

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1285	2011-033-T	COOK	18	1
FED ROAD DIST NO. 1 ILLINOIS		CONTRACT NO. 60P39		

FOR INDEX OF SHEETS, SEE SHEET NO. 2

**PROPOSED
HIGHWAY PLANS**
FAU 1285 / PALATINE ROAD OVER SALT CREEK
CULVERT REPAIRS
SECTION: 2011-033-T
PROJECT NO. NHM-1285(004)
COOK COUNTY
C-91-524-11



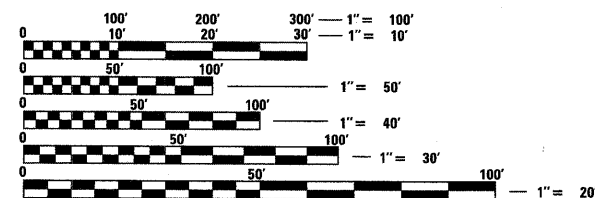
TRAFFIC DATA:

PALATINE ROAD
EXISTING 2006 ADT: 14,600
POSTED SPEED LIMIT: 35 MPH

DESIGN DESIGNATION:

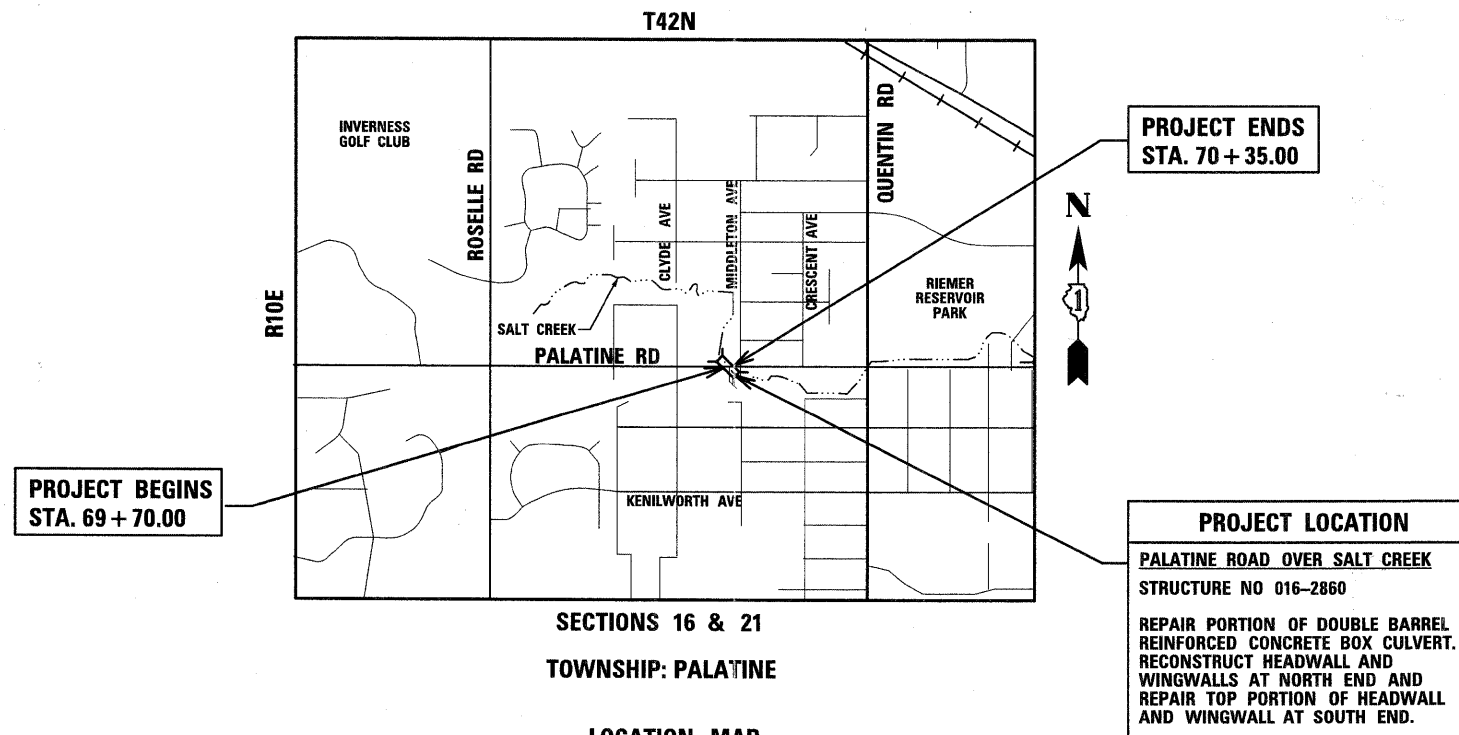
MINOR ARTERIAL

PROJECT IS LOCATED IN THE VILLAGE OF PALATINE IN COOK COUNTY



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123 OR 811



GROSS AND NET LENGTH OF PROJECT = 65.00 FEET = 0.012 MILES

PROJECT MANAGER ISSAM RAYYAN, PE (847) 705-4550
PROJECT ENGINEER ROBERT T. BORO, PE (847) 705-4178
CONTRACT NO. 60P39

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

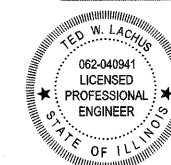
SUBMITTED October 11, 2011
John M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

February 3, 2012
John D. Baranowski, P.E.
acting ENGINEER OF DESIGN AND ENVIRONMENT

February 3, 2012
William R. Frey
acting DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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Primera
100 S. WACKER DRIVE SUITE 700 CHICAGO IL 60606.
P:312-606-0910 F:312-606-0415



Ted W. Lachus
TED W. LACHUS, P.E.
EXPIRES 11-30-2011
DATE 10-7-2011

INDEX OF SHEETS

- 1 COVER SHEET
- 2 INDEX OF SHEETS, STATE STANDARDS, GENERAL NOTES AND COMMITMENTS
- 3 SUMMARY OF QUANTITIES
- 4 TYPICAL SECTIONS
- 5 ALIGNMENT, TIES & BENCHMARKS
- 6 EXISTING AND PROPOSED ROADWAY PLAN
- 7 MAINTENANCE OF TRAFFIC (MOT) PLAN
- 8 PAVEMENT MARKING PLAN
- 9-14 STRUCTURE PLANS FOR CULVERT REPAIRS - SN 016-2860 PALATINE ROAD OVER SALT CREEK
- 15 TC-10 DISTRICT ONE - TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS
- 16 TC-11 DISTRICT ONE - TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT)
- 17 TC-13 DISTRICT ONE - TYPICAL PAVEMENT MARKINGS
- 18 TC-22 DISTRICT ONE - ARTERIAL ROAD INFORMATION SIGN

STATE STANDARDS

- 000001-06 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
- 001006 DECIMAL OF AN INCH AND OF A FOOT
- 280001-06 TEMPORARY EROSION CONTROL SYSTEMS
- 515001-03 NAME PLATE FOR BRIDGES
- 630001-10 STEEL PLATE BEAM GUARDRAIL
- 631011-08 TRAFFIC BARRIER TERMINAL, TYPE 2
- 642001-02 SHOULDER RUMBLE STRIPS, 16 IN.
- 701006-03 OFF-RD OPERATIONS, 2L, 2W, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
- 701011-02 OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
- 701301-04 LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
- 701901-02 TRAFFIC CONTROL DEVICES
- 704001-07 TEMPORARY CONCRETE BARRIER
- 780001-03 TYPICAL PAVEMENT MARKINGS

COMMITMENTS

NONE

GENERAL NOTES:

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS, WATER, SEWER AND CABLE TELEVISION FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED.)
2. ALL CONSTRUCTION WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE APPLICABLE SECTIONS OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" OF THE LATEST EDITION, SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS AND SPECIAL PROVISIONS OF THE BUREAU OF DESIGN AND ENVIRONMENT.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL ABOVE AND BELOW GROUND UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY, PUBLIC OR PRIVATE, THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE.
4. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
5. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
6. THE CONTRACTOR SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.
7. THE RESIDENT ENGINEER SHALL CONTACT WALTER CZARNY, THE AREA TRAFFIC FIELD ENGINEER, AT (847) 715-8419 AT LEAST TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
8. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
9. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
10. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKERS AND MONUMENTS UNTIL THE OWNER, AN AUTHORIZED SURVEYOR OR AGENT HAS WITNESSED THEIR LOCATION.
11. WHERE SECTION OR SUB-SECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED.
12. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES.
13. ALL UTILITY COMPANIES, SCHOOL DISTRICTS, AND LOCAL POLICE AND FIRE DEPARTMENTS SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST 72 HOURS PRIOR TO THE START OF CONSTRUCTION.
14. THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS, AS REQUIRED, PRIOR TO COMMENCING WITH CONSTRUCTION.
15. ALL RADII ARE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
16. THE CONTRACTOR SHALL ADHERE TO LIMITS OF RESTORATION SHOWN. AREAS OUTSIDE THESE LIMITS THAT ARE DAMAGED OR DISTURBED BY THE CONTRACTOR SHALL BE RESTORED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.
17. TEN FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED SHOULDERS TO EXISTING SHOULDERS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
18. BARRICADES: THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I OR TYPE II BARRICADE USED - ONE (1) WEIGHTED SANDBAG ACROSS EACH BOTTOM RAIL.
19. WHEN ARTIFICIAL LIGHTING IS USED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST CAUTION IN PREVENTING ADVERSE VISIBILITY CONDITIONS TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.
20. ANY REFERENCE TO A STANDARD SHALL BE INTERPRETED TO MEAN THE LATEST EDITION.
21. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY SIGNS THAT ARE DAMAGED OR RELOCATED BY THE CONTRACTOR'S WORK FORCE. SIGNS SHALL BE REPLACED BACK TO ORIGINAL CONDITION AND LOCATION AT THE CONTRACTOR'S EXPENSE.

22. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITHOUT WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING MACHINE IS SLOPED A MINIMUM OF 1:3 (V:H).
23. TYPE II BARRICADES WHEN USED FOR APPROACH TAPERS, AS INDICATED ON THE STATE STANDARDS OR SHOWN ON THE PLANS SHALL BE SAFETYCADE DIRECTION INDICATOR BARRICADES MANUFACTURED BY WLI INDUSTRIES, INC. 880 N. ADDISON, P.O. BOX 7050, VILLA PARK, IL 60181-7050 OR EQUIVALENT. THE CONTRACTORS BID PRICES FOR TRAFFIC CONTROL ITEMS SHALL INCLUDE THE COST OF THESE BARRICADES.
24. ALL ELEVATIONS HEREIN REFER TO USGS DATUM AND LOCAL BENCHMARKS.
25. MATCH EXISTING PAVEMENT SLOPES AND ELEVATIONS AT INTERFACES BETWEEN THE EXISTING AND PROPOSED PAVEMENT.
26. THE CONTRACTOR SHALL TAKE ALL NECESSARY SAFETY PRECAUTIONS TO PROTECT ABUTTING PROPERTY, UTILITIES, STRUCTURES, MONUMENTS, PEDESTRIANS AND VEHICULAR TRAFFIC.
27. THE CONTRACTOR SHALL TAKE ALL NECESSARY SAFETY PRECAUTIONS TO PROTECT THE EXISTING SITE CONDITIONS AND STRUCTURES TO REMAIN. ANY DAMAGE TO THE STRUCTURES OR SITE IS THE CONTRACTOR'S RESPONSIBILITY AND SHALL BE REPAIRED TO THEIR ORIGINAL CONDITIONS AT THE CONTRACTOR'S EXPENSE.
28. CONCRETE TRUCK WASHOUT SHALL NOT BE DISCHARGED INTO THE SURROUNDING AREAS. THE WASHOUT SHALL BE COLLECTED ONSITE AND TREATED OR DISPOSED OF AT AN APPROVED LOCATION (I.E. THE BATCH PLANT)
29. CONCRETE TRUCK WASHOUT LOCATIONS SHALL BE IDENTIFIED BY THE CONTRACTOR AT THE PRECONSTRUCTION MEETING FOR THE APPROVAL OF THE OWNER AND THE ENGINEER.
30. WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT.
31. ANY SEEDING/SODDING RESTORATION REQUIRED SHALL BE INCIDENTAL TO THE CONTRACT.
32. THIS PROJECT REQUIRES A US ARMY CORPS OF ENGINEERS 404 PERMIT. THE PERMIT ISSUED TO THE DEPARTMENT DOES NOT COVER THE IN STREAM WORK BY THE CONTRACTOR. THEREFORE AFTER AWARD, THE CONTRACTOR WILL NEED TO SUBMIT THE WORK PLAN TO THE CORPS. THE CORPS WILL NOT BE PROVIDING AN APPROVAL UNLESS STATED OTHERWISE IN THE PERMIT AND IN STREAM WORK CAN COMMENCE AT THE CONTRACTOR'S DISCRETION. GUIDELINES ON ACCEPTABLE IN STREAM WORK TECHNIQUES CAN BE FOUND ON THE CORPS WEBSITE: [HTTP://WWW.LRC.USACE.ARMY.MIL/](http://www.lrc.usace.army.mil/)

INCIDENTAL ITEMS ARE BOXED

FILE NAME = ...\\D160P39-shit-gennote.dgn		DESIGNED - ADW	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PALATINE ROAD OVER SALT CREEK INDEX OF SHEETS, STATE STANDARDS, GENERAL NOTES AND COMMITMENTS			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - GEW	REVISED -		1285	2011-033-T	COOK	18	2			
		CHECKED - RJD	REVISED -		CONTRACT NO. 60P39							
		DATE - 10/7/2011	REVISED -		ILLINOIS FED. AID PROJECT							
					SCALE: N/A	SHEET NO. 1 OF 1 SHEETS		STA.	TO STA.			

SUMMARY OF QUANTITIES

CODED PAY ITEM NUMBER	PAY ITEM	UNIT	TOTAL QUANTITIES	801 FED./201 STATE URBAN CONSTRUCTION TYPE CODE	
				ROADWAY 0004	BRIDGE REHABILITATION 0014 0040
20200100	EARTH EXCAVATION	CU YD	50	50	
28100107	STONE RIPRAP, CLASS A4	SQ YD	168		168
28200200	FILTER FABRIC	SQ YD	168		168
31101400	SUBBASE GRANULAR MATERIAL, TYPE B 6"	SQ YD	16	16	
35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SQ YD	22	22	
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	10	10	
40600300	AGGREGATE (PRIME COAT)	TON	1	1	
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	3	3	
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	8	8	
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	22	22	
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	16	16	
50102400	CONCRETE REMOVAL	CU YD	25.0		25.0
50200100	STRUCTURE EXCAVATION	CU YD	261		261
50300225	CONCRETE STRUCTURES	CU YD	40.0		40.0
50500405	FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	380		380
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	8080		8080
54003000	CONCRETE BOX CULVERTS	CU YD	15.4		15.4
64200116	SHOULDER RUMBLE STRIPS, 16 INCH	FOOT	432	432	
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	4	4	
67100100	MOBILIZATION	L SUM	1	1	
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	85	85	
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	2	2	
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	680	680	
70400100	TEMPORARY CONCRETE BARRIER	FOOT	100.0	100.0	
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	2020	2020	

* INDICATES SPECIALTY ITEMS

CODED PAY ITEM NUMBER	PAY ITEM	UNIT	TOTAL QUANTITIES	801 FED./201 STATE URBAN CONSTRUCTION TYPE CODE	
				ROADWAY 0004	BRIDGE REHABILITATION 0014 0040
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	14	14	
78300100	PAVEMENT MARKING REMOVAL	SQ FT	674	674	
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	14	14	
X0325201	SHOULDER RUMBLE STRIP REMOVAL	SQ YD	60	60	
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1	
X7030030	WET REFLECTIVE TEMPORARY TAPE TYPE III, 4 INCH	FOOT	2040	2040	
Z0001903	STRUCTURAL STEEL REMOVAL	POUND	5010		5010
Z0007122	REMOVING AND RE-ERECTING EXISTING RAILING	FOOT	60		60
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1	
Z0015500	DEBRIS REMOVAL	L SUM	1		1
Z0026407	TEMPORARY SHEET PILING	SQ FT	1802		1802
Z0030290	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2	2	
Z0030890	TEMPORARY INFORMATION SIGNING	SQ FT	52	52	
Z0062496	TEMPORARY PAVEMENT	SQ YD	204	204	

