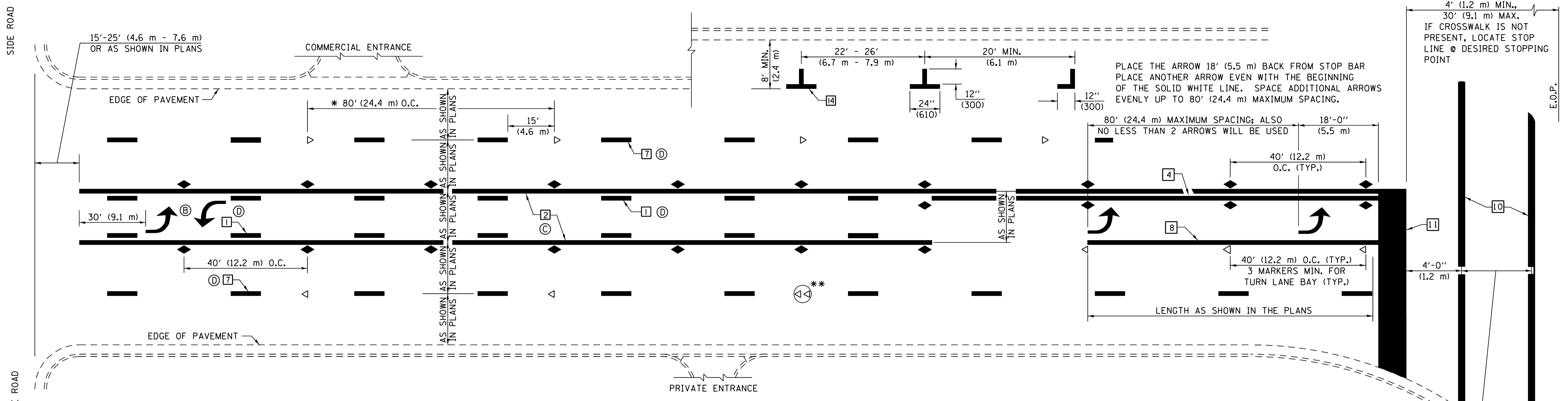


PAVEMENT MARKING AND MARKERS

(RURAL AND URBAN APPLICATIONS)

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
693	192 B-2	McLean	34	20
STA. 64+00		TO STA. 73+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 70519				

