



Illinois Department of Transportation

2300 South Dirksen Parkway / Springfield, Illinois / 62764

February 16, 2007

SUBJECT: FAS 418 (Vantine Road)
Project RS-418(110)
Section 04-00069-00-RS
Henderson County
Contract No. 89366
Item 89
March 9, 2007 Letting
Addendum (A)

TO PROSPECTIVE BIDDERS:

Due to clarify information necessary to revise the following:

Proposal – Added special provision for Coarse Aggregate, added LR SD 630 and LR SD 631. Revised Railroad Protective Liability Insurance (BDE).

Plans – Sheet 1, removed Highway Standard 630001-07.

Prime contractors must utilize the enclosed material when preparing their bid and must include any Schedule of Prices changes in their bidding proposal.

Bidders using computer-generated bids are cautioned to reflect any and all Schedule of Prices changes, if involved, into their computer programs.

Very truly yours,

Eric Harm
Interim Engineer of Design and Environment

A handwritten signature in black ink, reading "Ted B. Walschleger" with a small "P.E." to the right.

By: Ted B. Walschleger
Engineer of Project Development
and Implementation

Index of Special Provisions

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Added 2-16-07

INDEX LOCAL ROADS AND STREETS SPECIAL PROVISIONS

LR#	Title (Effective Date) (Revision Date)	Page #
LR SD 12	"Slab Movement Detection Device" (Eff. 11/1/84) (Rev. 1/1/07).....	
LR SD 13	"Required Cold Milled Surface Texture" (Eff. 11/1/87) (Rev. 1/1/07).....	
LR SD 630 X	"Steel Plate Beam Guardrail" (Eff. 2/1/07). Developed to allow local agencies to continue to use 27" guardrail with 6 inch blockouts.	48-51
LR SD 631 X	"Traffic Barrier Terminals" (Rev. 2/1/07). Developed to keep Traffic Barrier Terminals Type 1, 2 & 5A as an option for local agencies to use with 27" guardrail with 6 inch blockouts.	52-55
LR SD 633	"Remove and Reerect Steel Plate Beam Guardrail" (Eff. 2/1/07). Developed to allow local agencies to replace 27" guardrail with 6 inch blockouts.	
LR 102	"Protests on Local Lettings" (Eff. 1/1/07). Developed to allow local agencies to adopt the department's interested party protest procedures outlined in Title 44 of the IL Administrative Code.	
LR 105 X	"Cooperation with Utilities" (Eff 1/1/99) (Rev 1/1/07). Formerly issued as LRS 1 and was reissued as an LR Contract Special Provision based on industry concerns discussed at the Joint Coop.	14
LR 107-1	"Nationwide Permit No. 14" (Eff. 2/1/04) (Rev. 3/1/05). Developed to outline the necessary requirements to comply with No. 14 permits.	
LR 107-2	"Railroad Protective Liability Insurance for Local Lettings" (Eff. 3/1/05) (Rev 1/1/06). Developed to require insurance policies to be submitted to the letting agency rather than the department.	
LR 107-3	"Disadvantaged Business Enterprise Participation" (Eff. 1/1/07). Developed to require DBE utilization plans to be submitted to the local agency.	
LR 107-4	"Insurance" (Eff. 2/1/07). Developed based on recommendations from IACE Policy Committee to ensure local agencies are indemnified when their projects are on the state letting.	
LR 108	"Combination Bids (Eff. 1/1/94) (Rev. 3/1/05). Developed to allow the revision of working days and calendar days. Revised to incorporate applicable portions of deleted Sections 102 & 103.	
LR 212	"Shaping Roadway" (Eff. 8/1/69) (Rev. 1/1/02).	
LR 355-1	"Asphalt Stabilized Base Course, Road Mix or Traveling Plant Mix" (Eff. 10/1/73) (Rev. 1/1/07)	
LR 355-2	"Asphalt Stabilized Base Course, Plant Mix" (Eff. 2/20/63) (Rev. 1/1/07)	
LR 400	"Bituminous Treated Earth Surface (Eff. 1/1/07). Developed since Section 401 was eliminated from the 2007 Standard Specifications.	
LR 402	"Salt Stabilized Surface Course" (Eff. 2/20/63) (Rev. 1/1/07)	
LR 403-2	Bituminous Hot Mix Sand Seal Coat" (Eff. 8/1/69) (Rev. 1/1/07)	
LR 420	"PCC Pavement (Special)" (Eff. 5/12/64) (Rev. 1/1/07). Developed to allow local agencies to construct quality PCC pavements for low volume roads.	
LR 442	"Bituminous Patching Mixtures for Maintenance Use" (Eff 1/1/04) (Rev. 2/1/07). Developed to reference approved bituminous patching mixtures.	
LR 451	"Crack Filling Bituminous Pavement with Fiber-Asphalt" (Eff. 10/1/91) (Rev. 1/1/07)	
LR 503-1	"Furnishing Class SI Concrete" (Eff. 10/1/73) (Rev. 1/1/02)	
LR 503-2	"Furnishing Class SI Concrete (Short Load)" (Eff. 1/1/89) (Rev. 1/1/02). Developed to allow a load charge to be added when short loads are expected during the contract.	
LR 542	"Pipe Culverts, Type _____ (Furnished)" (Eff. 9/1/64) (Rev. 1/1/07)	
LR 663	"Calcium Chloride Applied" (Eff. 6/1/58) (Rev. 1/1/07)	
LR 702	"Construction and Maintenance Signs" (Eff 1/1/04) (Rev 1/1/07). Developed to require florescent orange sheeting and a minimum sign size of 48" X 48" on construction and maintenance signs.	
LR 1004	"Coarse Aggregate for Bituminous Surface Treatment" (Eff. 1/1/02) (Rev 1/1/07). Developed to provide a coarser mix when aggregate producers have adjusted the CA-16 gradation according to the Aggregate Gradation Control System (AGCS) to a finer mix for Hot-Mix Asphalt.	
LR 1013	"Rock Salt (Sodium Chloride)" (Eff. 8/1/69) (Rev. 1/1/02)	
LR 1032-1	"Penetrating Emulsions" (Eff. 1/1/07) (Rev. 2/1/07). Developed to combine Penetrating Emulsified Asphalt and Penetrating Emulsified Prime into a single special provision.	
LR 1032-2	"Multigrade Cold Mix Asphalt" (Eff. 1/1/07) (Rev. 2/1/07). Developed to provide the material specification for Multigrade cold mix asphalt.	
LR 1102	"Road Mix or Traveling Plan Mix Equipment" (Eff. 1/1/07). Developed to replace road mix and traveling plant mix bituminous equipment that was eliminated from the Standard Specifications.	

