12.192 m) TO 47.5 FT. (14.478 m) MOUNTING HEIGHT

SOIL CONDITIONS	DESIGN DEPTH "D" OF FOUNDATION	
	SINGLE ARM POLE	TWIN ARM POLE
SOFT CLAY O _u = 0.375 TON/SO. FT.	13'-0" (3.96 m)	15'-0" (4.57 m)
MEDIUM CLAY O _u = 0.75 TON/SO.FT	9'-6" (2.93 m)	10'-9" (3.23 m)
STIFF CLAY O _u = 1.50 TON/SO. FT.	7'-0" (2.13 m)	8'-0" (2.44 m)
LOOSE SAND φ = 34°	9'-0" (2.74 m)	10'-0" (3.05 m)
MEDIUM SAND φ = 37.5°	8'-3" (2.52 m)	9'-0" (2.74 m)
DENSE SAND φ = 40°	7'-9" (2.36 m)	(2.74 m)

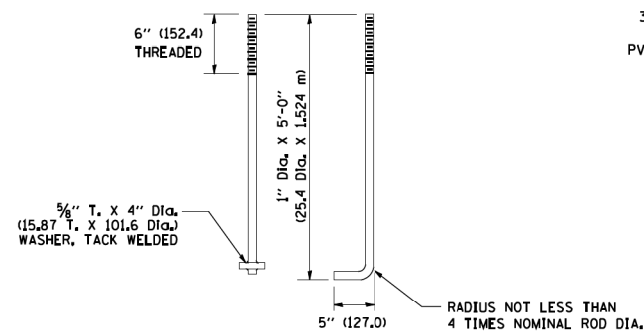


TOP VIEW

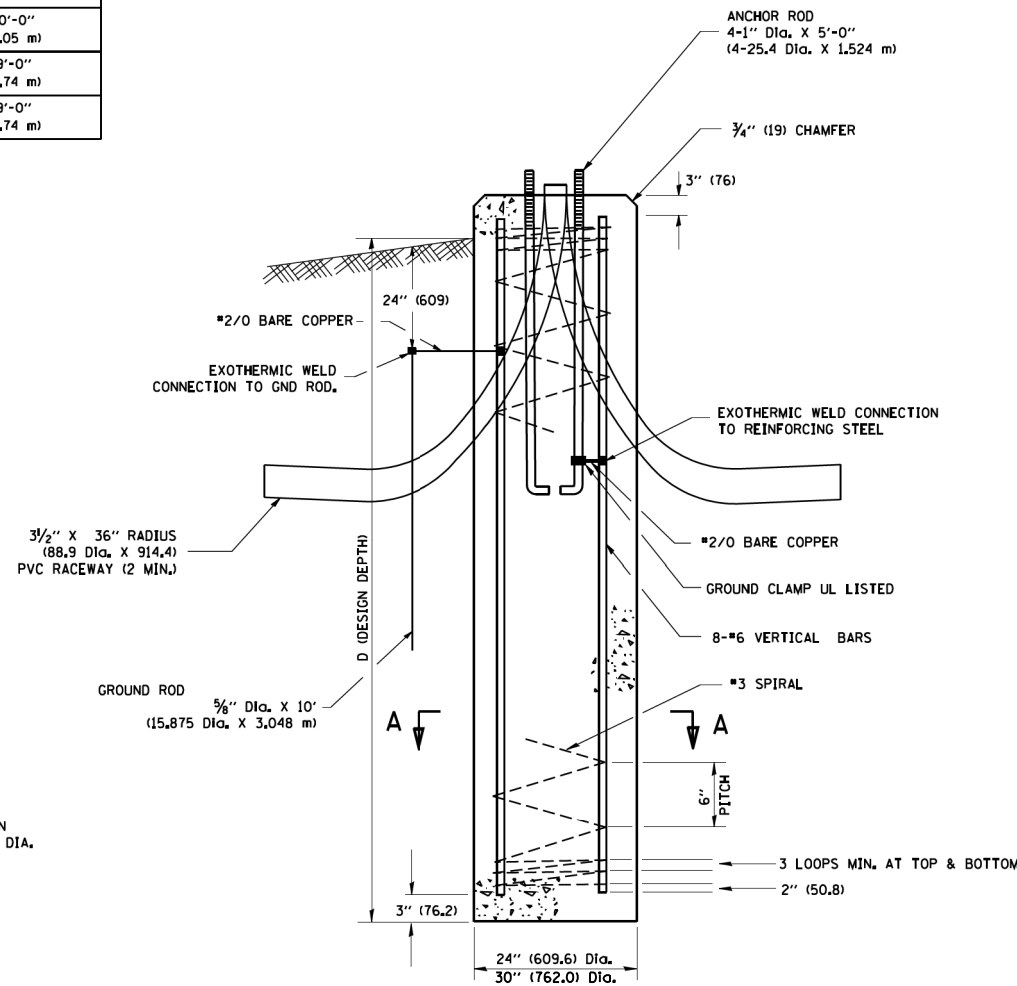
TOP VIEW

NOTES

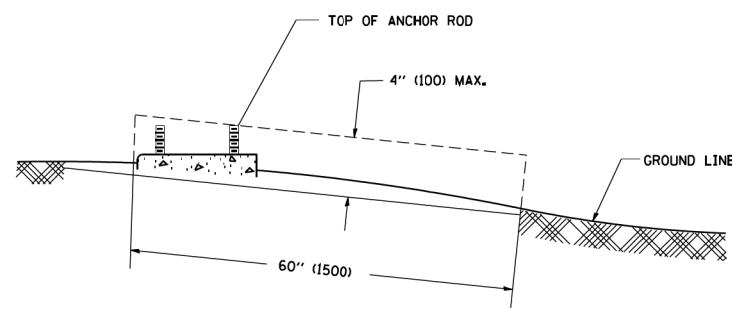
- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- THE ANCHOR RODS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED.
- THE FOUNDATION SHALL NOT PROTRUDE MORE THAN 4 IN. (100 MM) ABOVE THE FINISHED GRADE WITHIN A 60 IN. (1.5 m) CHORD ACROSS THE FOUNDATION, WITH ANCHOR RODS INCLUDED, IN ACCORDANCE WITH AASHTO GUIDELINES, IF THE FOUNDATION HEIGHT, INCLUDING ANCHOR RODS, EXTENDS BEYOND THESE SPECIFIED LIMITS, THE FOUNDATION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE. SEE FOUNDATION EXTENSION DETAIL.
- THE HOLE FOR THE FOUNDATION SHALL BE MADE BY DRILLING WITH AN AUGER, OF THE SAME DIAMETER AS THE FOUNDATION. IF SOIL CONDITIONS REQUIRE THE USE OF A LINER TO FORM THE HOLE, THE LINER SHALL BE WITHDRAWN AS THE CONCRETE IS DEPOSITED.
- THE TOP OF THE FOUNDATION SHALL BE CONSTRUCTED LEVEL. A LINER OR FORM SHALL BE USED TO PRODUCE A UNIFORM SMOOTH SIDE TO THE TOP OF THE FOUNDATION. FOUNDATION TOP SHALL BE CHAMFERED 3/4-IN. (20 mm).
- THE CONCRETE SHALL BE CLASS SI. CONCRETE SHALL CURE ACCORDING TO ARTICLE 1020.13 BEFORE LIGHT POLES ARE INSTALLED.
- THE ANCHOR ROD SHALL BE A HOOK ROD TYPE. COLD BENDING OF THE ANCHOR ROD WILL NOT BE ALLOWED. THE RADIUS OF THE HOOK BEND SHALL NOT BE LESS THAN 4 TIMES THE NOMINAL DIAMETER OF THE ANCHOR ROD. A TACK WELDED ANCHOR ROD MAY BE SUBSTITUTED WITH THE APPROVAL OF THE ENGINEER.
- THE ANCHOR RODS SHALL BE ACCORDING TO ASTM F1554 GRADE 725 (GRADE 105). NUTS SHALL BE HEXAGON NUTS ACCORDING TO ASTM A 194 2H OR ASTM A 563 DH, AND WASHERS SHALL BE ACCORDING TO ASTM F 436.
- ANCHOR RODS, NUTS AND WASHERS SHALL BE COMPLETELY GALVANIZED BY EITHER THE HOT-DIPPED PROCESS CONFORMING WITH AASHTO M 232, THE MECHANICAL PLATING METHOD CONFORMING TO AASHTO M 298, CLASS 50 WITH A MAXIMUM COATING THICKNESS OF 150 UMG (MILS) OR THE ELECTROLYTIC PROCESS ACCORDING TO ASTM F 1136.
- THE ANCHOR RODS SHALL BE THREADED A MINIMUM OF 6 INCHES (150 mm) WITH A MINIMUM OF 3 INCHES (75 mm) OF THREADED ANCHOR ROD EMBEDDED IN THE FOUNDATION.
- ANCHOR RODS SHALL PROJECT 2 3/4" (69.9 mm) ABOVE THE TOP OF THE FOUNDATION, IF BREAKAWAY COUPLINGS ARE SPECIFIED, THE CONTRACTOR SHALL CAREFULLY COORDINATE THE ANCHOR ROD PROJECTION WITH THE INSTALLATION REQUIREMENTS OF THE BREAKAWAY COUPLINGS.
- THE CONTRACTOR SHALL USE A #3 SPIRAL AT 6" (152.4 mm) PITCH OR MAY SUBSTITUTE #3 TIES AT 12" (304.8 mm) O.C. WITH THE APPROVAL OF THE ENGINEER.
- THE CABLE TRENCHES AND FOUNDATION SHALL BE BACK FILLED AND COMPACTED AS SPECIFIED BEFORE THE LIGHT POLE IS ERECTED.
- THE RACEWAYS SHALL PROJECT 1" (25.4 mm) ABOVE THE TOP OF THE FOUNDATION.



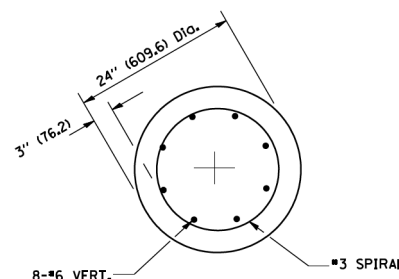
ANCHOR ROD DETAIL



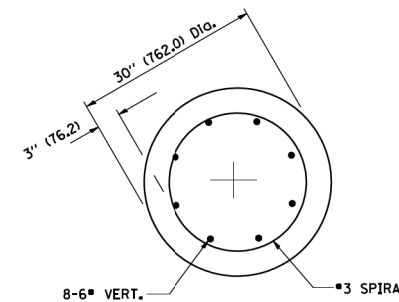
FOUNDATION DETAIL



FOUNDATION EXTENSION DETAIL



SECTION A-A



SECTION A-A

LIGHT POLE FOUNDATION
40' (12.192 m) TO 47' 1/2' (14.478 m) M.H. 15" (381 mm) BOLT CIRCLE

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 USER NAME = Josen Dove



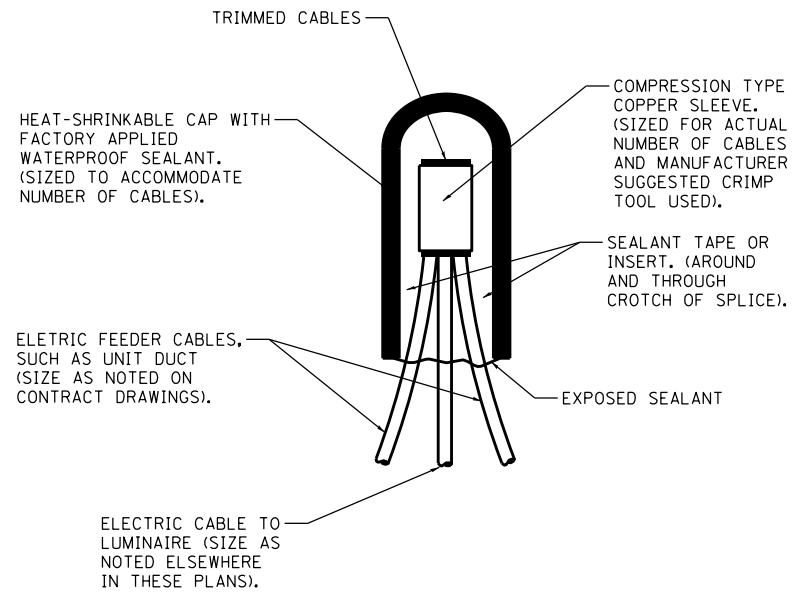
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY LIGHTING DETAIL
LIGHT POLE FOUNDATION (BE-301)

SCALE: N/A SHEET 24 OF 32 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32, 33) R-1	WINNEBAGO	705	510
			CONTRACT NO. 64821	
ILLINOIS FED. AID PROJECT				

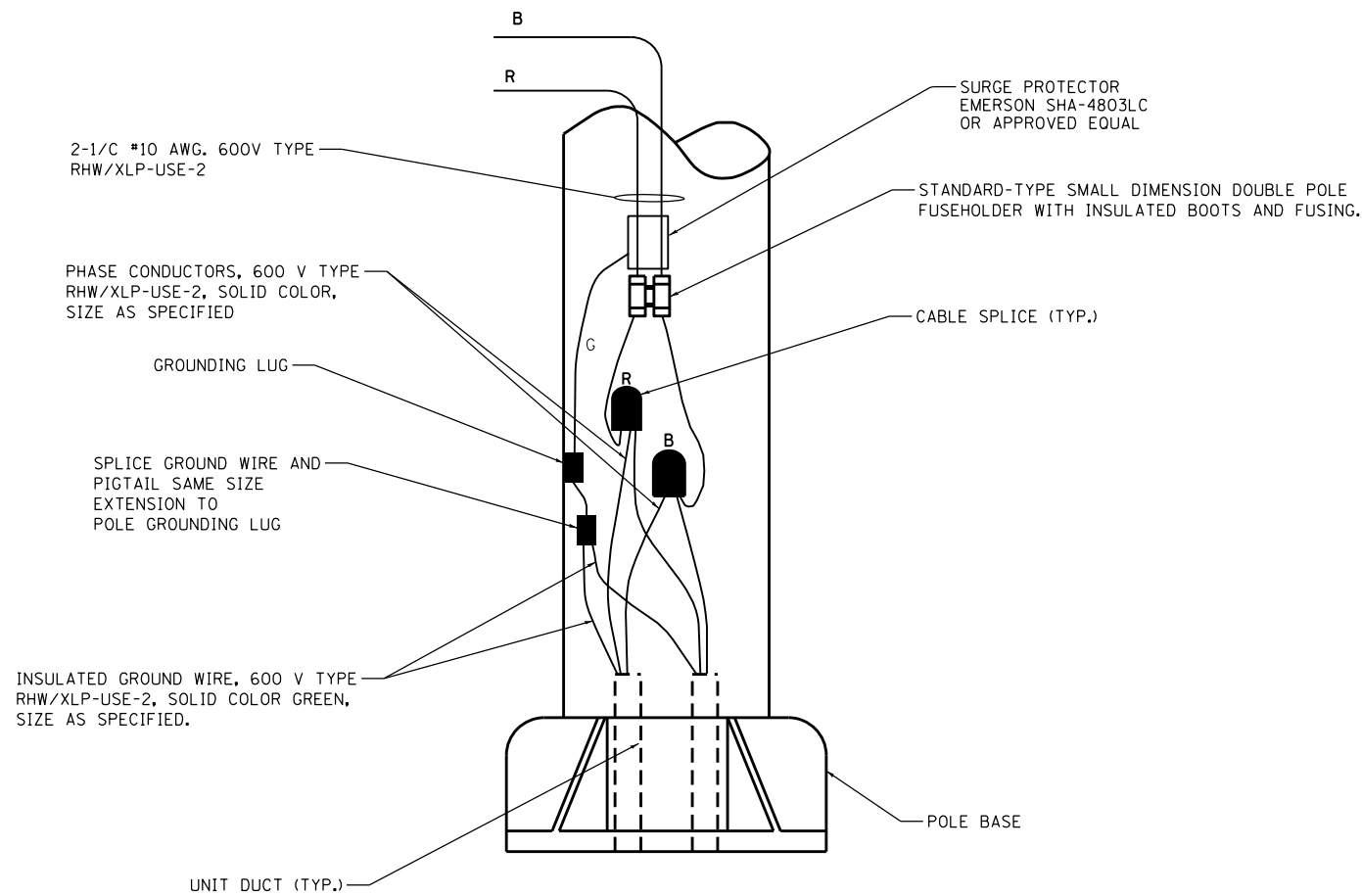


TYPICAL SPLICE DETAIL

N.T.S.

NOTES:

1. ALL TAPED SPLICES SHALL USE 2 LAYERS OF ELECTRICAL TAPE OVER 3 LAYERS OF RUBBER TAPE AS REQUIRED BY THE STANDARD SPECIFICATION. COAT THE FINISH TAPED SPLICE WITH BONDING COMPOUND.
2. ALL CABLE SPLICES SHALL BE TAPED UNLESS ANOTHER METHOD HAS BEEN SPECIFICALLY APPROVED BY THE ENGINEER.
3. THE NUMBER OF CABLES IN SPLICE MAY VARY



POLE WIRING DETAIL

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 USER NAME = Jensen Dove



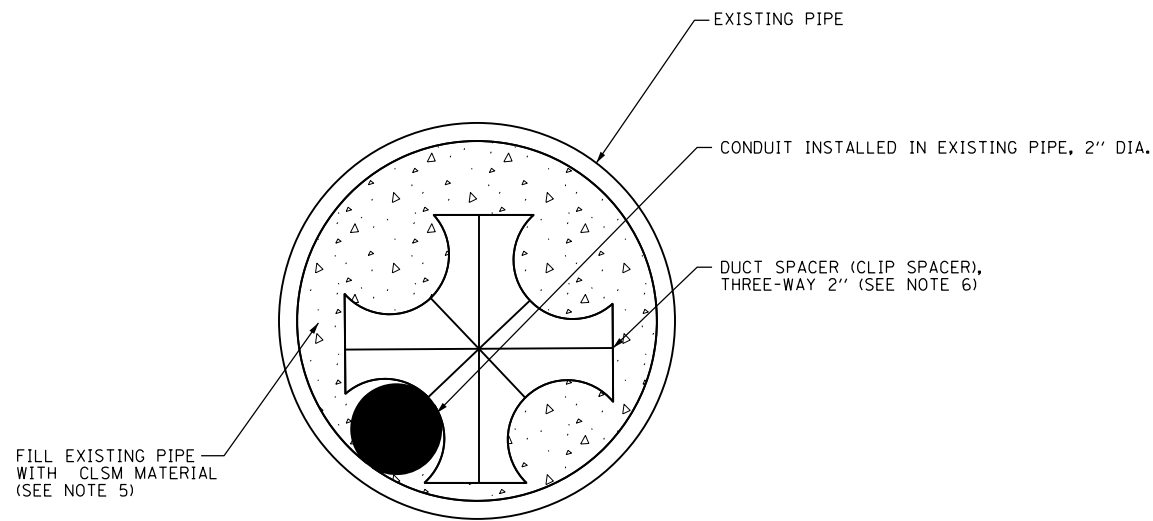
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

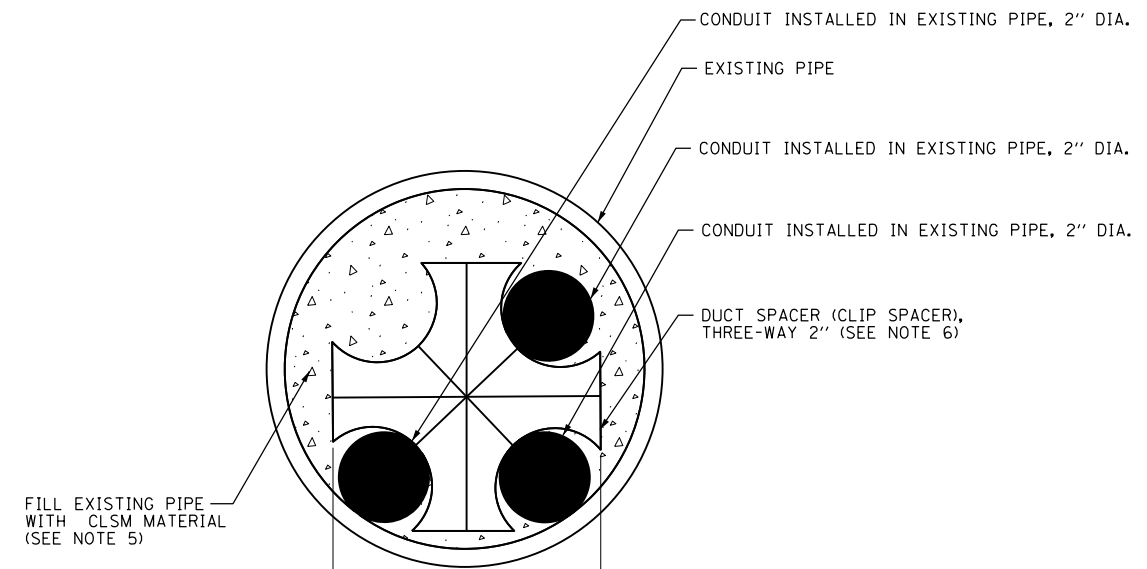
**ROADWAY LIGHTING DETAIL
TYPICAL SPLICE - POLE WIRING DETAIL (BE-702)**

SCALE: N/A SHEET 25 OF 32 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32, 33) R-1	WINNEBAGO	705	511
CONTRACT NO. 64821			ILLINOIS FED. AID PROJECT	



SINGLE CONDUIT ASSEMBLY



TRIPLE CONDUIT ASSEMBLY

NOTES:

1. USE ONE DUCT SPACER (CLIP SPACER) FOR EVERY 5 FEET OF EXISTING PIPE CASING.
2. INSTALL DUCT SPACERS PERPENDICULAR TO CONDUITS TO REDUCE THE TENDENCY OF CONDUITS TO CORKSCREW.
3. CONTRACTOR MUST COORDINATE WITH DUCT SPACER MANUFACTURER ON INSTALLATION MEANS TO PREVENT THE DUCTBANK FROM ROTATING DURING INSTALLATION. DUCT SPACERS MUST BE HELD IN PLACE WITH NON-METALLIC STRAPS AND HOLD DOWN BARS.
4. THE DUCTBANK MUST BE HELD IN POSITION AT BOTH ENDS TO ACCOMMODATE POSSIBLE UNEVEN THRUST LOADS THAT MAY BE GENERATED DURING THE GROUTING OPERATION.
5. THE COST FOR FILLING THE EXISTING PIPE WITH CLSM SHALL BE INCLUDED IN THE CONTRACT UNIT COST FOR CONDUIT INSTALLED IN EXISTING PIPE.
6. THE COST FOR DUCT SPACERS AND ANY ASSOCIATED STRAPS USED TO HOLD THE CONDUIT IN PLACE SHALL BE INCLUDED IN THE CONTRACT UNIT COST FOR CONDUIT INSTALLED IN EXISTING PIPE.

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 CONSULTING ENGINEERS
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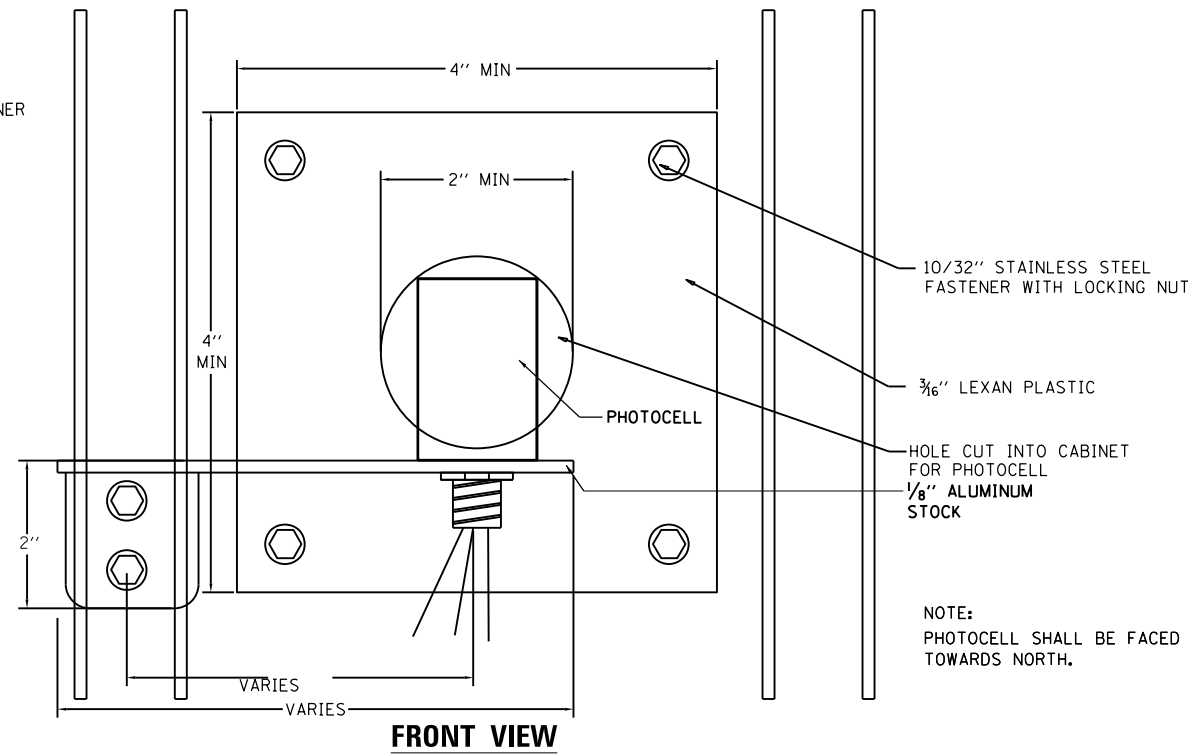
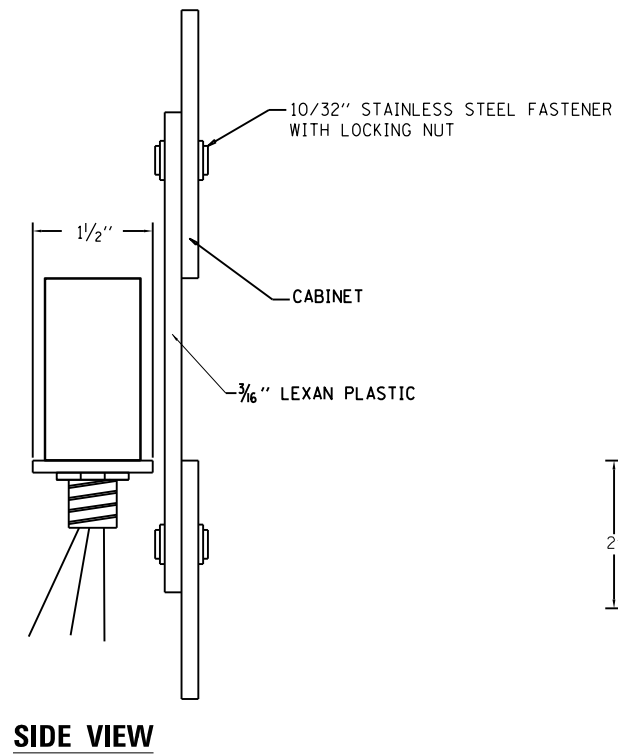
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY LIGHTING DETAIL
CONDUIT INSTALLED IN EXISTING PIPE WITH DUCT SPACERS

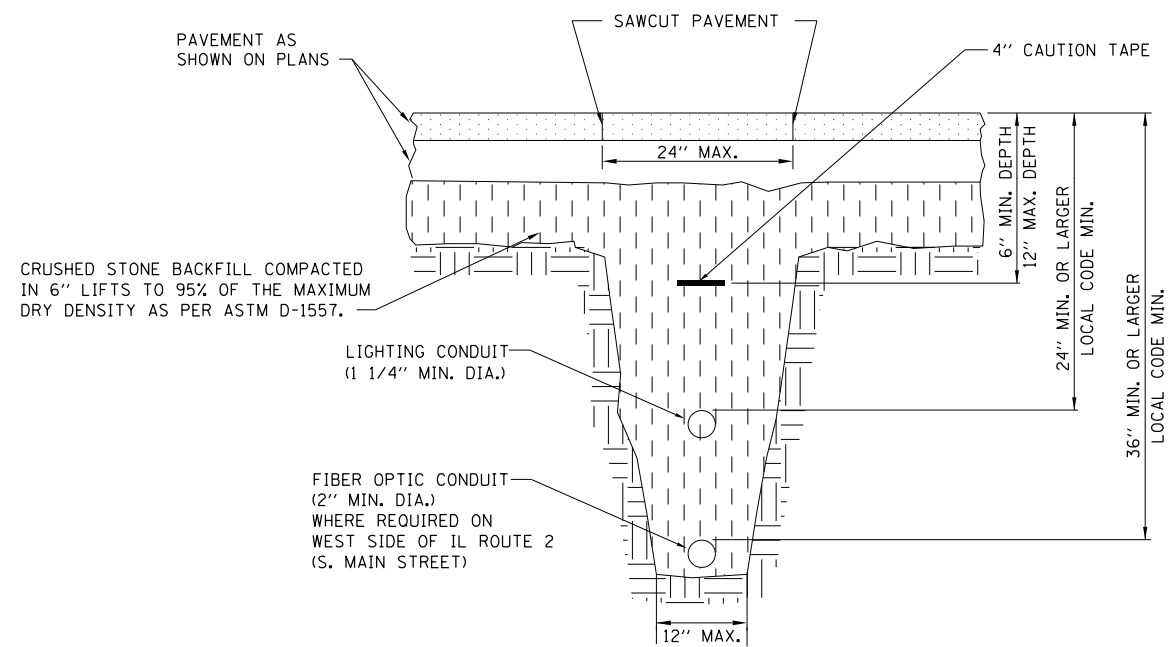
SCALE: N/A SHEET 26 OF 32 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 64821			ILLINOIS FED. AID PROJECT	

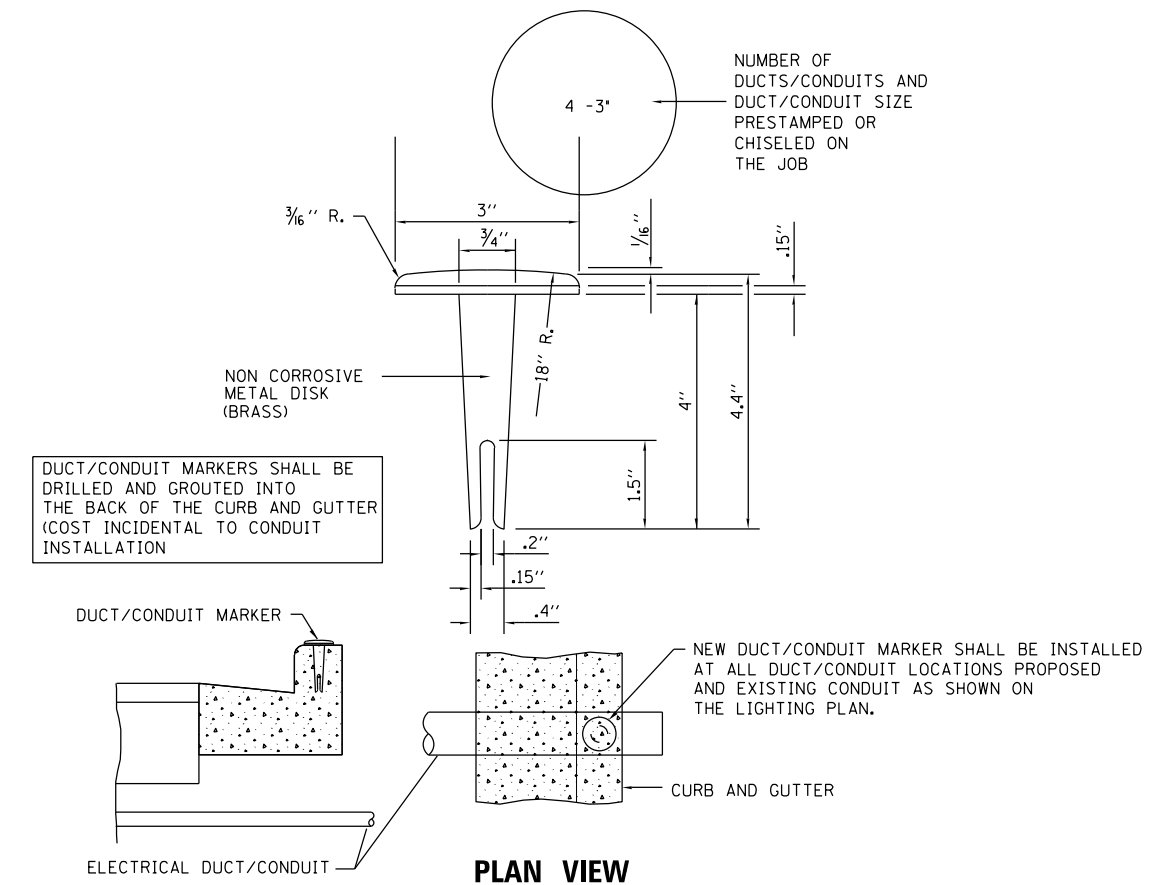


NOTE:
PHOTOCELL SHALL BE FACED TOWARDS NORTH.

PHOTOCELL MOUNTING DETAIL



FIBER OPTICS AND STREET LIGHTING CONDUIT TRENCH



DUCT MARKER DETAIL
NOT TO SCALE

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 USER NAME = Jason Dove

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CONSULTING ENGINEERS
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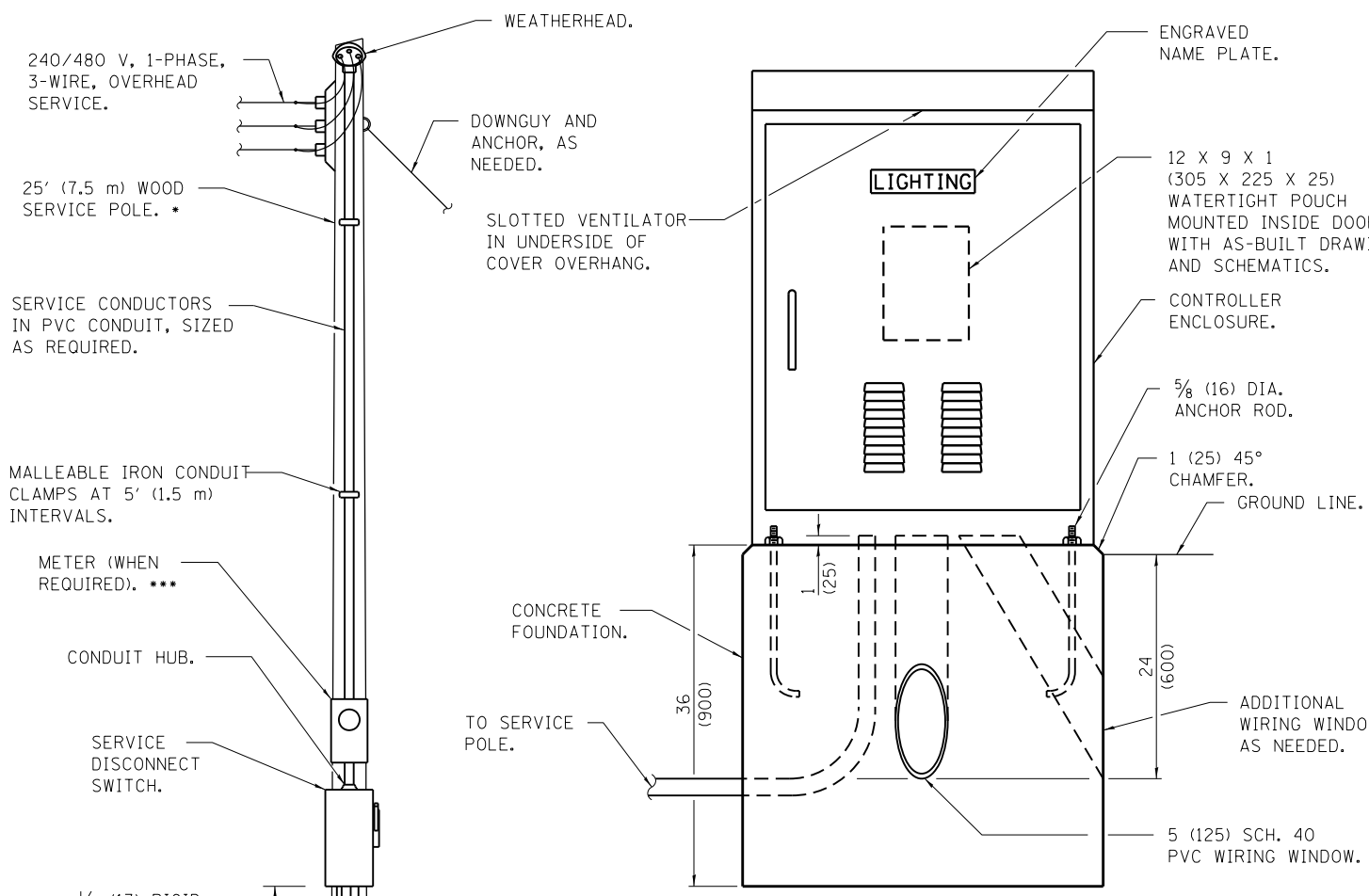
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

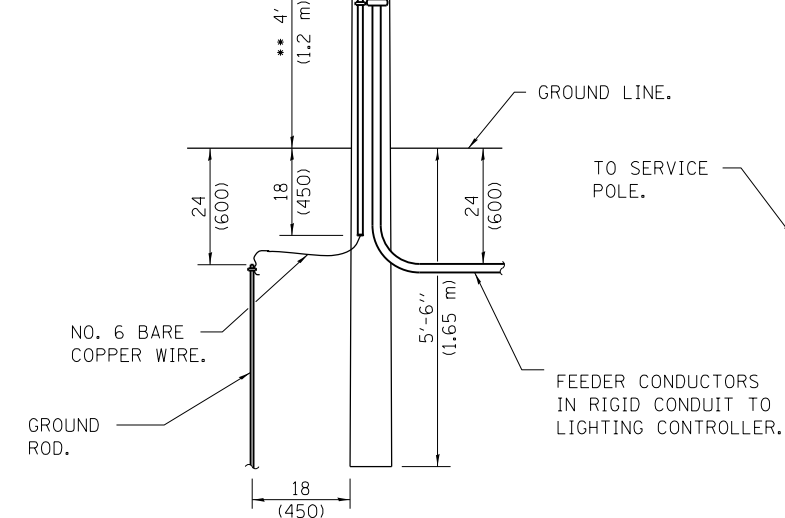
ROADWAY LIGHTING DETAIL
CONDUIT TRENCH - DUCT MARKER - PHOTOCELL MOUNTING

SCALE: N/A SHEET 27 OF 32 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32, 33) R-1	WINNEBAGO	705	513
CONTRACT NO. 64821			ILLINOIS FED. AID PROJECT	

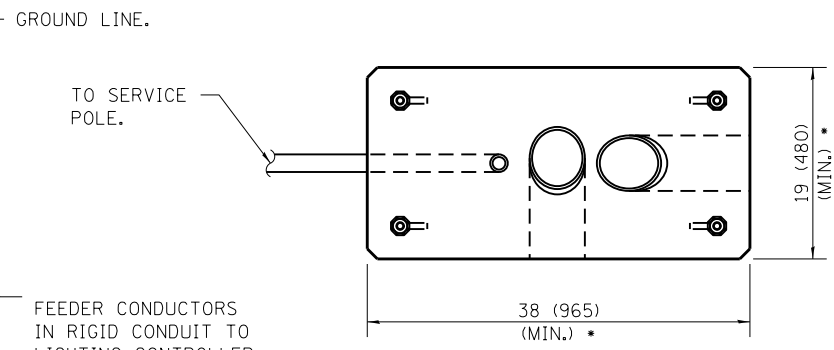


LIGHTING CONTROLLER

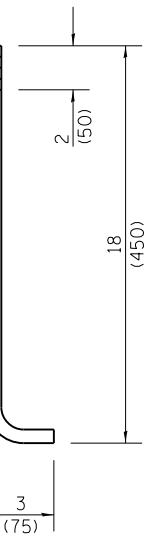


ELECTRIC SERVICE INSTALLATION

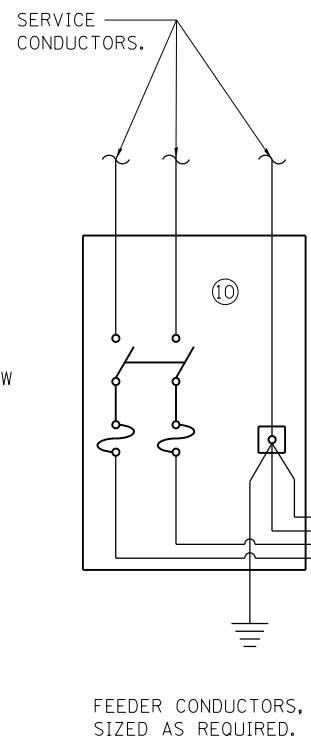
- SIZE LARGER AS NEEDED.
- OR AS DIRECTED BY UTILITY COMPANY.
- WHEN COLD SEQUENCING IS REQUIRED, PROVIDE A METER DISCONNECT SWITCH AS DIRECTED BY UTILITY COMPANY.



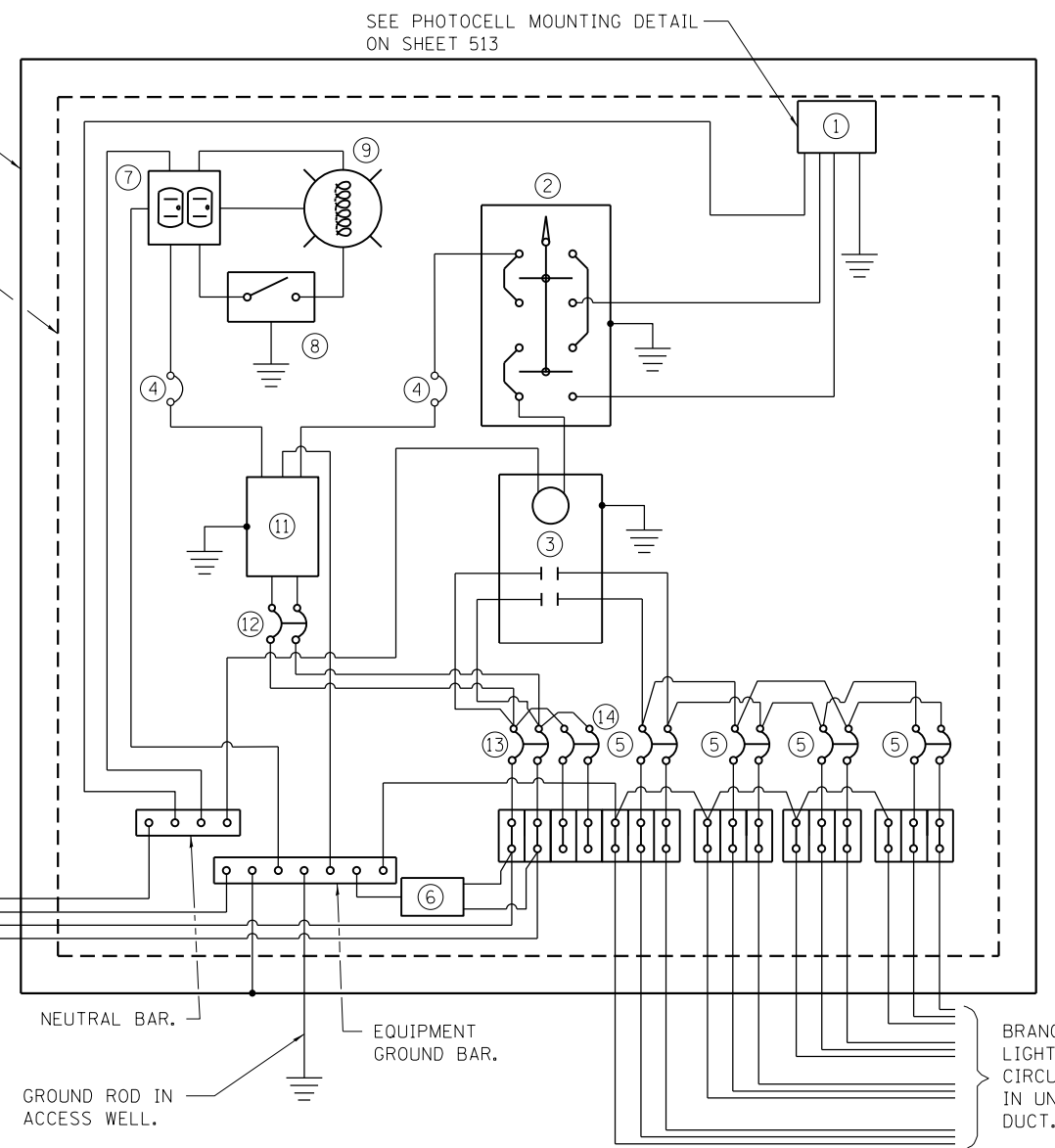
FOUNDATION (PLAN)
(WORK PAD NOT SHOWN.)



**ANCHOR ROD
DETAIL**



FEEDER CONDUCTORS,
SIZED AS REQUIRED.



CONTROL SCHEMATIC

- ① PHOTOCELL WITH INTEGRAL SURGE ARRESTER.
 - ② HAND-OFF-AUTO SELECTOR SWITCH.
 - ③ 100 AMP*, ELECTRICALLY HELD CONTACTOR.
 - ④ 15 AMP, 1-POLE CIRCUIT BREAKER.
 - ⑤ 20 AMP*, 2-POLE CIRCUIT BREAKER (TWO SPARES REQUIRED BUT NOT SHOWN).
 - ⑥ SURGE ARRESTER.
 - ⑦ GFCI DUPLEX RECEPTACLE.
 - ⑧ SINGLE-POLE, SINGLE-THROW SWITCH.
 - ⑨ INCANDESCENT LUMINAIRE, ENCLOSED AND GASKETED WITH 100 WATT LAMP.
 - ⑩ SERVICE DISCONNECT SWITCH - 2-POLE, 3-WIRE, 100 AMP*, FUSED AT 100 AMP*, SOLID NEUTRAL IN NEMA 4X ENCLOSURE HAVING LOCKABLE EXTERNAL HANDLE.
 - ⑪ TRANSFORMER - 1KVA*, 480V PRIMARY, 120/240V SECONDARY, SINGLE-PHASE, 60HZ.
 - ⑫ 15 AMP, 2-POLE CIRCUIT BREAKER.
 - ⑬ 100 AMP*, 2-POLE CIRCUIT BREAKER.
 - ⑭ 30 AMP, 2-POLE CIRCUIT BREAKER
- * SIZE LARGER AS NEEDED.

**LIGHTING CONTROLLER
BASE MOUNTED, 480V**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

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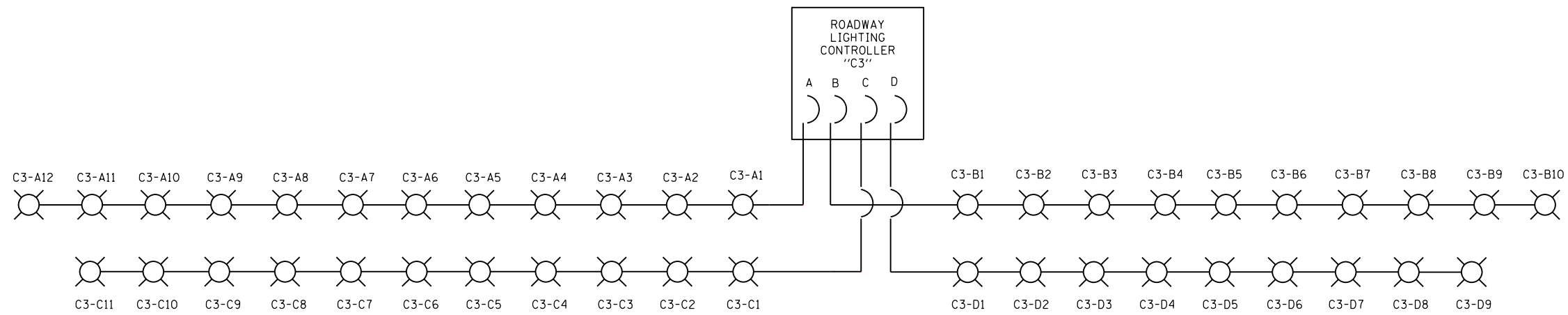


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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY LIGHTING DETAIL	
LIGHTING CONTROLLER BASE MOUNTED	
SCALE: N/A	SHEET 28 OF 32 SHEETS STA. TO STA.

F.A.P. RTE. 742	SECTION (32, 33) R-1	COUNTY WINNEBAGO	TOTAL SHEETS 705	SHEET NO. 514
CONTRACT NO. 64821			ILLINOIS FED. AID PROJECT	



ROADWAY LIGHTING CONTROLLER "C3" ONE-LINE

CONTROLLER "C3" LOAD SCHEDULE		
CIRCUIT #	AMPERES	WATTAGE
A	12.48	5760
B	10.40	4800
C	12.48	5280
D	9.38	4320

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 USER NAME = Jensen, Dave

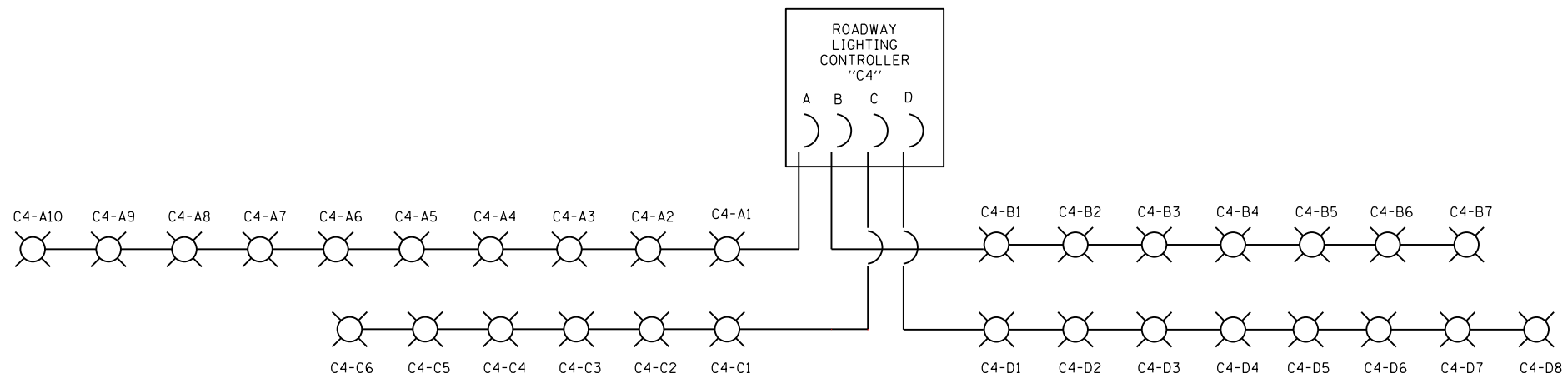


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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY LIGHTING DETAIL LIGHTING CONTROLLER "C3" ONE-LINE	
SCALE: N/A	SHEET 29 OF 32 SHEETS STA. TO STA.

F.A.P. RTE. 742	SECTION (32, 33) R-1	COUNTY WINNEBAGO	TOTAL SHEETS 705	SHEET NO. 515
ILLINOIS FED. AID PROJECT CONTRACT NO. 64821				



ROADWAY LIGHTING CONTROLLER "C4" ONE-LINE

CONTROLLER "C4" LOAD SCHEDULE		
CIRCUIT #	AMPERES	WATTAGE
A	12.5	4800
B	7.3	3360
C	6.25	2880
D	8.33	3840

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 USER NAME = Jensen Dove



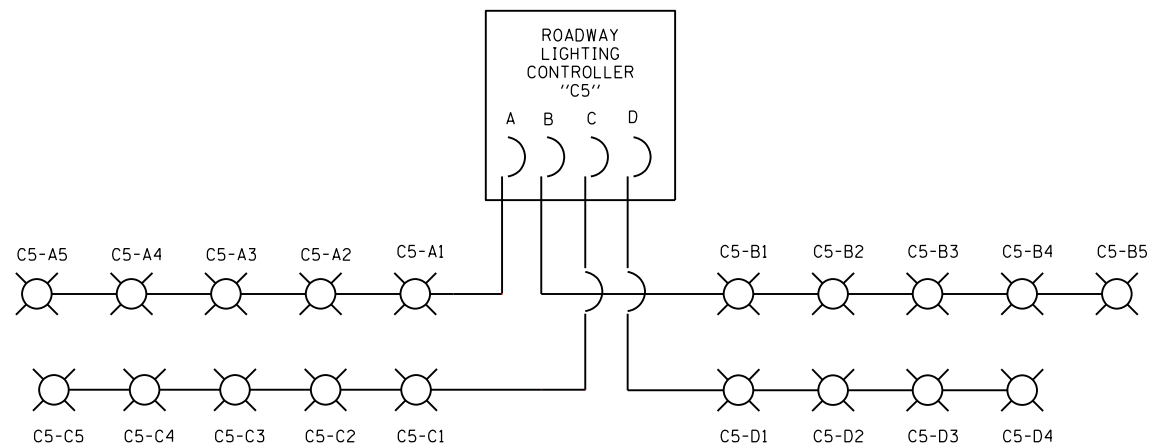
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY LIGHTING DETAIL
LIGHTING CONTROLLER "C4" ONE-LINE**

SCALE: N/A SHEET 30 OF 32 SHEETS STA. TO STA.

F.A.P. RTE. 742	SECTION (32, 33) R-1	COUNTY WINNEBAGO	TOTAL SHEETS 705	SHEET NO. 516
			CONTRACT NO. 64821	
ILLINOIS FED. AID PROJECT				



ROADWAY LIGHTING CONTROLLER "C5" ONE-LINE

CONTROLLER "C5" LOAD SCHEDULE		
CIRCUIT #	AMPERES	WATTAGE
A	5.2	2400
B	5.2	2400
C	5.2	2400
D	4.2	1920

DIRECTORY = L:\ROCKFORD\118004\Draw\CADD_Sheet\DR4821.DWG
 USER NAME = Jensen_Dave



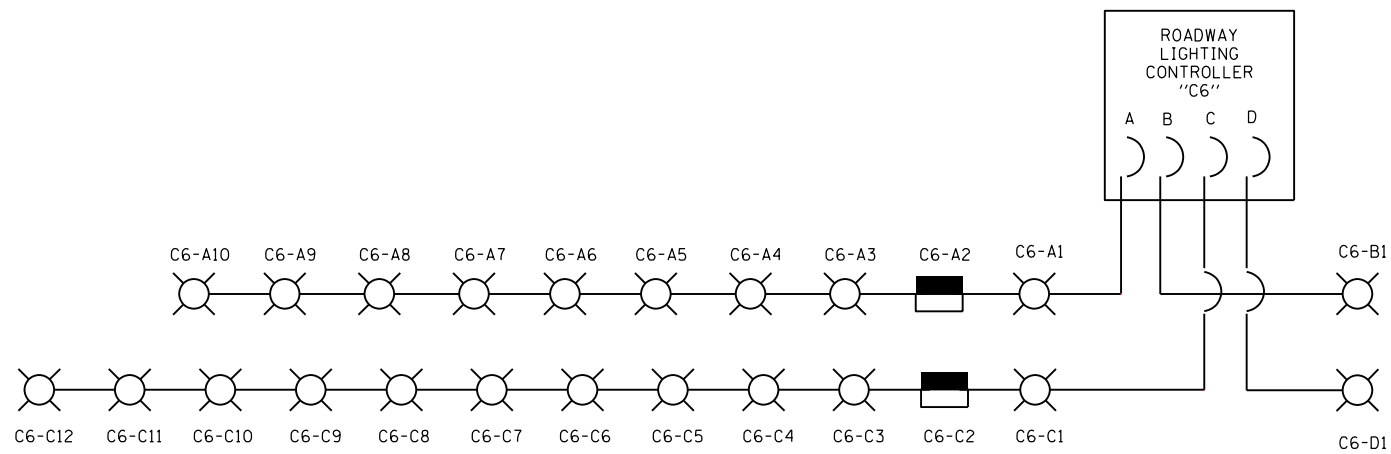
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY LIGHTING DETAIL
 LIGHTING CONTROLLER "C5" ONE-LINE**

SCALE: N/A SHEET 31 OF 32 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32, 33) R-1	WINNEBAGO	705	517
ILLINOIS FED. AID PROJECT			CONTRACT NO. 64821	



ROADWAY LIGHTING CONTROLLER "C6" ONE-LINE

CONTROLLER "C6" LOAD SCHEDULE		
CIRCUIT #	AMPERES	WATTAGE
A	9.63	4445
B	1.04	480
C	11.71	5405
D	1.04	480

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 USER NAME = Jensen, Dave

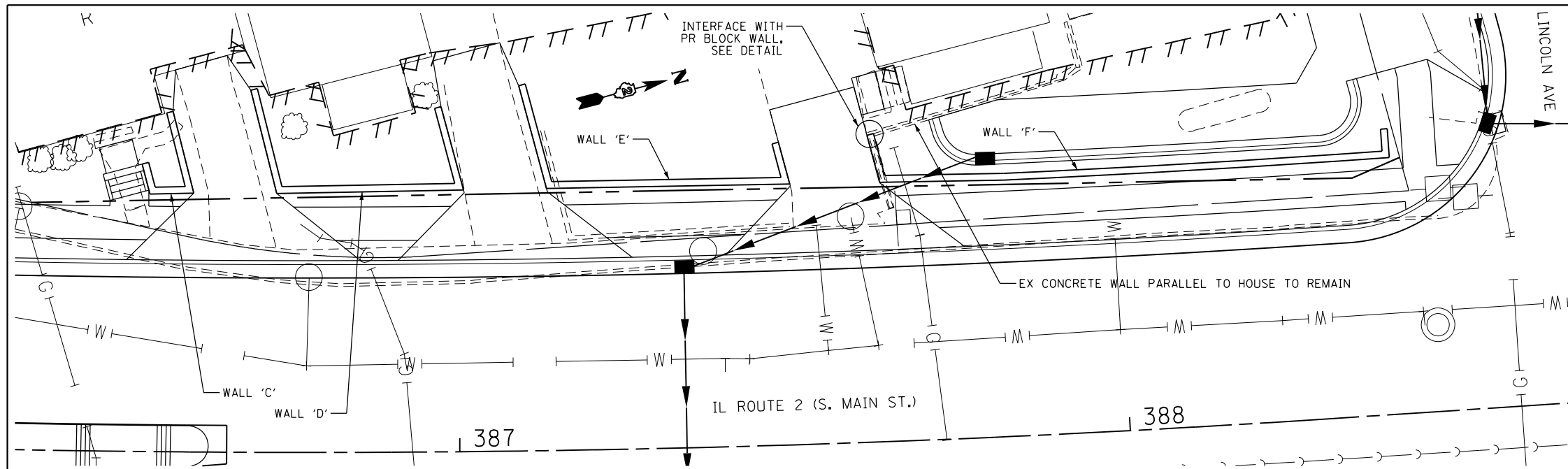


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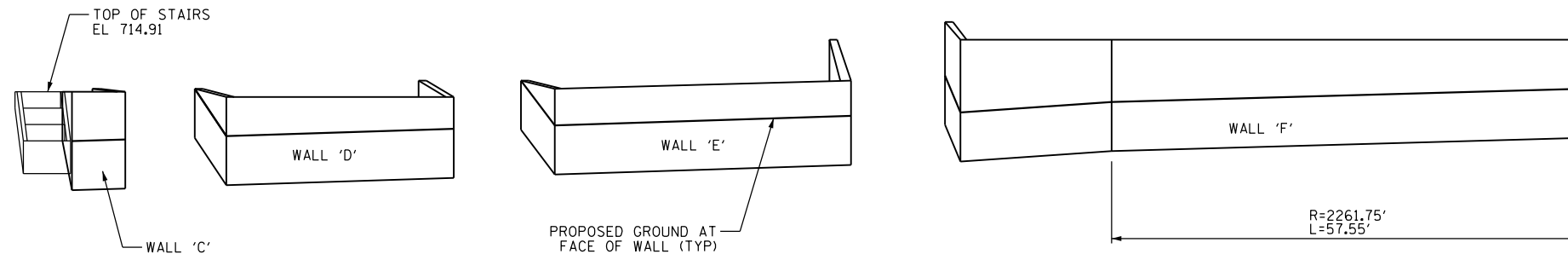
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

ROADWAY LIGHTING DETAIL LIGHTING CONTROLLER "C6" ONE-LINE		F.A.P. RTE. 742	SECTION (32, 33) R-1	COUNTY WINNEBAGO	TOTAL SHEETS 705	SHEET NO. 518
SCALE: N/A	SHEET 32 OF 32 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		

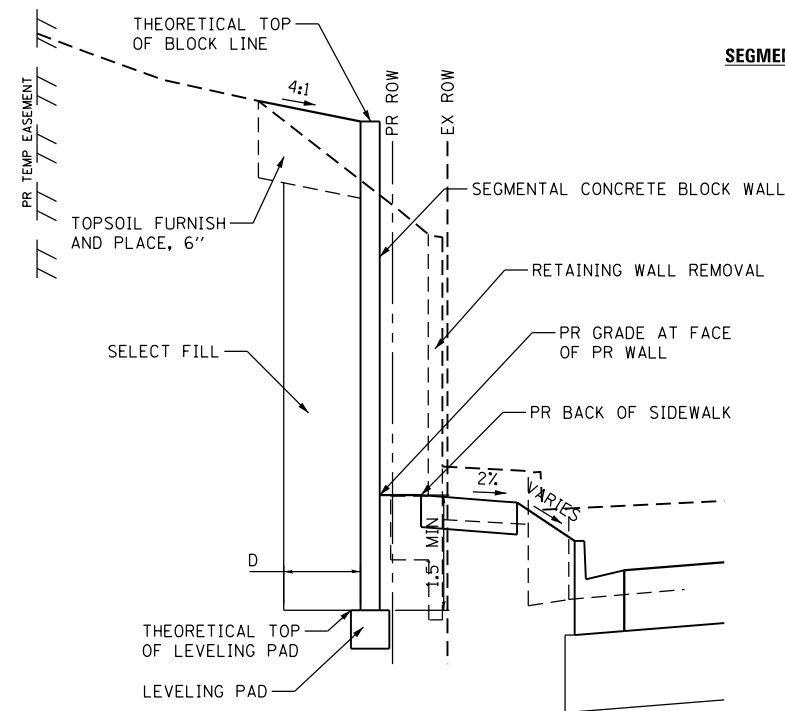
F.A.P. RTE. 742	SECTION (32, 33) R-1	COUNTY WINNEBAGO	TOTAL SHEETS 705	SHEET NO. 518
CONTRACT NO. 64821				
ILLINOIS FED. AID PROJECT				



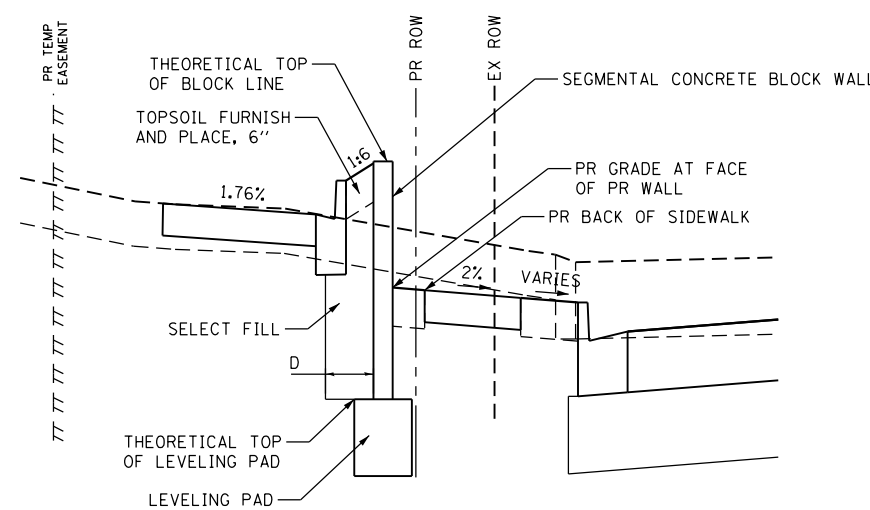
SEGMENTAL CONCRETE BLOCK WALL PLAN



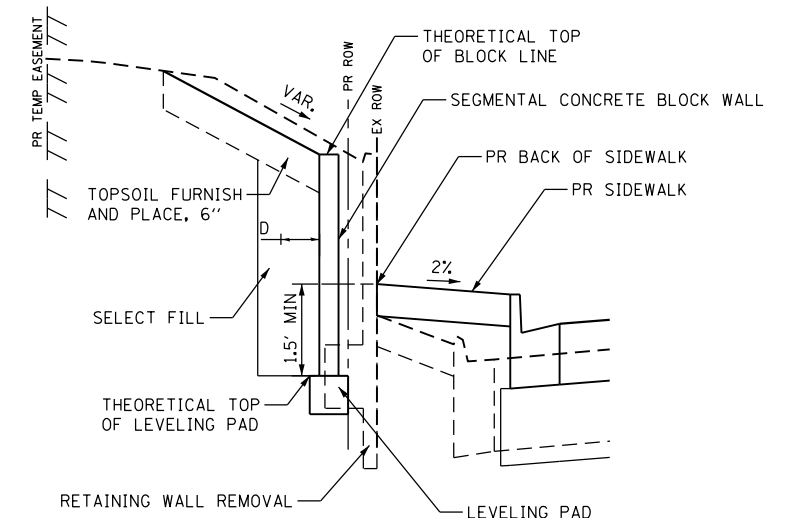
SEGMENTAL CONCRETE BLOCK WALL ELEVATION



**SEGMENTAL CONCRETE BLOCK WALL TYPICAL
(IL 2 WALL C, D, & E)**



**SEGMENTAL CONCRETE BLOCK WALL TYPICAL
(IL 2 WALL F)**

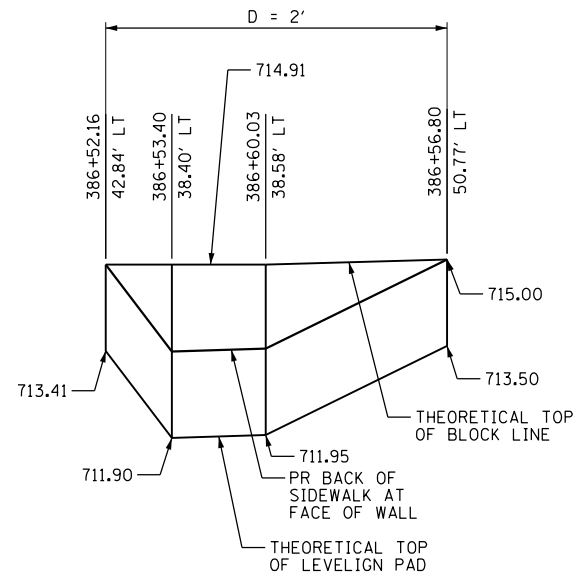


**SEGMENTAL CONCRETE BLOCK
WALL TYPICAL (IL 2)**

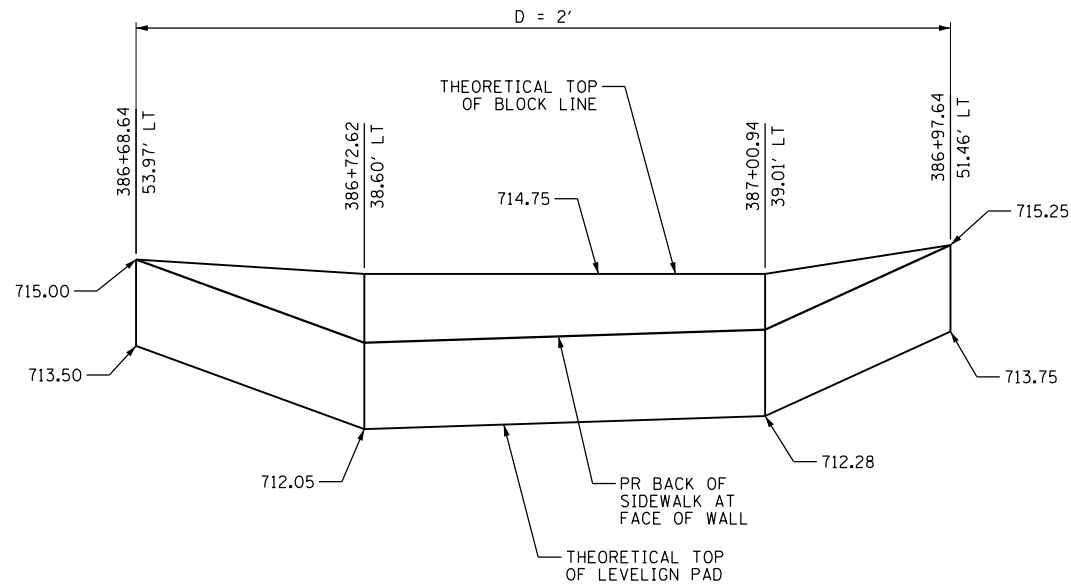
NOTES:

1. ALL STATIONS, OFFSETS, AND ELEVATIONS ARE GIVEN TO THE FRONT FACE OF THE PROPOSED SEGMENTAL CONCRETE BLOCK WALL.
2. SEE DISTRICT DETAIL 63.2, "PIPE HANDRAILS FOR STEPS", FOR DETAILS ON PIPE HANDRAILS.
3. SEE DISTRICT DETAIL 71.4, "DETAIL OF CONCRETE STEPS", FOR DETAILS ON CONCRETE STEPS.
4. SEE PLAT OF HIGHWAYS FOR EXISTING RIGHT-OF-WAY, PROPOSED RIGHT-OF-WAY AND TEMPORARY EASEMENTS.
5. THE PROPOSED STEPS SHALL MATCH EXISTING CONDITIONS. THE TOP OF STAIR ELEVATIONS SHALL BE VERIFIED PRIOR TO DESIGN OF THE SEGMENTAL CONCRETE BLOCK WALL.
6. IF THE WALL MANUFACTURER'S DESIGN REQUIRES PIPE UNDERDRAINS, THE PIPE UNDERDRAINS SHALL MEET THE MATERIAL AND CONSTRUCTION REQUIREMENTS OF GBSP "PIPE UNDERDRAINS FOR STRUCTURES." IF NEEDED THE PIPE UNDERDRAINS SHALL BE CONNECTED TO THE PROPOSED STORM SEWER. THE PIPE UNDERDRAINS SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR SEGMENTAL CONCRETE BLOCK WALL.
7. THE LIMITS OF SELECT BACKFILL HAVE BEEN SET THE FOLLOWING DISTANCE (D) BEHIND THE PROPOSED SEGMENTAL CONCRETE BLOCK WALL (0.70 X H) TO ESTABLISH EXCAVATION LIMITS. SEE THE SUPPLIERS SHOP DRAWINGS FOR ACTUAL EXCAVATION LIMITS FOR SELECT BACKFILL. EXCAVATION REQUIRED BEYOND THE LIMITS ESTABLISHED FOR THE SELECT BACKFILL BELOW, BY THE SUPPLIERS DESIGN, SHALL BE CONSIDERED OVER EXCAVATION AND SHALL NOT BE MEASURED FOR PAYMENT. THE COST OF EXCAVATION REQUIRED FOR WALL CONSTRUCTION SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR SEGMENTAL CONCRETE BLOCK WALL.
8. GEOSYNTHETIC REINFORCING, IF NEEDED, SHALL BE PER THE SUPPLIER'S DESIGN.
9. SEE SHEET 2 OF 2 FOR ADDITIONAL WALL ELEVATIONS.

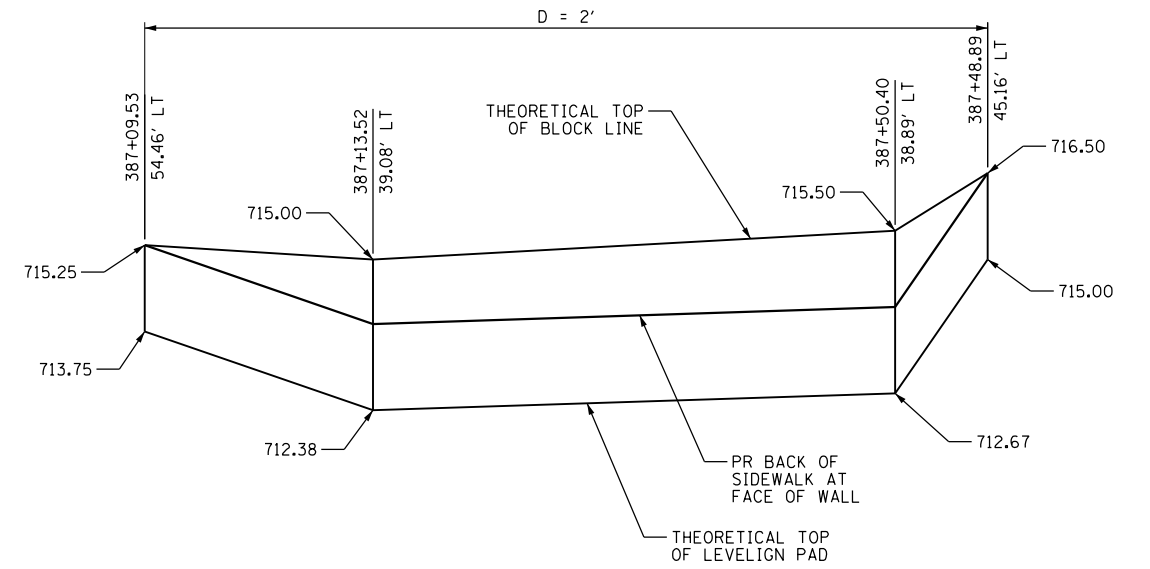
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#FILE#		DRAWN -	REVISED -		742	(32,33) R-1	WINNEBAGO	705	519			
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PLOT DATE = #DATE#		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							
					SCALE: NTS	SHEET NO. 1 OF 12 SHEETS	STA.	TO STA.				



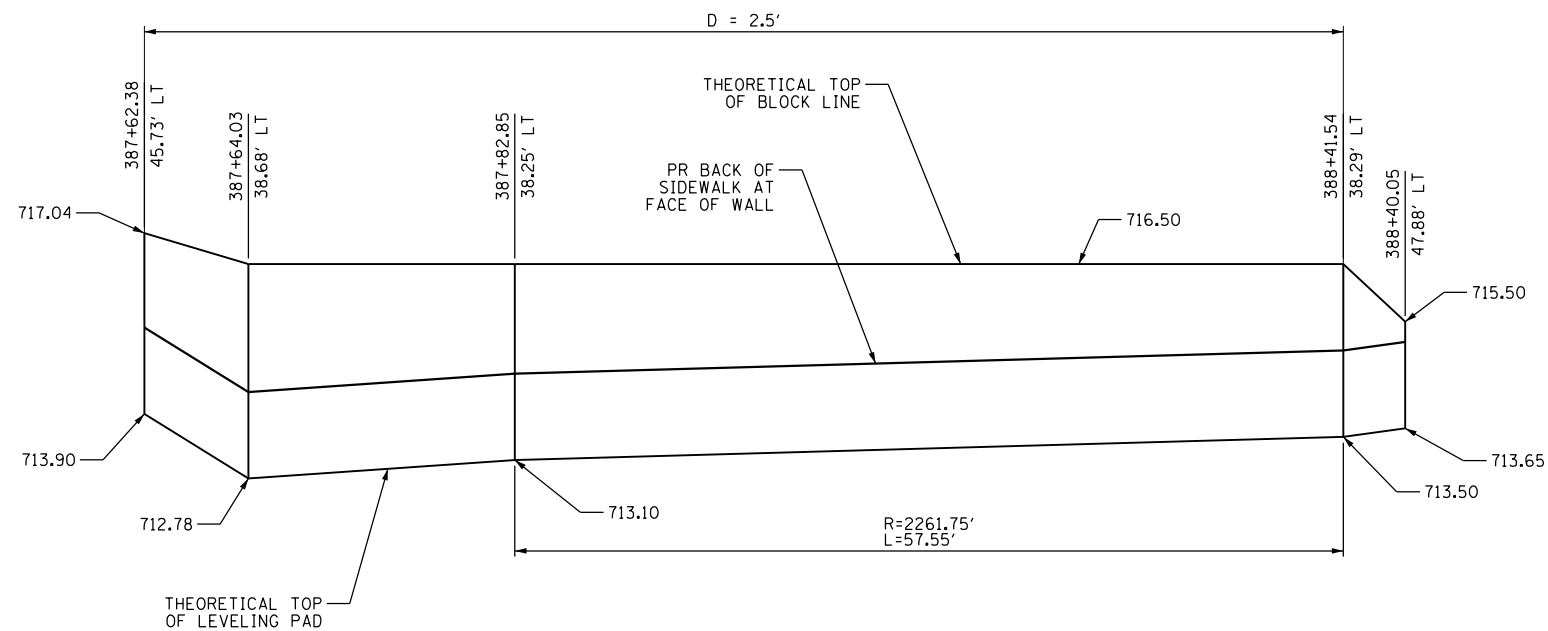
WALL 'C' ELEVATION
SEGMENTAL CONCRETE BLOCK WALL (58 SQ FT)



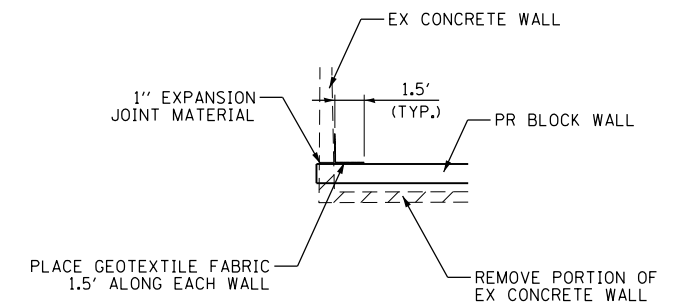
WALL 'D' ELEVATION
SEGMENTAL CONCRETE BLOCK WALL (131 SQ FT)



WALL 'E' ELEVATION
SEGMENTAL CONCRETE BLOCK WALL (145 SQ FT)

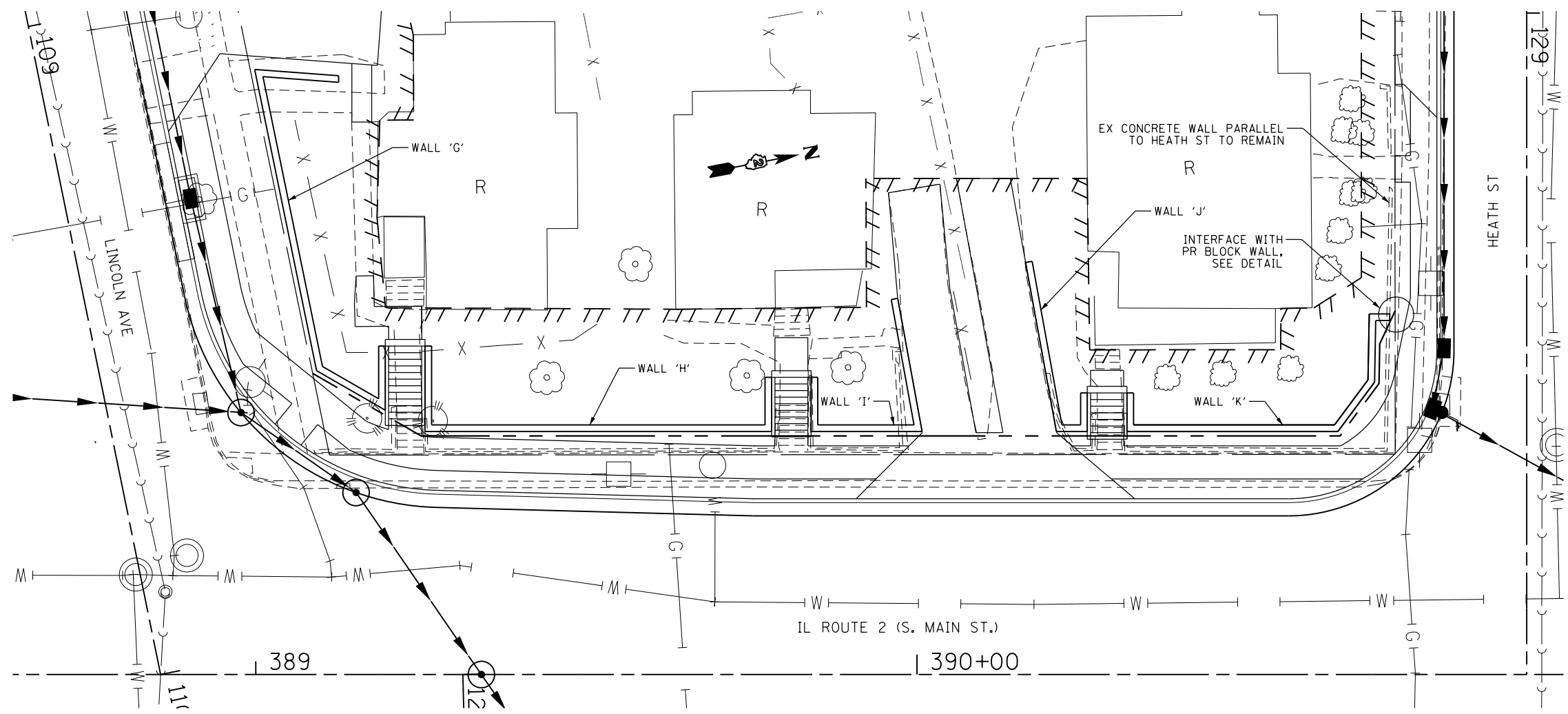


WALL 'F' ELEVATION
SEGMENTAL CONCRETE BLOCK WALL (285 SQ FT)

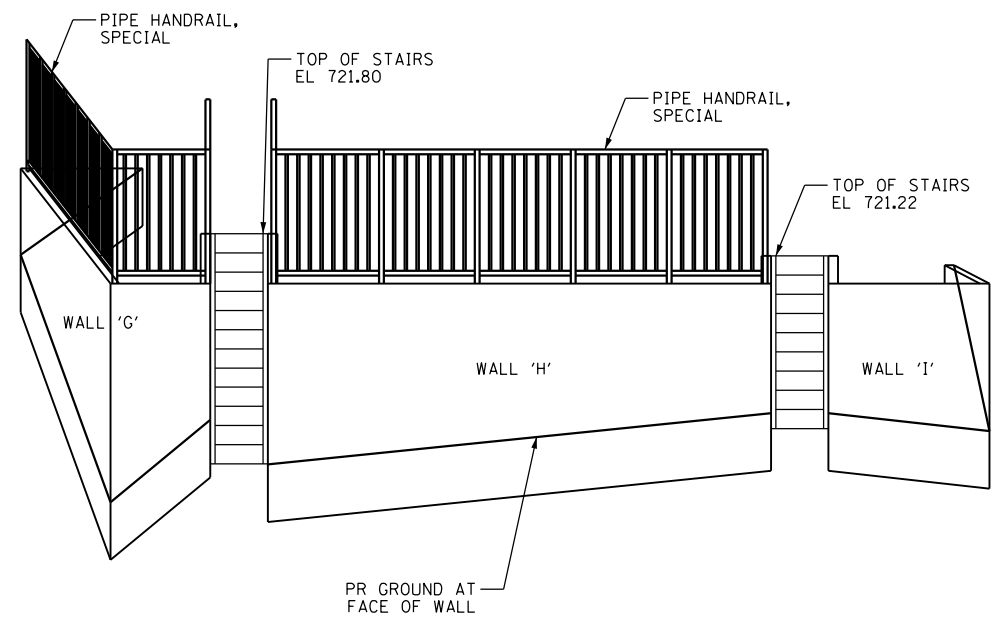


WALL INTERFACE DETAIL

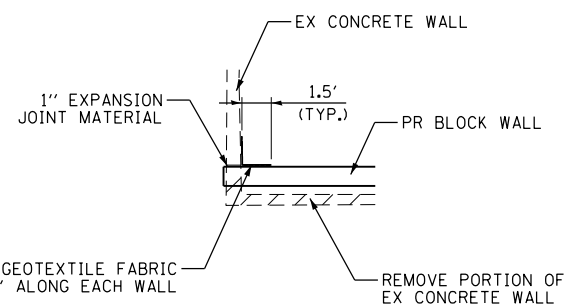
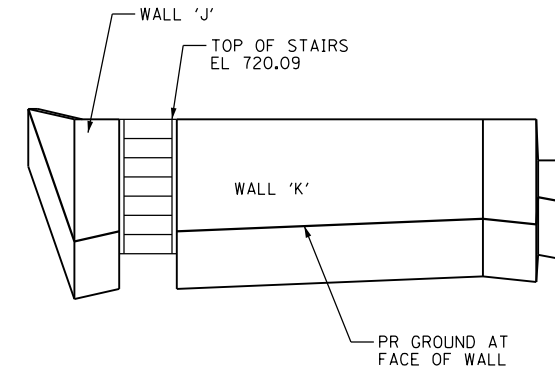
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	PLOT DATE = *DATE*	CHECKED -	REVISED -			CONTRACT NO. 64821					
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					
				SCALE: NTS	SHEET NO. 2 OF 12 SHEETS	STA.	TO STA.				