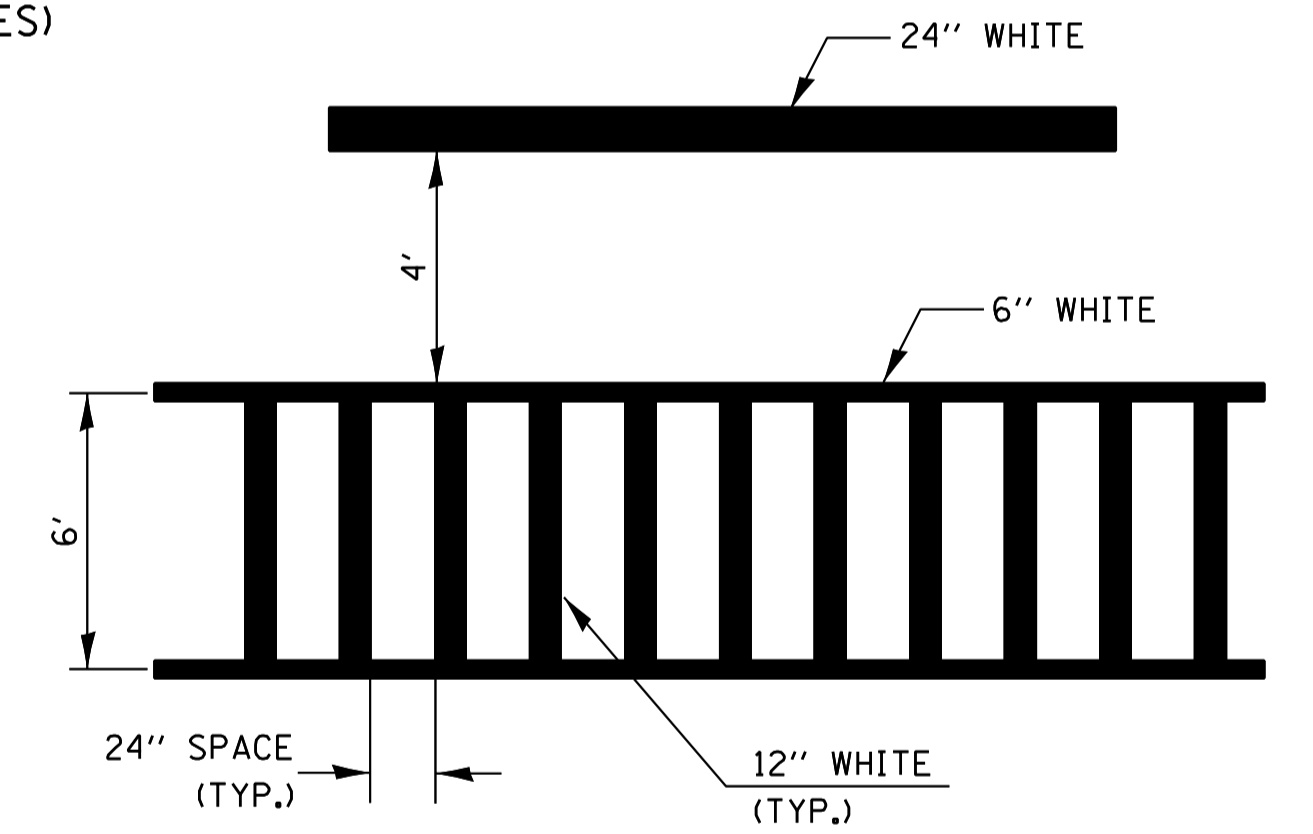


* REDUCE TO 40 FEET (12.2 METERS) ON CENTER ON CURVES WHERE ADVISORY SPEEDS ARE 10 MPH (15 km/h) LOWER THAN POSTED SPEEDS.

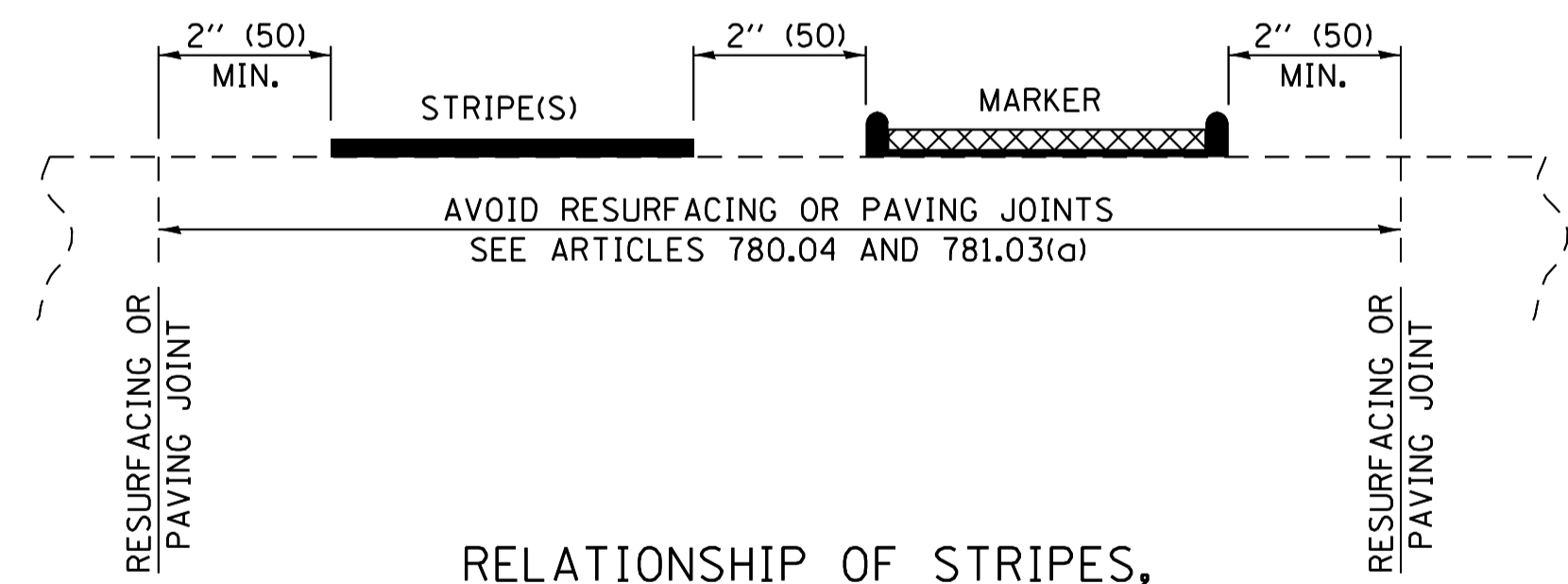
** DOUBLE LANE LINE MARKERS SHALL BE SPECIFIED AND SPACED AS SHOWN IN HIGHWAY STANDARD 781001 FOR MULTI-LANE DIVIDED AND UNDIVIDED HIGHWAYS.