Added 2-16-07

RAILROAD PROTECTIVE LIABILITY INSURANCE (5 and 10) (BDE)

Effective: January 1, 2006

Description. Railroad Protective Liability and Property Damage Liability Insurance shall be carried according to Article 107.11 of the Standard Specifications, except the limits shall be a minimum of \$5,000,000 combined single limit per occurrence for bodily injury liability and property damage liability with an aggregate limit of \$10,000,000 over the life of the policy. A separate policy is required for each railroad unless otherwise noted.

NAMED INSURED & ADDRESS	NUMBER & SPEED OF PASSENGER TRAINS	NUMBER & SPEED OF FREIGHT TRAINS
Burlington Northern Santa Fe Railway Company 80 - 44 NE Minneapolis, MN 55421	2 per day (1 to 79 mph)	32 per day @ 60 mph
DOT/AAR No: 079196T RR Division: Nebraska	RR Mile Post: 199.79 RR Sub-Division: Ottumwa	
For Freight/Passenger information Contact: Craig Rasmussen For Insurance Information Contact: Craig Rasmussen	Phone: 763-782-3468 Phone: 763-782-3468	

DOT/AAR No.: RR Division:	RR Mile Post: RR Sub-Division:	Phone: Phone:
For Freight/Passenger Information Contact: For Insurance Information Contact:		

Approval of Insurance. The original and one certified copy of each required policy shall be submitted to the following address for approval:

Illinois Department of Transportation
 Bureau of Design and Environment
 2300 South Dirksen Parkway, Room 326
 Springfield, Illinois 62764

The Contractor will be advised when the Department has received approval of the insurance from the railroad(s). Before any work begins on railroad right-of-way, the Contractor shall submit to the Engineer evidence that the required insurance has been approved by the railroad(s). The Contractor shall also provide the Engineer with the expiration date of each required policy.

Basis of Payment. Providing Railroad Protective Liability and Property Damage Liability Insurance will be paid for at the contract unit price per Lump Sum for RAILROAD PROTECTIVE LIABILITY INSURANCE.

