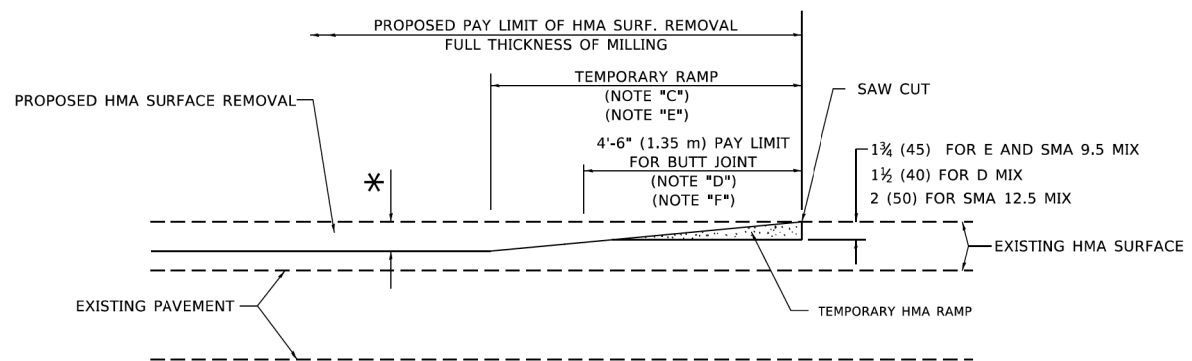


MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

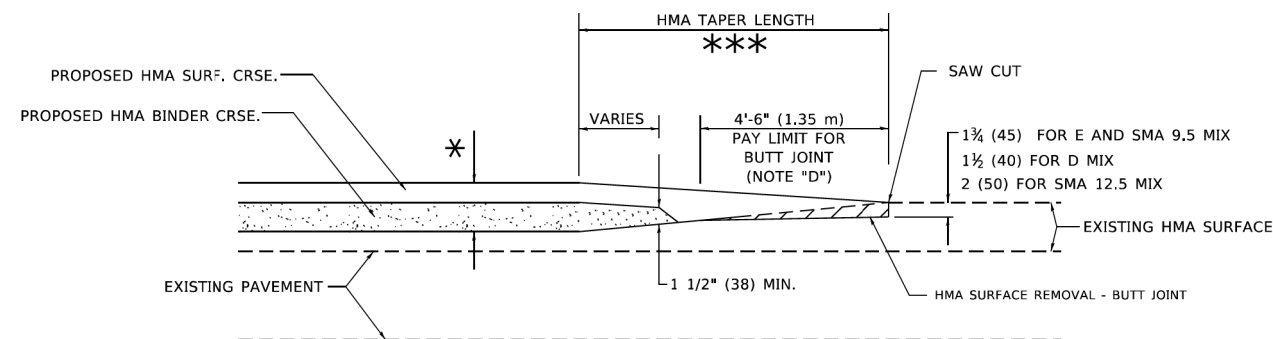
OPTION 1



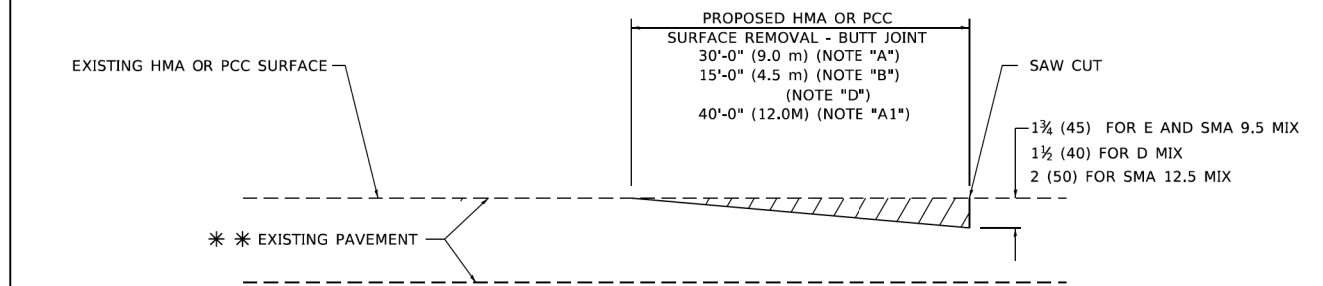
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

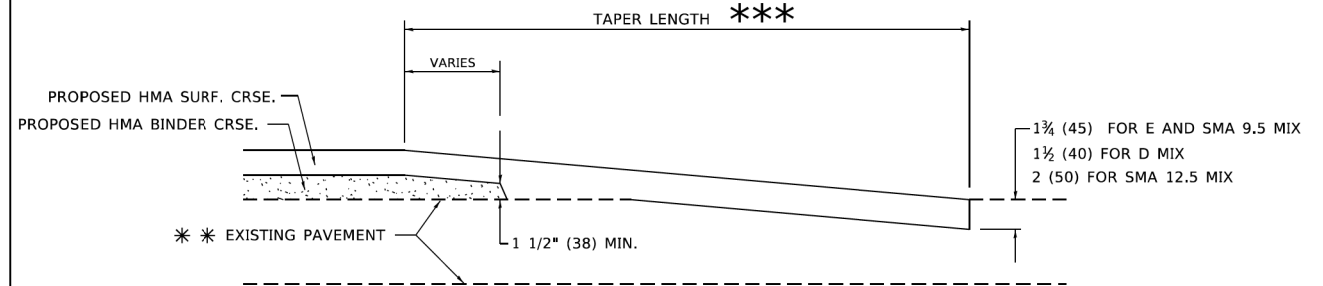
TYPICAL TEMPORARY RAMP



BUTT JOINT AND HMA TAPER
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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CHECKED: LB
DATE: 06-13-90
REVISIONS: A. ABBAS 03-21-97, M. GOMEZ 04-06-01, R. BORO 01-01-07, K. SMITH 11-18-22

USER NAME = Lawrence, DeManche	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97
PLOT SCALE = 100.0000' / in.	CHECKED - LB	REVISED - M. GOMEZ 04-06-01
PLOT DATE = 11/18/2022	DATE - 06-13-90	REVISED - R. BORO 01-01-07
		REVISED - K. SMITH 11-18-22

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			BD400-05	BD-32	CONTRACT NO.		
			ILLINOIS FED. AID PROJECT				

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DRAWN: IBOZENIUS
CHECKED: LB
DATE: 9/1/23
REVISIONS:



USER NAME = Ibozenius	DESIGNED - BAW	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED - LB	REVISED -
PLOT DATE = 12/28/2023	DATE - 9/1/23	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GENEVA ROAD
IDOT DISTRICT 1 STANDARD DRAWINGS**

SCALE: NTS	SHEET 4 OF 18 SHEETS	STA. TO STA.	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			1397	18-00206-10-BR	DUPAGE	134	102
			CONTRACT NO. 61J30				
			ILLINOIS FED. AID PROJECT				

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

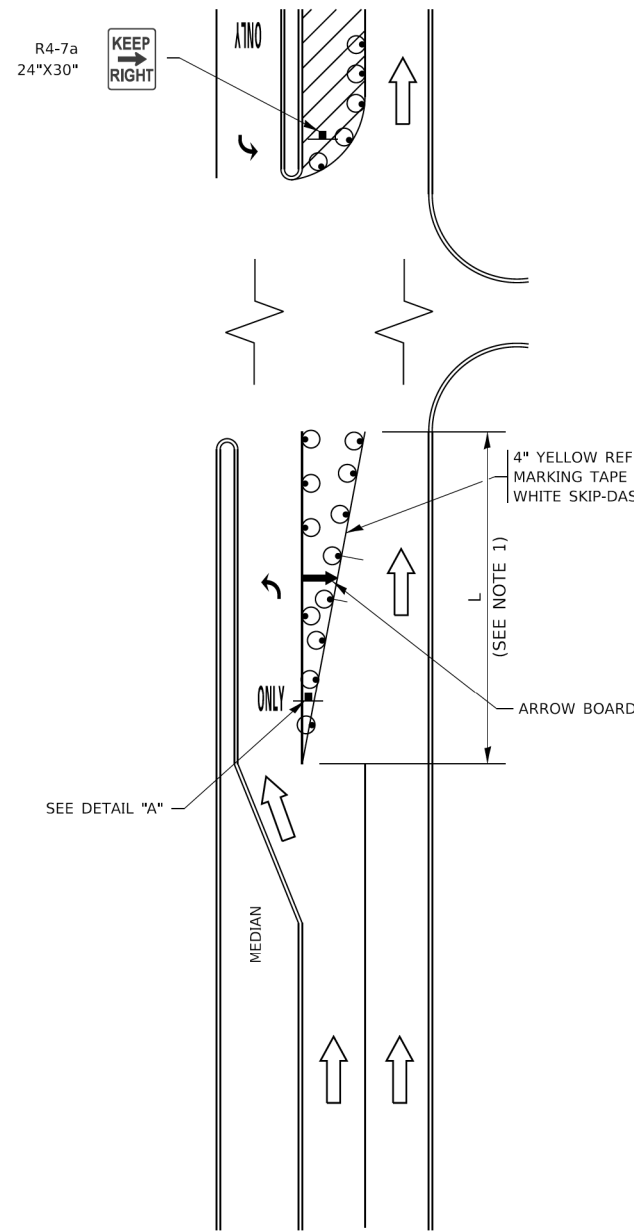
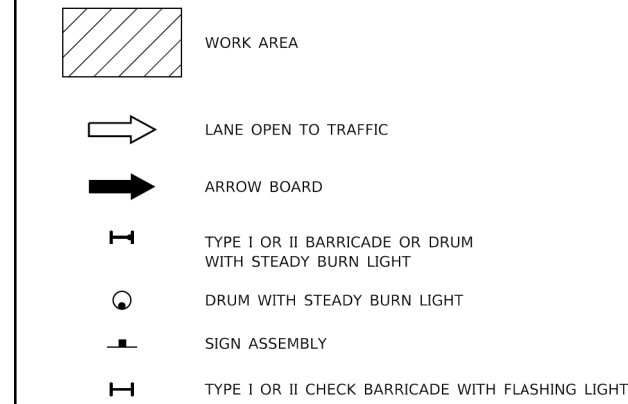