(2 LANES TO 3 LANES OR
4 LANES TO 5 LANES)

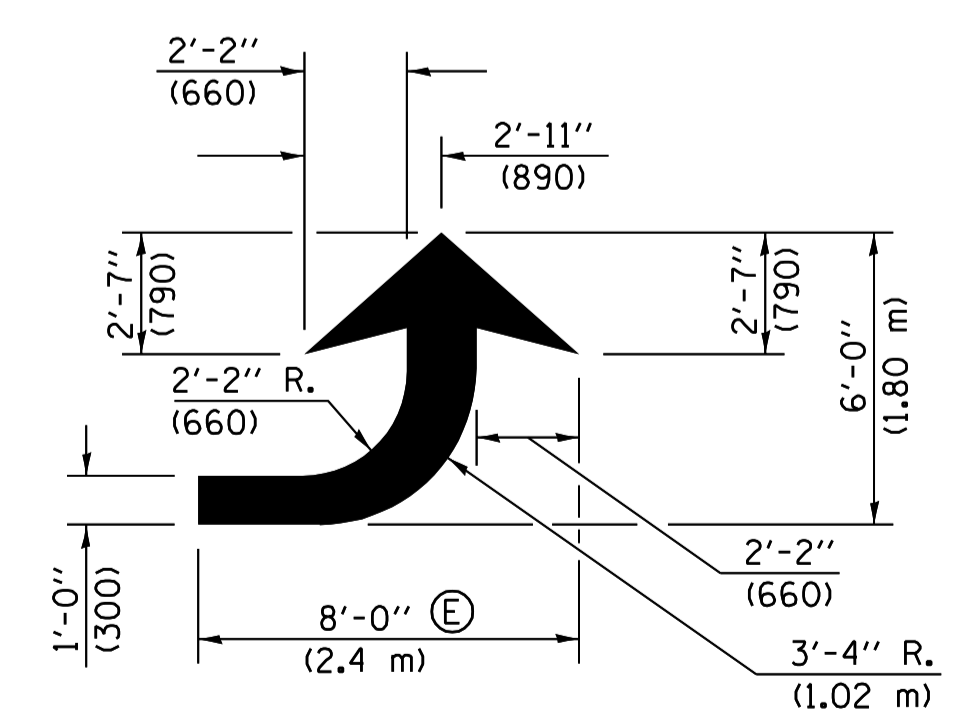
URBAN LEFT TURN



TYPICAL SPACING FOR
CROSSWALKS & STOP BARS

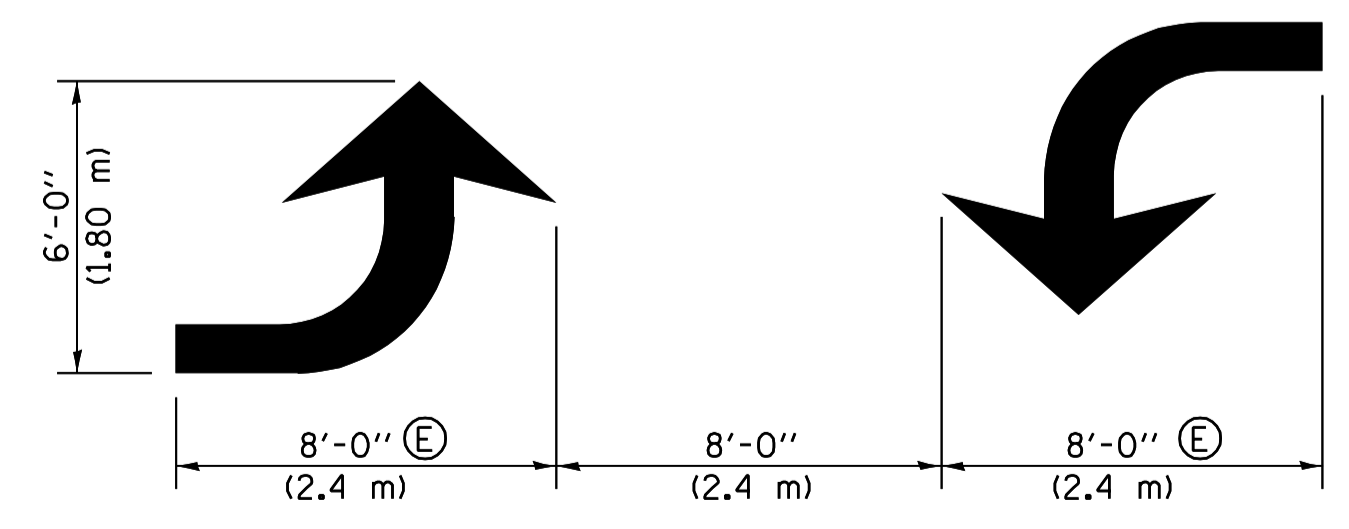


RELATIONSHIP OF STRIPES,
MARKERS AND JOINTS

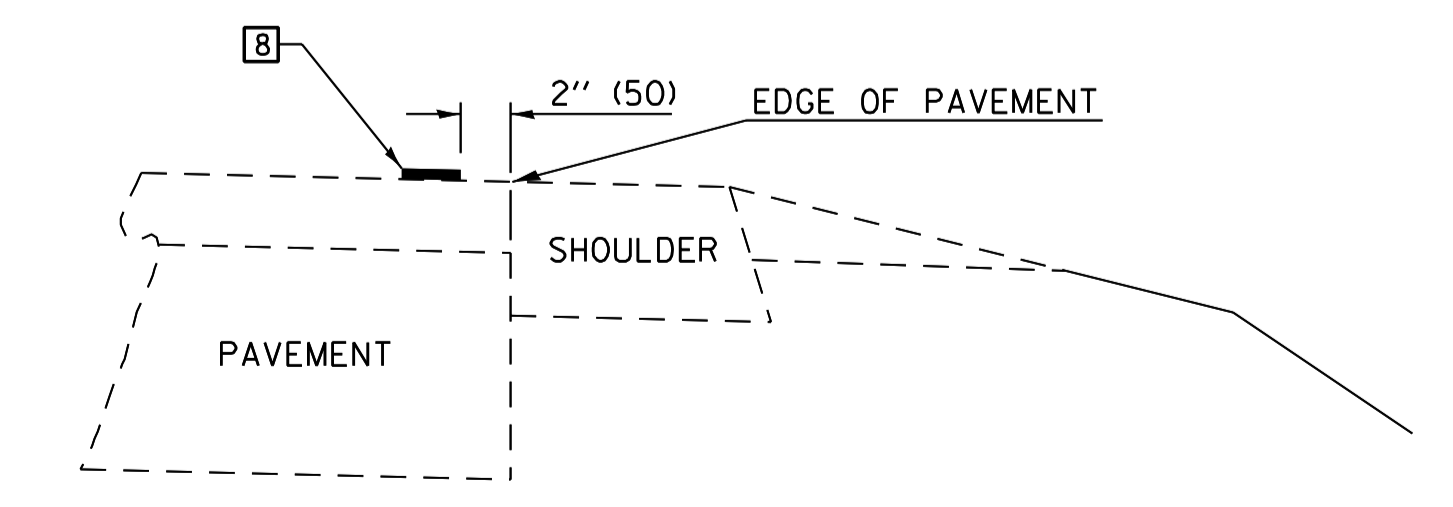


LEFT ARROW

REVERSE FOR RIGHT ARROW
AREA = 15.6 SQ. FT. (1.47 m²)
(WHITE)



TYPICAL DOUBLE
TURN ARROWS (WHITE)



RELATIONSHIP OF EDGE LINE TO EDGE OF PAVEMENT

(SAFETY SHOULDER OR PAVED SURFACE)
SEE ARTICLE 780.04

NOTES:

- Ⓑ TURN ARROW PAIRS SHALL BE PLACED AT 250' (75 m) INTERVALS AND SHALL BE EVENLY SPACED BETWEEN BOTH ENDS OF THE BIDIRECTIONAL LEFT TURN LANE.