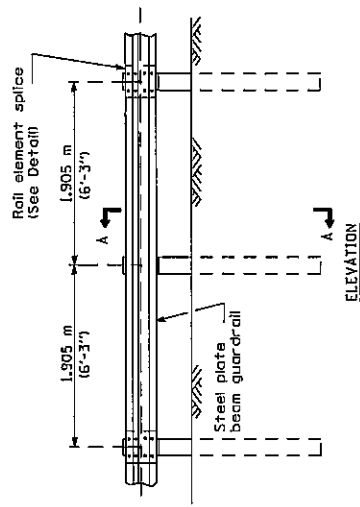
80157

COARSE AGGREGATE FOR AGGREGATE SHOULDER COURSES AND AGGREGATE SURFACE COURSES

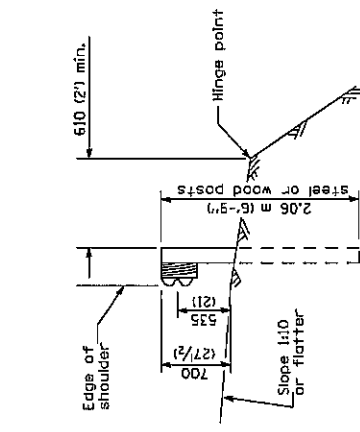
The aggregate shall conform to Section 1004.04 of the Standard Specifications for Road and Bridge construction, adopted by the Department of Transportation, except that all of the following revisions to Article 1004.04(b) will apply:

1. Revise the maximum allowable percentage of weighted average loss when the material is subjected to 5 cycles of the sodium sulfate soundness test from 25% as shown under Class D of the quality chart in Article 1004.01(b) of the Standard Specifications to 40% and,
2. Revise the maximum allowable percentage of wear as determined by the Los Angeles Abrasion method from 45% as shown under Class D of the quality chart in Article 1004.01(b) of the Standard Specifications to 65%, and
3. The sum of the percentage of weighted average loss when the material is subjected to 5 cycles of the sodium sulfate soundness test and the percentage of wear as determined by the Los Angeles Abrasion method shall not exceed 95%.

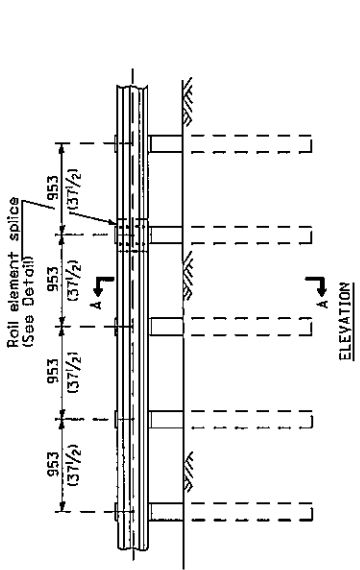
Added 2-16-07



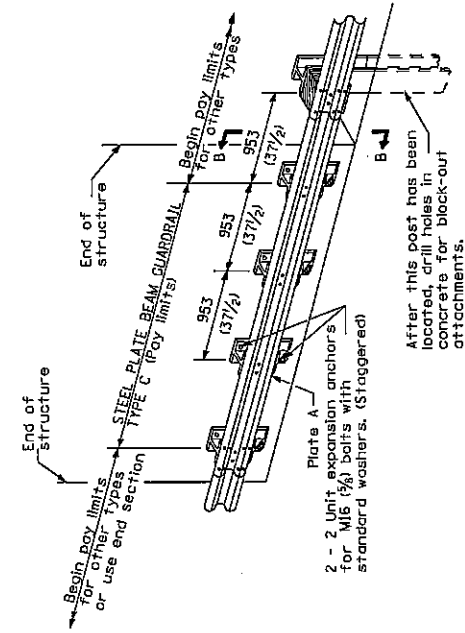
TYPE A
1,905 m (6'-3") Typical post spacing



