

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

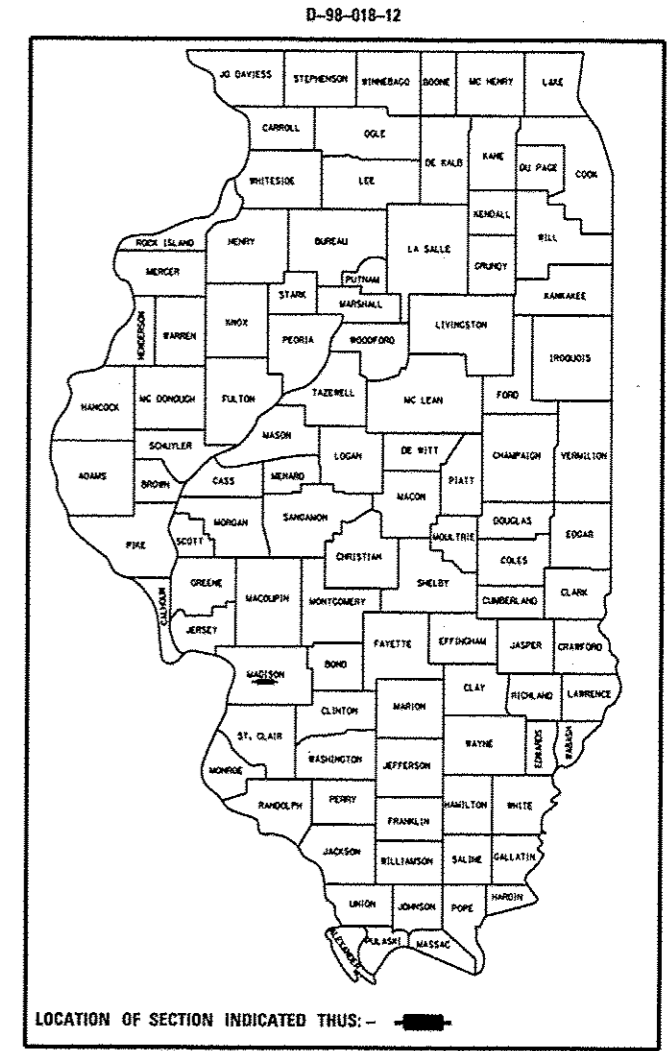
**PROPOSED  
HIGHWAY PLANS**

F.A.P. ROUTE 789 (IL 143)  
SECTION (54,58,68) RS-4  
PROJECT F-0789(050)  
RESURFACING  
MADISON COUNTY

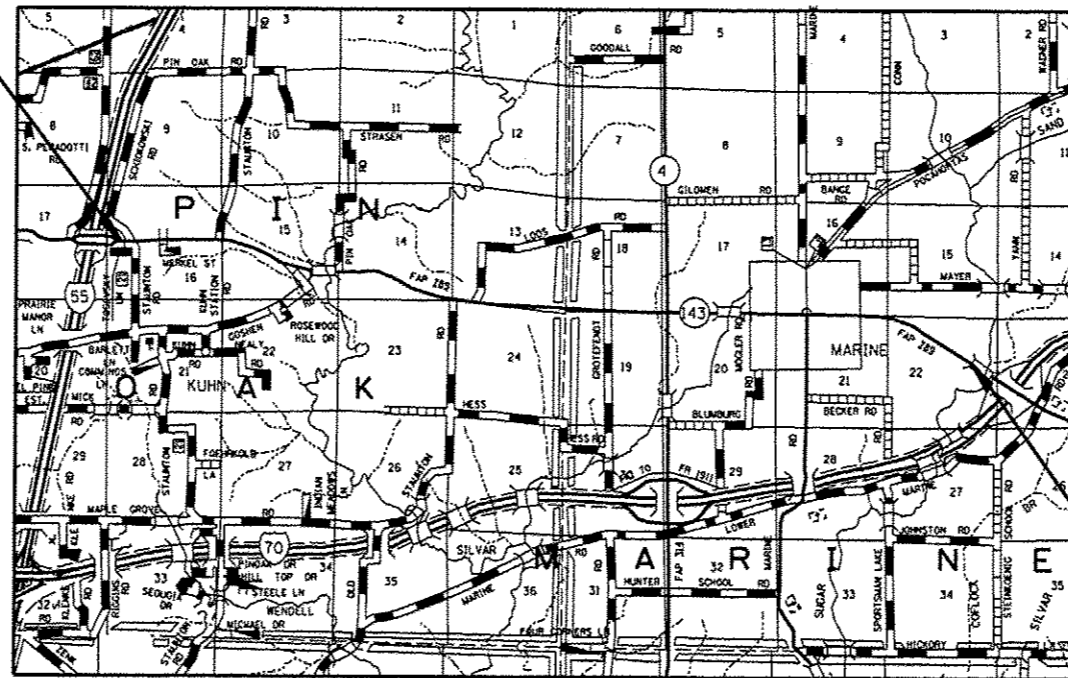
C-98-018-12

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
789	(54,58,68)RS-4	MADISON	44	1
		ILLINOIS	CONTRACT NO. 76F27	

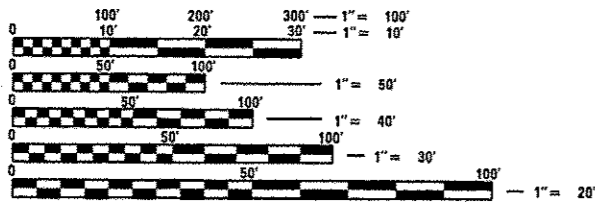
FOR INDEX OF SHEETS, SEE SHEET NO. 2



BEGIN PROJECT  
M.P. 0.00  
LAT: +38.7958  
LONG: -89.8950



END PROJECT  
M.P. 7.92  
LAT: +38.7771  
LONG: -89.7443



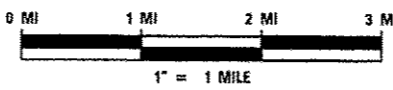
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER: PATTI LEBEAU (618) 346-3179  
PROJECT MANAGER: REBECCA THARP (618) 346-3323

CONTRACT NO. 76F27

TRAFFIC DATA:  
2011 ADT = 5050  
2031 ADT = 6150  
SU = 3.6%  
MU = 1.4%



DESIGN DESIGNATION  
N/A

GROSS LENGTH = 7.92 MI = 41,818 FT  
NET LENGTH = 7.91 MI = 41,768 FT

OMISSIONS:  
M.P. 4.90 TO M.P. 4.91 (IL RTE. 4)

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED December 13 20 12

*[Signature]*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION 5 ENGINEER

Feb 1 20 13  
*John D. Baranzelli PE, Ia*  
acting ENGINEER OF DESIGN AND ENVIRONMENT

Feb 1 20 13  
*Omec Osunen PE, Ia*  
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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OF THE STATE OF ILLINOIS**

GENERAL NOTES

- THE THICKNESS OF THE BITUMINOUS MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE BITUMINOUS MIXTURE IS PLACED.
- ILLINOIS STATE LAW REQUIRES A 48-HOUR NOTICE BE GIVEN TO ALL UTILITIES WITHIN THE PROJECT AREA BEFORE DIGGING. FIELD MARKING OF FACILITIES MAY BE OBTAINED BY CONTACTING J.U.L.I.E. OR FOR NON-MEMBERS, THE UTILITY COMPANY DIRECTLY. AGENCIES KNOWN TO HAVE FACILITIES WITHIN THE PROJECT AREA ARE AS FOLLOWS:

UTILITY	ABOVE GROUND	BELOW GROUND
• AMEREN ILLINOIS	X	X
• AT&T ILLINOIS	X	X
• AT&T CORPORATION		X
• BOND MADISON WATER COMPANY		X
• BUCKEYE PARTNERS L.P. - WOOD RIVER PIPELINE		X
• CENTERPOINT ENERGY		X
• CHARTER COMMUNICATIONS, INC.	X	X
• CITY OF EDWARDSVILLE	X	X
• CITY OF EDWARDSVILLE PUBLIC WORKS		X
• FRONTIER NORTH, INC.	X	X
• VILLAGE OF MARINE		X
• SOUTHWESTERN ELECTRIC COOPERATIVE, INC.	X	X

MEMBERS OF J.U.L.I.E. CALL TOLL FREE (800) 892-0123 OR 811 AND ARE INDICATED BY • NON-J.U.L.I.E. MEMBERS MUST BE NOTIFIED INDIVIDUALLY.

- NO OVERNIGHT LANE CLOSURES SHALL BE ALLOWED ON THE PROJECT.
- THE RESIDENT ENGINEER SHALL VERIFY THE EXISTENCE OF HIGHWAY LIGHTING AND/OR I.T.S. UTILITIES WITHIN THE PROJECT LIMITS. IF HIGHWAY LIGHTING AND/OR I.T.S. EXISTS WITHIN THE PROJECT LIMITS, AND IF THESE ITEMS REQUIRE LOCATING, THE CONTRACTOR SHALL BE DIRECTED TO DO SO ACCORDING TO SECTION 803 OF THE STANDARD SPECIFICATIONS. IF LOCATING UNDERGROUND CABLE IS NOT INCLUDED AS PART OF THE PLANS, THIS WORK SHALL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- ACCESS SHALL BE MAINTAINED TO ALL PROPERTIES UNLESS OTHERWISE NOTED IN THE PLANS.
- SHORT TERM PAVEMENT MARKING SHALL BE PLACED ON THE MILLED SURFACE, PRIMED SURFACE, AND THE FINAL HMA SURFACE. ONLY REMOVAL FROM THE HOT-MIX ASPHALT SURFACE COURSE SHALL BE PAID FOR AS "WORK ZONE PAVEMENT MARKING REMOVAL (50 FT)".
- THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT:

MIXTURE USE	SURFACE	PATCHING	INCIDENTAL SURF
AC/PG	PG 64-22	PG 64-22	PG 64-22
RAP % (MAX)	SEE SPEC.	SEE SPEC.	SEE SPEC.
DESIGN AIR VOIDS	4.0% @ Ndes=70	4.0% @ Ndes=70	4.0% @ Ndes=70
MIX COMPOSITION (GRADATION MIXTURE)	IL 9.5	IL 19.0	
FRICTION AGG	MIXTURE "D"	MIXTURE "B"	MIXTURE "D"

MIXTURE USE	SHOULDERS > 2.25'	SHOULDERS < 2.25'
AC/PG	PG 64-22	PG 64-22
RAP % (MAX)	See Contract RAP Special Provision	See Contract RAP Special Provision
DESIGN AIR VOIDS	**2.0% @ Ndes=30	**2.0% @ Ndes=30
MIX COMPOSITION (GRADATION MIXTURE)	NMAS 3/4"	NMAS 1/2"
FRICTION AGG		

\*\* Top Lift Shoulders - Design this mix at 2.0% voids and add asphalt to reduce voids to 1.5%.  
Plan quantities for Hot-Mix Asphalt Surface Course items are calculated using a unit weight of 112 lb/sq yd/in (59.8 kg/sq m/25 mm thickness).

- THE CONTRACTOR AND THE ENGINEER SHALL BE AWARE THAT NO SURVEY WAS PERFORMED FOR THIS PROJECT. THE STATIONING AND TOPOGRAPHY SHOWN IN THE PLANS WAS CREATED USING MICROFILM AND FIELD MEASUREMENTS. BOTH SHALL BE ASSUMED TO BE APPROXIMATE. THE CONTRACTOR SHALL VERIFY DIMENSIONS AND CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- THE ESTIMATED QUANTITY OF 6572 TONS OF CUTTINGS IS FROM THE HOT-MIX ASPHALT SURFACE REMOVAL OPERATION.
- ANY EXCAVATION OR DROP-OFF OF MORE THAN 3" AT THE EDGE OF PAVEMENT SHALL BE PROTECTED WITH EXTENDED LEG BARRICADES AND APPROPRIATE LIGHTING.

- THE PROPOSED PAVEMENT MARKING SHALL MATCH THE LOCATIONS OF THE EXISTING PAVEMENT MARKING, AS DIRECTED BY THE ENGINEER.
- A QUANTITY OF 0.75 ACRES OF 'SEEDING, CLASS 2' HAD BEEN INCLUDED IN THE PLANS FOR USE AT THE PROPOSED TERMINAL END SECTIONS. ALL OTHER TURF AREAS DISTURBED BY THE CONTRACTOR SHALL BE SEEDDED WITH THE APPROPRIATE EROSION CONTROL AS DIRECTED BY THE ENGINEER AT THE CONTRACTOR'S EXPENSE.
- A QUANTITY FOR TEMPORARY PAVEMENT MARKING EQUAL TO THE AMOUNT OF PERMANENT PAVEMENT MARKING HAS BEEN ADDED TO THE PLANS. REMOVAL OF TEMPORARY PAVEMENT MARKING SHALL BE PAID FOR AS "WORK ZONE PAVEMENT MARKING REMOVAL".
- THE USE OF VIBRATORY ROLLERS WILL NOT BE PERMITTED WITHIN THE CITY LIMITS OF MARINE. THIS DOES NOT RELIEVE THE CONTRACTOR OF DENSITY REQUIREMENTS FOR THE CONSTRUCTION OF THE BITUMINOUS PAVEMENTS AS SPECIFIED IN SECTIONS 406 & 407 OF THE STANDARD SPECIFICATIONS.
- EXISTING PAVEMENT MARKING SHALL BE TEMPORARILY COVERED PRIOR TO APPLICATION OF THE DECK SURFACE TREATMENT, TO PREVENT THE MATERIAL FROM BEING APPLIED TO THE MARKINGS AND TO PROTECT THE REFLECTIVE PROPERTIES OF THE MARKINGS. THE TEMPORARY COVERING MATERIAL USED SHALL BE SUCH THAT IT WILL NOT AFFECT THE MARKING'S REFLECTIVITY WHEN REMOVED AFTER APPLICATION OF THE DECK SURFACE TREATMENT AND PRIOR TO OPENING TO TRAFFIC. COST IS INCLUDED WITH THE 'CONCRETE SEALER' PAY ITEM.
- THE DEPARTMENT STRONGLY ENCOURAGES THE PRIME CONTRACTOR AND THEIR APPROVED SUB-CONTRACTORS TO HIRE MINORITY, WOMEN AND DISADVANTAGED INDIVIDUALS FROM ITS FEDERALLY FUNDED HIGHWAY CONSTRUCTION CAREERS TRAINING PROGRAM (HCCTP) TO HELP MEET WORKFORCE AND TRAINEE GOALS. THIS PROGRAM IS TRAINING MINORITIES, WOMEN AND DISADVANTAGED INDIVIDUALS IN HIGHWAY CONSTRUCTION-RELATED SKILLS, E.G., MATH FOR THE TRADES, JOB READINESS, TECHNICAL SKILLS COURSEWORK (CARPENTRY, CONCRETE FLATWORK, BLUEPRINT READING, SITE PLANS, SITE WORK, TOOLS USE, ETC.) AND OSHA 10 HOUR CERTIFICATION, TO PREPARE THEM FOR A CAREER IN THE HIGHWAY CONSTRUCTION TRADES. GRADUATES ARE WELL-TRAINED AND READY TO BECOME PRODUCTIVE ENTRY-LEVEL CONSTRUCTION WORKERS. CONTACT THE DISTRICT 8 EEO OFFICE AT 618-346-3360 AND/OR THE HCCTP COORDINATOR AT 618-874-6528 TO LEARN MORE ABOUT THE PROGRAM AND FOR ASSISTANCE IN MEETING WORKFORCE AND TRAINEE GOALS.
- HOT-MIX ASPHALT SURFACE REMOVAL SHALL BE DONE IN ACCORDANCE WITH ARTICLE 440.04 AND AS DETAILED IN THE CONTRACT PLANS. IF DAMAGE OCCURS BEYOND THE LIMITS OF PROPOSED MILLING DEPTH DUE TO THE CONDITION OF THE EXISTING HMA OVERLAY, THE CONTRACTOR SHALL HALT MILLING OPERATIONS AND CONFER WITH THE RESIDENT ENGINEER. THE RESIDENT ENGINEER MAY ELECT TO REMOVE MILLING FROM THE SCOPE OF WORK IN NONESSENTIAL LOCATIONS. ESSENTIAL LOCATIONS ARE NEAR THE BEGINNING OF THE PROJECT AT STAUNTON ROAD, IN THE CITY OF MARINE, STRUCTURES 060-0246 AND 060-0249, AND AT THE BIKE PATH CROSSING.
- FLAGGERS SHALL BE REQUIRED AT ALL TIMES DURING PATCHING OPERATIONS.

INDEX OF SHEETS

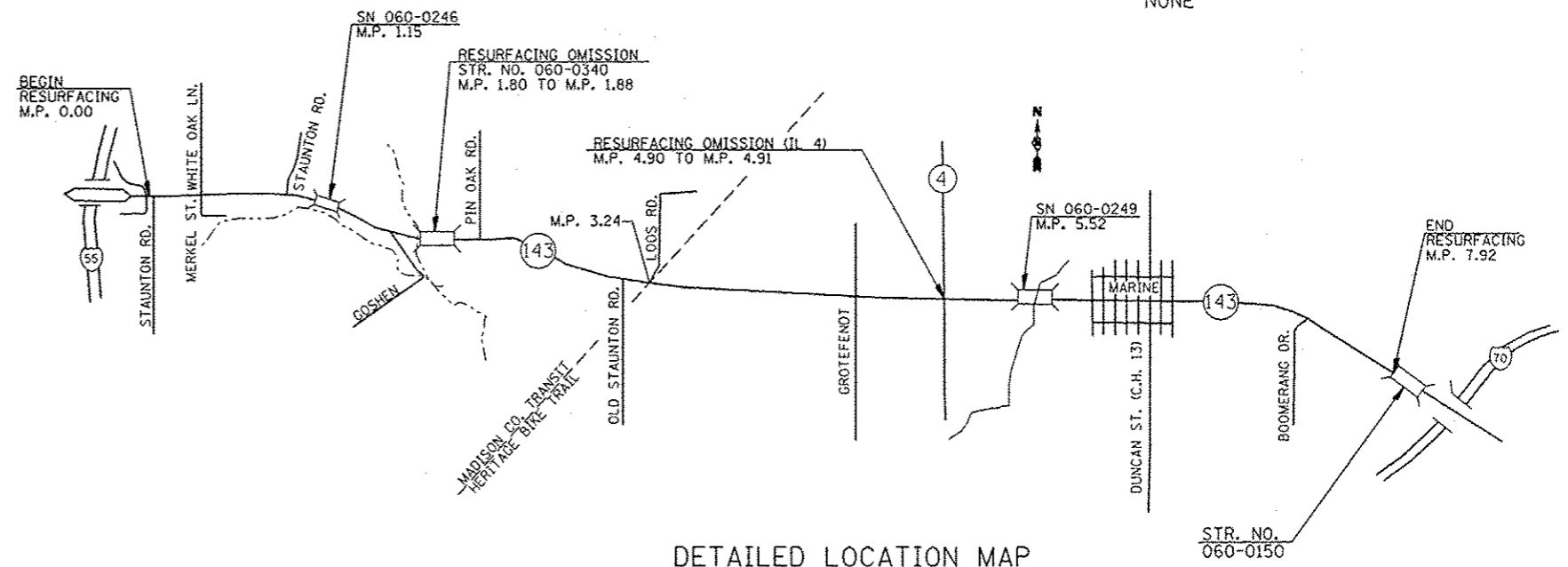
- COVER SHEET
- INDEX OF SHEETS, STANDARDS, COMMITMENTS, GENERAL NOTES & LOCATION MAP
- 8. SUMMARY OF QUANTITIES
- 10. TYPICAL SECTIONS
- 15. SCHEDULES
- 22. GUARDRAIL DETAILS
- STR. 060-0340 PLAN & CROSS SECTION FOR CONCRETE SEALER
- BIKE TRAIL DETAILS
- 27. RESURFACING DETAILS
- RESURFACING DETAILS, PAVEMENT MARKING DETAILS
- STAGE CONSTRUCTION STR. 060-0246
- STAGE CONSTRUCTION STR. 060-0249
- TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION
- STRUCTURE PLANS STR. 060-0246
- STRUCTURE PLANS STR. 060-0249

STANDARDS

000001-06	701201-04
001001-02	701306-03
001006	701311-03
406201-01	701321-13
442201-03	701326-04
630001-10	701336-06
630101-09	701501-06
630301-06	701801-05
631032-08	701901-02
635006-03	720001-01
635011-02	720006-03
701001-02	780001-03
701006-04	781001-03
701011-03	

COMMITMENTS

NONE



DETAILED LOCATION MAP

FILE NAME	USER NAME	DESIGNED	REVISED	GENERAL NOTES, INDEX OF SHEETS, STANDARDS, COMMITMENTS & LOCATION MAP		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ai\pwwork\paw\dat\dintelman\j\02281732\0876f27-shc\plandgn	dintelmanj	-	-	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		789	(54,58,68) RS-4	MADISON	44	2
		DRAWN	REVISION	SCALE: SHEET OF SHEETS STA. TO STA.						
		CHECKED	REVISION							
		DATE	REVISION							

# SUMMARY OF QUANTITIES

80% FED.  
20% ST.

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005		0014		
				RURAL		RURAL		
						060-0246	060-0249	060-0340
20400800	FURNISHED EXCAVATION	CU YD	504	504				
* 25000200	SEEDING, CLASS 2	ACRE	0.75	0.75				
* 25000400	NITROGEN FERTILIZER NUTRIENT	POUND	68	68				
* 25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	68	68				
* 25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	68	68				
* 25100115	MULCH, METHOD 2	ACRE	0.75	0.75				
40200800	AGGREGATE SURFACE COURSE, TYPE B	TON	89	89				
40300400	BITUMINOUS MATERIALS (COVER AND SEAL COATS)	TON	40	40				
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	38	38				
40600300	AGGREGATE (PRIME COAT)	TON	182	182				
40600895	CONSTRUCTING TEST STRIP	EACH	1	1				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	873	873				
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	42	42				

\*SPECIALTY ITEM

FILE NAME *	USER NAME * dintelman.jn	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ci:\pwwork\period\dintelman.jn\08281732\0876f27-sht-plan.dgn		DRAWN -	REVISED -			789	(54,58,68) RS-4	MADISON	44	3
	PLOT SCALE * 100.0000' / 1" =	CHECKED -	REVISED -							
	PLOT DATE * 12/14/2012	DATE -	REVISED -							
						SCALE:	SHEET NO. 1 OF 6 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT
										CONTRACT NO. 76F27

# SUMMARY OF QUANTITIES 80% FED. 20% ST.

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005		0014 RURAL		
				RURAL		060-0246	060-0249	060-0340
40600990	TEMPORARY RAMP	SQ YD	422	422				
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	15083	15,083				
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	55	55				
44000152	HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"	SQ YD	102350	102350				
44000153	HOT-MIX ASPHALT SURFACE REMOVAL, 1"	SQ YD	399	399				
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	18007	18,007				
44000162	HOT-MIX ASPHALT SURFACE REMOVAL, 3 1/4"	SQ YD	338	338				
44201815	CLASS D PATCHES, TYPE II, 14 INCH	SQ YD	267	267				
44201819	CLASS D PATCHES, TYPE III, 14 INCH	SQ YD	23	23				
44201821	CLASS D PATCHES, TYPE IV, 14 INCH	SQ YD	324	324				
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	82,674	82,674				
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	2408	2,408				
48203100	HOT-MIX ASPHALT SHOULDERS	TON	119	119				

FILE NAME =	USER NAME = dntelton.jn	DESIGNED -	REVISED -
01\pwwork\pwwork\dntelton.jn\0281732\876f27-sht-pln.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -
	PLOT DATE = 12/14/2012	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE: SHEET NO. 2 OF 6 SHEETS STA. TO STA.

F.A.P. RTE. 789	SECTION (54,58,68) RS-4	COUNTY MADISON	TOTAL SHEETS 44	SHEET NO. 4
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 76F27	

# SUMMARY OF QUANTITIES 80% FED. 20% ST.

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005		0014		
				RURAL		RURAL		
					060-0246	060-0249	060-0340	
50102400	CONCRETE REMOVAL	CU YD	24		10.5	13.5		
50104000	BRIDGE RAIL REMOVAL	FOOT	204		96	108		
50300255	CONCRETE SUPERSTRUCTURE	CU YD	24		10.5	13.5		
50400105	PRECAST CONCRETE BRIDGE SLAB	SQ FT	758		355	403		
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	3940		1710	2230		
* 50901050	STEEL RAILING, TYPE SM	FOOT	204		96	108		
54390180	INSERTION CULVERT LINER 24"	FOOT	41	41				
58100200	WATERPROOFING MEMBRANE SYSTEM	SQ YD	84.2		39.4	44.8		
<del>58700300</del>	<del>CONCRETE SEALER</del>	<del>SQ FT</del>	<del>19390</del>				<del>19390</del>	
* 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	2500	2500				
* 63000025	STEEL PLATE BEAM GUARDRAIL, ATTACHED TO STRUCTURES	FOOT	125	125				
* 63100087	TRAFFIC BARRIER TERMINAL, TYPE 6A	EACH	7	7				
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	27	27				

\*SPECIALTY ITEM

FILE NAME *	USER NAME * dntelmarjn	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwwork\pwwork\dntelmarjn\148201732	0876f27-shrplan.dgn	DRAWN -	REVISED -		889	(S4,58,68) RS-4	MADISON	44	5			
	PLOT SCALE * 1/8"=1'-0"	CHECKED -	REVISED -		SCALE:	SHEET NO. 3 OF 6 SHEETS	STA.	TO STA.	CONTRACT NO. 76F27			
	PLOT DATE * 12/14/2012	DATE -	REVISED -						FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			



SUMMARY OF QUANTITIES 80% FED  
20% ST

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005		0014		
				RURAL		RURAL		
					060-0246	060-0249	060-0340	
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	342	342				
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	72	72				
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	2764	2764				
70400100	TEMPORARY CONCRETE BARRIER	FOOT	300		162.5	137.5		
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	300		162.5	137.5		
70600250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	4		4			
70600350	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	EACH	3			3		
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	116262	116,262				
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	342	342				
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	72	72				
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	520	520				
* 78200410	GUARDRAIL MARKERS, TYPE A	EACH	50	50				

\*SPECIALTY ITEM

FILE NAME =	USER NAME = dintelmonyn	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>	F.A.P. RYE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pr-work\pmdat\dintelmonyn\12281732\876f27-shr-plan.dgn	876f27-shr-plan.dgn	DRAWN -	REVISED -			789	(54,58,68) RS-4	MADISON	44	7	
PLOT SCALE = 100.0000' / 1"		CHECKED -	REVISED -			CONTRACT NO. 76F27					
PLOT DATE = 12/14/2012		DATE -	REVISED -			SCALE:	SHEET NO. 5 OF 6 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

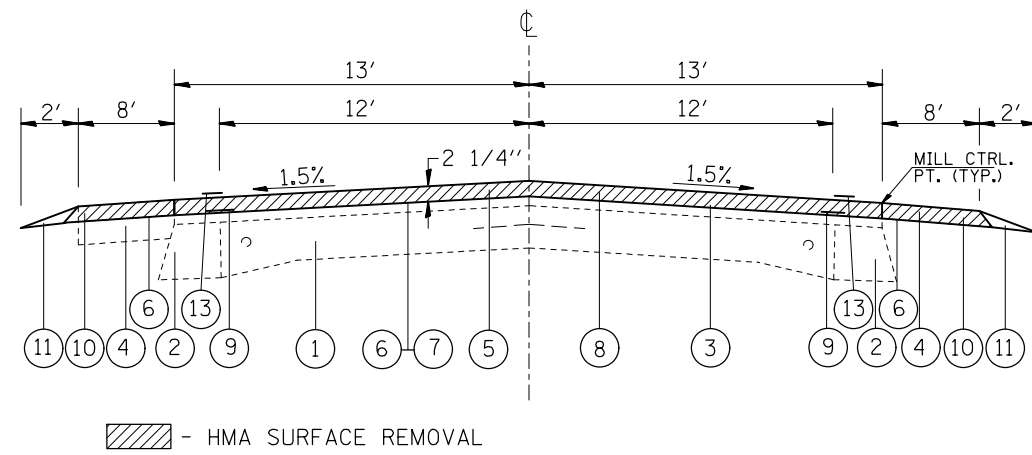
# SUMMARY OF QUANTITIES

80% FED.  
20% ST.

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005		0014		
				RURAL		RURAL		
					060-0246	060-0249	060-0340	
* 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	28	28				
78300100	PAVEMENT MARKING REMOVAL	SQ FT	476		232	244		
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	520	520				
X0324744	REMOVAL OF EXISTING PRECAST CONCRETE UNITS	SQ FT	758		355	403		
X5870015	BRIDGE DECK CONCRETE SEALER	SQ FT	19390				19390	
* X6310188	TRAFFIC BARRIER TERMINAL, TYPE 6A (MODIFIED)	EACH	1	1				
* X6310195	TRAFFIC BARRIER TERMINAL TYPE 1, SPECIAL (TANGENT), MODIFIED	EACH	1	1				
X7010202	TRAFFIC CONTROL AND PROTECTION, STANDARD 701321 (SPECIAL)	EACH	2		1	1		
70600280	IMPACT ATTENUATORS, TEMPORARY (SEVERE USE, NARROW), TEST LEVEL 3	EACH	1			1		
70600360	IMPACT ATTENUATORS, RELOCATE (SEVERE USE), TEST LEVEL 3	EACH	1			1		
Z0070100	SURVEY MONUMENT COVER ASSEMBLY	EACH	3	3				
+ Z0076600	TRAINEES	HOUR	1,000	1,000				
+ Z0076604	TRAINEES, TRAINING PROGRAM GRADUATE	HOUR	1,000	1,000				

*1/8 12* + 0042 \*SPECIALTY ITEM





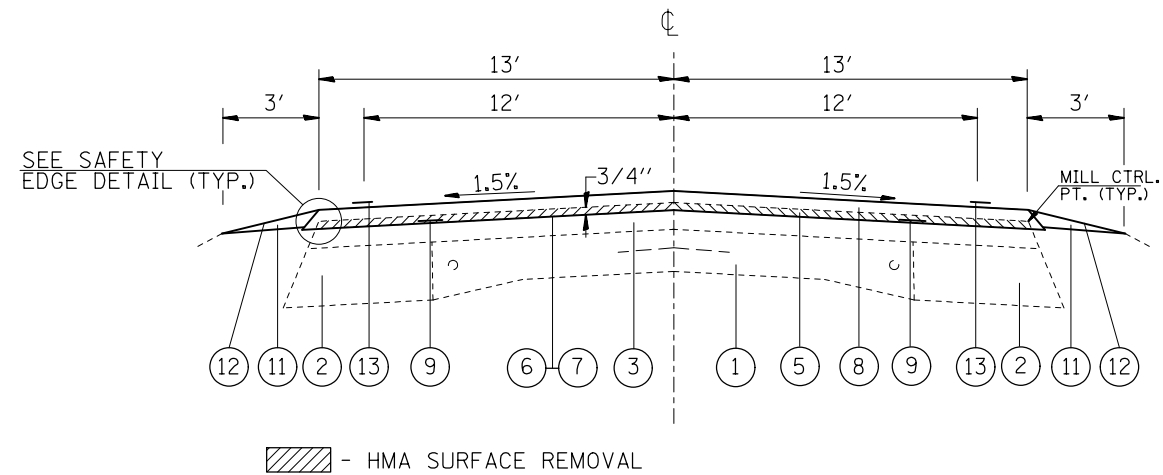
**TYPICAL SECTION (IL 143)**

(TANGENT SECTION)

M.P. 0.00 TO M.P. 0.10

**LEGEND**

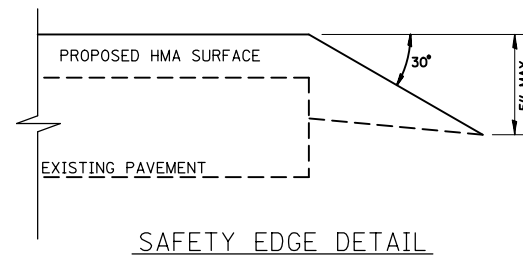
- ① EXISTING PAVEMENT 9-6-9
- ② EXISTING HMA WIDENING
- ③ EXISTING HMA RESURFACING, ±5"
- ④ EXISTING HMA SHOULDERS
- ⑤ PROPOSED HMA SURFACE REMOVAL
- ⑥ PROPOSED BITUMINOUS MATERIALS (PRIME COAT)
- ⑦ PROPOSED AGGREGATE (PRIME COAT)
- ⑧ PROPOSED HMA SURFACE COURSE 2 1/4"
- ⑨ PROPOSED STRIP REFLECTIVE CRACK CONTROL TREATMENT
- ⑩ PROPOSED HMA SHOULDERS, 2 1/4"
- ⑪ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑫ PROPOSED BITUMINOUS MATERIALS (COVER AND SEAL COATS)
- ⑬ PROPOSED THERMOPLASTIC PAVEMENT MARKING



**TYPICAL SECTION (IL 143)**

(TANGENT SECTION)

- M.P. 0.10 TO M.P. 0.82
- M.P. 0.93 TO M.P. 1.12
- M.P. 1.25 TO M.P. 1.38
- M.P. 1.55 TO M.P. 1.85
- M.P. 1.97 TO M.P. 2.14
- M.P. 2.33 TO M.P. 2.46
- M.P. 2.59 TO M.P. 2.66
- M.P. 2.79 TO M.P. 3.23
- M.P. 3.25 TO M.P. 3.91
- M.P. 4.03 TO M.P. 4.90
- M.P. 4.91 TO M.P. 5.66
- M.P. 6.66 TO M.P. 6.87
- M.P. 7.12 TO M.P. 7.92



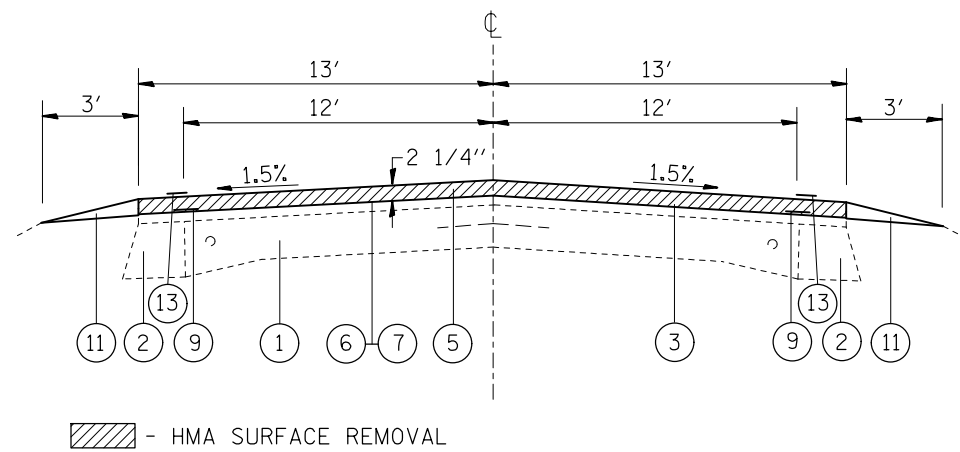
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/14/2012	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS**

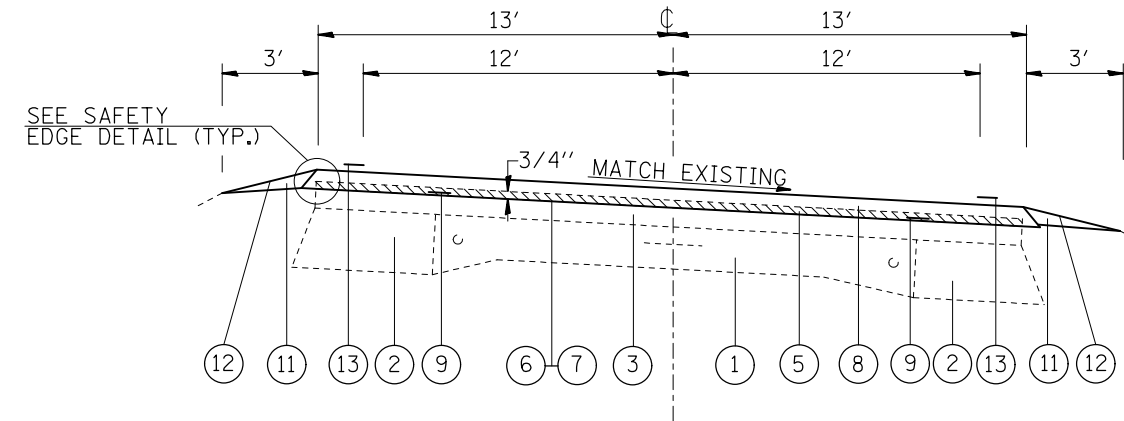
SCALE: SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
789	(54,58,68) RS-4	MADISON	44	9
CONTRACT NO. 76F27				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



**TYPICAL SECTION (IL 143)**

(TANGENT SECTION)  
M.P. 5.66 TO M.P. 6.66



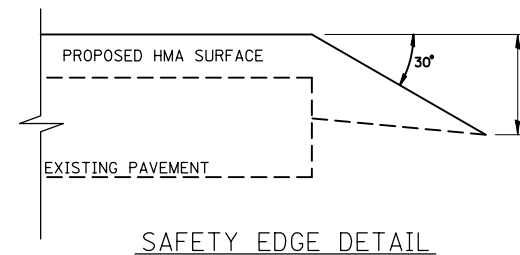
**TYPICAL SECTION (IL 143)**

(SUPERELEVATED SECTION)

M.P. 0.86	TO	M.P. 0.93	0.062% SLOPE RT.
M.P. 1.12	TO	M.P. 1.25	0.044% SLOPE RT.
M.P. 1.38	TO	M.P. 1.55	0.060% SLOPE LT.
M.P. 1.85	TO	M.P. 1.97	0.053% SLOPE RT.
M.P. 2.14	TO	M.P. 2.33	0.060% SLOPE RT.
M.P. 2.46	TO	M.P. 2.59	0.046% SLOPE RT.
M.P. 2.66	TO	M.P. 2.79	0.070% SLOPE LT.
M.P. 3.91	TO	M.P. 4.03	0.036% SLOPE LT.
M.P. 6.87	TO	M.P. 7.12	0.052% SLOPE RT.

