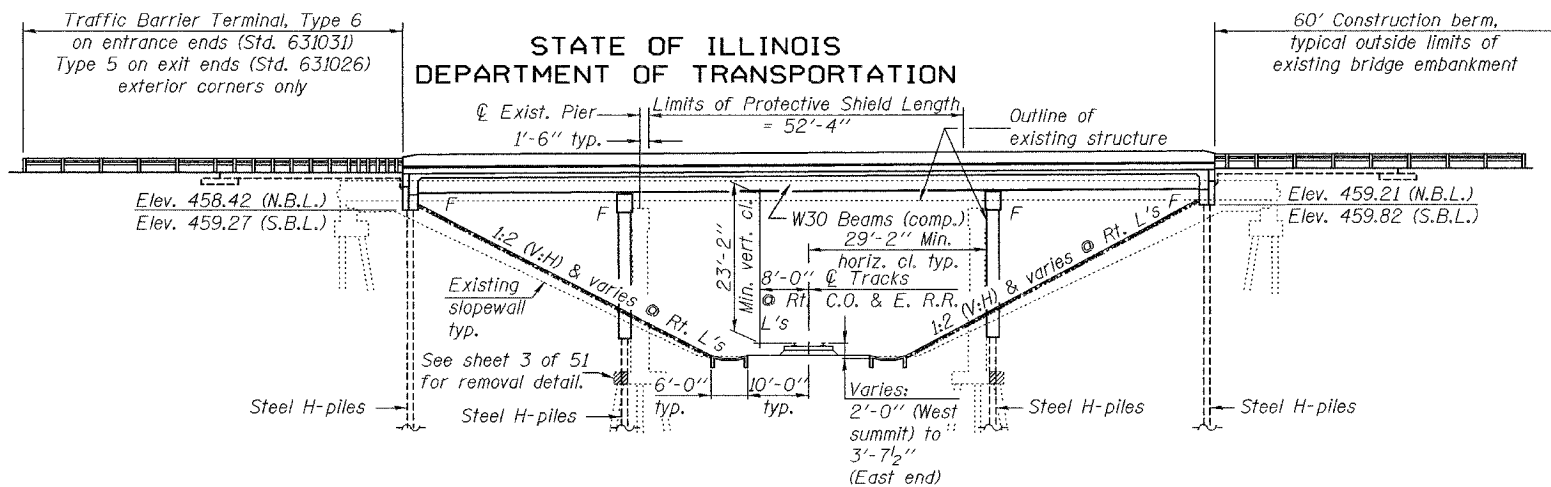


Benchmark: Painted mark on top of West corner of South Abutment, existing S.N. 100-0007 (S.B.) Sta. 1517+35.43, 55.5' Rt. @ F.A.I. 57, Elev. 469.42

Exist. Struct.: S.N. 100-0006 (N.B.) and S.N. 100-0007 (S.B.)
 The existing structures were built in 1961 under F.A.I. Route 57 Section XI-6VB. Both structures consist of three continuous spans (47'-3", 55'-4", 47'-3") for a total length of 153'-6" measured back to back of abutments. The superstructures have a 7" reinforced concrete deck on non-composite wide flange beams. The superstructures are supported by hammerhead piers on pile supported footings and pile bent abutments. The out to out deck width of the Northbound structure varies from 53'-4 1/4" to 55'-7". The out to out deck width of the Southbound structure varies from 37'-9 5/8" to 42'-10 7/8".
 The existing structures shall be removed and replaced using stage construction to maintain traffic.

Salvage: See Special Provisions for salvage information.



ROUTE NO.	SECTION	COUNTY	SHEET	SHEET NO.
F.A.I. 57	XI-6-2 VB-2	WILLIAMSON	917	834
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT-				

Contract #98950

DESIGN SPECIFICATIONS
 2004 AASHTO LRFD Bridge Design Specifications with 2005-2006 Interims

DESIGN STRESSES
 FIELD UNITS

f'c = 3,500 psi
 fy = 60,000 psi (reinf.)
 fy = 50,000 psi (M270 Grade 50)
 fy = 36,000 psi (M270 Grade 36)

LOADING HL-93

Allow 50 psf for future wearing surface.

