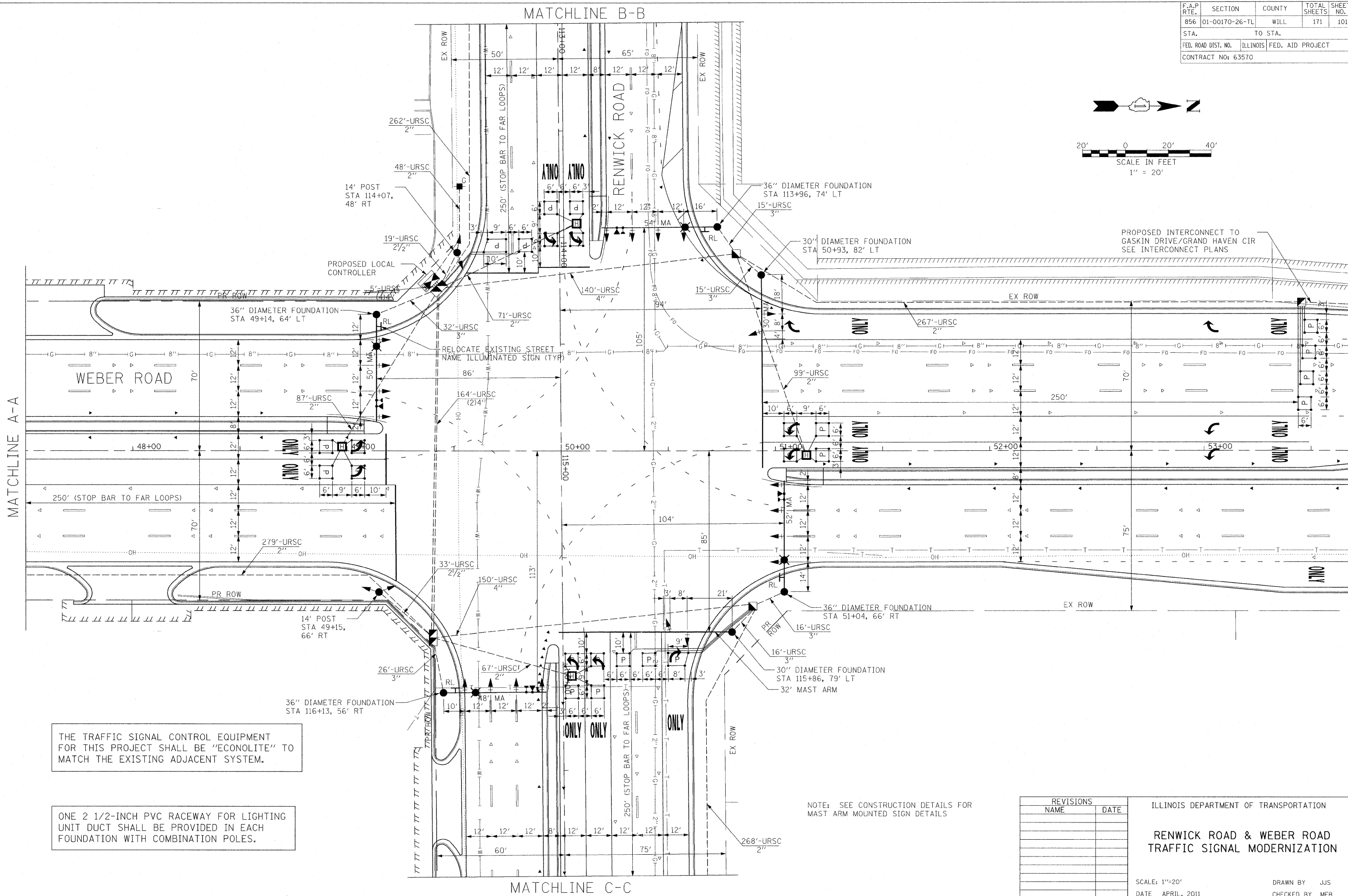
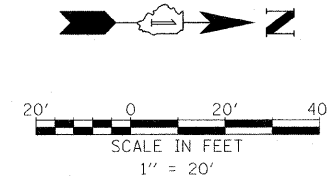


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	101
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO: 63570				



PROPOSED INTERCONNECT TO GASKIN DRIVE/GRAND HAVEN CIR
SEE INTERCONNECT PLANS

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

ONE 2 1/2-INCH PVC RACEWAY FOR LIGHTING UNIT DUCT SHALL BE PROVIDED IN EACH FOUNDATION WITH COMBINATION POLES.

NOTE: SEE CONSTRUCTION DETAILS FOR MAST ARM MOUNTED SIGN DETAILS

REVISIONS	
NAME	DATE

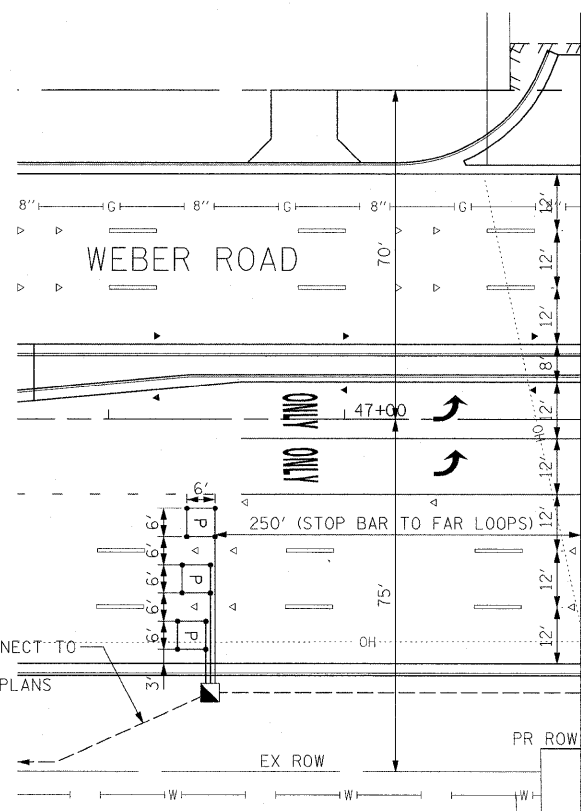
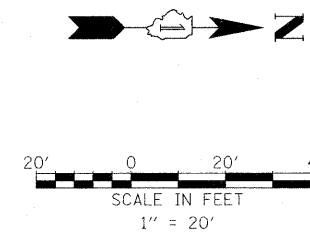
ILLINOIS DEPARTMENT OF TRANSPORTATION
**RENWICK ROAD & WEBER ROAD
TRAFFIC SIGNAL MODERNIZATION**

SCALE: 1"=20'
DATE APRIL, 2011

DRAWN BY JJS
CHECKED BY MFB

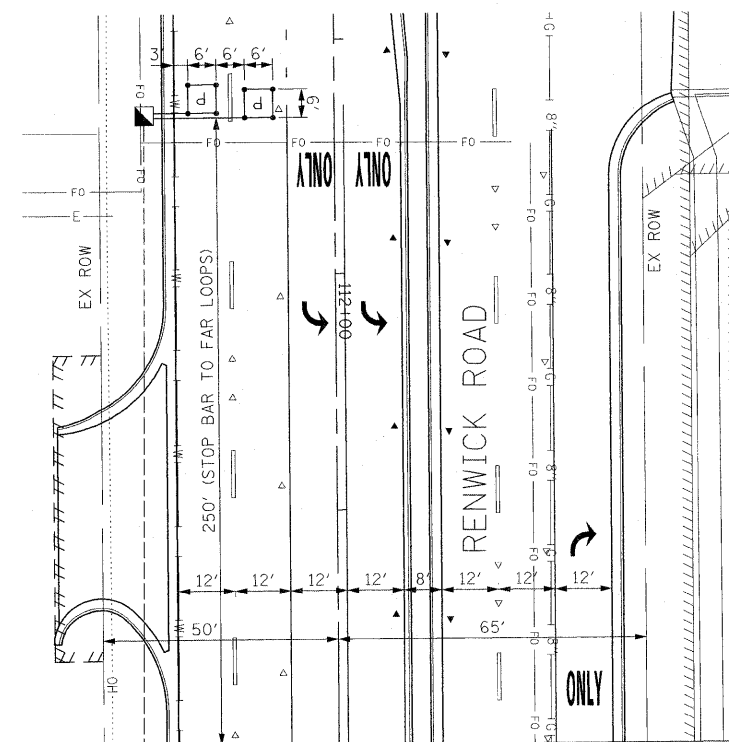
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	102
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO: 63570				

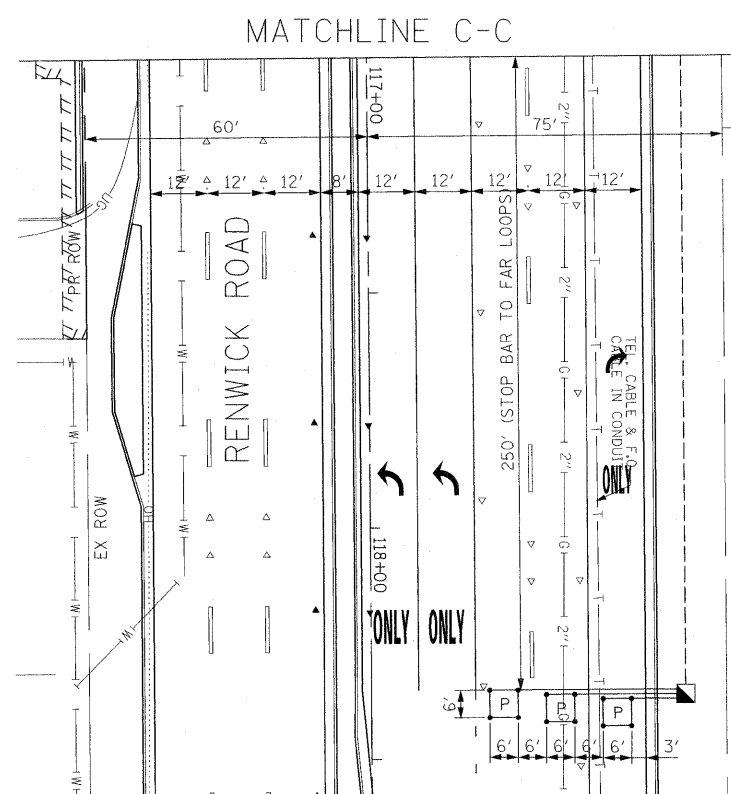


PROPOSED INTERCONNECT TO RYAN DRIVE
SEE INTERCONNECT PLANS

MATCHLINE A-A



MATCHLINE B-B



MATCHLINE C-C

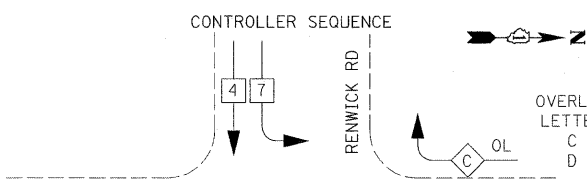
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

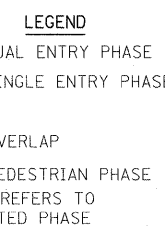
RENWICK ROAD & WEBER ROAD
TRAFFIC SIGNAL MODERNIZATION

SCALE: 1"=20'
DATE: APRIL, 2011

DRAWN BY: JJS
CHECKED BY: MFB

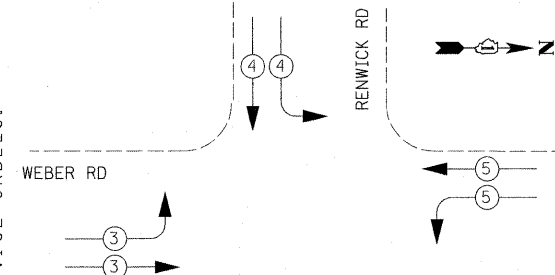


OVERLAP PERMISSIVE PROTECTED
 LETTER PHASE PHASE
 C = 6 + 7
 D = 8 + 1



PHASE DESIGNATION DIAGRAM

EMERGENCY VEHICLE PREEMPTION SEQUENCE



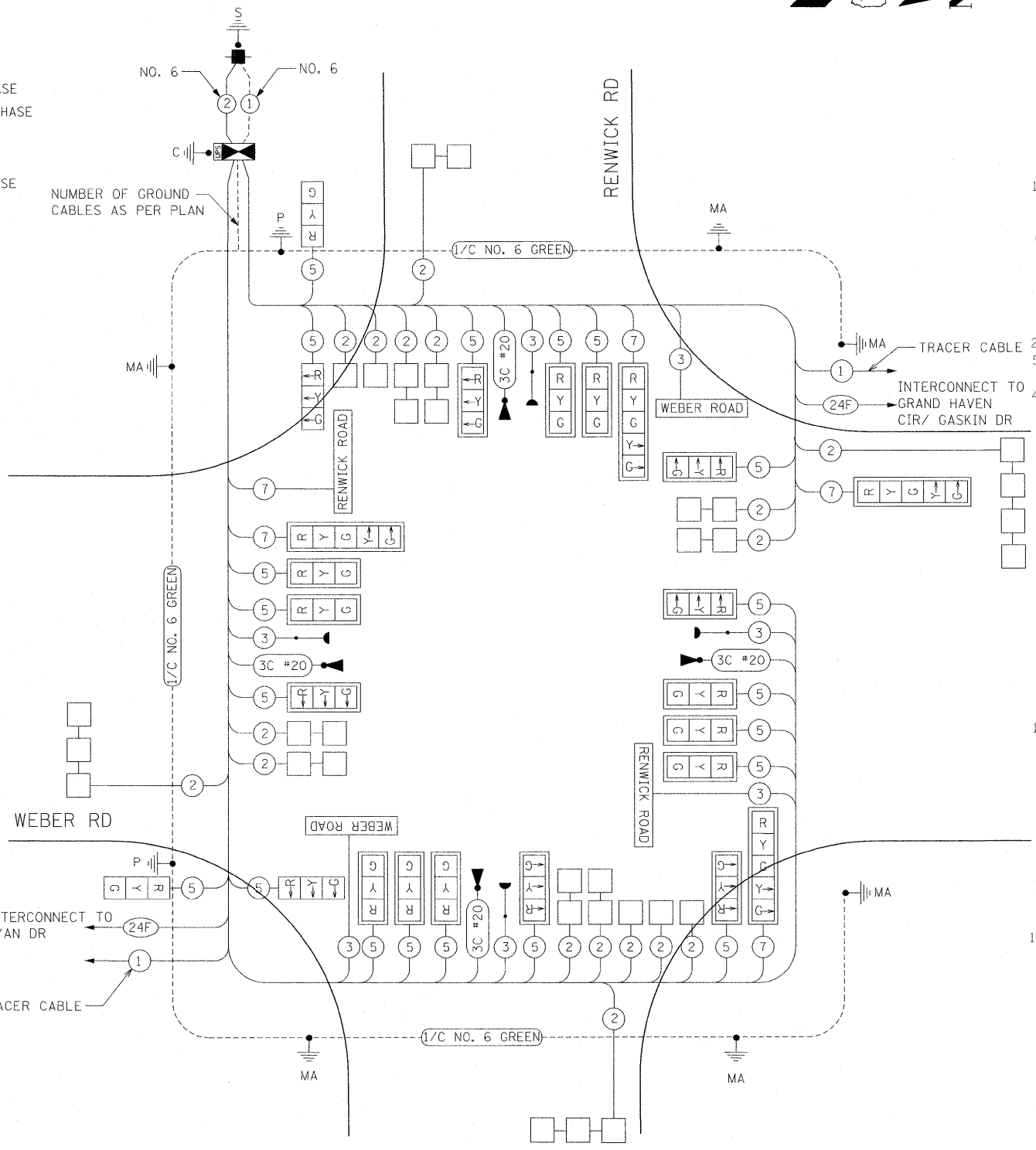
EMERGENCY VEHICLE PREEMPTOR	3	4	5	6
MOVEMENT				

NOTE: EQUIPMENT GROUND CONDUCTOR (GREEN COLOR CODED) SPLICE TO FRAME AND COVER IS REQUIRED FOR ALL HANDHOLES OR DOUBLE HANDHOLES THAT CARRY SIGNAL CABLES AND SERVICE CABLES.

TYPE	NO. LAMPS	WATTAGE		% OPERATION	TOTAL WATTAGE
		INCAND.	LED		
SIGNAL (RED)	16		17	0.50	136.00
(YELLOW)	16		25	0.25	100.00
(GREEN)	16		15	0.25	60.00
ARROW	32		12	0.10	38.40
PED. SIGNAL	0		25	1.00	0.00
CONTROLLER	1		100	1.00	100.00
ILLUM. SIGN	4		25	0.50	50.00
FLASHER	0			0.50	0.00
TOTAL =					484.40

ENERGY COSTS TO:
 CITY OF CREST HILL
 1610 PLAINFIELD ROAD
 CREST HILL, ILLINOIS 60403

ENERGY SUPPLY CONTACT: JIM GLOVER
 PHONE: (815) 724-5054
 COMPANY: COMED



THE END OF TRACER CABLE SHALL BE CONTINUOUS AND EXTEND INTO THE CONTROLLER CABINET

CABLE PLAN
 NOT TO SCALE

SCHEDULE OF QUANTITIES

QTY	UNIT	ITEM DESCRIPTION
1	EACH	SERVICE INSTALLATION - GROUND MOUNTED
1448	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.
52	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.
120	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.
638	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.
6	EACH	HANDHOLE
4	EACH	HEAVY-DUTY HANDHOLE
2	EACH	DOUBLE HANDHOLE
1	EACH	UNINTERRUPTIBLE POWER SUPPLY, STANDARD
1	EACH	TRANSCEIVER-FIBER OPTIC
2092	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
5213	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
741	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
4489	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
60	FOOT	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C
771	FOOT	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C
2	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 14 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE 30 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE 32 FT.
1	EACH	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 48 FT.
1	EACH	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 50 FT.
1	EACH	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 52 FT.
1	EACH	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 54 FT.
8	FOOT	CONCRETE FOUNDATION, TYPE A
4	FOOT	CONCRETE FOUNDATION, TYPE C
27	FOOT	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER
58	FOOT	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER
16	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED
4	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED
2	EACH	SIGNAL HEAD, LED, 2-FACE, 3-SECTION, BRACKET MOUNTED
20	EACH	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM
17	EACH	INDUCTIVE LOOP DETECTOR
1394	FOOT	PREFORMED DETECTOR LOOP
4	EACH	LIGHT DETECTOR
1	EACH	LIGHT DETECTOR AMPLIFIER
1	EACH	TEMPORARY TRAFFIC SIGNAL INSTALLATION
4	EACH	RELOCATE EXISTING ILLUMINATED SIGN
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
8	EACH	REMOVE EXISTING HANDHOLE
9	EACH	REMOVE EXISTING CONCRETE FOUNDATION
1	EACH	FULL-ACTUATED CONTROLLER AND CABINET, TYPE IV, SPECIAL
1116	FOOT	ELECTRIC CABLE IN CONDUIT NO. 20 3C, TWISTED, SHIELDED
1	EACH	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 2
1	EACH	TEMPORARY TRAFFIC SIGNAL TIMING
12109	FOOT	REMOVE ELECTRIC CABLE FROM CONDUIT

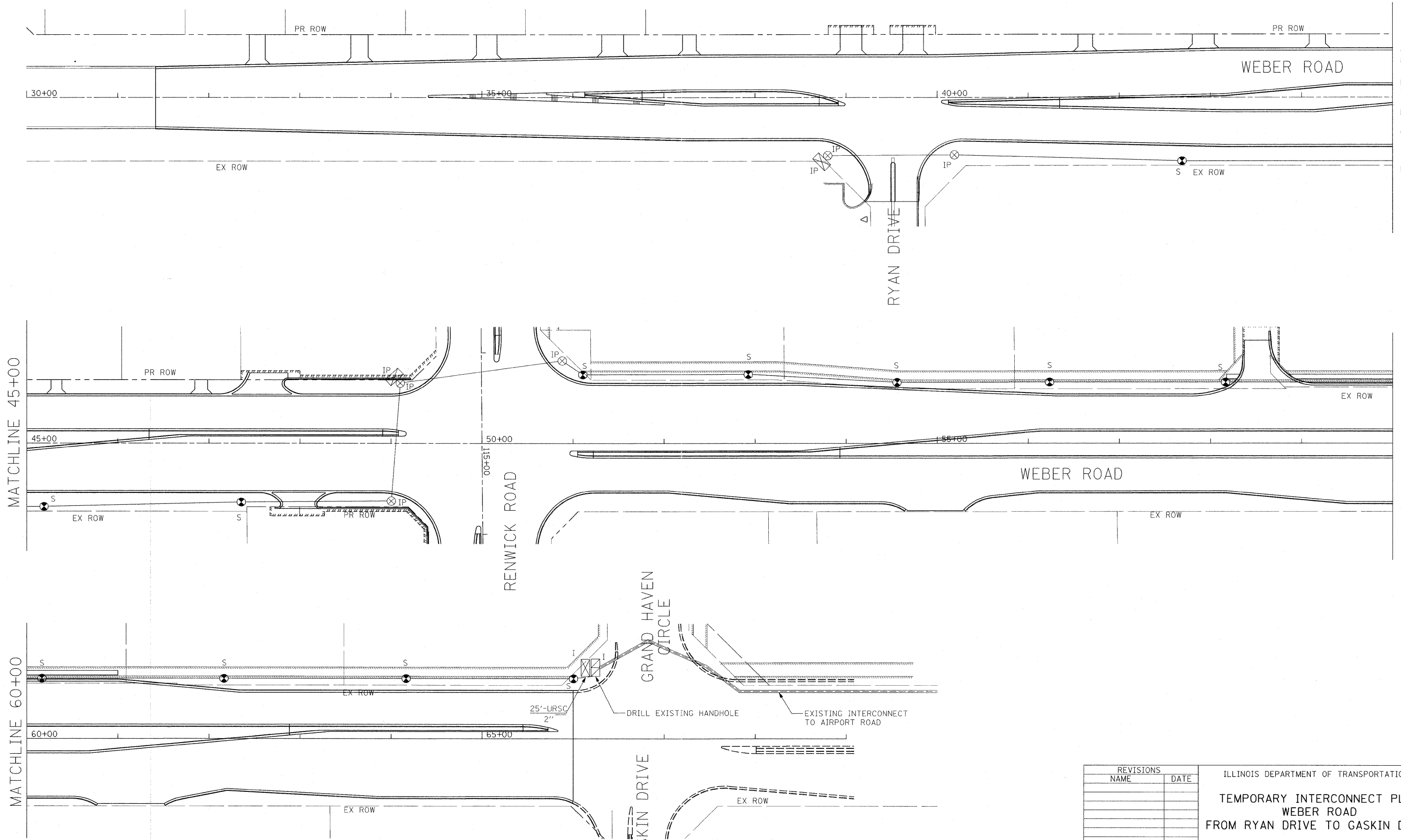
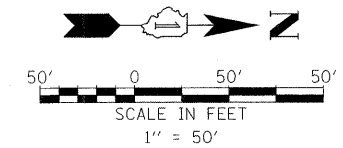
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
RENWICK ROAD & WEBER ROAD
 SCHEDULE OF QUANTITIES,
 CABLE PLAN

SCALE: 1"=20'
 DATE: APRIL, 2011
 DRAWN BY: JJS
 CHECKED BY: MFB

10/20/2011 P:\79766\cadd\dr-arwings\103-Cable-Renwick.dgn

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	104
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO: 63570				



REVISIONS	
NAME	DATE

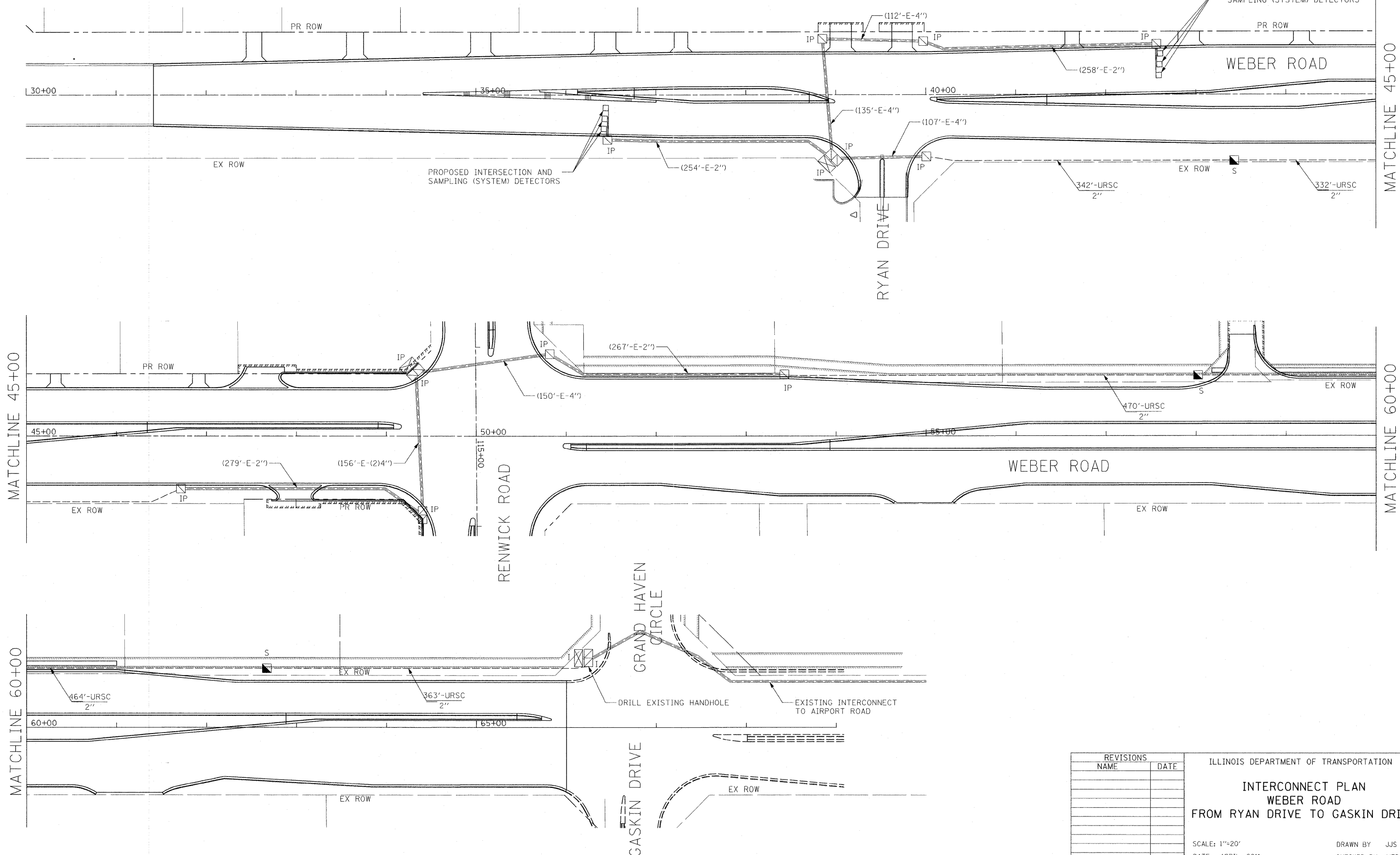
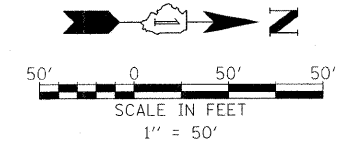
ILLINOIS DEPARTMENT OF TRANSPORTATION
TEMPORARY INTERCONNECT PLAN
WEBER ROAD
FROM RYAN DRIVE TO GASKIN DRIVE

SCALE: 1"=20'
DATE: APRIL, 2011

DRAWN BY: JJS
CHECKED BY: MFB

10/20/2011
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	105
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO: 63570				



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**INTERCONNECT PLAN
WEBER ROAD
FROM RYAN DRIVE TO GASKIN DRIVE**

SCALE: 1"=20'
DATE: APRIL, 2011

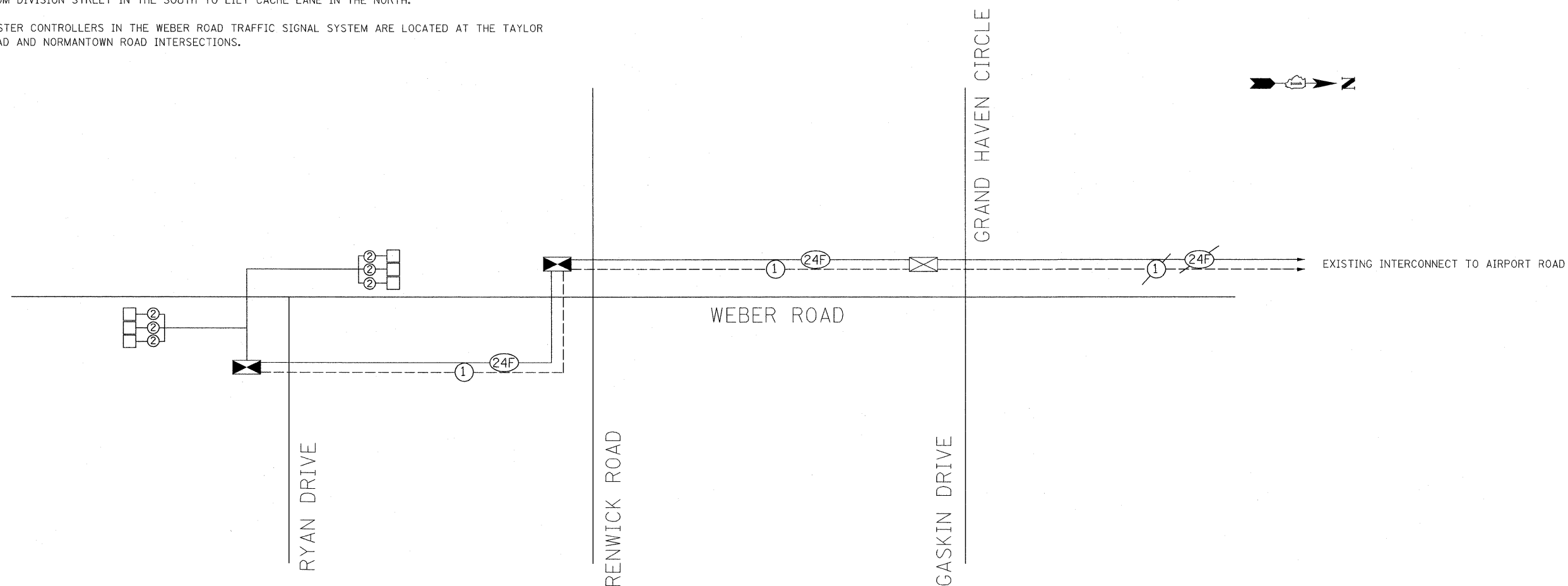
DRAWN BY: JJS
CHECKED BY: MFB

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	106
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO: 63570				

NOTES:

1. THE PROPOSED TRAFFIC SIGNAL CONTROLLERS AT THE RYAN DRIVE AND RENWICK ROAD INTERSECTIONS AND THE EXISTING TRAFFIC SIGNAL CONTROLLER AT THE GASKIN DRIVE/GRAND HAVEN CIRCLE INTERSECTION ARE PART OF THE WEBER ROAD FIBER OPTIC TRAFFIC SIGNAL SYSTEM THAT EXTENDS FROM DIVISION STREET IN THE SOUTH TO LILY CACHE LANE IN THE NORTH.
2. MASTER CONTROLLERS IN THE WEBER ROAD TRAFFIC SIGNAL SYSTEM ARE LOCATED AT THE TAYLOR ROAD AND NORMANTOWN ROAD INTERSECTIONS.