**LEGEND**

- ① EXISTING PAVEMENT 9-6-9
- ② EXISTING HMA WIDENING
- ③ EXISTING HMA RESURFACING, ±5"
- ④ EXISTING HMA SHOULDERS
- ⑤ PROPOSED HMA SURFACE REMOVAL
- ⑥ PROPOSED BITUMINOUS MATERIALS (PRIME COAT)
- ⑦ PROPOSED AGGREGATE (PRIME COAT)
- ⑧ PROPOSED HMA SURFACE COURSE 2 1/4"
- ⑨ PROPOSED STRIP REFLECTIVE CRACK CONTROL TREATMENT
- ⑩ PROPOSED HMA SHOULDERS, 2 1/4"
- ⑪ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑫ PROPOSED BITUMINOUS MATERIALS (COVER AND SEAL COATS)
- ⑬ PROPOSED THERMOPLASTIC PAVEMENT MARKING



FILE NAME =	USER NAME = dntelmonjn	DESIGNED -	REVISED -
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/14/2012	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS**

SCALE: SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
789	(54,58,68) RS-4	MADISON	44	10
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 76F27	

MAINLINE RESURFACING SCHEDULE								
LOCATION			HMA SURFACE REMOVAL, 3/4"	HMA SURFACE REMOVAL, 2 1/4"	BIT. MAT'L PRIME COAT	AGGREGATE PRIME COAT	HMA SURFACE COURSE MIX D, N 70	STRIP REFLECTIVE CRACK CONTROL TREATMENT
			(SQ YD)	(SQ YD)	(TON)	(TON)	(TON)	(FOOT)
0.00	TO	0.10		2464.00	0.77	2.29	192.19	1056.00
0.10	TO	1.80	25930.67		8.11	38.90	3267.26	17952.00
1.88	TO	3.23	20592.00		6.44	30.89	2594.59	14256.00
3.23	TO	3.25		288.89	0.09	0.43	36.40	200.00
3.25	TO	4.90	25168.00		7.88	37.75	3171.17	17424.00
4.91	TO	5.66	11440.00		3.58	17.16	1441.44	7920.00
5.66	TO	6.66		15253.33	4.77	22.88	1921.92	10560.00
6.66	TO	7.92	19219.20		6.01	28.83	2421.62	13305.60
TOTAL			102350	18007	37.7	180	15047	82674

TEMP. RAMP/BUTT JOINT SCHEDULE					
LOCATION	LT/RT	TEMPORARY RAMP	HMA SURFACE REMOVAL - BUTT JOINT	P.C.C SURFACE REMOVAL - BUTT JOINT	COMMENTS
MILE PT.		SQ YD	SQ YD	SQ YD	
0.00		22			BEG. RESURF.
0.31	LT	28	41.67		WHITE OAK LN.
0.31	RT	28	41.67		MERKEL ST.
0.81	LT	37	54.17		STAUNTON RD.
1.07	RT	23	33.33		GREENBRIAR EST.
1.25	RT	34	50.00		LAUREL OAK LN
1.48	RT	48	70.83		GOSHEN
1.80		15	87.00		060-0340
1.88		15	87.00		060-0340
1.91	LT	17	25.00		PIN OAK RD.
3.01	RT	28	41.67		OLD STAUNTON RD.
4.39	RT	34	50.00		GROTEFENDT
4.39	LT	20	29.17		GROTEFENDT
4.90		15	87.00		IL 4
4.91		15	87.00		IL 4
7.14	RT	28		42.00	BOOMERANG DR.
7.92		15	87.00		END RESURF.
TOTAL =		422	873	42	

SIDE ROAD SCHEDULE							
LOCATION	SIDE	LENGTH	WIDTH	BIT. MAT'L PRIME COAT	AGGREGATE PRIME COAT	INCIDENTAL HMA SURFACING	COMMENTS
MILE PT.	LT/RT	(FEET)	(FEET)	(TON)	(TON)	(TON)	
0.31	LT	7.5	50	0.01	0.06	3.50	WHITE OAK LN.
0.31	RT	7.5	50	0.01	0.06	3.50	MERKEL ST.
0.81	LT	7.5	65	0.02	0.08	4.55	STAUNTON RD.
1.07	RT	7.5	40	0.01	0.05	2.80	GREENBRIAR EST.
1.25	RT	7.5	60	0.02	0.08	4.20	LAUREL OAK LN
1.48	RT	7.5	85	0.02	0.11	5.95	GOSHEN
1.91	LT	7.5	30	0.01	0.04	2.10	PIN OAK RD.
3.01	RT	7.5	50	0.01	0.06	3.50	OLD STAUNTON RD.
4.39	RT	7.5	60	0.02	0.08	4.20	GROTEFENDT
4.39	LT	7.5	35	0.01	0.04	2.45	GROTEFENDT
7.14	RT	7.5	75	0.02	0.09	5.25	BOOMERANG DR.
TOTAL =				0.2	1	42	



# PAVEMENT MARKING SCHEDULE

LOCATION			LENGTH	THERMOPLASTIC PAVEMENT MARKING					RAISED RELFLECTIVE PAVEMENT MARKER REMOVAL	RAISED RELFLECTIVE PAVEMENT MARKERS	SHORT TERM PAVEMENT MARKING	WORK ZONE PAVEMENT MARKING REMOVAL
				4"			12"	24"				
				SOLID WHITE EDGE LINE	SKIP-DASH YELLOW CENTERLINE	SOLID YELLOW CENTER LINE	CROSS WALK	SOLID WHITE STOP BARS				
M.P.	TO	M.P.	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	EACH	EACH	FOOT	FOOT
EDGE LINE												
0.00	TO	1.78	9398.4	18797					118	118	1880	627
1.84	TO	4.90	16156.8	32314					202	202	3232	1077
4.91	TO	5.66	3960	7920					50	50	792	264
5.66	TO	6.66	5280	10560					66	66	1056	352
6.66	TO	7.92	6652.8	13306					84	84	1332	444
SKIP-DASH CENTER												
0.00	TO	0.62	818		818							
1.05	TO	1.43	502		502							
1.53	TO	1.70	224		224							
1.72	TO	1.85	172		172							
2.33	TO	3.21	1162		1162							
3.25	TO	4.90	2178		2178							
4.90	TO	5.19	383		383							
5.26	TO	7.92	3538		3538							
NPZ EB LANE												
0.45	TO	0.91	2429			2429						
1.27	TO	1.53	1373			1373						
1.70	TO	2.33	3326			3326						
3.05	TO	3.25	1056			1056						
3.81	TO	3.96	792			792						
4.54	TO	4.69	792			792						
4.96	TO	5.06	528			528						
5.19	TO	5.30	581			581						
5.51	TO	5.61	528			528						
7.46	TO	7.59	686			686						
7.89	TO	7.92	264			264						
NPZ WB LANE												
0.62	TO	1.05	2270			2270						
1.43	TO	1.72	1531			1531						
1.85	TO	2.48	3326			3326						
3.21	TO	3.41	1056			1056						
3.98	TO	4.11	686			686						
4.70	TO	4.86	845			845						
5.11	TO	5.26	792			792						
5.42	TO	5.51	475			475						
5.65	TO	5.75	526			526						
7.69	TO	7.79	526			526						
CROSS WALKS												
3.26 (BIKE TRAIL)							52					
5.98 (WINDMILL)							104					
6.16 (DUNCAN)							134					
6.25 (HUMBOLT)							52					
STOP BARS												
4.9 (IL 4)											18	
4.91 (IL 4)											18	
6.16 (DUNCAN)											36	
SUB-TOTALS				82897	8977	24388						
GRAND TOTALS				116262			342	72	520	520	8292	2764

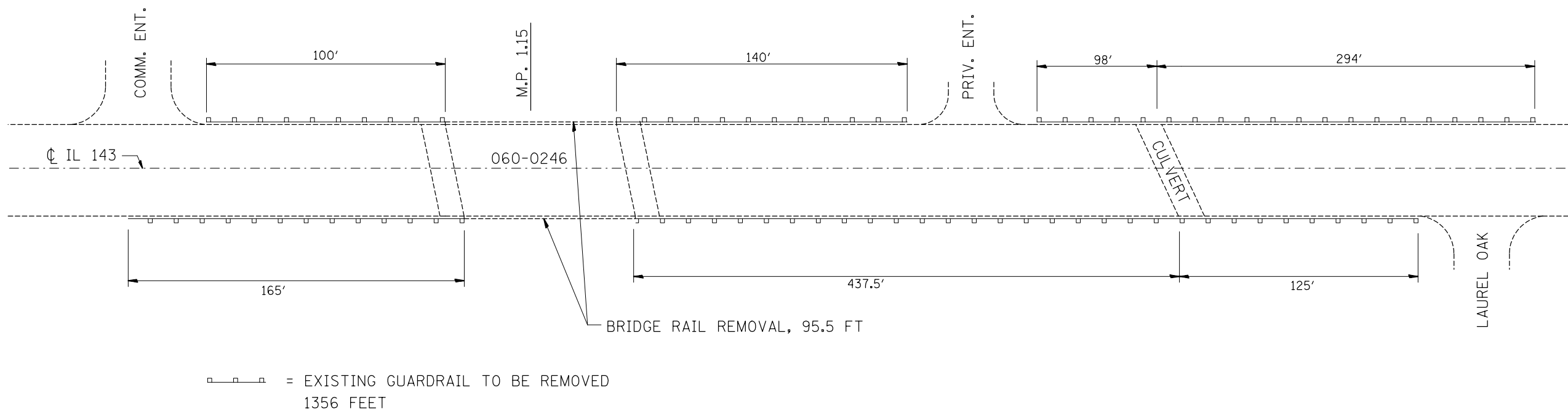
## STAGING SCHEDULE

LOCATION	TEMPORARY CONCRETE BARRIER	RELOCATE TEMPORARY CONCRETE BARRIER	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	IMPACT ATTENUATORS, TEMPORARY (SEVERE USE, NARROW), TEST LEVEL 3	IMPACT ATTENUATORS, RELOCATE (SEVERE USE, NARROW), TEST LEVEL 3	TEMPORARY RUMBLE STRIP	TEMPORARY BRIDGE TRAFFIC SIGNALS	HMA SURFACE COURSE MIX D, N70
	FOOT	FOOT	EACH	EACH	EACH	EACH	EACH	EACH	TON
SN 060-0246									
STAGE I	162.5		2						
STAGE II		162.5		2					
STAGE I & II							6	1	13
SN 060-0249									
STAGE I	137.5		1		1				
STAGE II		137.5		1		1			
STAGE I & II							6		
								1	22
<b>TOTAL</b>	<b>300</b>	<b>300</b>	<b>3</b>	<b>3</b>	<b>1</b>	<b>1</b>	<b>12</b>	<b>2</b>	<b>35</b>

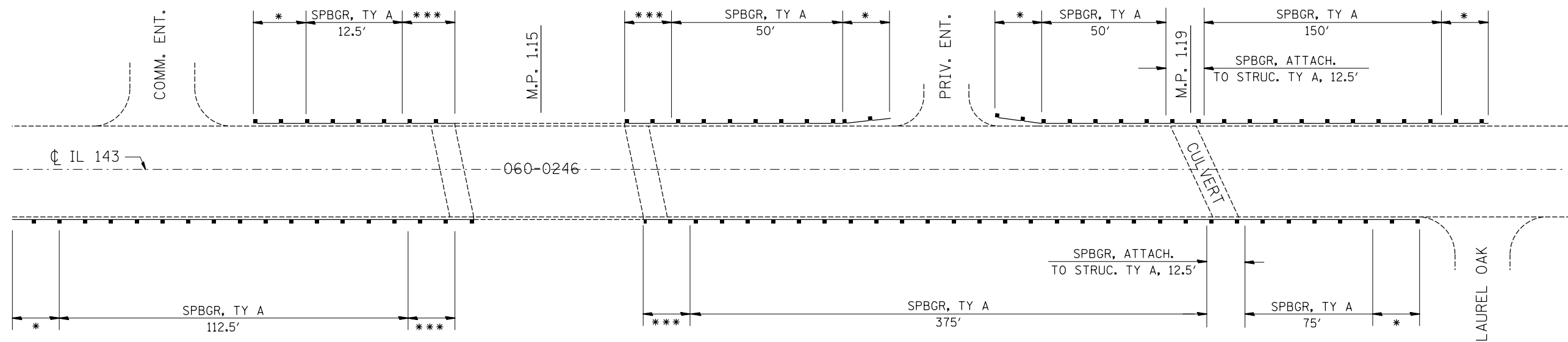
## GUARDRAIL SCHEDULE

LOCATION	GUARDRAIL REMOVAL	SPBGR, TYPE A 6' POSTS	SPBGR, ATTACH. TO STRUCT.	TRAFFIC BARRIER TERMINAL TYPE 6A	TRAFFIC BARRIER TERMINAL TYPE 6A (MODIFIED)	TRAFFIC BARRIER TERMINAL TYPE 1, SPECIAL (TANGENT)	TRAFFIC BARRIER TERMINAL TYPE 1, SPECIAL (TANGENT) (MODIFIED)	TERMINAL MARKER DIRECT APPLIED	GUARDRAIL MARKERS, TYPE A	FURNISHED EXCAVATION
MILE PT.	(FOOT)	(FOOT)	(FOOT)	(EACH)	(EACH)	(EACH)	(EACH)	(EACH)	(EACH)	(CU YD)
1.15 (LOCATION 1)	1359.5	825		4		6		6	21	108
1.30 (LOCATION 2)	255	237.5	25			2		2	6	36
3.75 (LOCATION 3)	475	250	25			4		4	6	72
4.85 (LOCATION 4)	565	337.5	25			4		4	8	72
4.94 (LOCATION 5)	308	312.5	25			4		4	8	72
5.52 (LOCATION 6)	680	275		3	1	3	1	4	7	72
6.53 (LOCATION 7)	246	262.5	25			4		4	7	72
<b>TOTALS</b>	<b>3888.5</b>	<b>2500</b>	<b>125</b>	<b>7</b>	<b>1</b>	<b>27</b>	<b>1</b>	<b>28</b>	<b>63</b>	<b>504</b>





GUARDRAIL REMOVAL  
(PLAN VIEW)



PROPOSED GUARDRAIL  
(PLAN VIEW)

- \* = TRAFFIC BARRIER TERM. TYPE 1 SPECIAL (TANGENT)
- \*\* = TRAFFIC BARRIER TERM. TYPE 1 SPECIAL (TANGENT), MODIFIED
- \*\*\* = TRAFFIC BARRIER TERM. TYPE 6A
- \*\* = TRAFFIC BARRIER TERM. TYPE 6A (MODIFIED)

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	PLOT DATE = 12/14/2012	DATE -	REVISED -

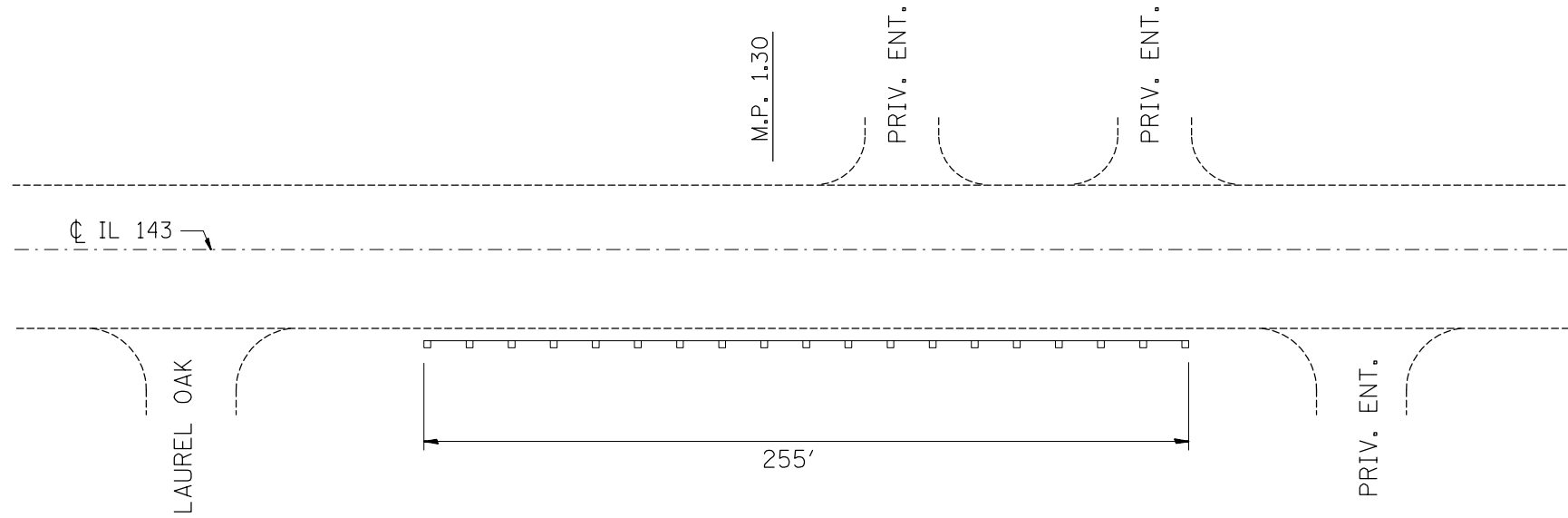
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

GUARDRAIL DETAILS  
(LOCATION #1)

SCALE: SHEET NO. OF SHEETS STA. TO STA.

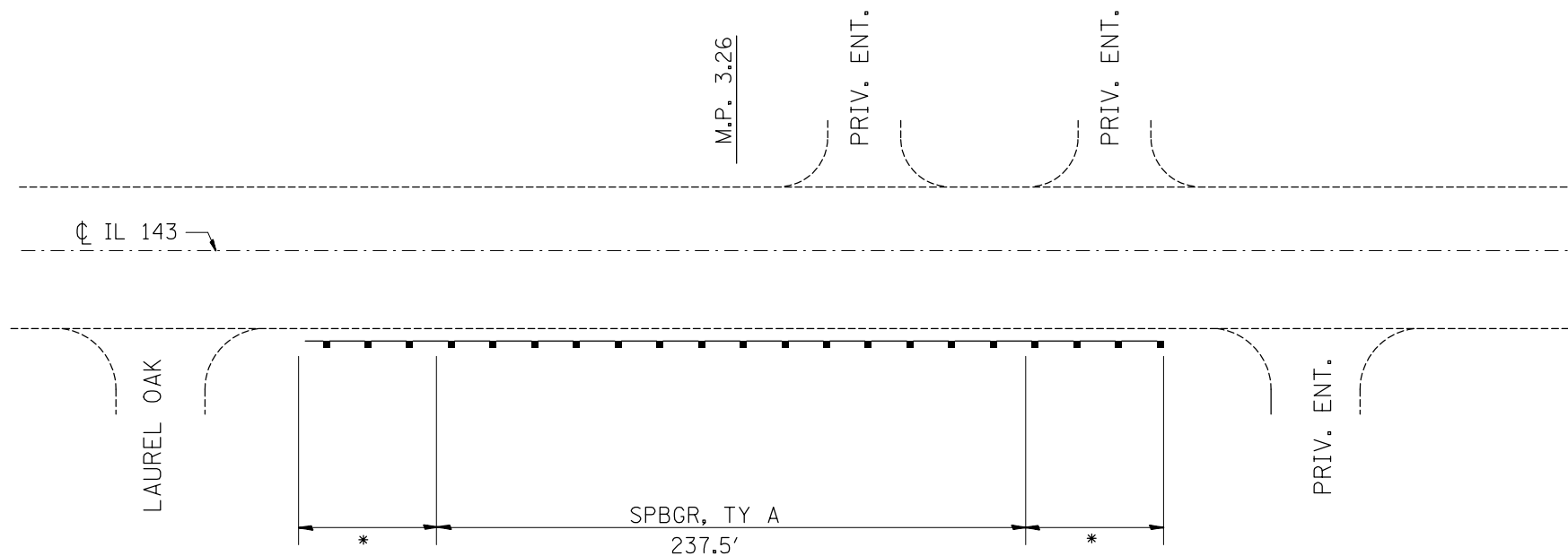
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
789	(54,58,68) RS-4	MADISON	44	16
CONTRACT NO. 76F27				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				





= EXISTING GUARDRAIL TO BE REMOVED  
 255 FEET

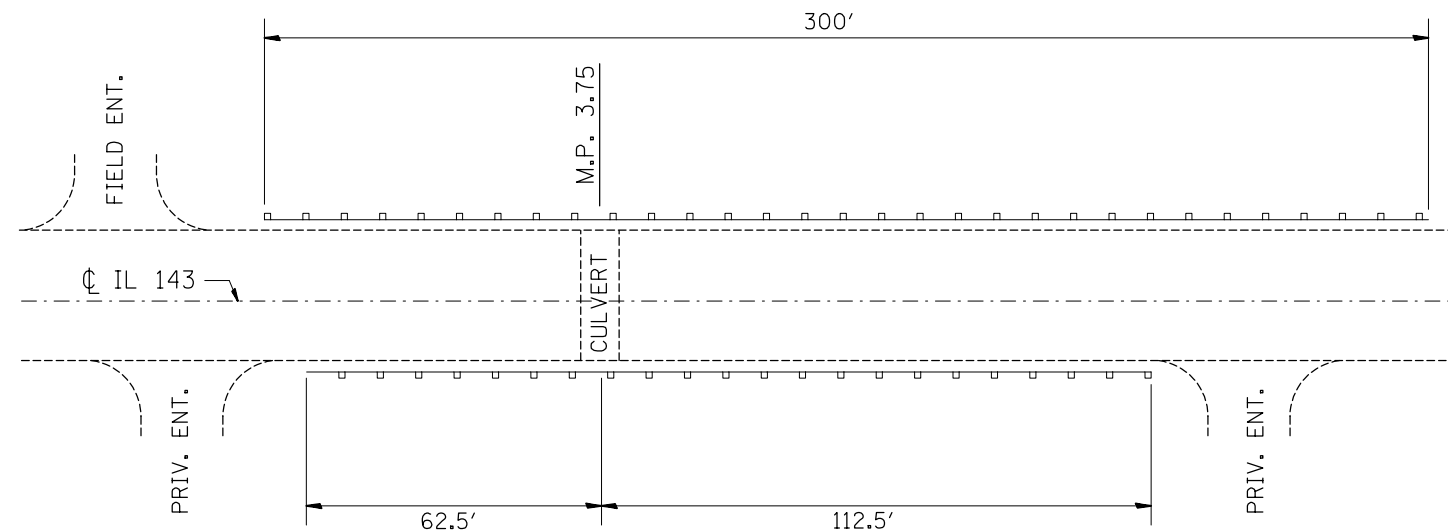
**GUARDRAIL REMOVAL**  
 (PLAN VIEW)



- \* = TRAFFIC BARRIER TERM. TYPE 1 SPECIAL (TANGENT)
- \*\* = TRAFFIC BARRIER TERM. TYPE 1 SPECIAL (TANGENT), MODIFIED
- \*\*\* = TRAFFIC BARRIER TERM. TYPE 6A
- \*\* = TRAFFIC BARRIER TERM. TYPE 6A (MODIFIED)

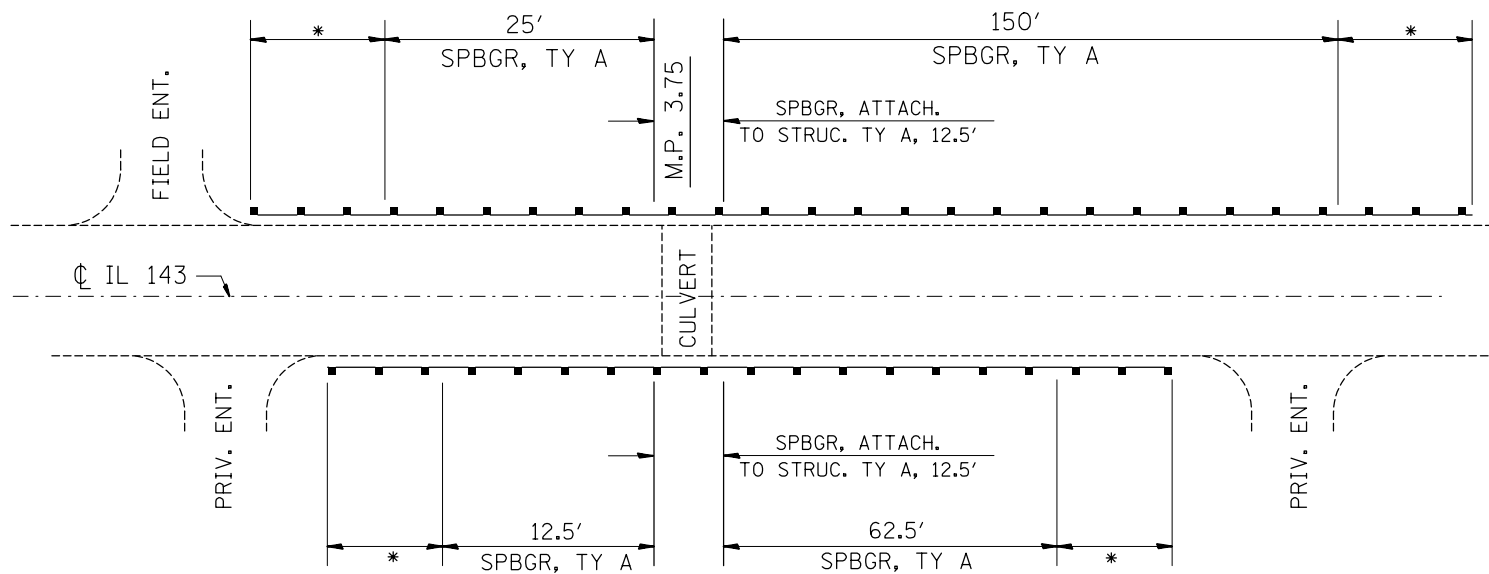
**PROPOSED GUARDRAIL**  
 (PLAN VIEW)

FILE NAME =	USER NAME = dntelmenjn	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>GUARDRAIL DETAILS (LOCATION #2)</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 76F27								
	PLOT DATE = 12/14/2012	DATE -	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		



= EXISTING GUARDRAIL TO BE REMOVED  
 475 FEET

**GUARDRAIL REMOVAL**  
 (PLAN VIEW)



- \* = TRAFFIC BARRIER TERM. TYPE 1 SPECIAL (TANGENT)
- \*\* = TRAFFIC BARRIER TERM. TYPE 1 SPECIAL (TANGENT), MODIFIED
- \*\*\* = TRAFFIC BARRIER TERM. TYPE 6A
- \*\* = TRAFFIC BARRIER TERM. TYPE 6A (MODIFIED)

**PROPOSED GUARDRAIL**  
 (PLAN VIEW)

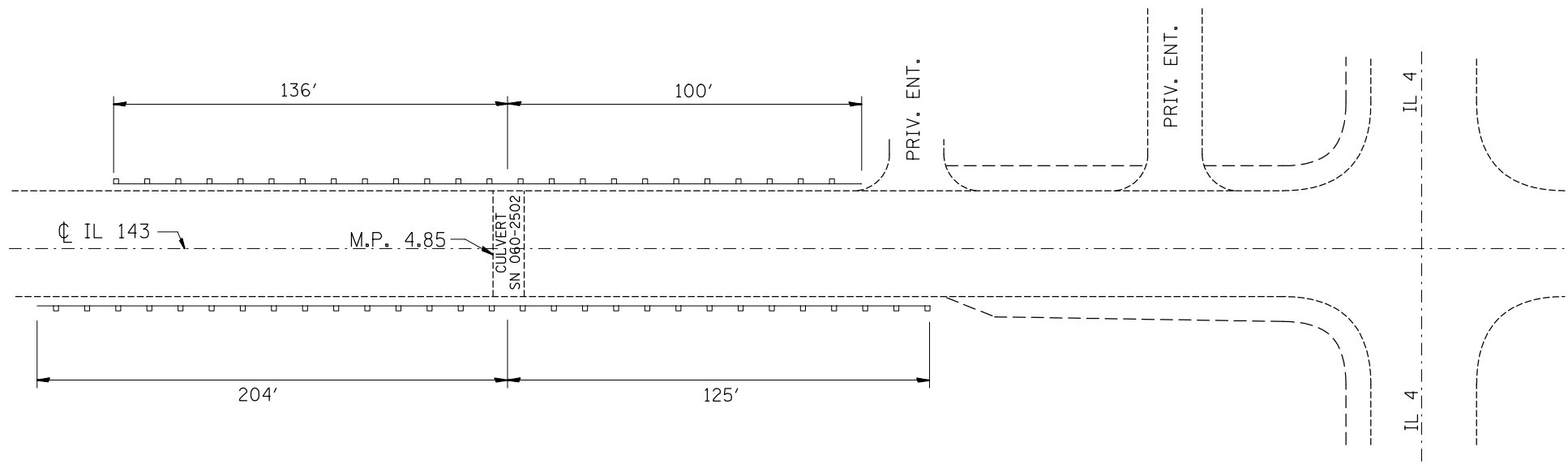
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	PLOT DATE = 12/14/2012	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**GUARDRAIL DETAILS**  
**(LOCATION #3)**

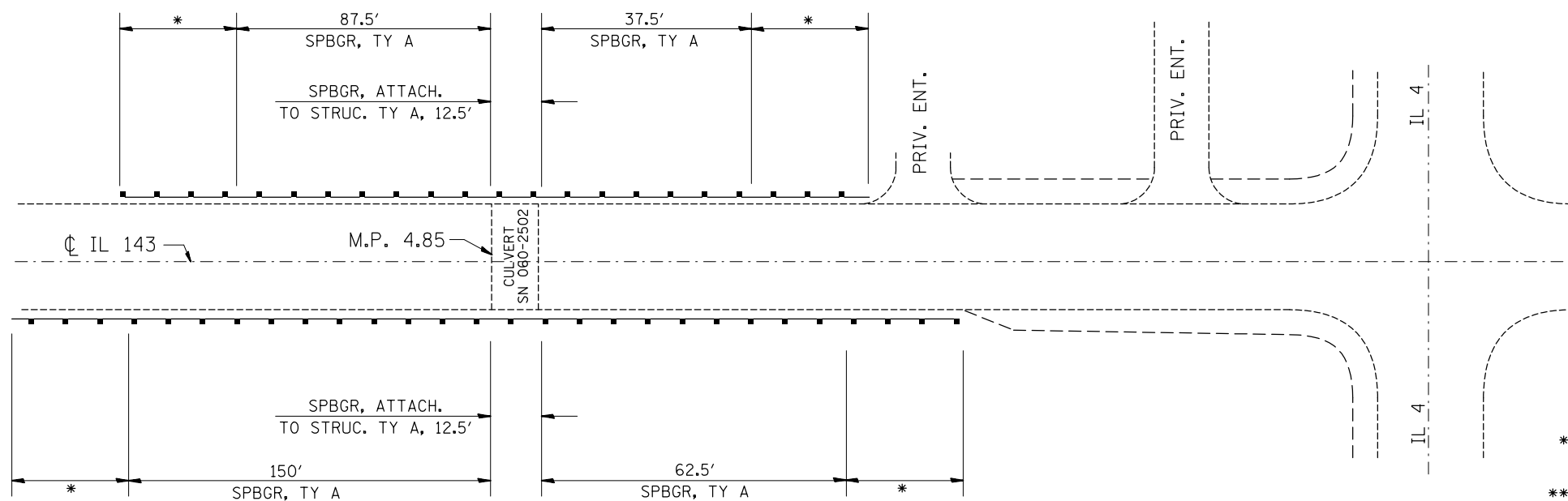
SCALE:      SHEET NO.    OF    SHEETS    STA.      TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
789	(54,58,68) RS-4	MADISON	44	18
CONTRACT NO. 76F27				
FED. ROAD DIST. NO.    ILLINOIS FED. AID PROJECT				



□ = EXISTING GUARDRAIL TO BE REMOVED  
565 FEET

GUARDRAIL REMOVAL  
(PLAN VIEW)



- \* = TRAFFIC BARRIER TERM. TYPE 1 SPECIAL (TANGENT)
- \*\* = TRAFFIC BARRIER TERM. TYPE 1 SPECIAL (TANGENT), MODIFIED
- \*\*\* = TRAFFIC BARRIER TERM. TYPE 6A
- \*\* = TRAFFIC BARRIER TERM. TYPE 6A (MODIFIED)

PROPOSED GUARDRAIL  
(PLAN VIEW)