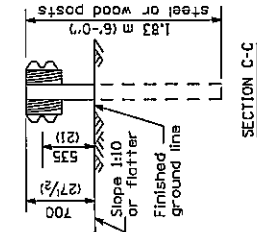
SECTION A-A



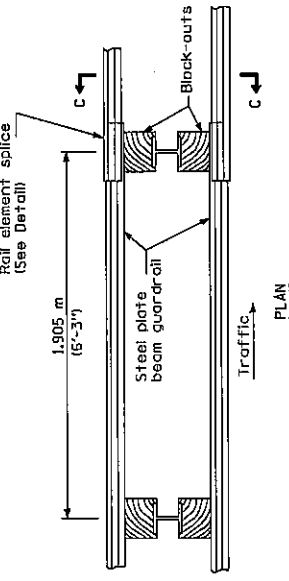
TYPE B
953 (37 1/2) Closed post spacing



TYPE C
953 (37 1/2) Block-out spacing



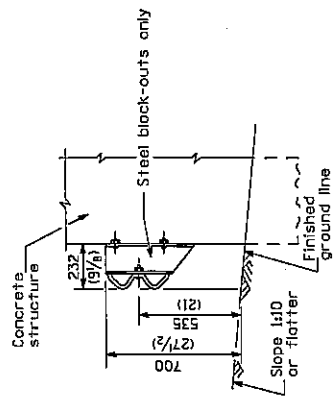
SECTION C-C



TYPE D
Double steel plate beam guardrail
1,905 m (6'-3") typical post spacing

GENERAL NOTES

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).
All dimensions are in millimeters (inches) unless otherwise shown.
The existing steel posts may be drilled to match the bolt pattern shown herein for the wood block-out, or a new steel post shall be provided.



SECTION B-B

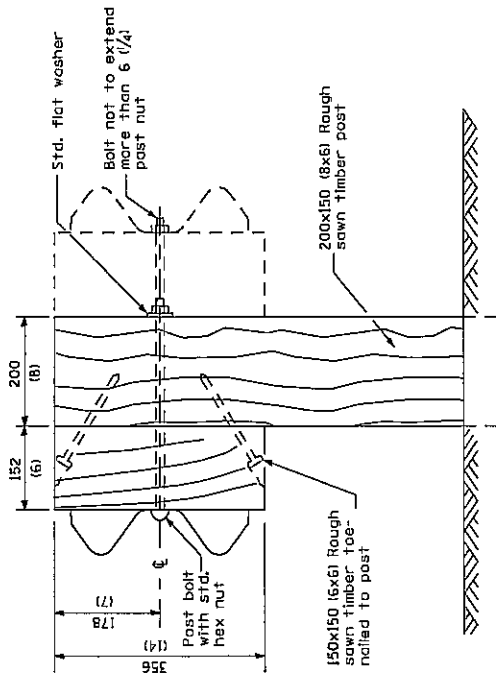
STEEL PLATE BEAM GUARDRAIL

(Sheet 1 of 4)