INTERCONNECT SCHEDULE OF QUANTITIES

QTY	UNIT	ITEM DESCRIPTION
1971	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.
3	EACH	HANDHOLE
3117	FOOT	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM 12F
3068	FOOT	ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1C
1	EACH	DRILL EXISTING HANDHOLE

REVISIONS	
NAME	DATE

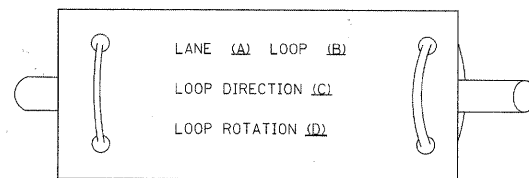
ILLINOIS DEPARTMENT OF TRANSPORTATION
**INTERCONNECT SCHEMATIC
AND SCHEDULE OF QUANTITIES
WEBER ROAD
FROM RYAN DRIVE TO GASKIN DRIVE**

SCALE: 1"=20'
DATE: APRIL, 2011
DRAWN BY: JJS
CHECKED BY: MFB

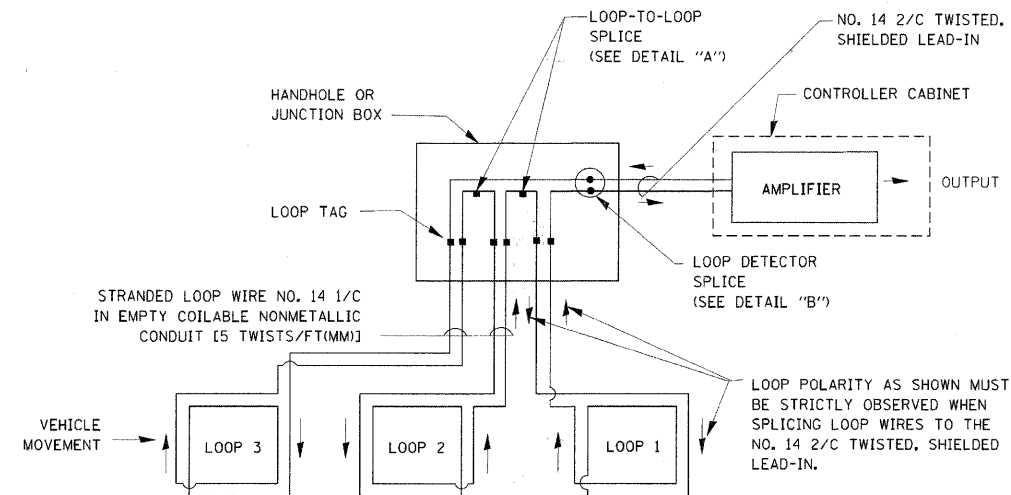
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

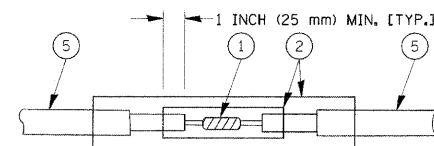


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

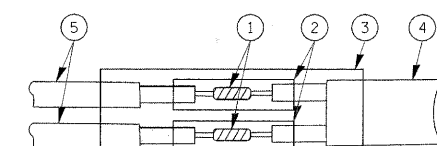


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm), IF IN CONCRETE. THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

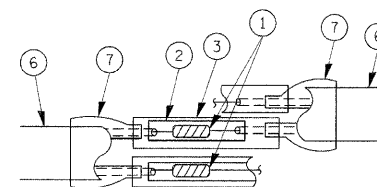


DETAIL "A"
LOOP-TO-LOOP SPLICE

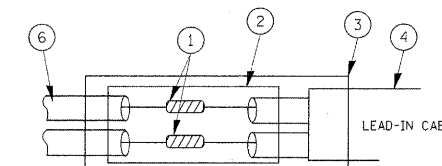


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

REVISIONS	
NAME	DATE
CADD	5/30/00
ADD NOTE NO. 8	11/12/01
BUREAU OF TRAFFIC	1-01-02
BCK	10/28/09

ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAILS

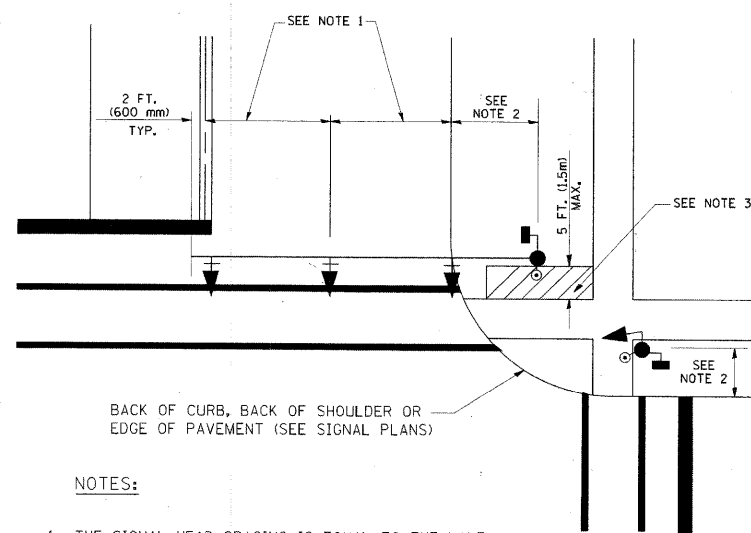
SCALE: NONE

DRAWN BY BCK
DESIGNED BY DAD
CHECKED BY DAD

TS-05
1 OF 6

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

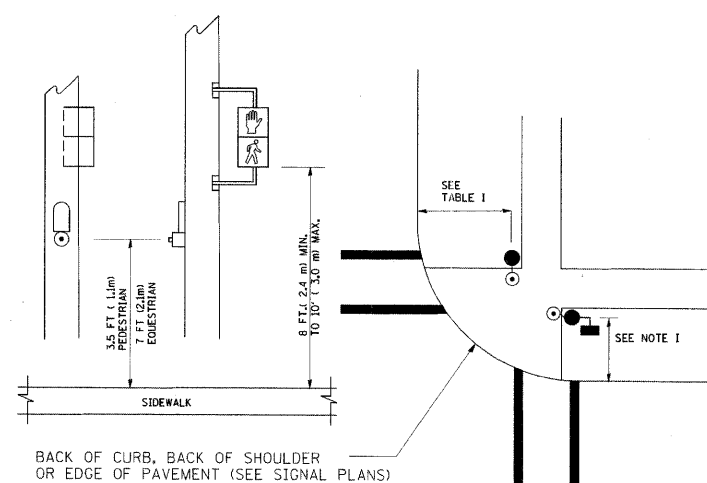
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

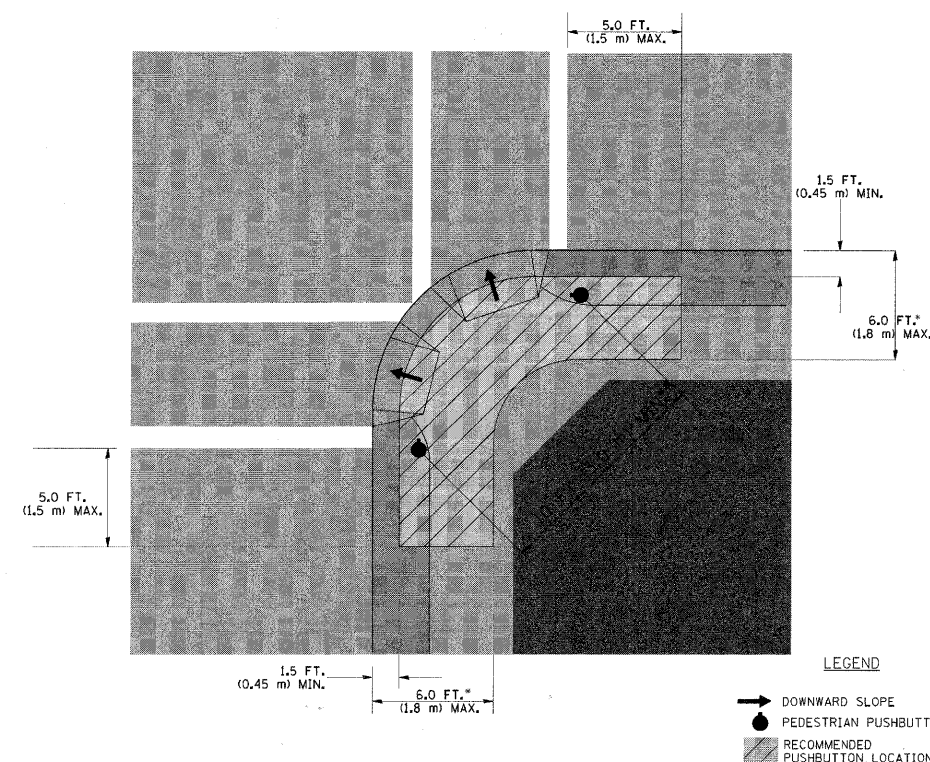
PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

REVISIONS	
NAME	DATE
BUREAU OF TRAFFIC	1-01-02
BCK	10/28/09

TS-05
2 OF 6

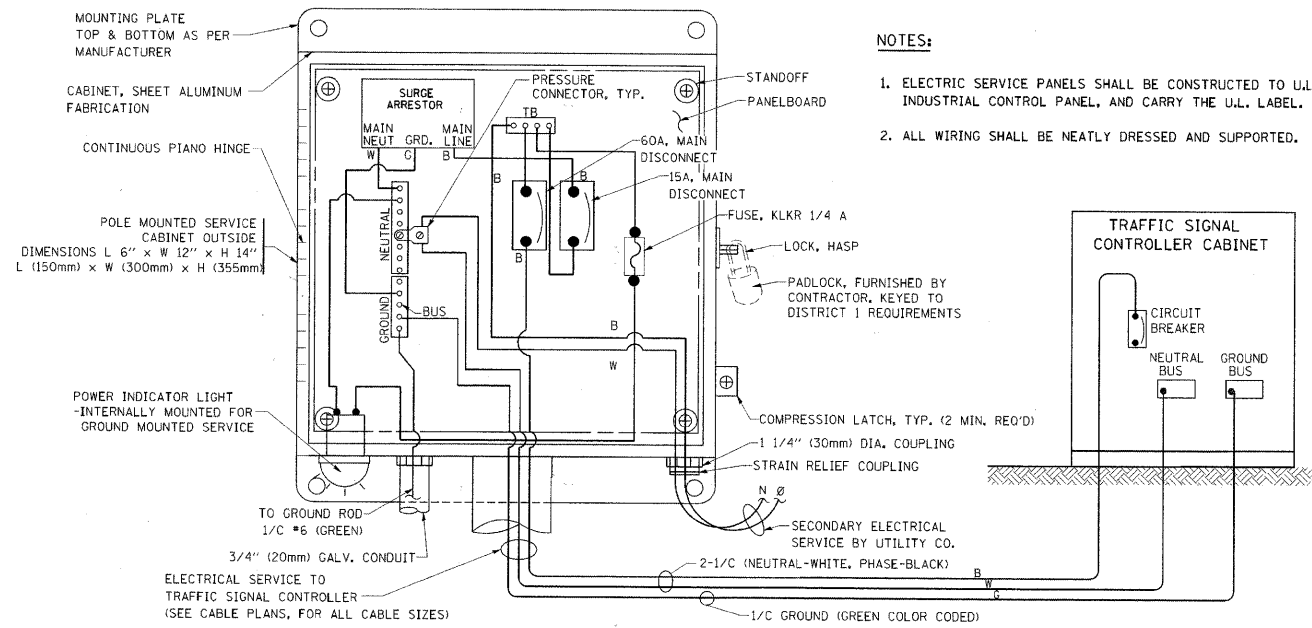
ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT 1
STANDARD TRAFFIC SIGNAL
DESIGN DETAILS

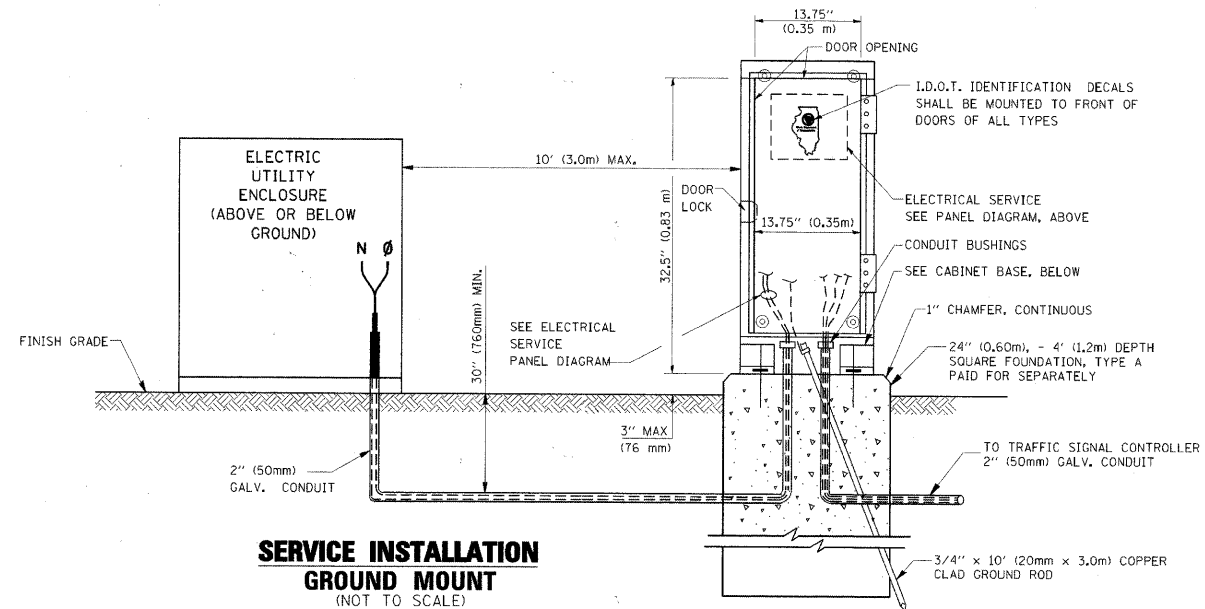
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CHECKED BY DAD

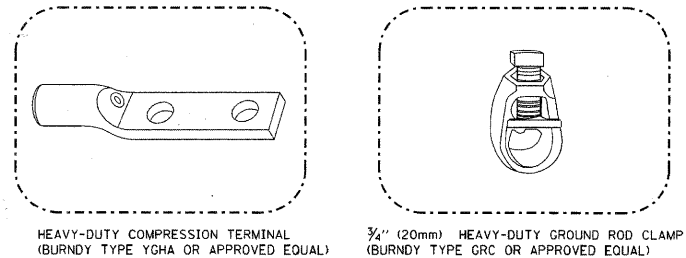
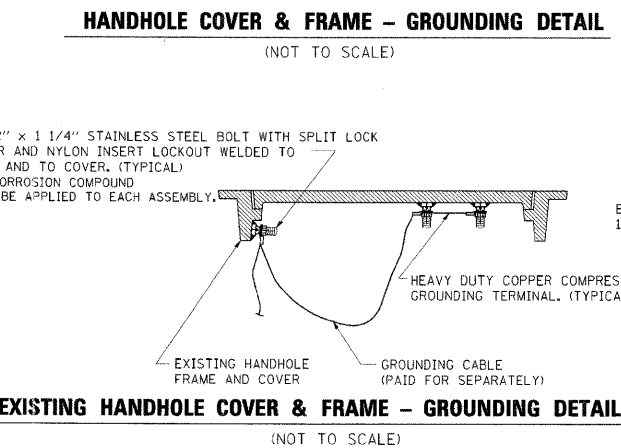
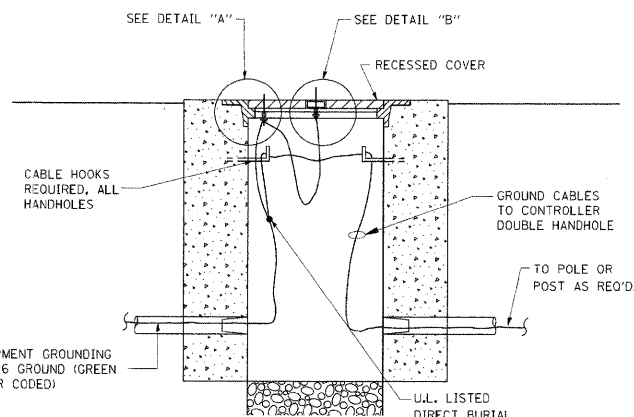
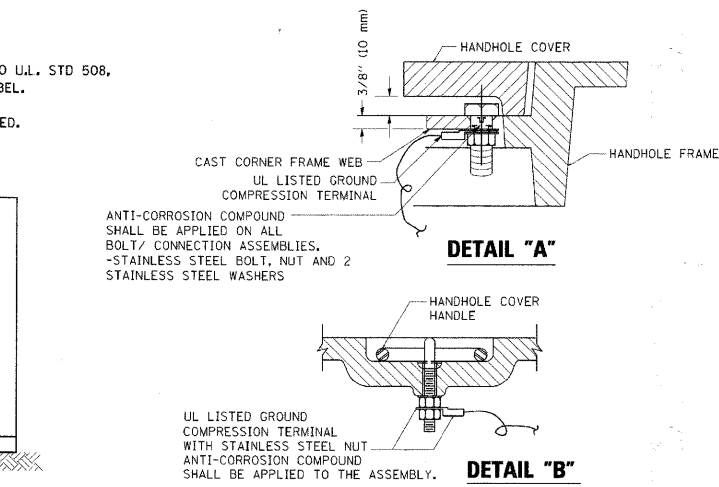
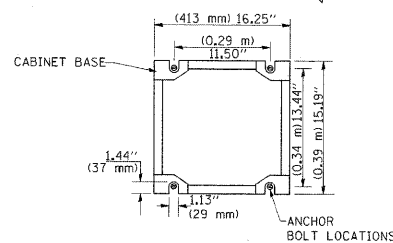
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STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO: 63570				



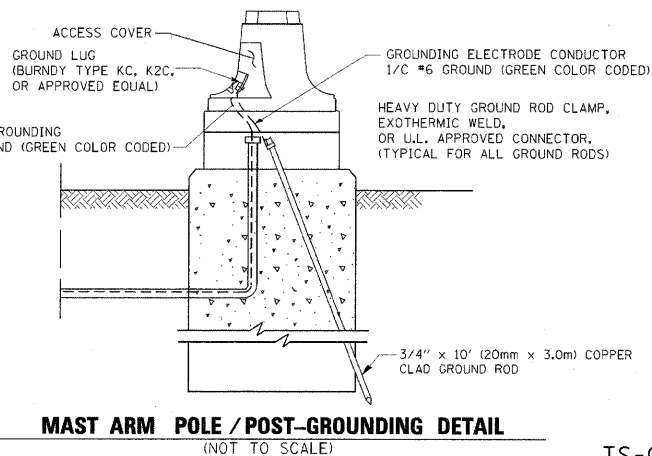
ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
 (NOT TO SCALE)



CABINET - BASE BOLT PATTERN
 (NOT TO SCALE)



- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, U.L. APPROVED.
 - GROUND CABLE SHALL BE LOOPEO OVER HOOKS IN THE HANDHOLES
 - 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES
 - 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES.
 - 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



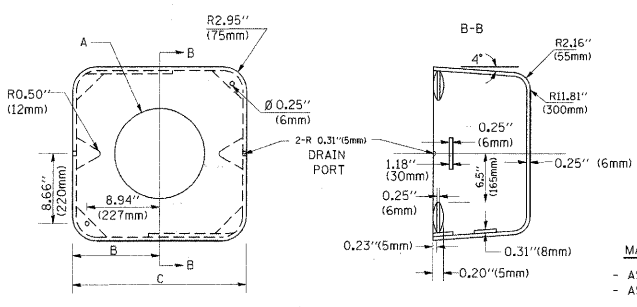
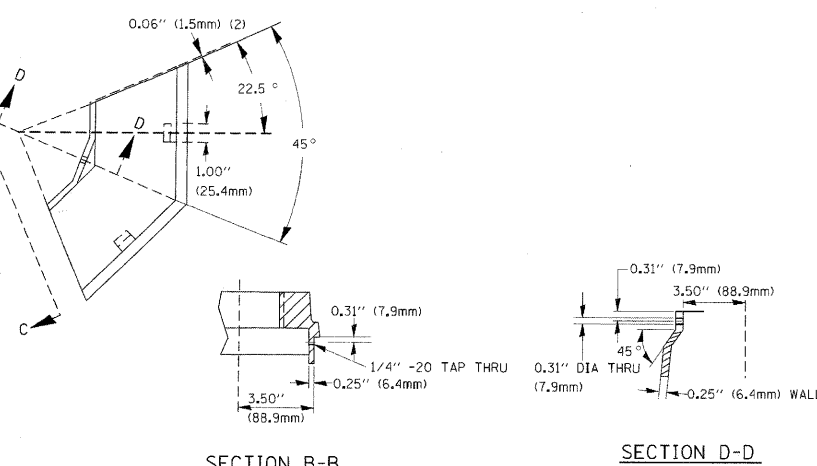
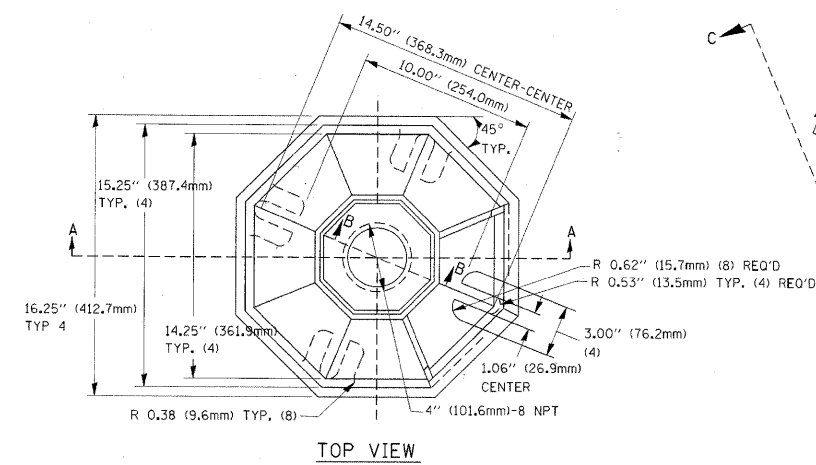
REVISIONS	
NAME	DATE
CADD	5/30/00
CADD	3/15/01
BUREAU OF TRAFFIC	1-01-02
BCK	10/28/09

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT 1
STANDARD TRAFFIC SIGNAL
DESIGN DETAILS

SCALE: NONE

DRAWN BY BCK
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 CHECKED BY DAD

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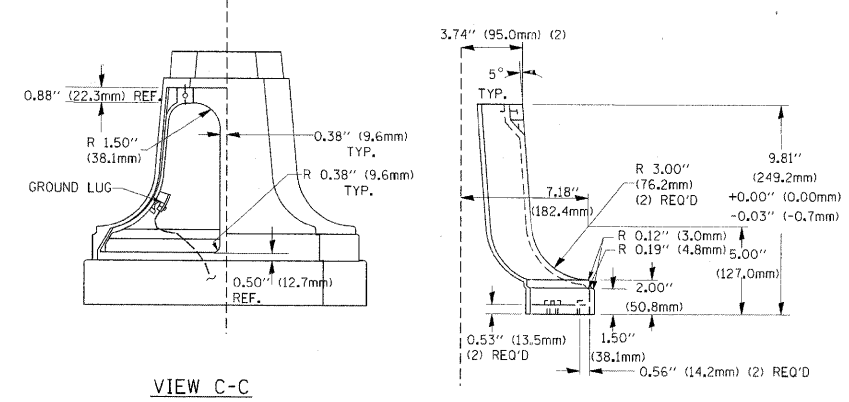
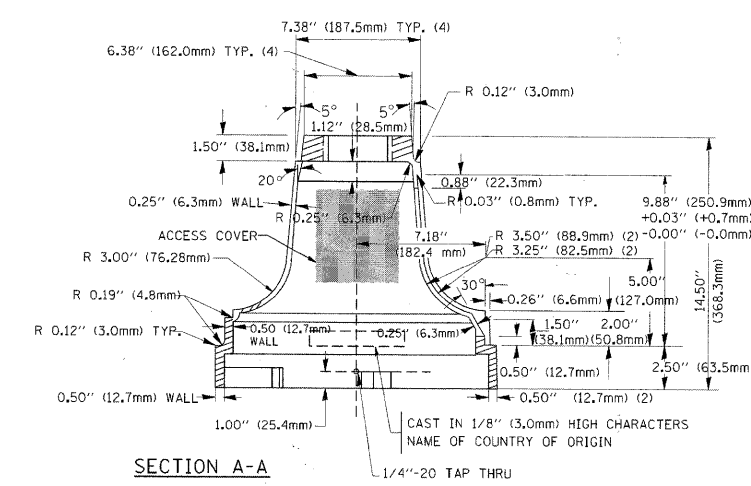


A	B	C	HEIGHT	WEIGHT
VARIES	9.5\"(241mm)	19\"(483mm)	7\"(178mm) - 12\"(300mm)	53 lbs (24kg)
VARIES	10.75\"(273mm)	21.5\"(546mm)	7\"(178mm) - 12\"(300mm)	68 lbs (31 kg)
VARIES	13.0\"(330mm)	26\"(660mm)	7\"(178mm) - 12\"(300mm)	81 lbs (37 kg)
VARIES	18.5\"(470mm)	37\"(940mm)	7\"(178mm) - 12\"(300mm)	126 lbs (57 kg)

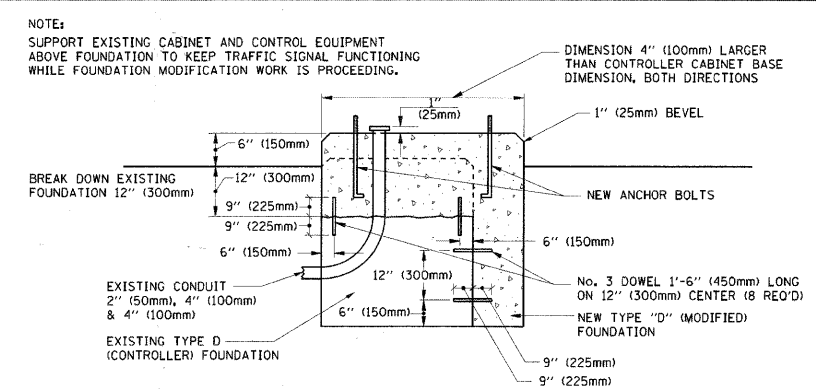
MATERIALS:
 - ASTM A36 STEEL
 - ASTM A-123 HOT DIPPED GALVANIZED

SHROUD

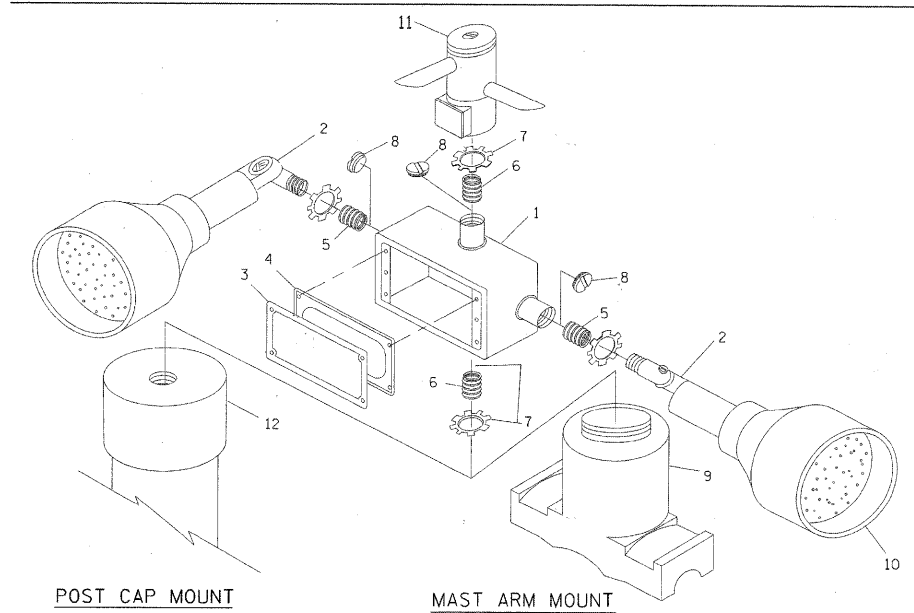
- NOTES:
- DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
 - THE SUPPLIER SHALL VERIFY THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
 - THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.



TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A



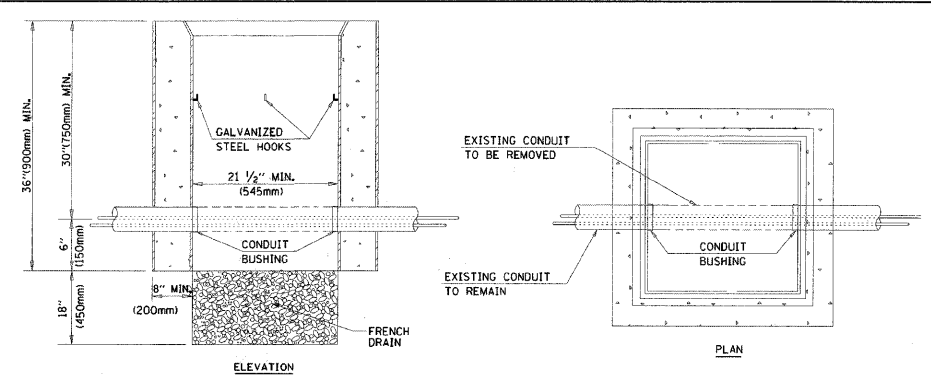
MODIFY EXISTING TYPE "D" FOUNDATION



ITEM NO.	IDENTIFICATION
1	OUTLET BOX - GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4\"(19 mm) CLOSE NIPPLE
7	3/4\"(19 mm) LOCKNUT
8	3/4\"(19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

- NOTES:
- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
 - ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
 ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
 ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
 - WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4\"(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.

POST CAP MOUNT
 MAST ARM MOUNT
 EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL



HANDHOLE TO INTERCEPT EXISTING CONDUIT

- NOTES:
- HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
 - REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCIDENTAL TO THE HANDHOLE.

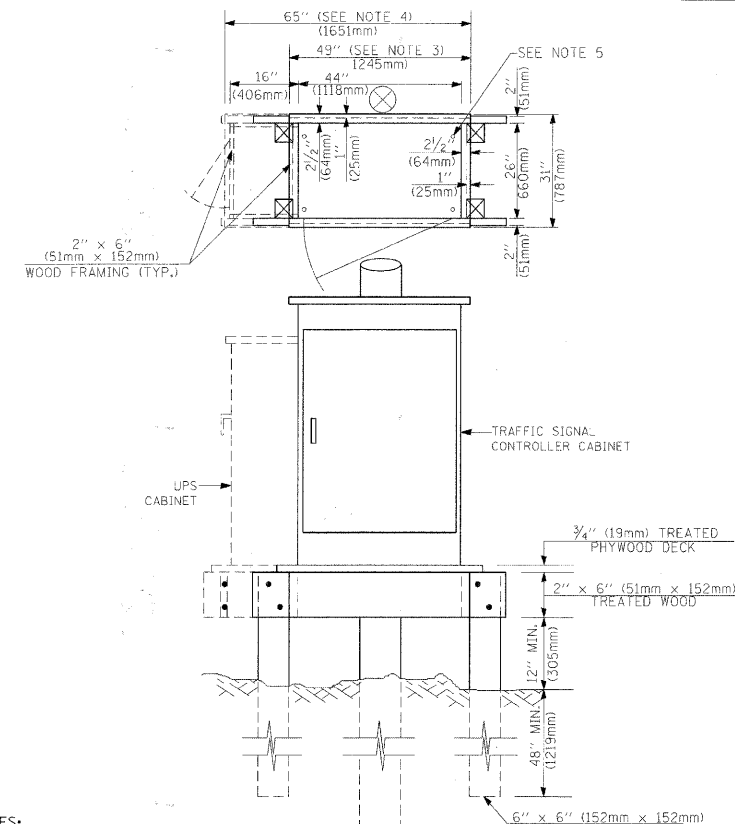
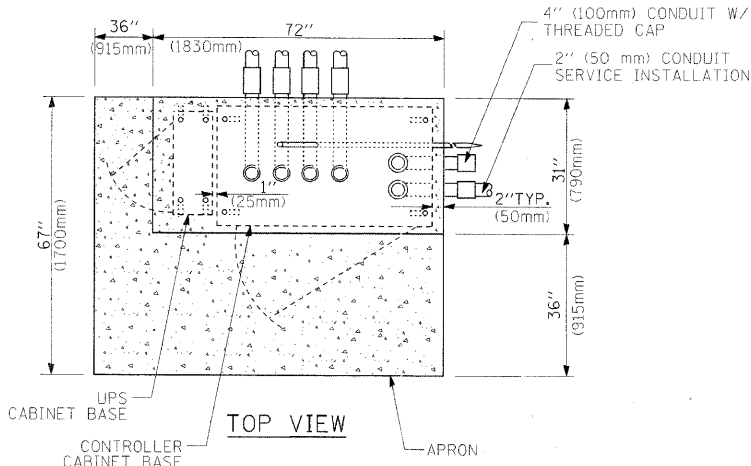
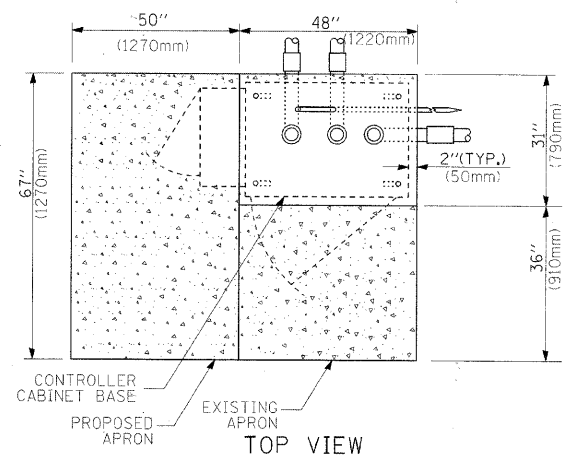
REVISIONS		
NAME	DATE	
BUREAU OF TRAFFIC	5/30/00	
BUREAU OF TRAFFIC	3/15/01	
BUREAU OF TRAFFIC	11/12/01	
BUREAU OF TRAFFIC	1-01-02	
BCK	10/28/09	

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT 1
 STANDARD TRAFFIC SIGNAL
 DESIGN DETAILS

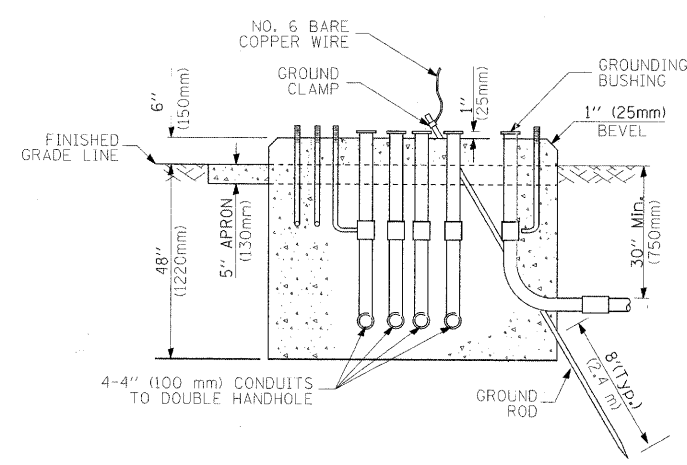
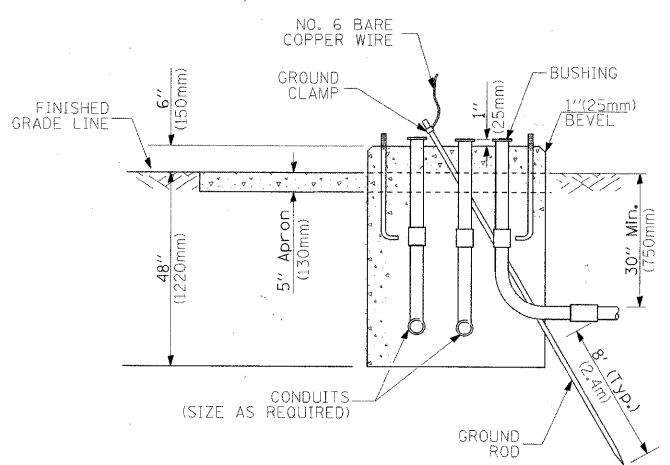
SCALE: NONE

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- NOTES:**
- BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
 - BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
 - PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
 - PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
 - DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
 - FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.



**TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET**

**TYPE C
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET**

**TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

MAST ARM LENGTH	FOUNDATION DEPTH	FOUNDATION DIAMETER	SPIRAL DIAMETER	QUANTITY OF REBARS	SIZE OF REBARS
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and less than 65' (19.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

- NOTES:**
- These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
 - Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
 - Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
 - For mast arm assemblies with dual arms refer to state standard 878001.

