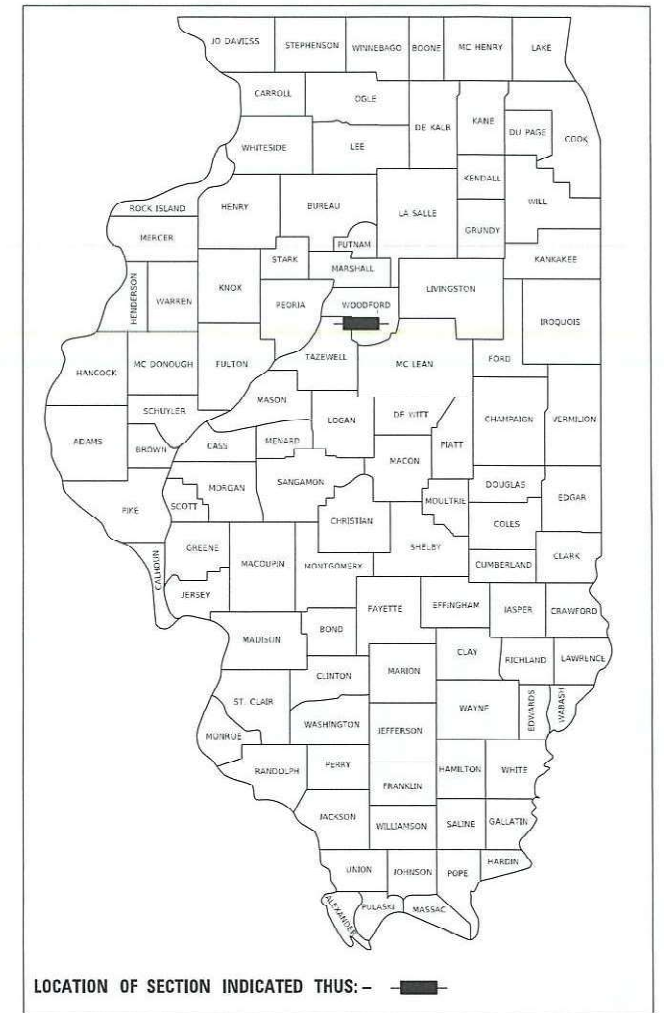


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(18-2VB)BRR	WOODFORD	39	1
		ILLINOIS	CONTRACT NO. 68H23	

D-94-044-22



INDEX OF SHEETS

1. COVER SHEET
2. GENERAL NOTES AND PROJECT SPECIFIC NOTES
- 3-6. SUMMARY OF QUANTITIES
- 7-8. SCHEDULE OF QUANTITIES
9. REMOVAL PLAN
10. PROPOSED PLAN
- 11-14. STAGING DETAILS
- 15-31. STRUCTURE DETAILS S.N. 102-0046
32. PAVEMENT MARKING PLAN
- 33-39. DISTRICT STANDARDS

**PROPOSED
HIGHWAY PLANS**

FAP 317 (US ROUTE 24)
SECTION (18-2VB)BRR
PROJECT NHPP-G983(664)
BRIDGE PRESERVATION
WOODFORD COUNTY

C-94-062-22
R2W

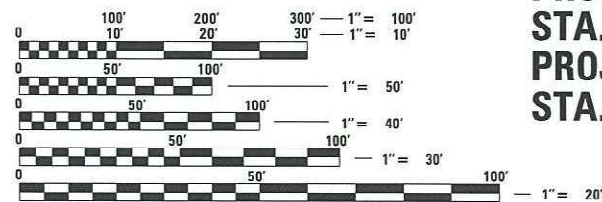
HIGHWAY STANDARDS

000001-08	701006-05
001001-02	701201-05
420001-10	701301-04
420401-13	701311-03
601101-02	701321-18
630001-13	701901-09
631031-18	704001-08
631033-09	780001-05
701001-02	782006-01

ADT=5000 (2021)
SU = 4.0%
MU = 4.0%

US 24 OVER TP&W
RR (S.N. 102-0046)

PROJECT BEGINS
STA. 452 + 10.02
PROJECT ENDS
STA. 456 + 96.43

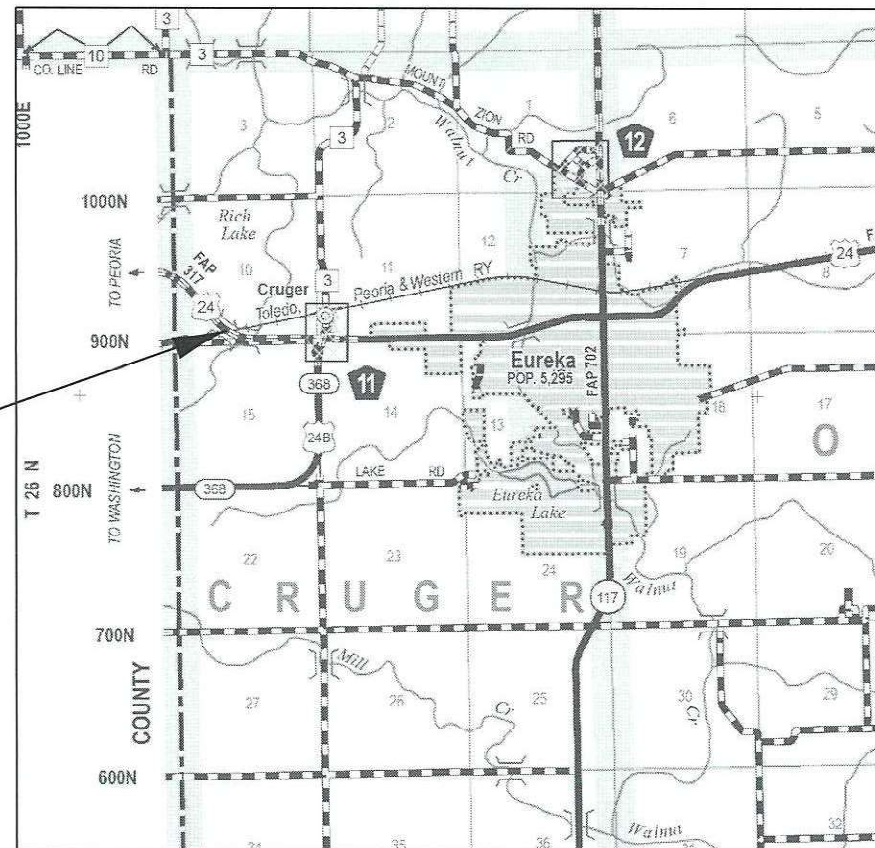


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS
1-800-892-0123
OR 811

PROJECT MANAGER: JACOB WECK (309)671-3473

CONTRACT NO. 68H23



GROSS LENGTH = 486 FT. = 0.09 MILE
NET LENGTH = 486 FT. = 0.09 MILE

PROJECT DESCRIPTION:

WORK INCLUDES SCARIFICATION AND MICROSILICA OVERLAY ON BRIDGE DECK, APPROACH PAVEMENTS AND CONNECTOR PAVEMENTS; JOINT REPLACEMENTS; RIP RAP SWALES; AND REPLACEMENT OF EXISTING GUARDRAIL

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED December 15, 2023
Kessal A. Desmettonis
REGIONAL ENGINEER

February 2, 2024
Scott A. Etk
ENGINEER OF DESIGN AND ENVIRONMENT

February 2, 2024
Steph M. Smith
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

COMMITMENTS

Commitments are not to be altered without the written approval of all parties to which the commitment was made.

No commitments have been made on this contract.

ENVIRONMENTAL REVIEWS

Prior to the use of any proposed borrow areas, use areas (temporary access roads, detours, run-arounds, etc.) and/or waste areas, the Contractor shall file the required environmental resource request surveys according to Section 107.22 of the Standard Specifications. These surveys are required in order for the Department to conduct cultural and biological resource surveys for the proposed site.

The required environmental resource documentation shall include the following:

- * BDE Form 2289 (Borrow Site Review)
- * BDE Form 2290 (Waste/Use Area Review)
- * A location map showing the size limits and location of the use area
- * Color photographs depicting the use area
- * Borrow Area Entry Agreement form – D4 PI0101

Prior to any waste materials being removed from the construction site the required environmental resource surveys shall be obtained and filed by the Contractor. Excess waste products removed from the construction site shall be disposed of as required in Section 202.03 of the Standard Specifications.

Any protruding metal bars shall be removed prior to the disposal of broken concrete at approved disposal sites.

Please note that a minimum of four weeks shall be allowed for the District to obtain the required waste site environmental clearances and six weeks for the required borrow site environmental clearances.

BRIDGE OVERLAY NOTIFICATION

After placement of the bridge deck overlay, the Resident Engineer shall notify the District Bridge Maintenance Engineer of the "as constructed" milling depth and overlay thickness for updating the Illinois Highway Information System.

POLYMERIZED BITUMINOUS MATERIALS (TACK COAT) RATES

Surface Type	Residual Rate
Milled (HMA or PCC)	0.08 lb /sq ft
Existing Pavement	0.08 lb /sq ft
Fog Coat (between lifts)	0.08 lb /sq ft

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

Locations	Mainline & Shoulders	Shoulders
Mixture Use(s):	Surface 2"	Partial Depth Patching
AC/PC:	PG 58-28	PG 58-28
Design Air Voids:	4.0% @ N = 50	4.0% @N = 50
Mixture Composition: (Gradation Mixture)	IL 9.5	IL 9.5
Friction Aggregate:	Mix C	Mix D
Quality Management:	QCQA	QCQA
MTD (YES OR NO):	No	No

Notes:

- 1) Individual lift thickness of each mix type will be no less than 3 times nominal maximum aggregate size and no more than 6 times nominal maximum aggregate size, unless otherwise approved by the Engineer.
- 2) For design purposes, mixture weight for all mixes is determined to be 112.0 lb/s.y/in., unless otherwise noted.
- 3) Sublot size for PFP and QCP mixes will be 600 tons, unless otherwise agreed to by the Engineer and the paving contractor.

BUTT JOINT CUTTING TIME RESTRICTION

Butt joints shall not be milled more than three (3) days prior to placement of the HMA surface course.

PAVING SURFACE COURSE

Continuous paving operations on the main roadway shall be maintained at all times during the construction of the hot-mix asphalt surface. No interruptions for side roads, entrances, turn lanes, etc. will be allowed.

SECURING DRAINAGE STRUCTURE GRATES

Prior to routing traffic onto the shoulders as shown in the staging plans, the Contractor shall secure gratings on shoulder inlets as directed by the Engineer. This work will not be paid for separately, but shall be included in the cost of the traffic control pay item.

PROJECT SPECIFIC NOTES

THE FULL 8" THICKNESS OF GUARDRAIL AGGREGATE EROSION CONTROL SHALL BE INSTALLED ALONG THE LEFT SIDE OF US 24 AT THE LOCATIONS NOTED IN THE PLANS, ALONG THE RIGHT SIDE OF US 24, A VARIABLE THICKNESS SHALL BE INSTALLED ON TOP OF EXISTING AGGREGATE AT THE LOCATIONS NOTED IN THE PLANS TO ELIMINATE ANY DROF-OFF AT THE EDGE OF THE SHOULDER.

THE CONTRACTOR SHALL ADJUST THE DEPTH OF PROPOSED RIPRAP, AS NECESSARY, TO AVOID ANY UTILITY CONFLICTS.

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PLOT DATE = 12/15/2023	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES AND PROJECT
SPECIFIC NOTES**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(18-2VB)BRR	WOODFORD	39	2
CONTRACT NO. 68H23				
		ILLINOIS	FED. AID PROJECT	

CONST. CODE
80% FED 20% STATE
BRIDGE
0047

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	SN 102-0046
20200100	EARTH EXCAVATION	CU YD	31	31
28100225	STONE RIPRAP, CLASS B3	TON	102	102
40600295	POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)	POUND	704	704
40600982	HOT - MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	489	489
40600990	TEMPORARY RAMP	SQ YD	489	489
40604050	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "C", N50	TON	48	48
42000060	WELDED WIRE REINFORCEMENT	SQ YD	243	243
42000080	PAVEMENT CONNECTOR (PCC) FOR BRIDGE APPROACH SLAB	SQ YD	448	448
44000100	PAVEMENT REMOVAL	SQ YD	449	449
44000300	CURB REMOVAL	FOOT	134	134
44213208	TIE BARS 1 1/4"	EACH	95	95
50102400	CONCRETE REMOVAL	CU YD	21	21
50105220	PIPE CULVERT REMOVAL	FOOT	130	130
50300255	CONCRETE SUPERSTRUCTURE	CU YD	23.2	23.2

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PLOT DATE = 12/15/2023	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: 1"=100' SHEET 1 OF 4 SHEETS STA. N/A TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(18-2V8)BRR	WOODFORD	39	3
			CONTRACT NO. 68H23	
		ILLINOIS	FED. AID PROJECT	

CONST. CODE
80% FED 20% STATE
BRIDGE
0047

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	SN 102-0046
50300260	BRIDGE DECK GROOVING	SQ YD	1386	1386
50300300	PROTECTIVE COAT	SQ YD	1900	1900
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	3650	3650
50800515	BAR SPLICERS	EACH	32	32
52000050	PREFORMED JOINT SEAL 4	FOOT	133	133
52000110	PREFORMED JOINT STRIP SEAL	FOOT	137	137
60100060	CONCRETE HEADWALL FOR PIPE DRAINS	EACH	1	1
60600605	CONCRETE CURB, TYPE B	FOOT	35	35
* 63000001	STEEL PLATE BEAM GUARD RAIL, TYPE A, 6 FOOT POSTS	FOOT	337.5	337.5
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	2	2
* 63100089	TRAFFIC BARRIER TERMINAL, TYPE 6B	EACH	2	2
63200310	GUARDRAIL REMOVAL	FOOT	515	515
66201120	CONCRETE SHOULDER CURB	FOOT	21	21
67100100	MOBILIZATION	L SUM	1	1

*= SPECIALTY ITEM

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: 1"=100' SHEET 2 OF 4 SHEETS STA. N/A TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(18-2V8)BRR	WOODFORD	39	4
			CONTRACT NO. 68H23	
			ILLINOIS FED. AID PROJECT	

CONST. CODE
80% FED 20% STATE
BRIDGE
0047

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	SN 102-0046
70100405	TRAFFIC CONTROL AND PROTECTION, STANDARD 701321	EACH	1	1
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	4	4
70106700	TEMPORARY RUMBLE STRIP	EACH	6	6
70107005	PAVEMENT MARKING BLACKOUT TAPE, 5"	FOOT	1802	1802
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	752	752
70400100	TEMPORARY CONCRETE BARRIER	FOOT	613	613
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	613	613
70600250	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2	2
70600350	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2	2
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	1946	1946
* 78200011	BARRIER WALL REFLECTORS, TYPE C	EACH	6	6
X0326444	SURFACE FILLER (SPECIAL)	GALLON	0.4	0.4
X4421000	PARTIAL DEPTH PATCHING	TON	60	60

*= SPECIALTY ITEM

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: 1"=100' SHEET 3 OF 4 SHEETS STA. N/A TO STA. N/A

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(18-2VB)BRR	WOODFORD	39	5
				CONTRACT NO. 68H23
ILLINOIS FED. AID PROJECT				

CONST. CODE
80% FED 20% STATE
BRIDGE
0047

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	SN 102-0046
X4421002	PARTIAL DEPTH PATCHING (SPECIAL)	SQ YD	353	353
X5030550	PROTECTIVE COAT (SPECIAL)	SQ YD	342	342
X6350204	LINEAR DELINEATOR PANELS, 4 INCH	EACH	4	4
Z0001002	GUARDRAIL AGGREGATE EROSION CONTROL	TON	63	63
Z0012130	BRIDGE DECK SCARIFICATION 3/4"	SQ YD	1386	1386
Z0012162	BRIDGE DECK MICROSILICA CONCRETE OVERLAY 2 1/4"	SQ YD	1386	1386
70013798	CONSTRUCTION LAYOUT	L SUM	1	1
Z0015802	PLUG EXISTING DECK DRAINS	EACH	1	1
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1
Z0023200	FILLING DRAINAGE STRUCTURES	EACH	2	2
Z0065730	SLOPE WALL SLURRY PUMPING	CU YD	7	7
Ø Z0076600	TRAINEES	HOUR	1,000	1,000
Ø Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOUR	1,000	1,000

Ø 0042

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PLOT DATE = 12/15/2023	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: 1"=100' SHEET 4 OF 4 SHEETS STA. N/A TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(18-2V8)BRR	WOODFORD	39	6
CONTRACT NO. 68H23				
ILLINOIS FED. AID PROJECT				

MAINLINE SCHEDULE							
LOCATION				POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)	HOT-MIX ASPHALT SURFACE REMOVAL-BUTT JOINT	TEMPORARY RAMP	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "C", N50
				40600295	40600982	40600990	40604050
STA.	TO	STA.	SIDE	POUND	SQ YD	SQ YD	TON
452+10.02		452+60.02	LT	352	244	244.4	27
456+46.43		456+96.43	LT	352	244	244.4	21
TOTALS				704	489	488.9	48

CONCRETE HEADWALL FOR PIPE DRAINS		
LOCATION		QUANTITY
		60100060
STA.	SIDE	EACH
456+30.37	LT	1
TOTALS		1

PATCHING SCHEDULE					
LOCATION				PARTIAL DEPTH PATCHING (SPECIAL)	PARTIAL DEPTH PATCHING
				X4421002	X4421000
STA.	TO	STA.	SIDE	SQ YD	TON
450+40.97	TO	452+60.02	LT/RT	219	37
456+46.43	TO	457+80.47	LT/RT	134	23
TOTALS				353	60

DRAINAGE SCHEDULE		
LOCATION		PIPE CULVERT REMOVAL
		50105220
STA.	SIDE	FOOT
453+43	LT	65
456+08	LT	65
TOTALS		130

MOBILIZATION	
LOCATION	QUANTITY
	67100100
	L SUM
JOBSITE	1
TOTALS	1

TRAFFIC CONTROL AND PROTECTION, STANDARD 701321	
LOCATION	QUANTITY
	70100405
	EACH
JOBSITE	1
TOTALS	1

EROSION CONTROL SCHEDULE						
LOCATION				STONE RIPRAP, CLASS B3	GUARDRAIL AGG. EROSION CONTROL	EARTH EXCAVATION
				28100225	Z0001002	20200100
STA.	TO	STA.	SIDE	TON	TON	CU YD
453+14.62	TO	453+34.62	LT	48.2		
456+27.29	TO	456+33.29	LT	4.5		
456+75.82	TO	456+95.82	LT	49.7		
451+99.27	TO	452+97.53	RT		7	3.4
452+06.32	TO	453+44.79	LT		30	14.8
455+53.19	TO	457+00.88	RT		5	2.5
456+05.70	TO	457+06.25	LT		20	9.9
TOTALS				102.4	63	31

TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	
LOCATION	QUANTITY
	70100450
	L SUM
JOBSITE	1
TOTALS	1

TEMPORARY BRIDGE TRAFFIC SIGNALS		
LOCATION		QUANTITY
		70106500
STA.	SIDE	EACH
450+15.97	RT	1
450+40.97	LT	1
458+65.47	RT	1
458+90.47	LT	1
TOTALS		4

GUARDRAIL SCHEDULE									
LOCATION				CURB REMOVAL	CONCRETE CURB, TYPE B	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FT POSTS	TRAFFIC BARRIER TERMINAL, TYPE 6	TRAFFIC BARRIER TERMINAL, TYPE 6B	GUARDRAIL REMOVAL
				44000300	60600605	63000001	63100085	63100089	63200310
STA.	TO	STA.	SIDE	FOOT	FOOT	FOOT	EACH	EACH	FOOT
456+46.43	TO	456+95.82	LT	50					
456+46.43	TO	456+80.89	LT		35				
451+99.27	TO	452+61.06	RT			62.5			63
452+61.06	TO	453+10.49	RT					1	50
452+06.32	TO	453+07.48	LT			100			100
453+07.48	TO	453+45.42	LT				1		38
455+40.24	TO	455+89.67	RT					1	50
455+89.67	TO	457+00.88	RT			112.5	1		113
456+05.08	TO	456+43.02	LT						38
456+43.02	TO	457+06.25	LT			62.5			63
TOTALS				50	35	337.5	2	2	515

TEMPORARY RUMBLE STRIPS	
LOCATION	QUANTITY
	70106700
	EACH
JOB SITE	6
TOTALS	6

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: 1"=100' SHEET 1 OF 2 SHEETS STA. N/A TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(18-2V8)BRR	WOODFORD	39	7
				CONTRACT NO. 68H23
		ILLINOIS	FED. AID PROJECT	

BARRIER WALL REFLECTORS, TYPE C		
LOCATION		QUANTITY
		78200011
STA. TO STA.	SIDE	EACH
453+10.49 TO 455+40.24	RT	3
453+45.42 TO 456+05.08	LT	3
TOTALS		6

CONSTRUCTON LAYOUT		
LOCATION		QUANTITY
		Z0013798
STA.	SIDE	L SUM
JOBSITE		1
TOTALS		1

LINEAR DELINEATOR PANEL, 4 INCH		
LOCATION		QUANTITY
		X6350204
STA. TO STA.	SIDE	EACH
451+99.27 TO 453+10.49	RT	1
452+06.32 TO 453+44.79	LT	1
455+40.24 TO 457+00.88	RT	1
456+05.08 TO 457+06.25	LT	1
TOTALS		4

SHORT TERM PAVEMENT MARKING						
LOCATION				70107005	70300150	
				PAVEMENT MARKING BLACKOUT TAPE, 5"	SHORT TERM PAVEMENT MARKING REMOVAL	
STAGE 1				SOLID	DOUBLE YELLOW	SOLID
STA.	TO	STA.	SIDE	FOOT	FOOT	SQ FT
449+65.97	TO	459+40.47	CENTER		894	
451+30.47	TO	457+75.97	RT	650		271
STAGE 2						
451+30.47	TO	457+75.97	LT	258		108
449+65.97	TO	459+40.47	CENTER			373
SUB-TOTALS				908	894	752
TOTALS				1802		752

PAVEMENT MARKINGS					
LOCATION			78009004		
			MODIFIED URETHANE PAVEMENT MARKING - LINE 4"		
			SOLID WHITE	DOUBLE YELLOW	
			FOOT	FOOT	
STA.	TO	STA.	973	973	
452+10.02	TO	456+96.43			
SUB-TOTALS			973	973	
TOTAL			1946		

TEMPORARY CONCRETE BARRIER SCHEDULE					
LOCATION				TEMPORARY CONCRETE BARRIER	RELOCATE TEMPORARY CONCRETE BARRIER
				70400100	70400200
STAGE 1				FOOT	FOOT
STA.	TO	STA.	LT/RT		
451+51.22	TO	457+55.22	LT/RT	613	
STAGE 2					
451+51.22	TO	457+55.22	LT/RT		613
TOTAL				613	613

TEMPORARY IMPACT ATTENUATOR SCHEDULE					
LOCATION			IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	
STATION	OFFSET (FT)	LT/RT	70600250	70600350	
STAGE 1			EACH	EACH	
451+51.22	4.00	LT	1		
457+55.22	4.00	LT	1		
STAGE 2					
451+51.22	4.00	RT			1
457+55.22	4.00	RT			1
TOTAL			2		2

RAILROAD PROTECTIVE LIABILITY INSURANCE	
LOCATION	QUANTITY
	Z0048665
	L SUM
JOB SITE	1
TOTALS	1

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

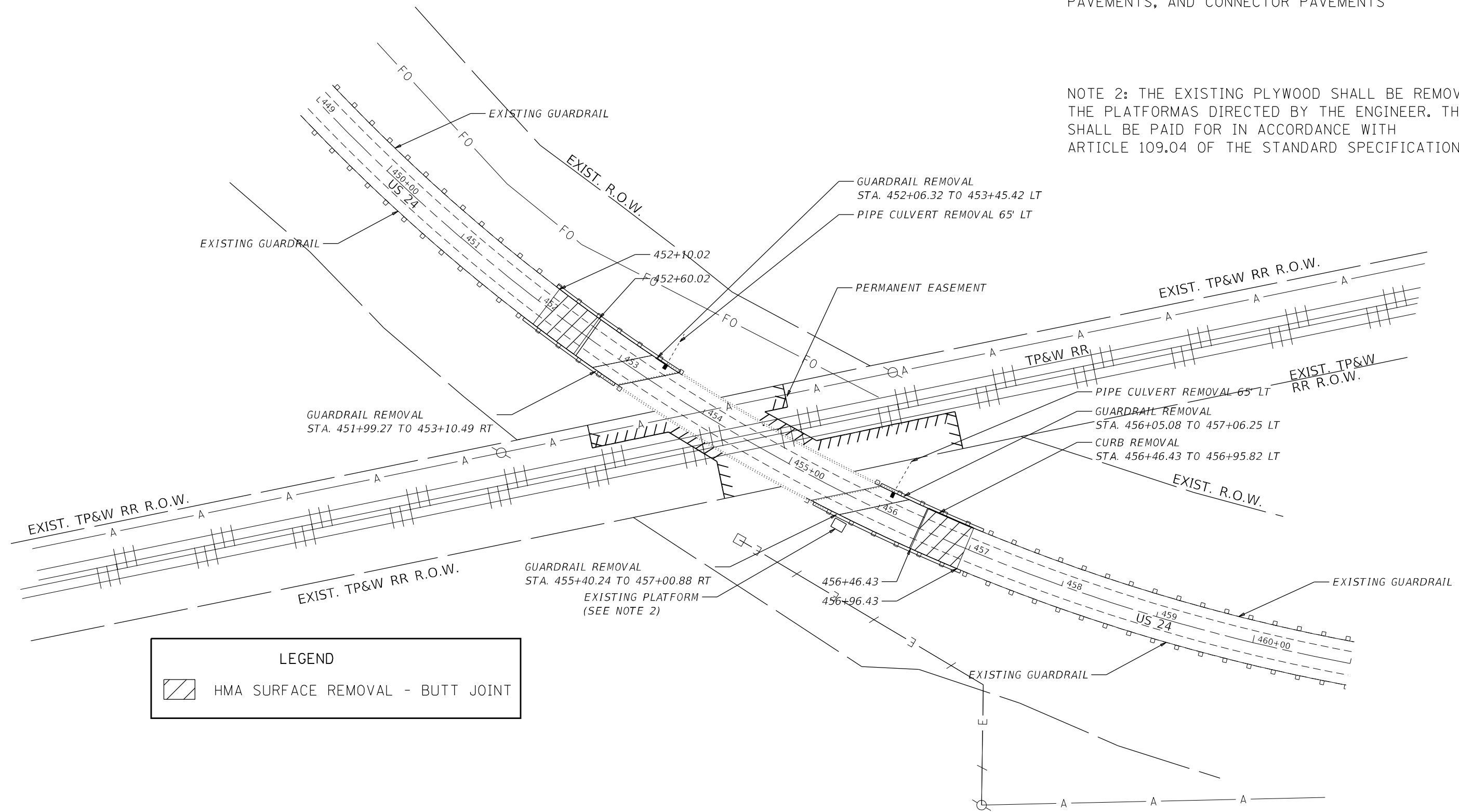
SCHEDULE OF QUANTITIES			
SCALE: 1"=100'	SHEET 2	OF 2 SHEETS	STA. N/A TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(18-2VB)BRR	WOODFORD	39	8
			CONTRACT NO. 68H23	
ILLINOIS FED. AID PROJECT				