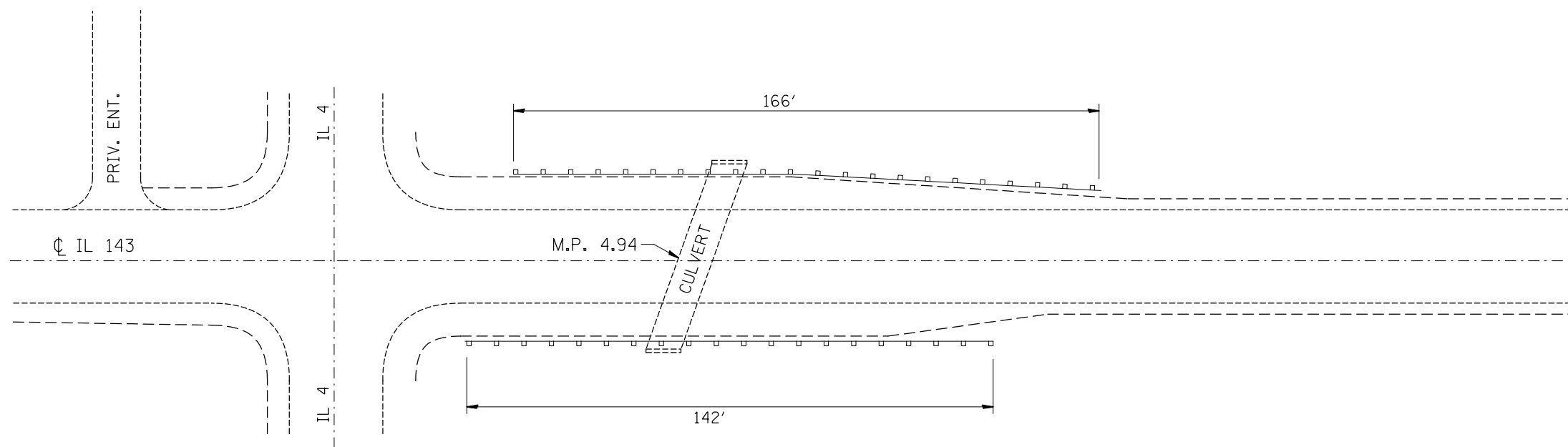
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	PLOT DATE = 12/14/2012	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

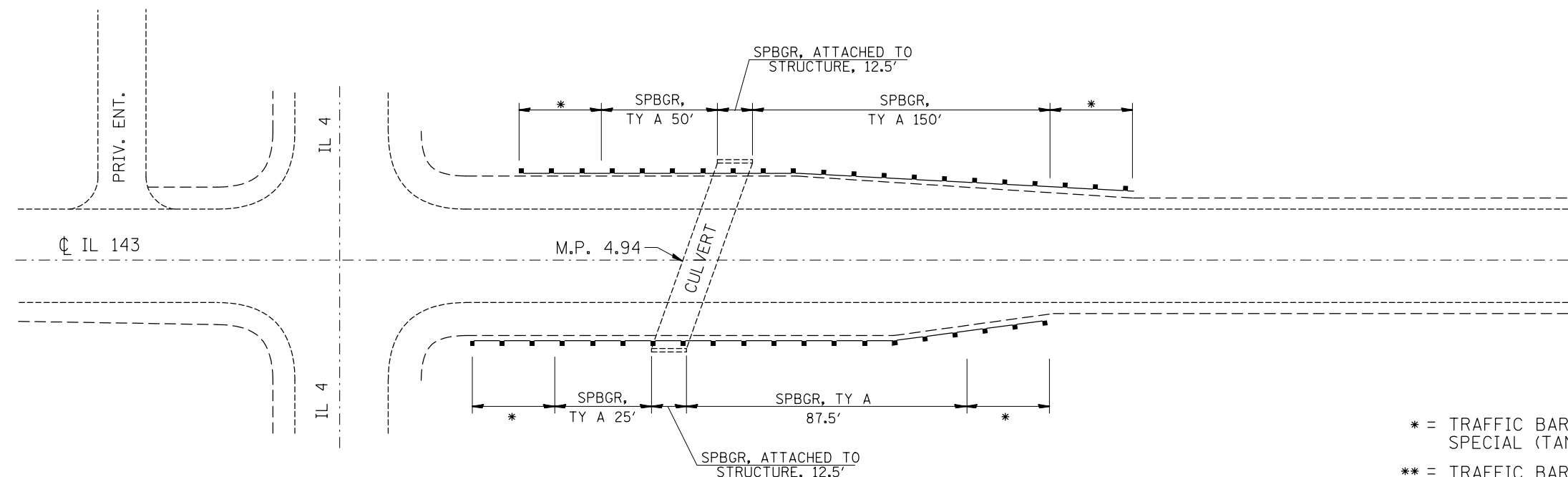
**GUARDRAIL DETAILS**  
**(LOCATION #4)**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
789	(54,58,68) RS-4	MADISON	44	19
CONTRACT NO. 76F27				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



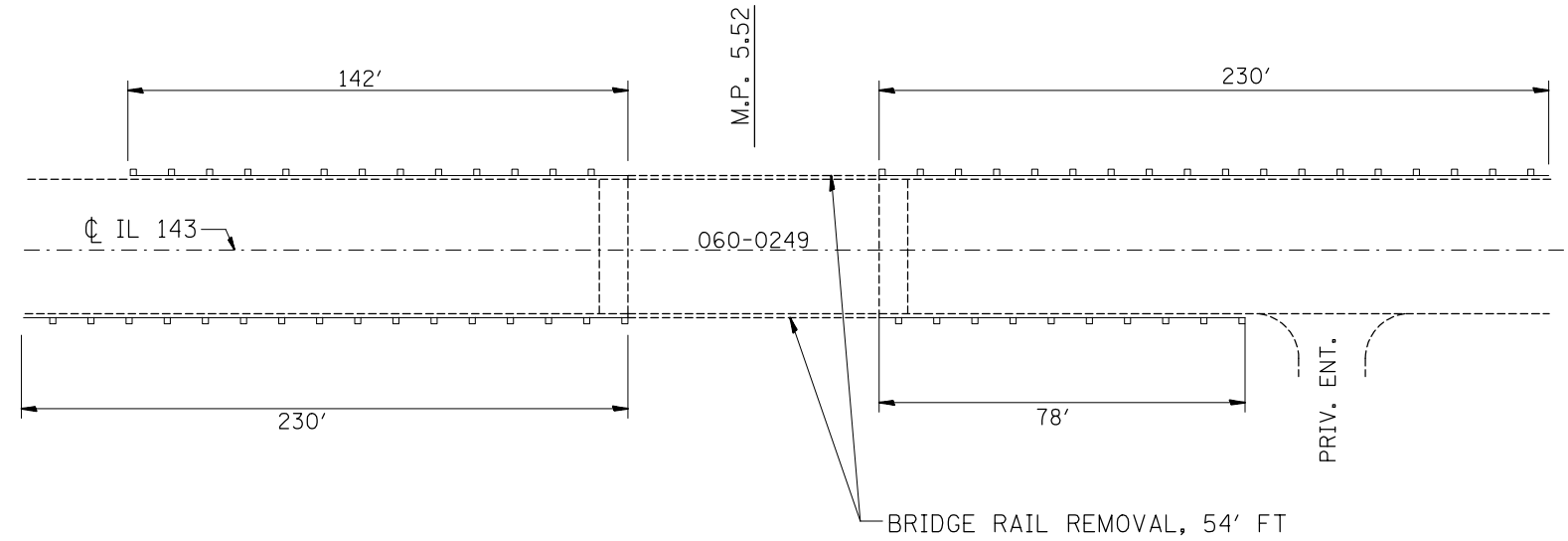
GUARDRAIL REMOVAL  
(PLAN VIEW)



PROPOSED GUARDRAIL  
(PLAN VIEW)

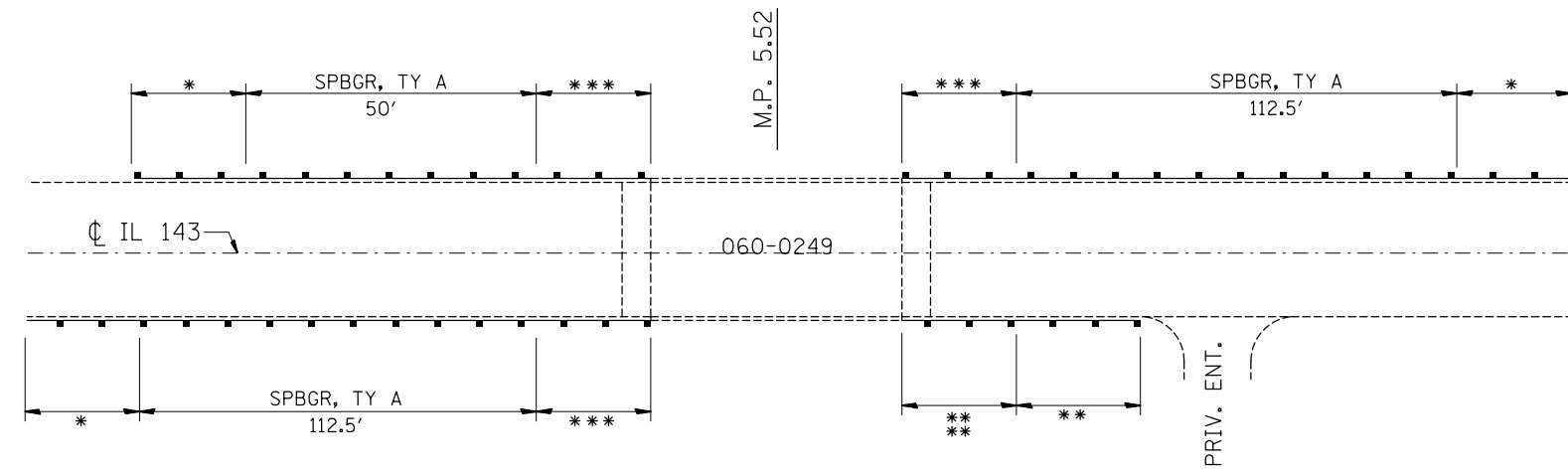
- \* = TRAFFIC BARRIER TERM. TYPE 1 SPECIAL (TANGENT)
- \*\* = TRAFFIC BARRIER TERM. TYPE 1 SPECIAL (TANGENT), MODIFIED
- \*\*\* = TRAFFIC BARRIER TERM. TYPE 6A
- \*\* = TRAFFIC BARRIER TERM. TYPE 6A (MODIFIED)

FILE NAME =	USER NAME = dntelmenjn	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>GUARDRAIL DETAILS (LOCATION #5)</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -						CONTRACT NO. 76F27					
	PLOT DATE = 12/14/2012	DATE -	REVISED -						FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					



□ = EXISTING GUARDRAIL TO BE REMOVED  
680 FEET

GUARDRAIL REMOVAL  
(PLAN VIEW)



NOTE 1: THE 'TBT, TYPE 1 (SPECIAL), MODIFIED' AT THE SOUTH EAST QUAD. SHALL MEASURE A TOTAL OF 37'-6" IN LENGTH

NOTE 2: THE SINGLE THICKNESS THRIE BEAM SECTION OF THE 'TBT, TYPE 6A (MODIFIED)' AT THE SOUTH EAST QUAD. SHALL BE SHORTENED FROM A FULL SECTION (12'-6") TO A HALF SECTION (6'-3") LEAVING A TOTAL LENGTH OF 38'-11 1/2"

\* = TRAFFIC BARRIER TERM. TYPE 1 SPECIAL (TANGENT)

\*\* = TRAFFIC BARRIER TERM. TYPE 1 SPECIAL (TANGENT), MODIFIED - SEE NOTE NO. 1

\*\*\* = TRAFFIC BARRIER TERM. TYPE 6A

\*\* = TRAFFIC BARRIER TERM. TYPE 6A (MODIFIED) - SEE NOTE NO. 2

PROPOSED GUARDRAIL  
(PLAN VIEW)

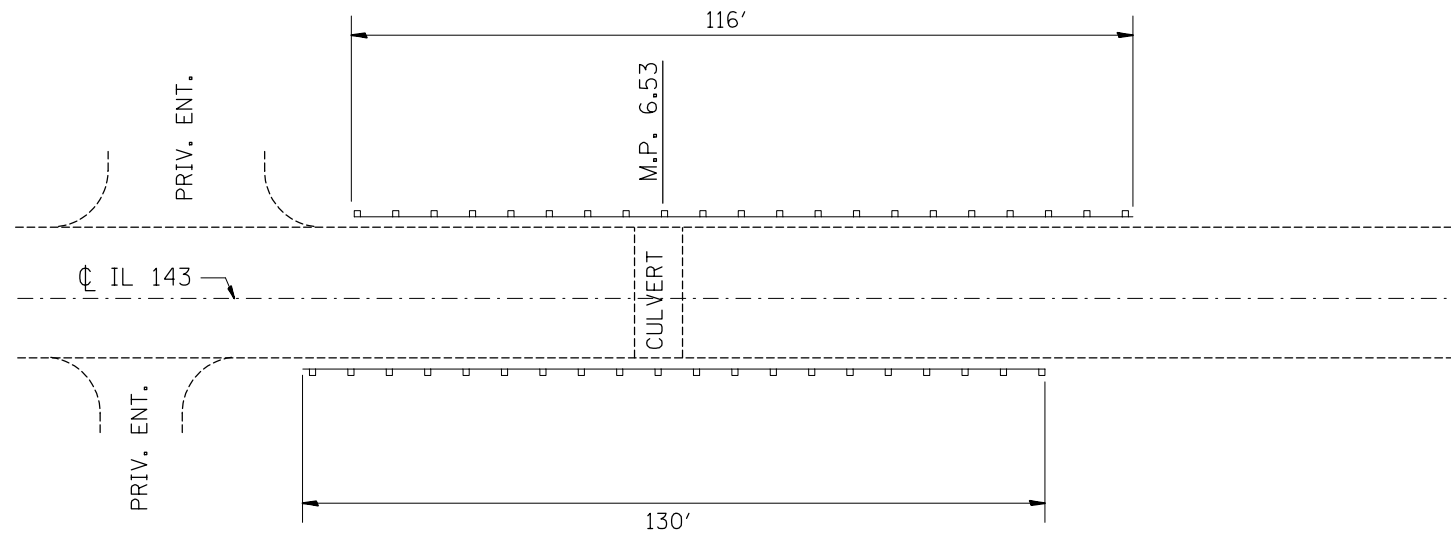
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/14/2012	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

GUARDRAIL DETAILS  
(LOCATION #6)

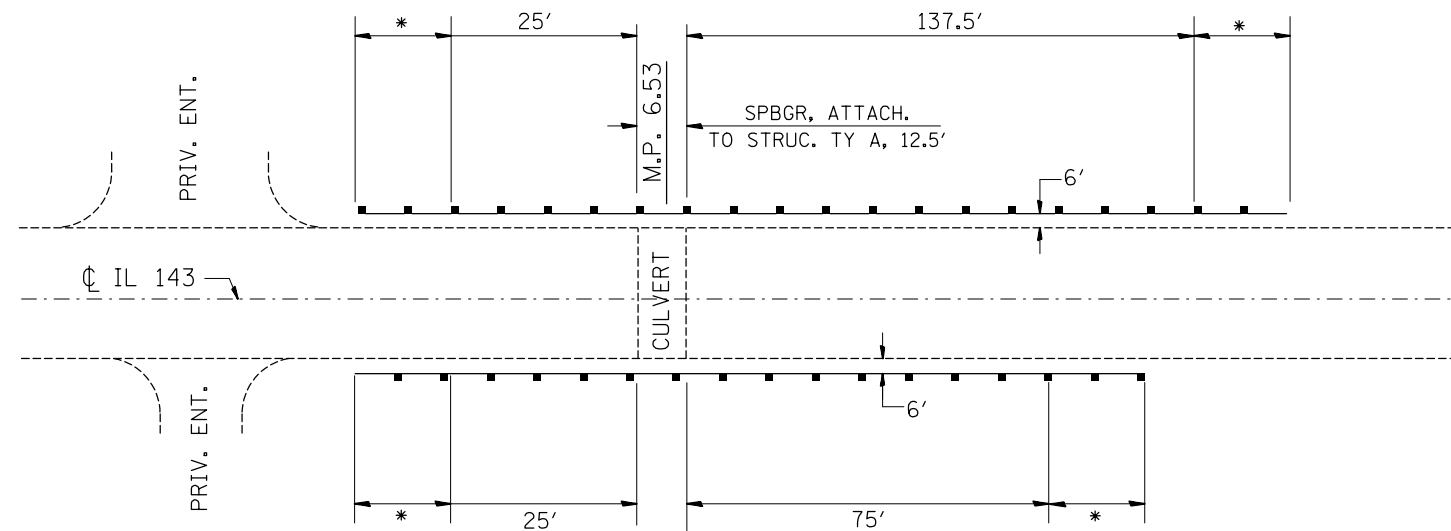
SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
789	(54,58,68) RS-4	MADISON	44	21
CONTRACT NO. 76F27				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



= EXISTING GUARDRAIL TO BE REMOVED  
 246 FEET

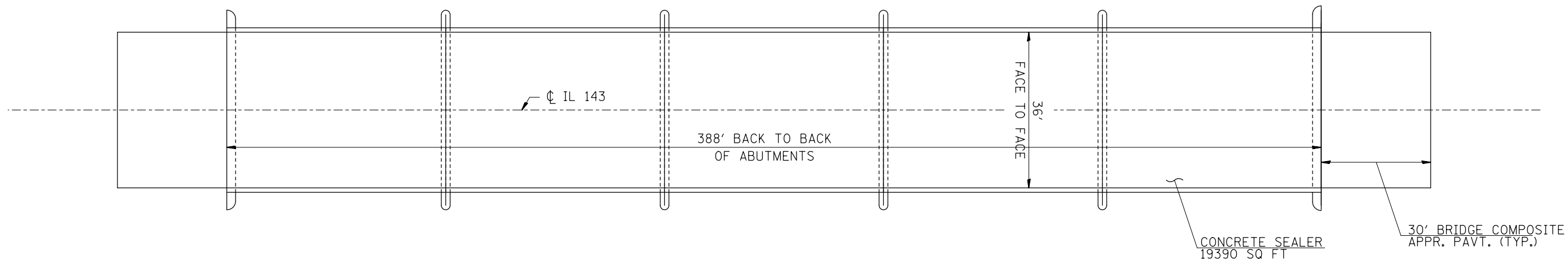
**GUARDRAIL REMOVAL**  
 (PLAN VIEW)



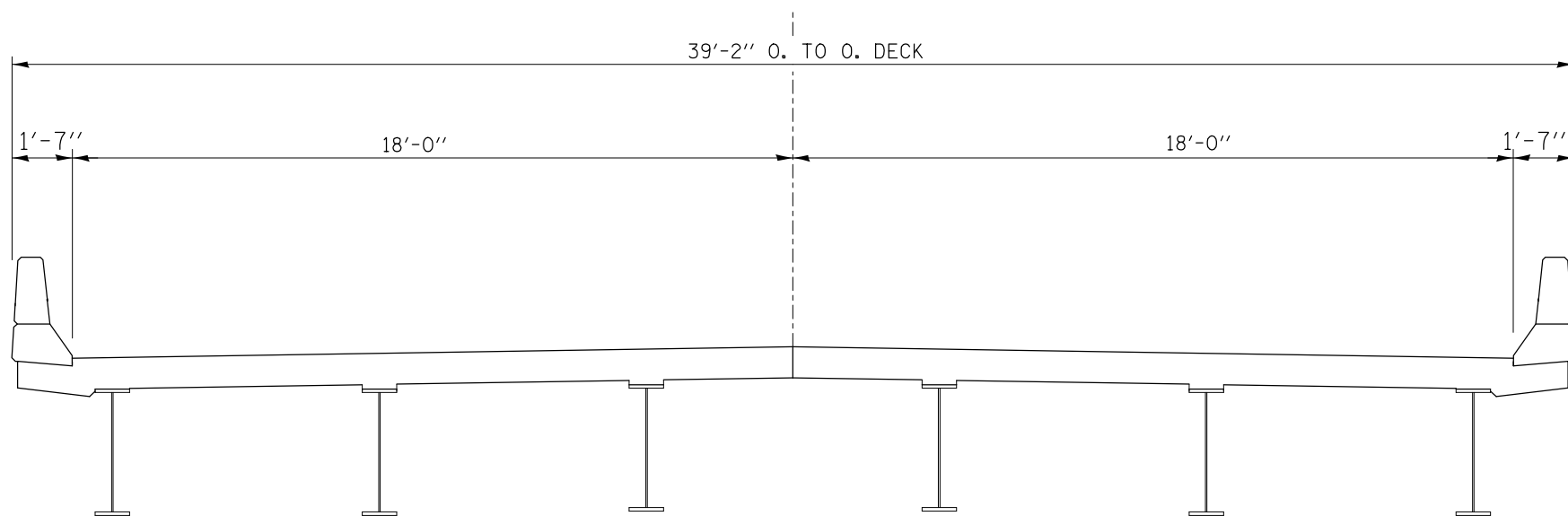
- \* = TRAFFIC BARRIER TERM. TYPE 1 SPECIAL (TANGENT)
- \*\* = TRAFFIC BARRIER TERM. TYPE 1 SPECIAL (TANGENT), MODIFIED
- \*\*\* = TRAFFIC BARRIER TERM. TYPE 6A
- \*\* = TRAFFIC BARRIER TERM. TYPE 6A (MODIFIED)

**PROPOSED GUARDRAIL**  
 (PLAN VIEW)

FILE NAME = c:\pwork\pwork\pwork\dintelman\j\0281732\p876f27-shr-plan.dgn	USER NAME = dintelmanjn	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>GUARDRAIL DETAILS (LOCATION #7)</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -						789	(54,58,68) RS-4	MADISON	44	22
PLOT DATE = 12/14/2012	DATE -	REVISED -	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	CONTRACT NO. 76F27			
									FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



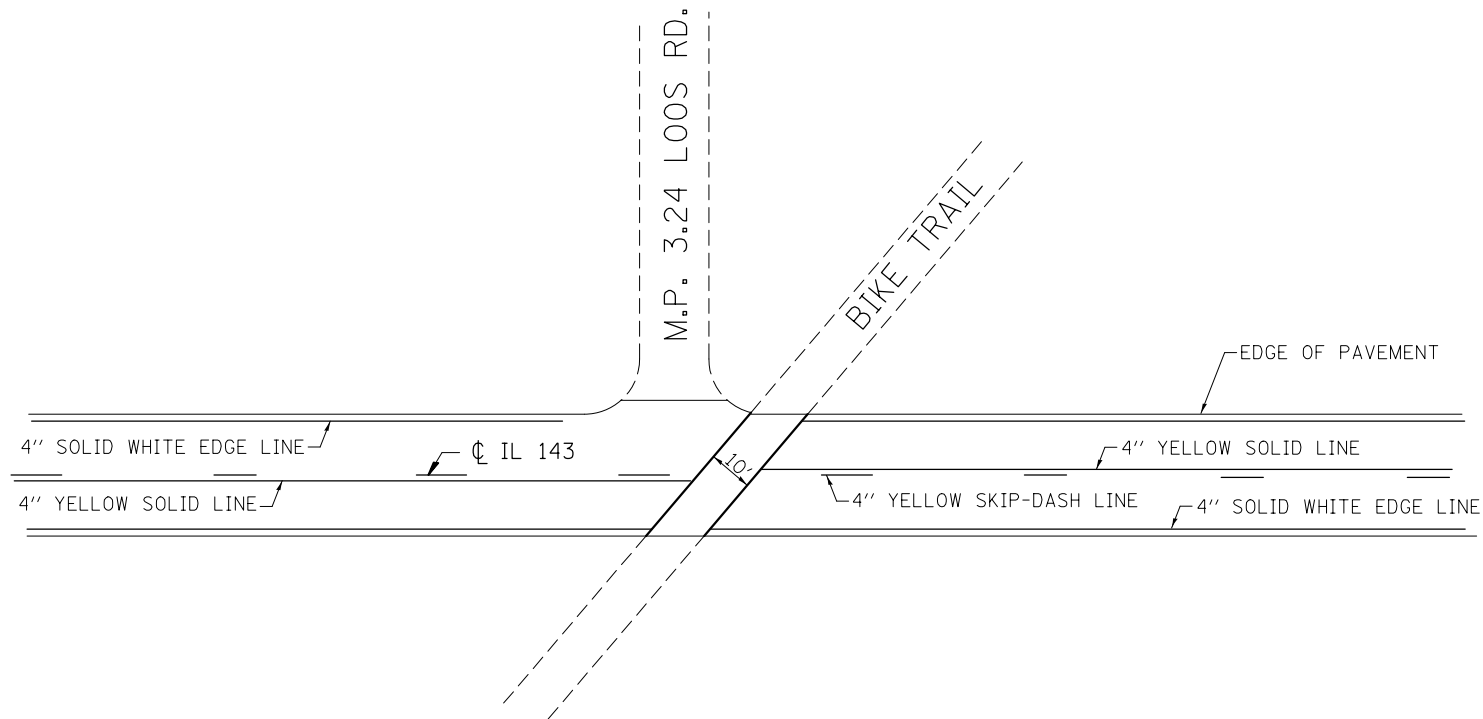
PLAN VIEW  
S.N. 060-0340



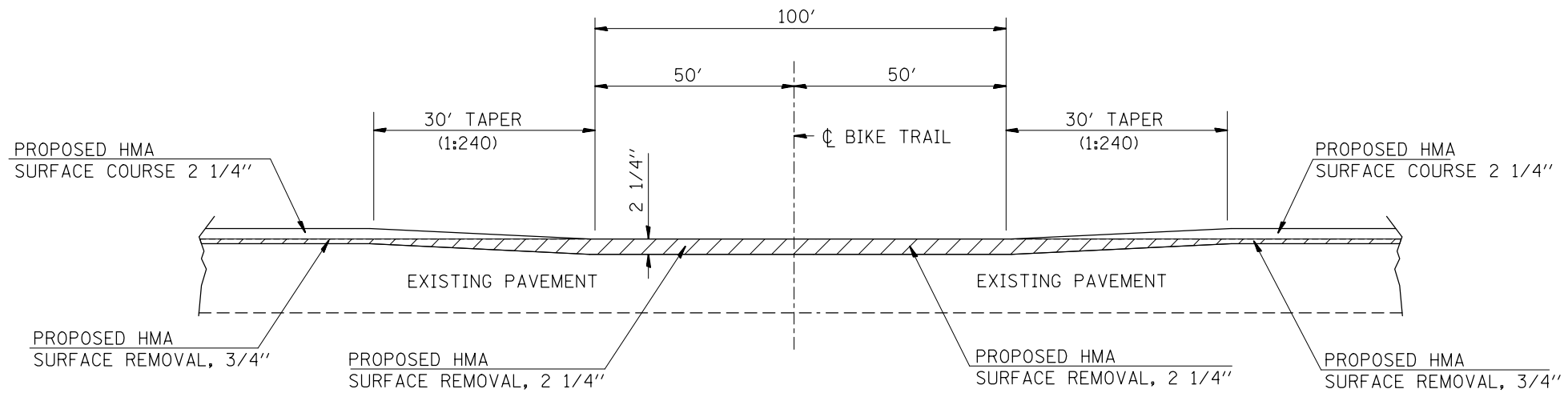
CROSS SECTION  
S.N. 060-0340

NOTE: CONCRETE SEALER SHALL BE APPLIED TO BOTH APPROACH SLABS AS WELL AS THE STRUCTURE.

USER NAME = dntelmerjn	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STR.060-0340 PLAN &amp; CROSS SECTION</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -						789	(54,58,68) RS-4	MADISON	44	23
PLOT DATE = 12/14/2012	CHECKED -	REVISED -		SCALE:      SHEET NO.    OF    SHEETS    STA.            TO STA.				CONTRACT NO. 76F27				
	DATE -	REVISED -						FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



RESURFACING DETAIL  
PLAN VIEW  
(@BIKE TRAIL)



RESURFACING DETAIL  
ELEVATION VIEW  
(@BIKE TRAIL)

USER NAME = dntelmerjn	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/14/2012	DATE -	REVISED -

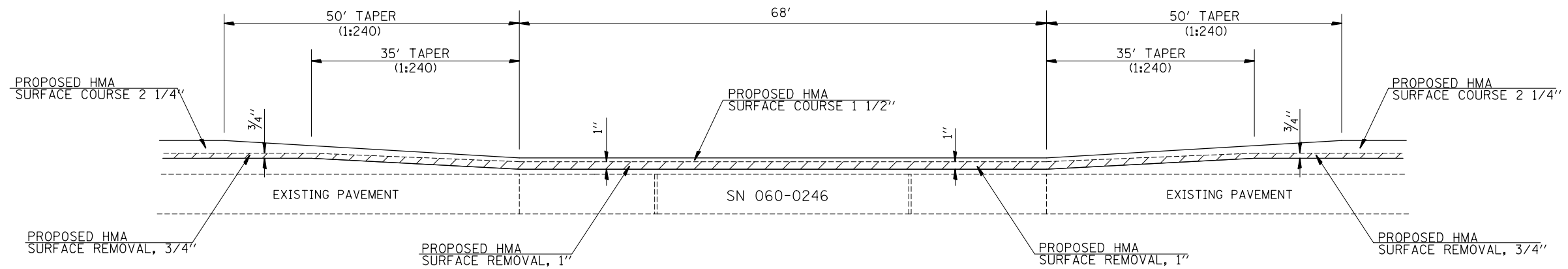
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BIKE TRAIL AND  
RESURFACING DETAILS

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
789	(54,58,68) RS-4	MADISON	44	24
CONTRACT NO. 76F27				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

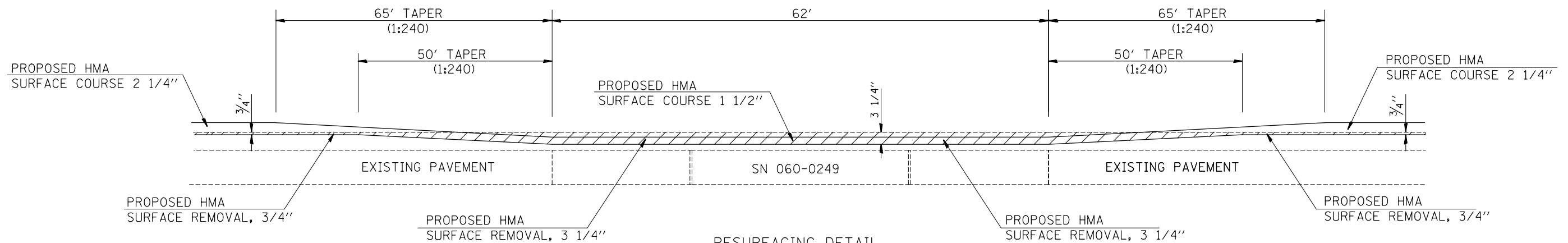




RESURFACING DETAIL  
ELEVATION VIEW  
(@SN 060-0246)

NOTE: THE TRANSITION FROM 3/4" HMA SURFACE REMOVAL TO 1" WILL BE INCLUDED IN THE PAY ITEM PROPOSED HMA SURFACE REMOVAL, 1"

PROPOSED HMA SURFACE REMOVAL, 1" = 399 sq yd



RESURFACING DETAIL  
ELEVATION VIEW  
(@SN 060-0249)

NOTE: THE TRANSITION FROM 3/4" HMA SURFACE REMOVAL TO 3 1/4" WILL BE INCLUDED IN THE PAY ITEM FOR PROPOSED HMA SURFACE REMOVAL, 3 1/4"

PROPOSED HMA SURFACE REMOVAL, 3 1/4" = 338 sq yd

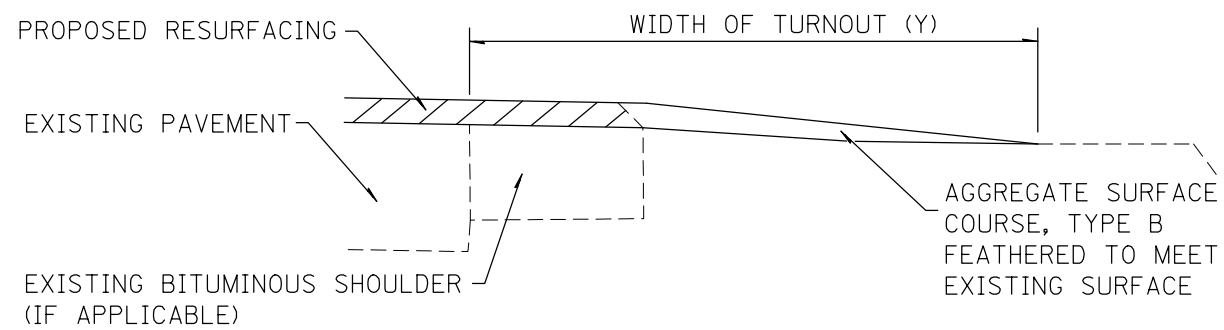
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	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/14/2012	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

RESURFACING DETAILS

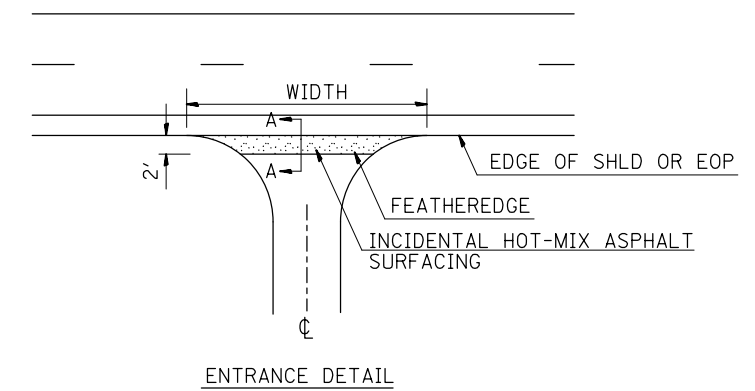
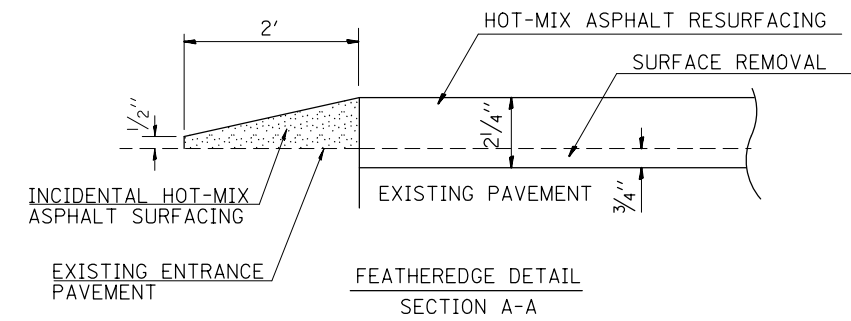
SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
789	(54,58,68) RS-4	MADISON	44	25
CONTRACT NO. 76F27				
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			

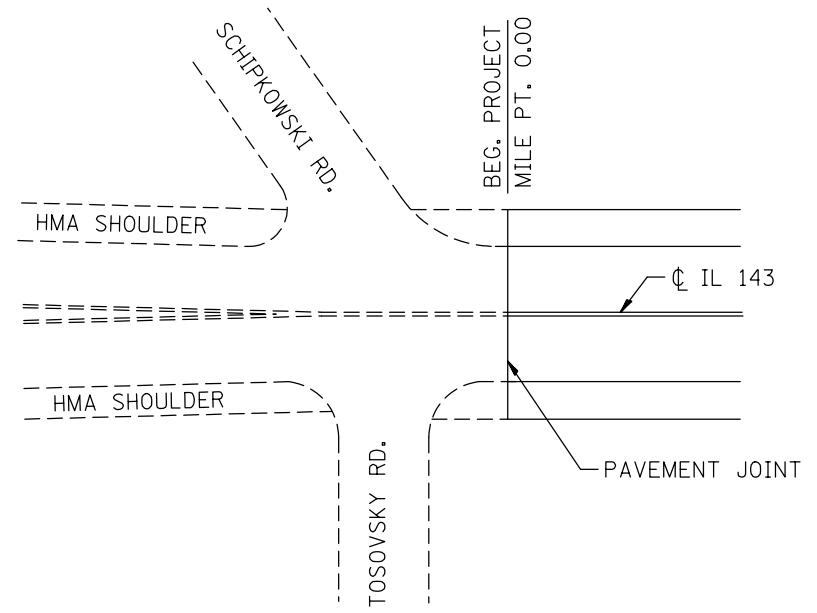


TYPICAL SECTION AT MAILBOX TURNOUT

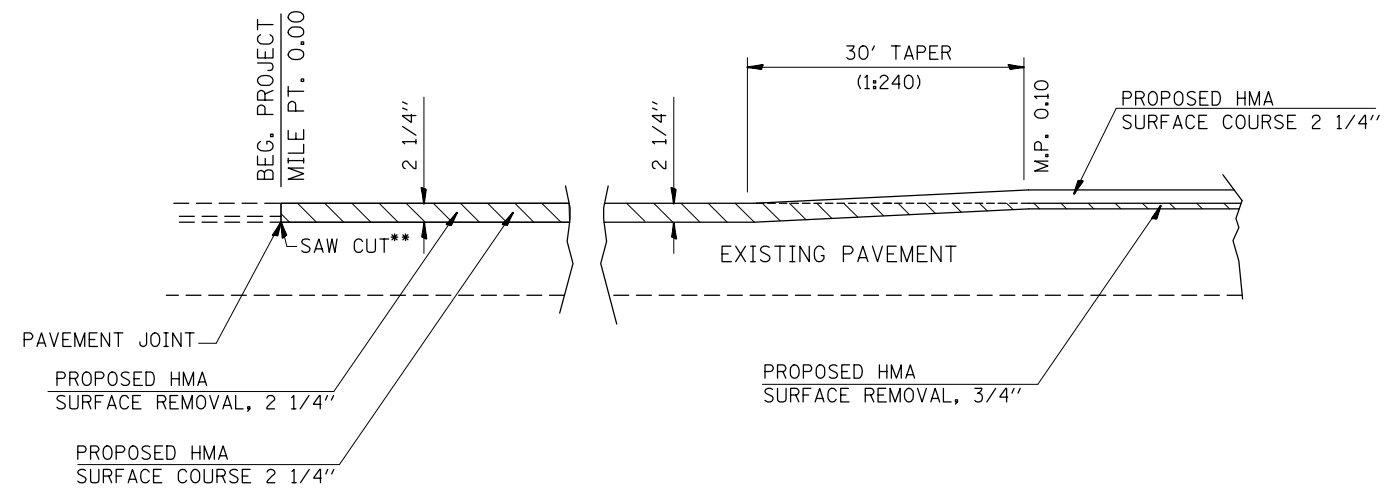
NOTE: SEE STANDARD 406201 FOR MAILBOX TURNOUT DETAILS



USER NAME = dntelmerjn	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BIKE TRAIL AND RESURFACING DETAILS</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -					789	(54,58,68) RS-4	MADISON	44	26
PLOT DATE = 12/14/2012	CHECKED -	REVISED -					CONTRACT NO. 76F27				
	DATE -	REVISED -					FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			
			SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.				