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

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ILLINOIS DEPARTMENT OF TRANSPORTATION
**DISTRICT 1
STANDARD TRAFFIC SIGNAL
DESIGN DETAILS**

SCALE: NONE

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TRAFFIC SIGNAL LEGEND

ITEM	REMOVAL	EXISTING	PROPOSED
CONTROLLER CABINET			
RAILROAD CONTROL CABINET			
COMMUNICATIONS CABINET			
MASTER CONTROLLER			
MASTER MASTER CONTROLLER			
UNINTERRUPTIBLE POWER SUPPLY			
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT			
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT			
STEEL MAST ARM ASSEMBLY AND POLE			
ALUMINUM MAST ARM ASSEMBLY AND POLE			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA			
SIGNAL POST			
TEMPORARY WOOD POLE (CLASS 5, OR BETTER) 45 FOOT (13.7m) MINIMUM.			
GUY WIRE			
SIGNAL HEAD			
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)			
SIGNAL HEAD WITH BACKPLATE			
SIGNAL HEAD OPTICALLY PROGRAMMED			
FLASHER INSTALLATION (S DENOTES SOLAR POWER)			
PEDESTRIAN SIGNAL HEAD			
PEDESTRIAN PUSHBUTTON DETECTOR			
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR			
ILLUMINATED SIGN "NO LEFT TURN"			
ILLUMINATED SIGN "NO RIGHT TURN"			
DETECTOR LOOP, TYPE I			
PREFORMED DETECTOR LOOP			
MICROWAVE VEHICLE SENSOR			
VIDEO DETECTION CAMERA			
VIDEO DETECTION ZONE			
PAN, TILT, ZOOM CAMERA			
WIRELESS DETECTOR SENSOR			
WIRELESS ACCESS POINT			

ITEM	REMOVAL	EXISTING	PROPOSED
EMERGENCY VEHICLE LIGHT DETECTOR			
CONFIRMATION BEACON			
HANDHOLE			
HEAVY DUTY HANDHOLE			
DOUBLE HANDHOLE			
JUNCTION BOX			
GALVANIZED STEEL CONDUIT IN TRENCH (T) OR PUSHED (P)			
TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE			
COMMON TRENCH			CT
COILABLE NONMETALLIC CONDUIT (EMPTY)			CNC
SYSTEM ITEM		S	S
INTERSECTION ITEM		I	IP
REMOVE ITEM	R		
RELOCATE ITEM	RL		
ABANDON ITEM	A		
12" (300mm) TRAFFIC SIGNAL SECTION			
12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE			
SIGNAL FACE			
SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD			
12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL			
12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED			
12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID			
PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER			
RADIO INTERCONNECT			
RADIO REPEATER			
DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED			
GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)			

ITEM	REMOVAL	EXISTING	PROPOSED
ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE			
COAXIAL CABLE			
VENDOR CABLE FOR CAMERA			
COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED			
FIBER OPTIC CABLE NO. 62.5/125, MM12F			
FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F			
FIBER OPTIC CABLE NO. 62.5/125, (NUMBER OF FIBERS & TYPE TO BE NOTED ON PLANS)			
GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE			
CONTROLLER CABINET AND FOUNDATION TO BE REMOVED			
STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED			
ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED			
SIGNAL POST AND FOUNDATION TO BE REMOVED			
INTERSECTION & SAMPLING (SYSTEM) DETECTOR			
SAMPLING (SYSTEM) DETECTOR			
EXISTING INTERSECTION LOOP DETECTOR			
PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
EXISTING PREFORMED INTERSECTION LOOP DETECTOR			
PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
PREFORMED SAMPLING (SYSTEM) DETECTOR			

RAILROAD SYMBOLS

	EXISTING	PROPOSED
RAILROAD CONTROL CABINET		
RAILROAD CANTILEVER MAST ARM		
FLASHING SIGNAL		
CROSSING GATE		
CROSSBUCK		

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REVISIONS	
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	5/30/00
	3/15/01
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









ILLINOIS DEPARTMENT OF TRANSPORTATION
**DISTRICT 1
STANDARD TRAFFIC SIGNAL
DESIGN DETAILS**

SCALE: NONE

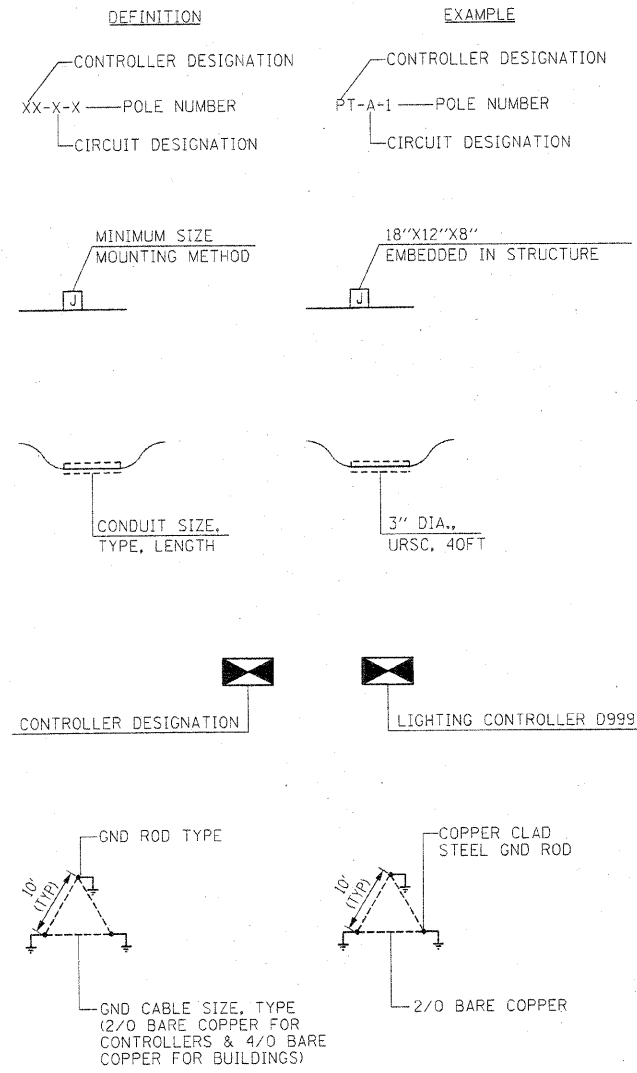
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DESIGNED BY DAD
CHECKED BY DAD

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	113
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

ELECTRICAL SYMBOLS

-  LIGHTING UNIT:
45 FT. M.H., 10 FT. M.A.,
310W HPS M-C-II LUMINAIRE, UNO
9" BREAKAWAY TRANSFORMER BASE
-  COMBINATION TRAFFIC SIGNAL AND
LUMINAIRE LIGHTING UNIT
-  UNIT DUCT
2 #6 XLP-TYPE USE & 1 #6 XLP GND IN 1/4" DIA.
SCHEDULE 40 POLYETHYLENE DUCT
-  LIGHTING CONTROLLER CABINET
-  EXISTING UTILITY POLE
-  GROUND ROD
-  AERIAL CABLE
-  UNIT DUCT INSTALLED IN CASING IN TRENCH
-  DIRECT BURIED CONDUIT IN TRENCH
-  ELECTRIC UTILITY SERVICE

CALL-OUT SAMPLES



ABBREVIATIONS

- A AMPERE
- AC ALTERNATING CURRENT
- A/C AERIAL CABLE
- AFG ABOVE FINISHED GRADE
- CB CIRCUIT BREAKER
- CKT CIRCUIT
- COMED COMMONWEALTH EDISON
- DIA DIAMETER
- DP DISTRIBUTION PANEL
- E EXISTING LIGHTING UNIT TO REMAIN
- EOP EDGE OF PAVEMENT
- EPR ETHYLENE PROPYLENE RUBBER
- FT FEET OR FOOT
- FU FUSE
- GND GROUND
- HID HIGH INTENSITY DISCHARGE
- HPS HIGH PRESSURE SODIUM
- IDOT ILLINOIS DEPARTMENT OF TRANSPORTATION
- IN INCH/INCHES
- JB JUNCTION BOX
- KV KILOVOLT
- M METER
- MA MAST ARM
- MH MOUNTING HEIGHT
- MIN MINIMUM
- NO, # NUMBER
- PB PUSH BUTTON
- P POLE
- PH PHASE
- PNL PANEL
- PVC POLYVINYL CHLORIDE
- R EXISTING LIGHTING UNIT TO BE REMOVED (OWNER SALVAGED UNLESS NOTED OTHERWISE)
- RECP RECEPTACLE
- RGSC RIGID GALVANIZED STEEL CONDUIT
- SS STAINLESS STEEL
- STA STATION
- TC TRAY CABLE
- TYP TYPICAL
- UD UNIT DUCT
- UNO UNLESS NOTED OTHERWISE
- URSC UNDERGROUND RIGID SHEET CONDUIT
- V VOLT
- VA VOLT-AMPERE
- W WATT
- WP WEATHERPROOF
- XFMR TRANSFORMER

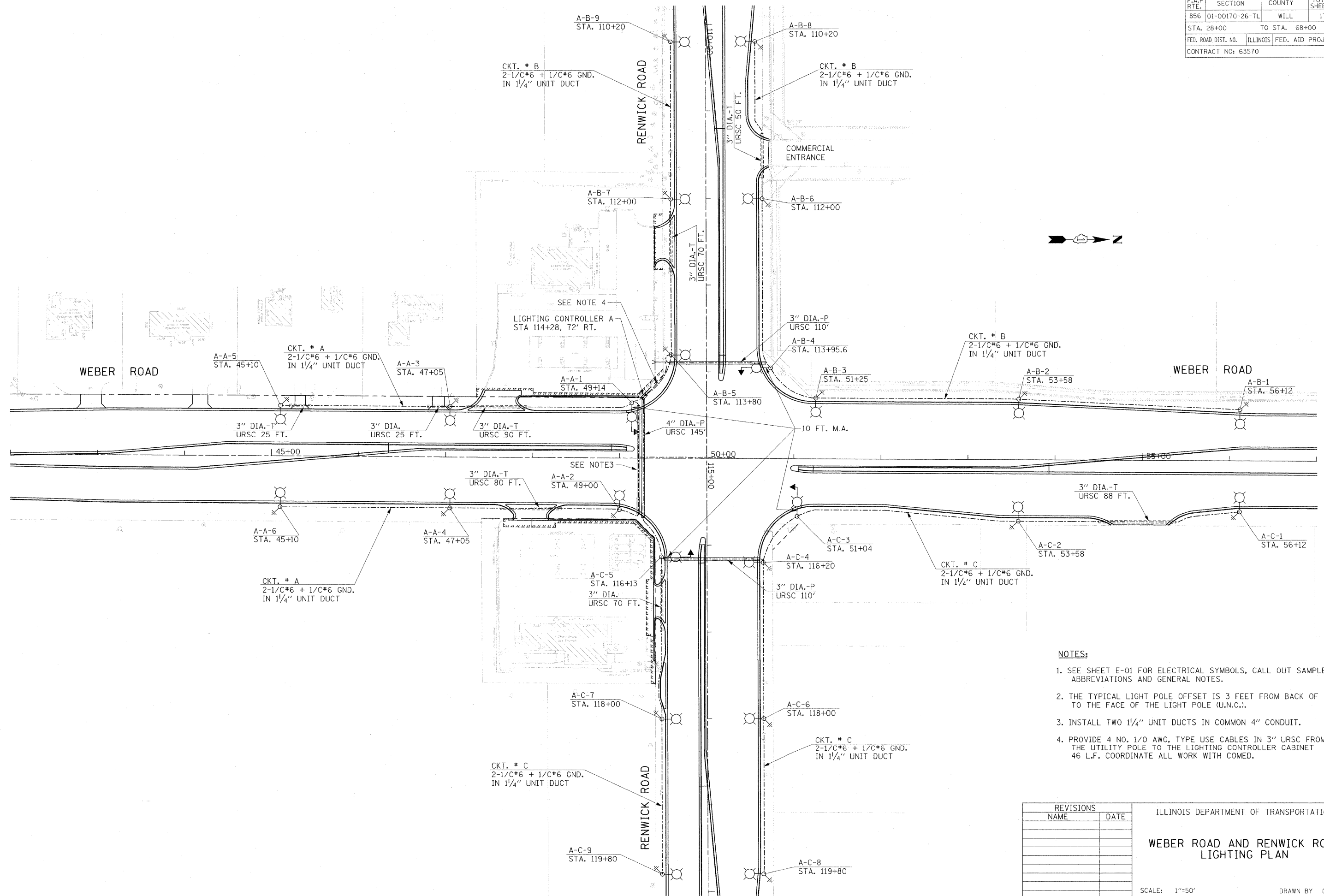
GENERAL NOTES

- PRIOR TO THE INSTALLATION OF NEW UNIT DUCTS, CONDUITS, HANDHOLES, JUNCTION BOXES, LIGHT STANDARD FOUNDATIONS AND APPURTENANCES, THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF EXISTING LIGHTING CONDUITS, CABLE AND UNDERGROUND UTILITIES.
- THE CONTRACTOR SHALL VERIFY ALL OF THE DATA SHOWN ON THE CONTRACT PLANS WHICH WOULD AFFECT HIS/HER WORK UNDER THIS CONTRACT.
- ALL NEW UNIT DUCTS, CONDUITS, HANDHOLES, JUNCTION BOXES, APPURTENANCES ARE ILLUSTRATED DIAGRAMATICALLY. THE ACTUAL LOCATION IN THE FIELD SHALL BE APPROVED BY THE ENGINEER.
- CONDUITS, UNIT DUCT, HANDHOLES, JUNCTION BOXES, LIGHT STANDARD FOUNDATION AND APPURTENANCES SHALL BE POSITIONED IN THE FIELD TO AVOID CONFLICT WITH DRAINS AND ALL OTHER UTILITIES, BOTH UNDERGROUND AND ABOVE GROUND.
- ALL DISTURBED AREAS WHERE RESTORATION IS NOT COVERED BY APPLICABLE SECTIONS OF THE SPECIAL PROVISIONS SHALL BE RESTORED TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE CONTRACT. SEPARATE PAYMENT WILL NOT BE MADE.
- THE CABLE INSTALLATION FROM THE LIGHTING CONTROL CABINET TO THE LIGHT STANDARDS SHALL BE CONTINUOUS WITHOUT UNDERGROUND SPLICES. SPLICING OF CABLES IS PERMITTED ONLY IN THE BASE OF THE LIGHT STANDARDS AND IN ABOVE GROUND JUNCTION BOXES.
- ALL PITS USED FOR BORING AND PULLING CONDUITS UNDER ROADWAYS AND DRIVEWAYS SHALL BE LOCATED FIVE (5) FEET (MINIMUM) CLEAR FROM THE EDGE OF SHOULDER. LOCATIONS OF THE CONDUIT CROSSINGS SHOWN ARE APPROXIMATE AND MAY BE SHIFTED AS NECESSARY TO MEET THE MINIMUM CLEARANCE REQUIREMENTS. THE PITS SHALL BE ADEQUATELY GUARDED TO PROTECT THE MOTORIST. THE CONTRACTOR SHALL SUBMIT PLANS OF EACH PIT TO THE ENGINEER PRIOR TO EXCAVATING EACH PIT. THE PIT PLANS SHALL SHOW THE PIT SIZE, PIT LOCATION, PROTECTION TO BE INSTALLED, AND MAINTENANCE OF TRAFFIC AT PIT SITE.
- THE CONTRACTOR SHALL FURNISH AND INSTALL ALL LUMINAIRE LAMPS IN ACCORDANCE WITH THE LUMINAIRE SUPPLIER RECOMMENDATIONS. THE CONTRACTOR SHALL COORDINATE THE LAMP TYPE, VOLTAGE, AND WATTAGE WITH THE LUMINAIRE SUPPLIER.
- THE CONTRACTOR SHALL CONTACT THE ELECTRIC UTILITY COMPANY TO COORDINATE THE ELECTRICAL SERVICE WORK.
CONTACT: COMED
MR. JOSEPH ISABELLI
815.724.5699
- TO MAINTAIN THE STRUCTURAL INTEGRITY OF THE LIGHT POLES, THE LIGHT POLES SHALL NOT BE ERECTED AND/OR LEFT TO STAND WITHOUT LUMINAIRES. THE LIGHT POLES WILL NOT BE PAID FOR UNTIL THE POLES ARE FULLY APPROVED WITH THE LUMINAIRES INSTALLED, CONNECTED, AND TESTED.
- THE EQUIPMENT GROUNDING CONDUCTORS SHALL BE SPLICED AND BONDED TO EACH JUNCTION BOX AND PULL BOX THAT THE CONDUCTORS PASS THROUGH. JUNCTION BOXES AND PULL BOXES SHALL BE EQUIPPED WITH GROUND LUGS FOR THE GROUND WIRE TERMINATION. THE GROUND LUG INSTALLATION SHALL NOT DEGRADE THE JUNCTION BOX RATING.
- TRENCHES FOR LIGHTING RACEWAYS SHALL HAVE A MINIMUM DEPTH OF 30".
- PROPOSED LIGHT POLES ARE TO BE INSTALLED WITH BREAKAWAY DEVICE.

E-01

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		RENWICK ROAD & WEBER ROAD ELECTRICAL SYMBOLS, CALL-OUT SAMPLES, ABBREVIATIONS, AND GENERAL NOTES
SCALE: NONE		DRAWN BY: CJO
DATE: APRIL, 2011		CHECKED BY: RH

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	114
STA. 28+00		TO STA. 68+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				



NOTES:

1. SEE SHEET E-01 FOR ELECTRICAL SYMBOLS, CALL OUT SAMPLES, ABBREVIATIONS AND GENERAL NOTES.
2. THE TYPICAL LIGHT POLE OFFSET IS 3 FEET FROM BACK OF CURB TO THE FACE OF THE LIGHT POLE (U.N.O.).
3. INSTALL TWO 1/4" UNIT DUCTS IN COMMON 4" CONDUIT.
4. PROVIDE 4 NO. 1/0 AWG, TYPE USE CABLES IN 3" URSC FROM THE UTILITY POLE TO THE LIGHTING CONTROLLER CABINET 46 L.F. COORDINATE ALL WORK WITH COMED.

E-02

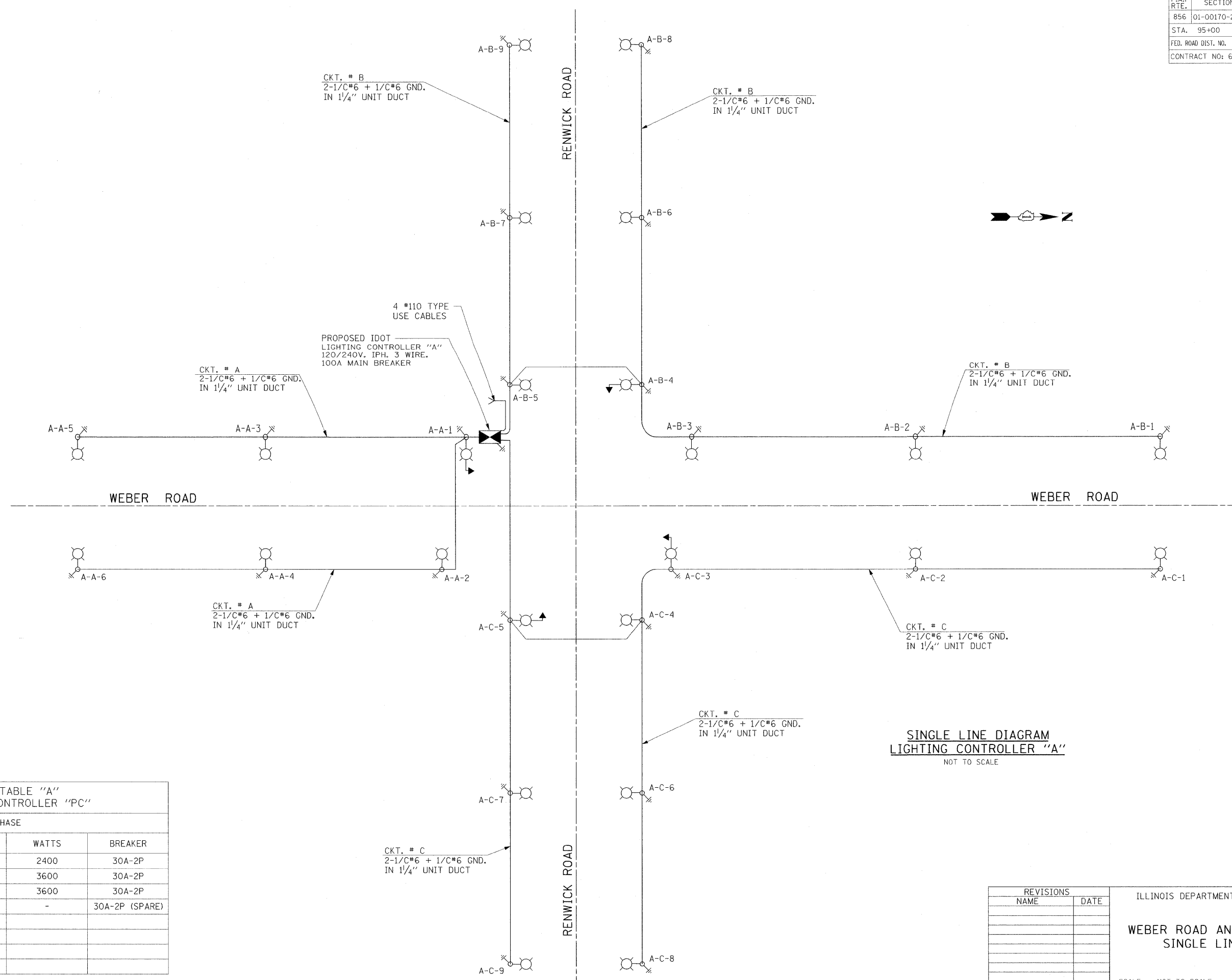
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
WEBER ROAD AND RENWICK ROAD LIGHTING PLAN

SCALE: 1"=50'
 DATE: APRIL, 2011

DRAWN BY: CJO
 CHECKED BY: MFB

10/19/2011 P:\79786\cadd\drawings\114\LightWeb.dgn



**SINGLE LINE DIAGRAM
LIGHTING CONTROLLER "A"**
NOT TO SCALE

LOAD TABLE "A" LIGHTING CONTROLLER "PC"			
PHASE			
CIRCUIT	AMPERES	WATTS	BREAKER
A	10	2400	30A-2P
B	15	3600	30A-2P
C	15	3600	30A-2P
D	-	-	30A-2P (SPARE)

TOTAL LOAD 9600 WATTS

REVISIONS	
NAME	DATE

E-03

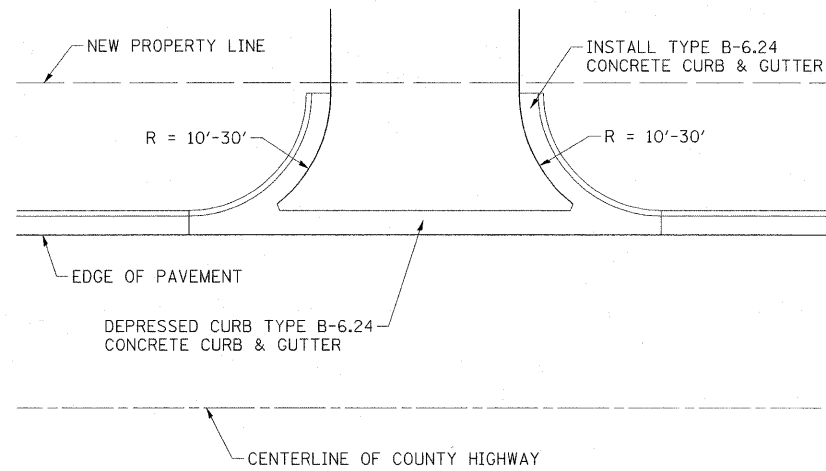
ILLINOIS DEPARTMENT OF TRANSPORTATION

**WEBER ROAD AND RENWICK ROAD
SINGLE LINE DIAGRAM**

SCALE: NOT TO SCALE DRAWN BY PHP
DATE: APRIL, 2011 CHECKED BY MFB

8/22/2011 P:\79766\cadd\drawings\1151\Light\Ren.dgn

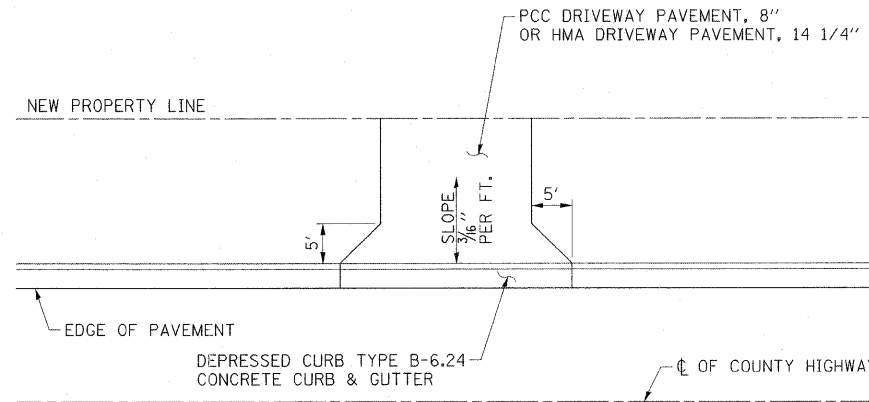
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	116
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO: 63570				



COMMERCIAL DRIVEWAY

NOTES:

- PREMOLDED EXPANSION JOINTS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO PCC DRIVEWAY PAVEMENT 8".
- COMBINATION CONCRETE CURB & GUTTER AND CONCRETE CURB SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY RETURN. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB TRANSITION SECTIONS.
- ALL POURS MUST BE SEPARATE. NO MONOLITHIC POURS WITH CURB & GUTTER, DRIVEWAY APPROACHES, AND SIDEWALKS.
- ALL SAWCUTS ARE TO BE FULL DEPTH.
- PROTECTIVE COATING SHALL BE INSTALLED ON ALL CONCRETE WORK.



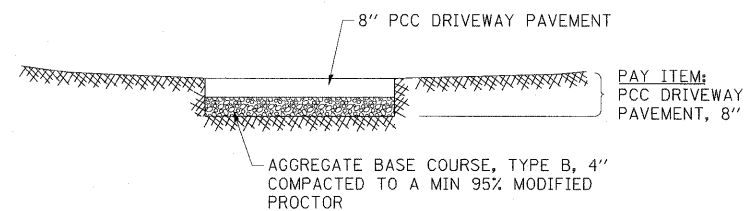
RESIDENTIAL DRIVEWAY

NOTE:

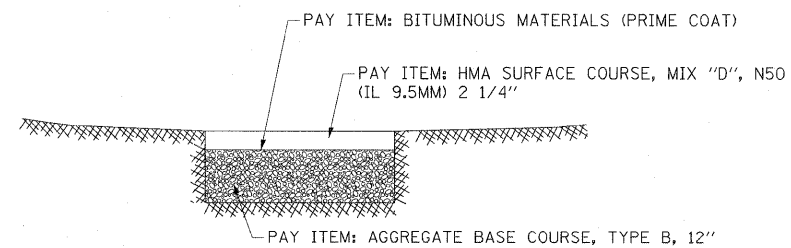
ALL CONSTRUCTION TO BE DONE ACCORDING TO STATE OF ILLINOIS "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION".

ALL GROUND AREAS DISTURBED SHALL BE RESEEDED AND MULCHED OR JUTE MATTED AS SOON AFTER CONSTRUCTION AS POSSIBLE.

REMOVE AND REPLACE SIDEWALK AS NECESSARY TO PROVIDE ADEQUATE TRANSITION TO EXISTING GRADES.



P.C.C. DRIVEWAY PAVEMENT, 8"



HMA DRIVEWAY PAVEMENT, 14 1/4"

DRIVEWAY CONSTRUCTION DETAILS

REVISIONS	
NAME	DATE

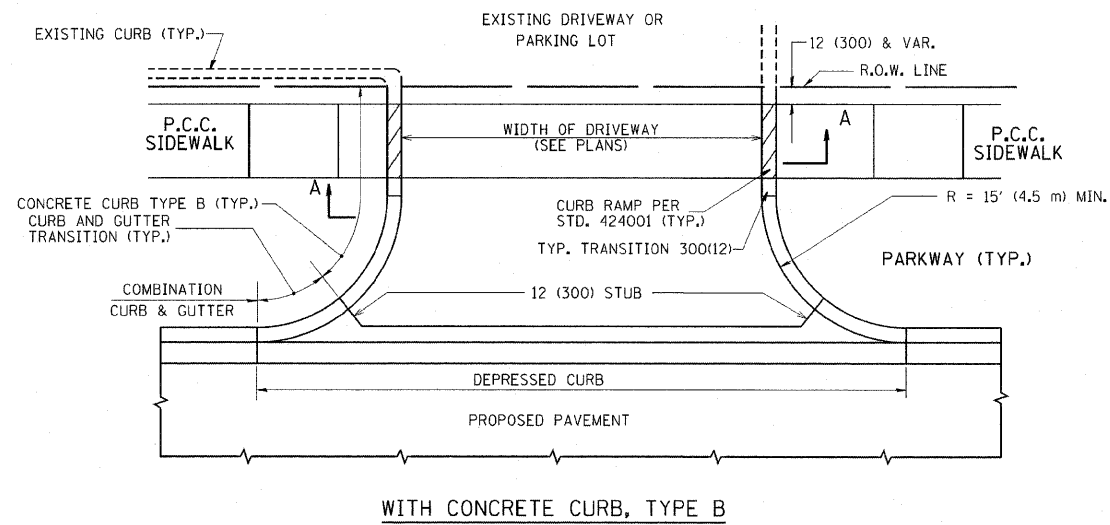
ILLINOIS DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS

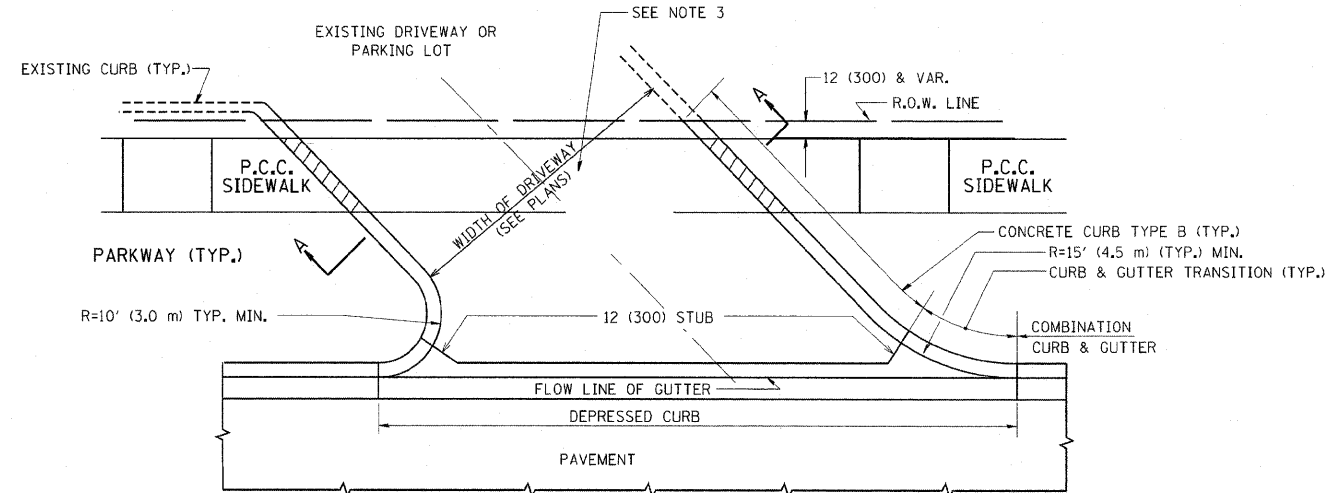
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DATE: APRIL, 2011

DRAWN BY: PHP
CHECKED BY: MFB

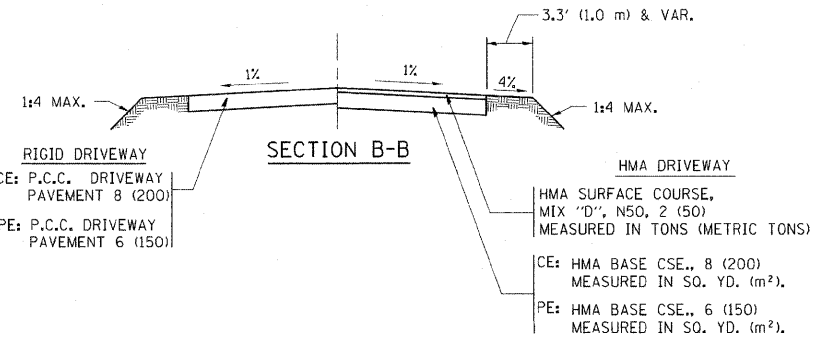
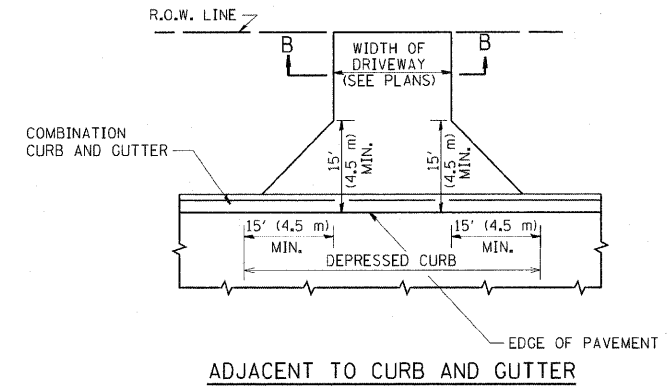
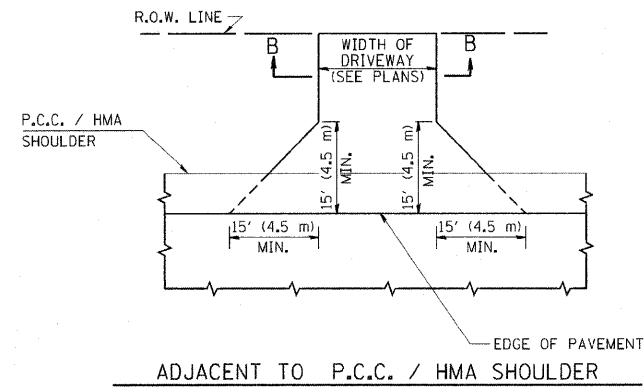
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	117
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO: 63570				



WITH CONCRETE CURB, TYPE B



WITH CONCRETE CURB, TYPE B



RURAL FIELD ENTRANCE (FE)
HMA SURFACE COURSE,
MIX "D", N50, 2 (50)
MEASURED IN TONS (METRIC TONS)
AGGREGATE BASE CSE., TYPE B, 8 (200)
MEASURED IN SQ. YD. (m²).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

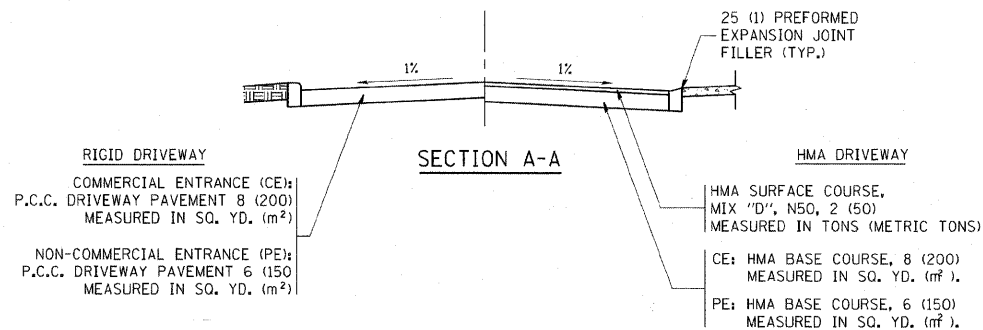
COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.



BD-01

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

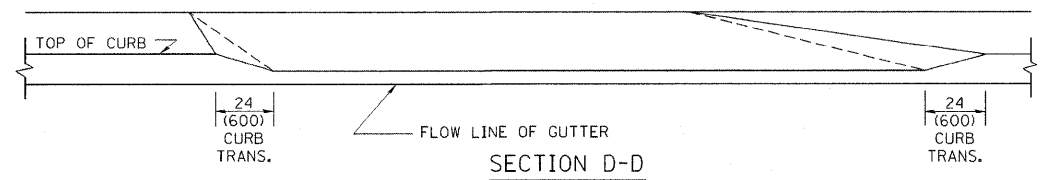
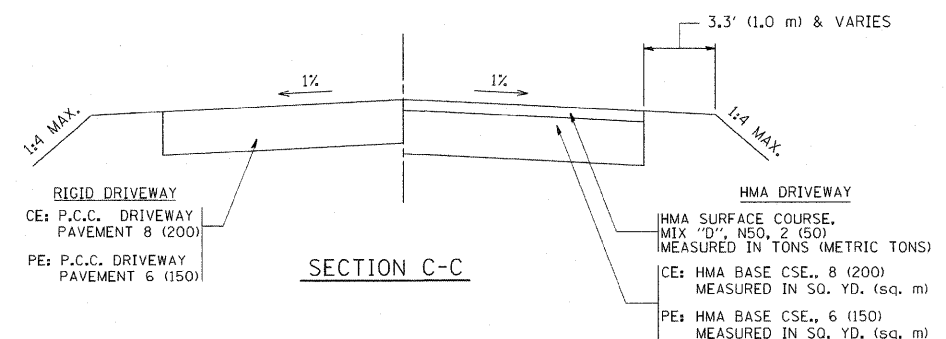
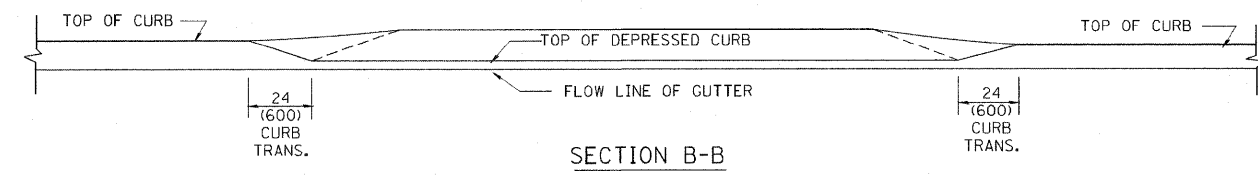
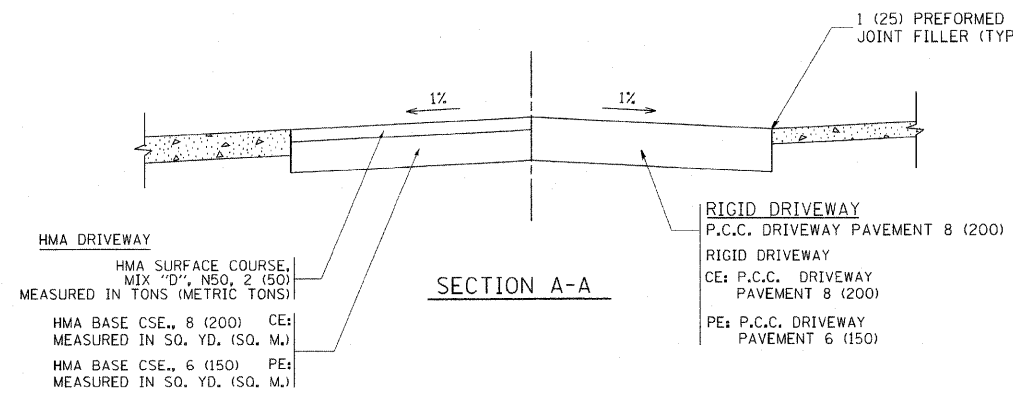
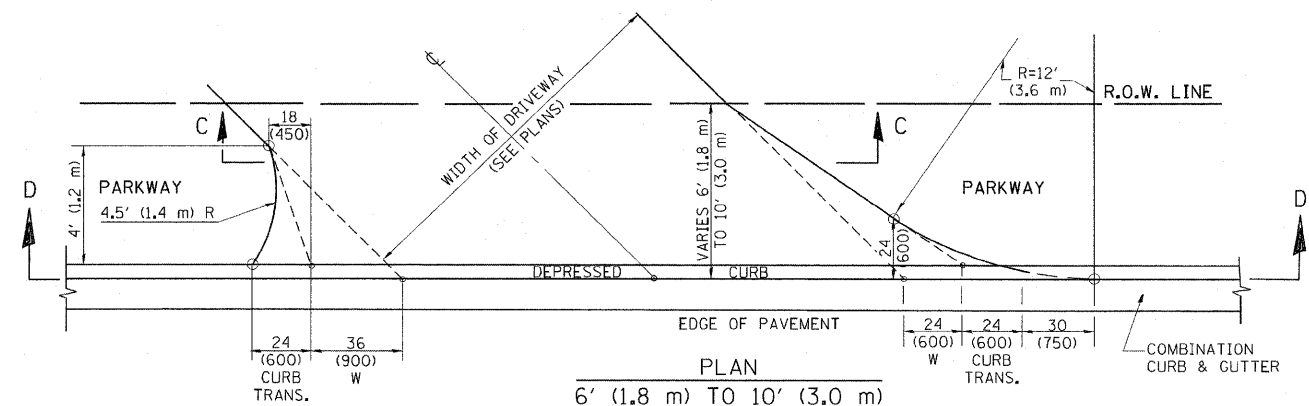
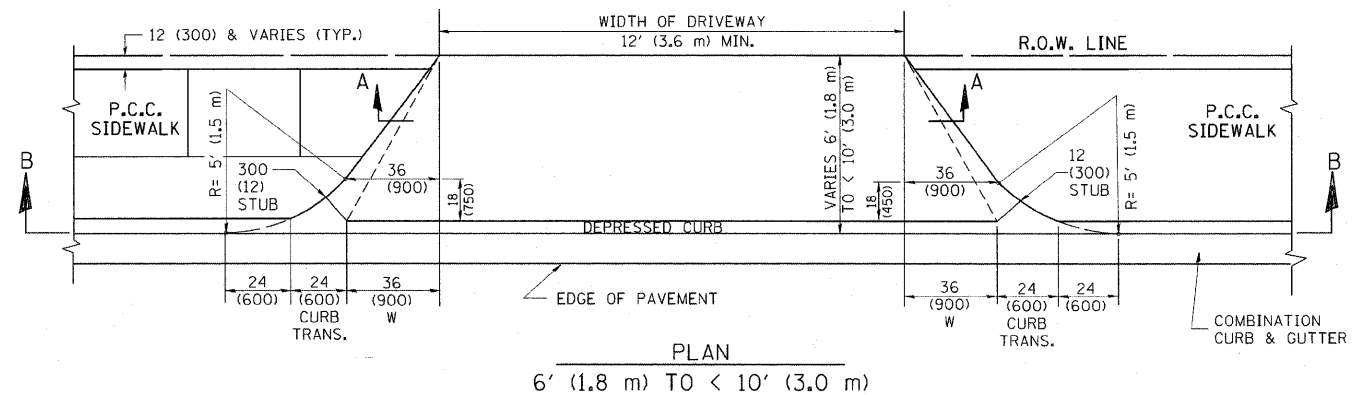
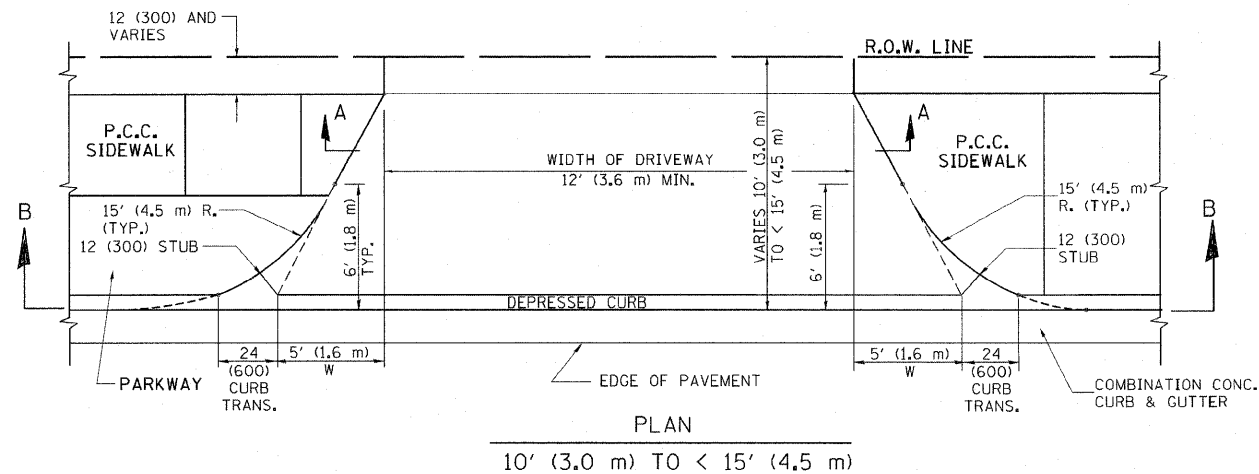
ILLINOIS DEPARTMENT OF TRANSPORTATION
DRIVEWAY DETAILS
DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 M)