SEGMENTAL CONCRETE BLOCK WALL PLAN



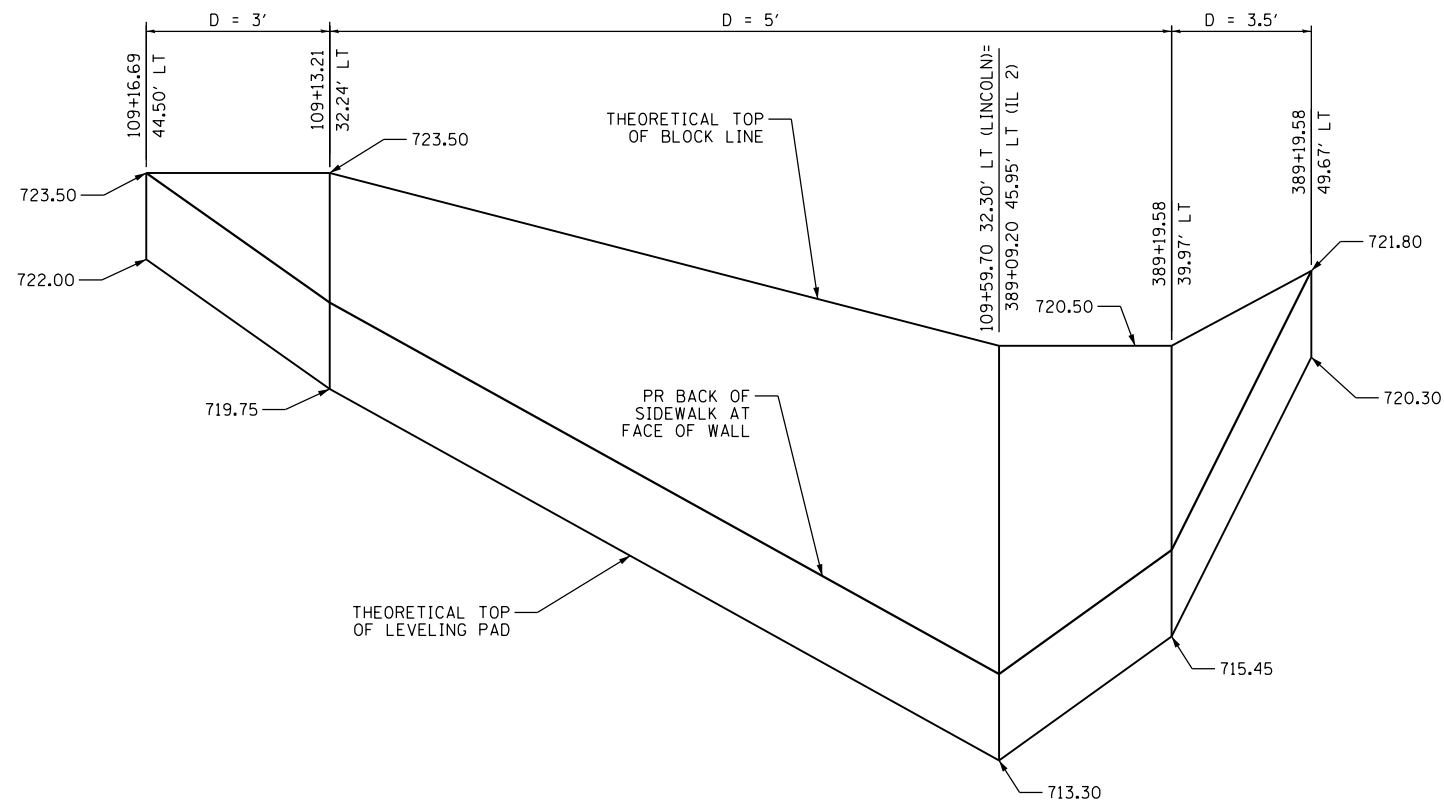
SEGMENTAL CONCRETE BLOCK WALL ELEVATION



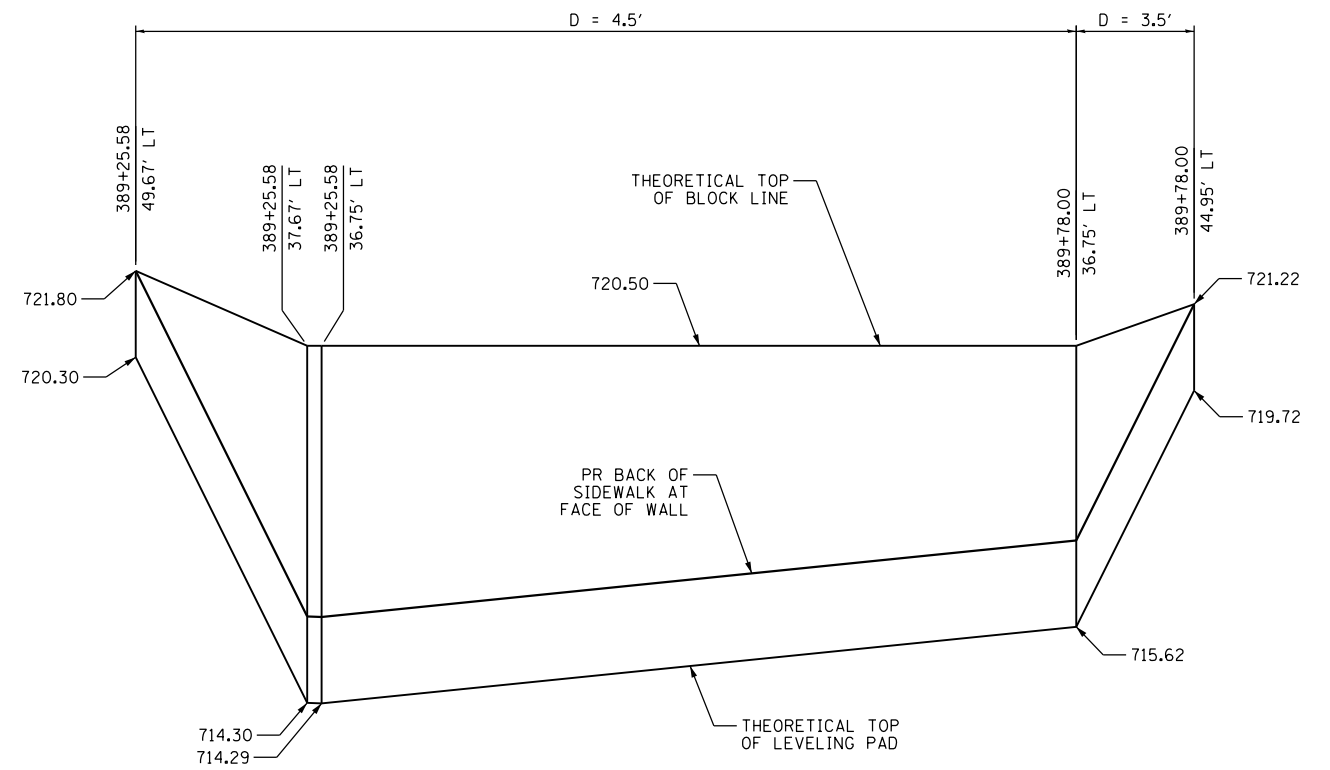
WALL INTERFACE DETAIL

- NOTES:
1. ALL STATIONS, OFFSETS, AND ELEVATIONS ARE GIVEN TO THE FRONT FACE OF THE PROPOSED SEGMENTAL CONCRETE BLOCK WALL.
 2. SEE DISTRICT DETAIL 63.2, PIPE HANDRAILS FOR STEP", FOR DETAILS ON PIPE HANDRAILS.
 3. SEE DISTRICT DETAIL 71.4, "DETAIL OF CONCRETE STEPS ", FOR DETAILS ON CONCRETE STEPS.
 4. SEE PLAT OF HIGHWAYS FOR EXISTING RIGHT-OF-WAY, PROPOSED RIGHT-OF-WAY AND TEMPORARY EASEMENTS.
 5. THE PROPOSED STEPS SHALL MATCH EXISTING CONDITIONS. THE TOP OF STAIR ELEVATIONS SHALL BE VERIFIED PRIOR TO DESIGN OF THE SEGMENTAL CONCRETE BLOCK WALL.
 6. IF THE WALL MANUFACTURER'S DESIGN REQUIRES PIPE UNDERDRAINS, THE PIPE UNDERDRAINS SHALL MEET THE MATERIAL AND CONSTRUCTION REQUIREMENTS OF GBSP "PIPE UNDERDRAINS FOR STRUCTURES." IF NEEDED THE PIPE UNDERDRAINS WILL BE CONNECTED TO THE PROPOSED STORM SEWER. THE PIPE UNDERDRAINS SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR SEGMENTAL CONCRETE BLOCK WALL.
 7. THE LIMITS OF SELECT BACKFILL HAVE BEEN SET THE FOLLOWING DISTANCE (D) BEHIND THE PROPOSED SEGMENTAL CONCRETE BLOCK WALL (0.70 X H) TO ESTABLISH EXCAVATION LIMITS. SEE THE SUPPLIER'S SHOP DRAWINGS FOR ACTUAL EXCAVATION LIMITS FOR SELECT BACKFILL. EXCAVATION REQUIRED BEYOND THE LIMITS ESTABLISHED FOR THE SELECT BACKFILL BELOW BY THE SUPPLIER'S DESIGN SHALL BE CONSIDERED OVER EXCAVATION AND SHALL NOT BE MEASURED FOR PAYMENT. THE COST OF EXCAVATION REQUIRED FOR WALL CONSTRUCTION SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR SEGMENTAL BLOCK WALL.
 8. GEOSYNTHETIC REINFORCING, IF NEEDED, SHALL BE PER THE SUPPLIER'S DESIGN.
 9. SEE SHEET 2 OF 3 FOR ADDITIONAL WALL ELEVATIONS. SEE SHEET 3 OF 3 FOR WALL TYPICAL SECTIONS.
 10. SEE DISTRICT DETAIL 64.2 - PIPE HANDRAIL, SPECIAL - FOR RETAINING WALLS.

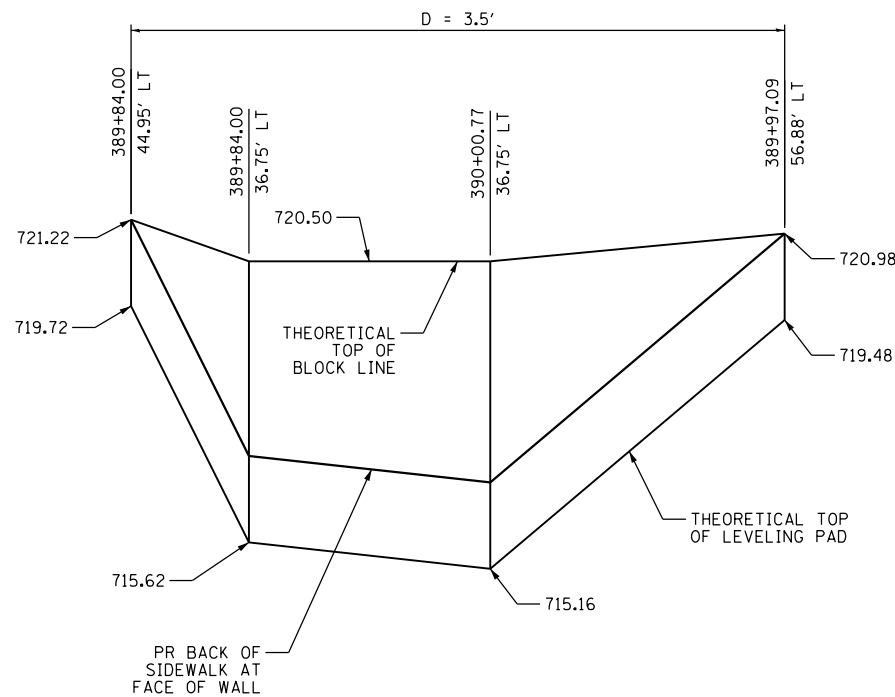
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	PLOT SCALE = *SCALE*	DRAWN -	REVISED -		SCALE: NTS	SHEET NO. 3 OF 12 SHEETS	STA.	TO STA.	742	(32,33) R-1	WINNEBAGO	705	521
	PLOT DATE = *DATE*	CHECKED -	REVISED -										
		DATE -	REVISED -										
ILLINOIS FED. AID PROJECT													



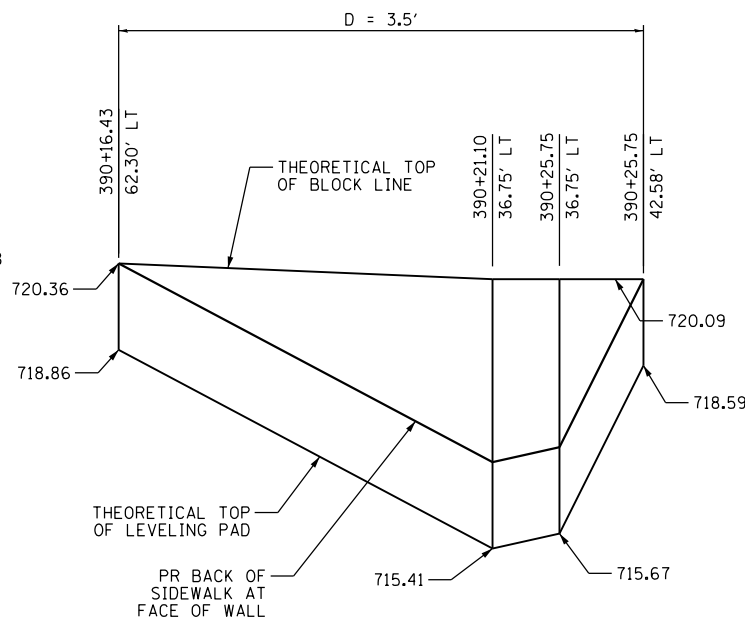
WALL 'G' ELEVATION
SEGMENTAL CONCRETE BLOCK WALL (393 SQ FT)



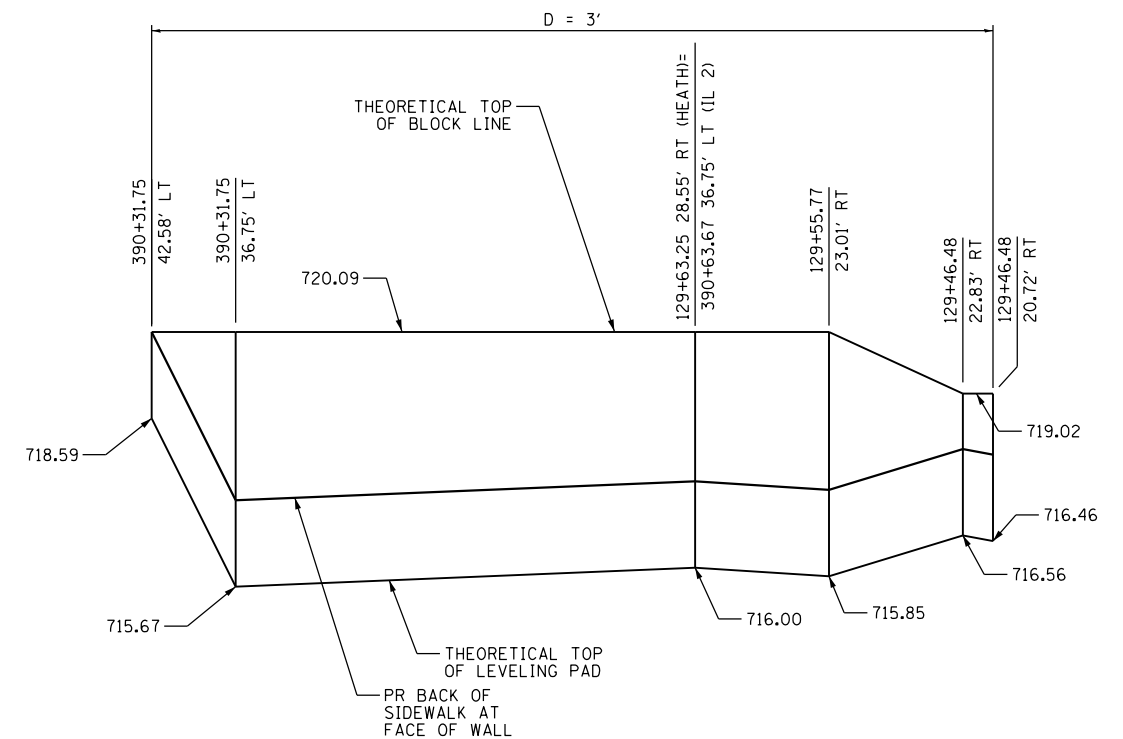
WALL 'H' ELEVATION
SEGMENTAL CONCRETE BLOCK WALL (369 SQ FT)



WALL 'I' ELEVATION
SEGMENTAL CONCRETE BLOCK WALL (182 SQ FT)



WALL 'J' ELEVATION
SEGMENTAL CONCRETE BLOCK WALL (119 SQ FT)



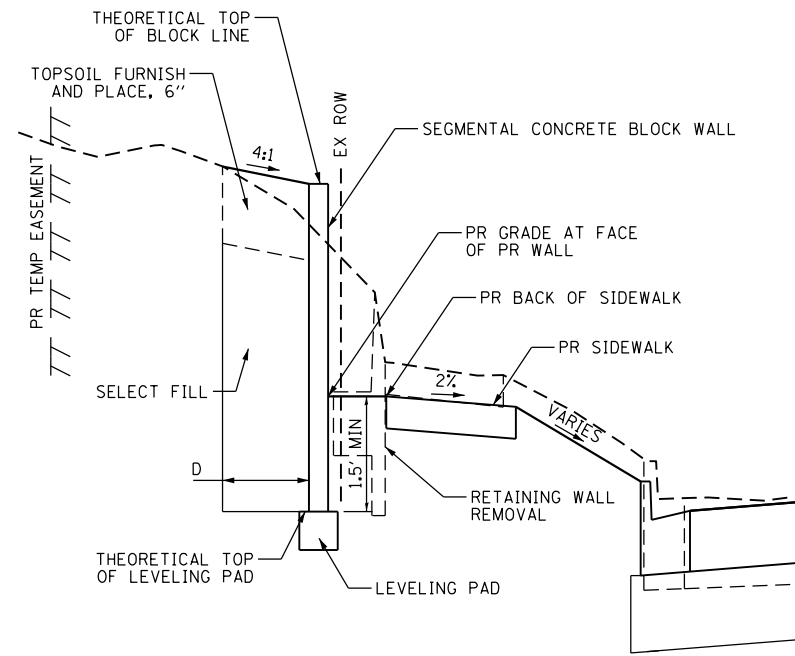
WALL 'K' ELEVATION
SEGMENTAL CONCRETE BLOCK WALL (228 SQ FT)

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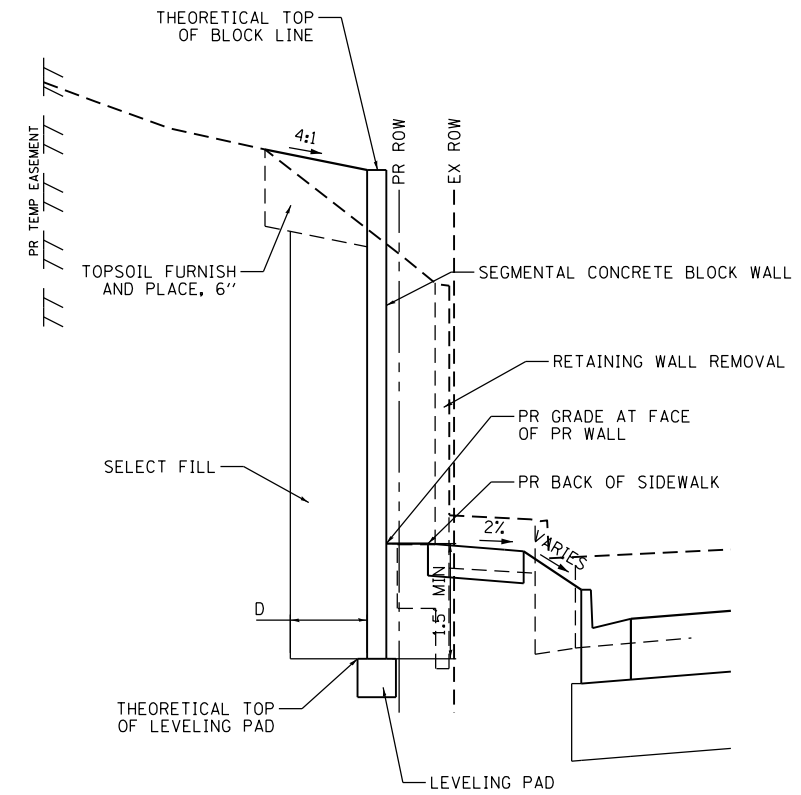
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SEGMENTAL BLOCK WALL DETAIL	
IL 2 LT, FROM LINCOLN AVE TO HEATH ST (WEST LEG)	
SCALE: NTS	SHEET NO. 4 OF 12 SHEETS
STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32,33) R-1	WINNEBAGO	705	522
CONTRACT NO. 64821				
ILLINOIS FED. AID PROJECT				



**SEGMENTAL CONCRETE BLOCK
WALL TYPICAL (LINCOLN)**



**SEGMENTAL CONCRETE BLOCK
WALL TYPICAL (IL 2)**

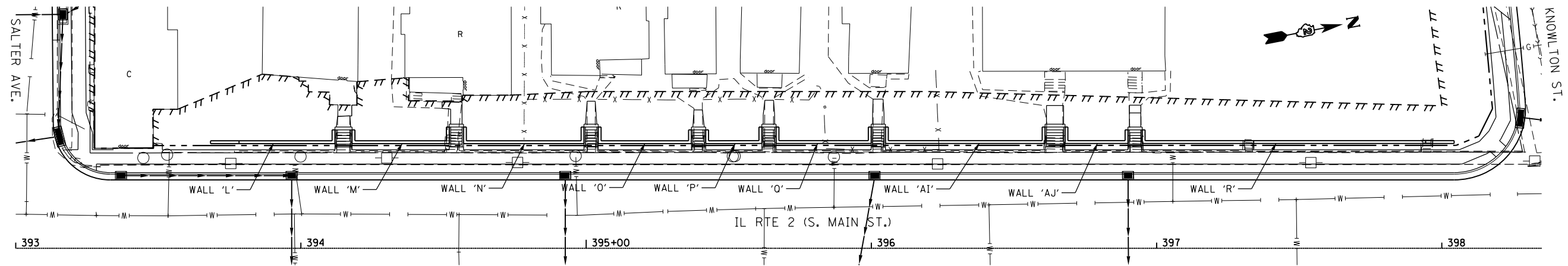
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

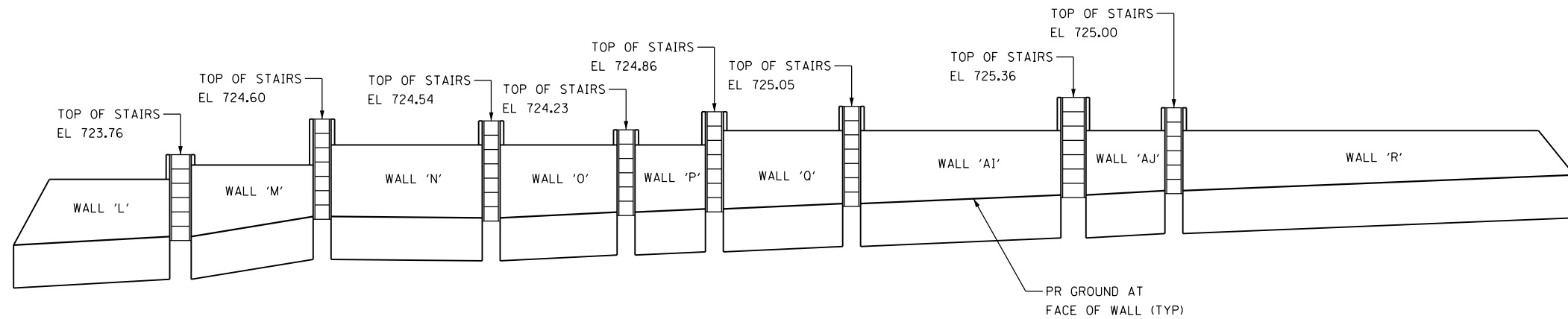
**SEGMENTAL BLOCK WALL DETAIL
IL 2 LT, FROM LINCOLN AVE TO HEATH ST (WEST LEG)**

SCALE: NTS SHEET NO. 5 OF 12 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32,33) R-1	WINNEBAGO	705	523
CONTRACT NO. 64821				
ILLINOIS FED. AID PROJECT				



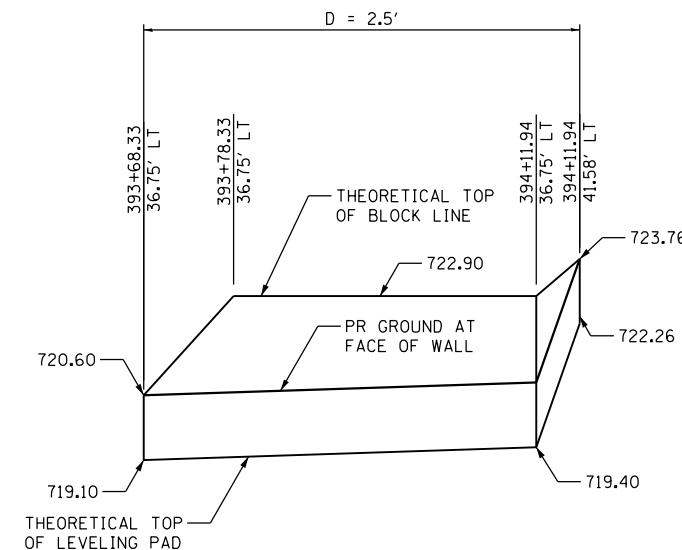
SEGMENTAL CONCRETE BLOCK WALL PLAN



SEGMENTAL CONCRETE BLOCK WALL ELEVATION

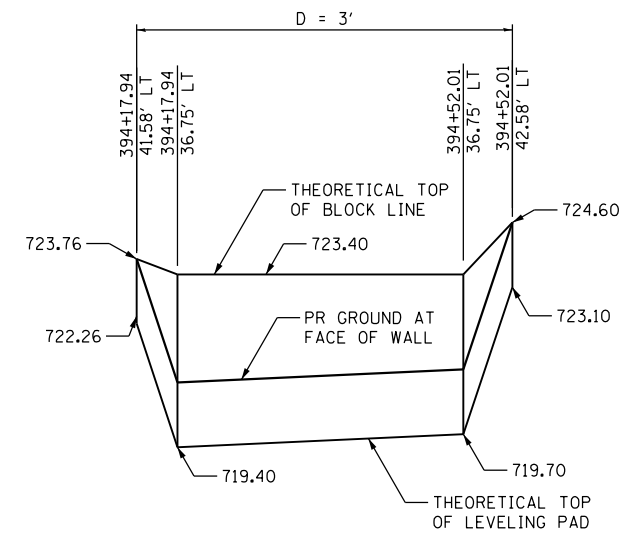
NOTES:

1. ALL STATIONS, OFFSETS AND ELEVATIONS ARE GIVEN TO THE FRONT FACE OF THE PROPOSED SEGMENTAL CONCRETE BLOCK WALL.
2. SEE DISTRICT DETAIL 63.2, "PIPE HANDRAILS FOR STEPS", FOR DETAILS ON PIPE HANDRAILS.
3. SEE DISTRICT DETAIL 71.4, "DETAIL OF CONCRETE STEPS", FOR DETAILS ON CONCRETE STEPS.
4. SEE PLAT OF HIGHWAYS FOR EXISTING RIGHT-OF-WAY, PROPOSED RIGHT-OF-WAY AND TEMPORARY EASEMENTS.
5. THE PROPOSED STEPS SHALL MATCH EXISTING CONDITIONS. THE TOP OF STAIR ELEVATIONS SHALL BE VERIFIED PRIOR TO DESIGN OF THE SEGMENTAL CONCRETE BLOCK WALL.
6. IF THE WALL MANUFACTURES DESIGN REQUIRES PIPE UNDERDRAINS, THE PIPE UNDERDRAINS SHALL MEET THE MATERIAL AND CONSTRUCTION REQUIREMENTS OF GBSP "PIPE UNDERDRAINS FOR STRUCTURES." IF NEEDED THE PIPE UNDERDRAINS SHALL BE CONNECTED TO THE PROPOSED STORM SEWER. THE PIPE UNDERDRAINS SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR SEGMENTAL CONCRETE BLOCK WALL.
7. THE LIMITS OF SELECT BACKFILL HAVE BEEN SET THE FOLLOWING DISTANCE (D) BEHIND THE PROPOSED SEGMENTAL CONCRETE BLOCK WALL (0.70 X H) TO ESTABLISH EXCAVATION LIMITS. SEE THE SUPPLIERS SHOP DRAWINGS FOR ACTUAL EXCAVATION LIMITS FOR SELECT BACKFILL. EXCAVATION REQUIRED BEYOND THE LIMITS ESTABLISHED FOR THE SELECT BACKFILL BELOW, BY THE SUPPLIERS DESIGN SHALL BE CONSIDERED OVER EXCAVATION AND SHALL NOT BE MEASURED FOR PAYMENT. THE COST OF EXCAVATION REQUIRED FOR WALL CONSTRUCTION SHALL BE INCLUDED IN THE UNIT PRICE FOR SEGMENTAL CONCRETE BLOCK WALL.
8. GEOSYNTHETIC REINFORCING, IF NEEDED, SHALL BE PER THE SUPPLIER'S DESIGN.
9. SEE SHEET 2 OF 2 FOR ADDITIONAL WALL ELEVATIONS AND TYPICAL SECTION.



WALL 'L' ELEVATION

SEGMENTAL CONCRETE BLOCK WALL (160 SQ FT)



WALL 'M' ELEVATION

SEGMENTAL CONCRETE BLOCK WALL (160 SQ FT)

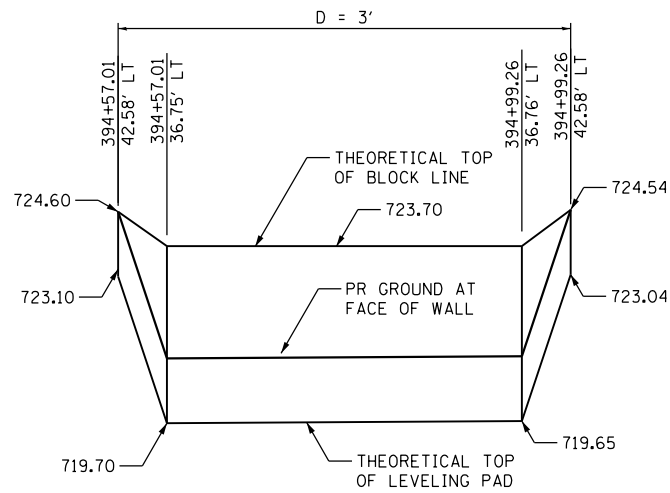
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

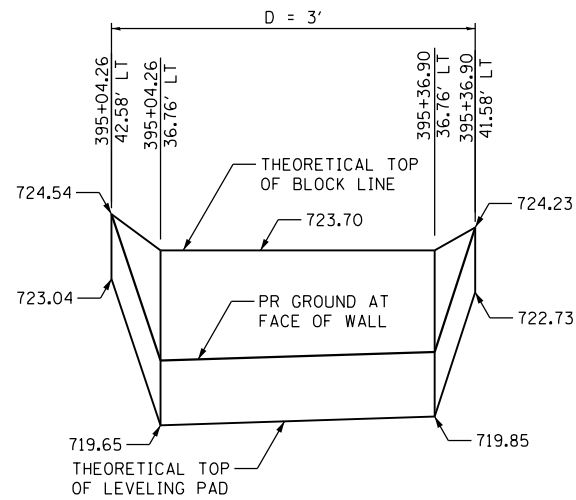
SEGMENTAL BLOCK WALL DETAIL
 IL 2 LT, FROM SALTER AVE TO KNOWLTON ST

SCALE: NTS SHEET NO. 6 OF 12 SHEETS STA. TO STA.

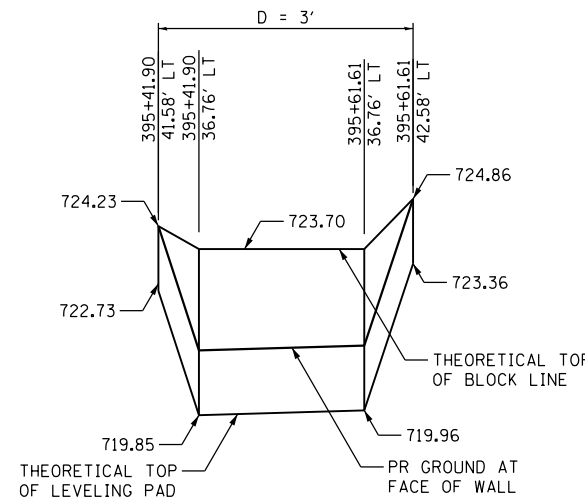
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32,33) R-1	WINNEBAGO	705	524
CONTRACT NO. 64821			ILLINOIS FED. AID PROJECT	



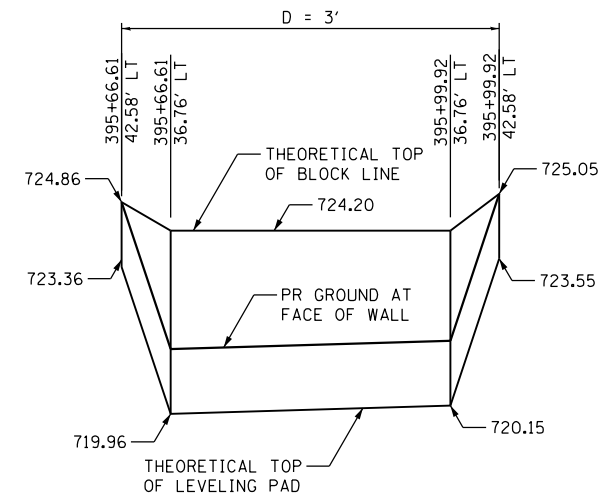
WALL 'N' ELEVATION
SEGMENTAL CONCRETE BLOCK WALL (205 SQ FT)



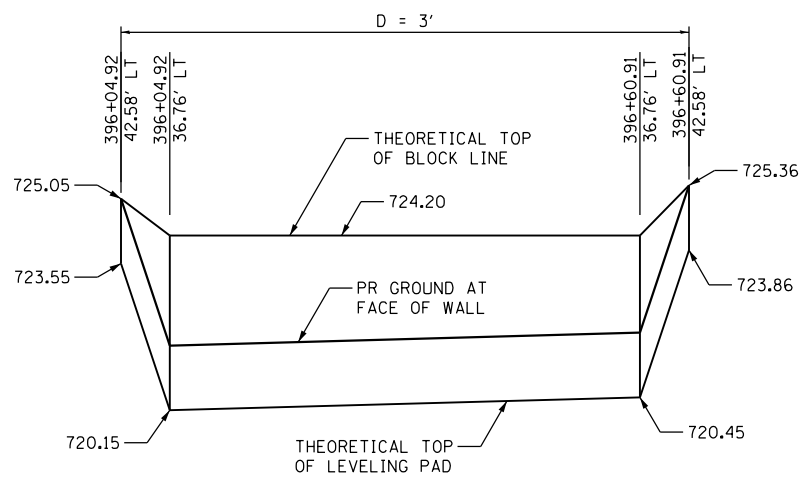
WALL 'O' ELEVATION
SEGMENTAL CONCRETE BLOCK WALL (158 SQ FT)



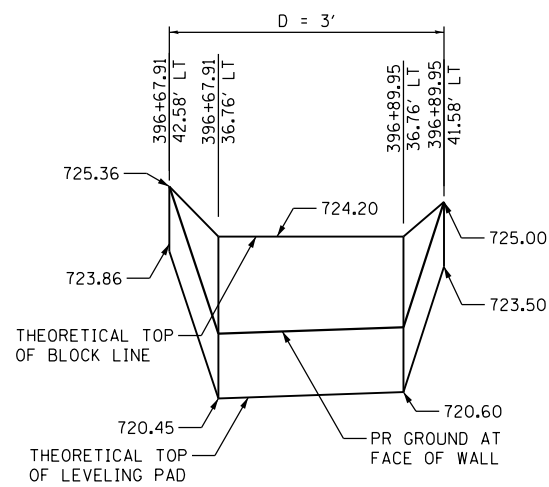
WALL 'P' ELEVATION
SEGMENTAL CONCRETE BLOCK WALL (103 SQ FT)



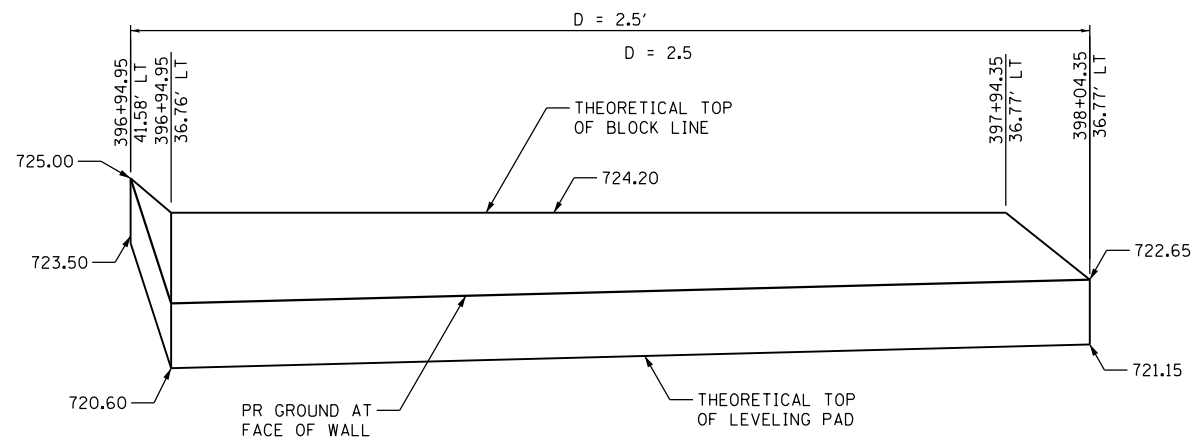
WALL 'Q' ELEVATION
SEGMENTAL CONCRETE BLOCK WALL (171 SQ FT)



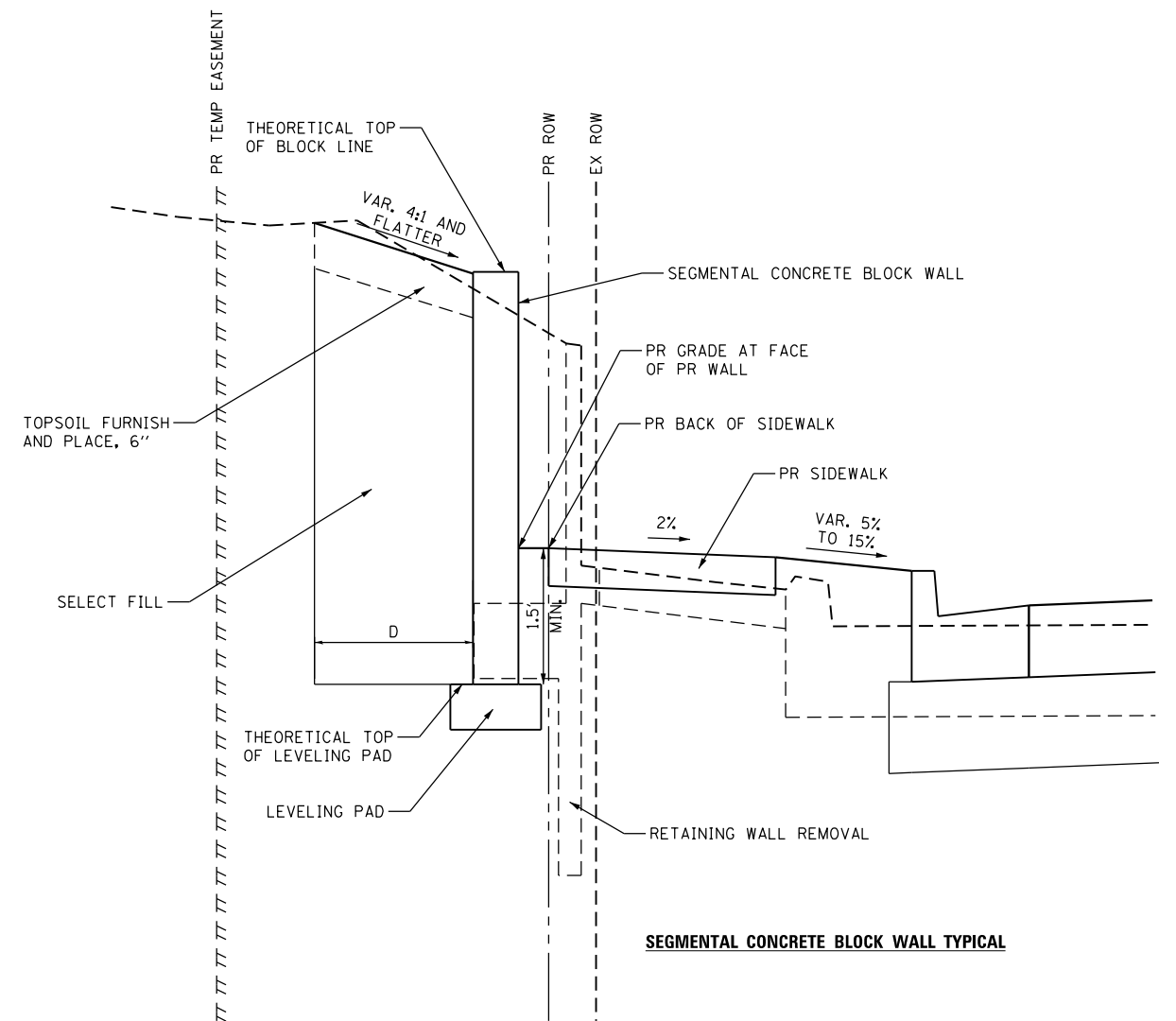
WALL 'A' ELEVATION
SEGMENTAL CONCRETE BLOCK WALL (250 SQ FT)



WALL 'AJ' ELEVATION
SEGMENTAL CONCRETE BLOCK WALL (109 SQ FT)

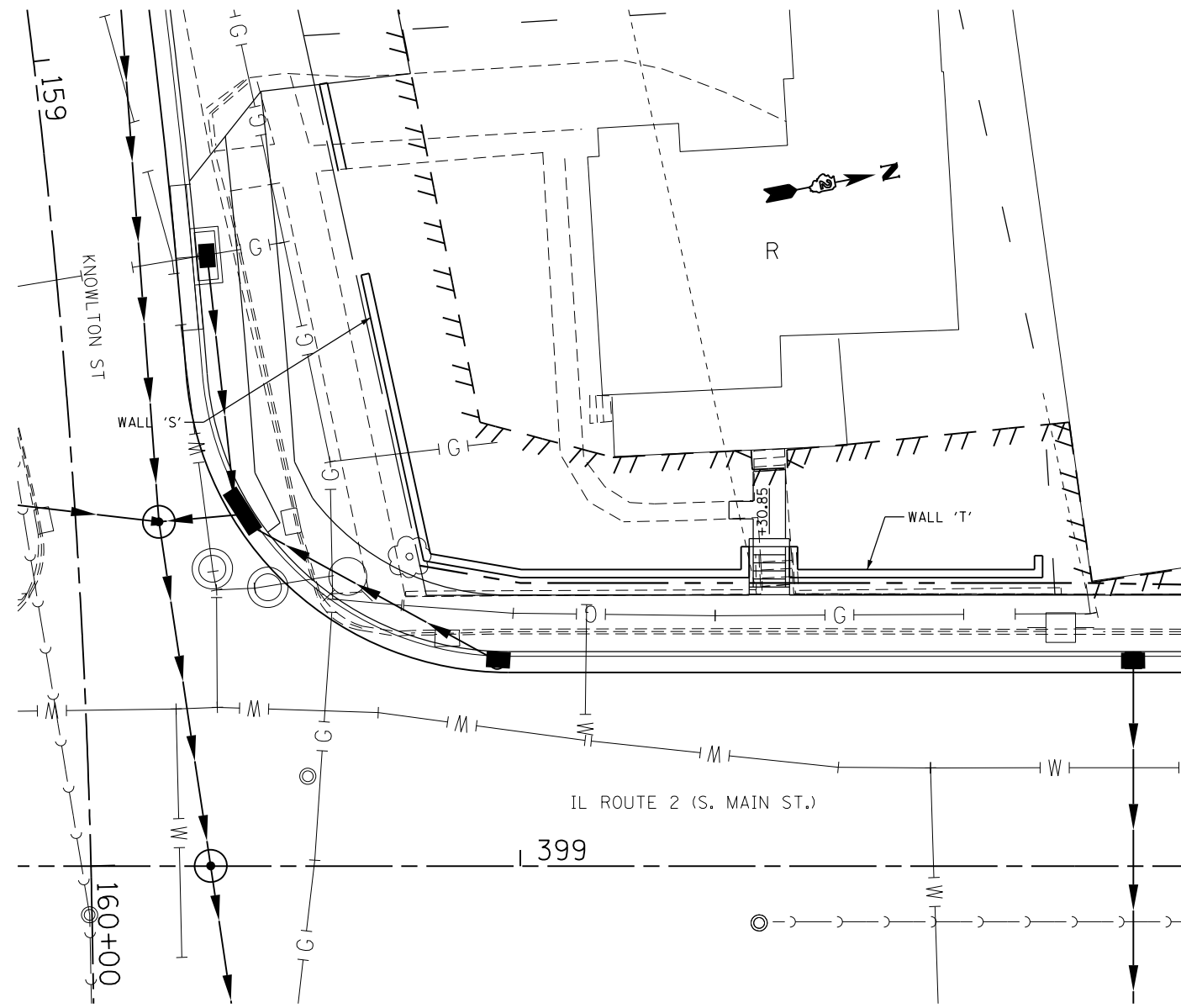


WALL 'R' ELEVATION
SEGMENTAL CONCRETE BLOCK WALL (368 SQ FT)

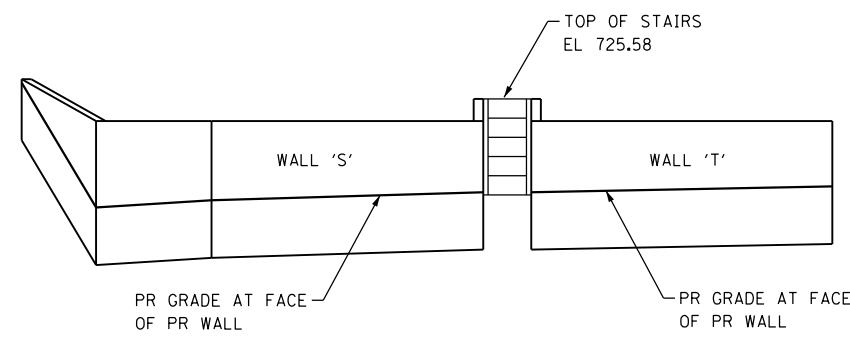


SEGMENTAL CONCRETE BLOCK WALL TYPICAL

FILE NAME = *FILEL*	USER NAME = *USER*	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SEGMENTAL BLOCK WALL DETAIL IL 2 LT, FROM SALTER AVE TO KNOWLTON ST		F.A.P. RTE. 742	SECTION (32,33) R-1	COUNTY WINNEBAGO	TOTAL SHEETS 705	SHEET NO. 525			
	PLOT SCALE = *SCALE*	CHECKED -	REVISED -				SCALE: NTS	SHEET NO. 7 OF 12 SHEETS	STA. TO STA.	CONTRACT NO. 64821				
	PLOT DATE = *DATE*	DATE -	REVISED -				ILLINOIS FED. AID PROJECT							



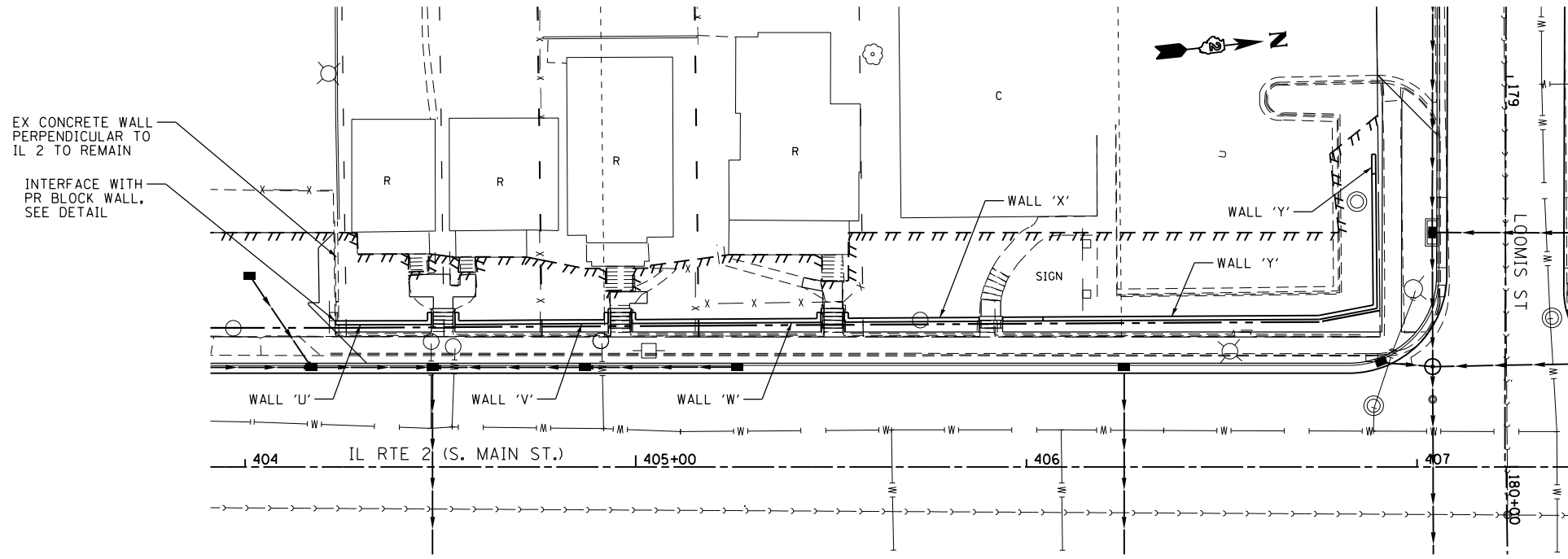
SEGMENTAL CONCRETE BLOCK WALL PLAN



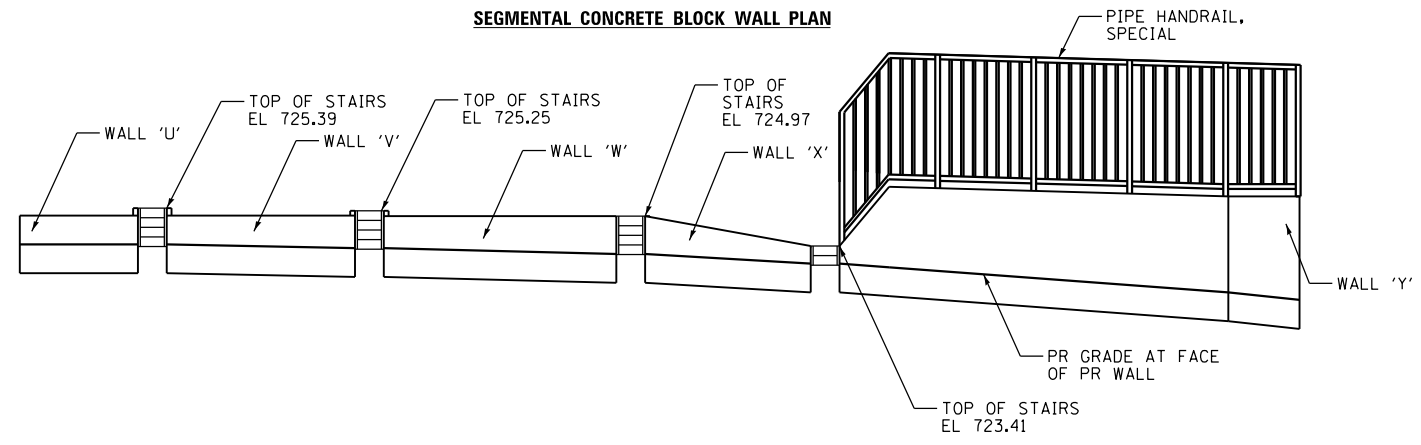
SEGMENTAL CONCRETE BLOCK WALL ELEVATION

- NOTES:
1. ALL STATIONS, OFFSETS, AND ELEVATIONS ARE GIVEN TO THE FRONT FACE OF THE PROPOSED SEGMENTAL CONCRETE BLOCK WALL.
 2. SEE DISTRICT DETAIL 63.2, "PIPE HANDRAILS FOR STEPS", FOR DETAILS ON PIPE HANDRAILS.
 3. SEE DISTRICT DETAIL 71.4 "DETAIL OF CONCRETE STEPS", FOR DETAILS ON CONCRETE STEPS.
 4. SEE PLAT OF HIGHWAYS FOR EXISTING RIGHT-OF-WAY, PROPOSED RIGHT-OF-WAY AND TEMPORARY EASEMENTS.
 5. THE PROPOSED STEPS SHALL MATCH EXISTING CONDITIONS. THE TOP OF STAIR ELEVATIONS SHALL BE VERIFIED PRIOR TO DESIGN OF THE SEGMENTAL CONCRETE BLOCK WALL.
 6. IF THE WALL MANUFACTURER'S DESIGN REQUIRES PIPE UNDERDRAINS, THE PIPE UNDERDRAINS SHALL MEET THE MATERIAL AND CONSTRUCTION REQUIREMENTS OF GBSP "PIPE UNDERDRAINS FOR STRUCTURES." IF NEEDED THE PIPE UNDERDRAINS SHALL BE CONNECTED TO THE PROPOSED STORM SEWER. THE PIPE UNDERDRAINS SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR SEGMENTAL CONCRETE BLOCK WALL.
 7. THE LIMITS OF SELECT BACKFILL HAVE BEEN SET THE FOLLOWING DISTANCE (D) BEHIND THE PROPOSED SEGMENTAL CONCRETE BLOCK WALL (0.70 X H) TO ESTABLISH EXCAVATION LIMITS. SEE THE SUPPLIERS SHOP DRAWINGS FOR ACTUAL EXCAVATION LIMITS FOR SELECT BACKFILL. EXCAVATION REQUIRED BEYOND THE LIMITS ESTABLISHED FOR THE SELECT BACKFILL BELOW, BY THE SUPPLIERS DESIGN, SHALL BE CONSIDERED OVER EXCAVATION AND SHALL NOT BE MEASURED FOR PAYMENT. THE COST OF EXCAVATION REQUIRED FOR WALL CONSTRUCTION SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR SEGMENTAL CONCRETE BLOCK WALL.
 8. GEOSYNTHETIC REINFORCING, IF NEEDED, SHALL BE PER THE SUPPLIER'S DESIGN.
 9. SEE SHEET 2 OF 2 FOR ADDITIONAL WALL ELEVATIONS AND TYPICAL SECTIONS.

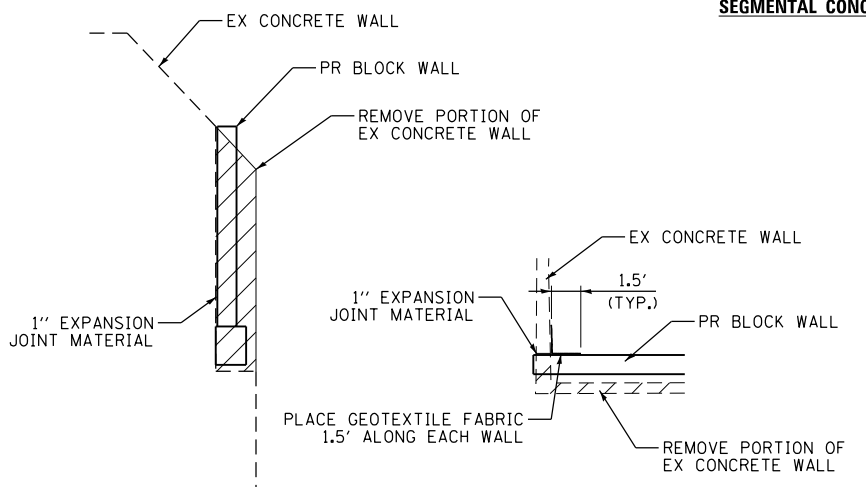
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FILEL		DRAWN -	REVISED -		IL 2 LT, FROM KNOWLTON ST TO MONTAGUE ST			742	(32,33) R-1	WINNEBAGO	705	526
		CHECKED -	REVISED -		SCALE: NTS			SHEET NO. 8 OF 12 SHEETS		CONTRACT NO. 64821		
		DATE -	REVISED -		SCALE: NTS			SHEET NO. 8 OF 12 SHEETS		CONTRACT NO. 64821		ILLINOIS FED. AID PROJECT



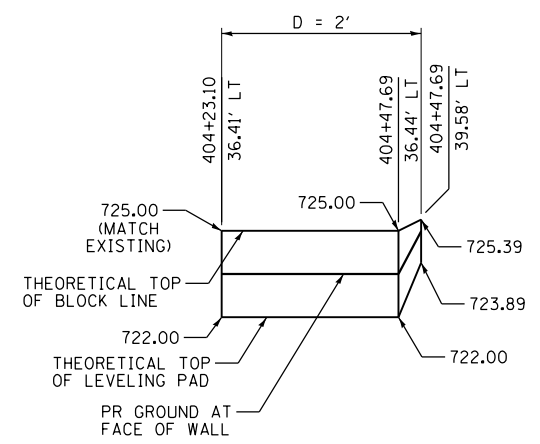
SEGMENTAL CONCRETE BLOCK WALL PLAN



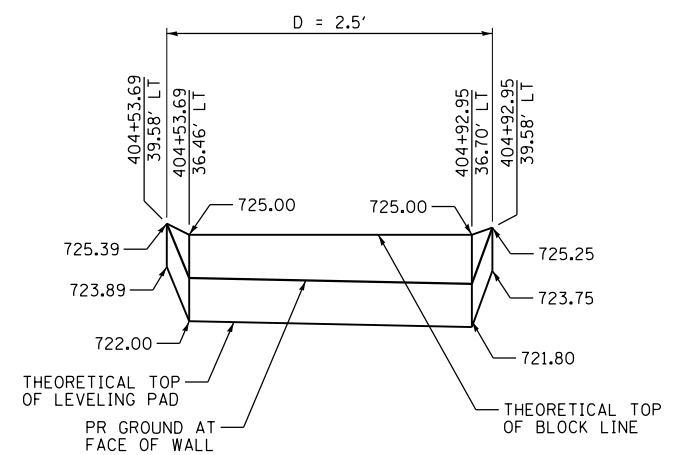
SEGMENTAL CONCRETE BLOCK WALL ELEVATION



WALL INTERFACE DETAIL



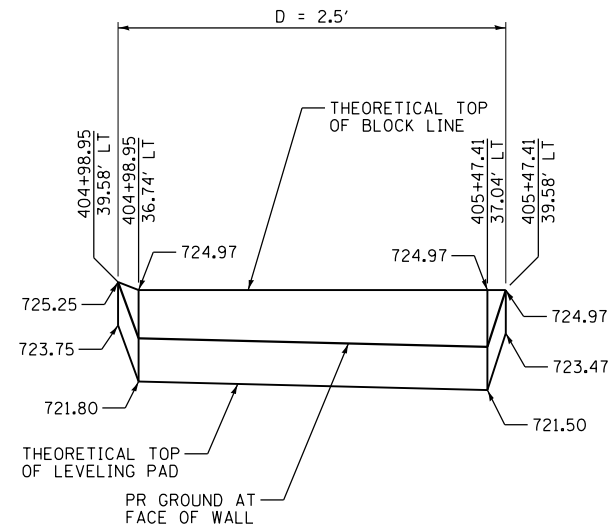
WALL 'U' ELEVATION
SEGMENTAL CONCRETE BLOCK WALL (81 SO FT)



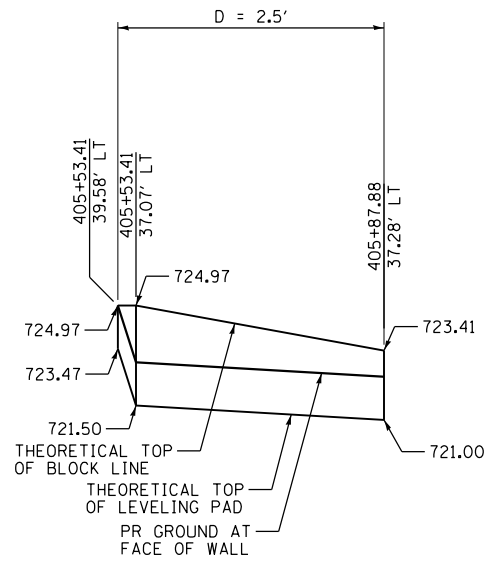
WALL 'V' ELEVATION
SEGMENTAL CONCRETE BLOCK WALL (136 SO FT)

- NOTES:
- ALL STATIONS, OFFSETS, AND ELEVATIONS ARE GIVEN TO THE FRONT FACE OF THE PROPOSED SEGMENTAL CONCRETE BLOCK WALL.
 - SEE DISTRICT DETAIL 63.2, "PIPE HANDRAILS FOR STEPS", FOR DETAILS ON PIPE HANDRAILS.
 - SEE DISTRICT DETAIL 71.4, "DETAIL OF CONCRETE STEPS" FOR DETAILS ON CONCRETE STEPS.
 - SEE PLAT OF HIGHWAYS FOR EXISTING RIGHT-OF-WAY, PROPOSED RIGHT-OF-WAY AND TEMPORARY EASEMENTS.
 - THE PROPOSED STEPS SHALL MATCH EXISTING CONDITIONS. THE TOP OF STAIR ELEVATIONS SHALL BE VERIFIED PRIOR TO DESIGN OF THE SEGMENTAL CONCRETE BLOCK WALL.
 - IF THE WALL MANUFACTURER'S DESIGN REQUIRES PIPE UNDERDRAINS, THE PIPE UNDERDRAINS SHALL MEET THE MATERIAL AND CONSTRUCTION REQUIREMENTS OF GBSP "PIPE UNDERDRAINS FOR STRUCTURES." IF NEEDED THE PIPE UNDERDRAINS SHALL BE CONNECTED TO THE PROPOSED STORM SEWER. THE PIPE UNDERDRAINS SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR SEGMENTAL CONCRETE BLOCK WALL.
 - THE LIMITS OF SELECT BACKFILL HAVE BEEN SET THE FOLLOWING DISTANCE (D) BEHIND THE PROPOSED SEGMENTAL CONCRETE BLOCK WALL (0.70 X H) TO ESTABLISH EXCAVATION LIMITS. SEE THE SUPPLIER'S SHOP DRAWINGS FOR ACTUAL EXCAVATION LIMITS FOR SELECT BACKFILL. EXCAVATION REQUIRED BEYOND THE LIMITS ESTABLISHED FOR THE SELECT BACKFILL BELOW BY THE SUPPLIER'S DESIGN SHALL BE CONSIDERED OVER EXCAVATION AND SHALL NOT BE MEASURED FOR PAYMENT. THE COST OF EXCAVATION REQUIRED FOR WALL CONSTRUCTION SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR CONCRETE BLOCK WALL.
 - GEOSYNTHETIC REINFORCING, IF NEEDED, SHALL BE PER THE SUPPLIER'S DESIGN.
 - SEE SHEET 2 OF 2 FOR ADDITIONAL WALL ELEVATIONS AND TYPICAL SECTIONS.
 - SEE DISTRICT DETAIL 64.2 - PIPE HANDRAIL, SPECIAL - FOR RETAINING WALLS.

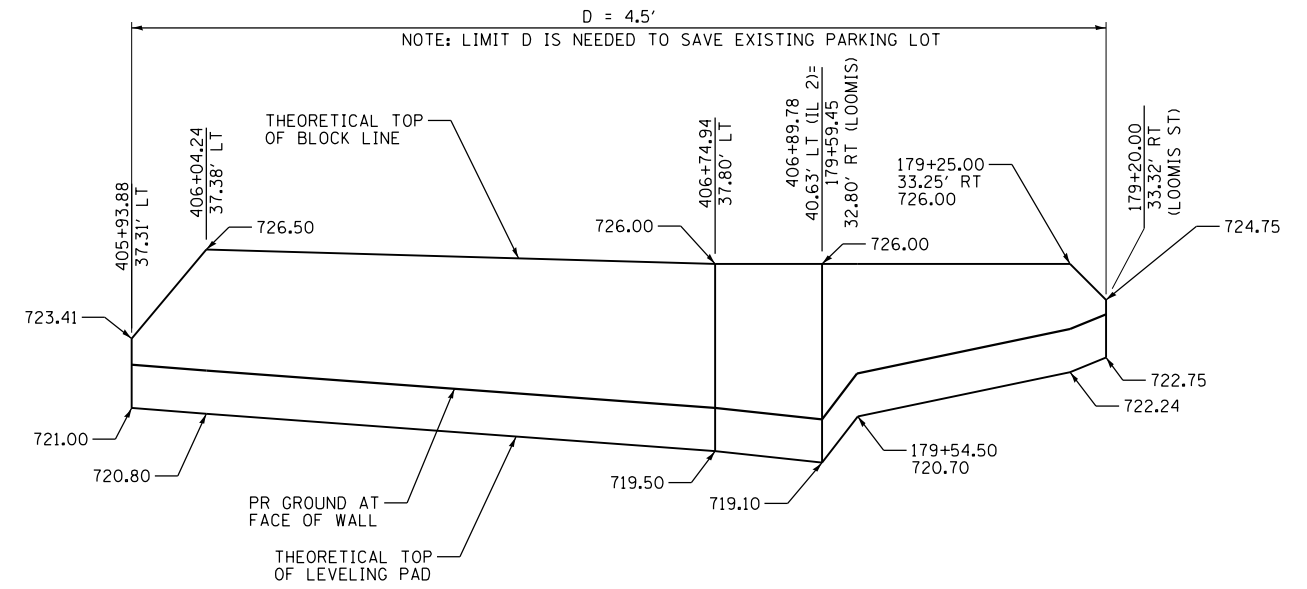
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		DRAWN -	REVISED -		SCALE: NTS	SHEET NO. 10 OF 12 SHEETS	STA.	742	(32,33) R-1	WINNEBAGO	705	528
		CHECKED -	REVISED -				TO STA.					
		DATE -	REVISED -									
							CONTRACT NO. 64821		ILLINOIS FED. AID PROJECT			



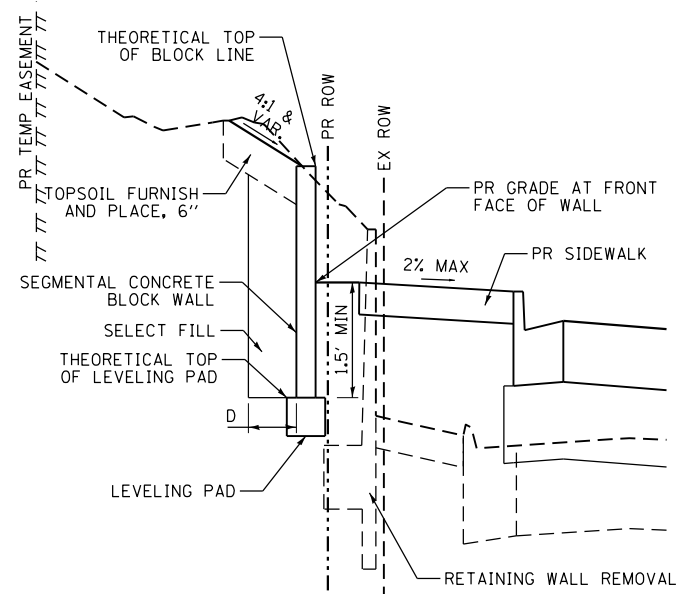
WALL 'W' ELEVATION
SEGMENTAL CONCRETE BLOCK WALL (174 SQ FT)



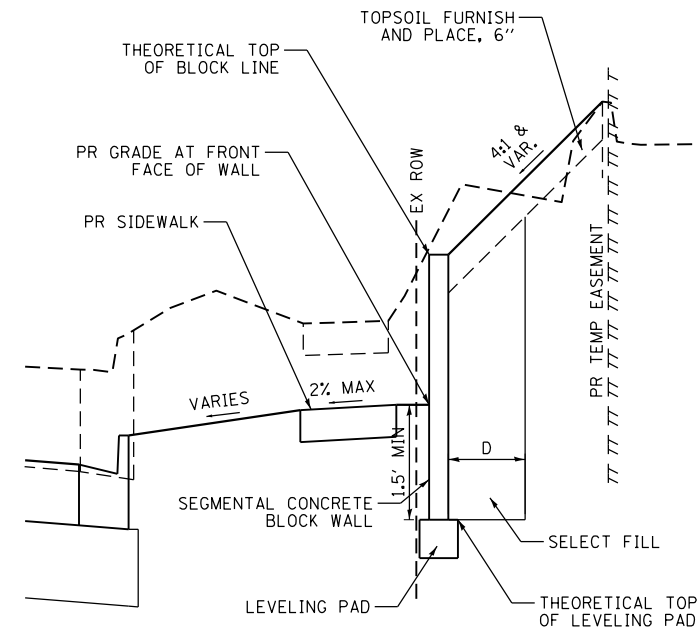
WALL 'X' ELEVATION
SEGMENTAL CONCRETE BLOCK WALL (108 SQ FT)



WALL 'Y' ELEVATION
SEGMENTAL CONCRETE BLOCK WALL (756 SQ FT)



SEGMENTAL CONCRETE BLOCK WALL TYPICAL (IL 2)



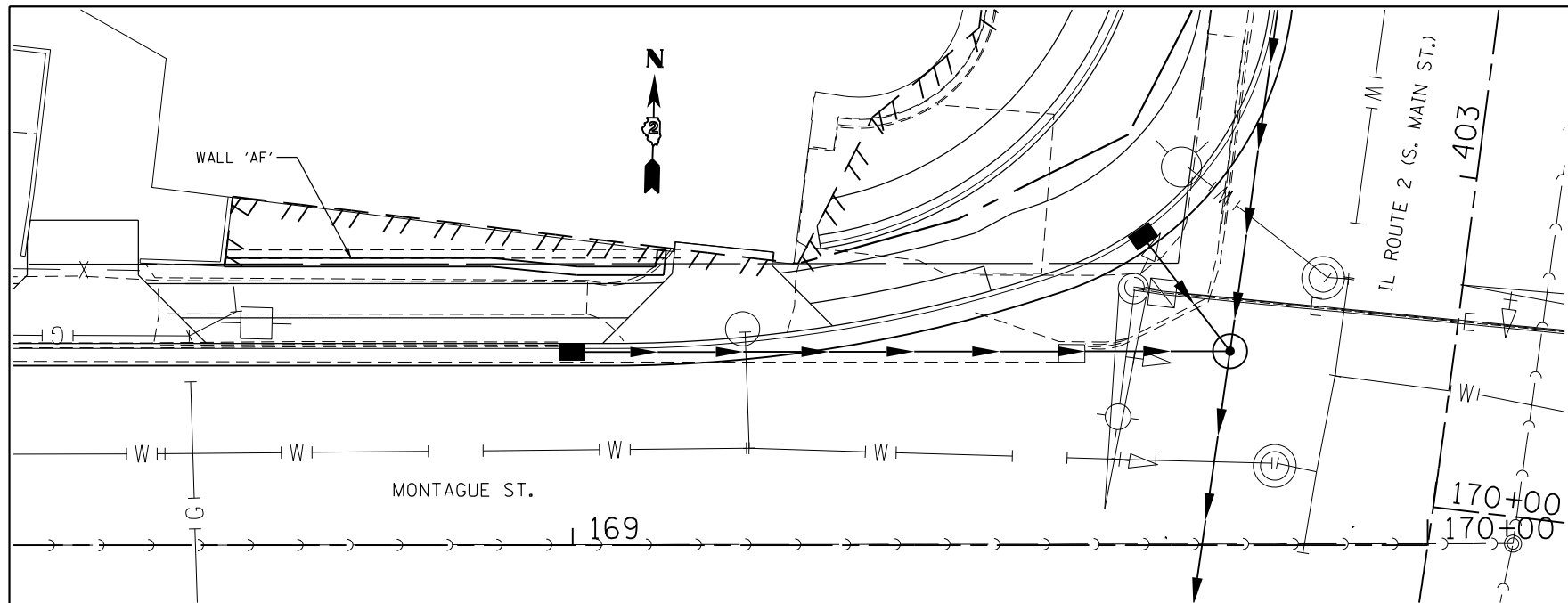
SEGMENTAL CONCRETE BLOCK WALL TYPICAL (LOOMIS)

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FILEL		DRAWN -	REVISED -
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	PLOT DATE = \$DATE*	DATE -	REVISED -

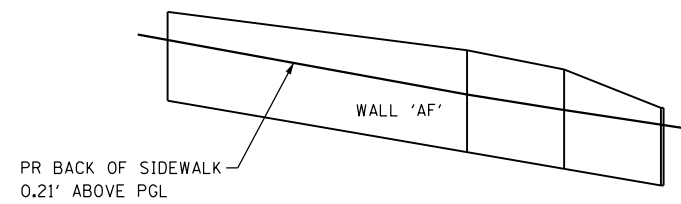
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SEGMENTAL BLOCK WALL DETAIL			
IL 2 LT, FROM MONTAGUE ST TO LOOMIS ST			
SCALE: NTS	SHEET NO. 11 OF 12 SHEETS	STA.	TO STA.

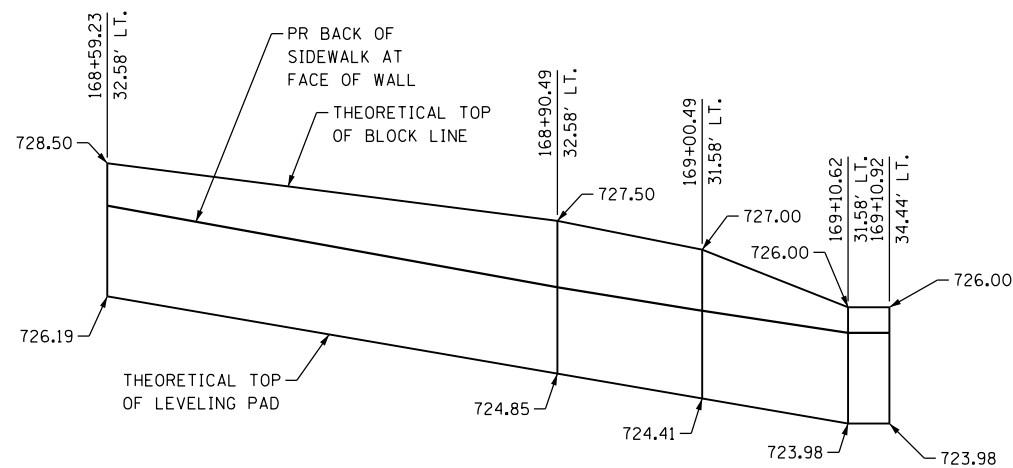
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32,33) R-1	WINNEBAGO	705	529
CONTRACT NO. 64821				
ILLINOIS FED. AID PROJECT				



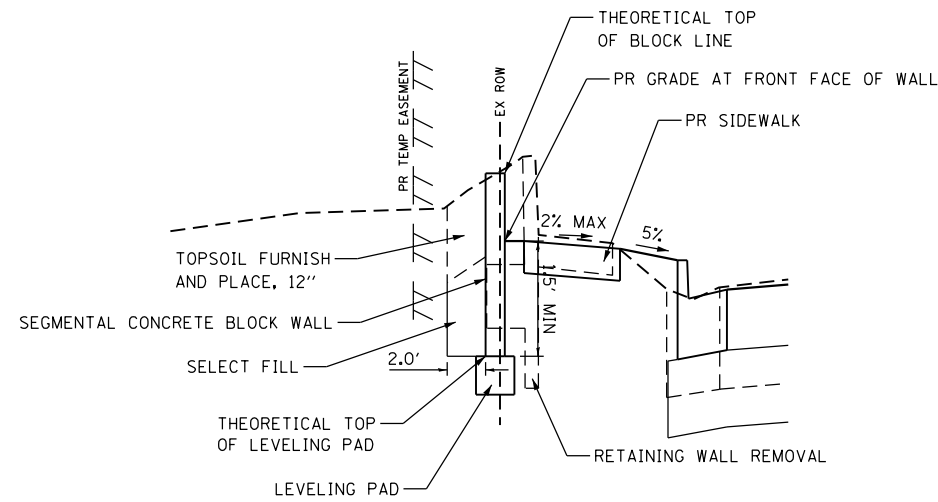
SEGMENTAL CONCRETE BLOCK WALL PLAN



SEGMENTAL CONCRETE BLOCK WALL ELEVATION



WALL 'AF' ELEVATION
SEGMENTAL CONCRETE BLOCK WALL (133 SQ FT)

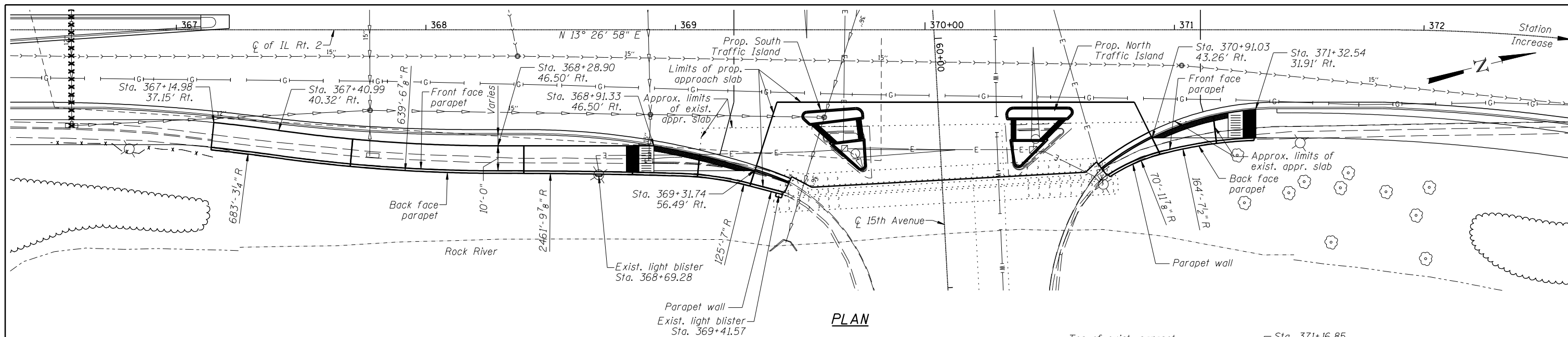


SEGMENTAL CONCRETE BLOCK WALL TYPICAL (MONTAGUE)

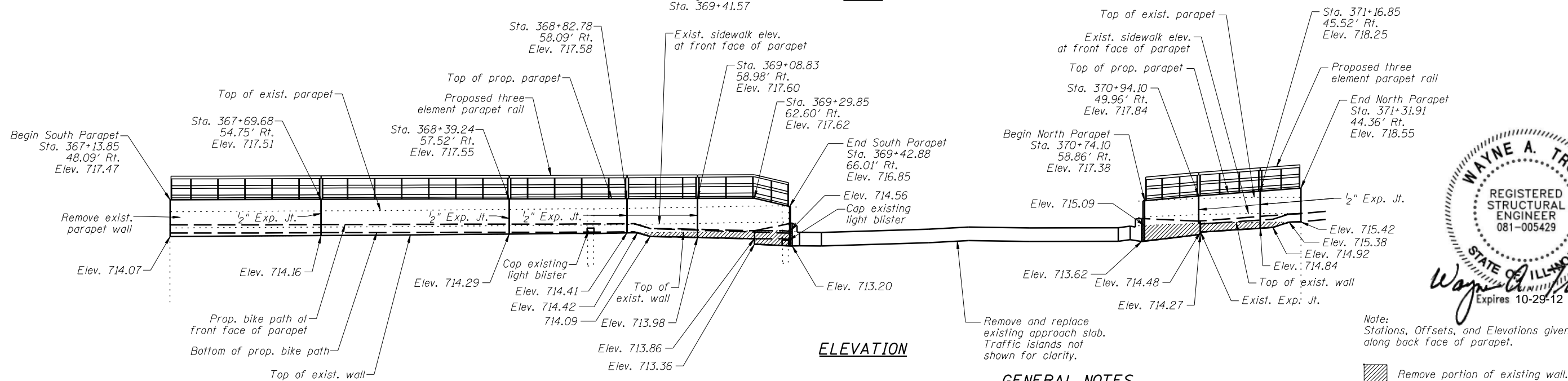
NOTES:

1. ALL ELEVATIONS ARE GIVEN TO THE FRONT FACE OF THE PROPOSED SEGMENTAL CONCRETE BLOCK WALL.
 2. SEE DISTRICT DETAIL 63.2, "PIPE HANDRAILS FOR STEPS", FOR DETAILS ON PIPE HANDRAILS.
 3. SEE DISTRICT DETAIL 71.4, "DETAIL OF CONCRETE STEPS", FOR DETAILS ON CONCRETE STEPS.
 4. SEE PLAT OF HIGHWAYS FOR EXISTING RIGHT-OF-WAY, PROPOSED RIGHT-OF-WAY AND TEMPORARY EASEMENTS.
 5. THE PROPOSED STEPS SHALL MATCH EXISTING CONDITIONS. THE TOP OF STAIR ELEVATIONS SHALL BE VERIFIED PRIOR TO DESIGN OF THE SEGMENTAL CONCRETE BLOCK WALL.
 6. IF THE WALL MANUFACTURER'S DESIGN REQUIRES PIPE UNDERDRAINS, THE PIPE UNDERDRAINS SHALL MEET THE MATERIAL AND CONSTRUCTION REQUIREMENTS OF GBSP "PIPE UNDERDRAINS FOR STRUCTURES." IF NEEDED THE PIPE UNDERDRAINS SHALL BE CONNECTED TO THE PROPOSED STORM SEWER. THE PIPE UNDERDRAINS SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR SEGMENTAL CONCRETE BLOCK WALL.
 7. THE LIMITS OF SELECT BACKFILL HAVE BEEN SET THE FOLLOWING DISTANCE (D) BEHIND THE PROPOSED SEGMENTAL CONCRETE BLOCK WALL (0.70 X H) TO ESTABLISH EXCAVATION LIMITS. SEE THE SUPPLIERS SHOP DRAWINGS FOR ACTUAL EXCAVATION LIMITS FOR SELECT BACKFILL. EXCAVATION REQUIRED BEYOND THE LIMITS ESTABLISHED FOR THE SELECT BACKFILL BELOW BY THE SUPPLIERS DESIGN SHALL BE CONSIDERED OVER EXCAVATION AND SHALL NOT BE MEASURED FOR PAYMENT. THE COST OF EXCAVATION REQUIRED FOR WALL CONSTRUCTION SHALL BE INCLUDED IN THE CONTRACT UNIT COST FOR SEGMENTAL CONCRETE BLOCK WALL.
- LOCATION: D:
STA 168+59.23 (MONTAGUE)
TO STA 169+10.92 (MONTAGUE) = 2.0'
8. GEOSYNTHETIC REINFORCING, IF NEEDED, SHALL BE PER THE SUPPLIER'S DESIGN.

FILE NAME = *FILEL*	USER NAME = *USER*	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SEGMENTAL BLOCK WALL DETAIL MONTAGUE ST LT, WEST OF IL 2			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISED -		SCALE: NTS	SHEET NO. 12 OF 12 SHEETS	STA.	TO STA.	742	(32,33) R-1	WINNEBAGO	705	530
		CHECKED -	REVISED -					CONTRACT NO. 64821					
		DATE -	REVISED -					ILLINOIS FED. AID PROJECT					



PLAN



ELEVATION



Note:
Stations, Offsets, and Elevations given along back face of parapet.

Remove portion of existing wall.