* INDICATES SPECIALTY ITEMS

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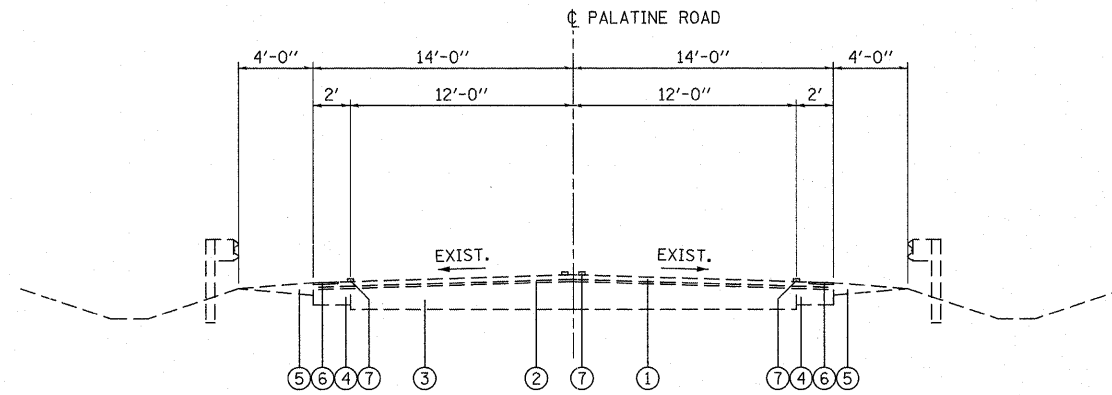
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DATE - 12/12/2011	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

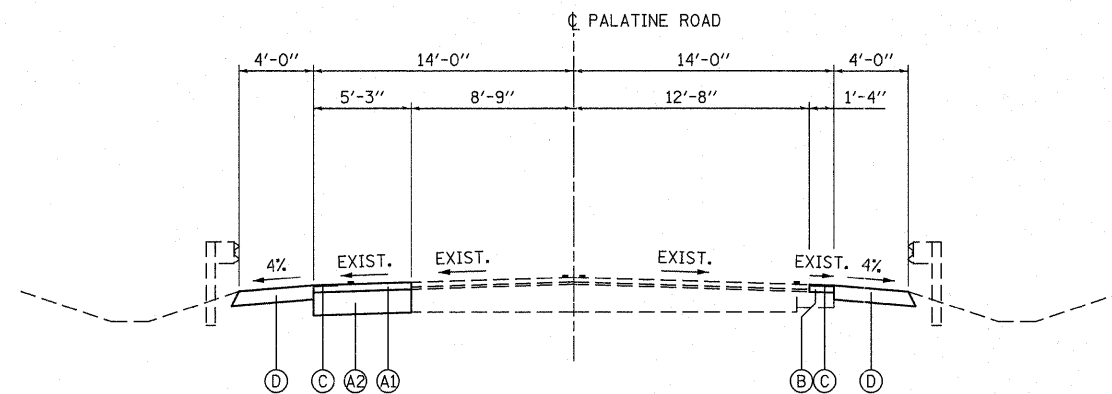
**PALATINE ROAD OVER SALT CREEK
SUMMARY OF QUANTITIES**

F.A.U. RTE. 1285	SECTION 2011-033-T	COUNTY COOK	TOTAL SHEETS 18	SHEET NO. 3
SCALE: N/A			SHEET NO. 1 OF 1 SHEETS STA. TO STA.	
ILLINOIS FED. AID PROJECT				

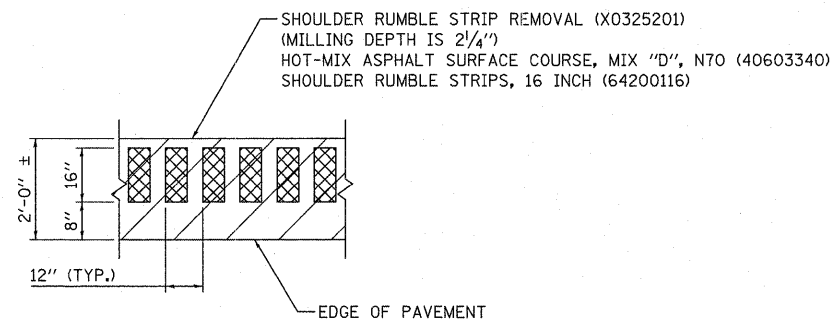
CONTRACT NO. 60P39



EXISTING TYPICAL SECTION
STA. 69 + 70.00 TO STA. 70 + 35.00



PROPOSED TYPICAL SECTION
STA. 69 + 70.00 TO STA. 70 + 35.00



SHOULDER RUMBLE STRIP REMOVAL DETAIL

EXISTING LEGEND

- ① EXISTING HMA SURFACE COURSE, 1/2"
- ② EXISTING POLYMERIZED LEVELING BINDER, 3/4"
- ③ EXISTING HMA BASE COURSE, 8"
- ④ EXISTING HMA AGGREGATE MIXTURE BASE COURSE
- ⑤ EXISTING AGGREGATE SHOULDERS, TYPE B
- ⑥ EXISTING HMA RUMBLE SHOULDER
- ⑦ EXISTING PAVEMENT MARKING LINE

PROPOSED LEGEND

- A CLASS D PATCHES, TYPE IV, 10 INCH
- A1 HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N70 (2")
- A2 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 (8")
- B HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N70 (2/4")
- C SHOULDER RUMBLE STRIPS, 16 INCH
- D TEMPORARY PAVEMENT (10")

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS
SHOULDER RUMBLE STRIP REPLACEMENT	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm) 2 1/4"	4% @ 70 Gyr.
CLASS D PATCH	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm) 2"	4% @ 70 Gyr.
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 8"	4% @ 70 Gyr.
DRIVEWAY PAVEMENT	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5mm) 2"	4% @ 50 Gyr.
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19mm) PE-6"; CE-8"	4% @ 50 Gyr.
TEMPORARY PAVEMENT	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5mm) 2"	4% @ 50 Gyr.
HOT-MIX ASPHALT BINDER, IL-19mm, N50 8"	4% @ 50 Gyr.

NOTE FOR TEMPORARY PAVEMENT:
When PCC Temporary Pavement is used as an option, it shall consist of Class PV Concrete meeting the requirements of Art. 1020 of the Standard Specifications; typically 10" thick.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS:
112 LBS/SQ YD/IN.

The "AC Type" for Polymerized HMA Mixes SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" shall be "PG 64-22" UNLESS modified by District ONE Special Provisions.

FOR "PERCENT OF RAP/RAS" SEE DISTRICT ONE SPECIAL PROVISIONS.

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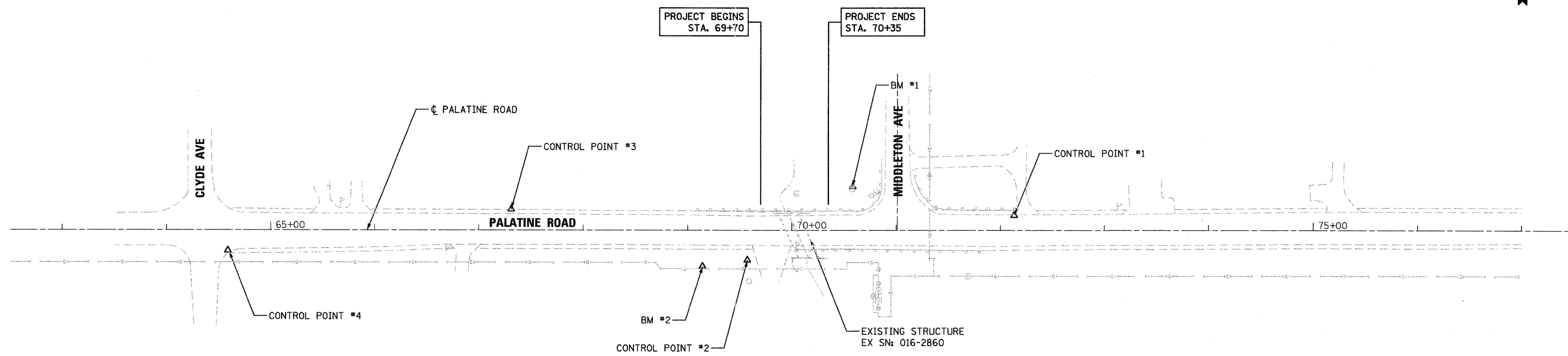
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PALATINE ROAD OVER SALT CREEK
TYPICAL SECTIONS

SCALE: 1/4" = 1' (HORIZ.) 1/8" = 1' (VERT.) SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1285	2011-033-T	COOK	18	4
			CONTRACT NO. 60P39	
ILLINOIS FED. AID PROJECT				

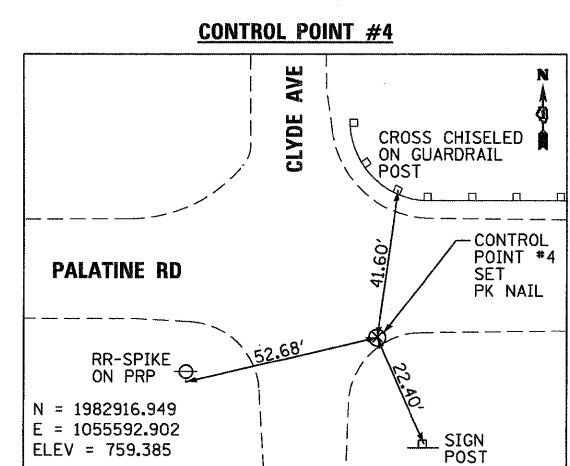
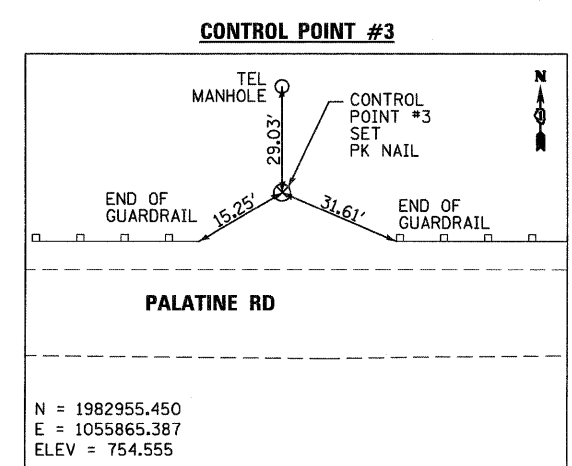
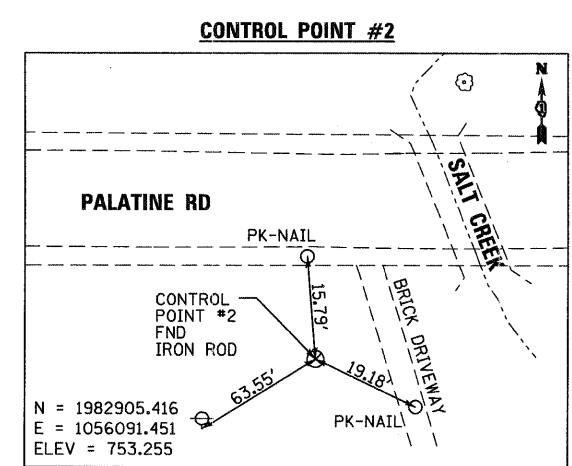
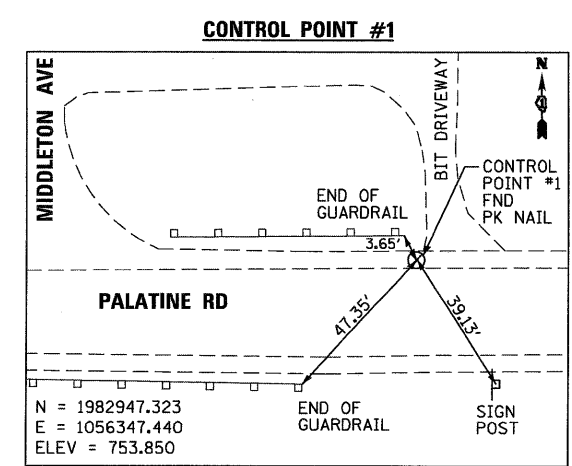


BENCHMARK:
 BM #1
 CROSS CUT ON NW BOLT OF HYDRANT
 AT NW CORNER OF PALATINE RD. AND MIDDLETON AVE.
 ELEV = 750.61

BM #2
 SET RR-SPIKE ON N SIDE OF POWER POLE
 AT ±35 FT SOUTH OF PALATINE RD. CL AND ±100 FT
 WEST OF SALT CREEK CL
 ELEV = 754.50

ALIGNMENT COORDINATES - PALATINE ROAD

STATION	NORTHING	EASTING
65+00	1,982,936.542	1,055,634.252
69+70	1,982,934.060	1,056,104.245
70+00	1,982,933.901	1,056,134.245
70+35	1,982,933.716	1,056,169.245
75+00	1,982,931.260	1,056,634.238



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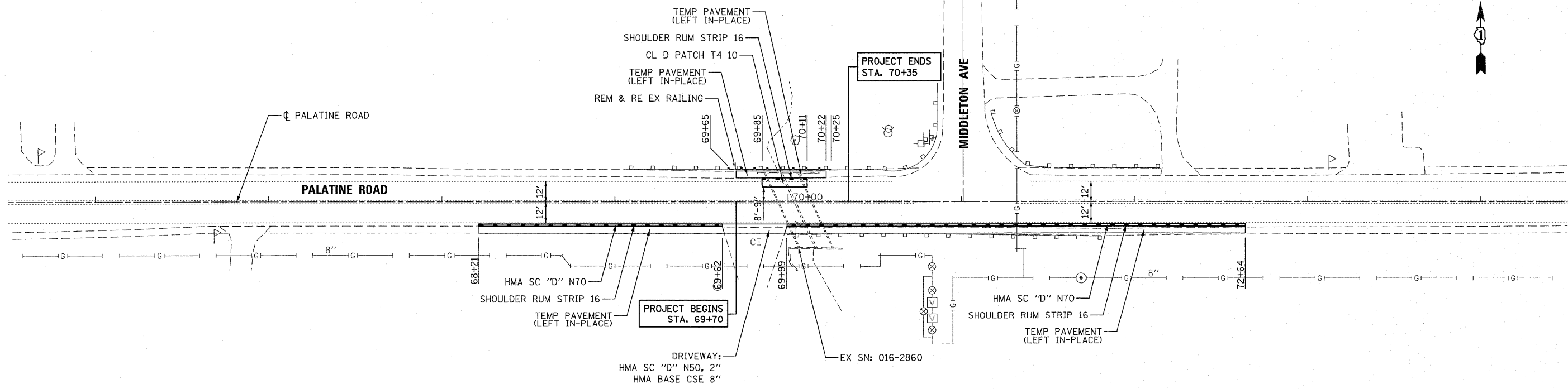
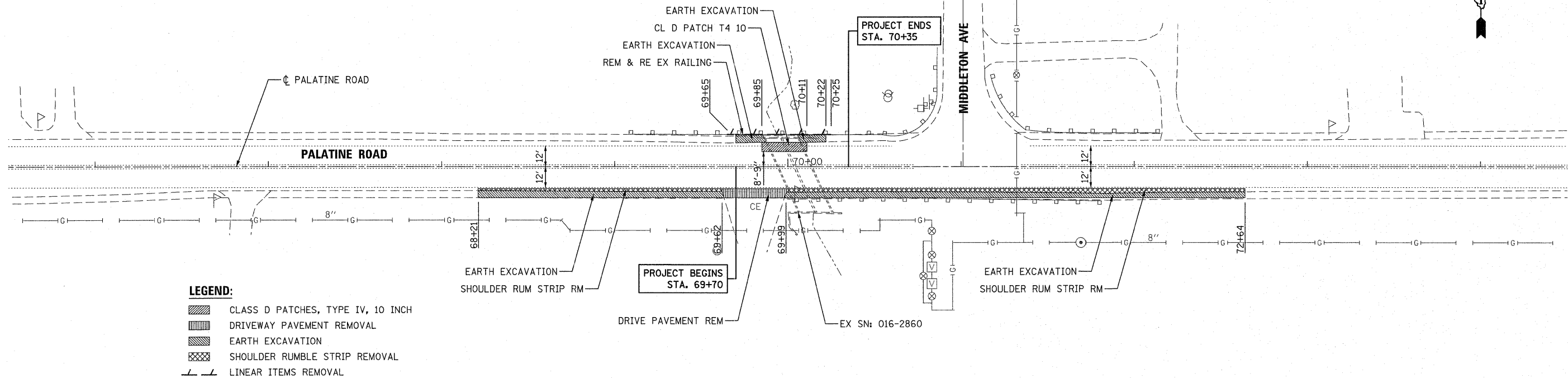
DESIGNED - ADW	REVISED -
DRAWN - GEW	REVISED -
CHECKED - RJD	REVISED -
DATE - 10/7/2011	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PALATINE ROAD OVER SALT CREEK
 ALIGNMENT, TIES & BENCHMARKS

SCALE: 1"=50' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 1285	SECTION 2011-033-T	COUNTY COOK	TOTAL SHEETS 18	SHEET NO. 5
CONTRACT NO. 60P39				
[ILLINOIS] FED. AID PROJECT				



FILE NAME =
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DESIGNED - ADW	REVISED - RJD 11/15/2011
DRAWN - GEW	REVISED -
CHECKED - RJD	REVISED -
DATE - 12/12/2011	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PALATINE ROAD OVER SALT CREEK
EXISTING & PROPOSED PLAN