REVISIONS	
NAME	DATE
R. SHAH	11-04-95
J. POLLASTRINI	08-12-96
J. POLLASTRINI	12-14-96
A. ABBAS	03-21-97
T. HOLTZ	04-08-97
M. GOMEZ	04-06-01
P. LoFLEUR	04-15-03
R. BORO	01-01-07
R. BORO	06-11-08
R. BORO	09-06-11

SCALE: NONE

DRAWN BY
CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	118
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO: 63570				



GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

BD-02

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED

ILLINOIS DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS
DISTANCE BETWEEN R.O.W. AND FACE OF CURB < 15' (4.5 M)

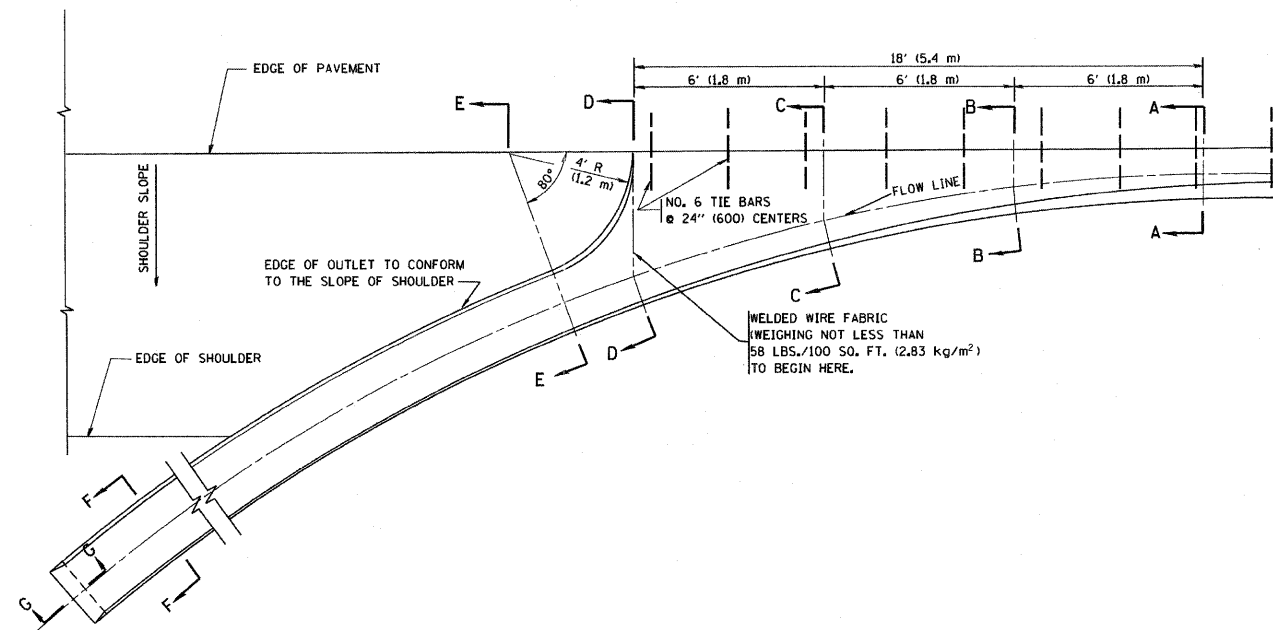
SCALE: NONE

DRAWN BY
CHECKED BY

REVISIONS	
NAME	DATE
R. SHAH	11-06-95
J. POLLASTRINI	08-12-96
J. POLLASTRINI	12-14-96
A. ABBAS	03-21-97
T. HOLTZ	04-08-97
M. GOMEZ	04-06-01
P. LOFLEUR	04-15-03
R. BORO	01-01-07
R. BORO	09-06-11

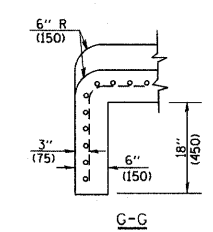
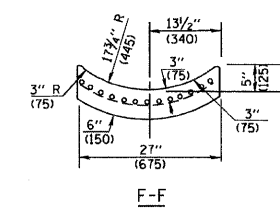
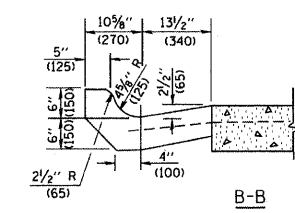
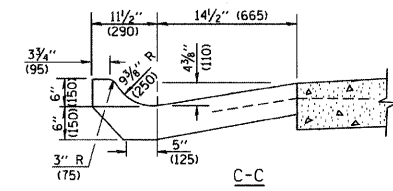
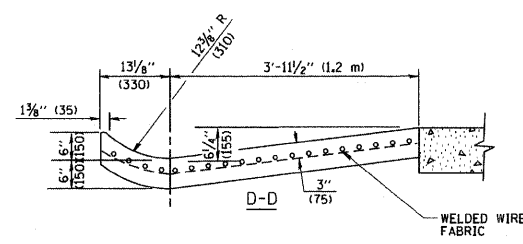
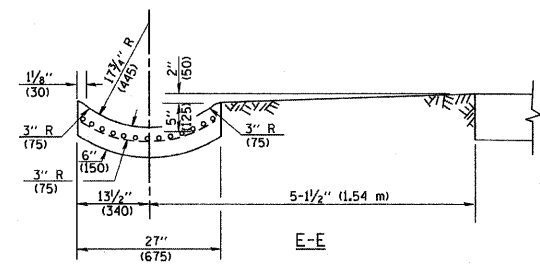
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	119
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO: 63570				



A-A *

* DIMENSIONS OF THE CURB & GUTTER AT SECTION A-A ARE SHOWN ON STATE STANDARD 606001. FOR DETAILS OF OUTLET FOR CONCRETE CURB & GUTTER, TYPE B-6.24 (B-15.60) SEE STATE STANDARD 606006.



GENERAL NOTES

GUTTER OUTLET SHALL BE TIED TO THE PAVEMENT IN ACCORDANCE WITH DETAILS FOR LONGITUDINAL CONSTRUCTION JOINT SHOWN ON STANDARD 420001.

TIE BARS SHALL BE NO. 20 (NO. 6) AT 24\" (600) CENTERS UNLESS OTHERWISE SHOWN.

IF THE AVERAGE GRADE OF PAVEMENT FOR THE DISTANCE FROM SECTION A-A TO D-D EXCEEDS 2%, THIS DISTANCE SHALL BE INCREASED 6\" (1.8 m) FOR EACH 1% INCREASE IN GRADE.

QUANTITIES

FOR SECTION A-A TO E-E AND CURTAIN WALL =
 1.25 CU. YDS. (0.96 m³) CLASS SI CONCRETE (OUTLET) FOR 9\" (225) PAV'T.
 1.27 CU. YDS. (0.96 m³) CLASS SI CONCRETE (OUTLET) FOR 10\" (250) PAV'T.
 FOR SECTION F-F =
 0.045 CU. YDS. (0.03 m³) CLASS SI CONCRETE PER FT. (M).

BD-03

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
M. DE YONG	08-04-86
R. SHAH	09-09-94
R. SHAH	10-25-94
E. GOMEZ	12-21-00

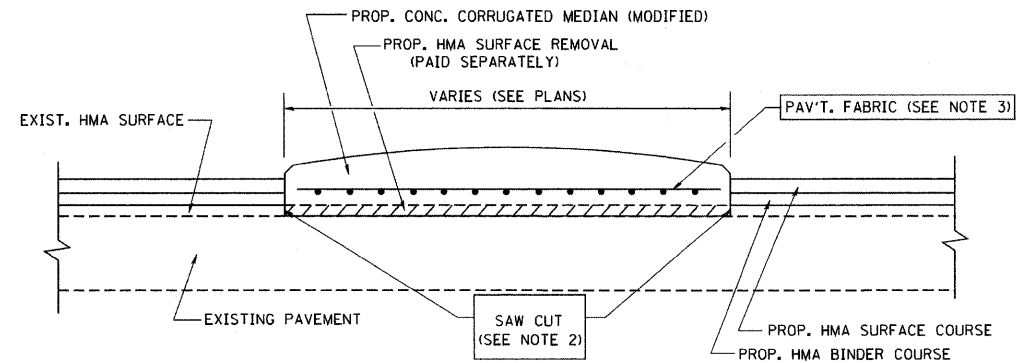
ILLINOIS DEPARTMENT OF TRANSPORTATION

OUTLET FOR CONCRETE CURB AND GUTTER

SCALE: NONE

DRAWN BY
CHECKED BY

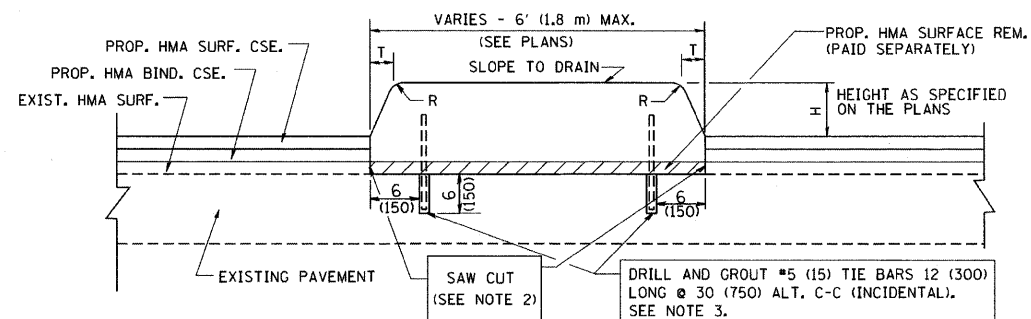
8/22/2011
P: 179766_cadd\drawings\119BD003.dgn



- NOTES:
1. CORRUGATED MEDIAN (MODIFIED) SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 606 OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE PORTIONS OF STATE STANDARD 606306.
 2. WITH THE APPROVAL OF THE ENGINEER, THE CONTRACTOR MAY DELETE THE SAW CUT IF A NEAT JOINT CAN BE OBTAINED BY MILLING THE HMA SURFACE TO BE REMOVED. SAW CUT WILL BE INCLUDED IN THE COST OF CORRUGATED MEDIAN (MODIFIED)
 3. PAVEMENT FABRIC WILL BE INCLUDED IN THE COST OF CORRUGATED MEDIAN (MODIFIED)

DETAILS FOR CORRUGATED MEDIAN (MODIFIED)

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT (SQUARE METER) FOR "CORRUGATED MEDIAN (MODIFIED)"



H	R	T
6(150)	1(25)	1(25)
9(225)	1(25)	2(150)

- NOTES:
1. CONCRETE MEDIAN TYPE SB (DOWELLED) SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF STATE STANDARD 606301 AND SECTION 606 OF THE STANDARD SPECIFICATIONS.
 2. WITH THE APPROVAL OF THE ENGINEER, THE CONTRACTOR MAY DELETE THE SAW CUT IF A NEAT JOINT CAN BE OBTAINED BY MILLING THE HMA SURFACE TO BE REMOVED. SAW CUT WILL BE INCLUDED IN THE COST OF "CONCRETE MEDIAN TYPE SB (DOWELLED)"
 3. FOR MEDIAN WIDTH LESS THAN 4' (1.2 m) USE ONE ROW OF #5 (15) BARS @ 30 (750) C-C ALONG THE MEDIAN CENTERLINE. TIE BARS WILL BE INCLUDED IN THE COST OF "CONCRETE MEDIAN TYPE SB (DOWELLED)"

DETAILS FOR CONCRETE MEDIAN

TYPE SB (DOWELLED)

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT (SQUARE METER) FOR "CONCRETE MEDIAN TYPE SB (DOWELLED)"

BD-05

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

ILLINOIS DEPARTMENT OF TRANSPORTATION

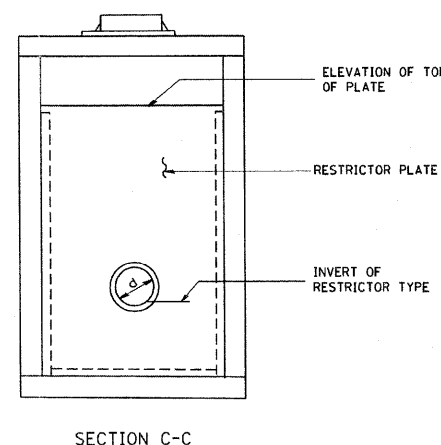
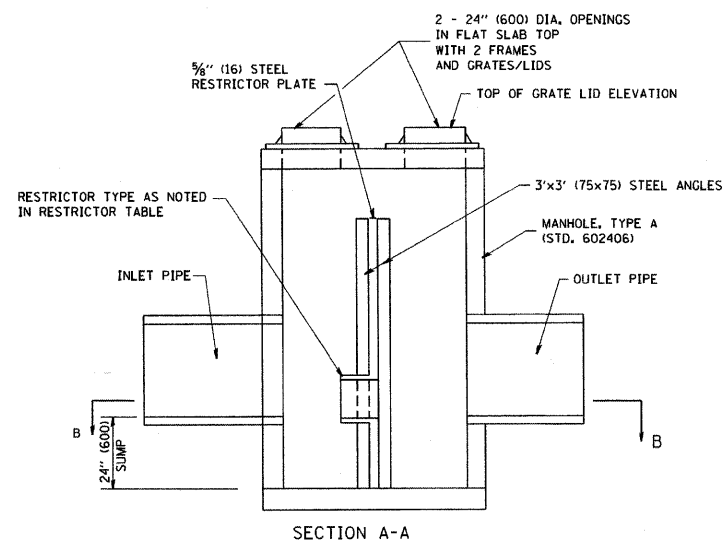
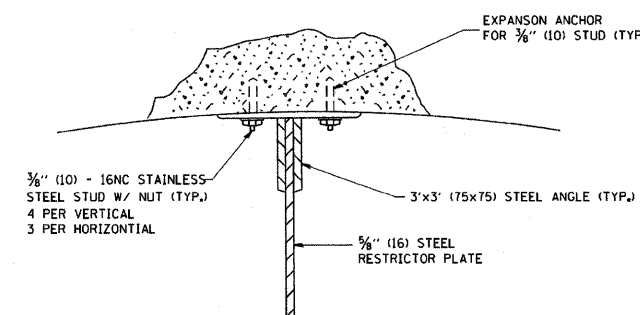
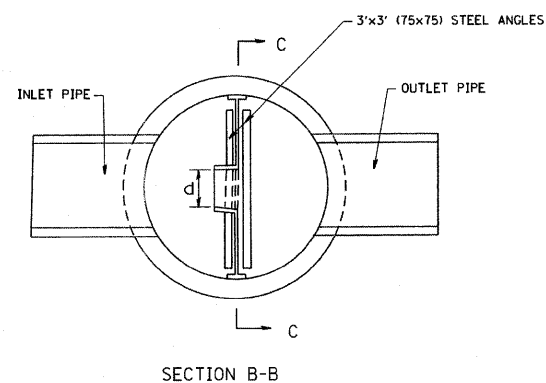
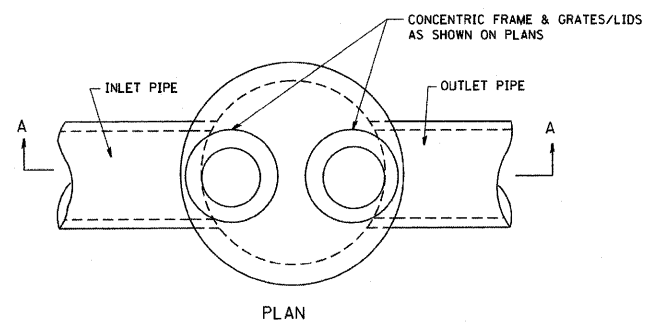
DETAILS FOR CONCRETE MEDIAN TYPE SB (DOWELLED) CORRUGATED MEDIAN (MODIFIED)

REVISIONS	
NAME	DATE
M. DE YONG	05-14-90
M. DE YONG	06-13-90
M. DE YONG	06-14-90
M. DE YONG	06-20-90
M. DE YONG	09-21-90
M. DE YONG	10-12-90
R. SHAH	09-09-94
R. SHAH	10-25-94
E. GOMEZ	08-28-00
R. BORO	01-01-07

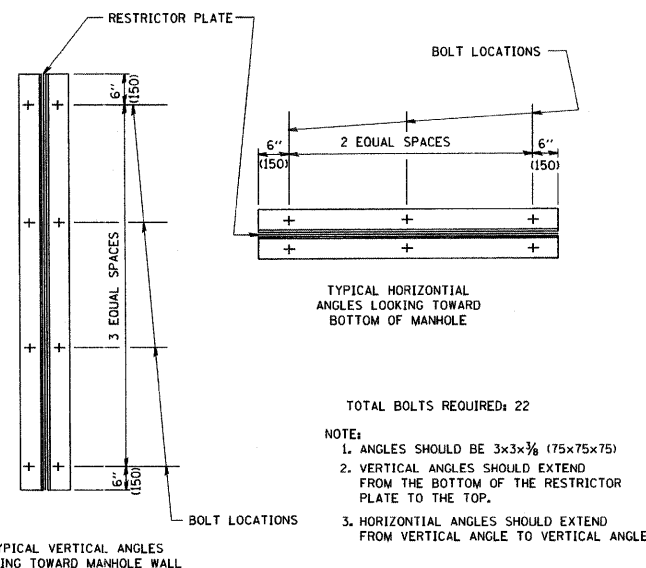
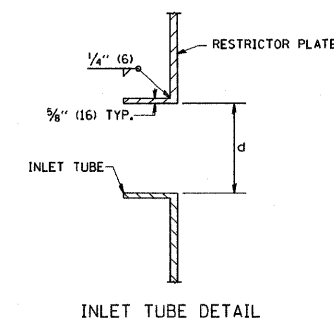
SCALE: NONE

DRAWN BY

CHECKED BY



- NOTES:
1. ALL STEEL ANGLES AND PLATES TO BE GALVANIZED AFTER FABRICATION.
 2. ALL RESTRICTOR PLATES, ANGLES AND HARDWARE TO BE INCLUDED IN THE COST OF THE MANHOLE.
 3. BASIS OF PAYMENT: "MANHOLES, TYPE A, 6 FT. (1.8 m)-DIAMETER, TYPE I FRAME, CLOSED LID, RESTRICTOR PLATE" EACH



RESTRICTOR TYPE					
1	2	3	4	5	6
RE-ENTRANT TUBE	SHARP EDGED	SQUARE EDGED	RE-ENTRANT TUBE	SQUARE EDGED	ROUNDED
LENGTH 1/2 TO 1 DIA.		STREAM CLEARS SIDES	LENGTH: 2-1/2 DIA.	LENGTH: 2-1/2 DIA.	
C=.52	C=.61	C=.61	C=.73	C=.82	C=.98

VALUES OF "C" FOR CIRCULAR AND SQUARE ORIFICES

STR. NO.	STATION	OFFSET	ROADWAY	RESTRICTOR DIAMETER (INCHES)	RESTRICTOR INVERT	TOP OF PLATE ELEVATION	100-YR HWL	RIM ELEVATION
S 104	31+50	27.00'	RT WEBER	18	631.00	636.30	636.45	638.32
S 128A	42+65	61.40'	RT WEBER	15	637.15	642.02	642.02	644.78
S 182	66+05	62.00'	LT WEBER	21	634.92	638.00	638.25	640.05
S 183	89+28	33.00'	LT RENWICK	16	628.70	631.00	631.25	633.11
S 197A	91+85	34.00'	RT RENWICK	15	632.60	635.40	635.60	637.46
S 225A	105+15	58.00'	LT RENWICK	10	637.39	640.92	640.92	645.05
S 231A	105+10	40.50'	RT RENWICK	12	638.38	641.80	641.80	645.05
S 303	134+50	27.00'	LT RENWICK	12	644.56	646.90	647.15	648.91
S 309	134+35	49.50'	RT RENWICK	12	643.29	645.90	646.15	647.94

BD-12
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
R. SHAH	09-09-94
R. SHAH	10-25-94
E. GOMEZ	08-28-00
M. GOMEZ	01-08-01

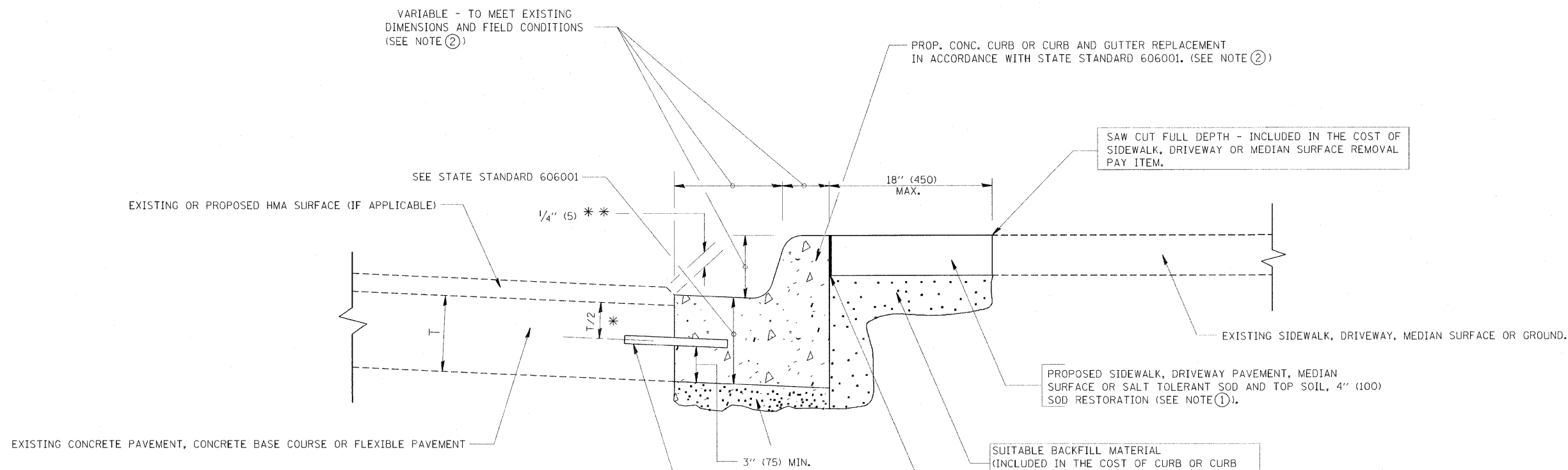
ILLINOIS DEPARTMENT OF TRANSPORTATION

MANHOLE WITH RESTRICTOR PLATE

SCALE: NONE

DRAWN BY
CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	122
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO: 63570				



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
 - * * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

- PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

BD-24

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
A. HOUSEH	03-11-94
R. SHAH	02-24-95
R. SHAH	03-02-95
R. SHAH	08-19-96
R. SHAH	09-12-96
R. SHAH	09-19-96
R. SHAH	10-03-96
A. ABBAS	03-21-97
M. GOMEZ	01-22-01
R. BORO	12-15-09

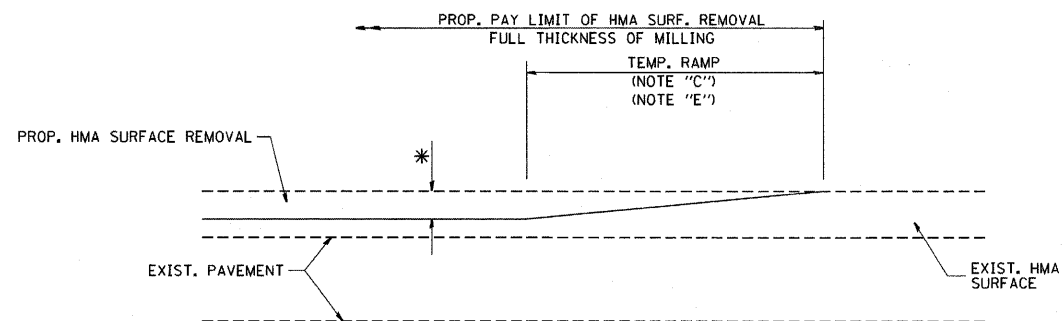
ILLINOIS DEPARTMENT OF TRANSPORTATION

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

SCALE: NONE DRAWN BY
CHECKED BY

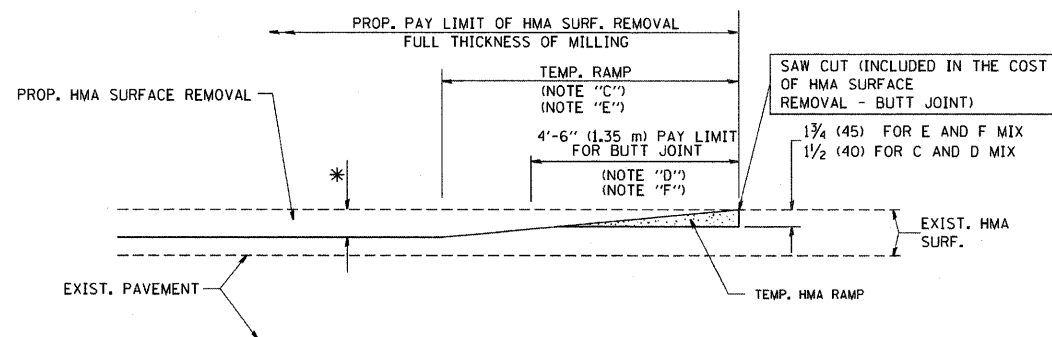
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	123
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO: 63570				



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

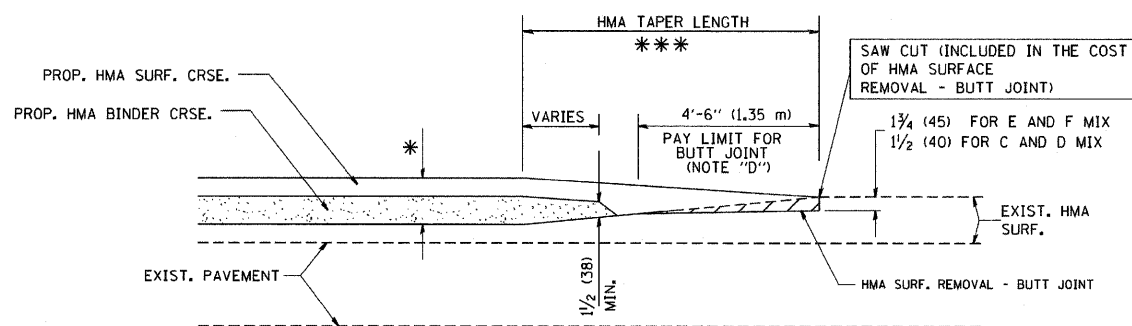
OPTION 1



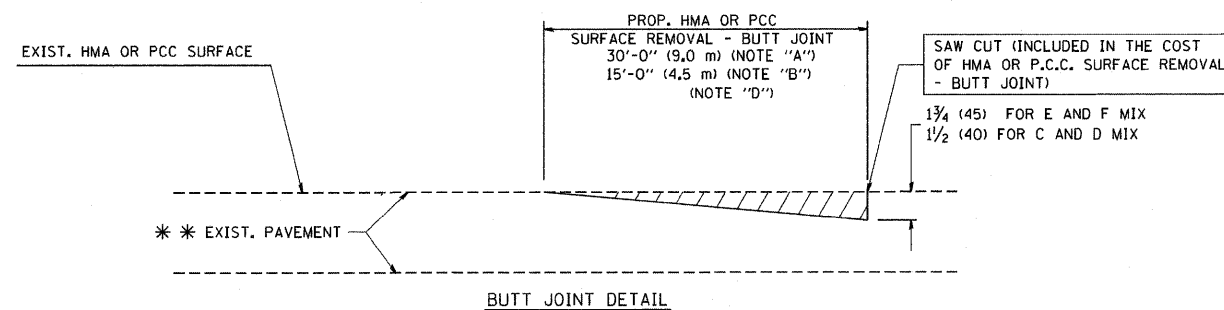
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

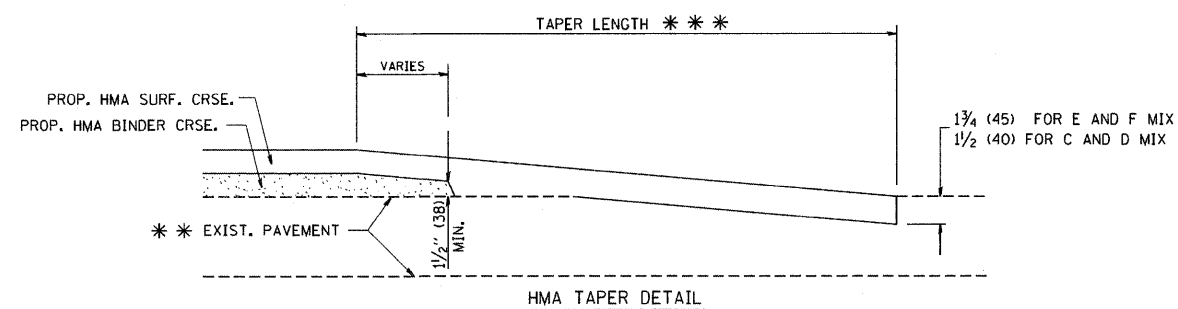
TYPICAL TEMPORARY RAMP



BUTT JOINT AND HMA TAPER
TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- ** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

BD-32

ILLINOIS DEPARTMENT OF TRANSPORTATION

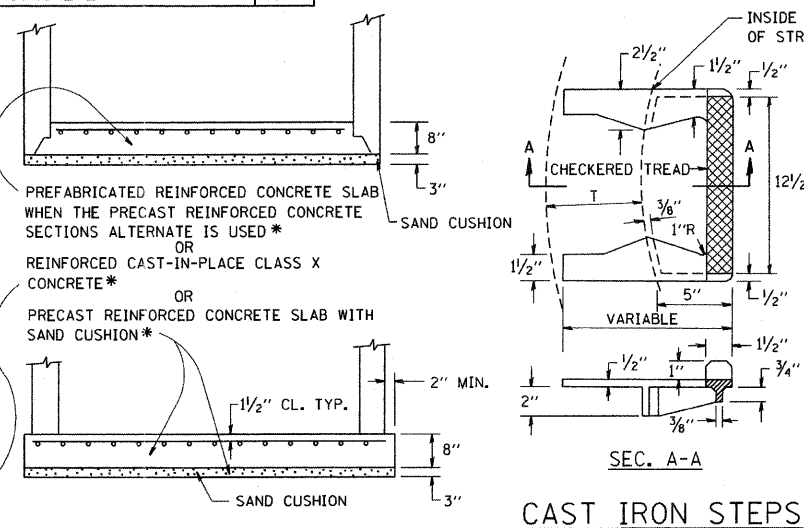
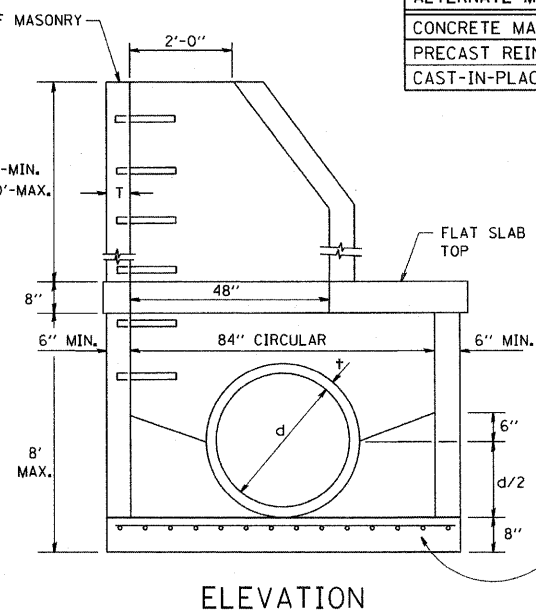
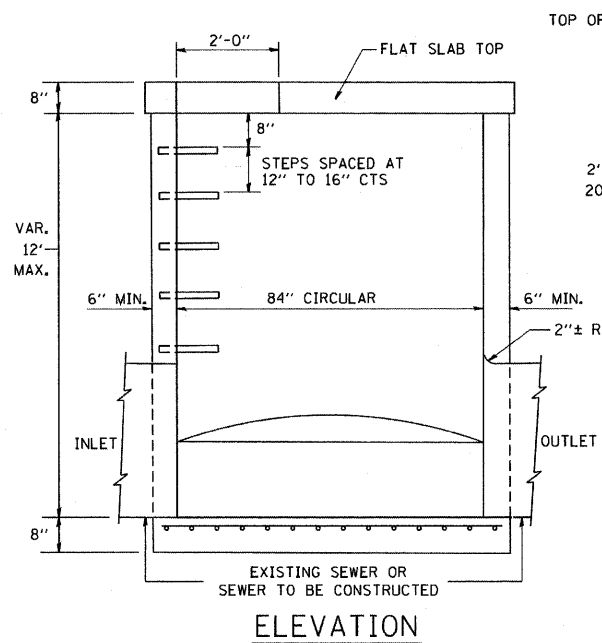
BUTT JOINT AND HMA TAPER
DETAILS

REVISIONS	
NAME	DATE
M. DE YONG	06-13-90
M. DE YONG	07-03-90
M. DE YONG	03-27-92
R. SHAH	09-09-94
R. SHAH	10-25-94
A. ABBAS	03-21-97
M. GOMEZ	04-06-01
R. BORO	01-01-07

SCALE: NONE

DRAWN BY
CHECKED BY

ALTERNATE MATERIALS FOR RISERS	(MIN.)
CONCRETE MASONRY UNITS	5"
PRECAST REINFORCED CONCRETE SECTIONS	4"
CAST-IN-PLACE CONCRETE	6"



GENERAL NOTES

ALTERNATE MATERIAL FOR THE WALLS MAY BE CONCRETE MASONRY UNITS, PRECAST REINFORCED CONCRETE SECTIONS OR CAST-IN-PLACE CONCRETE. THE CAST IRON STEPS AS DETAILED HEREON ARE TYPICAL. STEPS OF OTHER DESIGN AND MATERIAL THAT CONFORM TO THE MINIMUM REQUIREMENTS OF THE STEPS SHOWN MAY BE USED WHEN APPROVED BY THE ENGINEER.