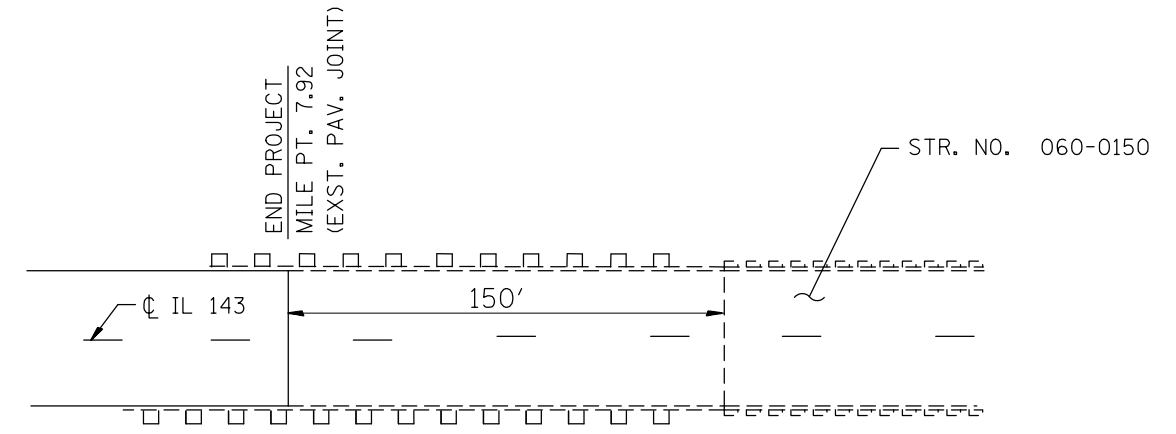
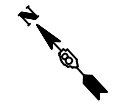


BEGIN RESURFACING  
PLAN VIEW

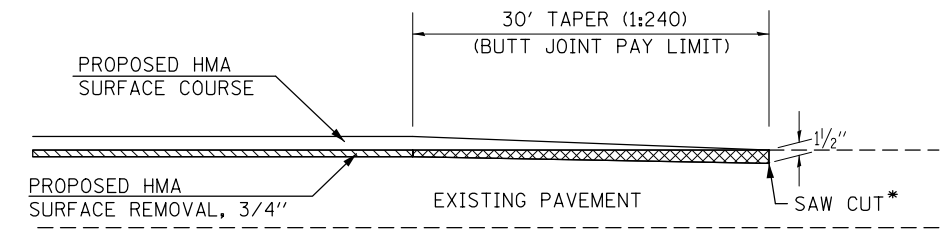


BEG. RESURFACING &  
RESURFACING TRANSITION  
ELEVATION DETAIL

\*\*COST OF SAWCUT TO BE INCLUDED IN HOT-MIX ASPHALT SURFACE REMOVAL PAY ITEM.



END RESURFACING  
PLAN VIEW



\* COST OF SAWCUT TO BE INCLUDED IN HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

BUTT JOINT  
ELEVATION VIEW

- M.P. 1.80
- M.P. 1.88
- M.P. 4.90
- M.P. 4.91
- M.P. 7.92

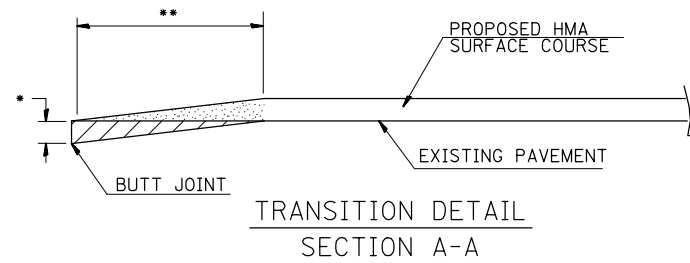
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	PLOT DATE = 12/14/2012	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

RESURFACING DETAILS

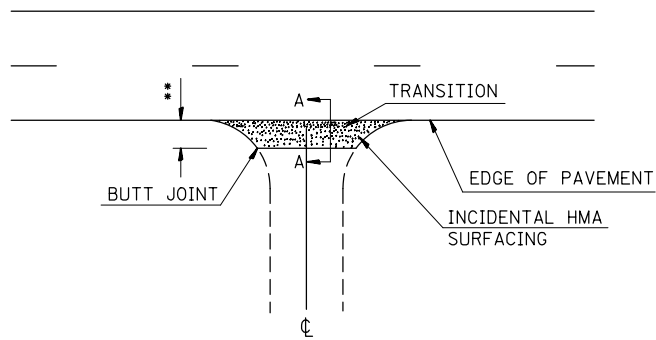
SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
789	(54,58,68) RS-4	MADISON	44	27
CONTRACT NO. 76F27				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



\* 1 1/2" HMA BUTT JOINT  
1" P.C.C. BUTT JOINT

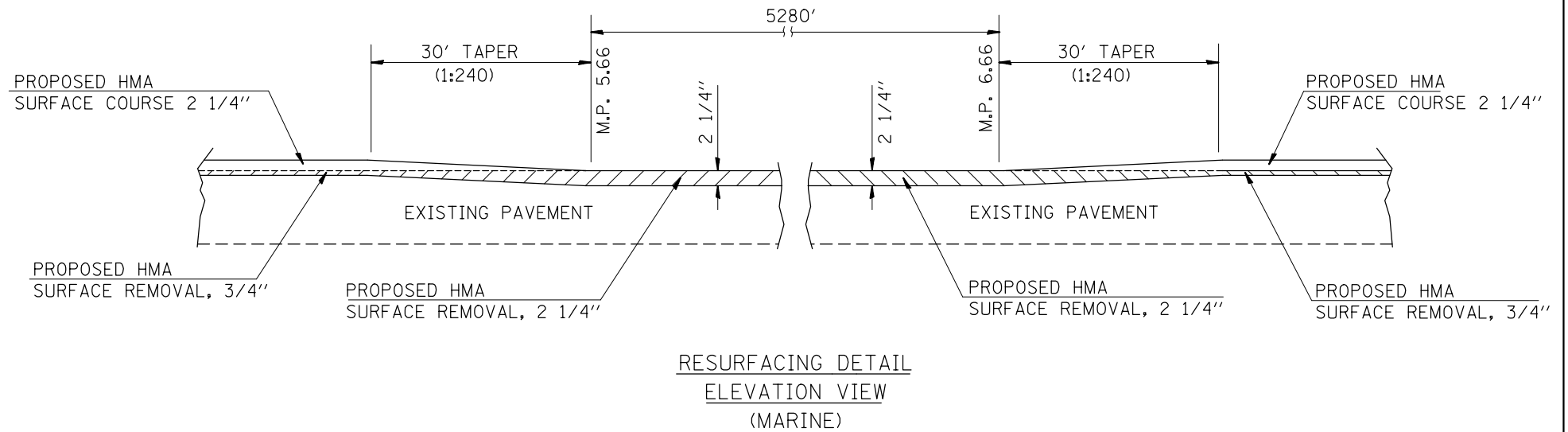
HOT-MIX ASPHALT SURFACE REMOVAL-BUTT JOINT OR P.C.C. SURFACE REMOVAL-BUTT JOINT



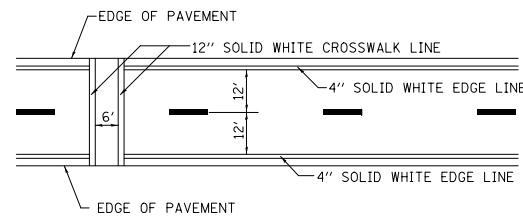
SIDEROAD DETAIL

EOP ELEVATION CHANGE	**TOTAL TRANSITION LENGTH
3/4"	3.5'
1"	5'
1-1/2"	7.5'
2"	10'
2-1/2"	12.5'
3"	15'
3-1/2"	17.5'
4"	20'
4-1/2"	22.5'
5"	25'
5-1/2"	27.5'
6"	30'

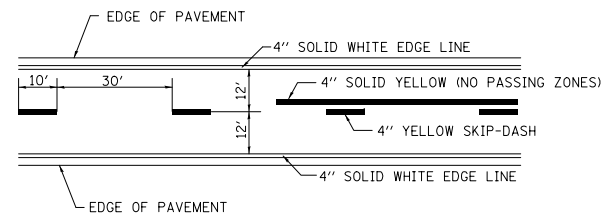
NOTE:  
WHERE THE HMA TRANSITION IS MATCHING INTO AN EXISTING HMA SIDE ROAD SURFACE, A MILLED BUTT JOINT SHALL BE CONSTRUCTED WITHIN THE LIMITS OF THE TOTAL TRANSITION LENGTH ON THE LOCAL ROUTE.



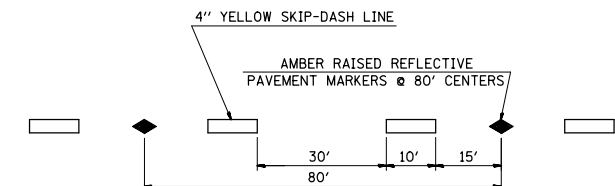
RESURFACING DETAIL  
ELEVATION VIEW  
(MARINE)



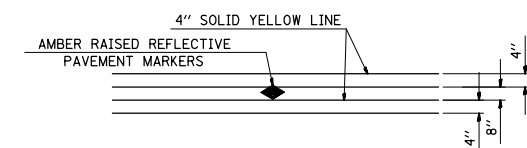
CROSSWALK PAVEMENT MARKING DETAIL



PAVEMENT MARKING DETAIL



TYPICAL APPLICATION  
FOR YELLOW SKIP-DASH LINES  
WITH RAISED REFLECTIVE PAVEMENT MARKERS



TYPICAL APPLICATION  
FOR DOUBLE YELLOW LANE LINES  
WITH RAISED REFLECTIVE PAVEMENT MARKERS

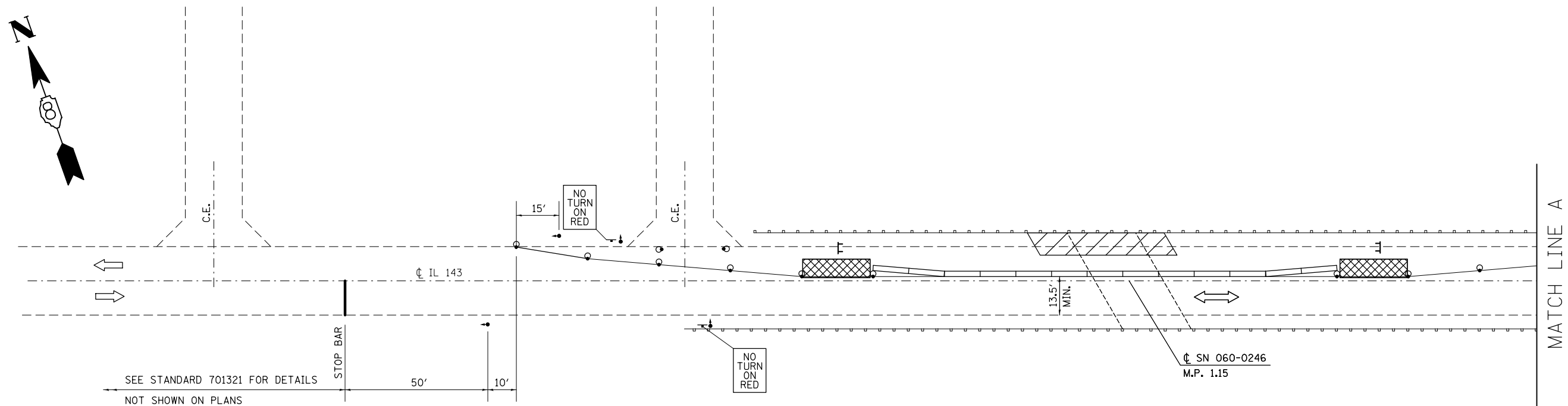
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/14/2012	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

RESURFACING, PAVEMENT MARKING  
DETAILS

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
789	(54,58,68) RS-4	MADISON	44	28
CONTRACT NO. 76F27				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



**SUGGESTED SEQUENCE OF CONSTRUCTION (SN 060-0246 & 060-0249)**

**STAGE I:**

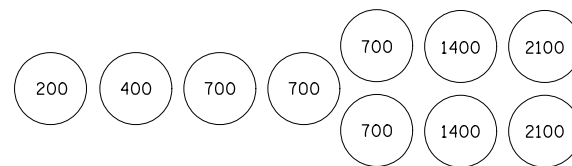
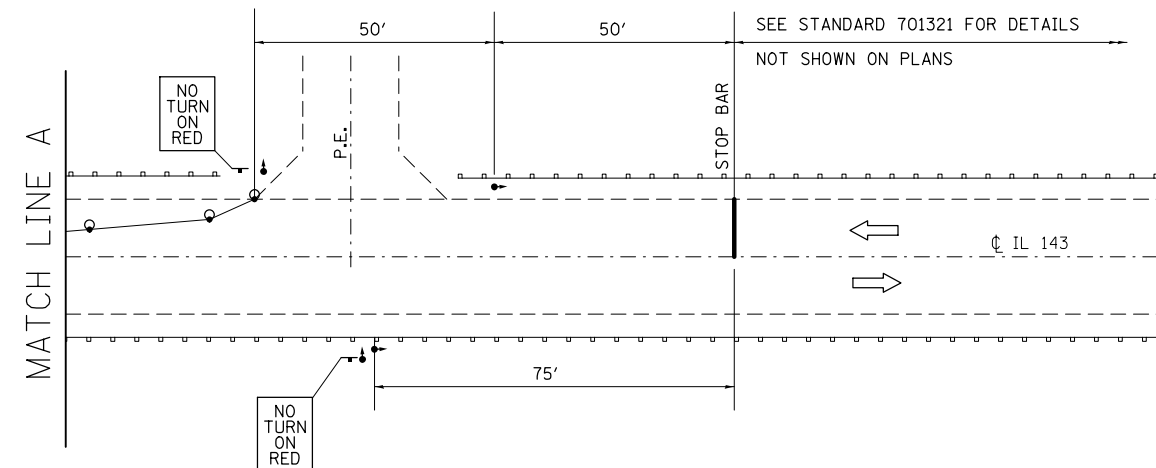
- PLACE STOP BARS AS SHOWN ON THE PLANS.
- REMOVE CONFLICTING PAVEMENT MARKING BETWEEN STOP BARS.
- PLACE TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATORS, TEMPORARY.
- SEE STANDARD 701321 FOR DETAILS NOT SHOWN ON PLANS.
- PERFORM STRUCTURAL REPAIR WORK AS SHOWN ON THE STRUCTURE PLANS WITHIN THE STAGE I WORK AREA.
- PLACE 3.5" HMA SURFACE COURSE OVER PRECAST CONCRETE BRIDGE SLAB AND CONCRETE SUPERSTRUCTURE ON SN 060-0246 AND 5" HMA SURFACE COURSE OVER PRECAST CONCRETE BRIDGE SLAB AND CONCRETE SUPERSTRUCTURE ON SN 060-0249 FOR STAGE I CONSTRUCTION. HMA SURFACE COURSE SHALL BE PLACED TO MATCH EXISTING CONDITIONS.

**STAGE II:**

- RELOCATE TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATORS, TEMPORARY.
- PERFORM STRUCTURAL REPAIR WORK AS SHOWN ON THE STRUCTURE PLANS WITHIN THE STAGE II WORK AREA.
- PLACE 3.5" HMA SURFACE COURSE OVER PRECAST CONCRETE BRIDGE SLAB AND CONCRETE SUPERSTRUCTURE ON SN 060-0246 AND 5" HMA SURFACE COURSE OVER PRECAST CONCRETE BRIDGE SLAB AND CONCRETE SUPERSTRUCTURE ON SN 060-0249 FOR STAGE I CONSTRUCTION. HMA SURFACE COURSE SHALL BE PLACED TO MATCH EXISTING CONDITIONS.

**POST-STAGE II:**

MILL PROJECT TO THE LIMITS SPECIFIED IN THE PLANS. RESURFACE AS SPECIFIED IN THE PLANS. MILLING DEPTH AND PROPOSED HMA SURFACE COURSE FOR SN 060-0246 & SN 060-0249 WERE DETERMINED BASED ON OBSERVABLE FIELD CONDITIONS AND THE 3 1/8" MAXIMUM HMA THICKNESS REQUIREMENT SHOWN ON THE STEEL RAILING SHEETS IN THE STRUCTURE PLANS. IF ADJUSTMENTS PROVE NECESSARY, THE CONTRACTOR SHALL STILL MEET THE 3 1/8" MAXIMUM HMA THICKNESS REQUIREMENT.



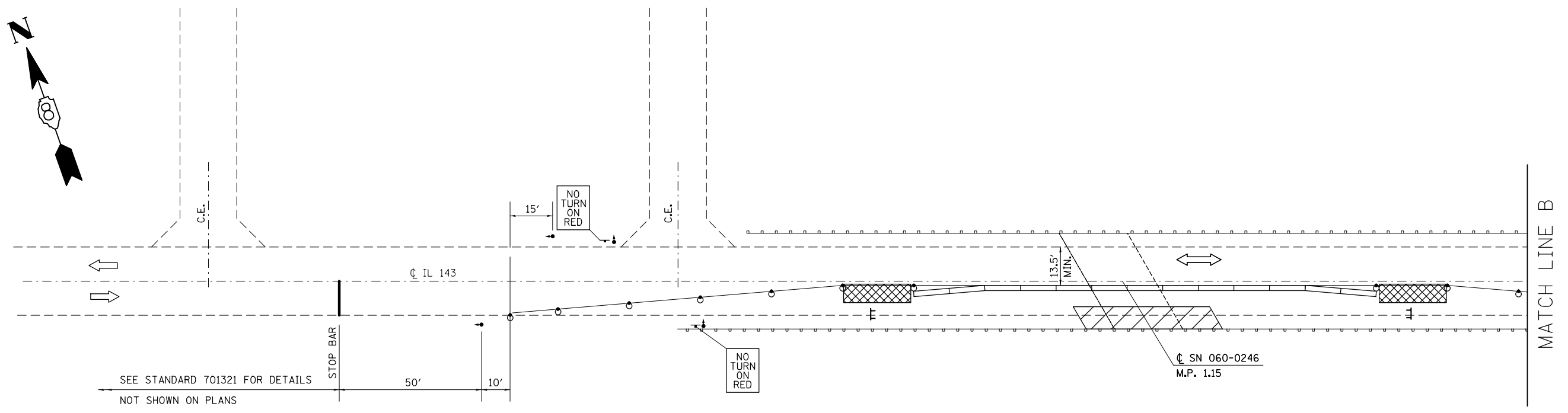
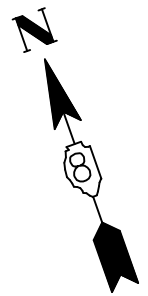
SAND MODULE IMPACT ATTENUATOR LAYOUT (IF OPTION USED)

**LEGEND**

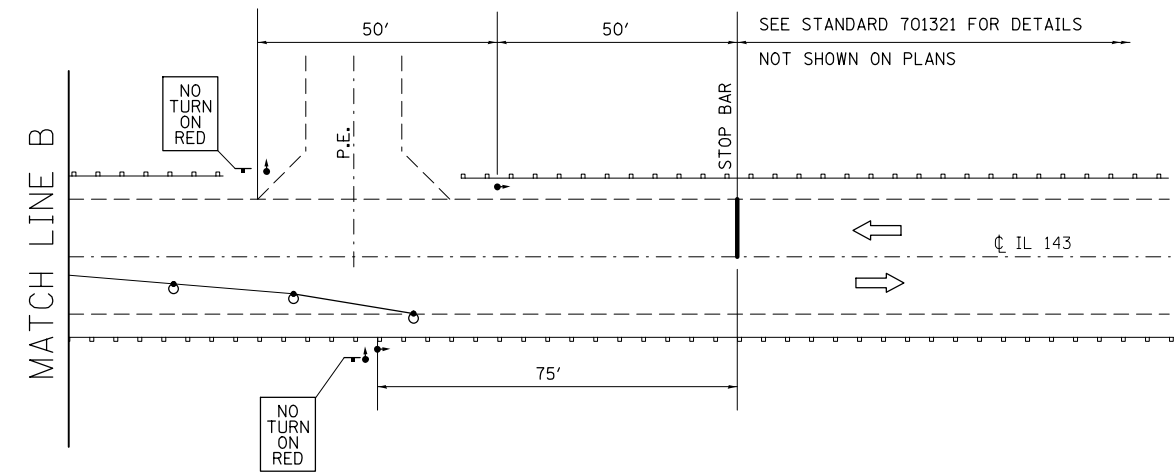
- WORK AREA
- IMPACT ATTENUATOR
- TEMPORARY CONCRETE BARRIER
- BARRELS WITH STEADY BURNING BI-DIRECTIONAL LIGHT
- TEMPORARY BRIDGE TRAFFIC SIGNALS
- TYPE III BARRICADE

NOTE: NOT TO SCALE

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	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -				789	(54,58,68) RS-4	MADISON	44	29
PLOT DATE = 12/14/2012	DATE -	REVISED -	REVISED -	SHEET NO. 1 OF 2 SHEETS		STA.	TO STA.		CONTRACT NO. 76F27		
ILLINOIS FED. AID PROJECT											









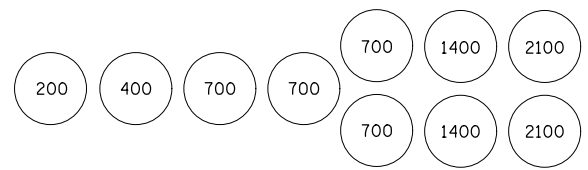
SEE STANDARD 701321 FOR DETAILS  
NOT SHOWN ON PLANS



SEE STANDARD 701321 FOR DETAILS  
NOT SHOWN ON PLANS

**LEGEND**

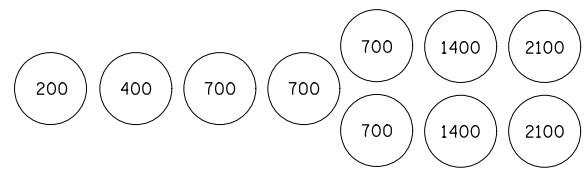
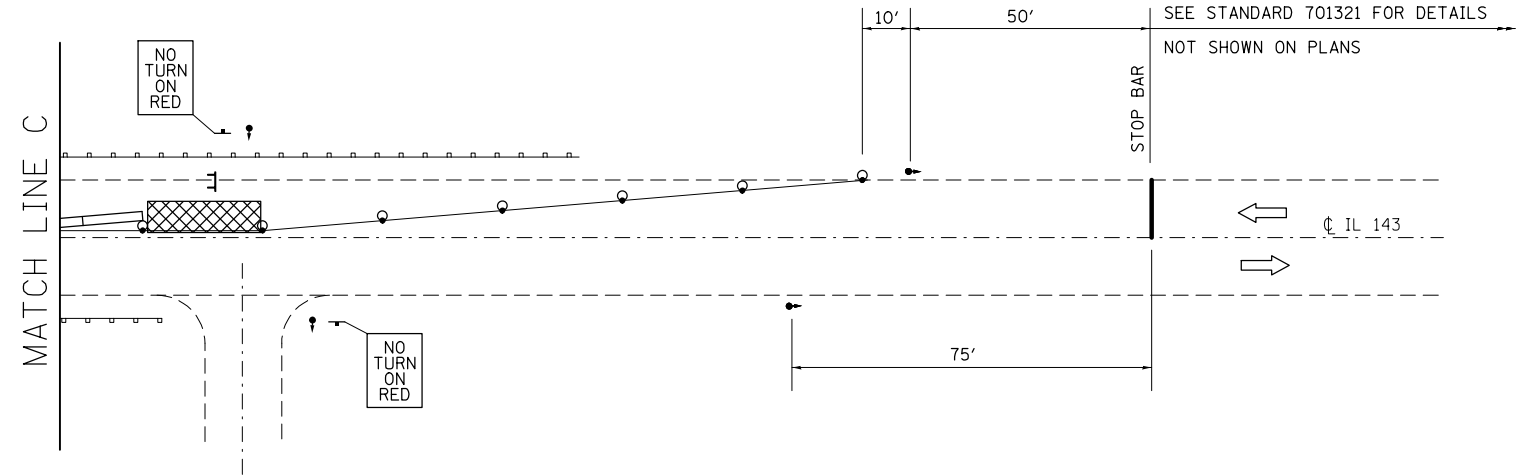
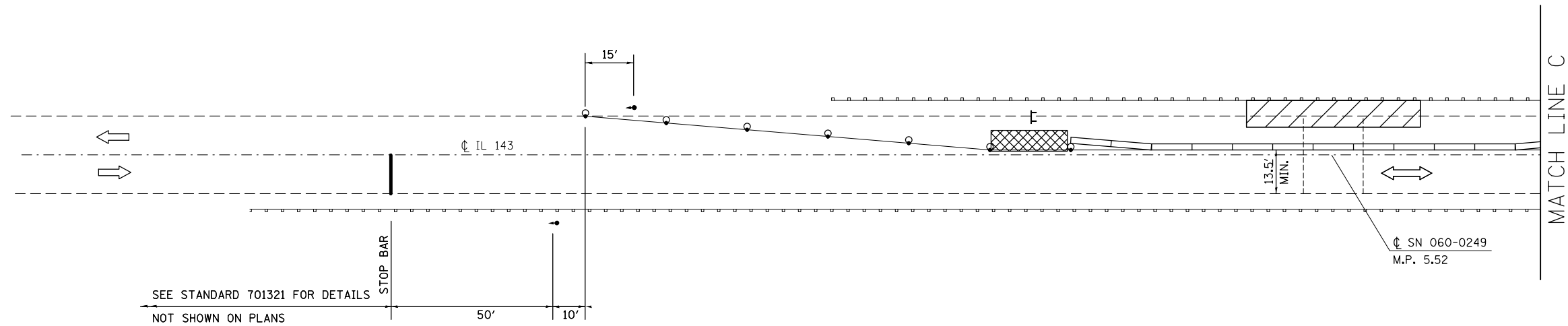
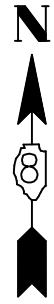
-  WORK AREA
-  IMPACT ATTENUATOR
-  TEMPORARY CONCRETE BARRIER
-  BARRELS WITH STEADY BURNING BI-DIRECTIONAL LIGHT
-  TEMPORARY BRIDGE TRAFFIC SIGNALS
-  TYPE III BARRICADE



SAND MODULE IMPACT ATTENUATOR LAYOUT  
(IF OPTION USED)







NOTE: NOT TO SCALE

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	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -		SHEET NO. 2 OF 2 SHEETS	STA.	TO STA.	789	(54,58,68) RS-4	MADISON	44
PLOT DATE = 12/14/2012	DATE -	REVISED -	REVISED -	ILLINOIS FED. AID PROJECT CONTRACT NO. 76F27							



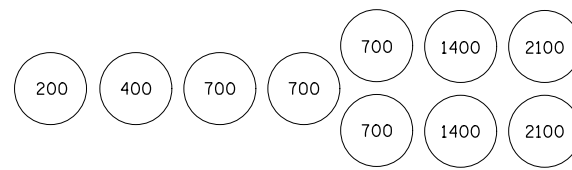
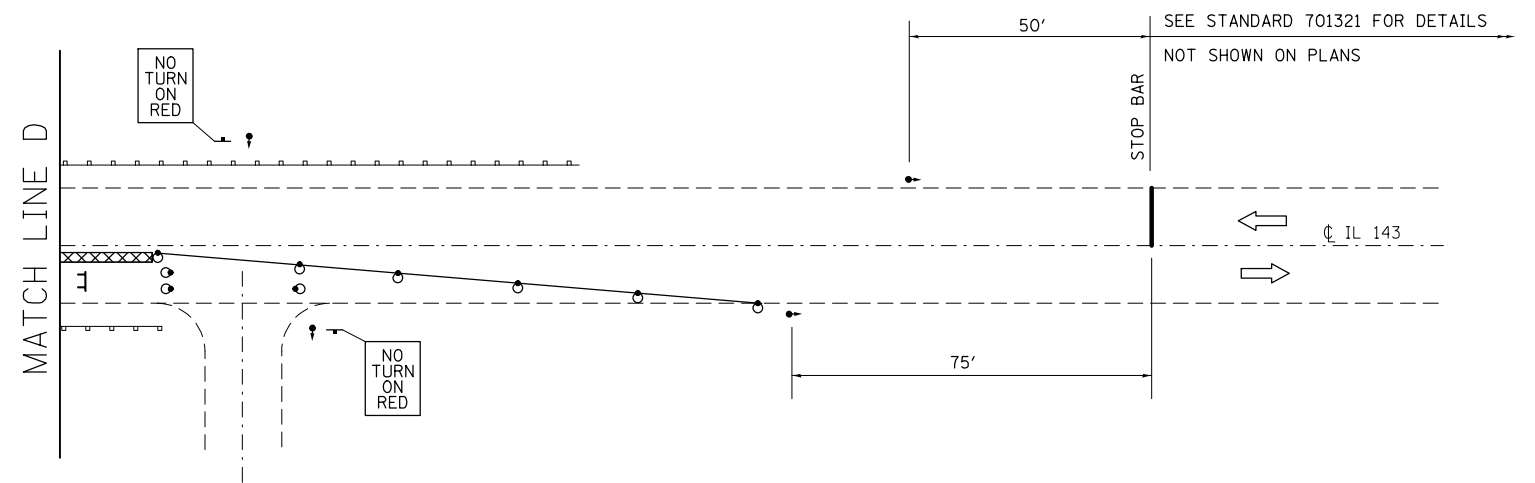
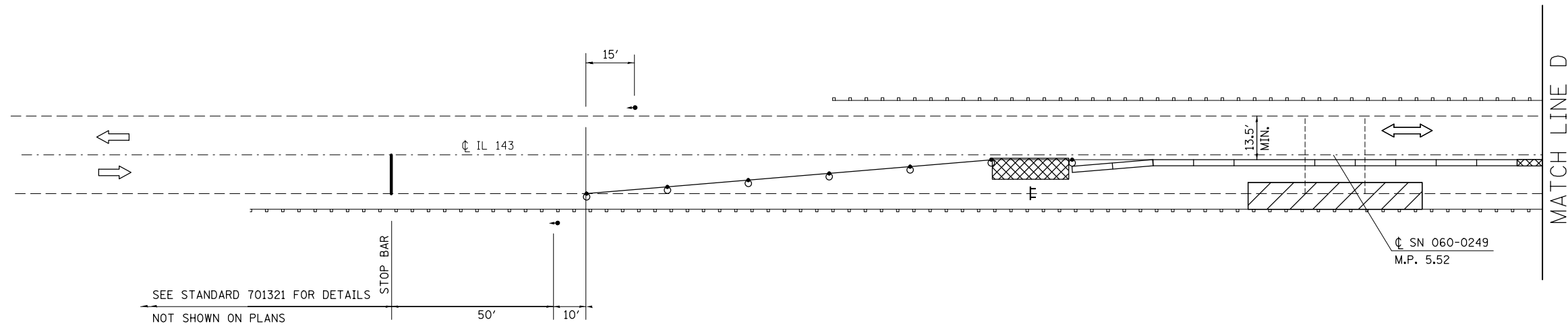
SAND MODULE IMPACT ATTENUATOR LAYOUT  
(IF OPTION USED)

LEGEND

-  WORK AREA
-  IMPACT ATTENUATOR
-  TEMPORARY CONCRETE BARRIER
-  BARRELS WITH STEADY BURNING BI-DIRECTIONAL LIGHT
-  TEMPORARY BRIDGE TRAFFIC SIGNALS
-  TYPE III BARRICADE







NOTE: NOT TO SCALE

FILE NAME = c:\pwork\work\pwork\dot\dintelmanjn\1\d0281732\	USER NAME = dintelmanjn	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STAGE I CONSTRUCTION S.N. 060-0249</b>		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	876f27-shr-plan.dgn	DRAWN -	REVISED -		789	(54,58,68) RS-4	MADISON	44	31		
PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 76F27						
PLOT DATE = 12/14/2012	DATE -	REVISED -			SHEET NO. 1 OF 2 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT			



SAND MODULE IMPACT ATTENUATOR LAYOUT  
(IF OPTION USED)

LEGEND

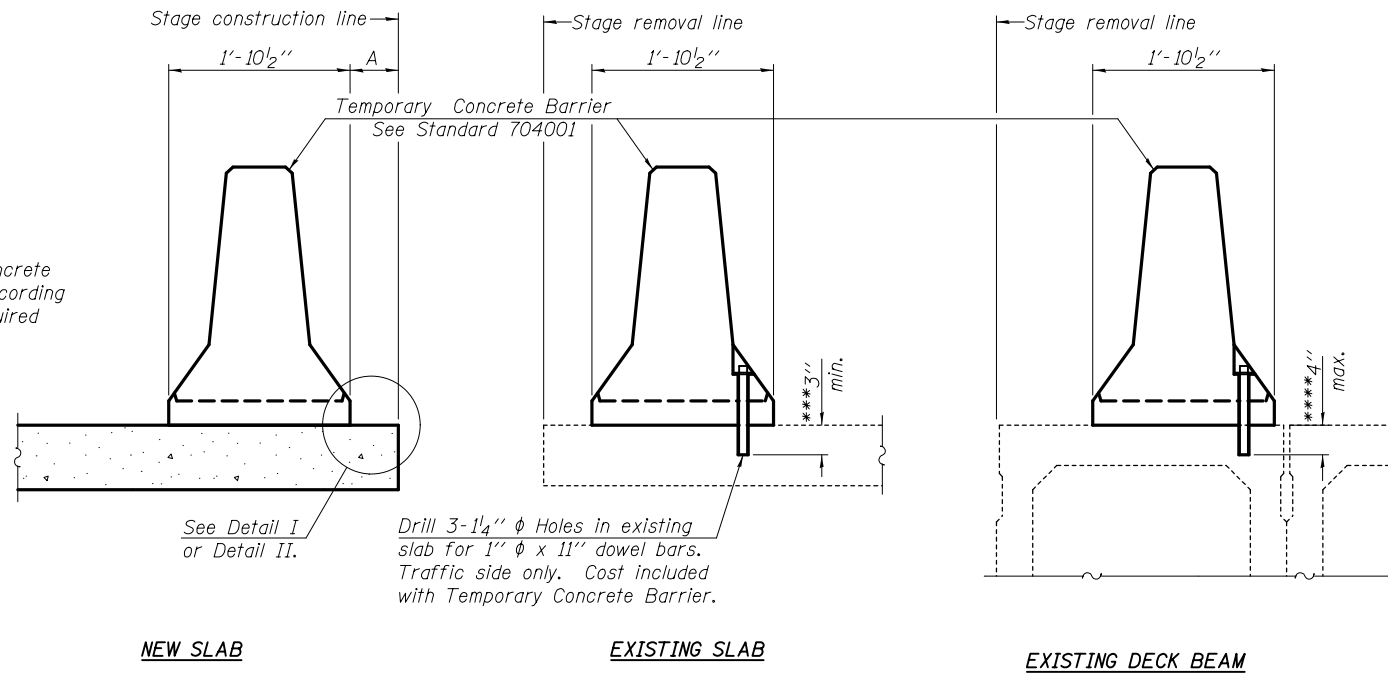
-  WORK AREA
-  IMPACT ATTENUATOR
-  TEMPORARY CONCRETE BARRIER
-  BARRELS WITH STEADY BURNING BI-DIRECTIONAL LIGHT
-  TEMPORARY BRIDGE TRAFFIC SIGNALS
-  TYPE III BARRICADE

NOTE: NOT TO SCALE

FILE NAME = c:\pwwork\pwwork\dntel\manj\1\0281732\	USER NAME = dntel\manj	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STAGE II CONSTRUCTION S.N. 060-0249</b>		F.A.P. RTE. 789	SECTION (54,58,68) RS-4	COUNTY MADISON	TOTAL SHEETS 44	SHEET NO. 32	
	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -		SHEET NO. 2 OF 2 SHEETS	STA.	TO STA.	CONTRACT NO. 76F27				
PLOT DATE = 12/14/2012	DATE -	REVISED -	REVISED -		ILLINOIS FED. AID PROJECT							



When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".