- Ⓒ THE SOLID YELLOW PAVEMENT MARKINGS [2] SHOULD GENERALLY START OR END NEAR THE RADIUS POINT OF EACH STREET RETURN EXCEPT WHERE ONE OR BOTH ENDS WOULD INCLUDE STOP BARS.
- Ⓓ THE SKIP-DASH PAVEMENT MARKINGS [1] OR [7] SHOULD BE CENTERED BETWEEN BOTH ENDS OF EACH CITY BLOCK AND SHALL BE PLACED SO THEY LINE UP ACROSS FROM EACH OTHER. SEE EXAMPLE ON SHEET 2 OF 3.
- Ⓔ TURN ARROW SIZE DEPENDS ON THE LOCATION. RURAL LOCATION - LARGE ARROW SIZE URBAN LOCATION - SMALL ARROW SIZE

SHEET 2 OF 3

	NAME	DATE	REVISIONS	REVISIONS
DESIGNED	J.M.H.	5/85	6/88	
CHECKED	FMS	6/85	6/88	GEOMETRICS/K.A.G. 07/02
CADD NO.	7800XXXX			K.A.G. 09/05

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

PLOT DATE = \$DATE\$
 FILE NAME = \$FILES\$
 PLOT SCALE = \$SCALE\$
 USER NAME = \$USER\$