

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
343	700-Y-B-R	COOK	283	156
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 62897

GENERAL NOTES

- Fasteners shall be high strength bolts. Bolts M22, open holes 24 mm ϕ , unless otherwise noted.
- Calculated weight of Structural Steel:
AASHTO (M270M GR 345) = 149,100 kg (Erection Only-Included in Beam Fabrication Contract)
AASHTO (M270M GR 250) = 15,950 kg (Erection Only-Included in Beam Fabrication Contract)
- Field welding of construction accessories will not be permitted to girders.
- Anchor bolts shall be set before bolting diaphragms over supports.
- The main load carrying member components subject to tensile stress shall conform to the Supplemental Requirements for Notch Toughness Zone 2. These components are the tension flanges and webs of the plate girders.
- Reinforcement bars shall conform to the requirements of AASHTO M31M, M322M Grade 400.
- Metal Shell Piles at east abutment shall be driven in holes precored through the embankment according to Article 512.09(c) of the Standard Specifications.
- The contractor shall drive 2-305 mm metal shell test piles in a permanent location, one for each abutment as directed by the Engineer before ordering the remainder of the piles.
- All dimensions are in millimeters (mm) except as noted.
- Concrete Sealer shall be applied to the seat area of the Abutments.
- All construction joints shall be bonded.
- The organic zinc rich primer/epoxy/urethane paint system shall be used for painting of new structural steel except where otherwise noted. The entire system shall be shop applied, with the exception that masked off connection surfaces, field installed fasteners and damaged areas shall be touched up in the field. The color of the final finish coat for all interior steel surfaces shall be gray, Munsell No. 5HB 7/1. The color of the final finish coat for the exterior and bottom flange of the fascia beams shall be Reddish Brown, Munsell No. 2.5YR 3/4. See Special Provision for "Cleaning and Painting New Metal Structures."
- Slipforming of parapets containing conduit is not allowed.

UPRR NOTES

- Railroad review and approval of shoring, demolition, erection, and falsework is required.
- All shoring systems that impacts the Railroad's operations and/or supports the Railroad's embankment shall be designed and constructed per current Union Pacific Railroad Guidelines for Temporary Shoring.
- All demolitions within the Railroad's right-of-way and/or demolition that may impact the Railroad's tracks or operations shall be in compliance with the Railroad's Demolition Guidelines.
- Erection over the Railroad's right-of-way shall be designed to cause no interruption to Railroad's operations. Erection over the Railroad's track shall be developed such that it enables the tracks to remain open to train traffic per Railroad's requirements.
- Minimum Construction Clearance Envelope of 21 feet vertical above the plane of top-of-rail and 12 feet horizontal at right angle from centerline of track shall be maintained at all time during construction.
- Falsework clearance shall comply with the Railroad's Minimum Construction Clearance Envelope.
- For Railroad coordination please refer to the Railroad Minimum Requirements as part of special provisions.
- The contractor must submit a proposed method of erosion and sediment control and have the method approved by the railroad.
- The proposed grade separation project shall not change the quantity and/or characteristics of the flow in the Railroad ditches and/or drainage structures.
- The elevation of the existing top-of-rail profile shall be verified before beginning construction. All discrepancies shall be brought to the attention of the Railroad prior to construction.