CAST IRON STEPS SHALL BE GRAY IRON CONFORMING TO THE REQUIREMENTS OF ARTICLE 1006.14 OF THE STANDARD SPECIFICATIONS.

STEPS SHALL BE EMBEDDED INTO THE WALL A MINIMUM OF THREE (3) INCHES. STEPS SHALL NOT BE EXTENDED ON THE OUTSIDE.

STEPS SHALL BE OMITTED FOR WORK IN COOK COUNTY WHEN THE DEPTH OF THE MANHOLE IS TEN (10') OR LESS.

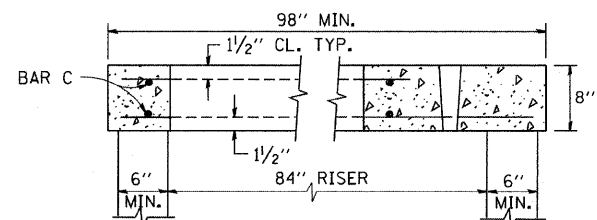
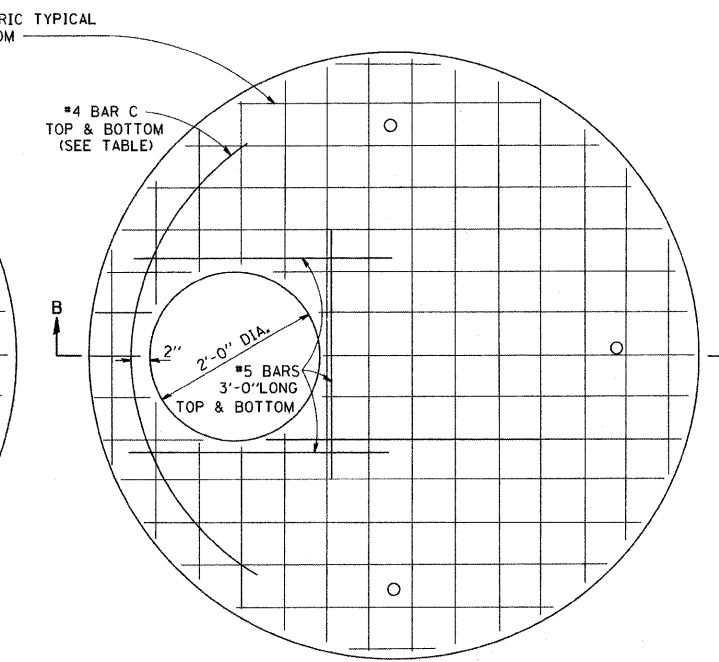
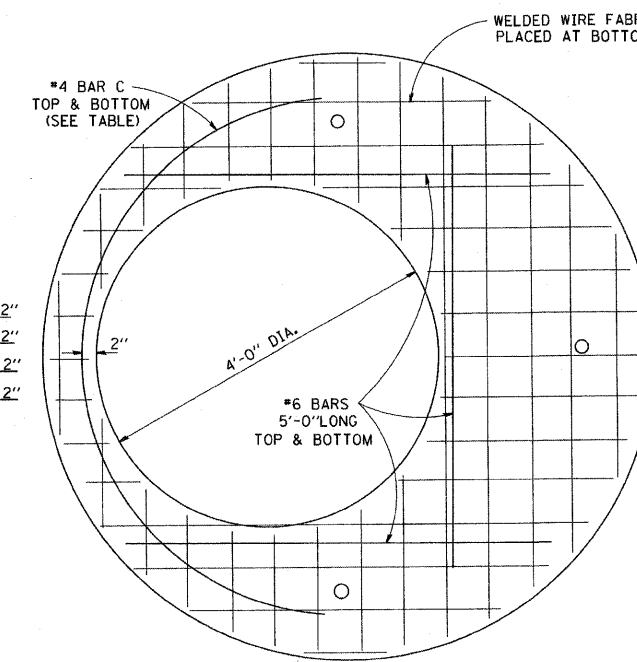
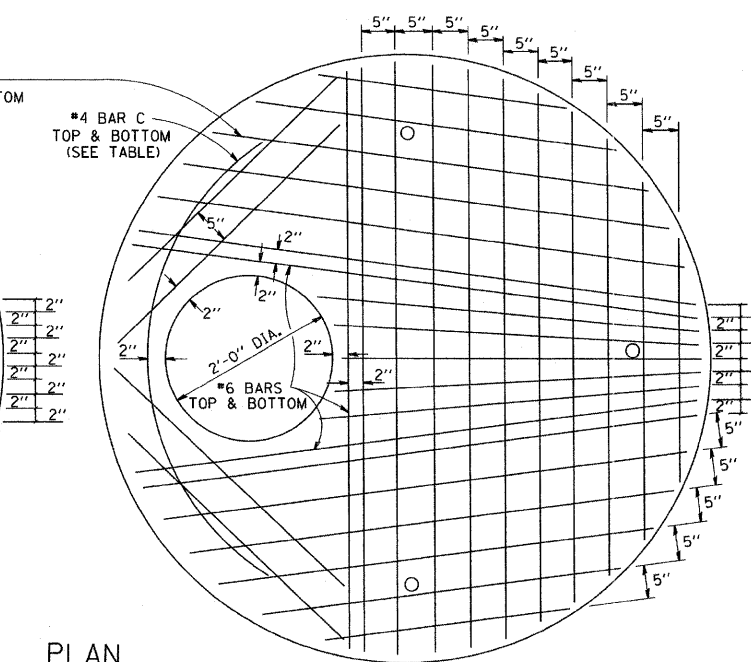
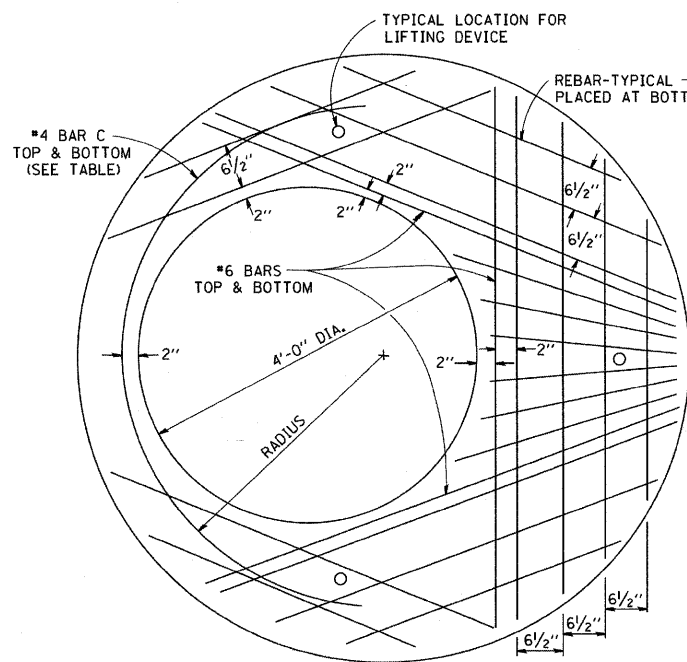
IN ADDITION TO THE REQUIREMENTS OF ARTICLE 612.13 OF THE STANDARD SPECIFICATIONS, THE CONTRACT UNIT PRICE FOR MANHOLES, TYPE A, 7'-DIAMETER SHALL INCLUDE THE SAND CUSHION WHEN REQUIRED, FURNISHING AND INSTALLING STEPS WHEN REQUIRED, FURNISHING AND COMPACTING THE SPECIFIED BACKFILL MATERIAL, AND FURNISHING AND INSTALLING FLAT SLAB TOP.

PRECAST FLAT SLAB TOP SHALL CONFORM TO ARTICLES 505.01 THRU 505.05 OF THE STANDARD SPECIFICATIONS EXCEPT THAT THE CONCRETE STRENGTH SHALL BE 4,000 PSI AFTER 28 DAYS. REINFORCEMENT BARS AND WELDED WIRE FABRIC SHALL CONFORM TO THE REQUIREMENTS OF ARTICLE 1006.10. ONLY GRADE 60 REINFORCEMENT BARS WILL BE PERMITTED.

BOTTOM SLAB SHALL BE REINFORCED BY EITHER REINFORCEMENT BARS OR WELDED WIRE FABRIC. THE MINIMUM REINFORCEMENT SHALL BE 0.46 SQUARE INCH PER LINEAR FOOT IN BOTH DIRECTIONS.

JOINT CONFIGURATION AND DIMENSIONS OF FLAT SLAB TOP SHALL MATCH AND FIT THE RISER JOINT DETAIL.

LIFTING DEVICES SHALL BE APPROVED BY THE ENGINEER.



DIAMETER OF OPENING	REINFORCEMENT "A _s " WWF EACH DIRECTION	OR BAR SIZE	BAR C		
			SIZE	LENGTH	RADIUS
2'-0"	1.06 SO.IN./LIN.FT.	#6	#4	6'-0"	38"
4'-0"	0.82 SO.IN./LIN.FT.	#6	#4	9'-0"	38"

NOTE: THIS STRUCTURE SHOULD BE USED WITH PIPES SIZE 54" DIA. OR SMALLER.

REVISIONS	
NAME	DATE

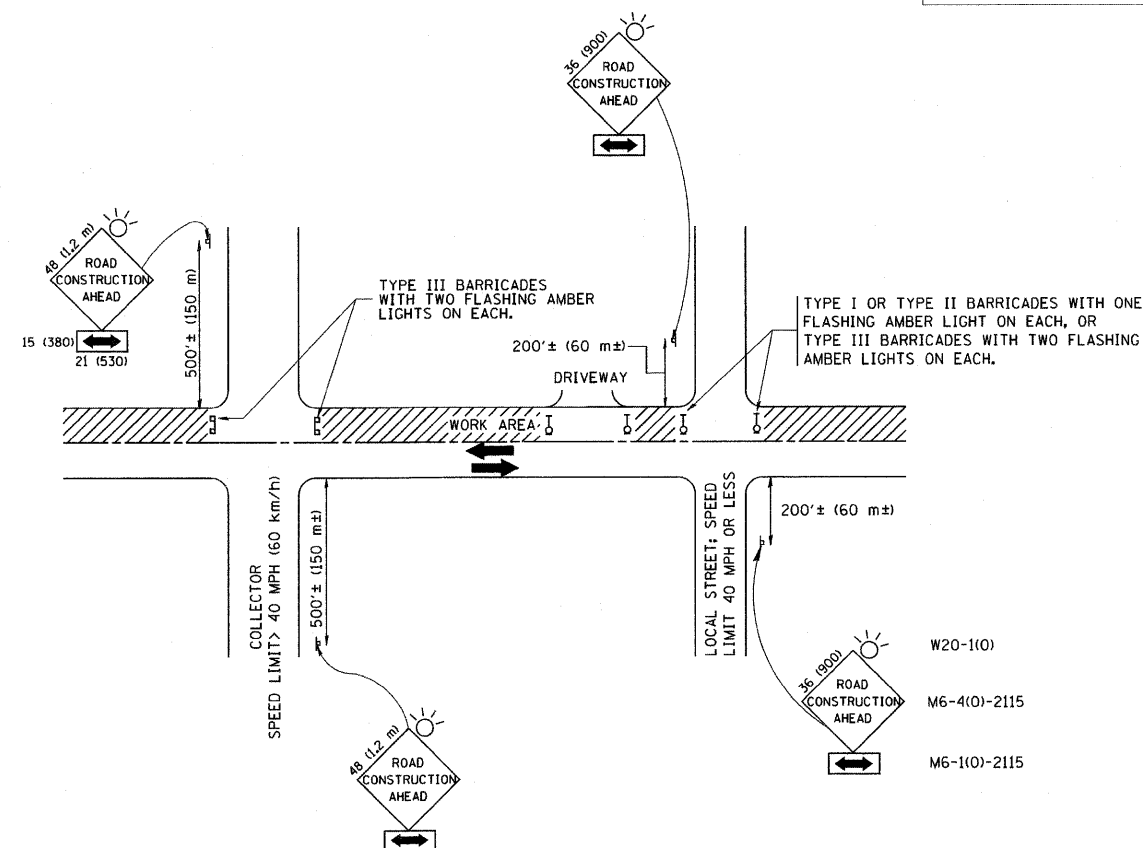
ILLINOIS DEPARTMENT OF TRANSPORTATION

**MANHOLE TYPE A
7 FOOT DIAMETER**

SCALE: NONE

DRAWN BY
CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
B56	01-00170-26-TL	WILL	171	125
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO: 63570				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS**
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).**
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:**
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.**
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

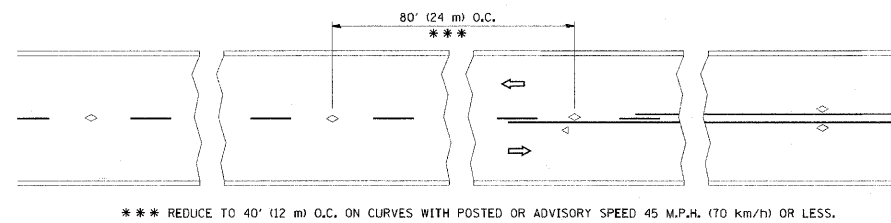
TC-10

ILLINOIS DEPARTMENT OF TRANSPORTATION
**TRAFFIC CONTROL AND PROTECTION
 FOR
 SIDE ROADS, INTERSECTIONS, AND
 DRIVEWAYS**

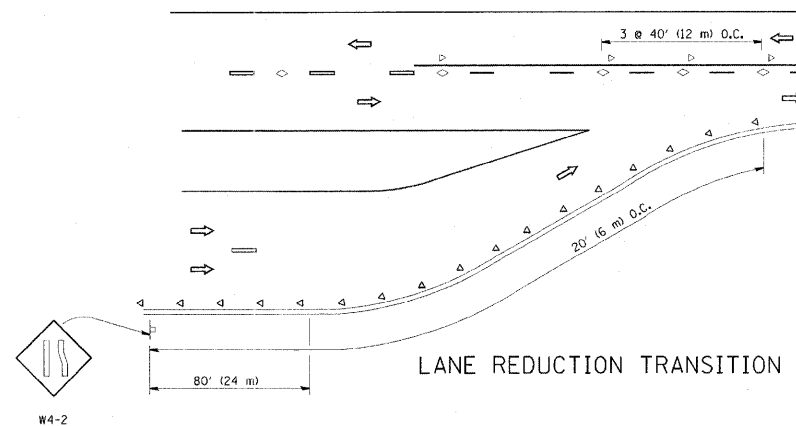
REVISIONS	
NAME	DATE
LHA	06-89
T. RAMMACHER	09-08-94
J. OBERLE	10-18-95
A. HOUSEH	03-06-96
A. HOUSEH	10-15-96
T. RAMMACHER	01-06-00

SCALE: NONE

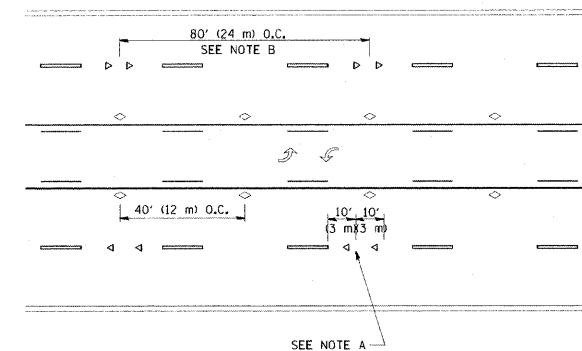
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 CHECKED BY



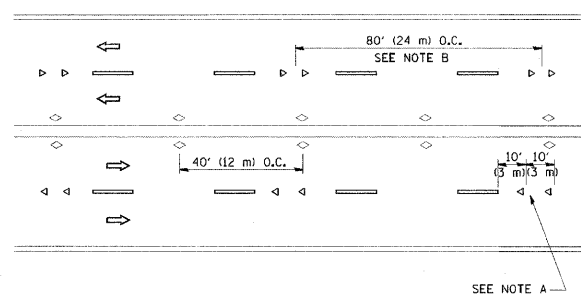
TWO-LANE/TWO-WAY



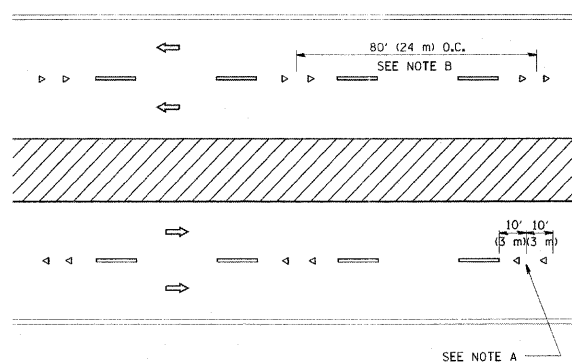
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

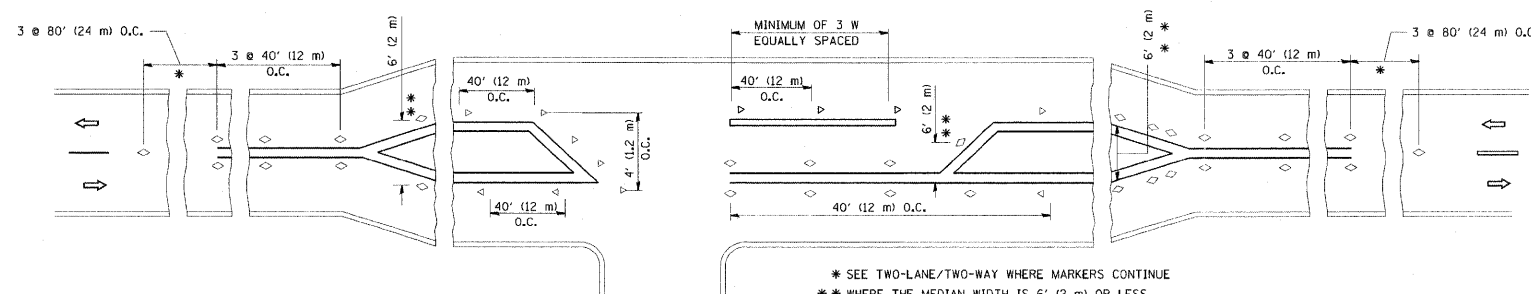
- YELLOW STRIPE
- WHITE STRIPE
- ◁ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/D)
- ◊ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
 B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

TC-11

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

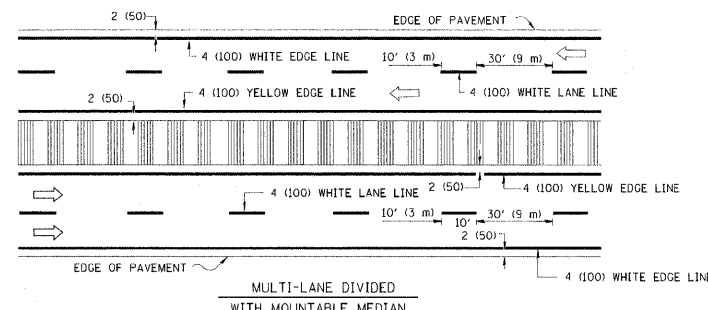
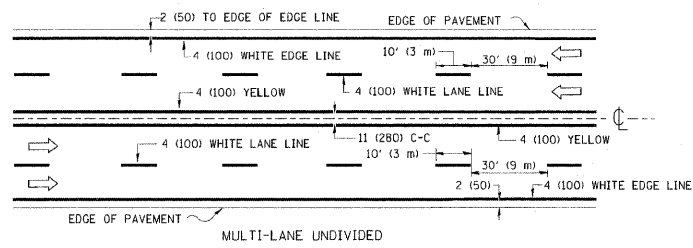
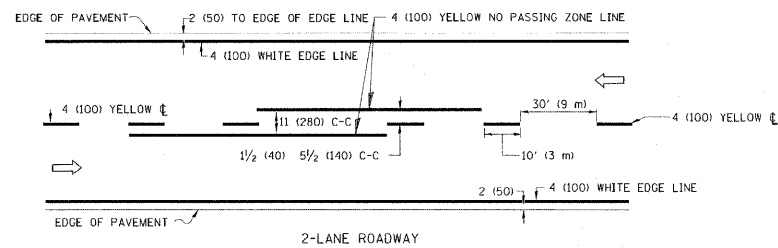
REVISIONS	
NAME	DATE
T. RAMMACHER	09-19-94
T. RAMMACHER	03-12-99
T. RAMMACHER	01-06-00
C. JUXTUS	09-09-09

ILLINOIS DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT
 MARKERS (SNOW-PLOW RESISTANT)

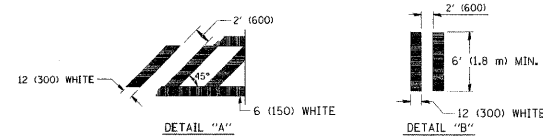
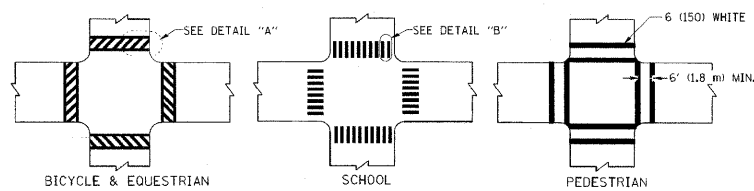
SCALE: NONE

DRAWN BY
 CHECKED BY

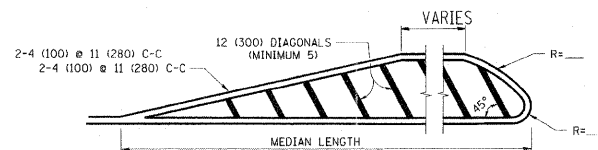
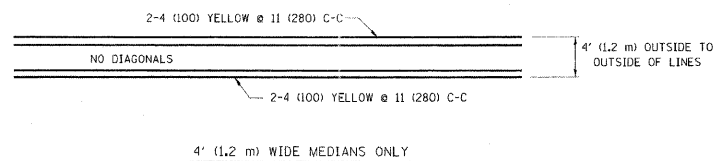


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

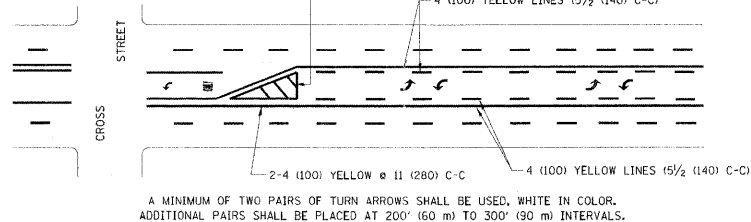


TYPICAL CROSSWALK MARKING

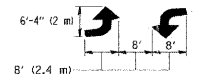


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

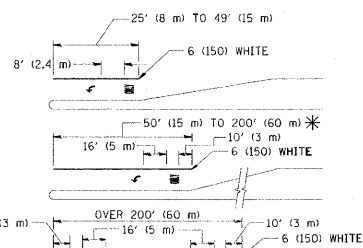


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

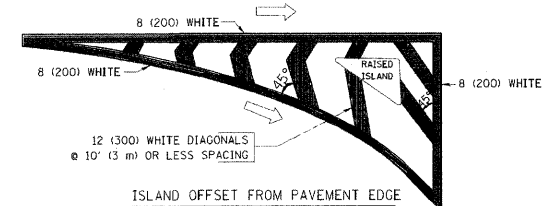
TYPICAL PAINTED MEDIAN MARKING



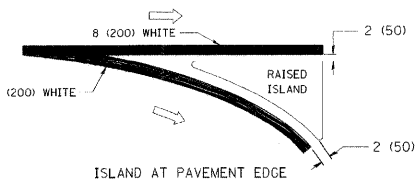
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW. EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 78000L

All dimensions are in inches (millimeters) unless otherwise shown.

REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
C. JUCIUS	09-09-09

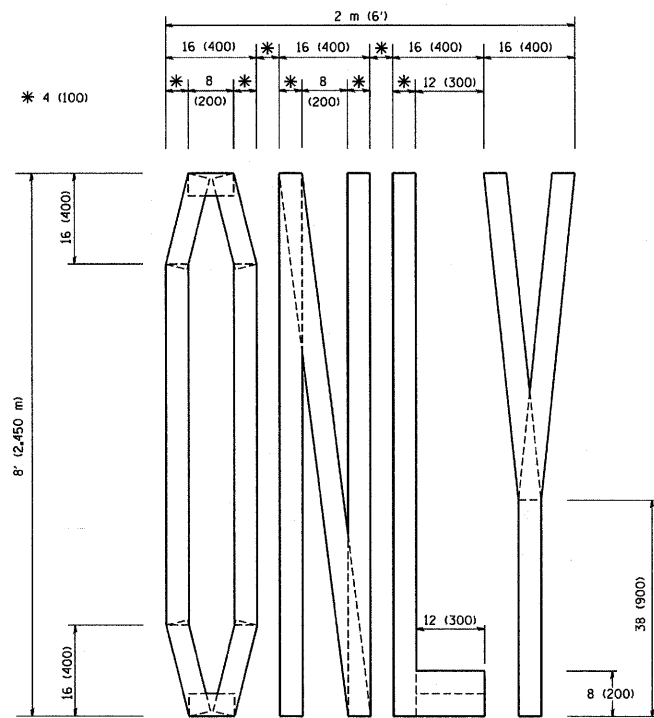
ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT
MARKINGS

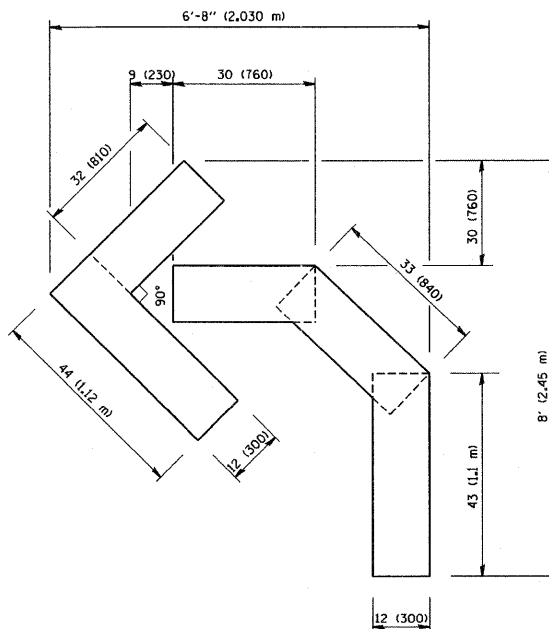
SCALE: NONE

DRAWN BY
CHECKED BY

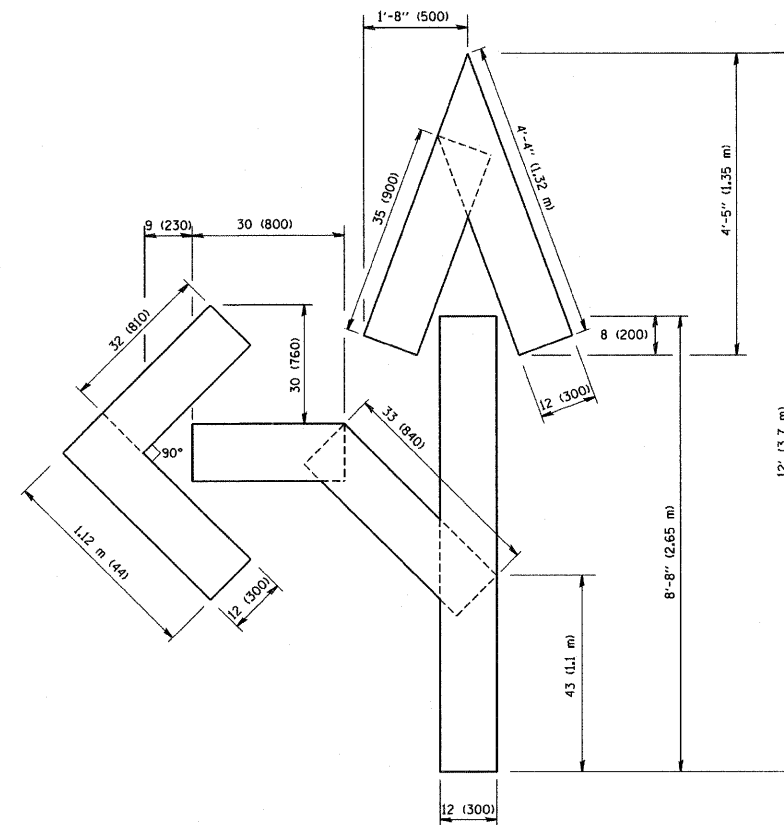
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
B56	01-00170-26-TL	WILL	171	128
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				



QUANTITY
4 (100) LINE = 64.1 ft. (19.7 m)
21.1 sq. ft. (1.97 sq. m)



QUANTITY
4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.39 sq. m)



QUANTITY
4 (100) LINE = 82.5 ft. (25.3 m)
27.5 sq. ft. (2.53 sq. m)

8/22/2011
P:\79766\cadd\drawings\128\TC16.dgn

TC-16

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING
LETTERS AND SYMBOLS
FOR TRAFFIC STAGING

REVISIONS	
NAME	DATE
T. RAMMACHER	09-18-94
J. OBERLE	06-01-96
T. RAMMACHER	06-05-96
T. RAMMACHER	11-04-97
T. RAMMACHER	03-02-98
E. GOMEZ	08-28-00

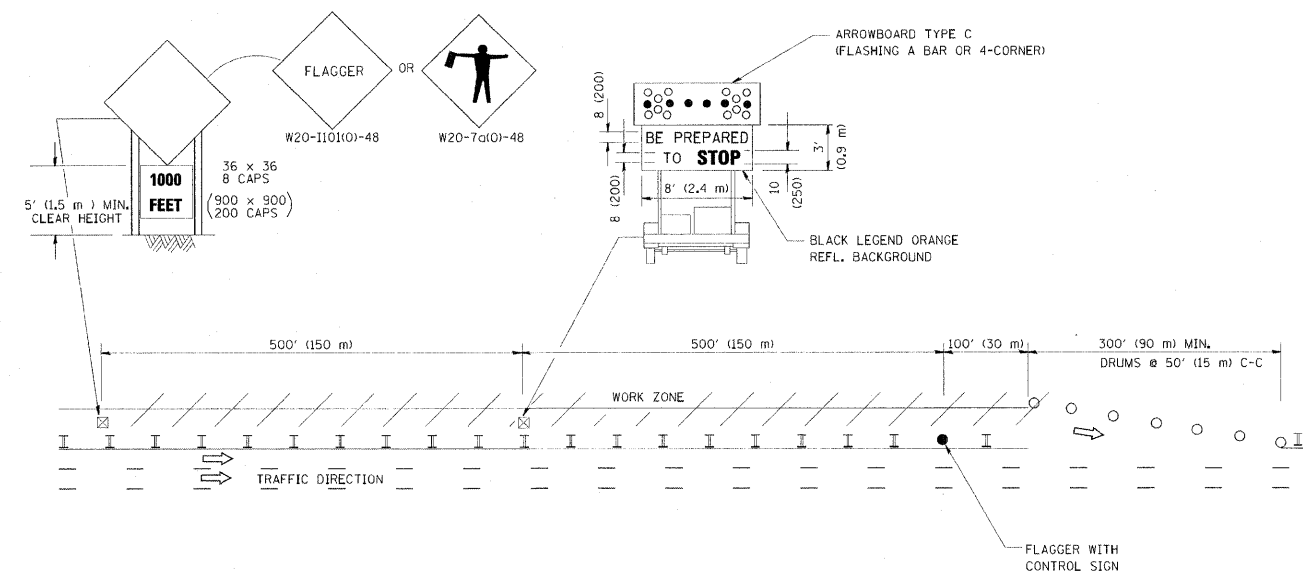
SCALE: NONE

DRAWN BY
CHECKED BY

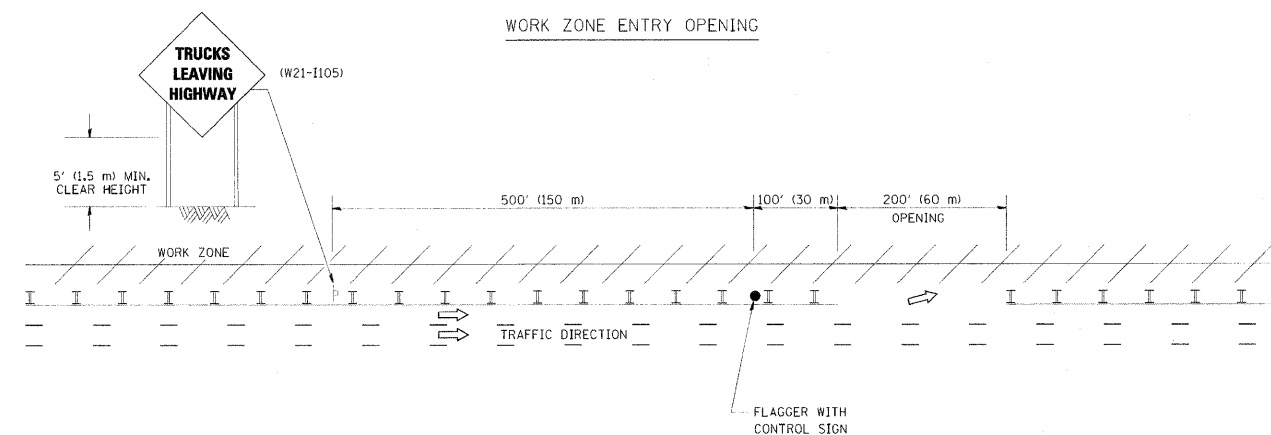
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	129
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:

1. THE ARROWBOARD, THE FLAGGER AHEAD SIGN AND THE TRUCKS LEAVING HIGHWAY SIGN SHALL BE REMOVED OR TURNED AWAY FROM TRAFFIC AND THE EXIT AND ENTRY OPENINGS SHALL BE CLOSED WHEN THE FLAGGING OPERATION CEASES. NON OPERATING EQUIPMENT SHALL COMPLY WITH ARTICLE 701.11
2. WORK ZONE EXIT OPENINGS SHOULD BE A MINIMUM OF ONE HALF MILE APART.
3. EXITING THE WORK ZONE AT ANY PLACE OTHER THAN AT A WORK ZONE EXIT OPENING WILL BE PROHIBITED.
4. ALL VEHICLES SHALL ENTER THE WORK ZONE AT ENTRY OPENINGS, USING THEIR TURN SIGNALS TO WARN MOTORISTS

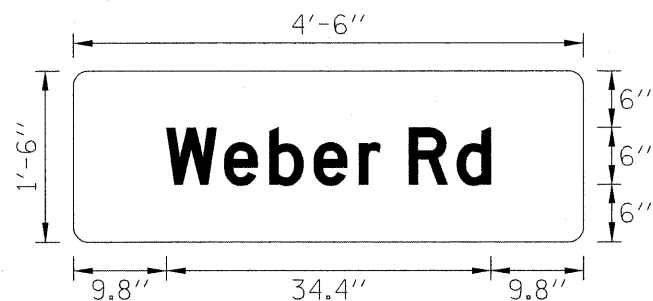
REVISIONS	
NAME	DATE
DWS	08-98
JAF	04-03
JAF	02-06
SPB	01-07
SPB	12-09

TC-18
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

ILLINOIS DEPARTMENT OF TRANSPORTATION
**SIGNING FOR FLAGGING OPERATIONS
AT WORK ZONE OPENINGS**

SCALE: NONE
DRAWN BY
CHECKED BY

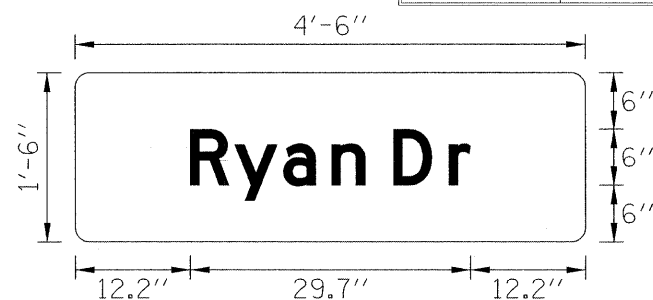
PANEL SIGN DESIGN TYPE 1



LETTER POSITIONS (X)
(INCHES FROM LEFT EDGE OF PANEL)

W	e	b	e	r		R	d
9.8	16	20.8	25.4	30.2	32.5	35.5	40.6

SIGN NUMBER	6-3in No. Centered (no border)
WIDTH x HGHT.	4'-6" x 1'-6"
BORDER WIDTH	0"
CORNER RADIUS	1.5"
MOUNTING	Ground
BACKGROUND	TYPE: Reflective COLOR: Green
LEGEND/BORDER	TYPE: Reflective COLOR: White/Green



LETTER POSITIONS (X)
(INCHES FROM LEFT EDGE OF PANEL)

R	y	a	n		D	r
12.2	16.9	22	27.1	30.7	33.7	39.6

SIGN NUMBER	6-3in No. Centered (no border)
WIDTH x HGHT.	4'-6" x 1'-6"
BORDER WIDTH	0"
CORNER RADIUS	1.5"
MOUNTING	Ground
BACKGROUND	TYPE: Reflective COLOR: Green
LEGEND/BORDER	TYPE: Reflective COLOR: White/Green

GENERAL NOTES

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 877001, 877002, 877006, 877011 AND 877012, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 8'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- ALL SIGNS SHALL HAVE A WHITE REFLECTORIZED LEGEND AND BORDER ON A GREEN REFLECTORIZED BACKGROUND, TYPE A SHEETING.
- THE SIGN LENGTH SHOULD BE INCREASED IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHOULD NOT EXCEED 8'-0".
- ALL BORDERS SHALL BE 3/4" WIDE AND CORNER RADIUS SHALL BE 2-1/4".
- SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS. LOCAL SUPPLIERS OF THE SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM ARE:

* J.O. HERBERT CO.
MIDLOTHIAN, VA.