NOTE 1: SEE STRUCTURE DETAILS FOR REMOVAL ITEMS ON THE STRUCTURE, BRIDGE APPROACH PAVEMENTS, AND CONNECTOR PAVEMENTS

NOTE 2: THE EXISTING PLYWOOD SHALL BE REMOVED FROM THE PLATFORMS DIRECTED BY THE ENGINEER. THE WORK SHALL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS



LEGEND

 HMA SURFACE REMOVAL - BUTT JOINT



MODEL: Print sheets
FILE NAME: \\lms01\ou\DOT\ID\Share\PD\GEN\W\W\ORD\ST\US 24 - Eureka - Bridge\CADD\US 24 - Sheets.dgn

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	DRAWN -	REVISED -
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PLOT DATE = 12/15/2023	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

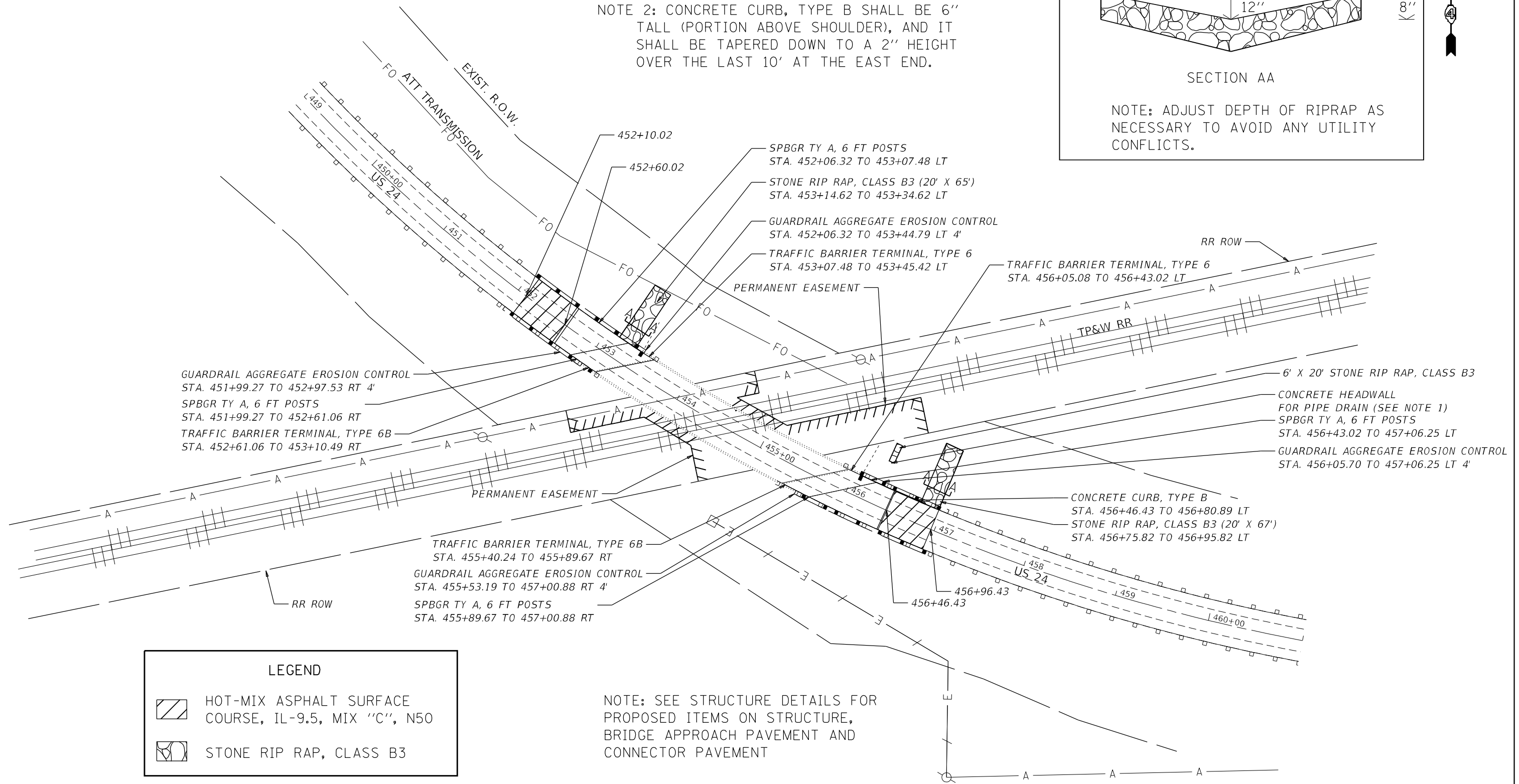
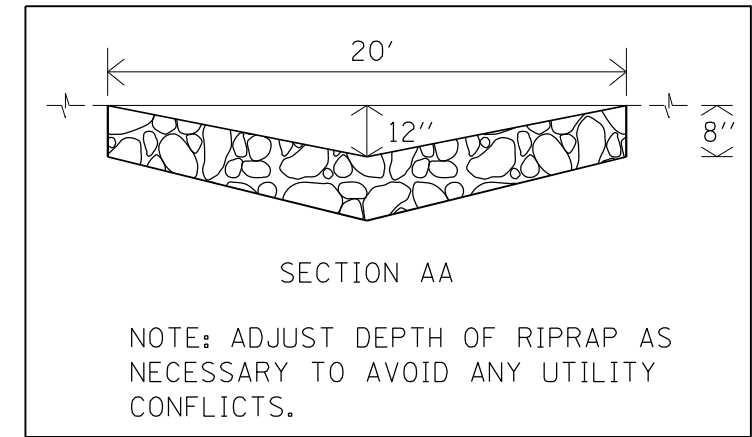
**REMOVAL PLAN
S.N. 102-0046**



SCALE: 1" = 100' SHEET 1 OF 1 SHEETS STA. 449+00 TO STA. 461+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(18-2VB)BRR	WOODFORD	39	9
CONTRACT NO. 68H23				
ILLINOIS FED. AID PROJECT				

NOTE 1: INSTALL A CONCRETE HEADWALL FOR THE PIPE DRAIN AT THE END OF THE EXISTING 4" PIPE DRAIN

NOTE 2: CONCRETE CURB, TYPE B SHALL BE 6" TALL (PORTION ABOVE SHOULDER), AND IT SHALL BE TAPERED DOWN TO A 2" HEIGHT OVER THE LAST 10' AT THE EAST END.



LEGEND	
	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "C", N50
	STONE RIP RAP, CLASS B3



NOTE: SEE STRUCTURE DETAILS FOR PROPOSED ITEMS ON STRUCTURE, BRIDGE APPROACH PAVEMENT AND CONNECTOR PAVEMENT

MODEL: Print Sheets
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	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/15/2023	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

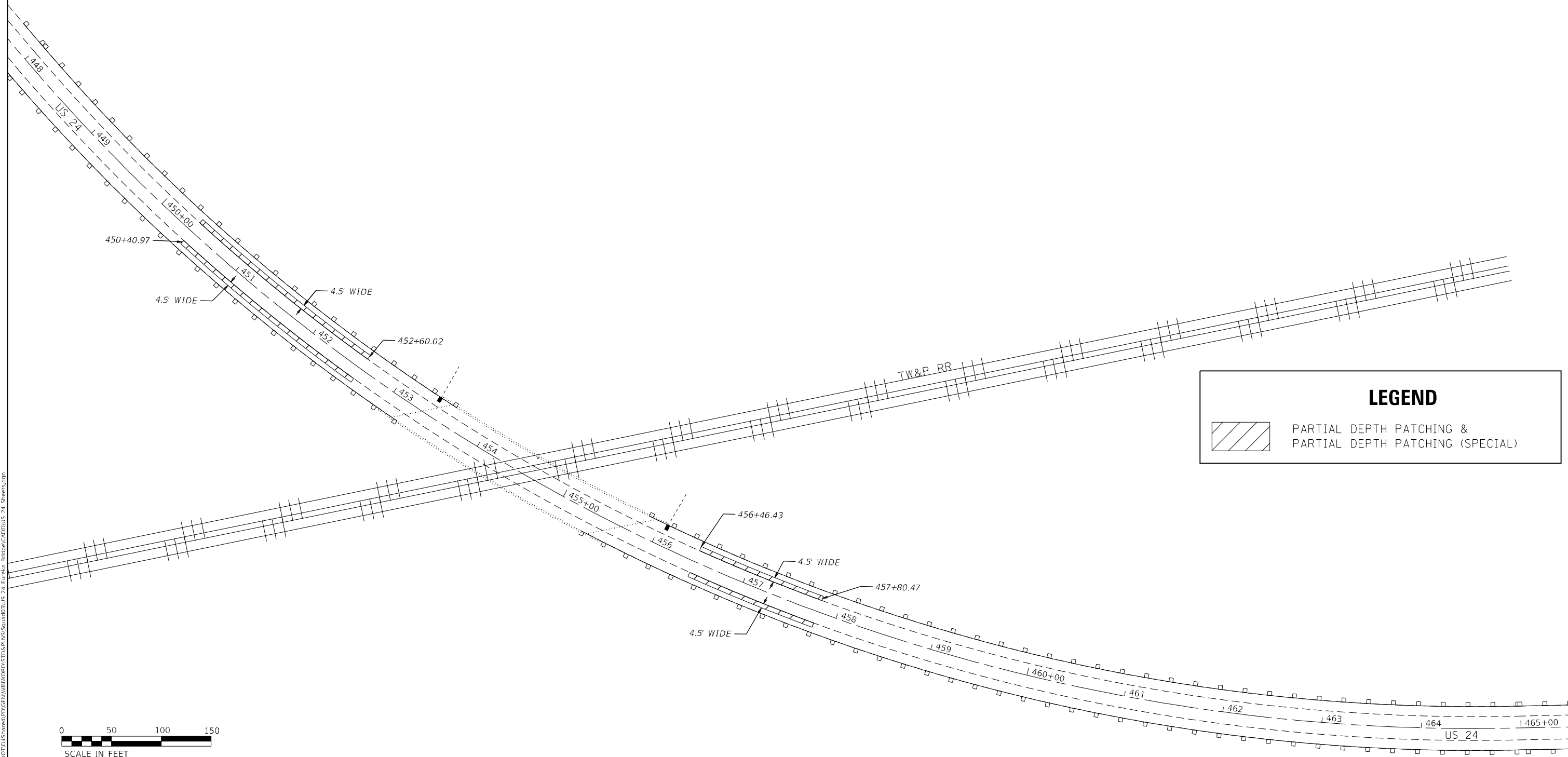
PROPOSED PLAN			
SCALE: 1" = 100'	SHEET 1	OF 1 SHEETS	STA. 449+00 TO STA. 461+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(18-2VB)BRR	WOODFORD	39	10
				CONTRACT NO. 68H23
ILLINOIS FED. AID PROJECT				



NOTES:

**USE TRAFFIC CONTROL AND PROTECTION,
STANDARD 701201 AND COMPLETE HOT-MIX
ASPHALT WORK.**



LEGEND

	PARTIAL DEPTH PATCHING & PARTIAL DEPTH PATCHING (SPECIAL)
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MODEL: Print sheets
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PLOT DATE = 12/15/2023	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGING DETAILS - PRE-STAGE

SCALE: 1"=100' SHEET 1 OF 4 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(18-2VB)BRR	WOODFORD	39	11
CONTRACT NO. 68H23				
ILLINOIS FED. AID PROJECT				

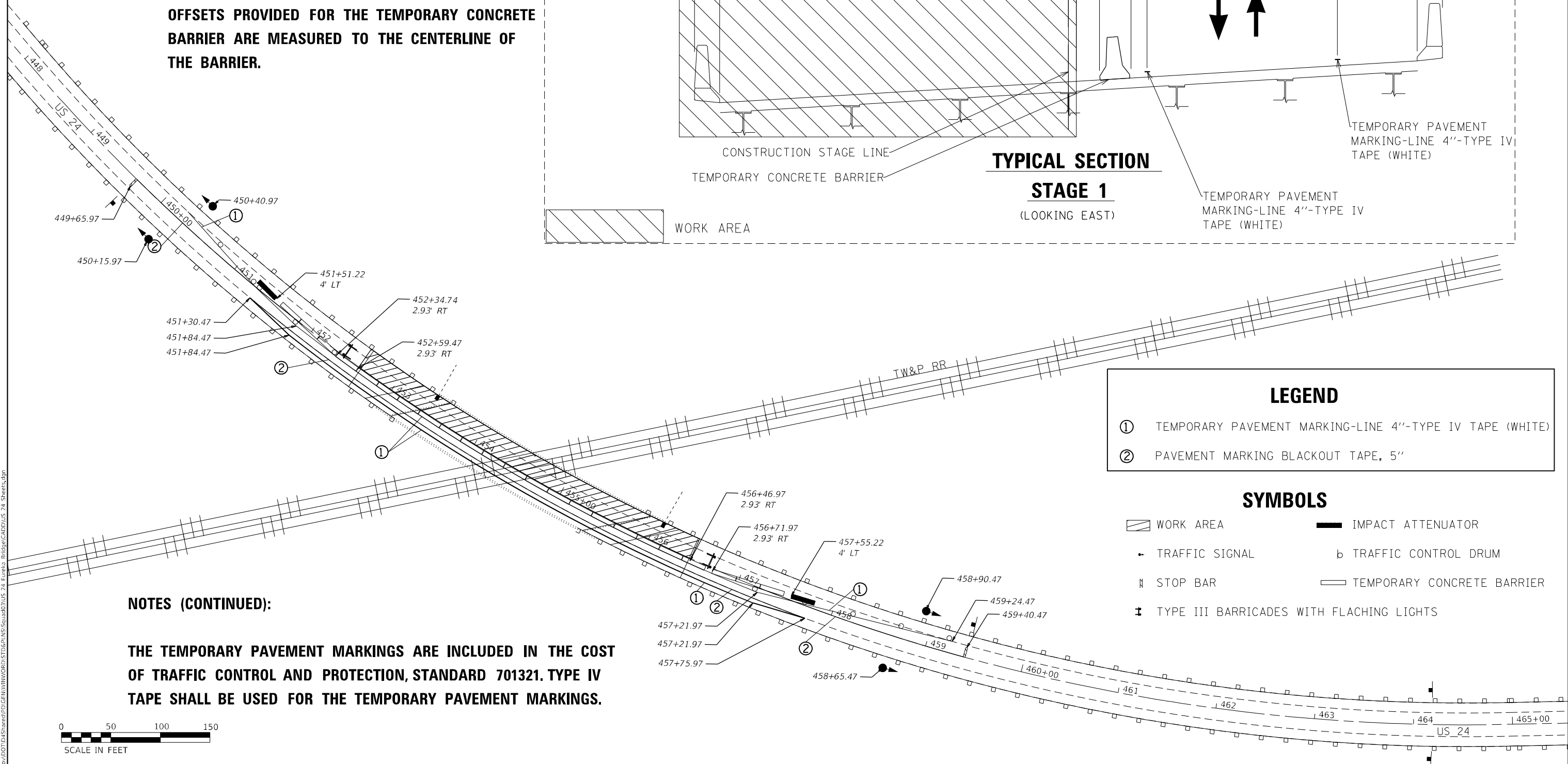
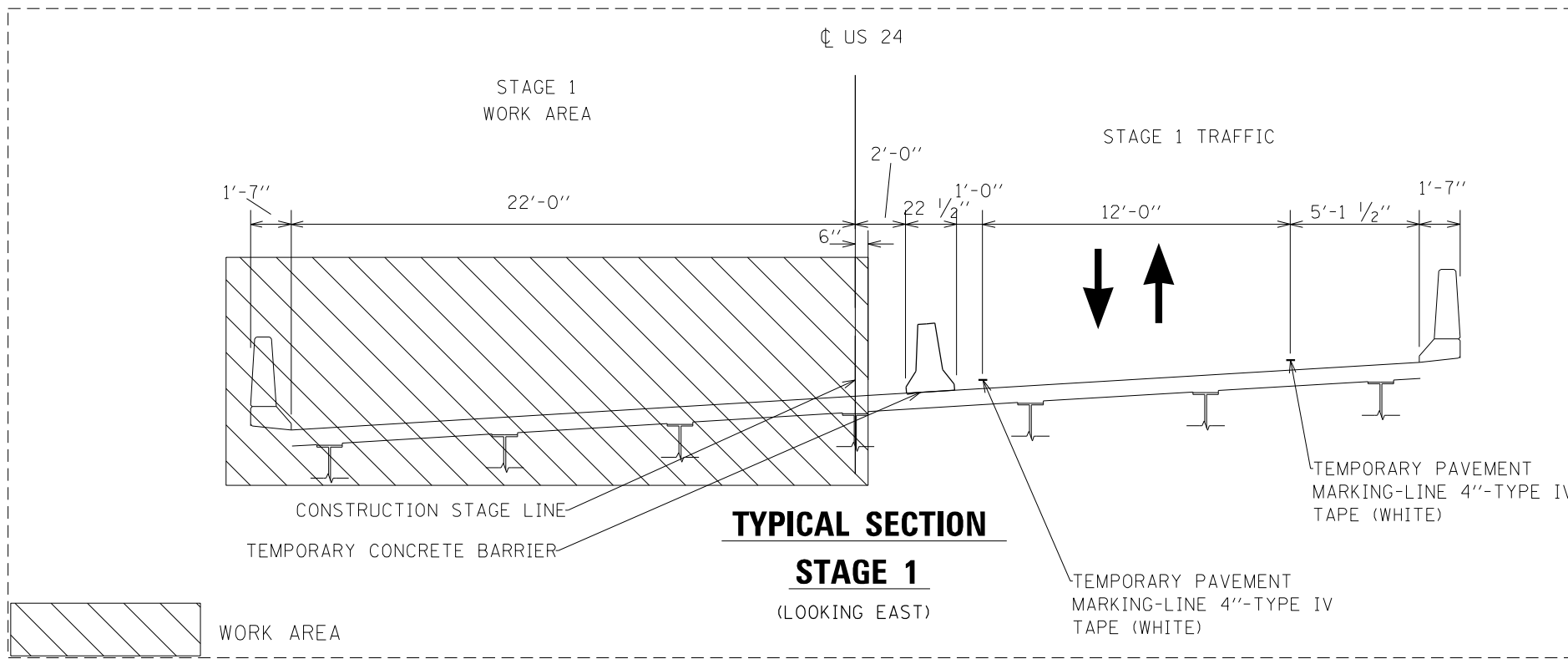


NOTES:

TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH HIGHWAY STANDARD 701321 AND THESE DETAILS.

TEMPORARY RUMBLE STRIPS SHALL BE USED AT LOCATIONS DETERMINED BY THE ENGINEER.

OFFSETS PROVIDED FOR THE TEMPORARY CONCRETE BARRIER ARE MEASURED TO THE CENTERLINE OF THE BARRIER.



NOTES (CONTINUED):

THE TEMPORARY PAVEMENT MARKINGS ARE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, STANDARD 701321. TYPE IV TAPE SHALL BE USED FOR THE TEMPORARY PAVEMENT MARKINGS.

LEGEND

- ① TEMPORARY PAVEMENT MARKING-LINE 4''-TYPE IV TAPE (WHITE)
- ② PAVEMENT MARKING BLACKOUT TAPE, 5''

SYMBOLS

- WORK AREA
- IMPACT ATTENUATOR
- TRAFFIC SIGNAL
- TRAFFIC CONTROL DRUM
- STOP BAR
- TEMPORARY CONCRETE BARRIER
- TYPE III BARRICADES WITH FLASHING LIGHTS



MODEL: Print sheets
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DRAWN -	REVISED -	
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/15/2023	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGING DETAILS - STAGE 1

SCALE: 1"=100' SHEET 2 OF 4 SHEETS STA. TO STA.

F.A.P. RTE. 317	SECTION (18-2VB)BRR	COUNTY WOODFORD	TOTAL SHEETS 39	SHEET NO. 12
ILLINOIS FED. AID PROJECT			CONTRACT NO. 68H23	

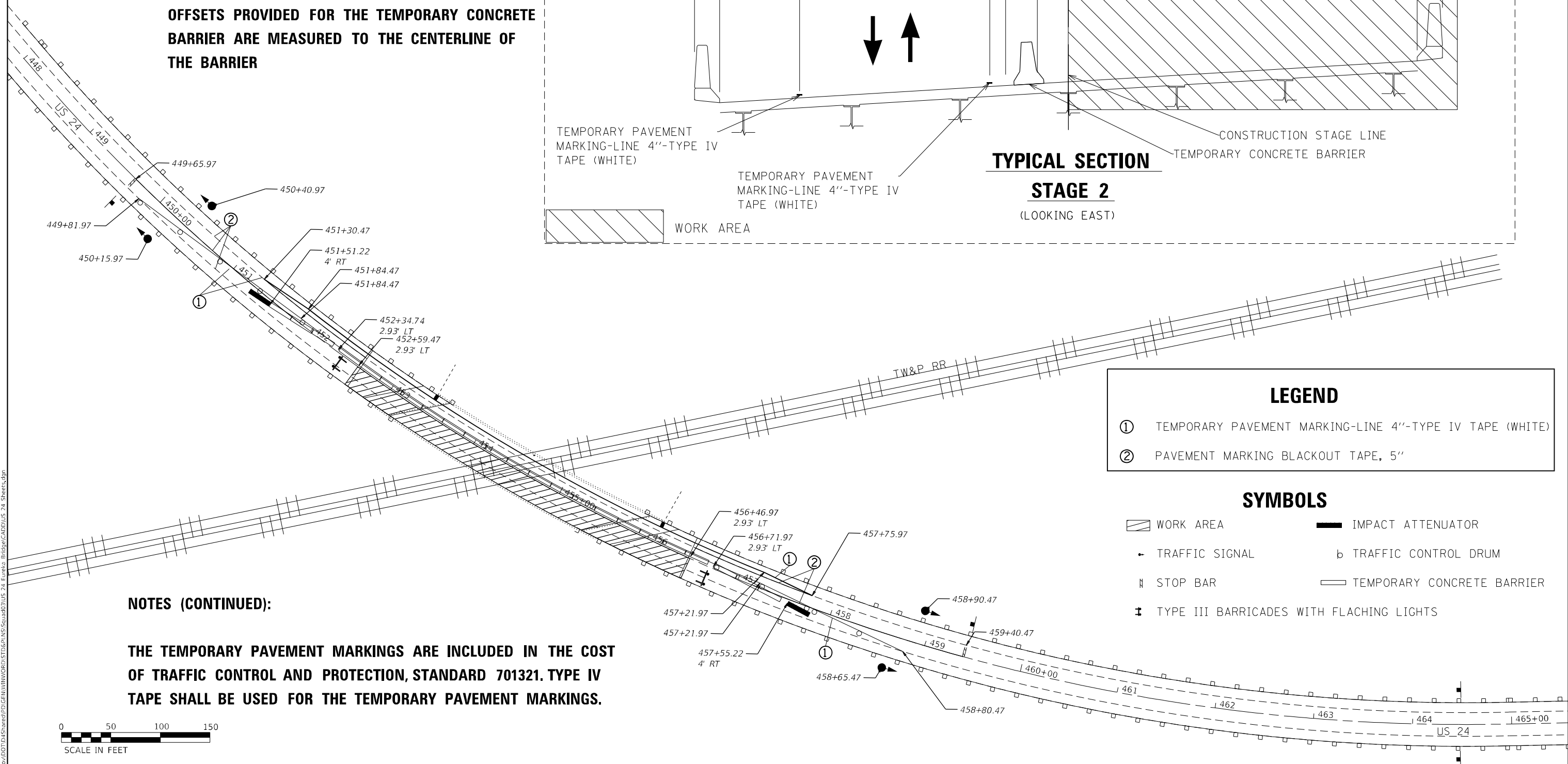
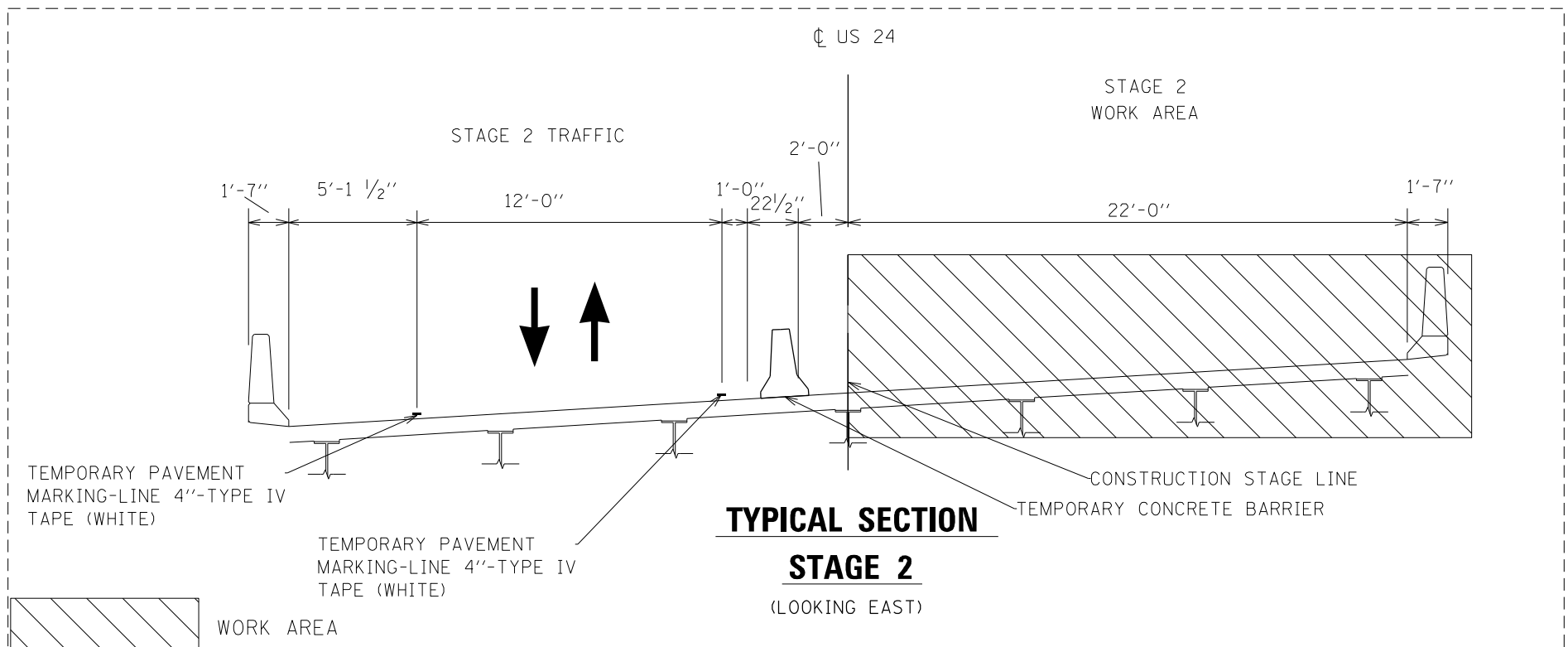


NOTES:

TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH HIGHWAY STANDARD 701321 AND THESE DETAILS

TEMPORARY RUMBLE STRIPS SHALL BE USED AT LOCATIONS DETERMINED BY THE ENGINEER

OFFSETS PROVIDED FOR THE TEMPORARY CONCRETE BARRIER ARE MEASURED TO THE CENTERLINE OF THE BARRIER



NOTES (CONTINUED):

THE TEMPORARY PAVEMENT MARKINGS ARE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, STANDARD 701321. TYPE IV TAPE SHALL BE USED FOR THE TEMPORARY PAVEMENT MARKINGS.