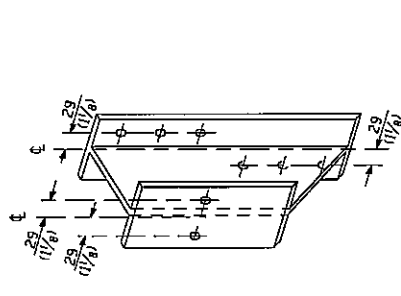
Effective 2/1/07

LR SD 630

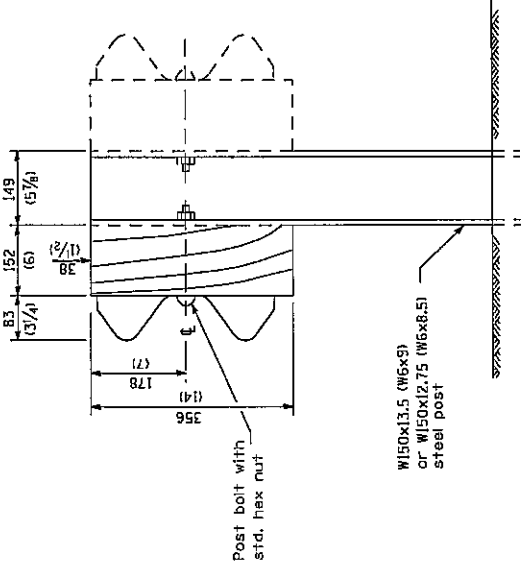
Added 2-16-07



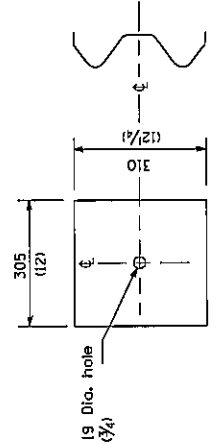
WOOD POST CONSTRUCTION



STEEL BLOCK-OUT DETAIL



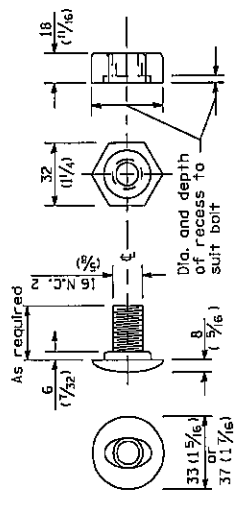
STEEL POST CONSTRUCTION



NOTE

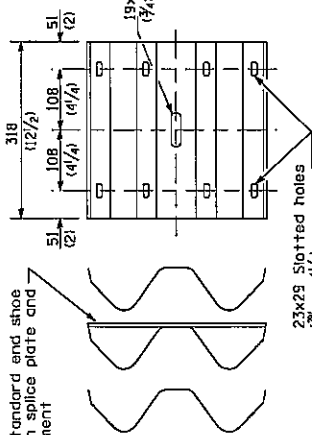
Plate A shall be placed between rail element and block-out at non-splice mounting points only when steel block-outs are used.

PLATE A



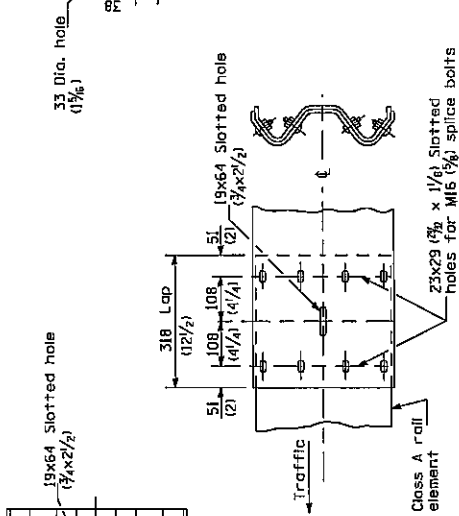
POST OR SPLICE BOLT & NUT

Place standard end shoe between splice plate and rail element

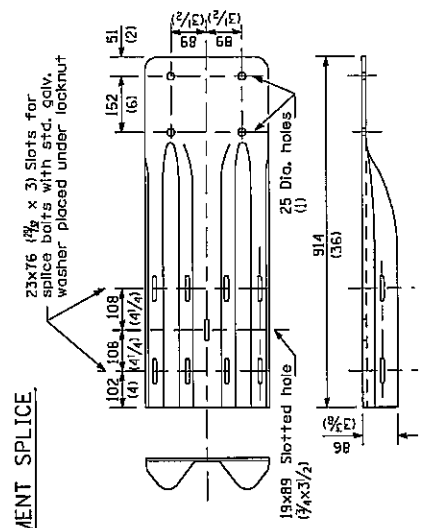


SPLICE PLATE

23x29 Slotted holes (7/8 x 1 1/8)

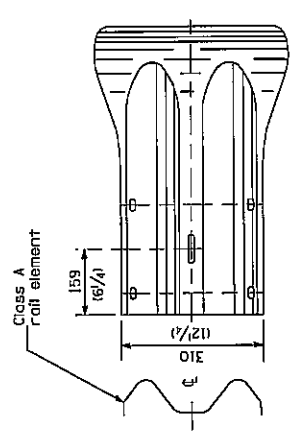


RAIL ELEMENT SPLICE

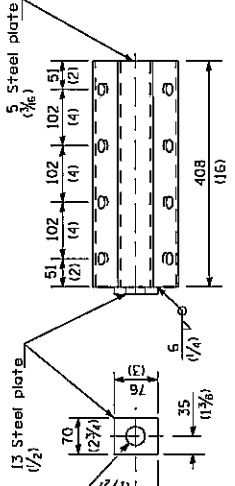


NOTE

When end shoe is attached to a bridge parapet which has an expansion joint, the bolts shall be provided with a locknut or double nut and shall be tightened only to a point that will allow guard-rail movement. The standard end shoe shall be attached to the concrete with pre-drilled or self-drilling anchor bolts. The anchor cone shall be set flush with the surface of the concrete. Externally threaded studs protruding from the surface of the concrete will not be permitted.

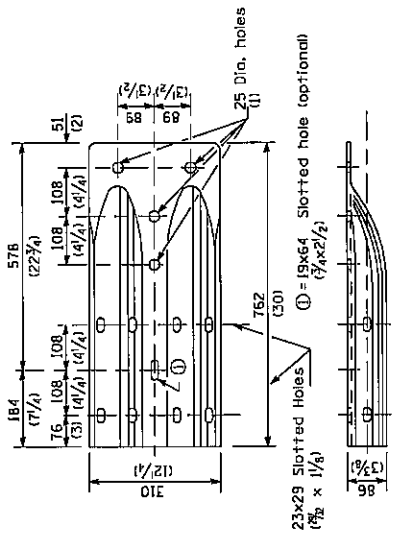


END SECTION



ANCHOR PLATE T DETAILS

NOTE
Anchor plate T shall be used to attach cable assembly to guardrail when required on traffic barrier terminals.