FIGURE 1

LEGEND



NOTES:

1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

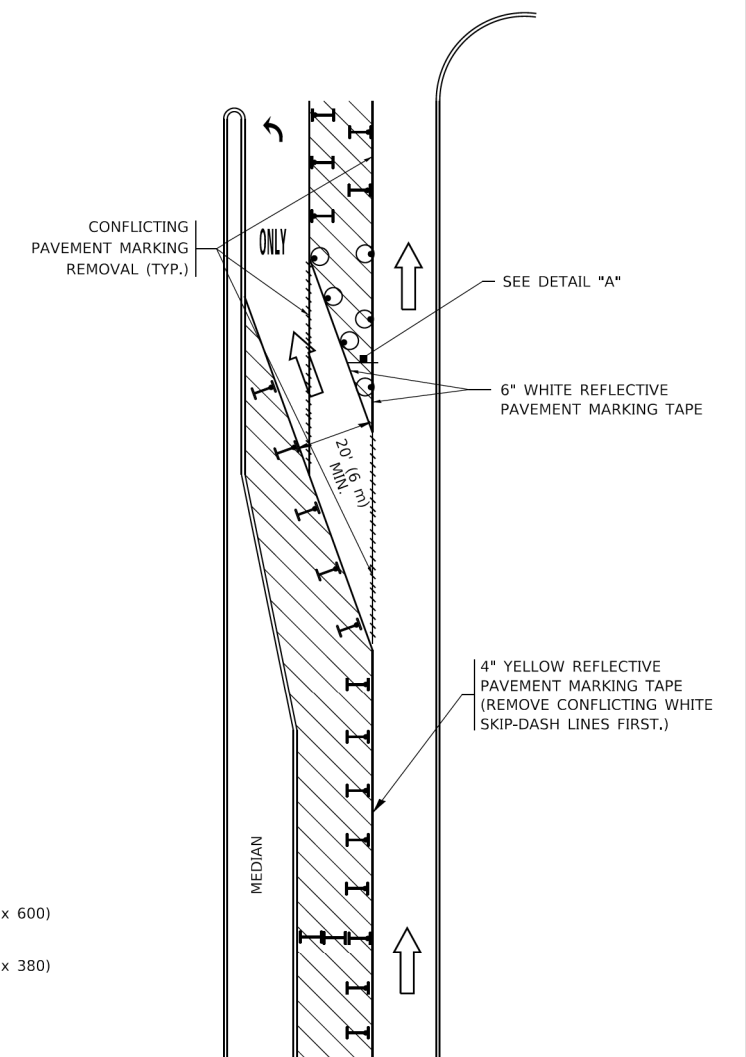
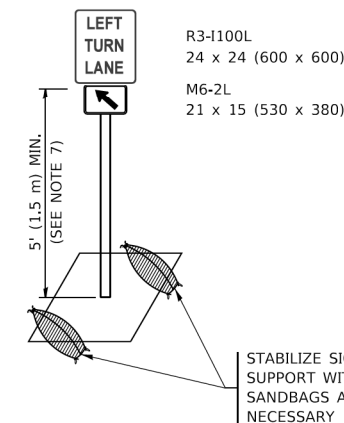


FIGURE 2



DETAIL A

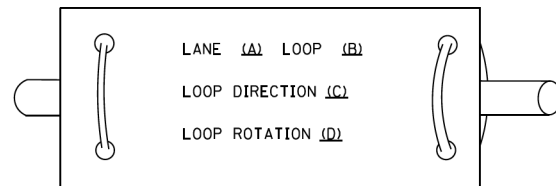
All dimensions are in inches (millimeters) unless otherwise shown.

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	USER NAME = Ibolzenius DESIGNED - BAW DRAWN - BAW CHECKED - LB PLCT SCALE = 100.0000" / in. PLCT DATE = 12/28/2023	DESIGNED - BAW DRAWN - BAW CHECKED - LB DATE - 9/1/23	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENEVA ROAD IDOT DISTRICT 1 STANDARD DRAWINGS	F.A. U. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO. 1397 18-00206-10-BR DUPAGE 134 106 ILLINOIS FED. AID PROJECT

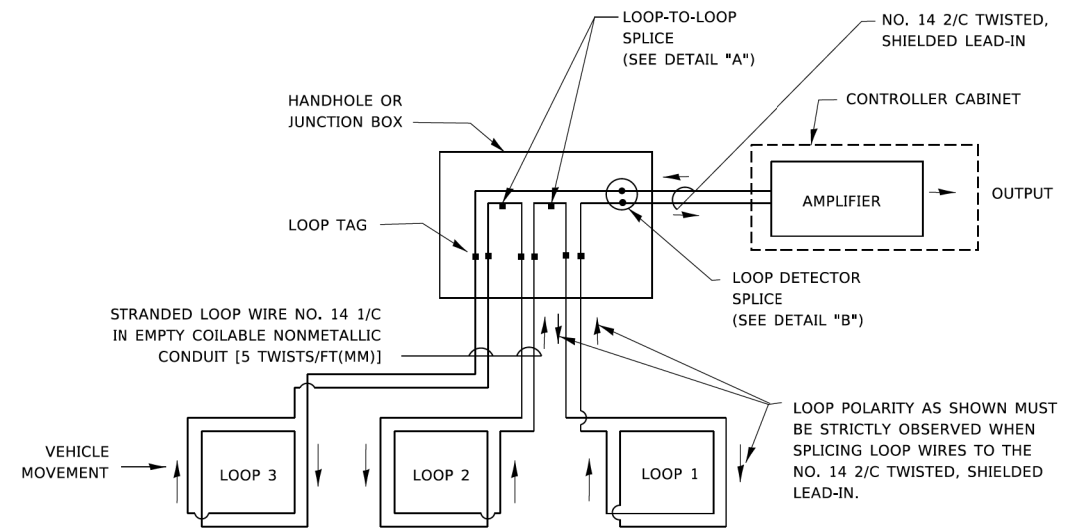
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

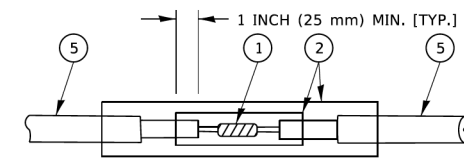


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

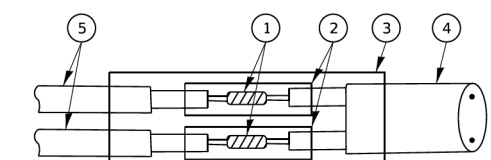


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES. SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE,
- THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

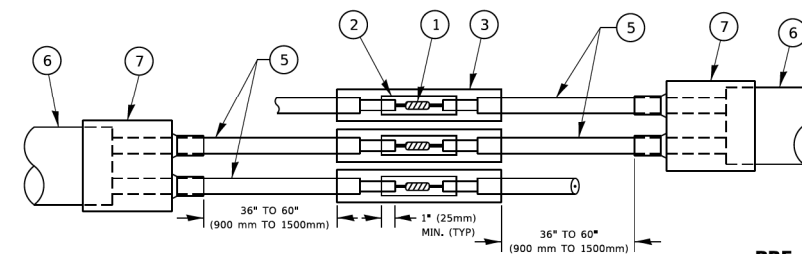


DETAIL "A"
LOOP-TO-LOOP SPLICE

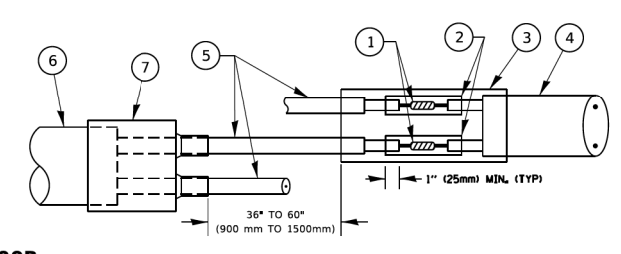


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PREFORMED LOOP

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE. PREFORMED LOOP
- 6 XL POLYOLEFIN 2 CONDUCTOR
- 7 BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

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	DATE -	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	
SCALE: NONE	SHEET 2 OF 7 SHEETS STA. TO STA.

DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS	
SCALE: NONE	SHEET 2 OF 7 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TS-05		CONTRACT NO.		
ILLINOIS		FED. AID PROJECT		

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PLOT DATE = 12/28/2023	CHECKED - LB	REVISED -
	DATE - 9/1/23	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	
SCALE: NTS	SHEET 14 OF 18 SHEETS STA. TO STA.

GENEVA ROAD IDOT DISTRICT 1 STANDARD DRAWINGS	
SCALE: NTS	SHEET 14 OF 18 SHEETS STA. TO STA.