LEGEND

- ① TEMPORARY PAVEMENT MARKING-LINE 4''-TYPE IV TAPE (WHITE)
- ② PAVEMENT MARKING BLACKOUT TAPE, 5''

SYMBOLS

- WORK AREA
- IMPACT ATTENUATOR
- TRAFFIC SIGNAL
- TRAFFIC CONTROL DRUM
- STOP BAR
- TEMPORARY CONCRETE BARRIER
- TYPE III BARRICADES WITH FLASHING LIGHTS

MODEL: Print sheets
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DRAWN -	REVISED -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/15/2023	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGING DETAILS - STAGE 2

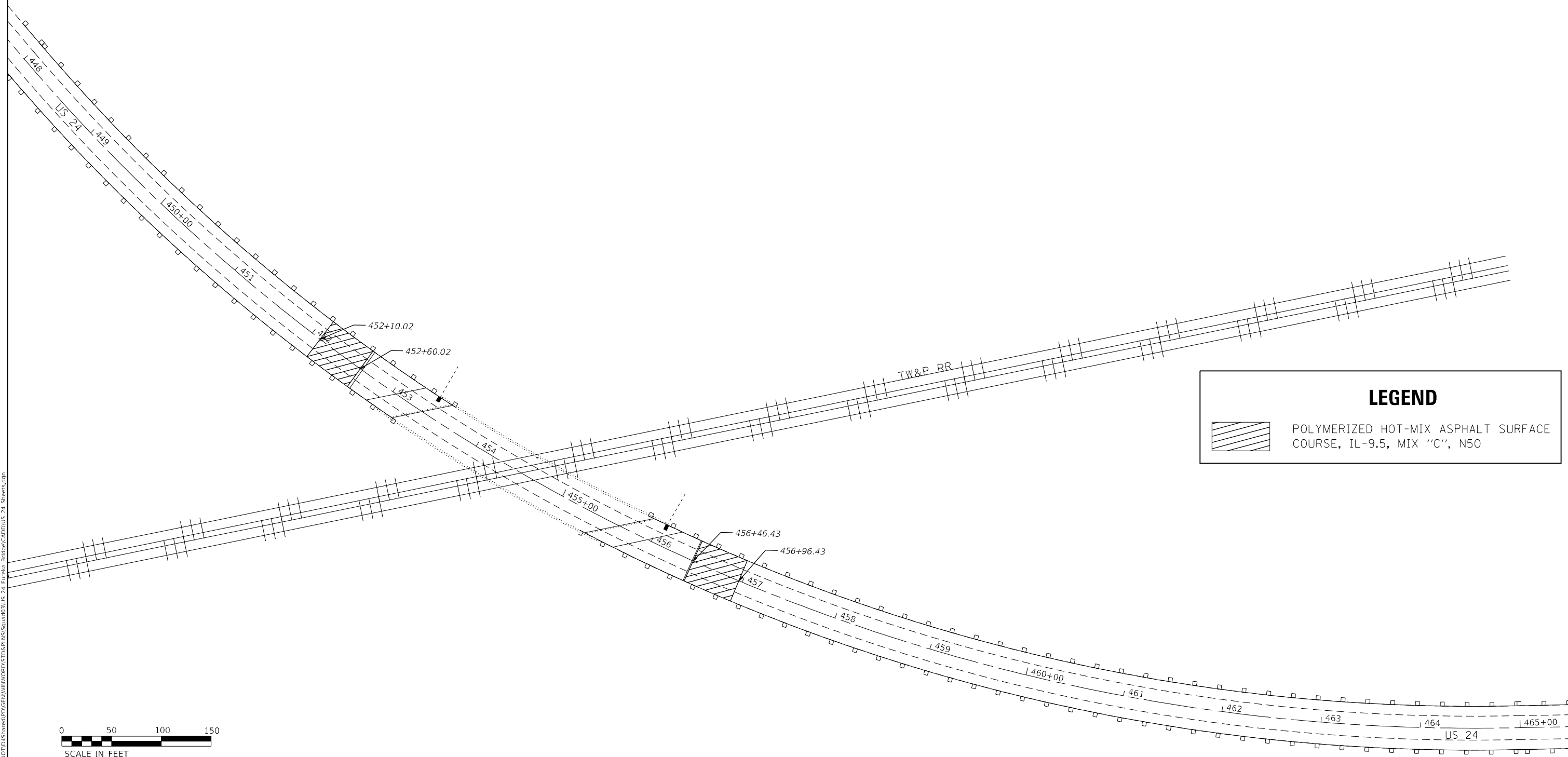
SCALE: 1"=100' SHEET 3 OF 4 SHEETS STA. TO STA.


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(18-2VB)BRR	WOODFORD	39	13
CONTRACT NO. 68H23				
ILLINOIS FED. AID PROJECT				



NOTES:

**USE TRAFFIC CONTROL AND PROTECTION,
STANDARD 701201 AND COMPLETE HOT-MIX
ASPHALT WORK.**



LEGEND	
	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "C", N50



MODEL: Print Sheets
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PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/15/2023	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGING DETAILS - STAGE 3

SCALE: 1"=100' SHEET 4 OF 4 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(18-2V8)BRR	WOODFORD	39	14
CONTRACT NO. 68H23				
ILLINOIS FED. AID PROJECT				

Existing Structure: SN 102-0046 was originally built in 1994 as Section 18-2VB. The existing structure is a 3-span wide flange steel beam bridge with a reinforced concrete deck, all supported on multi-column concrete piers and open abutments. The bridge length is 232'-6 1/2" from back to back of abutments (measured along local tangent) and the width is 47'-2" out to out of deck (measured radially) and is built on a curve. The substructures are parallel to the centerline of the railroad and are skewed 48°19'55" to the local tangent. The abutments and piers are supported by precast concrete piles.

Salvage: None

DESIGN SPECIFICATIONS

2002 AASHTO Std. Spec. for Highway Bridges, 17th Edition

LOADING HS20-44

No future wearing surface allowance.

DESIGN STRESSES

EXISTING STRUCTURE (1994 CONSTRUCTION)

f'c = 3,500 psi
 fy = 60,000 psi (Reinforcement)
 fy = 50,000 psi (AASHTO M-223 Grade 50)
 fy = 36,000 psi (AASHTO M-183)

FIELD UNITS

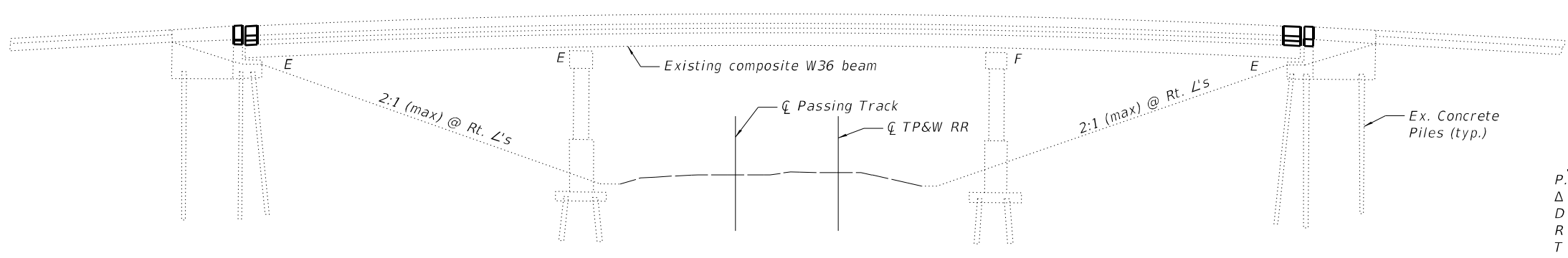
f'c = 4,000 psi (Superstructure)
 f'c = 3,500 psi (Substructure)
 fy = 60,000 psi (Reinforcement)

INDEX OF SHEETS

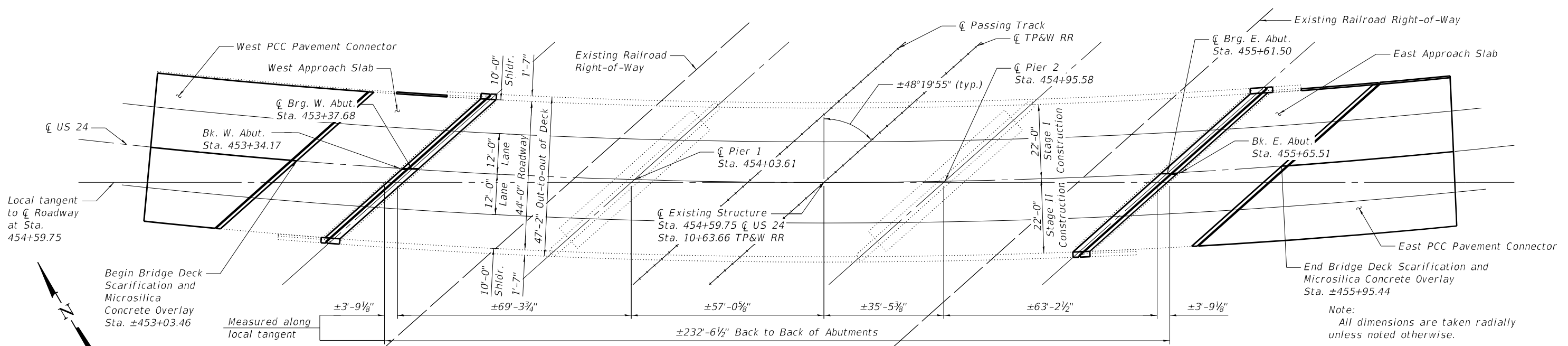
- 1 - General Plan & Elevation
- 2 - General Data
- 3 - Temporary Concrete Barrier
- 4 - Superstructure Concrete Removal
- 5 - Superstructure
- 6 - Superstructure Details
- 7 - Superstructure Typical Section
- 8 - Superstructure Repair Plan
- 9 - Superstructure Repair Details
- 10 - Preformed Joint Strip Seal
- 11 - Pavement Expansion Joint Replacement Plan and Details
- 12 - West Abutment Concrete Removal
- 13 - West Abutment Plan and Elevation
- 14 - East Abutment Concrete Removal
- 15 - East Abutment Plan and Elevation
- 16 - Slope Wall Repair Plan
- 17 - Bar Splicer Assembly and Mechanical Splicer Details

CURVE DATA

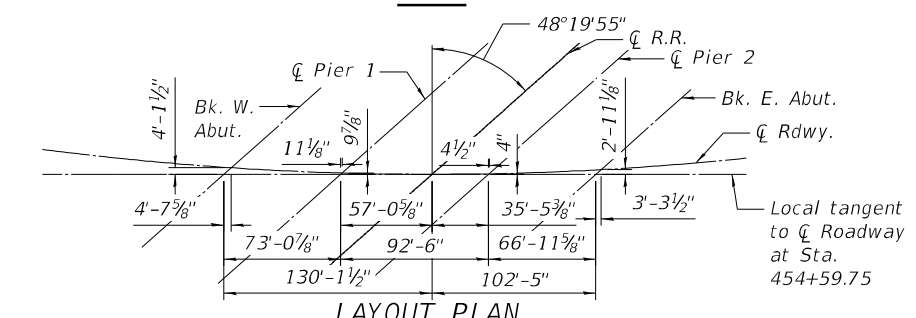
P.I. Sta. = 457+10.09
 Δ = 50°51'50.29"
 D = 3°00'00"
 R = 1,909.86'
 T = 908.18'
 L = 1,695.47'
 E = 204.93'
 P.C. Sta. = 448+01.91
 P.T. Sta. = 464+97.38
 S.E. = 0.0601'
 S.A. = Sta 446+68.58 to Sta. 448+68.58
 Sta. 464+30.71 to Sta. 466+30.71



ELEVATION

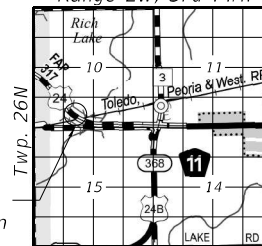


PLAN



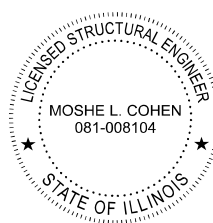
LAYOUT PLAN

Range 2W, 3rd P.M.



LOCATION SKETCH

GENERAL PLAN AND ELEVATION
U.S. ROUTE 24 OVER TP&W RR
F.A.P. ROUTE 317 - SEC. (18-2VB)BRR
WOODFORD COUNTY
STATION 454+59.75
STRUCTURE NO. 102-0046



Signed: *Moshe L. Cohen*
 Dated: 2024-01-18
 Illinois Structural Engineer
 No. 081-008104
 License Expires: 11-30-2024

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHEET 1 OF 17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(18-2VB)BRR	WOODFORD	39	15
CONTRACT NO. 68H23				

ILLINOIS FED. AID PROJECT

USER NAME	DESIGNED	CHECKED	PLOT SCALE	PLOT DATE
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	MMC	MMC		
	MLC	MLC		
	MMC	MMC		

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GENERAL NOTES

1. Reinforcement bars designated (E) shall be epoxy coated.
2. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
3. Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications at ambient temperature other than 50°F.
4. All exposed concrete edges shall have a standard 3/4" chamfer unless noted otherwise.
5. Fasteners shall be high strength bolts. Bolts 3/4" diameter, open holes 13/16" diameter, unless otherwise noted.
6. All heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

SCOPE OF WORK

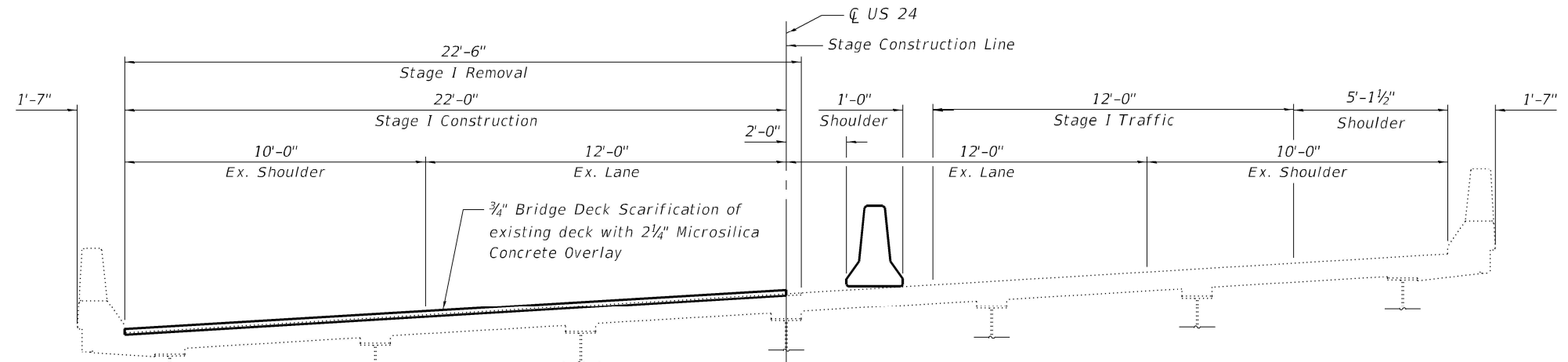
To be completed under stage construction:

1. Remove existing neoprene expansion joints at abutments.
2. Remove existing steel frame and grate from inlet structures and fill structures.
3. Plug existing deck drain and remove drainage system for drain over railroad tracks.
4. Remove existing deicing nozzles and pavement sensor.
5. Scarify bridge deck and approach slabs.
6. Remove and reconstruct abutment backwalls.
7. Reconstruct deck joints at the Abutments with preformed joint strip seal.
8. Remove existing PCC Pavement Connectors and Preformed Joint Seal joints at the Approach Slabs. Remove Approach Slab concrete adjacent to the joints as shown on the plans. Construct new PCC Pavement Connectors. Place new concrete at the end of the Approach Slabs at the joint location and install designated size of silicone joint sealer.
9. Remove and reconstruct curb on Approach Slabs and PCC Pavement Connectors.
10. Install Microsilica Concrete Overlay and perform Bridge Deck Grooving.
11. Apply Protective Coat (Special) to the existing parapet concrete and Protective Coat to new concrete (including PCC Pavement Connectors) and Microsilica Concrete Overlay.
12. Fill gap between abutments and slope walls. Fill voids under slope walls.

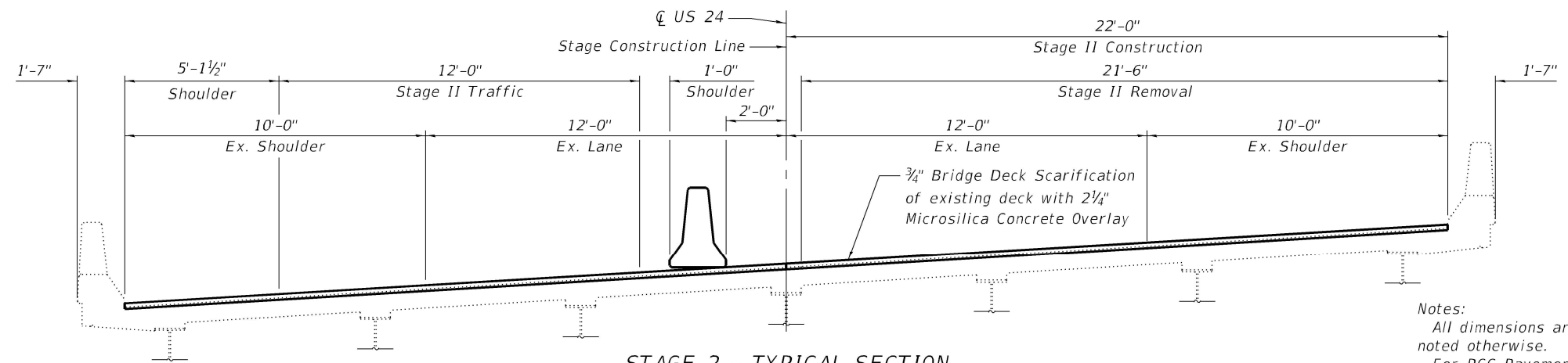
TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Welded Wire Reinforcement	Sq. Yd.	243		243
Pavement Connector (PCC) for Bridge Approach Slab	Sq. Yd.	448		448
Pavement Removal	Sq. Yd.	449		449
Curb Removal	Foot	84		84
Tie Bars 1 1/4"	Each	95		95
Concrete Removal	Cu. Yd.	21.0		21.0
Concrete Superstructure	Cu. Yd.	23.2		23.2
Bridge Deck Grooving	Sq. Yd.	1,386		1,386
** Protective Coat	Sq. Yd.	1,900		1,900
Reinforcement Bars, Epoxy Coated	Pound	3,650		3,650
Bar Splicers	Each	32		32
Preformed Joint Seal 4"	Foot	133		133
Preformed Joint Strip Seal	Foot	137		137
Concrete Shoulder Curb	Foot	21		21
Surface Filler (Special)	Gal.	0.4		0.4
Protective Coat (Special)	Sq. Yd.	342		342
Bridge Deck Scarification 3/4"	Sq. Yd.	1,386		1,386
Bridge Deck Microsilica Concrete Overlay 2 1/4"	Sq. Yd.	1,386		1,386
Plug Existing Deck Drains	Each	1		1
Filling Drainage Structures	Each	2		2
Slope Wall Slurry Pumping	Cu. Yd.		7	7

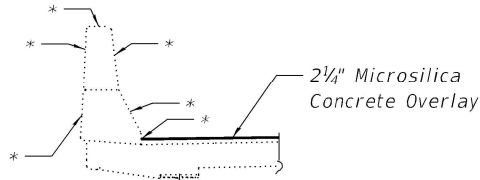
** On new concrete and Microsilica Concrete Overlay only



STAGE 1 - TYPICAL SECTION
(Looking Upstation)
(Bridge deck shown, approach slabs similar)



STAGE 2 - TYPICAL SECTION
(Looking Upstation)
(Bridge deck shown, approach slabs similar)



TYPICAL SECTION THRU PARAPET

For dimensions, see sections on sheets 4, 6, and 12 through 15 of 17.

* Protective Coat (Special) shall be applied to the existing parapets on both the deck and the abutments.

Following application of the Protective Coat (Special), the Contractor shall reapply the stenciled structure number on the parapet in black paint with 2" numbers with the same format xxx-xxxx. This work shall be considered included in the cost of Protective Coat (Special).

Notes:
All dimensions are taken radially unless noted otherwise.
For PCC Pavement Connector staging, see sheet 7 of 17.

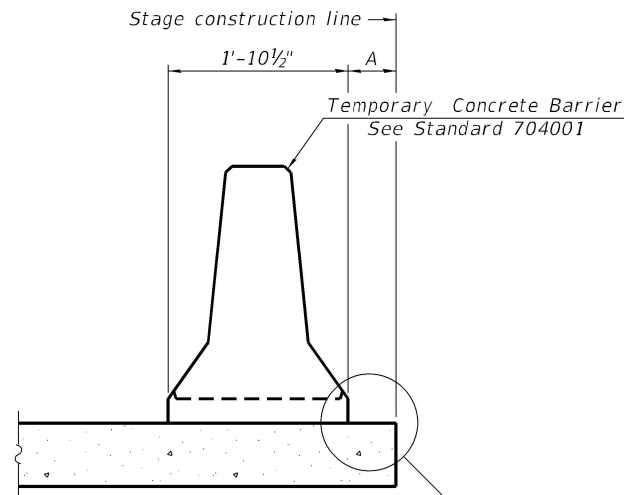
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	PLOT SCALE =	CHECKED - MMC	REVISED -
	PLOT DATE =	DRAWN - MLC	REVISED -
		CHECKED - MMC	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

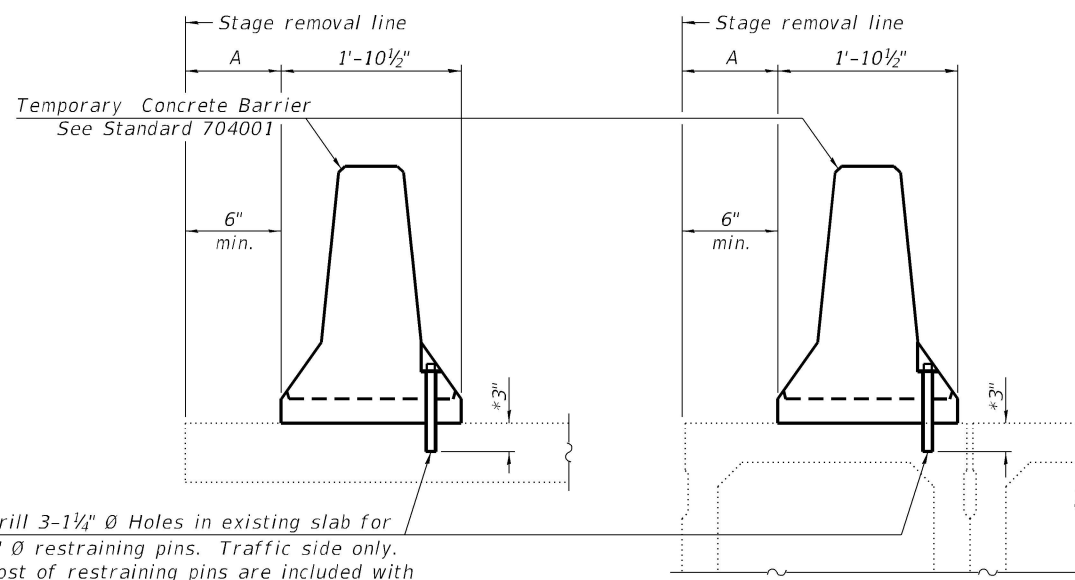
**GENERAL DATA
STRUCTURE NO. 102-0046**
SHEET 2 OF 17 SHEETS

F.A.P. RTE. 317	SECTION (18-2VB)BRR	COUNTY WOODFORD	TOTAL SHEETS 39	SHEET NO. 16
CONTRACT NO. 68H23				
ILLINOIS FED. AID PROJECT				



When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM



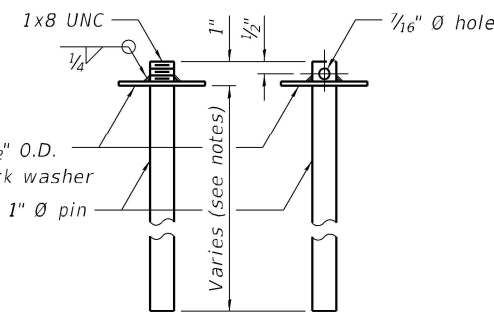
Drill 3-1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