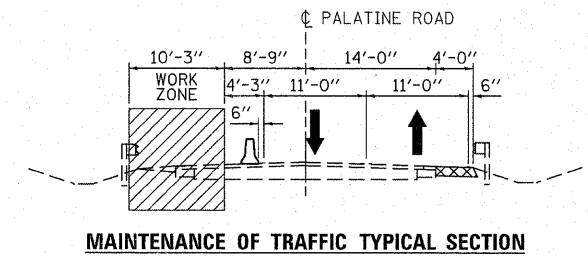
SCALE: 1"=30' SHEET NO. 1 OF 1 SHEETS STA. 69+70 TO STA. 70+35

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1285	2011-033-T	COOK	18	6
CONTRACT NO. 60P39				
ILLINOIS FED. AID PROJECT				

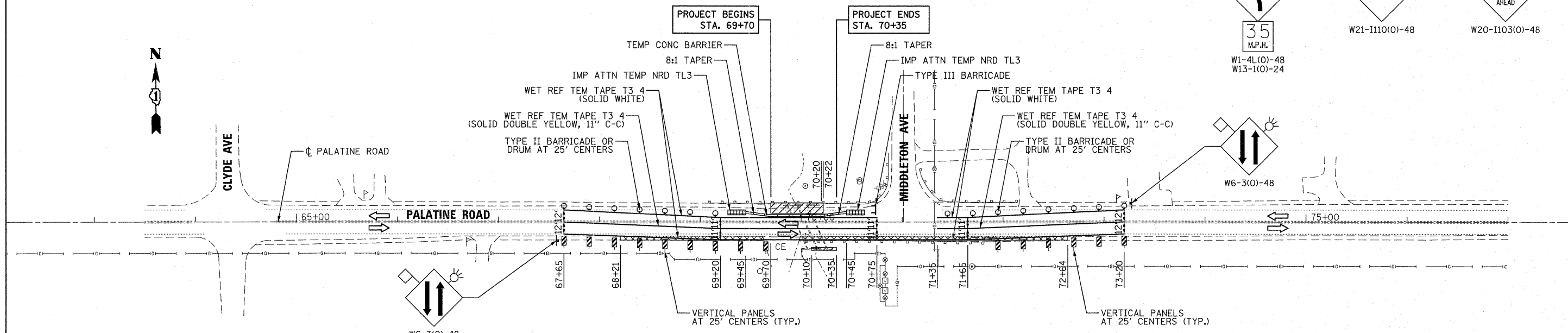
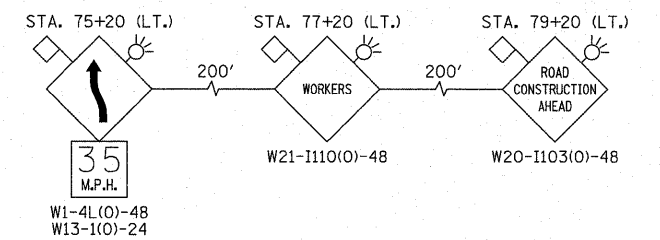
MAINTENANCE OF TRAFFIC GENERAL NOTES

1. THE MAINTENANCE OF TRAFFIC CONTROL (MOT) PLANS SHALL SERVE AS A GUIDE FOR SAFE DIVERSION OF TRAFFIC DURING EXECUTION OF THIS CONTRACT. HOWEVER, THE CONTRACTOR MAY MODIFY THE MOT PLANS TO MEET CONSTRUCTION NEEDS BUT NOT AT THE EXPENSE OF PUBLIC SAFETY OR CONVENIENCE. ANY CHANGES TO THE MOT PLANS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.
2. THE ENGINEER SHALL BE INFORMED 48 HOURS IN ADVANCE OF ANY CHANGE TO THE MOT PLANS.
3. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED SLIGHTLY TO FIT FIELD CONDITIONS.
4. ALL VEHICLES, EQUIPMENT, WORKERS, AND THEIR ACTIVITIES ARE RESTRICTED AT ALL TIMES TO ONE SIDE OF THE PAVEMENT UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER.
5. ALL EXISTING PAVEMENT MARKINGS IN CONFLICT WITH THE MAINTENANCE OF TRAFFIC STRIPING SHALL BE REMOVED. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT, "PAVEMENT MARKING REMOVAL".
6. THE CONTRACTOR SHALL REMOVE ALL TEMPORARY PAVEMENT MARKING TAPE WHICH CONFLICTS WITH THE NEXT STAGE OR FINAL STRIPING. REMOVAL OF TEMPORARY PAVEMENT MARKING TAPE WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT, "WORK ZONE PAVEMENT MARKING REMOVAL".
7. ALL TRAFFIC CONTROL DEVICES USED FOR THE MAINTENANCE OF TRAFFIC, AS DETAILED ON THE PLANS, OR HIGHWAY STANDARD SHALL BE REFLECTORIZED PRIOR TO INSTALLATION AND CLEANED AS SPECIFIED IN THE MAINTENANCE OF TRAFFIC SPECIAL PROVISION OR AS DIRECTED BY THE ENGINEER.
8. ALL DRUMS, VERTICAL PANELS, AND BARRICADES ADJACENT TO THE EDGE OF TRAVELED WAY SHALL BE EQUIPPED WITH STEADY-BURNING MONODIRECTIONAL LIGHTS.

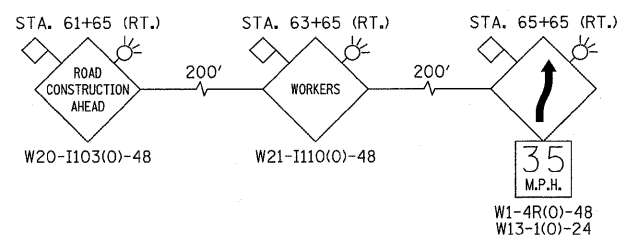
9. ALL EXISTING SIGNS WITHIN THE LIMITS OF MAINTENANCE OF TRAFFIC WHICH ARE OBTURED BY OR OTHERWISE INTERFERED WITH BY THE CONSTRUCTION OPERATIONS AND MAINTENANCE OF TRAFFIC, SHALL BE COVERED OR REMOVED BY THE CONTRACTOR UNLESS SPECIFIED IN THE PLANS OR WHEN DIRECTED BY THE ENGINEER. THIS WORK SHALL BE IN ACCORDANCE WITH ARTICLE 107.25 OF THE IDOT STANDARD SPECIFICATIONS.
10. THE CONTRACTOR SHALL PLACE A CHANGEABLE MESSAGE SIGN AT EACH END OF THE PROJECT AND/OR AS DIRECTED BY THE ENGINEER TO INFORM MOTORISTS OF UPCOMING CONSTRUCTION ACTIVITIES. THE MESSAGE SIGNS WITH THE APPROPRIATE INFORMATION SHALL BE IN PLACE TWO WEEKS BEFORE START OF CONSTRUCTION ACTIVITY. THIS WORK IS TO BE PAID FOR AT THE CONTRACT UNIT PRICE PER CALENDAR MONTH, "CHANGEABLE MESSAGE SIGN".
11. ALL TEMPORARY INFORMATION SIGNS SHALL BE PAID FOR SEPARATELY AT THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR "TEMPORARY INFORMATION SIGNING".
12. HIGH-INTENSITY FLASHING LIGHTS SHALL BE USED ON EACH APPROACH IN ADVANCE OF THE WORK ZONE DURING HOURS OF DARKNESS AND INSTALLED ABOVE THE FIRST TWO SIGNS IN EACH SERIES.
13. ALL HIGHWAY TRAFFIC CONTROL STANDARDS WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE INCLUDED IN THE LUMP SUM PRICE FOR "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".
14. IT IS THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE CONSTRUCTION TO MAINTAIN SAFE TRAFFIC FLOW THROUGH THE WORK ZONE.
15. TRAFFIC CONTROL DEVICES SHALL NOT IMPAIR THE DESIGNATED PEDESTRIAN MOVEMENT AROUND THE CONSTRUCTION AREA.
16. PLACE TEMPORARY PAVEMENT, DRIVEWAY PAVEMENT, AND HMA SURFACE COURSE ON EB PALATINE ROAD (SOUTH OF CENTERLINE) PRIOR TO SHIFTING TRAFFIC.
17. TEMPORARY PAVEMENT, DRIVEWAY PAVEMENT, AND HMA SURFACE COURSE ON EB PALATINE ROAD (SOUTH OF CENTERLINE) SHALL BE LEFT IN-PLACE AFTER CONSTRUCTION ACTIVITIES HAVE CONCLUDED AND AS DIRECTED BY THE ENGINEER.



ADVANCE SIGNING DETAIL



ADVANCE SIGNING DETAIL



LEGEND:

- WORK AREA
- TEMPORARY PAVEMENT
- TEMPORARY CONCRETE BARRIER
- SIGN
- DRUM WITH STEADY BURNING BIDIRECTIONAL LIGHT
- TYPE III BARRICADE
- VERTICAL PANEL
- IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE, NARROW) TEST LEVEL 3
- DIRECTION OF TRAFFIC

FILE NAME = ...\\D160P39-sh-t-staging.dgn



DESIGNED - ADW
 DRAWN - GEW
 CHECKED - RJD
 DATE - 12/12/2011

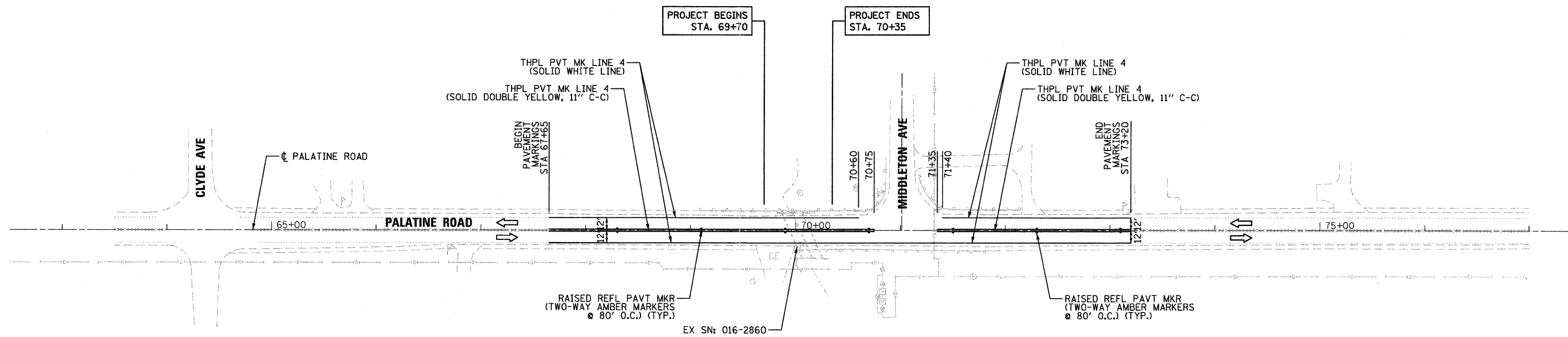
REVISED -
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PALATINE ROAD OVER SALT CREEK
 MAINTENANCE OF TRAFFIC PLAN

SCALE: 1"=50' SHEET NO. 1 OF 1 SHEETS STA. 67+65 TO STA. 73+20

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1285	2011-033-T	COOK	18	7
CONTRACT NO. 60P39			ILLINOIS FED. AID PROJECT	



PAVEMENT MARKING LEGEND:

- TWO-WAY AMBER

NOTES:

1. REFER TO DISTRICT 1 DETAILS "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" AND "TYPICAL PAVEMENT MARKINGS" FOR ADDITIONAL INFORMATION.
2. THE PROPOSED PAVEMENT MARKINGS SHALL BE PLACED IN THE SAME LOCATIONS AS THE EXISTING PAVEMENT MARKINGS OR AS DIRECTED BY THE ENGINEER.

FILE NAME =
...\\D160P39-shr-pmk.dgn



DESIGNED - ADW	REVISED -
DRAWN - GEW	REVISED -
CHECKED - RJD	REVISED -
DATE - 10/7/2011	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

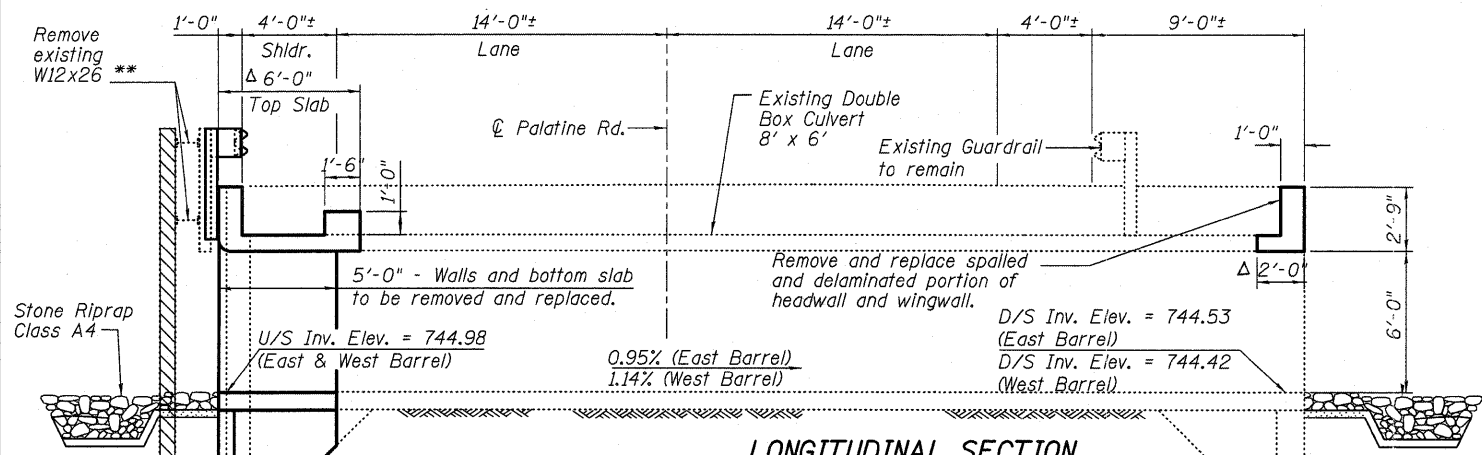
**PALATINE ROAD OVER SALT CREEK
PAVEMENT MARKING PLAN**

SCALE: 1"=50' SHEET NO. 1 OF 1 SHEETS STA. 67+65 TO STA. 73+20

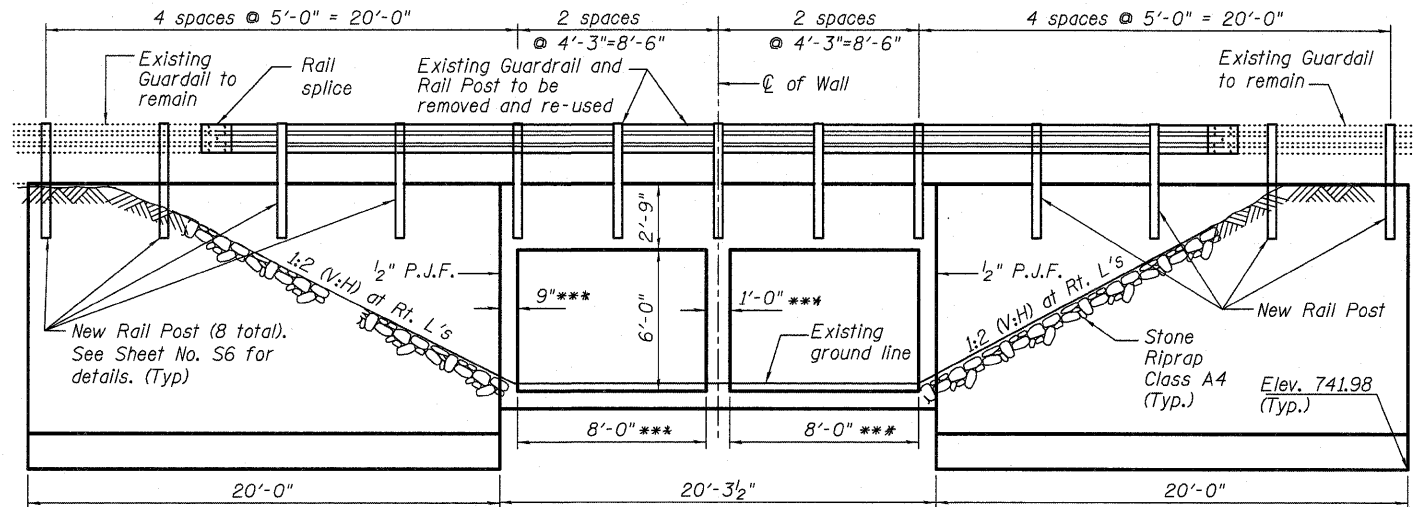
F.A.U. RTE. 1285	SECTION 2011-033-T	COUNTY COOK	TOTAL SHEETS 18	SHEET NO. 8
CONTRACT NO. 60P39				ILLINOIS FED. AID PROJECT

Benchmark:
Cross cut on northwest bolt of Hydrant at NW corner of Palatine Road and Middleton Ave. Elev. 750.66.

Existing Structure:
S.N. 016-2860 is a double barrel reinforced concrete box culvert measured 6 ft. clear height and 8 ft clear span. The total length of the structure is 48'-4". Traffic is to be maintained utilizing staged construction.