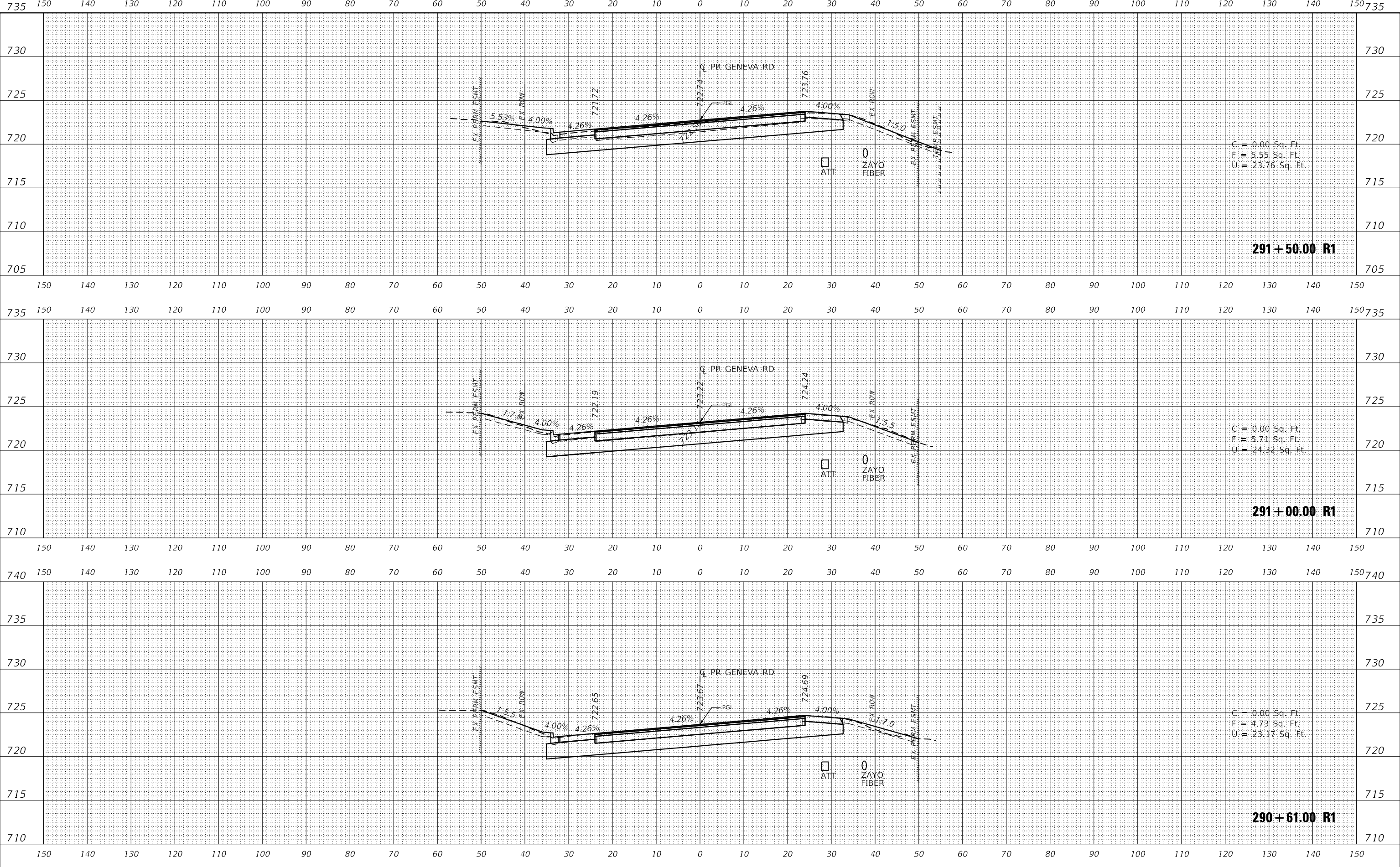
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1397	18-00206-10-BR	DUPAGE	134	111
		CONTRACT NO. 61J30		
		ILLINOIS FED. AID PROJECT		



FINAL SURVEY NO.	SURVEYED	DATE
NOTE BOOK	PLOTTED	
AREAS CHECKED	TEMPLATE	
	AREAS	

ORIGINAL SURVEY NO.	SURVEYED	DATE
	PLOTTED	
	TEMPLATE	
	AREAS	

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C = 0.00 Sq. Ft.
 F = 5.55 Sq. Ft.
 U = 23.76 Sq. Ft.

C = 0.00 Sq. Ft.
 F = 5.71 Sq. Ft.
 U = 24.32 Sq. Ft.

C = 0.00 Sq. Ft.
 F = 4.73 Sq. Ft.
 U = 23.17 Sq. Ft.

291+50.00 R1

291+00.00 R1

290+61.00 R1

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**



**GENEVA ROAD
 CROSS SECTIONS**

USER NAME = Ibolzenius	DESIGNED - EJB	REVISED -
	DRAWN - EJB	REVISED -
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PLOT DATE = 12/28/2023	DATE - 9/1/23	REVISED -

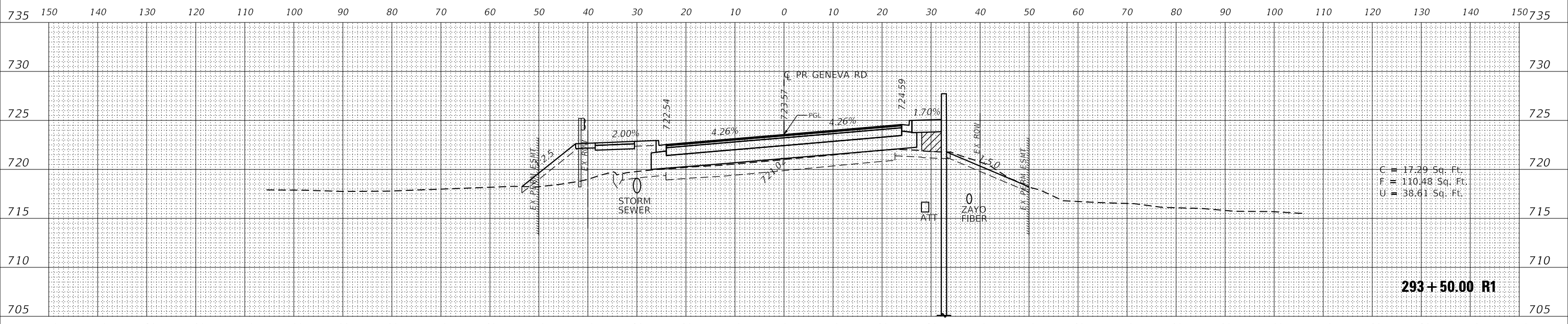
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F.A. U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 61130				
ILLINOIS		FED. AID PROJECT		

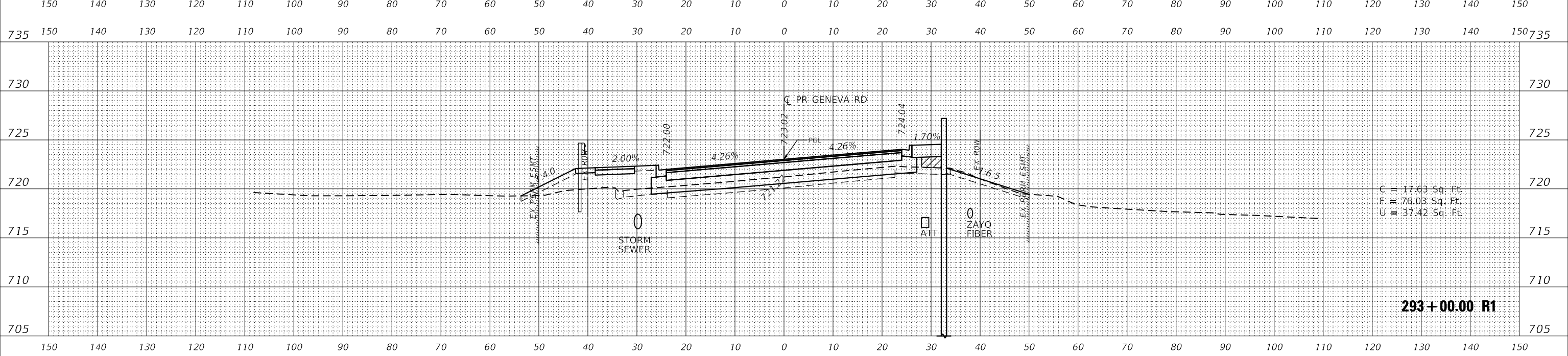


 POROUS GRANULAR BACKFILL
 TEMPORARY MSE WALL

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS CHECKED	
FINAL SURVEY NO.	
NOTE BOOK NO.	
AREAS CHECKED	



DATE	
BY	
SURVEYED	
PLOTTED	
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AREAS CHECKED	
ORIGINAL SURVEY NO.	
NOTE BOOK NO.	
AREAS CHECKED	



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	DRAWN - EJB	REVISED -
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PLOT DATE = 12/28/2023	DATE - 9/1/23	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENEVA ROAD
CROSS SECTIONS