INDEX OF SHEETS

Sheet No.	Description
1	General Plan and Elevation
2	Bike Path Elevations
3	Approach Slab Elevations
4	South Bike Path Plan & Elevation
5	North Bike Path Plan & Elevation
6	Bike Path Reinforcement & Details
7	Parapet Railing Details
8	Approach Slab Plan
9-10	Stage I Approach Slab
11-12	Stage II Approach Slab
13-14	Traffic Island Details
15	Approach Slab Details & Sections
16-17	Approach Slab Reinforcement
18	Bar Splacers

TOTAL BILL OF MATERIAL

Item	Unit	Quantity
Detectable Warnings	Sq Ft	212
Concrete Removal	Cu Yd	30
Concrete Structures	Cu Yd	161.2
Concrete Superstructure	Cu Yd	229.5
Bridge Deck Grooving	Sq Yd	499
Protective Coat	Sq Yd	920
Reinforcement Bars, Epoxy Coated	Pound	82,342
Bar Splacers	Each	112
Parapet Railing	Foot	291
Removal of Asbestos Cement Conduit	Foot	6
Approach Slab Removal	Sq Yd	456

GENERAL NOTES

1. Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.
2. Reinforcement bars designated (E) shall be epoxy coated.
3. All exposed edges shall be chamfered $\frac{3}{4}$ " except as noted.
4. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

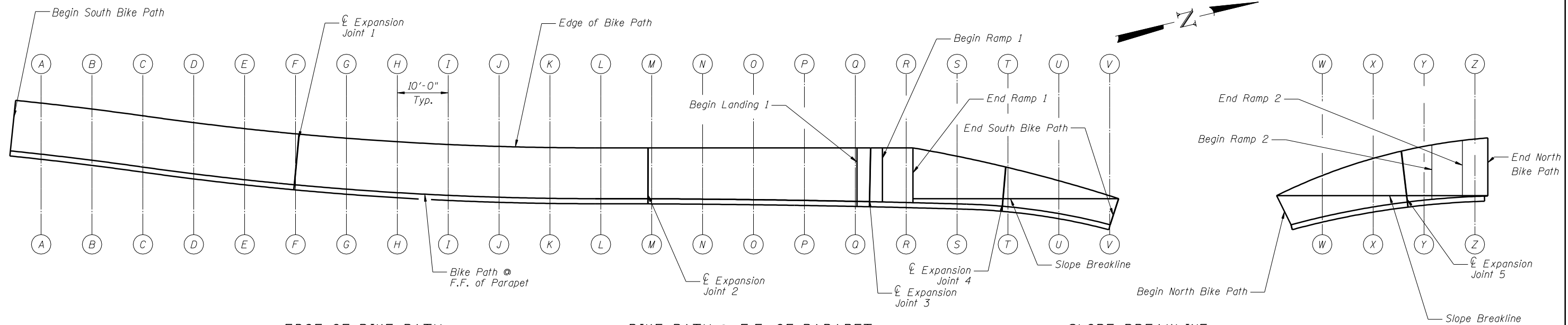
DESIGN SPECIFICATIONS

2007 AASHTO LRFD
Bridge Design Specifications
with 2008 and 2009 Interims

DESIGN STRESSES

$f'_c = 3,500$ psi (Concrete)
 $f_y = 60$ ksi (Reinforcement)

GENERAL PLAN & ELEVATION
IL 2 (RT.) BIKE PATH AT 15TH AVENUE
F.A.P. RTE. 742 - SEC. (32.33) R-1
WINNEBAGO COUNTY
STA. 367+13.85 TO STA. 371+32.54



EDGE OF BIKE PATH

Location	C of IL Rt. 2		Theoretical Grade Elevations
	Station	Offset	
Begin South Bike Path	367+14.98	37.15	714.94
A-A	367+20.00	37.68	714.94
B-B	367+30.00	38.86	714.95
C-C	367+40.00	40.18	714.96
D-D	367+50.00	41.53	714.98
E-E	367+60.00	42.71	714.99
F-F	367+70.00	43.73	715.01
C Expansion Joint 1	367+70.69	43.80	715.01
G-G	367+80.00	44.60	715.02
H-H	367+90.00	45.30	715.04
I-I	368+00.00	45.84	715.05
J-J	368+10.00	46.22	715.07
K-K	368+20.00	46.44	715.08
L-L	368+30.00	46.50	715.10
C Expansion Joint 2	368+39.28	46.50	715.12
M-M	368+40.00	46.50	715.12
N-N	368+50.00	46.50	715.11
O-O	368+60.00	46.50	715.10
P-P	368+70.00	46.50	715.09
Q-Q	368+80.00	46.50	715.08
Begin Landing 1	368+80.33	46.50	715.08
C Expansion Joint 3	368+83.03	46.50	715.09
Begin Ramp 1	368+85.33	46.50	715.10
R-R	368+90.00	46.50	714.73
End Ramp 1	368+91.33	46.50	714.62
S-S	369+00.00	48.14	714.59
C Expansion Joint 4	369+09.58	50.27	714.56
T-T	369+10.00	50.37	714.56
U-U	369+20.00	52.96	714.52
V-V	369+30.00	55.93	714.49
End South Bike Path	369+31.78	56.50	714.48
Begin North Bike Path	370+91.03	43.26	715.18
W-W	371+00.00	39.31	715.28
X-X	371+10.00	35.94	715.40
C Expansion Joint 5	371+15.54	34.50	715.46
Y-Y	371+20.00	33.56	715.50
Begin Ramp 2	371+21.54	33.28	715.52
End Ramp 2	371+27.54	32.40	716.02
Z-Z	371+30.00	32.13	716.05
End North Bike Path	371+32.54	31.91	716.07

BIKE PATH @ F.F. OF PARAPET

Location	C of IL Rt. 2		Theoretical Grade Elevations
	Station	Offset	
Begin South Bike Path	367+13.95	47.10	715.14
A-A	367+20.00	47.75	715.14
B-B	367+30.00	49.94	715.15
C-C	367+40.00	50.28	715.16
D-D	367+50.00	51.61	715.16
E-E	367+60.00	52.77	715.17
C Expansion Joint 1	367+69.77	53.76	715.18
F-F	367+70.00	53.78	715.18
G-G	367+80.00	54.63	715.18
H-H	367+90.00	55.31	715.19
I-I	368+00.00	55.85	715.20
J-J	368+10.00	56.22	715.20
K-K	368+20.00	56.44	715.21
L-L	368+30.00	56.50	715.22
C Expansion Joint 2	368+39.24	56.52	715.22
M-M	368+40.00	56.53	715.22
N-N	368+50.00	56.59	715.23
O-O	368+60.00	56.70	715.23
P-P	368+70.00	56.84	715.24
Q-Q	368+80.00	57.03	715.25
Begin Landing 1	368+80.33	57.04	715.25
C Expansion Joint 3	368+82.80	57.09	715.25
Begin Ramp 1	368+85.33	57.15	715.26
R-R	368+90.00	57.26	714.93
End Ramp 1	368+91.33	57.29	714.84
S-S	369+00.00	57.53	714.78
C Expansion Joint 4	369+08.91	57.99	714.73
T-T	369+10.00	58.09	714.72
U-U	369+20.00	59.43	714.67
V-V	369+30.00	61.61	714.61
End South Bike Path	369+30.08	61.63	714.61
Begin North Bike Path	370+93.86	48.99	715.50
W-W	371+00.00	47.46	715.62
X-X	371+10.00	45.49	715.80
C Expansion Joint 5	371+16.73	44.52	715.93
Y-Y	371+20.00	44.16	715.99
Begin Ramp 2	371+21.54	44.00	716.02
End Ramp 2	371+27.54	43.55	716.13
Z-Z	371+30.00	43.43	716.17
End North Bike Path	372+32.54	43.34	716.22

SLOPE BREAKLINE

Location	C of IL Rt. 2		Theoretical Grade Elevations
	Station	Offset	
End Ramp 1	368+91.33	56.50	714.82
S-S	369+00.00	56.50	714.75
C Expansion Joint 4	369+09.04	56.50	714.67
T-T	369+10.00	56.50	714.66
U-U	369+20.00	56.50	714.58
V-V	369+30.00	56.50	714.50
End South Bike Path	369+31.78	56.50	714.48
Begin North Bike Path	370+91.03	43.28	715.18
W-W	371+00.00	43.28	715.32
X-X	371+10.00	43.28	715.48
C Expansion Joint 5	371+16.59	43.28	715.59
Y-Y	371+20.00	43.28	715.65
Begin Ramp 2	371+21.54	43.28	715.67
End Ramp 2	371+27.54	43.28	716.15
Z-Z	371+30.00	43.28	716.17
End North Bike Path	372+32.54	43.28	716.22



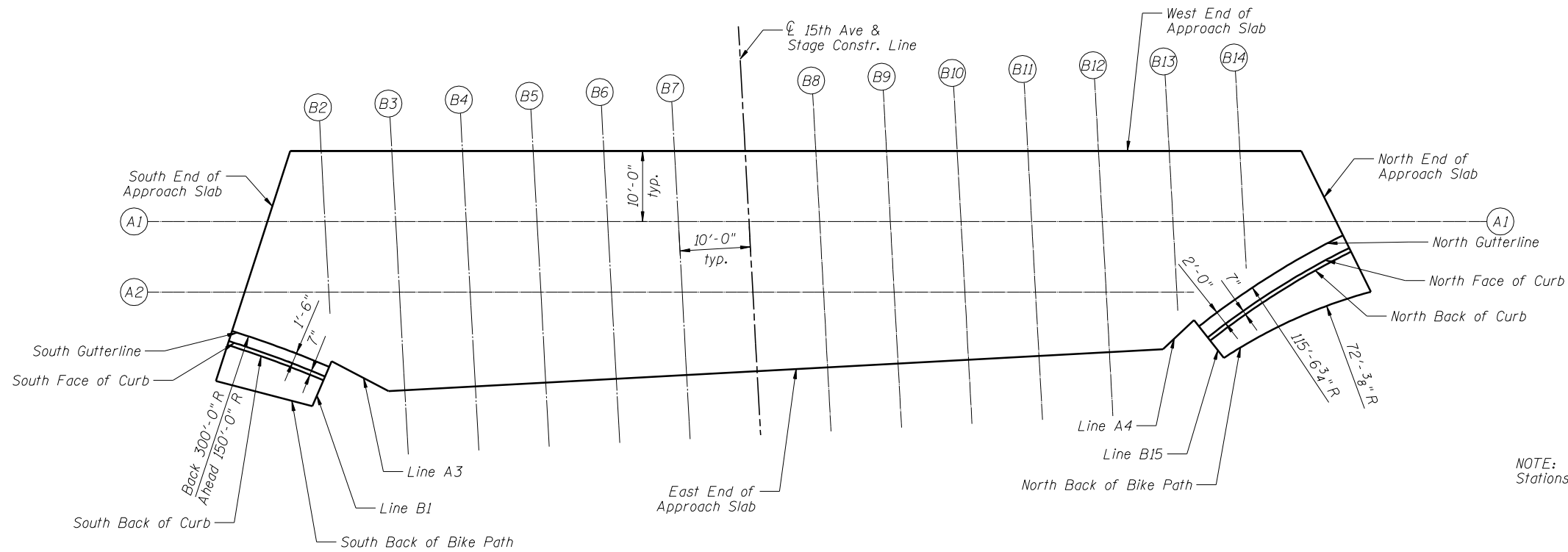
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		DRAWN - WSP	REVISED
		CHECKED - ASP	REVISED

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 2 (RT.) BIKE PATH AT 15TH AVENUE
BIKE PATH ELEVATIONS**

SHEET NO. 2 OF 18 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32.33) R-1	WINNEBAGO	705	532
			CONTRACT NO. 64821	
ILLINOIS FED. AID PROJECT				



NOTE:
Stations and offsets given from CL IL 2

WEST END OF APPROACH

Location	CL of IL Rt. 2		Theoretical Grade Elevations
	Station	Offset	
B2	369+45.05	29.00	715.19
B3	369+55.07	29.00	715.22
B4	369+65.08	29.00	715.25
B5	369+75.10	29.00	715.28
B6	369+85.11	29.00	715.31
B7	369+95.13	29.00	715.34
CL 15th Avenue	370+05.14	29.00	715.37
B8	370+15.16	29.00	715.40
B9	370+25.17	29.00	715.43
B10	370+35.19	29.00	715.46
B11	370+45.20	29.00	715.49
B12	370+55.22	29.00	715.52
B13	370+65.23	29.00	715.54
B14	370+75.24	29.00	715.56

LINE A1

Location	CL of IL Rt. 2		Theoretical Grade Elevations
	Station	Offset	
B2	369+45.60	39.00	714.96
B3	369+55.61	39.00	715.00
B4	369+65.63	39.00	715.03
B5	369+75.64	39.00	715.08
B6	369+85.66	39.00	715.11
B7	369+95.67	39.00	715.16
CL 15th Avenue	370+05.68	39.00	715.22
B8	370+15.70	39.00	715.24
B9	370+25.71	39.00	715.25
B10	370+35.73	39.00	715.25
B11	370+45.74	39.00	715.28
B12	370+55.76	39.00	715.30
B13	370+65.77	39.00	715.30
B14	370+75.79	39.00	715.30

LINE A2

Location	CL of IL Rt. 2		Theoretical Grade Elevations
	Station	Offset	
B2	369+46.14	49.00	714.72
B3	369+56.15	49.00	714.77
B4	369+66.17	49.00	714.82
B5	369+76.18	49.00	714.87
B6	369+86.20	49.00	714.92
B7	369+96.21	49.00	714.99
CL 15th Avenue	370+06.23	49.00	715.08
B8	370+16.24	49.00	715.08
B9	370+26.26	49.00	715.07
B10	370+36.27	49.00	715.03
B11	370+46.29	49.00	715.06
B12	370+56.30	49.00	715.07
B13	370+66.32	49.00	715.06

EAST END OF APPROACH*

Location	CL of IL Rt. 2		Theoretical Grade Elevations
	Station	Offset	
A3	369+54.62	63.06	714.44
B3	369+56.91	62.94	714.46
B4	369+66.90	62.40	714.53
B5	369+76.88	61.85	714.61
B6	369+86.87	61.31	714.68
B7	369+96.85	60.77	714.78
CL 15th Avenue	370+06.84	60.23	714.91
B8	370+16.82	59.69	714.90
B9	370+26.81	59.15	714.89
B10	370+36.79	58.61	714.83
B11	370+46.78	58.07	714.87
B12	370+56.77	57.53	714.88
A4	370+64.34	57.12	714.88

SOUTH END OF APPROACH

Location	CL of IL Rt. 2		Theoretical Grade Elevations
	Station	Offset	
W. End of Approach	369+40.66	29.00	715.18
A1	369+37.43	39.00	714.92
A2	369+34.20	49.00	714.66
S. Gutterline	369+32.42	54.52	714.52
S. Face of Curb	369+31.96	55.94	714.44
S. Back of Curb	369+31.78	56.50	714.48
S. Back of Sidewalk	369+30.13	61.60	714.61

LINE B1*

Location	CL of IL Rt. 2		Theoretical Grade Elevations
	Station	Offset	
A3	369+46.53	58.79	714.47
S. Gutterline	369+46.19	59.60	714.47
S. Face of Curb	369+45.60	60.98	714.46
S. Back of Curb	369+45.37	61.52	715.09
S. Back of Sidewalk	369+43.82	65.18	715.31

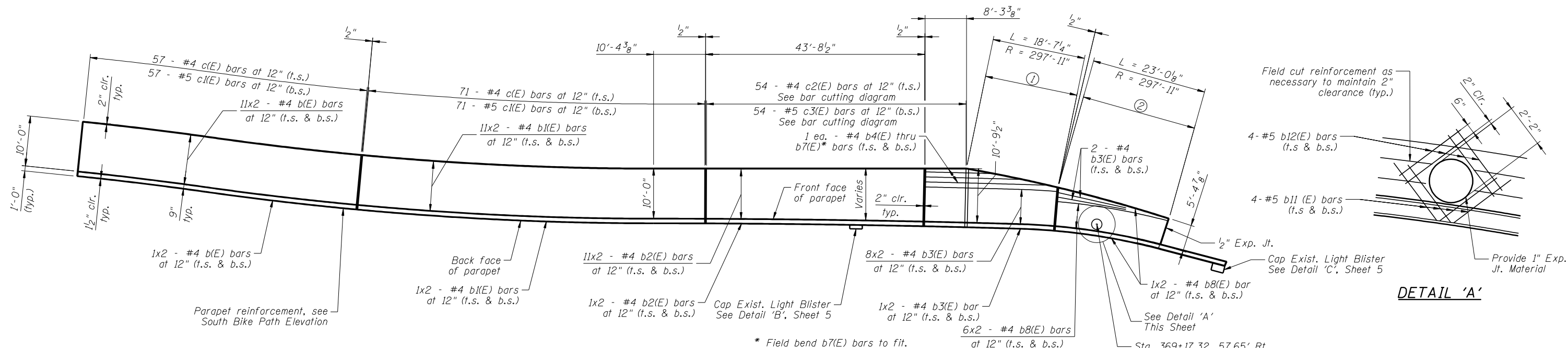
LINE B15*

Location	CL of IL Rt. 2		Theoretical Grade Elevations
	Station	Offset	
A4	370+68.75	52.96	715.02
N. Gutterline	370+69.46	53.86	715.00
N. Face of Curb	370+70.70	55.43	714.95
N. Back of Curb	370+71.06	55.88	715.69
N. Back of Sidewalk	370+72.99	58.34	715.73

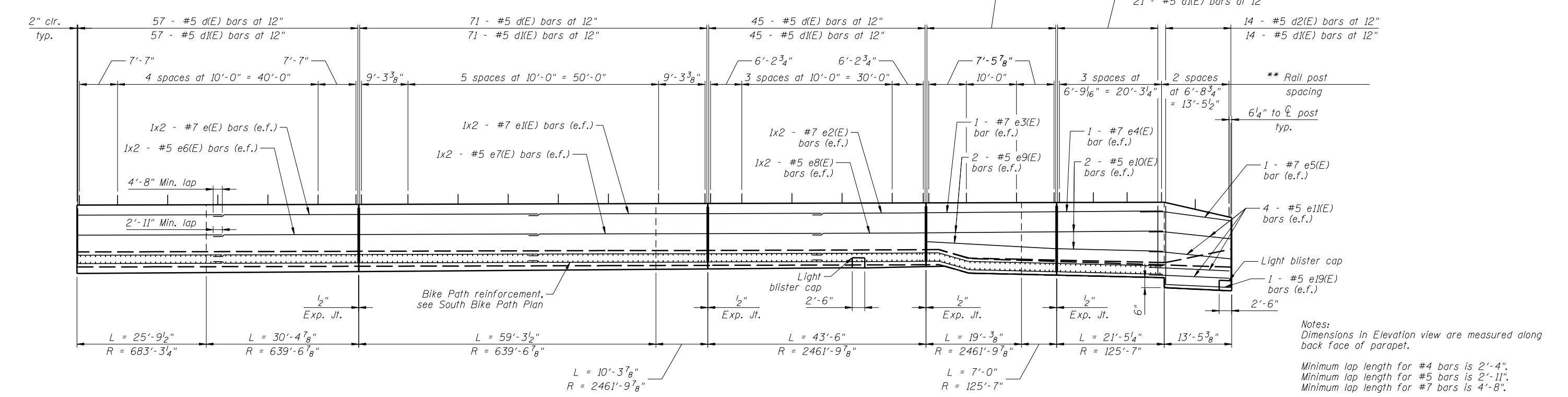
NORTH END OF APPROACH

Location	CL of IL Rt. 2		Theoretical Grade Elevations
	Station	Offset	
W. End of Approach	370+83.93	29.00	715.60
A1	370+88.88	39.00	715.30
N. Gutterline	370+89.85	40.97	715.24
N. Face of Curb	370+90.73	42.76	715.14
N. Back of Curb	370+90.99	43.28	715.18
N. Back of Sidewalk	370+93.81	48.97	715.50

* Finished elevations should match existing 15th Avenue at bridge abutment backwall. Contractor to field verify elevations and adjust transition elevations (Line A1 & A2) as necessary and submit to the Engineer for approval.



SOUTH BIKE PATH PLAN



SOUTH BIKE PATH ELEVATION

- * Field bend b7(E) bars to fit.
- ① 20 - #4 c4(E) bars at 12" (f.s.)
20 - #5 c5(E) bars at 12" (b.s.)
See bar cutting diagram
- ② 24 - #4 c6(E) bars at 12" (f.s.)
24 - #5 c7(E) bars at 12" (b.s.)
See bar cutting diagram

LEGEND

e.f. denotes each face
t.s. denotes top of slab
b.s. denotes bottom of slab

** Contractor to field verify existing expansion joint locations prior to ordering or fabricating parapet railing.

Notes:
Dimensions in Elevation view are measured along back face of parapet.
Minimum lap length for #4 bars is 2'-4".
Minimum lap length for #5 bars is 2'-11".
Minimum lap length for #7 bars is 4'-8".

Bars indicated thus 11x2 - #6 etc. indicates 11 lines of bars with 2 lengths per line.

Location of expansion joints should match existing.

Existing reinforcement shall be cleaned and incorporated into the new construction. Cost included with Concrete Removal.

Longitudinal bars in parapet and bike path shall be bent in field to match curvature and maintain clearances.



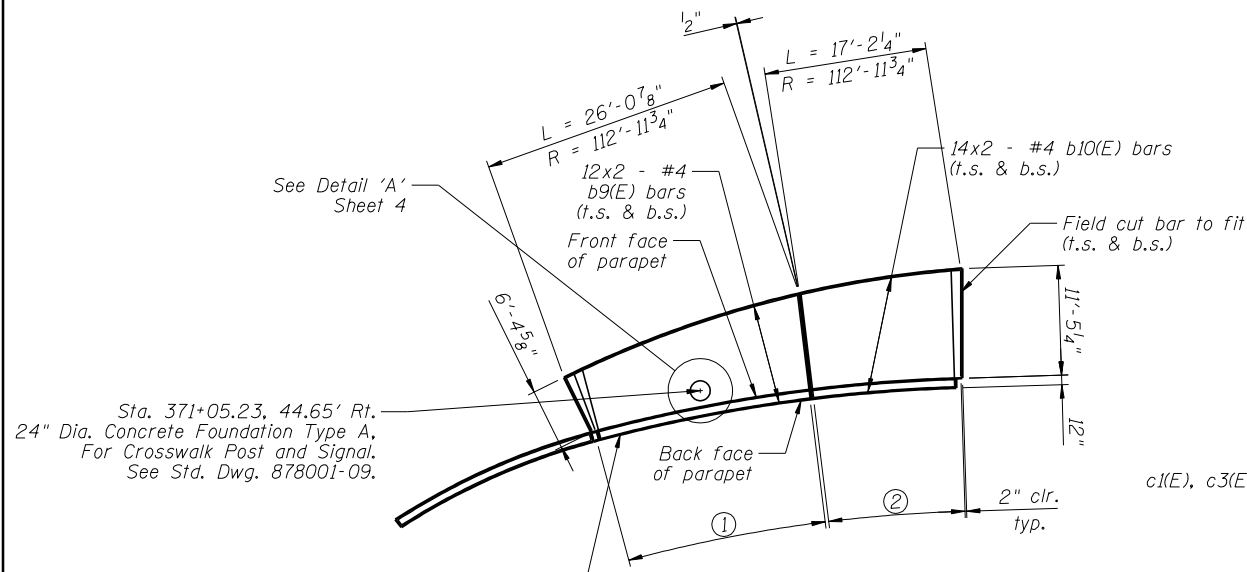
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		CHECKED - ASP	REVISED
PLOT SCALE =	DRAWN - WSP	REVISED	
PLOT DATE =	CHECKED - ASP	REVISED	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 2 (RT.) BIKE PATH AT 15TH AVENUE
SOUTH BIKE PATH PLAN AND ELEVATION**

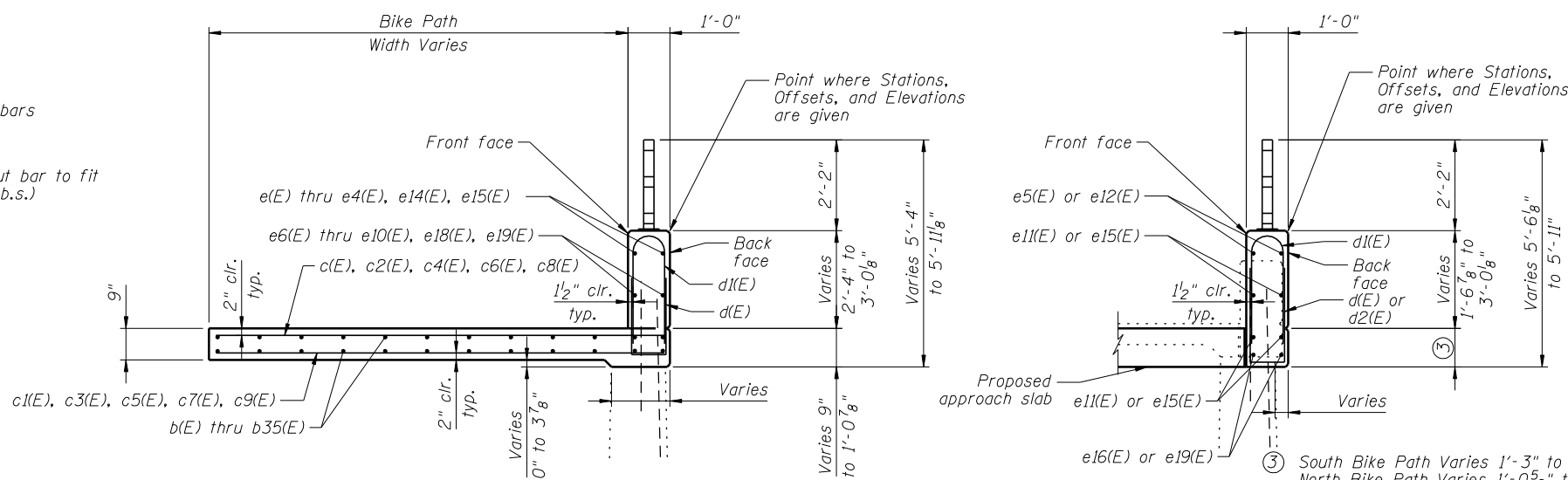
SHEET NO. 4 OF 18 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32.33) R-1	WINNEBAGO	705	534
CONTRACT NO. 64821			ILLINOIS FED. AID PROJECT	



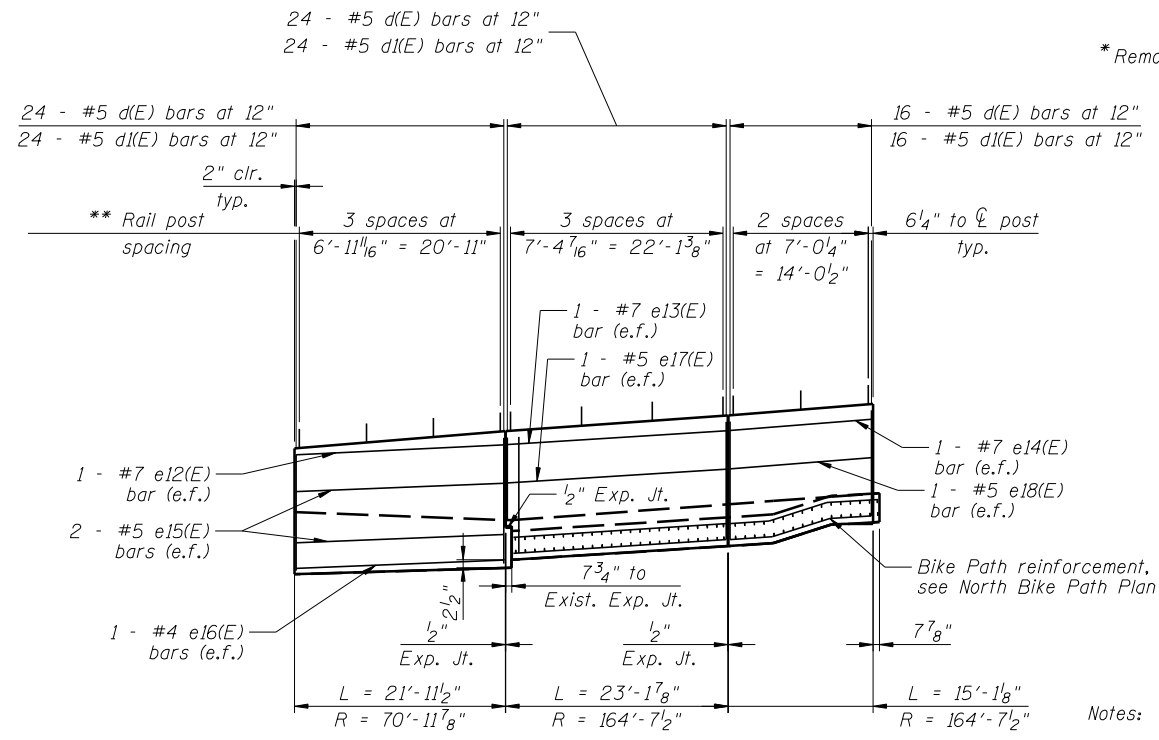
NORTH BIKE PATH PLAN

- ① 28 - #4 c8(E) bars at 12" (f.s.)
28 - #5 c9(E) bars at 12" (b.s.)
See bar cutting diagram
- ② 18 - #4 c10(E) bars at 12" (f.s.)
18 - #5 c11(E) bars at 12" (b.s.)
See bar cutting diagram



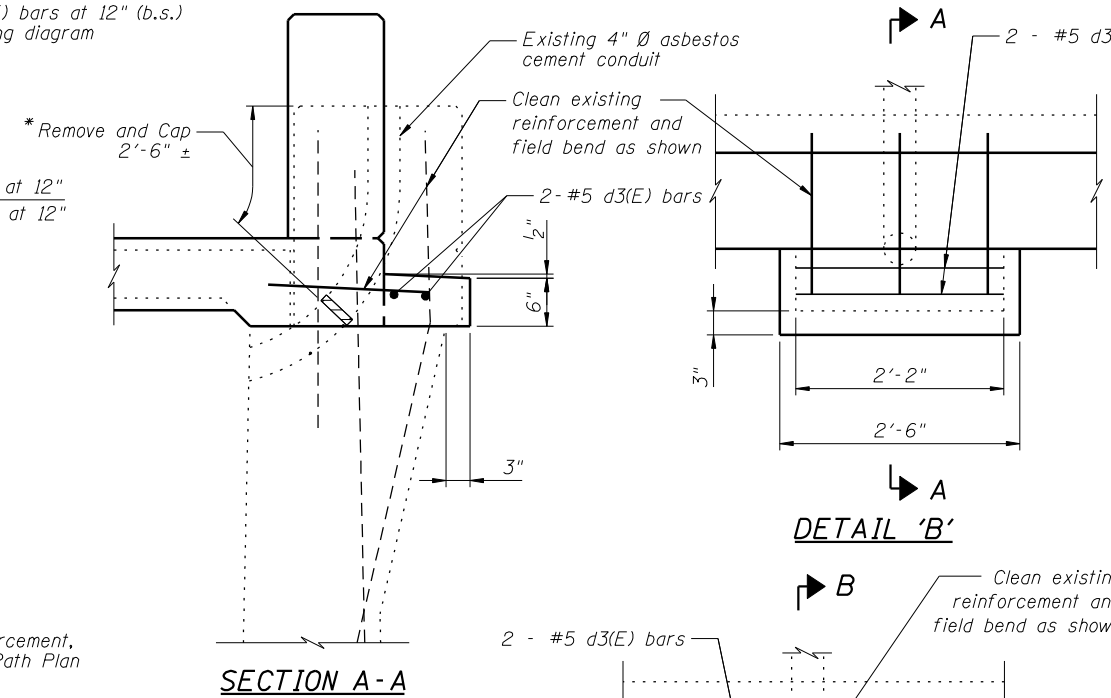
SECTION THROUGH BIKE PATH AND PARAPET

SECTION THROUGH PARAPET WALL AT APPROACH SLAB



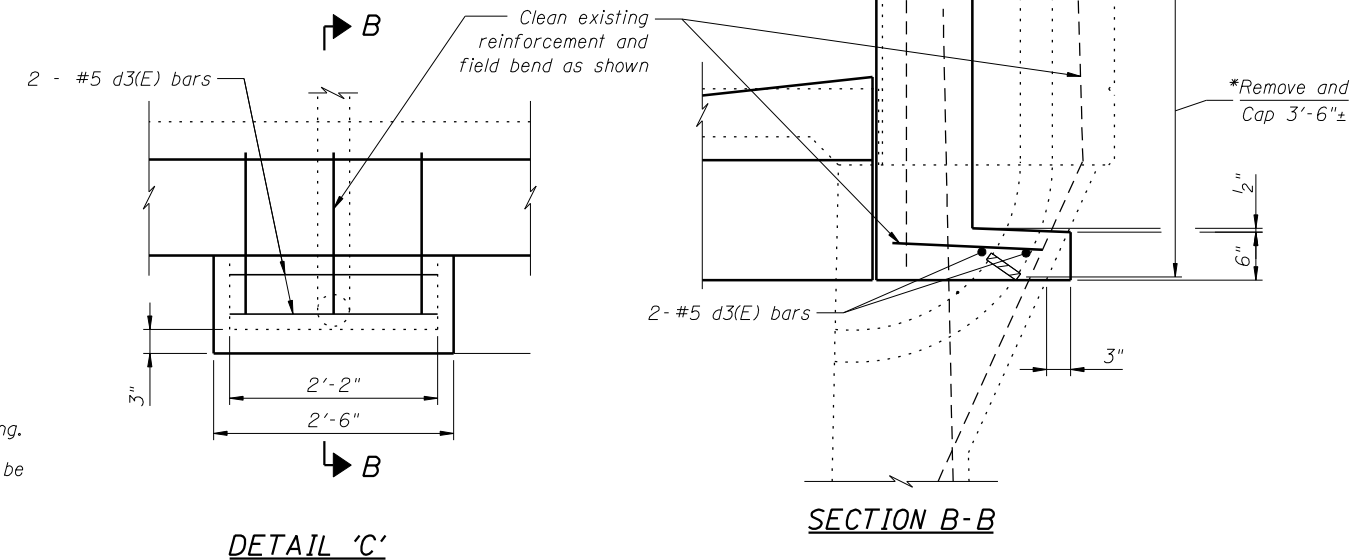
NORTH BIKE PATH ELEVATION

** Contractor to field verify expansion joint locations prior to ordering or fabricating parapet railing.



SECTION A-A

DETAIL 'A'



DETAIL 'C'

SECTION B-B

Notes:
Dimensions in Elevation view are measured along back face of parapet.
Existing reinforcement shall be cleaned and incorporated into the new construction. Cost included with Concrete Removal.
Location of expansion joints should match existing.
Longitudinal bars in parapet and bike path shall be bent in field to match curvature and maintain clearances.

LEGEND

e.f. denotes each face
t.s. denotes top of slab
b.s. denotes bottom of slab



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		CHECKED - ASP	REVISED
PLOT SCALE =		DRAWN - WSP	REVISED
PLOT DATE =		CHECKED - ASP	REVISED

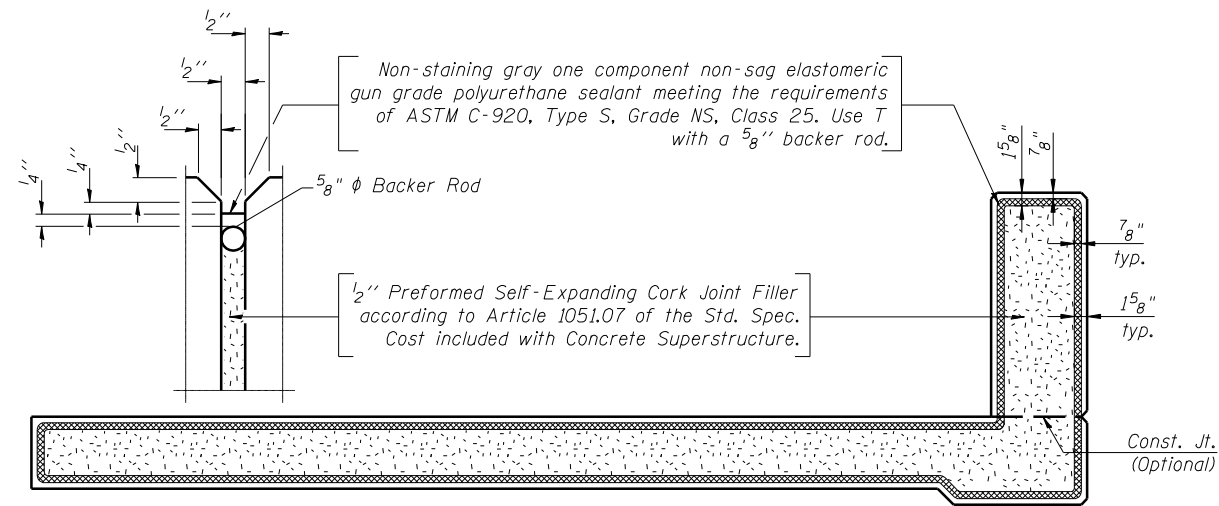
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 2 (RT.) BIKE PATH AT 15TH AVENUE
NORTH BIKE PATH PLAN AND ELEVATION**

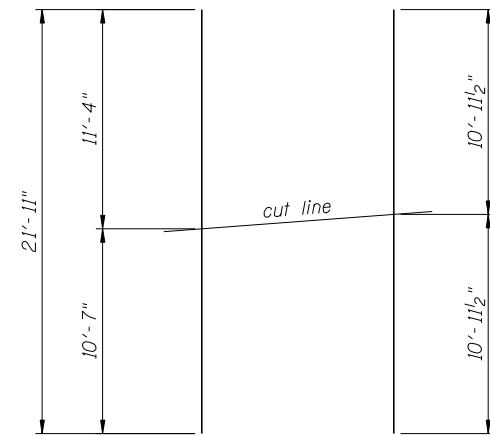
SHEET NO. 5 OF 18 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32.33) R-1	WINNEBAGO	705	535
			CONTRACT NO. 64821	
ILLINOIS FED. AID PROJECT				

**ELEVATED BIKE PATH
BILL OF MATERIAL**

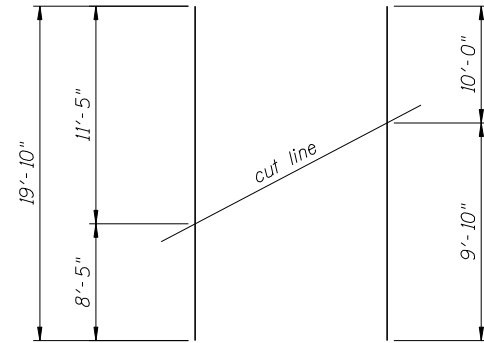


BIKE PATH JOINT DETAILS



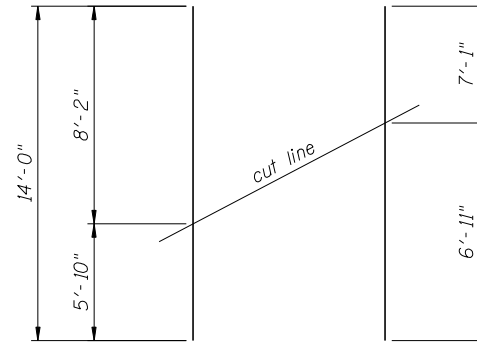
BARS c2(E) & c3(E)

27 bars - 1 set



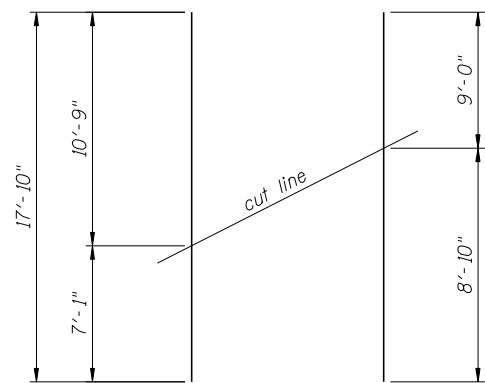
BARS c4(E) & c5(E)

10 bars - 1 set



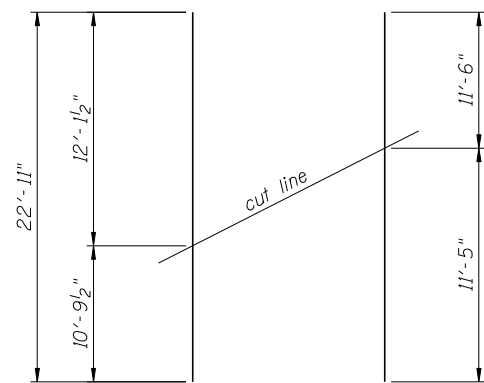
BARS c6(E) & c7(E)

12 bars - 1 set



BARS c8(E) & c9(E)

14 bars - 1 set

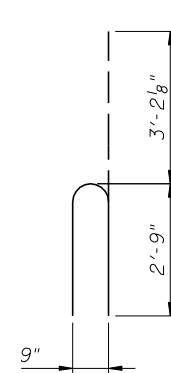


BARS c10(E) & c11(E)

9 bars - 1 set



BAR d(E)



BAR d1(E)



BAR d2(E)

Bar	No.	Size	Length	Shape
b1(E)	48	4	29'-1"	—
b2(E)	48	4	35'-10"	—
b3(E)	40	4	22'-11"	—
b4(E)	2	4	14'-3"	—
b5(E)	2	4	23'-11"	—
b6(E)	2	4	18'-8"	—
b7(E)	2	4	12'-10"	—
b8(E)	2	4	26'-6"	—
b9(E)	32	4	12'-4"	—
b10(E)	48	4	14'-0"	—
b11(E)	56	4	9'-7"	—
b12(E)	16	5	3'-7"	—
b12(E)	16	5	3'-10"	—
c(E)	128	4	10'-8"	—
c1(E)	128	5	10'-8"	—
c2(E)	27	4	21'-11"	—
c3(E)	27	5	21'-11"	—
c4(E)	10	4	19'-10"	—
c5(E)	10	5	19'-10"	—
c6(E)	12	4	14'-0"	—
c7(E)	12	5	14'-0"	—
c8(E)	14	4	17'-10"	—
c9(E)	14	5	17'-10"	—
c10(E)	9	4	22'-11"	—
c11(E)	9	5	22'-11"	—
d(E)	285	5	5'-1"	□
d1(E)	299	5	5'-11"	□
d2(E)	14	5	6'-1"	□
d3(E)	4	5	2'-2"	—
e(E)	4	7	30'-6"	—
e1(E)	4	7	37'-2"	—
e2(E)	4	7	23'-11"	—
e3(E)	2	7	25'-8"	—
e4(E)	2	7	20'-11"	—
e5(E)	2	7	18'-5"	—
e6(E)	4	5	29'-7"	—
e7(E)	4	5	36'-3"	—
e8(E)	4	5	23'-1"	—
e9(E)	4	5	25'-8"	—
e10(E)	4	5	20'-11"	—
e11(E)	8	5	16'-8"	—
e12(E)	2	7	21'-8"	—
e13(E)	2	7	22'-11"	—
e14(E)	2	7	14'-10"	—
e15(E)	4	5	21'-8"	—
e16(E)	2	4	21'-8"	—
e17(E)	2	5	22'-11"	—
e18(E)	2	5	14'-10"	—
e19(E)	2	5	13'-0"	—
Reinforcement Bars, Epoxy Coated Item # 50800205	Pound	14,898		
Protective Coat Item # 50300300	Sq. Yd.	386		
Concrete Structures Item # 50300225	Cu. Yd.	106.4		
Concrete Removal* Item # 50102400	Cu. Yd.	30		
Parapet Railing Item # 50901750	Foot	291		
Defectable Warnings Item # 42400800	Sq. Ft.	136		
Removal of Asbestos Cement Conduit Item # X0324198	Foot	6		

* Include the cost of existing rail removal with the unit price bid for Concrete Removal.



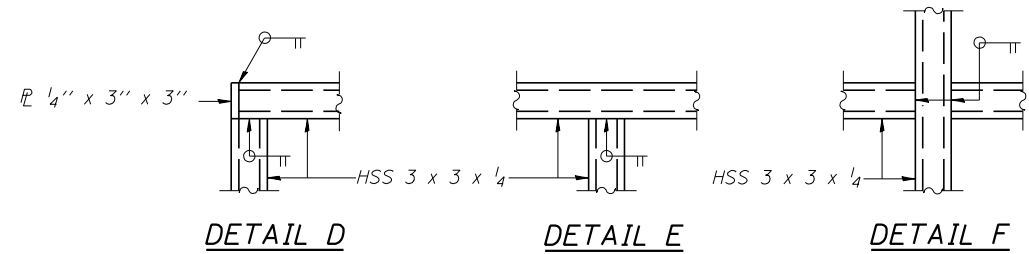
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

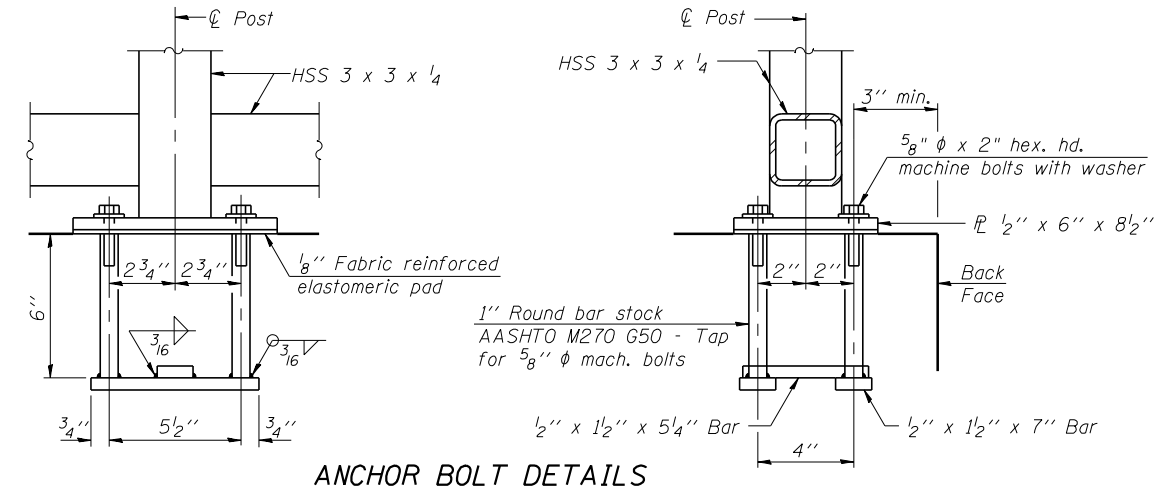
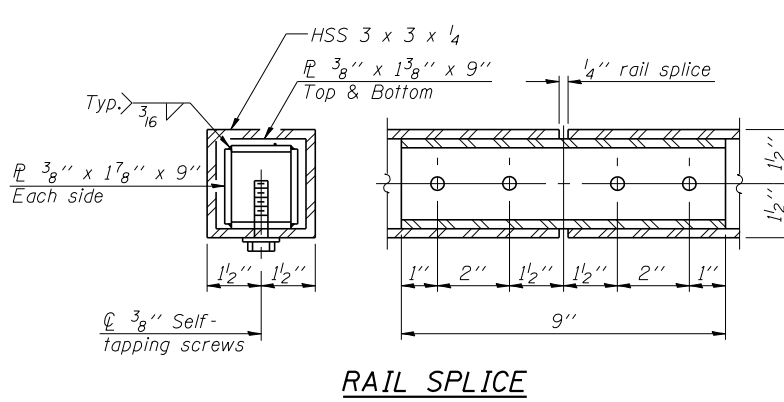
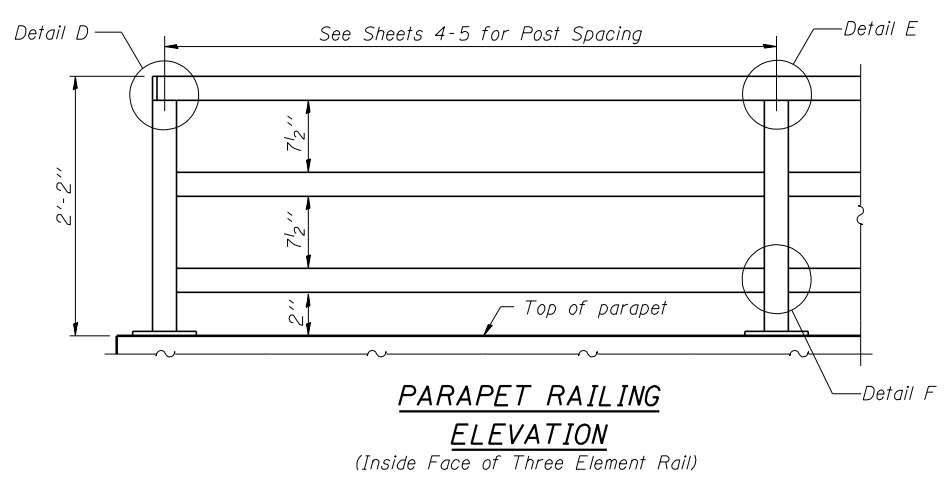
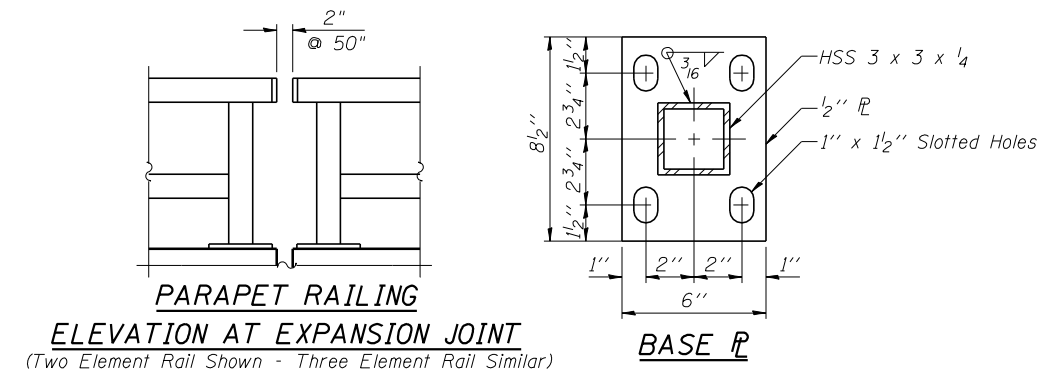
**IL 2 (RT.) BIKE PATH AT 15TH AVENUE
REINFORCEMENT & DETAILS**

SHEET NO. 6 OF 18 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32.33) R-1	WINNEBAGO	705	536
			CONTRACT NO. 64821	
ILLINOIS FED. AID PROJECT				



All steel rail elements shall be galvanized according to Article 509.05 of the Standard Specifications.



In lieu of the cast-in-place anchor device shown, the Contractor has the option of drilling and setting 5/8\"/>

R-29 7-1-10 (10'-0\"/>



FILE NAME =	USER NAME =	DESIGNED - ASP	REVISED
		CHECKED - WSP	REVISED
		DRAWN - LNB	REVISED
		CHECKED - ASP	REVISED

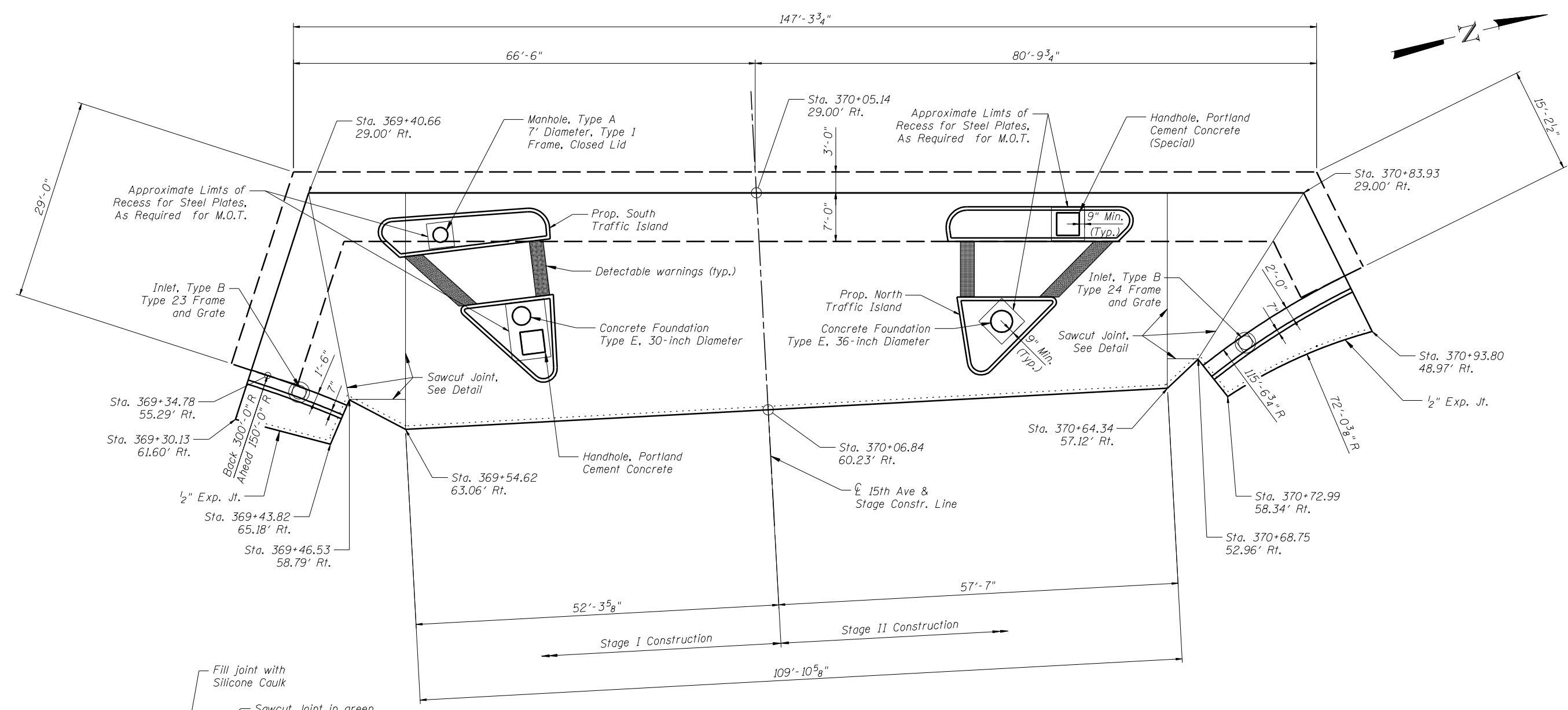
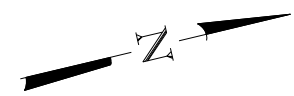
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 2 (RT.) BIKE PATH AT 15TH AVENUE
PARAPET RAILING DETAILS

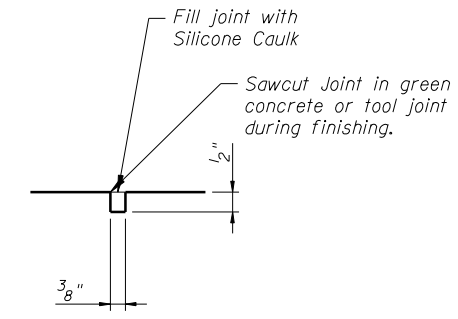
SHEET NO. 7 OF 18 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32.33) R-1	WINNEBAGO	705	537
			CONTRACT NO. 64821	

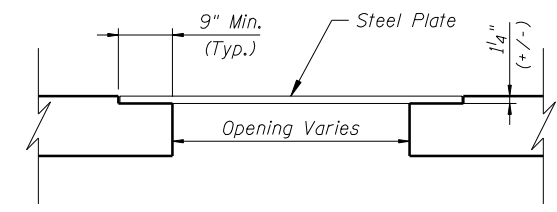
ILLINOIS FED. AID PROJECT



PLAN
Stations and Offsets given from \hat{C} IL 2



SAWCUT JOINT
Include cost of sawcut joint in unit price bid for Concrete Superstructure.



RECESS FOR STEEL PLATES
Note: Limits of Recess Shall Stay Within Limits of Traffic Island.



FILE NAME =	USER NAME =	DESIGNED - DPW	REVISED
		CHECKED - ASP	REVISED
		DRAWN - LNB	REVISED
		CHECKED - ASP	REVISED

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 2 (RT.) BIKE PATH AT 15TH AVENUE
APPROACH SLAB PLAN**

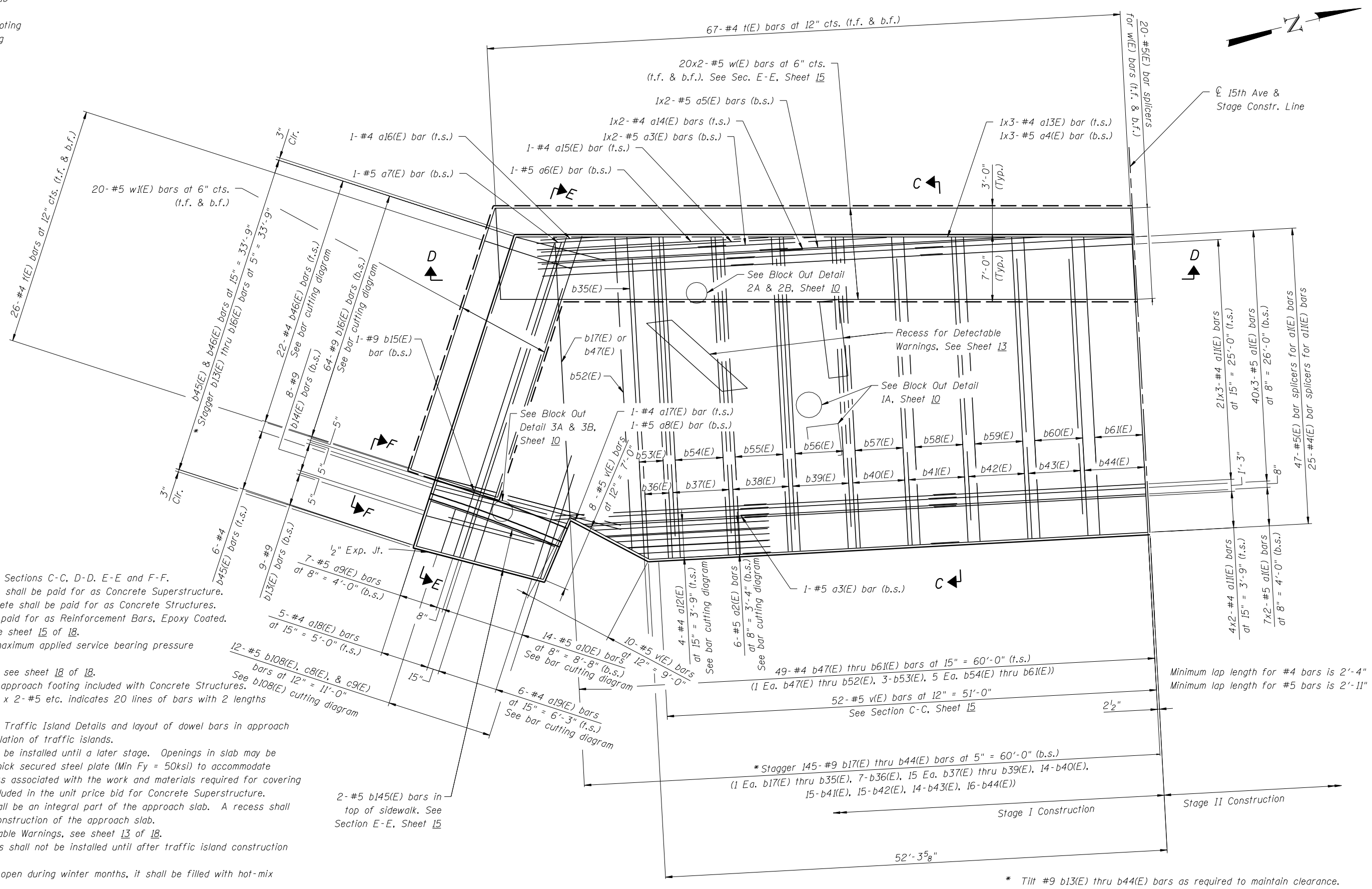
SHEET NO. 8 OF 18 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32.33) R-1	WINNEBAGO	705	538
CONTRACT NO. 64821				

ILLINOIS FED. AID PROJECT

LEGEND

b.s. denotes bottom of slab
 t.s. denotes top of slab
 b.f. denotes bottom of footing
 t.f. denotes top of footing
 Ea. denotes each



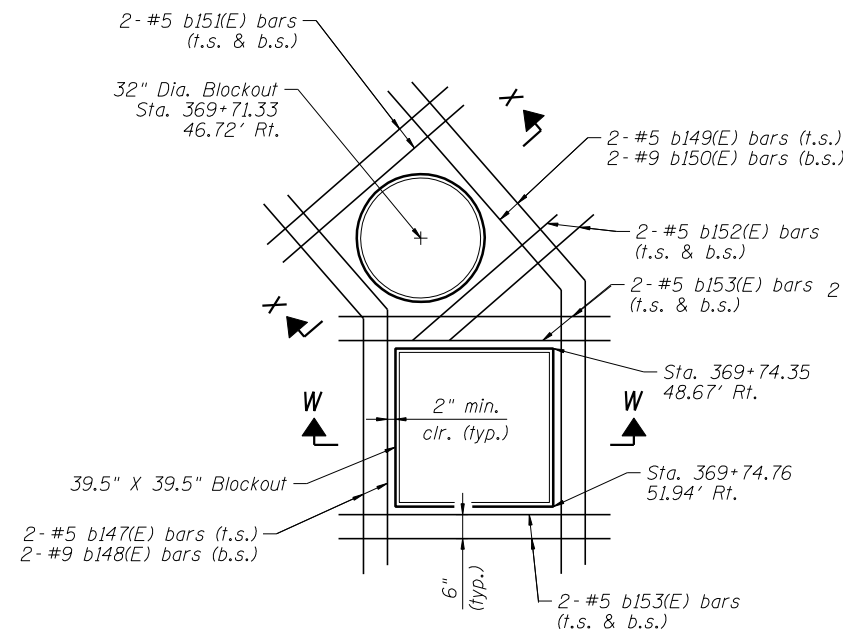
Notes:
 See sheet 15 of 18 for Sections C-C, D-D, E-E and F-F.
 Approach slab concrete shall be paid for as Concrete Superstructure.
 Approach footing concrete shall be paid for as Concrete Structures.
 Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated.
 For v(E) bar details see sheet 15 of 18.
 The approach footing maximum applied service bearing pressure (Qmax) = 2.0 ksf
 For bar splicer details, see sheet 18 of 18.
 Cost of excavation for approach footing included with Concrete Structures.
 Bars indicated thus 20 x 2-#5 etc. indicates 20 lines of bars with 2 lengths per line.
 See sheet 13 of 18 for Traffic Island Details and layout of dowel bars in approach slab for future installation of traffic islands.
 Traffic Islands will not be installed until a later stage. Openings in slab may be covered with a 1/4" thick secured steel plate (Min Fy = 50ksi) to accommodate M.O.T. plan. All costs associated with the work and materials required for covering openings shall be included in the unit price bid for Concrete Superstructure.
 Detectable Warnings shall be an integral part of the approach slab. A recess shall be provided during construction of the approach slab.
 For Location of Detectable Warnings, see sheet 13 of 18.
 The Detectable Warnings shall not be installed until after traffic island construction is complete.
 If recess is to remain open during winter months, it shall be filled with hot-mix asphalt surfacing.
 All costs associated with the work and materials required for installation of Detectable Warnings shall be included in the unit price bid for Detectable Warnings.

STAGE I PLAN

FILE NAME = DLZ DLZ Illinois, Inc.	USER NAME =	DESIGNED - DPW	REVISIONS	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ILLINOIS FED. AID PROJECT
	PLOT SCALE =	CHECKED - ASP	REVISIONS		
PLOT DATE =	DRAWN - WSP	REVISIONS	REVISIONS	ILLINOIS FED. AID PROJECT	ILLINOIS FED. AID PROJECT

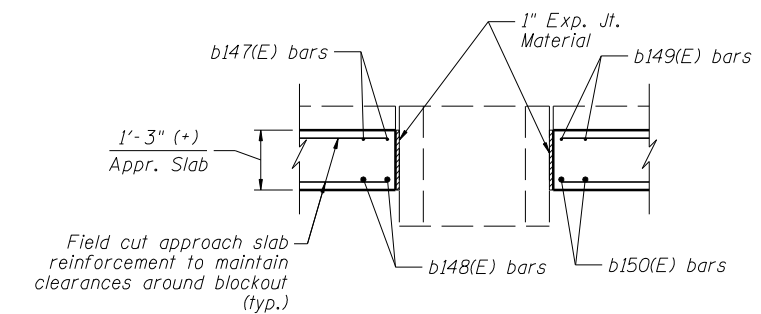
IL 2 (RT.) BIKE PATH AT 15TH AVENUE
STAGE I APPROACH SLAB
 SHEET NO. 9 OF 18 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32.33) R-1	WINNEBAGO	705	539
CONTRACT NO. 64821				

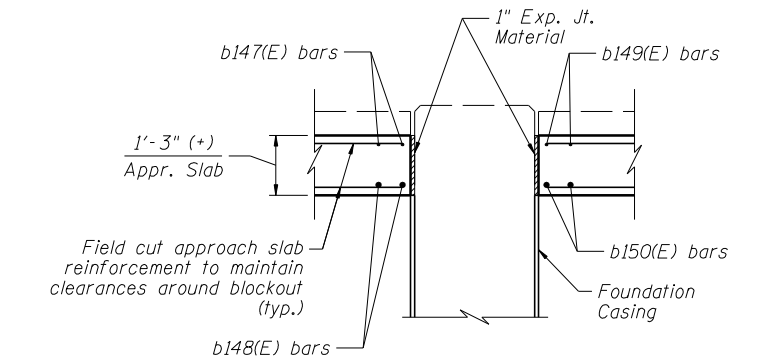


DETAIL 1A

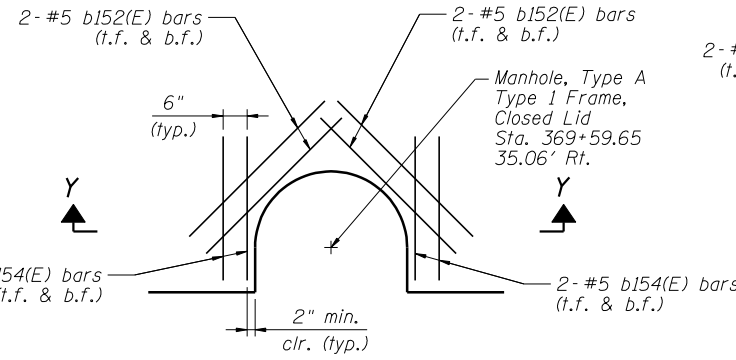
Approach slab reinforcement not shown for clarity.



SECTION W-W

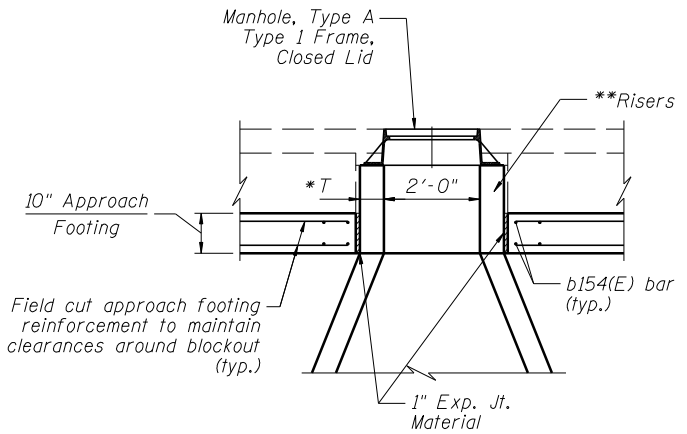


SECTION X-X

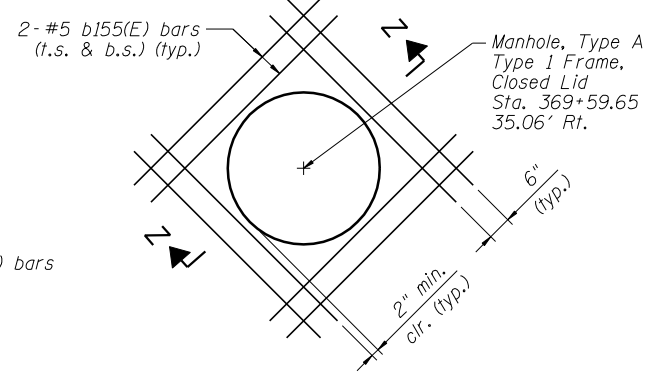


DETAIL 2A

Approach footing reinforcement not shown for clarity.

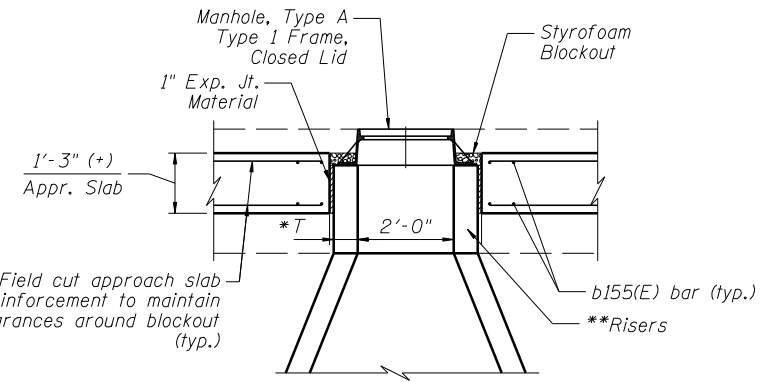


SECTION Y-Y

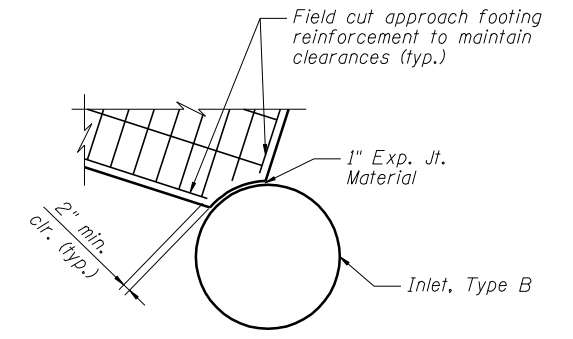


DETAIL 2B

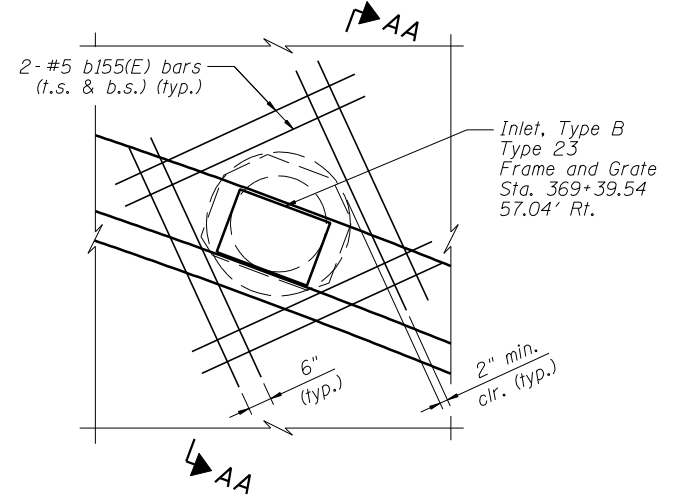
Approach slab reinforcement not shown for clarity.



SECTION Z-Z

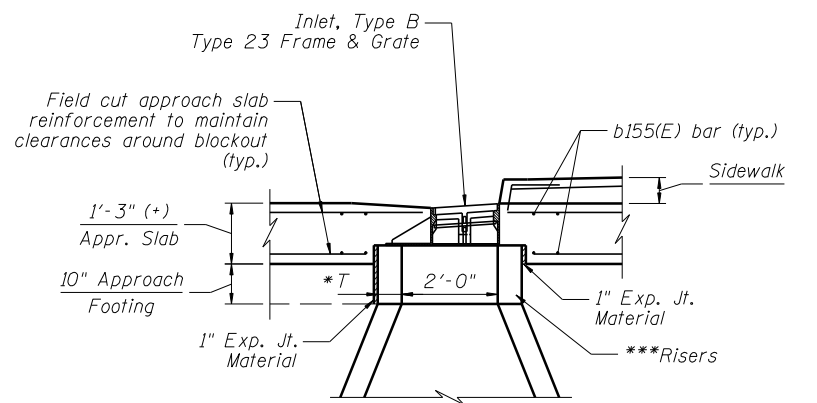


DETAIL 3A



DETAIL 3B

Approach slab reinforcement not shown for clarity.



SECTION AA-AA

LEGEND

b.f. denotes bottom of footing
t.f. denotes top of footing
b.s. denotes bottom of slab
t.s. denotes top of slab

* See Std. Dwg. 602401-03 for wall thickness.
** Include cost of risers in unit price bid for Manhole, Type A.
*** Include cost of risers in unit price bid for Inlet, Type B.



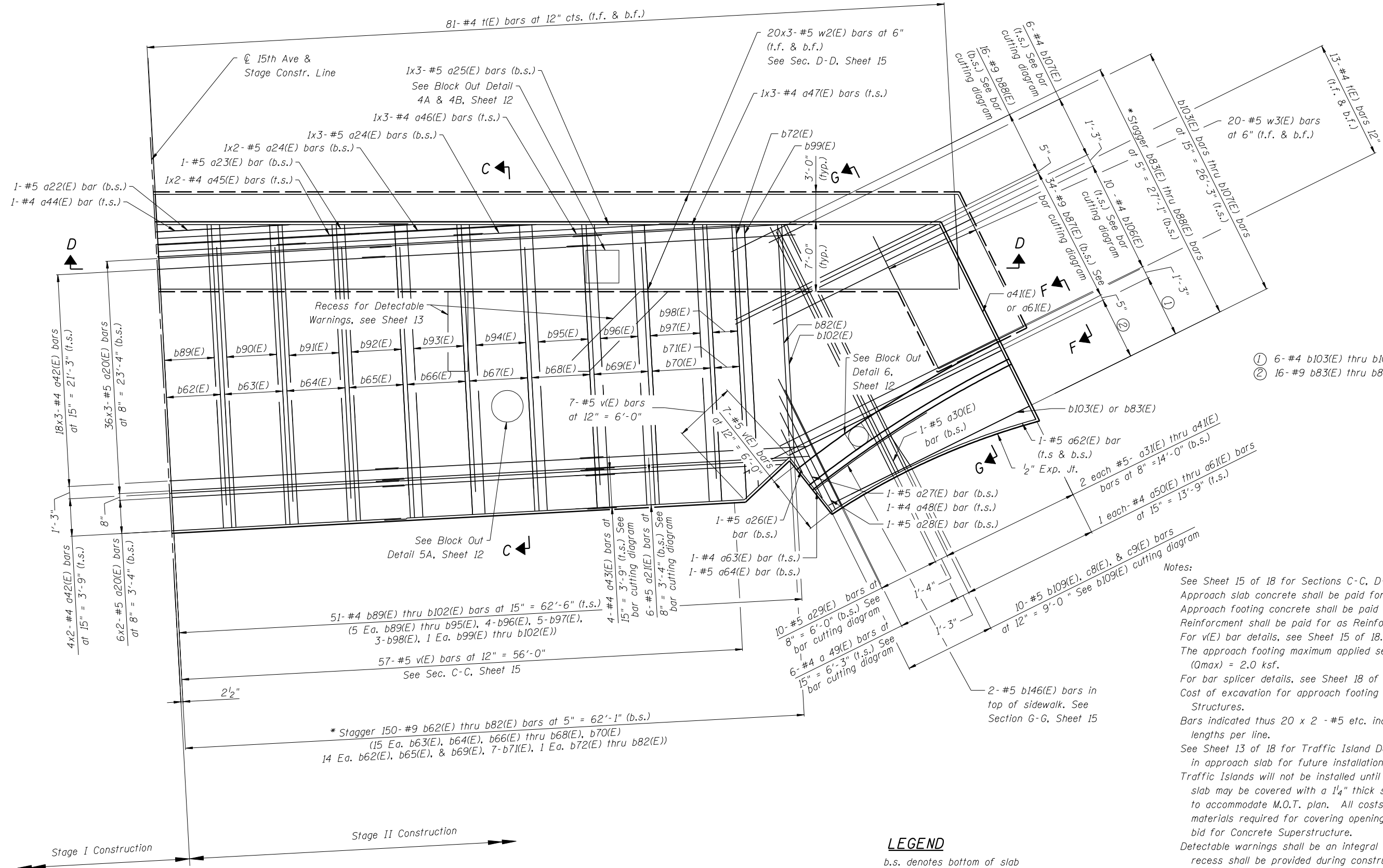
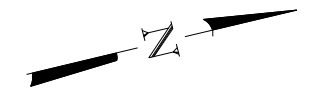
FILE NAME =	USER NAME =	DESIGNED - ASP	REVISED
		CHECKED - LNB	REVISED
		DRAWN - LNB	REVISED
		CHECKED - ASP	REVISED

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 2 (RT.) BIKE PATH AT 15TH AVENUE
STAGE I BLOCKOUT DETAILS**

SHEET NO. 10 OF 18 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32.33) R-1	WINNEBAGO	705	540
			CONTRACT NO. 64821	
ILLINOIS FED. AID PROJECT				



Notes:

See Sheet 15 of 18 for Sections C-C, D-D, G-G, and F-F. Approach slab concrete shall be paid for as Concrete Superstructure. Approach footing concrete shall be paid for as Concrete Structures. Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated. For v(E) bar details, see Sheet 15 of 18. The approach footing maximum applied service bearing pressure (Qmax) = 2.0 ksf. For bar splicer details, see Sheet 18 of 18. Cost of excavation for approach footing included with Concrete Structures. Bars indicated thus 20 x 2 - #5 etc. indicates 20 lines of bars with 2 lengths per line. See Sheet 13 of 18 for Traffic Island Details and layout of dowel bars in approach slab for future installation of traffic islands. Traffic Islands will not be installed until a later stage. Openings in slab may be covered with a 1/4" thick secured steel plate (Min. Fy = 50ksi) to accommodate M.O.T. plan. All costs associated with the work and materials required for covering openings shall be included in the unit price bid for Concrete Superstructure. Detectable warnings shall be an integral part of the approach slab. A recess shall be provided during construction of the approach slab. For location of Detectable Warnings, see Sheet 13 of 18. The Detectable Warnings shall not be installed until after traffic island construction is complete. If recess is to remain open during winter months, it shall be filled with hot-mix asphalt surfacing. All costs associated with work and materials required for installation of Detectable Warnings shall be included in the unit price bid for Detectable Warnings.

LEGEND

b.s. denotes bottom of slab
 t.s. denotes top of slab
 b.f. denotes bottom of footing
 t.f. denotes top of footing
 Ea. denotes each

STAGE II PLAN

Minimum lap length for #4 bars is 2'-4".
 Minimum lap length for #5 bars is 2'-11".
 * Tilt #9 b62(E) thru b88(E) bars as required to maintain clearance.



FILE NAME =	USER NAME =	DESIGNED - DPW	REVISED
		CHECKED - ASP	REVISED
		DRAWN - WSP	REVISED
		CHECKED - ASP	REVISED

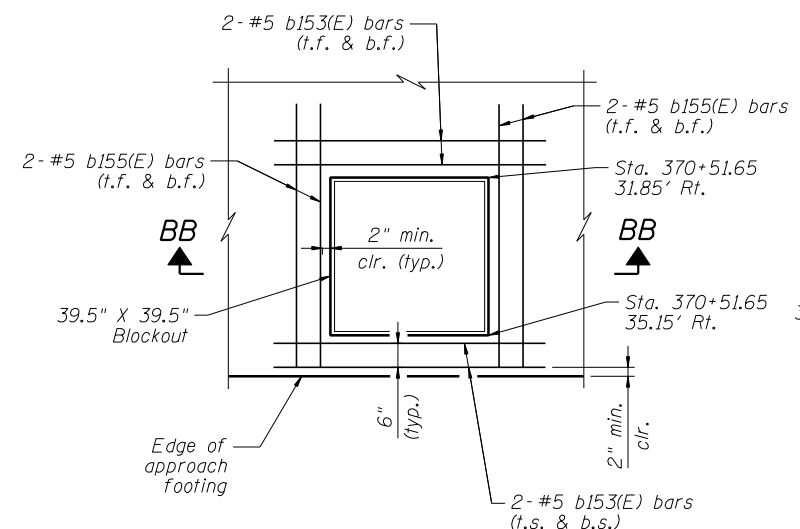
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**IL 2 (RT.) BIKE PATH AT 15TH AVENUE
 STAGE II APPROACH SLAB**

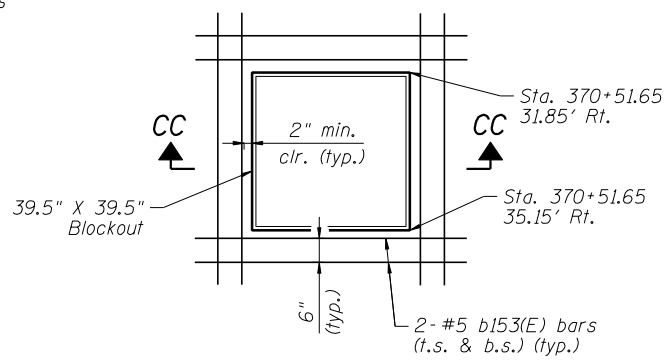
SHEET NO. 11 OF 18 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32.33) R-1	WINNEBAGO	705	541
CONTRACT NO. 64821				

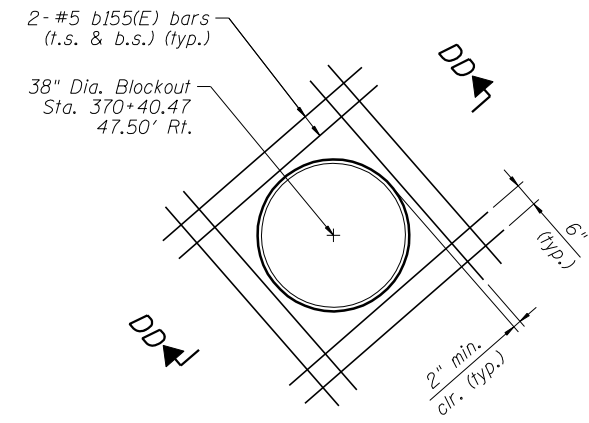
ILLINOIS FED. AID PROJECT



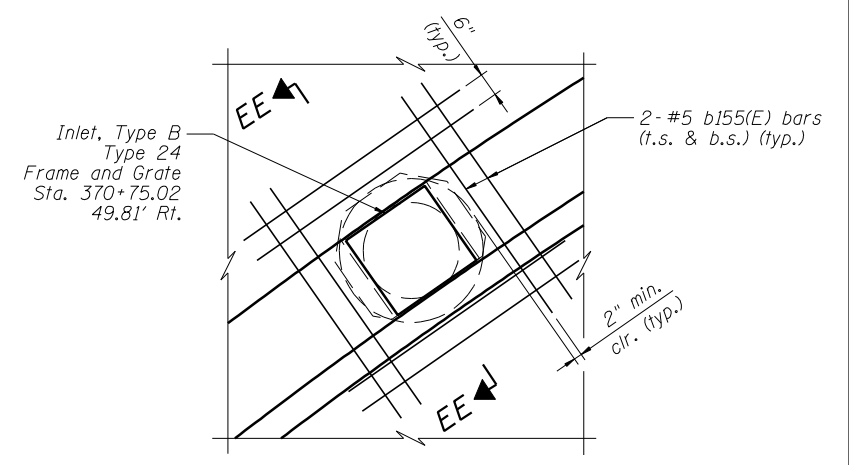
DETAIL 4A
Approach footing reinforcement
not shown for clarity.



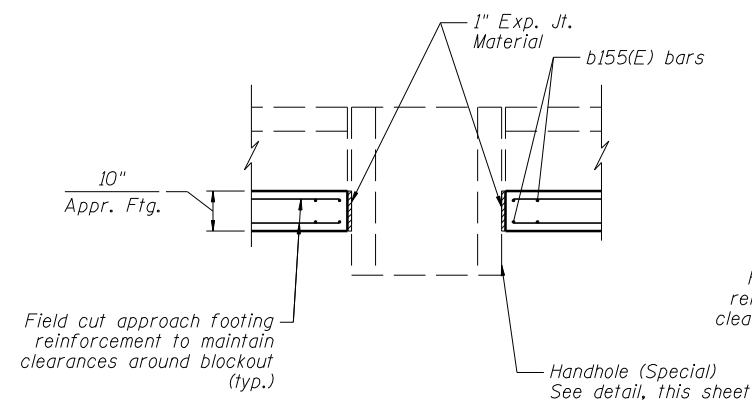
DETAIL 4B
Approach slab reinforcement
not shown for clarity.



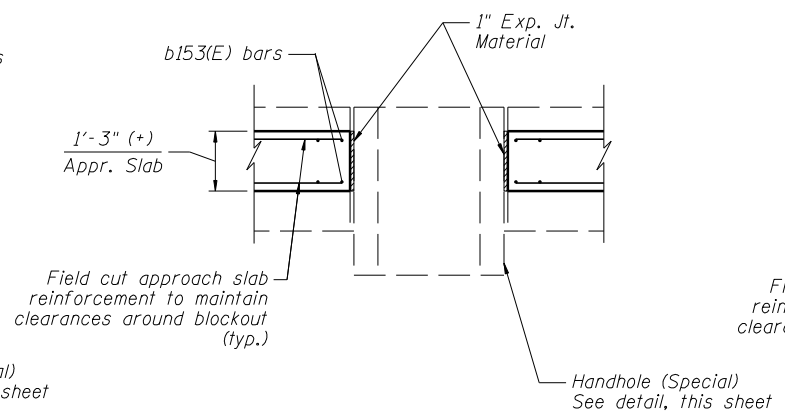
DETAIL 5A
Approach slab reinforcement
not shown for clarity.



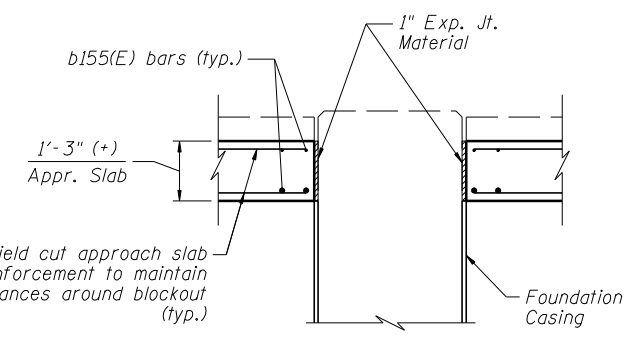
DETAIL 6
Approach slab reinforcement
not shown for clarity.



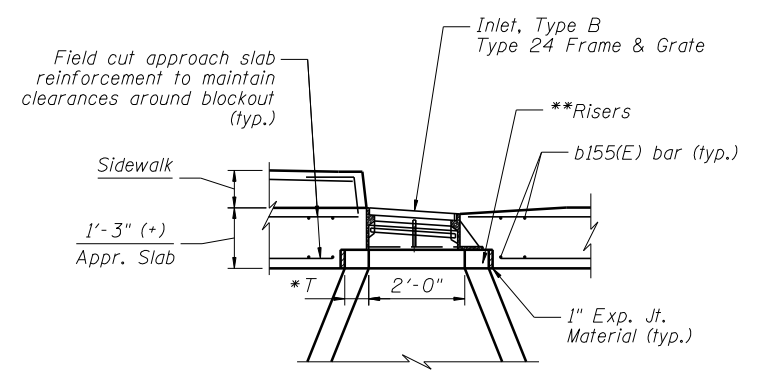
SECTION BB-BB



SECTION CC-CC

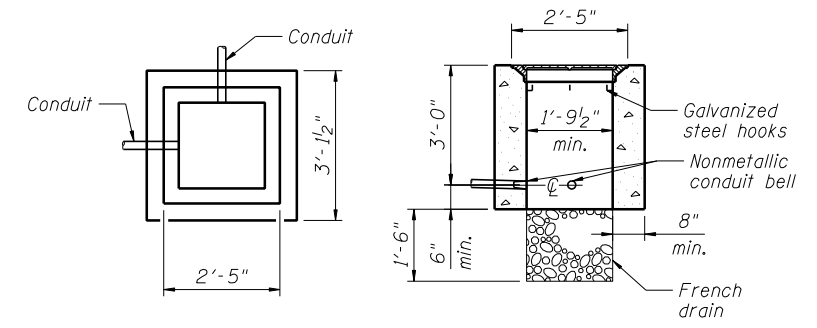


SECTION DD-DD



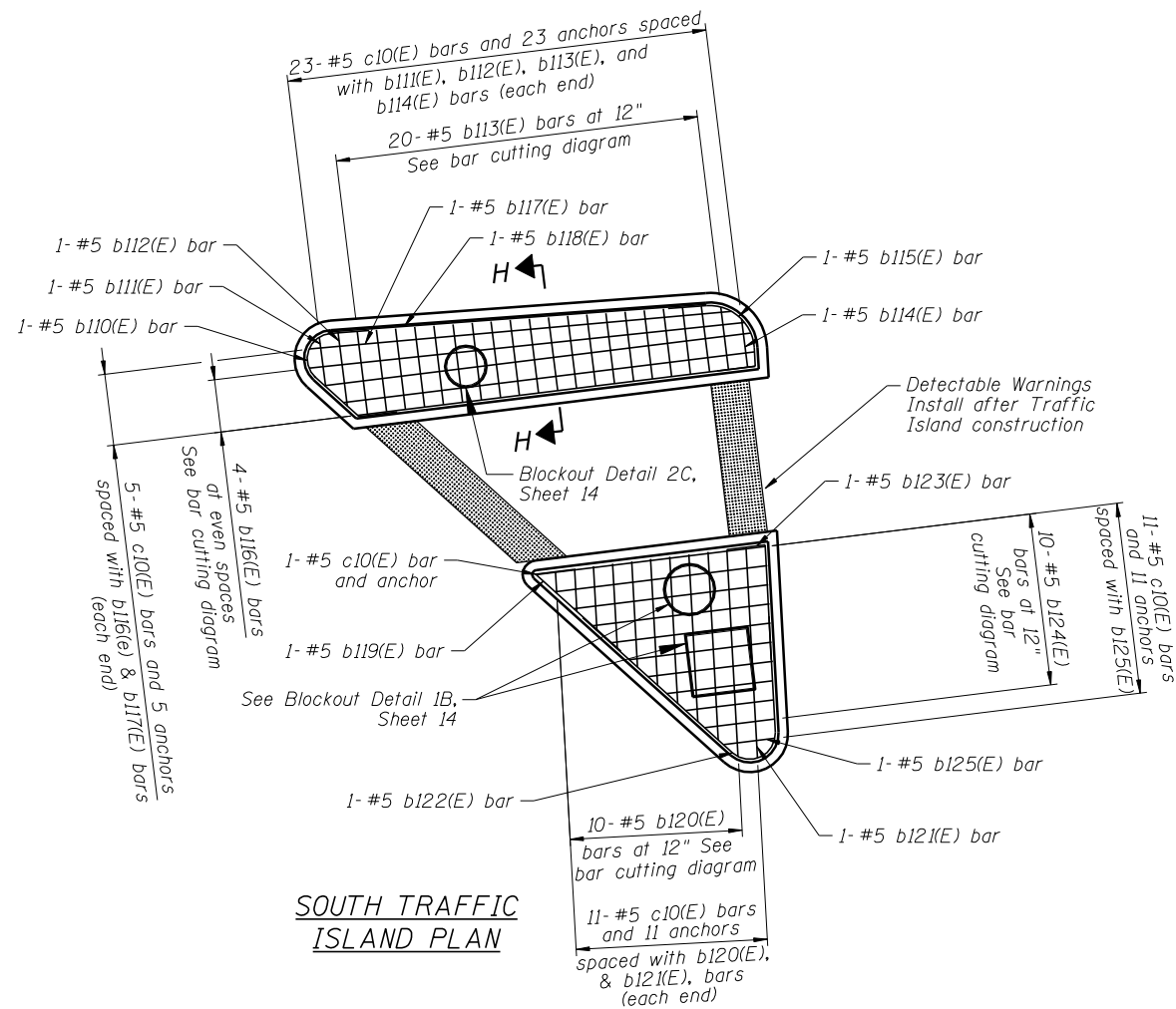
SECTION EE-EE

* See Std. Dwg. 602401-03 for wall thickness.
** Include cost of risers in unit price bid for Inlet, Type B.

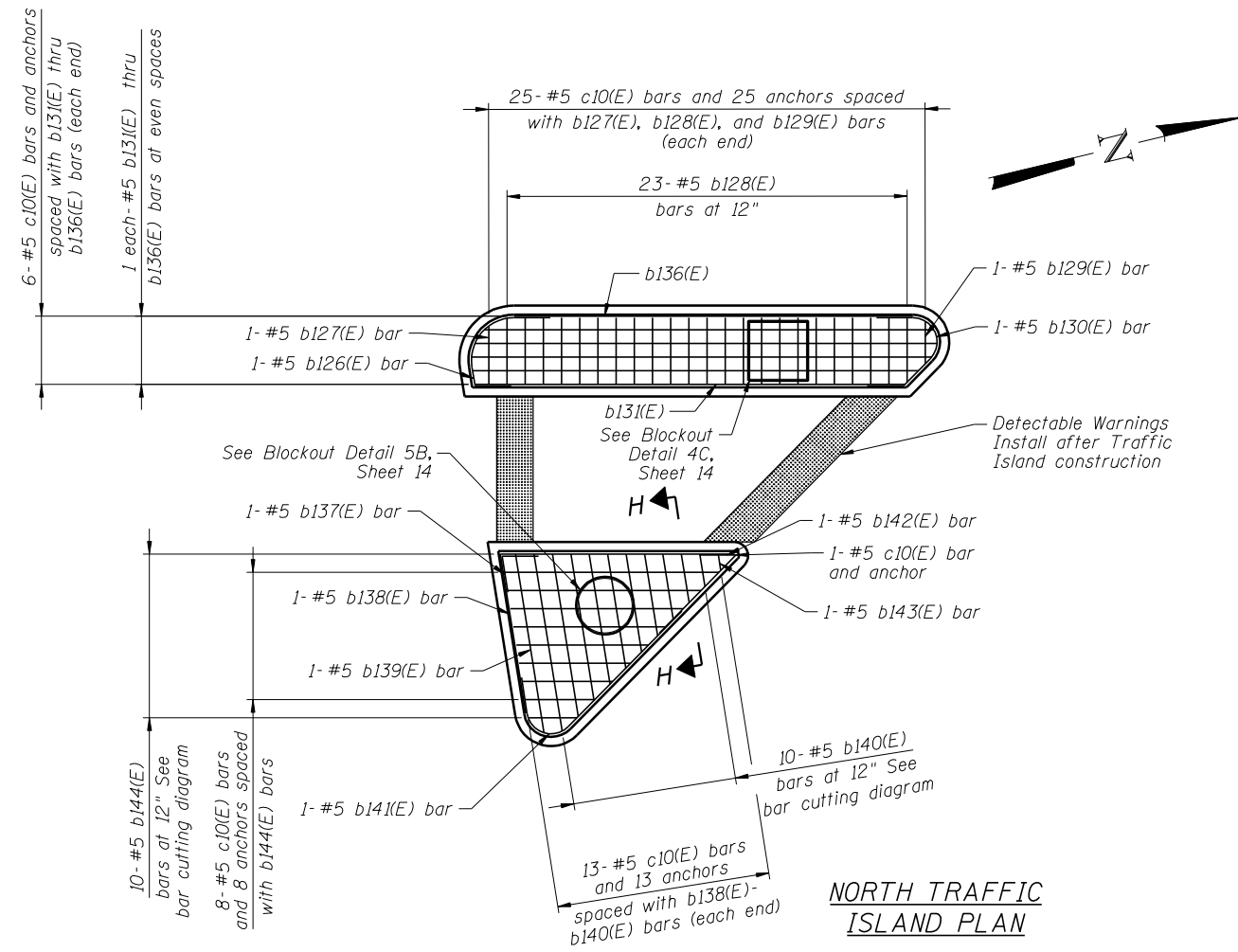


PLAN
ELEVATION
HANDHOLE (SPECIAL)

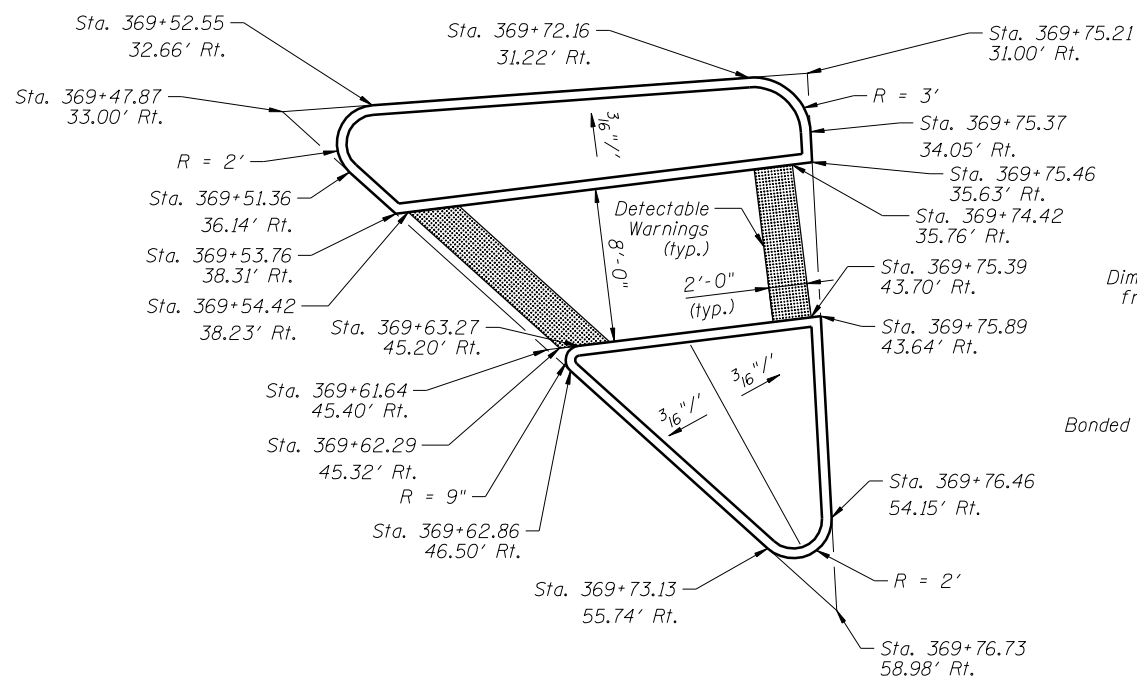
LEGEND
b.f. denotes bottom of footing
t.f. denotes top of footing
b.s. denotes bottom of slab
t.s. denotes top of slab



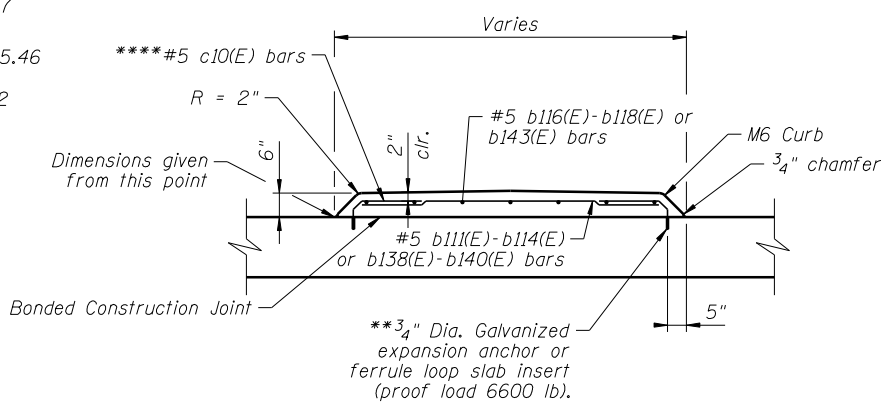
SOUTH TRAFFIC ISLAND PLAN



NORTH TRAFFIC ISLAND PLAN

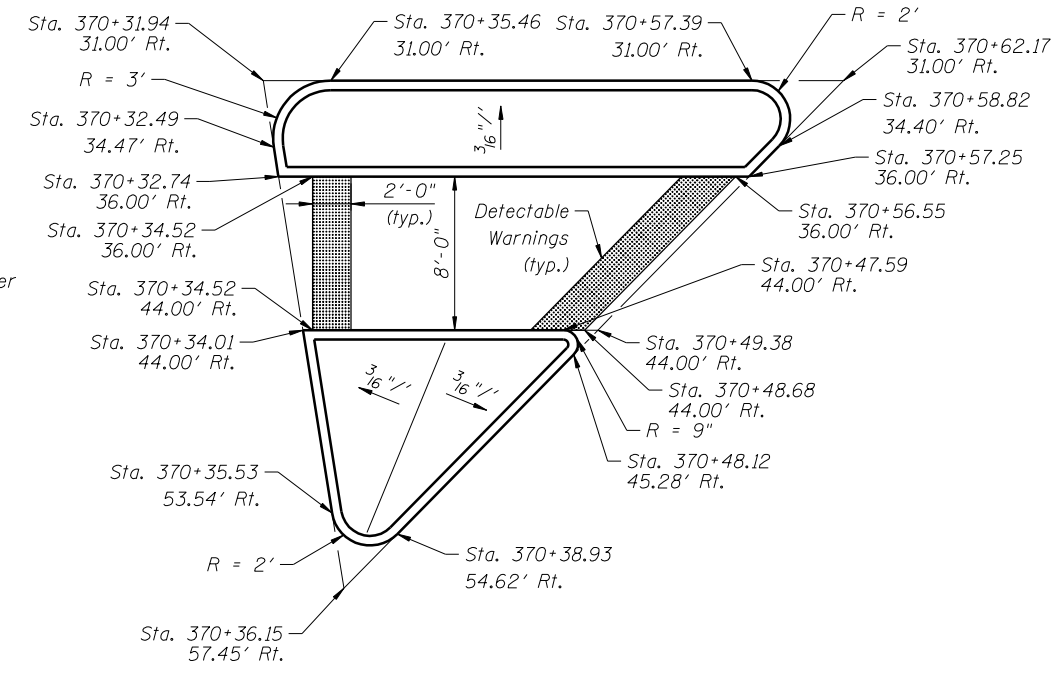


SOUTH TRAFFIC ISLAND LAYOUT



SECTION H-H

** Install expansion anchors/inserts during Stage Construction and set top of insert flush with approach slab. The cost of expansion anchors/inserts is included in the cost of Reinforcement Bars, Epoxy Coated.
 **** Rotate #5 c10(E) bars as required to maintain clearance.