LONGITUDINAL SECTION
Dimensions shown are at Rt. angle to ϕ Roadway, unless noted otherwise.



ELEVATION
(Looking South)

*** Measurement at right angle. To be verified in field.

TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Concrete Box Culverts	Cu. Yd.	15.4
Reinforcement Bars, Epoxy Coated	Pound	8,080
Riprap, Class A4	Sq. Yd.	168
Filter Fabric	Sq. Yd.	168
Structural Steel Removal	Pound	5,010
Removing and Re-erecting Existing Railing	Foot	60
Concrete Structures	Cu. Yd.	40.0
Temporary Sheet Piling	Sq. Ft.	1,802
Concrete Removal	Cu. Yd.	25.0
Structure Excavation	Cu. Yd.	261
Furnishing & Erecting Structural Steel	Pound	380
Debris Removal	L. Sum	1

Note: Dimensions for the existing culvert were obtained by field measurements. The Contractor shall verify the dimensions of the culvert prior to start of work.

DESIGN SPECIFICATIONS

2002 AASHTO Standard Specifications for Highway Bridges, 17th Edition.

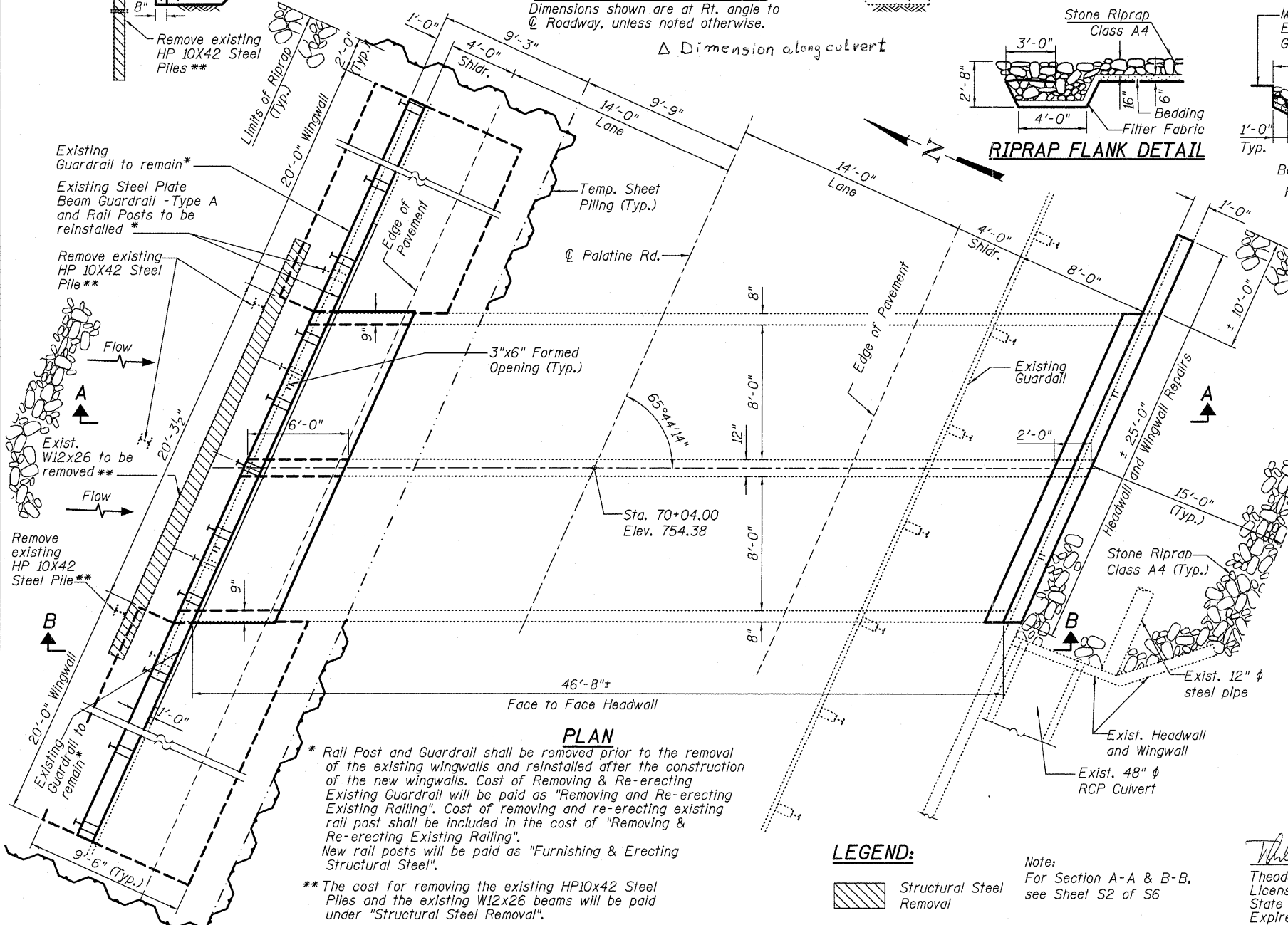
DESIGN STRESSES

FIELD UNITS
f'c = 3,500 psi
fy = 60,000 psi

LAST DELAMINATION SURVEY

June 21, 2005

GENERAL PLAN
FAU 1285 (PALATINE ROAD)
OVER SALT CREEK
COOK COUNTY
STA. 70+04.00
STRUCTURE NO. 016-2860



INDEX OF SHEETS

- S1 General Plan
- S2 Stage Removal and Construction Details
- S3 Temporary Concrete Barrier
- S4 Plan and Details
- S5 Wingwall Details
- S6 Boring Log & Guardrail Details

SCOPE OF WORK

1. Remove and replace HMA and sub-base pavement at the North side of the road, right above the North end of the box culvert.
2. Remove the existing HP 10x42 Steel Piles and W12x26 beams.
3. Repair portion of the box culvert's top and bottom slab, exterior and center walls, and reconstruct the headwall at North end.
4. Remove and replace wingwalls at North end.
5. Repair top portion of the headwall and wingwall at South end of the box culvert.
6. Remove and reinstall "Steel Plate Beam Guardrail - Type A" and Guardrail Posts at North side of the road.
7. Remove all debris/soil from the inside of culvert boxes.

LEGEND:

Structural Steel Removal

Note: For Section A-A & B-B, see Sheet S2 of S6

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN
STRUCTURE NO. 016-2860 STA. 70+04.00

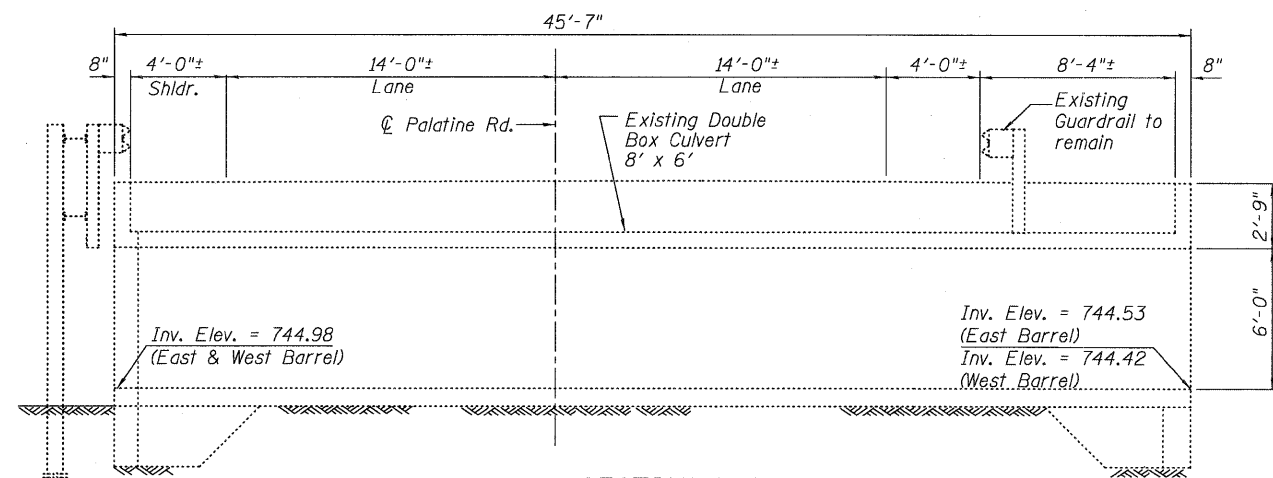
SHEET NO. S1 OF S6 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1285	2011-033-T	COOK	18	9

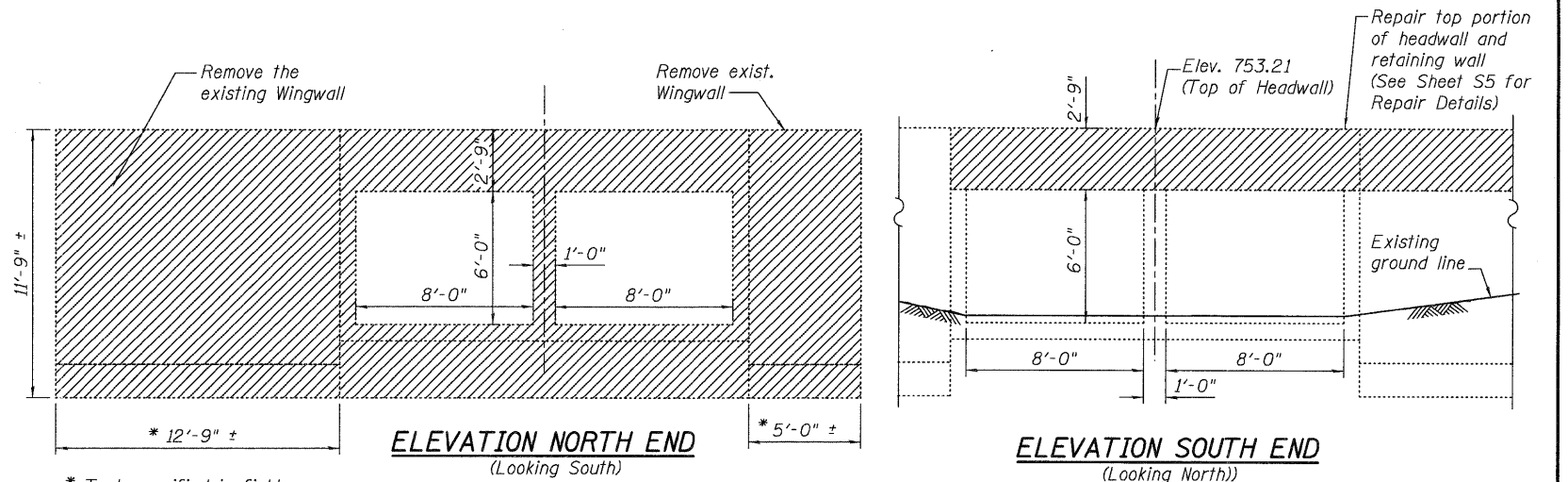
DATE: OCTOBER 6-2011 ILLINOIS FED. AID PROJECT CONTRACT NO. 60P39



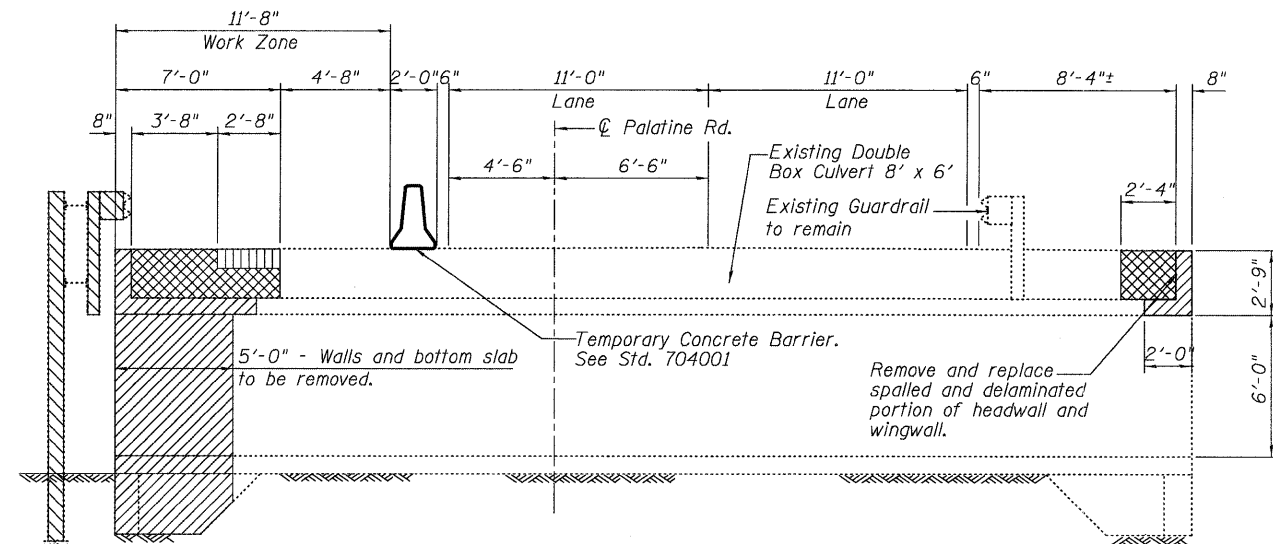
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JXH	JXH	-
MPS	MPS	-
JXH/TPG	JXH/TPG	-



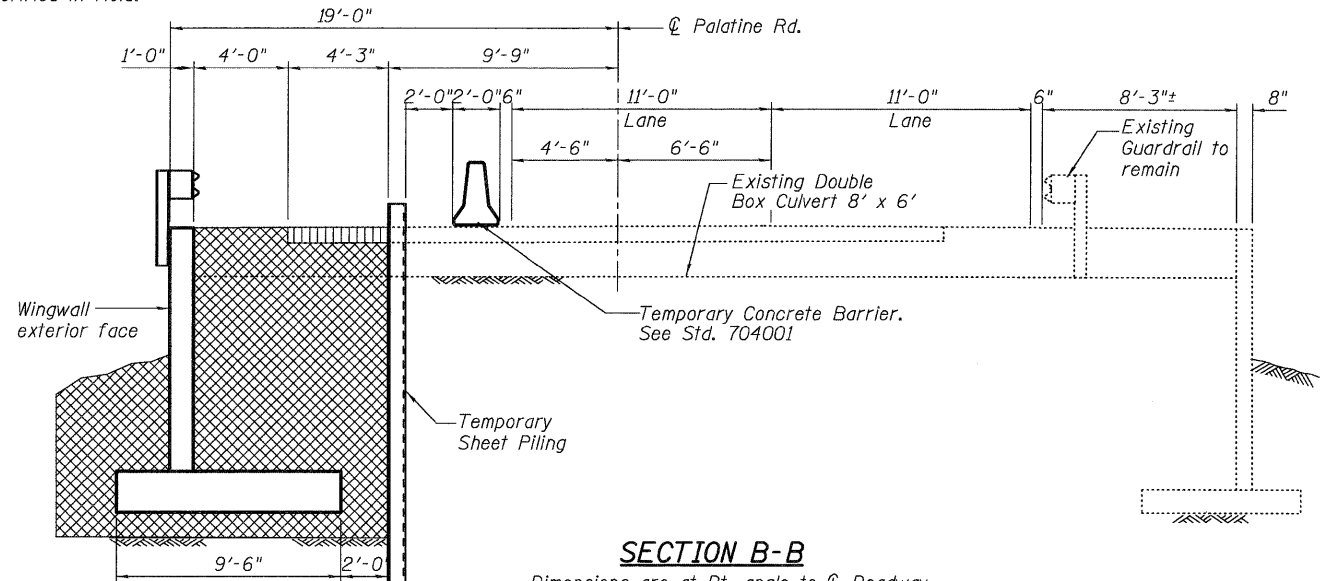
SECTION A-A
EXISTING CONDITION
Dimensions are at Rt. angle to ϕ Roadway



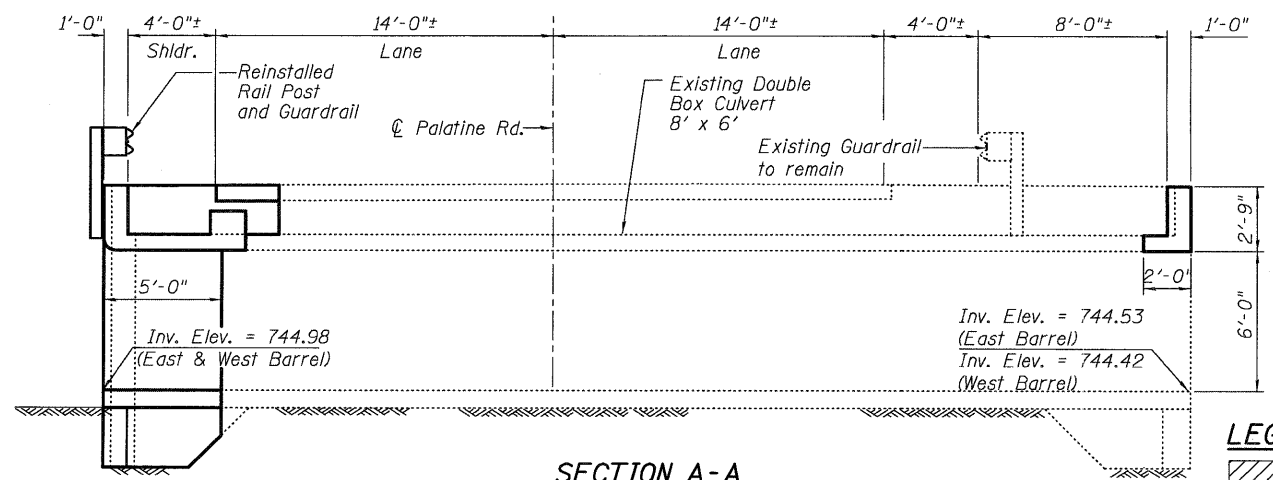
* To be verified in field.