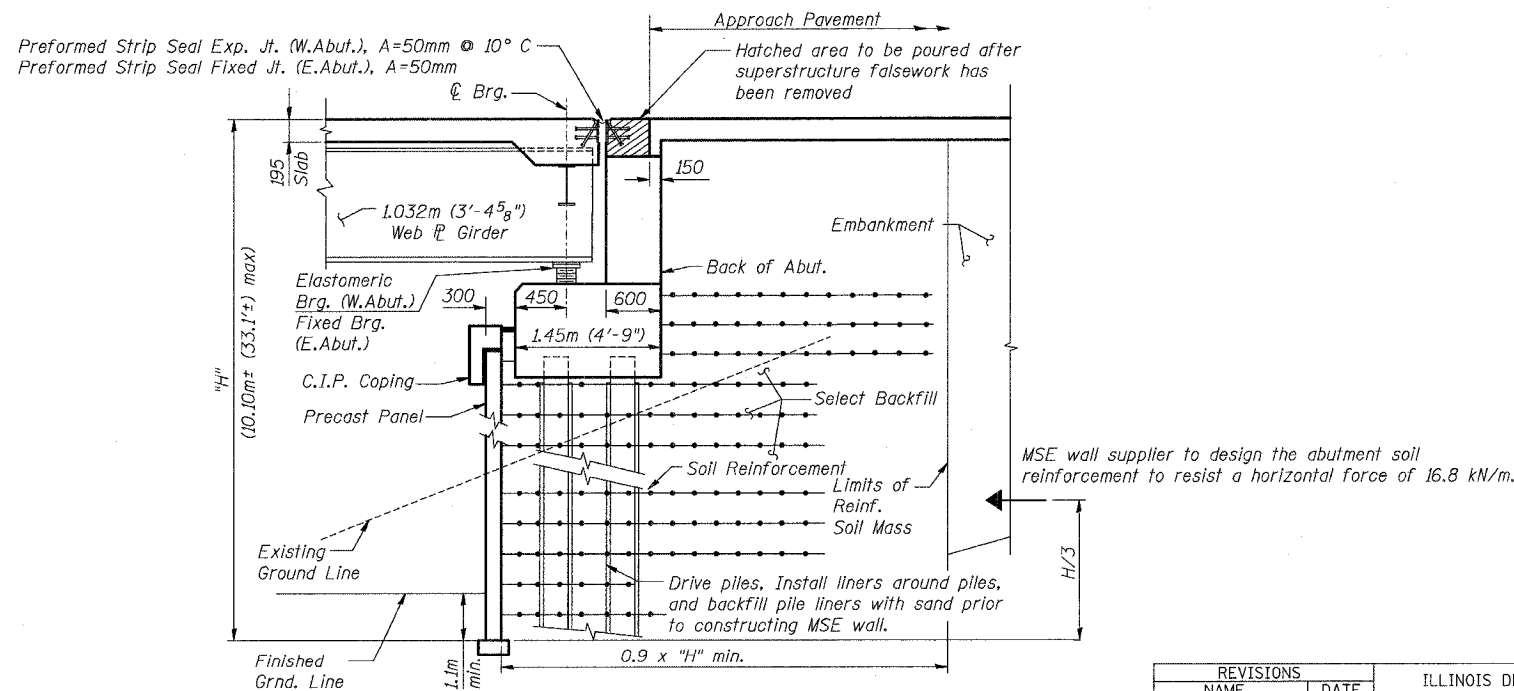
TOTAL BILL OF MATERIAL

ITEM	UNIT	SUB-STRUCT.	SUPER-STRUCT.	TOTAL
Removal and Disposal of Unsuitable Material	CU M	511		511
Porous Granular Embankment, Subgrade	CU M	511		511
Removal of Existing Structures	EACH			1
Structure Excavation	CU M	5279		5279
Concrete Structures	CU M	183.6		183.6
Concrete Superstructure	CU M		303.3	303.3
Bridge Deck Grooving	SQ M		747	747
Protective Coat	SQ M		1074	1074
Erecting Elastomeric Bearing Assembly, Type I	EACH		10	10
Erecting Structural Steel	L.S.		0.45	0.45
Stud Shear Connectors	EACH		2550	2550
Reinforcement Bars, Epoxy Coated	KG	10750	42390	53140
Furnishing Metal Shell Piles 305mm	METER	1396		1396
Driving Piles	METER	1396		1396
Anchor Bolts, M24	EACH	20		20
Anchor Bolts, M36	EACH	20		20
Test Pile Metal Shells	EACH	2		2
Temporary Soil Retention System	SQ M	218		218
Name Plates	EACH	1		1
Concrete Sealer	SQ M	57		57
Temporary Mechanically Stabilized Earth Retaining Wall	SQ M	255		255
Drainage Scuppers, DS-11	EACH		4	4
Drainage Scuppers, DS-33	EACH		1	1
Drainage System No. 2	EACH		1	1
Bar Splicers	EACH	256	519	775
Preformed Joint Strip Seal	METER		65.8	65.8
Mechanically Stabilized Earth Retaining Wall	SQ M	790		790
Erecting Precast Prestressed Concrete Deck Beam, (686 MM Depth)	SQ M		12.58	12.58
Hot-Mix Asphalt Replacement Over Patches	M TON		3.1	3.1
Protective Shield	SQ M		891	891
Asbestos Bearing Pad Removal	Each		100	100

* Removal of Crib Wall in Front of Existing West Abutment is Included in "Removal of Existing Structures".

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SECTION THRU ABUT.

Dimensions at Right Angles

Bearing Capacity of soil below MSE Wall is 215 kPa with a corresponding Factor of Safety of 2.5.

REVISIONS	
NAME	DATE

SHT. S-02 OF S-34

ILLINOIS DEPARTMENT OF TRANSPORTATION
IL ROUTE 68 OVER UPRR
F.A.P. ROUTE 343 SECTION 700-Y-B-R
COOK COUNTY STATION 9+900.324
STRUCTURE NO. 016-2732

GENERAL NOTES, B.O.M., & INDEX OF SHEETS

DESIGNED: BTO
CHECKED: JAN

DRAWN: BTO
CHECKED: JAN

DATE: 10/06