NORTH TRAFFIC ISLAND LAYOUT



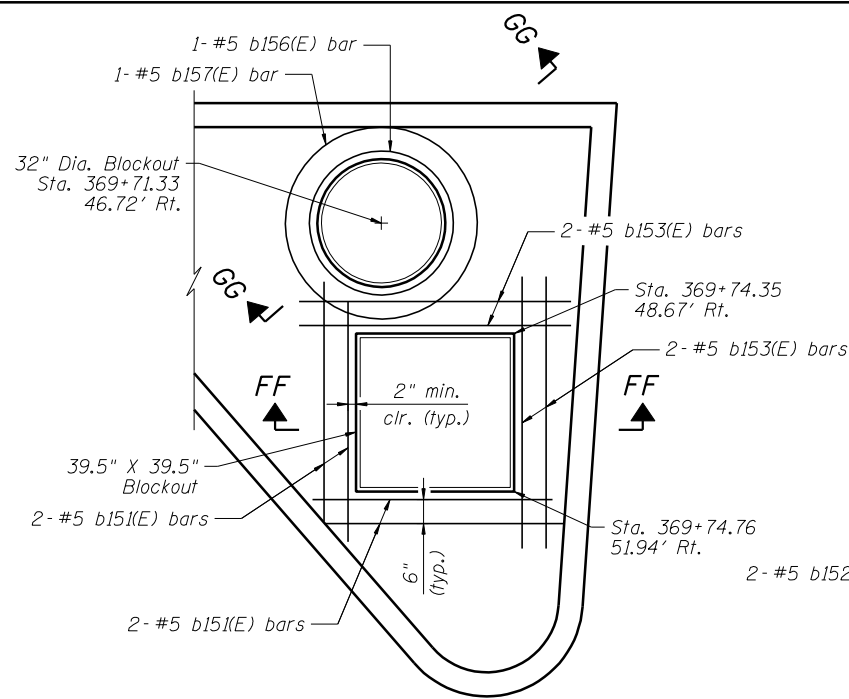
FILE NAME =	USER NAME =	DESIGNED - DPW	REVISED
		CHECKED - ASP	REVISED
		DRAWN - WSP	REVISED
		CHECKED - ASP	REVISED

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 2 (RT.) BIKE PATH AT 15TH AVENUE
TRAFFIC ISLANDS**

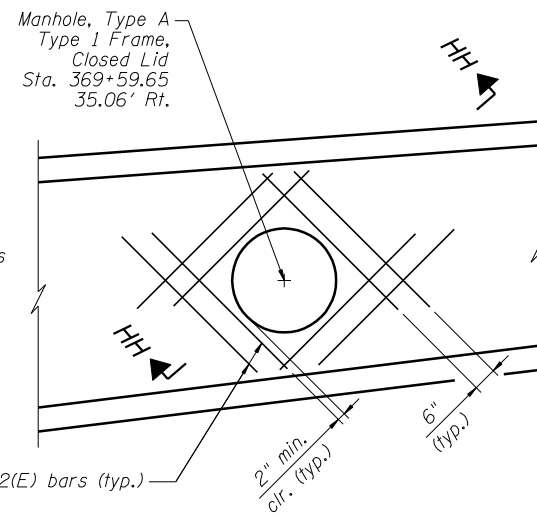
SHEET NO. 13 OF 18 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32.33) R-1	WINNEBAGO	705	543
ILLINOIS FED. AID PROJECT			CONTRACT NO. 64821	



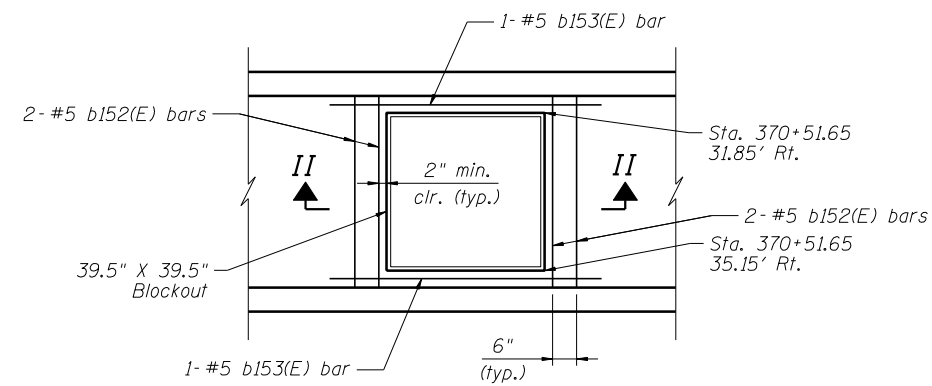
DETAIL 1B

Traffic island reinforcement not shown for clarity.



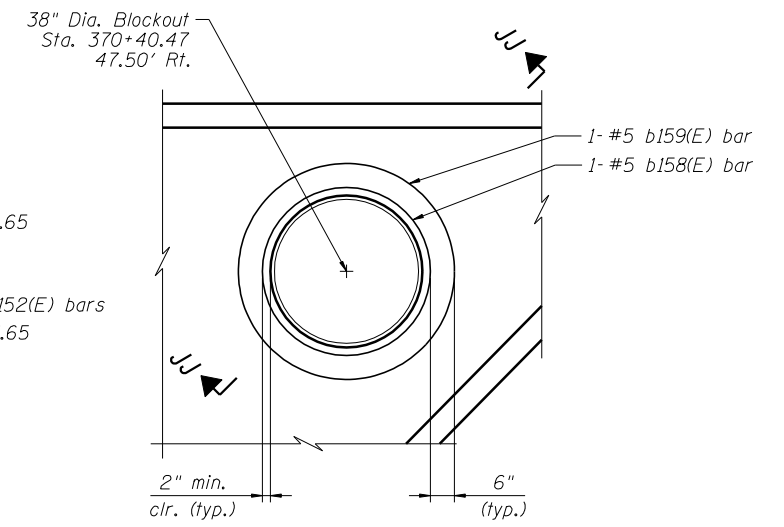
DETAIL 2C

Traffic island reinforcement not shown for clarity.



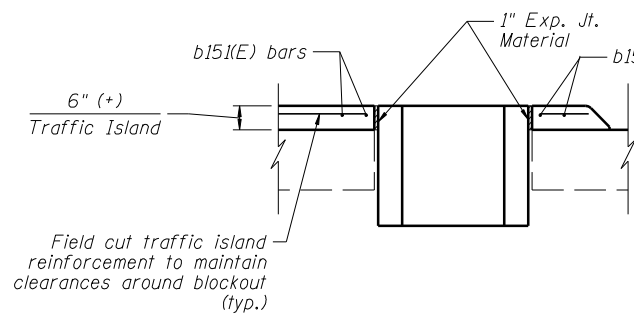
DETAIL 4C

Traffic island reinforcement not shown for clarity.

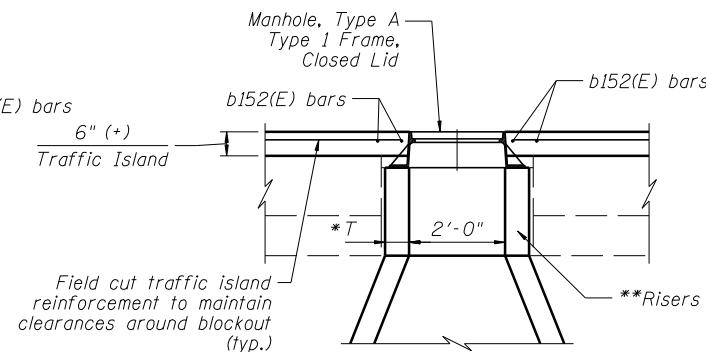


DETAIL 5B

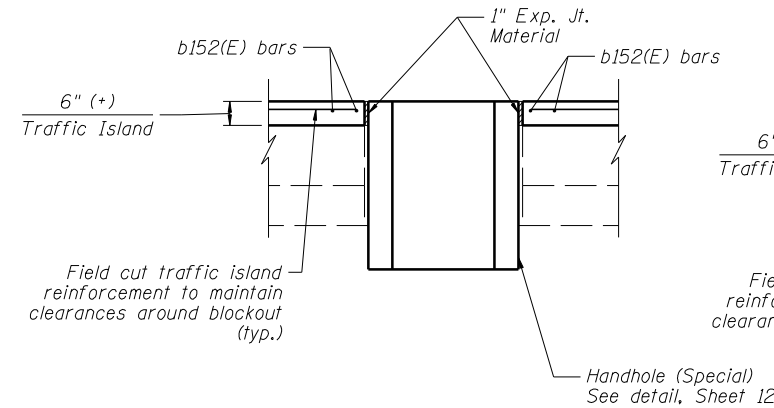
Traffic island reinforcement not shown for clarity.



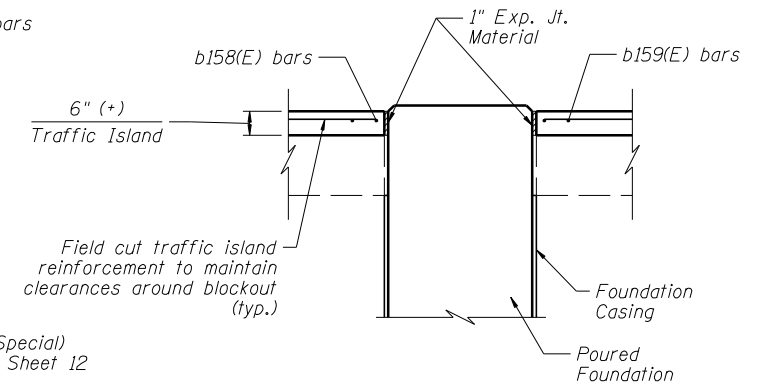
SECTION FF-FF



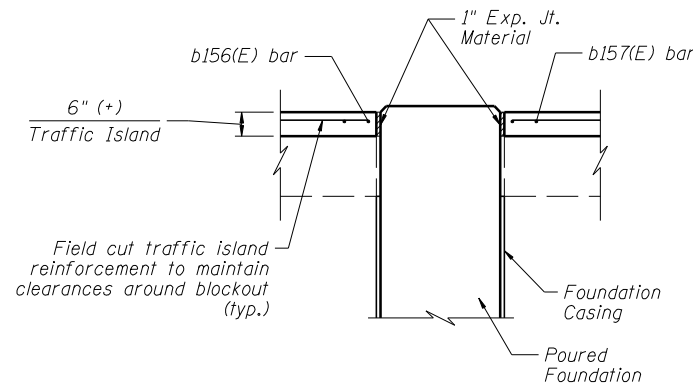
SECTION HH-HH



SECTION II-II



SECTION JJ-JJ

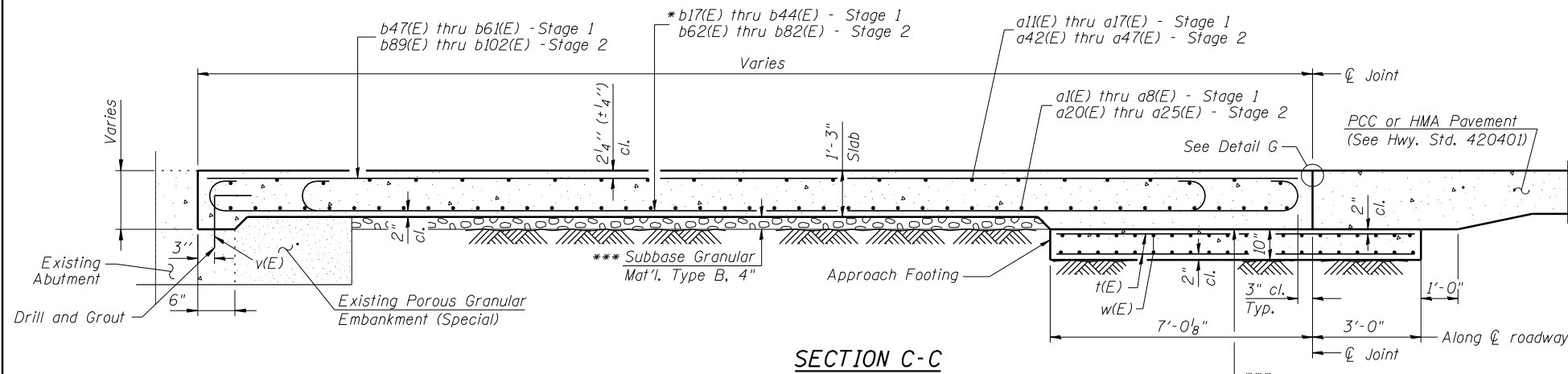


SECTION GG-GG

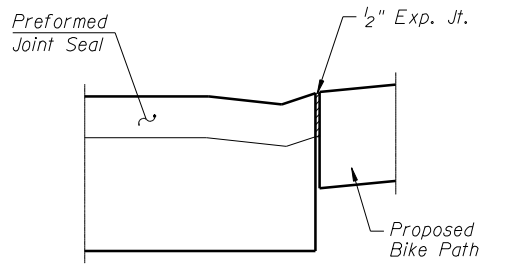
LEGEND

b.f. denotes bottom of footing
 t.f. denotes top of footing
 b.s. denotes bottom of slab
 t.s. denotes top of slab

* See Std. Dwg. 602401-03 for wall thickness.
 ** Include cost of risers in unit price bid for Manhole, Type A.



SECTION C-C



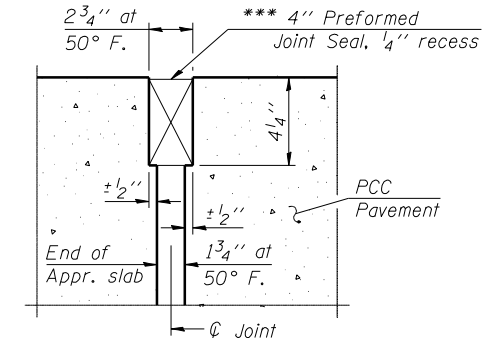
SECTION F-F
Angle Preformed Joint Seal along curbs.

For #5 v(E) bars, drill 3/4" Dia. hole and epoxy grout a minimum of 6" into existing pavement ledge to develop a pullout strength of 9.2 kips.

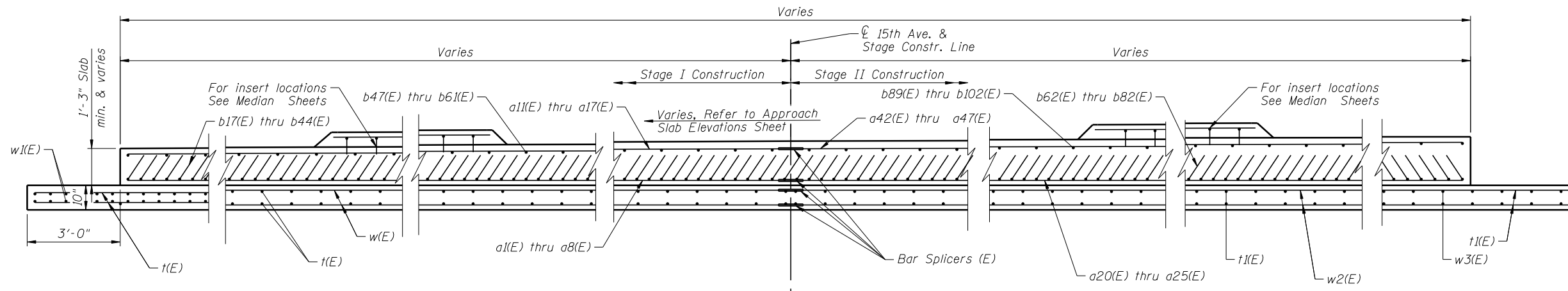
*** 10 mil. Polyethylene bond breaker on steel trowel finish

* Tilt #9 b17(E) thru b44(E) and b62(E) thru b82(E) bars as required to maintain clearance.

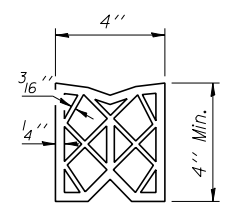
*** Cost included with Concrete Superstructure.



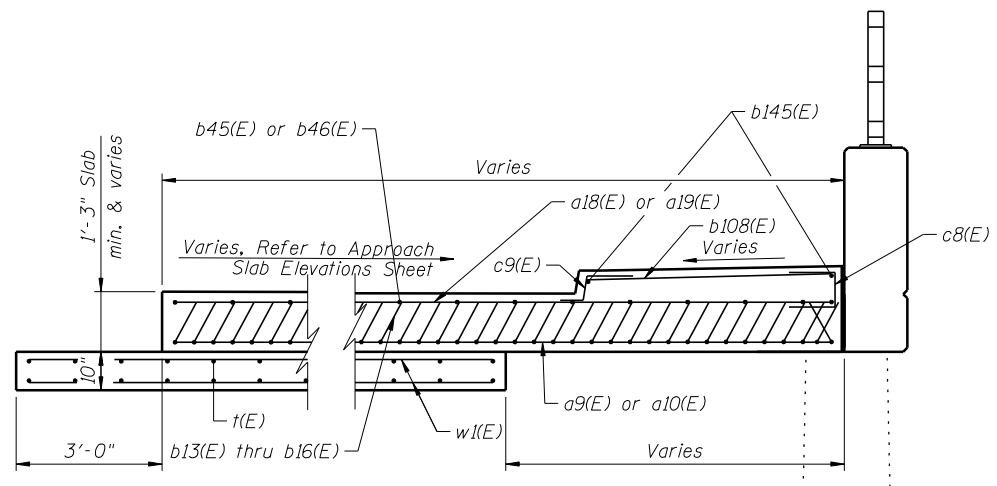
DETAIL G



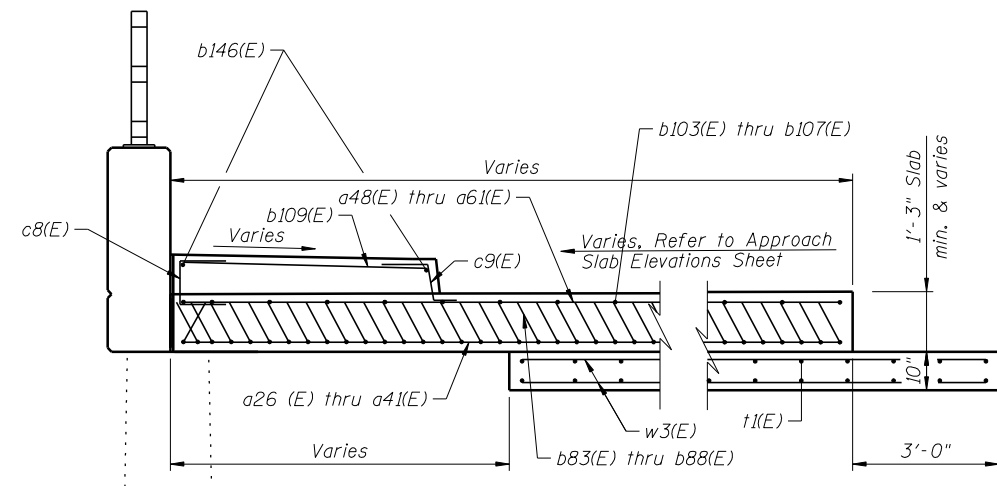
SECTION D-D



PREFORMED JOINT SEAL



SECTION E-E



SECTION G-G



FILE NAME =	USER NAME =	DESIGNED - DPW	REVISED
		CHECKED - ASP	REVISED
		DRAWN - WSP	REVISED
		CHECKED - ASP	REVISED

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 2 (RT.) BIKE PATH AT 15TH AVENUE
APPROACH SLAB DETAILS**

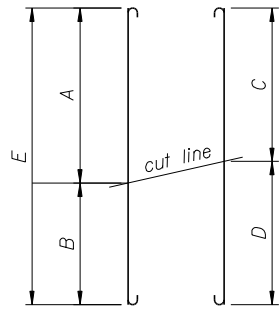
SHEET NO. 15 OF 18 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32.33) R-1	WINNEBAGO	705	545
			CONTRACT NO. 64821	

ILLINOIS FED. AID PROJECT

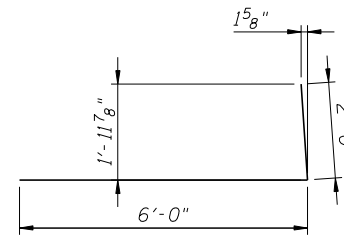
BAR CUTTING TABLE

Bar	Hook	A	B	C	D	E	X	Y
a2(E)	N	30'-11"	12'-8"	18'-3"	14'-11"	16'-0"	3	1
a10(E)	N	46'-0"	11'-6"	34'-6"	22'-1"	23'-11"	7	1
a12(E)	N	31'-5"	16'-9"	14'-8"	18'-10"	12'-7"	4	1
a19(E)	N	43'-2"	13'-3"	29'-11"	19'-11"	23'-3"	3	1
a21(E)	N	35'-5"	18'-1"	17'-4"	19'-8"	15'-9"	3	1
a29(E)	N	42'-0"	21'-9"	20'-3"	27'-9"	14'-3"	5	1
a43(E)	N	36'-6"	19'-0"	17'-6"	20'-6"	16'-0"	2	1
a49(E)	N	40'-0"	12'-11"	27'-1"	18'-7"	21'-5"	3	1
b16(E)	Y	29'-6"	9'-9"	19'-9"	14'-8"	14'-10"	32	1
b46(E)	N	21'-5"	10'-11"	10'-6"	15'-8"	5'-9"	11	1
b87(E)	Y	51'-3"	22'-7"	28'-8"	25'-6"	25'-9"	17	1
b88(E)	Y	27'-10"	6'-3"	21'-7"	13'-5"	14'-5"	8	1
b106(E)	N	43'-5"	24'-2"	19'-3"	22'-0"	21'-5"	5	1
b107(E)	N	23'-3"	4'-4"	18'-11"	10'-1"	13'-2"	3	1
b108(E)	N	8'-4"	3'-7"	4'-9"	4'-0"	4'-4"	6	1
b109(E)	N	6'-5"	2'-9"	3'-8"	3'-2"	3'-3"	5	1
b113(E)	N	7'-7"	4'-4"	3'-3"	3'-10"	3'-9"	10	1
b116(E)	N	43'-9"	20'-11"	22'-10"	21'-5"	22'-4"	2	1
b120(E)	N	11'-10"	10'-9"	1'-1"	6'-5"	5'-5"	5	1
b124(E)	N	15'-8"	12'-0"	3'-8"	8'-4"	7'-4"	5	1
b140(E)	N	11'-7"	9'-10"	1'-9"	6'-3"	5'-4"	5	1
b144(E)	N	15'-6"	12'-10"	2'-8"	8'-4"	7'-2"	5	1

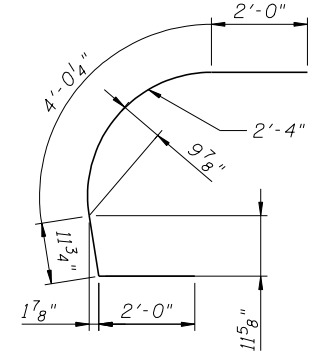


CUTTING DIAGRAM

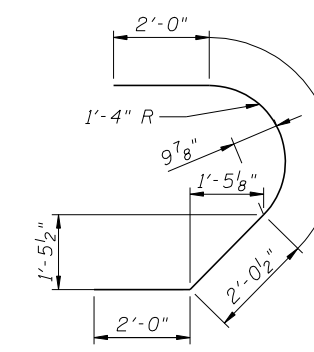
X bars - Y set(s)
(Use Remainder of
Bars at Opposite End)
(See Bar Cutting Table)



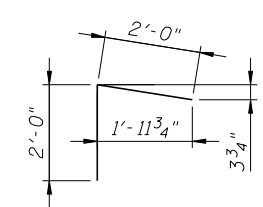
BARS b123(E)



BARS b126(E)

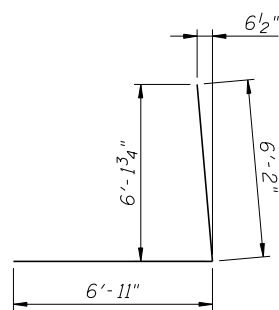


BARS b130(E)

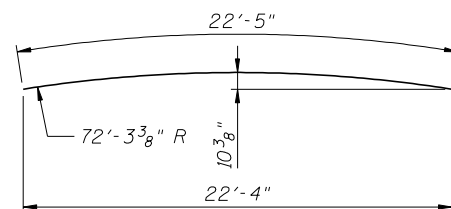


BARS b137(E)

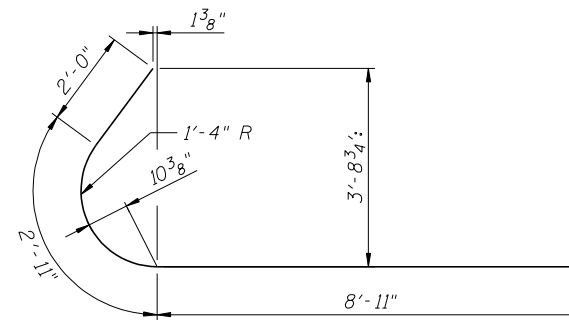
BARS a8(E) & a17(E)



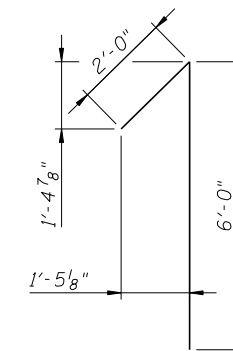
BARS a63(E) & a64(E)



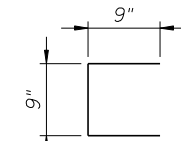
BARS a62(E)



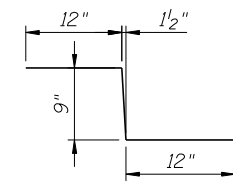
BARS b141(E)



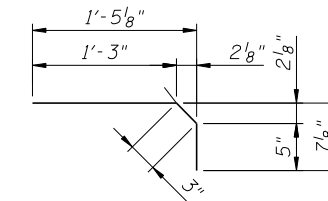
BARS b142(E)



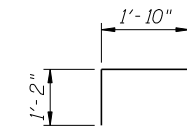
BARS c8(E)



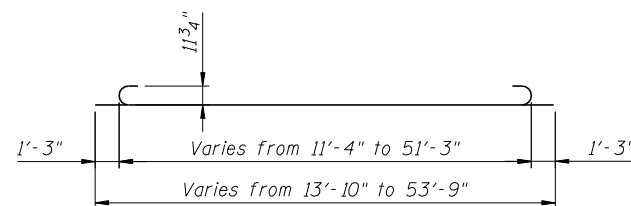
BARS c9(E)



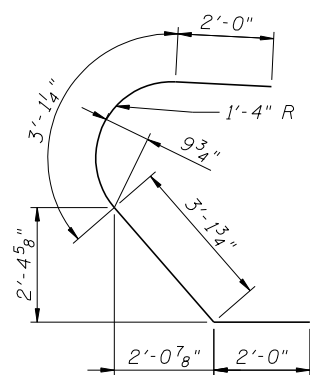
BARS c10(E)



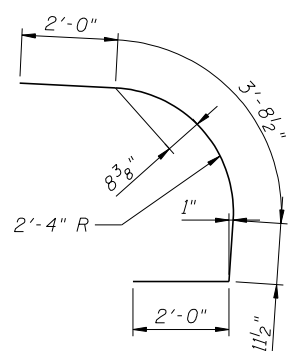
v(E) bar



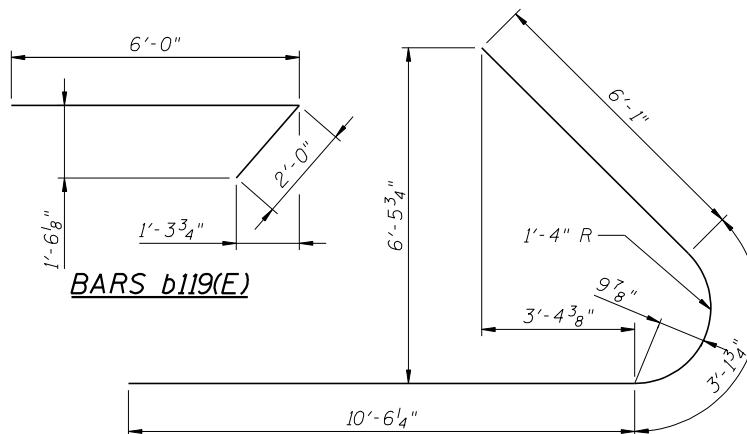
BARS b13(E) thru b44(E) and b62(E) thru b88(E)



BARS b110(E)

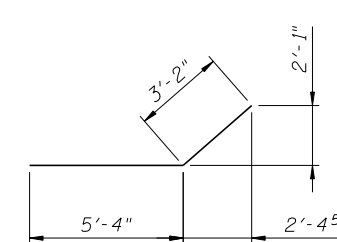


BARS b115(E)

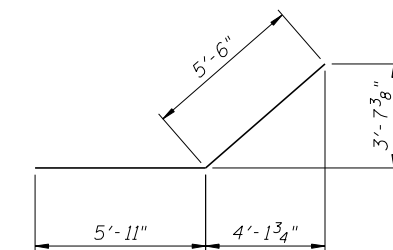


BARS b119(E)

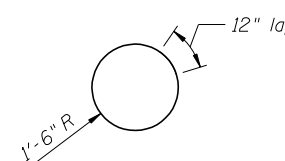
BARS b122(E)



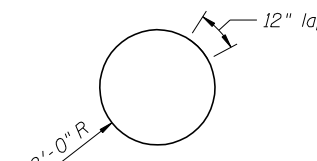
BARS b147(E) & b148(E)



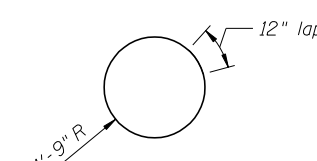
BARS b149(E) & b150(E)



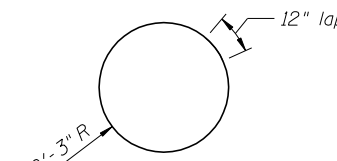
BARS b156(E)



BARS b157(E)



BARS b158(E)



BARS b159(E)

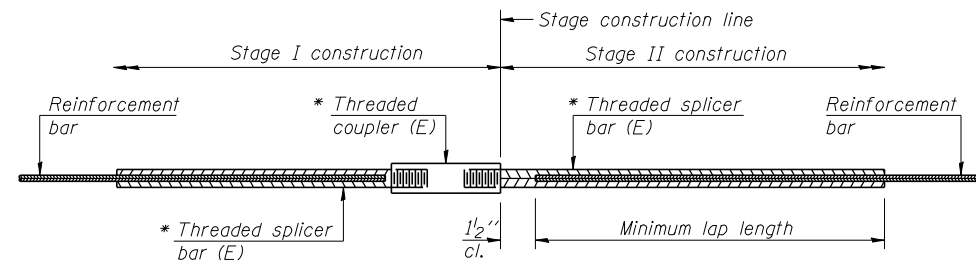
APPROACH SLAB BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a1(E)	NOT USED			
a1(E)	134	5	22'-7"	—
a2(E)	3	5	30'-11"	—
a3(E)	3	5	19'-8"	—
a4(E)	3	5	23'-4"	—
a5(E)	2	5	25'-10"	—
a6(E)	1	5	24'-2"	—
a7(E)	1	5	11'-10"	—
a8(E)	1	5	16'-4"	└
a9(E)	7	5	33'-9"	—
a10(E)	7	5	46'-0"	—
a11(E)	71	4	22'-1"	—
a12(E)	2	4	31'-5"	—
a13(E)	3	4	23'-0"	—
a14(E)	2	4	27'-4"	—
a15(E)	1	4	28'-8"	—
a16(E)	1	4	6'-1"	—
a17(E)	1	4	16'-4"	└
a18(E)	5	4	33'-9"	—
a19(E)	3	4	43'-2"	—
a20(E)	120	5	23'-4"	—
a21(E)	3	5	35'-5"	—
a22(E)	1	5	13'-4"	—
a23(E)	1	5	25'-7"	—
a24(E)	5	5	20'-8"	—
a25(E)	3	5	28'-1"	—
a26(E)	1	5	4'-10"	—
a27(E)	1	5	9'-6"	—
a28(E)	1	5	12'-8"	—
a29(E)	5	5	42'-0"	—
a30(E)	1	5	27'-5"	—
a31(E)	2	5	27'-1"	—
a32(E)	2	5	26'-4"	—
a33(E)	2	5	25'-10"	—
a34(E)	2	5	25'-2"	—
a35(E)	2	5	24'-8"	—
a36(E)	2	5	24'-1"	—
a37(E)	2	5	23'-7"	—
a38(E)	2	5	23'-1"	—
a39(E)	2	5	22'-8"	—
a40(E)	2	5	22'-2"	—
a41(E)	2	5	21'-9"	—
a42(E)	62	4	22'-11"	—
a43(E)	2	4	36'-6"	—
a44(E)	1	4	13'-4"	—
a45(E)	2	4	19'-5"	—
a46(E)	3	4	21'-5"	—
a47(E)	3	4	27'-9"	—
a48(E)	1	4	9'-3"	—
a49(E)	3	4	40'-0"	—
a50(E)	1	4	27'-3"	—
a51(E)	1	4	26'-8"	—
a52(E)	1	4	26'-1"	—
a53(E)	1	4	25'-6"	—
a54(E)	1	4	25'-0"	—
a55(E)	1	4	24'-5"	—
a56(E)	1	4	23'-11"	—
a57(E)	1	4	23'-6"	—
a58(E)	1	4	23'-0"	—
a59(E)	1	4	22'-7"	—
a60(E)	1	4	22'-5"	—
a61(E)	1	4	21'-9"	—
a62(E)	2	5	22'-5"	—
a63(E)	1	4	13'-1"	└
a64(E)	1	5	13'-1"	└

Bar	No.	Size	Length	Shape
b13(E)	9	9	13'-10"	—
b14(E)	8	9	14'-2"	—
b15(E)	1	9	17'-0"	—
b16(E)	32	9	32'-0"	—
b17(E)	1	9	29'-4"	—
b18(E)	1	9	29'-8"	—
b19(E)	1	9	29'-10"	—
b20(E)	1	9	30'-1"	—
b21(E)	1	9	30'-4"	—
b22(E)	1	9	30'-7"	—
b23(E)	1	9	30'-9"	—
b24(E)	1	9	31'-0"	—
b25(E)	1	9	31'-2"	—
b26(E)	1	9	31'-6"	—
b27(E)	1	9	31'-8"	—
b28(E)	1	9	31'-11"	—
b29(E)	1	9	32'-1"	—
b30(E)	1	9	32'-4"	—
b31(E)	1	9	32'-7"	—
b32(E)	1	9	32'-10"	—
b33(E)	1	9	33'-0"	—
b34(E)	1	9	33'-3"	—
b35(E)	1	9	33'-6"	—
b36(E)	7	9	33'-8"	—
b37(E)	15	9	33'-4"	—
b38(E)	15	9	33'-0"	—
b39(E)	15	9	32'-8"	—
b40(E)	14	9	32'-4"	—
b41(E)	15	9	32'-0"	—
b42(E)	15	9	31'-8"	—
b43(E)	14	9	31'-4"	—
b44(E)	16	9	31'-0"	—
b45(E)	6	4	13'-7"	—
b46(E)	11	4	21'-5"	—
b47(E)	1	4	29'-2"	—
b48(E)	1	4	29'-10"	—
b49(E)	1	4	30'-6"	—
b50(E)	1	4	31'-2"	—
b51(E)	1	4	31'-10"	—
b52(E)	1	4	32'-6"	—
b53(E)	3	4	33'-5"	—
b54(E)	5	4	33'-1"	—
b55(E)	5	4	32'-9"	—
b56(E)	5	4	32'-5"	—
b57(E)	5	4	32'-1"	—
b58(E)	5	4	31'-9"	—
b59(E)	5	4	31'-5"	—
b60(E)	5	4	31'-1"	—
b61(E)	5	4	30'-9"	—
b62(E)	14	9	30'-8"	—
b63(E)	15	9	30'-4"	—
b64(E)	15	9	30'-0"	—
b65(E)	14	9	29'-8"	—
b66(E)	15	9	29'-4"	—
b67(E)	15	9	29'-0"	—
b68(E)	15	9	28'-8"	—
b69(E)	14	9	28'-4"	—
b70(E)	15	9	28'-0"	—
b71(E)	7	9	27'-8"	—
b72(E)	1	9	27'-5"	—
b73(E)	1	9	26'-11"	—
b74(E)	1	9	26'-8"	—
b75(E)	1	9	26'-2"	—
b76(E)	1	9	25'-11"	—
b77(E)	1	9	25'-5"	—

Bar	No.	Size	Length	Shape
b78(E)	1	9	25'-2"	—
b79(E)	1	9	24'-8"	—
b80(E)	1	9	24'-5"	—
b81(E)	1	9	23'-11"	—
b82(E)	1	9	23'-8"	—
b83(E)	4	9	22'-5"	—
b84(E)	4	9	22'-9"	—
b85(E)	4	9	23'-1"	—
b86(E)	4	9	23'-5"	—
b87(E)	17	9	53'-9"	—
b88(E)	8	9	30'-4"	—
b89(E)	5	4	30'-5"	—
b90(E)	5	4	30'-1"	—
b91(E)	5	4	29'-9"	—
b92(E)	5	4	29'-5"	—
b93(E)	5	4	29'-1"	—
b94(E)	5	4	28'-9"	—
b95(E)	5	4	28'-5"	—
b96(E)	4	4	28'-1"	—
b97(E)	5	4	27'-9"	—
b98(E)	3	4	27'-5"	—
b99(E)	1	4	26'-9"	—
b100(E)	1	4	25'-2"	—
b101(E)	1	4	24'-0"	—
b102(E)	1	4	22'-11"	—
b103(E)	2	4	22'-5"	—
b104(E)	2	4	23'-0"	—
b105(E)	2	4	23'-6"	—
b106(E)	5	4	43'-5"	—
b107(E)	3	4	23'-3"	—
b108(E)	6	5	8'-4"	—
b109(E)	5	5	6'-5"	—
b110(E)	1	5	10'-3"	└
b111(E)	1	5	2'-7"	—
b112(E)	1	5	3'-10"	—
b113(E)	10	5	7'-7"	—
b114(E)	1	5	2'-6"	—
b115(E)	1	5	8'-8"	└
b116(E)	2	5	43'-9"	—
b117(E)	1	5	22'-1"	—
b118(E)	1	5	19'-8"	—
b119(E)	1	5	8'-0"	└
b120(E)	5	5	11'-10"	—
b121(E)	1	5	11'-0"	—
b122(E)	1	5	19'-9"	└
b123(E)	1	5	8'-0"	└
b124(E)	5	5	15'-8"	—
b125(E)	1	5	2'-8"	—
b126(E)	1	5	9'-0"	└
b127(E)	1	5	3'-2"	—
b128(E)	23	5	3'-8"	—
b129(E)	1	5	2'-4"	—
b130(E)	1	5	9'-2"	└
b131(E)	1	5	23'-8"	—
b132(E)	1	5	24'-6"	—
b133(E)	1	5	25'-3"	—
b134(E)	1	5	25'-4"	—
b135(E)	1	5	24'-8"	—
b136(E)	1	5	21'-11"	—
b137(E)	1	5	4'-0"	└
b138(E)	1	5	8'-9"	—
b139(E)	1	5	9'-11"	—
b140(E)	5	5	11'-7"	—
b141(E)	1	5	13'-10"	└
b142(E)	1	5	8'-0"	└

Bar	No.	Size	Length	Shape
b143(E)	1	5	0'-10"	—
b144(E)	5	5	15'-6"	—
b145(E)	2	5	11'-4"	—
b146(E)	2	5	9'-4"	—
b147(E)	2	5	8'-6"	—
b148(E)	2	9	8'-6"	—
b149(E)	2	5	11'-5"	—
b150(E)	2	9	11'-5"	—
b151(E)	8	5	5'-0"	—
b152(E)	24	5	4'-0"	—
b153(E)	38	5	5'-8"	—
b154(E)	8	5	3'-0"	—
b155(E)	72	5	5'-6"	—
b156(E)	1	5	10'-6"	○
b157(E)	1	5	13'-7"	○
b158(E)	1	5	12'-0"	○
b159(E)	1	5	15'-2"	○
c8(E)	22	5	2'-3"	└
c9(E)	22	5	2'-9"	└
c10(E)	187	5	1'-11"	└
t(E)	374	4	9'-8"	—
v(E)	141	5	3'-0"	└
w(E)	80	5	34'-10"	—
w1(E)	40	5	28'-6"	—
w2(E)	120	5	28'-10"	—
w3(E)	40	5	14'-9"	—
Detectable Warnings				
Item # 42400800	Sq. Ft.	76		
Concrete Structures				
Item # 50300225	Cu. Yd.	54.8		
Concrete Superstructure				
Item # 50300255	Cu. Yd.	229.5		
Bridge Deck Grooving				
Item # 50300260	Sq. Yd.	499		
Protective Coat				
Item # 50300300	Sq. Yd.	534		
Reinforcement Bars, Epoxy Coated				
Item # 50800205	Pound	67,444		
Bar Splicers				
Item # 50800515	Each	112		
Approach Slab Removal				
Item # Z0004552	Sq. Yd.	456		



STANDARD BAR SPLICER ASSEMBLY

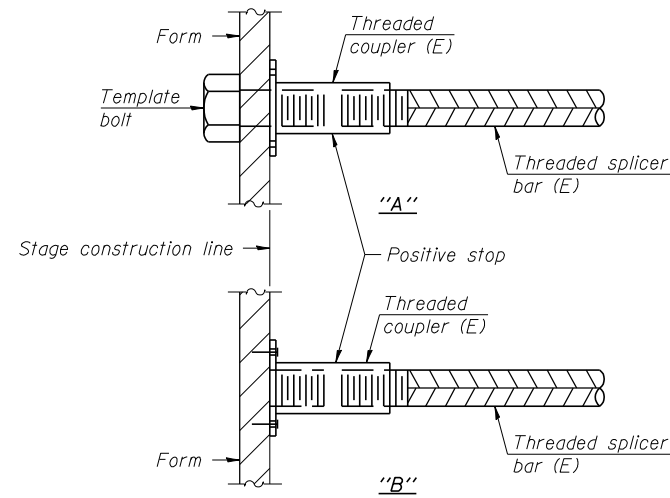
Minimum Lap Lengths					
Bar size to be spliced	Table 1	Table 2	Table 3	Table 4	Table 5
3, 4	1'-5"	1'-11"	2'-1"	2'-4"	2'-3"
5	1'-9"	2'-5"	2'-7"	2'-11"	2'-10"
6	2'-1"	2'-11"	3'-1"	3'-6"	3'-4"
7	2'-9"	3'-10"	4'-2"	4'-8"	4'-6"
8	3'-8"	5'-1"	5'-5"	6'-2"	5'-10"
9	4'-7"	6'-5"	6'-10"	7'-9"	7'-5"

- Table 1: Black bar, 0.8 Class C
- Table 2: Black bar, Top bar lap, 0.8 Class C
- Table 3: Epoxy bar, 0.8 Class C
- Table 4: Epoxy bar, Top bar lap, 0.8 Class C
- Table 5: Epoxy bar, Top bar lap, Class B

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length
Approach Footing	5	40	3
Approach Slab-Top	4	25	4
Approach Slab-Bottom	5	47	3



INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

7-1-10

Notes:

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

See roadway plans for: elevations and transitions for the roadway, curbs, and sidewalks; sidewalk ramp details; locations of manholes, utility poles, and crossing gates.

Design Stresses
 $f'_c = 3,500$ psi (Concrete)
 $f_y = 60,000$ psi (Reinforcement)

See sheet 2 of 3 for Sections C-C & D-D and Views A-A & C-C.

See sheet 3 of 3 for railing details.

Reinforcement bars designated (E) shall be epoxy coated.

$a_1(E)$, $a_2(E)$, $a_3(E)$, and $a_4(E)$ bar spacing measured along ϕ Rdwy.

Bars indicated thus 70x2-#9 etc. indicates 70 lines of bars with 2 lengths per line.

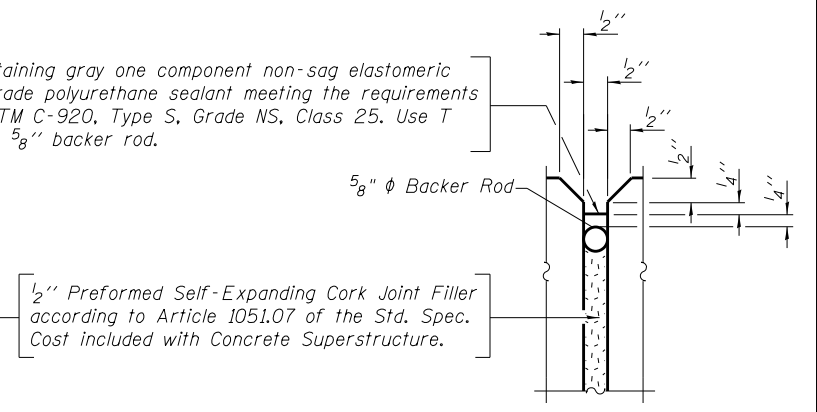
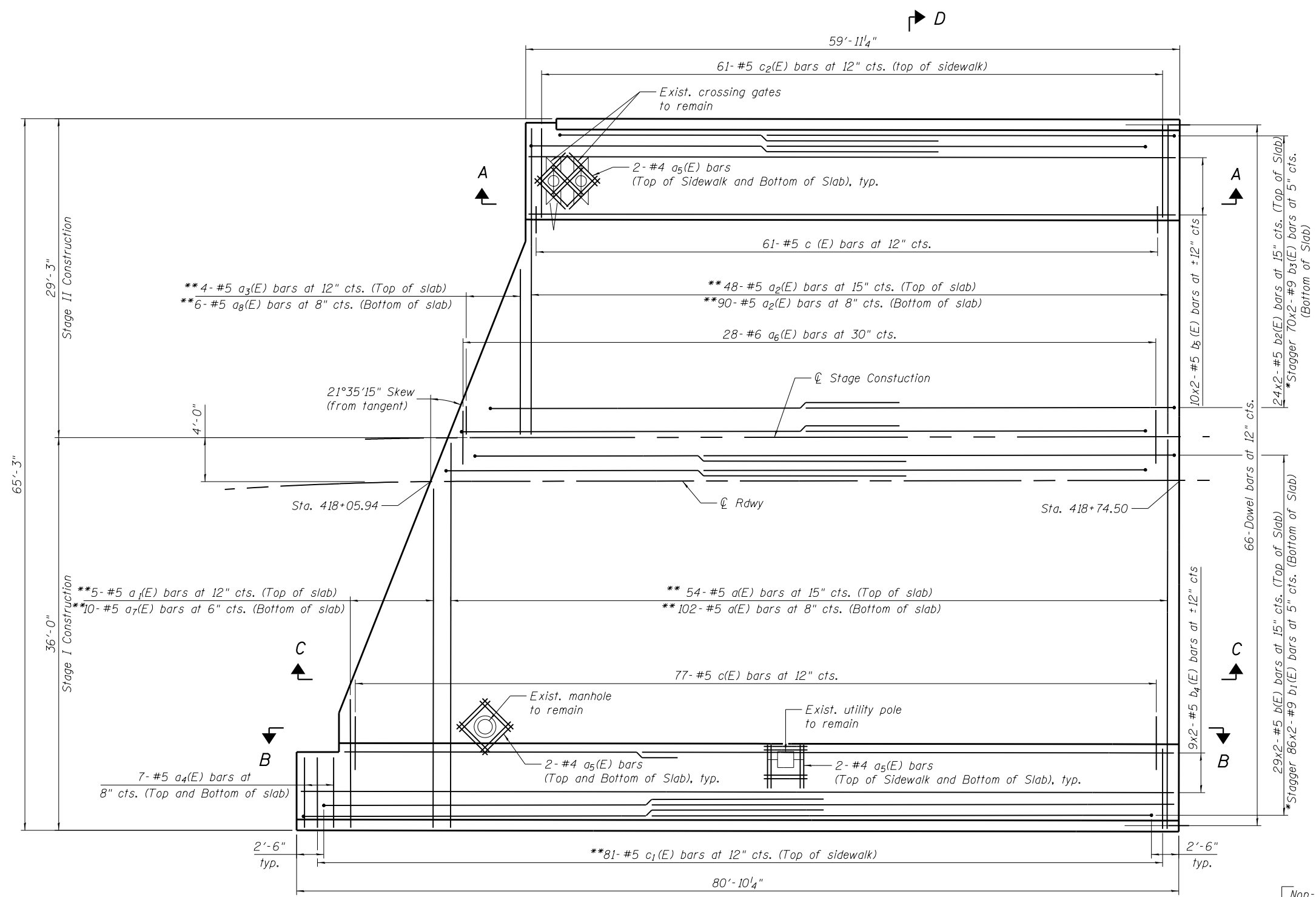
The existing roadway pavement, concrete sidewalk, and concrete parapet with steel railing shall be removed for the proposed cast-in-place concrete slab, sidewalk, and parapets with steel railings to be constructed. Paid for as Removal of Existing Superstructure.

Roadway slab, sidewalk, and parapet concrete shall be paid for as Concrete Superstructure.

Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated.

The dowels, joints, and finishing shall be according to Section 420 of the Standard Specifications. The dowels shall be included in the cost of Concrete Superstructures.

The roadway subgrade and subbase shall follow Standard 420501-04 and be compacted over the structure using hand equipment approved by the Engineer.



PARAPET JOINT DETAILS

MIN. BAR LAP
 #5 Bar = 3'-3"
 #9 Bar = 8'-7"

* Tilt #9 $b_1(E)$ and $b_3(E)$ bars as required to maintain clearance.
 ** Trim to fit

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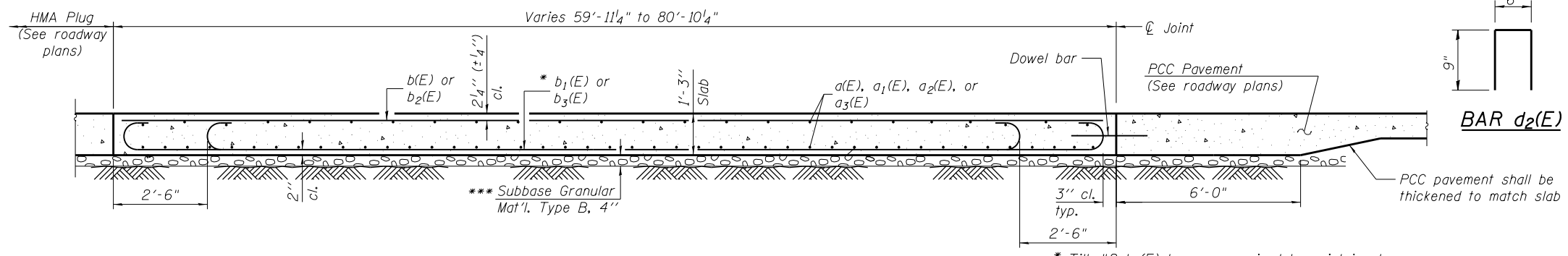
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 DEPARTMENT OF TRANSPORTATION

ROADWAY SLAB DETAILS
 STRUCTURE NO. 101-6110
 SHEET NO. 1 OF 3 SHEETS

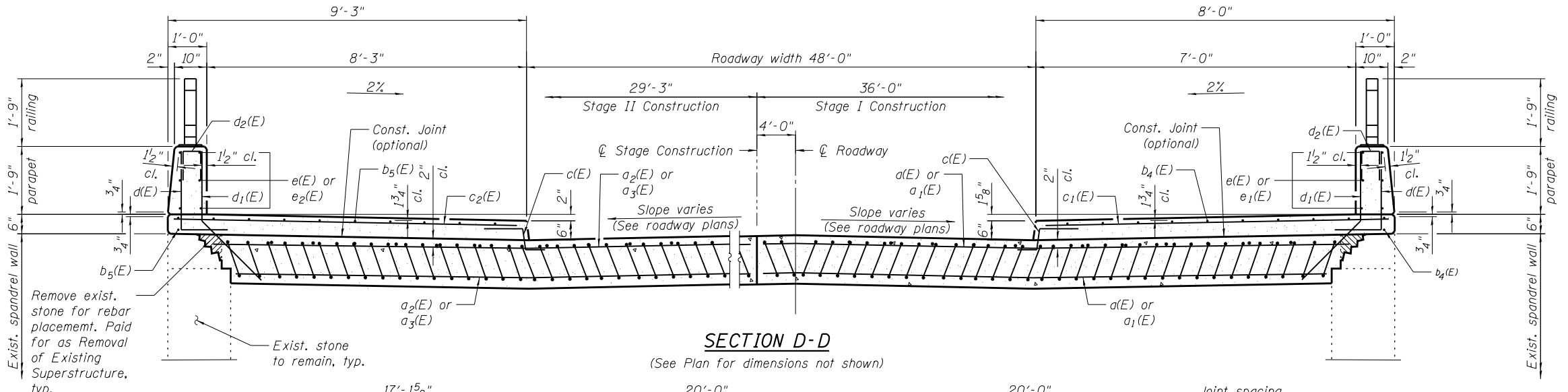
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CONTRACT NO. 64821				
ILLINOIS FED. AID PROJECT				



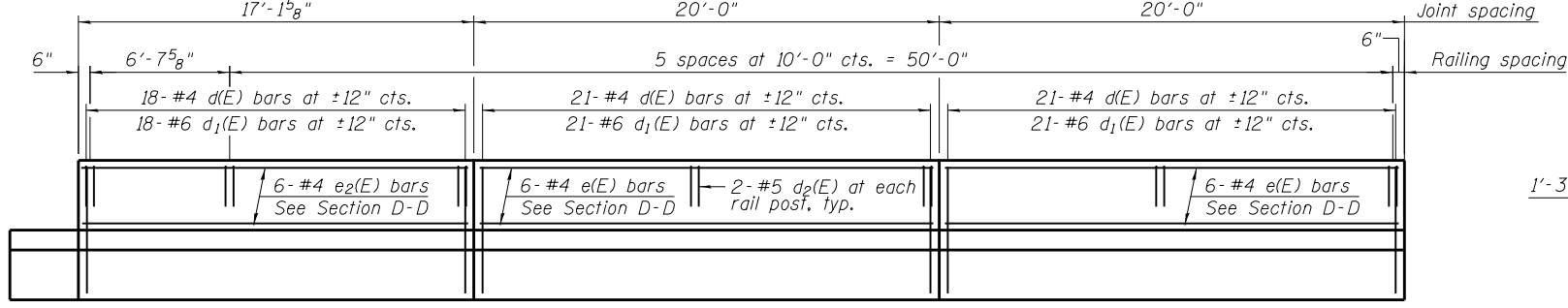
David F. Maxwell, S.E. Reg. No. 081-005455
 Date: October 29, 2012
 My registration expires November 30, 2012



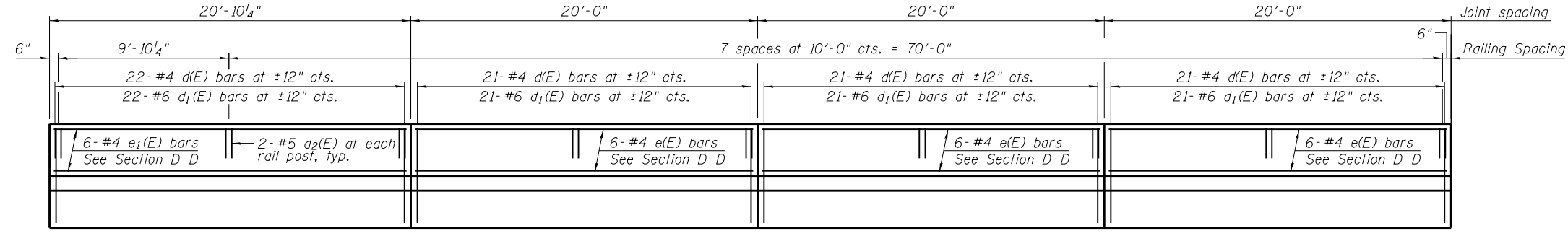
SECTION C-C



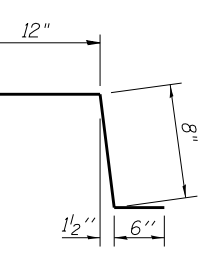
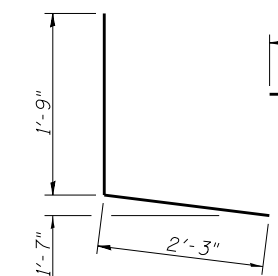
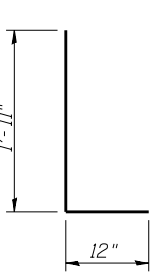
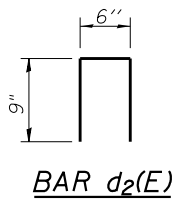
SECTION D-D
(See Plan for dimensions not shown)



VIEW A-A



VIEW B-B



BAR d(E)

BAR d1(E)

BAR c(E)

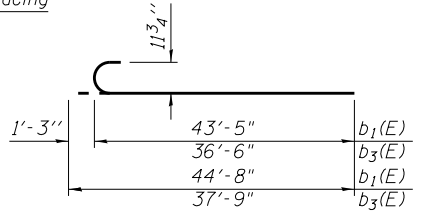
34'-1"	11'-3"	a1(E)
16'-1"	3'-4"	a3(E)
34'-8"	10'-7"	a7(E)
16'-6"	4'-8"	a8(E)

21'-5"	23'-11"	a1(E)
8'-5"	11'-0"	a3(E)
22'-0"	23'-3"	a7(E)
9'-8"	11'-6"	a8(E)

BAR CUT DIAGRAM

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a (E)	156	#5	35'- 0"	—
a1(E)	5	#5	45'- 4"	—
a2(E)	138	#5	28'- 3"	—
a3(E)	4	#5	19'- 5"	—
a4(E)	14	#5	6'- 10"	—
a5(E)	64	#4	4'- 0"	—
a6(E)	28	#6	2'- 6"	—
a7(E)	10	#5	45'- 3"	—
a8(E)	6	#5	21'- 2"	—
b (E)	58	#5	41'- 10"	—
b1(E)	172	#9	44'- 8"	C
b2(E)	48	#5	34'- 11"	—
b3(E)	140	#9	37'- 9"	C
b4(E)	18	#5	41'- 10"	—
b5(E)	20	#5	59'- 7"	—
c (E)	138	#5	2'- 2"	L
c1(E)	81	#5	7'- 8"	—
c2(E)	61	#5	8'- 11"	—
d (E)	145	#4	2'- 11"	L
d1(E)	145	#6	4'- 0"	L
d2(E)	32	#5	2'- 0"	n
e (E)	30	#4	19'- 8"	—
e1(E)	6	#4	16'- 9"	—
e2(E)	6	#4	20'- 6"	—



BAR b1(E) and b3(E)

Item No.	Description	Unit	Quantity
50101500	Removal of Existing Superstructures	Each	1
50300255	Concrete Superstructure	Cu. Yd.	238.2
50300300	Protective Coat	Sq. Yd.	535
50800205	Reinforcement Bars, Epoxy Coated	Pound	64,750

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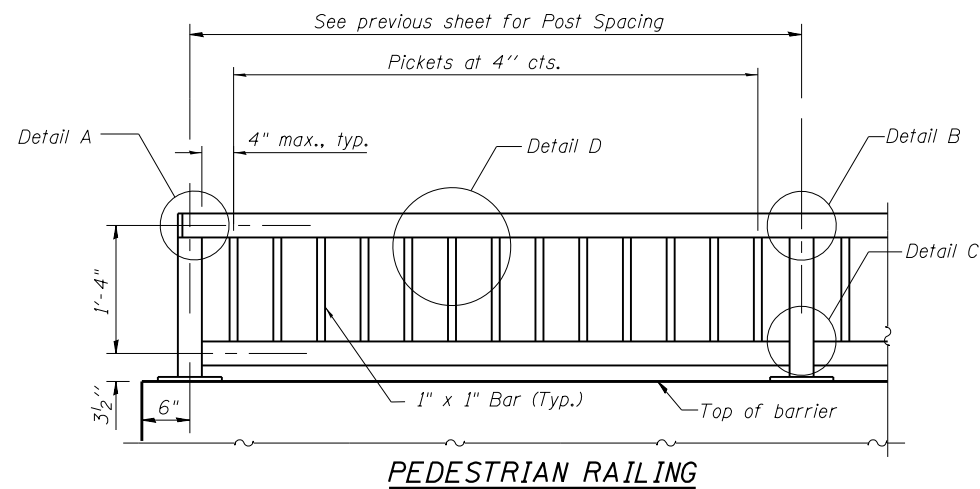
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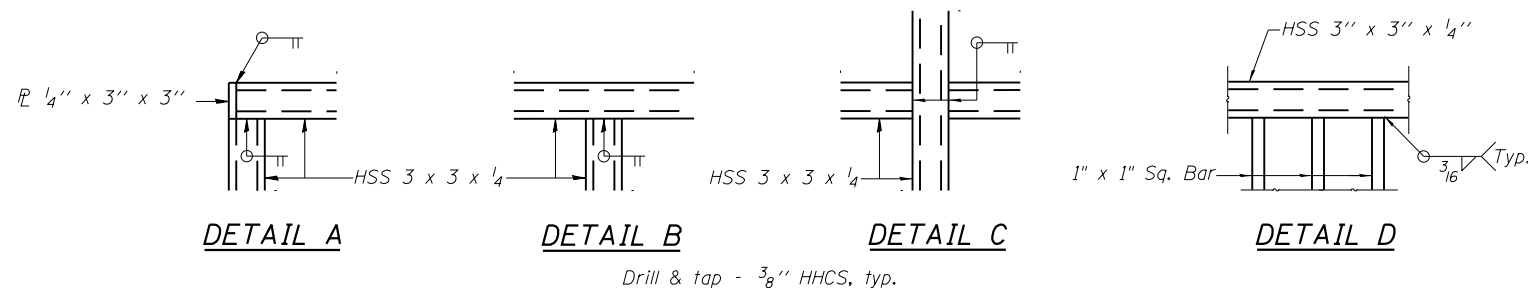
ROADWAY SLAB DETAILS
STRUCTURE NO. 101-6110

SHEET NO. 2 OF 3 SHEETS

F.A.P. RTE. 742	SECTION (32, 33) R-1	COUNTY WINNEBAGO	TOTAL SHEETS 705	SHEET NO. 550
CONTRACT NO. 64821				
ILLINOIS FED. AID PROJECT				

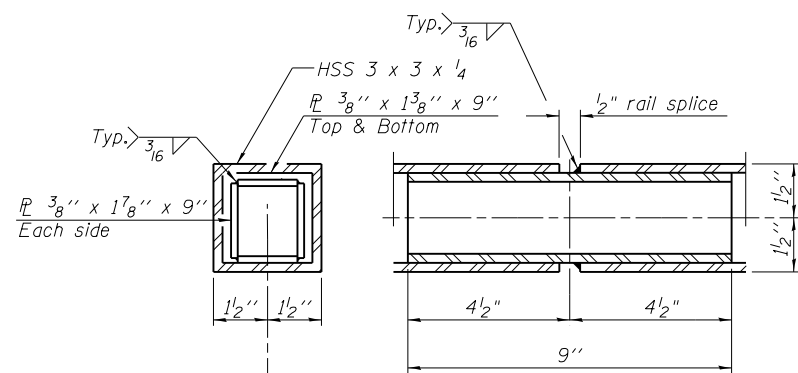


PEDESTRIAN RAILING



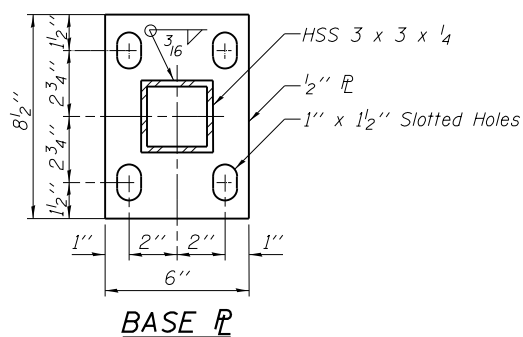
NOTES

All post, railing, pickets, splices, and anchor devices shall be galvanized and powder coated black per the Special Provisions. The galvanizing and powder coating system shall be approved by the engineer prior to the coating process. The galvanizing and powder coating shall be paid for as Painting Steel Railing.

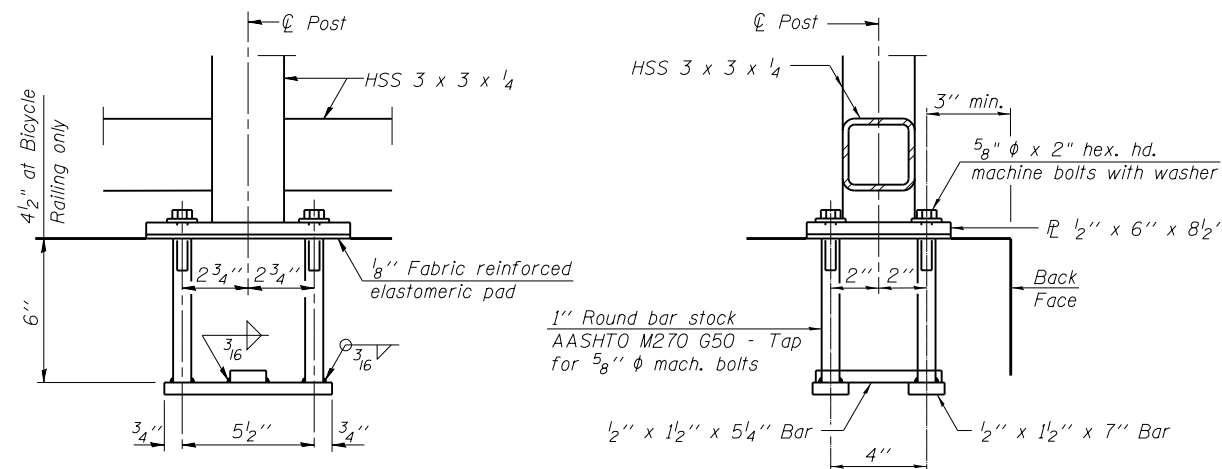


RAIL SPLICE

Rail Splice Spacing = 35'-0" Maximum.



BASE PLATE



ANCHOR BOLT DETAILS

In lieu of the cast-in-place anchor device shown, the Contractor has the option of drilling and setting 5/8" ϕ anchor rods according to Article 509.06 of the Standard Specifications. Embedment shall be according to the manufacturer's specifications, but no greater than 6".

BILL OF MATERIAL

Code No.	Item	Unit	Quantity
50600200	Painting Steel Railing	Foot	138
50900805	Pedestrian Railing	Foot	138

COMPANY NAME: HRGreen
 PROJECT CONTACT: Dave Maxwell
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PLOT DATE = 10/29/2012	CHECKED - MGH	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

RAILING DETAILS
 STRUCTURE NO. 101-6110

SHEET NO. 3 OF 3 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32, 33) R-1	WINNEBAGO	705	551
CONTRACT NO. 64821				

ILLINOIS FED. AID PROJECT

GENERAL NOTES

DESIGN: AASHTO Standard Specifications for Structural Supports for Highway Signs, Luminaires and Traffic Signals. ("AASHTO Specifications")

CONSTRUCTION: Current (at time of letting) Illinois Department of Transportation Standard Specifications for Road and Bridge Construction, Supplemental Specifications and Special Provisions. ("Standard Specifications")

LOADING: 90 M.P.H. WIND VELOCITY

WALKWAY LOADING: Dead load plus 500 lbs. concentrated live load.

DESIGN STRESSES:

Field Units

$f'_c = 3,500$ p.s.i.

$f_y = 60,000$ p.s.i. (reinforcement)

WELDING: All welds to be continuous unless otherwise shown. All welding to be done in accordance with current AWS D1.1 and D1.2 Structural Welding Codes (Steel and Aluminum) and the Standard Specifications.

MATERIALS: Aluminum Alloys as shown throughout plans. All Structural Steel Pipe shall be ASTM A53 Grade B or A500 Grade B or C. If A500 pipe is substituted for A53, then the outside diameter shall be as detailed and wall thickness greater than or equal to A53.

All Structural Steel Plates and Shapes shall conform to AASHTO M270 Gr. 36, Gr. 50 or Gr. 50W*. Stainless steel for shims, sleeves and handhole covers shall be ASTM A240, Type 302 or 304, or another alloy suitable for exterior exposure and acceptable to the Engineer.

The steel pipe and stiffening ribs at the base plate for the column shall have a minimum longitudinal Charpy V-Notch (CVN) energy of 15 lb.-ft. at 40° F. (Zone 2) before galvanizing.

FASTENERS FOR ALUMINUM TRUSSES: All bolts noted as "high strength" must satisfy the requirements of AASHTO M164 (ASTM A325), or approved alternate, and must have matching lock nuts. Threaded studs for splices (if Members interfere) must satisfy the requirements of ASTM A449, ASTM A193, Grade B7, or approved alternate, and must have matching lock nuts. Bolts and lock nuts not required to be high strength must satisfy the requirements of ASTM A307. All bolts and lock nuts must be hot dip galvanized per AASHTO M232. The lock nuts must have nylon or steel inserts. A stainless steel flat washer conforming to ASTM A240 Type 302 or 304, is required under both head and nut or under both nuts where threaded studs are used. High strength bolt installation shall conform to Article 505.04 (f) (2)d of the IDOT Standard Specifications for Road and Bridge Construction. Rotational capacity ("ROCAP") testing of bolts will not be required.

U-BOLTS AND EYEBOLTS: U-Bolts and Eyebolts must be produced from ASTM A276 Type 304, 304L, 316 or 316L, Condition A, cold finished stainless steel, or an equivalent material acceptable to the Engineer. All nuts for U-Bolts and Eyebolts must be lock nuts equivalent to ASTM A307 with nylon or steel inserts and hot dip galvanized per AASHTO M232. A stainless steel flat washer conforming to ASTM A240, Type 302 or 304, is required under each U-Bolt and Eyebolt lock nut.

GALVANIZING: All Steel Grating, Plates, Shapes and Pipe shall be Hot Dip Galvanized after fabrication in accordance with AASHTO M111. Painting is not permitted.

ANCHOR RODS: Shall conform to ASTM F1554 Gr. 105.

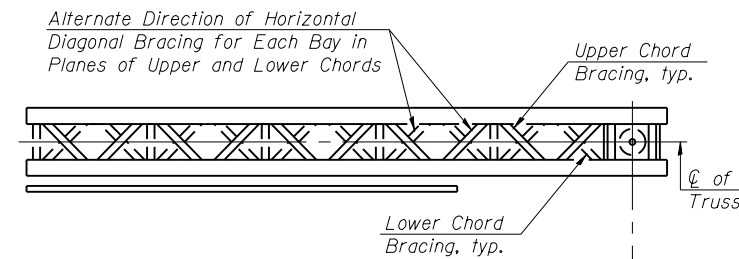
CONCRETE SURFACES: All concrete surfaces above an elevation 6" below the lowest final ground line at each foundation shall be cleaned and coated with Bridge Seat Sealer in accordance with the Standard Specifications.

REINFORCEMENT BARS: Reinforcement Bars designated (E) shall be epoxy coated in accordance with the Standard Specifications.

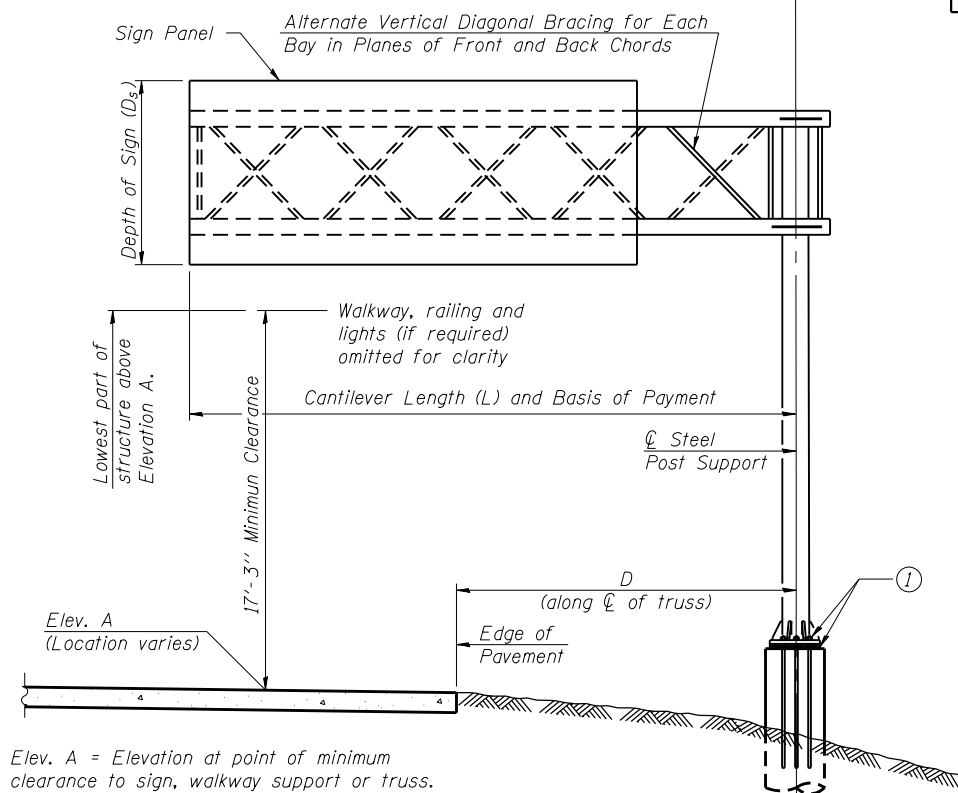
FOUNDATIONS: The contract unit price for Drilled Shaft Concrete Foundations shall include reinforcement bars complete in place.

Structure Number	Station	Design Truss Type	Cantilever Length (L)	Elev. A	Dim. D	D _s	Total Sign Area
2C101S002L008.2	434+55	III-C-A	40'-0"	715.84	6'-0"	15'-0"	37.5

Truss Type	Maximum Sign Area	Maximum Length
I-C-A	170 Sq. Ft.	25 Ft.
II-C-A	340 Sq. Ft.	30 Ft.
III-C-A	400 Sq. Ft.	40 Ft.



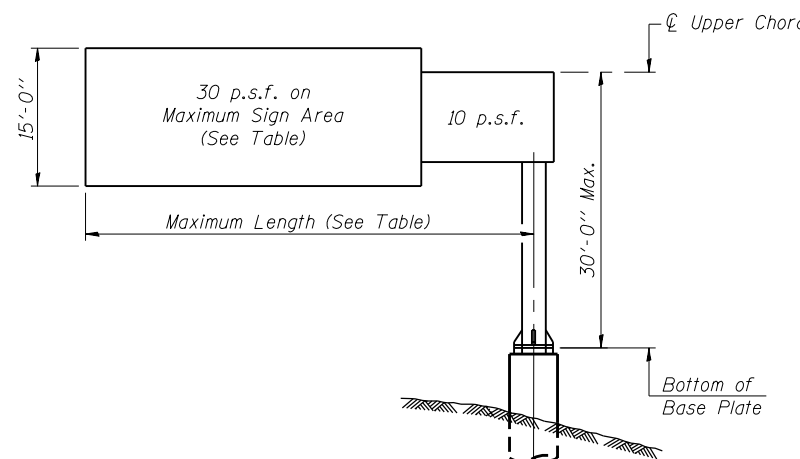
TYPICAL PLAN
(Walkway not shown)



Elev. A = Elevation at point of minimum clearance to sign, walkway support or truss.

TYPICAL ELEVATION
Looking in Direction of Traffic

Sign support structures may be subject to damaging vibrations and oscillations when sign panels are not in place during erection or maintenance of the structure. To avoid these vibrations and oscillations, consideration should be given to attaching temporary blank sign panels to the structure.



DESIGN WIND LOADING DIAGRAM

Parameters shown are basis for I.D.O.T. Standards. Installations not within dimensional limits shown require special analysis for all components.

Note:

Trusses shall be shipped individually with adequate provision to prevent detrimental motion during transport. This may require ropes between horizontals and diagonals or energy dissipating (elastic) ties to the vehicle. The contractor is responsible for maintaining the configuration and protection of the trusses.

① After adjustments to level truss and insure adequate vertical clearance, all top and leveling nuts shall be tightened against the base plate with a minimum torque of 200 lb.-ft. Stainless steel mesh shall then be placed around the perimeter of the base plate. Secure to base plate with stainless steel banding.

* If M270 Gr. 50W (M222) steel is proposed, chemistry for plate to be used shall first be approved by the Engineer as suitable for galvanizing and welding.

TOTAL BILL OF MATERIAL

ITEM #	ITEM	UNIT	TOTAL
73302210	OVERHEAD SIGN STRUCTURE CANTILEVER TYPE III-C-A	Foot	40
73301840	OVERHEAD SIGN STRUCTURE WALKWAY, TYPE A	Foot	34
73400200	DRILLED SHAFT CONCRETE FOUNDATIONS	Cu. Yds.	11.6

OSC-A-1

6-1-12

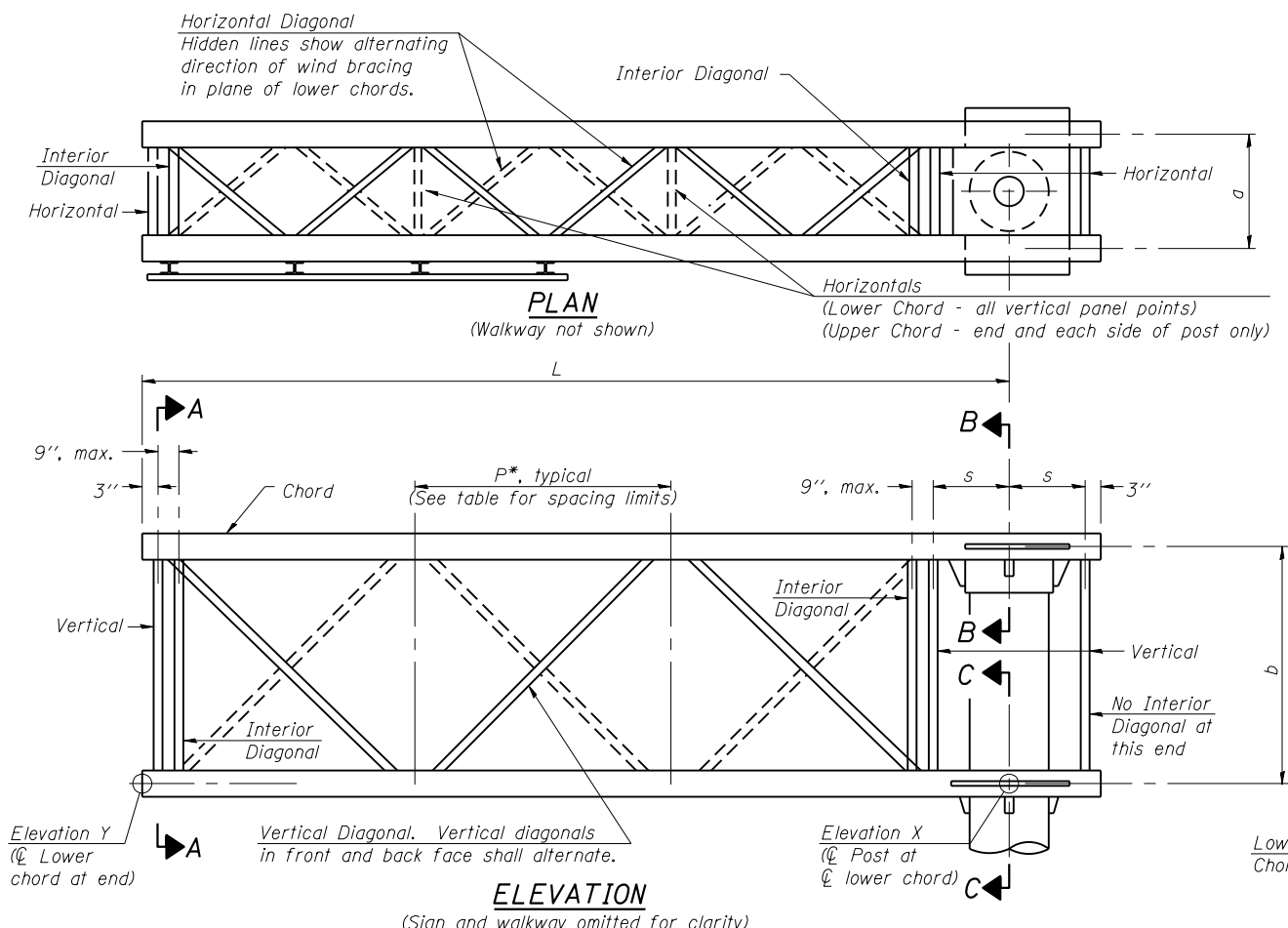
DESIGNED -	EXAMINED	DATE -
CHECKED -	ENGINEER OF BRIDGE DESIGN	
DRAWN -	PASSED	REVISED
CHECKED -	ENGINEER OF BRIDGES AND STRUCTURES	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CANTILEVER SIGN STRUCTURES - GENERAL PLAN & ELEVATION
ALUMINUM TRUSS & STEEL POST

SHEET NO. 1 OF 9 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32,33) R-1	WINNEBAGO	705	552
CONTRACT NO. 64821				
ILLINOIS FED. AID PROJECT				



TYPICAL TRUSS UNIT
(Sign and walkway omitted for clarity)

Note: For Section B-B and Section C-C, see Base Sheet OSC-A-3.