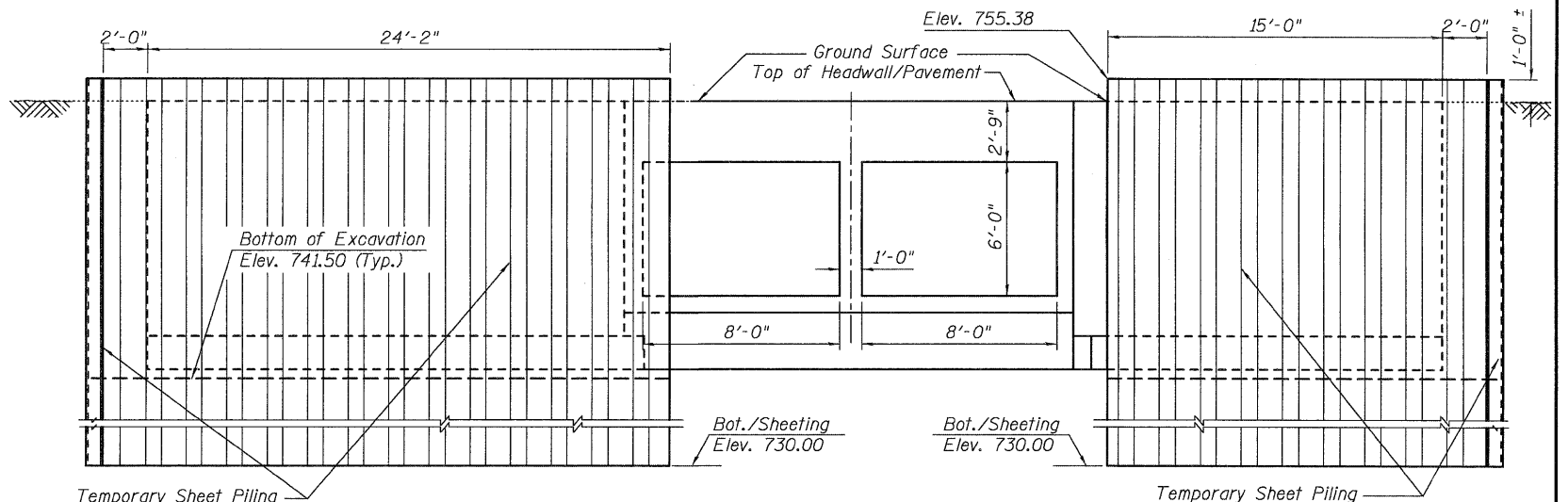
SECTION A-A
REMOVAL & CONSTRUCTION
Dimensions are at Rt. angle to ϕ Roadway



SECTION B-B
Dimensions are at Rt. angle to ϕ Roadway



SECTION A-A
FINAL CONDITION
Dimensions are at Rt. angle to ϕ Roadway



WEST END

EAST END

TEMPORARY SHEET PILING
Minimum Section Modulus = 11.20 in³/ft.

- LEGEND:**
- Concrete Removal
 - Structure Excavation
 - Structural Steel Removal
 - Pavement Removal



USER NAME =	DESIGNED - JPM	REVISED -
PLOT SCALE =	CHECKED - JXH	REVISED -
PLOT DATE =	DRAWN - MPS	REVISED -
	CHECKED - JXH/TPG	REVISED -

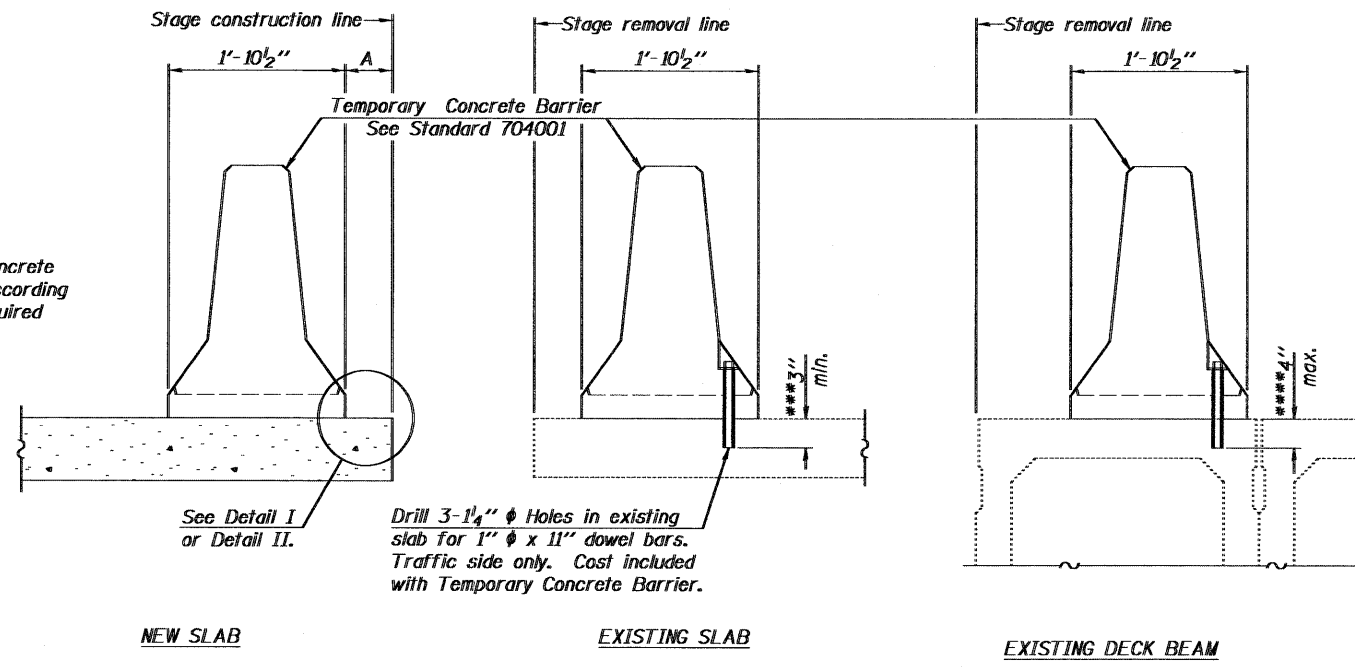
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGE REMOVAL AND CONSTRUCTION DETAILS
STRUCTURE NO. 016-2860 STA. 70+04.00

SHEET NO. S2 OF S6 SHEETS

F.A.U. RTE. 1285	SECTION 2011-033-T	COUNTY COOK	TOTAL SHEETS 18	SHEET NO. 10
DATE: OCTOBER 6-2011 [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 60P39	

When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".



SECTIONS THRU SLAB OR DECK BEAM

NOTES

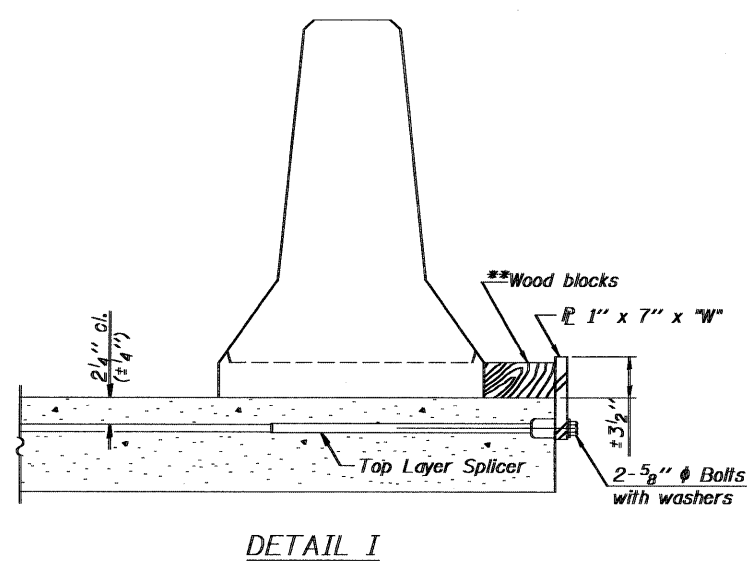
Detail I - With Bar Splicer or Couplers:
Connect one (1) 1" x 7" x "W" steel \bar{L} to the top layer of couplers with 2-5/8" ϕ bolts screwed to coupler at approximate \bar{C} of each barrier panel.

Detail II - With Extended Reinforcement Bars:
Connect one (1) 1" x 7" x "W" steel \bar{L} to the concrete slab or concrete wearing surface with 2-5/8" ϕ Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate \bar{C} of each barrier panel.

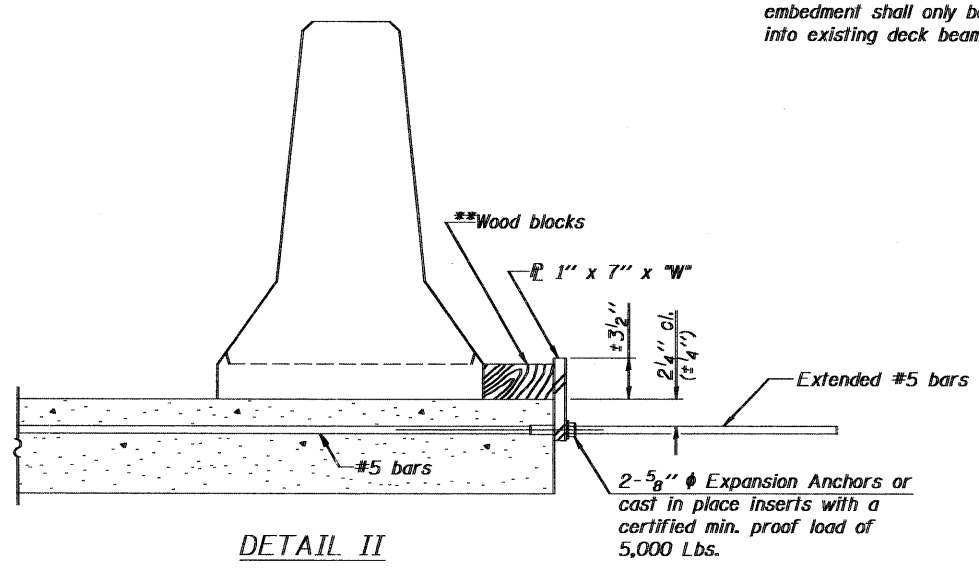
Cost of anchorage is included with Temporary Concrete Barrier. The 1" x 7" x "W" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

*** Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

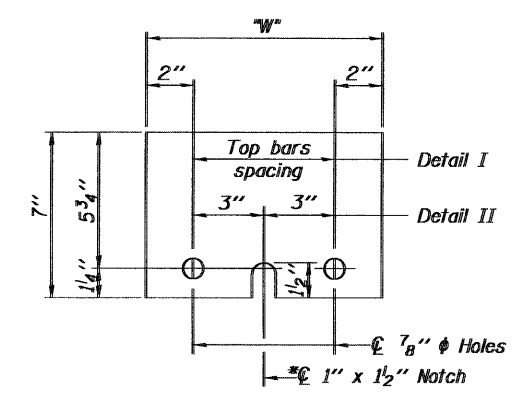
**** If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



DETAIL I



DETAIL II



STEEL RETAINER \bar{L} 1" x 7" x "W"
* Required only with Detail II

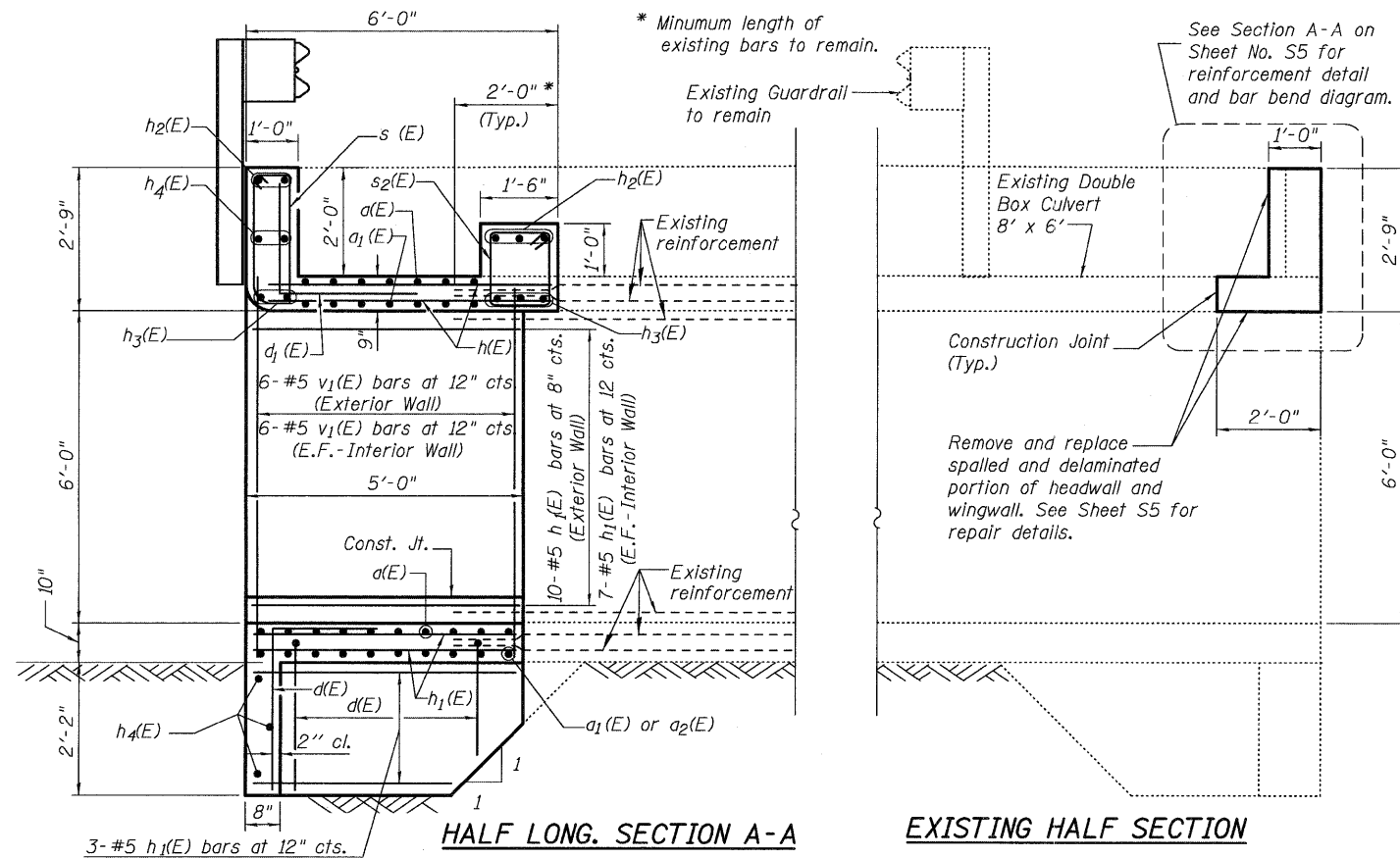
** Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

"W" = Top bars spacing + 4"

R-27

7-1-10

	USER NAME =	DESIGNED - JPM	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION STRUCTURE NO. 016-2860 STA. 70 + 04.00	F.A.U. RTE.:	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE =	CHECKED - JXH	REVISED -			1285	2011-033-T	COOK	18	11
	PLOT DATE =	DRAWN - MPS	REVISED -			CONTRACT NO. 60P39				
		CHECKED - JXH/TPG	REVISED -			DATE: OCTOBER 6-2011 [ILLINOIS] FED. AID PROJECT				



Note: Reinforcement bars shall conform to the requirements of ASTM A706 Grade 60. See Special Provisions.