**SECTIONS THRU SLAB OR DECK BEAM**

**NOTES**

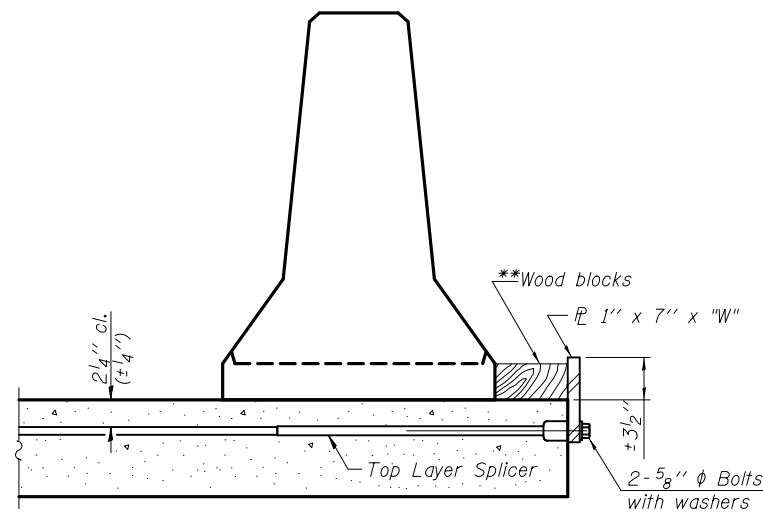
**Detail I - With Bar Splicer or Couplers:**  
Connect one (1) 1" x 7" x "W" steel PL to the top layer of couplers with 2-5/8" φ bolts screwed to coupler at approximate C of each barrier panel.

**Detail II - With Extended Reinforcement Bars:**  
Connect one (1) 1" x 7" x "W" steel PL to the concrete slab or concrete wearing surface with 2-5/8" φ Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate C of each barrier panel.

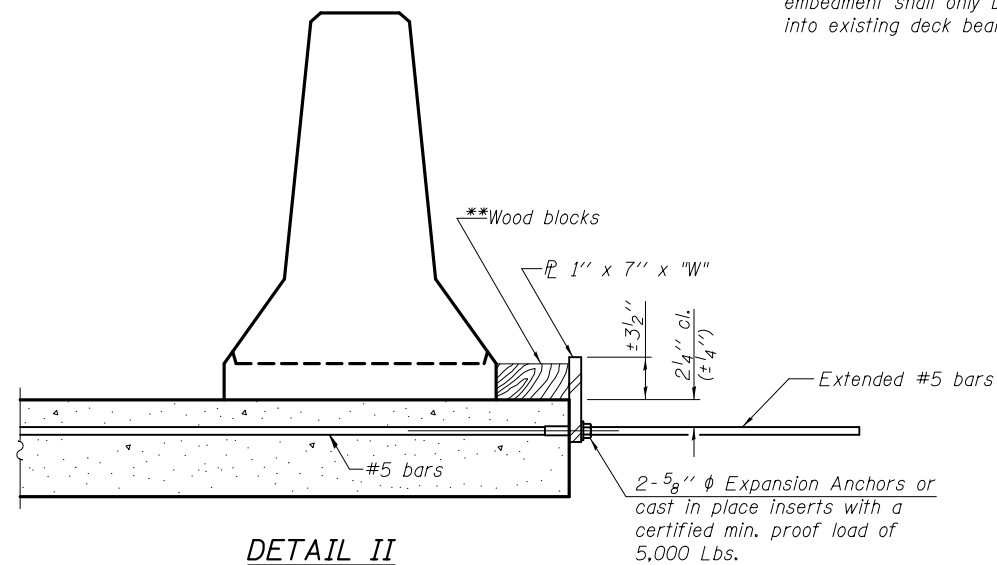
Cost of anchorage is included with Temporary Concrete Barrier. The 1" x 7" x "W" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

\*\*\* Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

\*\*\*\* If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



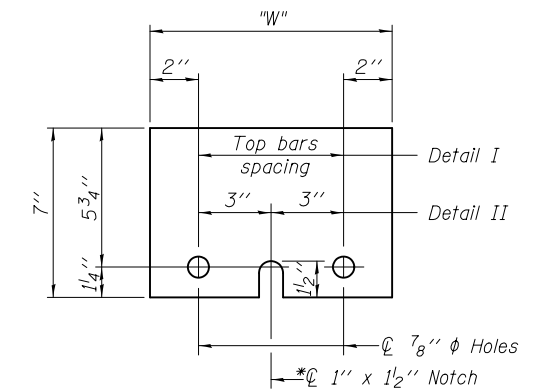
**DETAIL I**



**DETAIL II**

\*\* Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

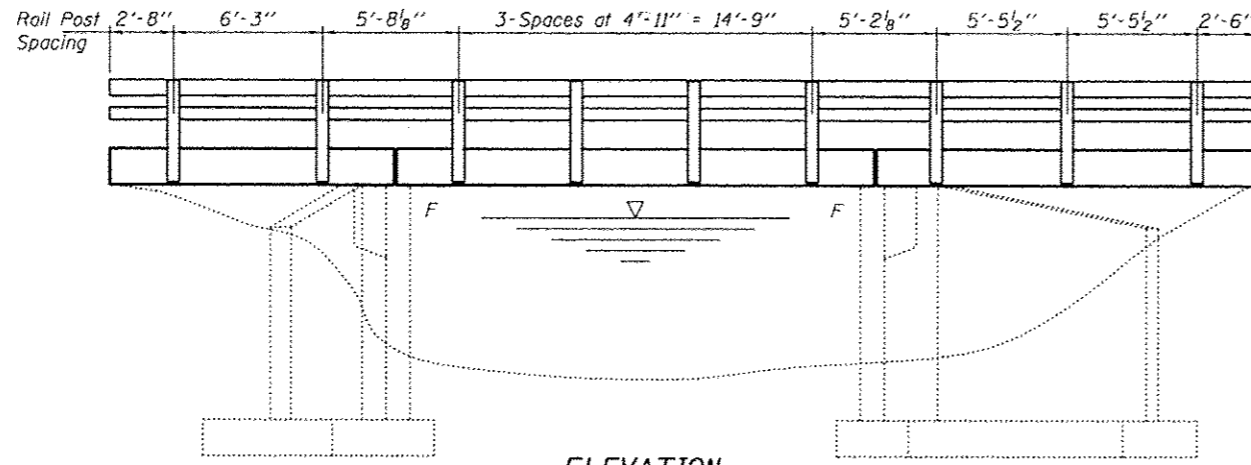
"W" = Top bars spacing + 4"



**STEEL RETAINER PL 1" x 7" x "W"**

\* Required only with Detail II

FILE NAME =	USER NAME = dntelmajn	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION STRUCTURE NO. 060-0246 &amp; 060-0249</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
et:\pwork\pwork\pwork\dntelmajn\1d0281732\	876f27-shr-plan.dgn	DRAWN -	REVISED -				(54,58,68) RS-4	MADISON	44	33	
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 76F27					
	PLOT DATE = 12/14/2012	DATE -	REVISED -			SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.



**ELEVATION**  
Looking North at South Railing.  
North Railing similar by 180° rotation.

**GENERAL NOTES**

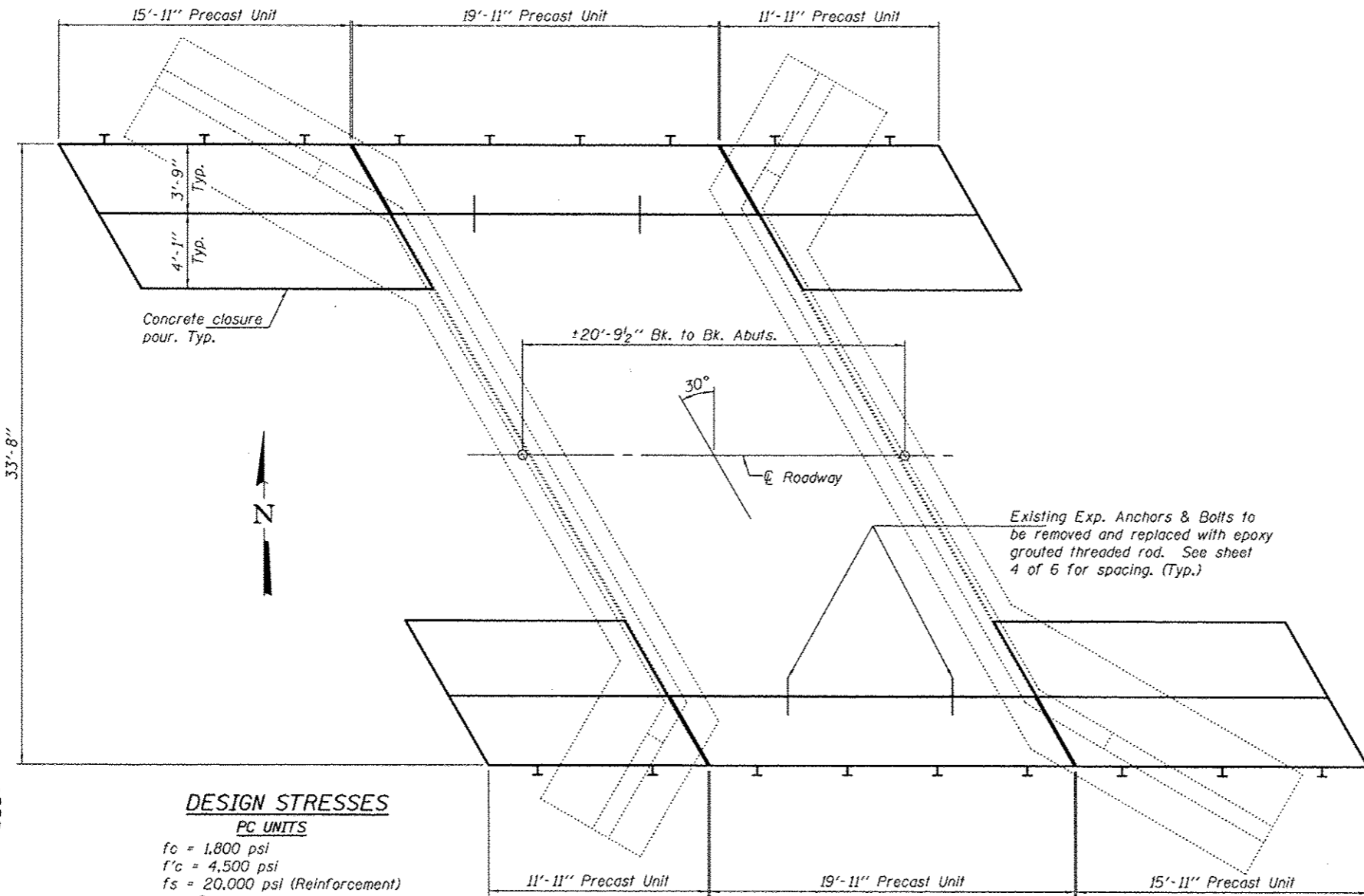
Plan dimensions and details relative to existing structure have been taken from existing plans and are subject to nominal construction variations. It shall be the Contractor's responsibility to verify such dimensions and details in the field and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in the scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Any damage done to the bridge during Precast Unit removal shall be repaired by the Contractor. Cost to be included in the cost of Removal of Existing Precast Concrete Units.

Temporary concrete barrier shall only be anchored into the overlay and not the Precast Concrete Units.

Reinforcement bars designated (E) shall be epoxy coated.

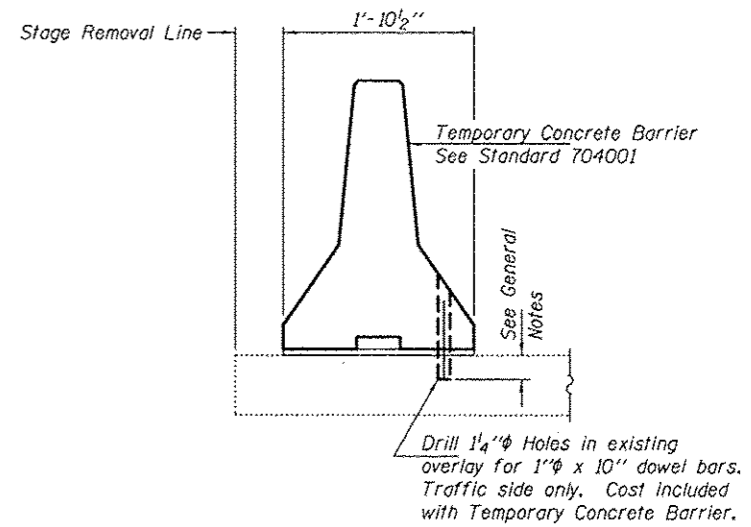
The contractor shall salvage the existing bridge rail and posts and shall deliver them to:  
IDOT Maintenance Facility  
12540 Sportsman Road  
Highland, Illinois 62249  
Contact: Todd Reilson at 618-659-5110  
Cost included with Bridge Rail Removal.



**PLAN**

**DESIGN STRESSES**  
**PC UNITS**  
 $f_c = 1,800$  psi  
 $f'_c = 4,500$  psi  
 $f_s = 20,000$  psi (Reinforcement)  
 $n = 8$

**FIELD UNITS**  
 $f'_c = 3,500$  psi  
 $f_y = 60,000$  psi (Reinforcement)

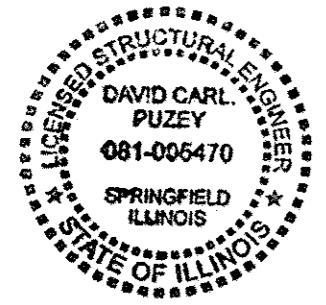


**SECTION THRU SLAB**

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	QUANTITY
Precast Concrete Bridge Slab	Sq. Ft.	355
Removal of Existing Precast Concrete Units	Sq. Ft.	355
Reinforcement Bars, Epoxy Coated	Pound	1710
Steel Railing, Type SM	Foot	96
Waterproofing Membrane System	Sq. Yd.	39.4
Concrete Removal	Cu. Yd.	10.5
Concrete Superstructure	Cu. Yd.	10.5
Bridge Rail Removal	Foot	96

\* On Precast Concrete Bridge Slab only.  
For quantity of HMA Surface Course Mix see roadway plans.



Expires: November 30, 2014

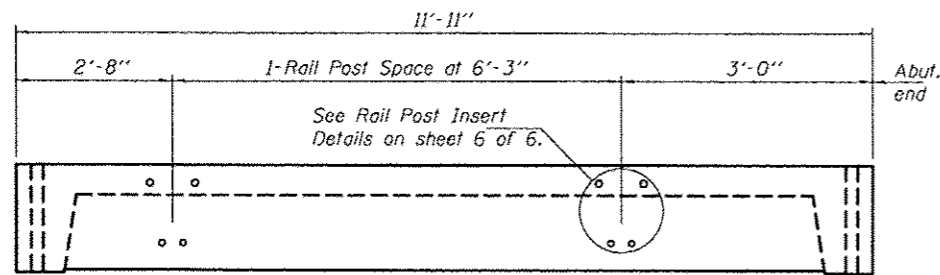
DESIGNED: <i>David Carl Puzey</i>	EXAMINED: <i>Timothy A. Adams</i>	DATE: JANUARY 31, 2013
CHECKED: <i>Kyle M. Steffen</i>	PASSED: <i>David Carl Puzey</i>	
DRAWN: <i>DAB</i>		
CHECKED: <i>DAB</i>		

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

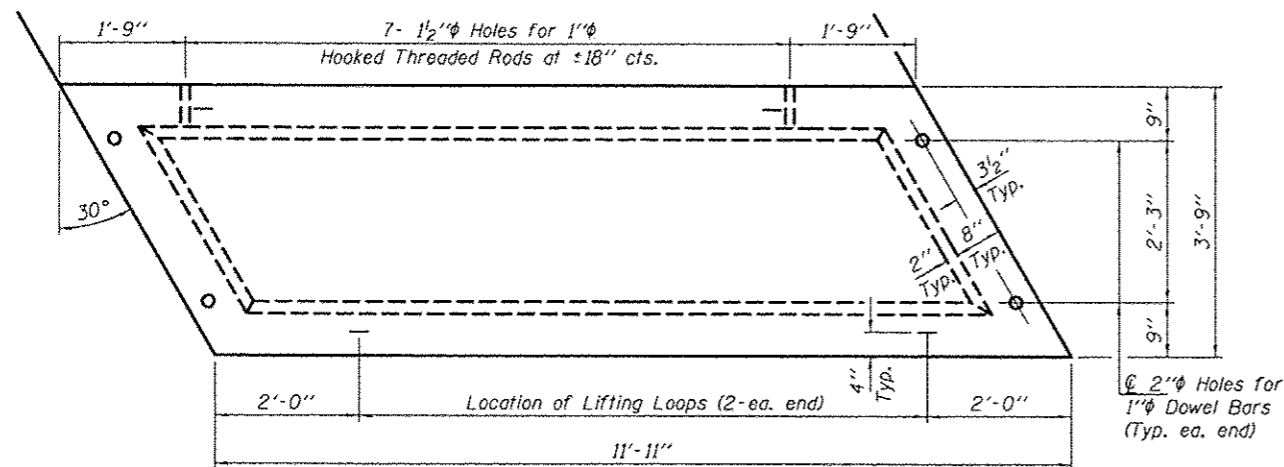
GENERAL PLAN & ELEVATION  
ILLINOIS ROUTE 143 OVER SILVER CREEK  
SN 060-0246

SHEET NO. 1 OF 6 SHEETS

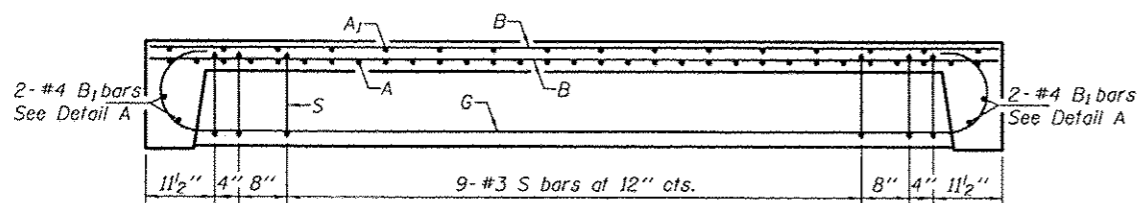
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
775	(54, 58, 60)RS-4	MADISON	44	34
CONTRACT NO. 76F27			ILLINOIS FED. AID PROJECT	



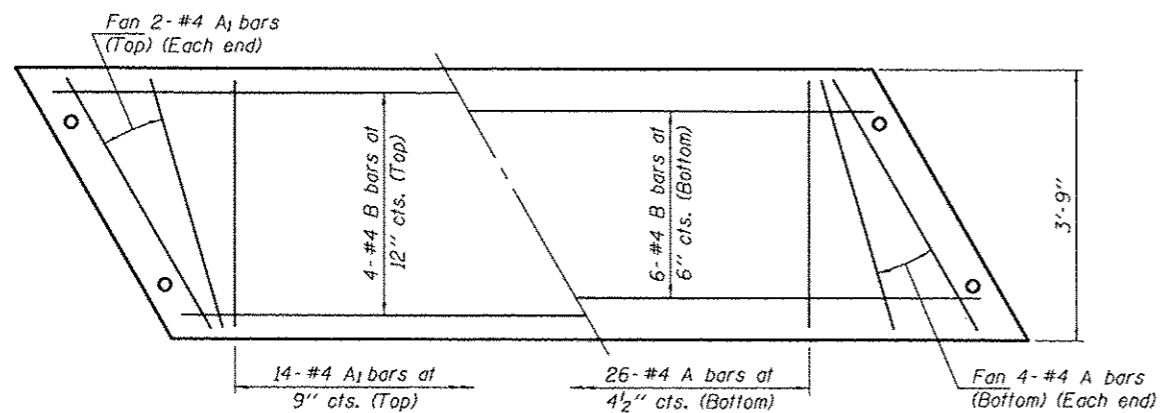
**ELEVATION**



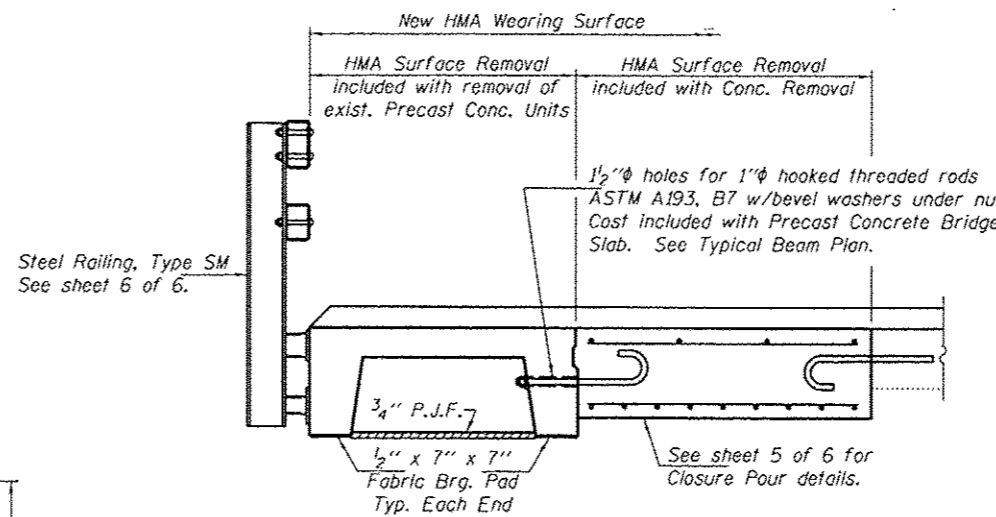
**TYPICAL BEAM PLAN**



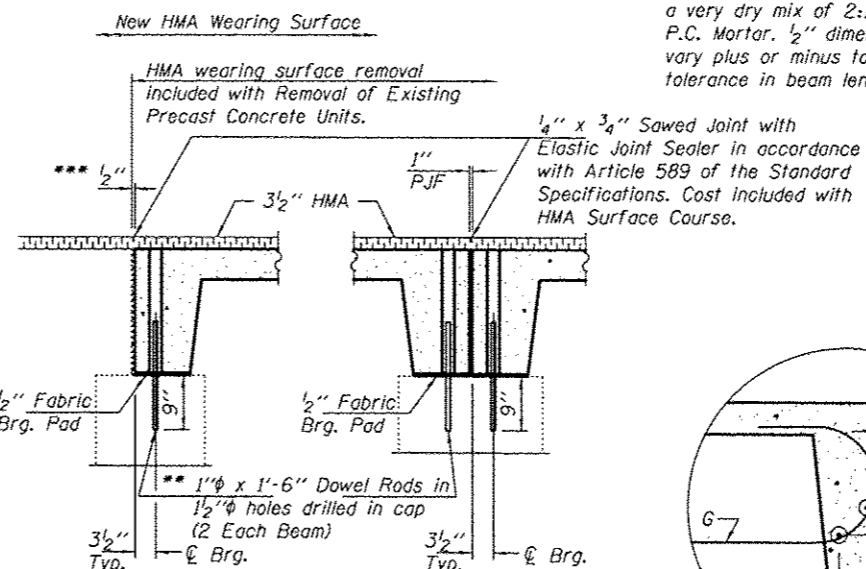
**LONGITUDINAL SECTION**



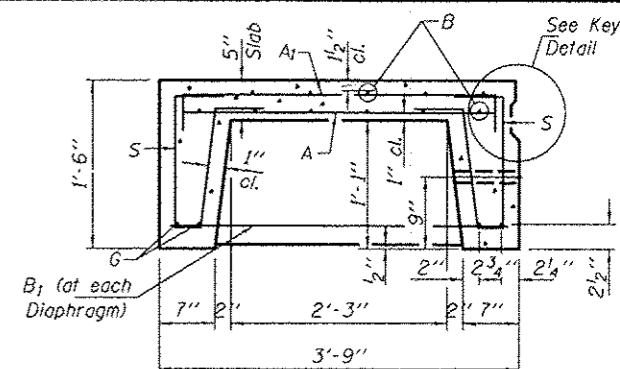
**SLAB REINFORCEMENT**



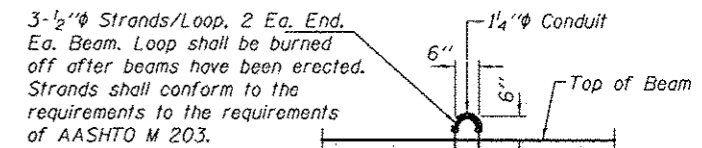
**PARTIAL CROSS-SECTION**



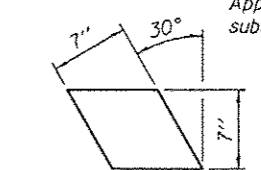
**SECTION THRU APPR. CAP (At Rt. Ls) SECTION THRU ABUT. (At Rt. Ls)**



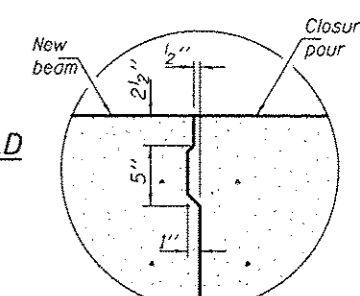
**TYPICAL SECTION THRU BEAM**



**LIFTING LOOP**



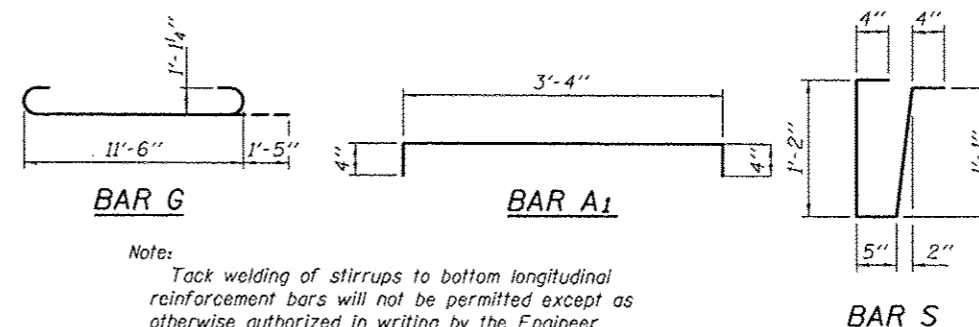
**FABRIC BRG. PAD**



**KEY DETAIL**

\*\* Existing dowel rods are to be burned off, ground flush, and sealed with epoxy prior to placement of new beams. Cast included in Removal of Existing Precast Concrete Units. After beams have been erected holes shall be drilled into cap and dowel rods placed. Dowel holes shall be filled with non-shrink grout to top of beam and allowed to cure a minimum of 24 hours prior to grouting the shear keys.

Note: The surface of the member shall not deviate more than 1/1200 of the full length of the member from a straight line connecting the two end points on the member's surface. (See Special Provision Precast Concrete Structures)



Note: Tack welding of stirrups to bottom longitudinal reinforcement bars will not be permitted except as otherwise authorized in writing by the Engineer.

**\* BILL OF REINFORCEMENT BARS FOR ONE BEAM**

Bar	No.	Size	Length	Shape
A	34	#4	3'-3"	—
A1	18	#4	4'-0"	—
B	10	#4	11'-5"	—
B1	4	#4	4'-1"	—
G	4	#10	14'-4"	—
S	26	#3	3'-4"	—

**\* ESTIMATED QUANTITIES FOR ONE BEAM**

	CONCRETE CU. YDS.	RE-BARS POUNDS
One Unit	1.8	490

\* For information of suppliers of Precast Slab Units only.