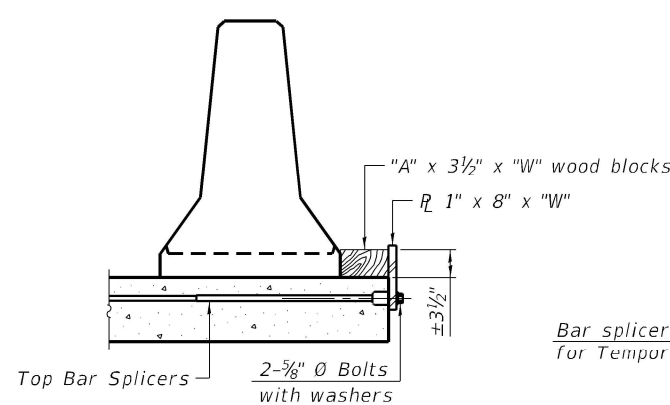
EXISTING DECK BEAM

SECTIONS THRU SLAB OR DECK BEAM

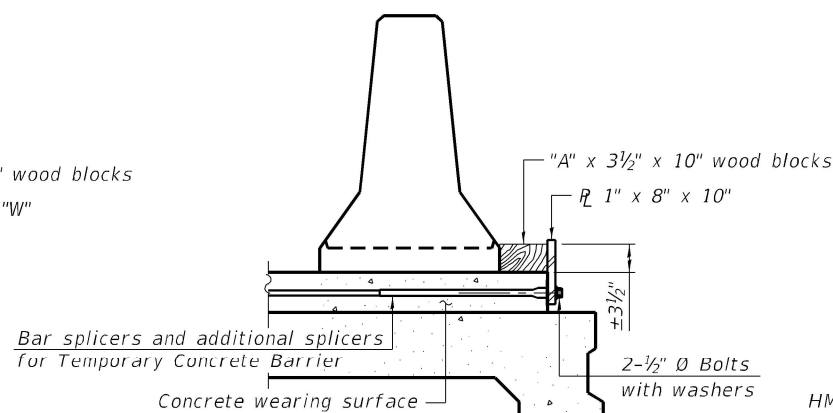


RESTRAINING PIN

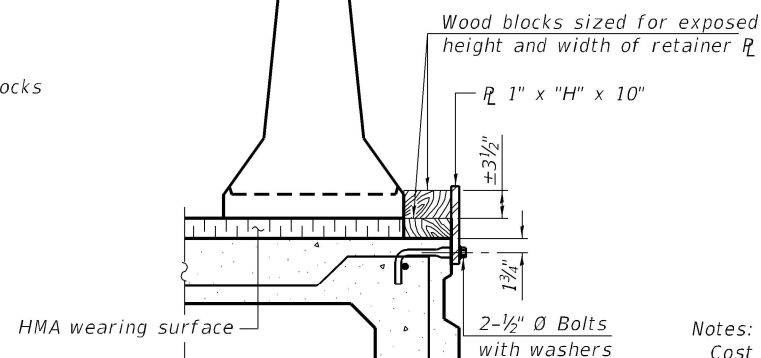
US Std. 1 1/16" I.D. x 2 1/2" O.D. x approx. 8 gauge thick washer



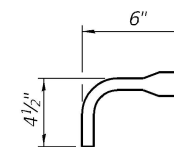
DETAIL I



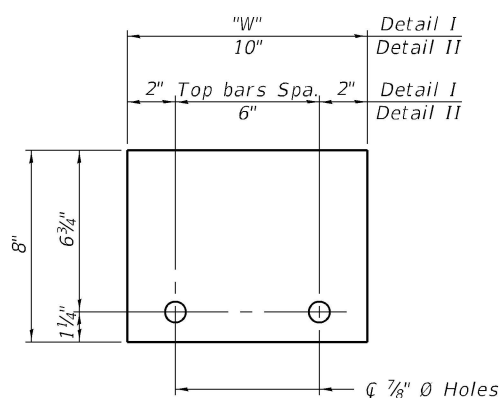
DETAIL II



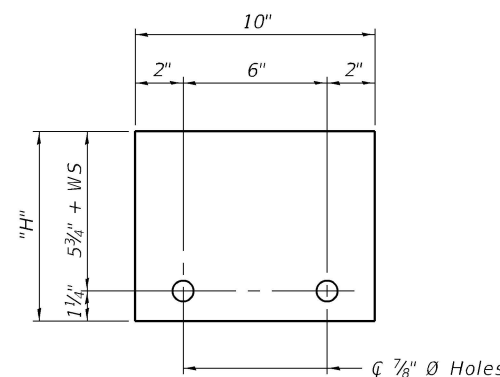
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER R 1" x 8" x "W" (Detail I and II)



STEEL RETAINER R 1" x "H" x 10" (Detail III)

Notes:
 Cost of retainer assembly is included with Temporary Concrete Barrier.
 A retainer assembly shall be located at the approximate \bar{c} of each temporary concrete barrier.
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.
 When the 'A' dimension is less than 1 1/2", the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.
 Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.
 Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

RAILING CRITERIA

NCHRP 350 Test Level	3
Railing Weight (plf)	440

R-27 10-12-2021

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY CONCRETE BARRIER
STRUCTURE NO. 102-0046

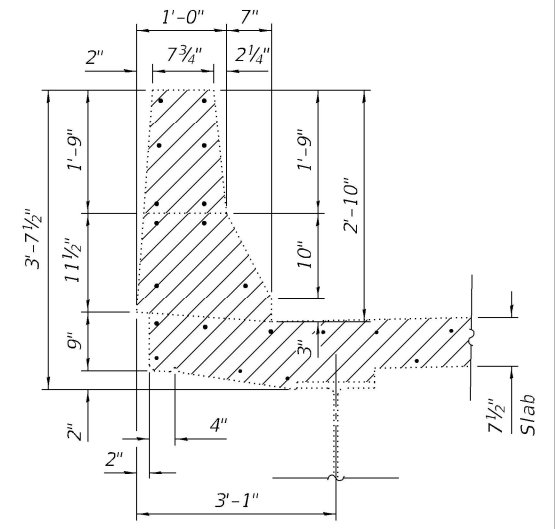
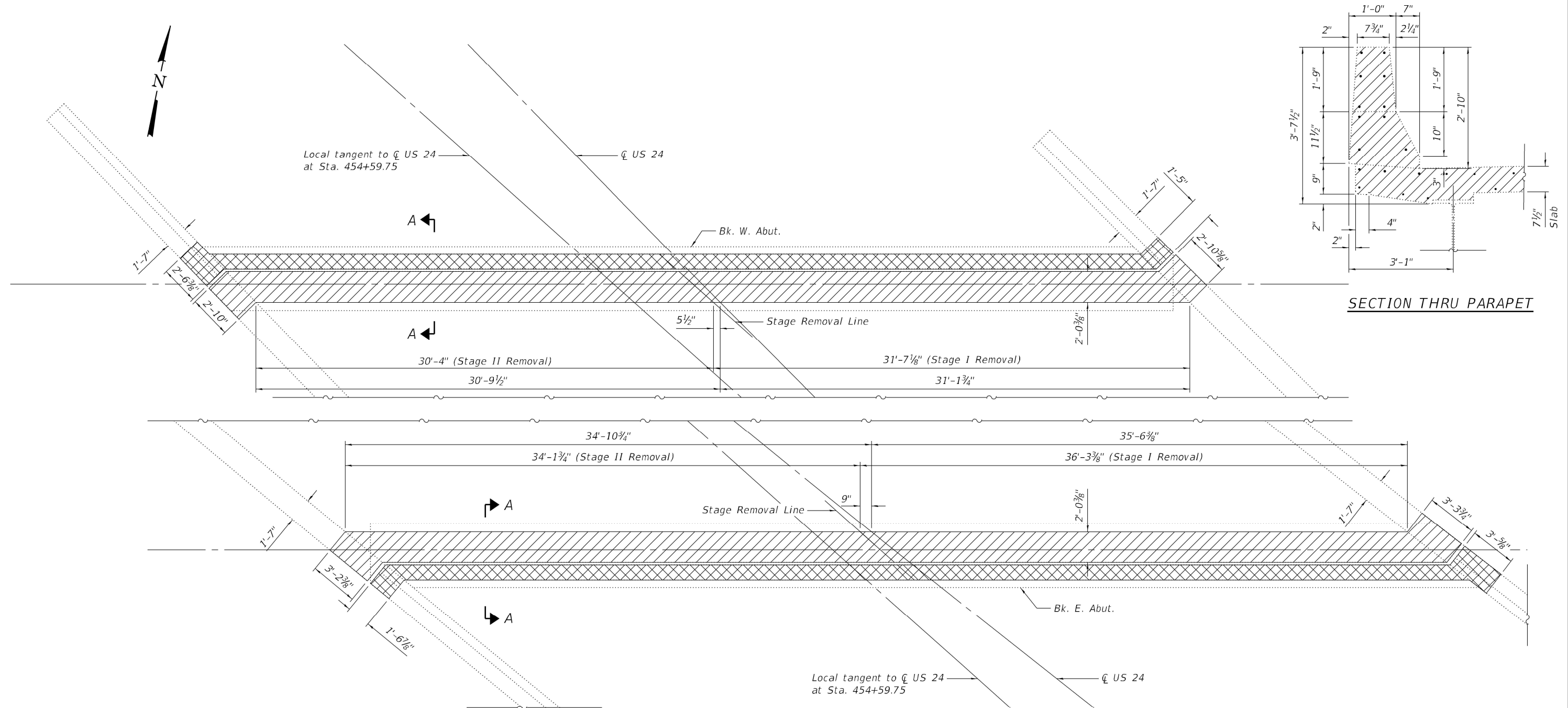
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317	(18-2VB)BRR	WOODFORD	39	17
CONTRACT NO. 68H23				

SHEET 3 OF 17 SHEETS

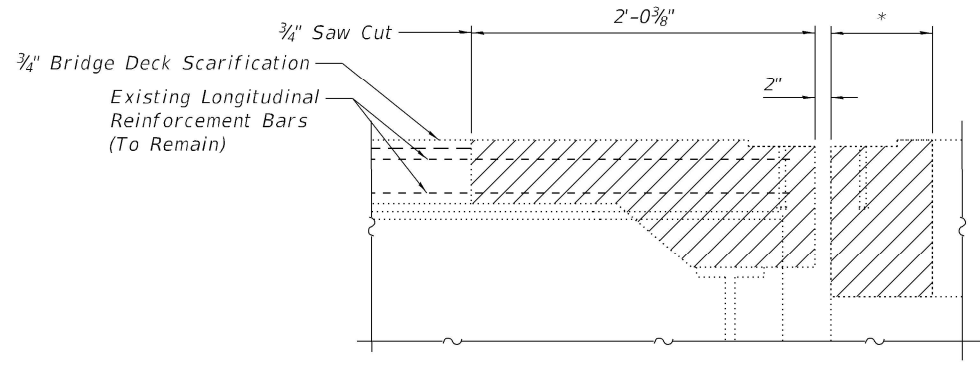
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PLAN



SECTION A-A

Notes:
 The Contractor shall use extreme care during concrete removal so as not to damage the existing steel beams. Any steel damaged during concrete removal operations shall be repaired at the Contractor's expense.
 Removal of existing expansion joint and associated material shall not be paid for separately, but shall be included in the cost of Concrete Removal.
 Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into new construction.
 Any reinforcement bars that are damaged during concrete removal operations shall be replaced at the Contractor's expense.
 Stud shear connectors damaged during concrete removal operations shall be removed and replaced per Section 505 of the Standard Specifications at the Contractor's expense.

* See sheets 12 through 15 of 17 for abutment concrete removal and reconstruction details.

LEGEND

- Indicates Concrete Removal
- Indicates Concrete Removal of abutment

BILL OF MATERIAL

Item	Unit	Total
Concrete Removal	Cu. Yd.	9.8

Kaskaskia
 Engineering Group, LLC
 1877 N. Lincoln Hwy.
 Woodstock, IL 60091
 (815) 486-1111

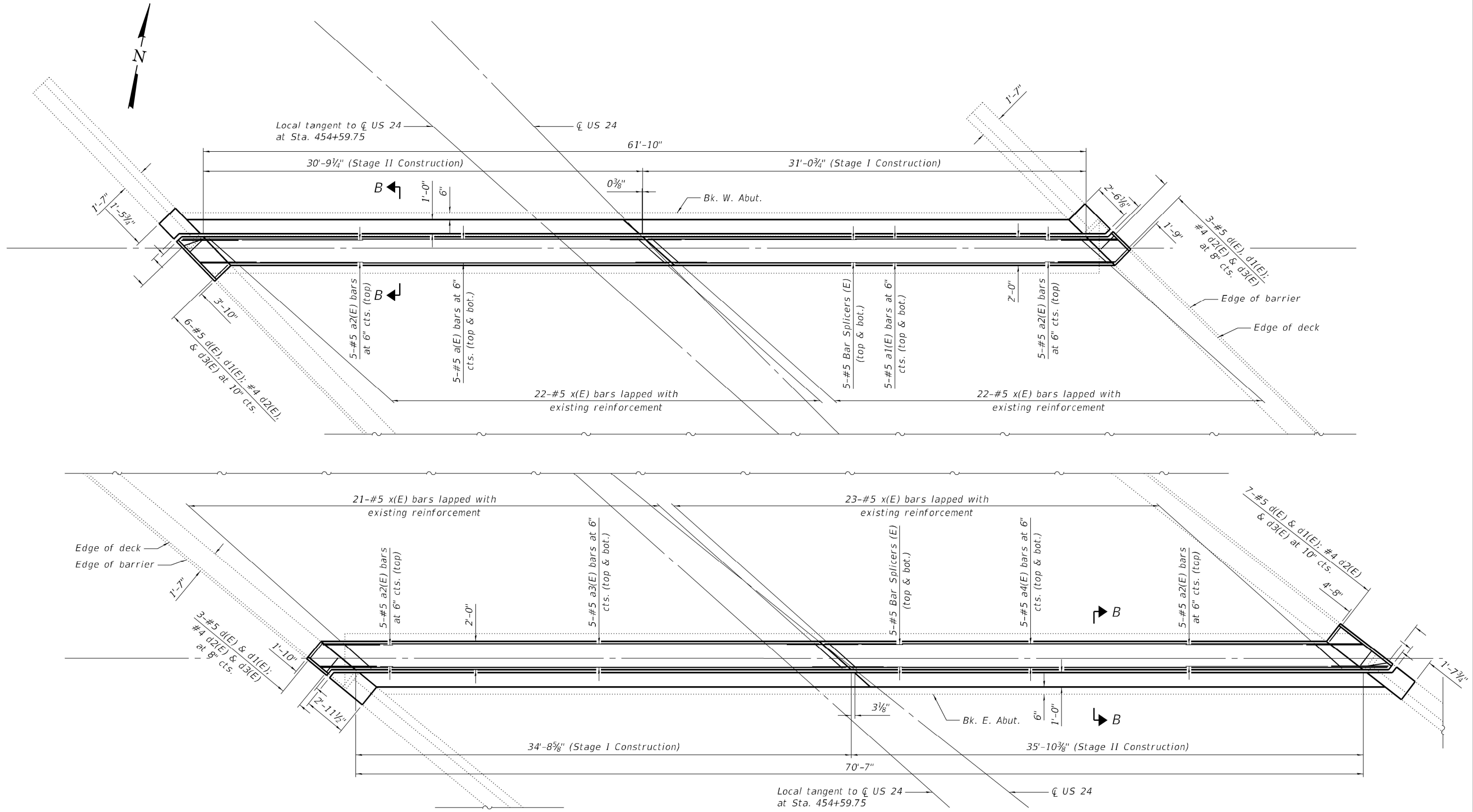
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SUPERSTRUCTURE CONCRETE REMOVAL
STRUCTURE NO. 102-0046

SHEET 4 OF 17 SHEETS

F.A.P. RTE. 317	SECTION (18-2VB)BRR	COUNTY WOODFORD	TOTAL SHEETS 39	SHEET NO. 18
CONTRACT NO. 68H23				
ILLINOIS FED. AID PROJECT				



PLAN

Notes:
 See Sheet 6 of 17 for Section B-B.
 Order a(E), a1(E), a3(E), and a4(E) bars
 full-length and cut to fit in the field.

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Kaskaskia
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 1127 N. W. 11th St.
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 (954) 533-8877
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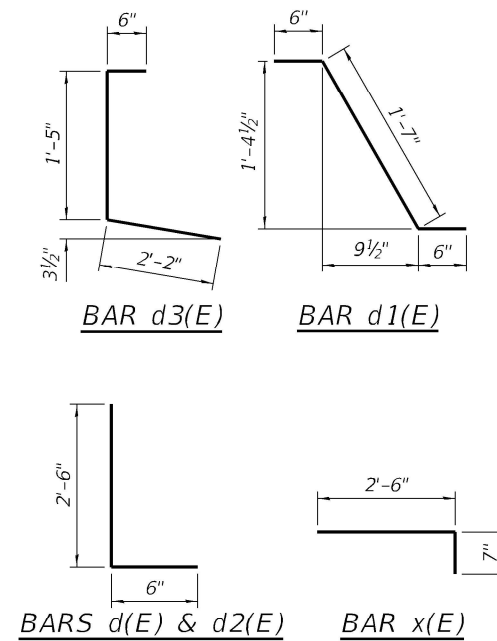
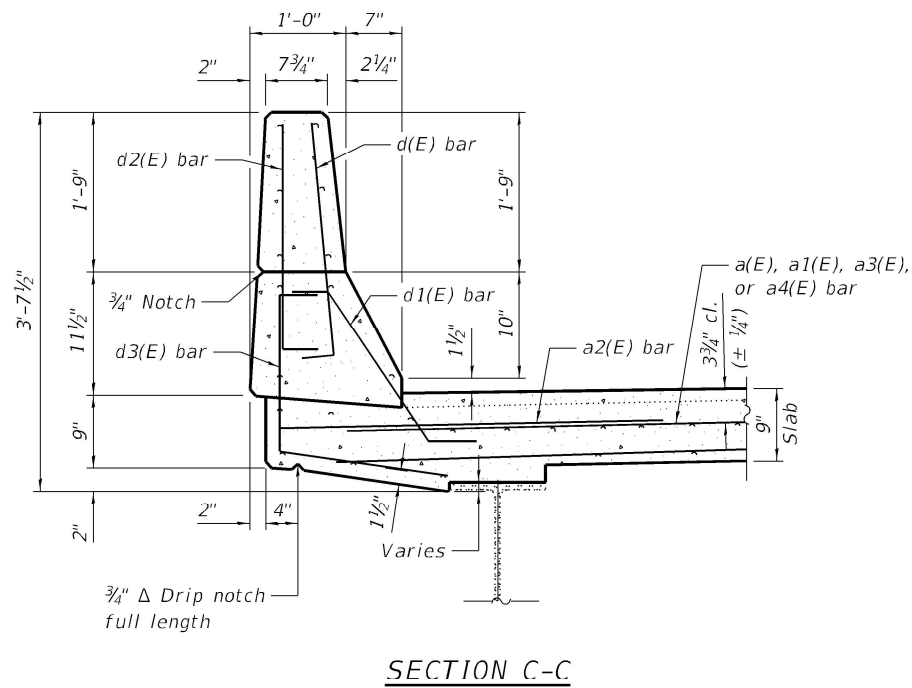
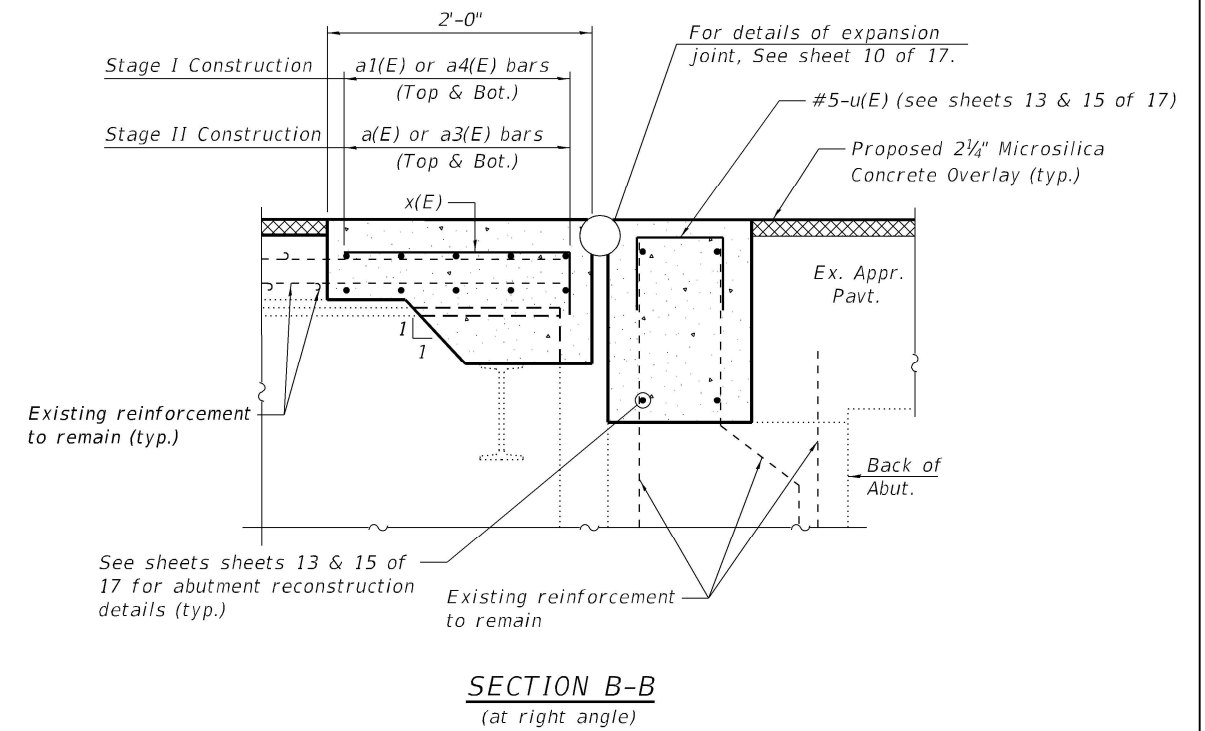
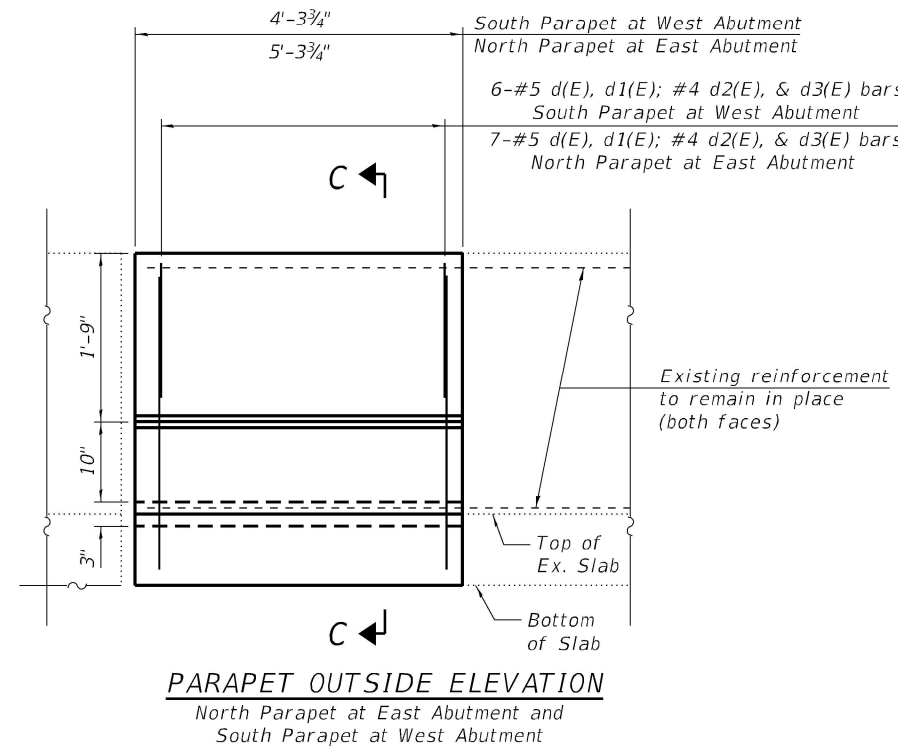
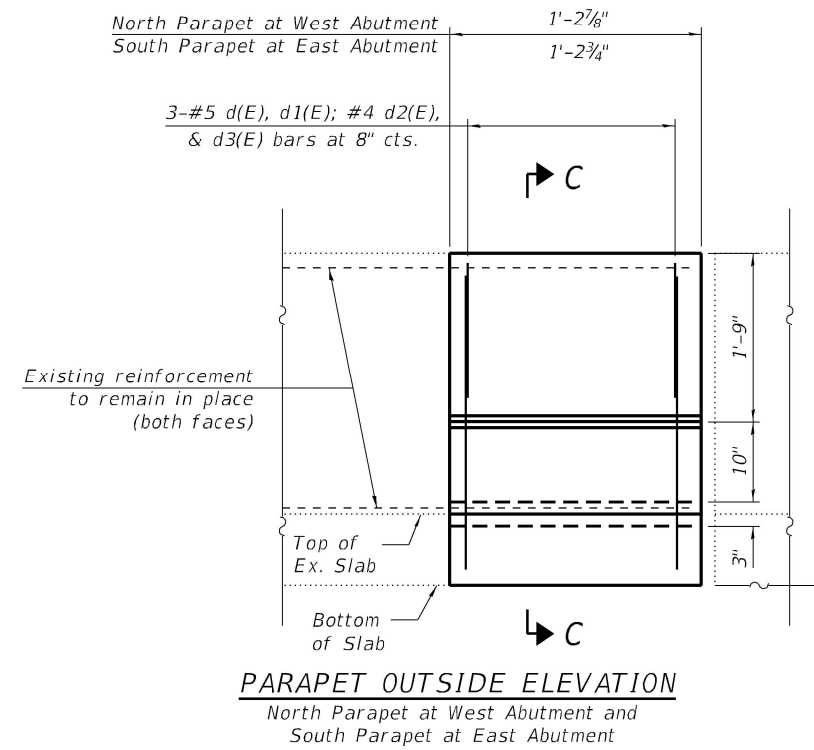
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SUPERSTRUCTURE
 STRUCTURE NO. 102-0046
 SHEET 5 OF 17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(18-2VB)BRR	WOODFORD	39	19
CONTRACT NO. 68H23				
ILLINOIS FED. AID PROJECT				

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BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	10	#5	32'-8"	▬
a1(E)	10	#5	32'-11"	▬
a2(E)	20	#5	4'-0"	▬
a3(E)	10	#5	36'-10"	▬
a4(E)	10	#5	38'-1"	▬
d(E)	19	#5	3'-0"	┌
d1(E)	19	#5	2'-7"	┌
d2(E)	19	#4	3'-0"	┌
d3(E)	19	#4	4'-1"	┌
x(E)	88	#5	3'-1"	┌
Concrete Superstructure			Cu. Yd.	10.9
Reinforcement Bars, Epoxy Coated			Pound	2,040

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 License No. 021-000000
 02-000000