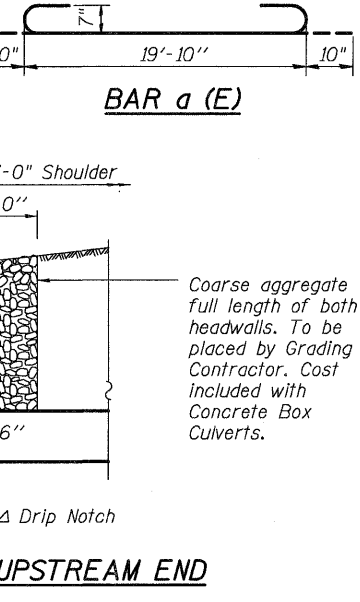
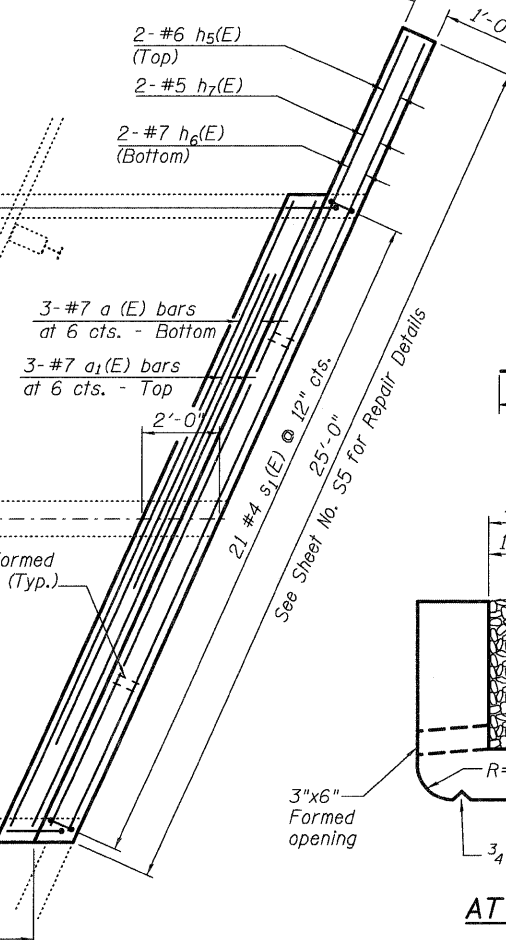
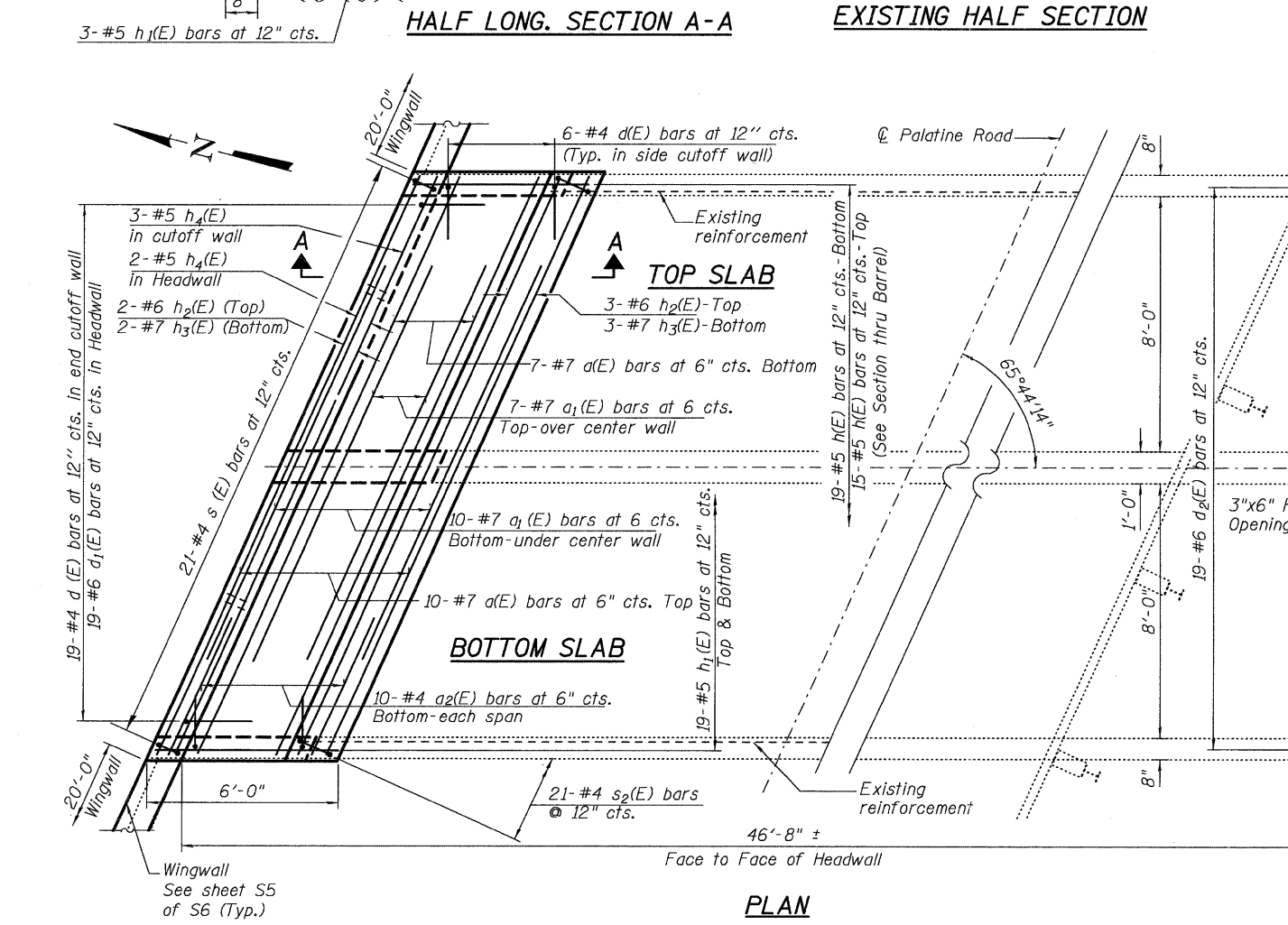
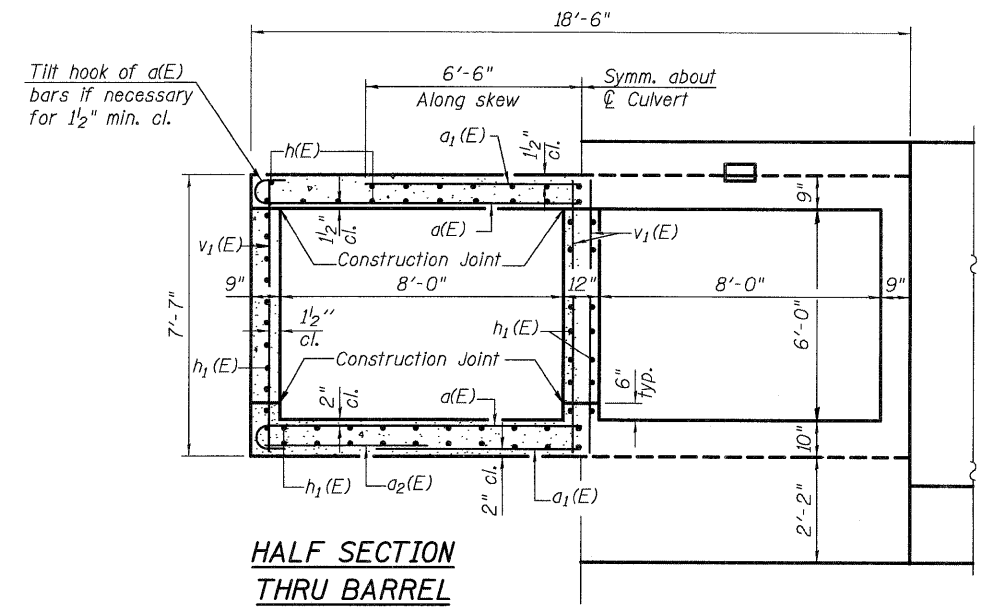
Reinforcement bars designated (E) shall be epoxy coated.

Existing bars to remain shall be cleaned, straightened and incorporated into new concrete.

Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal. See Sheet No. S6 for details depicting method of attaching guardrail posts to the headwall.

MIN. BAR LAP

Bar	Barrel	Winwall
#4	1'-4"	1'-8"
#5	1'-8"	2'-2"
#6	2'-0"	2'-7"
#7	2'-9"	3'-5"



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a (E)	20	#7	21'-6"	U
a1(E)	20	#7	13'-0"	U
a2(E)	20	#4	4'-10"	—
d (E)	31	#4	4'-6"	—
d1(E)	19	#6	5'-6"	—
d2(E)	19	#6	3'-9"	—
h (E)	34	#5	5'-10"	—
h1(E)	78	#5	4'-10"	—
h2(E)	5	#6	20'-0"	—
h3(E)	5	#7	20'-0"	—
h4(E)	5	#5	20'-0"	—
h5(E)	2	#6	24'-9"	—
h6(E)	2	#7	24'-9"	—
h7(E)	2	#5	24'-9"	—
s (E)	21	#4	7'-1"	U
s1(E)	21	#4	7'-3"	U
s2(E)	21	#4	6"-3"	U
v1(E)	24	#5	7'-4"	—
Concrete Box Culverts			Cu. Yd.	15.4
Reinforcement Bars, Epoxy Coated			Pound	3,600

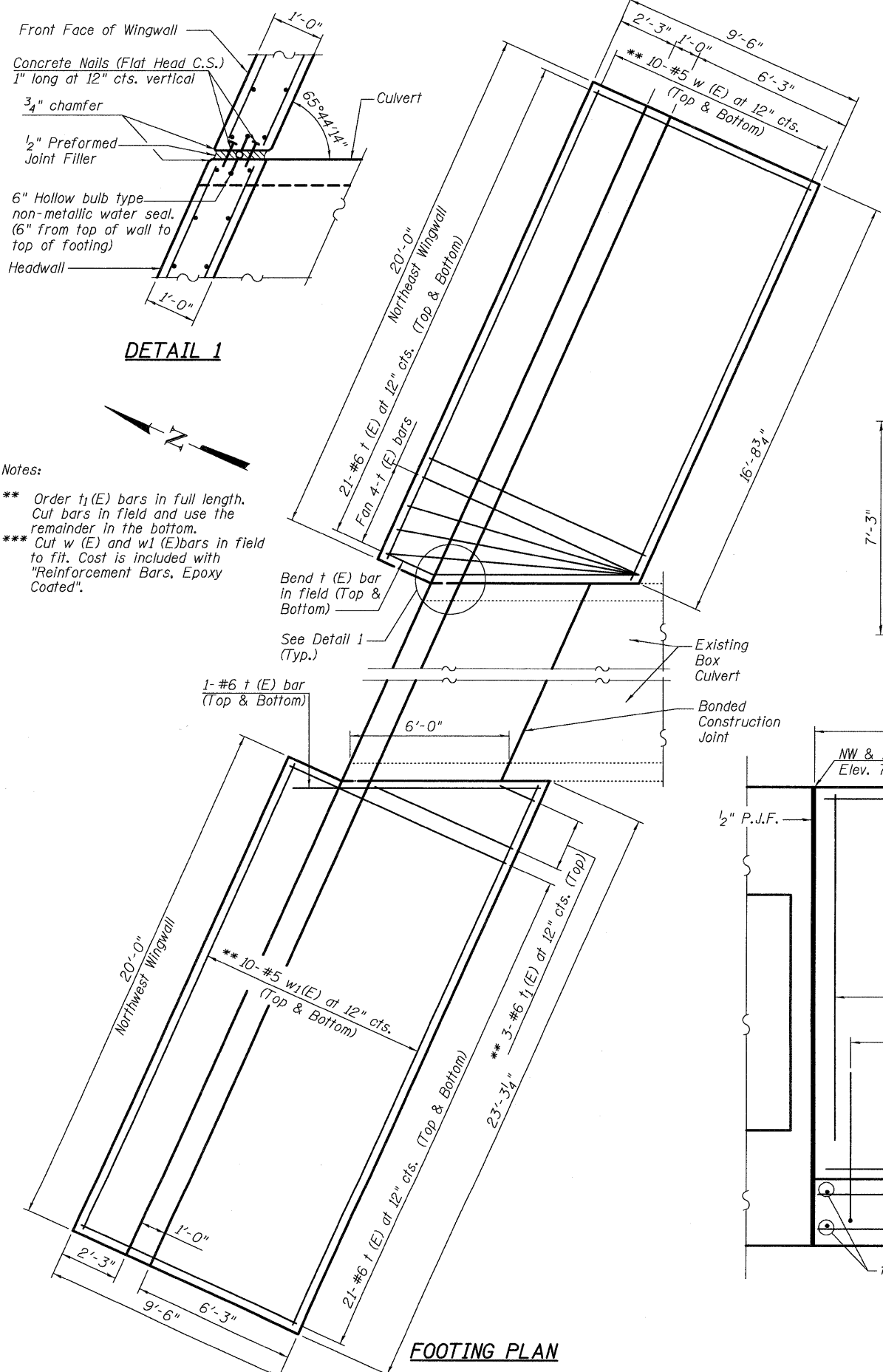


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PLOT SCALE =	CHECKED - JXH	REVISED -
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

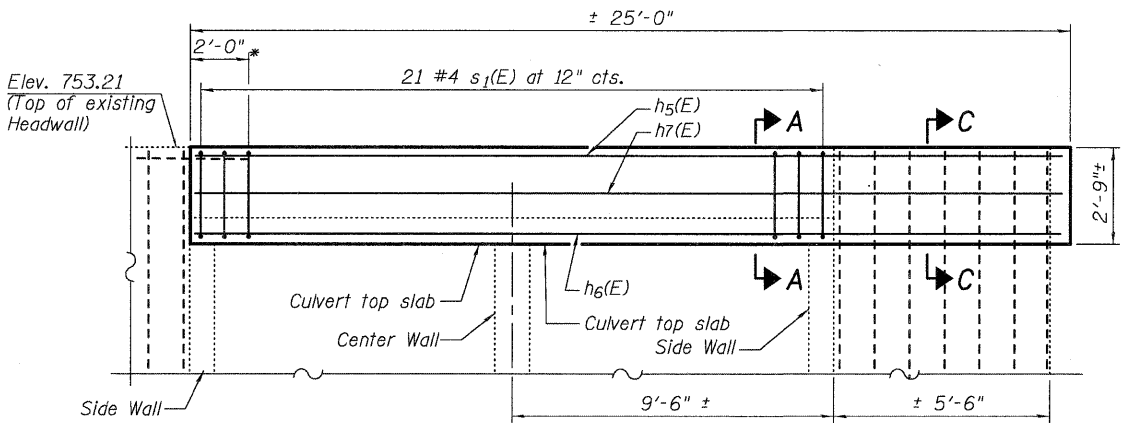
PLAN AND DETAILS
STRUCTURE NO. 016-2860 STA. 70 + 04.00
SHEET NO. S4 OF S6 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1285	2011-033-T	COOK	18	12
DATE: OCTOBER 6-2011 [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 60P39	



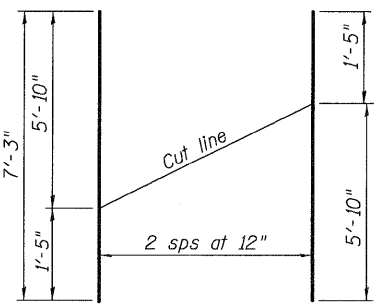
DETAIL 1

Notes:
 ** Order t₁(E) bars in full length. Cut bars in field and use the remainder in the bottom.
 *** Cut w(E) and w₁(E) bars in field to fit. Cost is included with "Reinforcement Bars, Epoxy Coated".

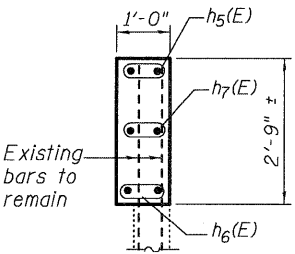


CULVERT/WINGWALL SOUTH END ELEVATION

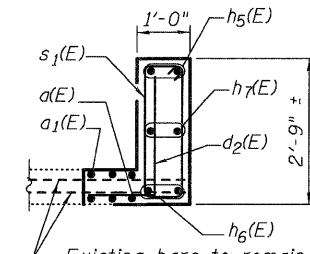
* Length of existing bars to remain.
 Note: Existing bars to remain shall be cleaned, straightened and incorporated into new concrete.