ALTERNATE END SHOE

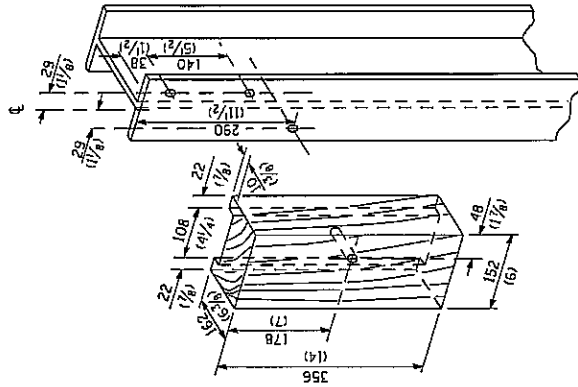
STEEL PLATE BEAM GUARDRAIL

(Sheet 3 of 4)

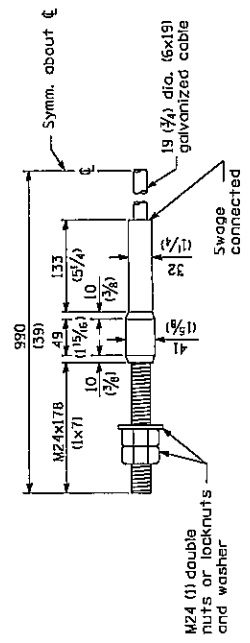
Effective 2/1/07

END SHOE

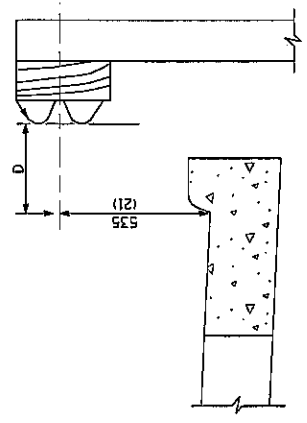
LR SD 630



WOOD BLOCK-OUT AND STEEL POST DETAILS

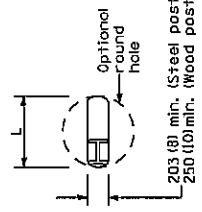


CABLE ASSEMBLY
(18,100 kg (40,000 lbs.) min. breaking strength)
Tighten to full tension.

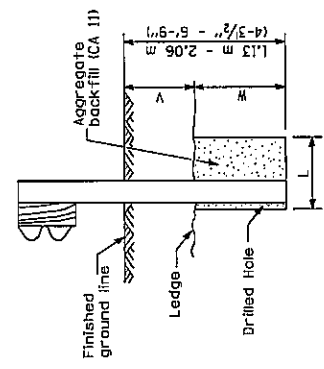


Note:
If it is necessary for D to be more than 300 (12) and less than 3.0 m (10'-0") Type M-5 (M-2) curb and gutter (Std. 505001) shall be used in front of and in advance of the guardrail.

GUARDRAIL PLACED BEHIND CURB
(D = 0 desirable to 300 (12) maximum)



PLAN



Note:
Ledge line is top of rock ledge or hard slag fill.

ELEVATION

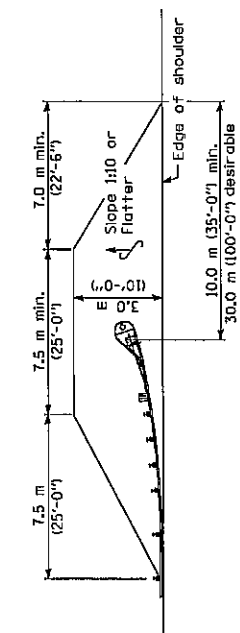
FOOTING FOR POST WHEN IMPERVIOUS MATERIAL IS ENCOUNTERED

V	W	L	
		Steel Post	Wood Post
0 - 460 (0 - 18)	610 (24)	530 (21)	580 (23)
>460 - 825 (18 - 41.5)	305 (12)	203 (8)	250 (10)
>825 - 1.13 m (>41.5 - 53.5)	305 - 0 (12 - 0)	203 (8)	250 (10)