DESIGNED - VHV  
 CHECKED - DAB  
 DRAWN - Kyle M. Steffan  
 CHECKED - VHV DAB

EXAMINED  
 PASSED  
 ACTING ENGINEER OF STRUCTURAL SERVICES  
 ACTING ENGINEER OF BRIDGES AND STRUCTURES

DATE - JANUARY 31, 2013

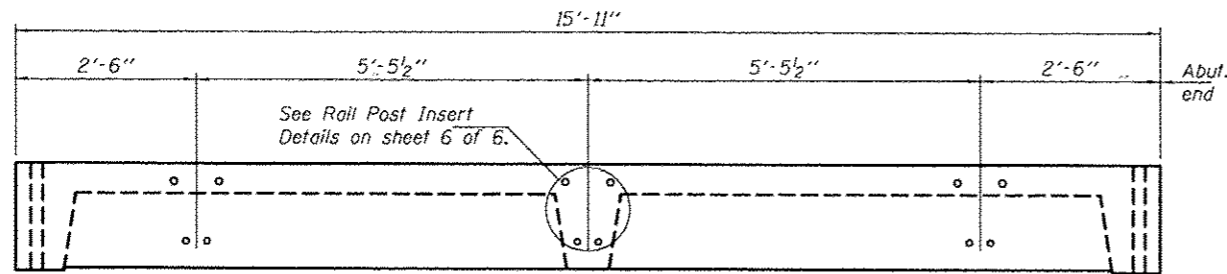
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

APPROACH BEAM DETAILS  
 SN 060-0246

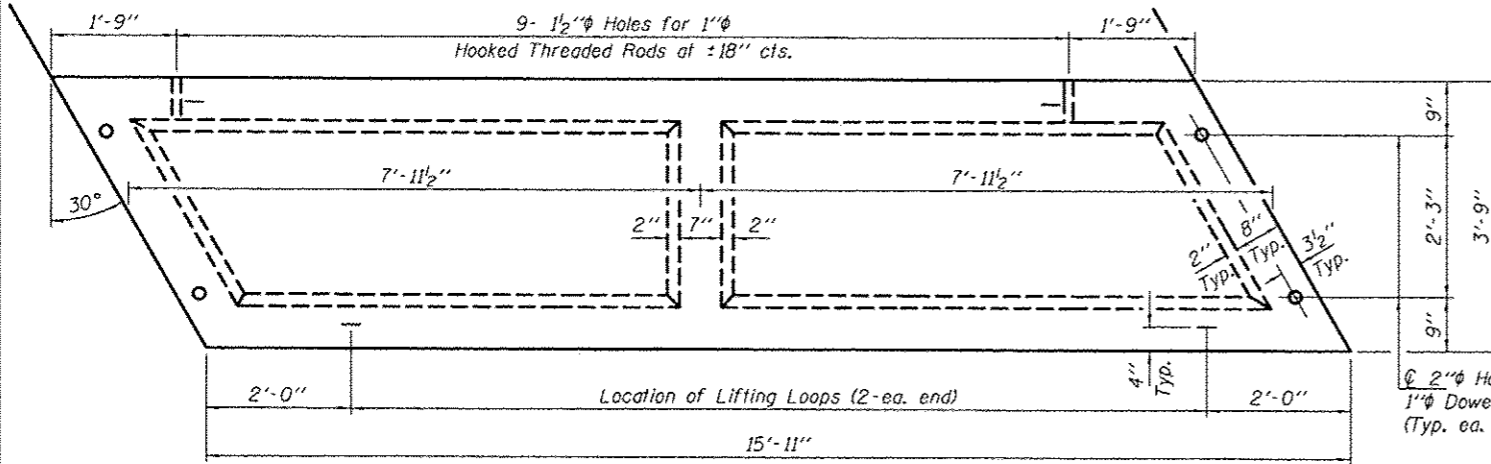
SHEET NO. 2 OF 6 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
775	(54, 58, 68)RS-4	MADISON	44	35

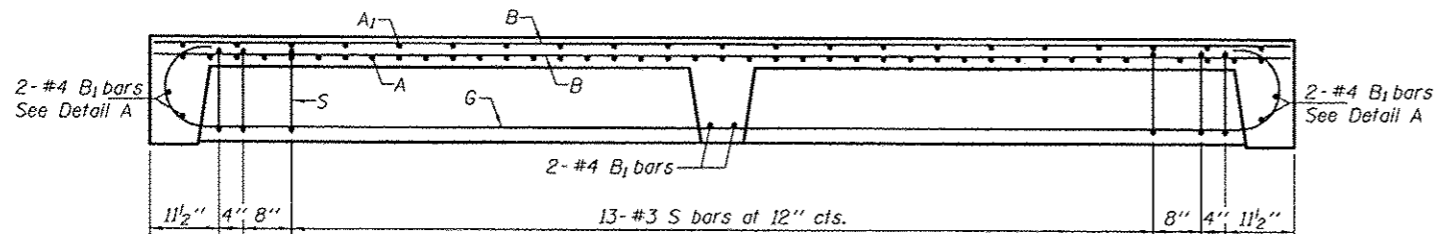
CONTRACT NO. T6F27  
 ILLINOIS FED. AID PROJECT



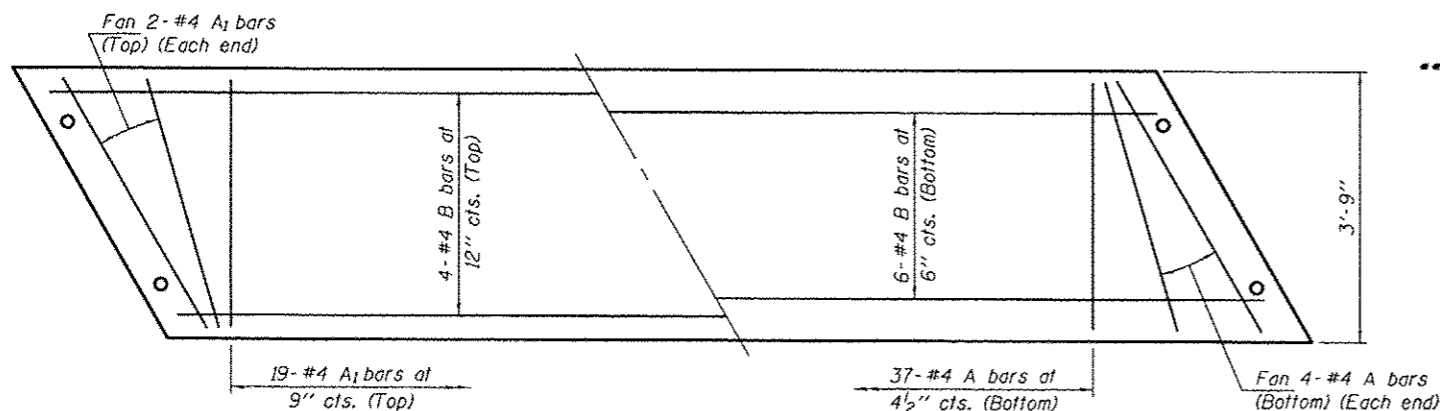
**ELEVATION**  
Showing rail post spacing



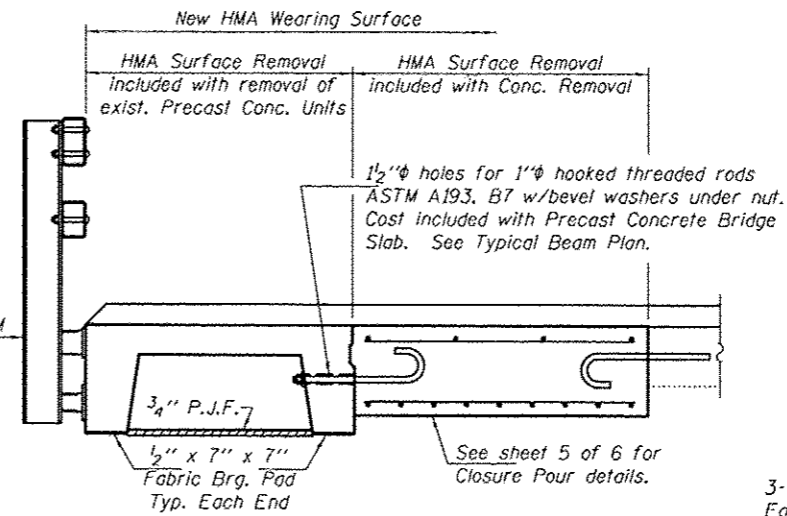
**TYPICAL BEAM PLAN**



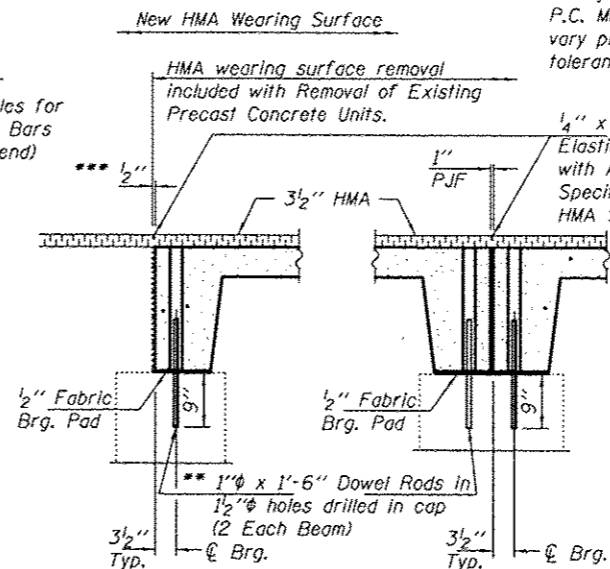
**LONGITUDINAL SECTION**



**SLAB REINFORCEMENT**

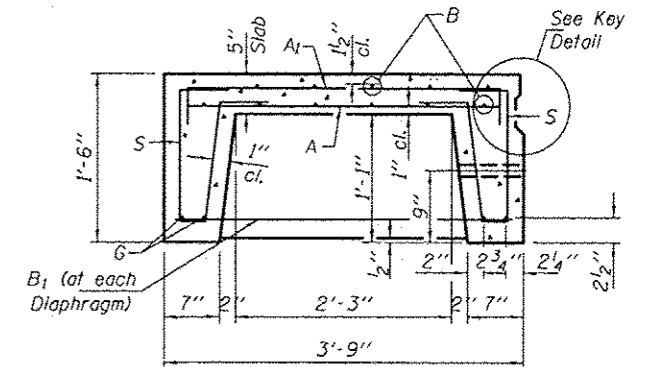


**PARTIAL CROSS-SECTION**

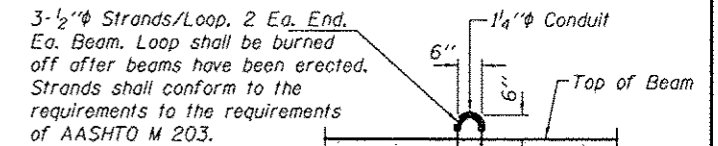


**SECTION THRU APPR. CAP**  
(At Rt. Ls)

**SECTION THRU ABUT.**  
(At Rt. Ls)



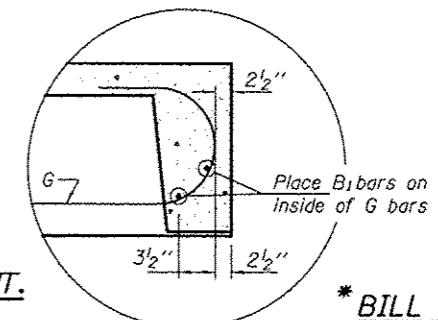
**TYPICAL SECTION THRU BEAM**



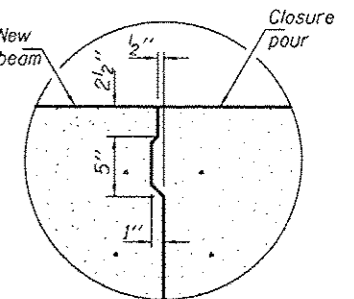
**LIFTING LOOP**

Approved alternate may be substituted for the above.

**FABRIC BRG. PAD**  
1/2" Fabric Pads  
(4 Req'd. per beam)



**DETAIL A**



**KEY DETAIL**

**\* BILL OF REINFORCEMENT BARS FOR ONE BEAM**

Bar	No.	Size	Length	Shape
A	45	#4	3'-3"	—
A1	23	#4	4'-0"	—
B	10	#4	15'-5"	—
B1	6	#4	4'-1"	—
G	4	#10	18'-4"	—
S	34	#3	3'-4"	U

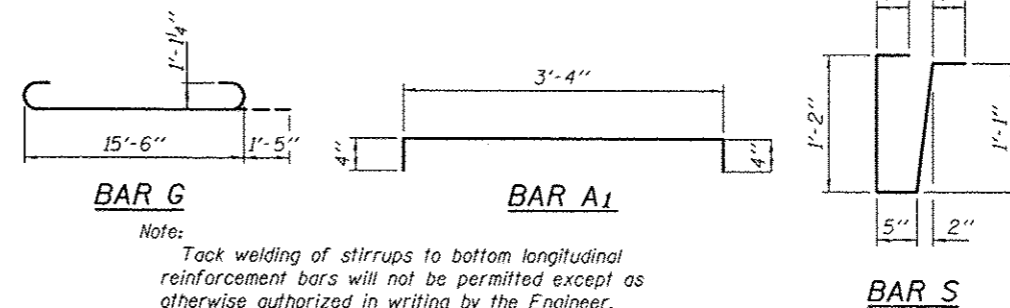
**\* ESTIMATED QUANTITIES FOR ONE BEAM**

	CONCRETE CU. YDS.	RE-BARS POUNDS
One Unit	2.1	640

\* For information of suppliers of Precast Slab Units only.

\*\* Existing dowel rods are to be burned off, ground flush, and sealed with epoxy prior to placement of new beams. Cast included in Removal of Existing Precast Concrete Units. After beams have been erected holes shall be drilled into cap and dowel rods placed. Dowel holes shall be filled with non-shrink grout to top of beam and allowed to cure a minimum of 24 hours prior to grouting the shear keys.

Note: The surface of the member shall not deviate more than 1/1200 of the full length of the member from a straight line connecting the two end points on the member's surface. (See Special Provision Precast Concrete Structures)



**BAR G**

**BAR A1**

**BAR S**

Note: Tack welding of stirrups to bottom longitudinal reinforcement bars will not be permitted except as otherwise authorized in writing by the Engineer.

DESIGNED - VHV  
CHECKED - DAB  
DRAWN - Kyle M. Steffen  
CHECKED - VHV DAB

EXAMINED  
PASSED  
ACTING ENGINEER OF STRUCTURAL SERVICES  
ACTING ENGINEER OF BRIDGES AND STRUCTURES

DATE - JANUARY 31, 2013

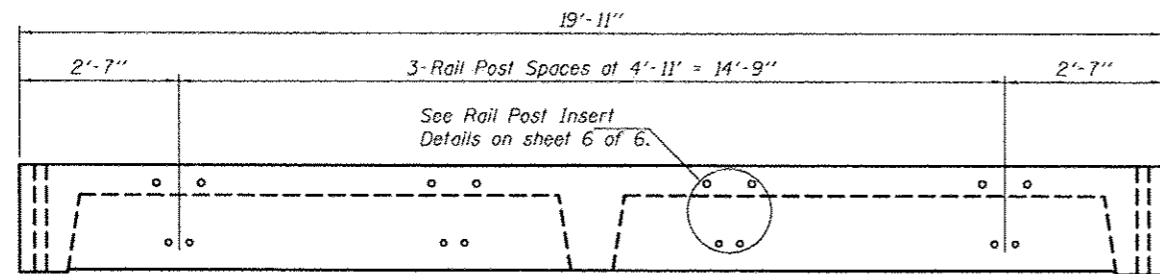
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

APPROACH BEAM DETAILS  
SN 060-0246

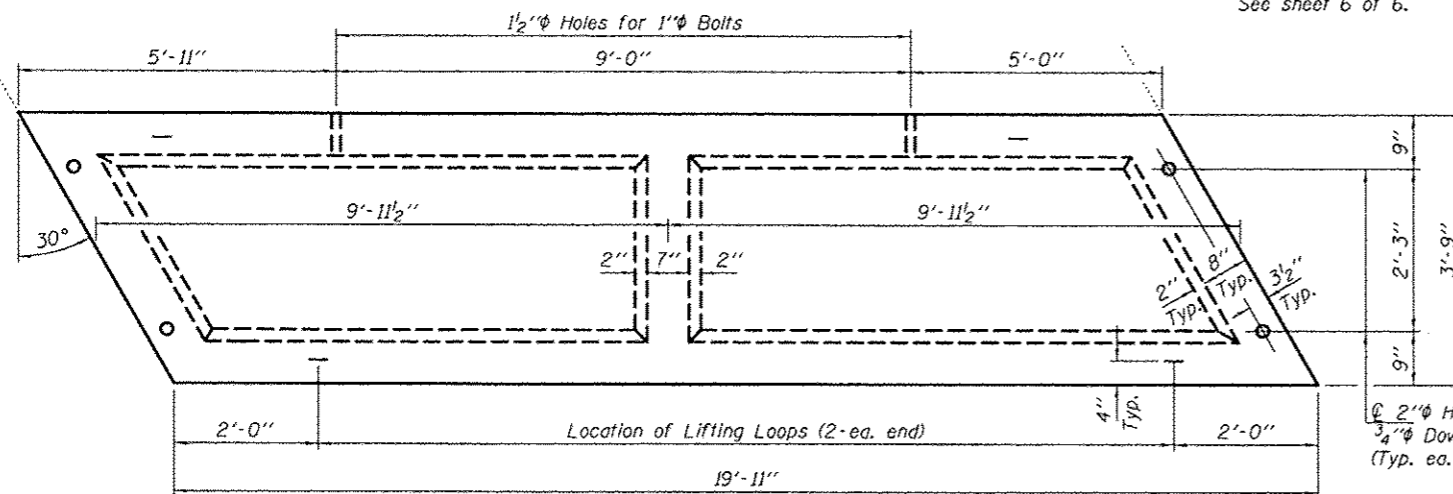
SHEET NO. 3 OF 6 SHEETS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
775	154, 58, 681R5-4	MADISON	44	36

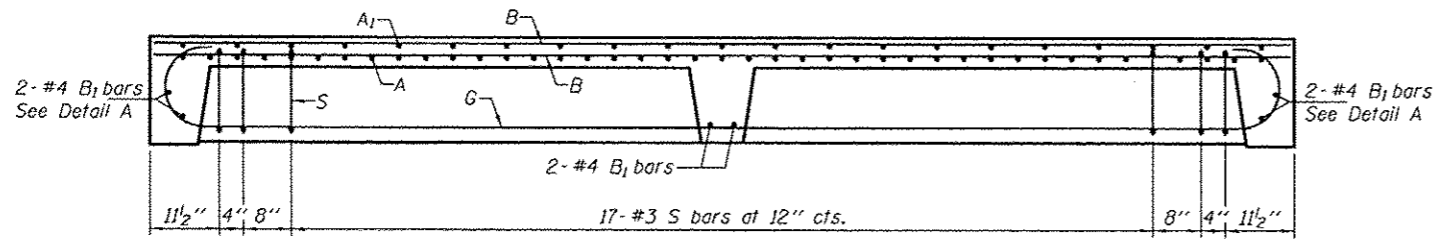
CONTRACT NO. 76F27  
ILLINOIS FED. AID PROJECT



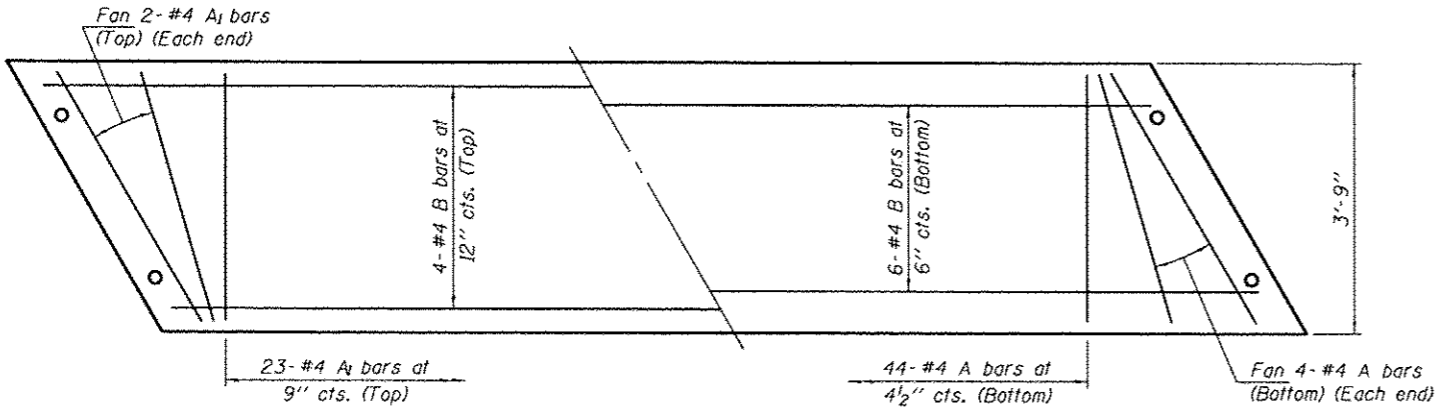
**ELEVATION**  
Showing rail post spacing



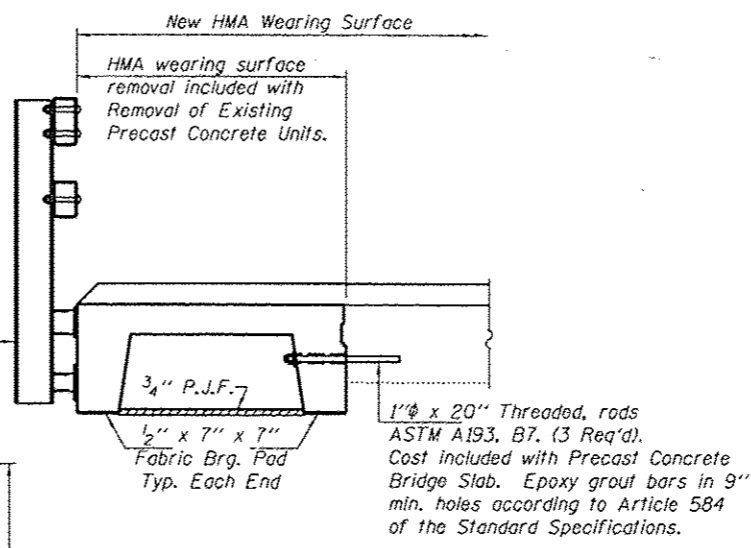
**TYPICAL BEAM PLAN**



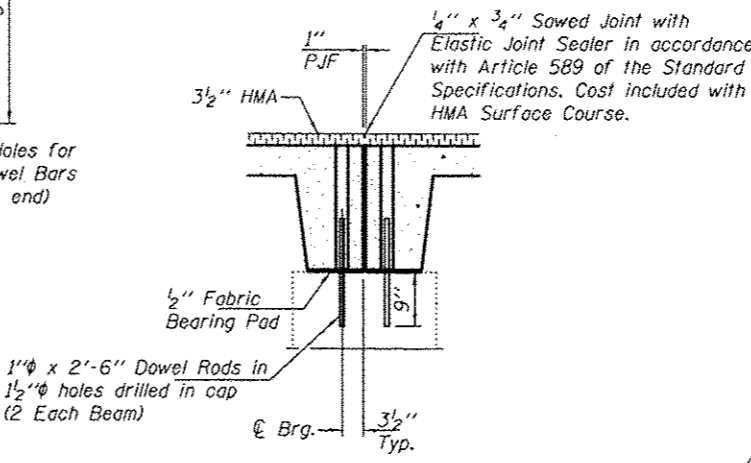
**LONGITUDINAL SECTION**



**SLAB REINFORCEMENT**

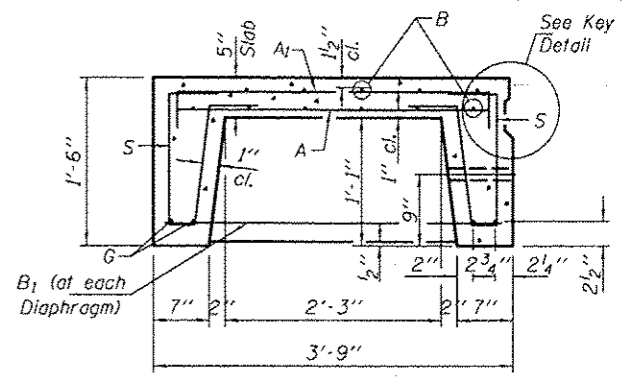


**PARTIAL CROSS-SECTION**



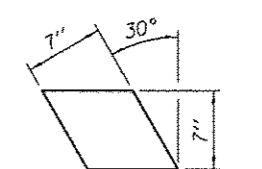
**SECTION THRU ABUTMENT**  
(At Rt. Ls.)

\*\* Existing dowel rods are to be burned off, ground flush, and sealed with epoxy prior to placement of new beams. Cost included in Removal of Existing Precast Concrete Units. After beams have been erected holes shall be drilled into cap and dowel rods placed. Dowel holes shall be filled with non-shrink grout to top of beam and allowed to cure a minimum of 24 hours prior to grouting the shear keys.

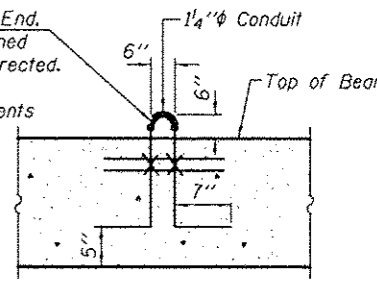


**TYPICAL SECTION THRU BEAM**

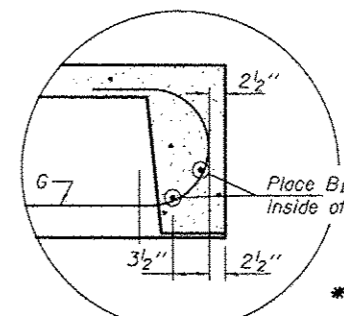
3-1/2" Strands/Loop. 2 Ea. End. Ea. Beam. Loop shall be burned off after beams have been erected. Strands shall conform to the requirements of AASHTO M 203.



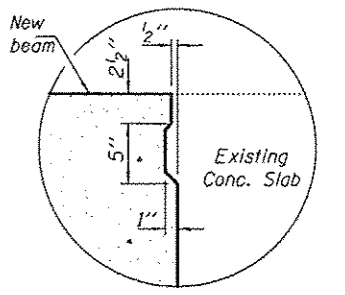
**FABRIC BRG. PAD**  
1/2" Fabric Pads  
(4 Req'd. per beam)



**LIFTING LOOP**  
Approved alternate may be substituted for the above.



**DETAIL A**



**KEY DETAIL**

**\* BILL OF REINFORCEMENT BARS FOR ONE BEAM**

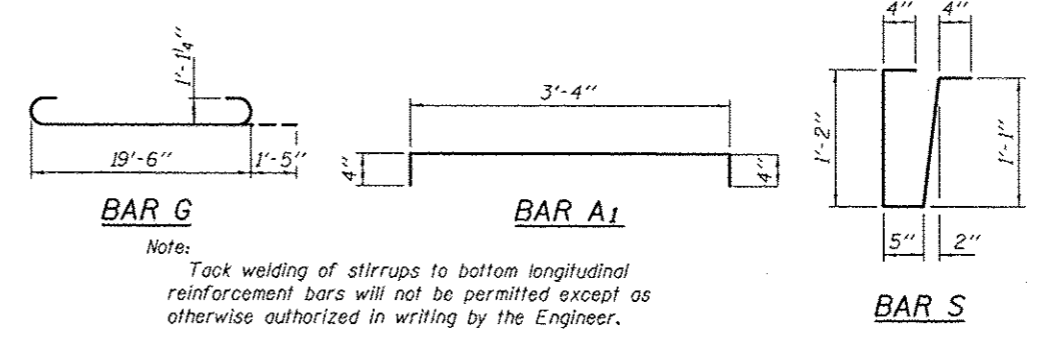
Bar	No.	Size	Length	Shape
A	52	#4	3'-3"	—
A1	27	#4	4'-0"	—
B	10	#4	19'-5"	—
B1	6	#4	4'-1"	—
G	4	#10	22'-4"	—
S	42	#3	3'-4"	U

Note: The surface of the member shall not deviate more than 1/1200 of the full length of the member from a straight line connecting the two end points on the member's surface. (See Special Provision Precast Concrete Structures)

**\* ESTIMATED QUANTITIES FOR ONE BEAM**

	CONCRETE CU. YDS.	RE-BARS POUNDS
One Unit	2.5	710

\* For information of suppliers of Precast Slab Units only.



Note: Tack welding of stirrups to bottom longitudinal reinforcement bars will not be permitted except as otherwise authorized in writing by the Engineer.

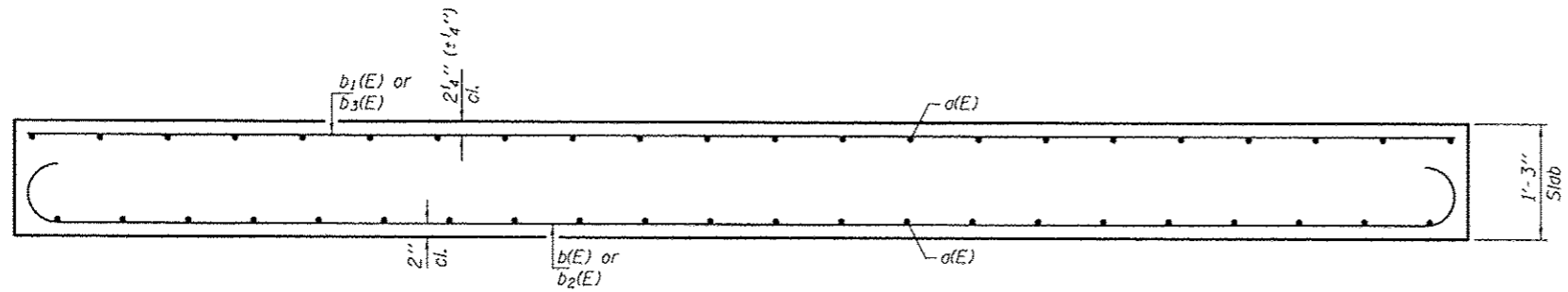
DESIGNED - VHV  
CHECKED - DAB  
DRAWN - Kyle M. Steffen  
CHECKED - VHV DAB

EXAMINED - *Timothy A. [Signature]*  
ACTING ENGINEER OF STRUCTURAL SERVICES  
PASSED - *[Signature]*  
ACTING ENGINEER OF BRIDGES AND STRUCTURES  
DATE - JANUARY 31, 2013

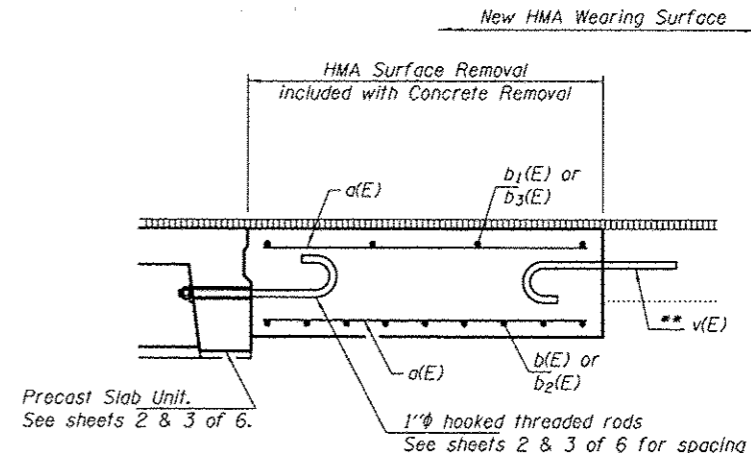
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE DETAILS  
SN 060-0246  
SHEET NO. 4 OF 6 SHEETS

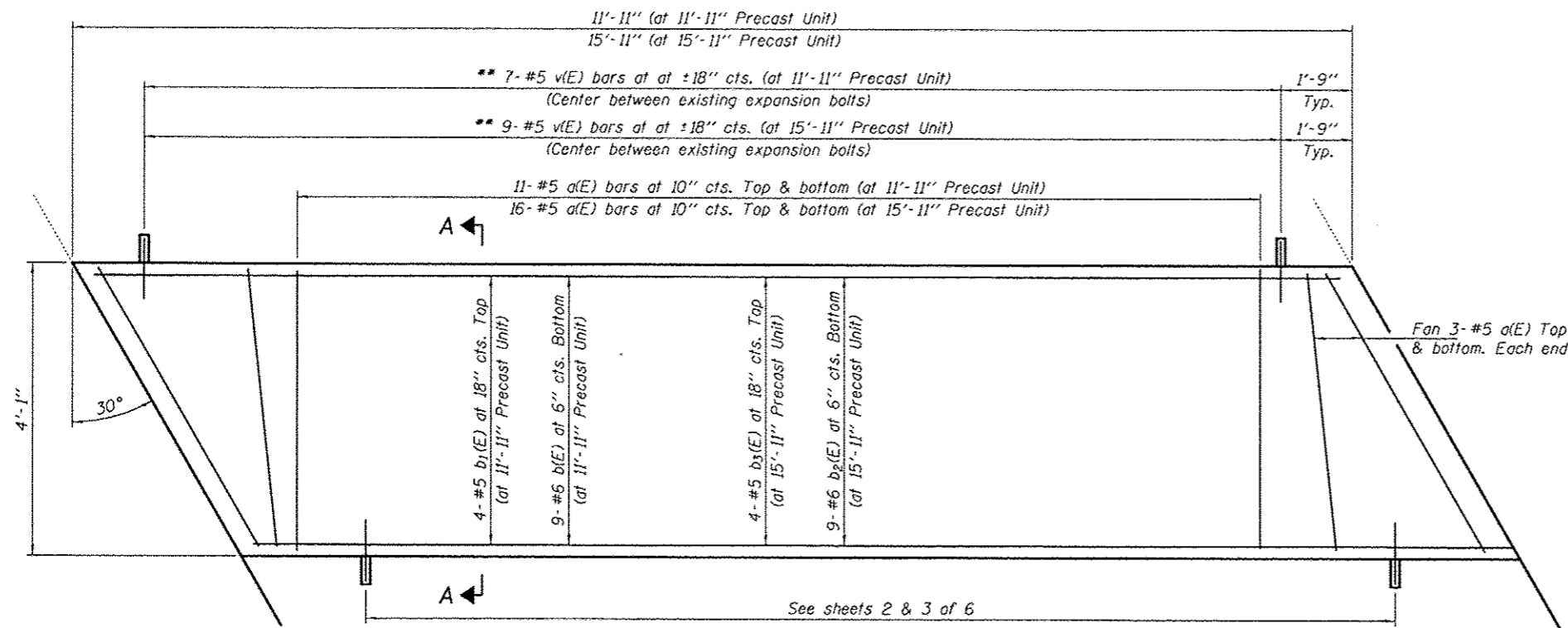
F.A.S. RTE. 775  
SECTION (54, 58, 68RS-4)  
COUNTY MADISON  
TOTAL SHEETS 44  
SHEET NO. 37  
CONTRACT NO. 76F27  
ILLINOIS FED. AID PROJECT



SECTION THRU SLAB

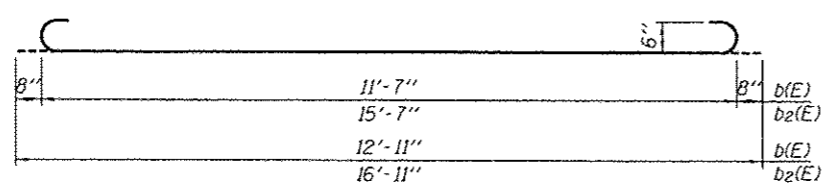


SECTION A-A



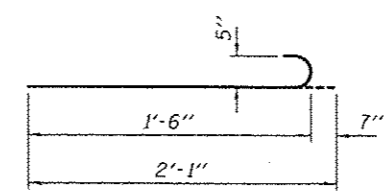
PLAN

Note:  
Slab to be poured after  
Precast Slab Unit is in place.

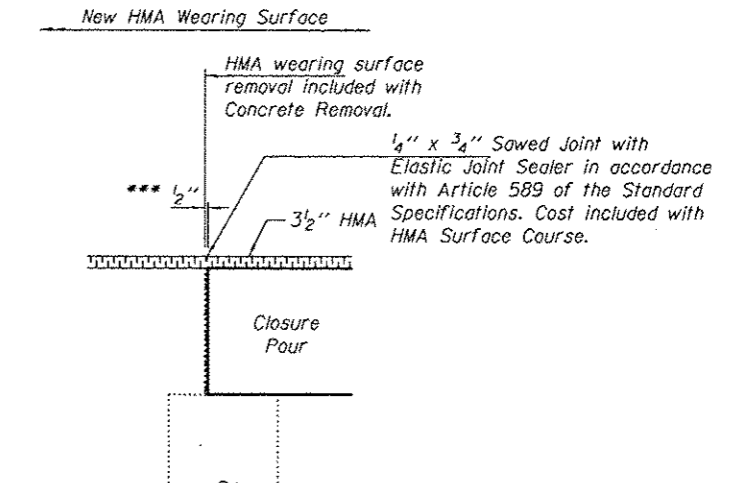


BARS b(E) & b2(E)

\*\* Epoxy grout v(E) bars in 9" min.  
holes according to Article 584 of  
the Standard Specifications.