SCALE: 1"=10'H/5'V SHEET 3 OF 11 SHEETS STA. 293+00.00 R1 TO STA. 293+50.00 R1

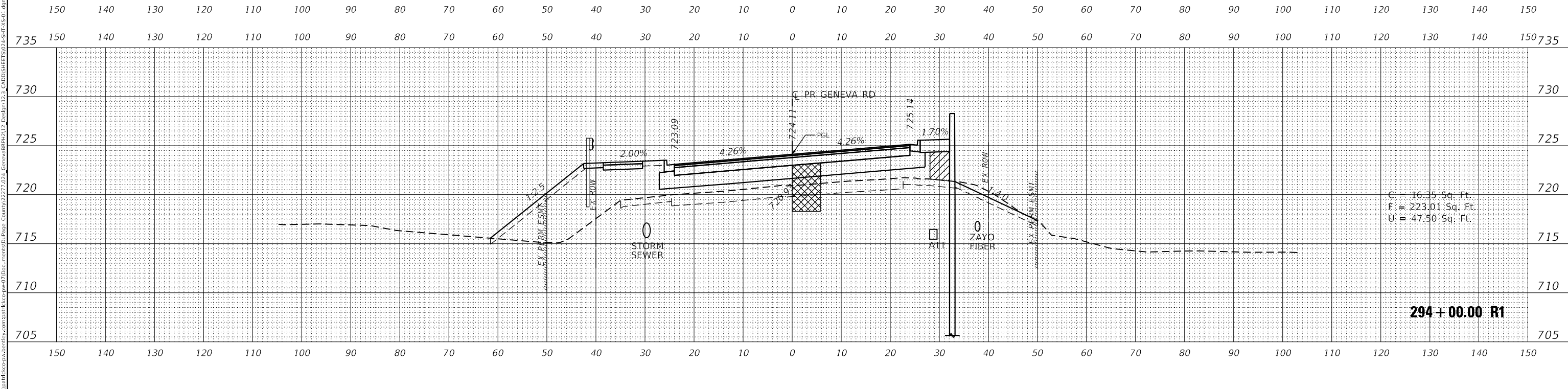
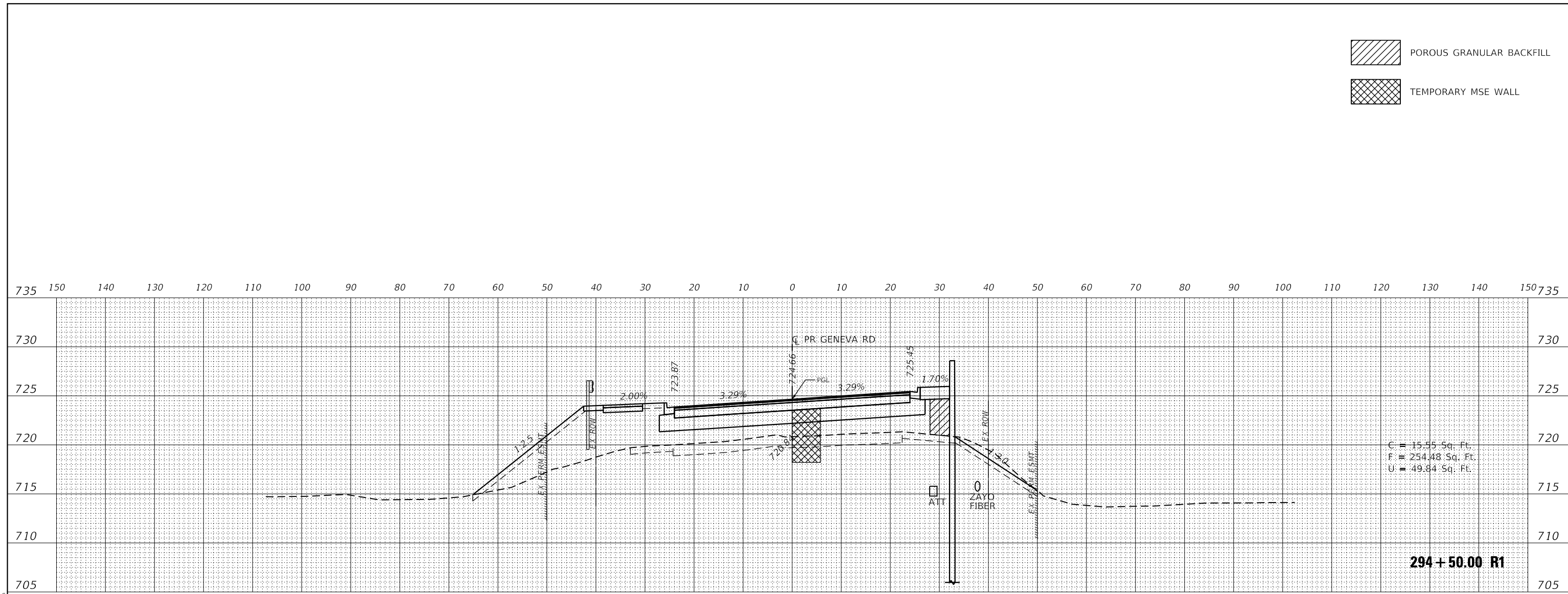
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1397	18-00206-10-BR	DUPAGE	134	119
CONTRACT NO. 61130			ILLINOIS FED. AID PROJECT	

 POROUS GRANULAR BACKFILL
 TEMPORARY MSE WALL

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
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NO.	

DATE	
BY	
SURVEYED	
PLOTTED	
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PLOT DATE = 12/28/2023	DATE - 9/1/23	REVISED -


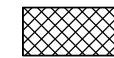
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENEVA ROAD
CROSS SECTIONS

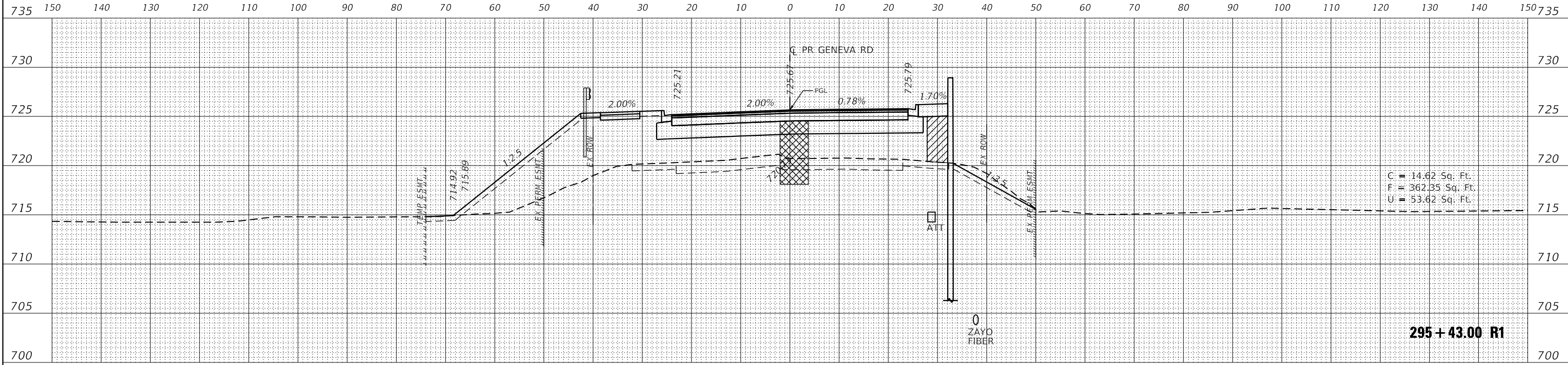
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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			CONTRACT NO. 61130	
		ILLINOIS FED. AID PROJECT		

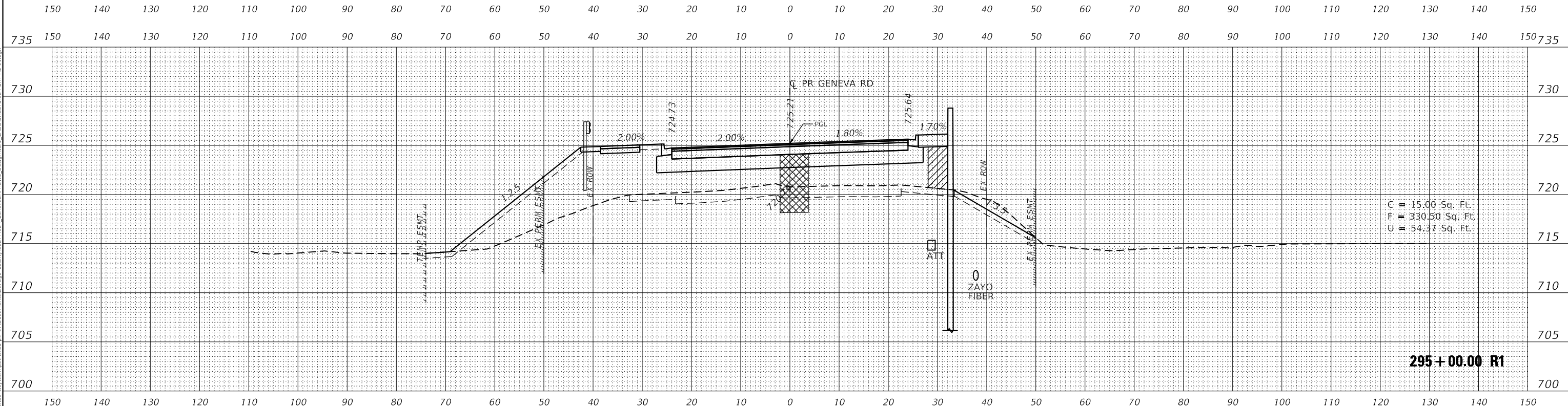
STA 295+42.88 TO STA 297+00.54
BRIDGE AND APPROACH PAVEMENTS
(NO CROSS SECTIONS)

 POROUS GRANULAR BACKFILL
 TEMPORARY MSE WALL

DATE	
BY	
FINAL SURVEY	
SURVEYED	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
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DATE	
BY	
ORIGINAL SURVEY	
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PLOT SCALE = 20,000' / in.	DRAWN - EJB	REVISED -
PLOT DATE = 12/28/2023	CHECKED - LB	REVISED -
	DATE - 9/1/23	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENEVA ROAD
CROSS SECTIONS