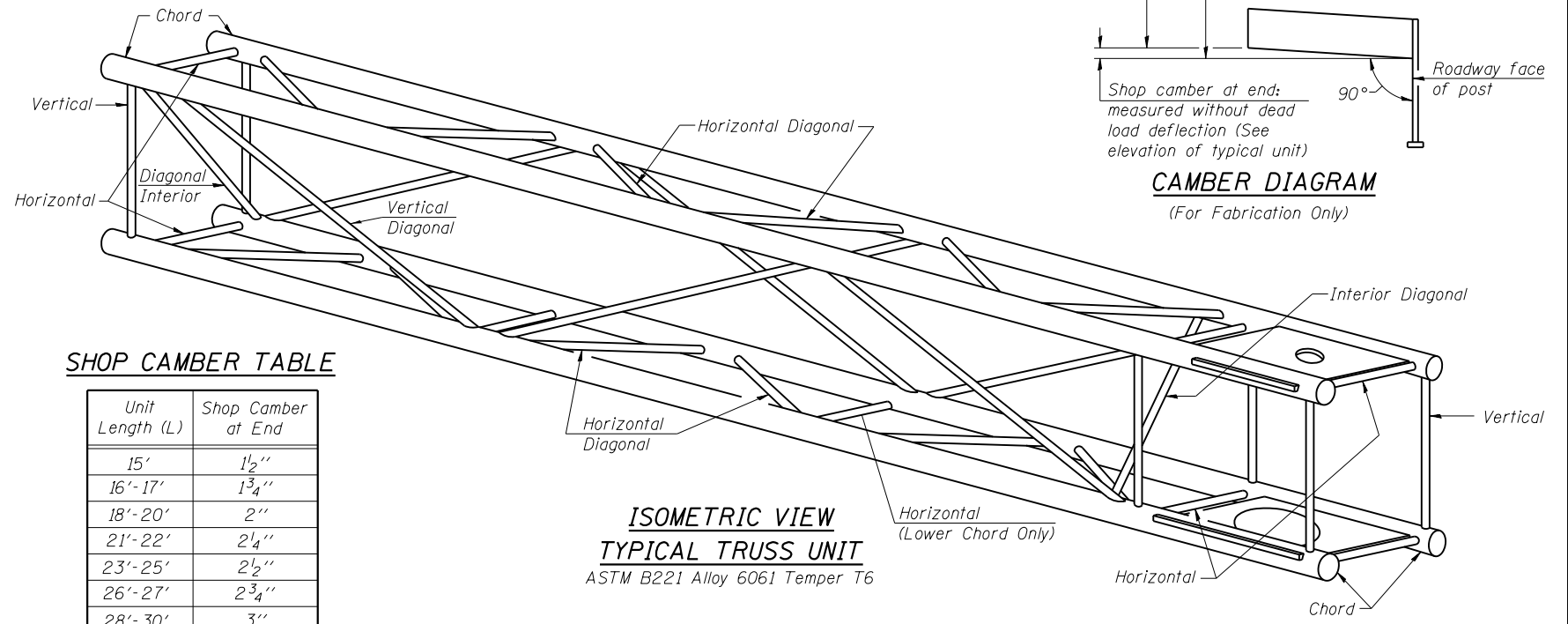
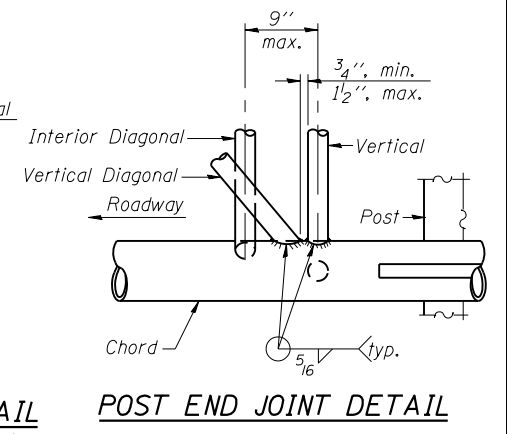
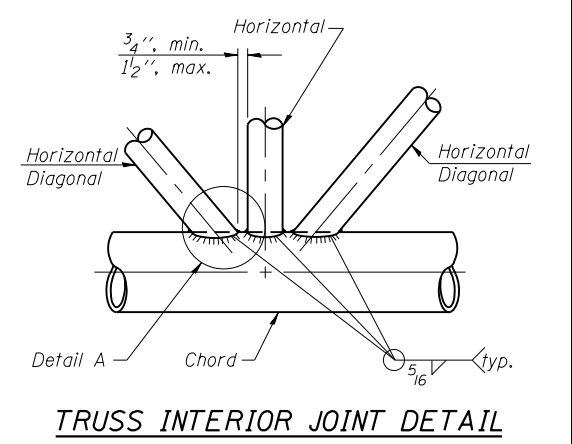
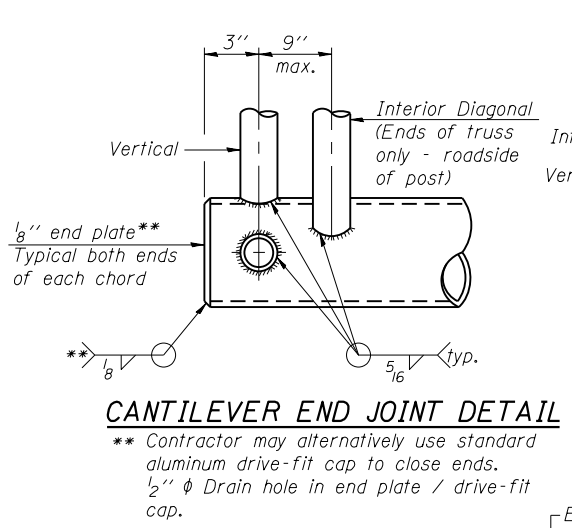
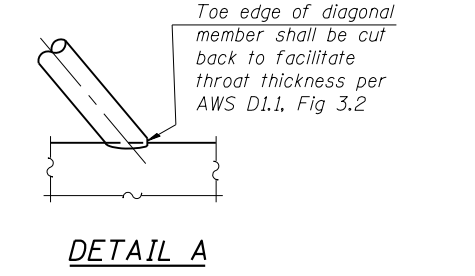
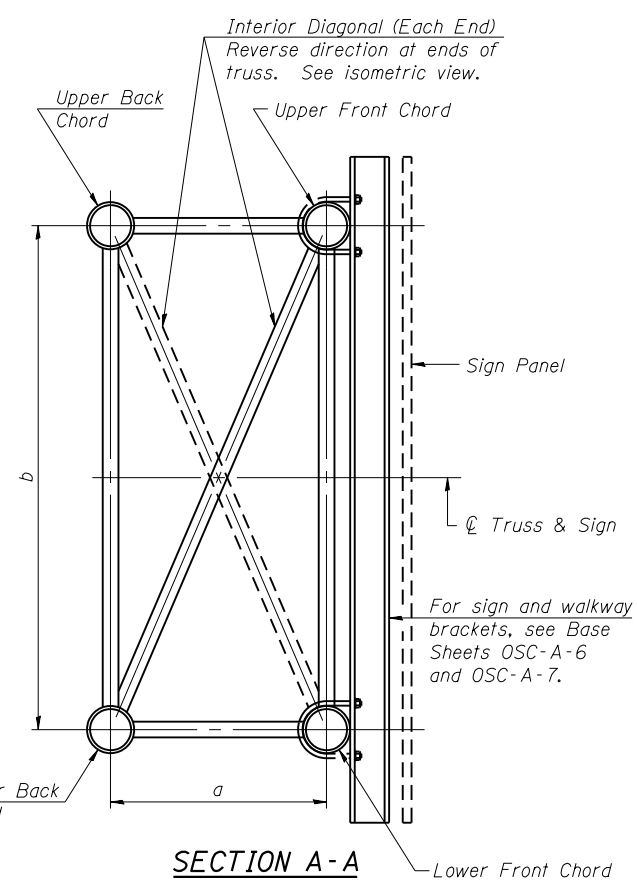
There are twice as many horizontal diagonals as there are vertical diagonals.

TRUSS UNIT TABLE

Truss Type	Dimension "a"	Dimension "b"	Dimension "s"	Limits for Panel Spacing (P)*	Up. & Low. Chord		Verticals; Horizontals; Vertical, Horizontal, and Interior Diagonals	
					O.D.	Wall	O.D.	Wall
I-C-A	24"	54"	16"	36" min. to 48" max.	5"	5/16"	2 1/2"	5/16"
II-C-A	36"	66"	21"	42" min. to 54" max.	6 1/2"	5/16"	3 1/4"	5/16"
III-C-A (35' Max.)	36"	84"	21"	48" min. to 66" max.	7"	3/8"	3 1/2"	3/8"
III-C-A (>35' to 40')	36"	84"	21"	48" min. to 66" max.	8"	3/8"	3 1/2"	3/8"

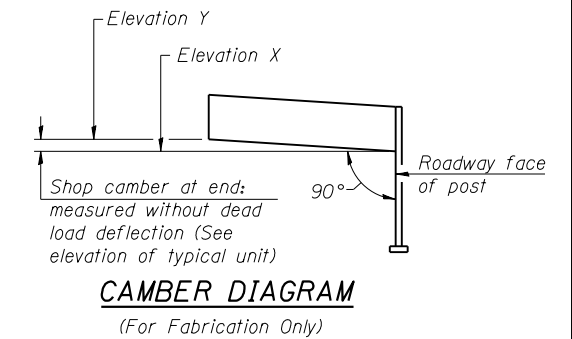
*P = $\frac{L-s-3"}{\# \text{ Panels}}$

Structure Number	Station	Truss Type	Design Length (L)	Number of Panels Per Unit	Panel Length (P)*
2C10IS002L008.2	434+55	III-C-A	40'-0"	8	57"



SHOP CAMBER TABLE

Unit Length (L)	Shop Camber at End
15'	1 1/2"
16'-17'	1 3/4"
18'-20'	2"
21'-22'	2 1/4"
23'-25'	2 1/2"
26'-27'	2 3/4"
28'-30'	3"
31'-32'	3 1/4"
33'-35'	3 1/2"
36'-37'	4"
38'-40'	4 1/2"



OSC-A-2 6-1-12

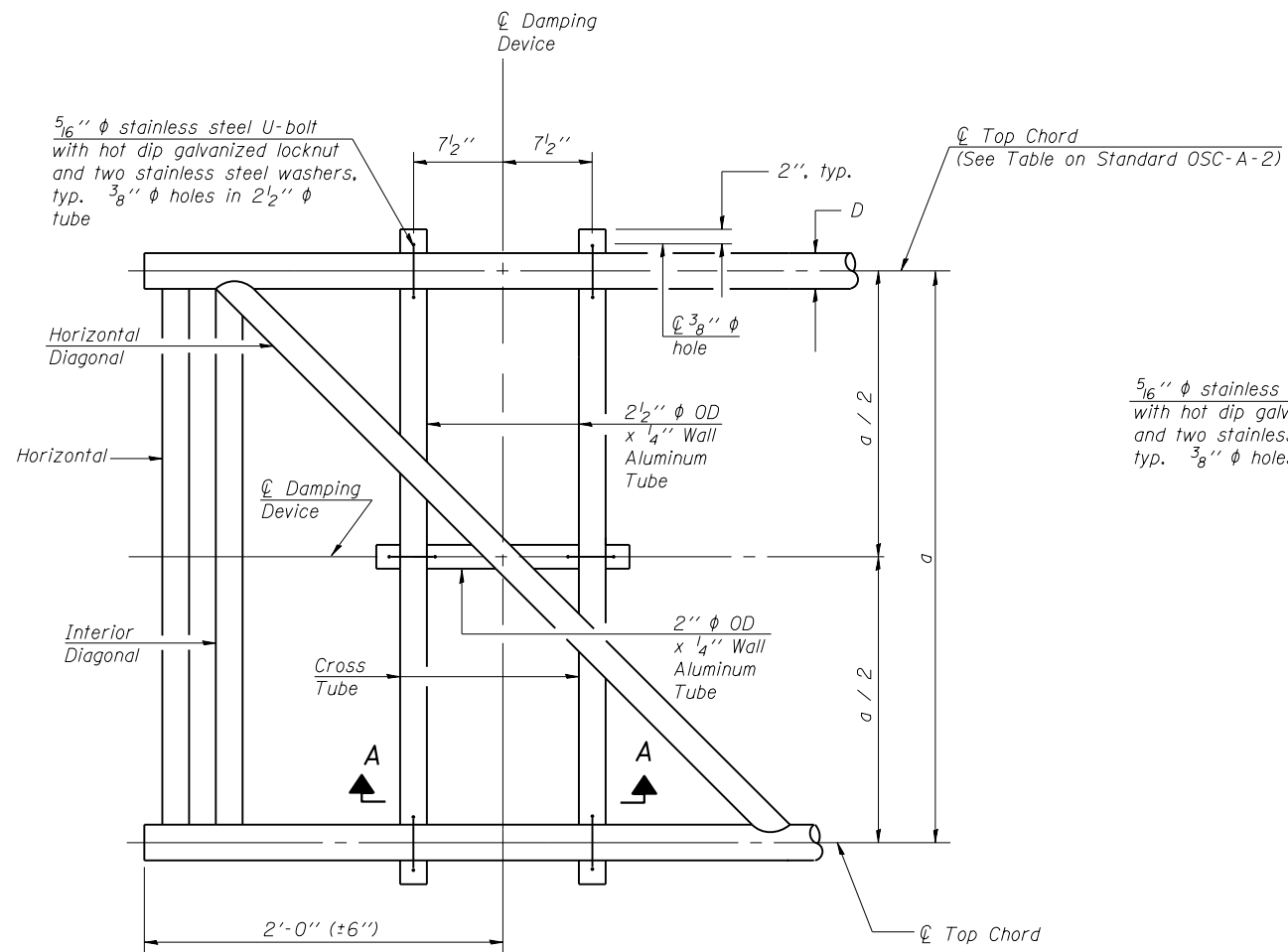
DESIGNED -	EXAMINED	DATE -
CHECKED -	ENGINEER OF BRIDGE DESIGN	
DRAWN -	PASSED	REVISED
CHECKED -	ENGINEER OF BRIDGES AND STRUCTURES	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

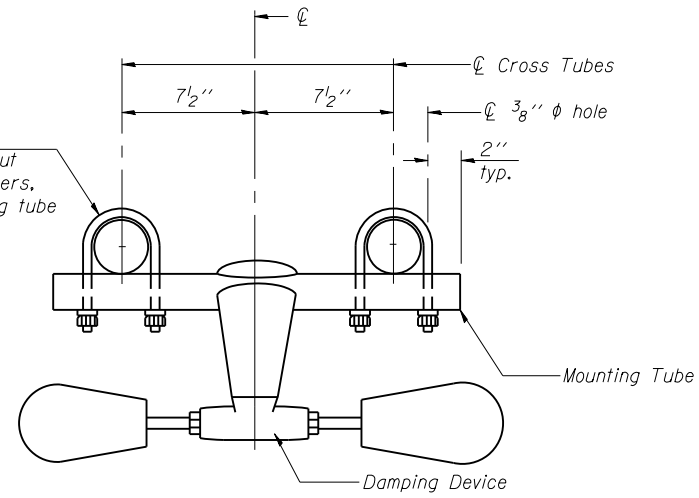
CANTILEVER SIGN STRUCTURES - TRUSS DETAILS
ALUMINUM TRUSS & STEEL POST

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32,33) R-1	WINNEBAGO	705	553
CONTRACT NO. 64821			ILLINOIS FED. AID PROJECT	

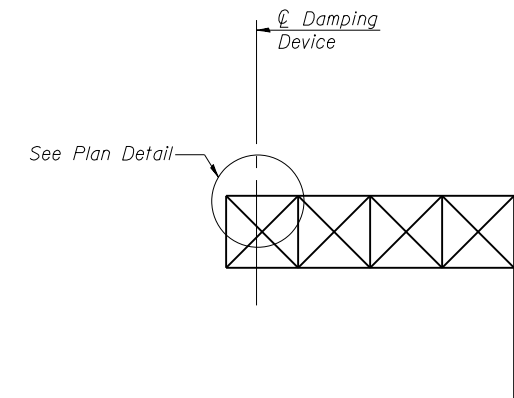
SHEET NO. 2 OF 9 SHEETS



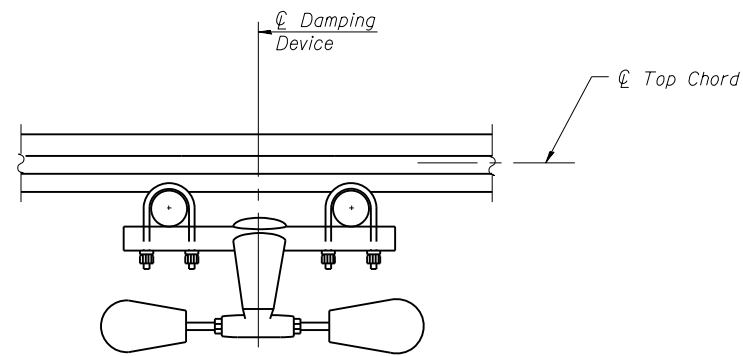
PLAN DETAIL



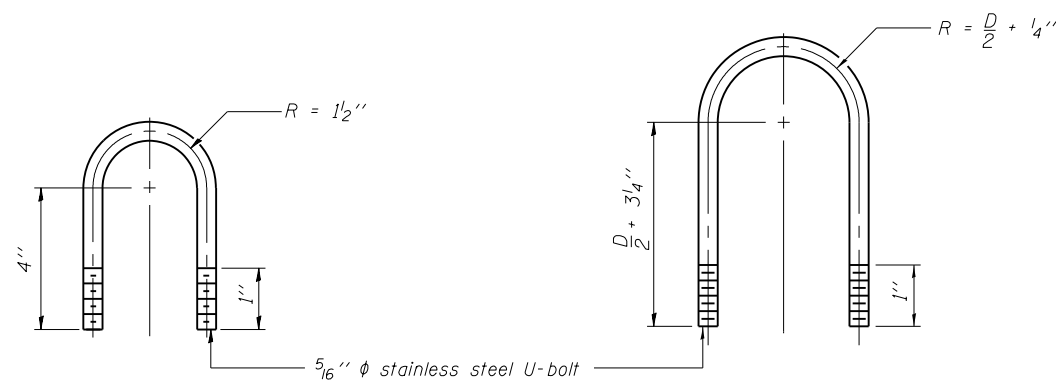
TRUSS DAMPING DEVICE CONNECTION DETAIL



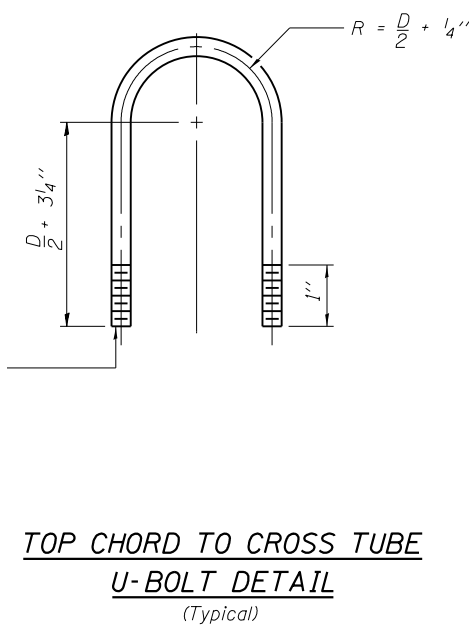
ELEVATION
Aluminum Cantilever Sign Structure



SECTION A-A



DAMPING DEVICE MOUNTING TUBE U-BOLT DETAIL
(Typical)



TOP CHORD TO CROSS TUBE U-BOLT DETAIL
(Typical)

- GENERAL NOTES**
- Damper: One damper per truss. (31 lbs. Stockbridge-Type Aluminum-29" minimum between ends of weights)
 - Materials: Aluminum tubes shall be ASTM B221 alloy 6061 temper T6

OSC-A-D 6-1-12

DESIGNED -	EXAMINED _____	DATE - _____
CHECKED -	ENGINEER OF BRIDGE DESIGN	
DRAWN -	PASSED _____	REVISED _____
CHECKED -	ENGINEER OF BRIDGES AND STRUCTURES	REVISED _____

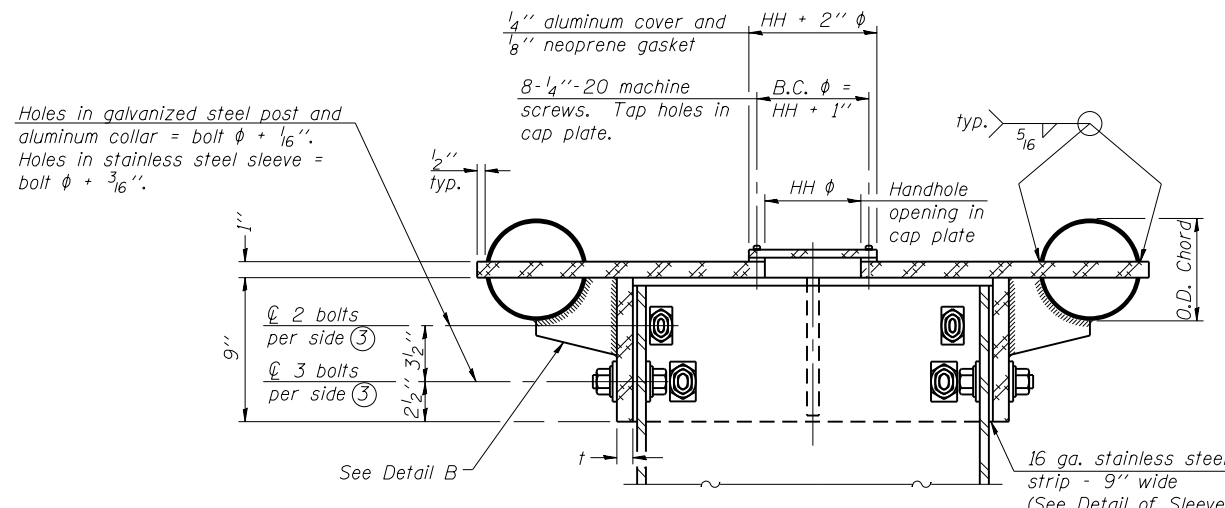
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CANTILEVER SIGN STRUCTURE
DAMPING DEVICE

SHEET NO. 3 OF 9 SHEETS

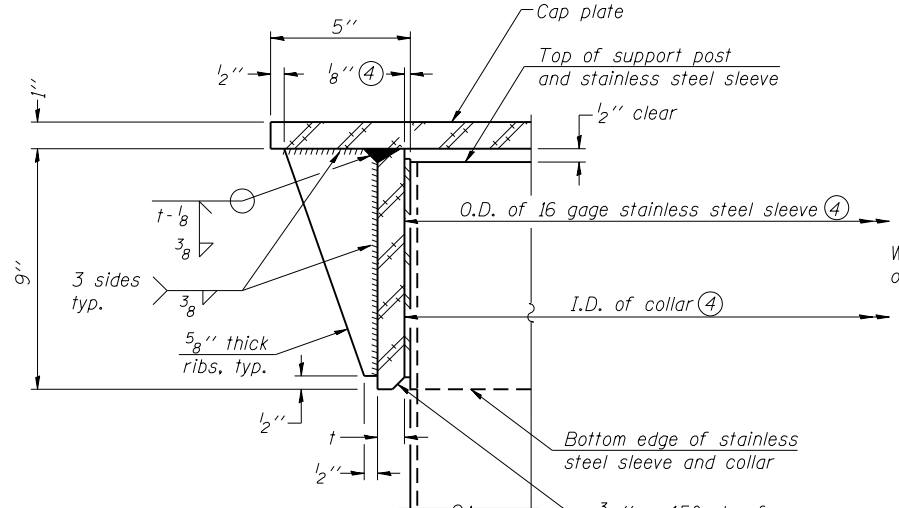
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32,33) R-1	WINNEBAGO	705	554
CONTRACT NO. 64821				

ILLINOIS FED. AID PROJECT

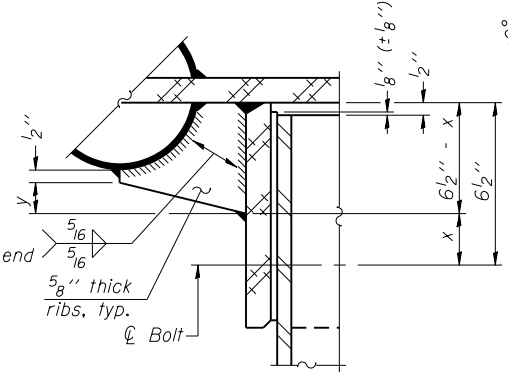


④ Collar I.D. shall be manufactured to correspond to O.D. of actual galvanized post and stainless steel sleeve plus 1/8" (±1/16"). Maximum gap between post and collar at any location equals 1/8" before tightening bolts.

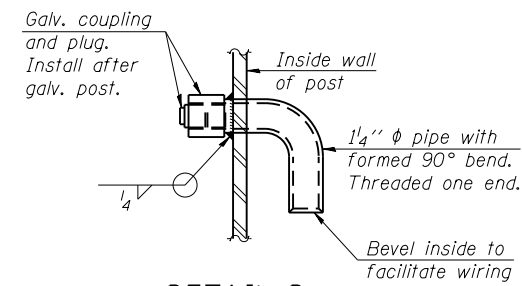
SECTION B-B
Bolts, washers (including contoured washers), and locknuts shall be stainless steel.



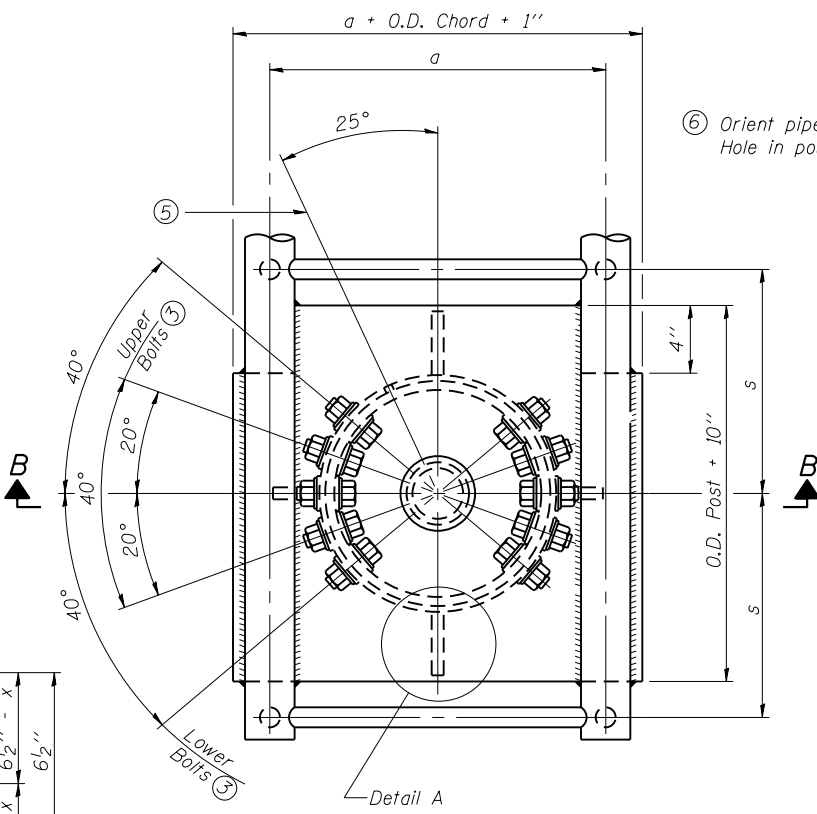
DETAIL A
(Two locations)
3/16" - 45° chamfer on inside of collar to facilitate field assembly



DETAIL B
Two locations
(For details not shown, see Detail C)

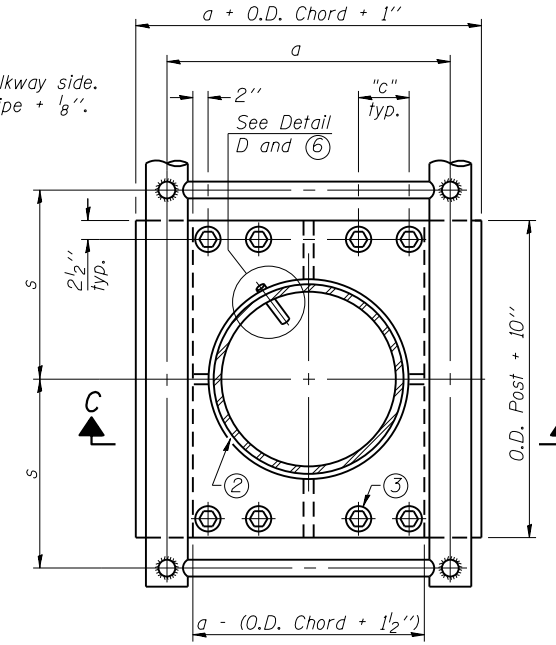


DETAIL D



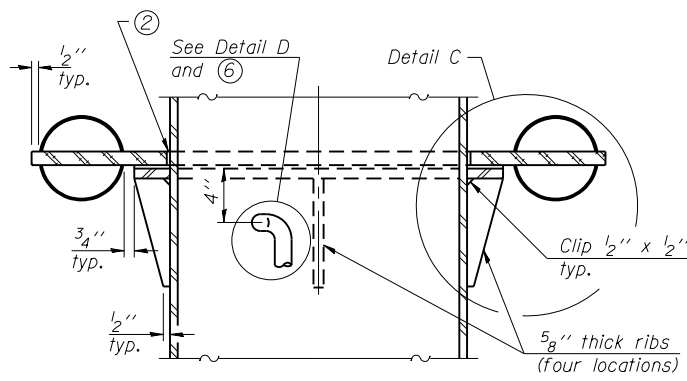
PLAN VIEW - TOP OF COLUMN

⑤ Optional full penetration weld in collar. (Two locations maximum....(180° apart)....X-ray or UT 100%)

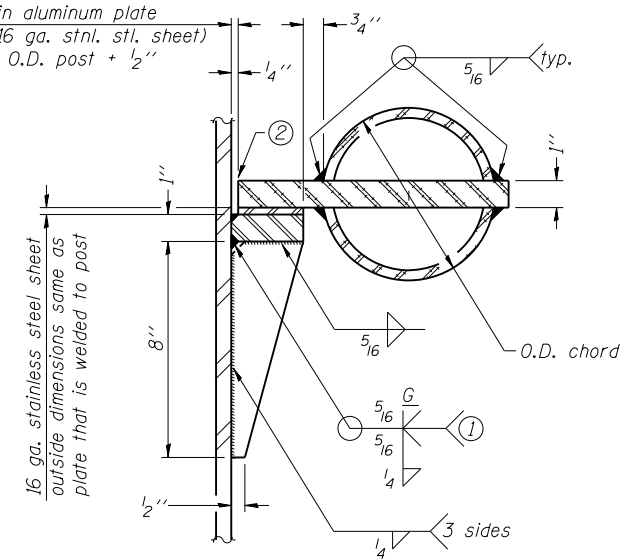


SECTION THRU POST ABOVE LOWER CHORDS

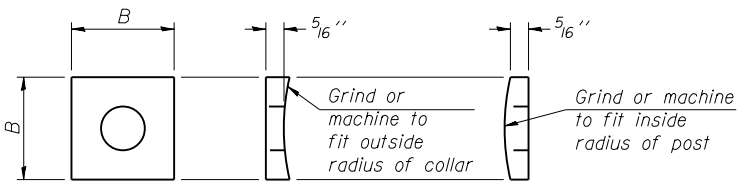
Hole in aluminum plate (and 16 ga. stnl. stl. sheet) to be O.D. post + 1/2"



SECTION C-C



DETAIL C



CONTOURED WASHERS

Bolt Size	Contoured Washers	
	Hole Dia.	B
7/8"	1"	2 1/2"
1"	1 1/8"	3"
1 1/4"	1 3/8"	3 1/4"

DETAIL OF STAINLESS STEEL SLEEVE

Weld to post after galvanizing. (Prepare post surface to insure tight, uniform fit and allow welding.) Welds to be 1 1/2" long at 6" cts. along top edge and at 1/4" opening.

Truss Type	Post Size	Upper & Lower Connection Bolt Diameter ③	Lower Juncture Bolt Spacing Dimension "c" ③	Opening in Cap Plate "HH"	Collar Thickness (t)	Side Ribs	
						x	y
I-C-A	16" phi (83#/')	7/8"	3 1/4"	8"	5/8"	1 3/4"	2 1/4"
II-C-A	24" phi (125#/')	1"	3 1/2"	12"	7/8"	2"	1 1/4"
III-C-A (35' max.)	24" phi (125#/')	1 1/4"	3 1/2"	12"	7/8"	2"	1"
III-C-A (>35' to 40')	24" phi (171#/')	1 1/4"	3 1/2"	12"	7/8"	2"	1"

- ① Grind top if required to fully seat aluminum plate and stainless steel sheet.
- ② After tightening lower connection bolts, fill gap with non-hardening, silicone caulk suitable for exterior exposure and acceptable to the Engineer. Cost is included in Overhead Sign Structure Cantilever.
- ③ Upper and lower connection bolts in collar and bolts at lower chord connection shall be high strength with matching locknuts. Connection bolts shall have 2 stainless steel flat washers each.

OSC-A-3

6-1-12

DESIGNED -	EXAMINED -	DATE -
CHECKED -	ENGINEER OF BRIDGE DESIGN	
DRAWN -	PASSED -	REVISOR -
CHECKED -	ENGINEER OF BRIDGES AND STRUCTURES	REVISOR -

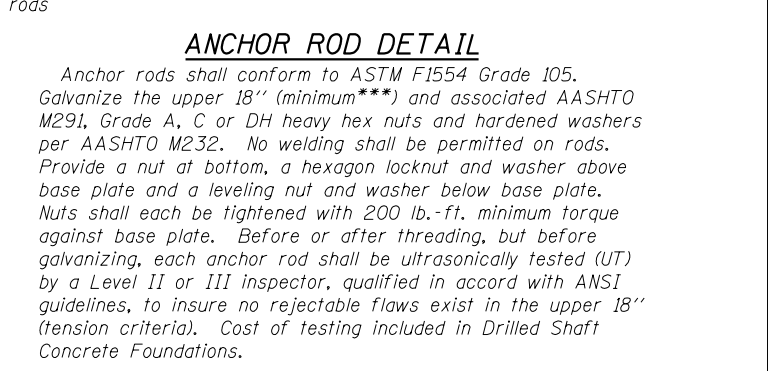
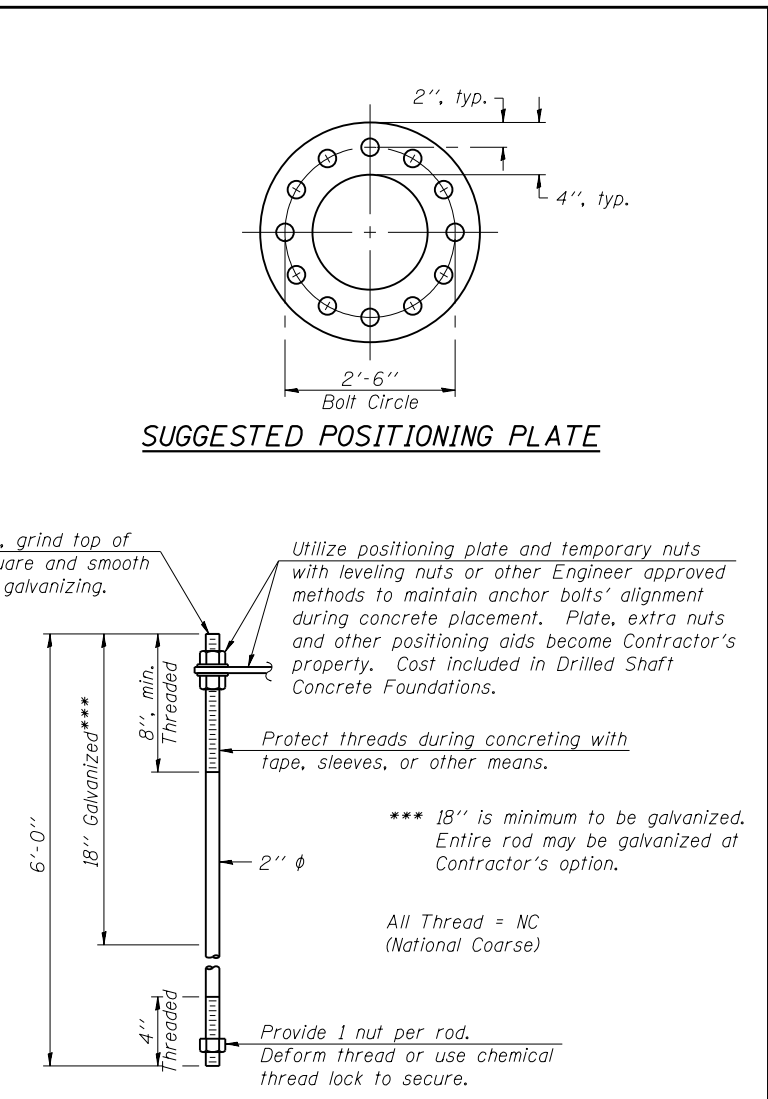
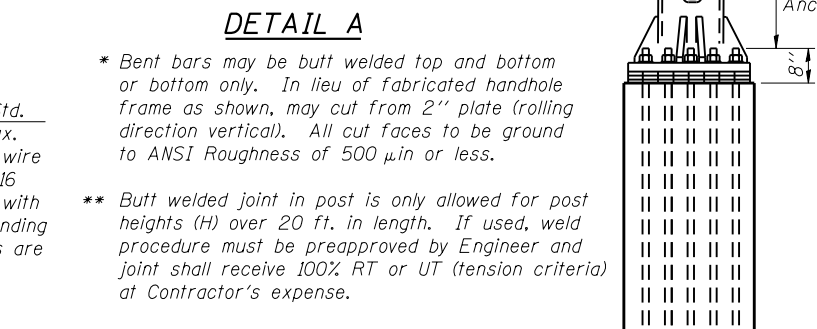
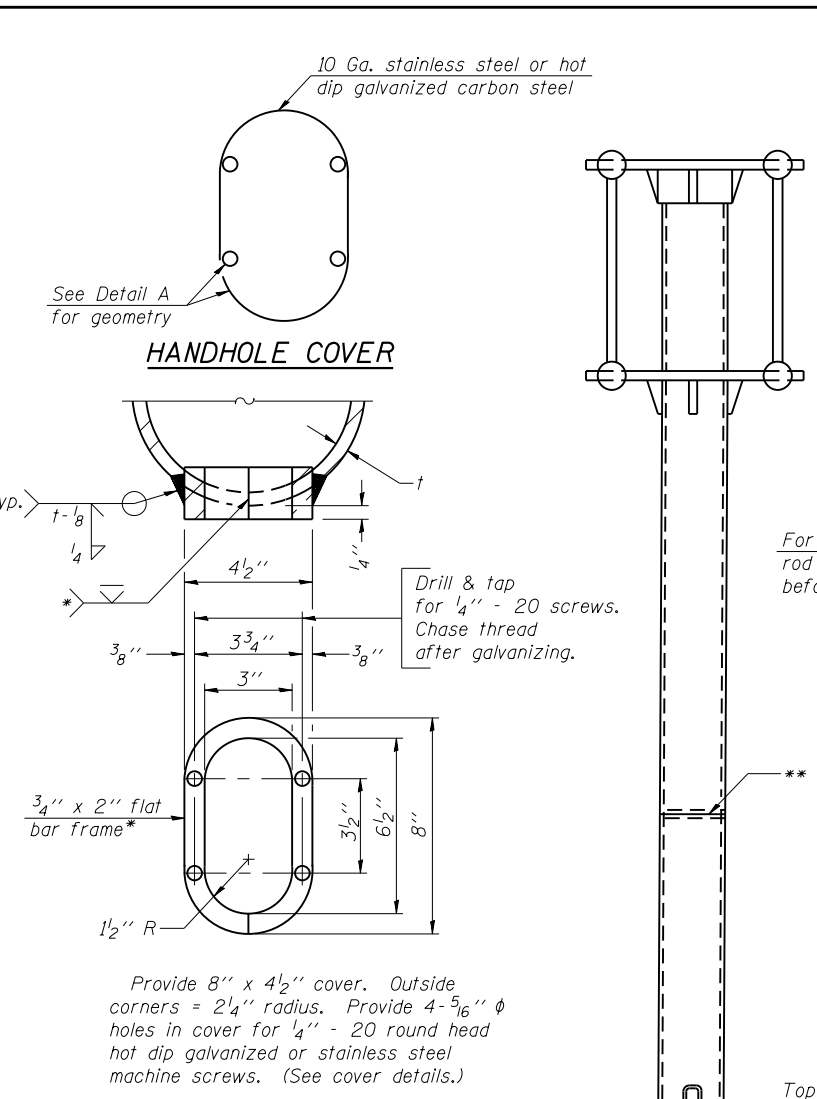
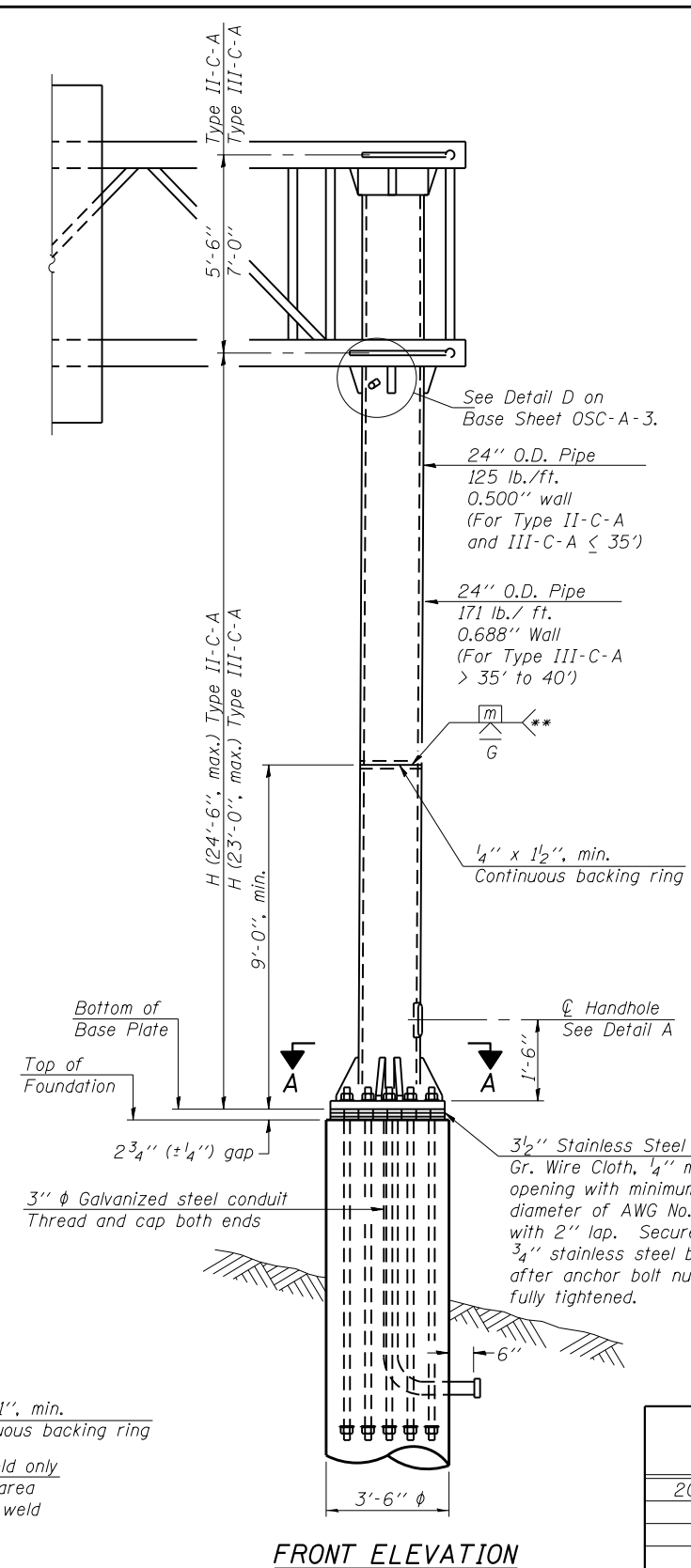
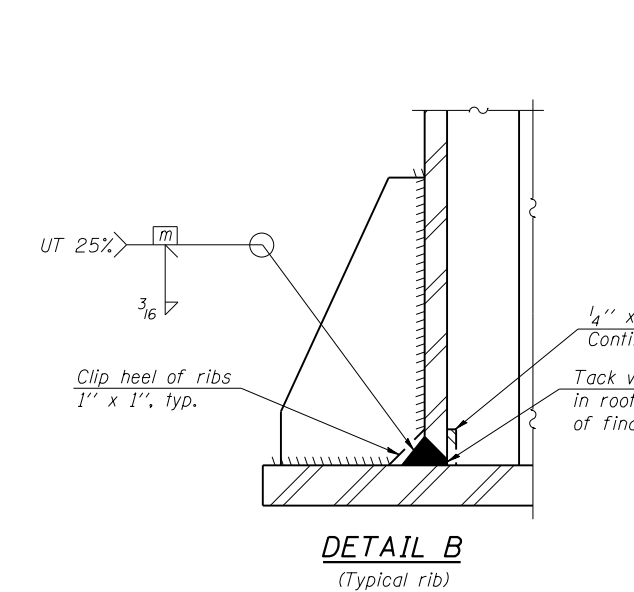
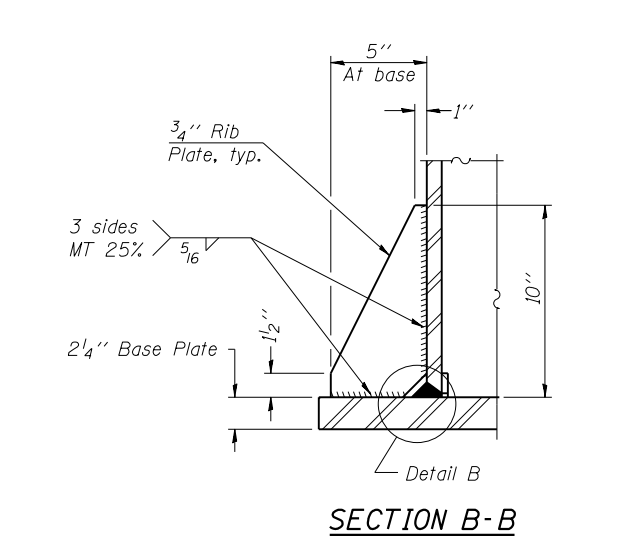
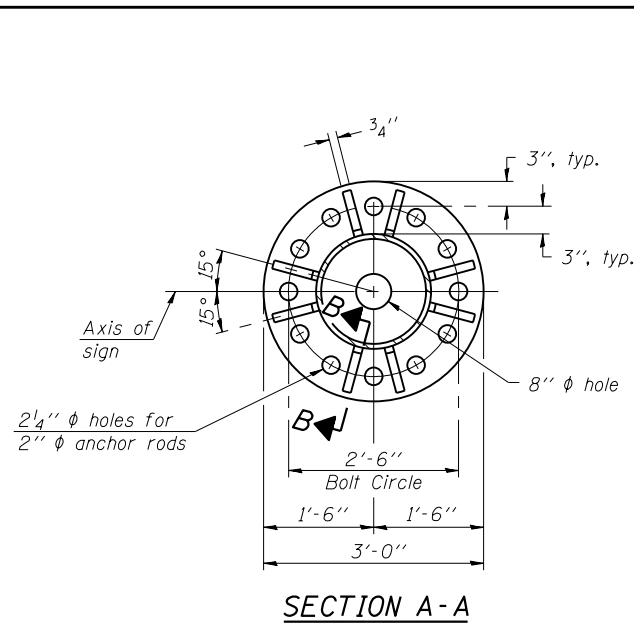
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CANTILEVER SIGN STRUCTURES - JUNCTURE DETAILS
ALUMINUM TRUSS & STEEL POST

SHEET NO. 4 OF 9 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32,33) R-1	WINNEBAGO	705	555
CONTRACT NO. 64821				

ILLINOIS FED. AID PROJECT



Structure Number	Station	H
2C10IS002L008.2	434+55	16'-0"

Note: "H" based on 15'-0" or actual sign height, whichever is greater.

OSC-A-5

6-1-12

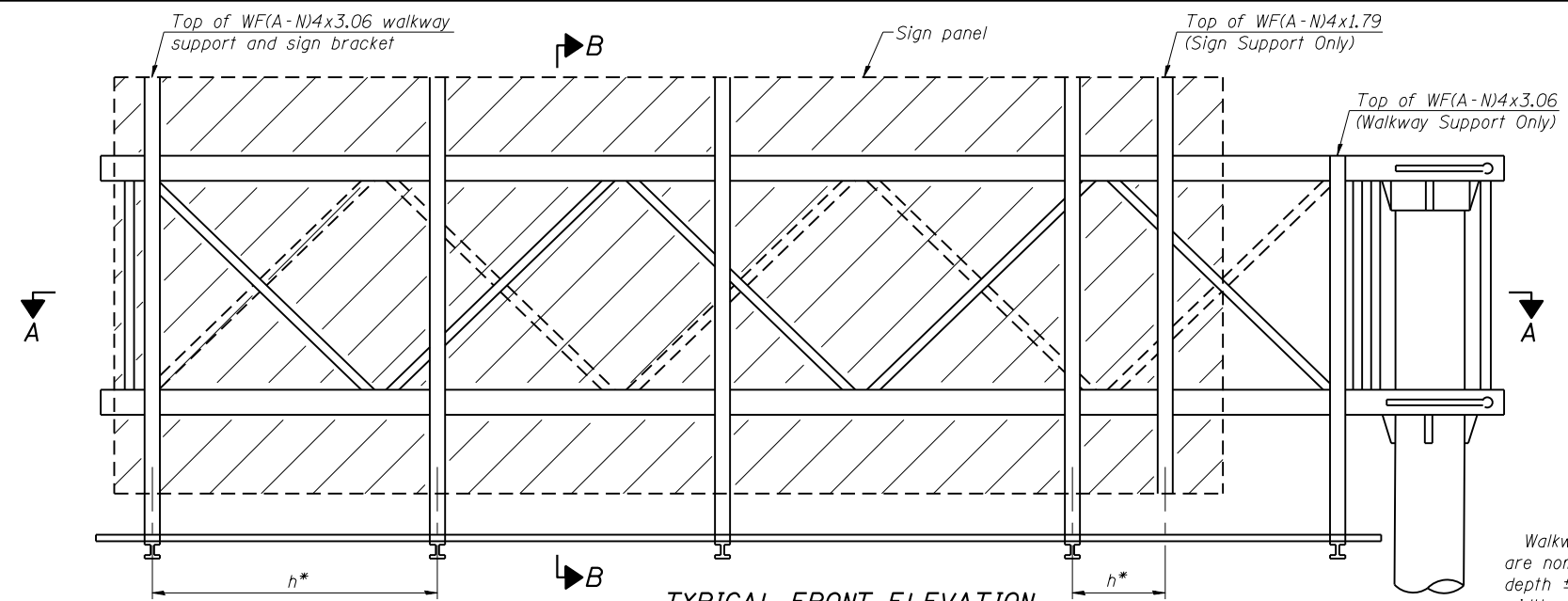
DESIGNED -	EXAMINED	DATE -
CHECKED -	ENGINEER OF BRIDGE DESIGN	
DRAWN -	PASSED	REVISED
CHECKED -	ENGINEER OF BRIDGES AND STRUCTURES	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CANTILEVER SIGN STRUCTURES - TYPE II-C-A & III-C-A
TRUSS SUPPORT POST - ALUMINUM TRUSS & STEEL POST

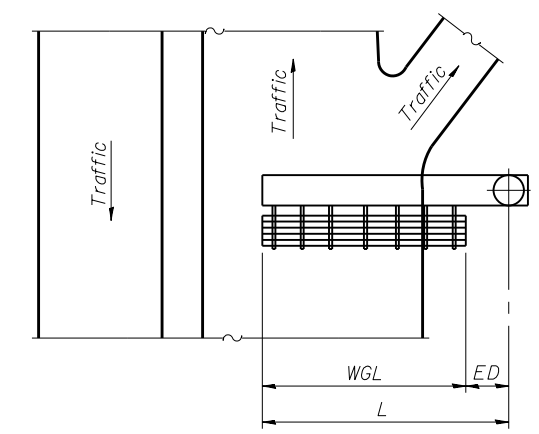
SHEET NO. 5 OF 9 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 64821				
ILLINOIS FED. AID PROJECT				

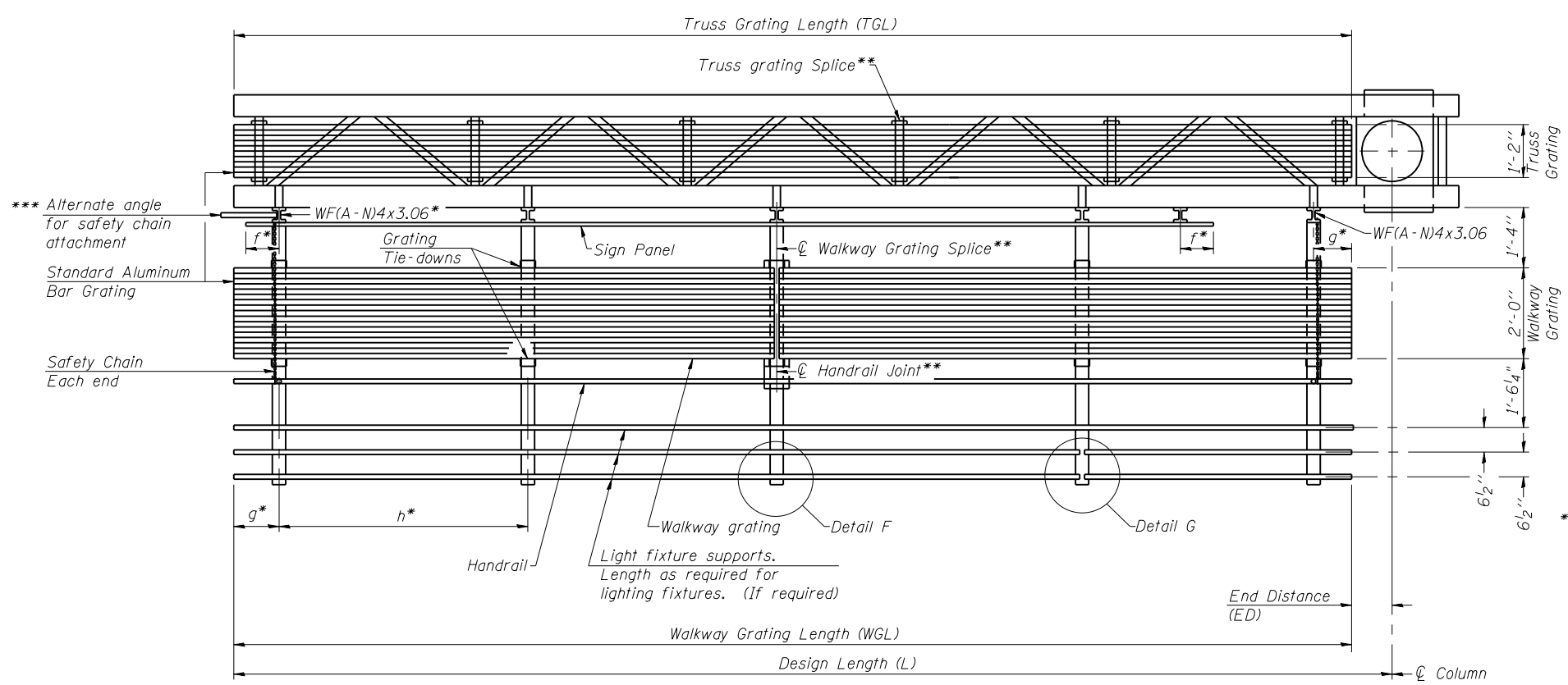


TYPICAL FRONT ELEVATION
With lights and handrail omitted for clarity.

Walkway and truss grating dimensions are nominal and may vary (width ±½", depth ±½") based on available standard widths.



PLAN WALKWAY AND HANDRAIL SKETCH
(Road plan beneath truss varies)



SECTION A-A

Truss grating to facilitate inspection shall run full length of cantilevers. Cost of truss grating is included in Overhead Sign Structure Cantilever.

Handrail and walkway grating shall span a minimum of three brackets between splices.
** Use and location of handrail joints or grating splices are optional, based on lengths needed and material availability.

$$TGL = L - \left(\frac{\text{Post O.D.}}{2} + 6'' \right)$$

Structure Number	Station	WGL	ED	TGL
2C101S002L008.2	434+55	34'-0"	6'-0"	38'-6"

Notes:
* Space walkway brackets WF(A-N)4x3.06 and sign brackets WF(A-N)4x1.79 for efficiency and within limits shown:
 $f = 12''$ maximum, $4''$ minimum (End of sign to ϕ of nearest bracket)
 $g = 12''$ maximum, $4''$ minimum (End of walkway to ϕ of nearest bracket)
 $h = 6'-0''$ maximum (ϕ to ϕ sign and/or walkway support brackets, WF(A-N)4x1.79 or WF(A-N)4x3.06)
*** If walkway bracket at safety chain location is behind sign, add angle to bracket. See alternate safety chain attachment on base sheet OSC-A-8
For details of sign placement, sign/walkway brackets, truss and walkway gratings, grating splices and Section B-B, see Base Sheet OSC-A-7.
For details of handrail, handrail joint, safety chain and Details F and G, see Base Sheet OSC-A-8.

BRACKET TABLE

Sign Width		Number Brackets Required
Greater Than	Less Than or Equal To	
	8'-0"	2
8'-0"	14'-0"	3
14'-0"	20'-0"	4
20'-0"	26'-0"	5
26'-0"	32'-0"	6

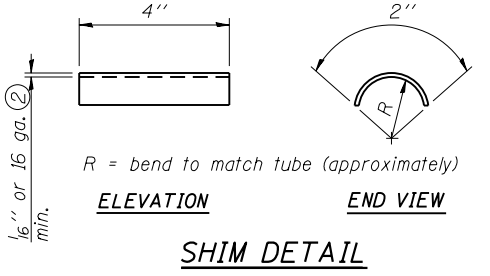
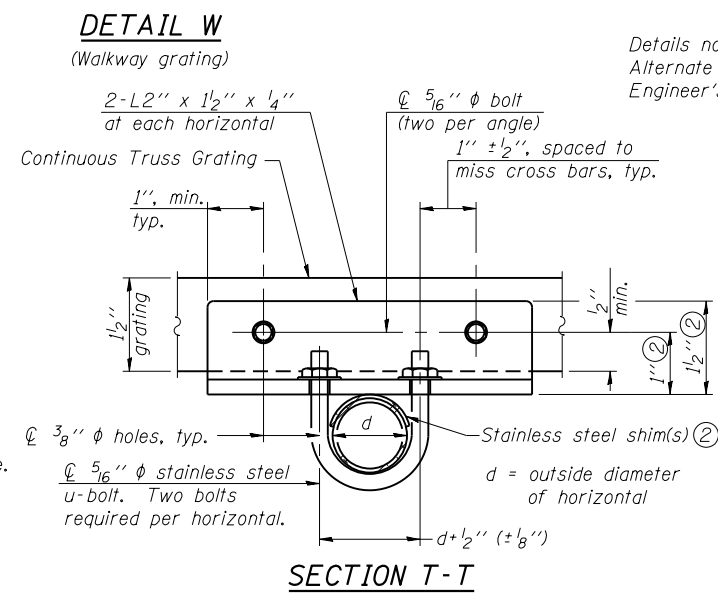
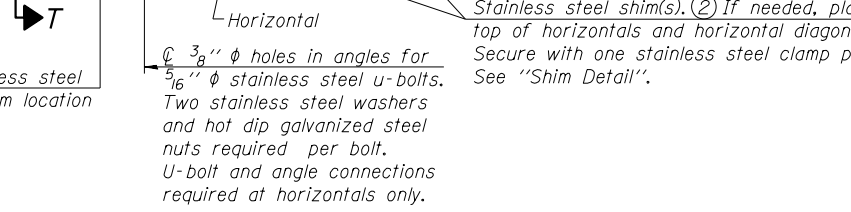
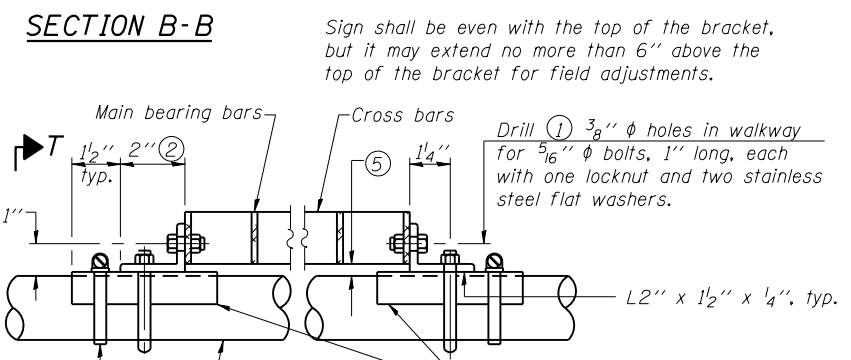
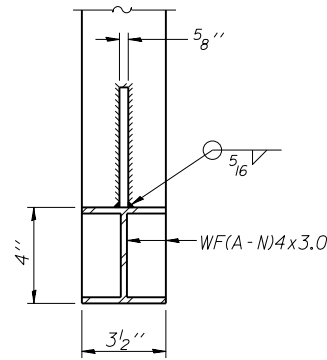
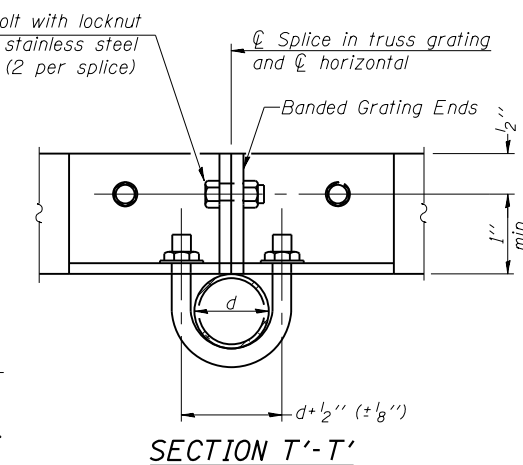
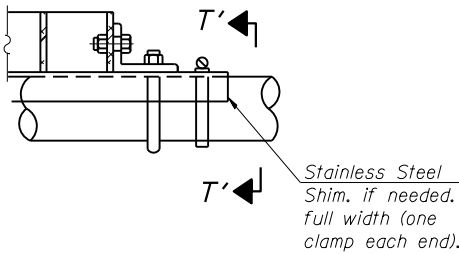
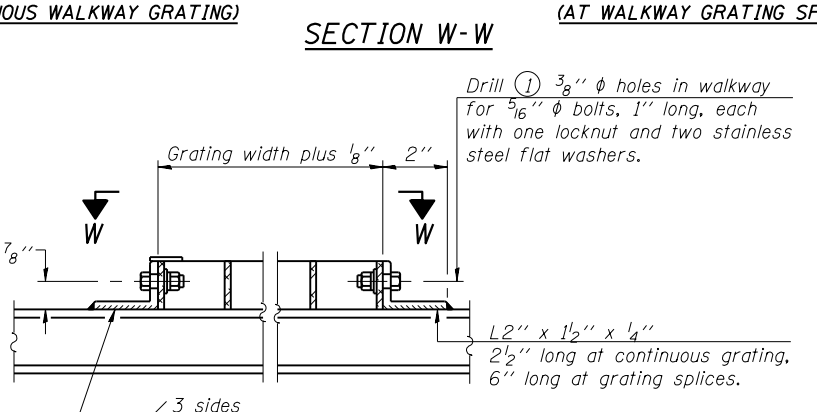
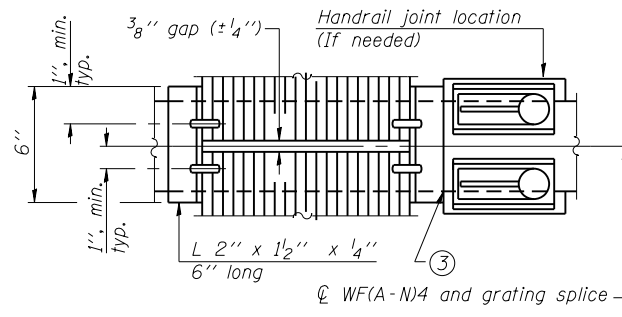
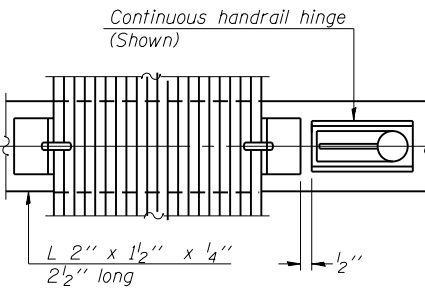
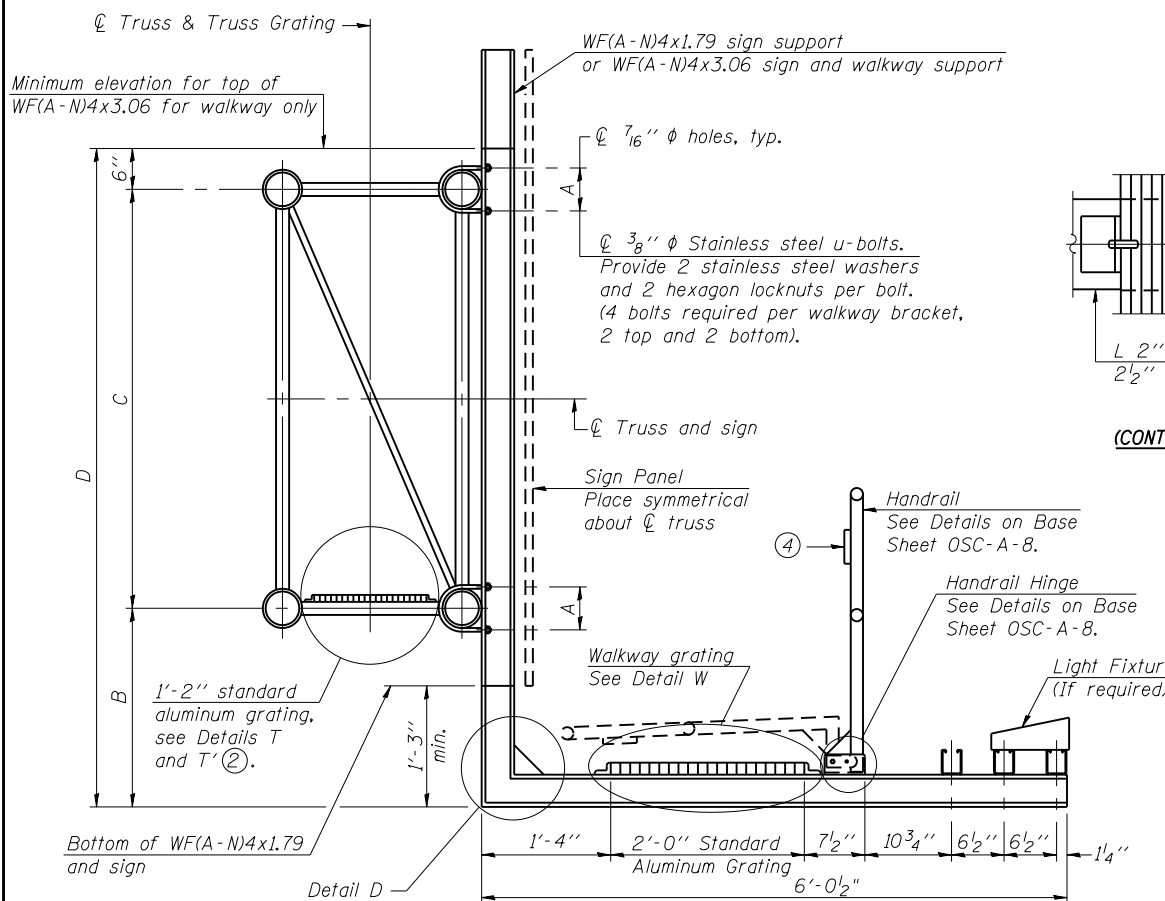
OSC-A-6 6-1-12

DESIGNED -	EXAMINED	DATE -
CHECKED -	ENGINEER OF BRIDGE DESIGN	
DRAWN -	PASSED	REVISED
CHECKED -	ENGINEER OF BRIDGES AND STRUCTURES	REVISED

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CANTILEVER SIGN STRUCTURES - ALUMINUM WALKWAY
DETAILS - ALUMINUM TRUSS & STEEL POST**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32,33) R-1	WINNEBAGO	705	557
CONTRACT NO. 64821				
ILLINOIS FED. AID PROJECT				



SPECIFICATIONS FOR STANDARD ALUMINUM GRATING
 Main Bearing Bars (MBB) shall be 3/16" x 1 1/2" on 1 3/16" centers and conform to ASTM B211 Alloy 6061-T6.
 Cross bars (CB) shall be 3/16" x 1 1/2" on 4" centers and conform to ASTM B221 Alloy 6063-T5 or 6061-T6.

OR
 Aluminum Grating with modified "t" sections for main bearing bars shall meet the following requirements:
 Main bars shall conform to ASTM B221 Alloy 6061-T6 and have a minimum section modulus equal to 0.0705 in.³ per bar, a depth of 1 1/2", spaced on 1 3/16" centers.
 Cross bars shall conform to ASTM B221 Alloy 6063-T5 or T-42 and spaced on 4" centers.

- ① Drilling holes in grating may be done in shop or field, based on Contractor's preference and subject to accurate alignment.
- ② Stainless steel shims shall be placed as shown in Detail T if needed to compensate for alignment variations between horizontal and diagonal pipes beyond adjustment provided by angles. Thicker shims may be used subject to shims performing properly.
- ③ If Handrail Joint present, weld angle to WF(A-N)4 and 1/4" extension bars. (See Base Sheet OSC-A-8.)
- ④ 1/8" x 1/2" x 2" welded to handrail posts to protect locations that contact grating.
- ⑤ Tube to grating gap may vary from 0 to 1/2", max. to align walkway, allow for camber, etc.
- ⑥ Based on actual sign height, D_s, given on OSC-A-1.

Structure Number	Station	A	⑥ B	C	⑥ D
2C101S002L008.2	434+55	8 1/2"	5'-3"	7'-0"	12'-9"

OSC-A-7 6-1-12

DESIGNED -	EXAMINED _____	DATE -
CHECKED -	ENGINEER OF BRIDGE DESIGN	
DRAWN -	PASSED _____	REVISED _____
CHECKED -	ENGINEER OF BRIDGES AND STRUCTURES	REVISED _____

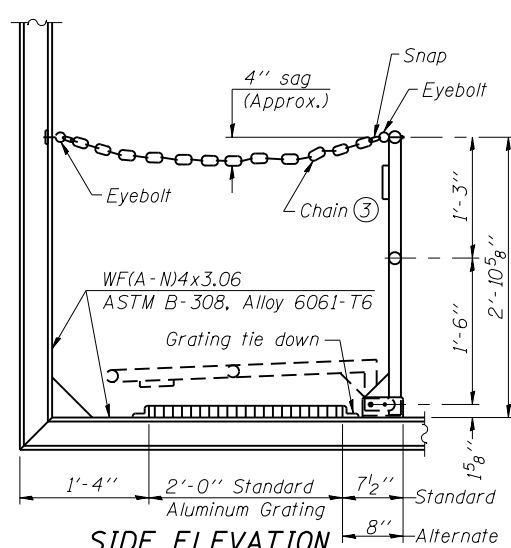
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CANTILEVER SIGN STRUCTURES - WALKWAY DETAILS
 ALUMINUM TRUSS & STEEL POST

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32,33) R-1	WINNEBAGO	705	558
CONTRACT NO. 64821				

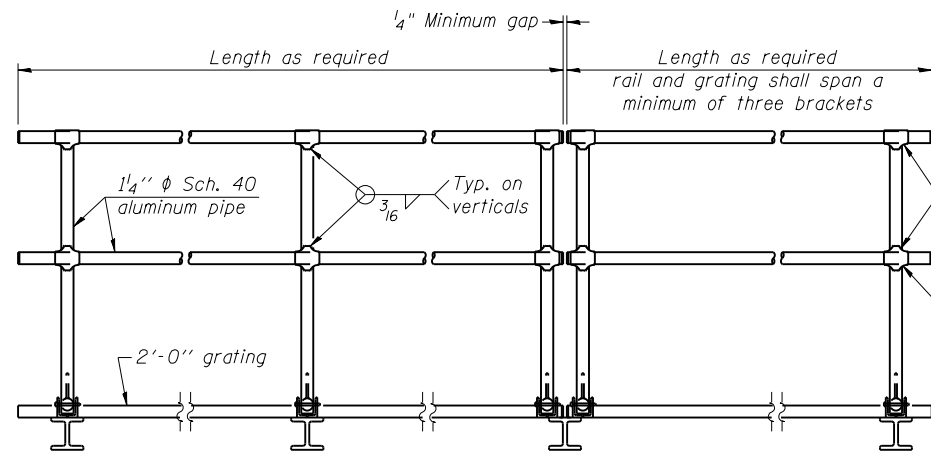
SHEET NO. 7 OF 9 SHEETS

ILLINOIS FED. AID PROJECT



SIDE ELEVATION

(Showing Safety Chain W/O Sign)

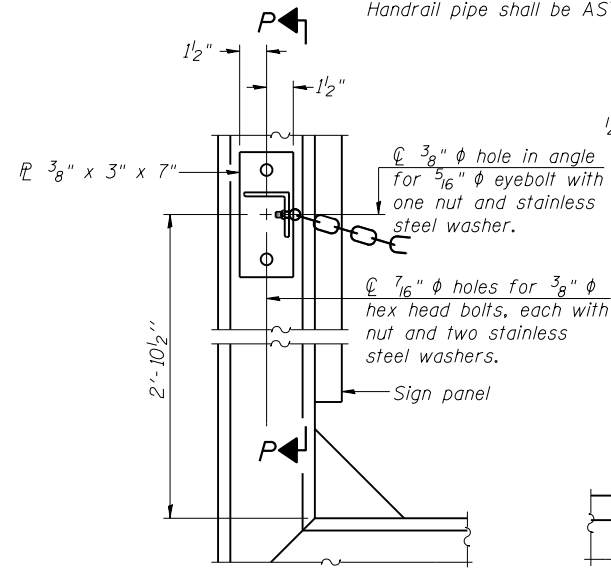


FRONT ELEVATION

HANDRAIL DETAILS

Handrail pipe shall be ASTM B241 or B429, Alloy 6063-T6 or Alloy 6061-T6.

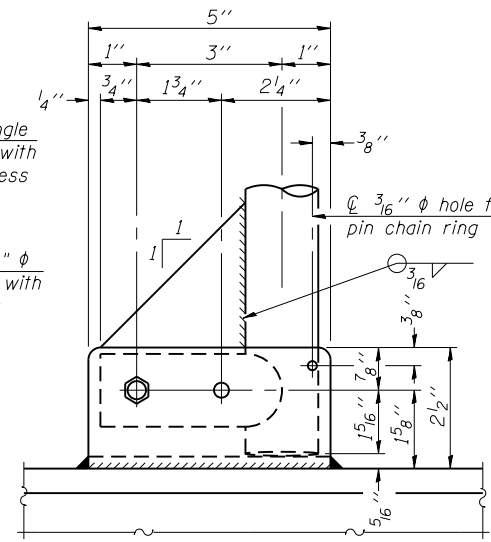
① Install standard force-fit end caps or weld 1/8" end plates with 1/8" c.f.w. and grind smooth. (All rail ends)
 ② Horizontal handrail member shall be continuous thru fitting. Provide 7/16" diameter hole in fitting for 3/8" diameter bolt. Field drill 7/16" diameter hole in horizontal rail member. Provide locknut and two stainless steel washers for bolt. (Use 5/16" eyebolts in 7/16" diameter holes on top rail at ends only.)



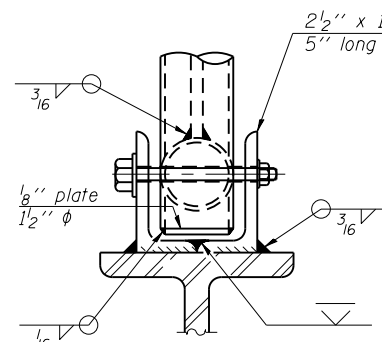
ALTERNATE SAFETY CHAIN ATTACHMENT

(With Sign Present)

Items not shown same as "Side Elevation" of "Handrail Details"



SIDE ELEVATION

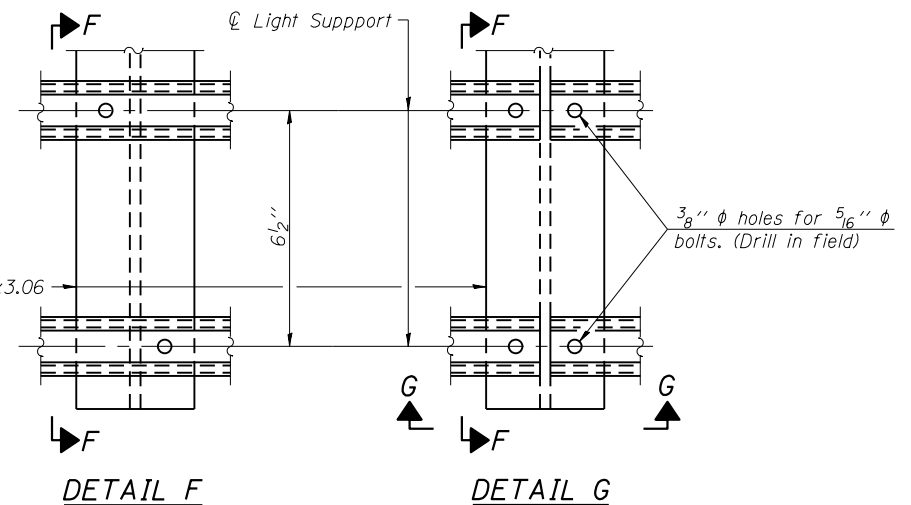


FRONT ELEVATION

Details not shown same as "ELEVATION" at right.

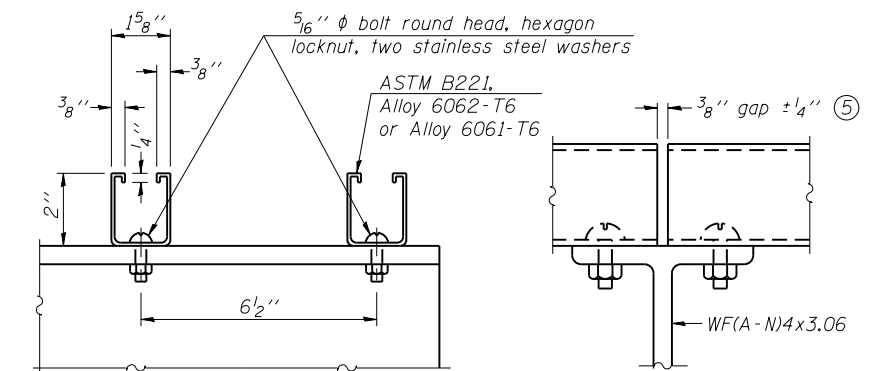
ELEVATION AT HANDRAIL JOINT

Details not shown same as "FRONT ELEVATION"



DETAIL F

DETAIL G

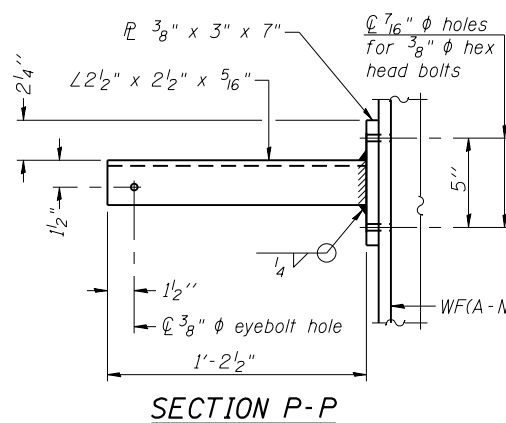


SECTION F-F

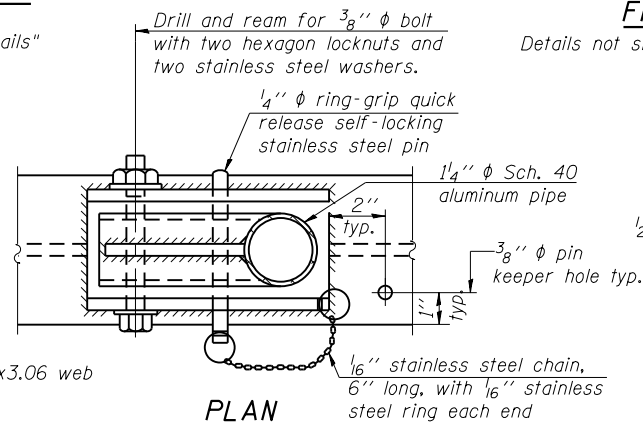
SECTION G-G

LIGHTING FIXTURE MOUNTS (IF REQUIRED)

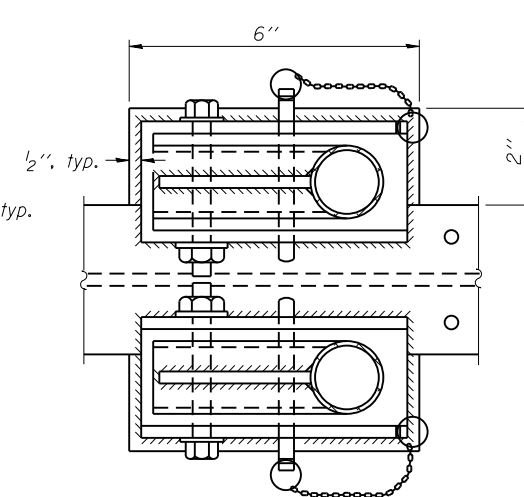
⑤ Field cut ends of light support channels shall be free of burrs or hazardous projections and coated with zinc-rich primer or equivalent.



SECTION P-P

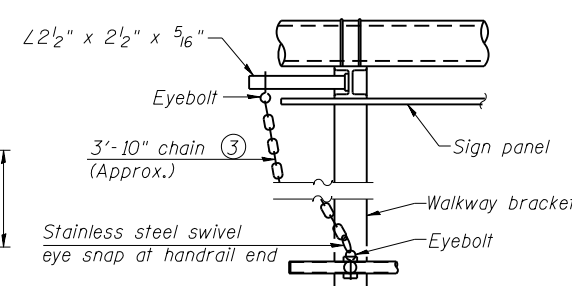


PLAN
DETAIL E HANDRAIL HINGE



PLAN AT HANDRAIL JOINT

Details not shown same as "PLAN"

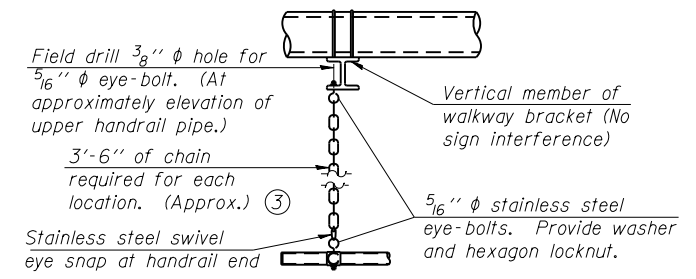


ALTERNATE SAFETY CHAIN ATTACHMENT

Details not shown similar to "Safety Chain" Details (Walkway omitted for clarity)

③ 3/16" Type 304L stainless steel chain, approximately 12 links per foot.

④ Extrusions may be used in lieu of the details shown, with approval of the Engineer.



SAFETY CHAIN

One required for each end of each walkway.

OSC-A-8

6-1-12

DESIGNED -	EXAMINED	DATE -
CHECKED -	ENGINEER OF BRIDGE DESIGN	
DRAWN -	PASSED	REVISED
CHECKED -	ENGINEER OF BRIDGES AND STRUCTURES	REVISED

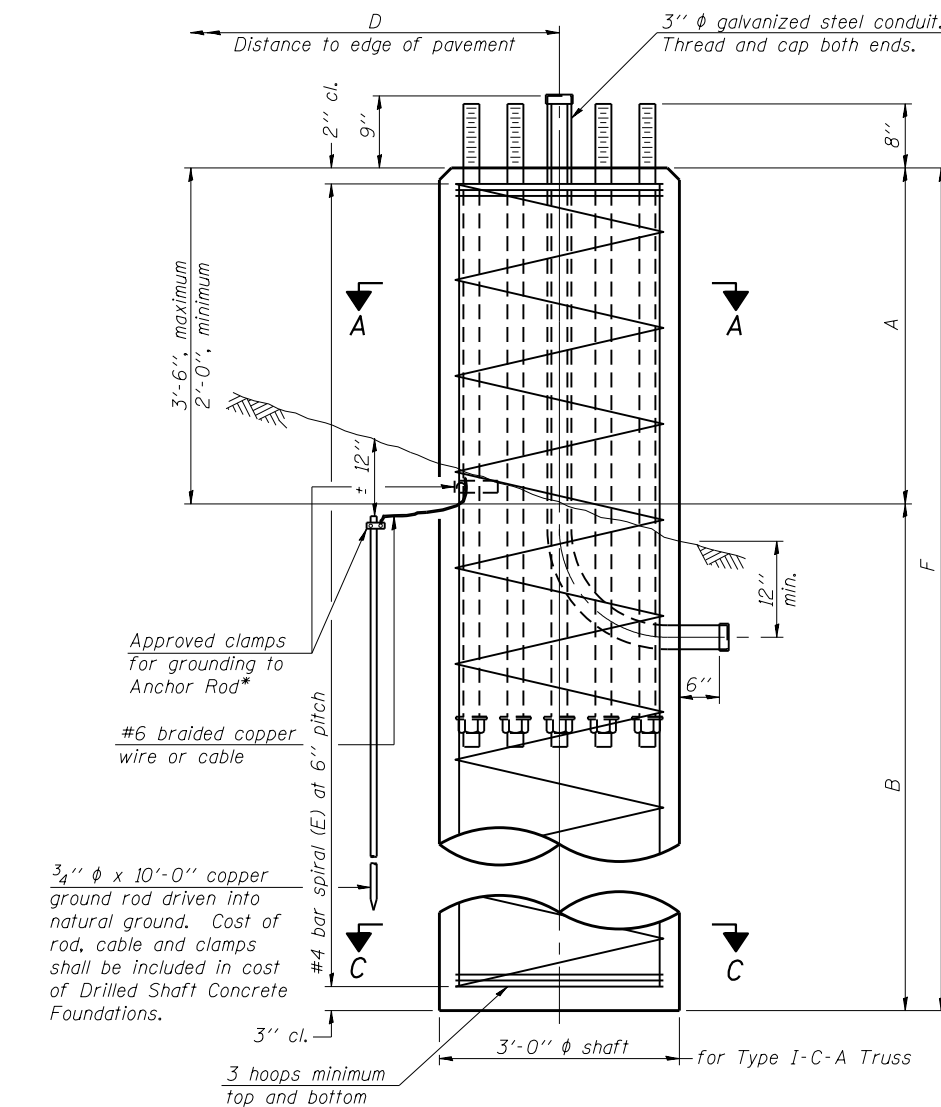
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CANTILEVER SIGN STRUCTURES - HANDRAIL DETAILS
ALUMINUM TRUSS & STEEL POST

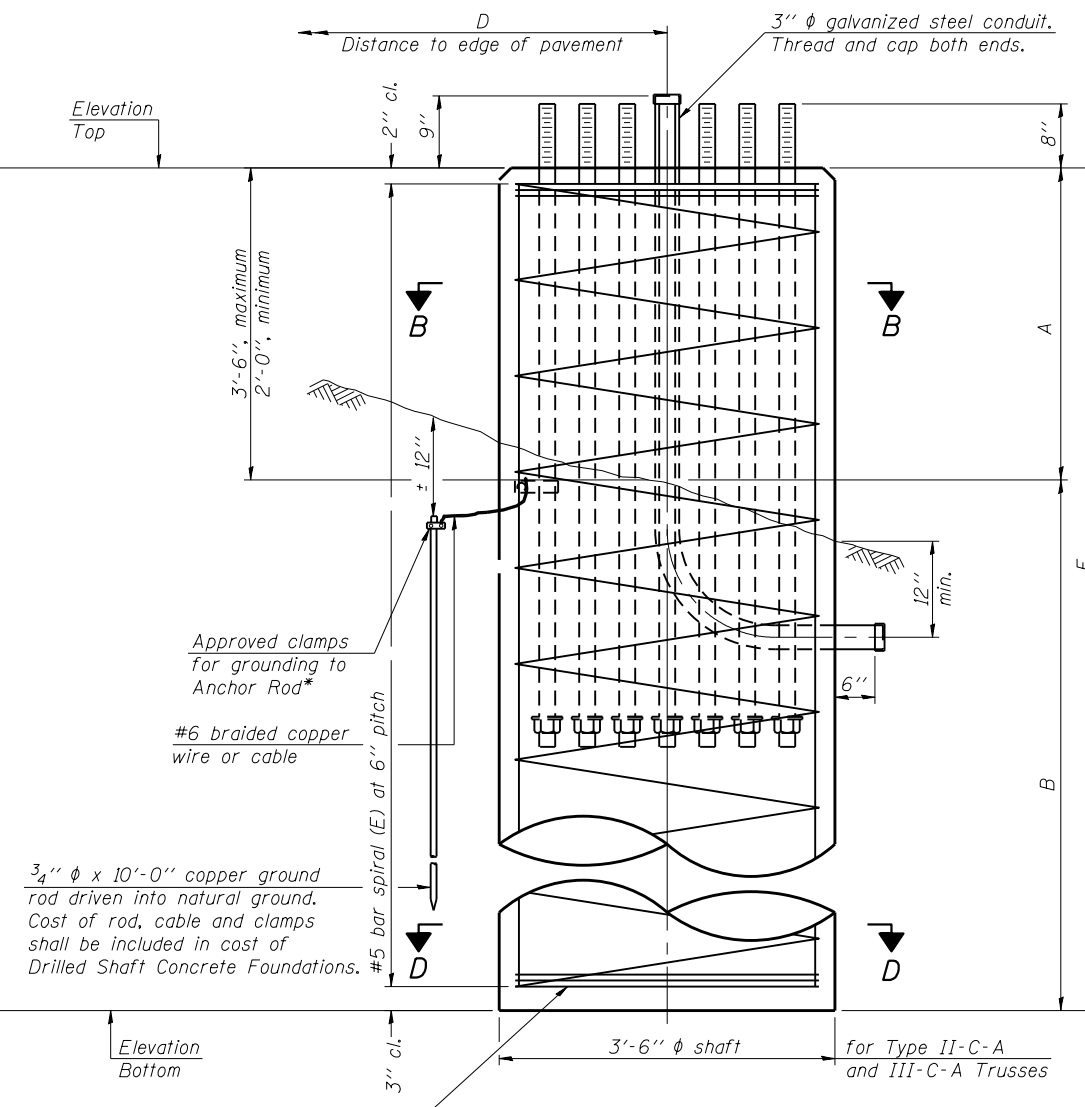
SHEET NO. 8 OF 9 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32,33) R-1	WINNEBAGO	705	559
			CONTRACT NO. 64821	
ILLINOIS FED. AID PROJECT				

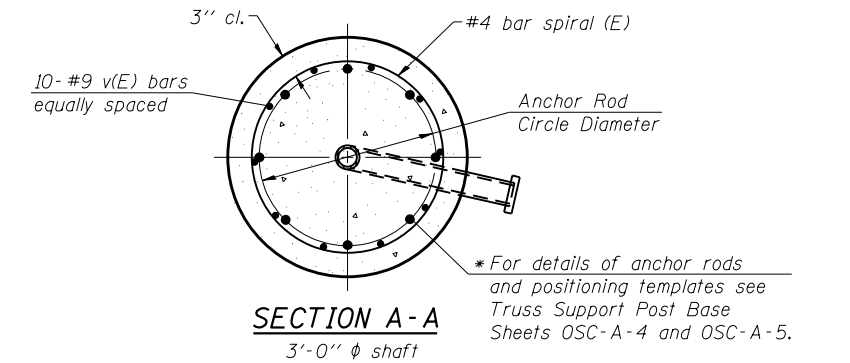
* Grind anchor rod to bright finish at ground clamp location before installing clamp.