BAR v(E)

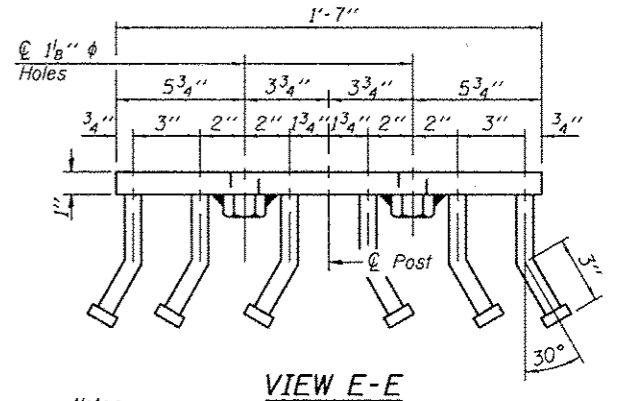
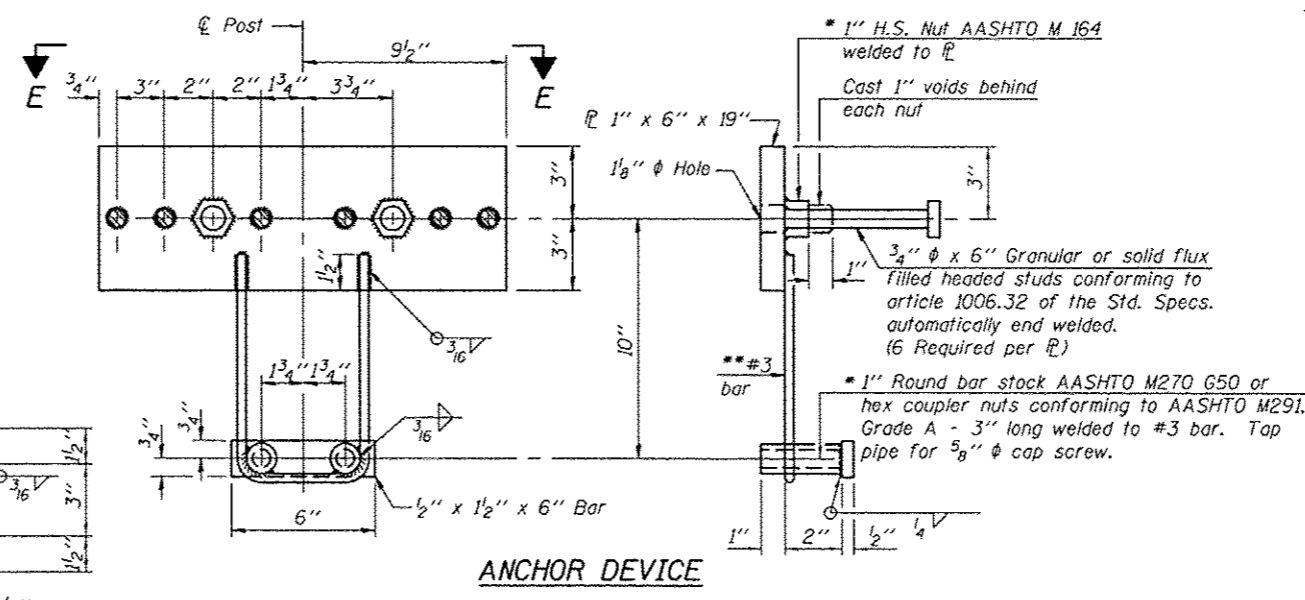
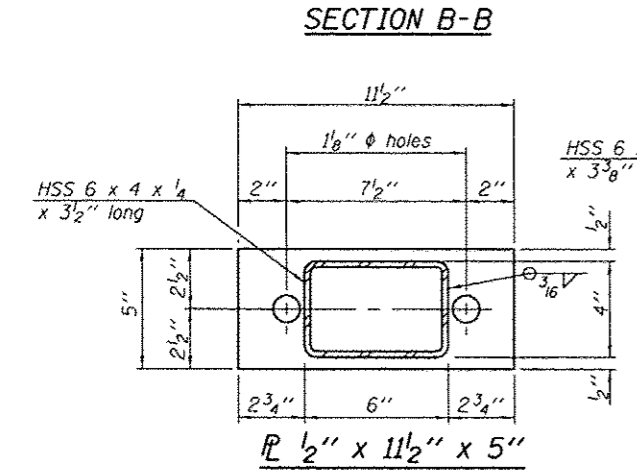
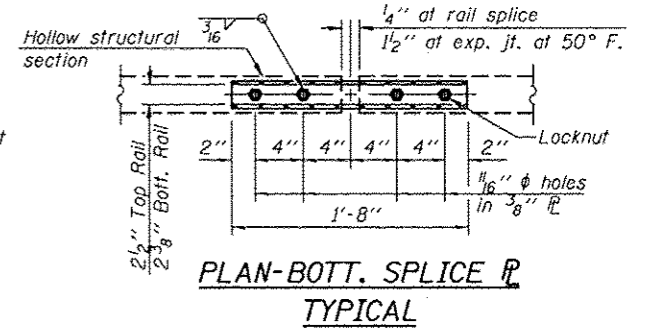
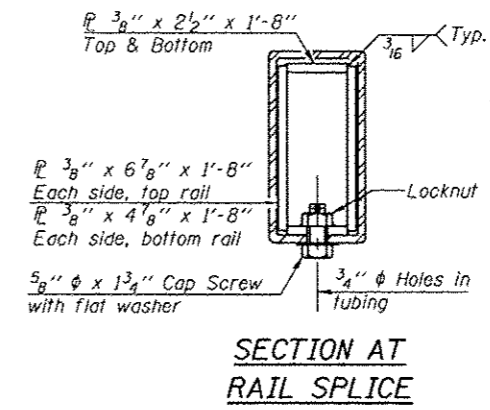
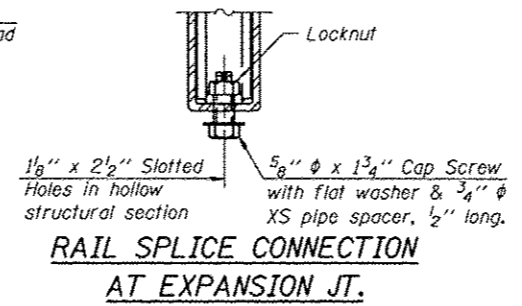
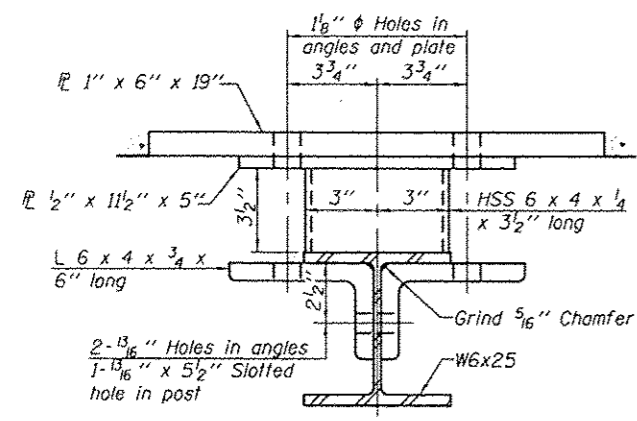
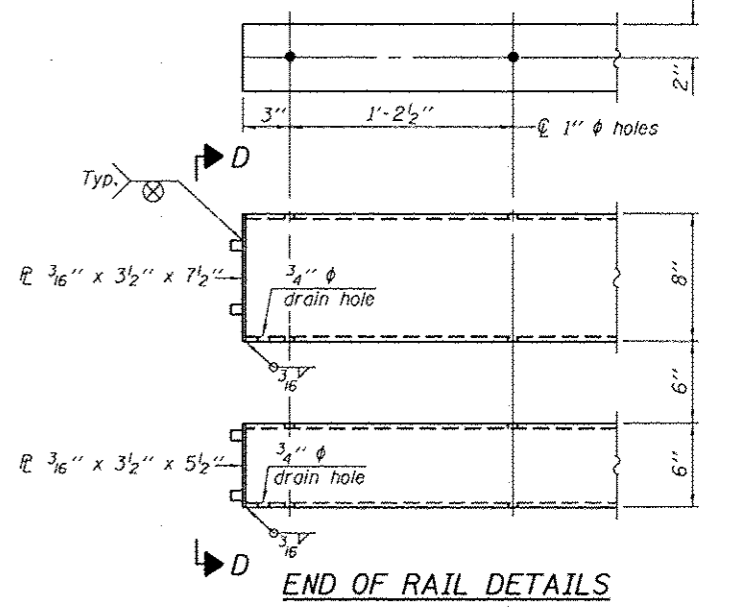
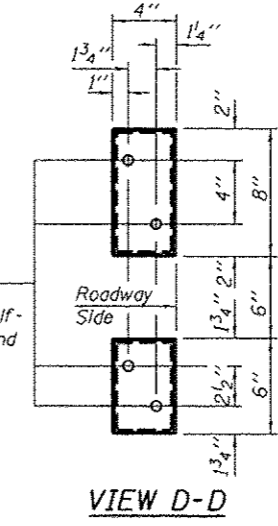
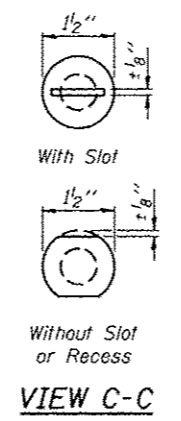
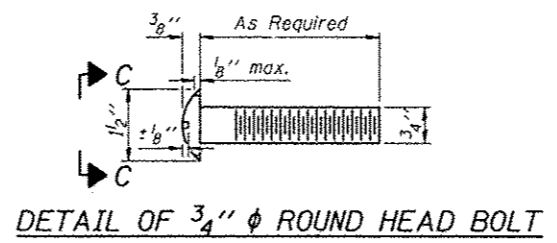
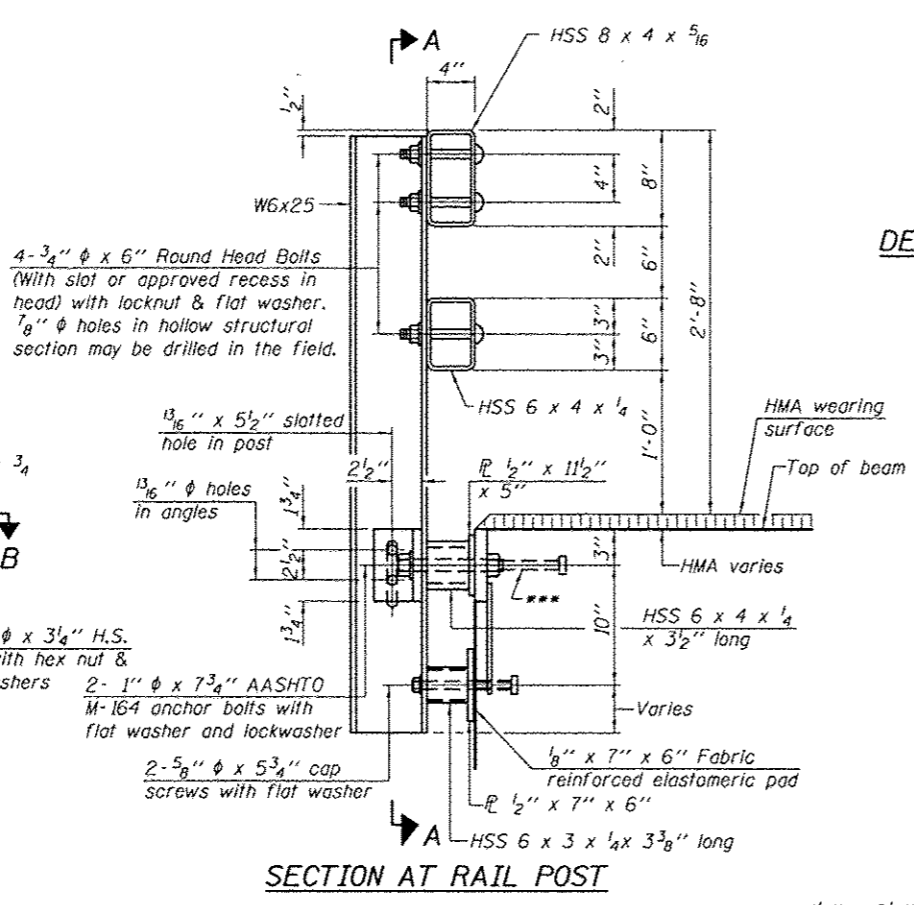
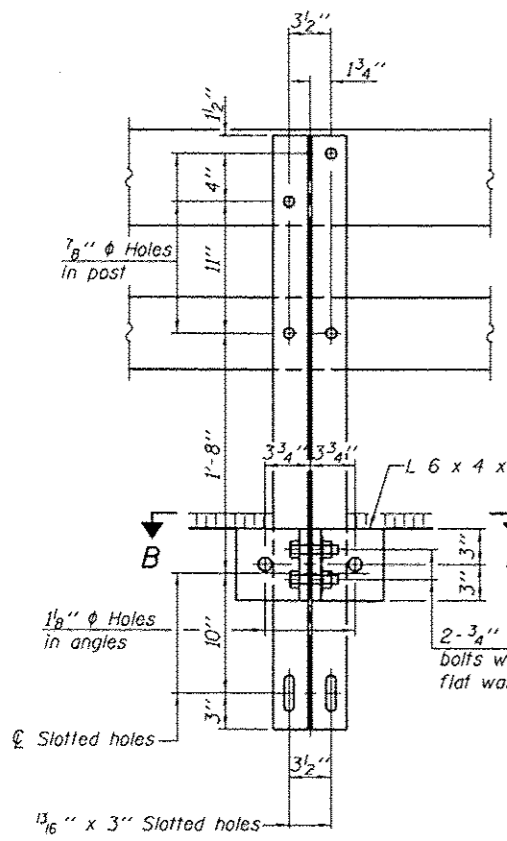


SECTION THRU APPROACH CAP  
(At Rt. Ls)

\*\*\* 1/2" joint shall be packed with  
a very dry mix of 2:1 sand and  
P.C. Mortar. 1/2" dimension may  
vary plus or minus to accommodate  
tolerance in beam lengths.

FOUR CLOSURE POURS  
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	156	#5	3'-9"	—
b(E)	18	#6	12'-11"	—
b1(E)	8	#5	11'-7"	—
b2(E)	18	#6	16'-11"	—
b3(E)	8	#5	15'-7"	—
v(E)	32	#5	2'-1"	—
Concrete Removal			Cu. Yd.	10.5
Concrete Superstructure			Cu. Yd.	10.5
Reinforcement Bars, Epoxy Coated			Pound	1710



Notes:  
 All field drilled holes shall be coated with an approved zinc rich paint before erection.  
 For multi-span bridges, sufficient 1/4" x 6" x 1'-2" galvanized steel shims shall be provided to align rail between adjacent spans. Cost included with Steel Railing, Type SM.  
 All steel rail members shall be galvanized according to Article 509.05 of the Standard Specifications.  
 \*\*\* The studs of the anchor devices shall be placed below the top reinforcement bars and the outermost longitudinal reinforcement bar shall be placed directly above the studs of the rail post anchor device.

\*\* Whenever the lower insert assemblies interfere with strand locations, the #3 bars shall be cut and adjusted in order to allow raising or lowering of the lower inserts. Maximum adjustment not to exceed 1/2".

BILL OF MATERIAL

Item	Unit	Quantity
Steel Railing, Type SM	Foot	96

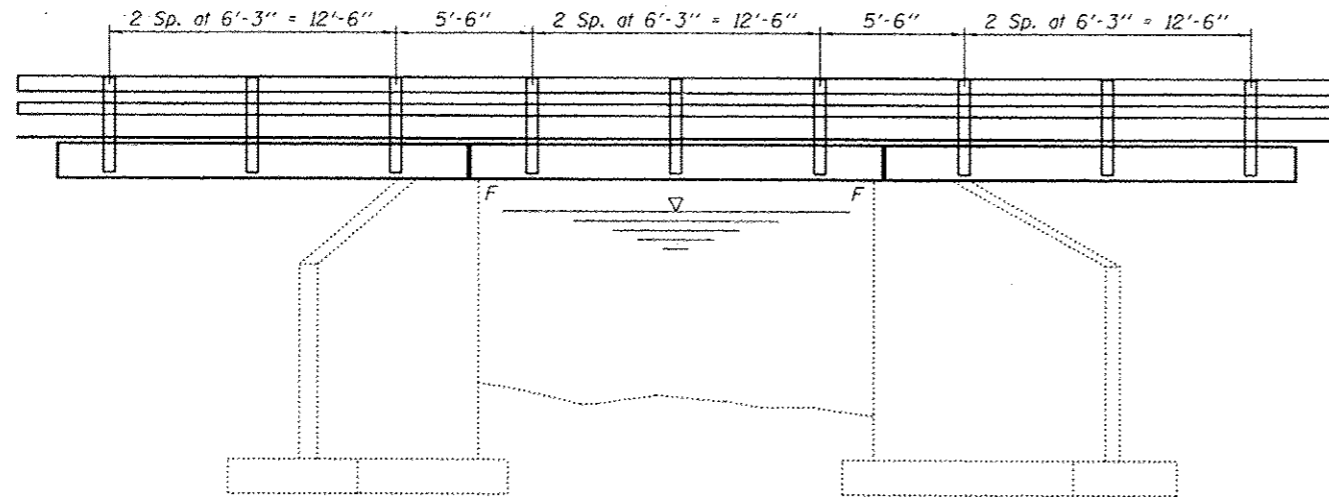
R-34HMAWS 7-1-10 (6'-3" Maximum Post Spacing) (1/4" minimum to 3/8" maximum HMA thickness)

DESIGNED - VHV	EXAMINED - Timothy A. [Signature]	DATE - JANUARY 31, 2013
CHECKED - DAB	ACTING ENGINEER OF STRUCTURAL SERVICES	
DRAWN - Kyle M. Steffen	PASSED - [Signature]	
CHECKED - VHV DAB	ACTING ENGINEER OF BRIDGES AND STRUCTURES	

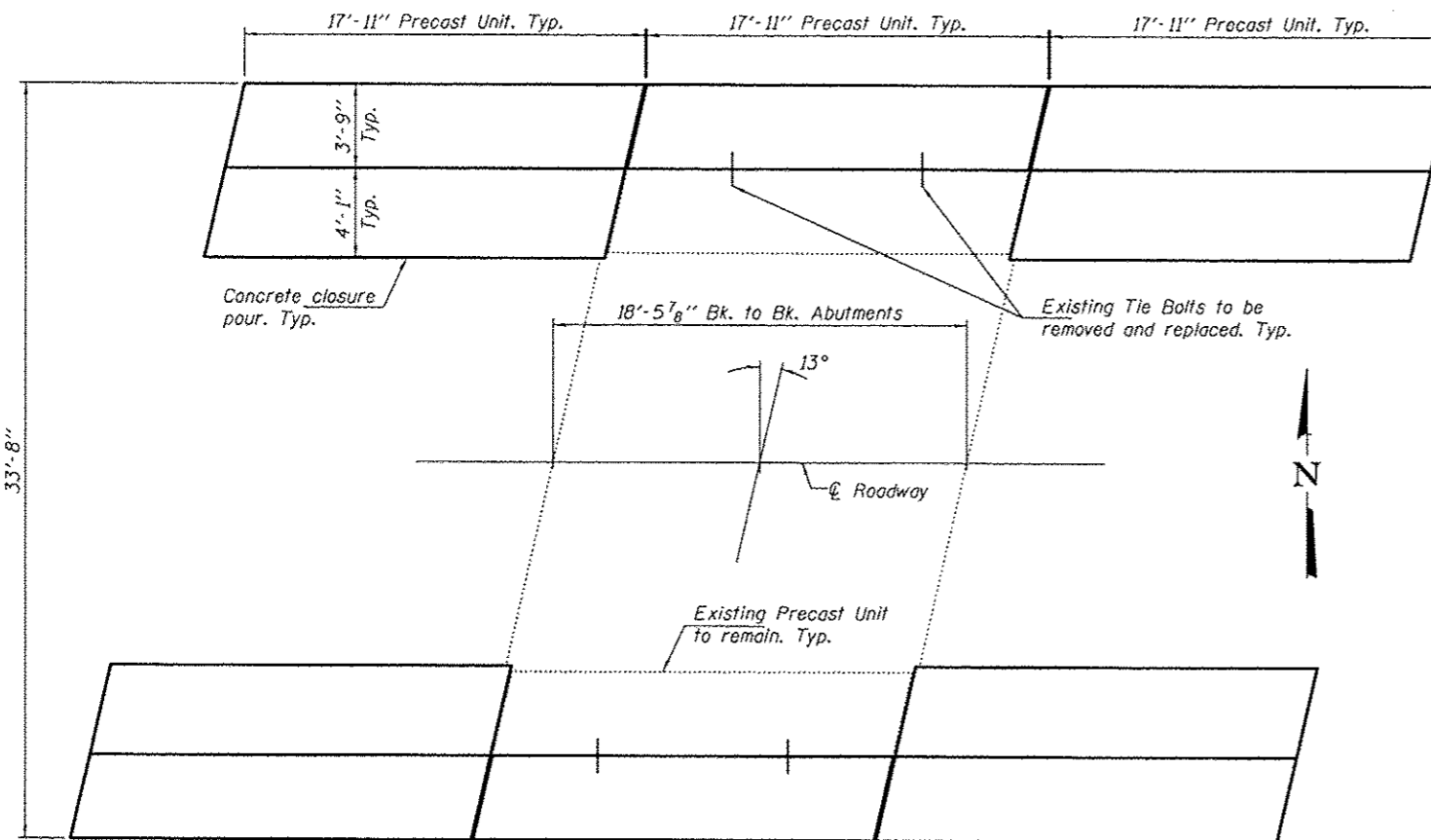
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

STEEL RAILING, TYPE SM  
SN 060-0246  
SHEET NO. 6 OF 6 SHEETS

F.A.S. RTE. 775	SECTION (54, 58, 68)RS-4	COUNTY MADISON	TOTAL SHEETS 44	SHEET NO. 39
				CONTRACT NO. 76F27
ILLINOIS FED. AID PROJECT				



**ELEVATION**



**PLAN**

**GENERAL NOTES**

Plan dimensions and details relative to existing structure have been taken from existing plans and are subject to nominal construction variations. It shall be the Contractor's responsibility to verify such dimensions and details in the field and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in the scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

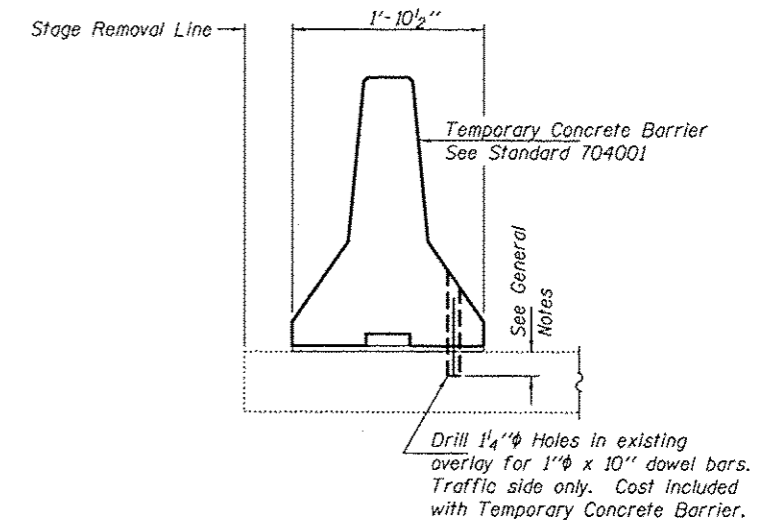
Any damage done to the bridge during Precast Unit removal shall be repaired by the Contractor. Cost to be included in the cost of Removal of Existing Precast Concrete Units.

Temporary concrete barrier shall only be anchored into the overlay and not the Precast Concrete Units.

Reinforcement bars designated (E) shall be epoxy coated.

The contractor shall salvage the existing bridge rail and posts and shall deliver them to:

IDOT Maintenance Facility  
12540 Sportsman Road  
Highland, Illinois 62249  
Contact: Todd Reilson at 618-659-5110  
Cost included with Bridge Rail Removal.



**SECTION THRU SLAB**

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	QUANTITY
Precast Concrete Bridge Slab	Sq. Ft.	403
Removal of Existing Precast Concrete Units	Sq. Ft.	403
Bridge Rail Removal	Foot	108
Reinforcement Bars, Epoxy Coated	Pound	2230
Steel Railing, Type SM	Foot	108
Waterproofing Membrane System	Sq. Yd.	44.8
Concrete Removal	Cu. Yd.	13.5
Concrete Superstructure	Cu. Yd.	13.5

\* On Precast Concrete Bridge Slab only.  
For quantity of HMA Surface Course, see roadway plans

**DESIGN STRESSES**

**PC UNITS**

$f_c = 1,800$  psi  
 $f'_c = 4,500$  psi  
 $f_s = 20,000$  psi (Reinforcement)  
 $n = 8$

**FIELD UNITS**

$f_c = 3,500$  psi  
 $f_y = 60,000$  psi (Reinforcement)



EXPIRES 11-30-2014

DESIGNED: <i>John S. Vecit</i>	EXAMINED: <i>Timothy A. ...</i>	DATE: JANUARY 31, 2013
CHECKED: <i>John S. Vecit</i>	PASSED: <i>David Carl Puzey</i>	REVISED:
DRAWN: <i>Doliva</i>	REVISED:	REVISED:
CHECKED: <i>VP</i>	ACTING ENGINEER OF BRIDGES AND STRUCTURES	

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

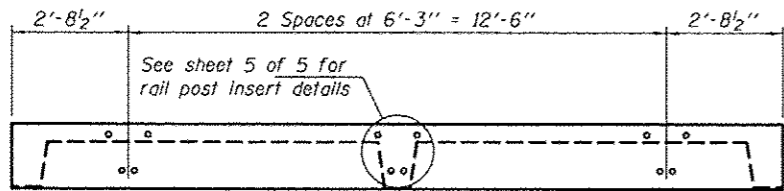
PLAN AND ELEVATION  
FAP 789 (IL. RTE. 143) OVER E. TRIBUTARY TO SILVER CREEK  
SN 060-0249

F.A.P. RTE. 789	SECTION 154.50, 60RS-4	COUNTY MADISON	TOTAL SHEETS 44	SHEET NO. 40
			CONTRACT NO. 76F27	

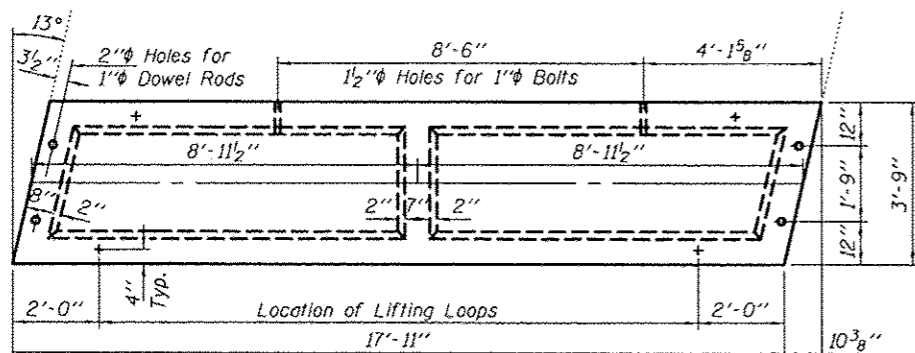
SHEET NO. 1 OF 5 SHEETS

ILLINOIS FED. AID PROJECT

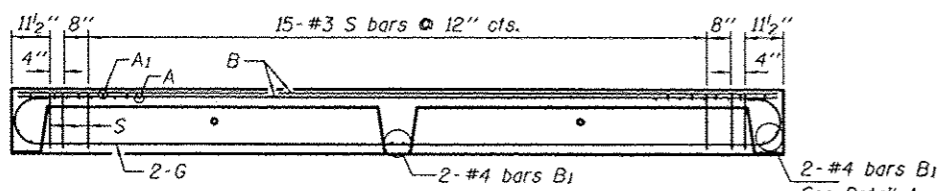




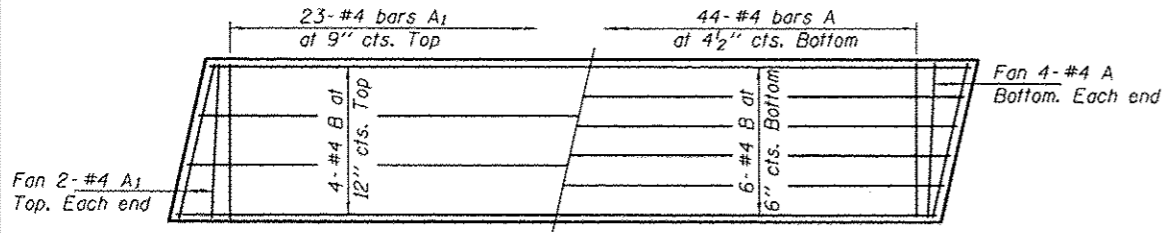
**ELEVATION**  
Showing rail post spacing



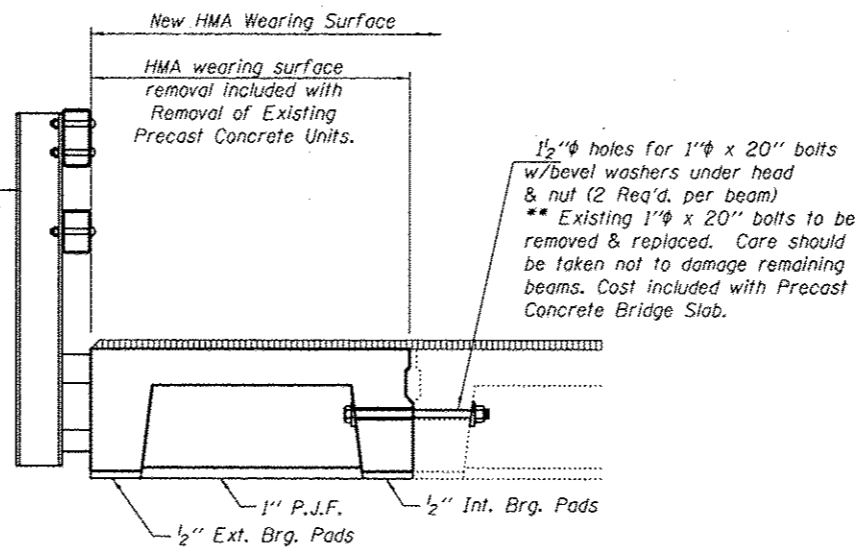
**TYPICAL PLAN OF BEAM**



**LONGITUDINAL SECTION**

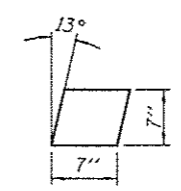


**PLAN**  
Showing Slab Reinforcement

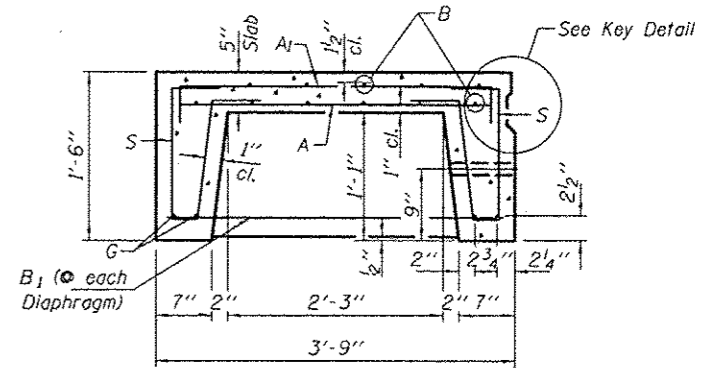


**PARTIAL CROSS SECTION**

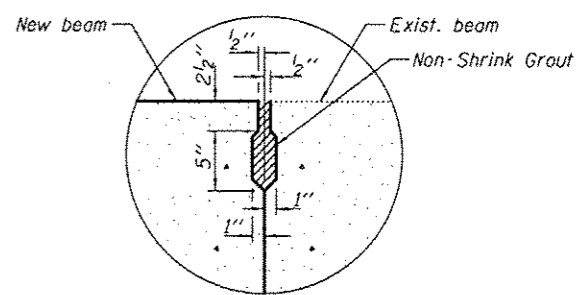
\*\* New bolts shall only be snug tightened after placement.



**BEARING PADS**  
1/2" Fabric Pads  
(4 Req'd. per beam)

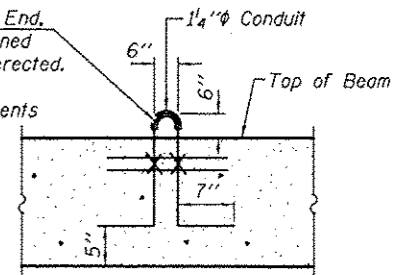


**TYPICAL SECTION THRU BEAM**



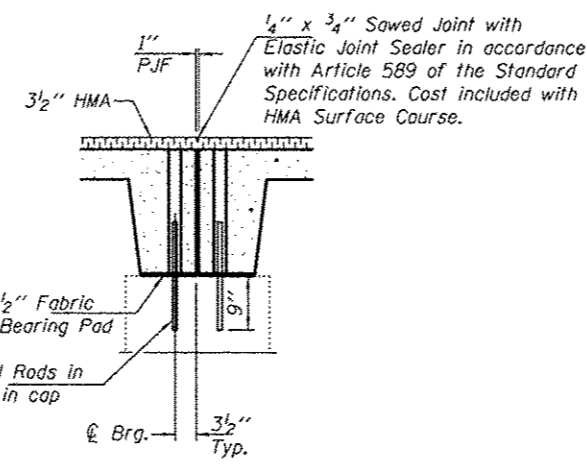
**KEY DETAIL**

3-1/2"φ Strands/Loop, 2 Ea. End, Ea. Beam. Loop shall be burned off after beams have been erected. Strands shall conform to the requirements of AASHTO M 203.



**LIFTING LOOP**

Approved alternate may be substituted for the above.

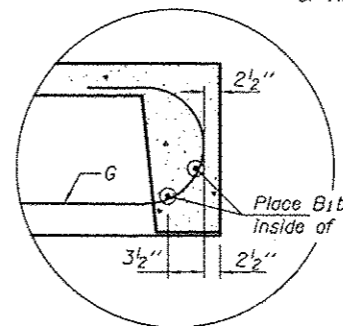


**SECTION THRU ABUTMENT**

(At Rt. Ls)

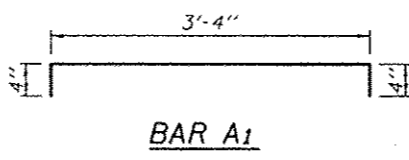
\* 1"φ x 1'-6" Dowel Rods in 1 1/2"φ holes drilled in cap (2 Each Beam)

\* Existing dowel rods are to be burned off, ground flush, and sealed with epoxy prior to placement of new beams. Cost included in Removal of Existing Precast Concrete Units. After beams have been erected holes shall be drilled into cap and dowel rods placed. Dowel holes shall be filled with non-shrink grout to top of beam and allowed to cure a minimum of 24 hours prior to grouting the shear keys.

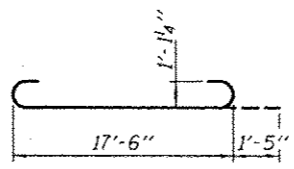


**DETAIL A**

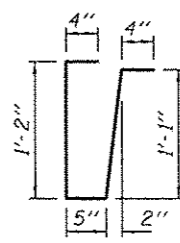
Note: The surface of the member shall not deviate more than 1/1200 of the full length of the member from a straight line connecting the two end points on the member's surface. (See Special Provision Precast Concrete Structures)



**BAR A1**



**BAR G**



**BAR S**

**\* BILL OF REINFORCEMENT BARS FOR ONE BEAM**

Bar	No.	Size	Length	Shape
A	52	#4	3'-3"	┌───┐
A1	27	#4	4'-0"	┌───┐
B	10	#4	17'-6"	┌───┐
B1	6	#4	3'-6"	┌───┐
G	4	#10	20'-4"	┌───┐
S	38	#3	3'-4"	┌──┐

**\* ESTIMATED QUANTITIES FOR ONE BEAM**

	CONCRETE CU. YDS.	RE-BARS POUNDS
One Unit	2.3	710

\* For information of suppliers of Precast Slab Units only.