SCALE: 1"=10'H/5'V SHEET 5 OF 11 SHEETS STA. 295+00.00 R1 TO STA. 295+43.00 R1

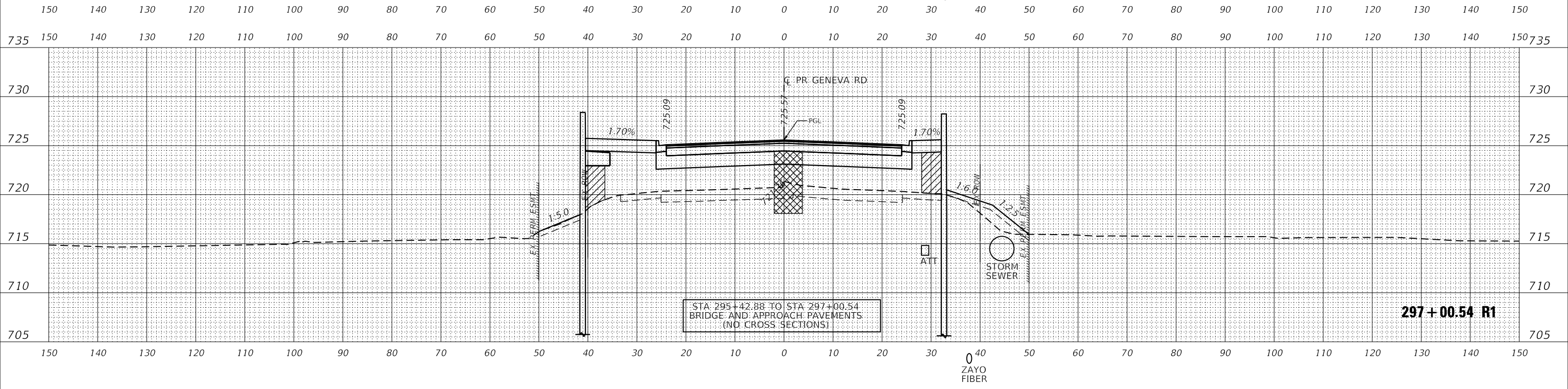
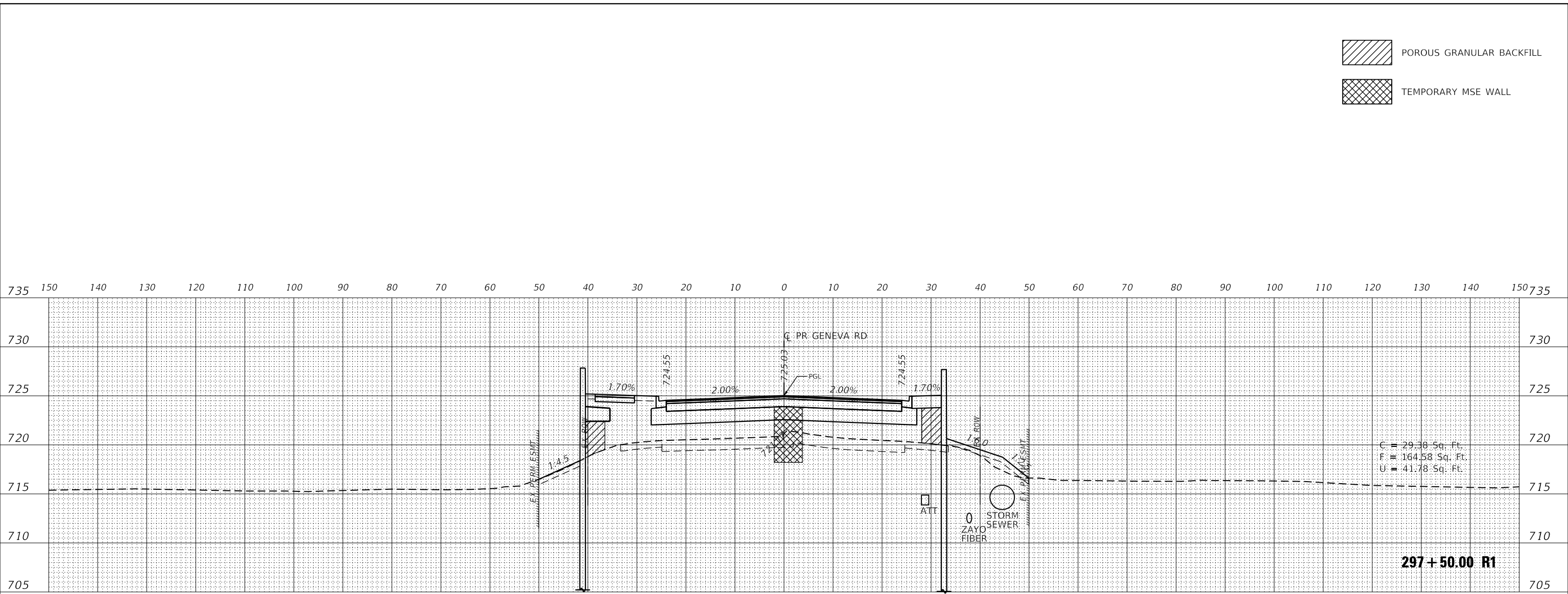
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1397	18-00206-10-BR	DUPAGE	134	121
CONTRACT NO. 61130			ILLINOIS FED. AID PROJECT	


DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS CHECKED	
FINAL SURVEY NO.	
NOTE BOOK NO.	
AREAS CHECKED	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS CHECKED	
ORIGINAL SURVEY NO.	
NOTE BOOK NO.	
AREAS CHECKED	

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 POROUS GRANULAR BACKFILL
 TEMPORARY MSE WALL





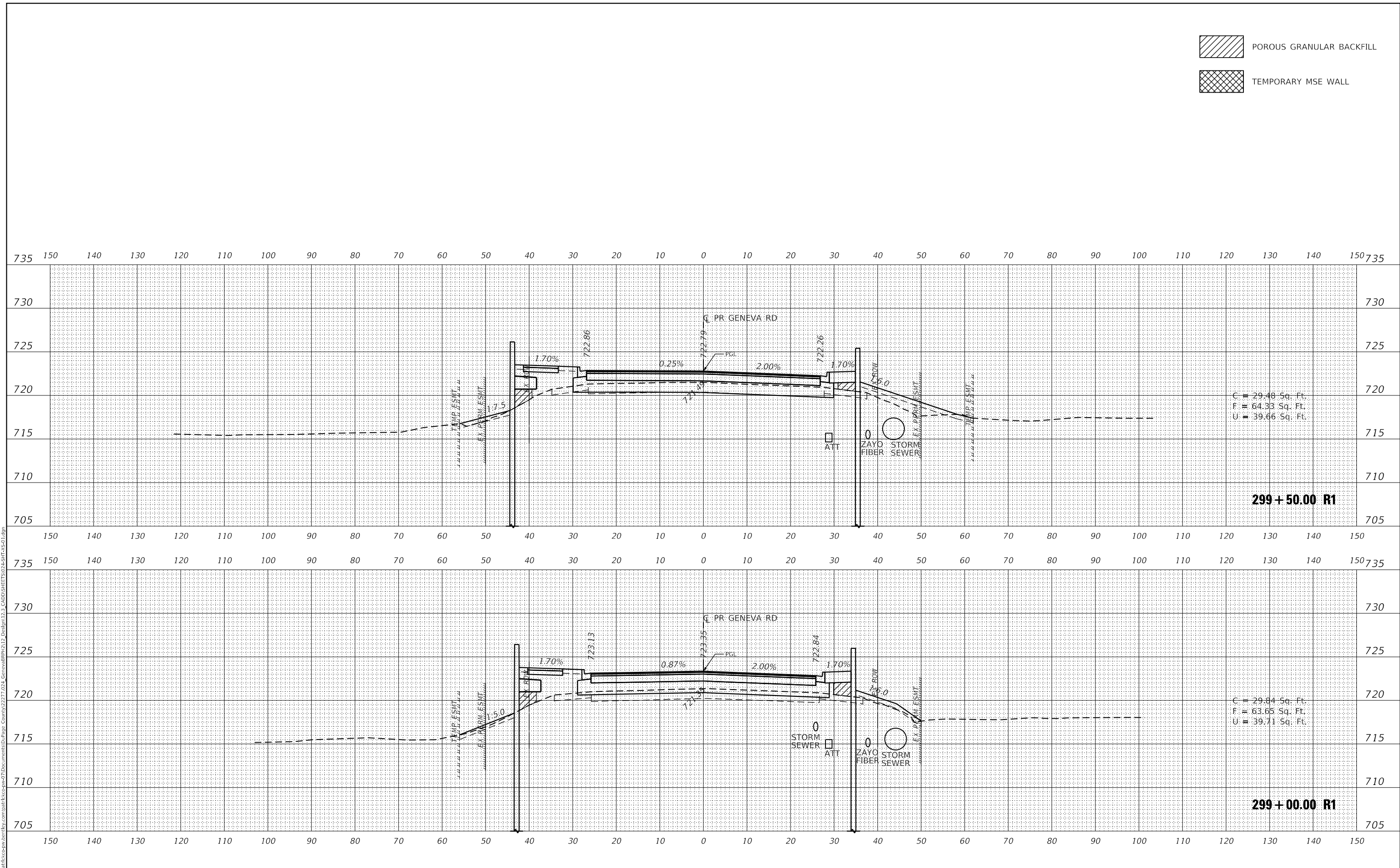
	USER NAME = Ibolzenius	DESIGNED - EJB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENEVA ROAD CROSS SECTIONS	F.A.U. RTE. 1397	SECTION 18-00206-10-BR	COUNTY DUPAGE	TOTAL SHEETS 134	SHEET NO. 122
	PLOT SCALE = 20.0000' / in.	CHECKED - LB	REVISED -			SCALE: 1"=10'H/5'V	SHEET 6 OF 11 SHEETS	STA. 297+00.00 R1 TO STA. 297+50.00 R1	CONTRACT NO. 61J30	
	PLOT DATE = 12/28/2023	DATE - 9/1/23	REVISED -							

FINAL SURVEY NO.	SURVEYED	DATE
NOTE BOOK	PLOTTED	
AREAS CHECKED	TEMPLATE	
	AREAS	
	CREATED	

ORIGINAL SURVEY NO.	SURVEYED	DATE
NOTE BOOK	PLOTTED	
AREAS CHECKED	TEMPLATE	
	AREAS	
	CREATED	

MODEL: EX_GENEVA_2
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 POROUS GRANULAR BACKFILL
 TEMPORARY MSE WALL



C = 29.48 Sq. Ft.
 F = 64.33 Sq. Ft.
 U = 39.66 Sq. Ft.