* WESTERN REMAC INC.
WOODRIDGE, IL.

PARTS LISTING:
SIGN CHANNEL
SIGN SCREWS

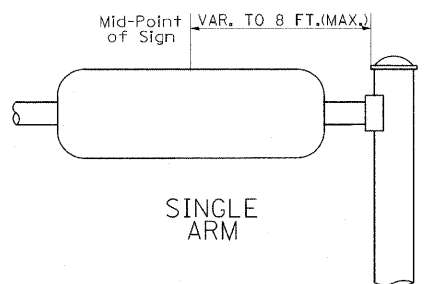
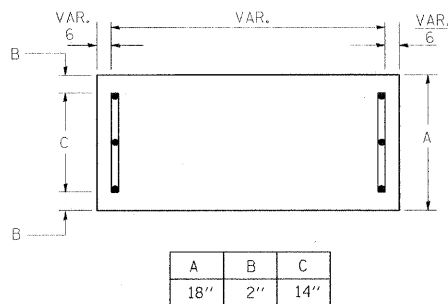
PART #HPN053 (MED. CHANNEL)
1/4" x 14 x 1" H.W.H. #3
SELF TAPPING WITH NEOPRENE WASHER

BRACKETS

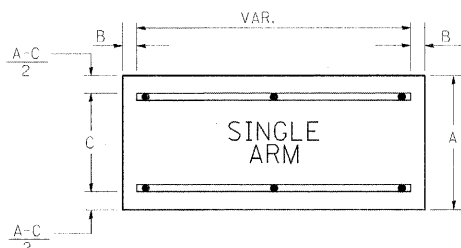
PART #HPN034 (UNIVERSAL)
CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING

OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BRACKET OF THE ABOVE PRODUCT.

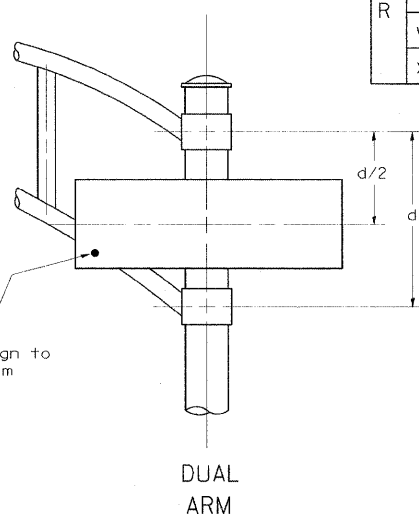
SUPPORTING CHANNELS



SUPPORTING CHANNELS



A	B	C
18"	2"	12"
30"	2"	22"



SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM shall be used. See Note #5.

Upper Case To Lower Case
Spacing Chart 8-6 Inch Series "C & D"

		SECOND LETTER																				
		acde goq				bhikl mnp ru				f w		j		s t		v y		x		z		
FIRST LETTER	SERIES	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	
	A W X	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ²	1 ⁴	1 ⁰	1 ⁰	1 ¹	1 ⁴	1 ⁰	1 ¹	1 ⁴	1 ⁰	1 ¹	1 ²	1 ²	1 ⁴	1 ²	1 ⁴	
	B	1 ⁴	1 ⁵	2 ⁰	2 ¹	1 ⁴	1 ⁵	1 ¹	1 ²	1 ⁴	1 ⁵	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴	1 ⁶	1 ⁷			
	C E G	1 ⁴	1 ⁵	2 ⁰	2 ¹	1 ²	1 ⁴	1 ⁰	1 ⁰	1 ²	1 ⁴	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ⁴	1 ⁵					
	D O Q R	1 ⁴	1 ⁵	2 ⁰	2 ¹	1 ⁴	1 ⁵	1 ⁰	1 ⁰	1 ²	1 ⁴	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ⁴	1 ⁵					
	F	0 ⁵	0 ⁶	1 ⁴	1 ⁵	0 ⁶	1 ⁰	0 ⁵	0 ⁶	0 ⁶	1 ⁰	0 ⁶	1 ⁰	0 ⁶	1 ⁰	1 ¹	1 ²					
	H I M N	2 ⁰	2 ¹	2 ²	2 ⁴	2 ⁰	2 ¹	1 ⁴	1 ⁵	1 ⁶	1 ⁷	1 ⁶	1 ⁷	2 ⁰	2 ¹	2 ⁰	2 ¹					
	J U	2 ⁰	2 ¹	2 ⁰	2 ¹	1 ⁶	1 ⁷	1 ⁴	1 ⁵	1 ⁶	1 ⁷	1 ⁶	1 ⁷	1 ⁶	1 ⁷	2 ⁰	2 ¹					
	K L	1 ¹	1 ²	1 ⁶	1 ⁷	1 ¹	1 ²	0 ⁵	0 ⁶	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ²	1 ⁴			
	P	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ²	1 ⁴	0 ⁵	0 ⁶	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ⁴	1 ²	1 ⁴		
	S	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ²	1 ⁴	0 ⁶	1 ⁰	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴			
T	1 ¹	1 ²	1 ⁶	1 ⁷	0 ⁶	1 ⁰	0 ⁶	1 ⁰	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ⁴			
V	0 ⁶	1 ⁰	1 ⁴	1 ⁵	1 ¹	1 ²	0 ⁶	1 ⁰	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴				
Y	0 ⁵	0 ⁶	1 ⁴	1 ⁵	0 ⁶	1 ⁰	0 ⁵	0 ⁶	0 ⁵	0 ⁷	0 ⁵	0 ⁶	0 ⁶	1 ⁰	1 ¹	1 ²						
Z	1 ⁶	1 ⁷	2 ²	2 ⁴	1 ⁶	1 ⁷	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ⁶	1 ⁷	1 ⁶	1 ⁷	2 ⁰	2 ¹						

Lower Case To Lower Case
Spacing Chart 6 Inch Series "C & D"

		SECOND LETTER																				
		acde goq				bhikl mnp ru				f w		j		s t		v y		x		z		
FIRST LETTER	SERIES	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	
	a d h g i j	1 ⁶	1 ⁷	2 ²	2 ⁴	1 ⁶	1 ⁷	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ⁶	1 ⁷	1 ⁶	1 ⁷					
	l m n q u																					
	b f k o p s	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ¹	1 ²	0 ⁵	0 ⁶	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ⁴	1 ²	1 ⁴		
	c e	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ²	1 ⁴	0 ⁶	1 ⁰	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴	1 ²	1 ⁴			
	r	0 ⁶	1 ⁰	1 ²	1 ⁴	0 ⁶	1 ⁰	0 ³	0 ³	0 ⁵	0 ⁶	0 ⁵	0 ⁶	0 ⁵	0 ⁶	1 ⁰	0 ⁶	1 ⁰				
	t z	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ²	1 ⁴	0 ⁶	1 ⁰	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ⁴	1 ²	1 ⁴		
	v y	1 ¹	1 ²	1 ⁴	1 ⁵	1 ¹	1 ²	0 ⁵	0 ⁶	0 ⁶	1 ⁰	0 ⁶	1 ⁰	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²			
	w	1 ¹	1 ²	1 ⁴	1 ⁵	1 ¹	1 ²	0 ⁵	0 ⁶	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ⁴		
	x	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ¹	1 ²	0 ⁵	0 ⁶	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ⁴		

Number To Number
Spacing Chart 8 Inch Series "C & D"

		SECOND NUMBER																			
		0		1		2		3		4		5		6		7		8		9	
FIRST NUMBER	SERIES	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
	0 9	1 ⁶	1 ⁷	1 ⁶	1 ⁷	1 ⁴	1 ⁵	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ⁶	1 ⁷	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ⁶	1 ⁷
	1	2 ⁰	2 ¹	2 ⁰	2 ¹	2 ⁰	2 ¹	1 ⁶	1 ⁷	1 ⁴	1 ⁵	2 ⁰	2 ¹	2 ⁰	2 ¹	1 ⁴	1 ⁵	2 ⁰	2 ¹	2 ⁰	2 ¹
	2 3 4	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ⁴	1 ⁵
	5	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ¹	1 ²	1 ¹	1 ²	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ¹	1 ²	1 ¹	1 ²	1 ⁴	1 ⁵
	6	1 ⁶	1 ⁷	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ⁴	1 ⁵	1 ¹	1 ²	1 ¹	1 ²	1 ¹	1 ²	1 ⁴	1 ⁵
	7	1 ²	1 ⁴	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ²	1 ⁵	0 ⁵	0 ⁶	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ¹	1 ²	1 ¹	1 ²	1 ⁴	1 ⁵
	8	1 ⁶	1 ⁷	1 ⁶	1 ⁷	1 ⁴	1 ⁵	1 ²	1 ⁵	1 ²	1 ⁴	1 ⁴	1 ⁵	1 ⁶	1 ⁷	1 ²	1 ⁴	1 ⁶	1 ⁷	1 ⁴	1 ⁵
	9																				

EXAMPLE, 2 DENOTES 3/8"

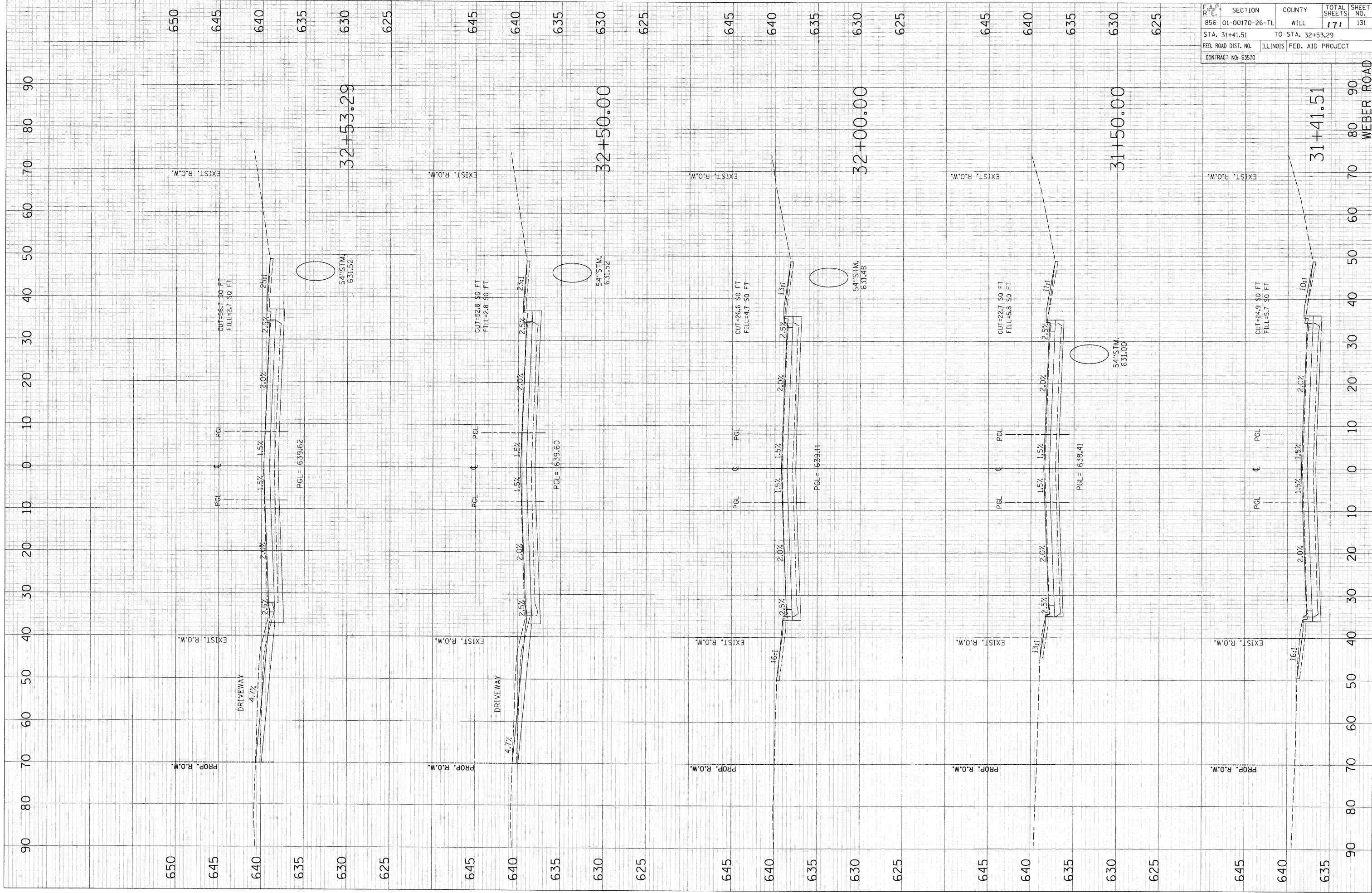
UPPER AND LOWER CASE
LETTER WIDTHS

LETTERS	6 INCH UPPER CASE LETTERS		8 INCH UPPER CASE LETTERS		LETTERS	6 INCH LOWER CASE LETTERS	
	SERIES		SERIES			SERIES	
	C	D	C	D		C	D
A	3 ⁶	5 ⁰	5 ⁰	6 ⁵	a	3 ⁵	4 ²
B	3 ²	4 ⁰	4 ³	5 ³	b	3 ⁵	4 ²
C	3 ²	4 ⁰	4 ³	5 ³	c	3 ⁵	4 ¹
D	3 ²	4 ⁰	4 ³	5 ³	d	3 ⁵	4 ²
E	3 ⁰	3 ⁵	4 ⁰	4 ⁷	e	3 ⁵	4 ²
F	3 ⁰	3 ⁵	4 ⁰	4 ⁷	f	2 ³	2 ⁶
G	3 ²	4 ⁰	4 ³	5 ³	g	3 ⁵	4 ²
H	3 ²	4 ⁰	4 ³	5 ³	h	3 ⁵	4 ²
I	0 ⁷	0 ⁷					

PLOT DATE = 8/22/2011
 FILE NAME = P:\79766\cadd\p\swings\131\swings.dwg
 PLOT SCALE = 10.000' / 1" = 1000
 USER NAME = pswalcz

ORIGINAL SURVEY PLOTTED TEMPLATE AREAS CHECKED
 NO. BY DATE

FINAL SURVEY PLOTTED TEMPLATE AREAS CHECKED
 NO. BY DATE



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	131
STA. 31+41.51		TO STA. 32+53.29		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

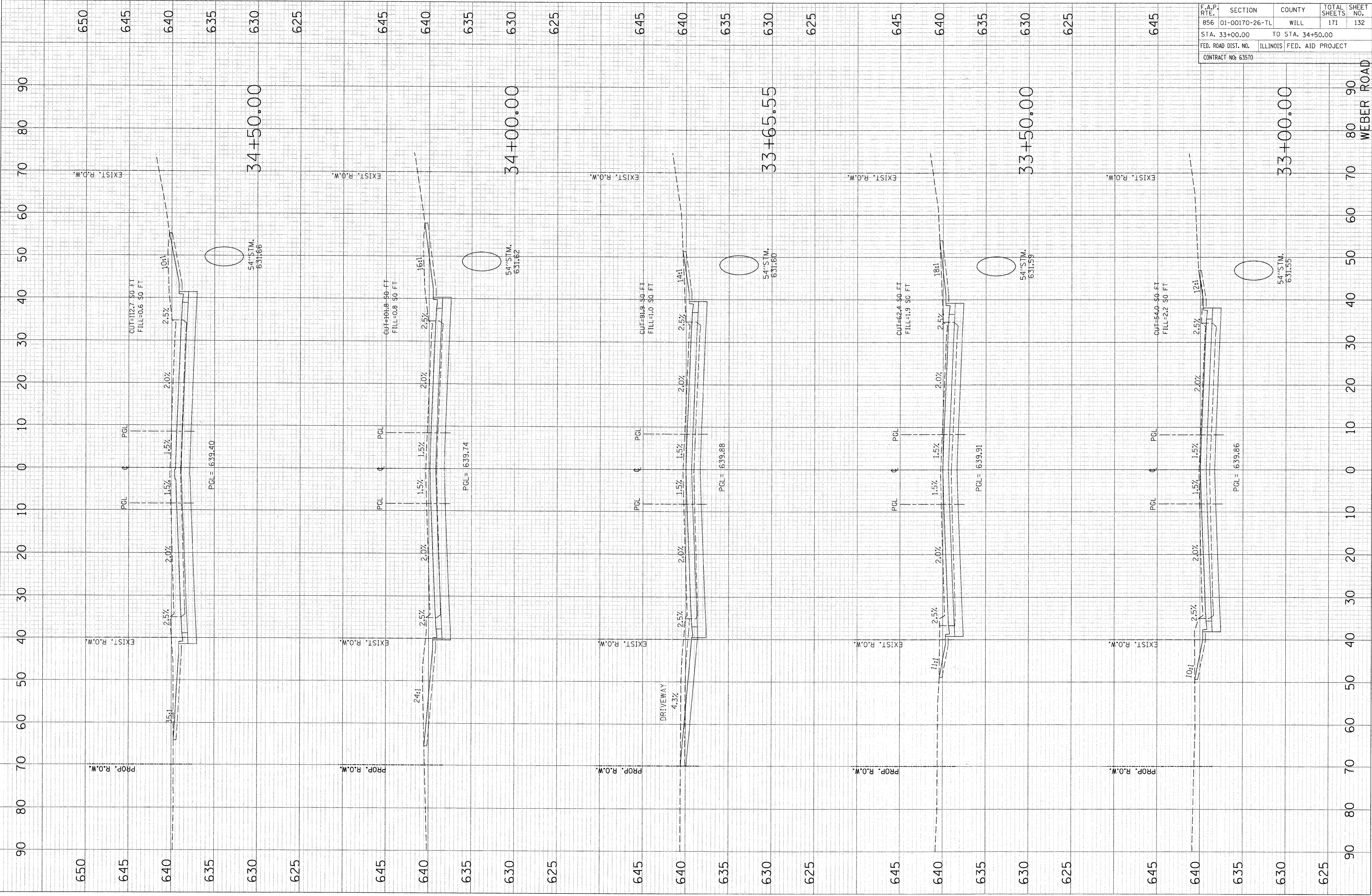
WEBER ROAD

PLOT DATE = 8/22/2011
 FILE NAME = P:\179765\cadd\drwg\131\rs_ah\11111111.dwg
 PLOT SCALE = 10/000' / in.
 USER NAME = pvtalp2

ORIGINAL SURVEY PLOTTED AREAS CHECKED
 NO. _____

FINAL SURVEY PLOTTED AREAS CHECKED
 NO. _____

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	132
STA. 33+00.00		TO STA. 34+50.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

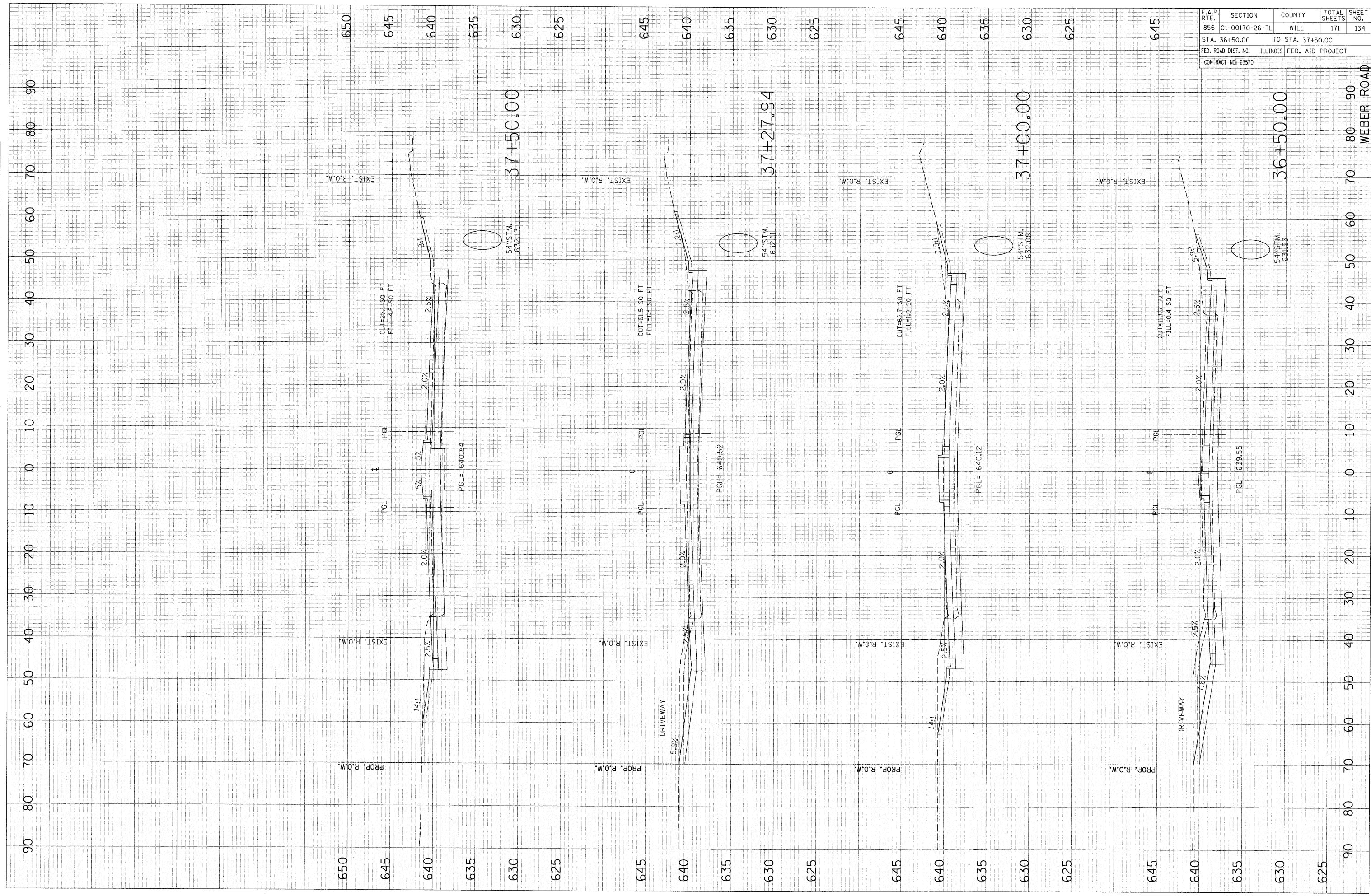


WEBER ROAD

PLOT DATE = 8/22/2011
 FILE NAME = P:\79766\cadd\drwg\181x\181x.dwg
 PLOT SCALE = 10.000 / 1"
 USER NAME = bbarlow

ORIGINAL SURVEY PLOTTED
 SURVEY PLOTTED
 TEMPLATE AREAS CHECKED

FINAL SURVEY PLOTTED
 SURVEY PLOTTED
 TEMPLATE AREAS CHECKED



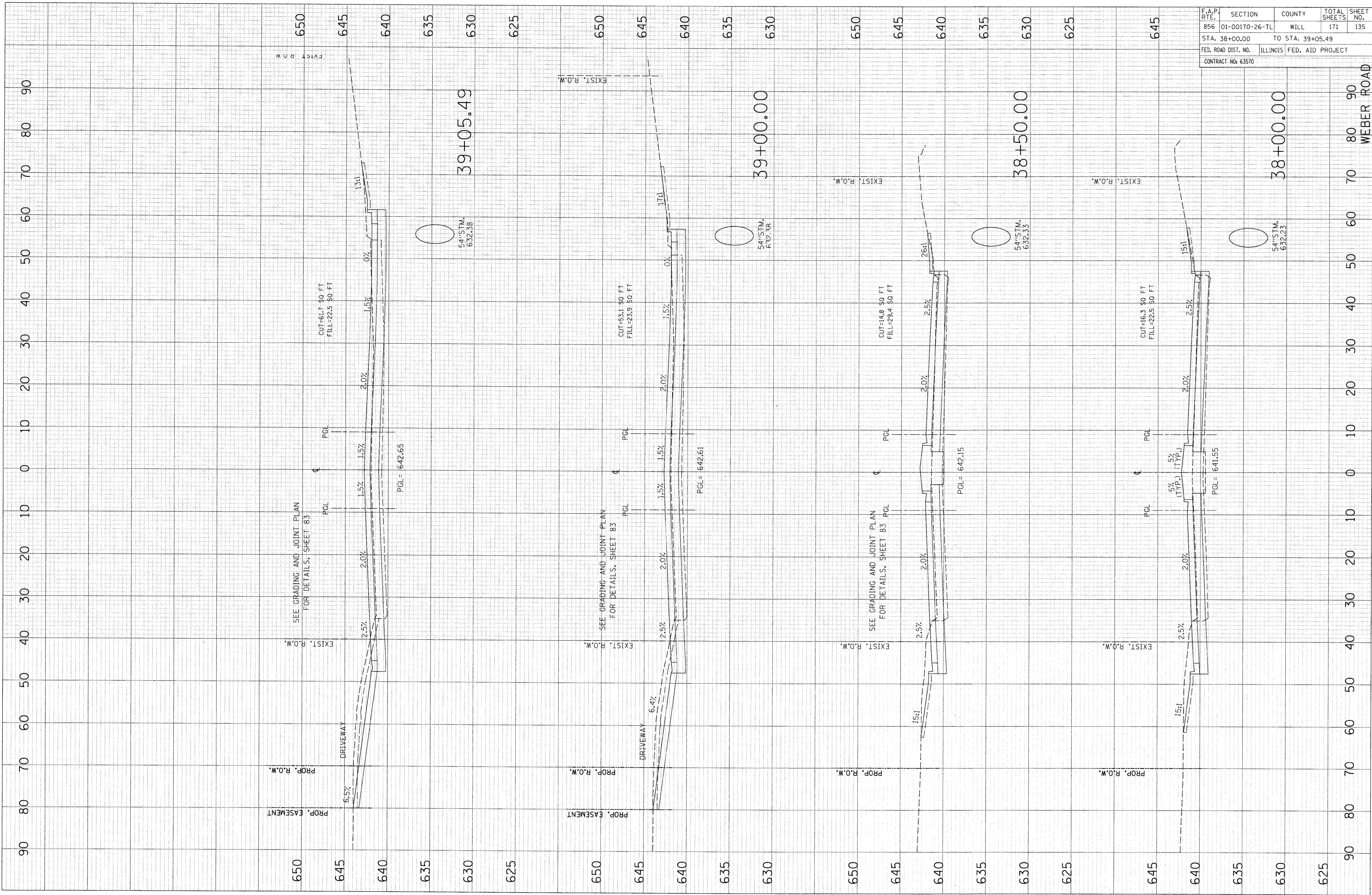
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	134
STA. 36+50.00		TO STA. 37+50.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

PLOT DATE = 8/22/2011
 FILE NAME = F:\79766\road\dwg\p131.rvt
 PLOT SCALE = 1/8" = 1' / in.
 USER NAME = p04162

ORIGINAL SURVEY
 SURVEY PLOTTED
 TEMPLATE AREAS CHECKED

FINAL SURVEY
 SURVEY PLOTTED
 TEMPLATE AREAS CHECKED

BY DATE

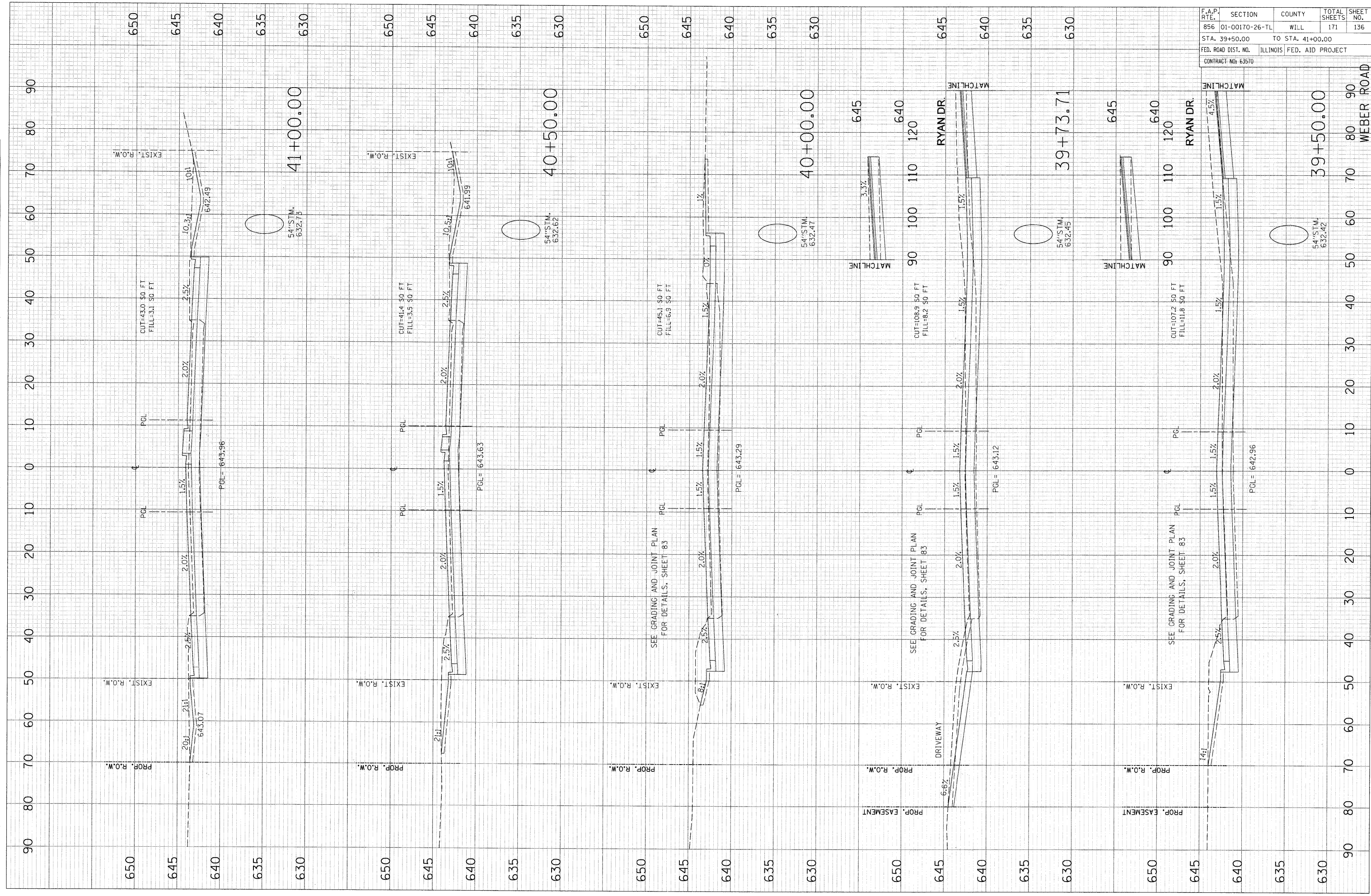


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	135
STA. 38+00.00		TO STA. 39+05.49		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

FINAL	SURVEYED	BY	DATE
NO.	NO.		
NOTE BOOK	TEMPLATE		
AREAS CHECKED	AREAS CHECKED		

ORIGINAL	SURVEYED	BY	DATE
NO.	NO.		
AREAS CHECKED	AREAS CHECKED		

PLOT DATE = 8/22/2011
 FILE NAME = P:\79766\road\dr-avg\avg131x-1111.dwg
 PLOT SCALE = 10.0000 / 1"
 USER NAME = pavelpz

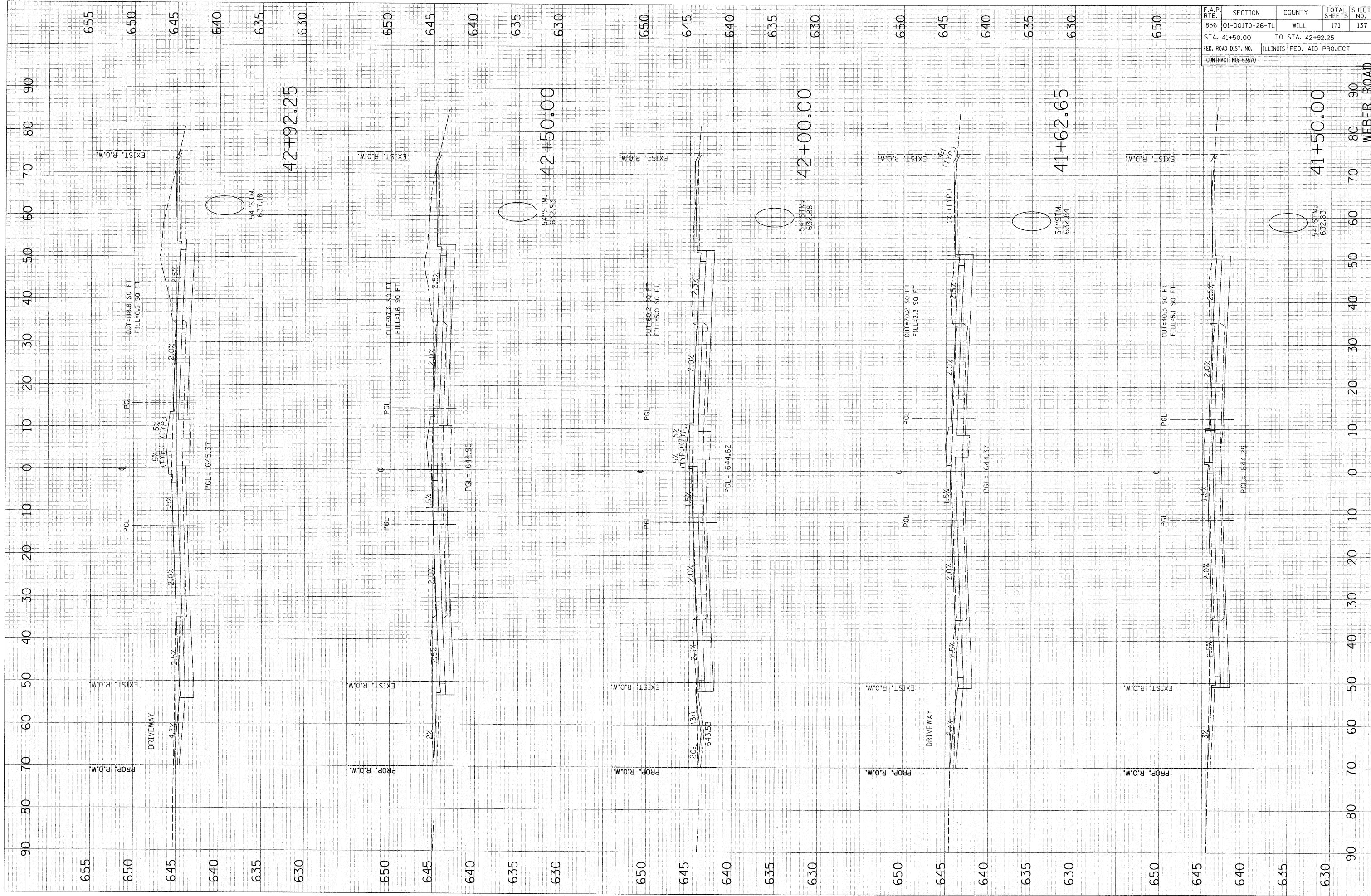


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	136
STA. 39+50.00		TO STA. 41+00.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

PLOT DATE = 8/22/2011
 FILE NAME = F:\79766\Cadd\Drawings\1314s_sht\1314s_sht.dwg
 PLOT SCALE = 10.000 / 1"
 USER NAME = p01012

ORIGINAL SURVEY PLOTTED AREAS CHECKED
 SURVEY PLOTTED AREAS CHECKED
 BY: DATE:

FINAL SURVEY NOTE BOOK NO.
 SURVEYED TEMPLATE AREAS CHECKED
 BY: DATE:



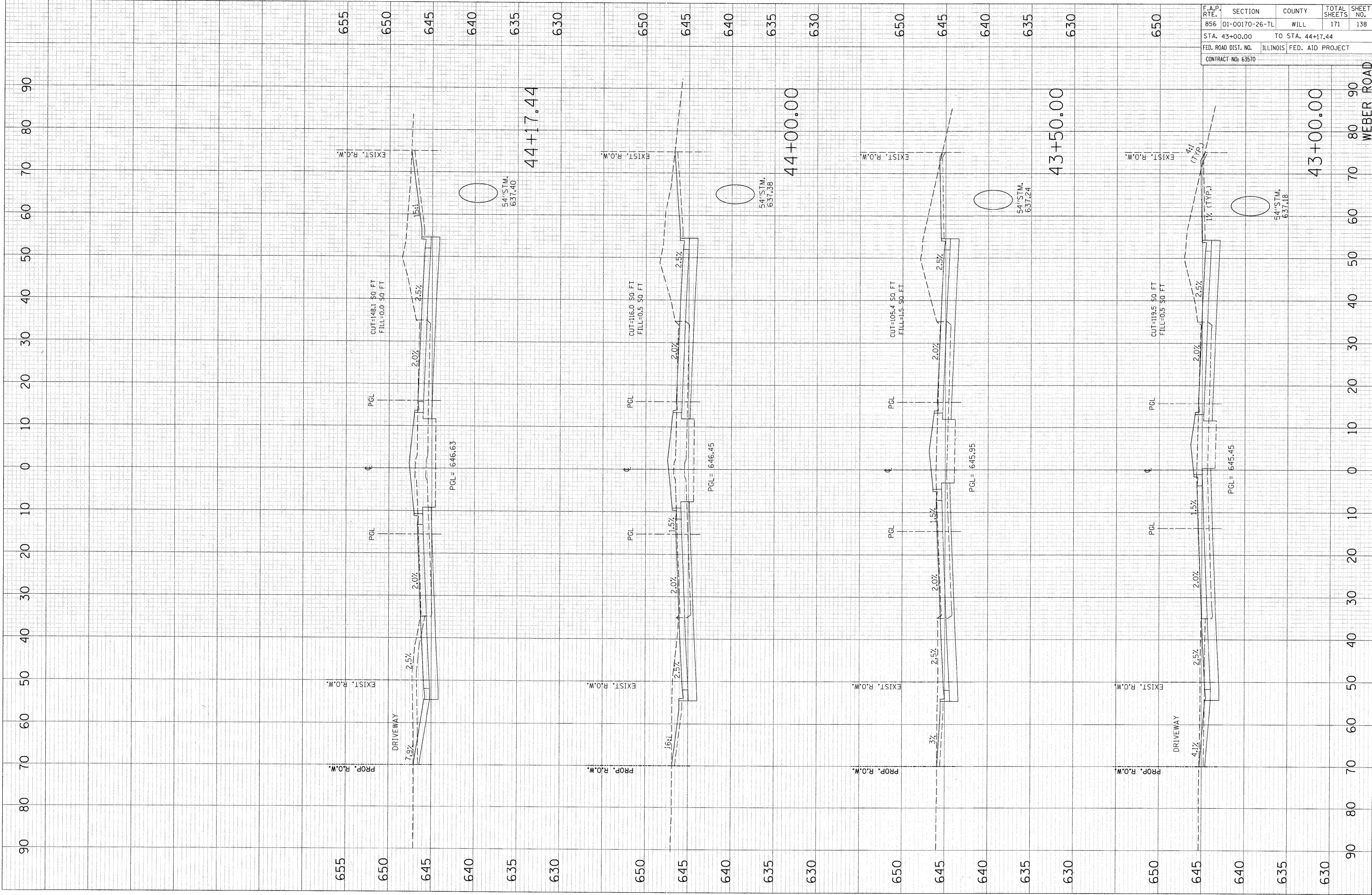
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	137
STA. 41+50.00		TO STA. 42+92.25		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

WEBER ROAD

PLOT DATE = 8/22/2011
 FILE NAME = F:\79766\road\dr-strings\131x-11.dwg
 PLOT SCALE = 10.0000 / in.
 USER NAME = pavelbz

ORIGINAL SURVEY PLOTTED AREAS CHECKED
 SURVEY PLOTTED AREAS CHECKED
 DATE

FINAL SURVEY NOTE BOOK NO.
 SURVEYED TEMPLATE AREAS CHECKED
 BY DATE



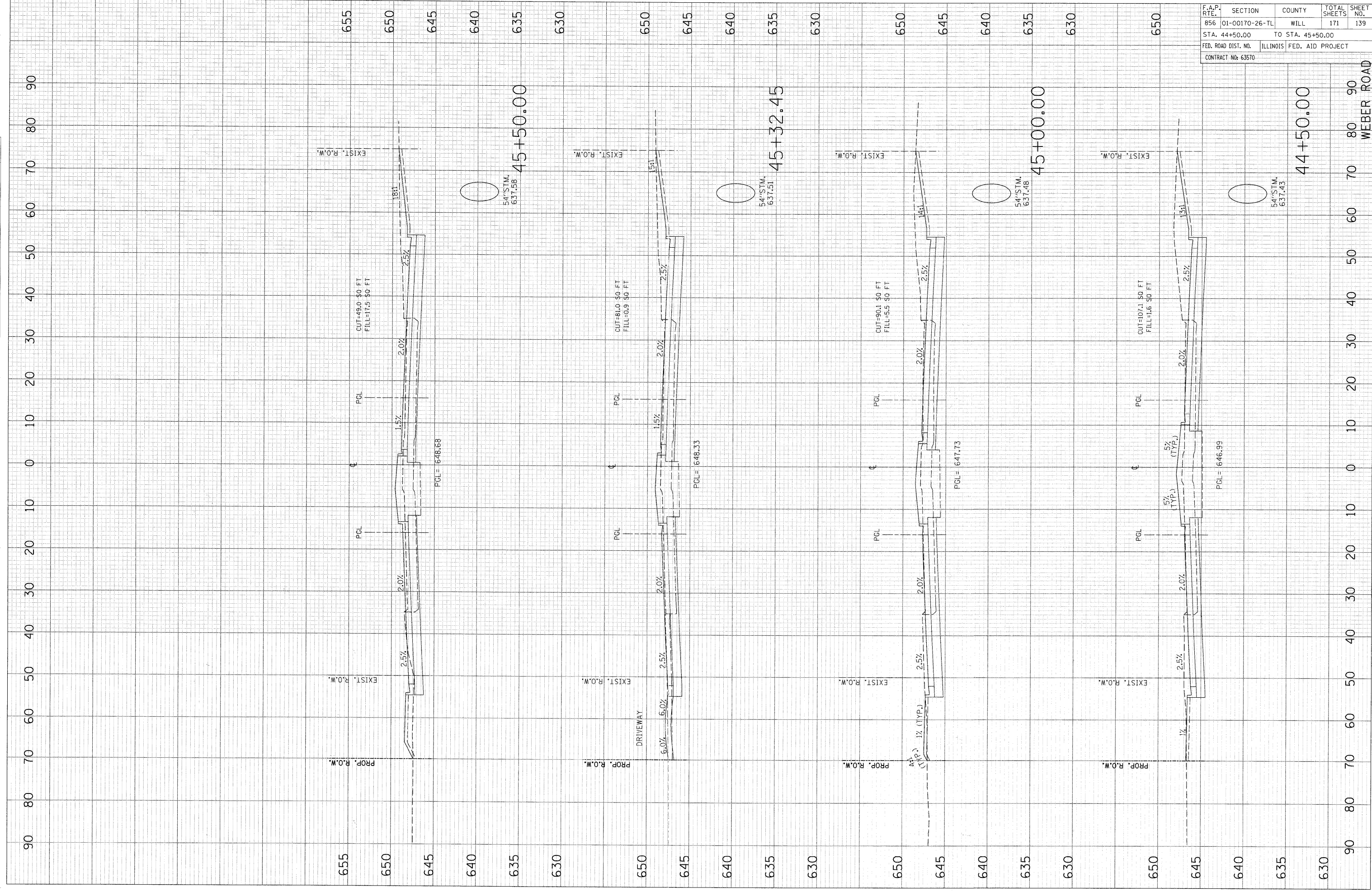
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	138
STA. 43+00.00 TO STA. 44+17.44				
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

FINAL SURVEY BY DATE
 SURVEYED BY DATE
 NOTE BOOK NO.
 TEMPLATE AREAS CHECKED
 AREAS CHECKED

ORIGINAL SURVEY BY DATE
 SURVEYED BY DATE
 PLOTTED TEMPLATE AREAS CHECKED
 AREAS CHECKED

PLOT DATE = 8/22/2011
 FILE NAME = P:\79766\cadd\p\01-00170-26-TL.dwg
 PLOT SCALE = 10.0000' / 1" = 10000
 USER NAME = pavelp2

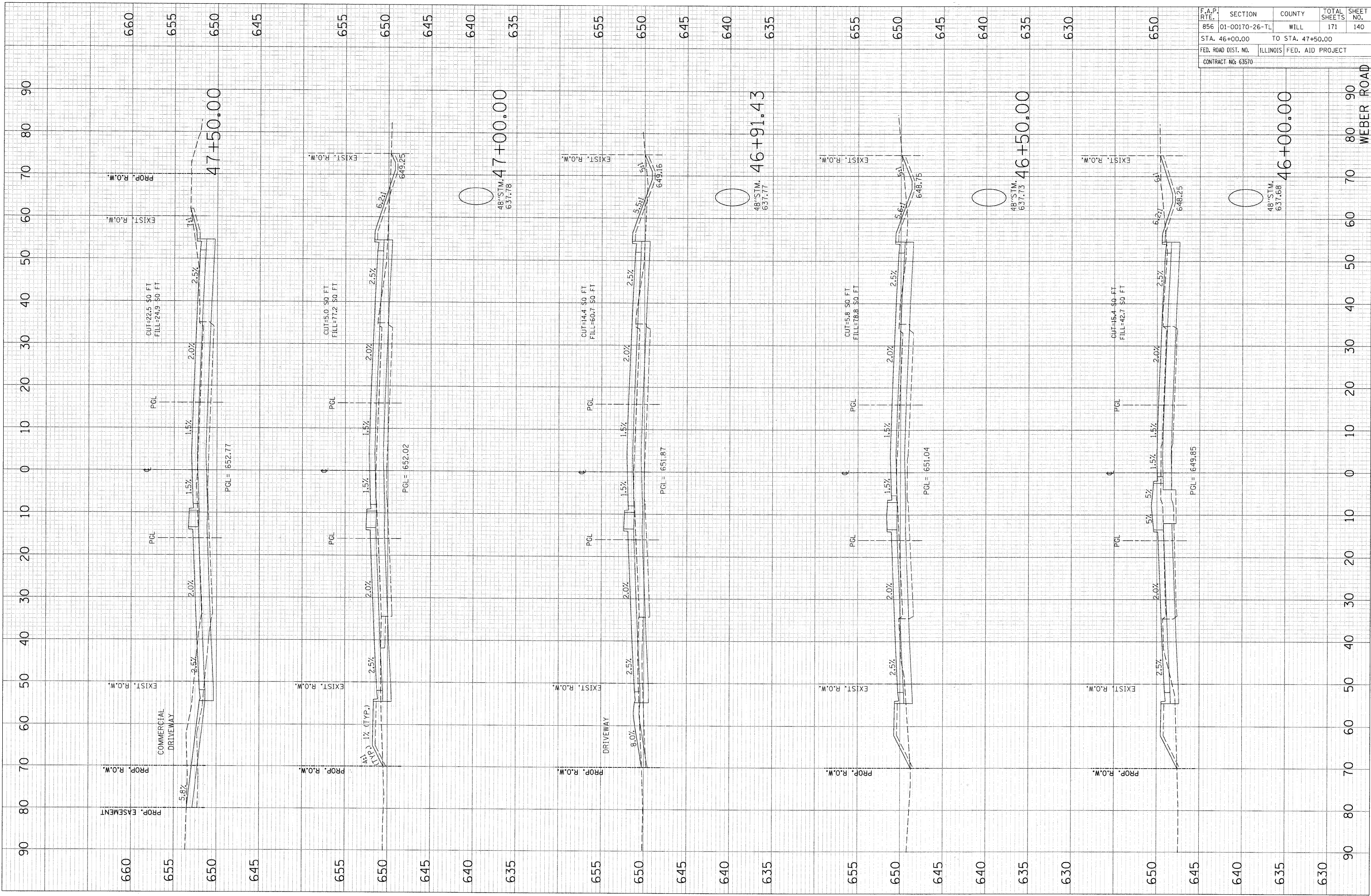
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	139
STA. 44+50.00		TO STA. 45+50.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				



PLOT DATE = 8/22/2011
 FILE NAME = P:\79766\cadd\Drawings\131x-sht\weber.dwg
 PLOT SCALE = 10.000' / 1"
 USER NAME = pwalzjz

ORIGINAL SURVEY
 SURVEY PLOTTED
 TEMPLATE AREAS CHECKED

FINAL SURVEY
 SURVEY PLOTTED
 NOTE BOOK AREAS CHECKED



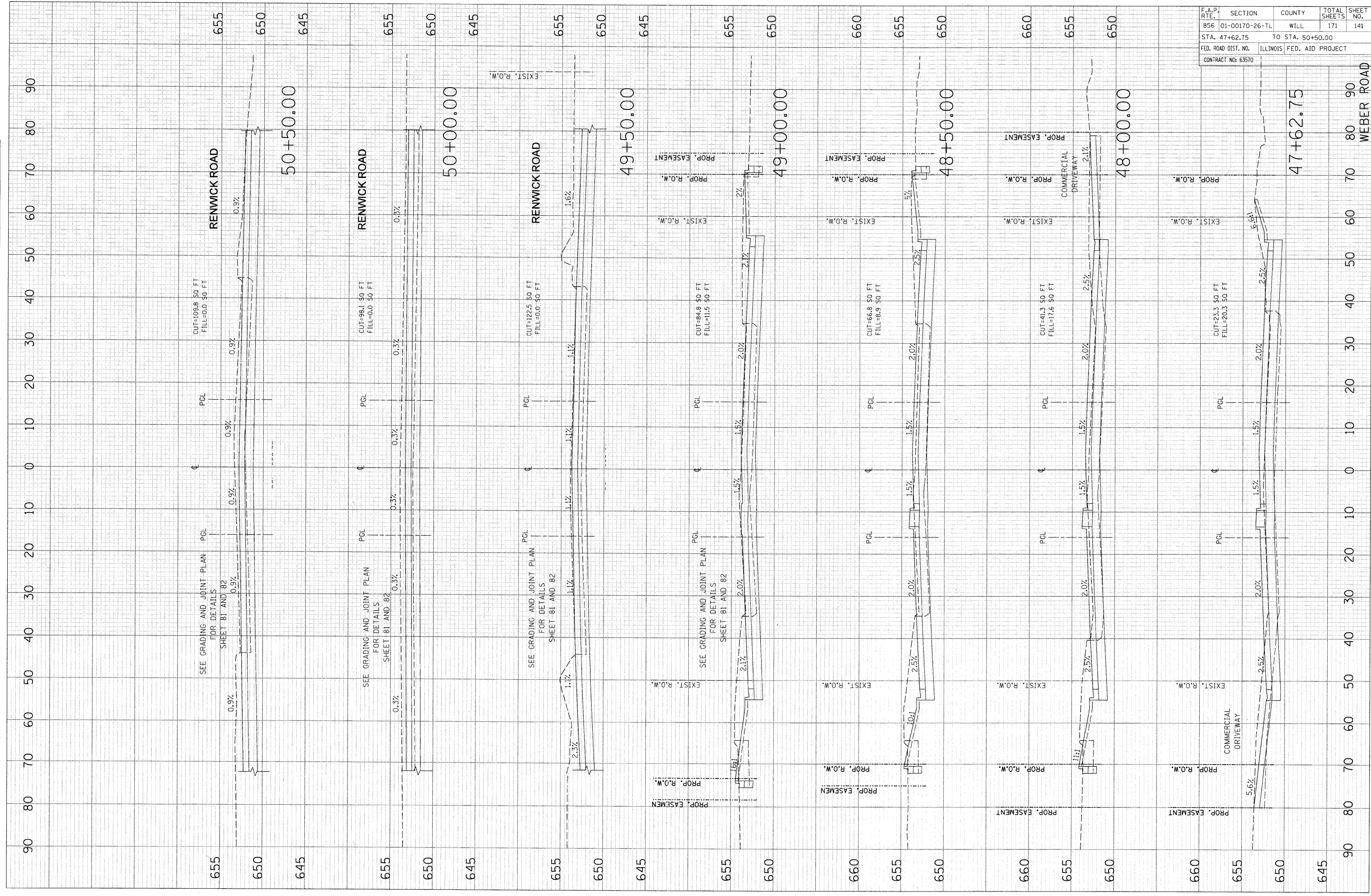
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	140
STA. 46+00.00		TO STA. 47+50.00		
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			
CONTRACT NO: 63570				

PLOT DATE = 8/22/2011
 FILE NAME = P:\75766\cadd\dr-wr-818\13\13x-818.dwg
 PLOT SCALE = 10.0000 / 1"
 USER NAME = p046162

ORIGINAL SURVEY
 SURVEY PLOTTED
 TEMPLATE AREAS CHECKED
 NO.

FINAL SURVEY
 SURVEY PLOTTED
 TEMPLATE AREAS CHECKED
 NO.

BY DATE



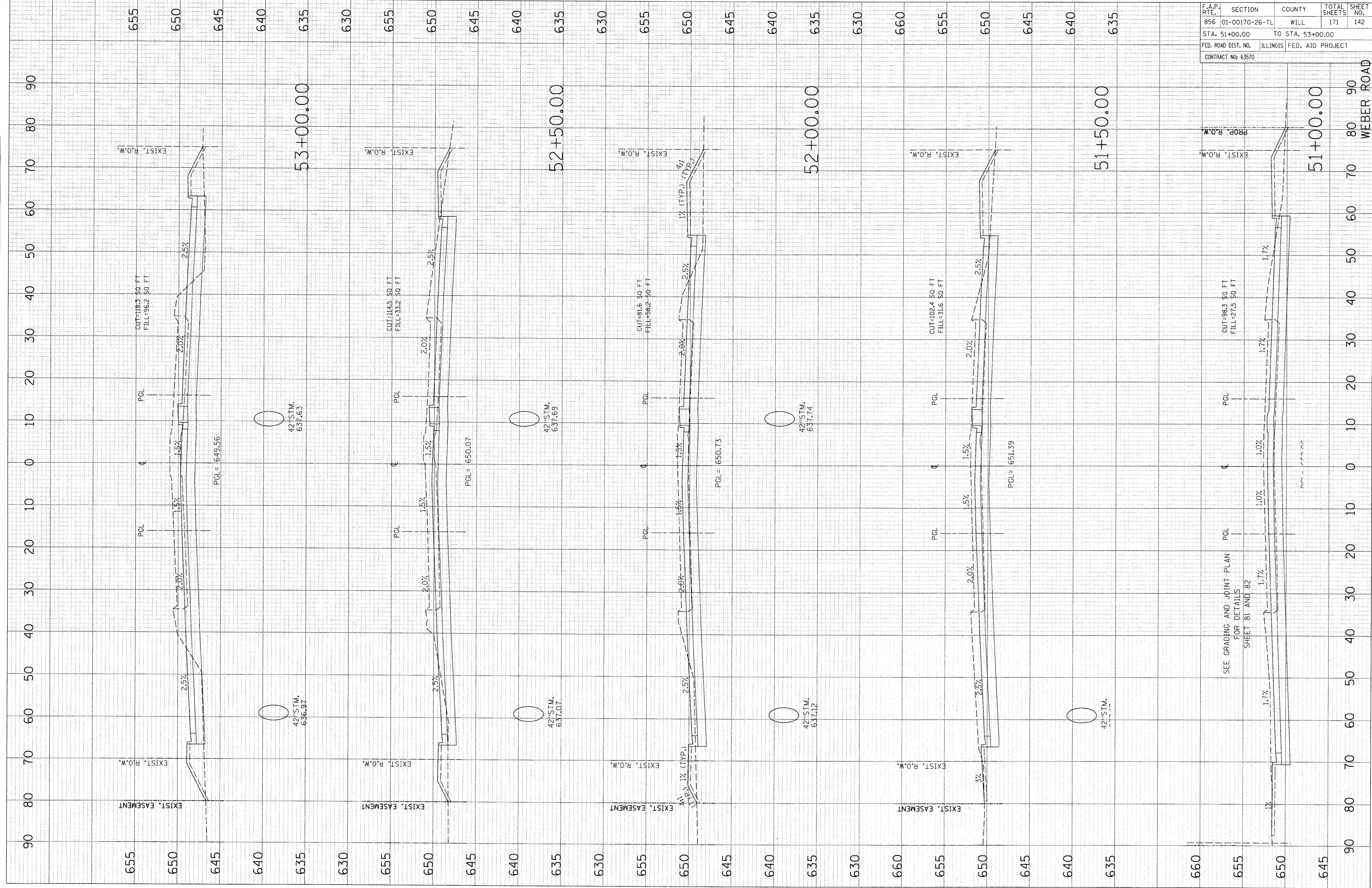
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	141
STA. 47+62.75		TO STA. 50+50.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

FINAL SURVEY	BY	DATE
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NOTED BODY		
AREAS CHECKED		
NO.		

ORIGINAL SURVEY	BY	DATE
STARTED		
PLOTTED		
TEMPLATE		
AREAS CHECKED		
NO.		

PLOT DATE = 8/22/2011
 FILE NAME = F:\79766\cad\1\dr-wrgrg\131x+1.shp
 PLOT SCALE = 18.0000 / 1"
 USER NAME = pvaibz

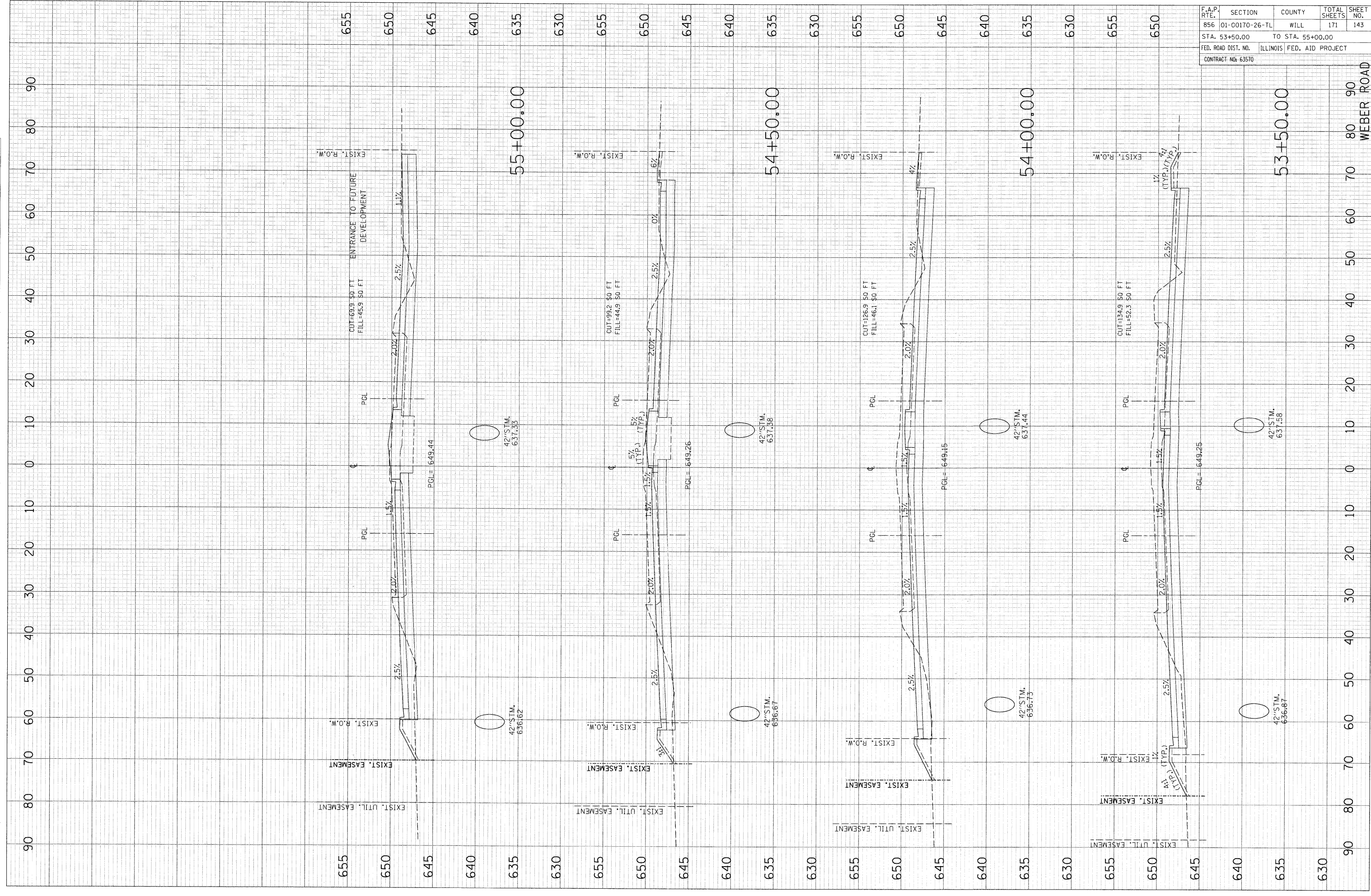
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	142
STA. 51+00.00		TO STA. 53+00.00		
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			
CONTRACT NO: 63570				



PLOT DATE = 8/22/2011
 FILE NAME = F:\79766\cadd\drwg\131\rs_ah\131\131.dwg
 PLOT SCALE = 10.0000' / in.
 USER NAME = patelp2

ORIGINAL SURVEY PLOTTED
 SURVEY PLOTTED
 TEMPLATE AREAS CHECKED

FINAL SURVEY NOTE BOOK NO.
 SURVEY SCHEMATIC AREAS CHECKED
 BY DATE



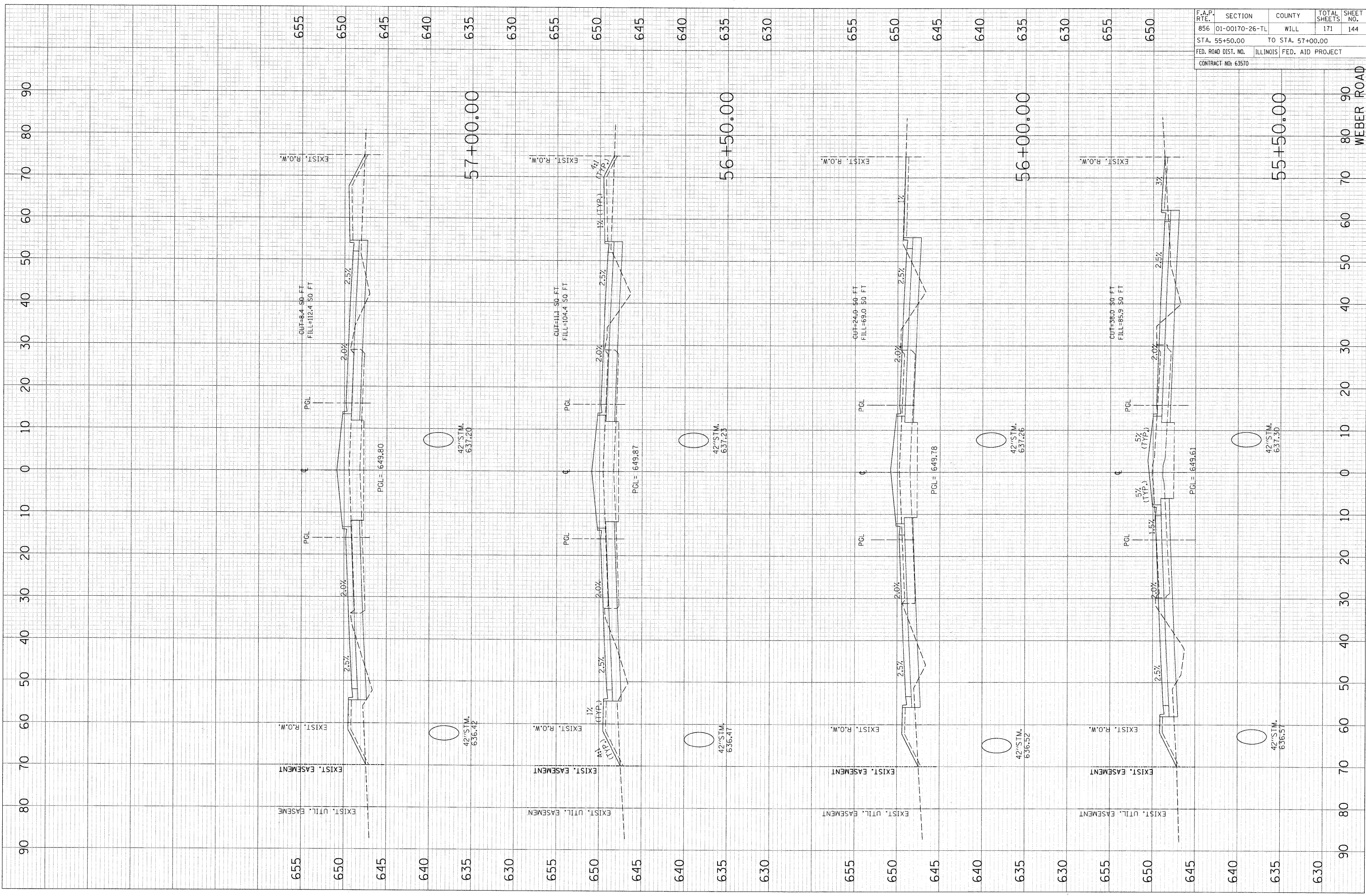
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	143
STA. 53+50.00		TO STA. 55+00.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

WEBER ROAD

PLOT DATE = 8/22/2011
 FILE NAME = F:\79766\Acad\Drawings\1311s.dwg
 PLOT SCALE = 10.0000 / 1"
 USER NAME = p0101jz

ORIGINAL SURVEY PLOTTED
 SURVEY PLOTTED TEMPLATE AREAS CHECKED

FINAL SURVEY NOTE BOOK NO.
 SURVEYED TEMPLATE AREAS CHECKED
 BY DATE



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	144
STA. 55+50.00 TO STA. 57+00.00				
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

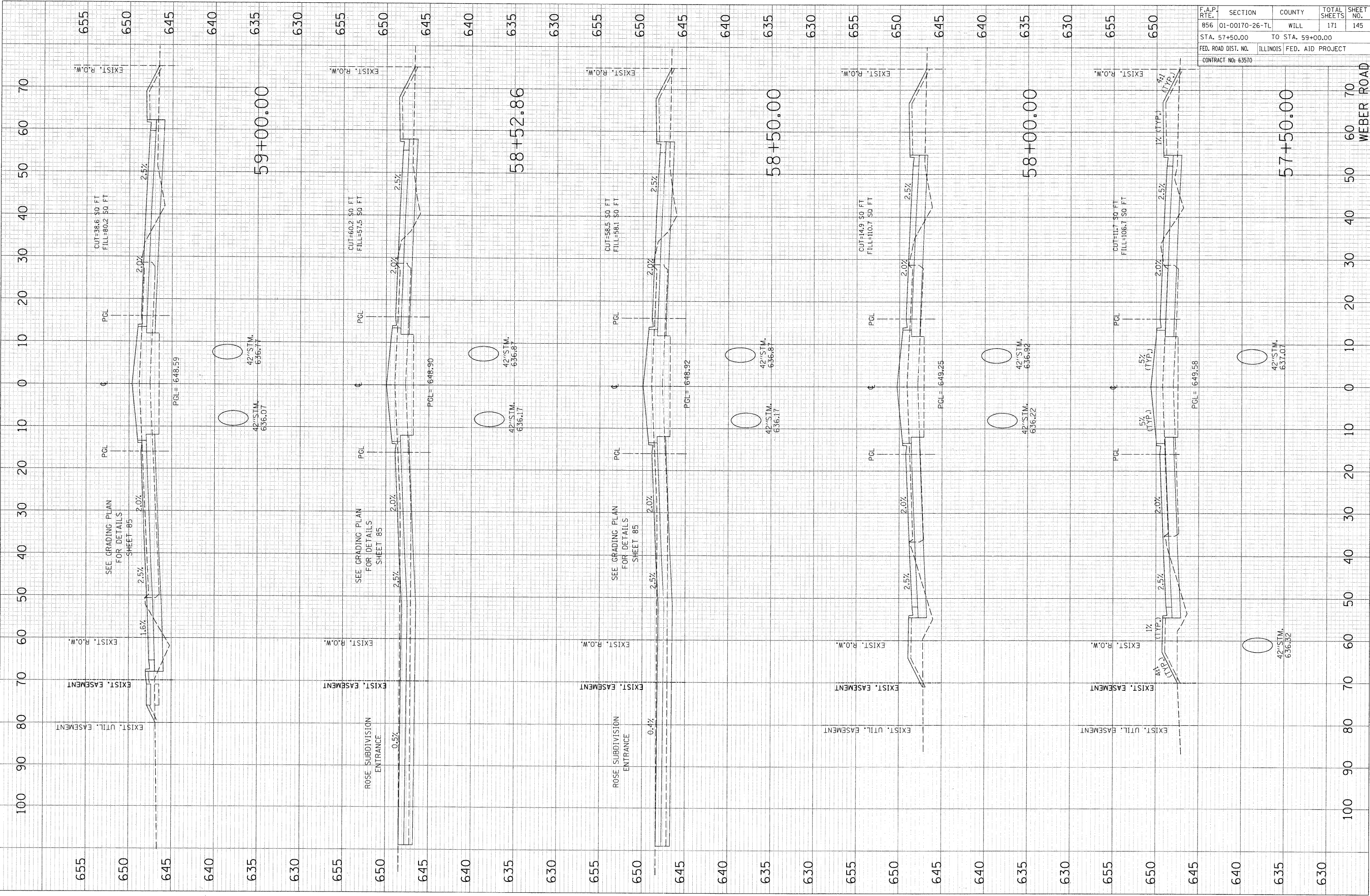
WEBER ROAD

FINAL SURVEY SHEET
 NO. _____
 DATE _____

ORIGINAL SURVEY SHEET
 NO. _____
 DATE _____

PLOT DATE = 8/22/2011
 FILE NAME = P:\79765\cadd\drwg\13\rs.sh\13\13.dwg
 PLOT SCALE = 10.000' / in.
 USER NAME = ppatel2

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	145
STA. 57+50.00		TO STA. 59+00.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				