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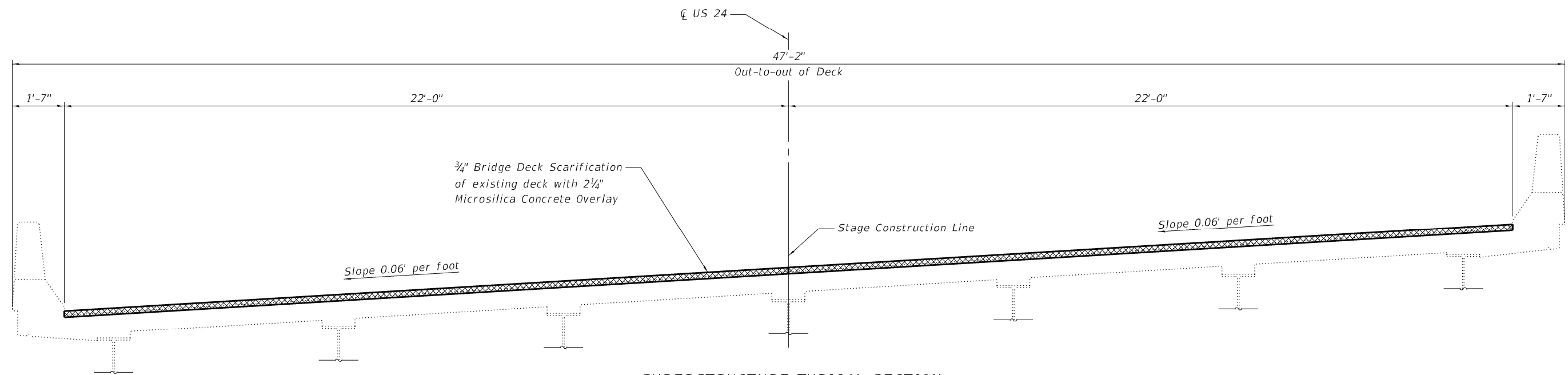
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE DETAILS
STRUCTURE NO. 102-0046

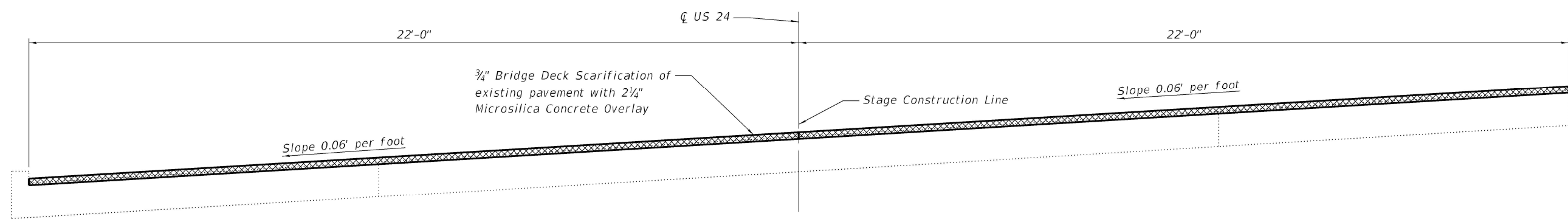
SHEET 6 OF 17 SHEETS

F.A.P. RTE. 317	SECTION (18-2VB)BRR	COUNTY WOODFORD	TOTAL SHEETS 39	SHEET NO. 20
CONTRACT NO. 68H23				
ILLINOIS FED. AID PROJECT				

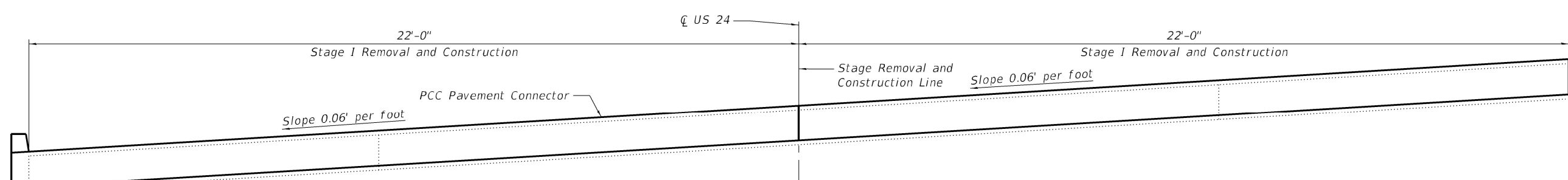
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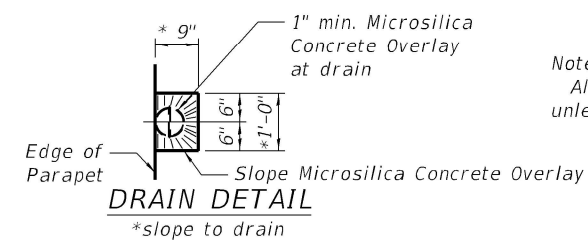
SUPERSTRUCTURE TYPICAL SECTION
 (Looking Upstation)



APPROACH SLAB TYPICAL SECTION
 (Looking Upstation)



PCC PAVEMENT CONNECTOR TYPICAL SECTION
 (Looking Upstation)



Note:
 All dimensions are taken radially
 unless noted otherwise.

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 11/27/2023
 01/06/2024

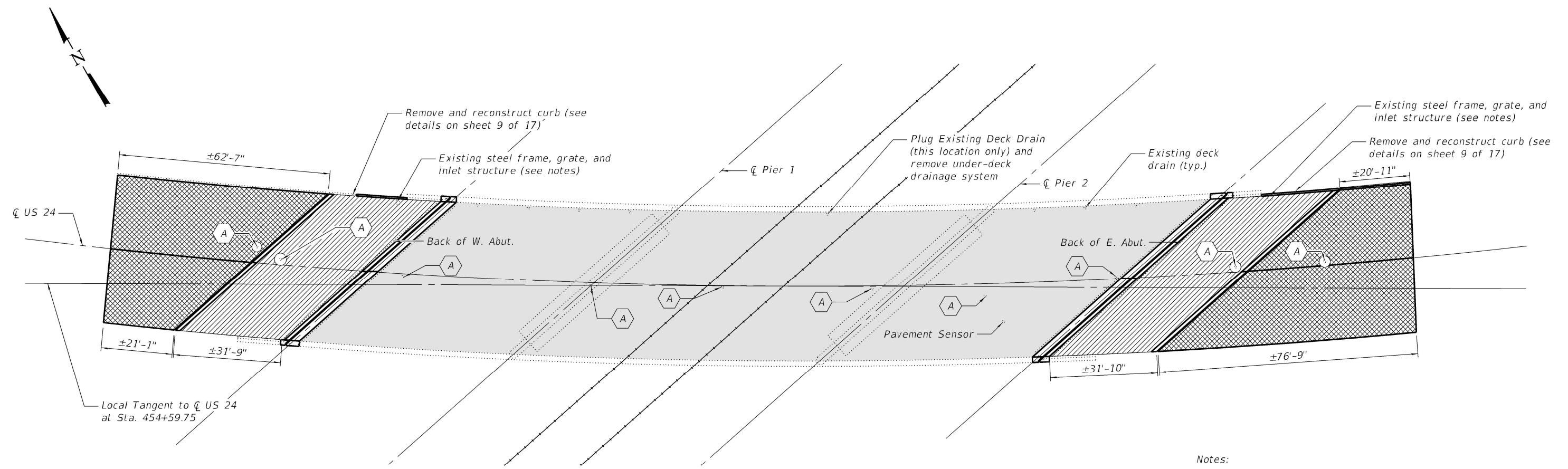
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SUPERSTRUCTURE TYPICAL SECTION
STRUCTURE NO. 102-0046
 SHEET 7 OF 17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(18-2VB)BRR	WOODFORD	39	21
CONTRACT NO. 68H23				
ILLINOIS FED. AID PROJECT				

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SUPERSTRUCTURE REPAIR PLAN

Notes:

See Roadway Plans for locations of riprap slope swales in the northwest and northeast quadrants.

Locations of deicing nozzles and pavement sensor are approximate.

Deicing nozzles, pavement sensor, flexible tubing, and pipe/conduits slotted into the approaches shall be removed. Removal of these items will be paid for in accordance with Article 109.04 of the Standard Specifications. The full depth 1" holes shall be cleaned and filled with high strength non shrink grout and allowed to fully cure prior to deck scarification. The 2" deep nozzle cavity will be filled monolithically with the microsilica concrete overlay.

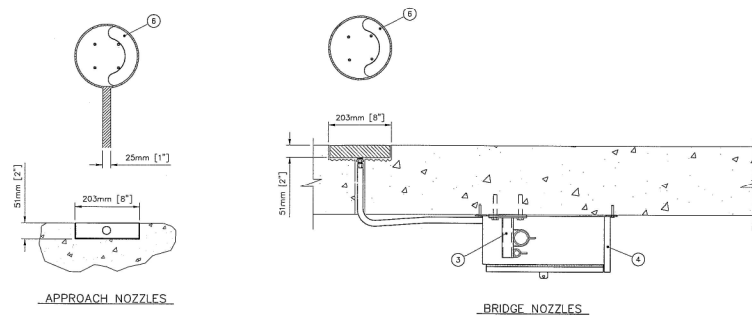
Contractor shall remove existing steel frame and grates from the locations indicated and fill the inlets with PCC before scarification. Contractor shall scarify the PCC and place microsilica overlay over the top. Payment for removing frame and grate and filling the inlet with concrete shall be completely covered by the unit price for Filling Drainage Structures (per each).

PCC Pavement Connector replacement shall be in accordance with Standard 420401.

The elevation of the proposed PCC Pavement Connectors shall be 1 1/2" above the existing PCC Pavement Connectors to match the elevation of the proposed Microsilica Concrete Overlay on the Approach Slabs.

LEGEND

- PCC Pavement Connector removal and replacement
- 3/4" Bridge Deck Scarification and 2 1/4" Microsilica Concrete Overlay on Approach Pavement
- 3/4" Bridge Deck Scarification and 2 1/4" Microsilica Concrete Overlay on Bridge Deck
- Deicing nozzle



ANTI-ICING NOZZLE TYPICAL DETAIL

See original Deicing System plans for keyed notes.

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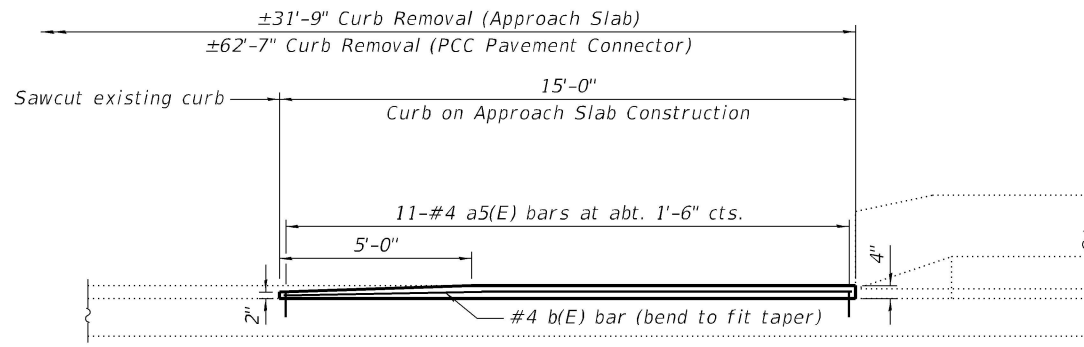
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 STRUCTURE NO. 102-0046

SHEET 8 OF 17 SHEETS

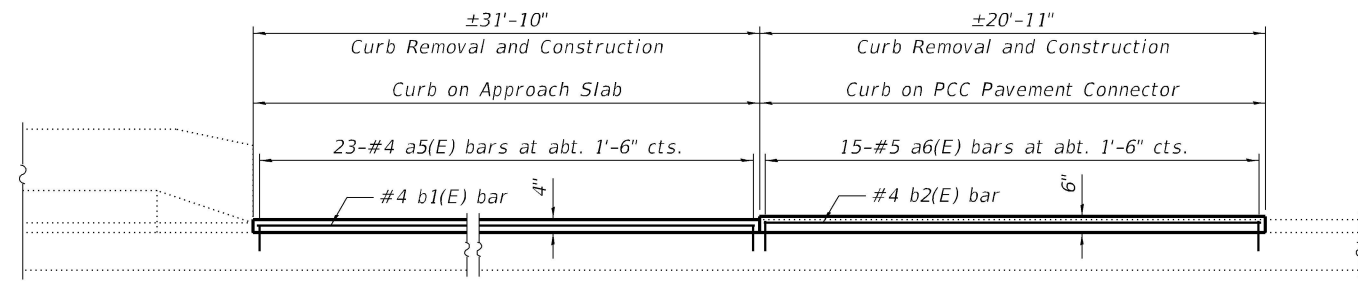
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317	(18-2VB)BRR	WOODFORD	39	22
CONTRACT NO. 68H23				
		ILLINOIS	FED. AID PROJECT	



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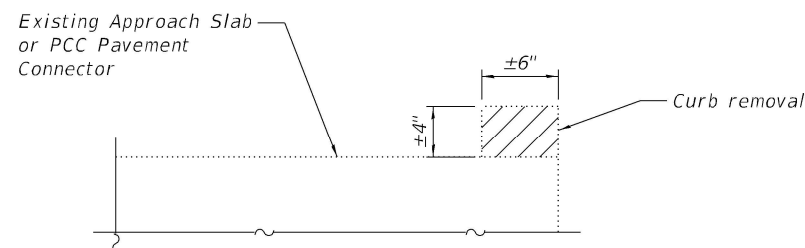


**INSIDE ELEVATION OF CURB AND PARAPET
AT NW CORNER OF BRIDGE**

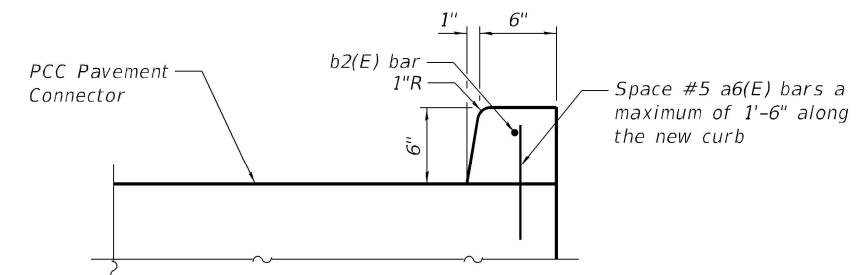


**INSIDE ELEVATION OF CURB AND PARAPET
AT NE CORNER OF BRIDGE**

Note:
Curb removal on Approach Slab paid as Concrete Removal (per cubic yard) and on PCC Pavement Connector as Curb Removal (per linear foot). Curb construction on Approach Slab paid as Concrete Superstructure (per cubic yard) and on PCC Pavement Connector as Concrete Shoulder Curb (per linear foot).



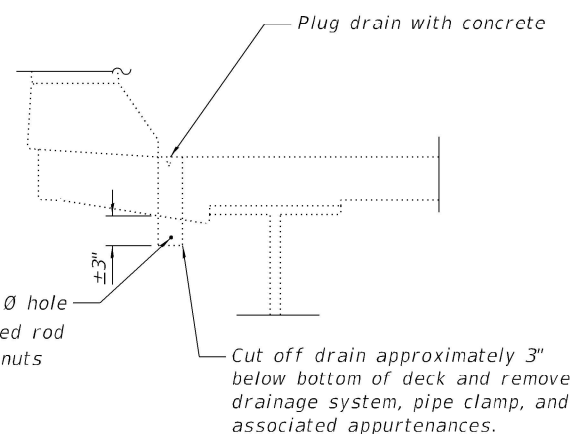
CURB REMOVAL DETAIL



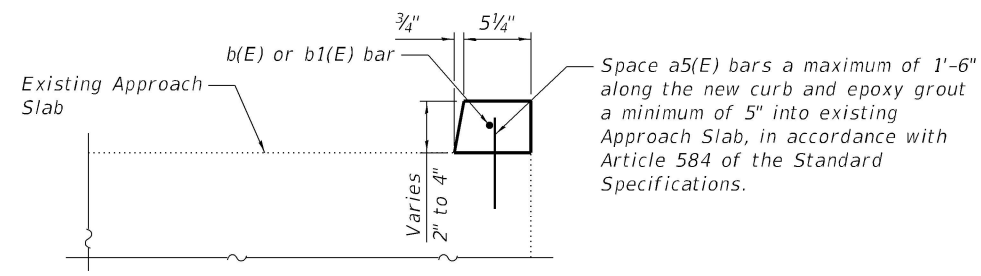
CURB ON PCC PAVEMENT CONNECTOR DETAIL

BILL OF MATERIAL

Bar	No.	Size	Length	Shape	
a5(E)	34	#4	0'-7"	—	
a6(E)	15	#5	1'-3"	—	
b(E)	1	#4	14'-8"	—	
b1(E)	1	#4	31'-6"	—	
b2(E)	1	#4	20'-7"	—	
Welded Wire Reinforcement				Sq. Yd.	243
Pavement Connector (PCC) for Bridge Approach Slab				Sq. Yd.	448
Pavement Removal				Sq. Yd.	449
Curb Removal				Foot	84
Tie Bars 1 1/4"				Each	95
Concrete Removal				Cu. Yd.	0.4
Concrete Superstructure				Cu. Yd.	0.3
Bridge Deck Grooving				Sq. Yd.	1,386
Reinforcement Bars, Epoxy Coated				Pound	80
Concrete Shoulder Curb				Foot	21
Bridge Deck Scarification 3/4"				Sq. Yd.	1,386
Bridge Deck Microsilica Concrete Overlay 2 1/4"				Sq. Yd.	1,386
Plug Existing Deck Drains				Each	1
Filling Drainage Structures				Each	2



FLOOR DRAIN PLUGGING DETAIL



CURB ON APPROACH SLAB DETAIL

Notes:
Cover hole in beam web from pipe clamp by placing 3/4"Ø high strength bolt with 2 washers and a locknut through the hole.
Cost of cutting off existing drain, removing existing drainage system and appurtenances, patching holes in the existing steel beam web, and plugging the drain with concrete shall be considered totally covered by the unit cost of Plug Existing Deck Drains (per Each).

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DEPARTMENT OF TRANSPORTATION**

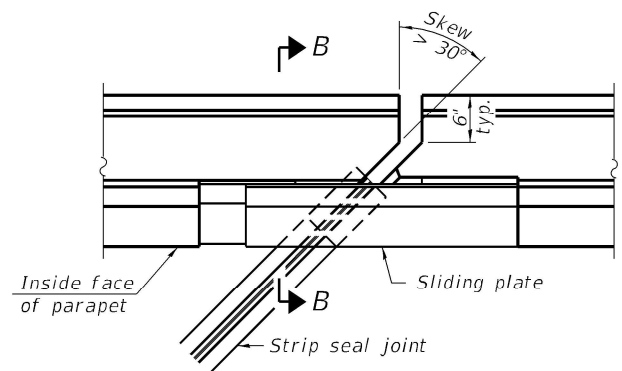
**SUPERSTRUCTURE REPAIR DETAILS
STRUCTURE NO. 102-0046**

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CONTRACT NO. 68H23				
ILLINOIS FED. AID PROJECT				

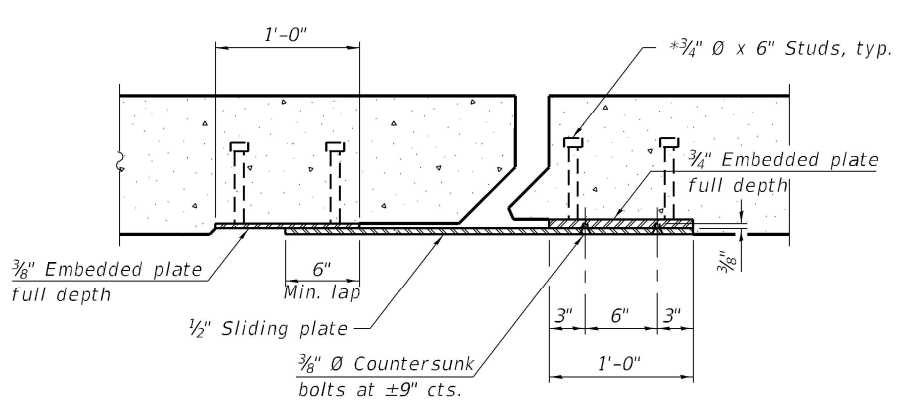
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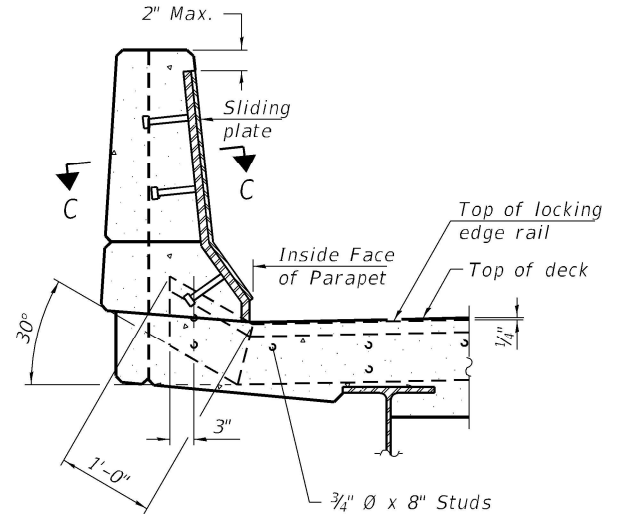
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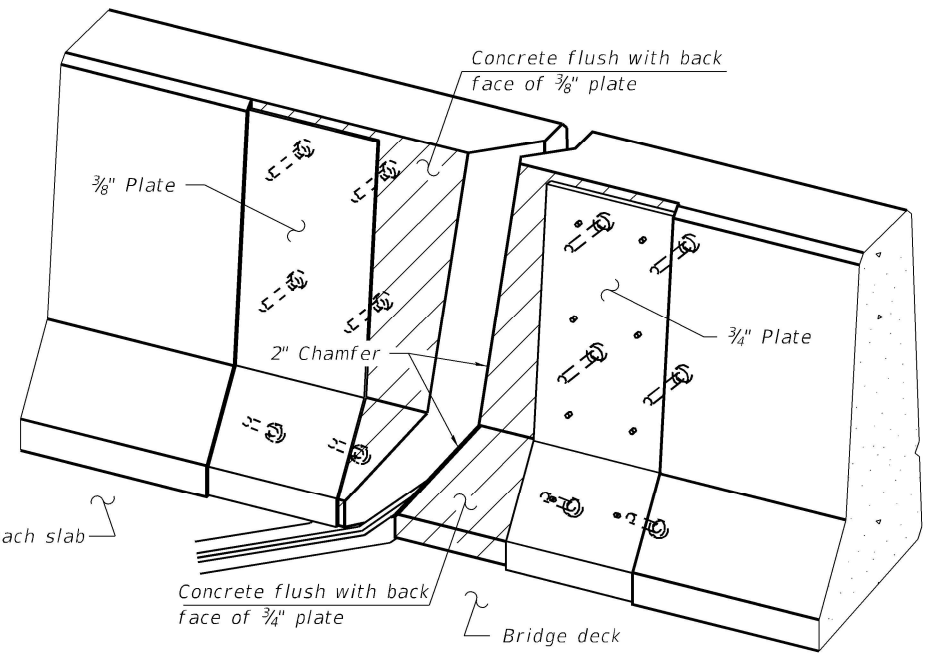
PLAN
(For skews > 30°)
Showing point block



SECTION C-C



SECTION B-B



TRIMETRIC VIEW
(Showing back plates only)

Notes:
The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.

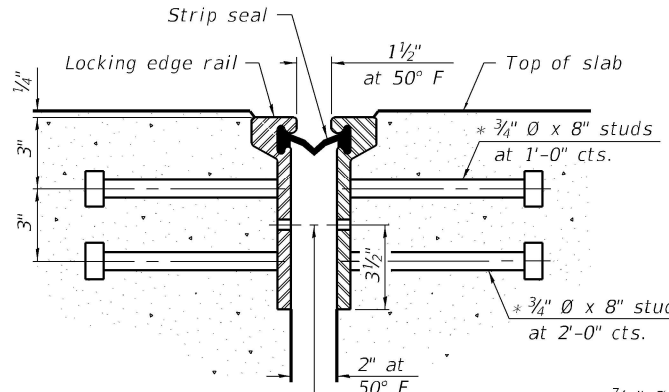
The manufacturer's recommended installation methods shall be followed.

The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.

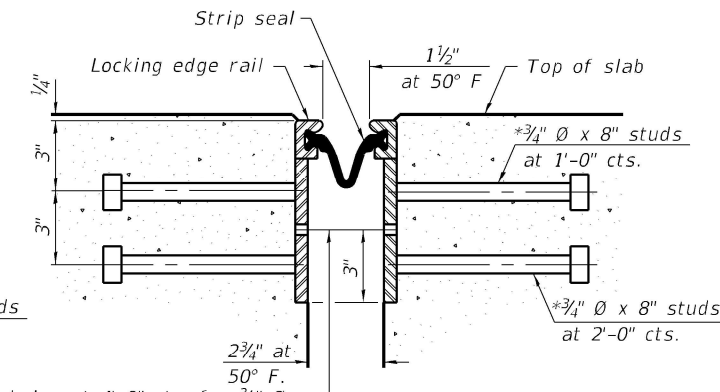
All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

Maximum space between rail segments shall be 1/16", sealed with a suitable sealant. Joints in rails within 10 ft. of curbs shall be welded.

Parapet plates and anchorage studs for skews > 30° included in the cost of Preformed Joint Strip Seal.



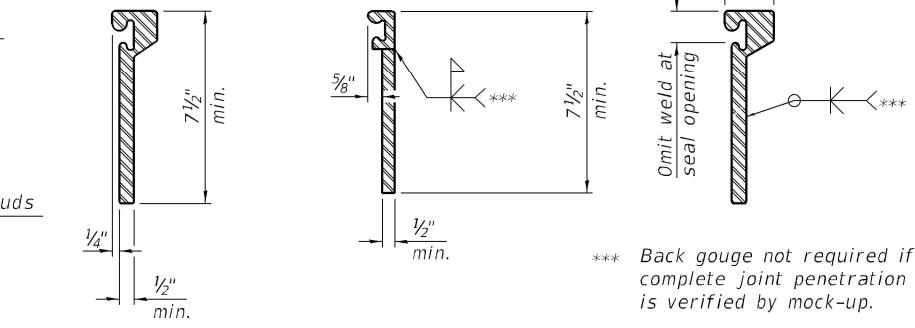
SECTION THRU ROLLED RAIL JOINT



SECTION THRU WELDED RAIL JOINT

1/16" Ø holes at 4'-0" cts. for 3/8" Ø bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

1/16" Ø holes at 4'-0" cts. for 3/8" Ø bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.