C = 29.84 Sq. Ft.
 F = 63.65 Sq. Ft.
 U = 39.71 Sq. Ft.

299+50.00 R1

299+00.00 R1



USER NAME = Ibolzenius	DESIGNED - EJB	REVISED -
PLOT SCALE = 20.0000' / in.	DRAWN - EJB	REVISED -
PLOT DATE = 12/28/2023	CHECKED - LB	REVISED -
	DATE - 9/1/23	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

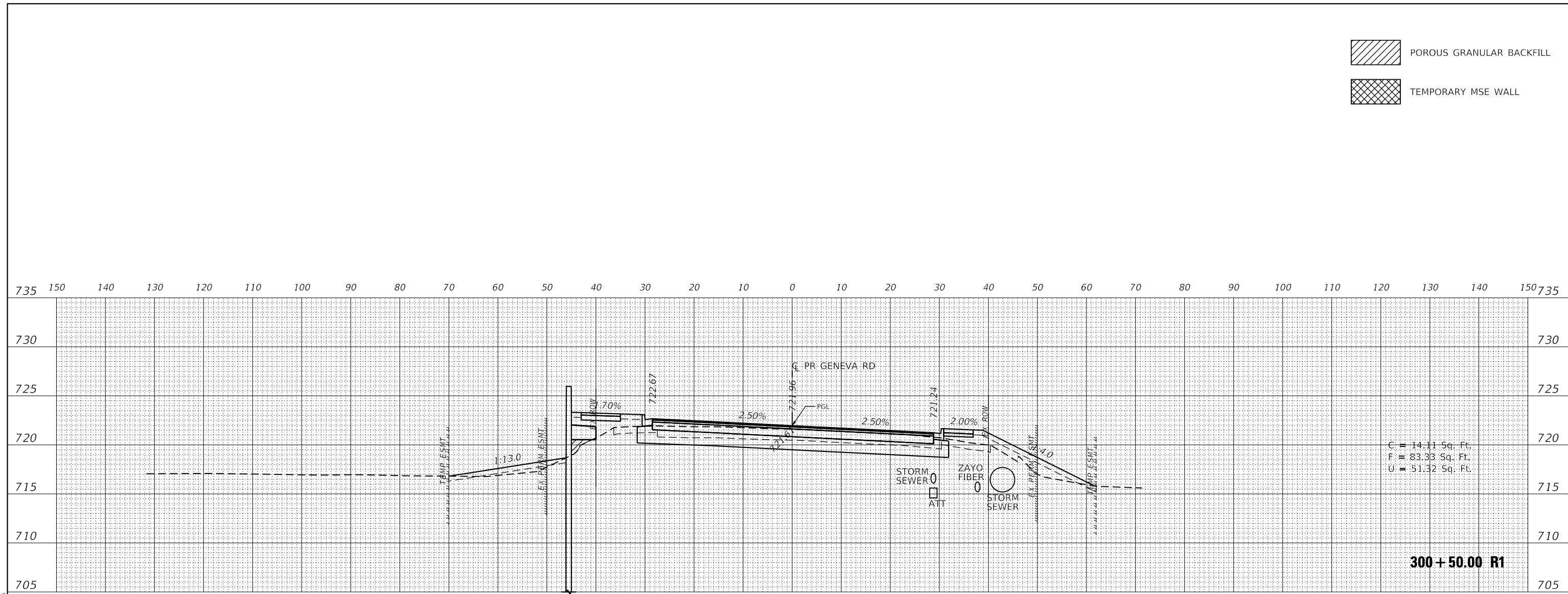
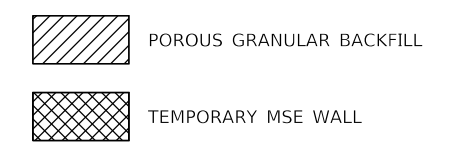
**GENEVA ROAD
CROSS SECTIONS**

SCALE: 1"=10'H/5'V SHEET 8 OF 11 SHEETS STA. 299+00.00 R1 TO STA. 299+50.00 R1

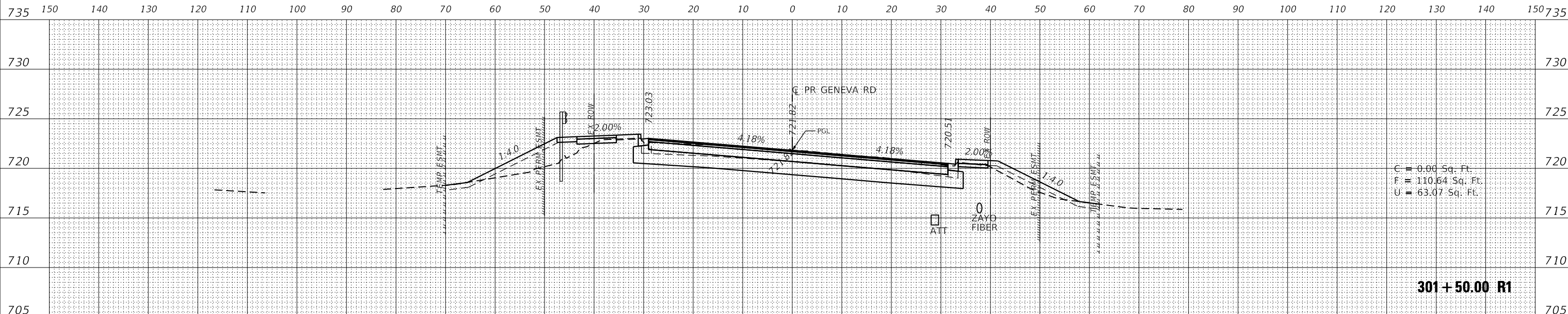
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	18-00206-10-BR	DUPAGE	134	124
			CONTRACT NO. 61J30	
		ILLINOIS FED. AID PROJECT		

FINAL SURVEY NO.	SURVEYED	DATE
NOTE BOOK	PLOTTED	
AREAS CHECKED	TEMPLATE	
	AREAS CHECKED	

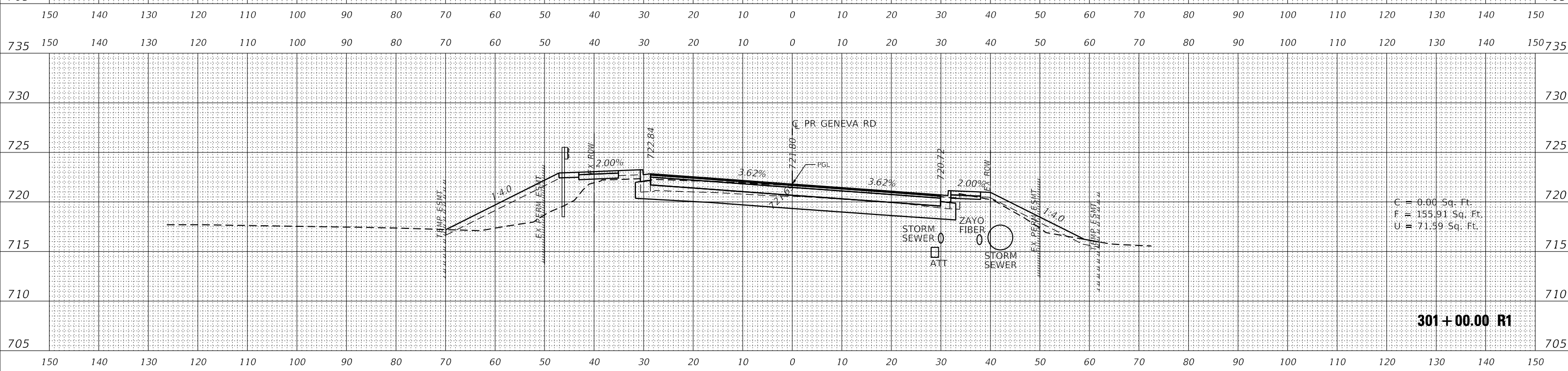
ORIGINAL SURVEY NO.	SURVEYED	DATE
NOTE BOOK	PLOTTED	
AREAS CHECKED	TEMPLATE	
	AREAS CHECKED	



FINAL SURVEY NO.	SURVEYED	DATE
NOTE BOOK	PLOTTED	
AREAS CHECKED	TEMPLATE	
	AREAS	
	CREATED	



ORIGINAL SURVEY NO.	SURVEYED	DATE
NOTE BOOK	PLOTTED	
AREAS CHECKED	TEMPLATE	
	AREAS	
	CREATED	



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USER NAME = Ibolzenius	DESIGNED - EJB	REVISED -
PLOT SCALE = 20.0000' / in.	DRAWN - EJB	REVISED -
PLOT DATE = 12/28/2023	CHECKED - LB	REVISED -
	DATE - 9/1/23	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**GENEVA ROAD
 CROSS SECTIONS**