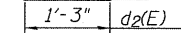
BAR t₁(E)
FIELD CUTTING DIAGRAM



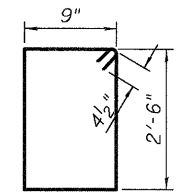
SECTION C-C



SECTION A-A



BAR d₂(E)



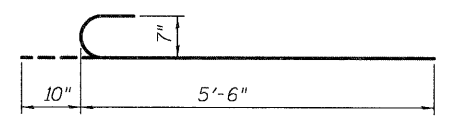
BAR s₁(E)

**BILL OF MATERIAL
 NW & NE WINGWALLS**

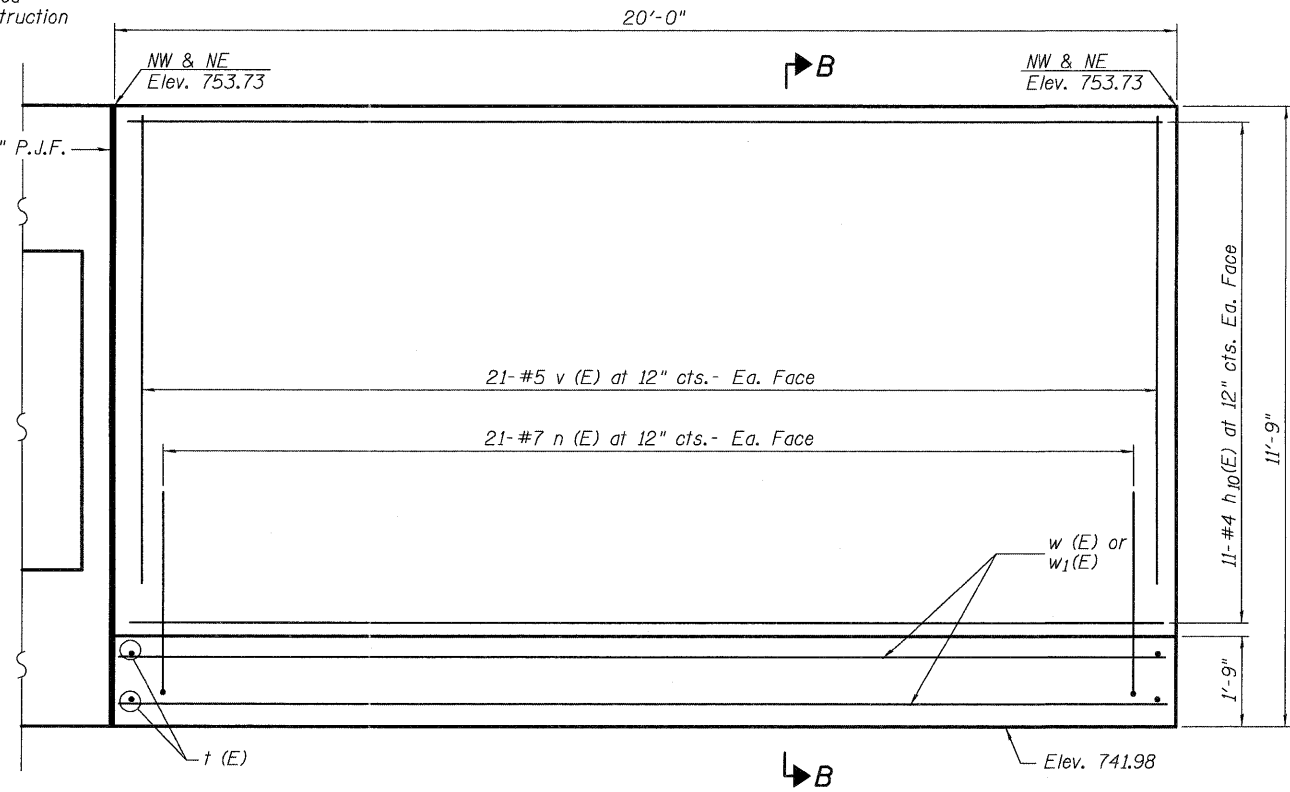
Bar	No.	Size	Length	Shape	
h ₁₀ (E)	44	#4	19'-8"		
n(E)	84	#7	6'-4"	C	
t(E)	86	#6	9'-2"		
t ₁ (E)	3	#6	7'-3"		
v(E)	84	#5	8'-0"		
w(E)	20	#5	19'-8"		
w ₁ (E)	20	#5	22'-11"		
Concrete Structures				Cu. Yd.	40.0
Reinforcement Bars, Epoxy Coated				Pound	4,480

MIN. BAR LAP

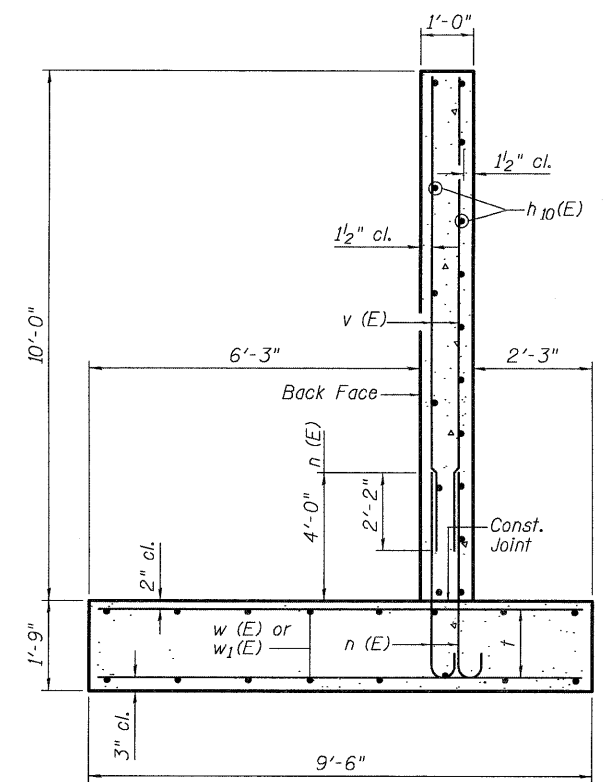
Bar	Barrel	Wingwall
#4	1'-4"	1'-8"
#5	1'-8"	2'-2"
#6	2'-0"	2'-7"
#7	2'-9"	3'-5"



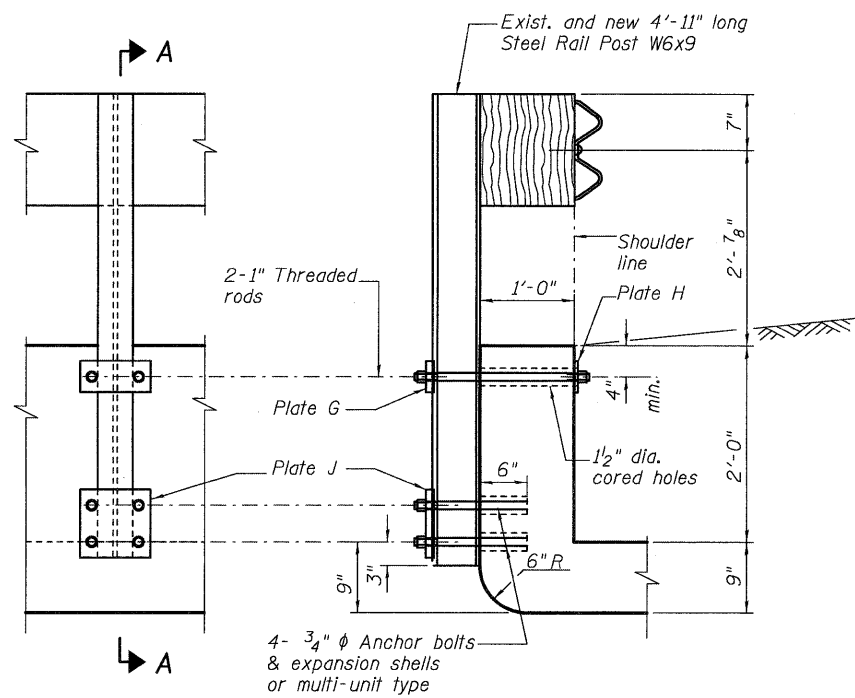
BAR n(E)



NW WINGWALL ELEVATION - (SHOWN)
NE WINGWALL ELEVATION - OPPOSITE HAND



SECTION B-B



ELEVATION

SECTION A-A

RAILING REINSTALLATION DETAIL

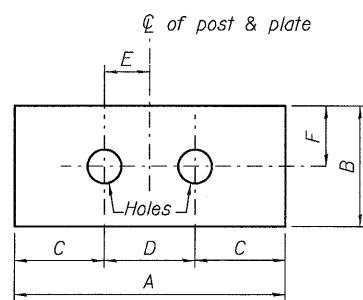


PLATE TYPES G & H

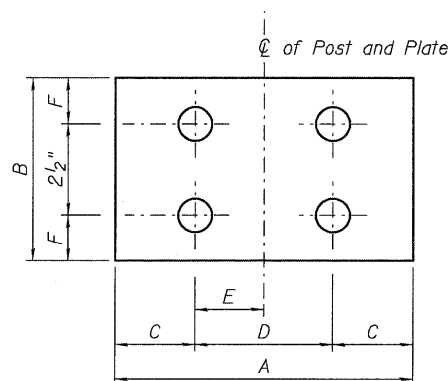


PLATE TYPE J

PLATE DIMENSIONS								
Type	A	B	C	D	E	F	Hole Dia.	Thickness
G	9"	4"	1 1/2"	6"	3"	2"	1 1/8"	1"
H	9"	4"	1 1/2"	6"	3"	2"	1 1/8"	1 1/2"
J	9"	5"	1 1/2"	6"	3"	1 1/4"	7/8"	1"

GENERAL NOTES

For details of guardrail elements not shown, see Standard 630001.

All threaded rods shall be installed with heavy hex nuts and standard washers.

Existing Rail Posts, Guardrail and Wood Blockout are to be re-used. Existing rail post is 5'-4" long. Cost of trimming shall be included with the pay item "Removing and Re-erecting Existing Railing".

Cost and installation of the wood blockout for the new Rail posts shall be included with the pay item "Furnishing & Erecting Structural Steel"

For detail and more information of guardrail post connection to culvert, See Standard 630101.

Geo Services, Inc. Geotechnical, Environmental & Civil Engineering 805 Amber Court, Suite 204 Naperville, Illinois 60565 (630) 355-2838		SOIL BORING LOG				PAGE 1 of 1									
ROUTE <u>FAU Rte. 1285 (Palatine Rd.)</u> DESCRIPTION <u>Palatine Road Over Salt Creek Culvert Improvements</u>		DATE <u>6/2/2011</u>				LOGGED BY <u>RJ</u>									
SECTION <u>0507 RS-1</u> LOCATION <u>Palatine Township, T 42 N. R 10 E, Sections 16 & 21, 3rd P.M.</u>		GSI JOB No. <u>11055</u>													
COUNTY <u>Cook</u> DRILLING METHOD <u>Hollow Stem Auger</u> HAMMER TYPE <u>CME Automatic</u>															
STRUCT. NO. <u>016-2860</u>		DEPT		UCS		MOIST		Surface Water Elev. <u>746.2</u>		DEPT		UCS		MOIST	
Station <u>-</u>		H		S		I		Stream Bed Elev. <u>744.3</u>		H		S		I	
BORING NO. <u>B-1</u>		T		Qu		T		Groundwater Elevation:		T		Qu		T	
Station: <u>70+88</u>		H		S		I		First Encounter <u>725.1</u>		H		S		I	
Offset: <u>24.0' Left</u>		T		Qu		T		Upon Completion <u>723.6</u>		T		Qu		T	
Ground Surface Elev. <u>753.6</u>		(ft)		(/6")		(tsf)		After _____ Hrs.		(ft)		(/6")		(tsf)	
10" ASPHALT & STONE		752.6		AS		1									
TOPSOIL-black and gray				10						4				110	
				6						4					
				6		NP		18		8		2.2B		19	
		750.6													
SILTY CLAY-dark brown-very stiff (A-6)				3				95		4				109	
				2						6					
				-5		4		2.0B		-25		9		3.2B 19	
		748.1													
CLAY LOAM-gray-very stiff to hard (A-6)				2				94		4				110	
				3						7					
				4		1.6B		27		9		3.4B		18	
				1				99		5				112	
				2						5					
				-10		3		0.9B		-30		9		8.7B 18	
		743.1													
End Of Boring @ -30.0' Hollow Stem Augers CME Automatic Hammer				2				104							
				3											
				6		2.0B		23							
				3				118							
				4											
				-15		6		2.1B		-35					
				3				111							
				5											
				7		3.2B		18							
				4				113							
				5											
				-20		5		3.1B		-40					

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) ST-Shelby Tube Sample VS-Vane Shear Test The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) The Unit Dry Weight (pcf) is noted in italics above moist (%) NR-No Recovery



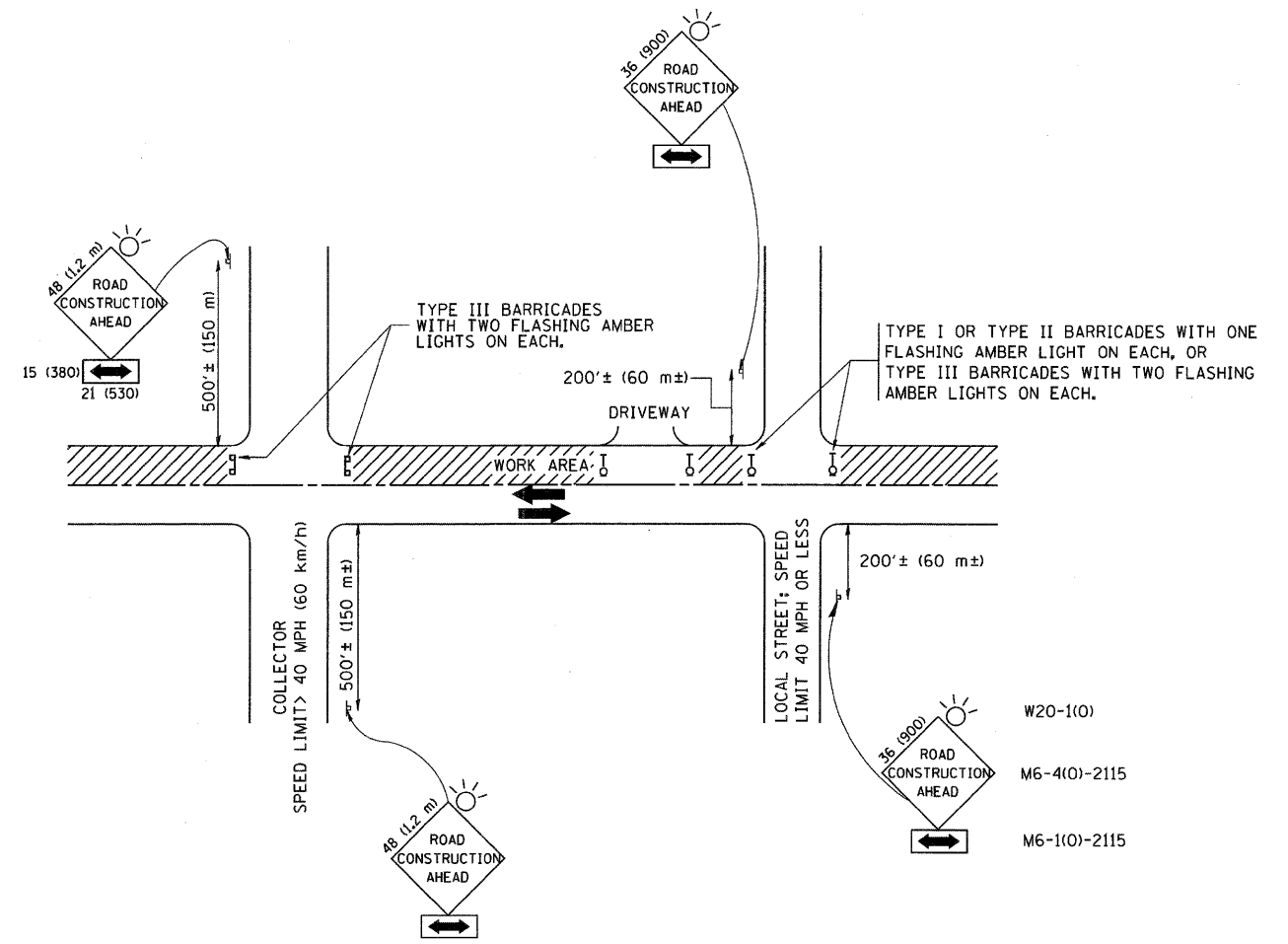
USER NAME =	DESIGNED - JPM	REVISED -
PLOT SCALE =	CHECKED - JXH	REVISED -
PLOT DATE =	DRAWN - MPS	REVISED -
	CHECKED - JXH/TPG	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BORING LOG & GUARDRAIL DETAILS
STRUCTURE NO. 016-2860 STA. 70+04.00

SHEET NO. 56 OF 56 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1285	2011-033-T	COOK	18	14
DATE: OCTOBER 6-2011 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60P39	



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

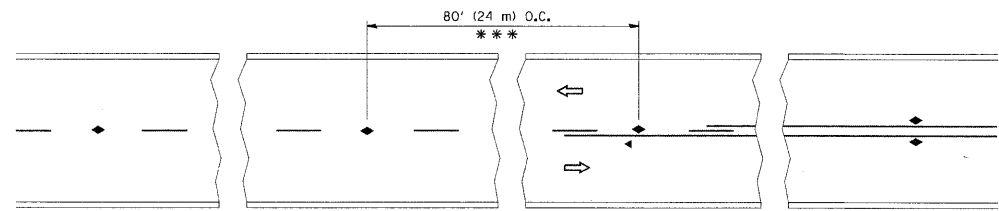
NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
 - C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
 - D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

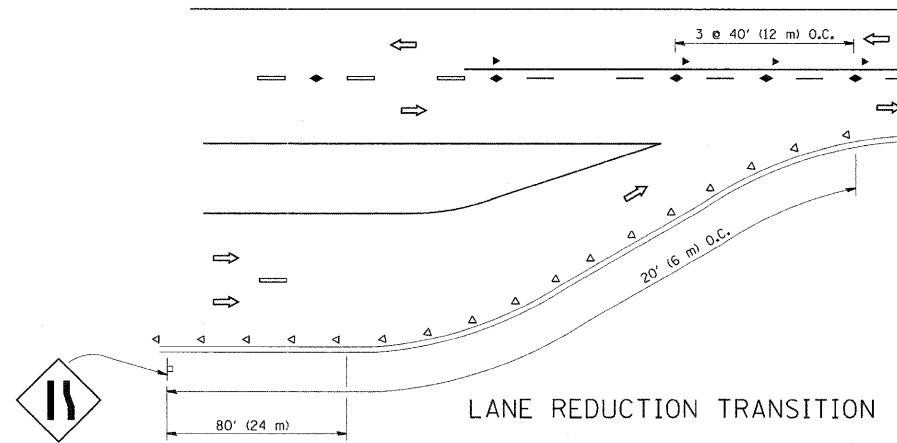
All dimensions are in millimeters (inches) unless otherwise shown.

