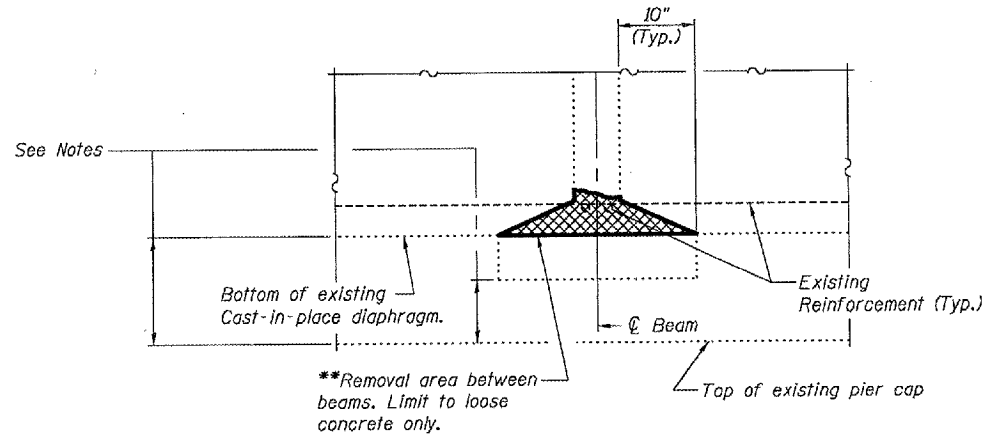


STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TABLE FOR REPAIR PLATES

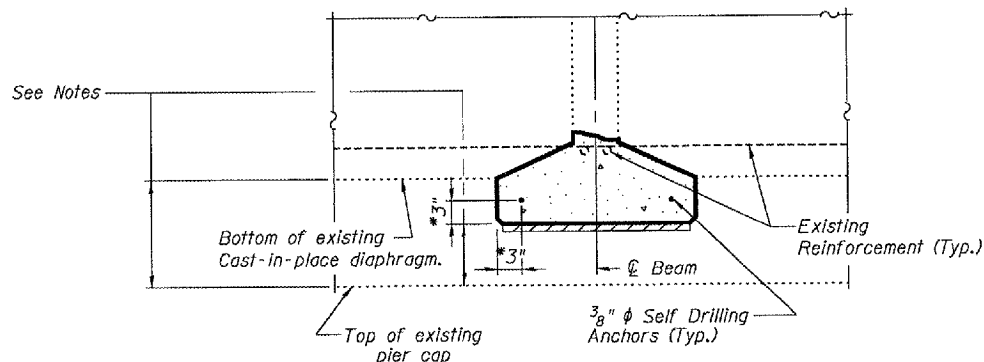
Girder	Mark	Pier 1	Pier 3	Pier 4	Pier 5	Pier 10	Pier 11	Pier 12	Pier 13	Pier 14	Pier 16
1	T	1'-3 1/4"	9 5/8"	1'-4 5/8"	8 1/2"	10"	11"	1'-3"	11"	10"	1'-2 1/2"
	B	1'-3"	8 3/8"	1'-3 3/8"	8"	10"	11"	1'-3"	11"	10"	1'-3 3/8"
	S	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"
2	T	1'-3 1/2"	9 5/8"	1'-4 5/8"	8 1/2"	10"	11"	1'-3"	11"	10"	1'-2 5/8"
	B	1'-3 1/8"	8 3/8"	1'-3 3/8"	8"	10"	11"	1'-3"	11"	10"	1'-3 3/8"
	S	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"
3	T	1'-3 3/4"	9 5/8"	1'-4 5/8"	8 1/2"	10"	11"	1'-3"	11"	10"	1'-2 5/8"
	B	1'-3 1/8"	8 3/8"	1'-3 3/8"	8"	10"	11"	1'-3"	11"	10"	1'-3 1/4"
	S	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"
4	T	1'-3 3/4"	9 5/8"	1'-4 3/8"	8 1/2"	10"	11"	1'-3"	11"	10"	1'-2 5/8"
	B	1'-3"	8 3/8"	1'-3 1/8"	8"	10"	11"	1'-3"	11"	10"	1'-3 1/4"
	S	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"
5	T	1'-4"	9 5/8"	1'-4 5/8"	8 1/2"	10"	11"	1'-3"	11"	10"	1'-2 3/4"
	B	1'-3 1/4"	8 3/8"	1'-3 3/8"	8"	10"	11"	1'-3"	11"	10"	1'-3 1/8"
	S	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"
6	T	1'-4 1/2"	9 5/8"	1'-4 5/8"	8 1/2"	10"	11"	1'-3"	11"	10"	1'-2 3/4"
	B	1'-3 1/4"	8 3/8"	1'-3 3/8"	8"	10"	11"	1'-3"	11"	10"	1'-3 1/8"
	S	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"
7	T	1'-4 3/4"	9 5/8"	1'-4 5/8"	8 1/2"	10"	11"	1'-3"	11"	10"	1'-3 1/4"
	B	1'-3 3/8"	8 3/8"	1'-3 3/8"	8"	10"	11"	1'-3"	11"	10"	1'-2 5/8"
	S	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"
8	T	1'-5 1/8"	9 5/8"	1'-4 5/8"	8 1/2"	10"	11"	1'-3"	11"	10"	1'-2 7/8"
	B	1'-3 1/2"	8 3/8"	1'-3 3/8"	8"	10"	11"	1'-3"	11"	10"	1'-3"
	S	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"
9	T	1'-5 3/8"	9 5/8"	1'-4 5/8"	8 1/2"	10"	11"	1'-3"	11"	10"	1'-3"
	B	1'-3 1/2"	8 3/8"	1'-3 3/8"	8"	10"	11"	1'-3"	11"	10"	1'-3"
	S	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"	2'-0 1/2"
10	T	1'-5 3/4"									1'-3"
	B	1'-3 5/8"									1'-3"
	S	2'-0 1/2"									2'-0 1/2"
11	T	1'-6"									
	B	1'-3 5/8"									
	S	2'-0 1/2"									
12	T	1'-6 3/8"									
	B	1'-3 3/4"									
	S	2'-0 1/2"									
13	T	1'-6 5/8"									
	B	1'-3 7/8"									
	S	2'-0 1/2"									