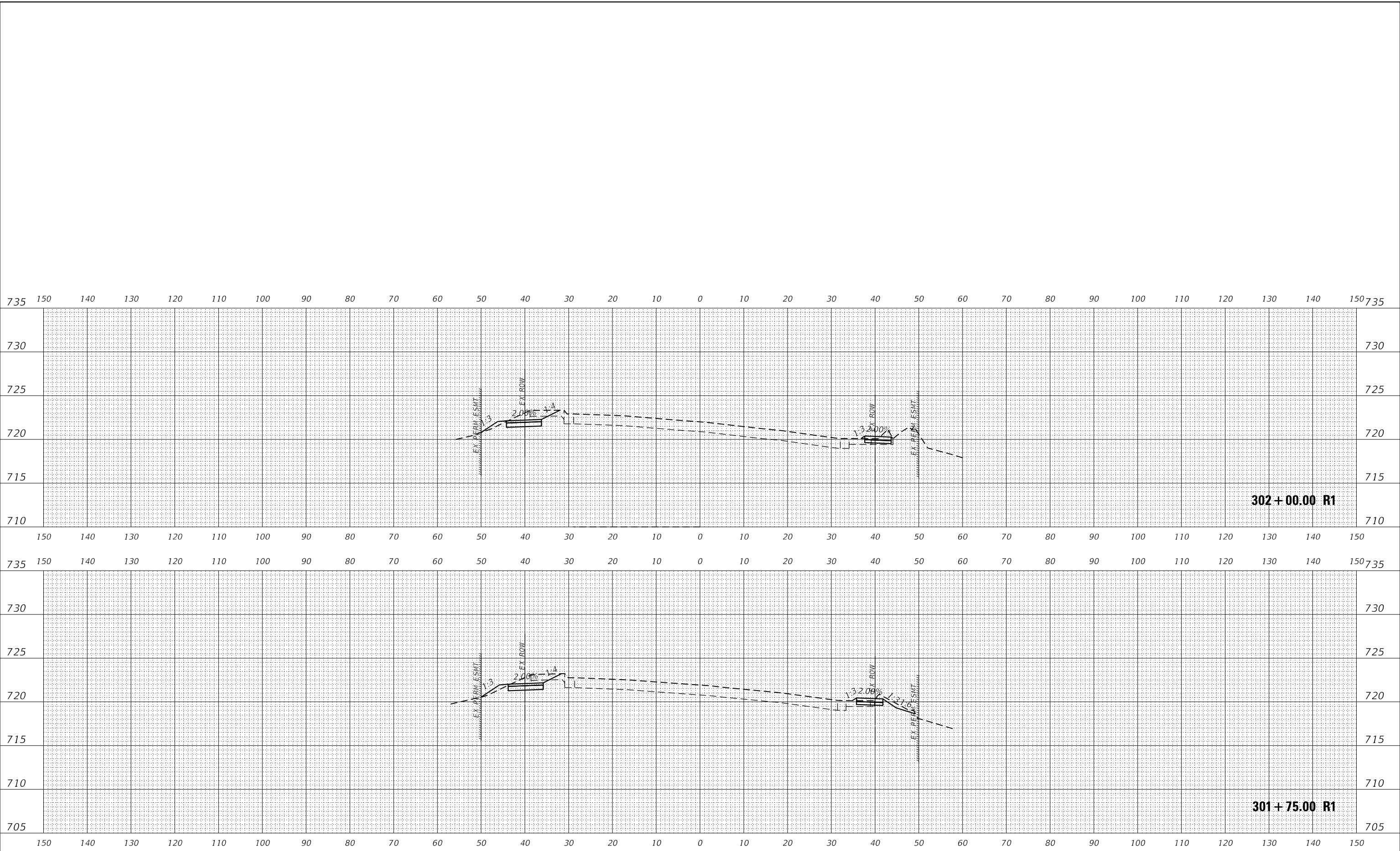
SCALE: 1"=10'H/5'V SHEET 10 OF 11 SHEETS STA. 301+00.00 R1 TO STA. 301+50.00 R1

F.A. U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	18-00206-10-BR	DUPAGE	134	126
			CONTRACT NO. 61J30	
ILLINOIS FED. AID PROJECT				

FINAL SURVEY NO.	SURVEYED	DATE
NOTE BOOK	PLOTTED	
AREAS CHECKED	TEMPLATE	
	AREAS	
	CHECKED	

ORIGINAL SURVEY NO.	SURVEYED	DATE
NOTE BOOK	PLOTTED	
AREAS CHECKED	TEMPLATE	
	AREAS	
	CHECKED	

MODEL: EX_GENEVA_2
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USER NAME = Ibolzenius	DESIGNED - EJB	REVISED -
PLOT SCALE = 20.0000' / in.	DRAWN - EJB	REVISED -
PLOT DATE = 12/28/2023	CHECKED - LB	REVISED -
	DATE - 9/1/23	REVISED -

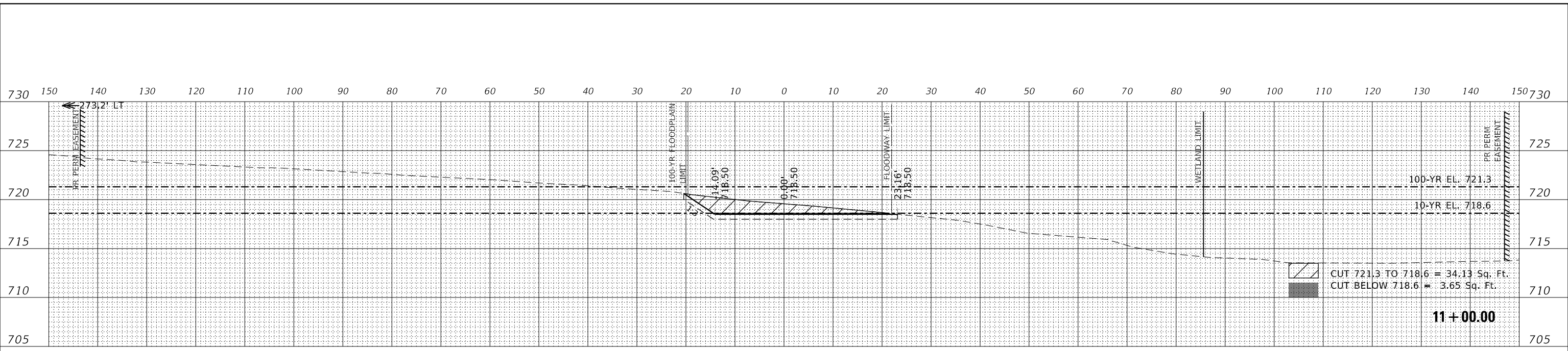
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GENEVA ROAD
CROSS SECTIONS**

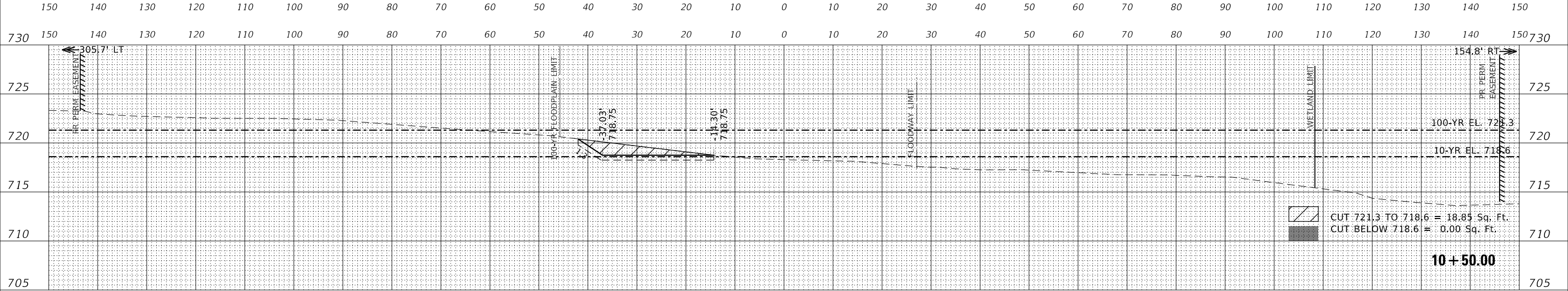
SCALE: 1"=10'H/5'V SHEET 11 OF 11 SHEETS STA. 301+75.00 R1 TO STA. 302+00.00 R1

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	18-00206-10-BR	DUPAGE	134	127
CONTRACT NO. 61130				
ILLINOIS		FED. AID PROJECT		

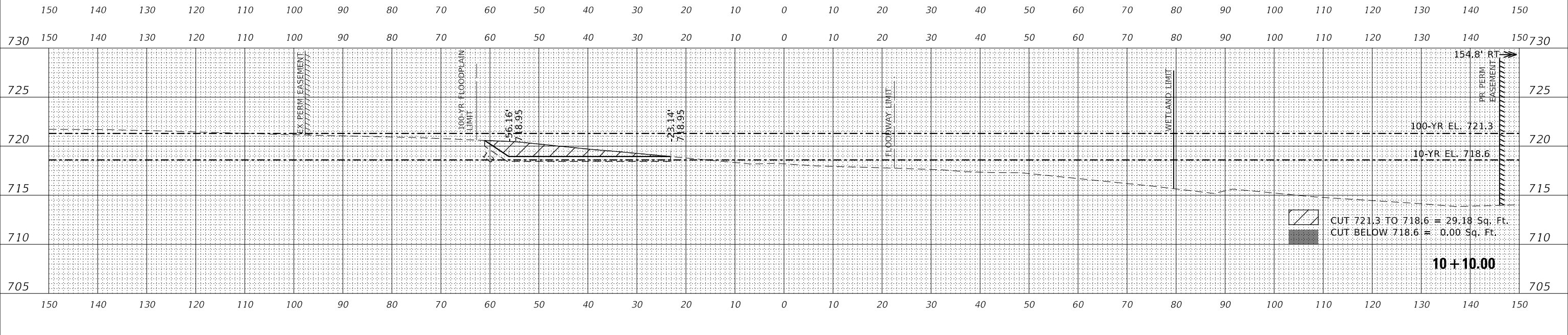
DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED



DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED



DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED



MODEL: P_DITCHMORT1
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USER NAME = sahmcd	DESIGNED - SA/JIF	REVISED -
	DRAWN - SA	REVISED -
PLOT SCALE = 20,0000' / in.	CHECKED - JIF	REVISED -
PLOT DATE = 2/2/2024	DATE - 02/01/2024	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GENEVA ROAD
COMPENSATORY STORAGE CROSS SECTIONS**