ROULDED EXTRUDED RAIL **WELDED RAIL**

LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue.
Rolled rail shown, welded rail similar.

LOCKING EDGE RAILS

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	137

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

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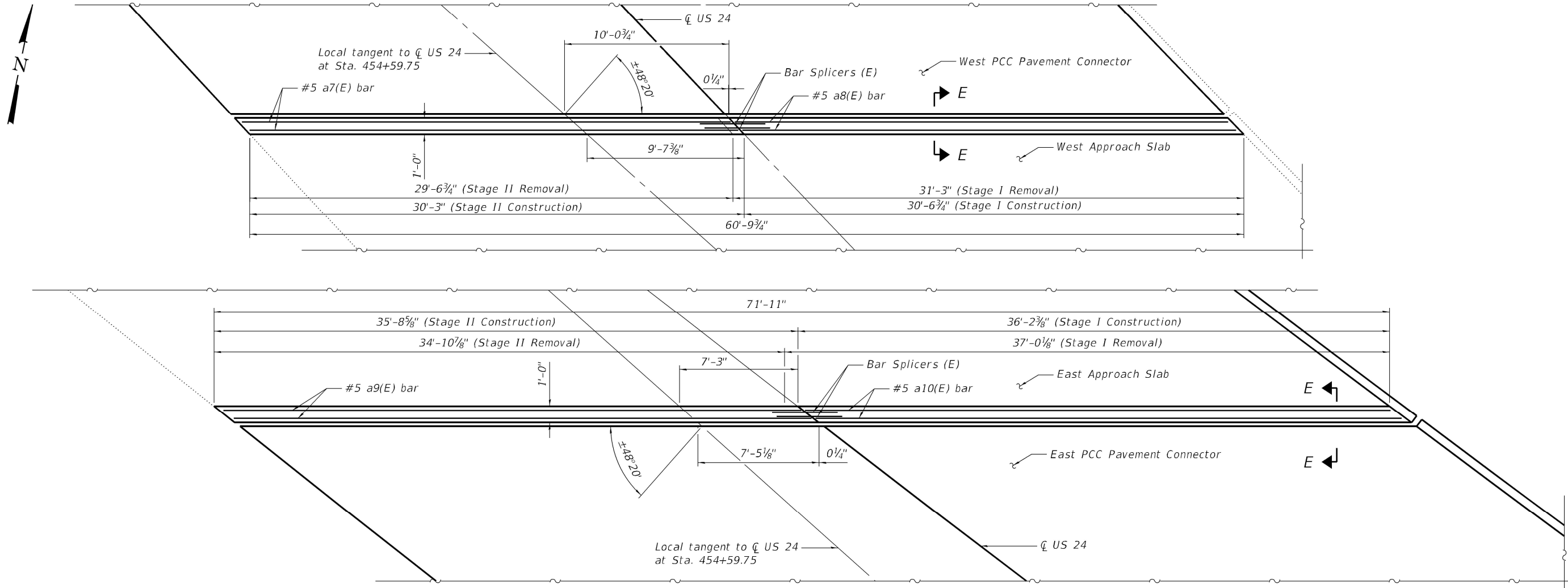
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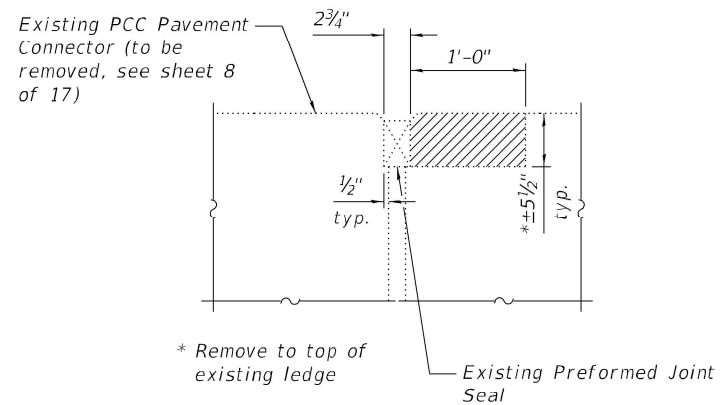
PREFORMED JOINT STRIP SEAL
STRUCTURE NO. 102-0046

SHEET 10 OF 17 SHEETS

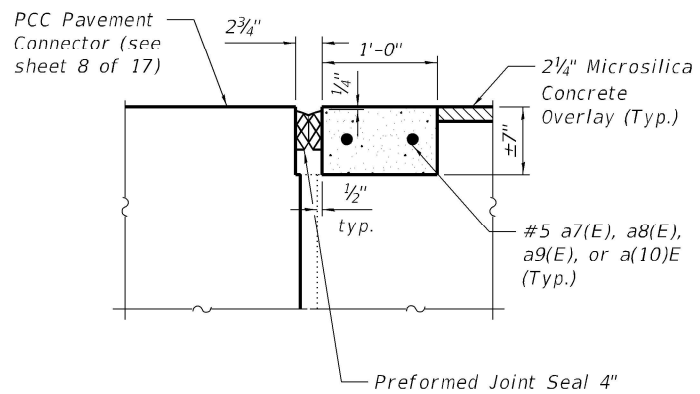
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CONTRACT NO. 68H23				
ILLINOIS FED. AID PROJECT				



PLAN SHOWING REMOVAL AND CONSTRUCTION



SECTION E-E
Showing removal



SECTION E-E
Showing construction

LEGEND

- Concrete Removal
- Concrete Superstructure
- Microsilica Concrete Overlay

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a7(E)	2	#5	29'-10"	—
a8(E)	2	#5	30'-2"	—
a9(E)	2	#5	35'-4"	—
a10(E)	2	#5	35'-9"	—
Concrete Removal			Cu. Yd.	2.3
Concrete Superstructure			Cu. Yd.	2.9
Reinforcement Bars, Epoxy Coated			Pound	280
Preformed Joint Seal 4"			Foot	133

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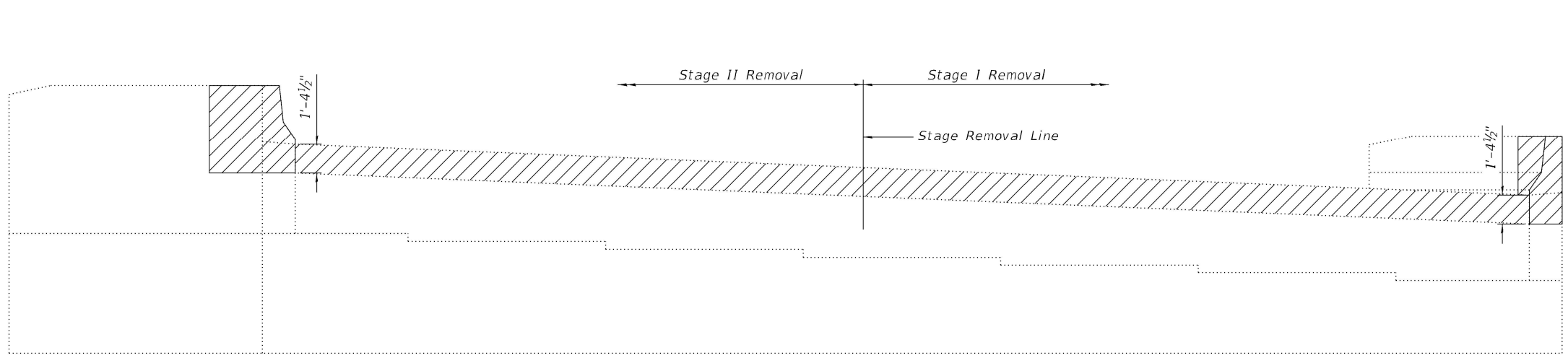
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

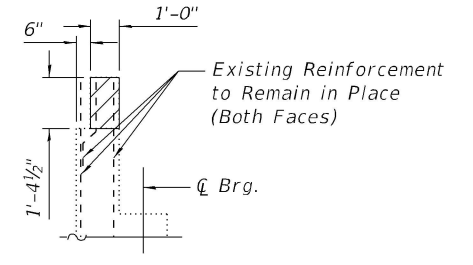
PAVEMENT EXPANSION JOINT REPLACEMENT PLAN AND DETAILS
STRUCTURE NO. 102-0046

F.A.P. RTE. 317	SECTION (18-2VB)BRR	COUNTY WOODFORD	TOTAL SHEETS 39	SHEET NO. 25
CONTRACT NO. 68H23				
ILLINOIS FED. AID PROJECT				

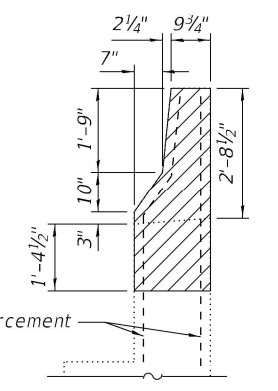
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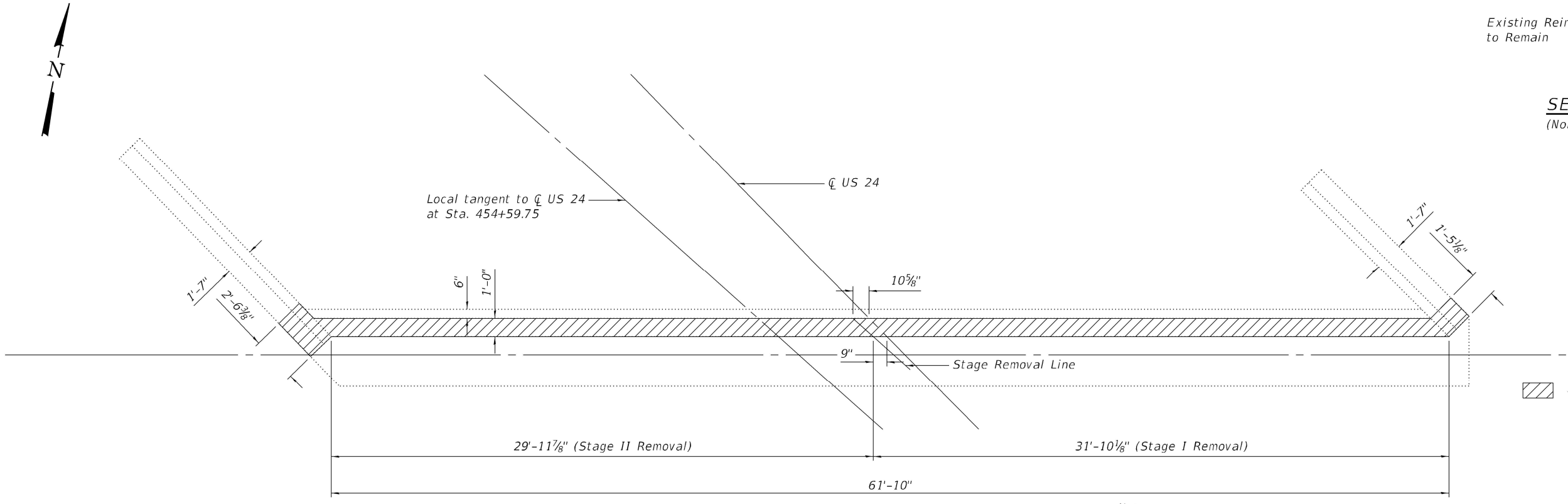
ELEVATION



SECTION THRU ABUTMENT



SECTIONS THRU WING WALL
 (North Wing Wall Shown, South Similar)



PLAN

- Indicates Limits of Concrete Removal

Notes:
 Existing reinforcement bars extending into the removal area shall be cleaned, straightened, and incorporated into new construction. Any reinforcement bars that are damaged during concrete removal operations shall be replaced at the Contractor's expense.
 The Contractor shall use extreme care during concrete removal so as to not damage the existing steel beams. Any steel damaged during concrete removal operations shall be repaired at the Contractor's expense.

BILL OF MATERIAL

Item	Unit	Total
Concrete Removal	Cu. Yd.	4.0

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11/27/2024
 01.00013
 20-086266

USER NAME =	DESIGNED - MLC	REVISED -
CHECKED - MMC	REVISOR -	
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PLOT DATE =	CHECKED - MMC	REVISED -

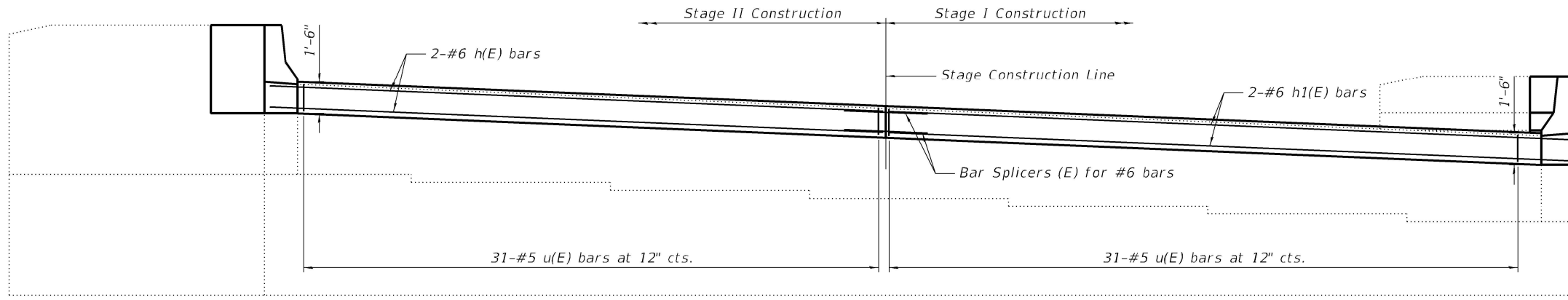
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

WEST ABUTMENT CONCRETE REMOVAL
STRUCTURE NO. 102-0046

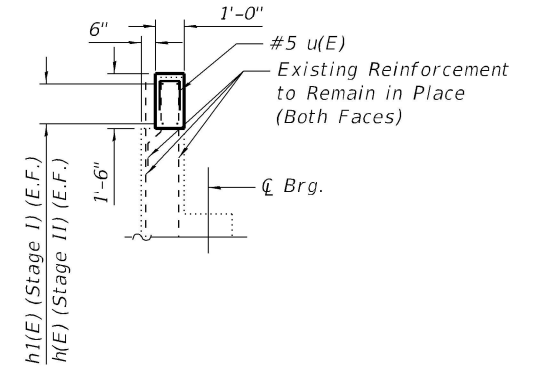
SHEET 12 OF 17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(18-2VB)BRR	WOODFORD	39	26
CONTRACT NO. 68H23				
ILLINOIS FED. AID PROJECT				

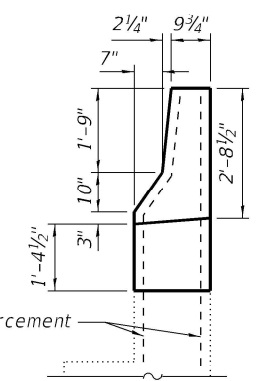
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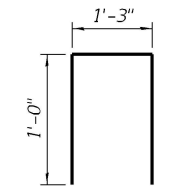
ELEVATION



SECTION THRU ABUTMENT



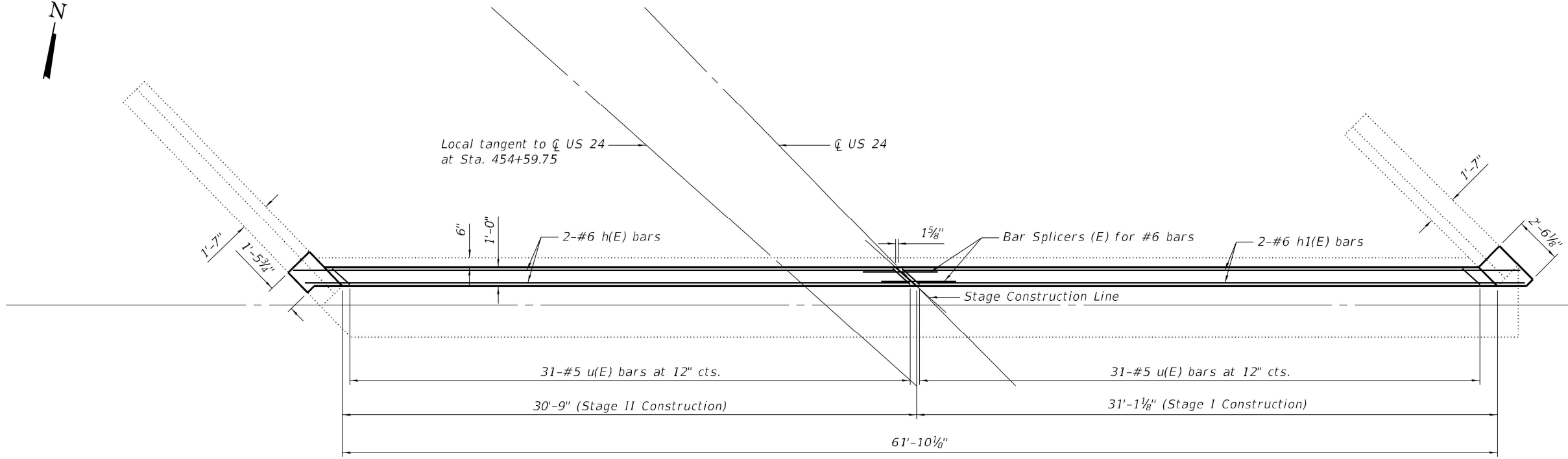
SECTIONS THRU WING WALL
 (North Wing Wall Shown, South Similar)



BAR u(E)

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h(E)	4	#6	30'-6"	—
h1(E)	4	#6	30'-10"	—
u(E)	62	#5	3'-3"	U
Concrete Superstructure			Cu. Yd.	4.3
Reinforcement Bars, Epoxy Coated			Pound	580



PLAN

Note:
 Cut h(E) and h1(E) bars to fit in the field.

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PLOT DATE =	CHECKED - MMC	REVISED -

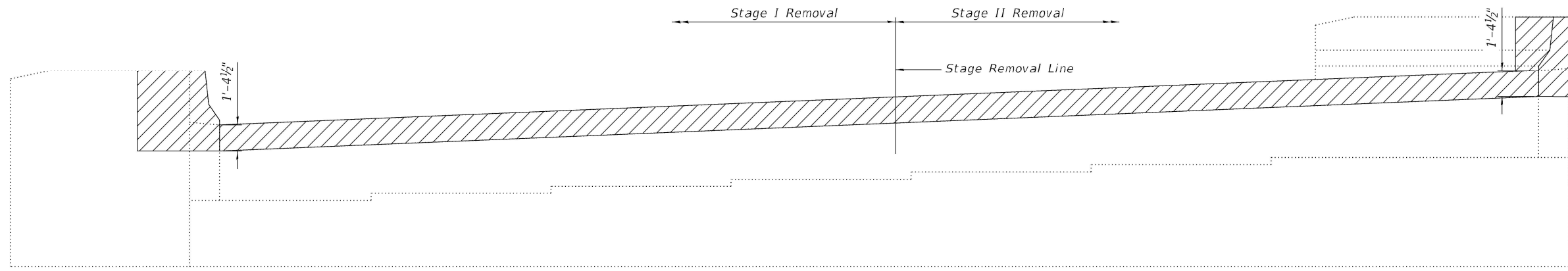
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

WEST ABUTMENT PLAN AND ELEVATION
STRUCTURE NO. 102-0046

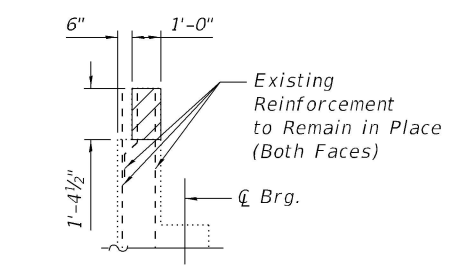
SHEET 13 OF 17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(18-2VB)BRR	WOODFORD	39	27
CONTRACT NO. 68H23				
ILLINOIS FED. AID PROJECT				

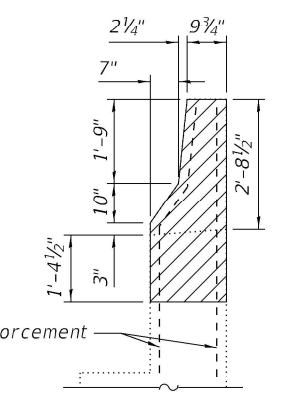
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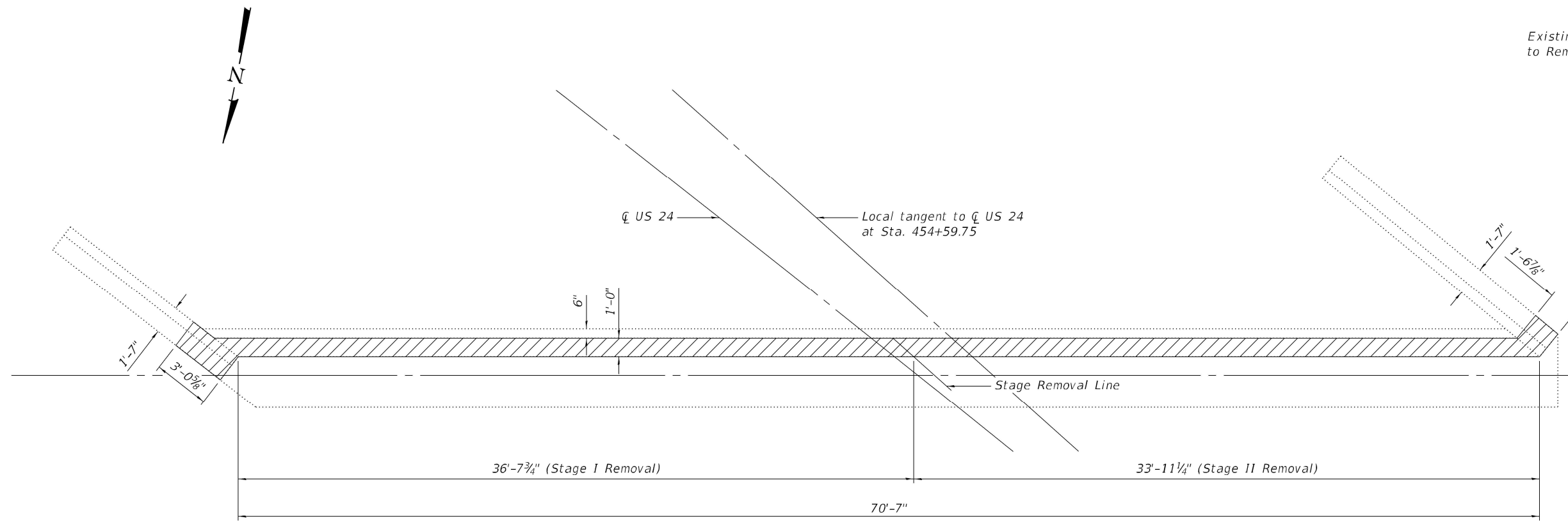
ELEVATION



SECTION THRU ABUTMENT



SECTIONS THRU WING WALL
 (South Wing Wall Shown, North Similar)



PLAN



- Indicates Limits of Concrete Removal

Note:
 Existing reinforcement bars extending into the removal area shall be cleaned, straightened, and incorporated into new construction. Any reinforcement bars that are damaged during concrete removal operations shall be replaced at the Contractor's expense.
 The Contractor shall use extreme care during concrete removal so as to not damage the existing steel beams. Any steel damaged during concrete removal operations shall be repaired at the Contractor's expense.

BILL OF MATERIAL

Item	Unit	Total
Concrete Removal	Cu. Yd.	4.5

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12/27/2013
 01/06/2014
 20-088054

USER NAME =	DESIGNED - MLC	REVISED -
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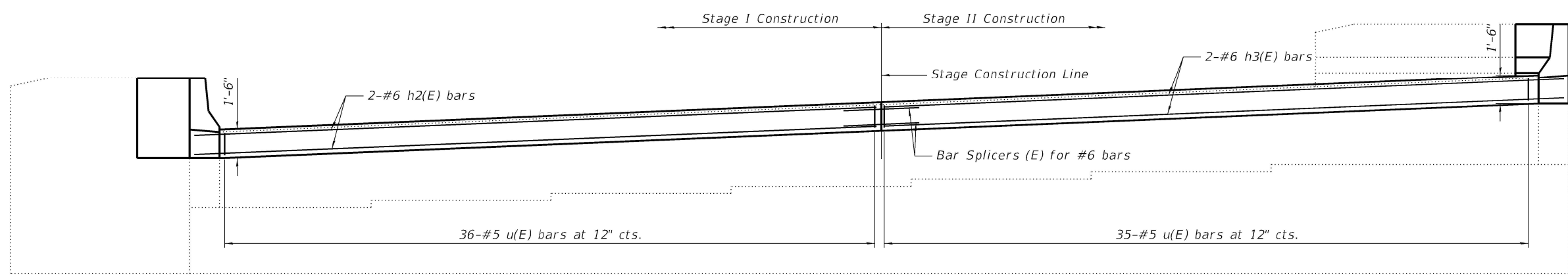
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EAST ABUTMENT CONCRETE REMOVAL
STRUCTURE NO. 102-0046

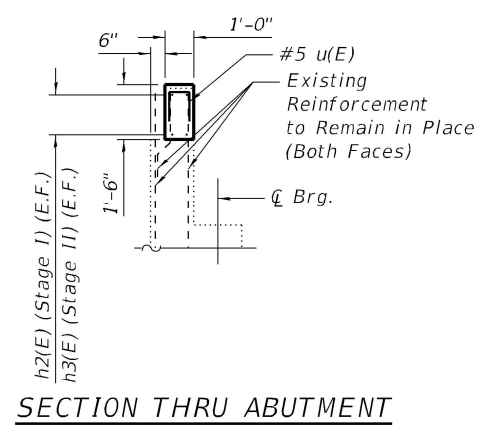
SHEET 14 OF 17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(18-2VB)BRR	WOODFORD	39	28
CONTRACT NO. 68H23				
ILLINOIS FED. AID PROJECT				

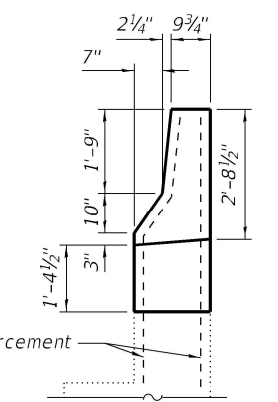
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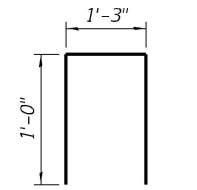
ELEVATION



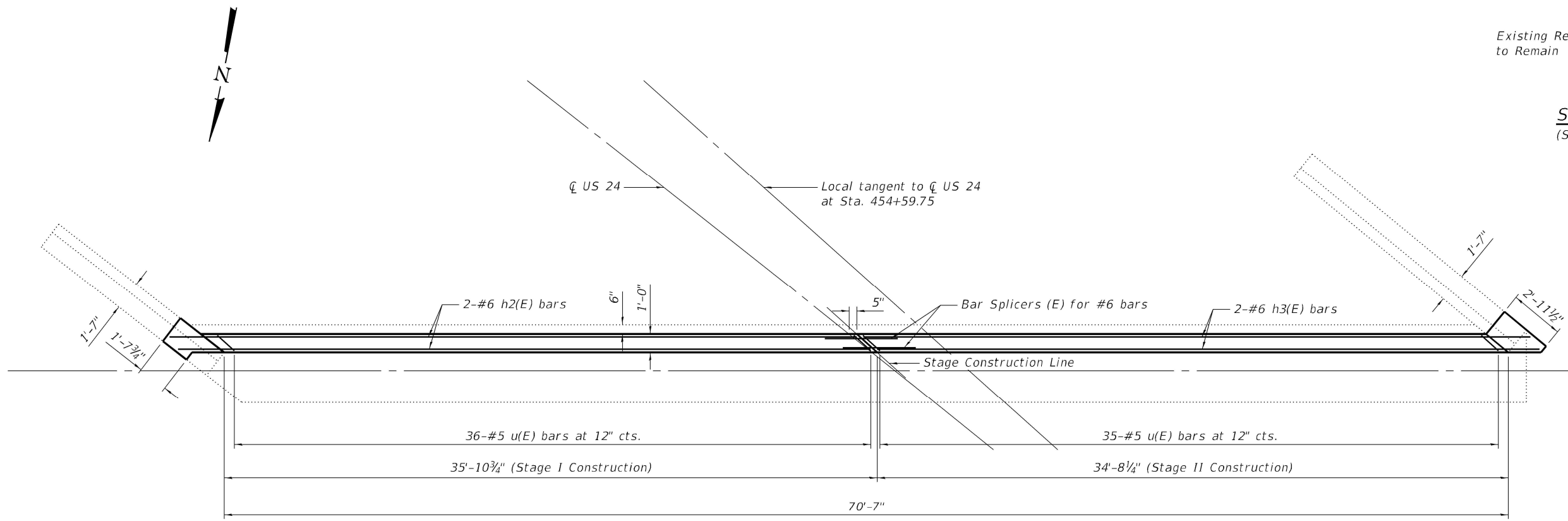
SECTION THRU ABUTMENT



SECTIONS THRU WING WALL
 (South Wing Wall Shown, North Similar)



BAR u(E)



PLAN

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h2(E)	4	#6	35'-8"	—
h3(E)	4	#6	34'-5"	—
u(E)	71	#5	3'-3"	□
Concrete Superstructure			Cu. Yd.	4.8
Reinforcement Bars, Epoxy Coated			Pound	670

Note:
 Cut h2(E) and h3(E) bars to fit in the field.