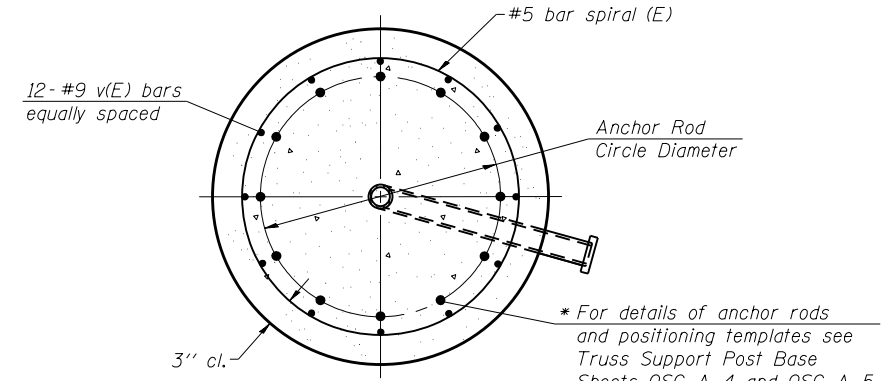
ELEVATION



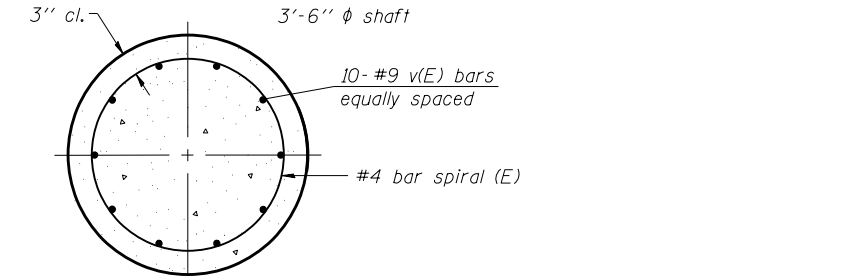
ELEVATION



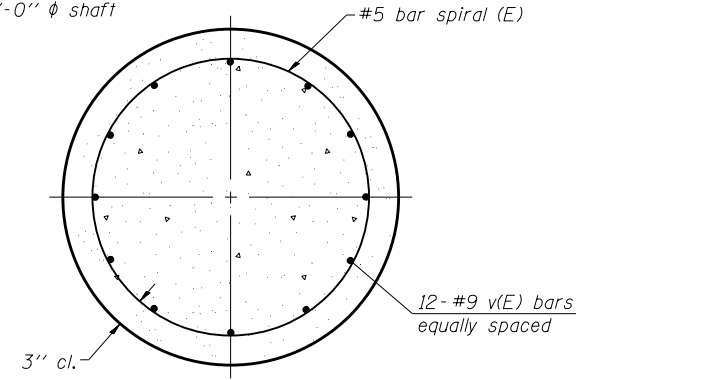
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

NOTES:
 The foundation dimensions shown in the Foundation Design Table are based on the presence of mostly cohesive soils with an average Unconfined Compressive Strength (Q_u) of at least 1.25 tsf, which must be determined by previous soil investigations at the jobsite. When other conditions are indicated, the boring data will be included in the plans and the foundation dimensions shown in the Foundation Data Table will be the result of site specific designs.
 If the conditions encountered are different than those indicated, the Contractor shall notify the Engineer to determine if the foundation dimensions need to be modified. If dimensions "B" or "F" are revised by more than 12" by the Contractor, "as-built" plans shall be prepared and submitted to the District Bureau of Operations for future reference.
 No sonotubes or decomposable forms shall be used below the lower conduit entrance. Permanent metal forms or other shielding may not be left in place below that elevation without the Engineer's written permission.
 Concrete shall be placed monolithically, without construction joints.
 Backfill shall be placed per Article 502 of Standard Specification and prior to erection of support column.
 A normal surface finish followed by a Bridge Seat Sealer application will be required on concrete surfaces above the lowest elevation 6" below finished ground line. Cost included in "Drilled Shaft Concrete Foundation".

Truss Type	Post Base Sheet	Maximum Cantilever Length (ft)	Maximum Total Sign Area (sq ft)	Shaft Diameter (in)	"B" Depth (ft)	Anchor Rods		Anchor Rod Circle Diameter (in)
						No.	Diameter (in)	
I-C-A	OSC-A-4	25	170	3.0	16.0	8	2	22
II-C-A	OSC-A-5	30	170	3.5	17.0	12	2	30
II-C-A	OSC-A-5	30	340	3.5	21.5	12	2	30
III-C-A	OSC-A-5	35	170	3.5	19.0	12	2	30
III-C-A	OSC-A-5	35	250	3.5	22.5	12	2	30
III-C-A	OSC-A-5	35	400	3.5	26.5	12	2	30
III-C-A	OSC-A-5	40	400	3.5	32.0	12	2	30

Structure Number	Station	Truss Type	Shaft Diameter	Elevation Top	Elevation Bottom	Q_u	A	B	F	Class DS Concrete Cubic Yards
2C10IS002L008.2	434+55	III-C-A	3'-6"	718.61	686.11	1.25	2'-6"	30'-0"	32'-6"	11.6

OSC-A-9 6-1-12

DESIGNED -	EXAMINED	DATE -
CHECKED -	ENGINEER OF BRIDGE DESIGN	
DRAWN -	PASSED	REVISED
CHECKED -	ENGINEER OF BRIDGES AND STRUCTURES	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CANTILEVER SIGN STRUCTURES - DRILLED SHAFT
ALUMINUM TRUSS & STEEL POST

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32,33) R-1	WINNEBAGO	705	560
CONTRACT NO. 64821				

SHEET NO. 9 OF 9 SHEETS

ILLINOIS FED. AID PROJECT

LEGEND	
I.F.	= Inside Face of Wall
O.F.	= Outside Face of Wall
(xxx.xx)	= Prop. Sidewalk Grade
	= Pipe Underdrains (See Sheet S-3)

Notes:
 Proposed Wall Radius' are provided for reference. Contractor may submit, for Engineer's review, a wall layout plan utilizing straight segmented forms. All clearances, stations and offsets show herein must be met.

See Roadway Plans for Exist. & Prop. Utility information.

INDEX OF SHEETS

- S-1 General Plan
- S-2 Wall Elevations
- S-3 Wall and Foundation Details
- S-4 Railing Details

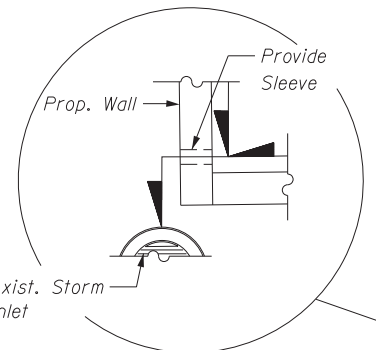
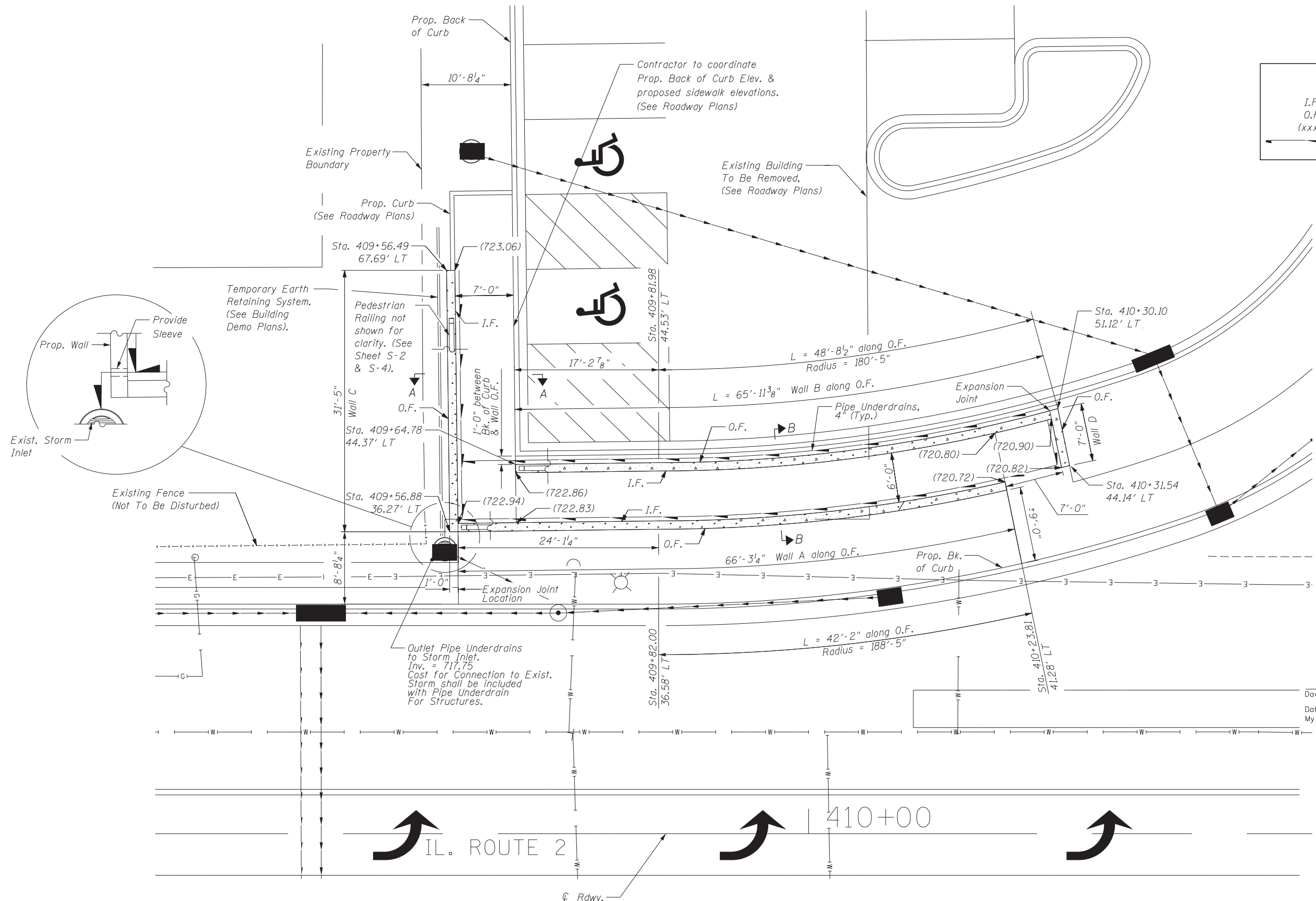
DESIGN SPECIFICATIONS

AASHTO LRFD 5th Edition
 Bridge Design Specifications

$f'c = 3,500 \text{ psi}$
 $fy = 60,000 \text{ psi (Reinforcement)}$



David F. Maxwell, S.E. Reg. No. 081-005455
 Date: September 28, 2012
 My registration expires November 30, 2012



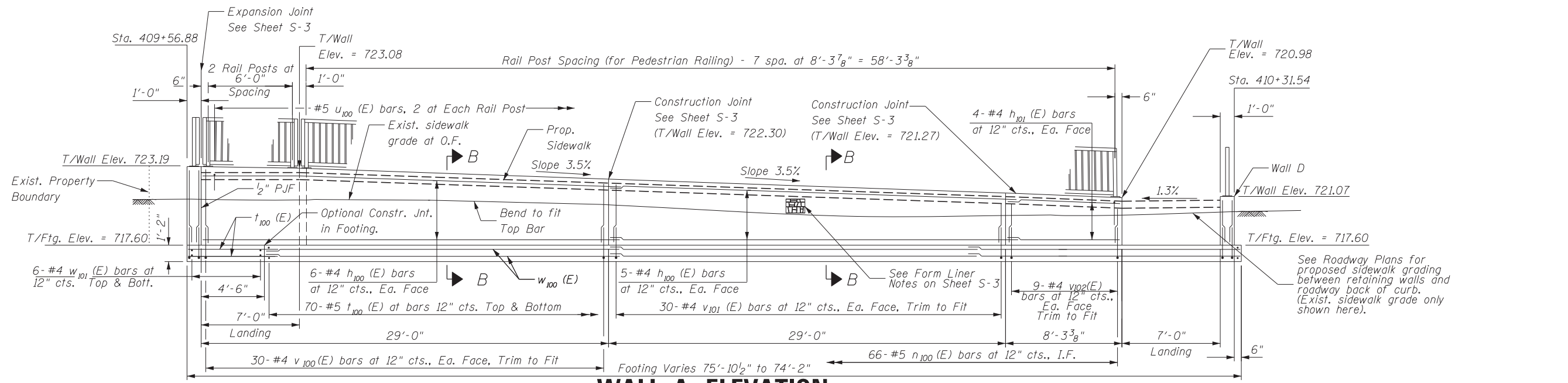
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DRAWN - JMW	REVISIONS -	
CHECKED - DFM	REVISIONS -	
DATE - 9/28/12	REVISIONS -	

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

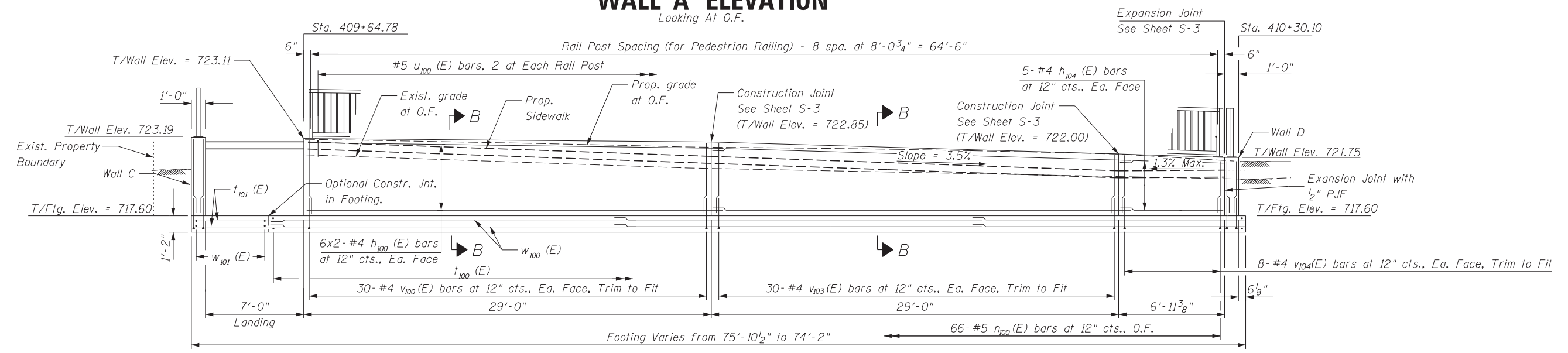
GENERAL PLAN

SHEET S-1 OF S-4 SHEETS STA. TO STA.

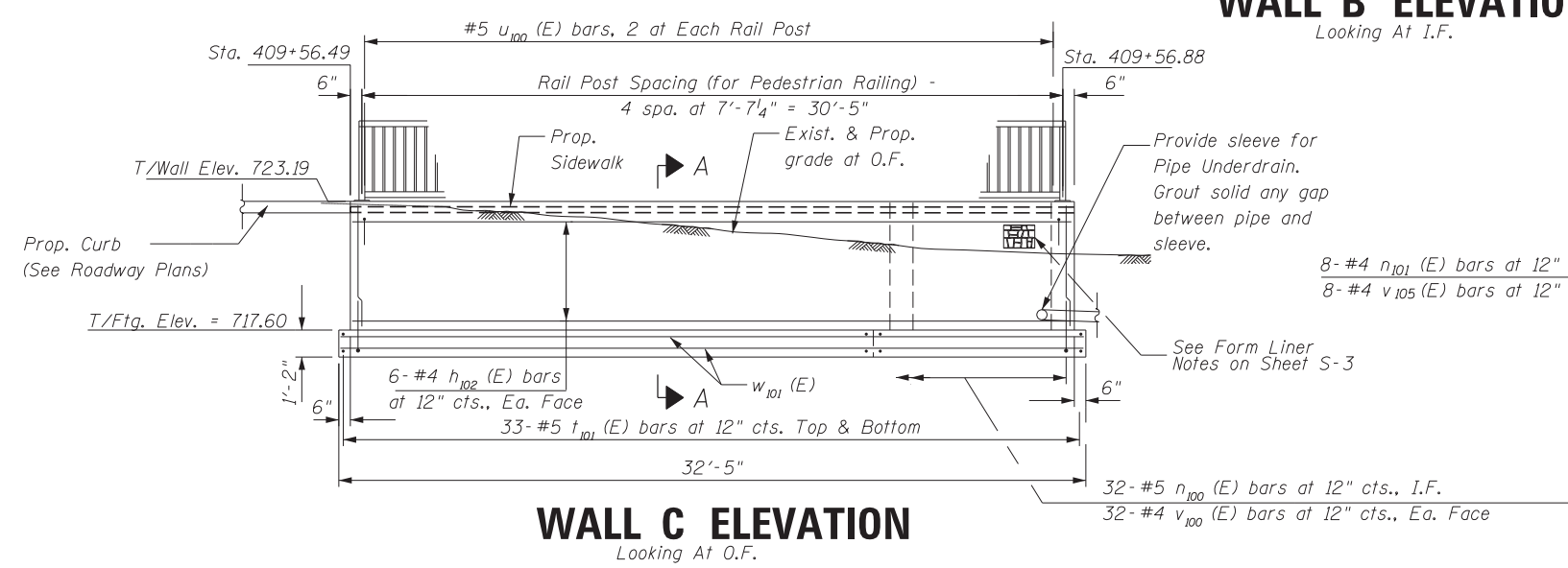
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 64821			ILLINOIS FED. AID PROJECT	



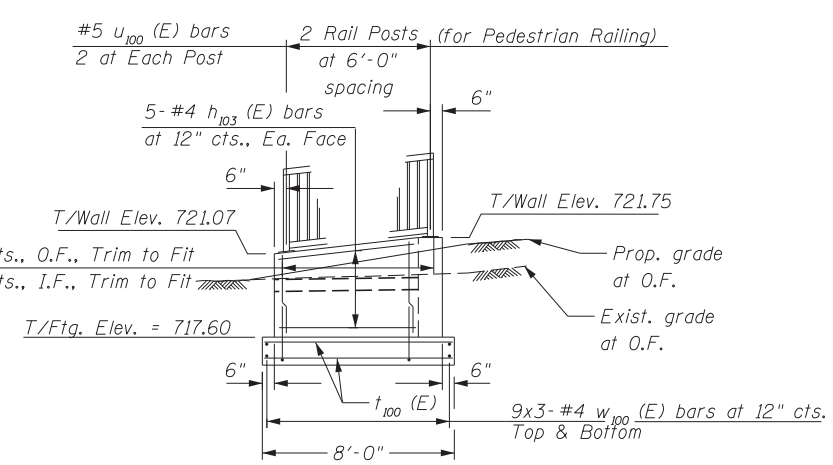
WALL A ELEVATION
Looking At O.F.



WALL B ELEVATION
Looking At O.F.



WALL C ELEVATION
Looking At O.F.



WALL D ELEVATION
Looking At O.F.
See Form Liner Notes on Sheet S-3

- Notes:
1. Bars indicated thus 20x2 - #4 etc. indicates 20 lines of bars with 2 lengths per line.
 2. Coordinate the structural details on Sheets S-1 thru S-4 with the demolition, roadway and sidewalk plans.
 3. See Roadway Plans for sidewalk pay items, details and additional grading information.
- I.F. = Inside Face
O.F. = Outside Face



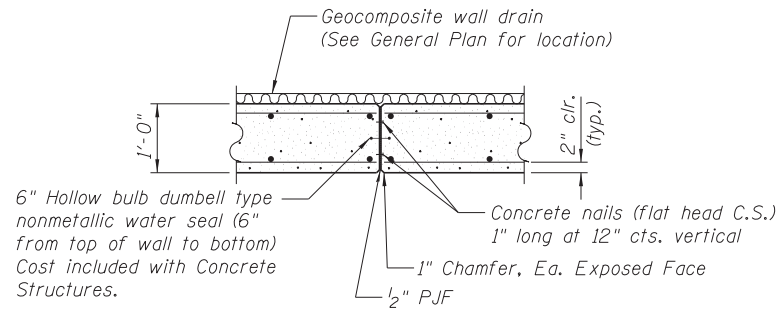
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PLOT SCALE =	DRAWN - JMW	REVISED -
PLOT DATE =	CHECKED - DFM	REVISED -
	DATE - 9/28/12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

WALL ELEVATIONS

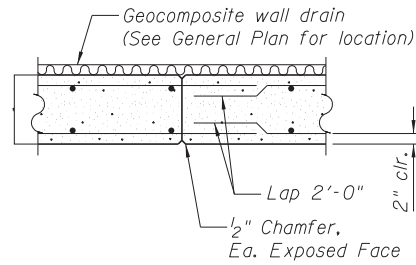
SHEET S-2 OF S-4 SHEETS STA. TO STA.

F.A. RTE. = 742	SECTION = (32,33) R-1	COUNTY = WINNEBAGO	TOTAL SHEETS = 705	SHEET NO. = 562
ILLINOIS FED. AID PROJECT			CONTRACT NO. 64821	



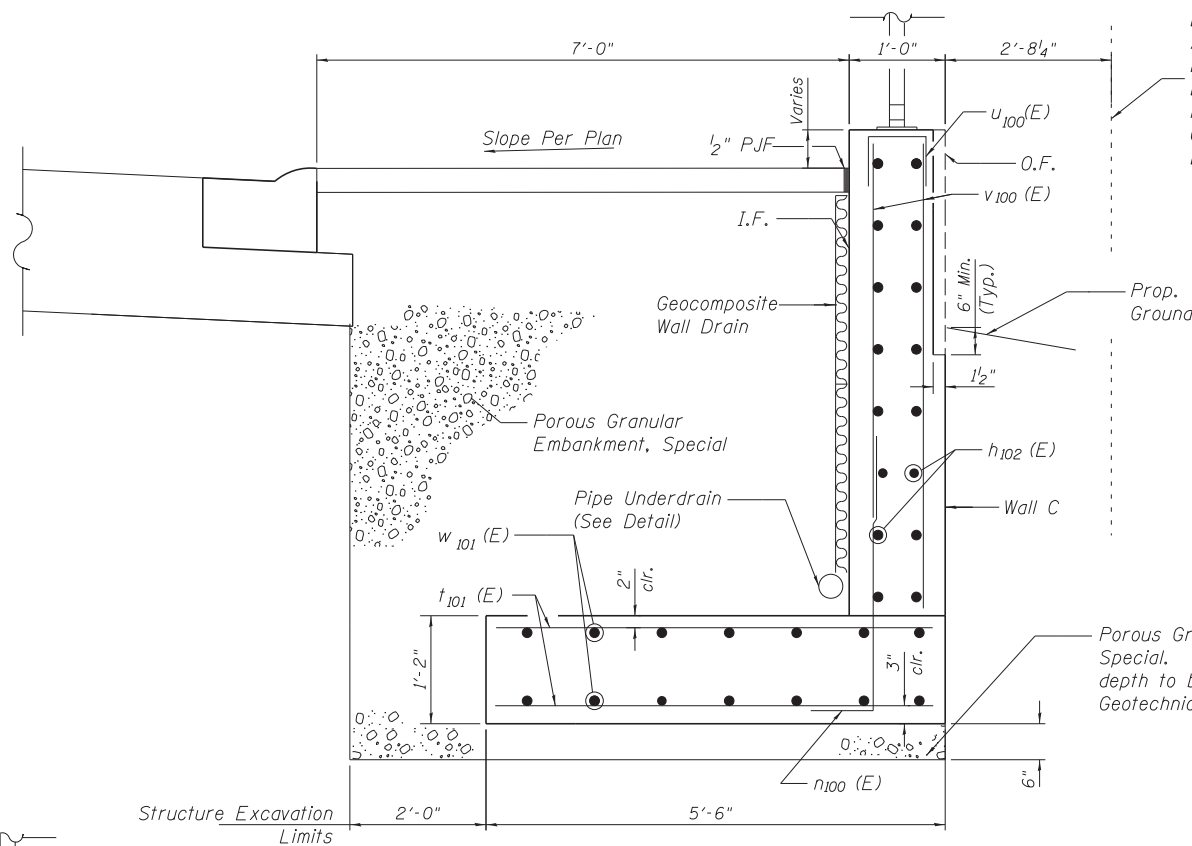
EXPANSION JOINT

Cost included with Concrete Structures



CONSTRUCTION JOINT

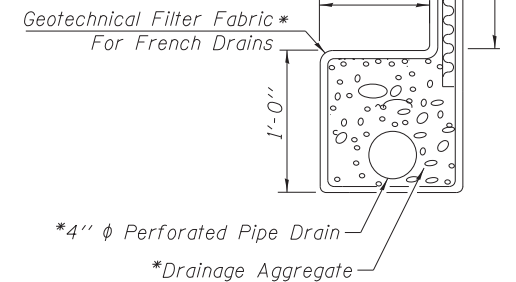
Cost included with Concrete Structures



SECTION A-A

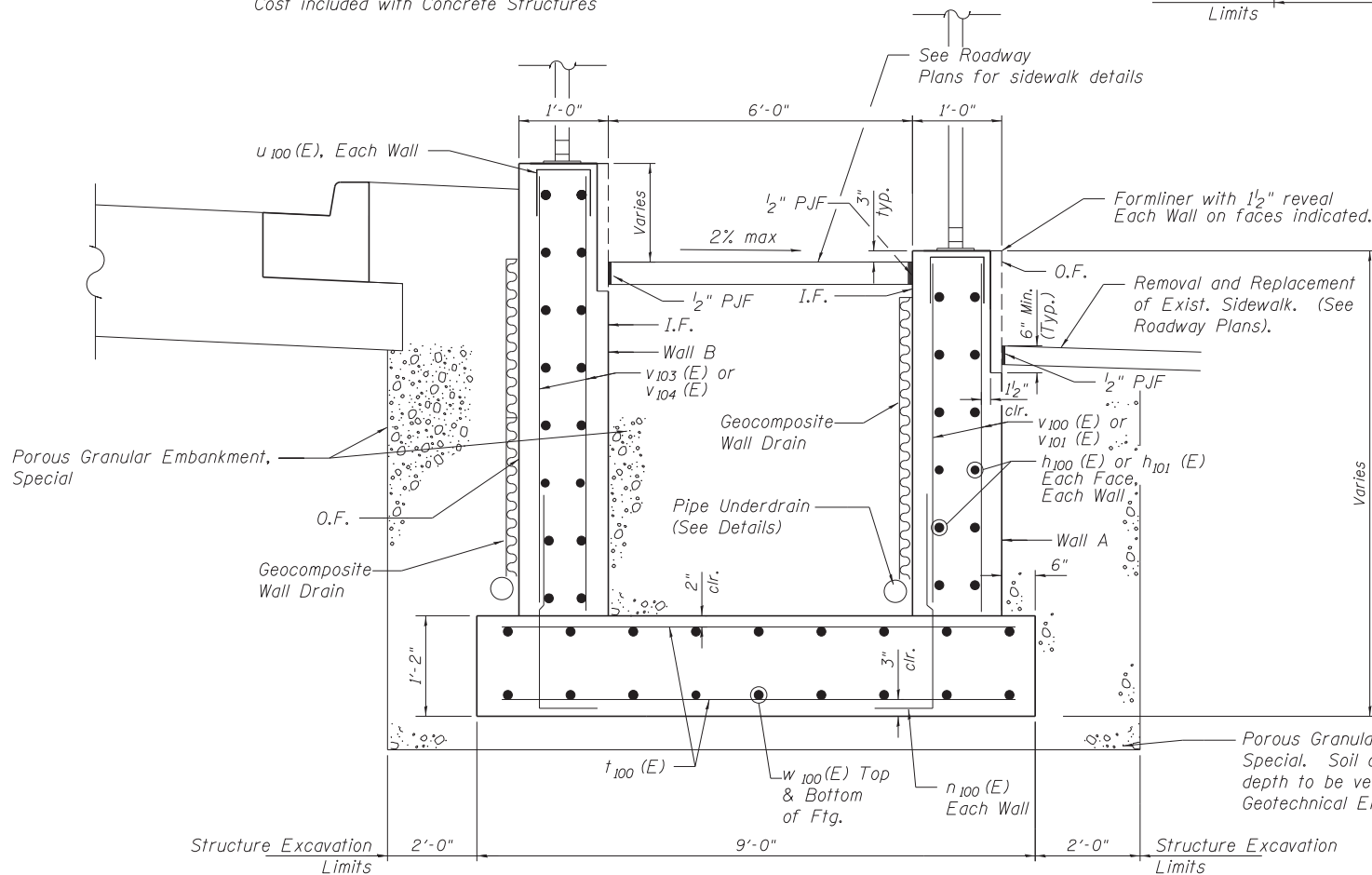
- Notes:
- Form Liner Pattern Shall Conform to Random Block Ashlar Stone Pattern Stone Sizes 6" to 42" wide Max Relief = 1 1/2"
 - Spec Formliners Pattern No. 1506, or Milestones Pattern No. 1019, or Equal.
 - Form Liner Pattern Shall Be Supplied and Constructed Per Article 503.06 of The IDOT Standard Specification For Road and Bridge Construction. Apply to All Faces of Walls Exposed Above Final Grade. Form Liner costs are included in Concrete Structures.
 - Clear Cover Shall be Measured From Relief, Not Thickest Portion of Concrete.
 - Reinforcement Bars Designated (E) Shall be Epoxy Coated.
 - Foundations are designed for an ultimate allowable soil bearing pressure of 2 ksf. Contractor shall have soil conditions confirmed by a qualified Geotechnical Engineer licensed in the State of Illinois.

Exist. Property Boundary. Adjacent Property shall not be disturbed. Temporary Earth Retention System, if required, is the responsibility of the Contractor (See Building Demolition Plans).



PIPE UNDERDRAIN DETAIL

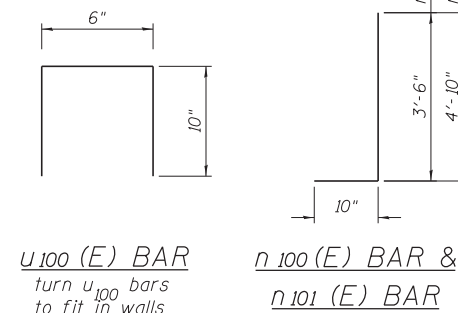
* Included in the cost of Pipe Underdrains for Structures.



SECTION B-B

MINIMUM BAR LAP

#4 bar = 2'-4"
#5 bar = 2'-7"



BILL OF MATERIALS

Bar	No.	Size	Length	Shape
h ₁₀₀ (E)	46	#4	30'-1"	—
h ₁₀₁ (E)	8	#4	8'-1"	—
h ₁₀₂ (E)	12	#4	31'-1"	—
h ₁₀₃ (E)	10	#4	6'-8"	—
h ₁₀₄ (E)	10	#4	6'-8"	—
h ₁₀₀ (E)	164	#4	4'-4"	J
h ₁₀₁ (E)	8	#4	5'-8"	J
t ₁₀₀ (E)	140	#5	8'-6"	—
t ₁₀₁ (E)	66	#5	5'-0"	—
u ₁₀₀ (E)	52	#4	2'-2"	□
v ₁₀₀ (E)	120	#4	5'-4"	—
v ₁₀₁ (E)	60	#4	4'-6"	—
v ₁₀₂ (E)	18	#4	3'-6"	—
v ₁₀₃ (E)	60	#4	5'-1"	—
v ₁₀₄ (E)	16	#4	4'-2"	—
v ₁₀₅ (E)	8	#4	3'-11"	—
w ₁₀₀ (E)	54	#4	25'-9"	—
w ₁₀₁ (E)	12	#4	5'-0"	—
CODE NO.				
50200100	Structure Excavation	Cu. Yd.	135	
50300225	Concrete Structures	Cu. Yd.	64.2	
50800205	Reinforcement Bars Epoxy Coated	Pound	5,360	
59100100	Geocomposite Wall Drain	Sq. Yd.	80	
X2070304	Porous Granular Embankment, Special	Cu. Yd.	170	
Z0046304	Pipe Underdrain For Structures, 4"	Foot	190	



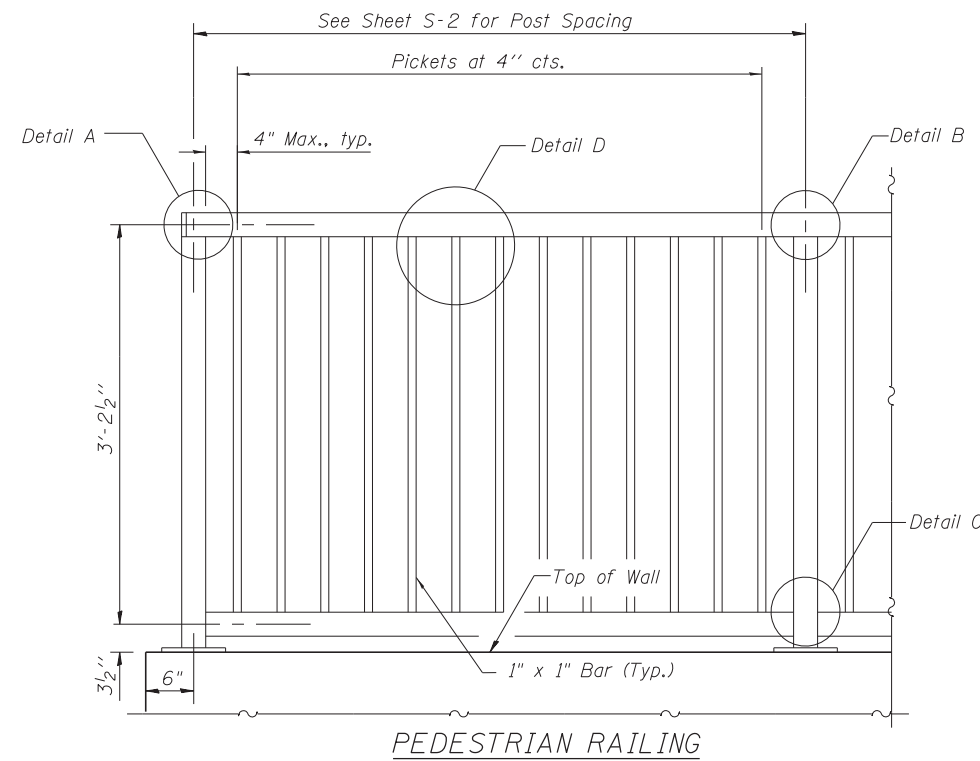
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	DRAWN - JMW	REVISED -
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PLOT DATE	DATE - 9/28/12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

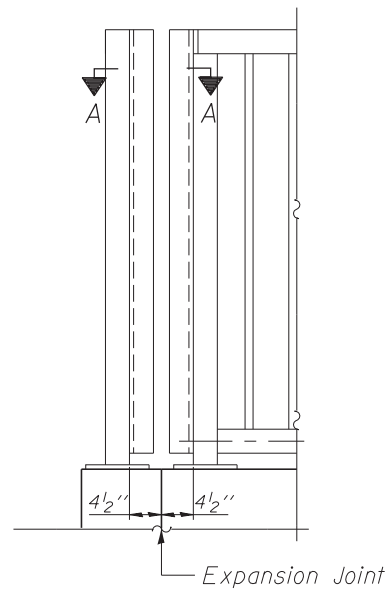
WALL AND FOUNDATION DETAILS

SHEET S-3 OF S-4 SHEETS STA. TO STA.

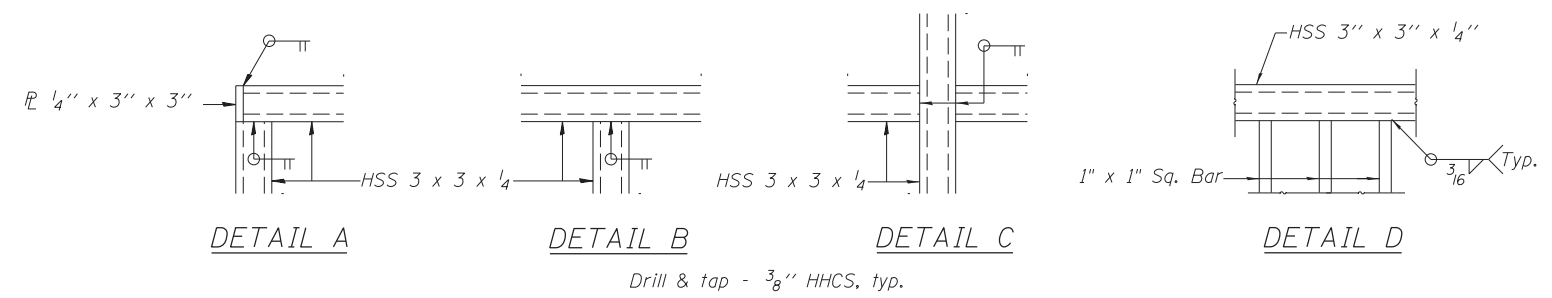
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32,33) R-1	WINNEBAGO	705	563
CONTRACT NO. 64821			ILLINOIS FED. AID PROJECT	



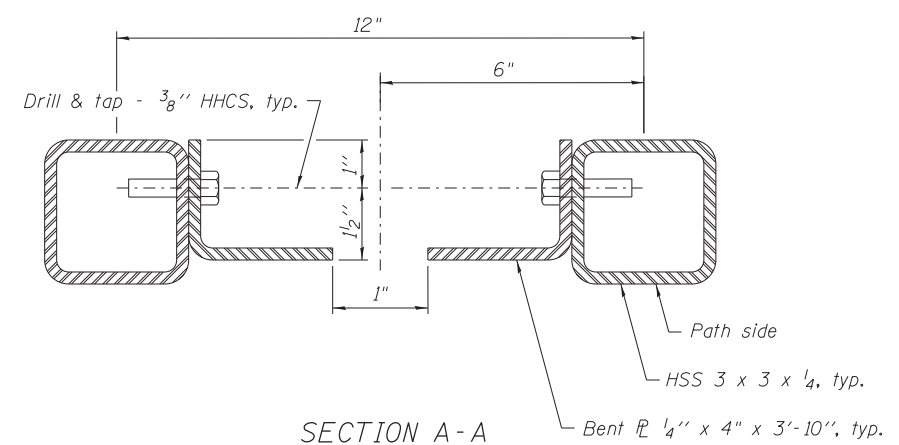
PEDESTRIAN RAILING



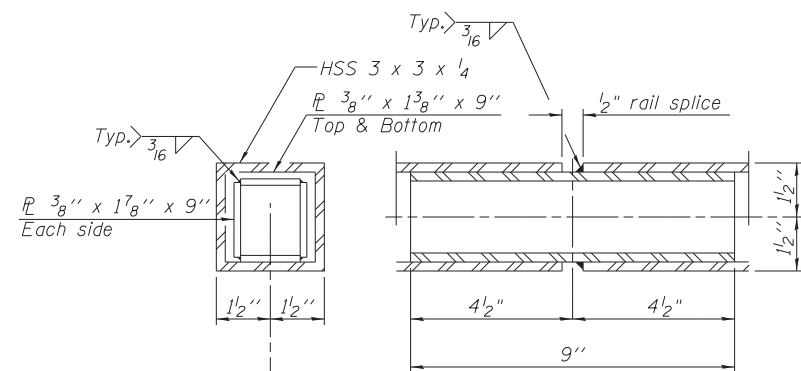
PEDESTRIAN RAILING
Southeast & Northwest Corners



NOTES
All post, railing, pickets, splices, and anchor devices shall be powder coated black. The powder coating system to be used shall be approved by the engineer prior to the coating process. The powder coating shall be paid for as Painting Steel Railing. See the Special Provision for the powder coating system to be used.

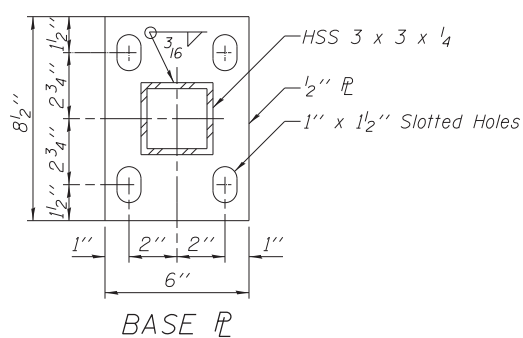


SECTION A-A

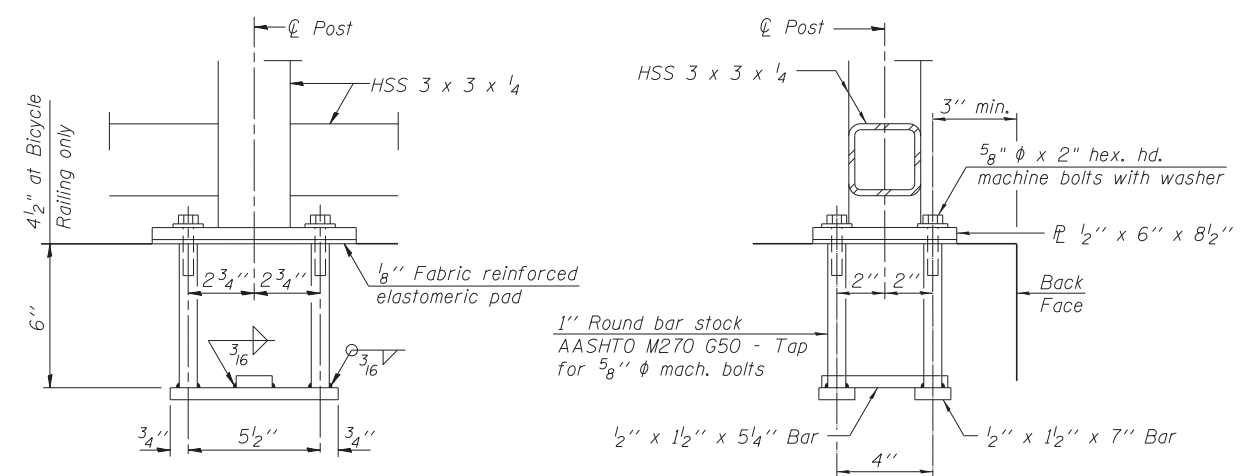


RAIL SPLICE

Rail Splice Spacing = 35'-0" Maximum.



BASE PL



ANCHOR BOLT DETAILS

In lieu of the cast-in-place anchor device shown, the Contractor has the option of drilling and setting 5/8" ϕ anchor rods according to Article 509.06 of the Standard Specifications. Embedment shall be according to the manufacturer's specifications, but no greater than 6".

BILL OF MATERIAL

Code No.	Item	Unit	Quantity
50900805	Pedestrian Railing	Foot	159.2
50600200	Painting Steel Railing	Foot	159.2



USER NAME = JMW	DESIGNED - JMW	REVISED -
PLOT SCALE =	DRAWN - JMW	REVISED -
PLOT DATE =	CHECKED - DFM	REVISED -
	DATE - 9/28/12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

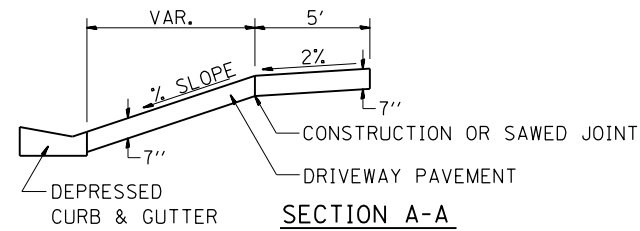
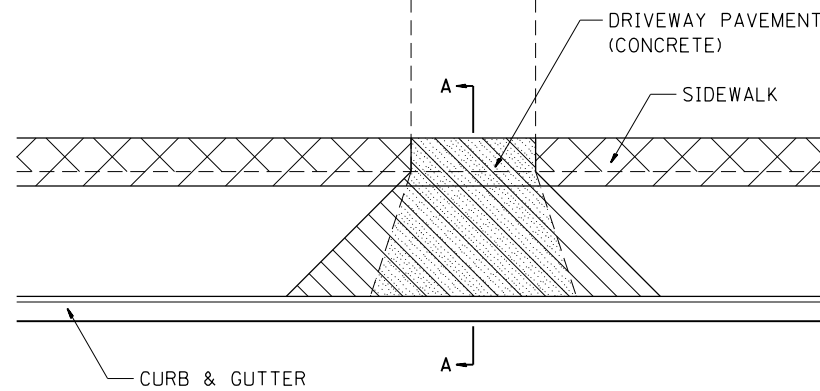
RAILING DETAILS

SHEET S-4 OF S-4 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32,33) R-1	WINNEBAGO	705	564
			CONTRACT NO. 64821	
ILLINOIS FED. AID PROJECT				

SIDEWALK AND DRIVEWAY PAVEMENT PAY AREAS

- PAY FOR AS
- SIDEWALK REMOVAL
 - DRIVEWAY PAVEMENT REMOVAL
 - PCC SIDEWALK 5
 - PCC DRIVEWAY PAVEMENT 7



FOR DETAILS ON DIMENSIONS AND GRADES, SEE DISTRICT STANDARD 25.1 OR PLANS.

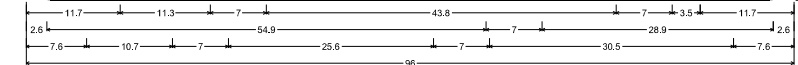
ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

REVISED - 10-3-11

SIDEWALK AND DRIVEWAY PAVEMENT PAY AREAS 35.4

ROAD CLOSED TO OVERSIZED LOADS

**NO OVERSIZE -
OVERWEIGHT LOADS
XX MILES AHEAD**



Permit Loads - Loads Over 13 Feet; 3.0' Radius, 1.3' Border, Black on Orange;
 (NO OVERSIZE -) D; (OVERWEIGHT LOADS) D 85% spacing; (XX MILES AHEAD) D;
 Table of letter and object heights:

N	O	V	E	R	S	I	Z	E	*					
11.7	18.1	30.0	38.2	42.8	48.4	54.4	60.7	63.5	69.5					
D	V	E	R	W	E	I	G	H	T	L	O	A	D	S
2.6	8.6	15.0	20.4	26.2	33.4	38.9	41.3	47.4	53.2	64.3	69.9	75.9	82.9	88.7
X	H	I	L	E	S	A	H	E	A	D				
7.8	13.8	25.3	32.3	35.1	40.8	46.2	57.9	65.1	71.4	78.8	83.7			

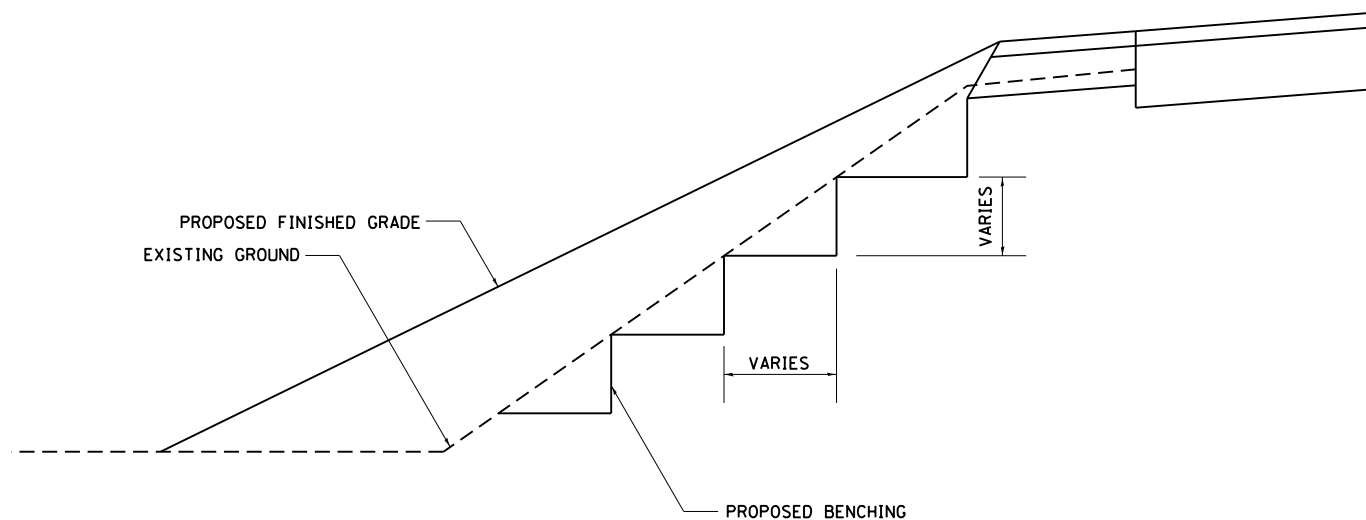
All work to furnish and install these signs shall be included in the cost of the Traffic Control Standards and shall not be paid for separately.

ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

REVISED - 3-11-09

ROAD CLOSED TO OVERSIZED LOADS 40.4

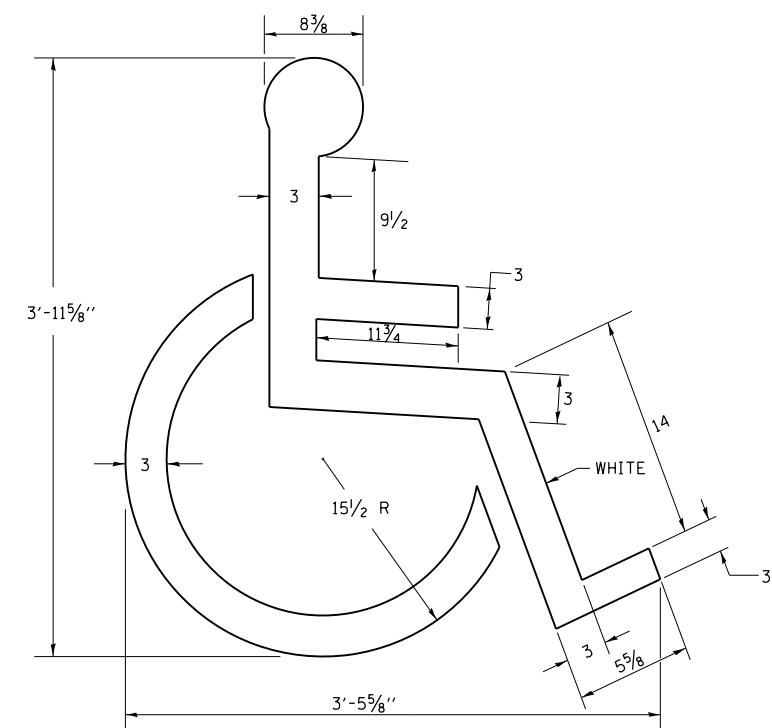
TYPICAL BENCHING ON EXISTING EMBANKMENT



REVISED - 2-22-06

TYPICAL BENCHING ON EXISTING EMBANKMENT 50.4

DISABLED SYMBOL



QUANTITY
2.9 ft²

ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

REVISED - 10-3-11	REGION 2 / DISTRICT 2 STANDARD		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
REVISED -	SCALE: *SCALE*	SHEET NO. 1 OF 19 SHEETS	742	(32, 33) R-1	WINNEBAGO	705	565
REVISED -	STA.	TO STA.	CONTRACT NO. 64821				
REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

DISABLED SYMBOL 69.4

DETAIL OF CONCRETE STEPS

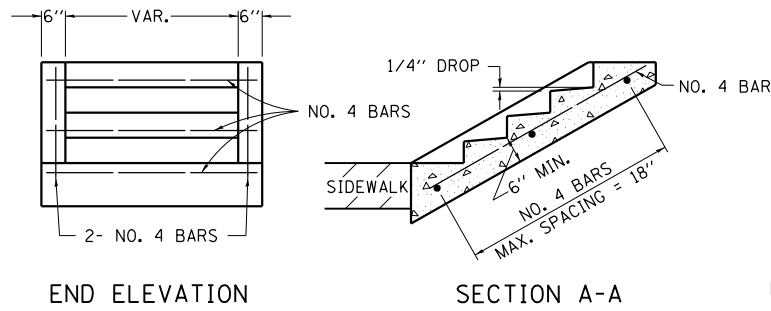


TABLE OF TREADS & RISERS

SLOPE	TREAD	RISER
1:2	12"	6"
1:3	15"	5"
1:4	17"	4 1/4"

WHERE SLOPES FALL BETWEEN THOSE SHOWN IN THE TABLE ABOVE, THE STAIR RAIL SHOULD FIT THE SLOPE AND THE TREAD IN INCHES x THE RISER IN INCHES SHOULD BE BETWEEN 72 AND 78.

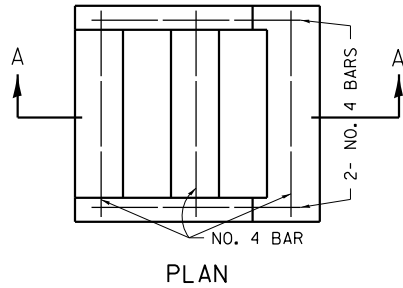
EXAMPLE:

FOR A 1:4 SLOPE USE $y = \text{RISER HEIGHT}$ $4y^2 = 75''$.
 SOLVING $y^2 = \frac{75''}{4}$, $y = 4.3''$ (USE 4 1/4" FOR CONVENIENCE.)
 TREAD WOULD THEN BE $4 \frac{1}{4}'' \times 4 = 17''$

COST OF REINFORCEMENT BARS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER LBS FOR REINFORCEMENT BARS.

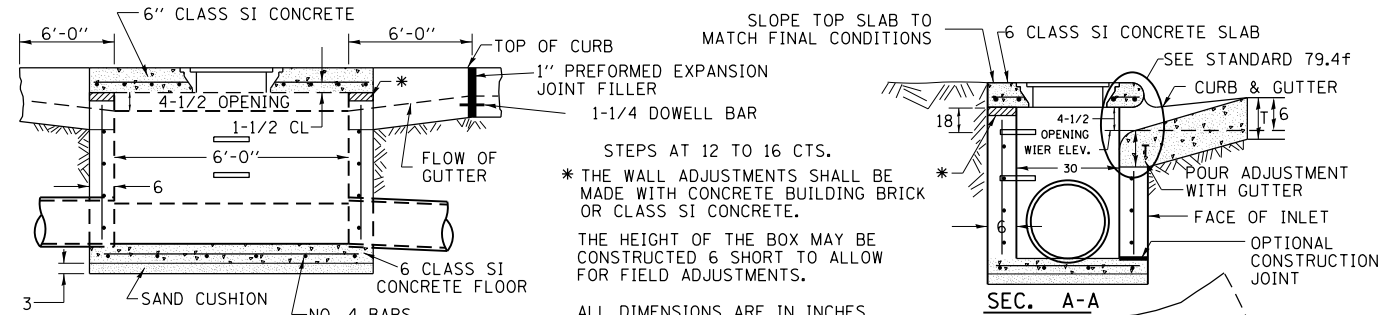
CLASS SI CONCRETE SHALL BE USED THROUGHOUT, WHICH SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC YARD FOR CLASS SI CONCRETE (MISCELLANEOUS).

ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.



REVISED - 10-3-11

INLET SPECIAL NO. 5



STEPS AT 12 TO 16 CTS.
 * THE WALL ADJUSTMENTS SHALL BE MADE WITH CONCRETE BUILDING BRICK OR CLASS SI CONCRETE.
 THE HEIGHT OF THE BOX MAY BE CONSTRUCTED 6 SHORT TO ALLOW FOR FIELD ADJUSTMENTS.

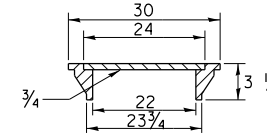
ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

NOTES

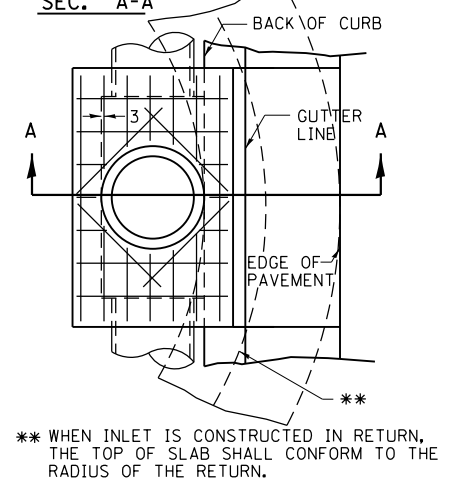
SEE STANDARD 602701 FOR DETAILS OF STEPS.
 1" PREFORMED EXPANSION JOINTS AS SHOWN SHALL BE PROVIDED ON EACH SIDE OF INLET.
 CLASS SI CONCRETE OR PRECAST CONCRETE SHALL BE USED THROUGHOUT.
 THE SIDE WALLS MAY BE BUILT AS PRECAST SEGMENTAL SECTIONS.
 REINFORCEMENT FOR INLET SPECIAL NO. 5 SHALL BE ACCORDING TO DISTRICT STANDARD 79.4g

STEPS SHALL BE OMITTED WHEN DEPTH OF INLET IS LESS THAN 5 FOOT
 THE INLET SHALL BE CAST IN PLACE OR PRECAST.
 EXCEPT AS NOTED HEREON INLET SPECIAL NO. 5 SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 602 OF THE STANDARD SPECIFICATIONS.
 THE CONTRACT UNIT PRICE EACH FOR INLET SPECIAL NO. 5 SHALL INCLUDE THE COST OF FURNISHING AND INSTALLING THE FRAME, LID, REINFORCEMENT BARS, FLOOR AND TOP SLABS, CAST IRON STEPS (IF USED).
 THE CURB AND GUTTER WILL BE PAID FOR SEPARATELY AND WILL BE MEASURED THROUGH THE INLET.
 THE CURB AND GUTTER ADJACENT TO AND 6 FOOT ON EITHER SIDE OF THE INLET SHALL BE CONSTRUCTED AS SHOWN WITH NO ADDITIONAL COMPENSATION FOR THE TRANSITION.
 ALL PIPE UNDERDRAIN CONNECTIONS WHEN SPECIFIED SHALL BE DONE IN ACCORDANCE WITH ART. 601 OF THE STANDARD SPECIFICATIONS. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER (FOOT) FOR PIPE UNDERDRAINS (SPECIAL) OF THE DIAMETER SPECIFIED WHICH PRICE SHALL INCLUDE THE CA7 OR CA16 AND THE CONNECTION TO THE INLET.

LIGHT WEIGHT MANHOLE CASTING



TOTAL WEIGHT 160 LBS.
 REVISED - 10-4-11

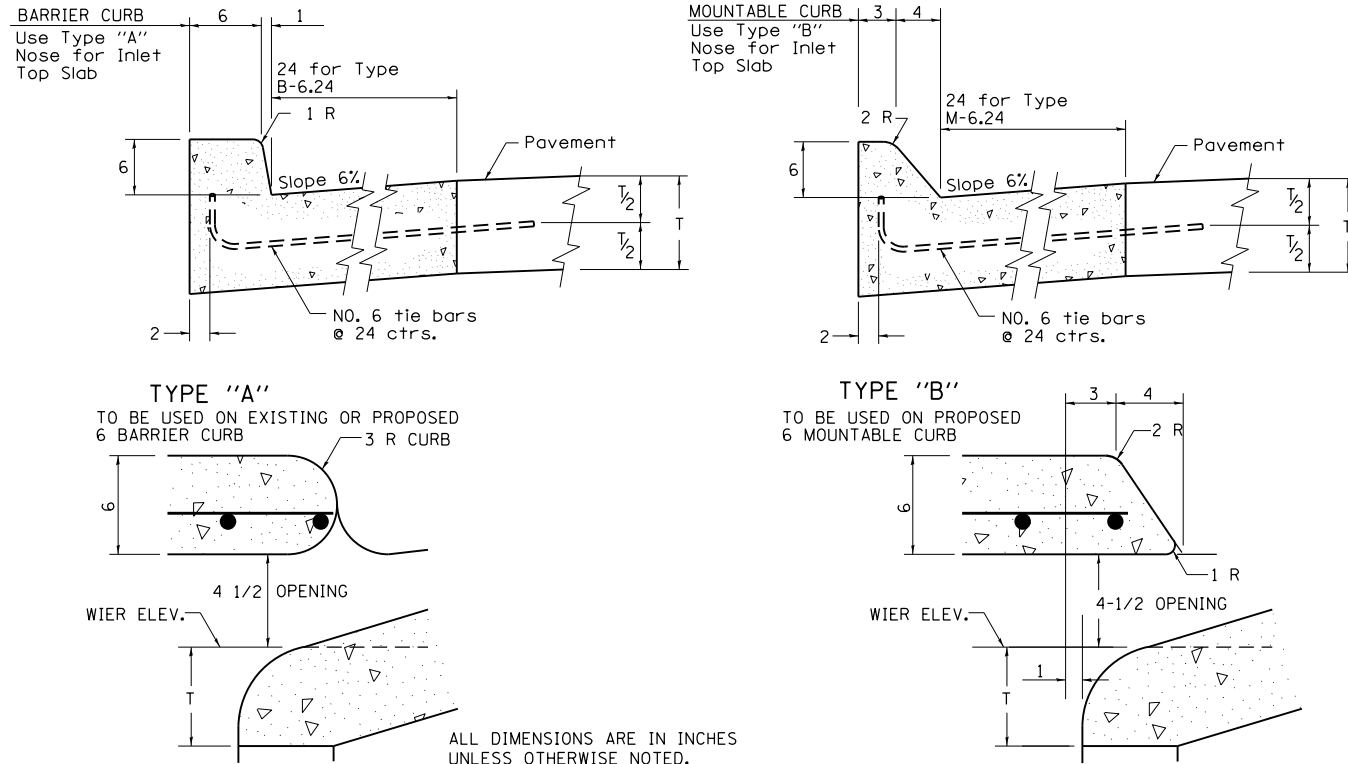


** WHEN INLET IS CONSTRUCTED IN RETURN, THE TOP OF SLAB SHALL CONFORM TO THE RADIUS OF THE RETURN.

DETAIL OF CONCRETE STEPS 71.4

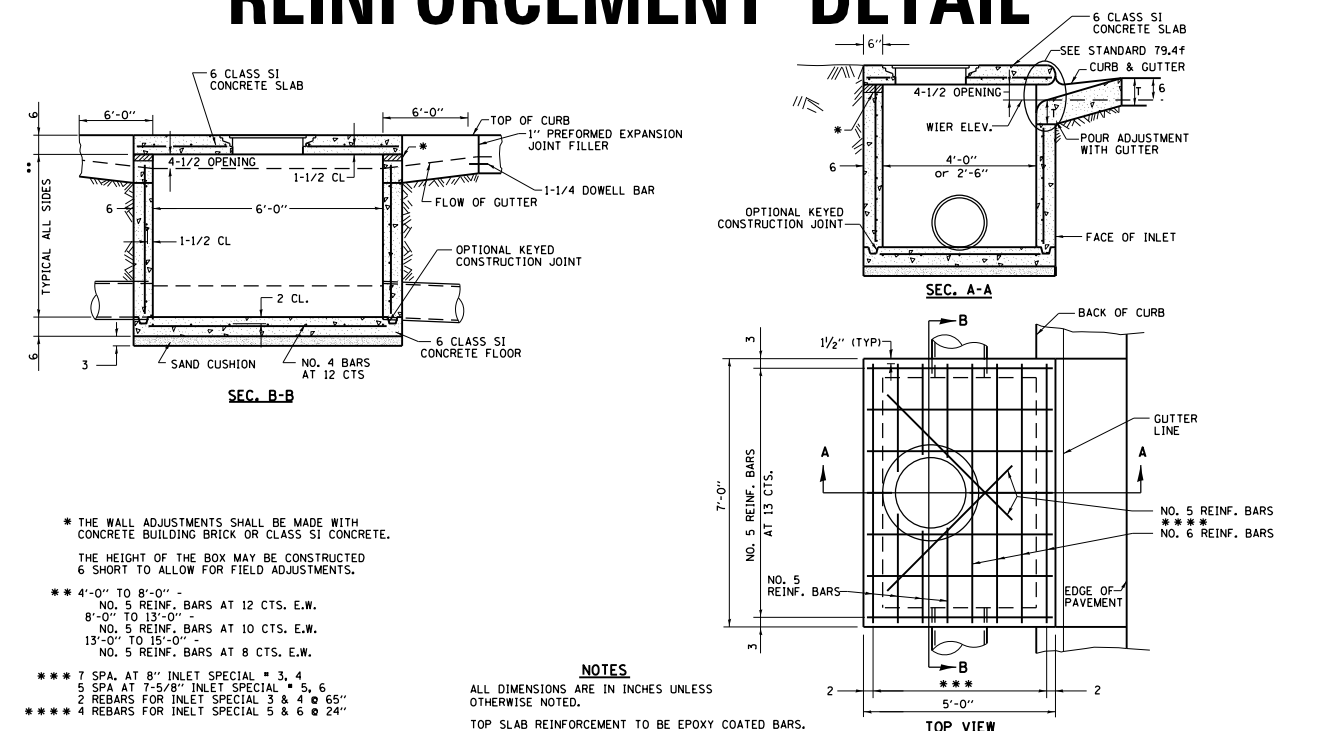
INLET SPECIAL NO. 5 79.4b

NOSE TYPE FOR INLET TOP SLAB



REVISED - 10-5-11

INLET SPECIAL NO. 3, 4, 5, 6 REINFORCEMENT DETAIL



* THE WALL ADJUSTMENTS SHALL BE MADE WITH CONCRETE BUILDING BRICK OR CLASS SI CONCRETE.
 THE HEIGHT OF THE BOX MAY BE CONSTRUCTED 6 SHORT TO ALLOW FOR FIELD ADJUSTMENTS.
 ** 4'-0" TO 8'-0" - NO. 5 REINF. BARS AT 12 CTS. E.W.
 8'-0" TO 13'-0" - NO. 5 REINF. BARS AT 10 CTS. E.W.
 13'-0" TO 15'-0" - NO. 5 REINF. BARS AT 8 CTS. E.W.
 *** 7 SPA. AT 8" INLET SPECIAL * 3, 4
 5 SPA AT 7-5/8" INLET SPECIAL * 5, 6
 2 REBARS FOR INLET SPECIAL 3 & 4 @ 65"
 **** 4 REBARS FOR INLET SPECIAL 5 & 6 @ 24"

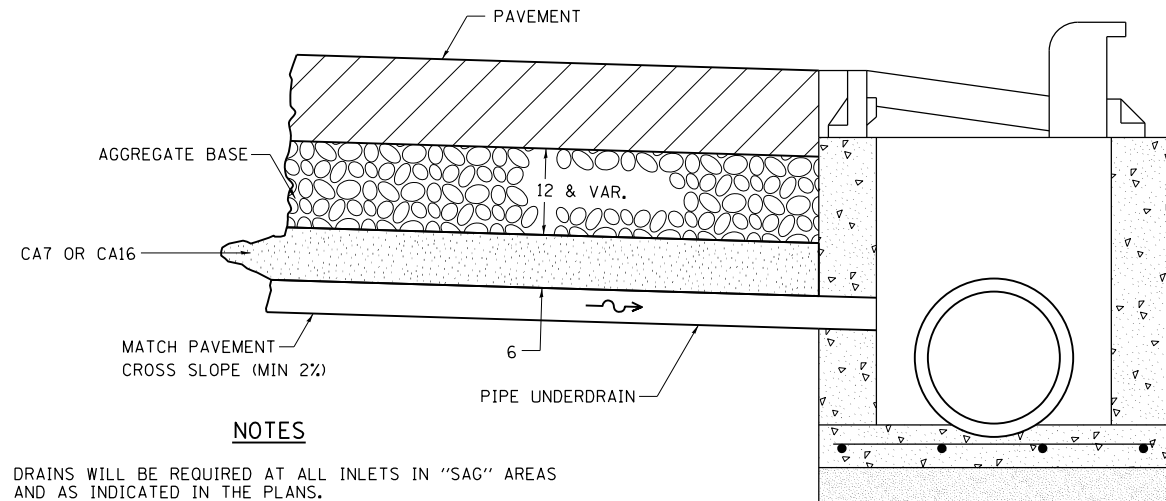
ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.
 TOP SLAB REINFORCEMENT TO BE EPOXY COATED BARS.

NOSE TYPE FOR INLET TOP SLAB 79.4f

INLET SPECIAL NO. 3, 4, 5, 6 REINFORCEMENT DETAIL 79.4g

REVISED - 10-5-11	REGION 2 / DISTRICT 2 STANDARD		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
REVISED -	SCALE: *SCALE*	SHEET NO. 2 OF 19 SHEETS	742	(32, 33) R-1	WINNEBAGO	705	566
REVISED -		STA. TO STA.	CONTRACT NO. 64821				
REVISED -			FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

DRAIN FOR AGGREGATE BASES IN URBAN AREAS



NOTES

DRAINS WILL BE REQUIRED AT ALL INLETS IN "SAG" AREAS AND AS INDICATED IN THE PLANS.

THIS WORK SHALL BE COMPLETED ACCORDING TO SECTION 601 OF THE STANDARD SPECIFICATIONS.

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR PIPE UNDERDRAINS OF THE DIAMETER SPECIFIED WHICH PRICE SHALL INCLUDE THE CA7 OR CA16 AND THE CONNECTION TO THE INLET.

ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

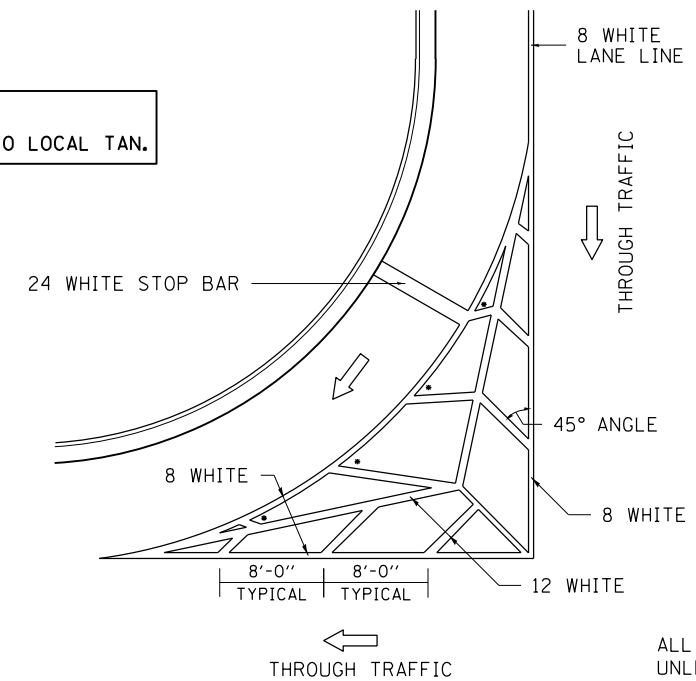
REVISED - 10-5-11

DRAIN FOR AGGREGATE BASES IN URBAN AREAS

88.4

TYPICAL MARKING FOR PAINTED ISLANDS

NOTE:
* 45° TO LOCAL TAN.



ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

REVISED - 10-05-11

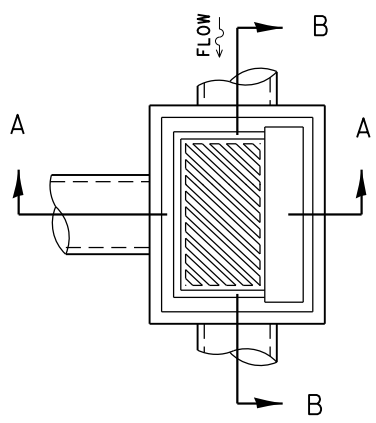
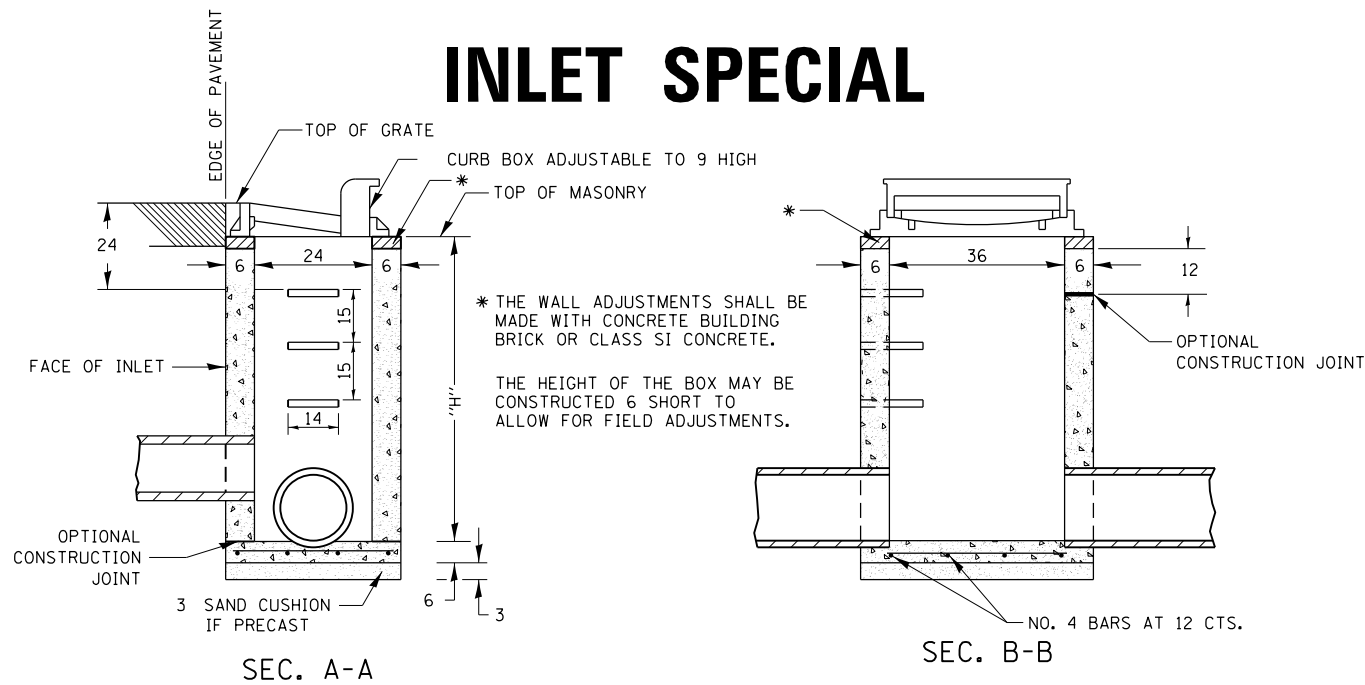
TYPICAL MARKING FOR PAINTED ISLANDS

93.4

REVISED -	REGION 2 / DISTRICT 2 STANDARD	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
REVISED -		742	(32, 33) R-1	WINNEBAGO	705	567
REVISED -		CONTRACT NO. 64821				
REVISED -		SCALE: *SCALE*	SHEET NO. 3 OF 19 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.

PLOT DATE = #DATE#

INLET SPECIAL

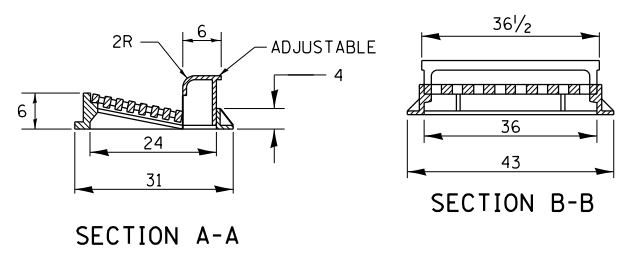
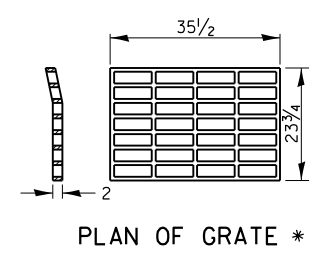
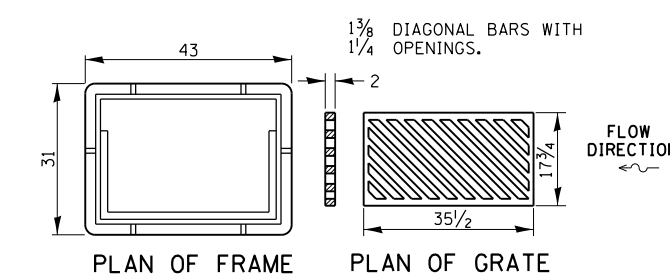


DETAIL OF FRAME & GRATE

NOTES

CLASS SI CONCRETE OR PRECAST CONCRETE SHALL BE USED THROUGHOUT. PRECAST CONCRETE SHALL BE IN ACCORDANCE WITH SECTION 504.01 THRU 504.05 OF THE STANDARD SPECIFICATIONS EXCEPT THAT CONCRETE STRENGTH SHALL BE 4,000 PSI AFTER 28 DAYS.

THE CONTRACT UNIT PRICE EACH FOR INLET SPECIAL SHALL INCLUDE THE COST OF CONSTRUCTING THE INLET BOX, FURNISHING AND INSTALLING THE FRAME AND GRATE, THE CAST IRON STEPS (IF USED), THE PRECAST FLOOR SLABS, SAND CUSHION (WHEN USED) AND REINFORCEMENT BARS.

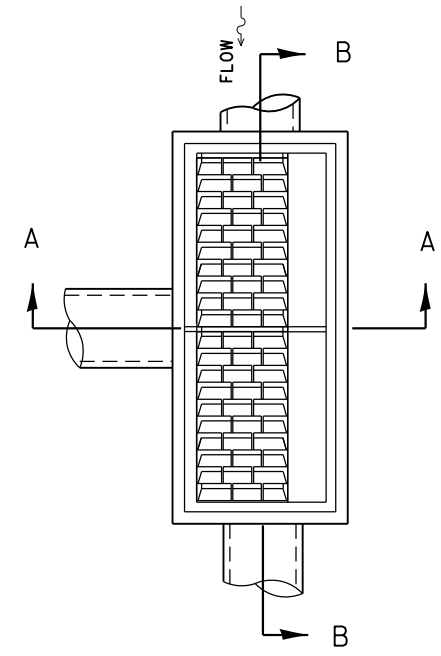
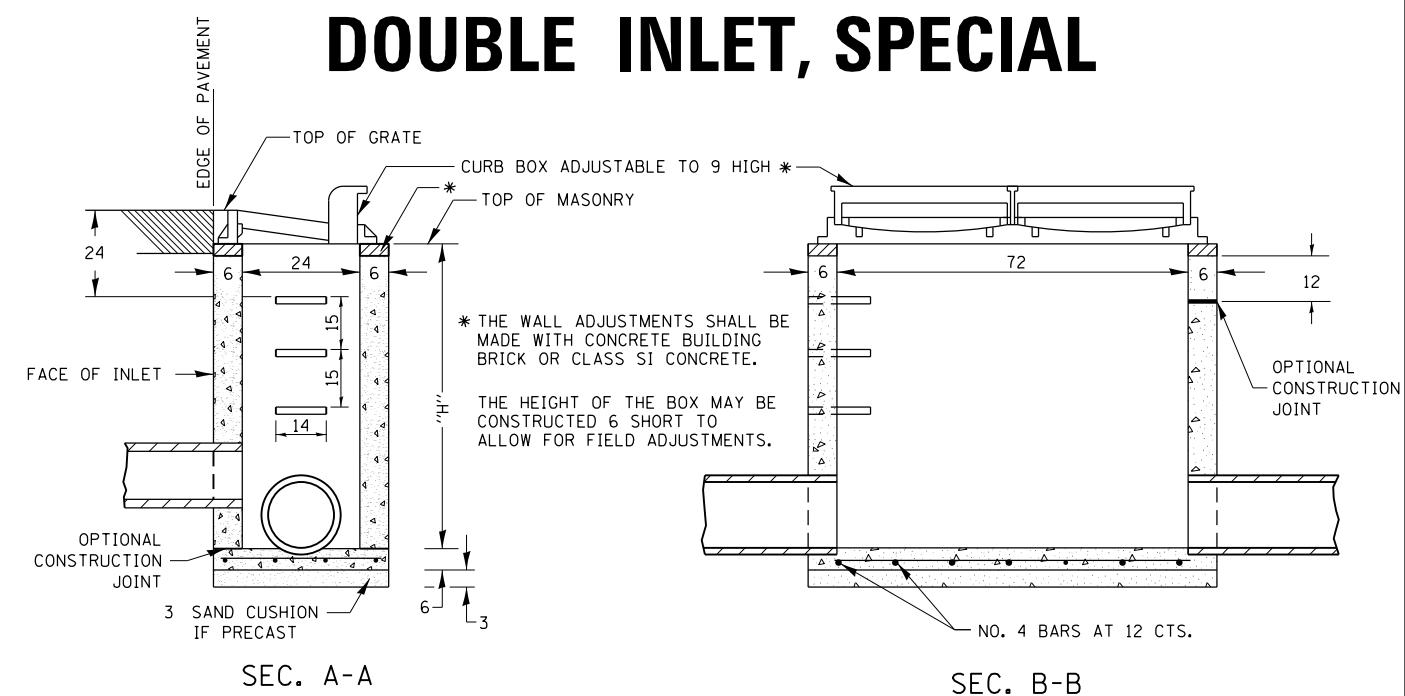


* THIS GRATE TO BE USED WITHOUT CURB BOX WHEN INLET IS IN DRIVEWAY.

ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

REVISED - 10-13-11

DOUBLE INLET, SPECIAL



NOTES

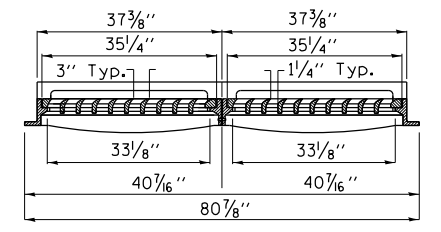
SEE STANDARD 602701 FOR DETAILS OF STEPS.

EXCEPT AS NOTED HEREON DOUBLE INLET SPECIAL SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 602 OF THE STANDARD SPECIFICATIONS.

THE SIDE WALLS MAY BE BUILT AS PRECAST SEGMENTED SECTIONS.

ALL VOIDS AROUND PIPE ENTRANCE, BOTH INSIDE AND OUTSIDE, SHALL BE SEALED WITH MORTAR.

R-3295-2 DOUBLE UNIT STEPS SHALL BE OMITTED WHEN DEPTH OF "H" IS LESS THAN 5 FOOT.

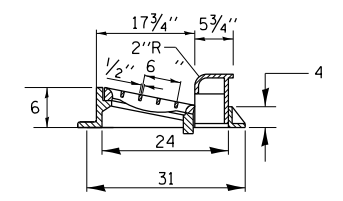


DETAIL OF FRAME & GRATE

NOTES

CLASS SI CONCRETE OR PRECAST CONCRETE SHALL BE USED THROUGHOUT. PRECAST CONCRETE SHALL BE IN ACCORDANCE WITH SECTION 504.01 THRU 504.05 OF THE STANDARD SPECIFICATIONS EXCEPT THAT CONCRETE STRENGTH SHALL BE 4,000 PSI AFTER 28 DAYS.

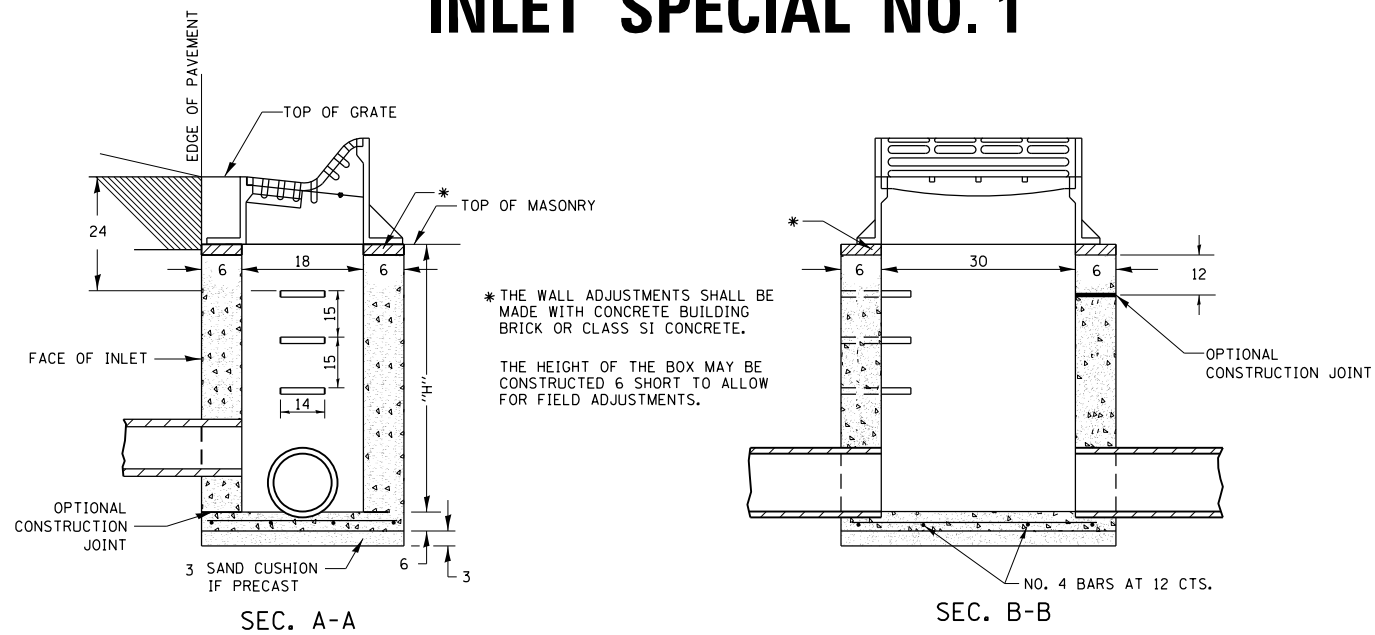
THE CONTRACT UNIT PRICE EACH FOR INLET SPECIAL SHALL INCLUDE THE COST OF CONSTRUCTING THE INLET BOX, FURNISHING AND INSTALLING THE FRAME AND GRATE, THE CAST IRON STEPS (IF USED), THE PRECAST FLOOR SLABS, SAND CUSHION (WHEN USED) AND REINFORCEMENT BARS.



ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

REVISED - 10-13-11	REGION 2 / DISTRICT 2 STANDARD		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
REVISED -	SCALE: *SCALE*	SHEET NO. 4 OF 19 SHEETS	742	(32, 33) R-1	WINNEBAGO	705	568
REVISED -	STA.	TO STA.	CONTRACT NO. 64821				
REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

INLET SPECIAL NO. 1



NOTES

- SEE STANDARD 602701 FOR DETAILS OF STEPS.
- EXCEPT AS NOTED HEREON INLET SPECIAL SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 602 OF THE STANDARD SPECIFICATIONS.
- THE SIDE WALLS MAY BE BUILT AS PRECAST SEGMENTED SECTIONS.
- ALL VOIDS AROUND PIPE ENTRANCE, BOTH INSIDE AND OUTSIDE, SHALL BE SEALED WITH MORTAR.
- STEPS SHALL BE OMITTED WHEN DEPTH OF "H" IS LESS THAN 5 FOOT.
- BOTH INLET SPECIAL NO. 1 SHALL DRAIN VERTICALLY TO THE ACROSS ROAD CULVERT LOCATED BENEATH.

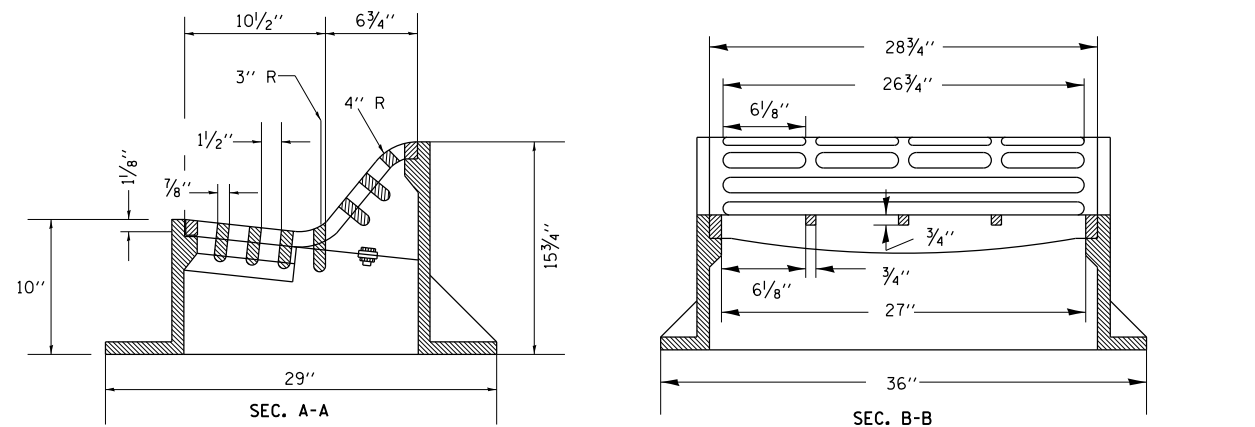
DETAIL OF FRAME & GRATE

NOTES

CLASS SI CONCRETE OR PRECAST CONCRETE SHALL BE USED THROUGHOUT. PRECAST CONCRETE SHALL BE IN ACCORDANCE WITH SECTION 504.01 THRU 504.05 OF THE STANDARD SPECIFICATIONS EXCEPT THAT CONCRETE STRENGTH SHALL BE 4,000 psi AFTER 28 DAYS.

THE CONTRACT UNIT PRICE EACH FOR INLET SPECIAL, NO.1 SHALL INCLUDE THE COST OF CONSTRUCTING THE INLET BOX, FURNISHING AND INSTALLING THE FRAME AND GRATE, THE CAST IRON STEPS (IF USED), THE PRECAST FLOOR SLABS, SAND CUSHION (WHEN USED) AND REINFORCEMENT BARS.