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	PLOT SCALE = 50,000' / IN.	DRAWN -	REVISED - A. HOUSEH 03-06-96		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	TC-10		CONTRACT NO. 60P39	
	PLOT DATE = 1/4/2008	CHECKED -	REVISED - A. HOUSEH 10-15-96		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00									

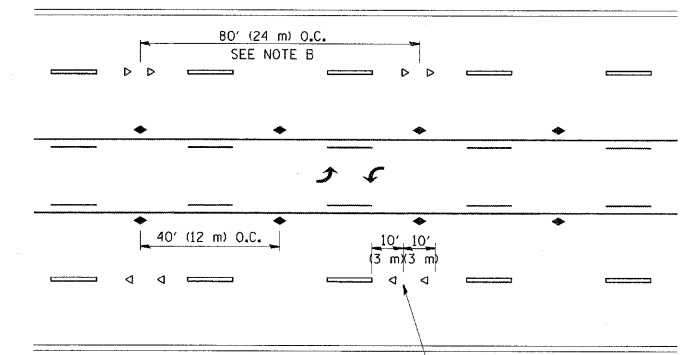


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

TWO-LANE/TWO-WAY

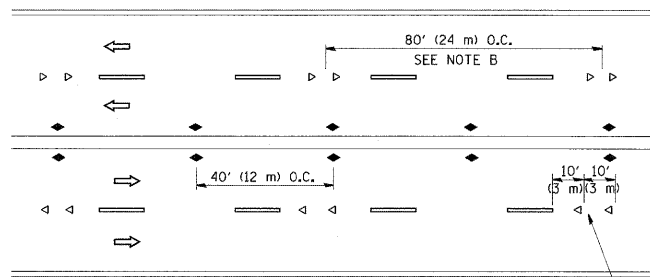


LANE REDUCTION TRANSITION



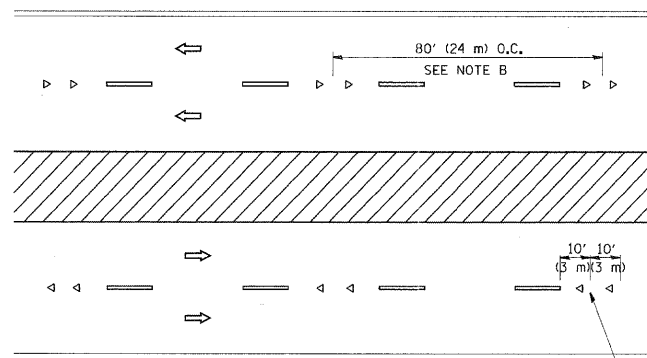
SEE NOTE A

TWO-WAY LEFT TURN



SEE NOTE A

MULTI-LANE/UNDIVIDED



SEE NOTE A

MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

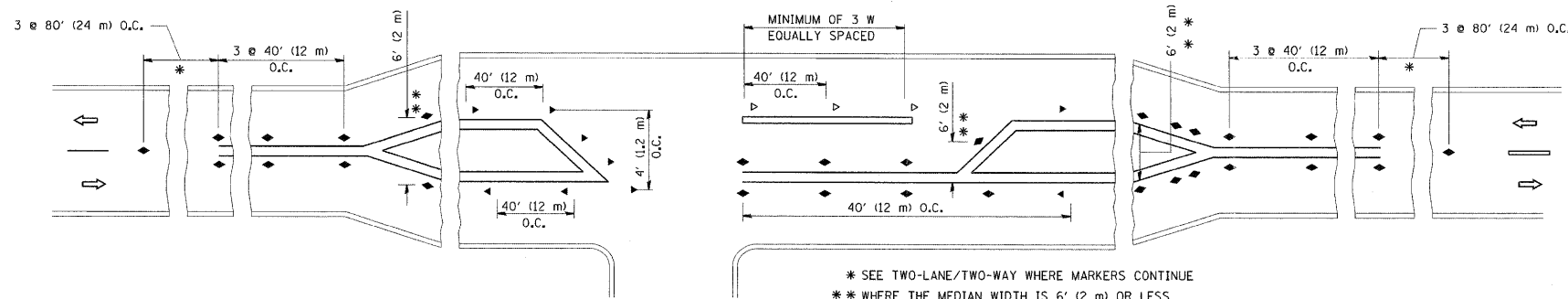
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

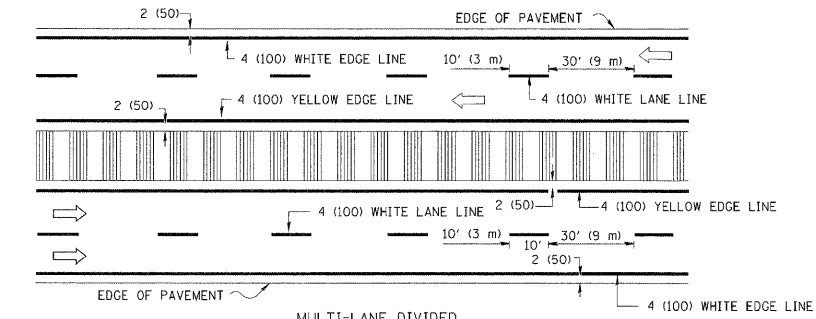
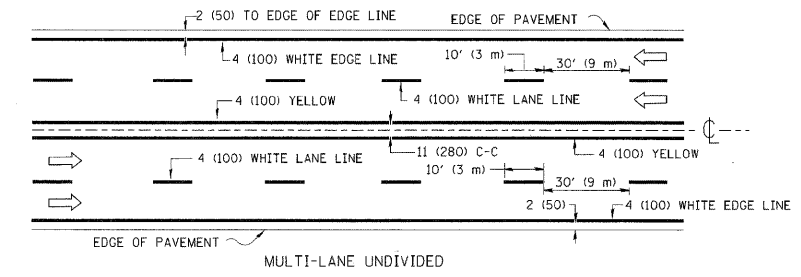
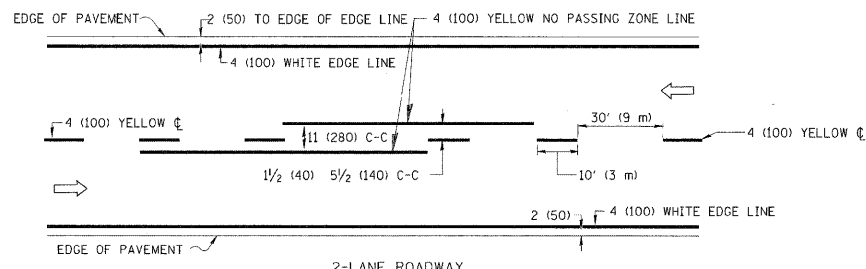


* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

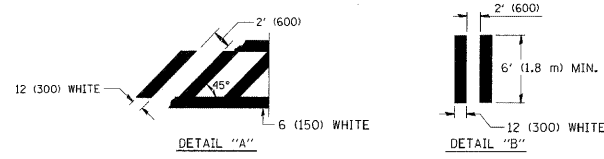
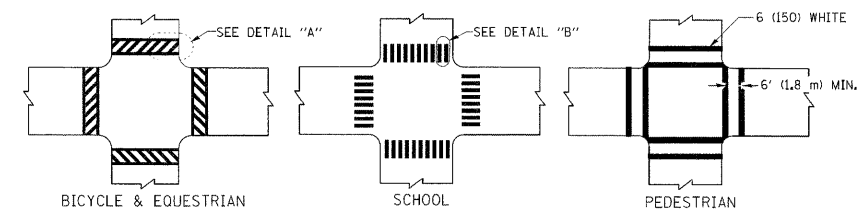
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = c:\pw_work\pwwdot\leisa\d0108315\td11.dgn	USER NAME = leisa	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			F.A.U. RTE. 1285	SECTION 2011-033-T	COUNTY COOK	TOTAL SHEETS 18	SHEET NO. 16
	PLOT SCALE = 50,000' / IN.	DRAWN -	REVISED - T. RAMMACHER 03-12-99		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
PLOT DATE = 3/2/2011	CHECKED -	REVISED - T. RAMMACHER 01-06-00	REVISED - C. JUCIUS 09-09-09									
	DATE -											

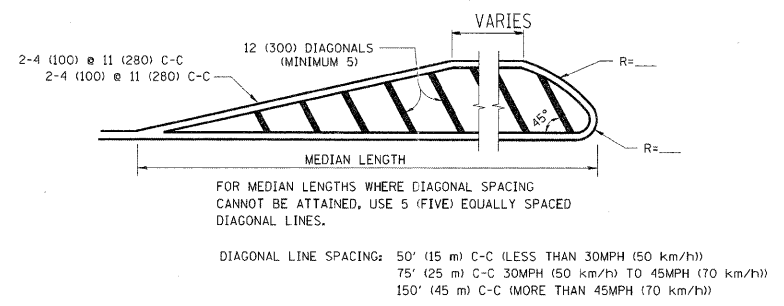
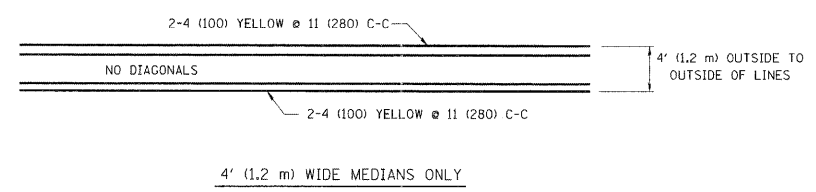


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

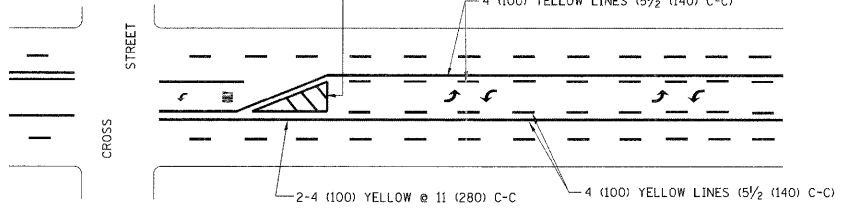
TYPICAL LANE AND EDGE LINE MARKING



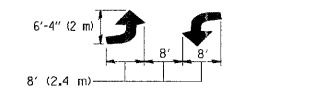
TYPICAL CROSSWALK MARKING



MEDIANS OVER 4' (1.2 m) WIDE

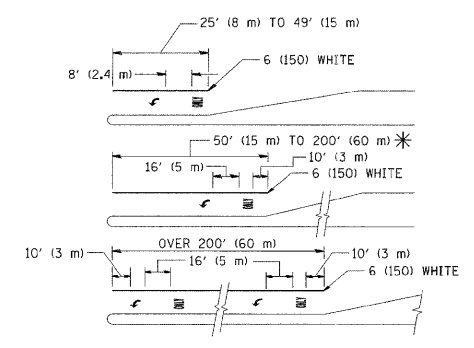


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

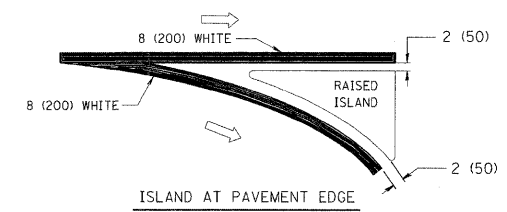
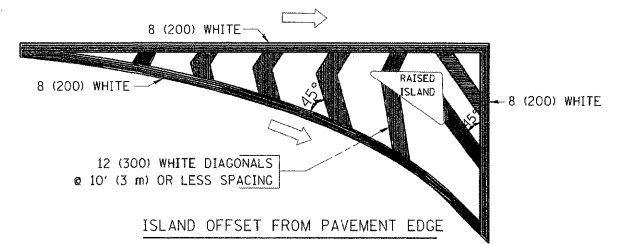


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

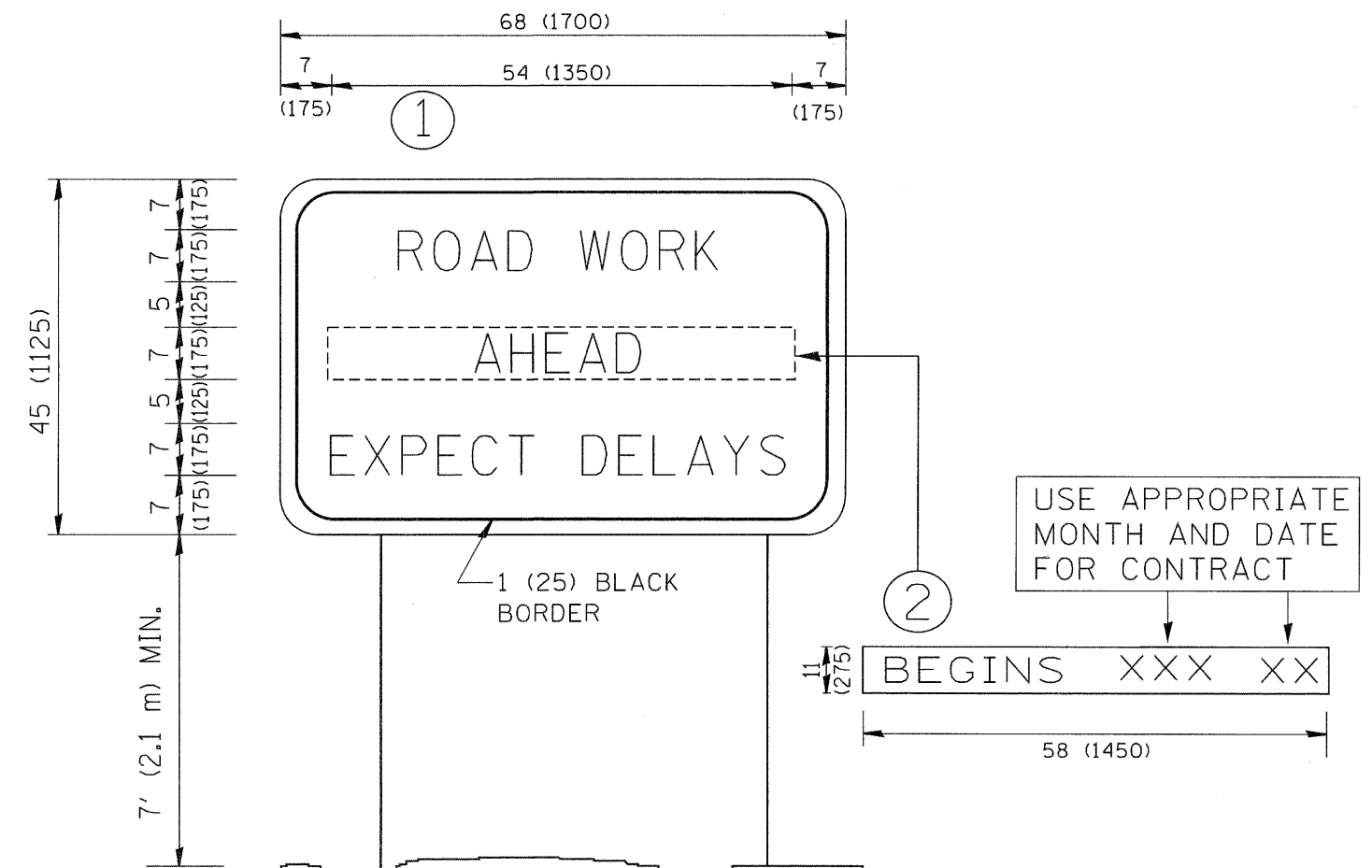
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = drvokosgn	DESIGNED - EVERS	REVISED -T. RAMMACHER 10-27-94
c:\pwork\p1dot\drvokosgn\d8108315\to3.dgn		DRAWN -	REVISED -C. JUCIUS 09-09-09
	PLOT SCALE = 50,000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TYPICAL PAVEMENT MARKINGS		1285	2011-033-T	COOK	18	17
SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA.	TO STA.	
		CONTRACT NO. 60P39		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\vc22.dgn	USER NAME = goglieno	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN	F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50,000' / IN.	DRAWN -	REVISED - R. MIRS 12-11-97			1285	2011-033-T	COOK	18	18
	PLOT DATE = 1/4/2008	CHECKED -	REVISED - T. RAMMACHER 02-02-99			TC-22		CONTRACT NO. 60P39		
	DATE -	REVISED - C. JUCIUS 01-31-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		