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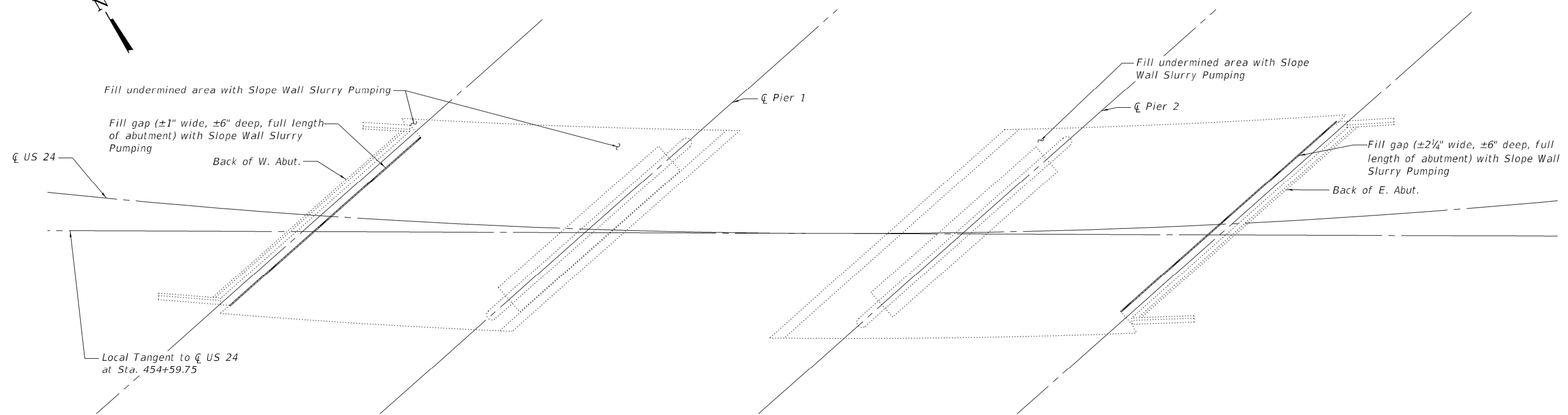
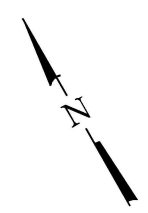
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	CHECKED - MMC	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EAST ABUTMENT PLAN AND ELEVATION
STRUCTURE NO. 102-0046

SHEET 15 OF 17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(18-2VB)BRR	WOODFORD	39	29
CONTRACT NO. 68H23				
ILLINOIS FED. AID PROJECT				



SLOPE WALL PLAN

Note:
Locations of slurry pumping are approximate. Actual locations and limits will be determined by the Engineer in the field.

BILL OF MATERIAL

Item	Unit	Quantity
Slope Wall Slurry Pumping	Cu. Yd.	7

MODEL: Default
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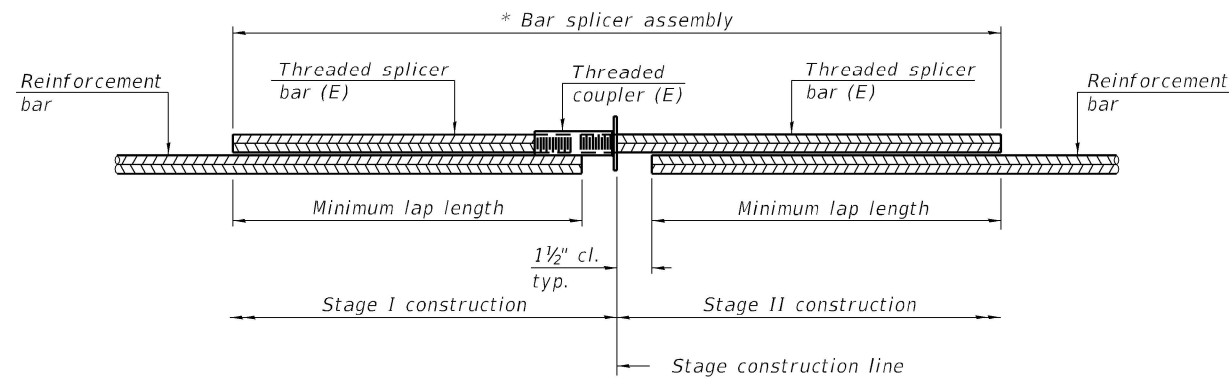
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PLOT DATE =	CHECKED - MMC	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SLOPE WALL REPAIR PLAN
STRUCTURE NO. 102-0046**

SHEET 16 OF 17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(18-2VB)BRR	WOODFORD	39	30
CONTRACT NO. 68H23				
ILLINOIS		FED. AID PROJECT		



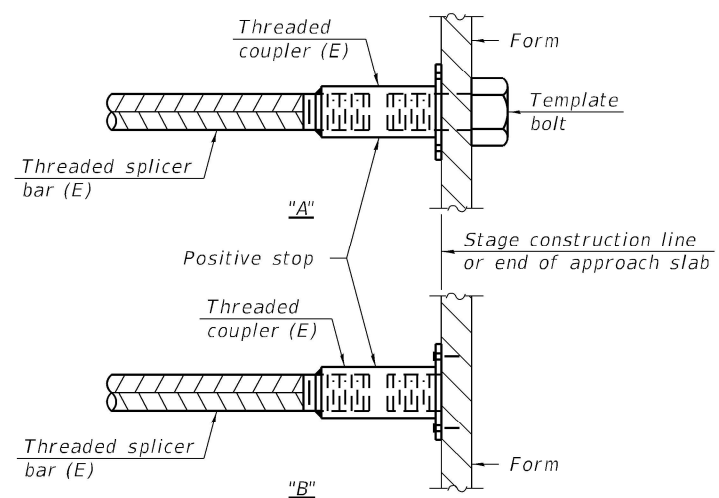
STANDARD BAR SPLICER ASSEMBLY PLAN

Only bar splicer assemblies as presented on the approved QPL list may be used.

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
West Abut.	#6	4	4'-0"
East Abut.	#6	4	4'-0"
Deck	#5	20	3'-6"
Pavement Expansion Joints	#5	4	3'-6"

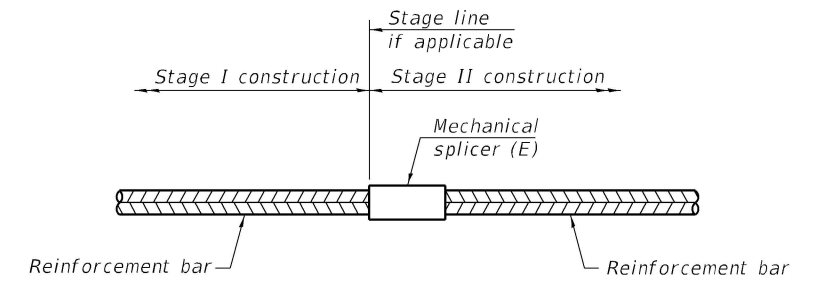


INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.

"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required

Notes:

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars.

Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

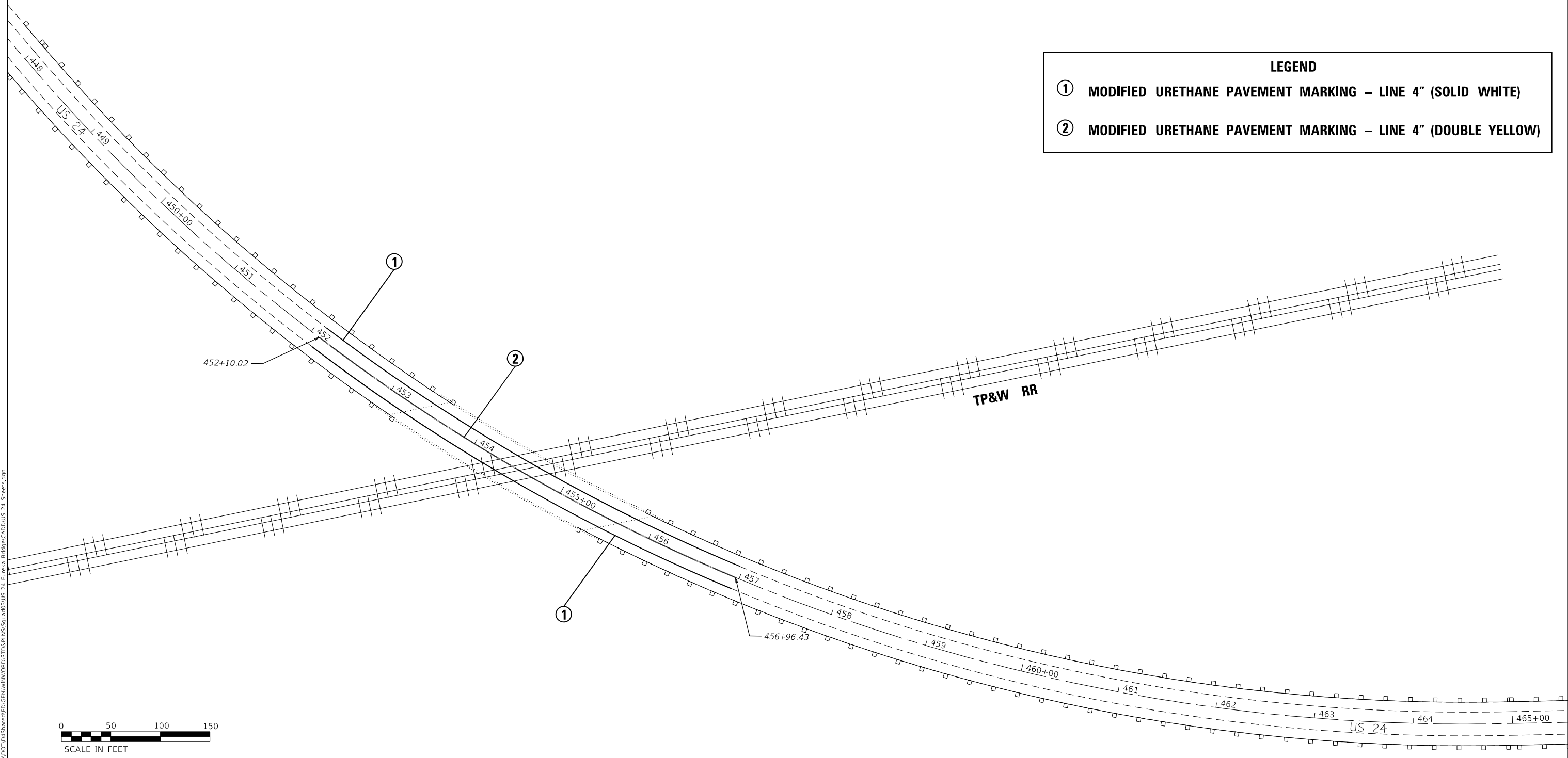
BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
 STRUCTURE NO. 102-0046

SHEET 17 OF 17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(18-2VB)BRR	WOODFORD	39	31
CONTRACT NO. 68H23				
ILLINOIS FED. AID PROJECT				



LEGEND	
①	MODIFIED URETHANE PAVEMENT MARKING – LINE 4" (SOLID WHITE)
②	MODIFIED URETHANE PAVEMENT MARKING – LINE 4" (DOUBLE YELLOW)



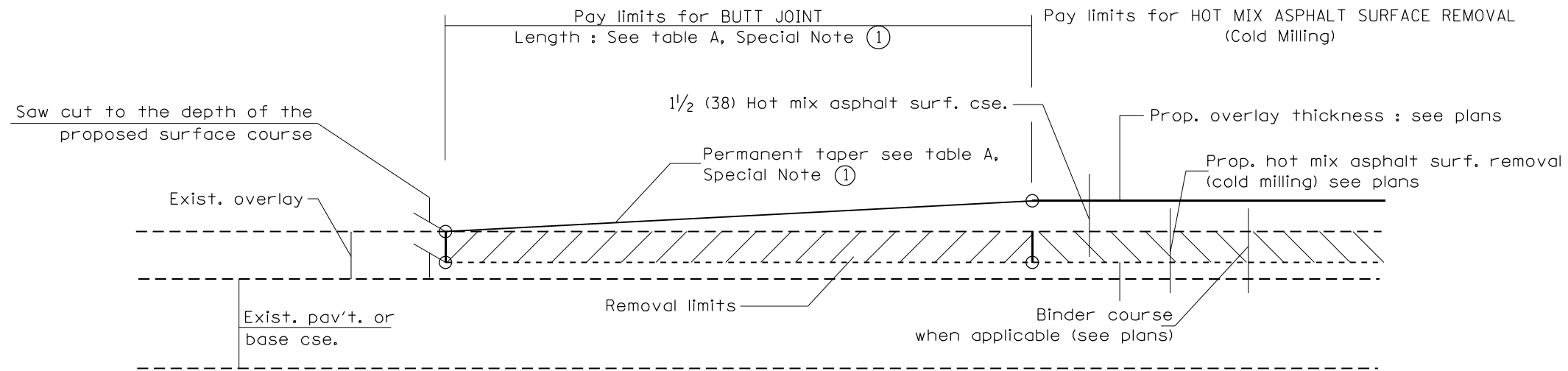
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PLOT DATE = 12/15/2023	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING PLAN S.N. 102-0046			
SCALE: 1"=100'	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(18-2VB)BRR	WOODFORD	39	32
ILLINOIS FED. AID PROJECT			CONTRACT NO. 68H23	



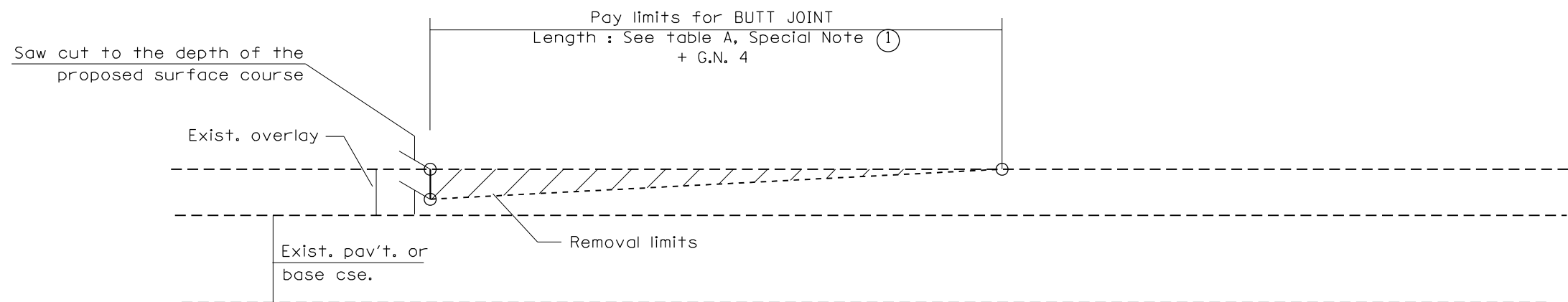
CASE 1 : WITH HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

**TABLE A
TAPER RATES**

SPECIAL NOTE NUMBER	ELEMENT	MAINLINE INTERSTATES & 4-LANE EXPRESSWAYS	ALL OTHERS
①	BUTT JOINT TAPER RATE	1:480	1:240
②	TEMPORARY RAMP TAPER RATE	1:80	1:40

GENERAL NOTES

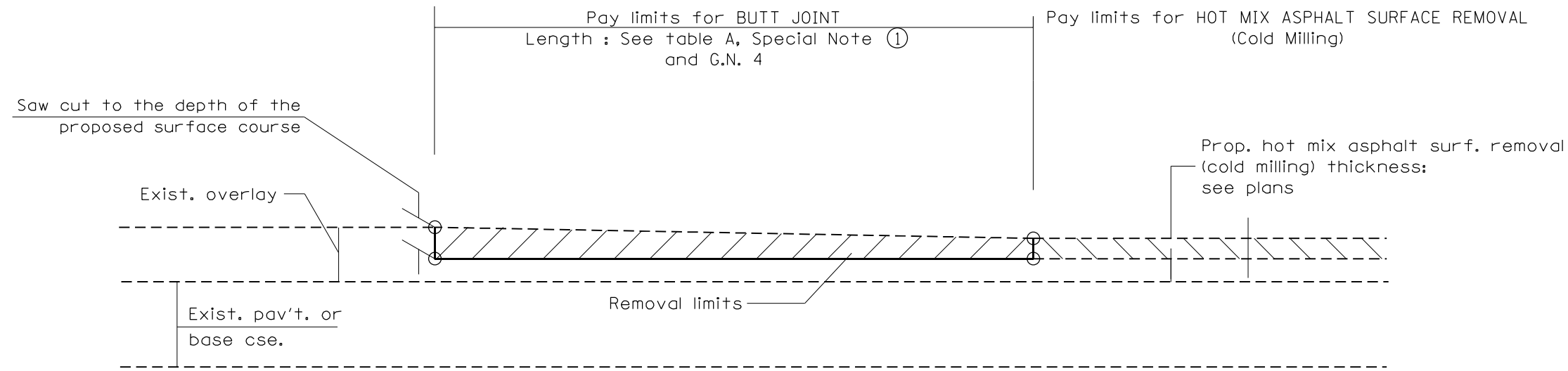
1. The work shall be done in accordance with Article 406.08 and the Special Provision for Butt Joints.
2. The pavement surface to be removed may be either bituminous or P.C. concrete. The work shall be performed in accordance with Article 440.04 and the Special Provisions for Butt Joints.
3. The saw cut joints shall be primed just prior to the placing of bituminous material. The work will be in accordance with the applicable portions of Article 406.05.
4. The length of butt joint is based on the taper rate times change in cold milling depth within the butt joint pay limits, unless otherwise indicated.
5. Temporary ramps are paid for separately and not included in the cost of the butt joints.



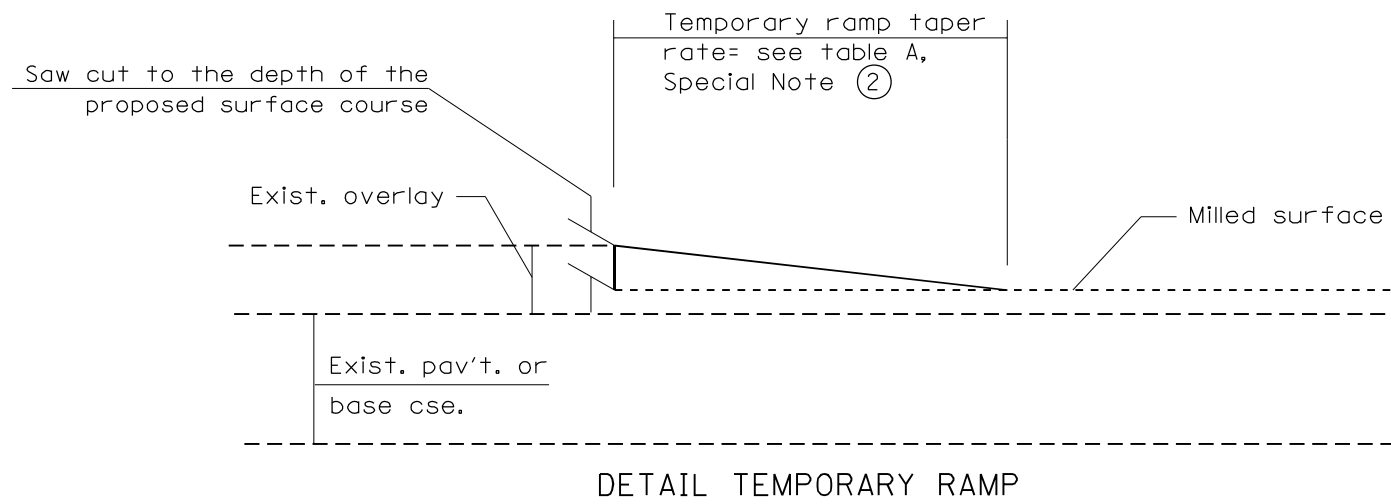
CASE 2 : NO HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

All dimensions are in inches (millimeters) unless otherwise noted.

01-01-97	RENUM. C-23.01, NEW REVISION BOX	T.P.	08-21-13	MAJOR MODIFICATIONS	R.D.	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINTS	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
04-01-97	CORRECTION TO DEPTH	J.A.	04-12-16	MINOR CORRECTIONS	R.D.			317	(12-2VB)BRR	WOODFORD	39	33
09-15-05	REVISED DESIGNER NOTE	M.M.A.	02-14-17	ADDED NOTE 5	R.D.			CONTRACT NO. 68H23				
10-16-06	REVISED TO 2007 SPEC.	M.A.	07-16-19	Wording and Spelling corrections	R.D.			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



CASE 3 : HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)
TIE-IN TO EXISTING BITUMINOUS TAPER

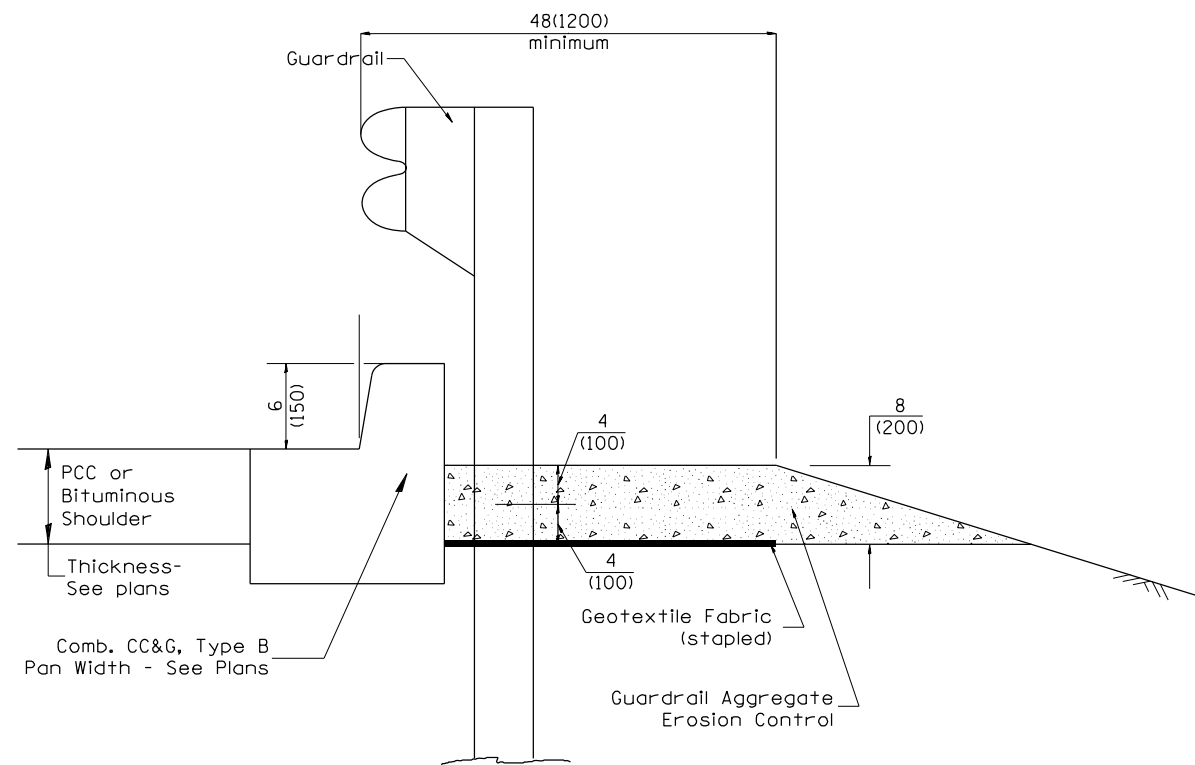


DETAIL TEMPORARY RAMP

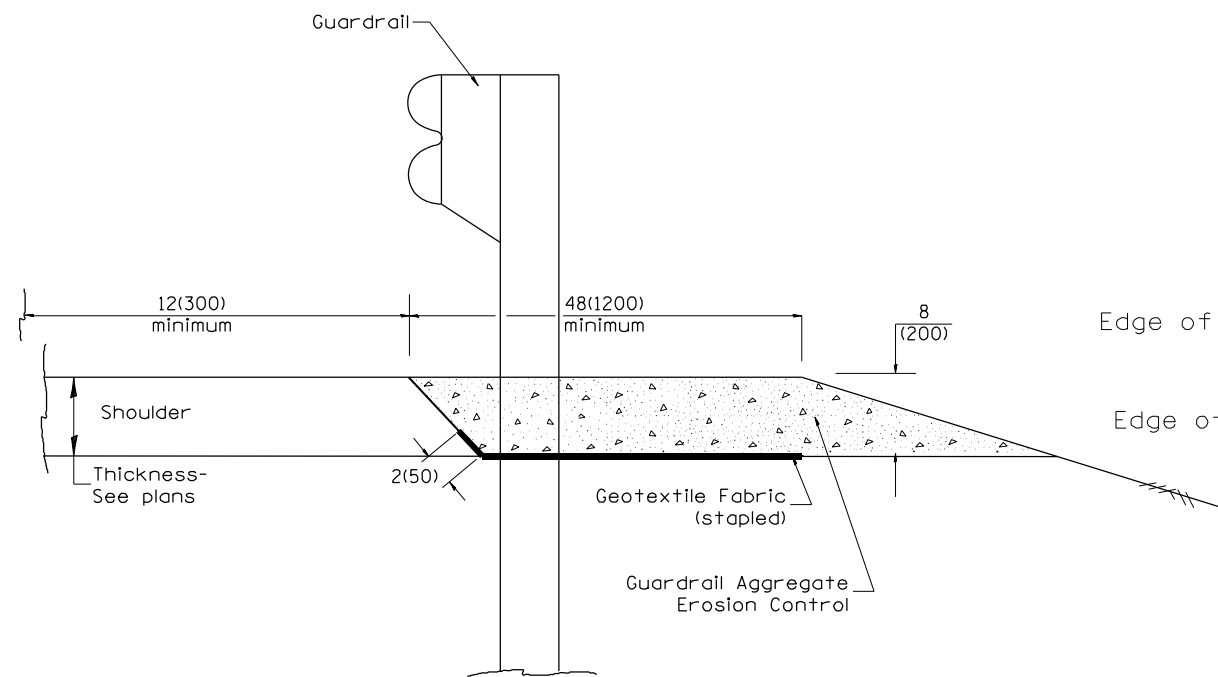
All dimensions are in inches (millimeters) unless otherwise noted.

				STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINTS	SHT. 2 OF 3 CADD STD. 406101-D4	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
							317	(12-2VB)BRR	WOODFORD	39	34
							CONTRACT NO. 68H23				
							FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

NOT TO SCALE



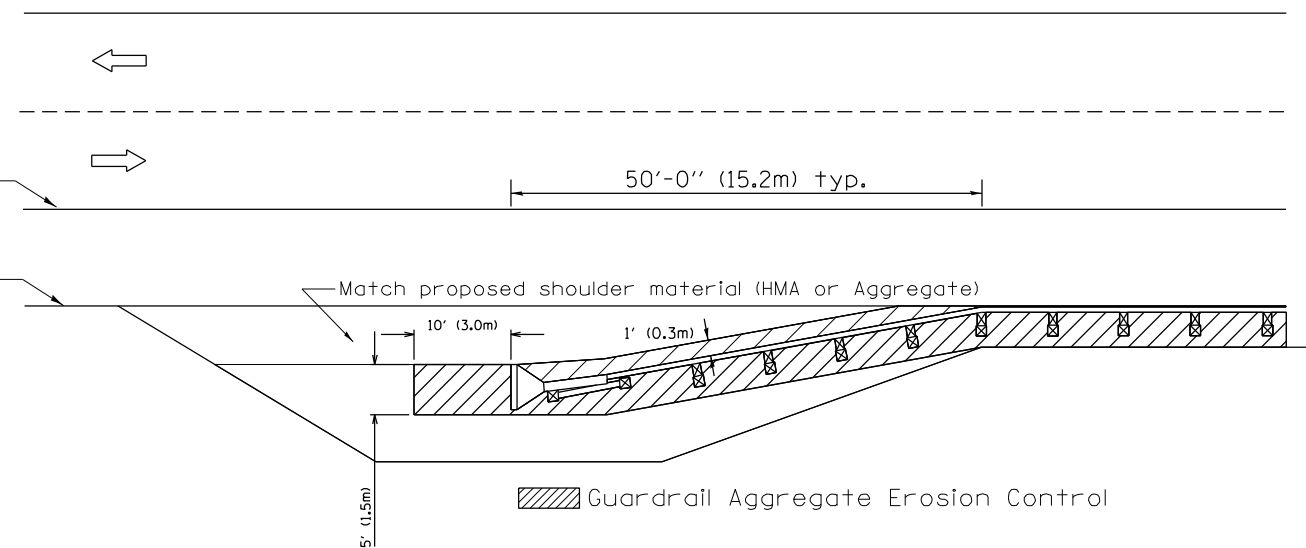
TYPICAL SECTION WITH COMBINATION CONCRETE CURB & GUTTER



TYPICAL SECTION WITHOUT EROSION CONTROL CURB

GENERAL NOTES: GUARDRAIL AGGREGATE EROSION CONTROL

1. This work shall consist of grading as needed, furnishing and installing geotextile fabric and staples, and furnishing, placing and shaping crushed aggregate around and behind Steel Plate Beam Guardrail posts in accordance with Plan Details.
2. Before placing the aggregate and the Geotextile Fabric, weeds and grass shall be removed from the area to be covered.
3. After the area has been prepared, and in a dry condition, the Geotextile fabric shall be placed with a 12(300) minimum overlap. A knife cut for guardrail post installation is necessary.
4. The aggregate shall be deposited, compacted and shaped by either mechanical or hand methods, in a manner reasonably true to line and grade.
5. The Contractor shall have the option of placing the guardrail before or after the Geotextile Fabric and Aggregate are in place. If the guardrail is placed after the Geotextile Fabric and Aggregate, then any voids must be filled and the aggregate returned to line and grade.
6. Materials shall meet the following requirements:
 - A. The crushed aggregate shall be CA1 gradation in accordance with Article 1004.01(c) of the Standard Specifications.
 - B. The Geotextile Fabric shall be nonwoven fabric in accordance with Article 1080.02 of the Standard Specifications.



All dimensions are in inches (millimeters) unless otherwise noted.

03-07-11	ADDED DETAIL SHOWING PLAN VIEW	R.D.	5-30-18	CHANGE B CURB TO CC&G	R.D.
08-10-12	REVISED CURB "B" AND AGGREGATE	R.D.	07-16-19	SPELLING CORRECTIONS	R.D.
07-15-15	ADDRESSED SHOULDER INLET CURB	R.D.			
01-26-17	REVISED	R.D.			

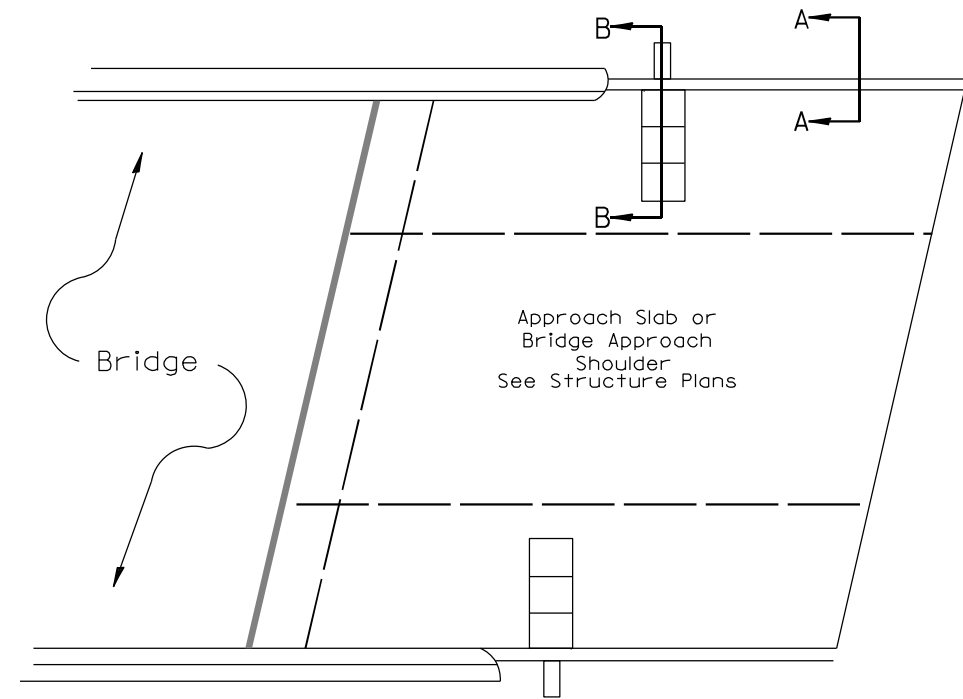
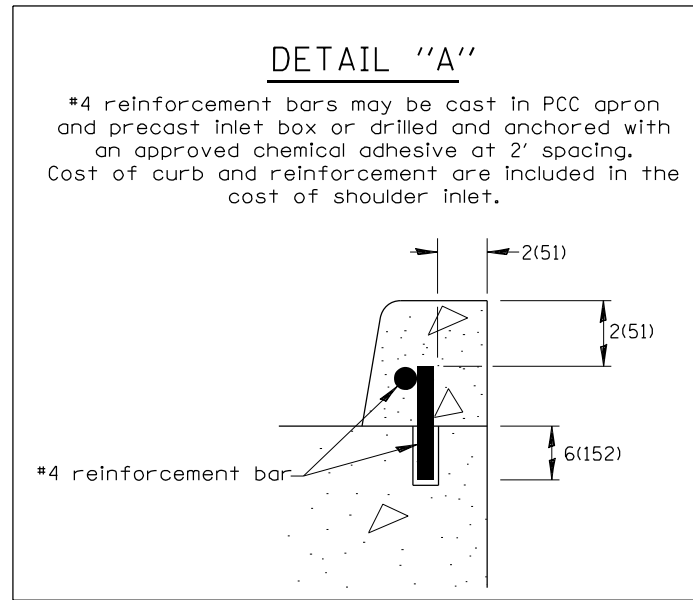
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

NOT TO SCALE

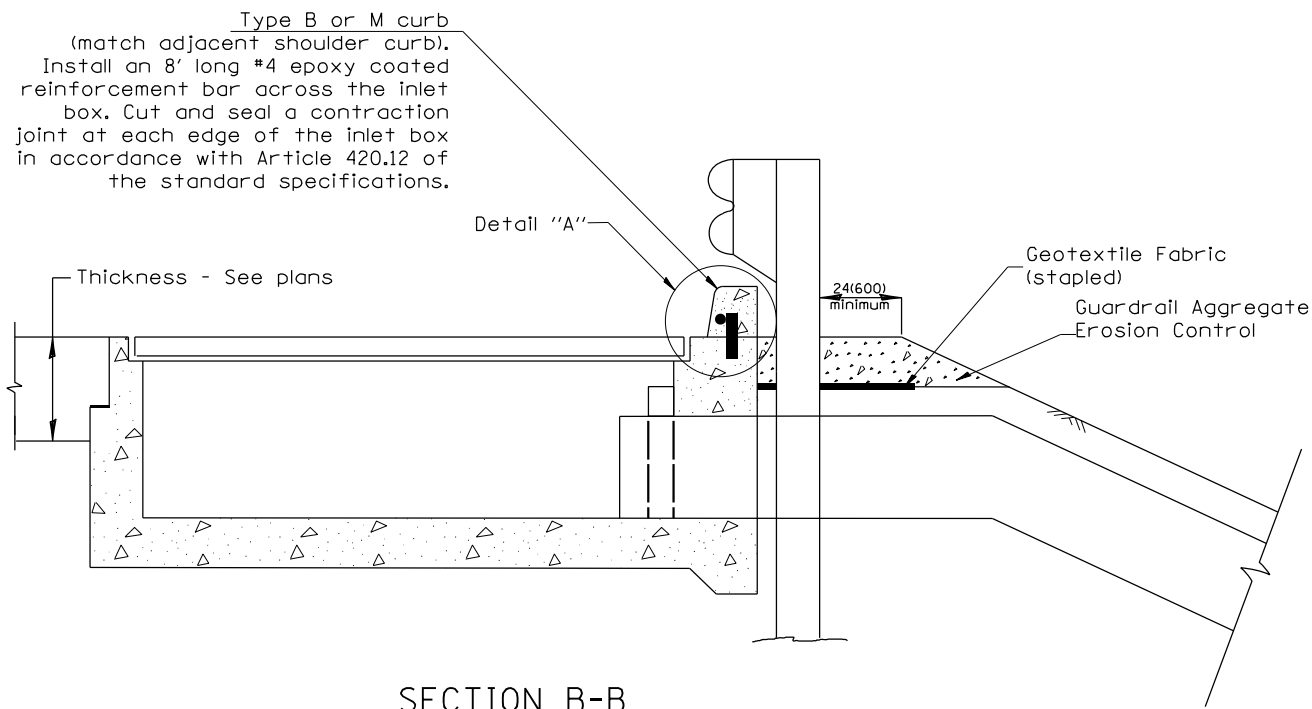
GUARDRAIL EROSION CONTROL TREATMENTS

SHT. 1 OF 2
CADD STD. 630101-D4

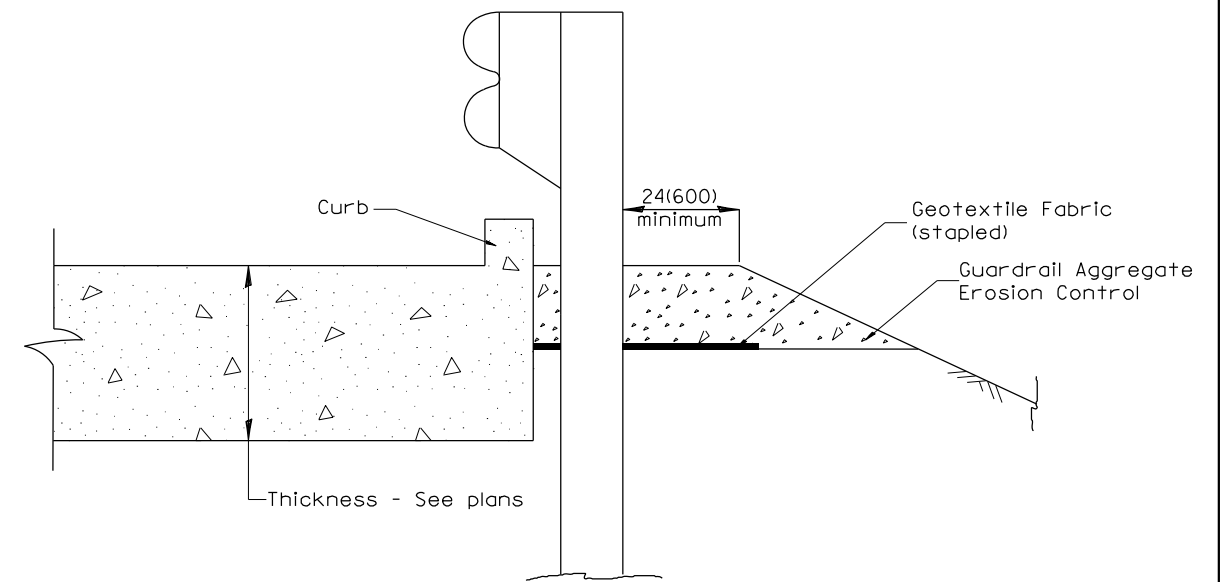
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	(12-2VB)BRR	WOODFORD	39	36
CONTRACT NO. 68H23				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



PLAN VIEW
APPROACH SLAB OR SHOULDER PLACEMENT



SECTION B-B
TYPICAL SECTION AT INLETS
TYPE E, F & G (HIGHWAY STANDARD 610001)

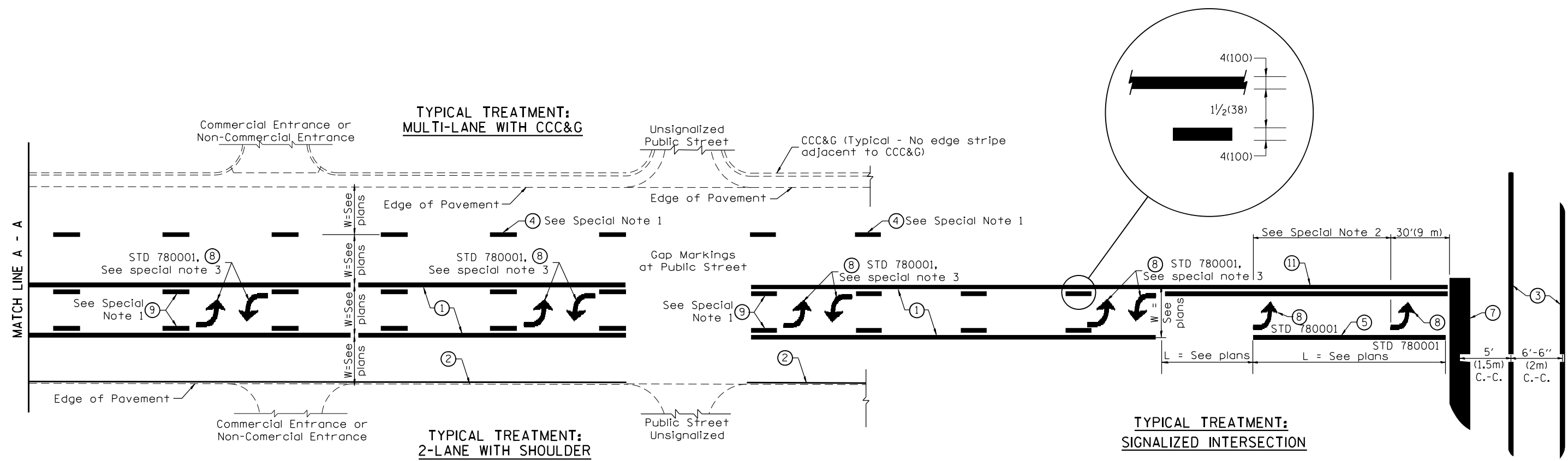


SECTION A-A
TYPICAL SECTION WITH BRIDGE APPROACH CURB

All dimensions are in inches (millimeters) unless otherwise noted.

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				GUARDRAIL EROSION CONTROL TREATMENTS				SHT. 2 OF 2 CADD STD. 630101-D4		F.A. RTE. 317		SECTION (12-2VB)BRR		COUNTY WOODFORD		TOTAL SHEETS 39		SHEET NO. 37	
										CONTRACT NO. 68H23									
										FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT									

NOT TO SCALE



FLUSH PAVED MEDIAN: TWO-WAY LEFT TURN LANE WITH ONE-WAY LEFT TURN LANE AT SIGNALIZED INTERSECTION

TYPICAL PAVEMENT MARKING LEGEND

(Note: This is a District Standard Legend. Some elements may not apply to specific project.)

- ① 4(100) Solid (Yellow)
- ② 4(100) Solid (White)
- ③ 2-6(150) Crosswalk @ 6'-6" (2m)min C.-C. (White)
2-8(200) Crosswalk @ 6'-6" (2m)min C.-C. (White) (When traffic signals are present.)
- ④ 6(150) Skip-Dash (White) (See Special Note 1)
- ⑤ 8(200) Solid (White)
- ⑥ 12(300) Diagonal (White) (Item ⑥ is shown on Std. 780001)
- ⑦ 24(600) Stop Bar (White)
- ⑧ Letters & Arrows (See Std. 780001 and Special Notes 2 & 3)
- ⑨ 4(100) Skip-Dash (Yellow) (See Special Note 1)
- ⑩ 12(300) Diagonal (Yellow) (See Table A) (See Table A)
- ⑪ 4(100) Double Solid (Yellow) (See Table A)

SPECIAL NOTES

1. Skip-Dash markings will be centered between both ends of city blocks and shall be placed in alignment transversely across the pavement.
2. The following shall apply to arrows located in one-way left turn lanes:
 - A. A minimum of two (2) arrows is required.
 - B. The maximum spacing between arrows is 80' (24 m).
 - C. Arrows shall be evenly spaced if three (3) or more are required.
3. The following shall apply to arrow pairs located in two-way left turn lanes:
 - A. A minimum of two (2) arrow pairs is required.
 - B. The maximum spacing between arrow pairs is 200' (61 m).
 - C. Arrow pairs shall be evenly spaced if three (3) or more are required.
 - D. The spacing between Bi Directional Left Turn Arrows is 33' (10 m).

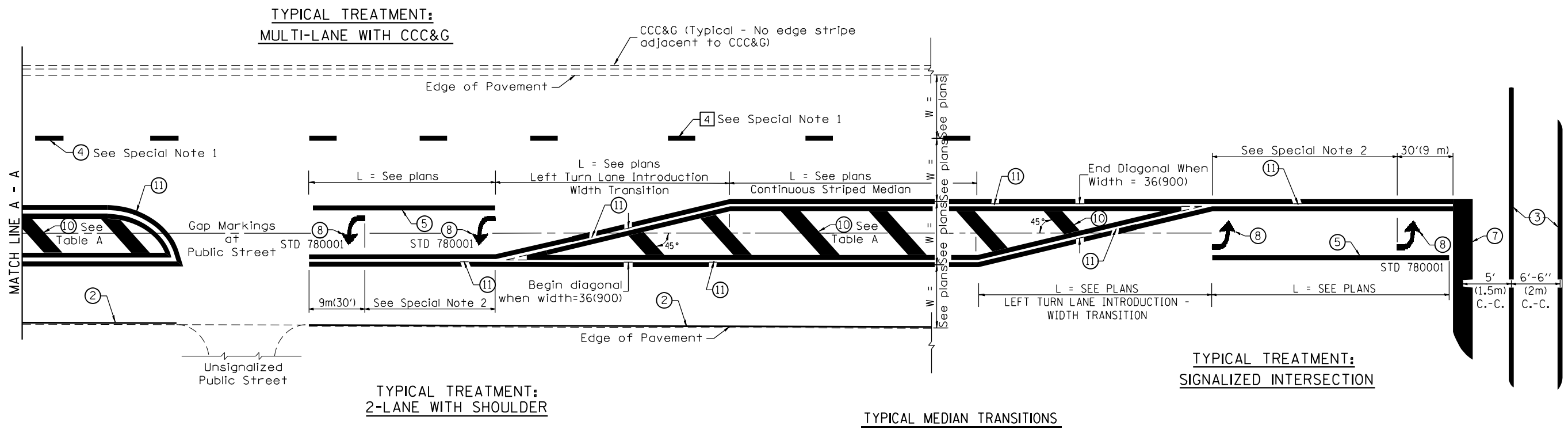
GENERAL NOTES

1. Refer to State Standard 780001 for additional Pavement Markings including letters & arrows.
2. See Plans for Pavement Markings adjacent to curbed islands and medians, and through lane reductions.
3. Refer to Article 780.13 for letter, number and symbol areas (sq. ft.)
4. Areas are grooved 1" beyond each edge for the following symbols:
Through Arrow= 14.8 sq. ft.
Large Left or Right Arrow= 21.9 sq. ft.
2 Arrow Combination Left (or Right) and Through= 34.9 sq. ft.
Wrong Way Arrow= 29.5 sq. ft.
Railroad Crossing Symbol= 69.8 sq. ft.
(For further information, refer to BDE Special Provision: Grooving for Recessed Pavement Markings)

01-01-97	RENUM. F-8.03, NEW REVISION BOX	T.P.	10-16-06	REVISED TO 2007 SPEC.		STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL PAVEMENT MARKINGS	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
02-07-97	ADD BI DIRECTIONAL DIMENSION	J.A.	2/29/16	ADDED GROOVING AREAS	R.D.			317	(12-2VB)BRR	WOODFORD	39	38
10-97	CORRECT BI DIRECTIONAL DIMENSION	J.A.	07-16-19	SPELLING CORRECTIONS	R.D.			CONTRACT NO. 68H23				
08-02	ADD CROSSWALK DMNS. WITH T.S.	M.A.						SHT. 1 OF 2 CADD STD. 780001-D4				

NOT TO SCALE

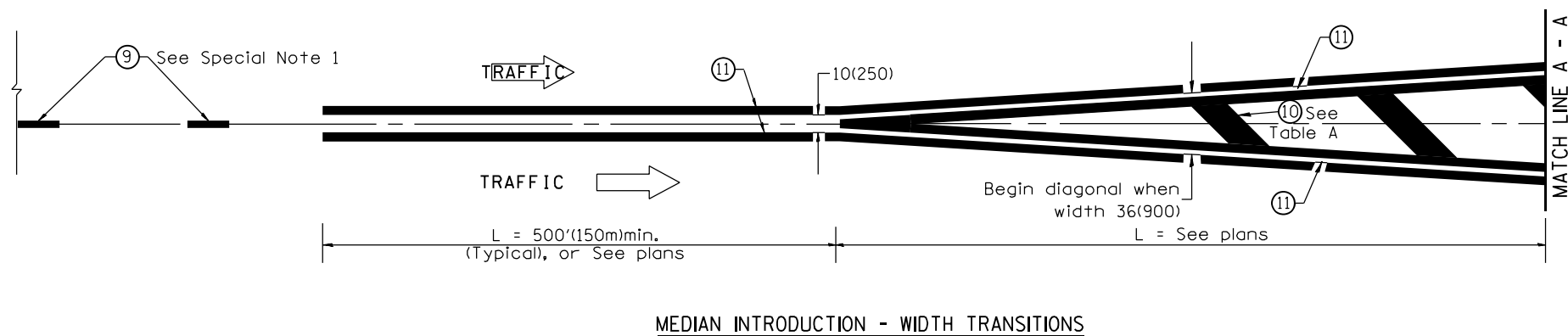
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT



FLUSH PAVED MEDIAN: RESTRICTED LEFT TURN LANE

TABLE A
RECOMMENDED SPACING BETWEEN DIAGONAL LINES

SPEED LIMIT RANGE	INTERSECTION CHANNELIZATION (Includes Width Transitions for Median and Left Turn Lane Introductions)	
	CONTINUOUS	
Less Than 30 mph (50 km/h)	50' (15m)	15' (5m)
30 - 45 mph (50 - 70 km/h)	75' (23m)	20' (6m)
Over 45 mph (70 km/h)	150' (46m)	30' (9m)



All dimensions are in inches (millimeters) unless otherwise noted.