DESIGNED VP  
CHECKED VHV  
DRAWN boliva  
CHECKED VP VHV

EXAMINED  
PASSED  
ACTING ENGINEER OF BRIDGES AND STRUCTURES

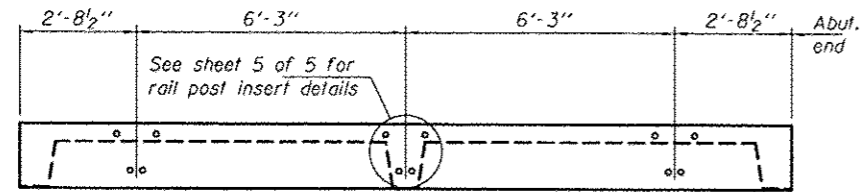
DATE JANUARY 31, 2013  
REVISED  
REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

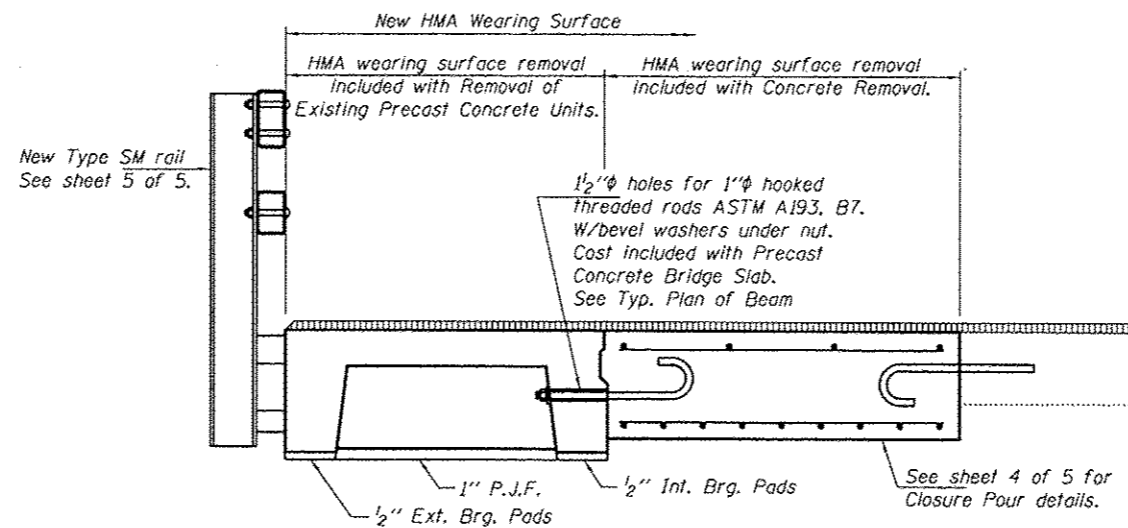
SUPERSTRUCTURE DETAILS  
SN 060-0249

SHEET NO. 2 OF 5 SHEETS

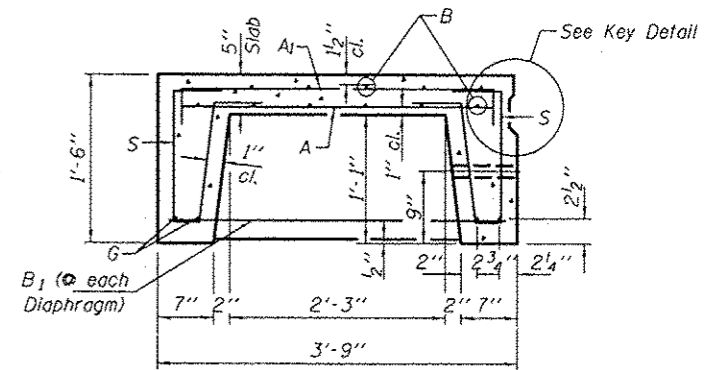
F.A.P. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.  
789 054,58,60RS-4 MADISON 44 41  
CONTRACT NO. 76F27  
ILLINOIS FED. AID PROJECT



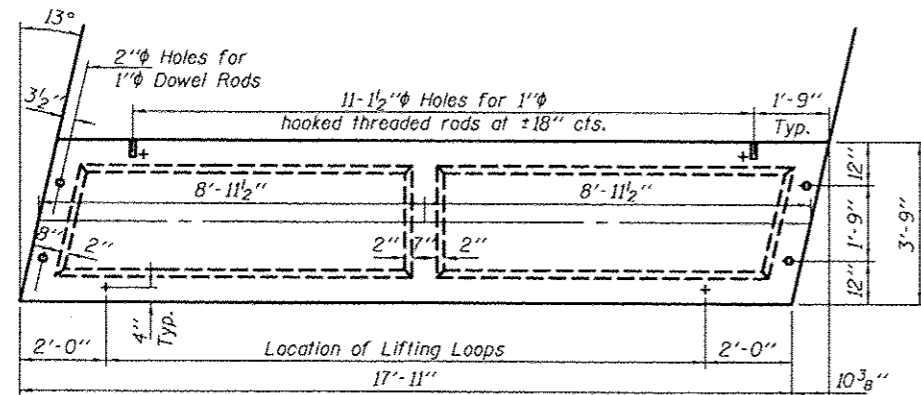
**ELEVATION**  
Showing rail post spacing



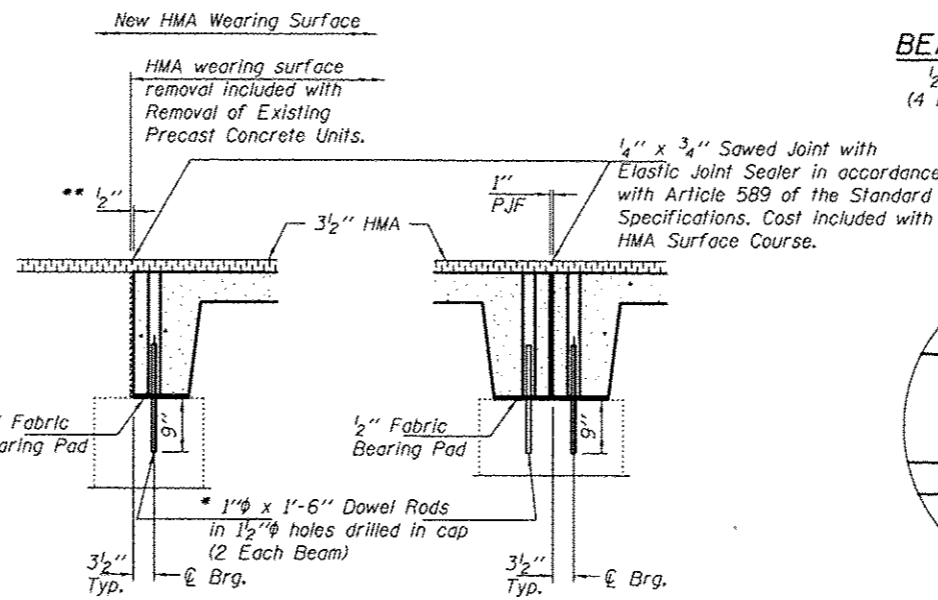
**PARTIAL CROSS SECTION**



**TYPICAL SECTION THRU BEAM**

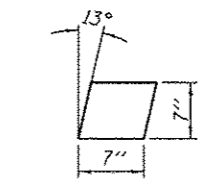


**TYPICAL PLAN OF BEAM**

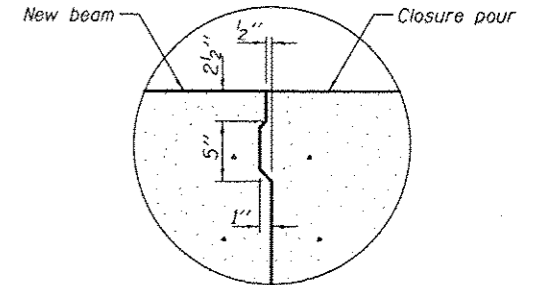


**SECTION THRU APPROACH CAP**  
(At Rt. Ls)

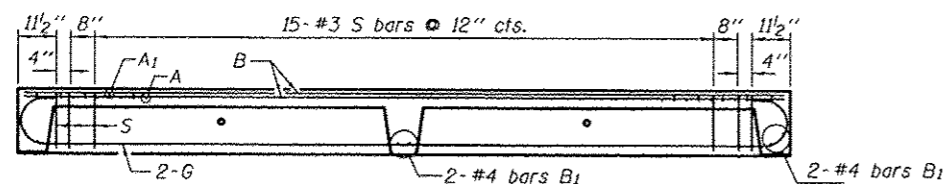
**SECTION THRU ABUTMENT**  
(At Rt. Ls)



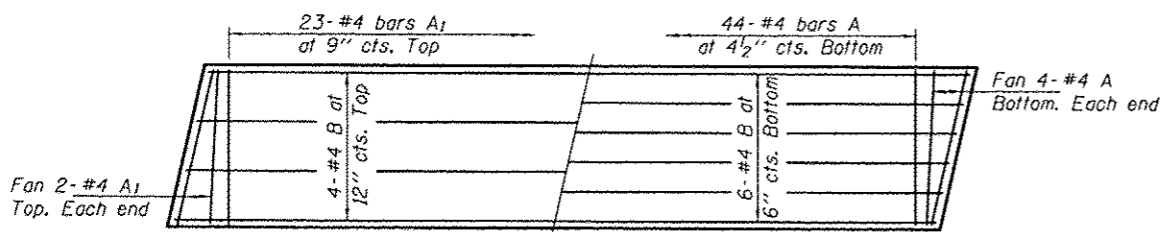
**BEARING PADS**  
1/2" Fabric Pads  
(4 Req'd. per beam)



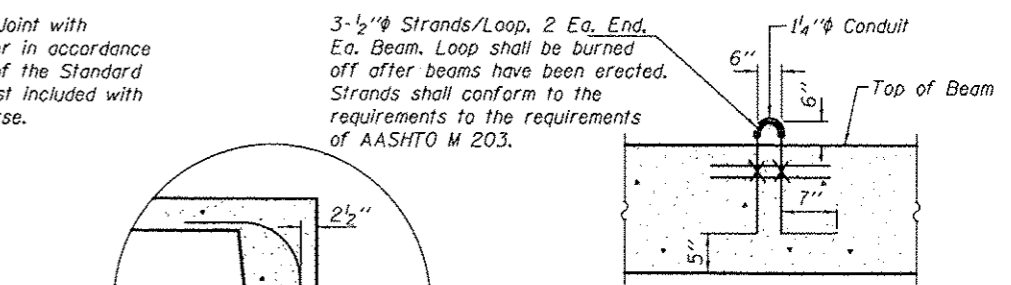
**KEY DETAIL**



**LONGITUDINAL SECTION**



**PLAN**  
Showing Slab Reinforcement



**LIFTING LOOP**

Approved alternate may be substituted for the above.

**\* BILL OF REINFORCEMENT BARS FOR ONE BEAM**

Bar	No.	Size	Length	Shape
A	52	#4	3'-3"	—
A1	27	#4	4'-0"	□
B	10	#4	17'-6"	—
B1	6	#4	3'-6"	—
G	4	#10	20'-4"	—
S	38	#3	3'-4"	U

**\* ESTIMATED QUANTITIES FOR ONE BEAM**

	CONCRETE CU. YDS.	RE-BARS POUNDS
One Unit	2.3	710

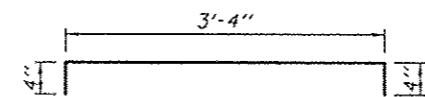
\* For information of suppliers of Precast Slab Units only.

\* Existing dowel rods are to be burned off, ground flush, and sealed with epoxy prior to placement of new beams. Cost included in Removal of Existing Precast Concrete Units. After beams have been erected holes shall be drilled into cap and dowel rods placed. Dowel holes shall be filled with non-shrink grout to top of beam and allowed to cure a minimum of 24 hours prior to grouting the shear keys.

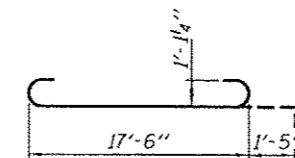
Note: The surface of the member shall not deviate more than 1/1200 of the full length of the member from a straight line connecting the two end points on the member's surface. (See Special Provision Precast Concrete Structures)

\*\* 1/2" joint shall be packed with a very dry mix of 2:1 sand and P.C. Mortar. 1/2" dimension may vary plus or minus to accommodate tolerance in beam lengths.

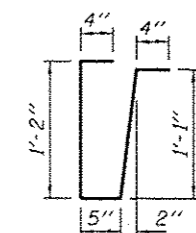
Note: Tack welding of stirrups to bottom longitudinal reinforcement bars will not be permitted except as otherwise authorized in writing by the Engineer.



**BAR A1**



**BAR G**



**BAR S**

DESIGNED VP  
CHECKED VHV  
DRAWN daliva  
CHECKED VP VHV

EXAMINED  
PASSED  
ACTING ENGINEER OF BRIDGES AND STRUCTURES

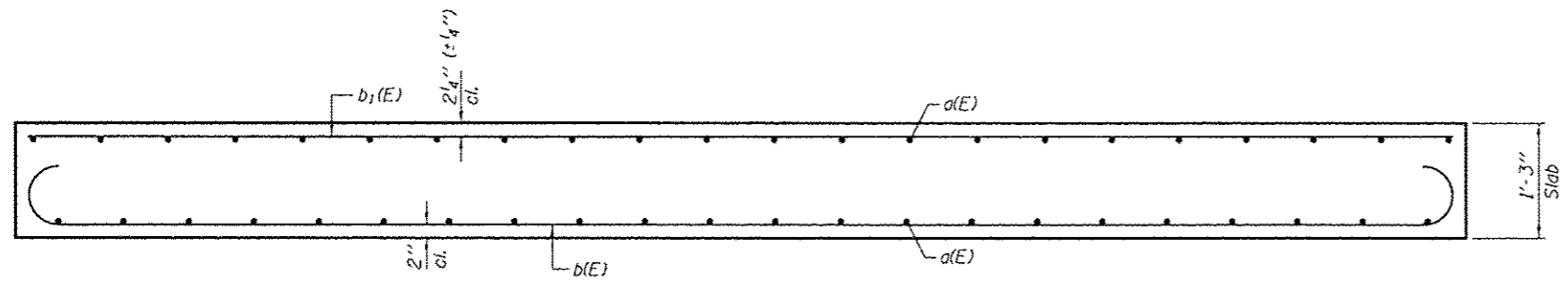
DATE JANUARY 31, 2013  
REVISED  
REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

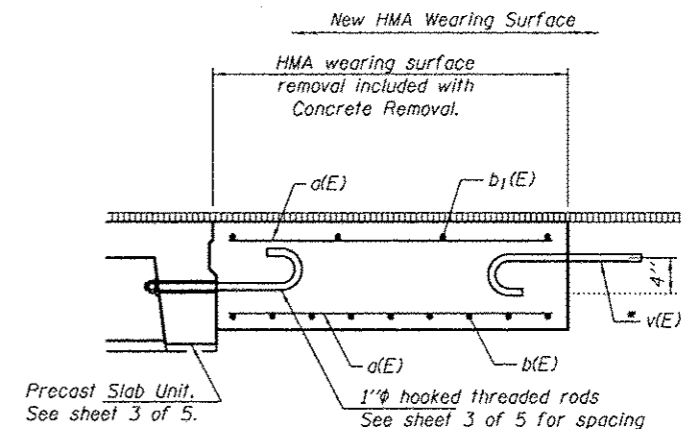
APPROACH DETAILS  
SN 060-0249

SHEET NO. 3 OF 5 SHEETS

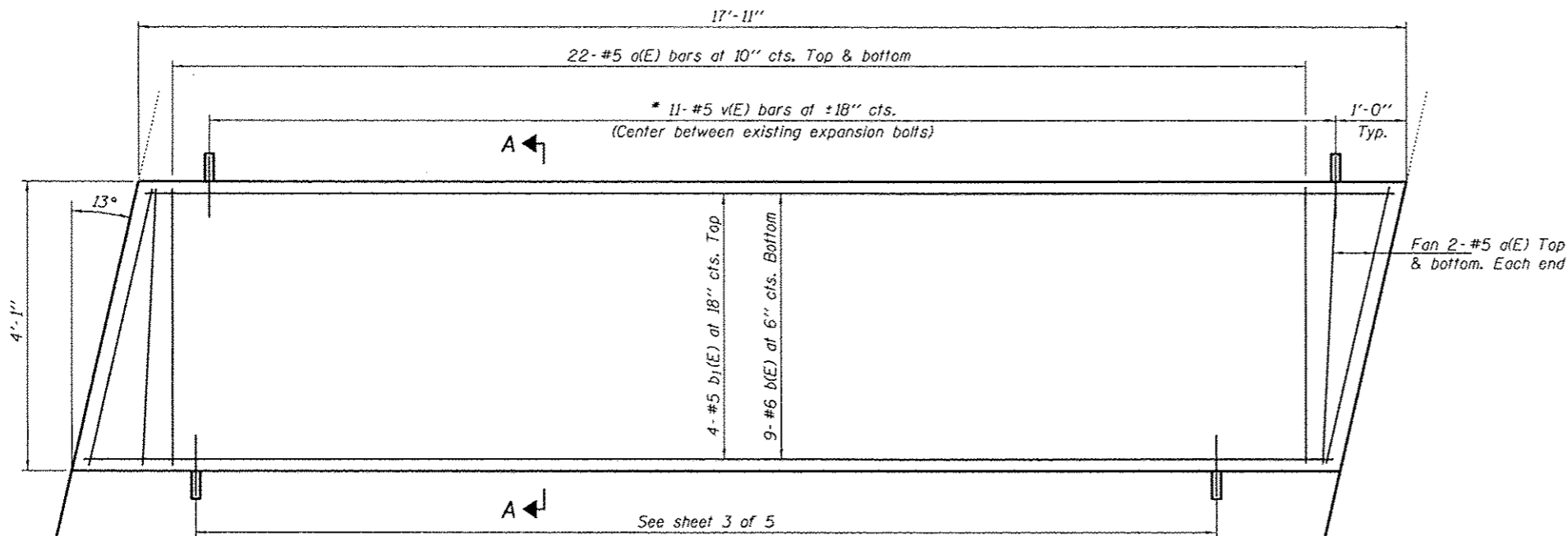
F.A.P. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.  
789 054,58,68,85-4 MADISON 44 42  
CONTRACT NO. 76F27  
[ILLINOIS] FED. AID PROJECT



SECTION THRU SLAB

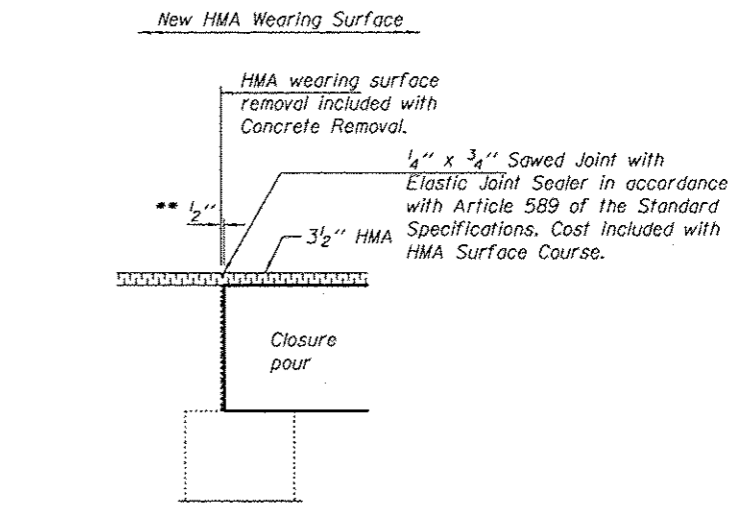


SECTION A-A



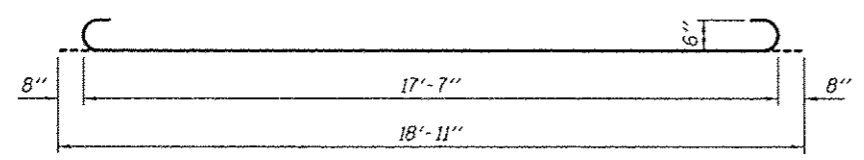
PLAN

Note:  
Slab to be poured after  
Precast Slab Unit is in place.



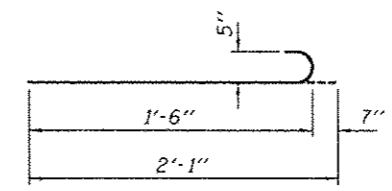
SECTION THRU APPROACH CAP  
(At Rt. Ls)

\*\* 1/2" joint shall be packed with a very dry mix of 2:1 sand and P.C. Mortar. 1/2" dimension may vary plus or minus to accommodate tolerance in beam lengths.



BAR b(E)

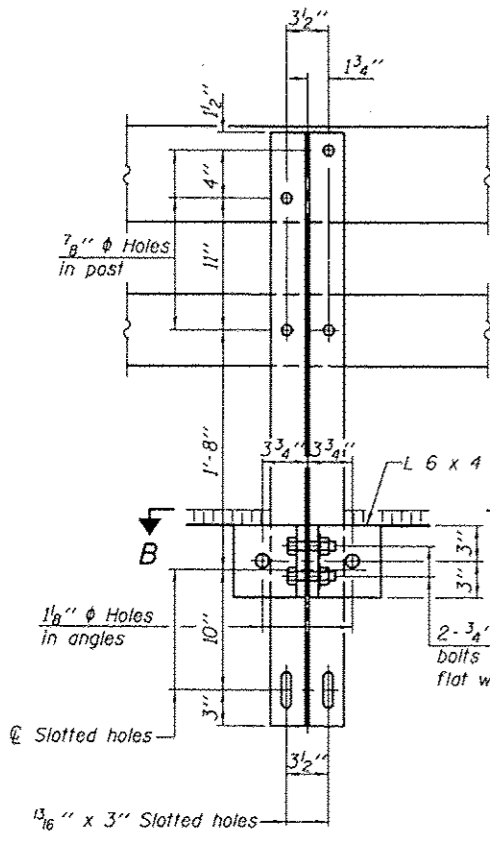
\* Epoxy grout v(E) bars in 9" min. holes according to Article 584 of the Standard Specifications.



BAR v(E)

FOUR CLOSURE POURS  
BILL OF MATERIAL

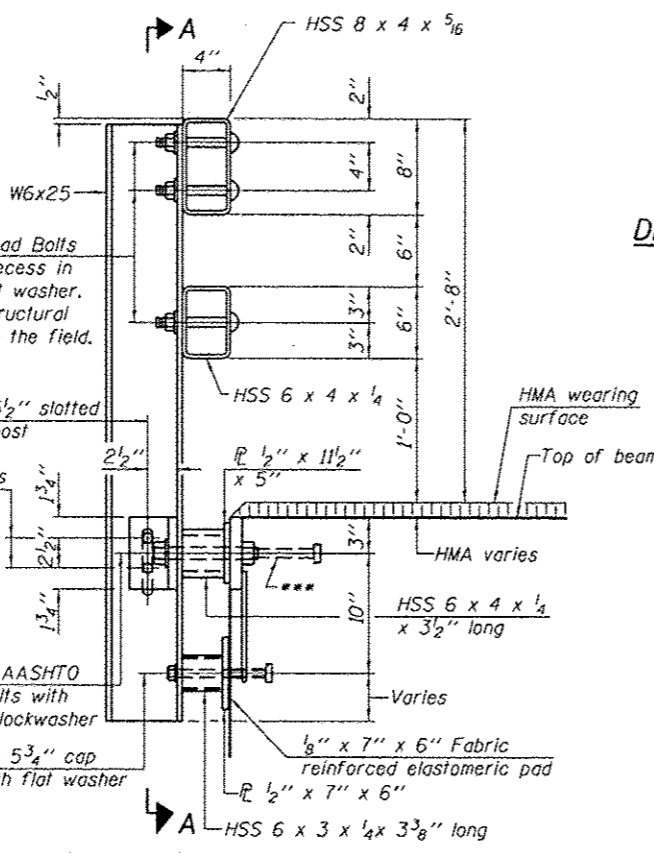
Bar	No.	Size	Length	Shape
a(E)	208	#5	3'-9"	—
b(E)	36	#6	18'-11"	⌋
b1(E)	16	#5	17'-7"	—
v(E)	44	#5	2'-1"	⌋
Concrete Removal			Cu. Yd.	13.5
Concrete Superstructure			Cu. Yd.	13.5
Reinforcement Bars, Epoxy Coated			Pound	2230



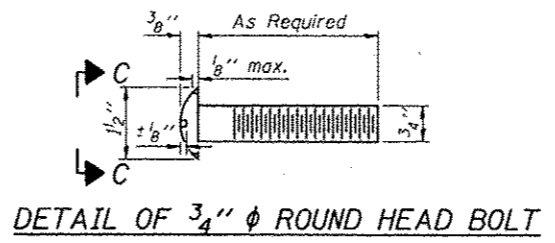
SECTION A-A

4-3/4"  $\phi$  x 6" Round Head Bolts (With slot or approved recess in head) with locknut & flat washer. 7/8"  $\phi$  holes in hollow structural section may be drilled in the field.

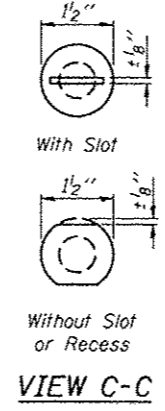
2-3/4"  $\phi$  x 3 1/4" H.S. bolts with hex nut & flat washers  
2-1"  $\phi$  x 7 3/4" AASHTO M-164 anchor bolts with flat washer and lockwasher  
2-5/8"  $\phi$  x 5 3/4" cap screws with flat washer



SECTION AT RAIL POST

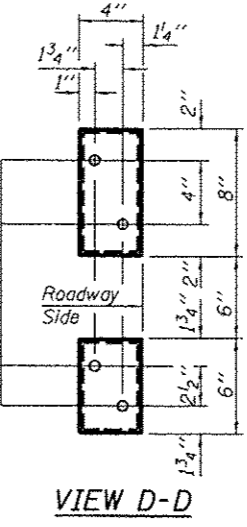


DETAIL OF 3/4"  $\phi$  ROUND HEAD BOLT

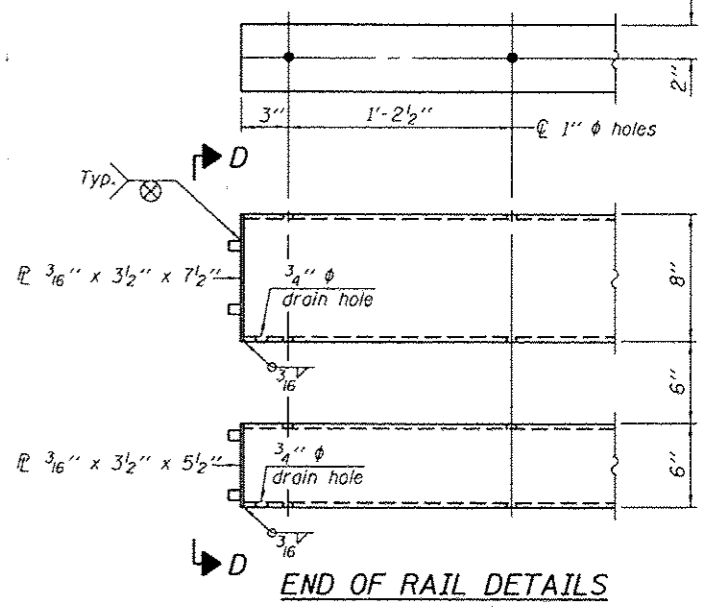


VIEW C-C

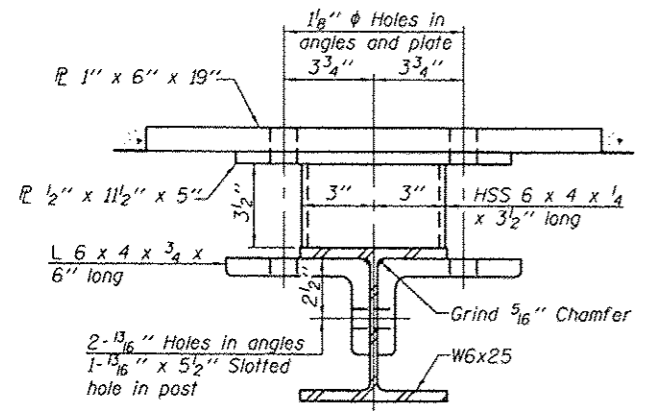
4-5/8" reduced base welded studs. Provide 4-5/8" washers and self-locking nuts or nuts and jam nuts for guardrail connection shown on Std. 631032.



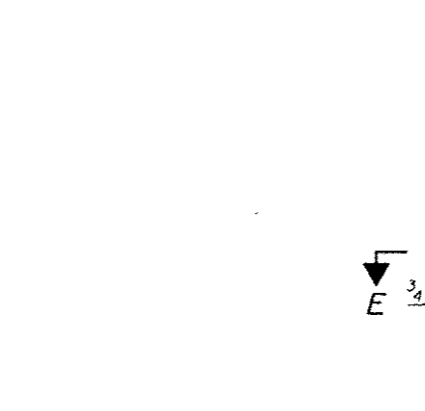
VIEW D-D



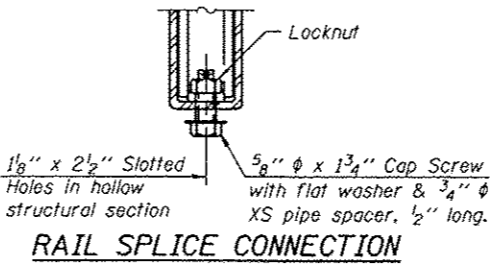
END OF RAIL DETAILS



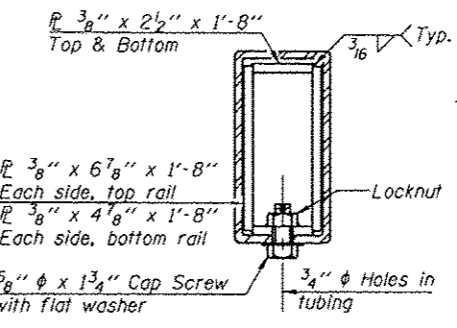
SECTION B-B



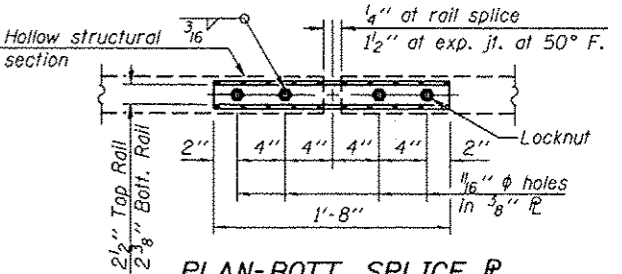
SECTION AT RAIL SPLICE



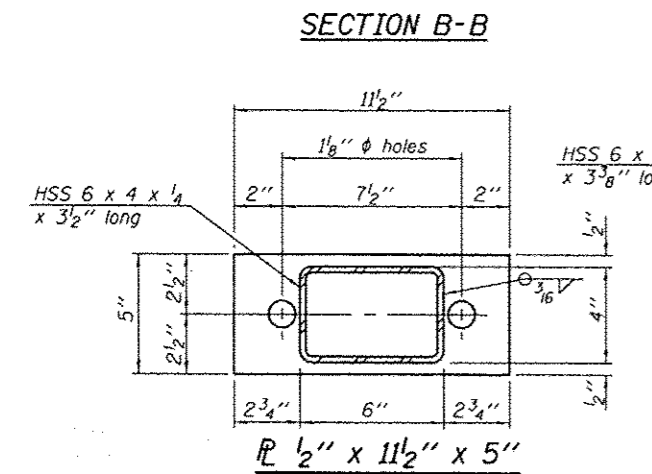
RAIL SPLICE CONNECTION AT EXPANSION JT.



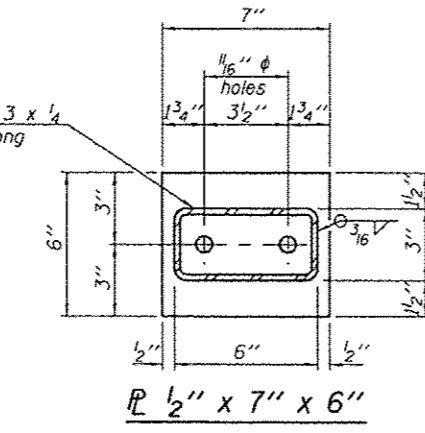
SECTION AT RAIL SPLICE



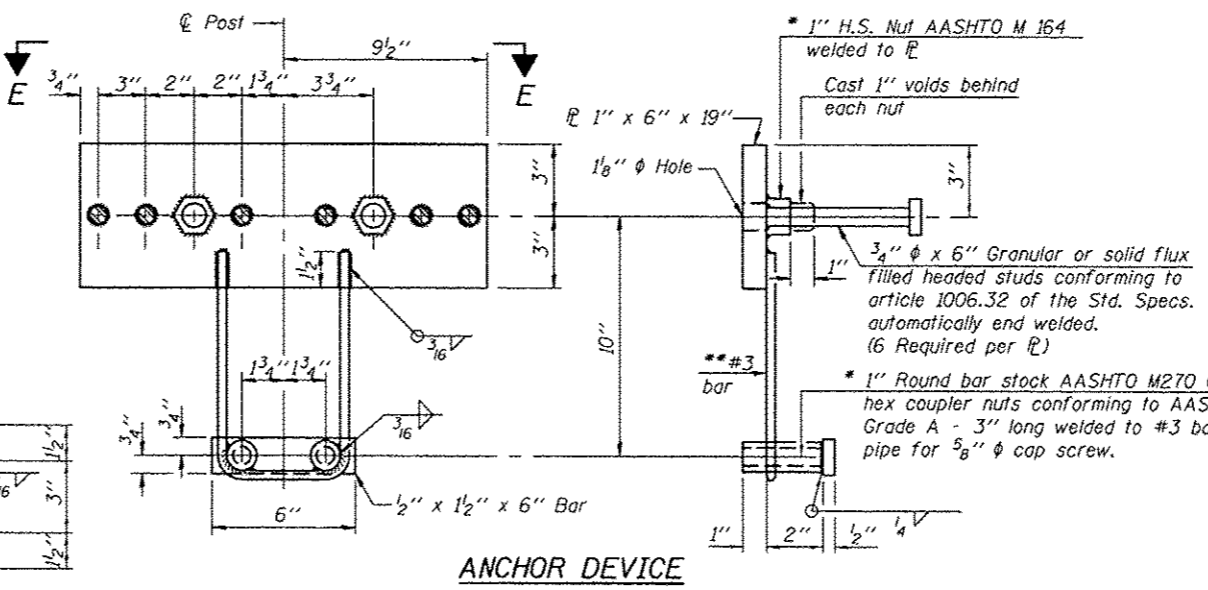
PLAN-BOTT. SPLICE TYPICAL



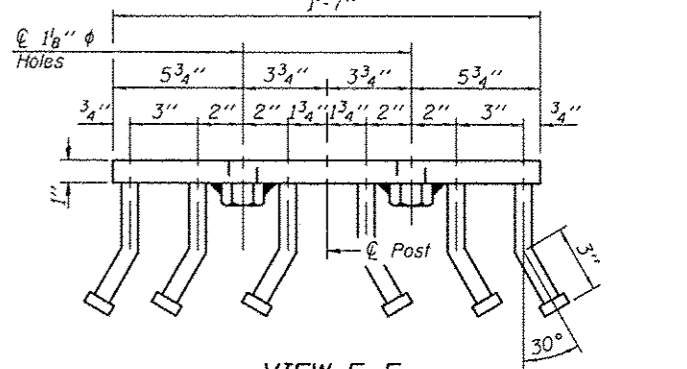
SECTION B-B (continued)



SECTION AT RAIL SPLICE (continued)



ANCHOR DEVICE



VIEW E-E

Notes:  
All field drilled holes shall be coated with an approved zinc rich paint before erection.  
For multi-span bridges, sufficient 1/4" x 6" x 1'-2" galvanized steel shims shall be provided to align rail between adjacent spans. Cost included with Steel Railing, Type SM.  
All steel rail members shall be galvanized according to Article 509.05 of the Standard Specifications.  
\*\*\* The studs of the anchor devices shall be placed below the top reinforcement bars and the outermost longitudinal reinforcement bar shall be placed directly above the studs of the rail post anchor device.

\* Threaded areas shall be plugged or blocked off during casting of beam. Galvanized after fabrication.

\*\* Whenever the lower insert assemblies interfere with strand locations, the #3 bars shall be cut and adjusted in order to allow raising or lowering of the lower inserts. Maximum adjustment not to exceed 1/2".

BILL OF MATERIAL

Item	Unit	Quantity
Steel Railing, Type SM	Foot	108

DESIGNED VP	EXAMINED	DATE
CHECKED VHV	<i>Timothy A. Anhalt</i>	JANUARY 31, 2013
DRAWN ballvo	ACTING ENGINEER OF STRUCTURAL SERVICES	
CHECKED VP VHV	PASSED	
	<i>H. Carl Long</i>	
	ACTING ENGINEER OF BRIDGES AND STRUCTURES	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
REVISIONS
REVISIONS

STEEL RAILING, TYPE SM
SN 060-0249
SHEET NO. 5 OF 5 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
789	IS4,58,68RS-4	MADISON	44	44
CONTRACT NO. 76F27			ILLINOIS FED. AID PROJECT	