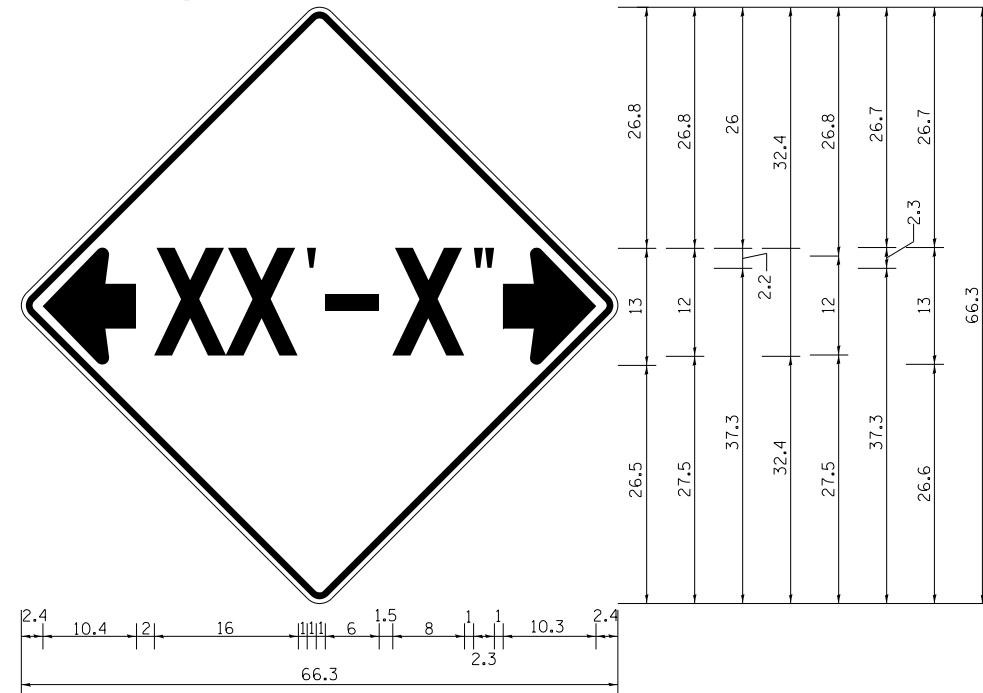
NEENAH # R-3503-B OR EQUIVALENT



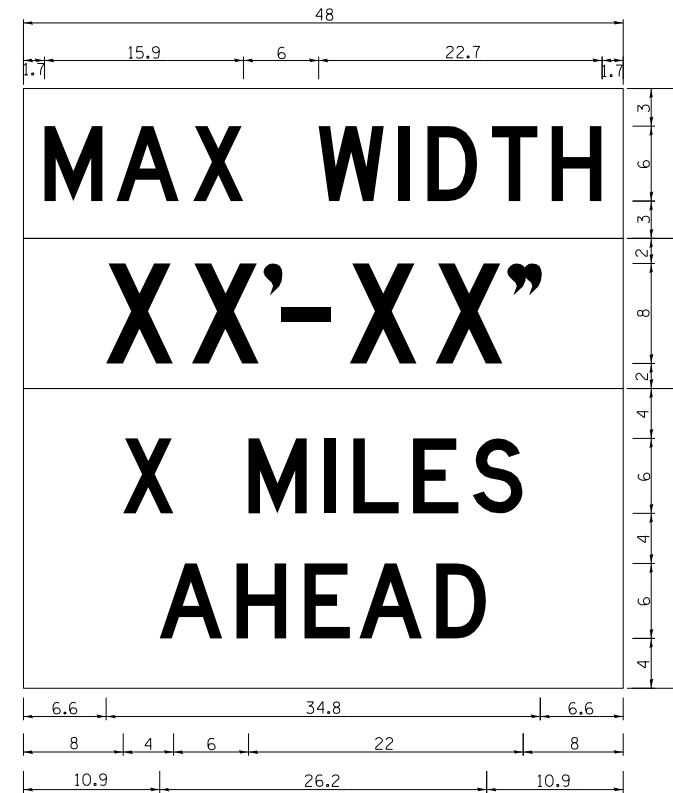
ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

REVISED - 10-14-11

INFORMATIONAL WARNING SIGN (FOR NARROW TRAVEL LANES)



NOTES
 W12-2 - Horizontal Clearance Sign
 48.0" across sides, 1.9" Radius,
 0.8" Border, 0.5" Indent, Black on
 Orange; Standard Arrow Custom
 10.4" X 8.1" 180° Black 11 Inch
 D Series Lettering; Standard Arrow
 Custom 10.4" X 8.1" 0°



W12-1103 (Width is 8D);
 No border, Black on White;
 [MAX WIDTH] D;

No border, Black on Orange;
 [XX'-XX''] D;

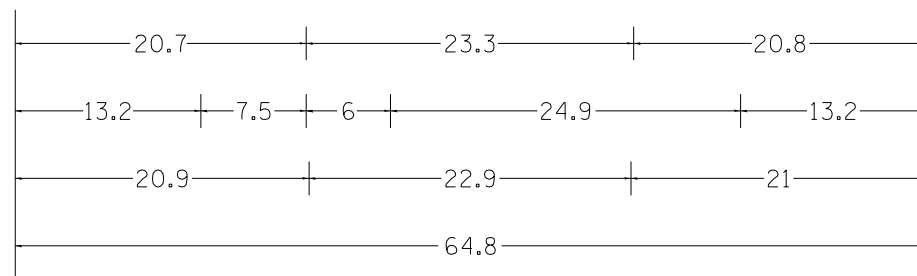
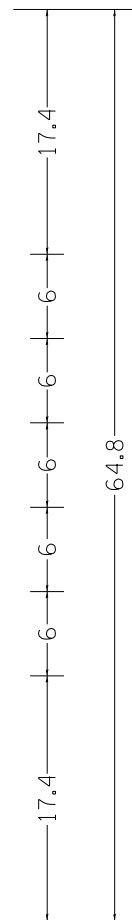
No border, Black on White;
 [X MILES] D; [AHEAD] D;

All work to furnish and install these signs shall be included in the cost of the Traffic Control Standards and shall not be paid for separately.

ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

REVISED - 5-15-09	REGION 2 / DISTRICT 2 STANDARD		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
REVISED -			742	(32, 33) R-1	WINNEBAGO	705	569
REVISED -			CONTRACT NO. 64821				
REVISED -	SCALE: *SCALE*	SHEET NO. 5 OF 19 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		

STAY IN YOUR LANE SIGN DETAIL



48.0" across sides 3.8" Radius, 1.0" Border, 0.6" Indent, Black on Orange
 "STAY" E Mod; "IN YOUR" E Mod; "LANE" E Mod;
 Table of letter and object lefts.

S	T	A	Y
20.7	26.8	31.6	38.0

I	N	Y	O	U	R
13.2	15.9	26.7	33.9	40.5	46.8

L	A	N	E
20.9	25.8	33.1	39.4

REVISED - 4-4-11

RESERVED PARKING SIGN DETAIL

IDOT NO. 10515
(R7-8)
12"x18"

IDOT NO. 10516
(R7-101)
12"x6"



SIGN PANEL, TYPE 1

NOTES

THE SIGN PANEL AND SIGN SUPPORT SHALL BE PLACED AT EACH DISABLED PARKING STALL. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR SIGN PANEL, TYPE 1 AND PER FOOT FOR TELESCOPING STEEL SIGN SUPPORT.

QUANTITIES

12 FT OF SIGN SUPPORT AND
 2 SQ. FT. OF SIGN PANEL IS REQUIRED AT EACH
 LOCATION.

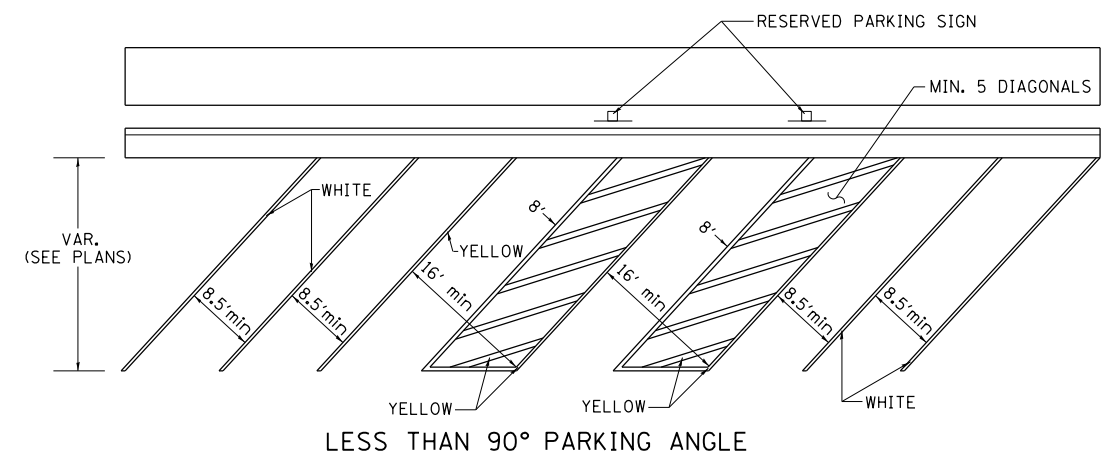
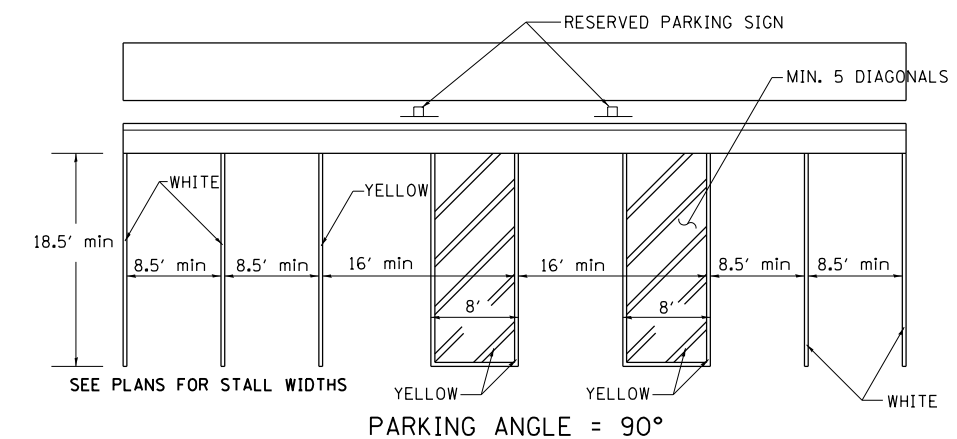
7 FT. MIN.

TELESCOPING STEEL SIGN SUPPORT

ALL DIMENSIONS ARE IN INCHES UNLESS
 OTHERWISE NOTED.

3 FT. IN GROUND

DISABLED PARKING STRIPING



REVISED - 10-14-11
REVISED -
REVISED -
REVISED -

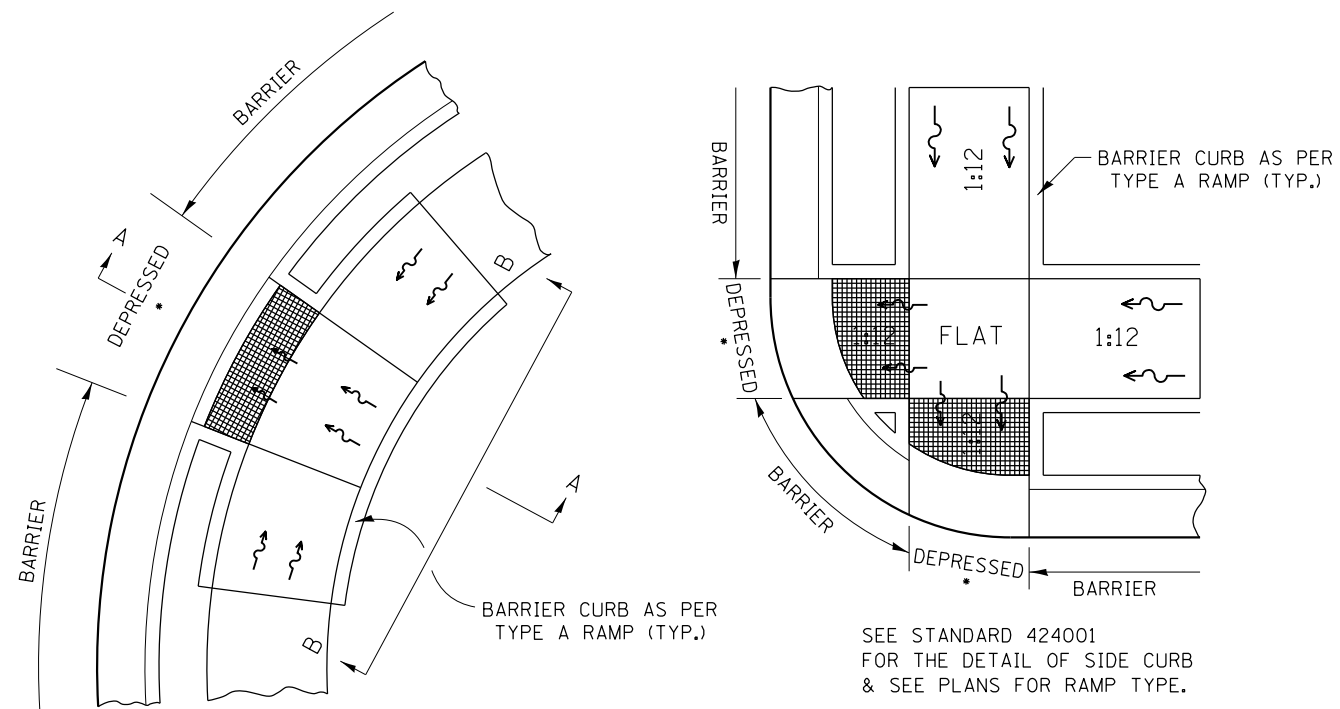
REGION 2 / DISTRICT 2 STANDARD

SCALE: *SCALE* SHEET NO. 6 OF 19 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32, 33) R-1	WINNEBAGO	705	570
CONTRACT NO. 64821				

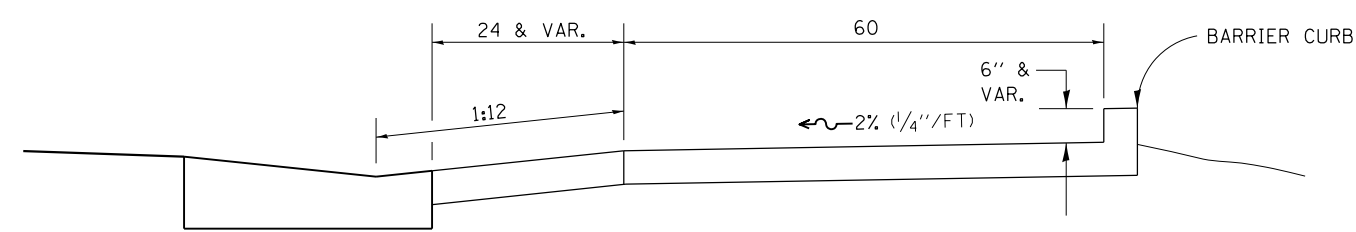
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

DISABLED RAMP DETAIL FOR TYPE A

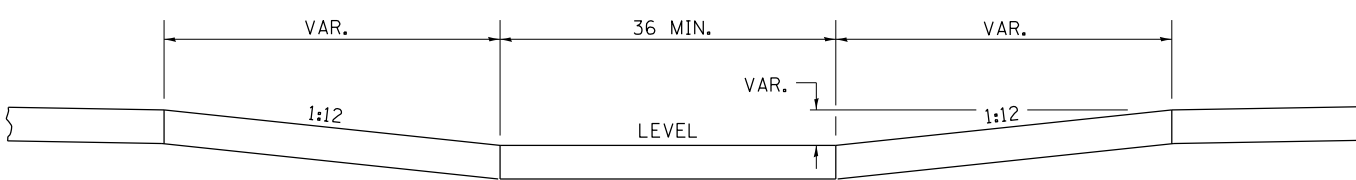


ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

• SEE NOTE BELOW



SECTION A-A

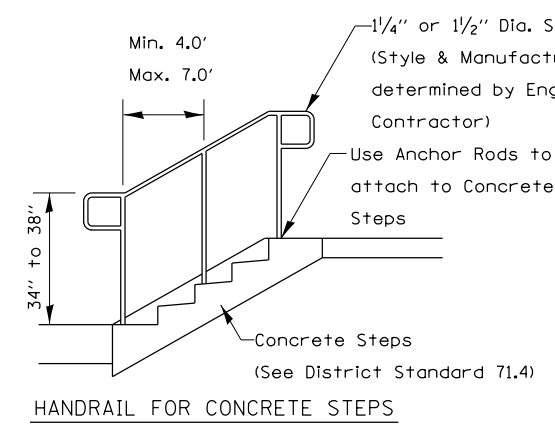


SECTION B-B

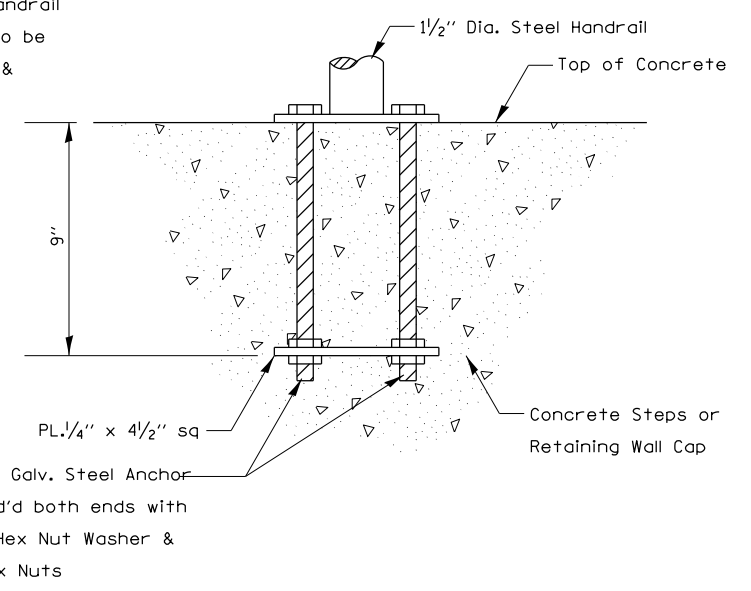
NOTES : THIS DETAIL TO BE USED IN CONJUNCTION WITH STATE STANDARD 424001. THE MAXIMUM ALLOWABLE CROSS SLOPE FOR SIDEWALK IS 2% (1/4"/FT) . THE MAXIMUM ALLOWABLE SIDEWALK GRADE IS 8% (1/2"/FT) . IF SPACE LIMITATIONS PROHIBIT THE USE OF THE 1:12 SLOPE, THEN SLOPES BETWEEN 1:10 ARE 1:12 ARE PERMITTED FOR A MAXIMUM RISE OF 6. SLOPES 1:8 AND 1:10 ARE ALLOWED FOR A MAXIMUM RISE OF 3. SLOPES STEEPER THAN 1:8 ARE NOT PERMITTED. THE DEPRESSED CURB IS NOT STANDARD. THE RISE IS 1/2 INSTEAD OF 1/2.

REVISED - 10-14-11

PIPE HANDRAILS FOR STEPS



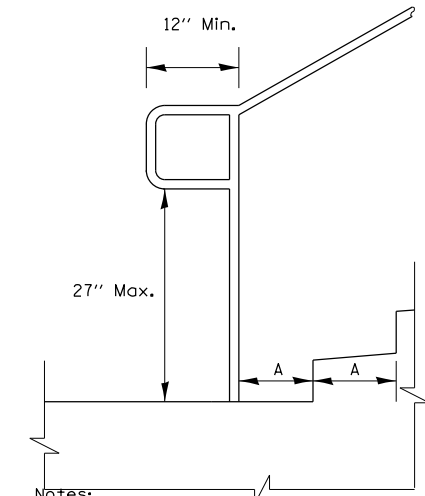
HANDRAIL FOR CONCRETE STEPS



3/4" Dia. Galv. Steel Anchor Rod Thrd'd both ends with Heavy Hex Nut Washer & two Hex Nuts

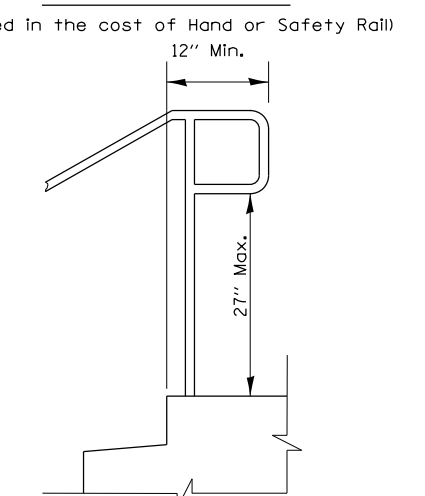
ANCHOR ROD DETAIL

(Included in the cost of Hand or Safety Rail)



Extension at Bottom of Run Detail

Stairways shall have continuous handrails both sides of all stairs.



Extension at Top of Run Detail

The inside handrail on switchback or dogleg stairs shall always be continuous.

Gripping surfaces shall be uninterrupted by newel posts, other construction elements, or obstructions.

Ends of handrail shall be either rounded or returned smoothly to floor, wall, or post.

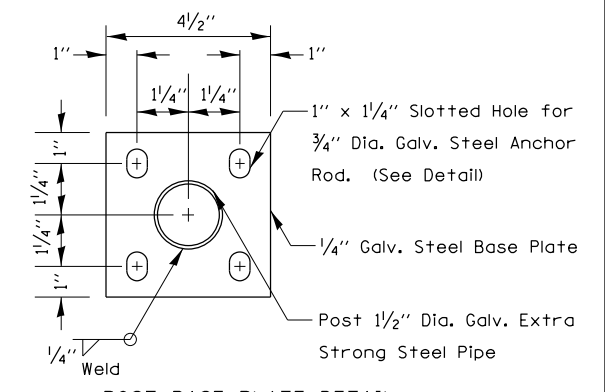
Hand & safety rails shall not rotate within their fittings.

The clear space between handrails and any wall shall be 1/2"

Handrail shall conform to Section 509 with the exception that all pipe and connections shall be welded galvanized or aluminum according to Article 1006.30, or 1006.34.

The diameter of the gripping surface of the handrail shall be 1-1/4" to 1-1/2"

This work shall be paid for at the contract unit price per FOOT for PIPE HANDRAIL.

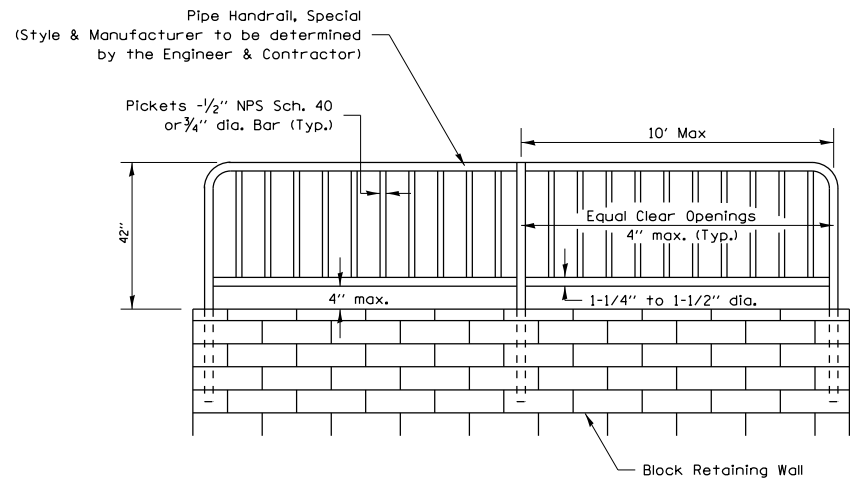


POST BASE PLATE DETAIL

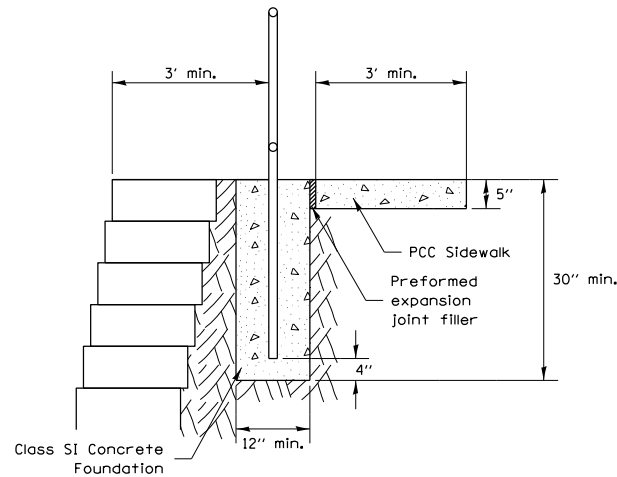
(Included in the cost of Hand or Safety Rail)

REVISED - 10-14-11	REGION 2 / DISTRICT 2 STANDARD		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
REVISED -	SCALE: *SCALE*	SHEET NO. 7 OF 19 SHEETS	742	(32, 33) R-1	WINNEBAGO	705	571
REVISED -		STA.	CONTRACT NO. 64821				
REVISED -		TO STA.	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

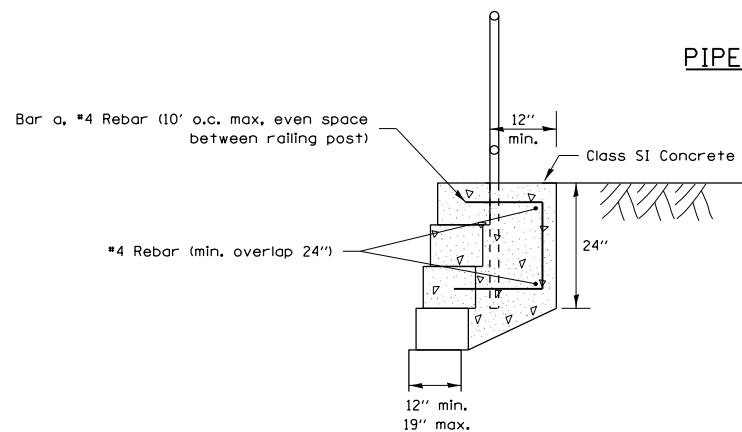
PIPE HANDRAIL, SPECIAL – FOR RETAINING WALLS



PIPE HANDRAIL, SPECIAL - FOR RETAINING WALL
(See details for installation options)



PIPE HANDRAIL, SPECIAL - FOR RETAINING WALL
(Option 1)



PIPE HANDRAIL, SPECIAL - FOR RETAINING WALL
(Option 3 - Block depth 12" to 19")

Notes:

Gripping surfaces shall be uninterrupted by construction elements, or obstructions.

Ends of handrail shall be rounded.

Handrail shall not rotate within their fittings.

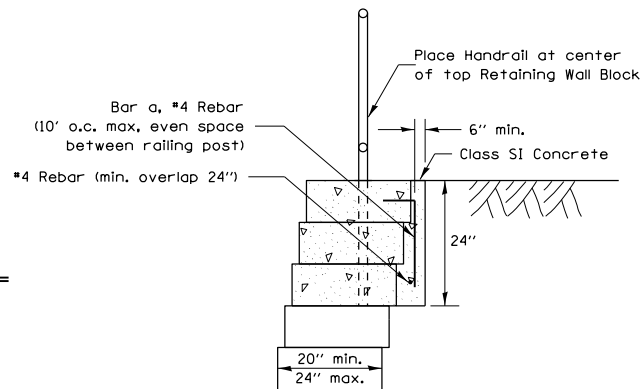
Handrail shall conform to Section 509 with the exception that all pipe and connections shall be welded galvanized or aluminum according to Article 1006.30, or 1006.34.

The diameter of the gripping surface of the handrail shall be 1-1/4" to 1-1/2"

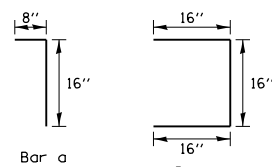
Handrail required when wall height difference is 4' or greater

Drilling of blocks will be necessary for reinforcement placement.

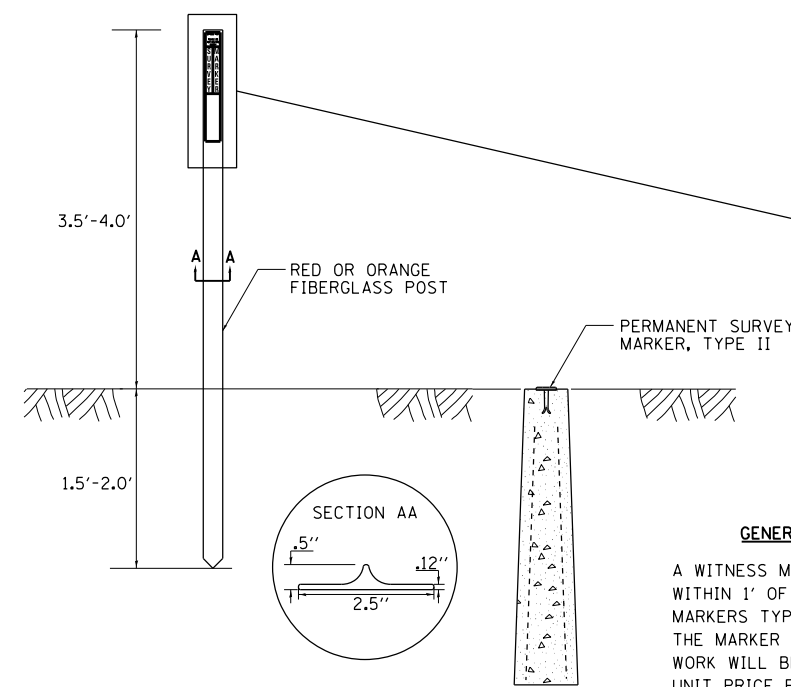
This work shall consist of furnishing and erecting Handrails as listed above and according to this detail. This work shall be paid for at the contract UNIT price per FOOT for PIPE HANDRAIL, SPECIAL.



PIPE HANDRAIL, SPECIAL - FOR RETAINING WALL
(Option 2-Block depth greater than 20")

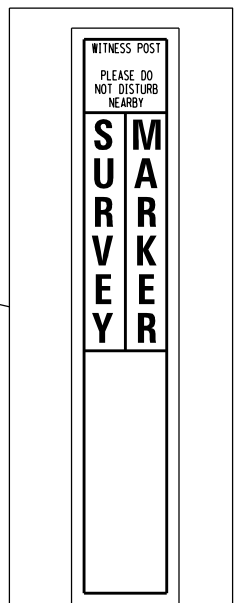


WITNESS MARKER FOR PERMANENT SURVEY MARKERS, TYPE II

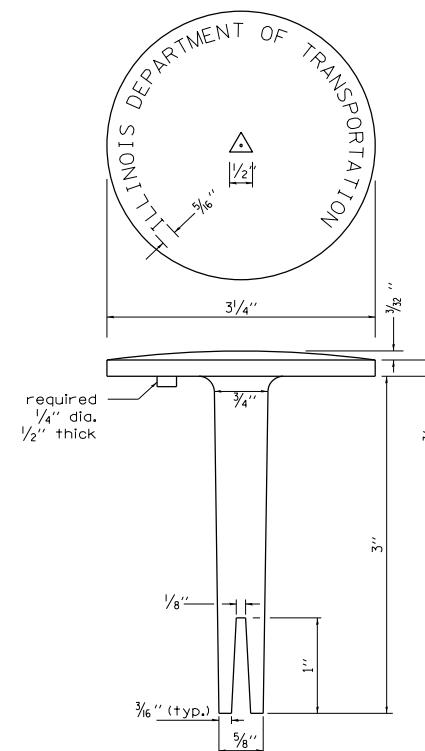


GENERAL NOTES

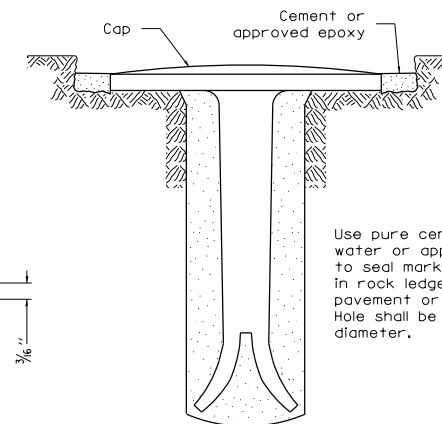
A WITNESS MARKER SHALL BE INSTALLED WITHIN 1' OF ALL PERMANENT SURVEY MARKERS TYPE II EXCEPT IN AREAS WHERE THE MARKER IS IN THE SIDEWALK. THIS WORK WILL BE INCLUDED TO THE CONTRACT UNIT PRICE PER EACH FOR PERMANENT SURVEY MARKERS, TYPE II.



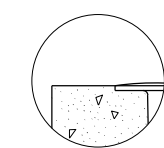
PERMANENT SURVEY MARKERS, TYPE II



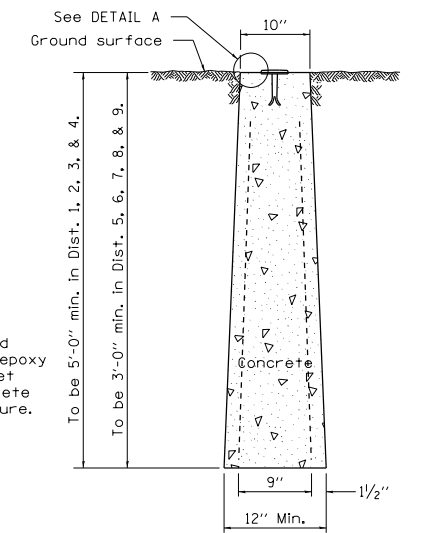
BRASS OR ALUMINUM TABLET



TYPE II



DETAIL A



TYPE II

CAST-IN-PLACE MARKER

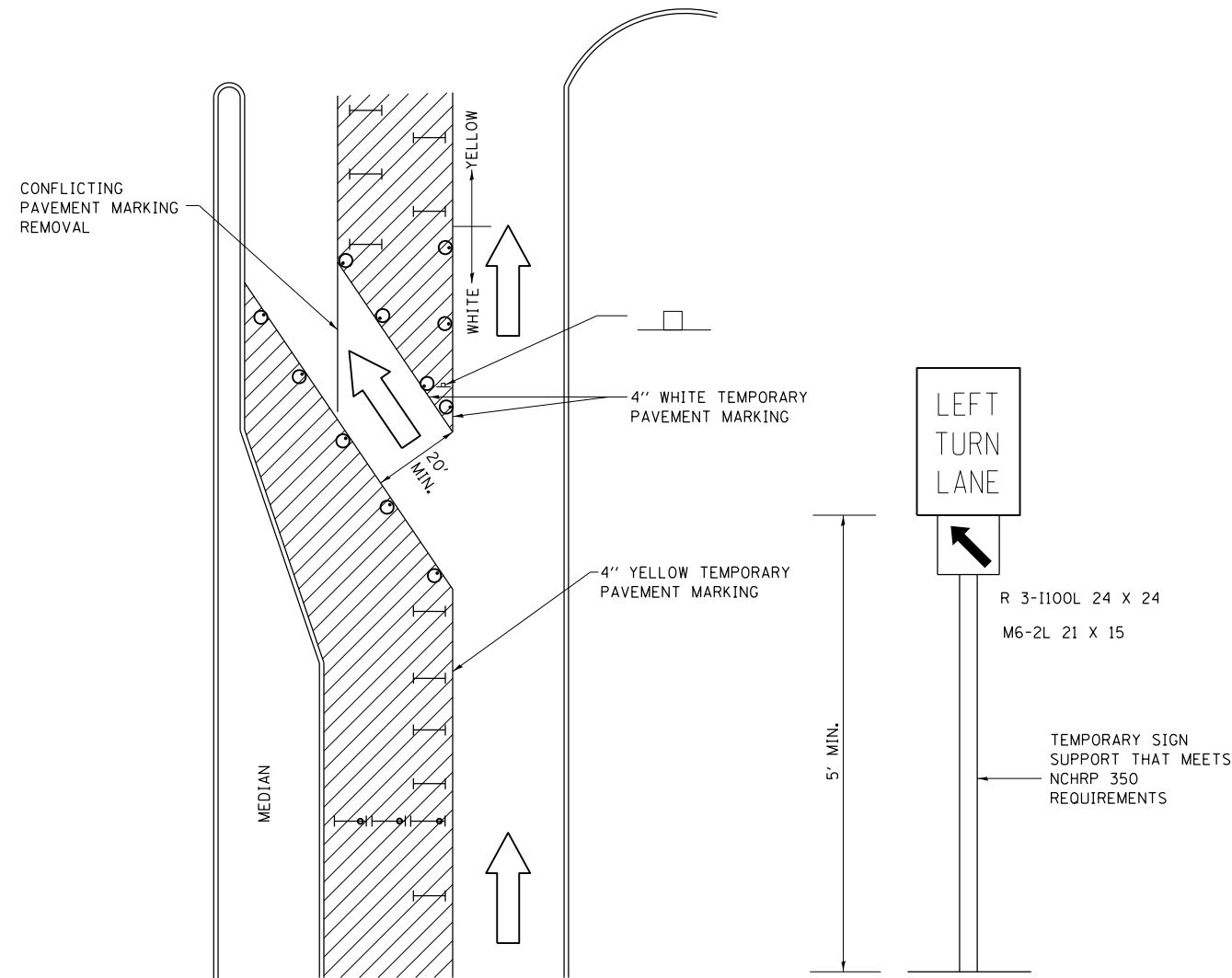
No. 3 bars to be 30" for 36" min. & 3'-6" for 4'-0" min.

REVISED - 2-01-10


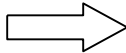




ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

REVISED - 10-14-11	REGION 2 / DISTRICT 2 STANDARD		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
REVISED -	SCALE: *SCALE*	SHEET NO. 8 OF 19 SHEETS	742	(32, 33) R-1	WINNEBAGO	705	572
REVISED -			CONTRACT NO. 64821				
REVISED -			FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)



LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE OR DRUM WITH FLASHING BURNING LIGHT
-  DRUM OR BARRICADE WITH STEADY BURN LIGHT
-  SIGN (SEE DETAIL)
-  TYPE I OR II CHECK BARRICADE WITH STEADY LIGHT BURN

REVISED - 10-14-11

GENERAL NOTES

- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 IN HEIGHT.
- STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS WILL BE MONODIRECTIONAL.
- TEMPORARY PAVEMENT MARKING SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
- THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 X 24 AND M6-2R 21 X 15 SHALL BE USED.
- THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
- TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.
- ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

LEFT
TURN
LANE

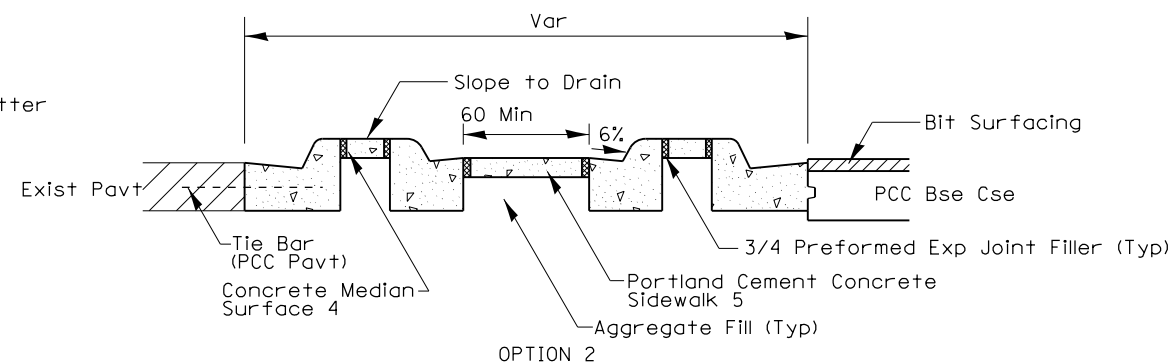
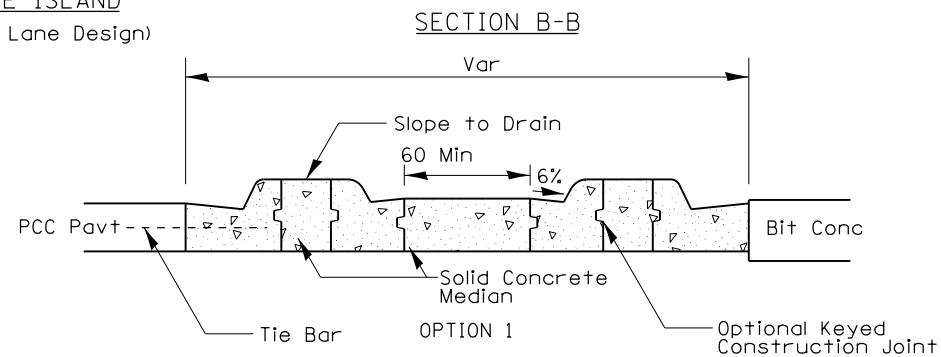
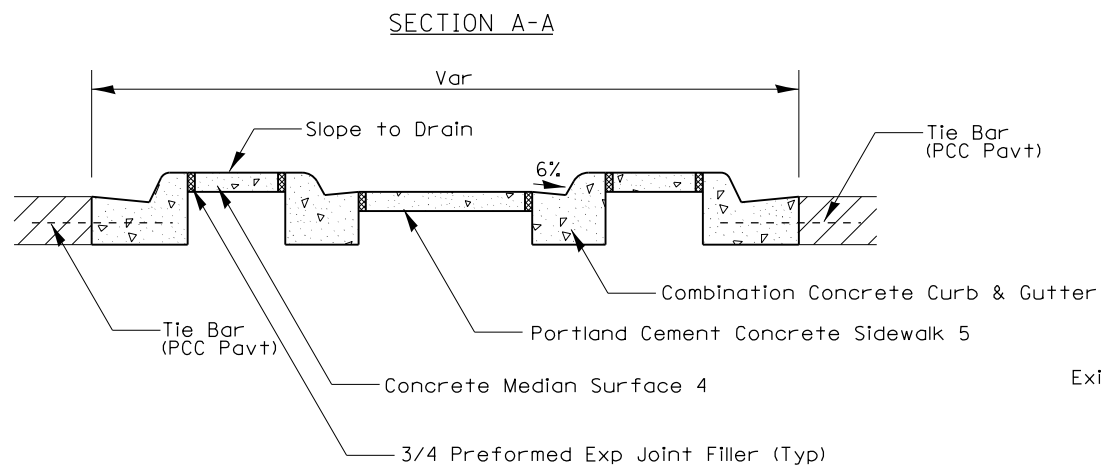
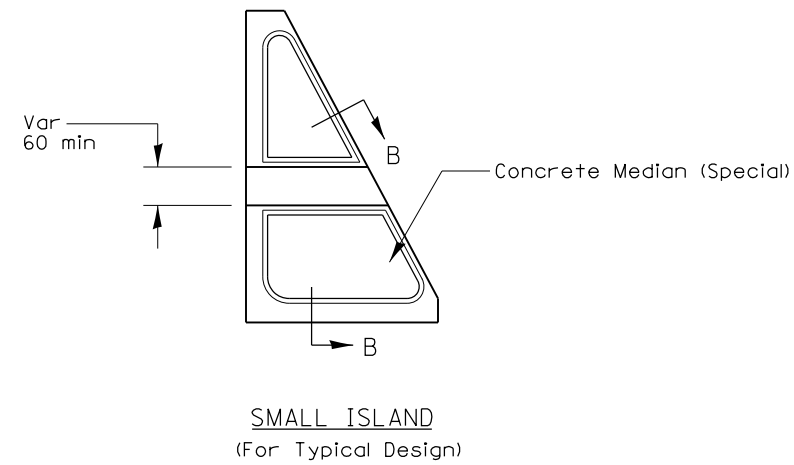
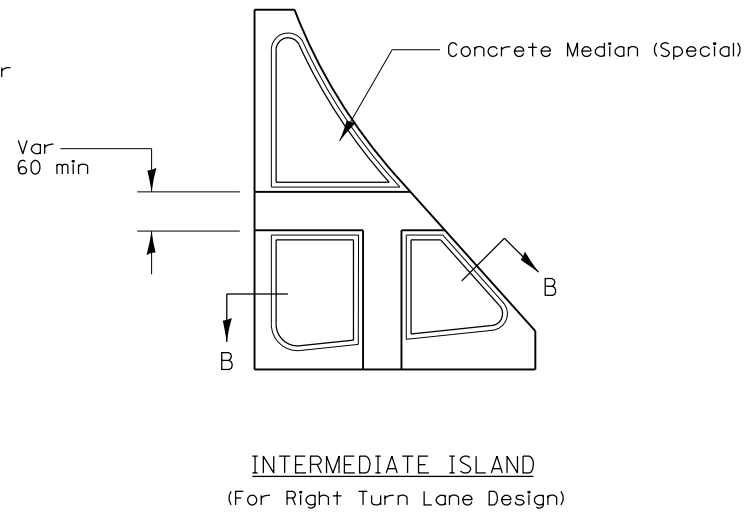
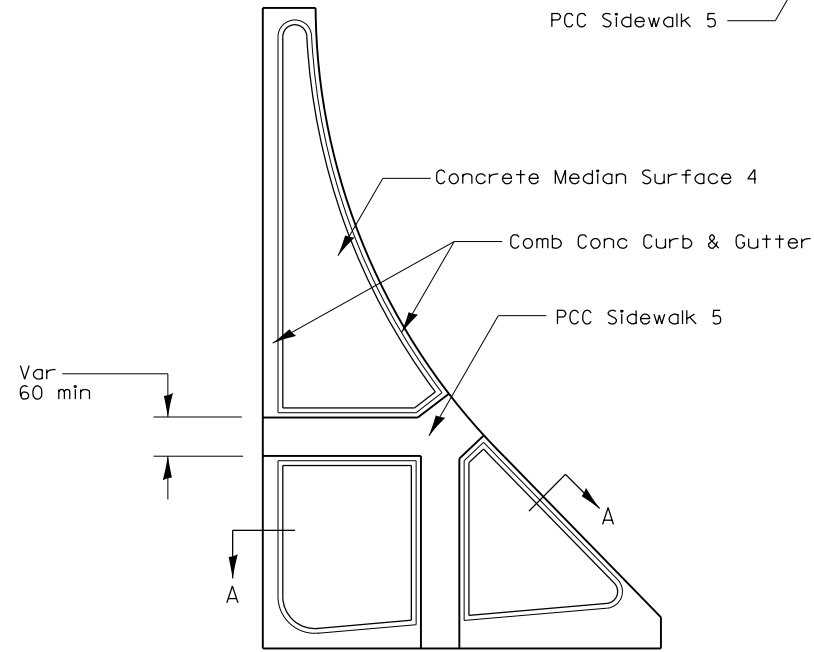
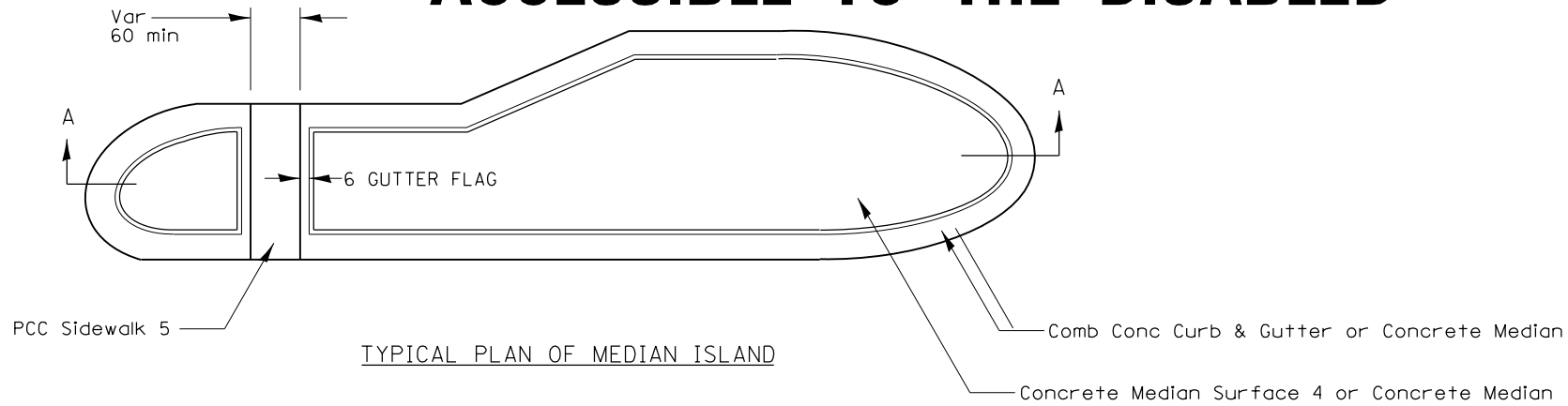
R 3-1100L 24 X 24
M6-2L 21 X 15

TEMPORARY SIGN
SUPPORT THAT MEETS
NCHRP 350
REQUIREMENTS

5' MIN.

REVISED -	REGION 2 / DISTRICT 2 STANDARD	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
REVISED -		742	(32, 33) R-1	WINNEBAGO	705	573	
REVISED -		CONTRACT NO. 64821					
REVISED -		SCALE: *SCALE*	SHEET NO. 9 OF 19 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT	
REVISED -							

PC CONCRETE ISLANDS AND MEDIANS ACCESSIBLE TO THE DISABLED



General Notes:
See Standard 606301 and plan sheets for station & offsets, radii, dimensions, and details not shown.

See Standard 424031 for sidewalk details not shown.

The sidewalk should drain to the low side of the island. If necessary the sidewalk shall be sloped to drain at a maximum 2% grade.

See the plan general notes for the type of curb & gutter to be used on islands.

Curb & gutter adjacent to the walkway in the interior of the island shall have 6 gutter flags.

The sidewalk should not be closer than 36 from the corner of the island.

Keyed longitudinal construction joints shall be constructed without tie bars.

Medians and large islands shall consist of PCC Sidewalk 5, Concrete Median Surface 4, and Combination Concrete Curb & Gutter, Type M or B of the size specified. Median island can also be solid concrete medians.

Locations, layouts, and widths of the flush sidewalk area, shall be determined by the designer and shown on the plans.

The intermediate and small islands will be measured for payment from E.O.P. to E.O.P. using either option 1 or option 2, as directed by the Engineer, and will be paid for at the contract unit price per SQ FT for CONCRETE MEDIAN (SPECIAL) which shall include the combination curb & gutter, sidewalk, aggregate fill, concrete median surface, and solid concrete median.

ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

FILE NAME =	USER NAME = Hogensonjd	DESIGNED -	REVISED - 10-09-12
et:\pwork\pwork\hogensonjd\0328875\0264821-sht-details-district.dgn		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

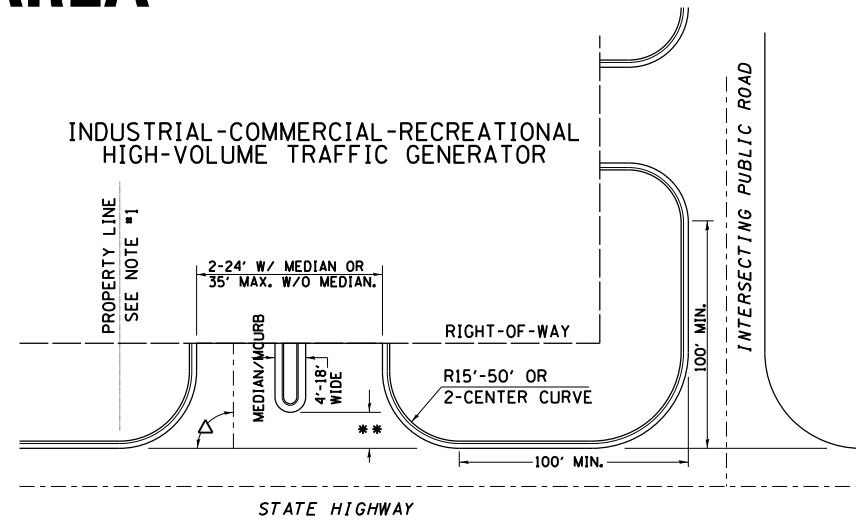
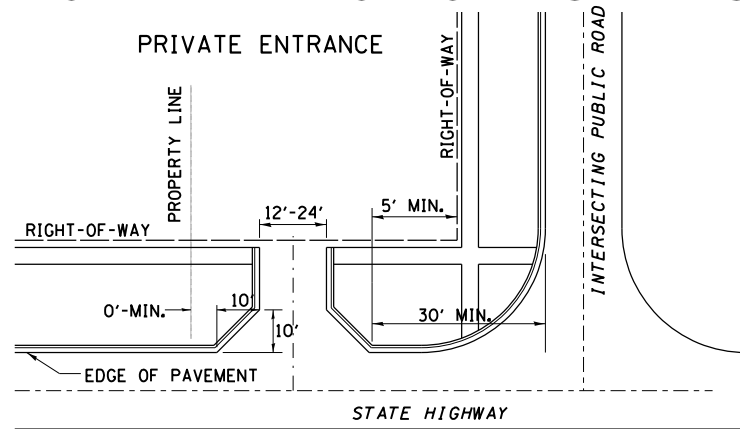
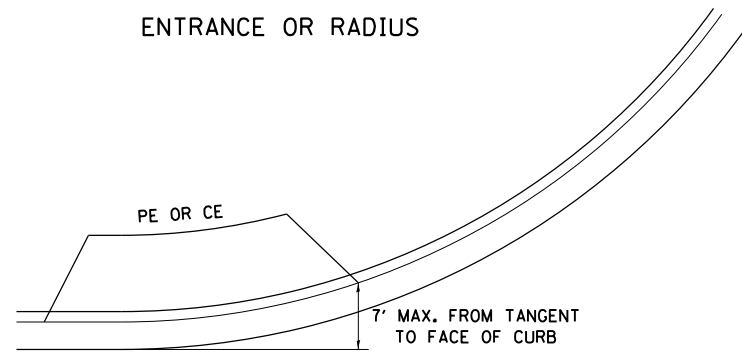
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REGION 2 / DISTRICT 2 STANDARD

SCALE: SHEET NO. 10 OF 19 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			705	574
CONTRACT NO.				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

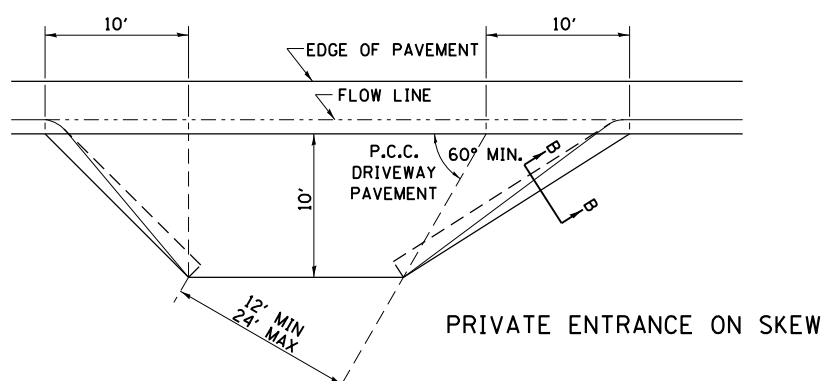
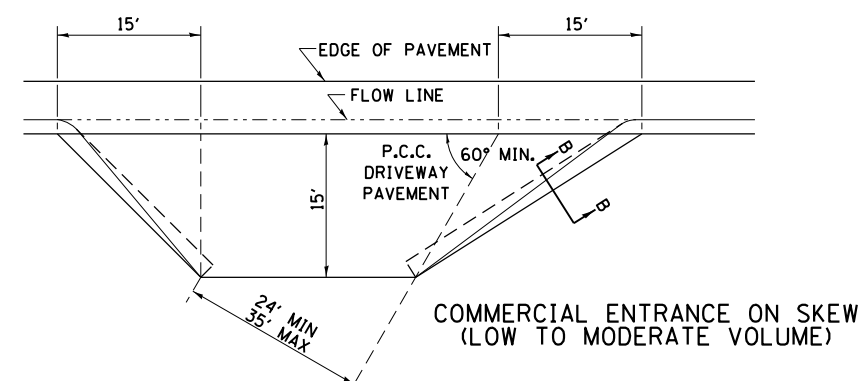
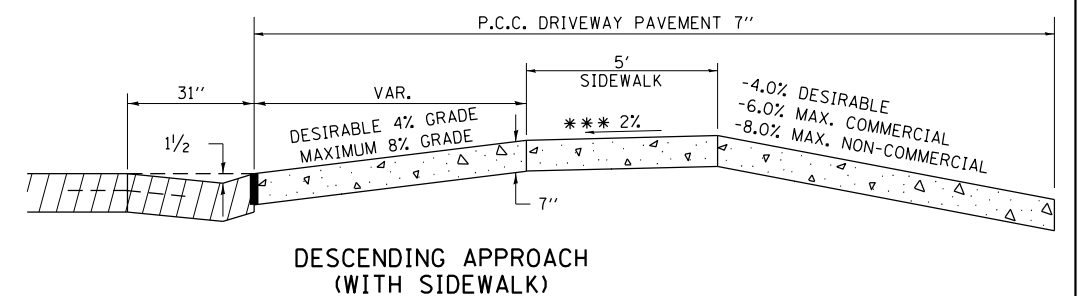
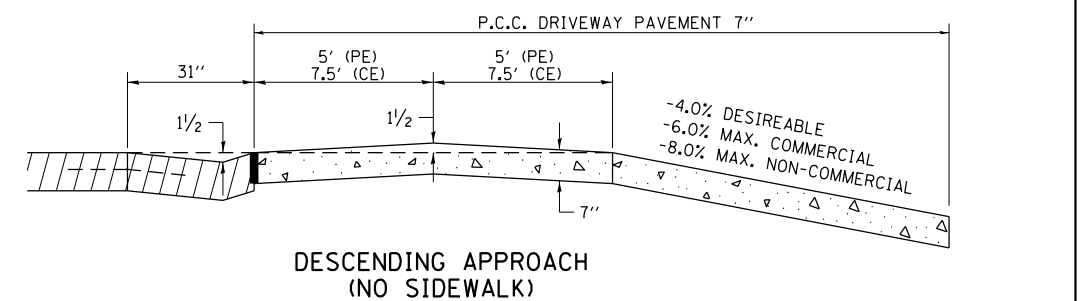
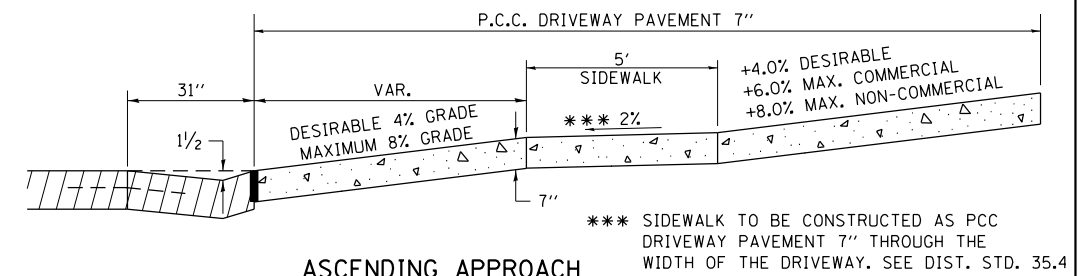
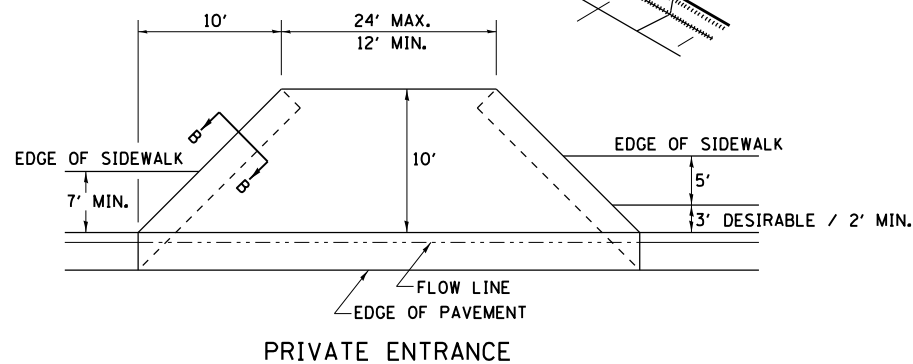
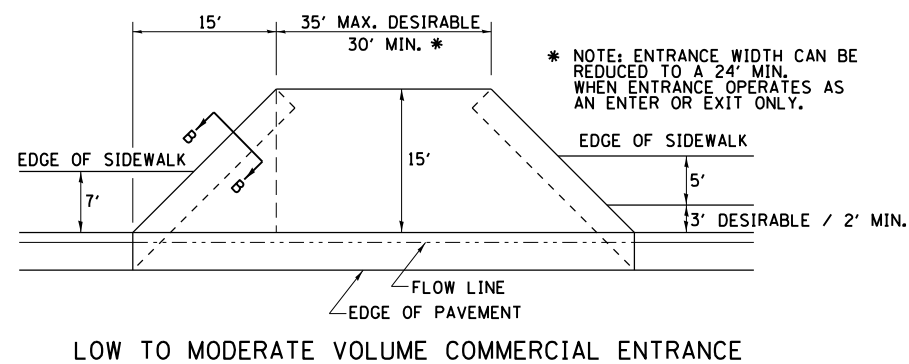
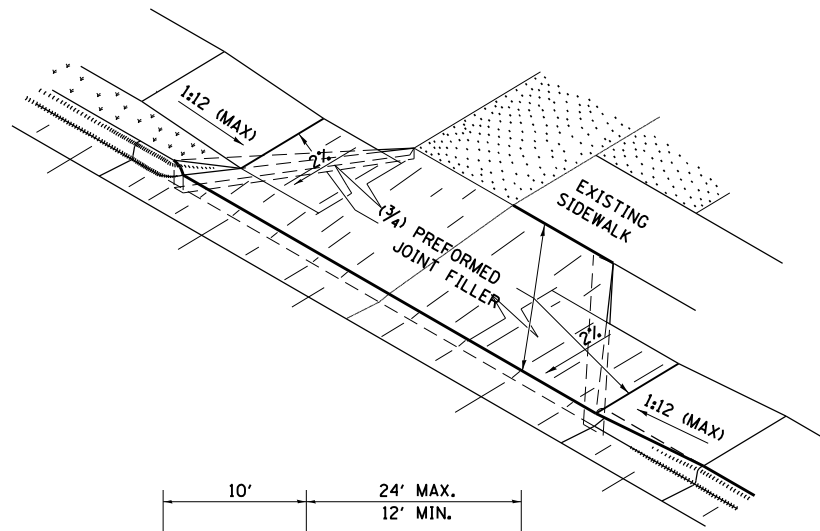
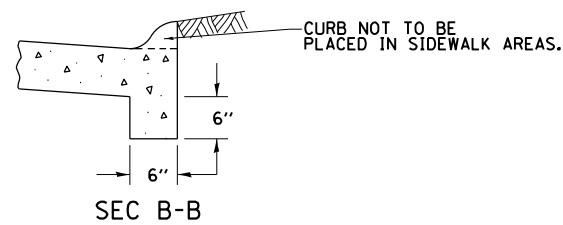
ENTRANCE APPROACHES – URBAN AREA



A MINIMUM OF 440 FEET SHALL BE MAINTAINED BETWEEN CENTER LINES OF ADJACENT DRIVEWAYS.
 Δ 90° DESIRABLE, 45° MIN. ANGLE PERMITTED ONLY FOR ONE-WAY DRIVEWAYS.
 60° MIN. ANGLE FOR TWO-WAY DRIVEWAYS.

NOTE: #1 ENCROACHMENT ON THE ADJACENT PROPERTY OWNER LAND REQUIRES HIS OR HER WRITTEN APPROVAL.

** 4'-10' IF HIGHWAY CURBED.
 AT EDGE OF SHOULDER IF HIGHWAY UNCURBED.



NOTE: CURVED ENTRANCE RETURNS MAY BE USED FOR LOW TO MODERATE VOLUME LOCATIONS WITH REVIEW ON A CASE-BY-CASE BASIS.

FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED - 12-07-10
#FILE#		DRAWN -	REVISED -
	PLOT SCALE = #SCALE#	CHECKED -	REVISED -
	PLOT DATE = #DATE#	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REGION 2 / DISTRICT 2 STANDARD

SCALE: SHEET NO. 11 OF 19 SHEETS STA. TO STA.

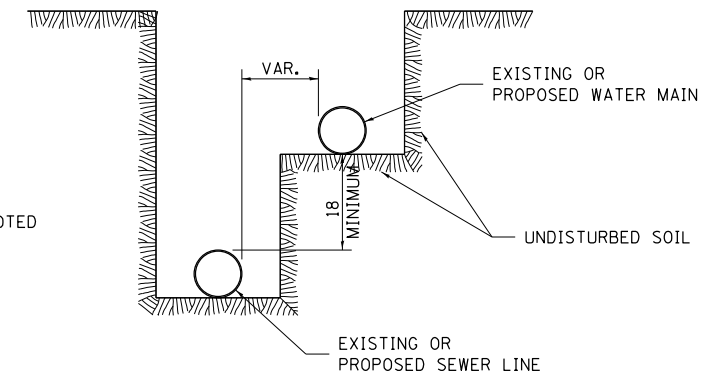
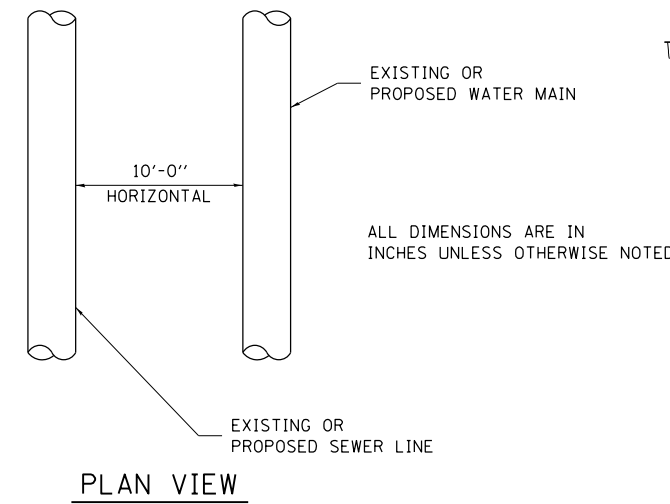
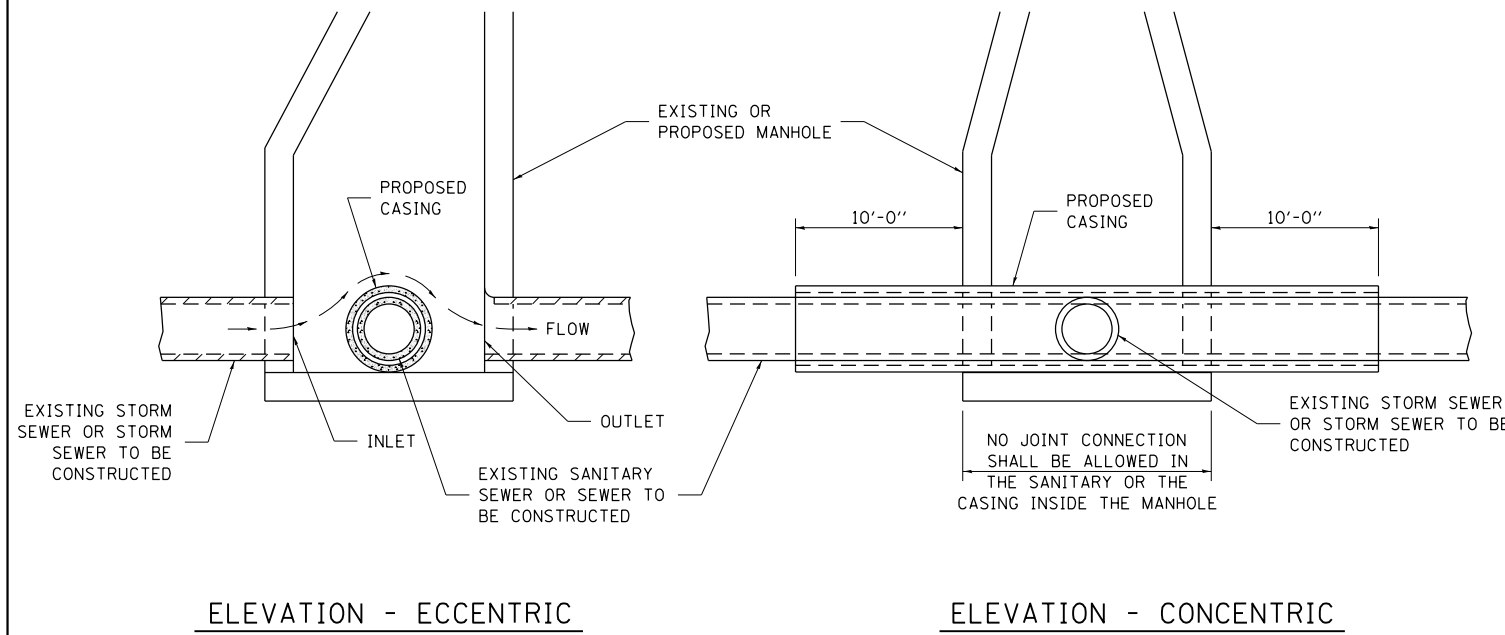
F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32, 33) R-1	WINNEBAGO	705	575
CONTRACT NO. 64821				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

SEWER AND WATER MAIN CROSSINGS

THIS DETAIL IS FOR UNKNOWN UTILITIES UNLESS QUANTITIES ARE INCLUDED IN THE PLANS THE EXTRA WORK WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04.

WHEN PROPOSED SEWER (OR WATER) IS LOCATED 10'-0" OR MORE FROM EXISTING WATER (OR SEWER) NO SPECIAL CONSTRUCTION REQUIRED.

WHEN PROPOSED SEWER (OR WATER) IS LOCATED LESS THAN 10'-0" FROM EXISTING WATER (OR SEWER) DETAILS BELOW SHALL APPLY.

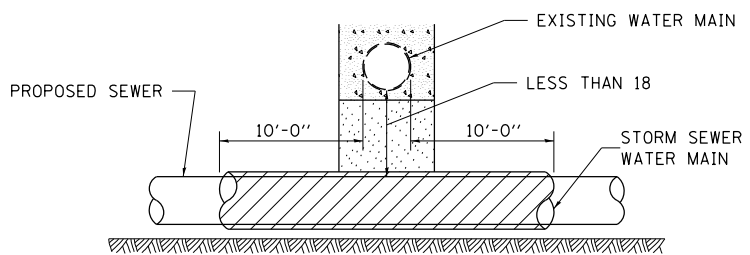


AT GRADE CROSSING OF SANITARY AND STORM SEWER

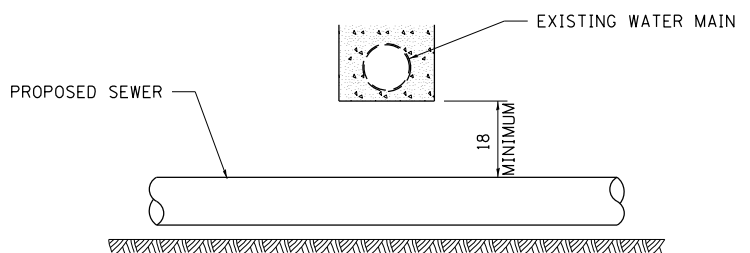
CASING SHALL BE CAST IRON WITH AN INSIDE DIAMETER 2" LARGER IN DIAMETER THAN ENCASED PIPE OUTSIDE DIAMETER WITH BOTH ENDS OF CASING SEALED

ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED

POINT LOADS SHALL NOT BE ALLOWED BETWEEN SEWER OR SEWER CASING AND WATER MAIN
PROVIDE ADEQUATE SUPPORT FOR EXISTING WATER MAIN TO PREVENT DAMAGE DUE TO SETTLEMENT OF SEWER TRENCH



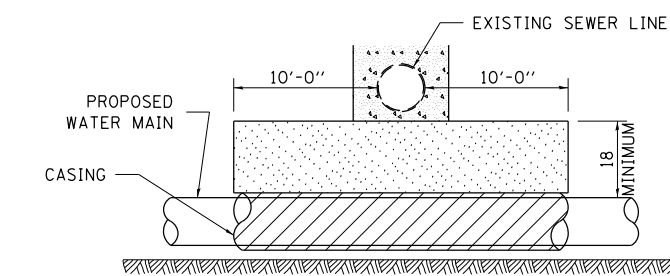
PROVIDE ADEQUATE SUPPORT FOR EXISTING WATER MAIN TO PREVENT DAMAGE DUE TO SETTLEMENT OF SEWER TRENCH
MAINTAIN 18 MINIMUM VERTICAL SEPARATION FOR 10 FT. HORIZONTALLY



ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED

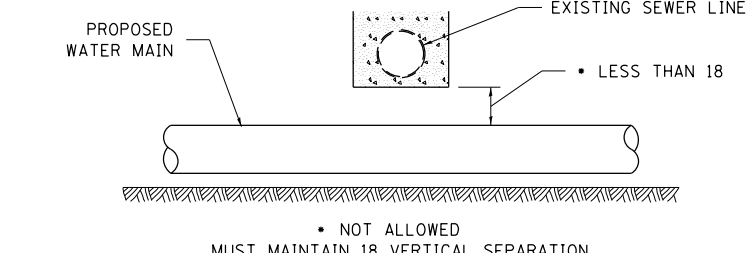
PROPOSED SEWER LINE BELOW EXISTING WATER MAIN

PROVIDE ADEQUATE SUPPORT FOR EXISTING SEWER LINE TO PREVENT DAMAGE DUE TO SETTLEMENT
PLACE TRENCH BACKFILL FOR 10 FT. ON EITHER SIDE OF SEWER LINE



CASING SHALL BE CAST IRON WITH AN INSIDE DIAMETER 2" LARGER IN DIAMETER THAN ENCASED PIPE OUTSIDE DIAMETER WITH BOTH ENDS OF CASING SEALED

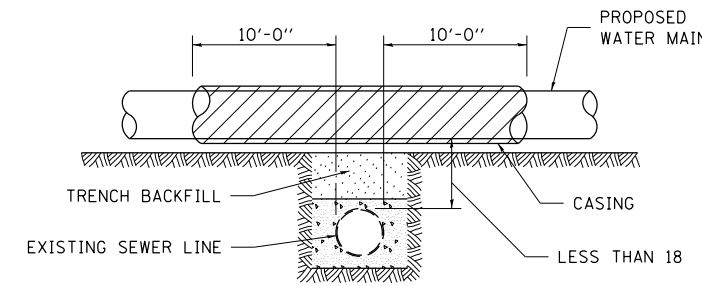
PROVIDE ADEQUATE SUPPORT FOR EXISTING WATER MAIN TO PREVENT DAMAGE DUE TO SETTLEMENT OF SEWER TRENCH
MAINTAIN 18 MINIMUM VERTICAL SEPARATION FOR 10 FT. HORIZONTALLY



ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED

PROPOSED WATER MAIN BELOW EXISTING SEWER LINE

POINT LOADS SHALL NOT BE ALLOWED BETWEEN WATER MAIN OR WATER MAIN CASING AND SEWER

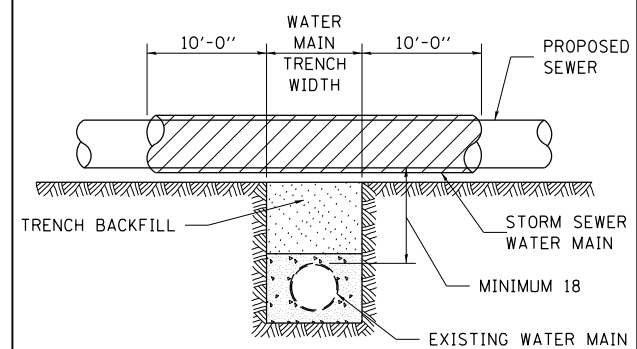


CASING SHALL BE CAST IRON WITH AN INSIDE DIAMETER 2" LARGER IN DIAMETER THAN ENCASED PIPE OUTSIDE DIAMETER WITH BOTH ENDS OF CASING SEALED

ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED

PROPOSED WATER MAIN ABOVE EXISTING SEWER LINE

PROVIDE ADEQUATE SUPPORT FOR SEWER TO PREVENT SETTLING AND BREAKING THE WATER MAIN.



ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED

EXISTING WATER MAIN BELOW PROPOSED SEWER LINE WITH MINIMUM 18 VERTICAL SEPARATION

FILE NAME =	USER NAME = *USER*	DESIGNED -	REVISED - 10-17-11
*FILE#		DRAWN -	REVISED -
	PLOT SCALE = *SCALE*	CHECKED -	REVISED -
	PLOT DATE = *DATE*	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REGION 2 / DISTRICT 2 STANDARD

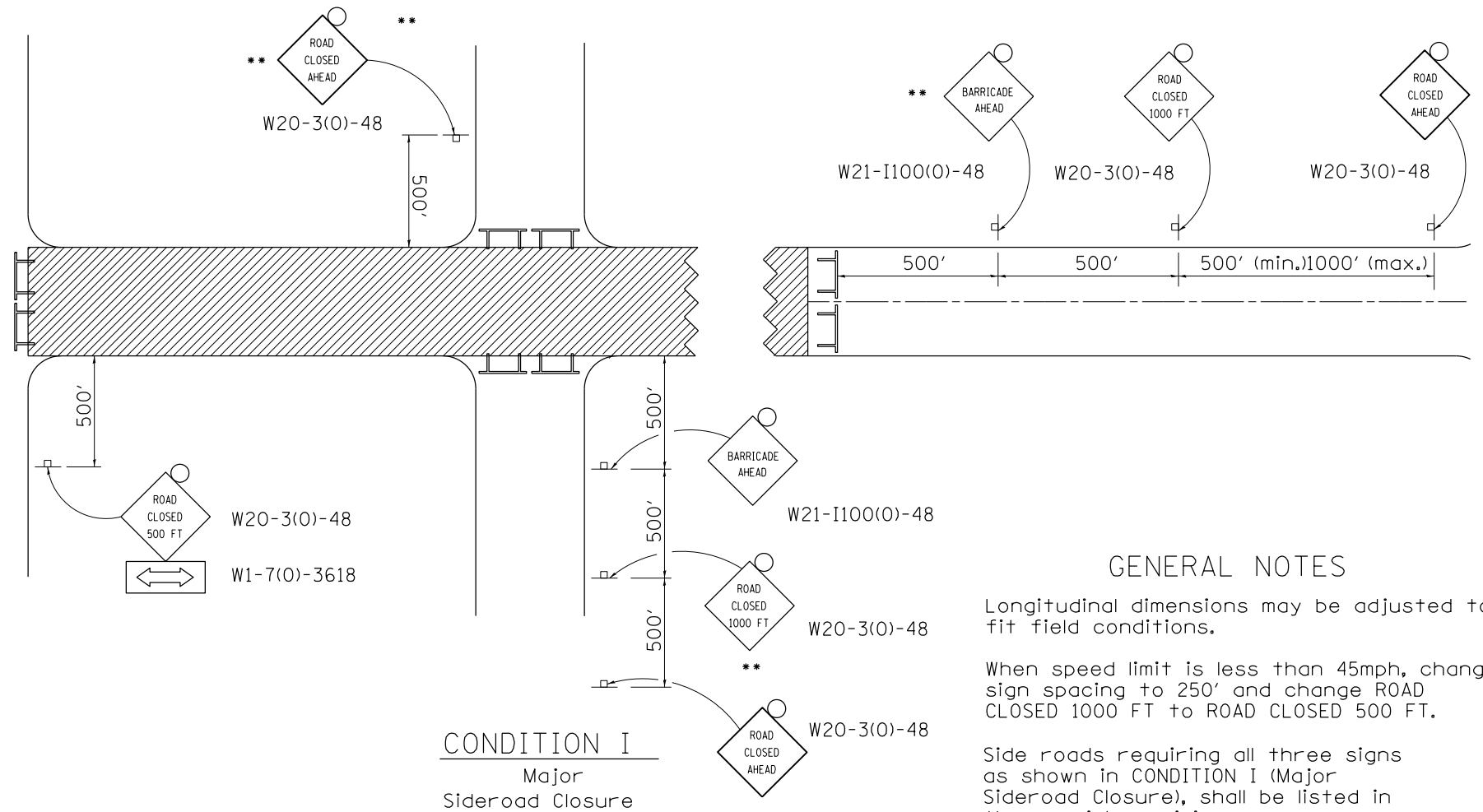
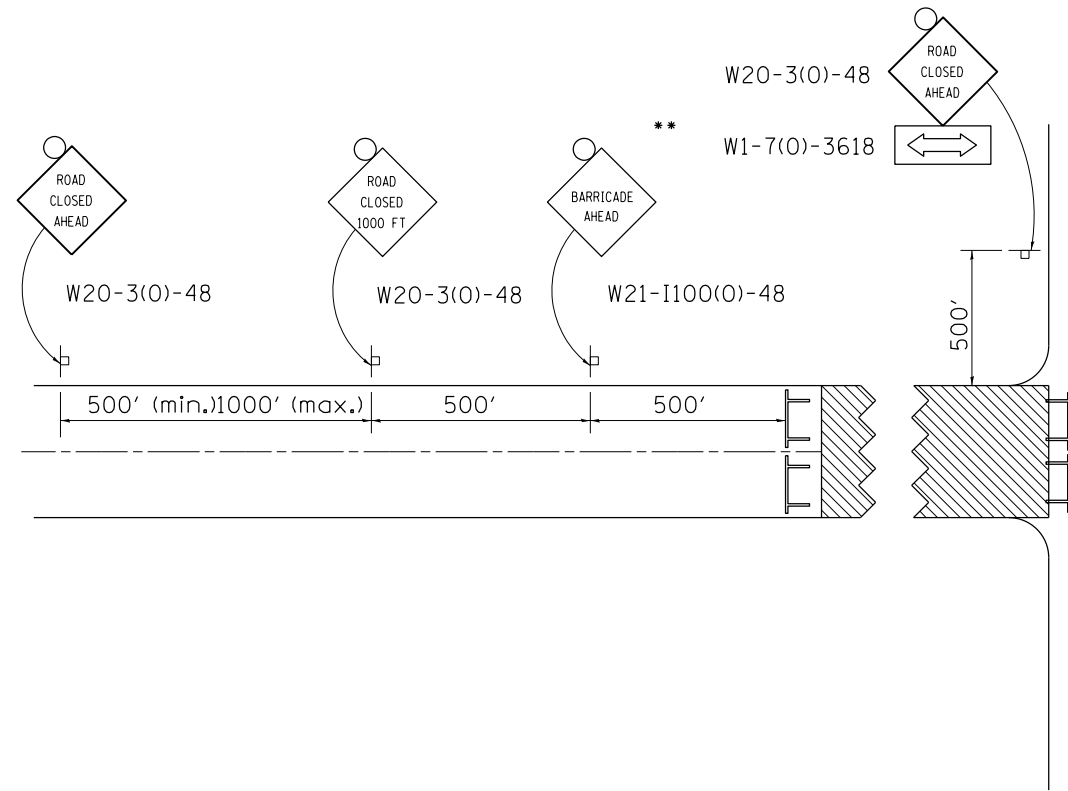
SCALE: SHEET NO. 12 OF 19 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32, 33) R-1	WINNEBAGO	705	576
CONTRACT NO. 64821				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

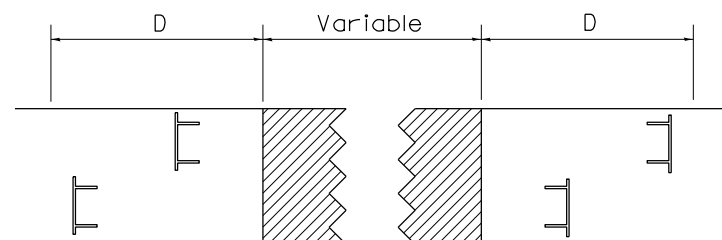
TRAFFIC CONTROL FOR ROAD CLOSURE

CONDITION II

Minor Sideroad Closure

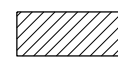




ROAD CLOSED TO THRU TRAFFIC BARRICADE SET UP



Type III Barricades and R11-4-4830 signs shall be as shown in "Road Closed To Thru Traffic" detail on Highway Standard 701901. If the distance "D" exceeds 2000' an additional set of barricades and R11-4-4830 shall be placed at each end of the work area.

SYMBOLS

-  Work area
-  Type III Barricade with Flashers
-  Sign with flashing light

GENERAL NOTES

Longitudinal dimensions may be adjusted to fit field conditions.

When speed limit is less than 45mph, change sign spacing to 250' and change ROAD CLOSED 1000 FT to ROAD CLOSED 500 FT.

Side roads requiring all three signs as shown in CONDITION I (Major Sideroad Closure), shall be listed in the special provision.

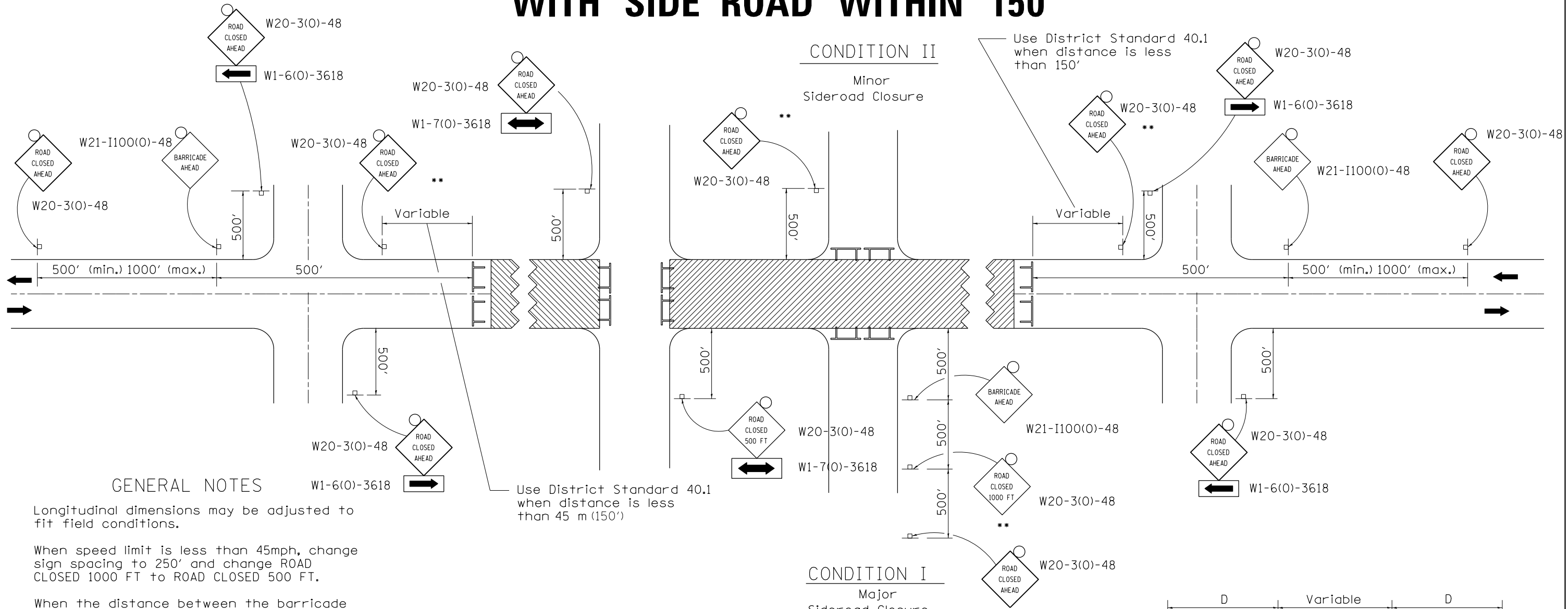
** Where local access is to be maintained, barricades are to be set up as shown in Road Closed to thru traffic. Type III Barricades and R11-2-4830 signs shall be as shown in "Road Closed To All Traffic" detail on Highway Standard 701901.

All dimensions are in inches unless otherwise shown.

TYPICAL APPLICATION FOR ROAD CLOSURE

FILE NAME =	USER NAME = *USER*	DESIGNED -	REVISED - 10-17-11	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REGION 2 / DISTRICT 2 STANDARD			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FILEL		DRAWN -	REVISED -					742	(32, 33) R-1	WINNEBAGO	705	577
	PLOT SCALE = *SCALE*	CHECKED -	REVISED -		SCALE:			SHEET NO. 13 OF 19 SHEETS		STA.	TO STA.	
	PLOT DATE = *DATE*	DATE -	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT CONTRACT NO. 64821							

TRAFFIC CONTROL FOR ROAD CLOSURE WITH SIDE ROAD WITHIN 150'



GENERAL NOTES

Longitudinal dimensions may be adjusted to fit field conditions.

When speed limit is less than 45mph, change sign spacing to 250' and change ROAD CLOSED 1000 FT to ROAD CLOSED 500 FT.

When the distance between the barricade and the intersection is between 1500' and 2000', the advance sign shall be placed at the intersection. When the distance between the barricade and the intersection is over 2000', an additional sign shall be placed at the intersection. The additional sign shall give the distance to the barricade in miles or fractions of a mile.

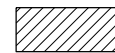


Side roads requiring all three signs as shown in CONDITION I (Major Sideroad Closure), shall be listed in the special provision.

** Where local access is to be maintained, barricades are to be set up as shown above in Road Closed to thru traffic.

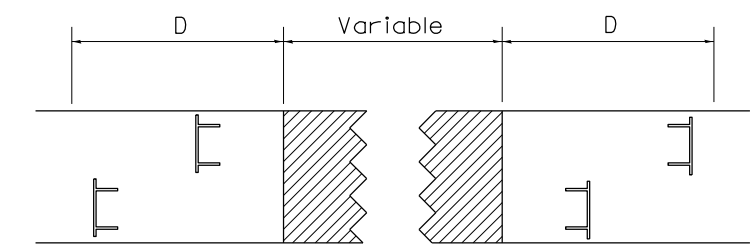
Type III Barricades and R11-2-4830 signs shall be as shown in "Road Closed To All Traffic" detail on Highway Standard 701901.