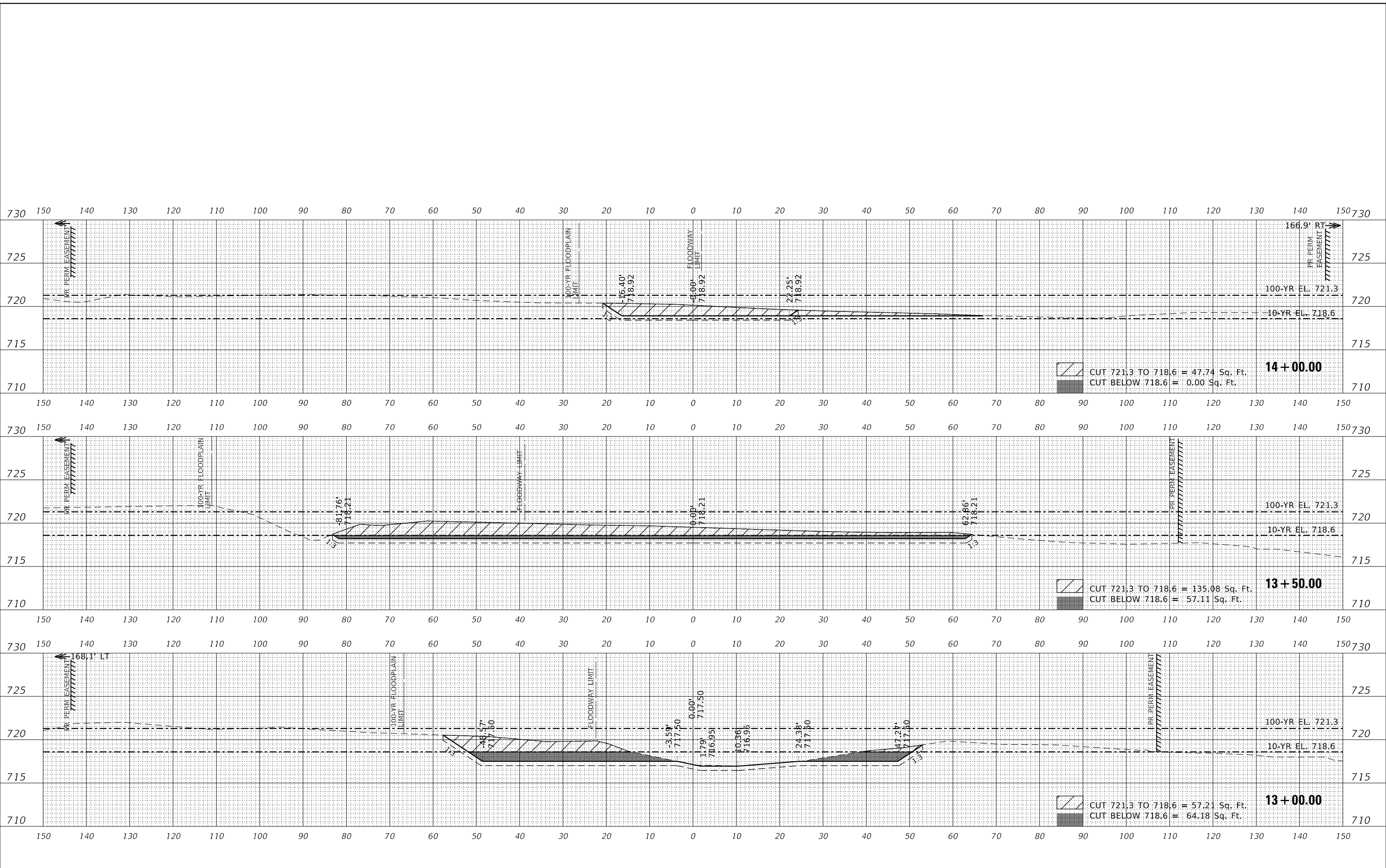
SCALE: 1"=10'H/5'V SHEET 1 OF 7 SHEETS STA. 10+10.00 TO STA. 11+00.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	18-00206-10-BR	DUPAGE	134	128
CONTRACT NO. 61J30			ILLINOIS FED. AID PROJECT	

FINAL SURVEY NO.	SHRIMPED PLOTTED TEMPLATE AREAS CHECKED
BY	DATE

ORIGINAL SURVEY NO.	SHRIMPED PLOTTED TEMPLATE AREAS CHECKED
BY	DATE

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USER NAME =	sahmed
DESIGNED -	SA/JIF
DRAWN -	SA
REVISOR -	JIF
DATE -	02/01/2024

REVISIONS	NO.	DESCRIPTION

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**GENEVA ROAD
 COMPENSATORY STORAGE CROSS SECTIONS**

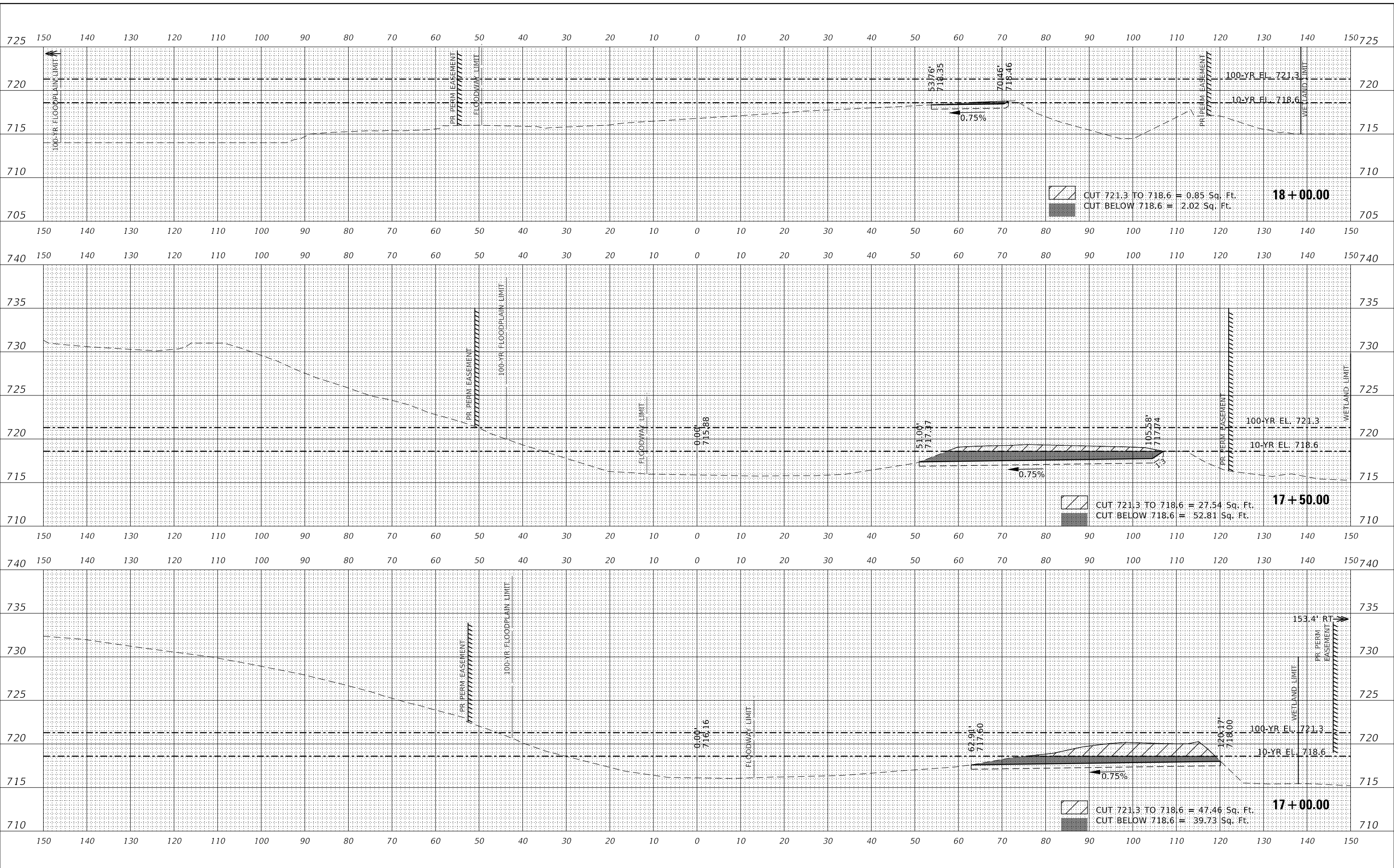
SCALE: 1"=10'H/5'V SHEET 3 OF 7 SHEETS STA. 13+00.00 TO STA. 14+00.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	18-00206-10-BR	DUPAGE	134	130
CONTRACT NO. 61J30			ILLINOIS FED. AID PROJECT	

FINAL SURVEY	NO.
SUBMITTED	DATE
FLIPPED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	

ORIGINAL SURVEY	NO.
SUBMITTED	DATE
FLIPPED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	

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USER NAME =	sahmed	DESIGNED -	SA/JIF	REVISED -	
		DRAWN -	SA	REVISED -	
PLOT SCALE =	20.0000' / in.	CHECKED -	JIF	REVISED -	
PLOT DATE =	2/2/2024	DATE -	02/01/2024	REVISED -	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GENEVA ROAD
COMPENSATORY STORAGE CROSS SECTIONS**

SCALE: 1"=10'H/5'V SHEET 6 OF 7 SHEETS STA. 17+00.00 TO STA. 18+00.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	18-00206-10-BR	DUPAGE	134	133
CONTRACT NO. 61J30			ILLINOIS FED. AID PROJECT	