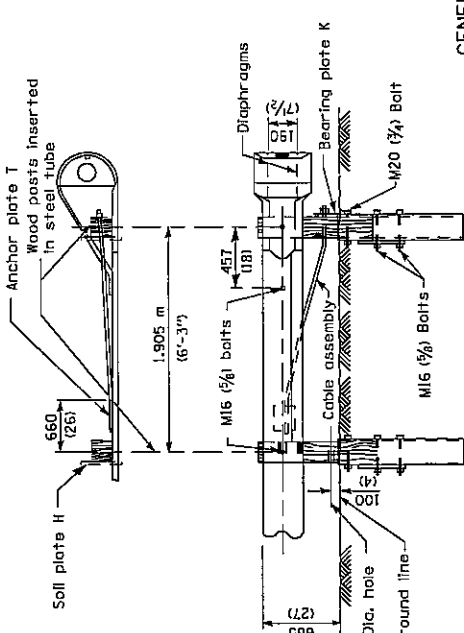
15

TYPE 1

OFFSETS TO FACE OF RAIL		
Post	X (ft)	Y (ft)
1	11.345 (37.22)	1.22 (4.0)
2	9.475 (31.03)	0.950 (2.75)
3	7.595 (24.92)	0.545 (1.75)
4	6.335 (20.78)	0.360 (1.25)
5	5.070 (16.64)	0.245 (0.80)
6	3.805 (12.49)	0.135 (0.45)
7	1.905 (6.25)	0.035 (0.11)
8	0 (0.00)	0 (0.00)

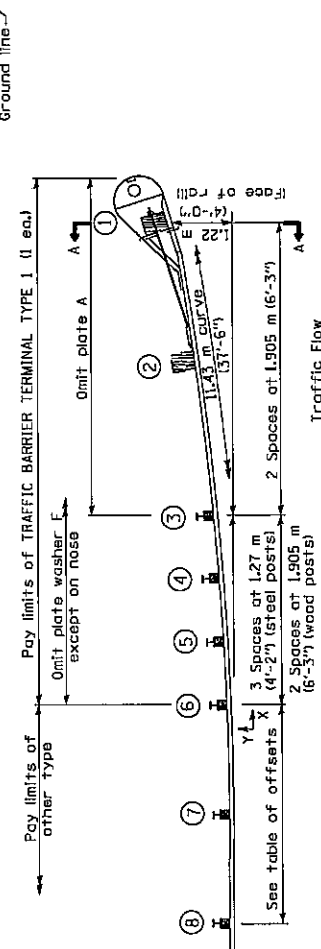


SHOULDER WIDENING TRANSITION



WOOD BREAKAWAY POSTS
TUBULAR STEEL FOUNDATIONS

GENERAL NOTES
See Standard 630001 for details of guardrail not shown.
Posts at location 1 & 2 shall be wood breakaway posts. Posts other than 1 & 2 may be either standard wood posts or steel posts, at the option of the contractor. If standard wood posts are used, one post shall be located midway between and in lieu of posts 4 & 5. The offset (Y) for this post shall be 300 mm (12 inches).
A two-piece assembly may be substituted for the one piece nose shown above.
Guardrail height transition shall be payed for as Steel Plate Beam Guardrail.
The bearing plate K shall be held in position by (2) two eightpenny nails driven into the post and bent over the top of the plate.
Higher guardrail shall transition down to the 685 mm (27 inches) height of the Traffic Barrier Terminal Type 1.
All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).
All dimensions are in millimeters (inches) unless otherwise shown.



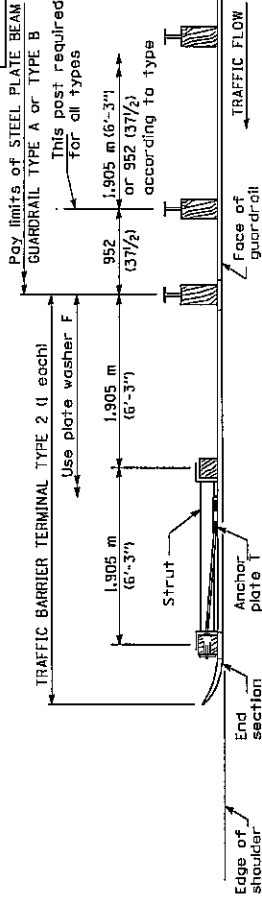
SECTION A-A

* If fill height exceeds 1.5 m (5'-0") use 1:3 max.