SECTION B-B

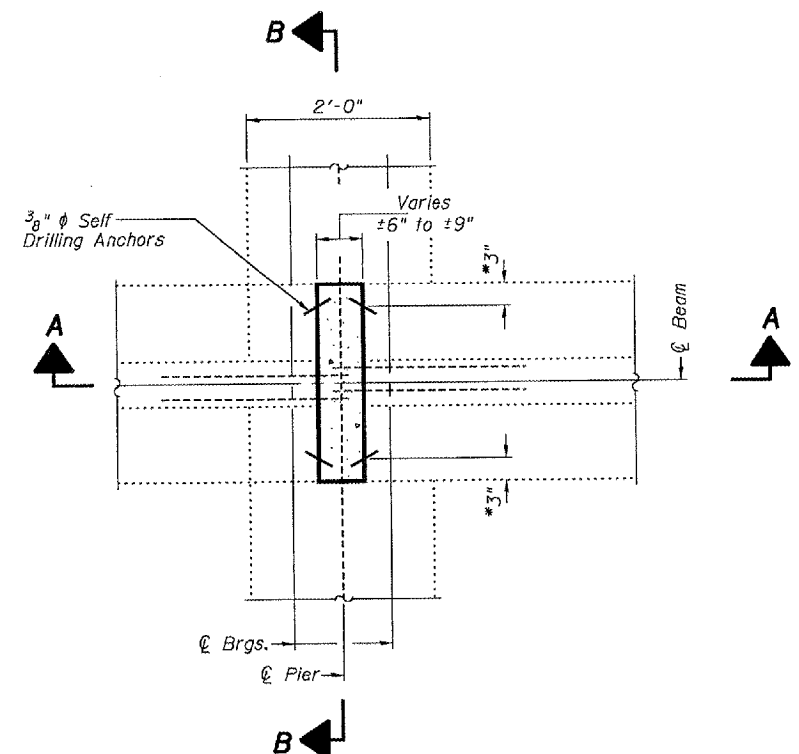
(Showing Concrete Removal)

\*\*Cost of Concrete Removal Included in cost of Pumpable Concrete Mix

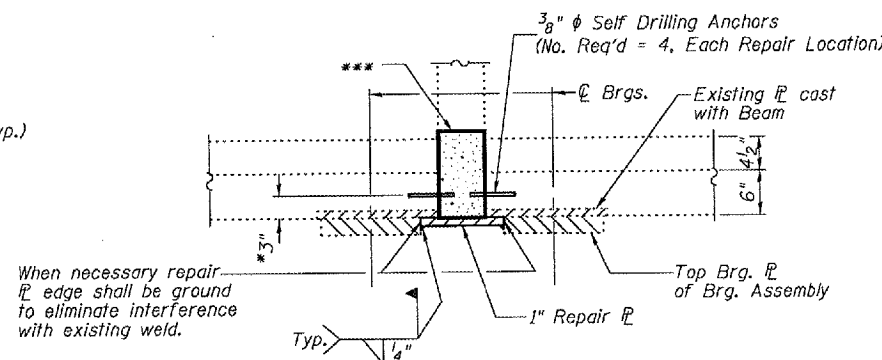


SECTION B-B

(Showing New Concrete)



PLAN



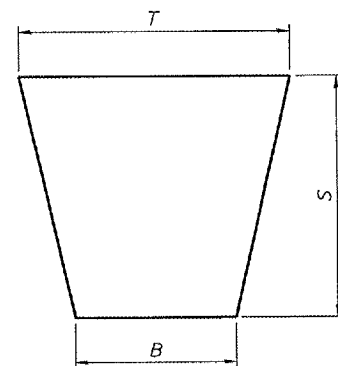
SECTION A-A

\*\*\*Concrete mix to be placed between beam ends. Fill entire void between beam ends from top of repair I to bottom of existing diaphragm including any voids left by concrete removal. Temporary forms shall be used on the sides of the beams. A vent tube large enough to accommodate the aggregate in the mix shall be placed opposite to where the concrete mix is being applied. The vent tube shall extend as high up into the repair area as possible. Cap vent when a steady flow of the concrete mix is evident.

\*Adjust location as required to miss prestressing strands.

Notes:

The contractor is advised that vertical clearance between top of pier caps and bottom of existing diaphragms as well as top of pier caps and bottom of beams is small. Limited space is available for placement of steel plates and concrete mix. Top of piers to bottom of diaphragms varies and is 9" min. Top of piers to bottom of beams varies and is 3" min.



REPAIR PLATE DETAIL

Verify dimensions in field prior to ordering materials.

DESIGNED	A.T.H.
CHECKED	V.H.V.
DRAWN	Drew Christopher
CHECKED	A.T.H. V.H.V.

January 12, 2007	
EXAMINED	<i>[Signature]</i>
PASSED	<i>[Signature]</i>

REPAIR DETAILS  
F.A.U. RT. 6713  
TAZEWELL COUNTY  
SN.090-0122