SEISMIC DATA

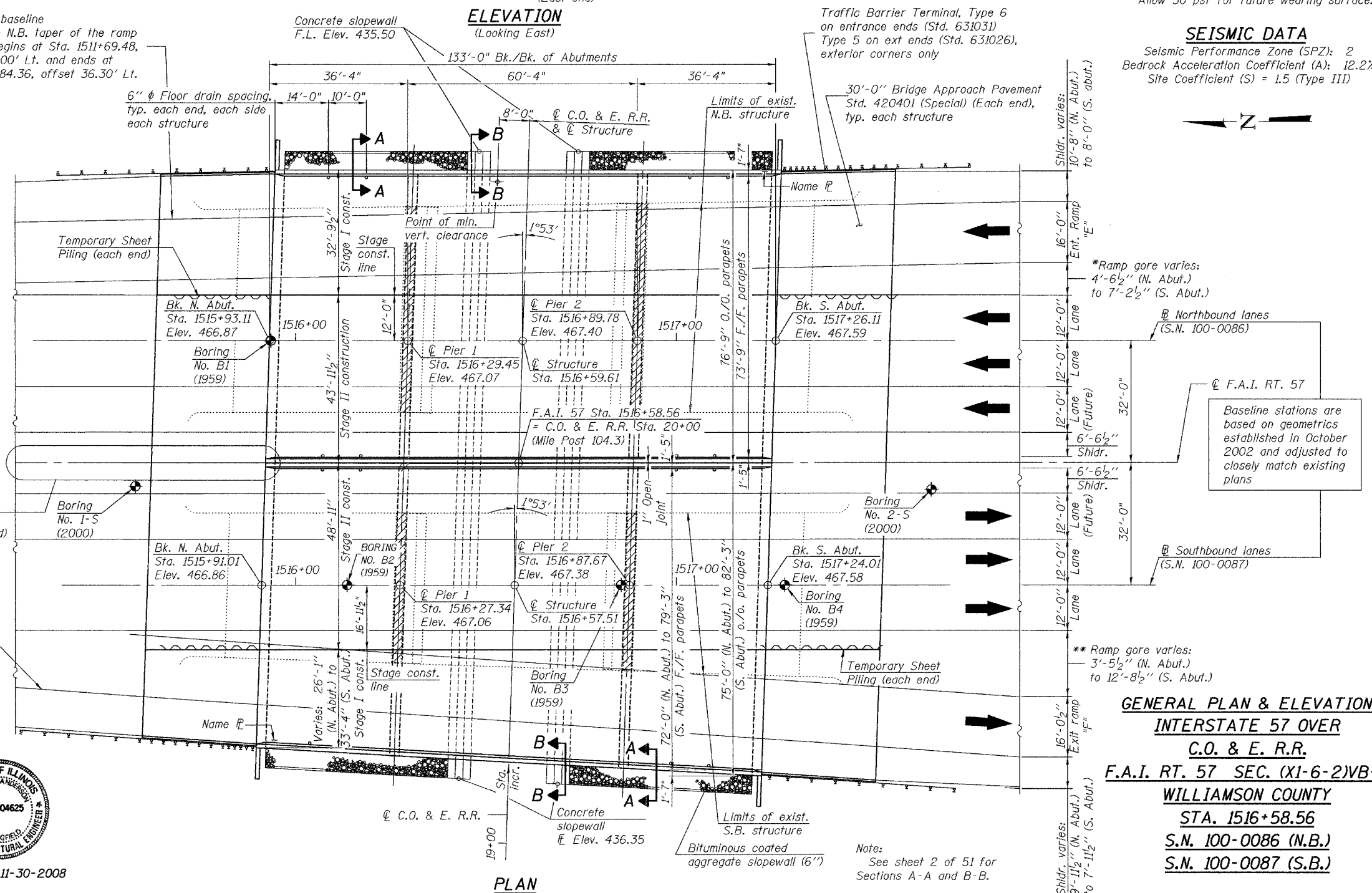
Seismic Performance Zone (SPZ): 2
 Bedrock Acceleration Coefficient (A): 12.2%
 Site Coefficient (S) = 1.5 (Type III)



INDEX OF SHEETS

1. General Plan & Elevation
2. General Data
- 3.-4. Stage Construction Details
5. Temporary Concrete Barrier
- 6.-11. Top of Slab Elevations
- 12.-17. Superstructure
18. Parapet Elevation
19. Superstructure Details
- 20.-24. Diaphragm Details
- 25.-26. Framing Plan
- 27.-28. Structural Steel Details
29. Bearing Details
30. Anchor Bolt
- 31.-34. North Abutments
- 35.-38. South Abutments
- 39.-42. Pier 1
- 43.-46. Pier 2
47. Pier Details
48. Bar Splicer Assembly Details
49. Pile Layout at Piers
- 50.-51. Boring Logs

* Ramp "E" baseline
 Note: The N.B. taper of the ramp baseline begins at Sta. 1511+69.48, offset 24.00' Lt. and ends at Sta. 1517+84.36, offset 36.30' Lt.

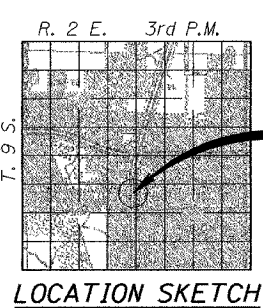


*Ramp gore varies: 4'-6 1/2" (N. Abut.) to 7'-2 1/2" (S. Abut.)

Baseline stations are based on geometrics established in October 2002 and adjusted to closely match existing plans

** Ramp gore varies: 3'-5 1/2" (N. Abut.) to 12'-8 1/2" (S. Abut.)

GENERAL PLAN & ELEVATION
INTERSTATE 57 OVER
C.O. & E. R.R.
F.A.I. RT. 57 SEC. (XI-6-2)VB-2
WILLIAMSON COUNTY
STA. 1516+58.56
S.N. 100-0086 (N.B.)
S.N. 100-0087 (S.B.)



** Ramp "F" Baseline
 Note: The S.B. taper of the ramp baseline begins at Sta. 1514+84.32, offset 24.00' Rt. and ends at Sta. 1519+38.03, offset 55.73' Rt.

DESIGNED	Michael D. Conn
CHECKED	Fredrick J. [unclear]
DRAWN	B.M.L.
CHECKED	MDC, FT

JANUARY 25, 2007
 EXAMINED: Thomas J. [unclear]
 PASSED: Ralph E. [unclear]



EXPIRES 11-30-2008

PLAN

Note: See sheet 2 of 51 for Sections A-A and B-B.