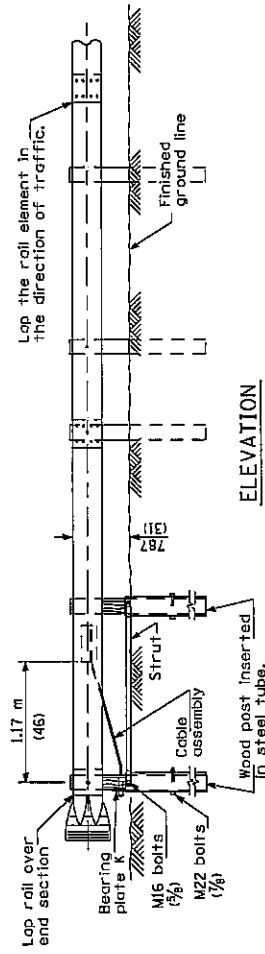
TRAFFIC BARRIER
TERMINAL
LR SD 631
Effective 1/1/07
Revised 2/1/07

Added 2-16-07

TYPE 2



PLAN



ELEVATION

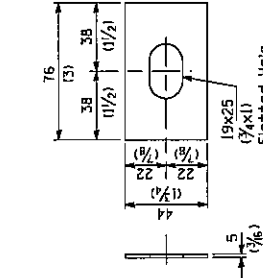
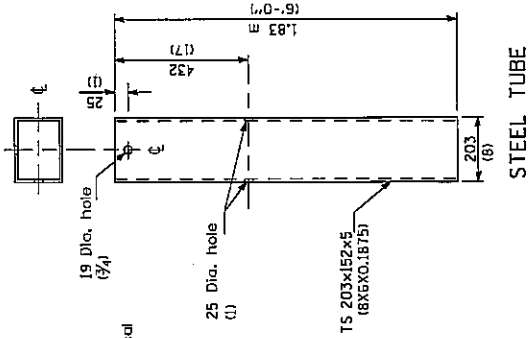
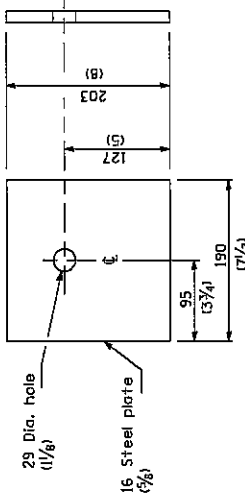


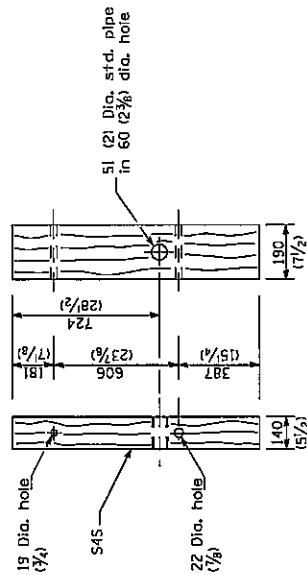
PLATE WASHER F



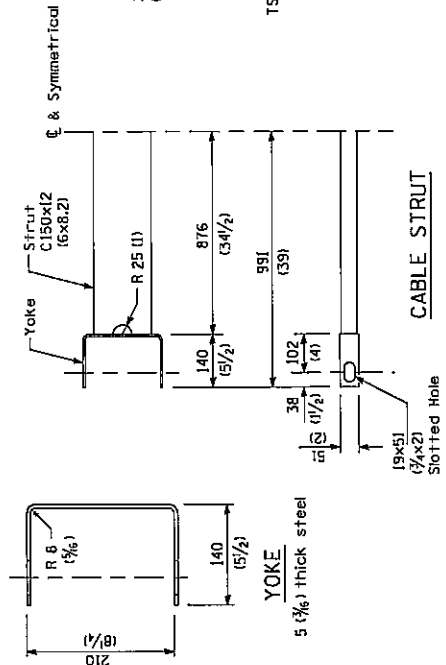
STEEL TUBE



BEARING PLATE K



WOOD POST



YOKES

CABLE STRUT

GENERAL NOTES

See Standard 630001 for details of guard-rail not shown.

The bearing plate K shall be held in position by (2) two eight penny nails driven into the post and bent over the top of the plate.

All dimensions are in millimeters (inches) unless otherwise shown.

TRAFFIC BARRIER TERMINAL

LR SD 631

Effective 1/1/07
 Revised 2/1/07

(Sheet 3 of 4)

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