Use District Standard 40.1 when distance is less than 45 m (150')

SYMBOLS

-  Work area
-  Type III Barricade with Flashers
-  Sign with flashing light

All dimensions are in inches unless otherwise shown.



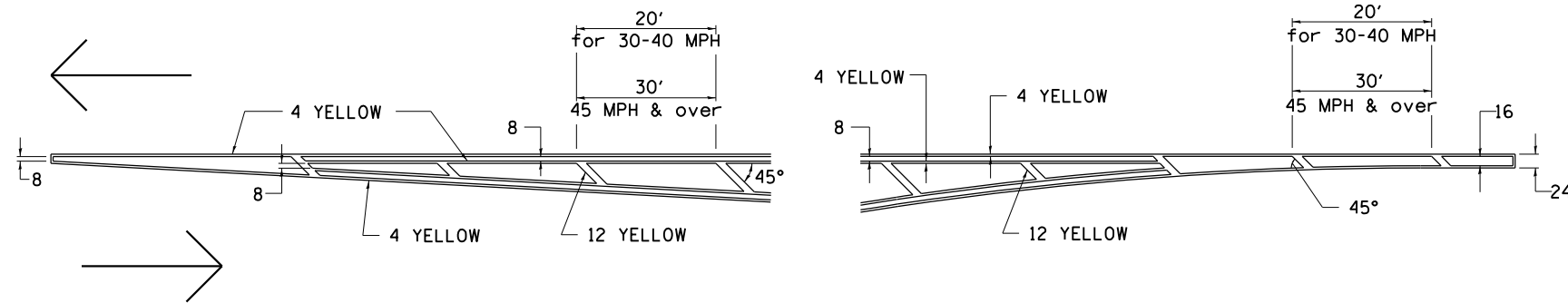
Type III Barricades and R11-4-4830 signs shall be as shown in "Road Closed To Thru Traffic" detail on Highway Standard 701901. If the distance "D" exceeds 2000' an additional set of barricades and R11-4-4830 shall be placed at each end of the work area.

TYPICAL APPLICATION FOR ROAD CLOSURE WITH SIDE ROAD WITHIN 150' OF CLOSURE

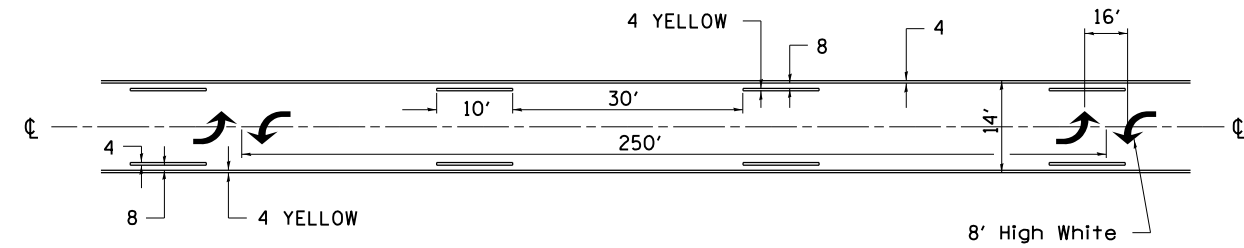
FILE NAME =	USER NAME = *USER*	DESIGNED -	REVISED - 10-17-11	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REGION 2 / DISTRICT 2 STANDARD		F.A. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
*FILE#		DRAWN -	REVISED -									705
	PLOT SCALE = *SCALE*	CHECKED -	REVISED -		SCALE:		SHEET NO. 14 OF 19 SHEETS	STA.	TO STA.	CONTRACT NO.		
	PLOT DATE = *DATE*	DATE -	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							

TYPICAL PAVEMENT MARKINGS

TYPICAL PAVEMENT MARKING FOR FLUSH MEDIAN AT LEFT TURN LANE

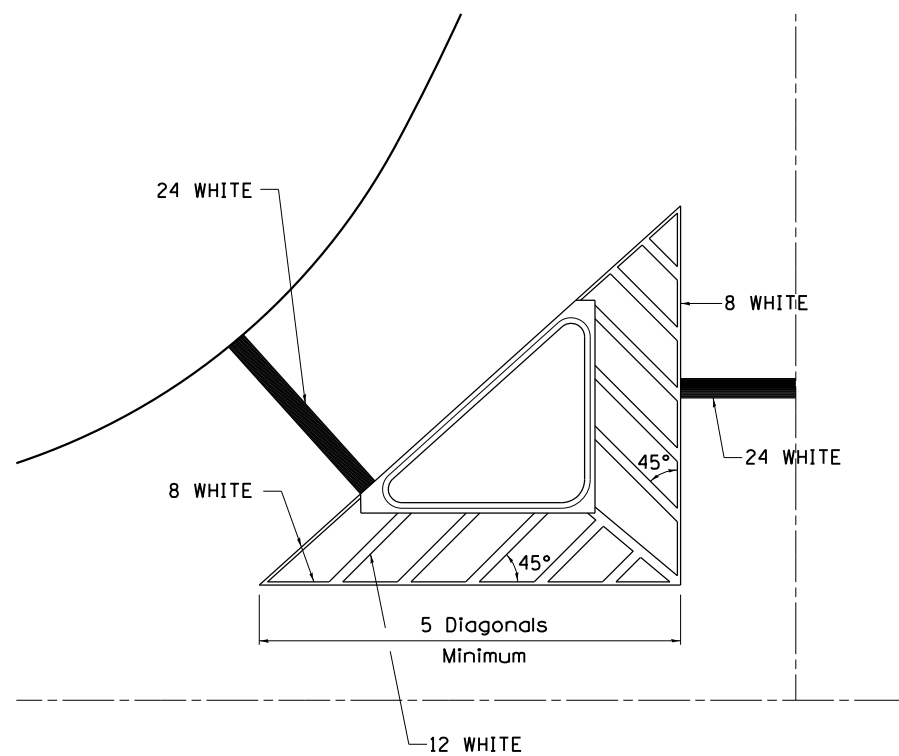


MEDIAN PAVEMENT MARKING

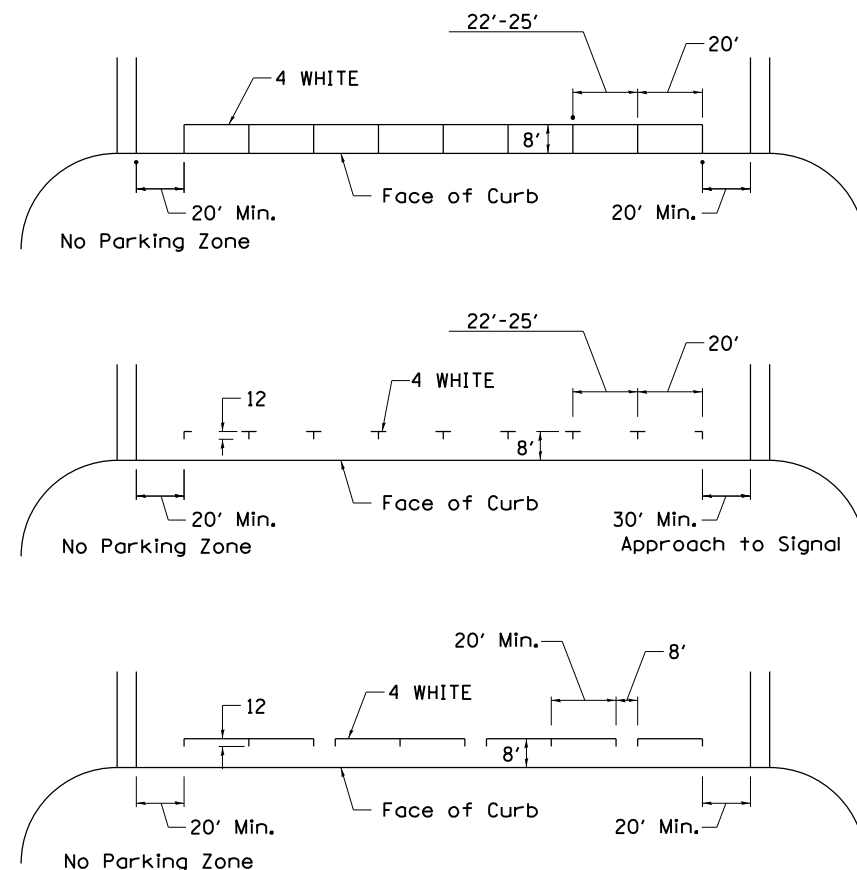


•• ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

TYPICAL ISLAND OFFSET SHOULDER WIDTH

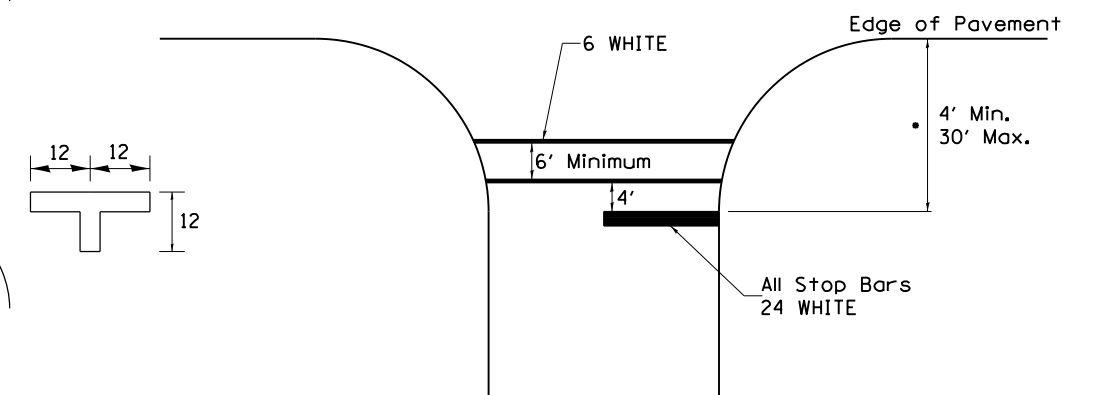


TYPICAL PARKING SPACING



STANDARD CROSSWALK MARKING

See Schedules for Locations

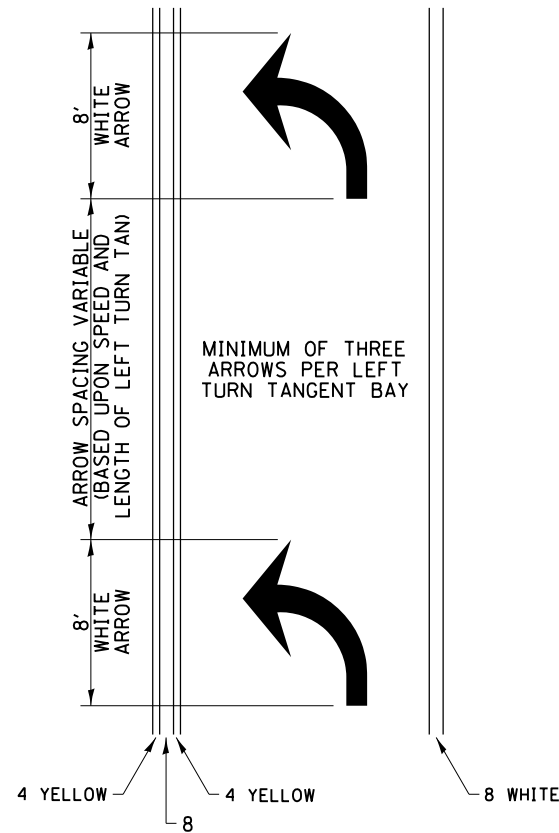


• Distance to the nearest edge of the intersecting roadway in the absence of a marked crosswalk.

FILE NAME =	USER NAME = #USER*	DESIGNED -	REVISED - 3-5-12	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REGION 2 / DISTRICT 2 STANDARD		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILE#		DRAWN -	REVISED -		742	(32, 33) R-1	WINNEBAGO	705	579		
		CHECKED -	REVISED -				CONTRACT NO. 64821				
		DATE -	REVISED -		SCALE:	SHEET NO. 15 OF 19 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	

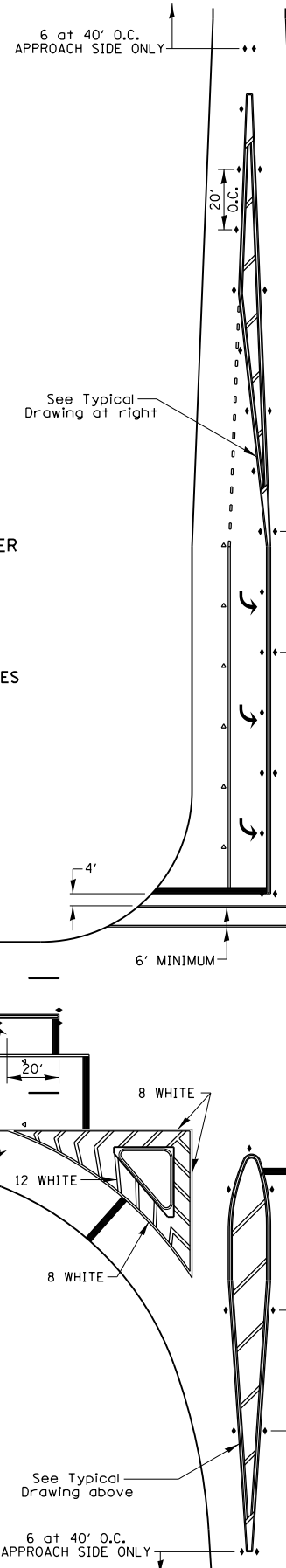
TYPICAL PAVEMENT MARKINGS

ARROW LAYOUT

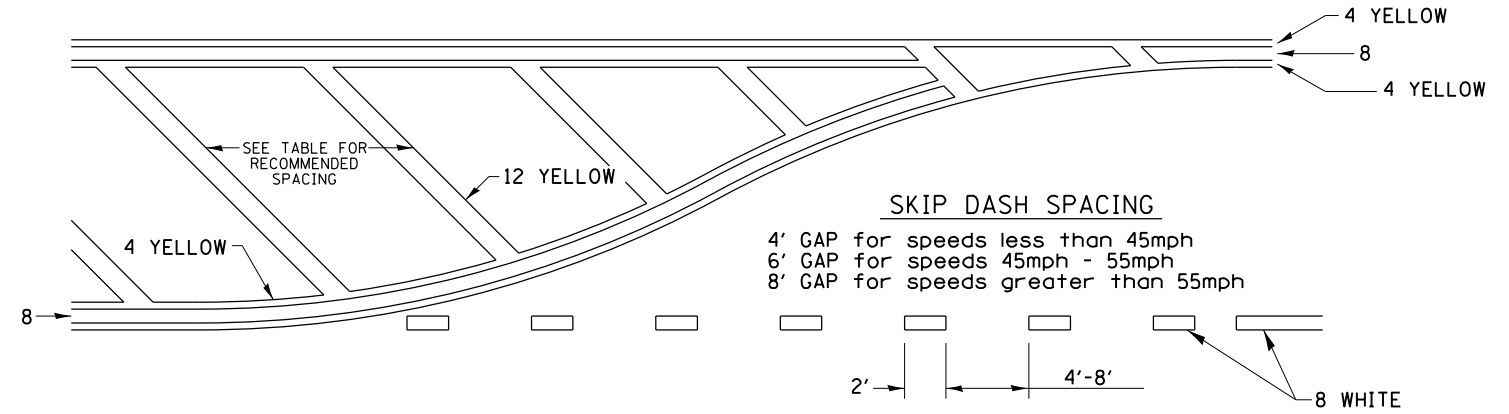


- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER
- ◆ TWO-WAY AMBER MARKER

ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.



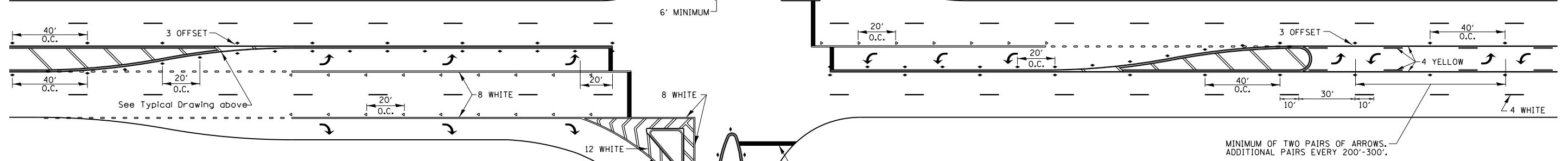
TYPICAL PAVEMENT MARKING FOR FLUSH MEDIAN



RECOMMENDED SPACING BETWEEN DIAGONALS (IN FEET)

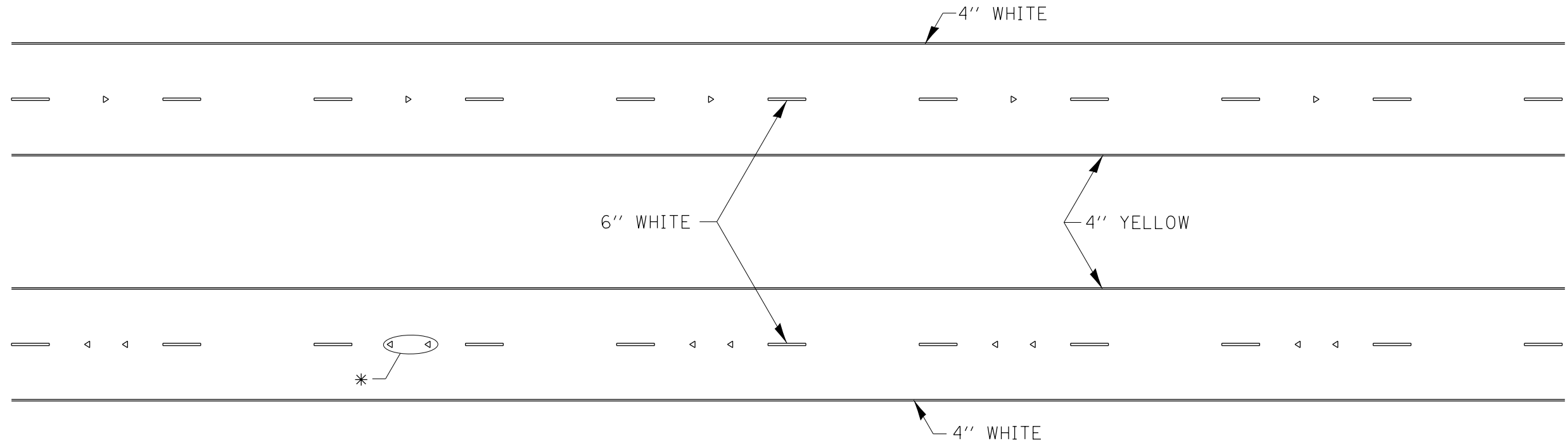
Speed Limit Range	Continuous Median Area	Intersection Channelization	Objects (Islands)
less than 30MPH	50'	15'	10'
30-40MPH	75'	20'	15'
45MPH & over	75'	30'	20'

NOTE: if the spacing recommended in the Table does not permit at least five diagonal lines in the area being marked, the spacing from the next lowest speed range should be used. The recommended spacing is measured parallel to the pavement center line.



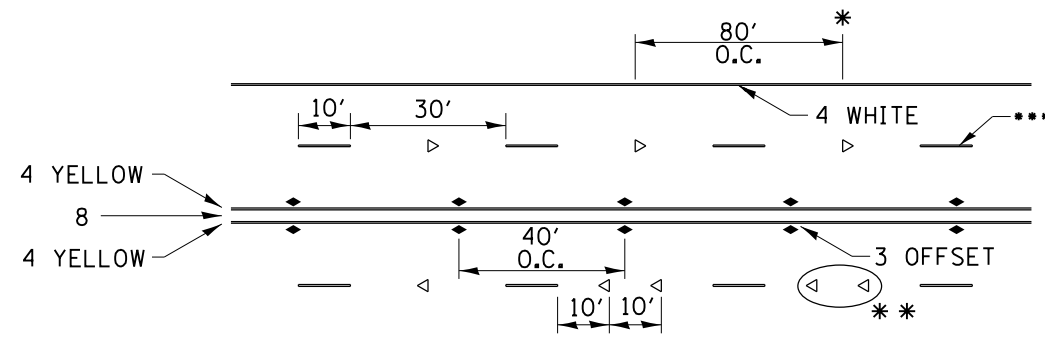
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*FILE#		DRAWN -	REVISED -		SCALE:	SHEET NO. 16 OF 19 SHEETS	STA.	742	(32, 33) R-1	WINNEBAGO	705	580
		CHECKED -	REVISED -				TO STA.	CONTRACT NO. 64821				
		DATE -	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							

TYPICAL PAVEMENT MARKINGS



* SEE HIGHWAY STANDARD 781001 FOR SPACING DETAILS.
USE DOUBLE MARKERS WHEN ADT \geq 25,000.

MULTI-LANE / DIVIDED

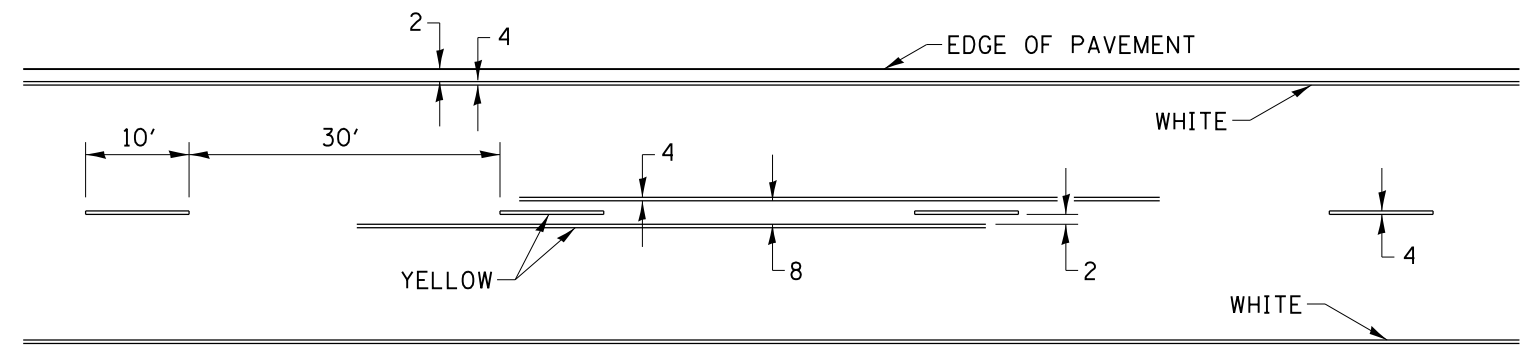


- * REDUCE TO 40' O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 MPH LOWER THAN POSTED SPEEDS.
- ** USE DOUBLE MARKERS WHEN ADT \geq 25,000
- *** CENTERLINE SKIP DASH PAVEMENT MARKING SPEED LIMIT LESS THAN 40 MPH USE 4" LINE SPEED LIMIT 40 MPH AND OVER USE 6" LINE

MULTI-LANE / UNDIVIDED & ONE WAY

(FOR MULTI-LANE UNDIVIDED HIGHWAYS USE THIS
DETAIL NOT HIGHWAY STANDARD 781001)

TYPICAL PAVEMENT MARKING FOR TWO LANE SECTION - NO PASSING ZONES

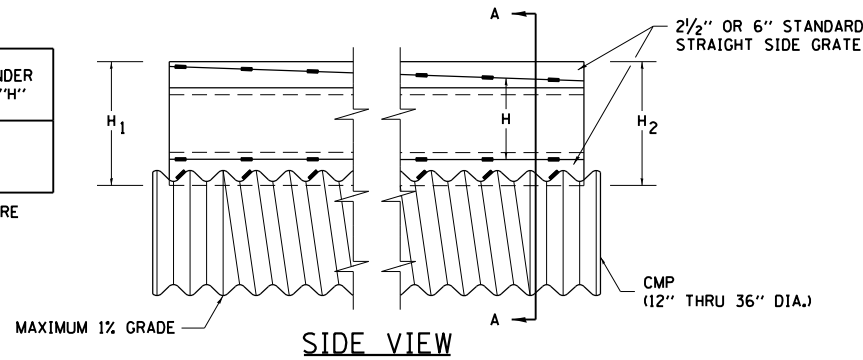


SYMBOLS

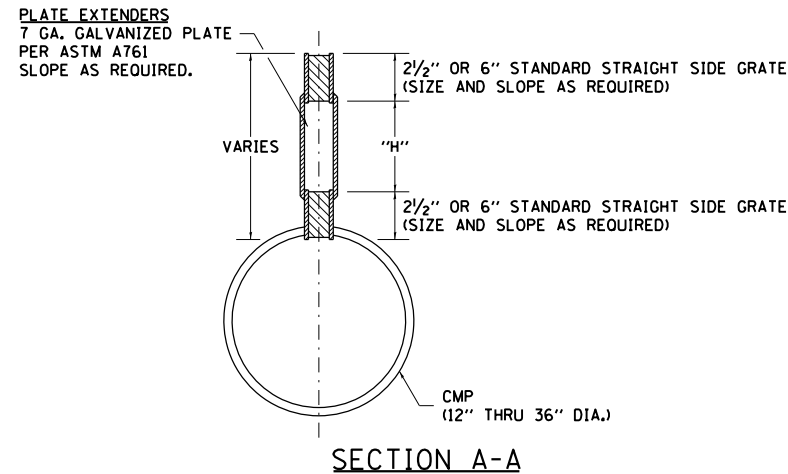
FILE NAME =	USER NAME = Hogensonjd	DESIGNED -	REVISED - 3-5-12	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REGION 2 / DISTRICT 2 STANDARD			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\dot\hogensonjd\0328875\0264821-sht-details-district.dgn	DRAWN -	REVISED -	742					(32, 33) R-1	WINNEBAGO	705	581	
PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -	SCALE:			SHEET NO. 17 OF 19 SHEETS	STA.	TO STA.	CONTRACT NO. 64821			
PLOT DATE = Thu Dec 13 12:02:48 2012	DATE -	REVISED -	TYPICAL PAVEMENT MARKINGS			SHEET 3 OF 3			41.1			

SLOTTED DRAIN PIPE

LOADING CONDITION	MAX. EXTENDER HEIGHT - "H"
H20/H25 • 750 PSI CONCRETE • 125 PSI TIRE PRESSURE	19"



DETAIL WITH VARIABLE HEIGHT GRATE



SECTION A-A

GENERAL

Class SI Concrete shall be used throughout. This specification covers Slotted Drain used for the removal of water as shown on the plans. The Slotted Drain shall be Corrugated Pipe Culvert with Integral Slotted Drains. Before placing the concrete adjacent to the pipe, the slot shall be covered by either thin, flat metal sheeting or by a board notched to fit over the grate bars. This covering must fit closely in the slot to prevent entry of concrete into the pipe. Paving over the slotted drain will then be one continuous operation over the protected drain. The protection for the drain slot shall then be removed. The pipe shall drain into the side of the inlet. The opening where the slot is removed shall be covered to prevent concrete from entering the pipe. The Corrugated Steel Pipe used in the Slotted Drain shall meet the requirements of AASHTO M36/ASTM A760. The CMP shall be ALUMINIZED STEEL Type 2. The diameter shall be as shown on the plans. Steel grating shall meet the galvanizing requirements of AASHTO M111. This work will be paid for at the contract unit price per foot for SLOTTED DRAIN of the pipe diameter specified WITH VARIABLE SLOT, or SLOTTED DRAIN, of the pipe diameter specified, WITH 6" SLOT, and shall include concrete and grating for depth specified on plans. Use approved end cap to prevent concrete entry into the pipe during gutter construction on the upstream end of the pipe.

CONNECTIONS

The Corrugated Steel Pipe shall have a minimum of two rerolled annular ends. The Slotted Drain bands shall be modified HUGGER Bands to secure the pipe and prevent infiltration of the backfill. When the Slotted Drain is banded together, the adjacent grates shall have a maximum 3" gap.

GRATES

The grates shall be manufactured from ASTM A670, Grade 36 steel. The spacers and bearing bars (sides) shall be 3/16" material ±0.008". The spacers shall be on 6" centers and welded on both sides to each bearing bar (sides) with four (4) 1-1/4" long 3/16" fillet welds on each side of the bearing bar. The plate extender shall be 7 gage steel meeting ASTM A761. The engineer may call for tensile strength tests on the grate if the grate is not in compliance with the above spacer specifications. If tensile strength tests are called for, minimum results for an in-place spacer pulled perpendicular to the bearing bar shall be:
T = 12,000 pounds for 2-1/2" grate
T = 15,000 pounds for 6" grate

GALVANIZING

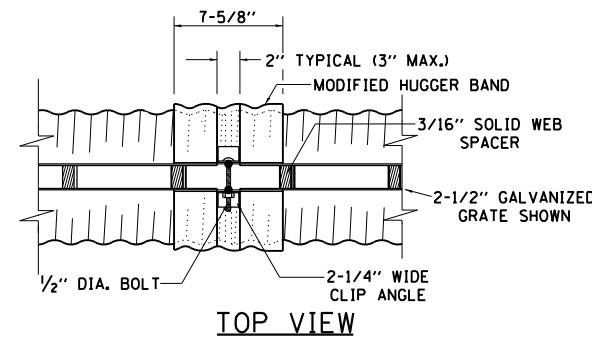
The grate and plate extenders shall be galvanized in accordance with ASTM A123 except with a 2 oz. galvanized coating.

GRATE ATTACHED TO CSP

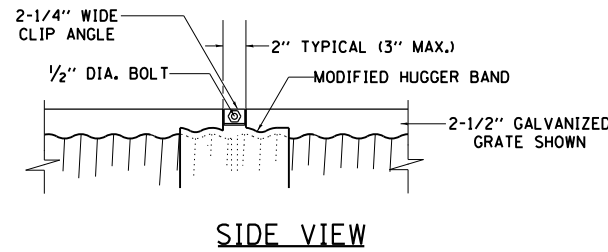
The grate shall be fillet welded with a minimum weld 1" long to the CSP on each side of the grate at every other corrugation.

TOLERANCES - FINISHED SLOTTED DRAIN - 20' LENGTHS

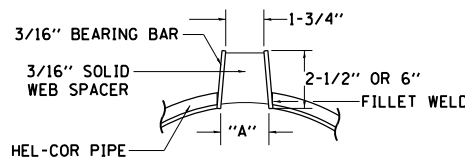
Vertical Bow = ± 3/8"
Horizontal Bow = ± 5/8"
Twist = ± 1/2"



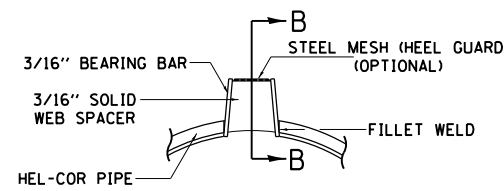
TOP VIEW



SIDE VIEW



SECTION A-A STANDARD DETAIL



SECTION A-A DETAIL WITH MESH

(TRAPEZOIDAL GALVANIZED GRATE SHOWN)

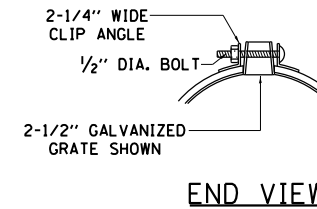
GAGE OF PIPE	DIAMETER OF PIPE					
	12"	15"	18"	24"	30"	36"
16	X	X	X	X	X	X
14	X	X	X	X	X	X
12	N.A.	N.A.	N.A.	N.A.	X	X

GRATE TYPE	"A"
VERT	2-1/2"
TRAP	6"
TRAP	3"

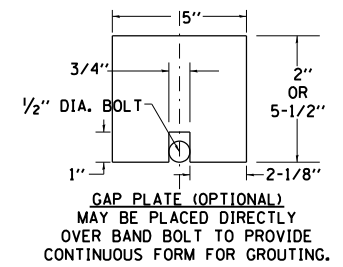
VERT = VERTICAL
TRAP = TRAPEZOIDAL

SLOTTED DRAIN NOTES

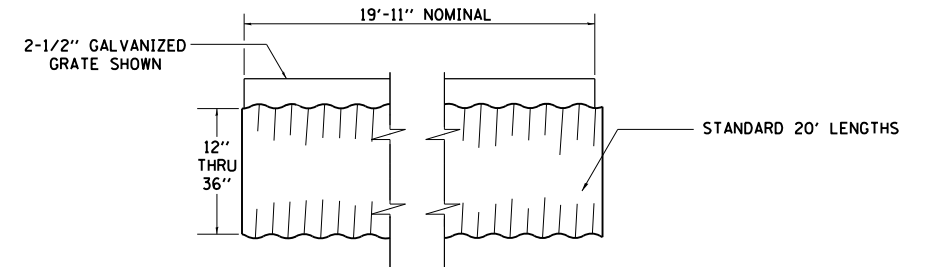
- GRATING IS AVAILABLE IN DEPTHS OF 2-1/2" AND 6".
- VERTICAL GRATING (STRAIGHT SIDES) WITH VERTICAL SPACERS IS ALSO AVAILABLE.
- FOR 6" VERTICAL & TRAPEZOIDAL REQUIREMENTS, THE SLOTTED DRAIN BAND MAY BE FURNISHED WITH THE 4" TECHCO BAND ANGLE.
- DIMENSIONS ARE SUBJECT TO MANUFACTURING TOLERANCES.
- DIMENSIONS FOR H₁ AND H₂ AS REQUIRED.
- H₁ AND H₂ MEASURED FROM TOP OF GRATE TO BOTTOM OF GRATE.



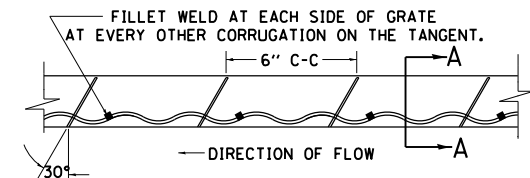
END VIEW



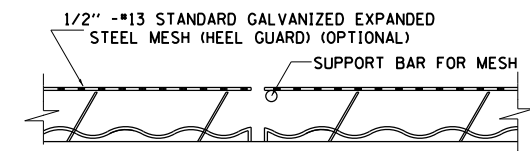
GAP PLATE (OPTIONAL) MAY BE PLACED DIRECTLY OVER BAND BOLT TO PROVIDE CONTINUOUS FORM FOR GROUTING.



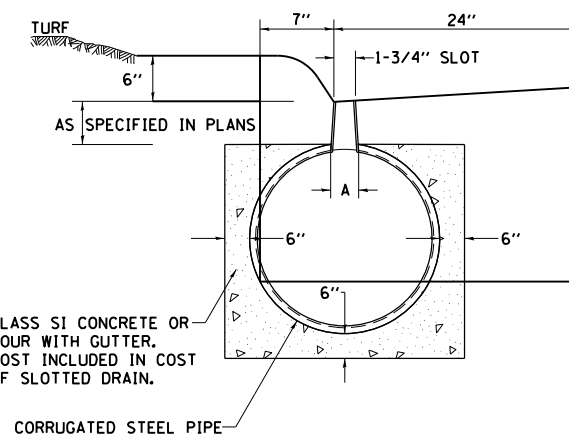
TYPICAL PIPE SECTION



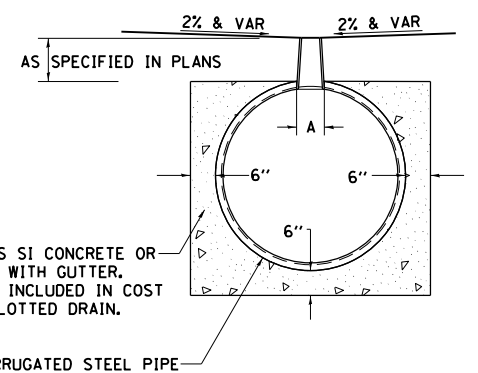
GRATE WELDING DETAIL



SECTION B-B



DETAIL FOR CURB & GUTTER



DETAIL FOR CROSSOVERS, DRIVEWAYS, OR PARKING LOTS

ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED - 10-18-11
#FILE#		DRAWN -	REVISED -
	PLOT SCALE = #SCALE#	CHECKED -	REVISED -
	PLOT DATE = #DATE#	DATE -	REVISED -

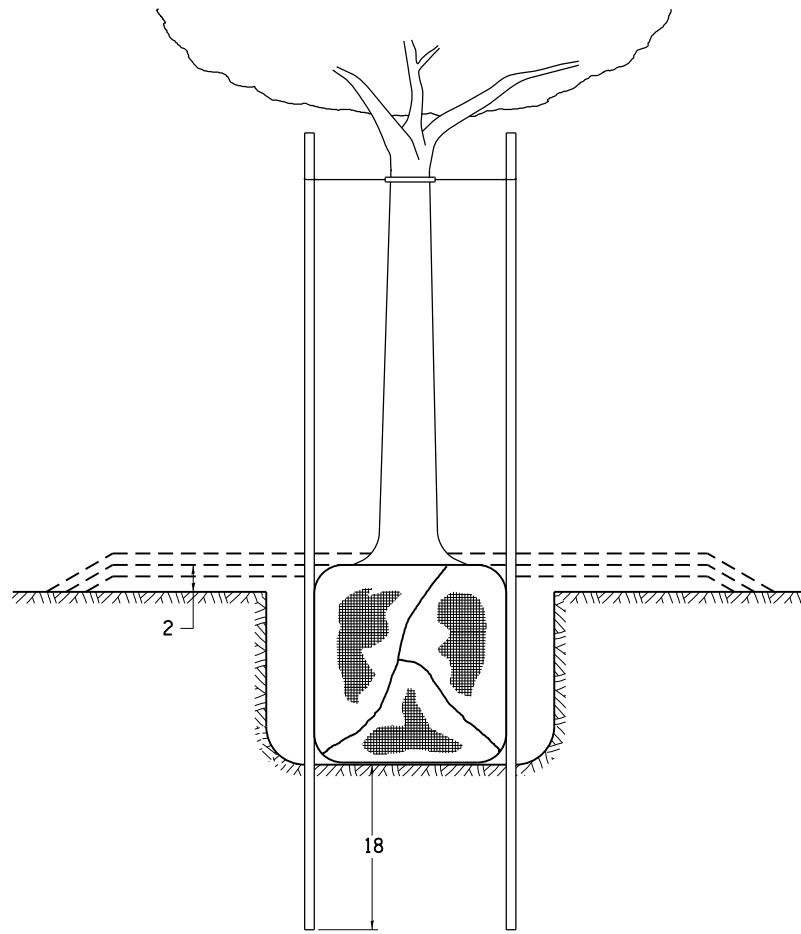
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REGION 2 / DISTRICT 2 STANDARD

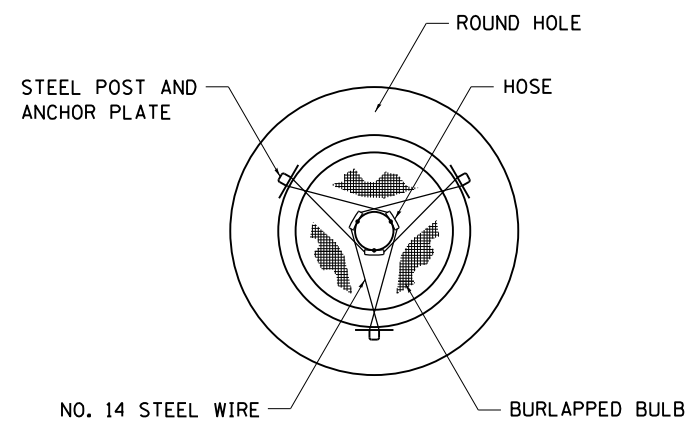
SCALE: SHEET NO. 18 OF 19 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32, 33) R-1	WINNEBAGO	705	582
CONTRACT NO. 64821				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

DETAILS OF PLANTING AND BRACING TREES

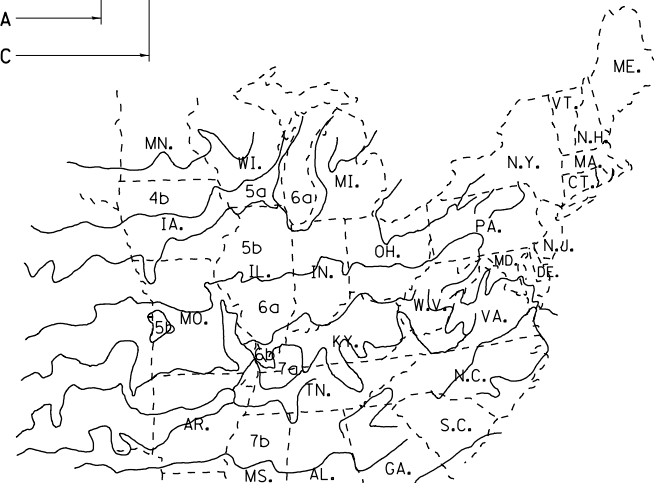
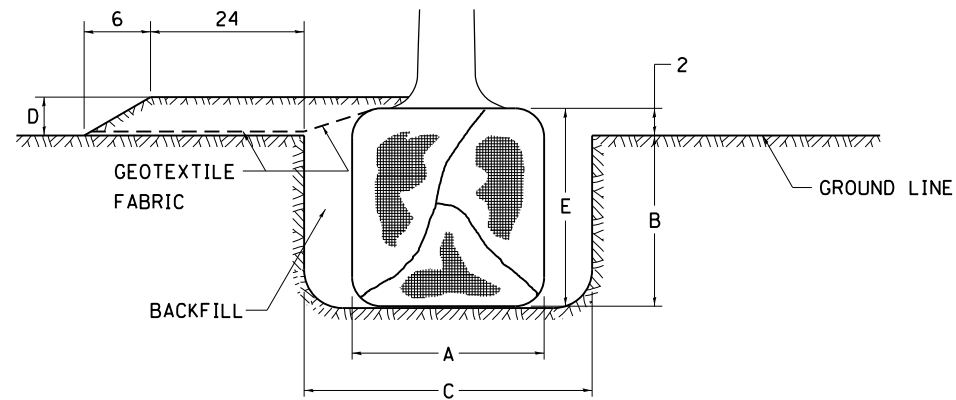


TREES SMALLER THAN 4 1/2 IN DIAMETER



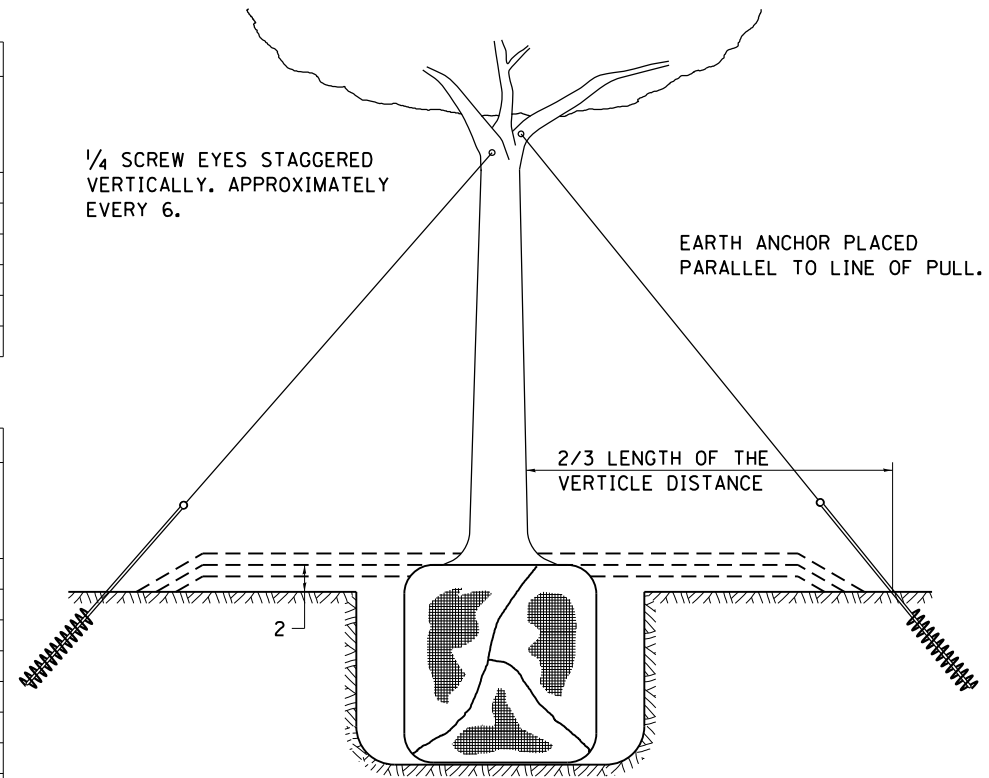
SMALL	A	B	C	D	E	F
TREE SIZE	DIAMETER OF BALL OR ROOT SYS.	DEPTH OF HOLE EXCAVATION	WIDTH OF HOLE EXCAVATION	THICKNESS OF MULCH COVER	DEPTH OF BALL OR ROOT SYS.	VOLUME OF MULCH COVER CU. YDS.
5'-6'	16	10	30	4	12	0.54
5'-6' BB	16	10	30	4	12	0.54
6'-7' BB	18	12	30	4	14	0.54
7'-8' BB	20	11	30	4	13	0.54
8'-10' BB	24	14	36	4	16	0.61
10'-12' BB	26	15	36	4	17	0.61

LARGE	A	B	C	D	E	F
TREE SIZE	DIAMETER OF BALL OR ROOT SYS.	DEPTH OF HOLE EXCAVATION	WIDTH OF HOLE EXCAVATION	THICKNESS OF MULCH COVER	DEPTH OF BALL OR ROOT SYS.	VOLUME OF MULCH COVER CU. YDS.
0-2	20	11	36	4	13	0.61
2-2 1/2 BB	24	14	48	4	16	0.78
2 1/2-3 BB	28	17	48	4	19	0.78
3-3 1/2 BB	32	17	60	4	19	0.96
3 1/2-4 BB	36	20	60	4	22	0.96
4-4 1/2 BB	40	22	72	4	24	1.16
4 1/2-5 BB	44	24	72	4	26	1.16
5-5 1/2 BB	48	27	84	4	29	1.38

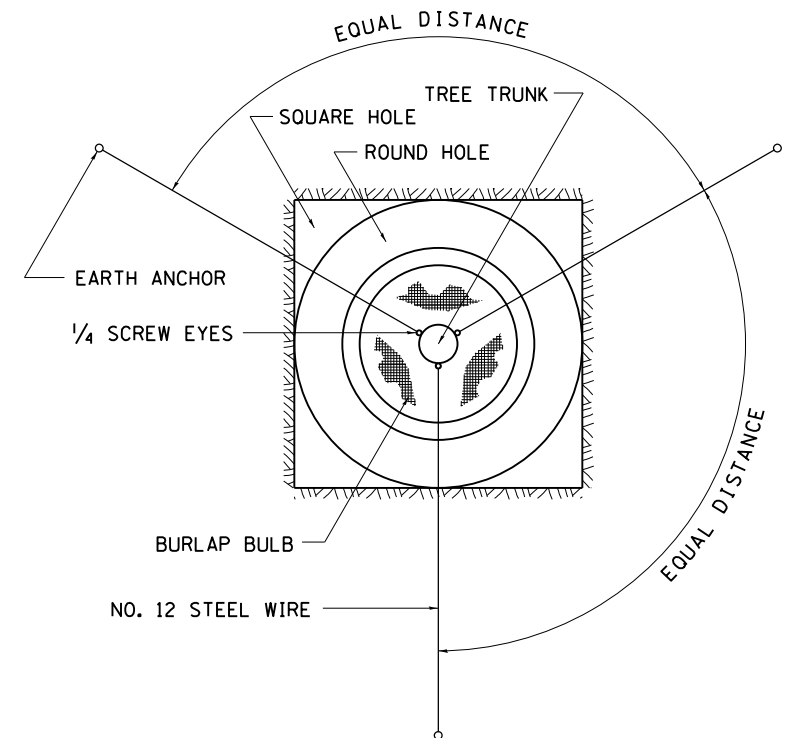


PLANT HARDINESS ZONE MAP

U.S. DEPARTMENT OF AGRICULTURE
AGRICULTURAL RESEARCH SERVICE
PUBLICATION NO. 814



TREES OVER 4 1/2 IN DIAMETER



ALL DIMENSIONS ARE IN INCHES
UNLESS OTHERWISE NOTED.

FILE NAME =	USER NAME = *USER*	DESIGNED -	REVISED - 10-18-11
*FILE#		DRAWN -	REVISED -
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

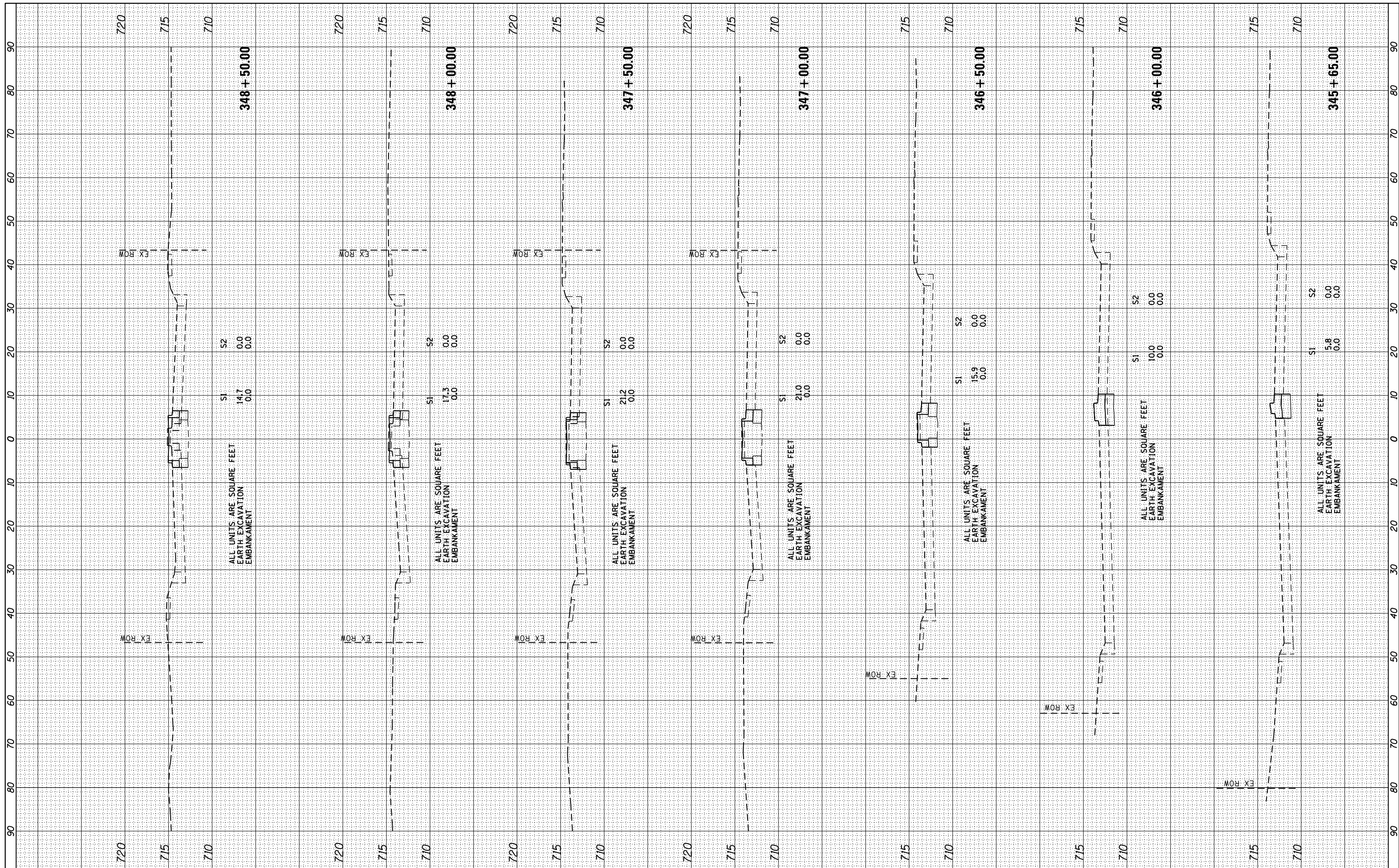
REGION 2 / DISTRICT 2 STANDARD

SCALE: SHEET NO. 19 OF 19 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32, 33) R-1	WINNEBAGO	705	583
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 64821	

FINAL SURVEY	SURVEYED	DATE
NO.	PLOTTED	
	TEMPLATE	
	AREAS	
	CHECKED	

ORIGINAL SURVEY	SURVEYED	DATE
NO.	PLOTTED	
	TEMPLATE	
	AREAS	
	CHECKED	



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

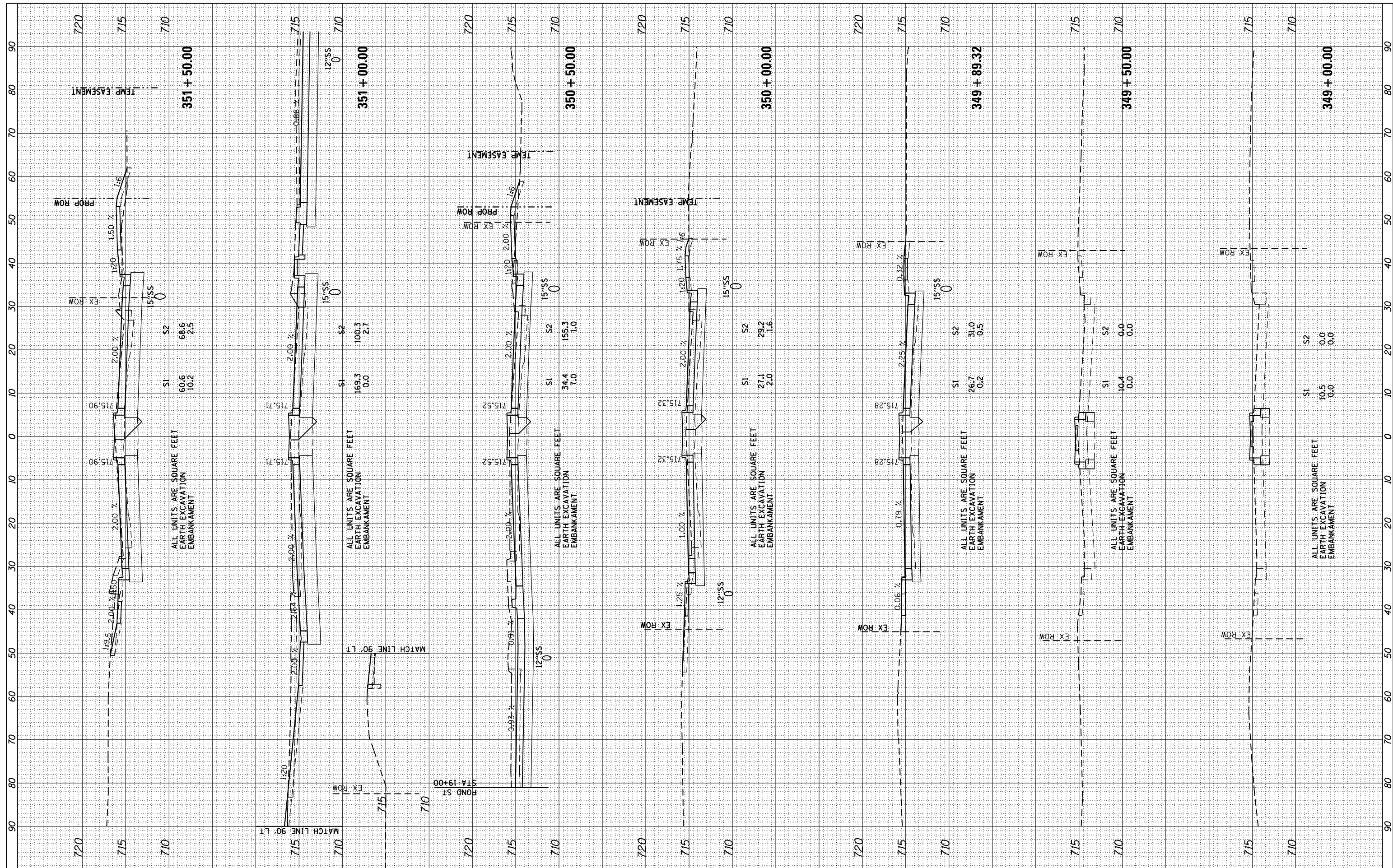
IL 2 (S MAIN ST) CROSS SECTIONS

SCALE: SHEET OF SHEETS STA. 345+65.00 TO STA. 348+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32, 33) R-1	WINNEBAGO	705	584
CONTRACT NO. 64821			ILLINOIS FED. AID PROJECT	

FINAL SURVEY	SURVEYED	BY	DATE
NO.	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NO.	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		



FILE NAME =	USER NAME = *USER*
FILEL	DESIGNED -
	DRAWN -
	CHECKED -
	DATE -

ALL UNITS ARE SQUARE FEET EARTH EXCAVATION EMBANKMENT	S1	60.6	10.2	S2	68.6	2.5
ALL UNITS ARE SQUARE FEET EARTH EXCAVATION EMBANKMENT	S1	169.3	0.0	S2	100.3	2.7
ALL UNITS ARE SQUARE FEET EARTH EXCAVATION EMBANKMENT	S1	34.4	7.0	S2	155.3	1.0
ALL UNITS ARE SQUARE FEET EARTH EXCAVATION EMBANKMENT	S1	27.1	2.0	S2	29.2	1.6
ALL UNITS ARE SQUARE FEET EARTH EXCAVATION EMBANKMENT	S1	26.7	0.2	S2	31.0	0.5
ALL UNITS ARE SQUARE FEET EARTH EXCAVATION EMBANKMENT	S1	10.4	0.0	S2	0.0	0.0
ALL UNITS ARE SQUARE FEET EARTH EXCAVATION EMBANKMENT	S1	10.5	0.0	S2	0.0	0.0

REVISIONS	REVISION NO.	DATE	DESCRIPTION
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-	-	-	-
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-	-	-	-

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

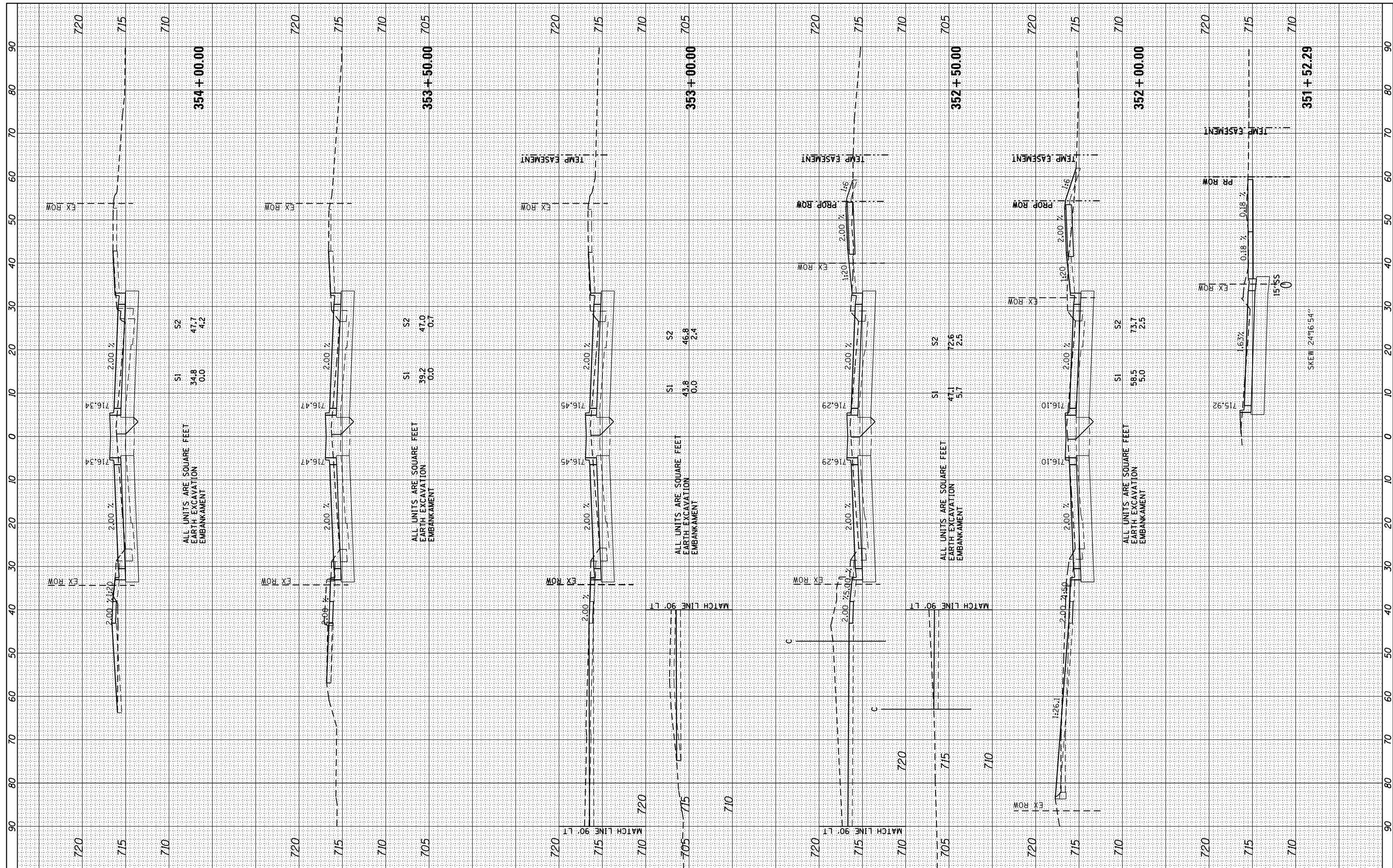
IL 2 (S MAIN ST) CROSS SECTIONS

SCALE: SHEET OF SHEETS STA. 349+00.00 TO STA. 351+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32, 33) R-1	WINNEBAGO	705	585
CONTRACT NO. 64821			ILLINOIS FED. AID PROJECT	

BY	DATE
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	AREAS CHECKED
	AREAS CHECKED

BY	DATE
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	AREAS CHECKED
	AREAS CHECKED



ALL UNITS ARE SQUARE FEET
EARTH EXCAVATION
EMBANKMENT

SI	S2
34.8	47.7
0.0	4.2

ALL UNITS ARE SQUARE FEET
EARTH EXCAVATION
EMBANKMENT

SI	S2
39.2	47.0
0.0	0.7

ALL UNITS ARE SQUARE FEET
EARTH EXCAVATION
EMBANKMENT

SI	S2
43.8	46.8
0.0	2.4

ALL UNITS ARE SQUARE FEET
EARTH EXCAVATION
EMBANKMENT

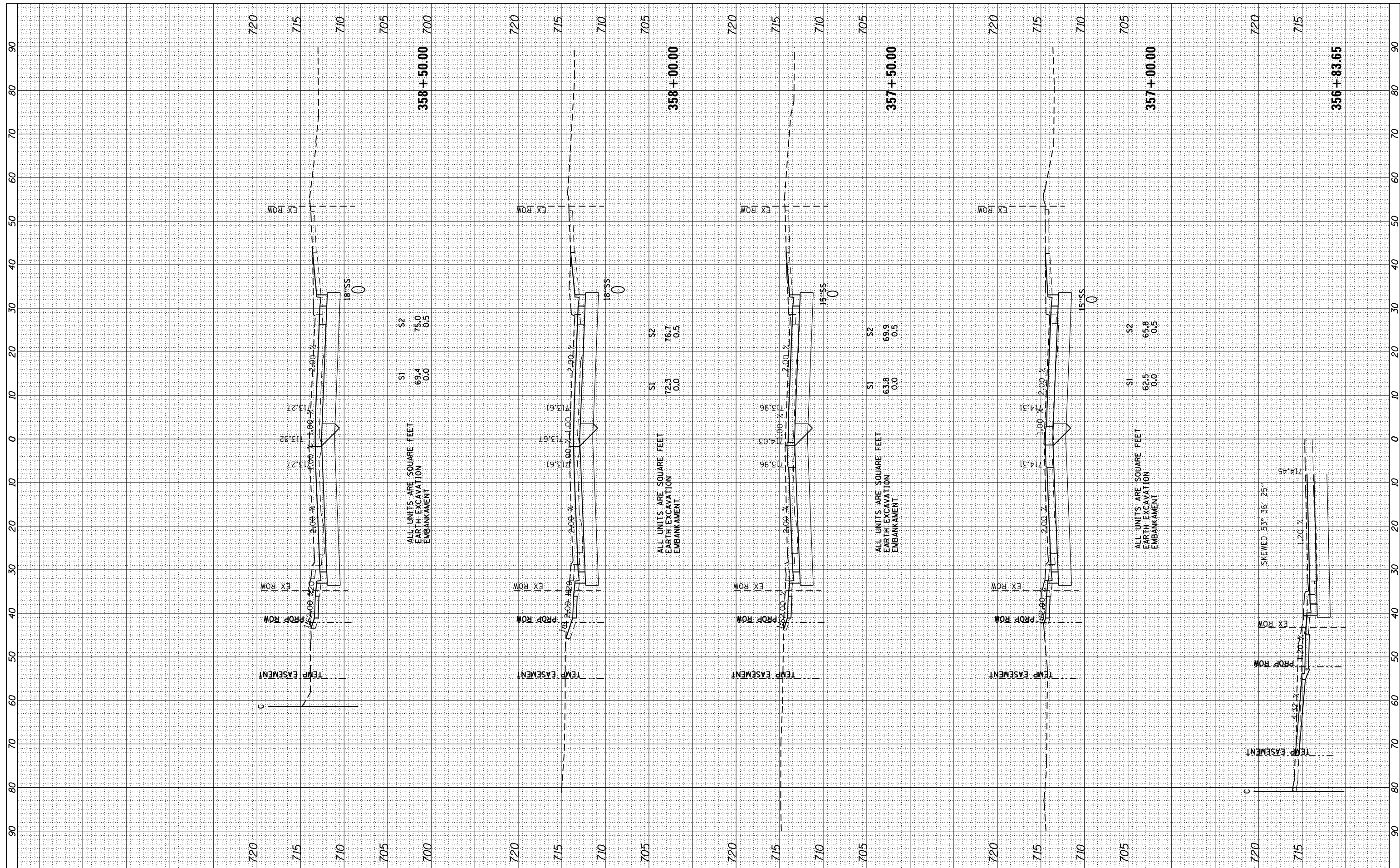
SI	S2
47.1	72.5
5.7	2.5

ALL UNITS ARE SQUARE FEET
EARTH EXCAVATION
EMBANKMENT

SI	S2
58.5	73.7
5.0	2.5

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		



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DESIGNED -
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DATE -

REVISIONS:
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

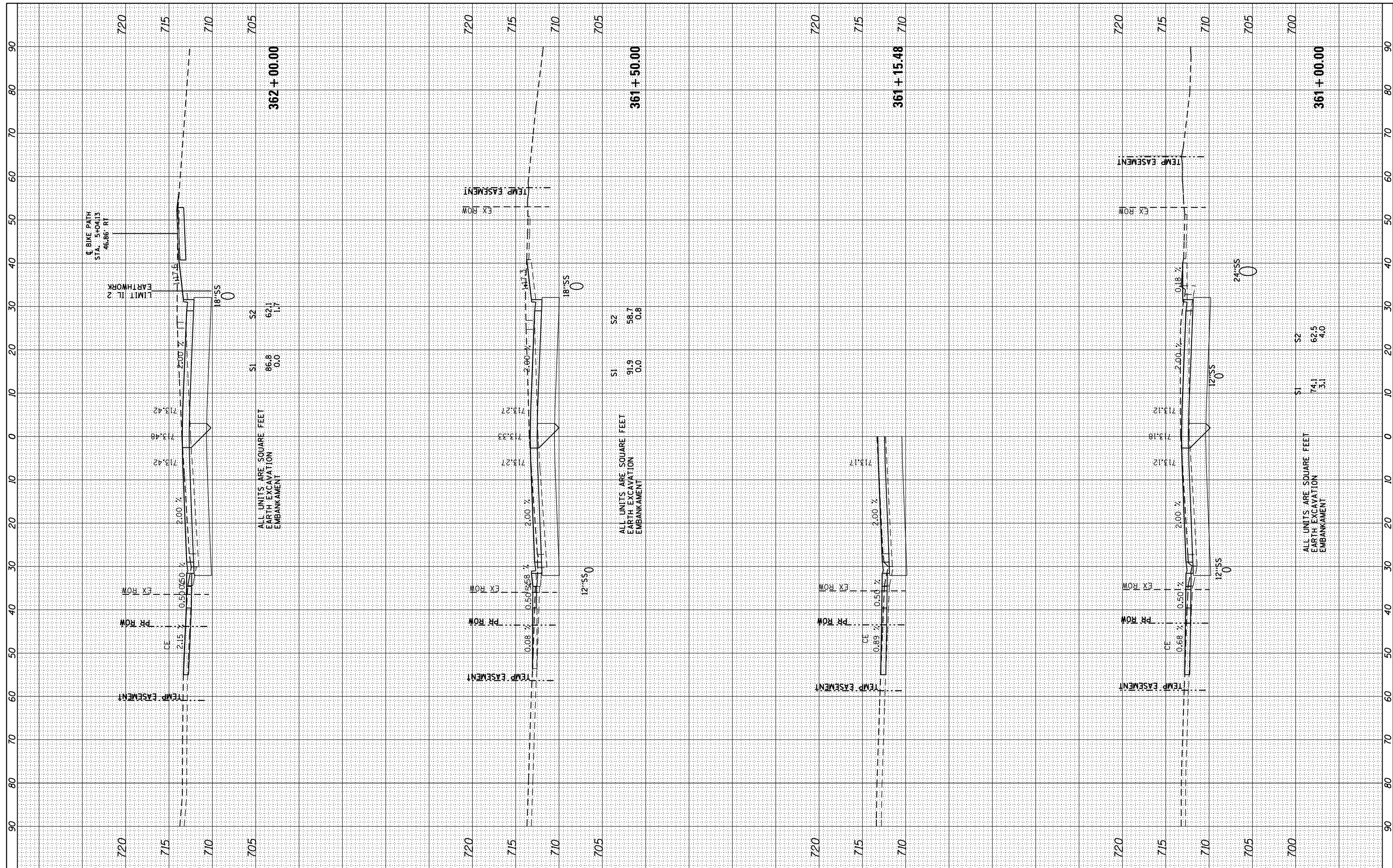
IL 2 (S MAIN ST) CROSS SECTIONS

SCALE: SHEET OF SHEETS STA. 356+83.65 TO STA. 358+50.00

F.A.P. RTE. 742	SECTION (32, 33) R-1	COUNTY WINNEBAGO	TOTAL SHEETS 705	SHEET NO. 588
ILLINOIS FED. AID PROJECT			CONTRACT NO. 64821	

FINAL SURVEY	SURVEYED	BY	DATE
NO.	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NO.	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		
	AREAS CHECKED		



FILE NAME =
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USER NAME = #USER#
 PLOT SCALE = #SCALE#
 PLOT DATE = #DATE#

DESIGNED -
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 CHECKED -
 DATE -

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

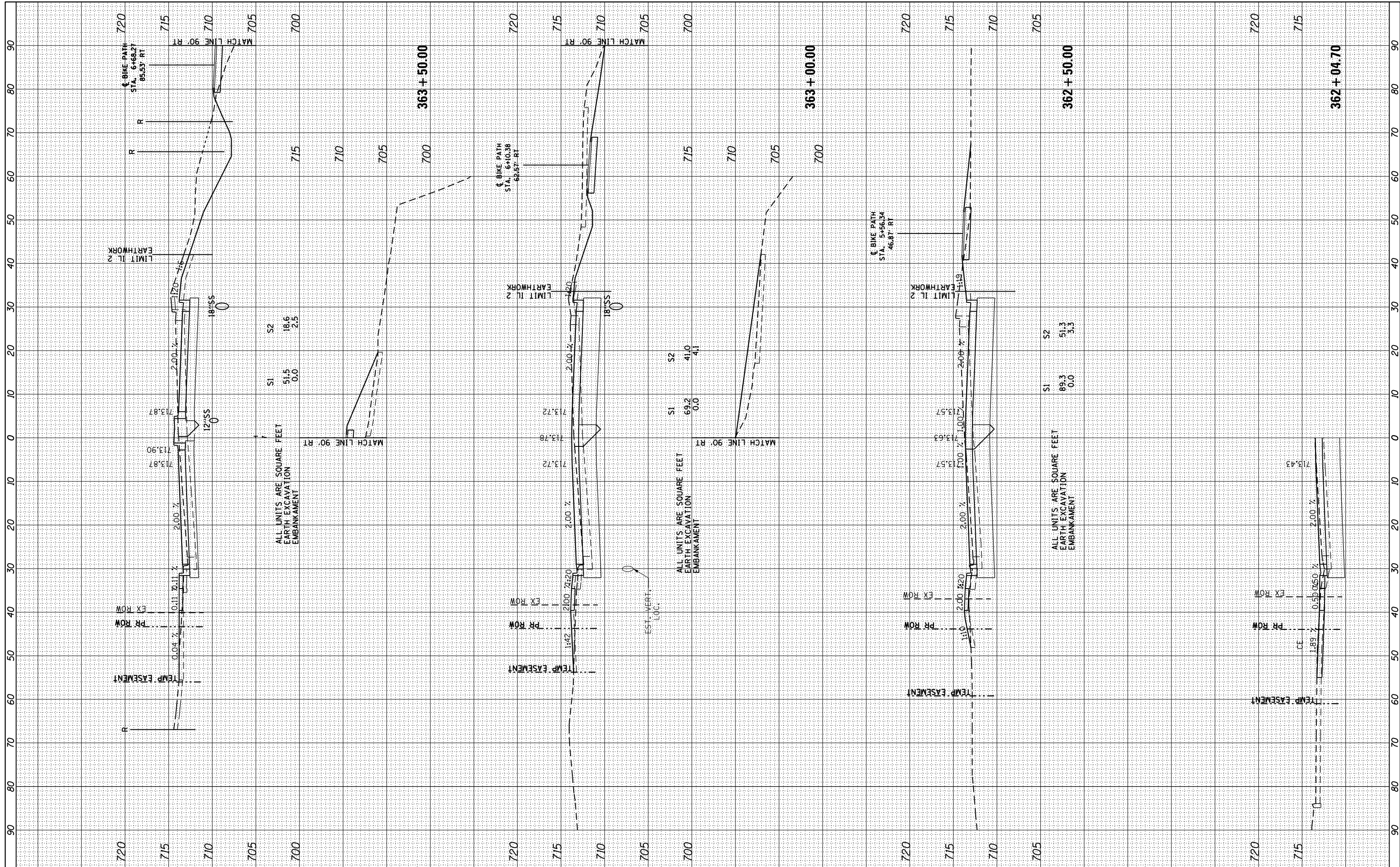
IL 2 (S MAIN ST) CROSS SECTIONS

SCALE: SHEET OF SHEETS STA. 361+00.00 TO STA. 362+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32, 33) R-1	WINNEBGAO	705	590
			CONTRACT NO.	64821
ILLINOIS FED. AID PROJECT				

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
NO.	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
NO.	AREAS CHECKED		



FILE NAME =
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USER NAME = *USER*
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 PLOT DATE = *DATE*

DESIGNED -
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

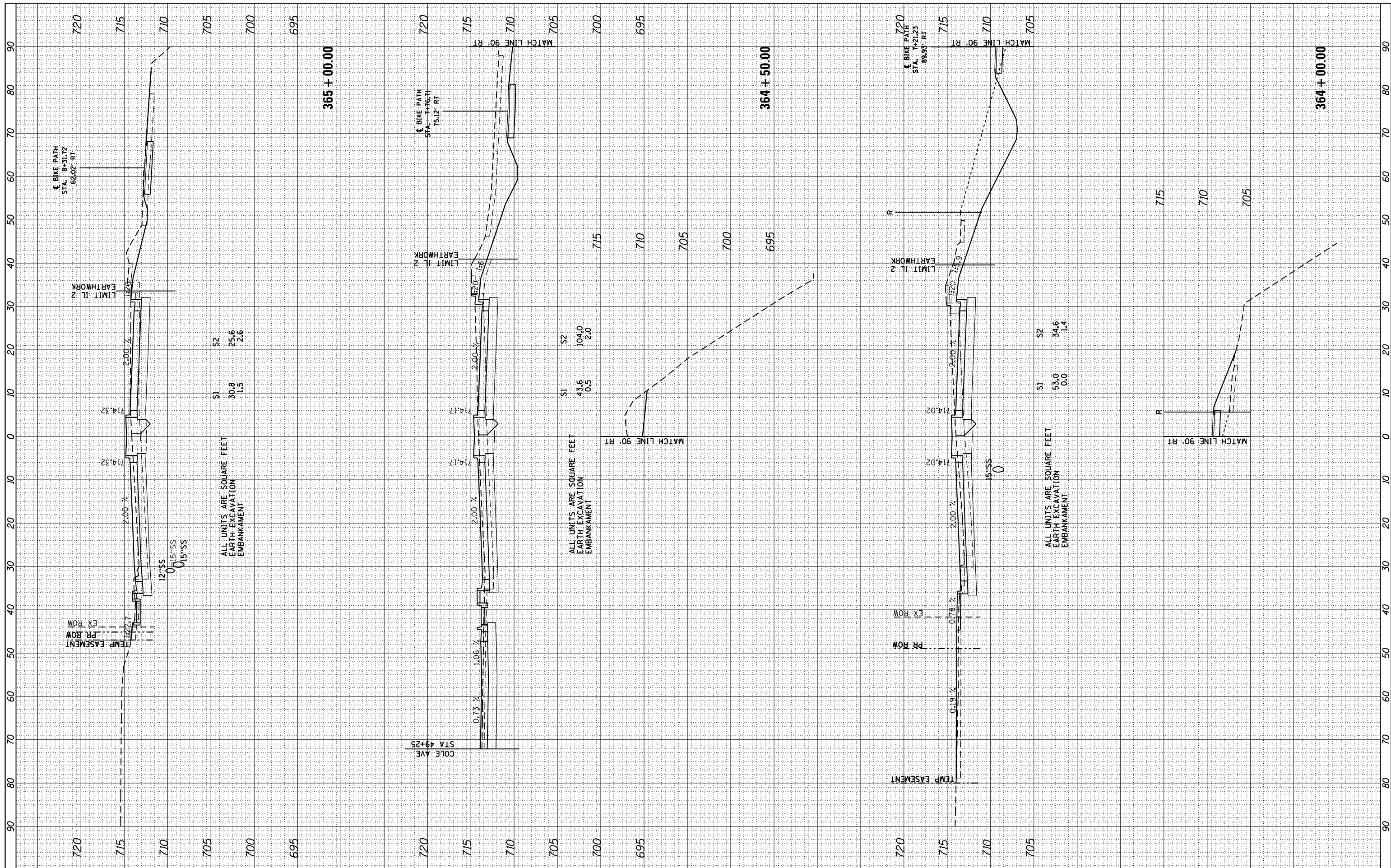
IL 2 (S MAIN ST) CROSS SECTIONS

SCALE: SHEET OF SHEETS STA. 362+04.70 TO STA. 363+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32, 33) R-1	WINNEBAGO	705	591
CONTRACT NO. 64821			ILLINOIS FED. AID PROJECT	

FINAL SURVEY	SURVEYED	BY	DATE
NO.	PLOTTED		
	TEMPLATE		
	AREAS		
	CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NO.	PLOTTED		
	TEMPLATE		
	AREAS		
	CHECKED		



ALL UNITS ARE SQUARE FEET
EARTH EXCAVATION
EMBANKMENT

S1	30.8
S2	25.6
	2.6

ALL UNITS ARE SQUARE FEET
EARTH EXCAVATION
EMBANKMENT

S1	43.6
S2	104.0
	2.0

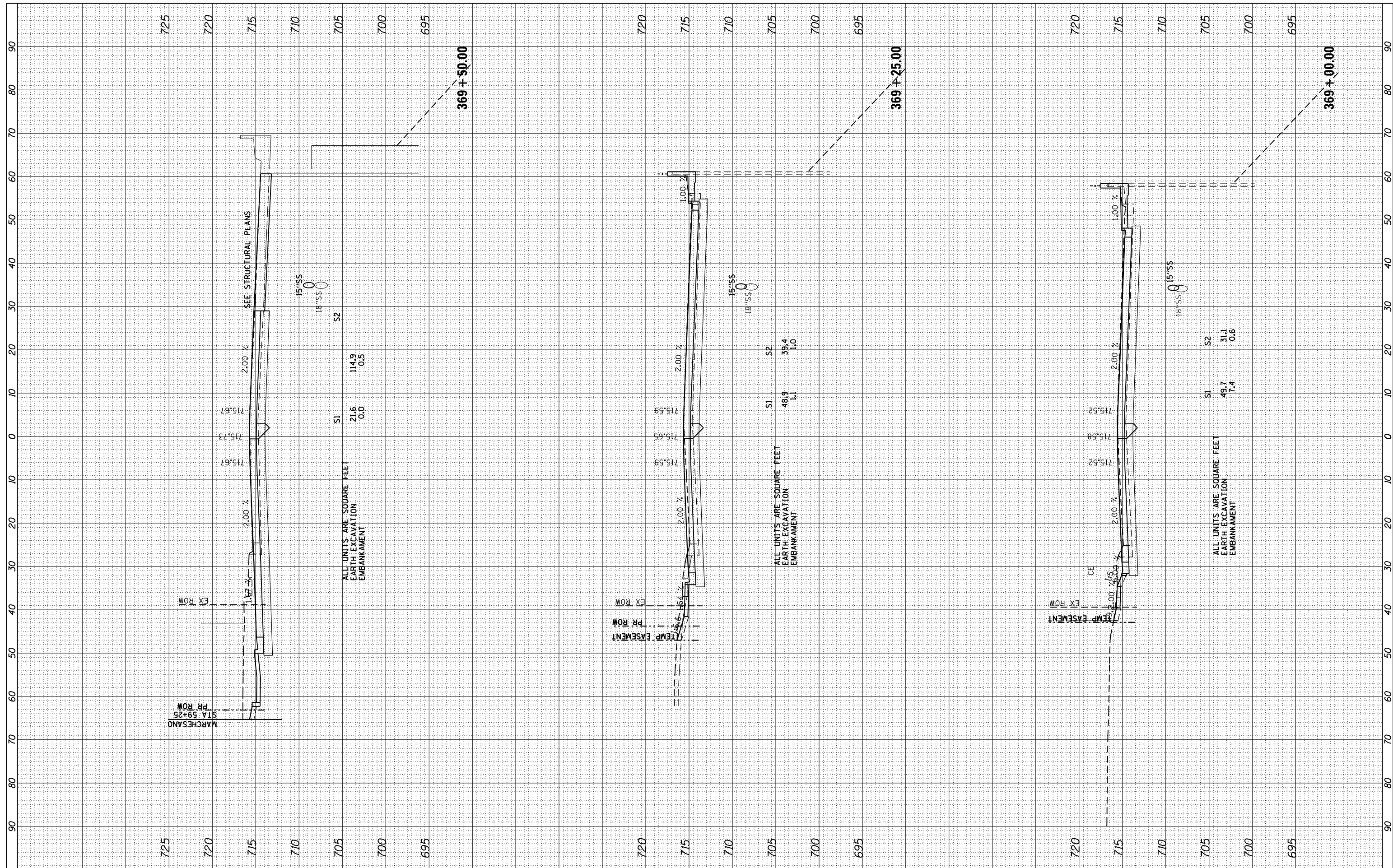
ALL UNITS ARE SQUARE FEET
EARTH EXCAVATION
EMBANKMENT

S1	55.0
S2	34.6
	1.4

FILE NAME =	USER NAME = *USER*	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL 2 (S MAIN ST) CROSS SECTIONS	F.A.P. R.T.E. 742	SECTION (32, 33) R-1	COUNTY WINNEBAGO	TOTAL SHEETS 705	SHEET NO. 592	
FILEL		DRAWN -	REVISED -			SCALE:	SHEET OF SHEETS	STA. 364+00.00 TO STA. 365+00.00	CONTRACT NO. 64821		ILLINOIS FED. AID PROJECT
PLOT SCALE = *SCALE*		CHECKED -	REVISED -								
PLOT DATE = *DATE*		DATE -	REVISED -								

FINAL SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

ORIGINAL SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE



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DATE -

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

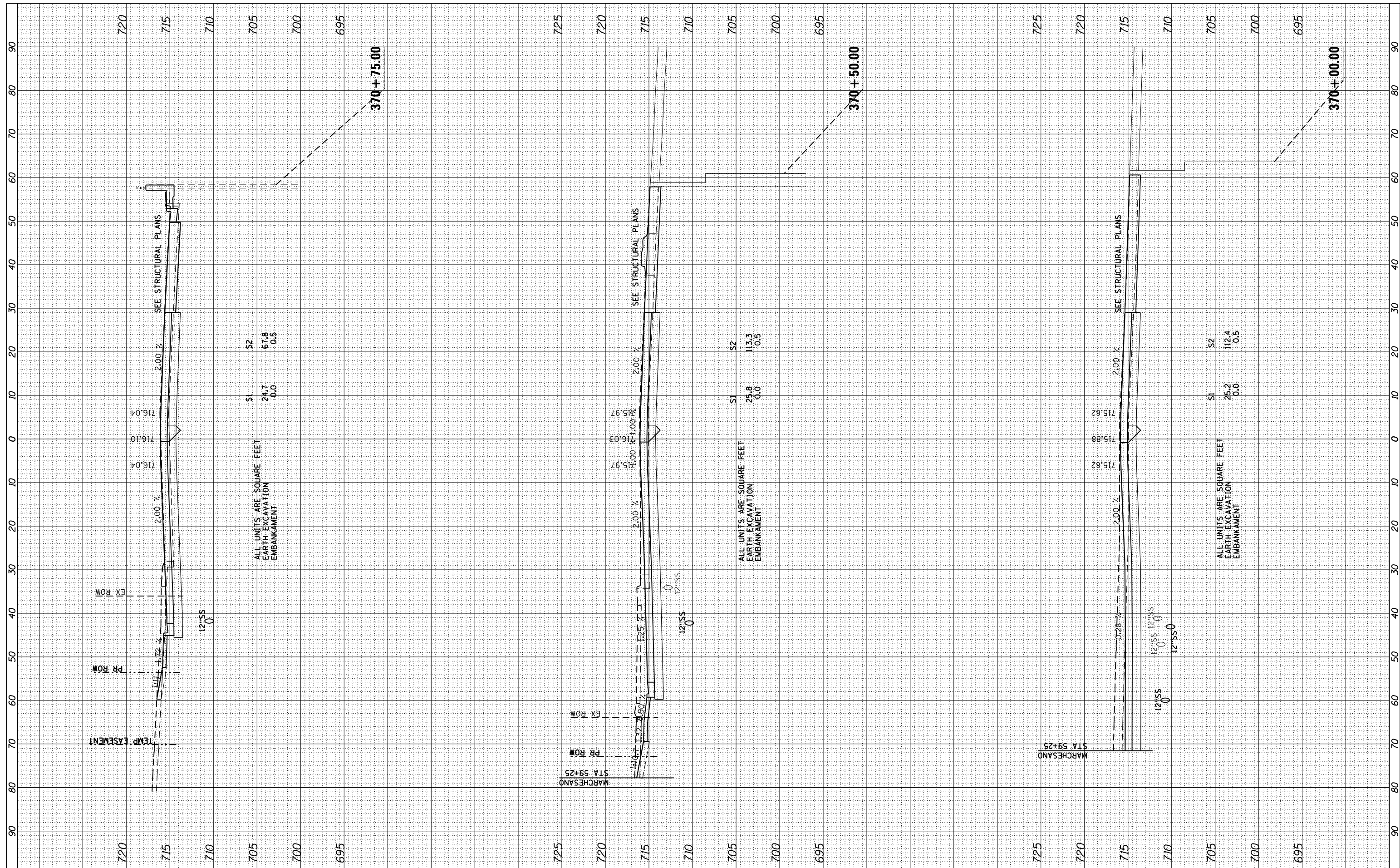
IL 2 (S MAIN ST) CROSS SECTIONS

SCALE: SHEET OF SHEETS STA. 369+00.00 TO STA. 369+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
742	(32, 33) R-1	WINNEBGAO	705	598
CONTRACT NO. 64821			ILLINOIS FED. AID PROJECT	

FINAL SURVEY NO.	SURVEYED PLOTTED AREAS CHECKED	BY	DATE
NO.	TEMPLATE AREAS CHECKED		
	NOTE BOOK		

ORIGINAL SURVEY NO.	SURVEYED PLOTTED AREAS CHECKED	BY	DATE
NO.	TEMPLATE AREAS CHECKED		
	NOTE BOOK		



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XS.SHEET VERTICAL

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

IL 2 (S MAIN ST) CROSS SECTIONS

SCALE: SHEET OF SHEETS STA. 370+00.00 TO STA. 370+75.00

F.A.P. RTE. 742	SECTION (32, 33) R-1	COUNTY WINNEBAGO	TOTAL SHEETS 705	SHEET NO. 599
			CONTRACT NO. 64821	
ILLINOIS FED. AID PROJECT				