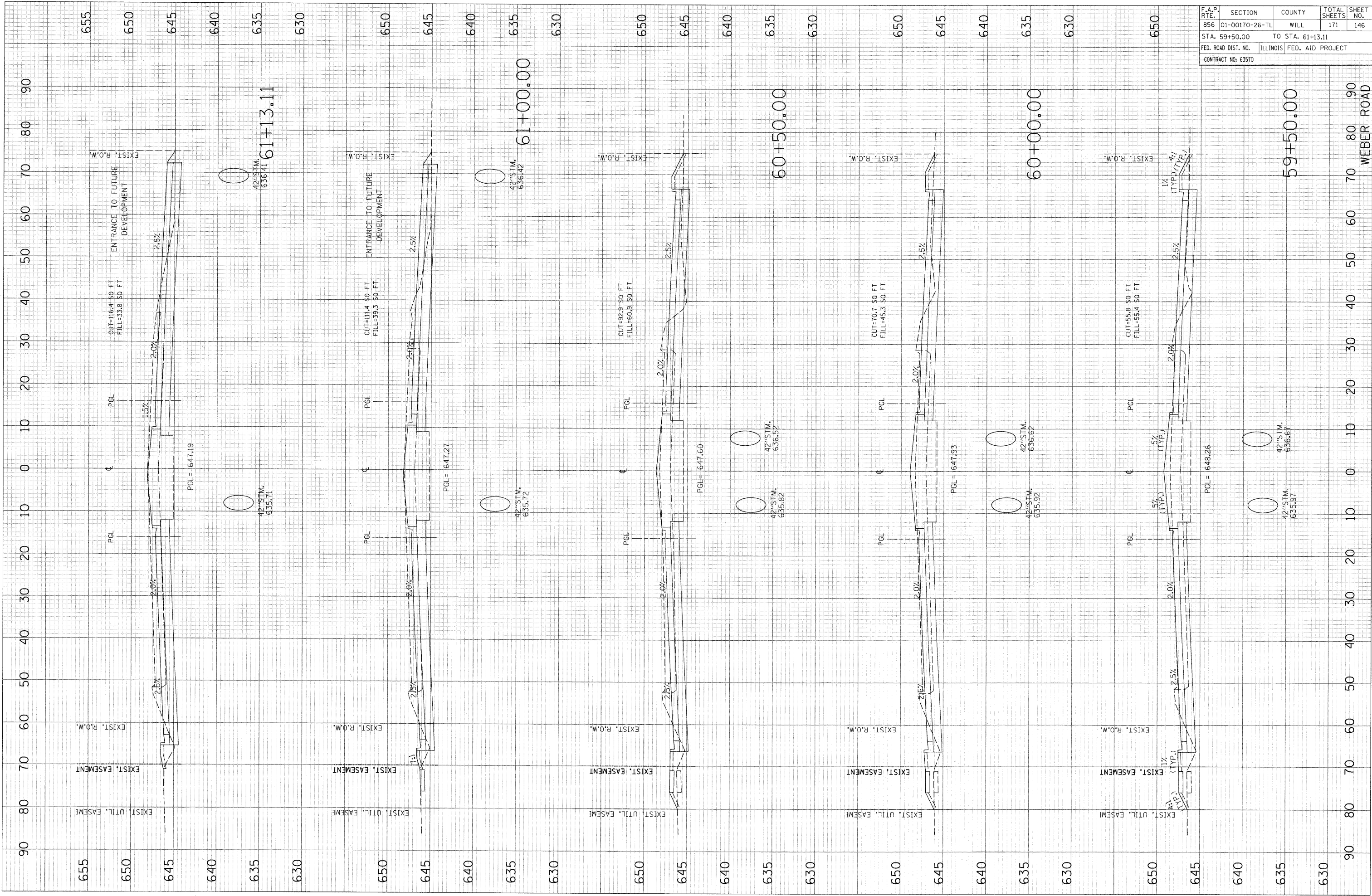
WEBER ROAD

PLOT DATE = 8/22/2011
 FILE NAME = P:\79766\cadd\dr-avrg\131x-8-ft\weber.dwg
 PLOT SCALE = 10.000 / 1"
 USER NAME = pavelpz

ORIGINAL SURVEY
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FINAL SURVEY
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	146
STA. 59+50.00		TO STA. 61+13.11		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

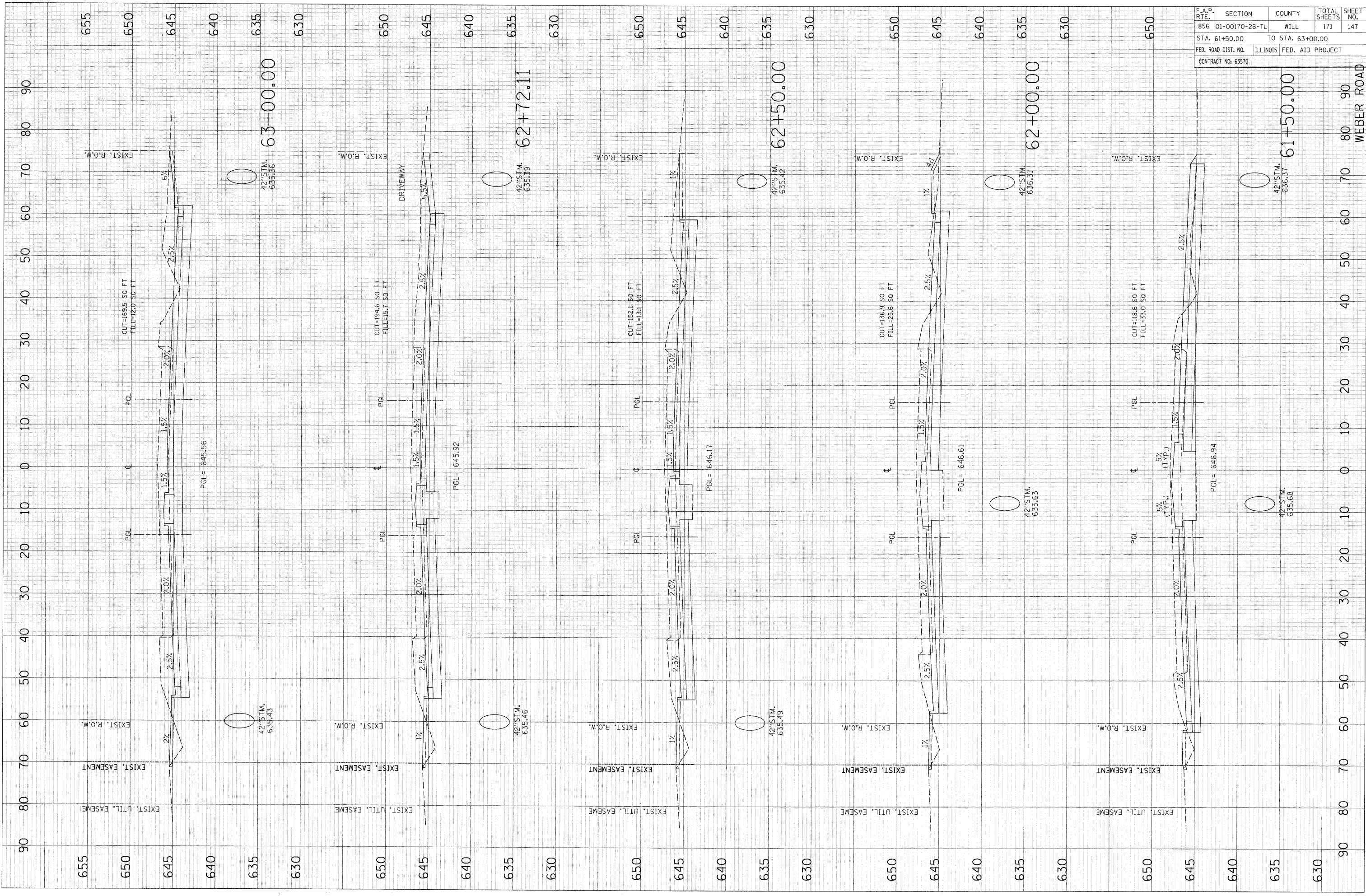


90
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90
WEBER ROAD

PLOT DATE = 8/22/2011
 FILE NAME = P:\79766\add\Drawings\131x-11\131x-11.dwg
 PLOT SCALE = 10/2000 / in.
 USER NAME = pvc162

ORIGINAL SURVEY PLOTTED
 SURVEY PLOTTED
 TEMPLATE AREAS CHECKED

FINAL SURVEY PLOTTED
 SURVEY PLOTTED
 TEMPLATE AREAS CHECKED



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	147
STA. 61+50.00		TO STA. 63+00.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

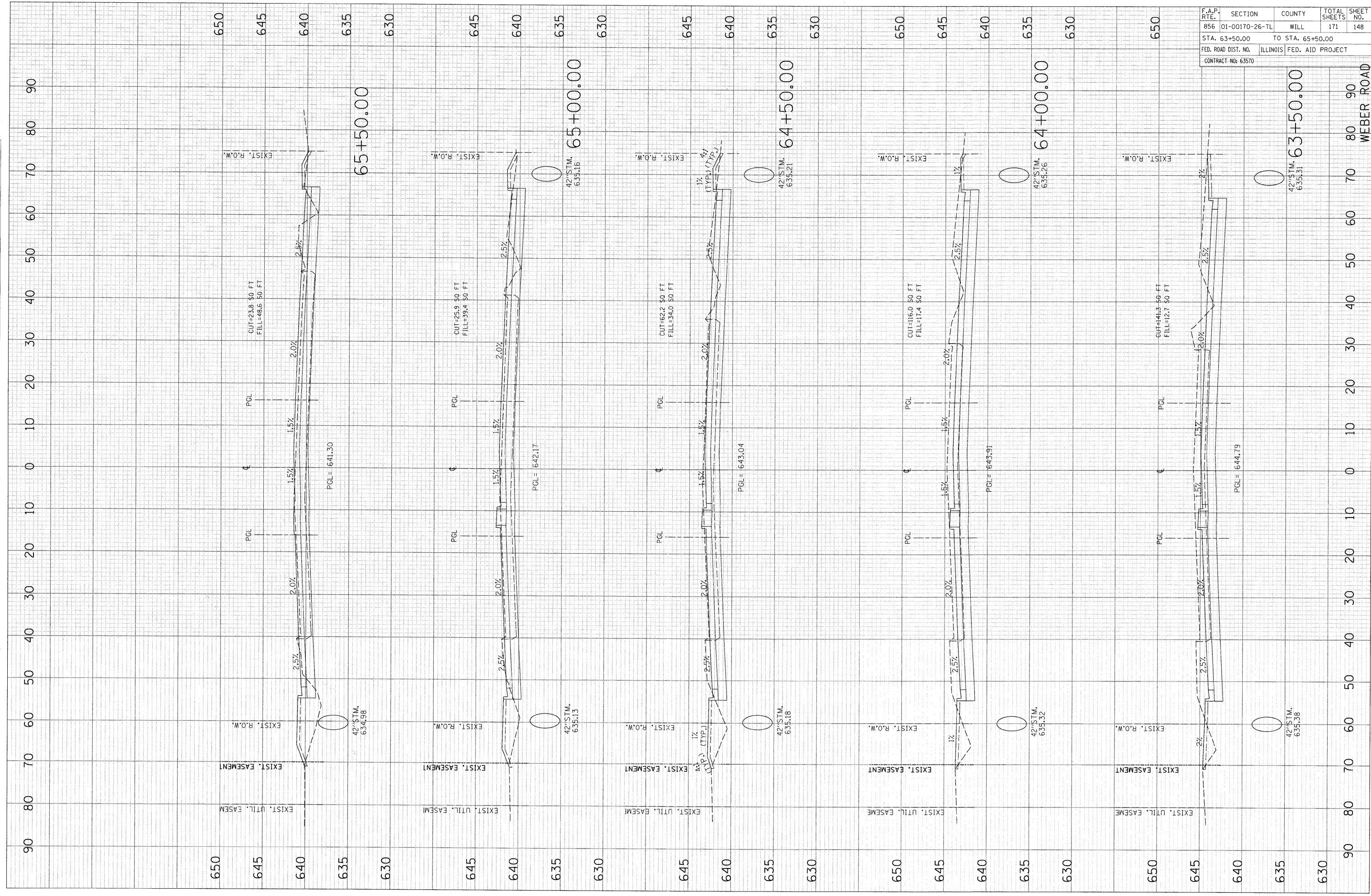
WEBER ROAD

FINAL SURVEY SURVEYED BY DATE
 NOTE BOOK PLOTTED TEMPLATE AREAS CHECKED

ORIGINAL SURVEY SURVEYED BY DATE
 PLOTTED TEMPLATE AREAS CHECKED

PLOT DATE = 8/22/2011
 FILE NAME = P:\79766\cadd\dr-strings\131x-51x-51x\weber.dwg
 PLOT SCALE = 10.000' / 1"
 USER NAME = bsejlpz

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	148
STA. 63+50.00		TO STA. 65+50.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

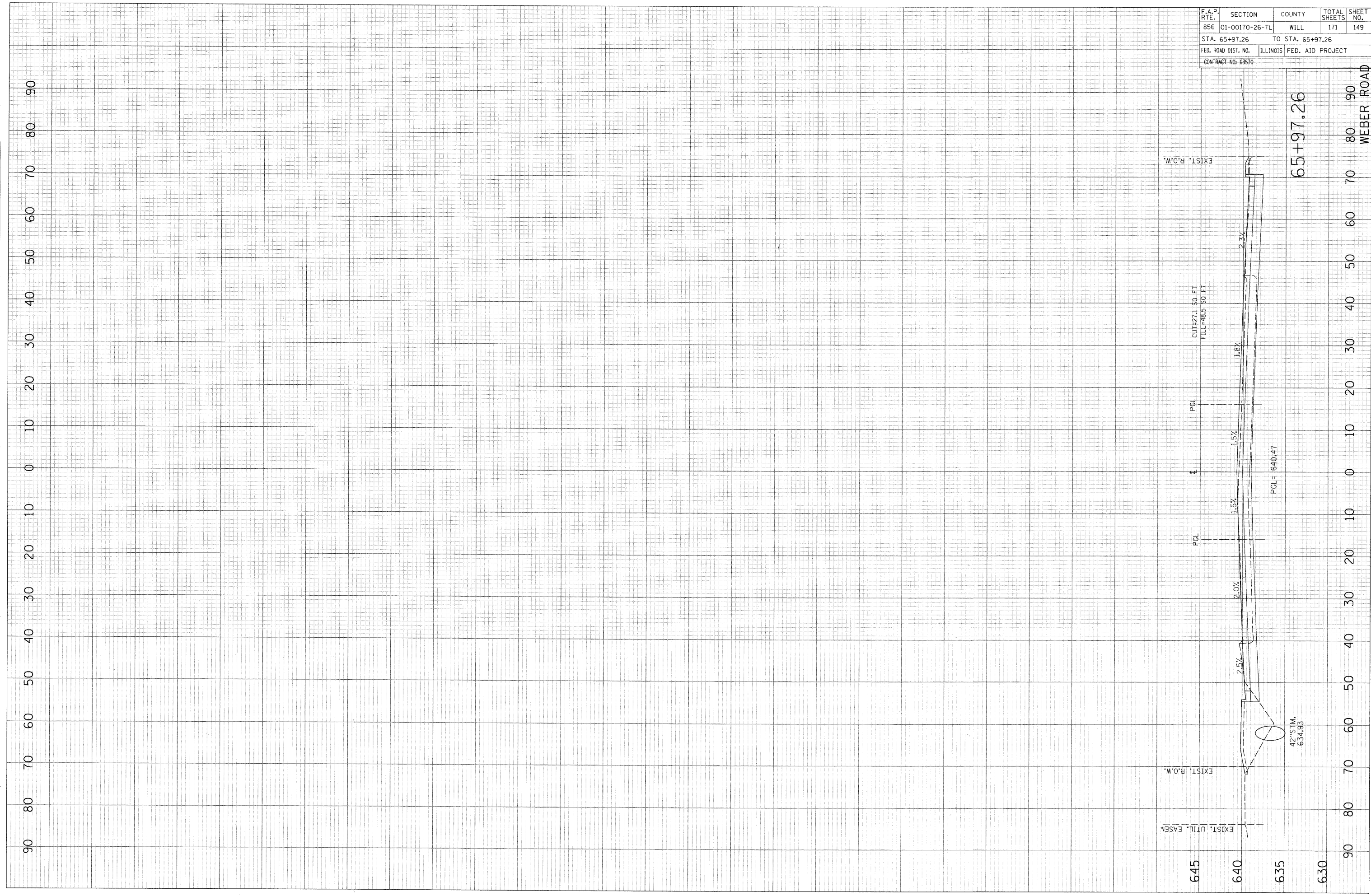


WEBER ROAD

PLOT DATE = 8/22/2011
 FILE NAME = P:\79766\Cadd\Drawings\1314.sht
 PLOT SCALE = 10.0000' / in.
 USER NAME = ppatriz

ORIGINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
TEMPLATE		
AREAS		
AREAS CHECKED		

FINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
TEMPLATE		
AREAS		
AREAS CHECKED		



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	149
STA. 65+97.26		TO STA. 65+97.26		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

65+97.26

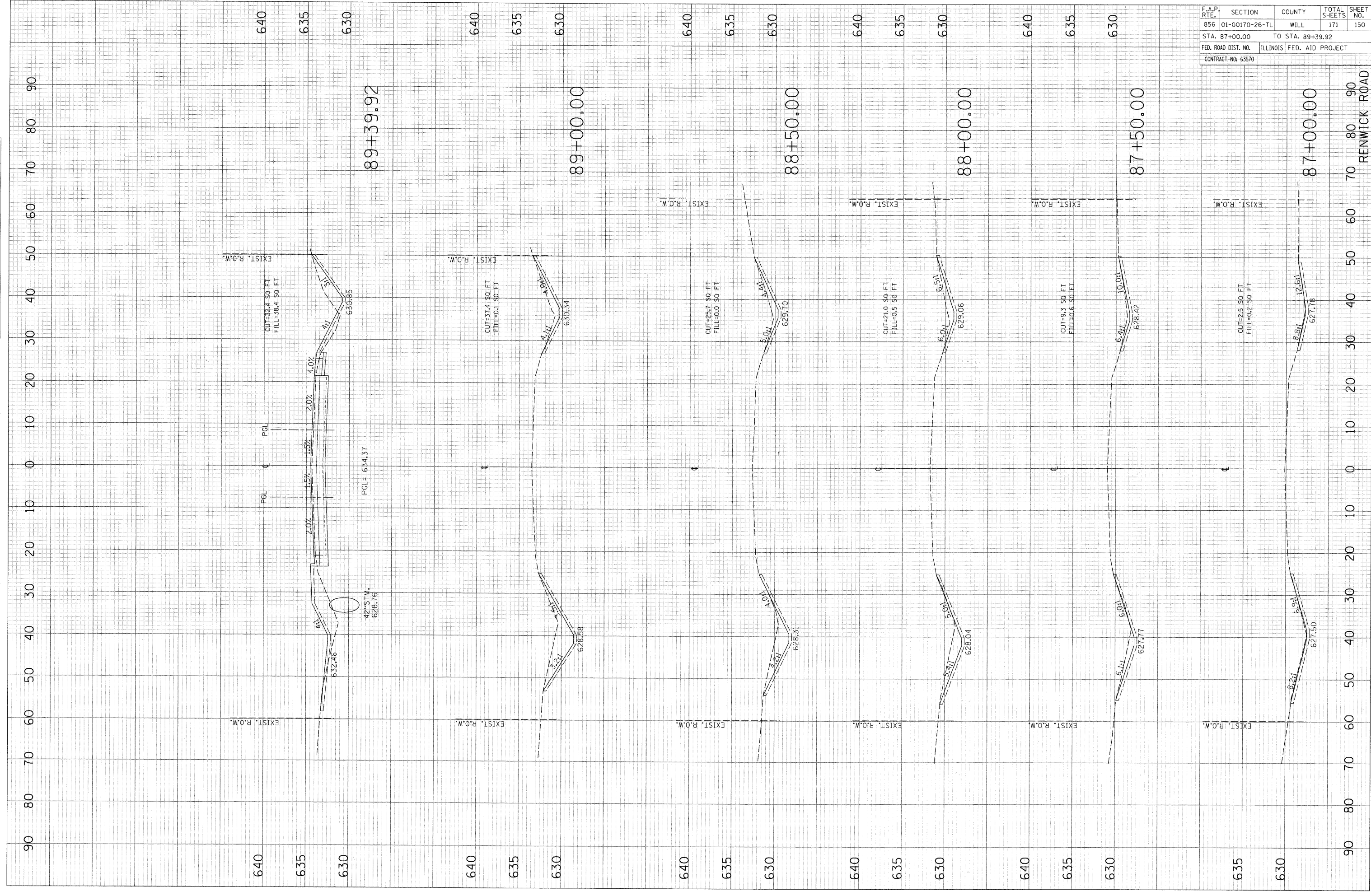
WEBER ROAD

PLOT DATE = 8/22/2011
 FILE NAME = P:\179165\cadd\drwg\89+39.92\150s.shp
 PLOT SCALE = 10.0000 / in.
 USER NAME = ppatilp2

ORIGINAL SURVEY PLOTTED
 SURVEY PLOTTED
 TEMPLATE AREAS CHECKED

FINAL SURVEY PLOTTED
 SURVEY PLOTTED
 TEMPLATE AREAS CHECKED

BY _____ DATE _____



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	150
STA. 87+00.00		TO STA. 89+39.92		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

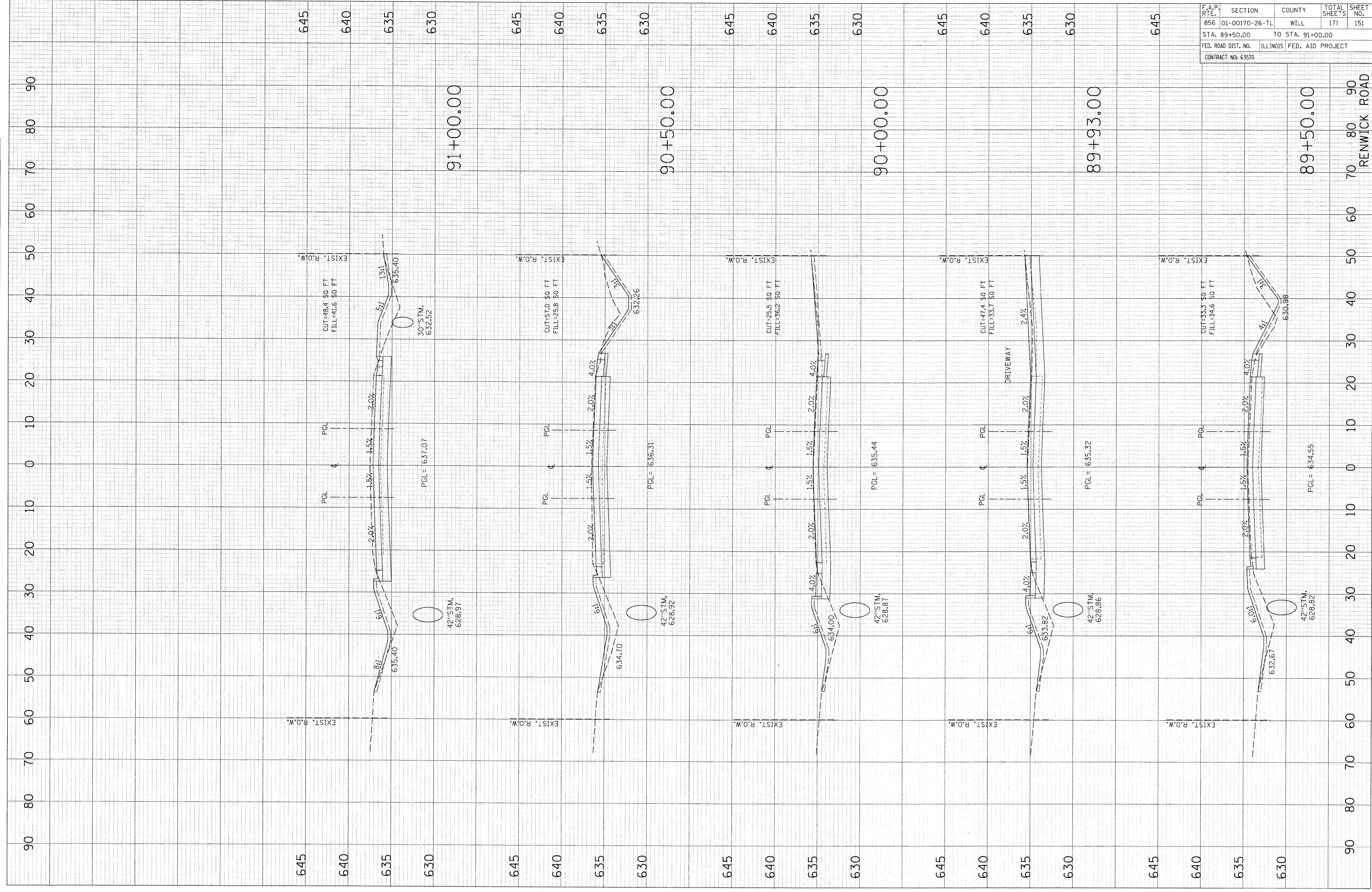
RENWICK ROAD

PLOT DATE = 8/22/2011
 FILE NAME = P:\179765\cadd\drawings\159s.sh\159s.dwg
 PLOT SCALE = 10.0000 / in.
 USER NAME = petalp2

ORIGINAL SURVEY PLOTTED
 SURVEY PLOTTED
 TEMPLATE AREAS CHECKED

FINAL SURVEY NOTE BOOK NO.
 SURVEYED TEMPERATURAS AREAS CHECKED

BY DATE



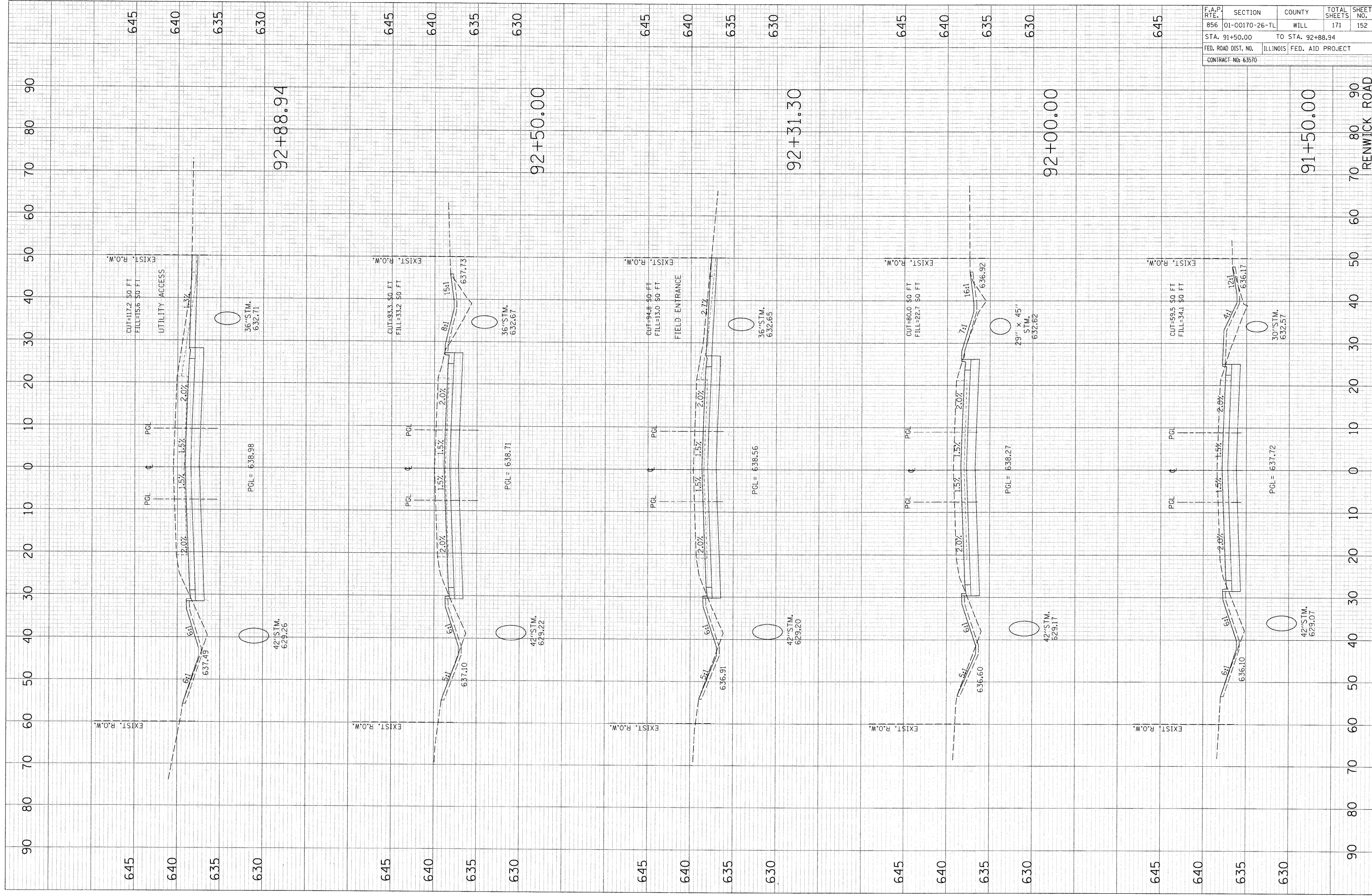
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	151
STA. 89+50.00		TO STA. 91+00.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

RENWICK ROAD

PLOT DATE = 8/22/2011
 FILE NAME = F:\79766\aved\dr-avrgs\188-s.dwg
 PLOT SCALE = 10.0000 / in.
 USER NAME = pavelic

ORIGINAL SURVEY	BY	DATE
SURVEY PLOTTED		
AREAS CHECKED		
AREAS CHECKED		

FINAL SURVEY	BY	DATE
SURVEY TEMPLATE		
AREAS CHECKED		
AREAS CHECKED		



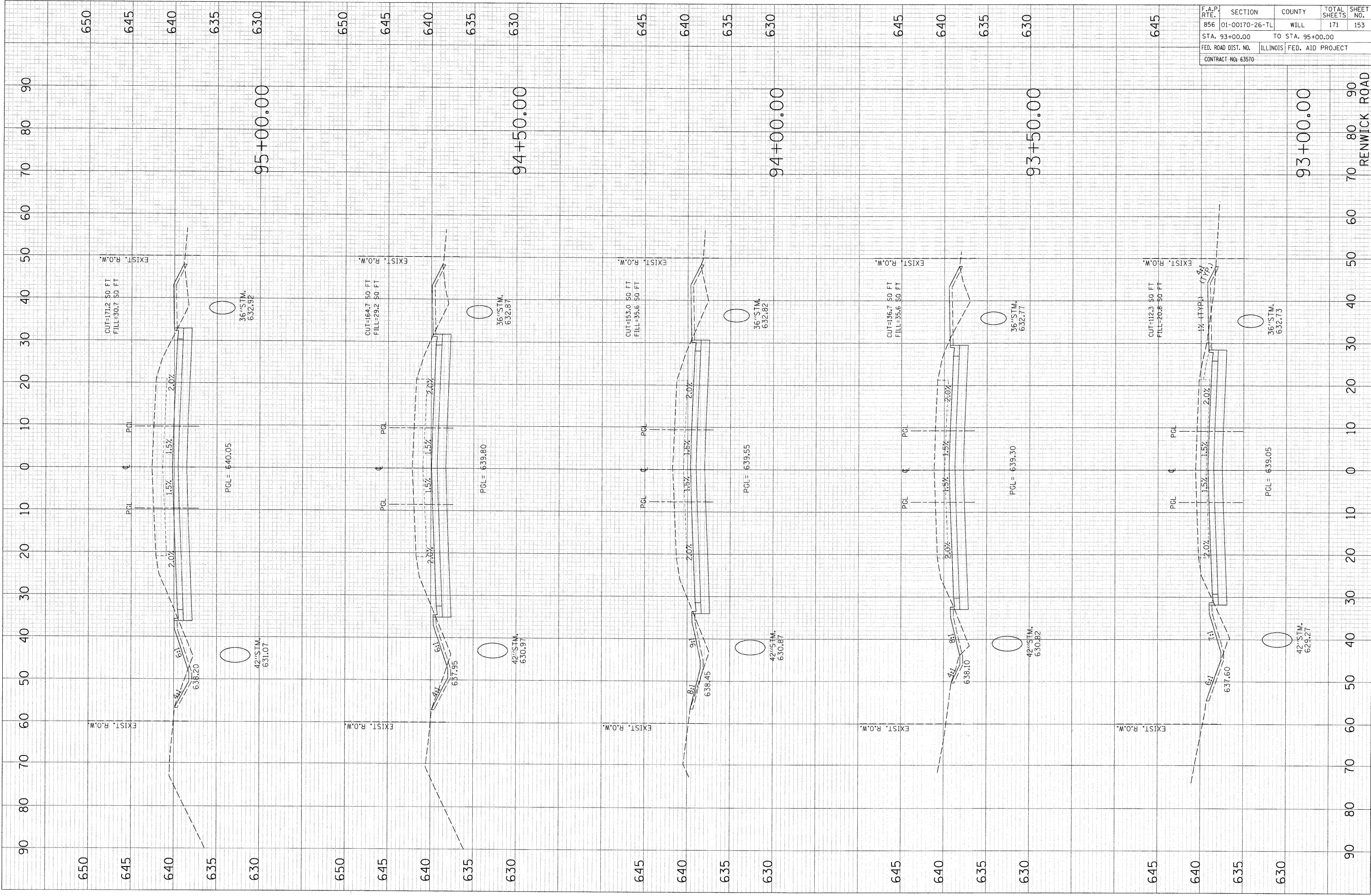
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	152
STA. 91+50.00		TO STA. 92+88.94		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

PLOT DATE = 8/22/2011
 FILE NAME = P:\79766\cadd\p\swing\180.s.dwg
 PLOT SCALE = 10.000 / 1"
 USER NAME = boraibz

ORIGINAL	BY	DATE
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PLOTTED		
TEMPLATE		
AREAS		
CHECKED		

FINAL	BY	DATE
SURVEY		
PLOTTED		
TEMPLATE		
AREAS		
CHECKED		

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	153
STA. 93+00.00		TO STA. 95+00.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				



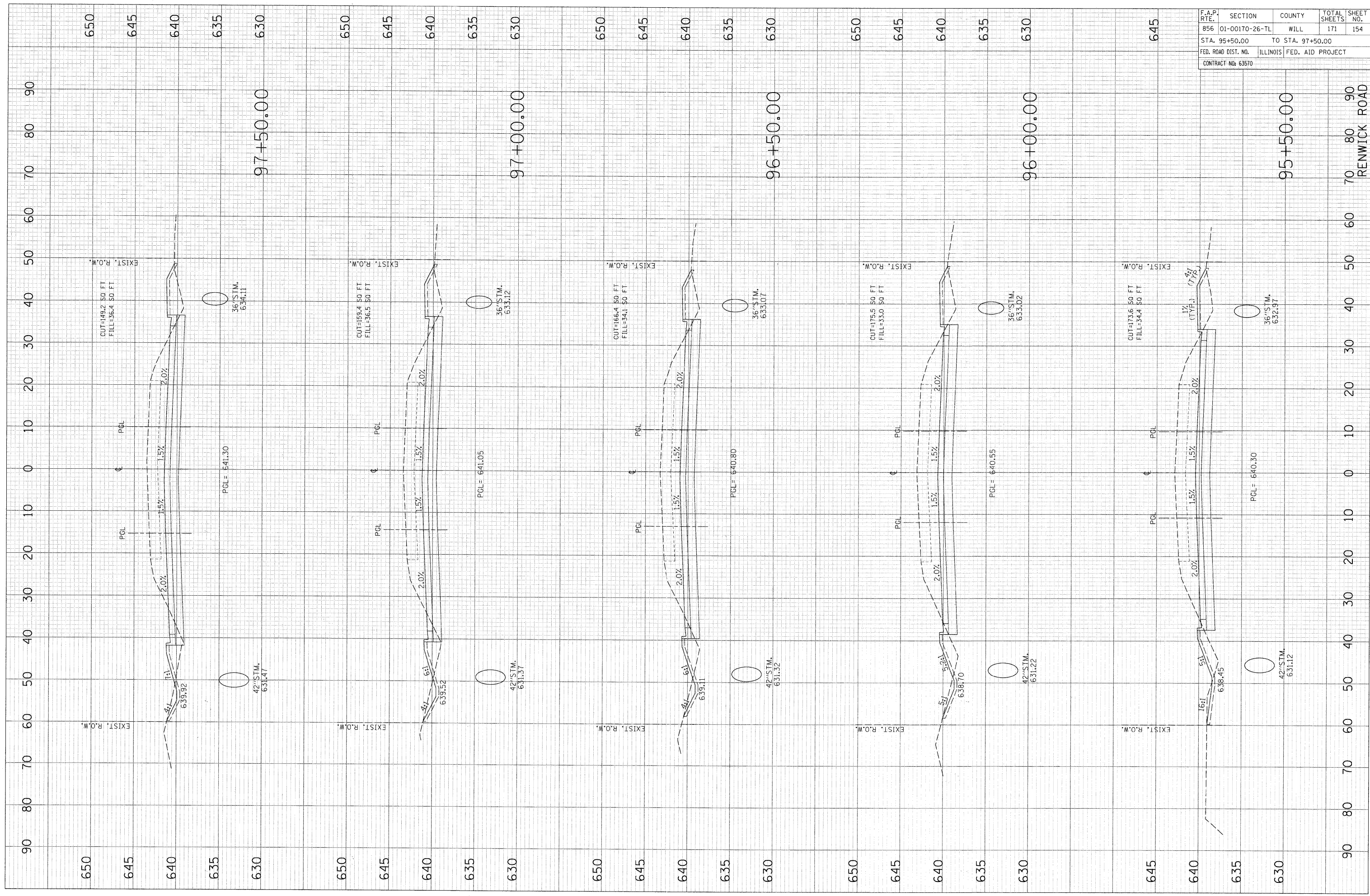
90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90
 RENWICK ROAD

PLOT DATE = 8/22/2011
 FILE NAME = P:\79766\cadd\Drawings\150x...
 PLOT SCALE = 10.000' / 1"
 USER NAME = pavelbz

ORIGINAL SURVEY PLOTTED
 SURVEYED SURVEY PLOTTED
 TEMPLATE AREAS CHECKED
 NO. AREAS CHECKED

FINAL SURVEY PLOTTED
 SURVEYED SURVEY PLOTTED
 TEMPLATE AREAS CHECKED
 NO. AREAS CHECKED

BY _____ DATE _____



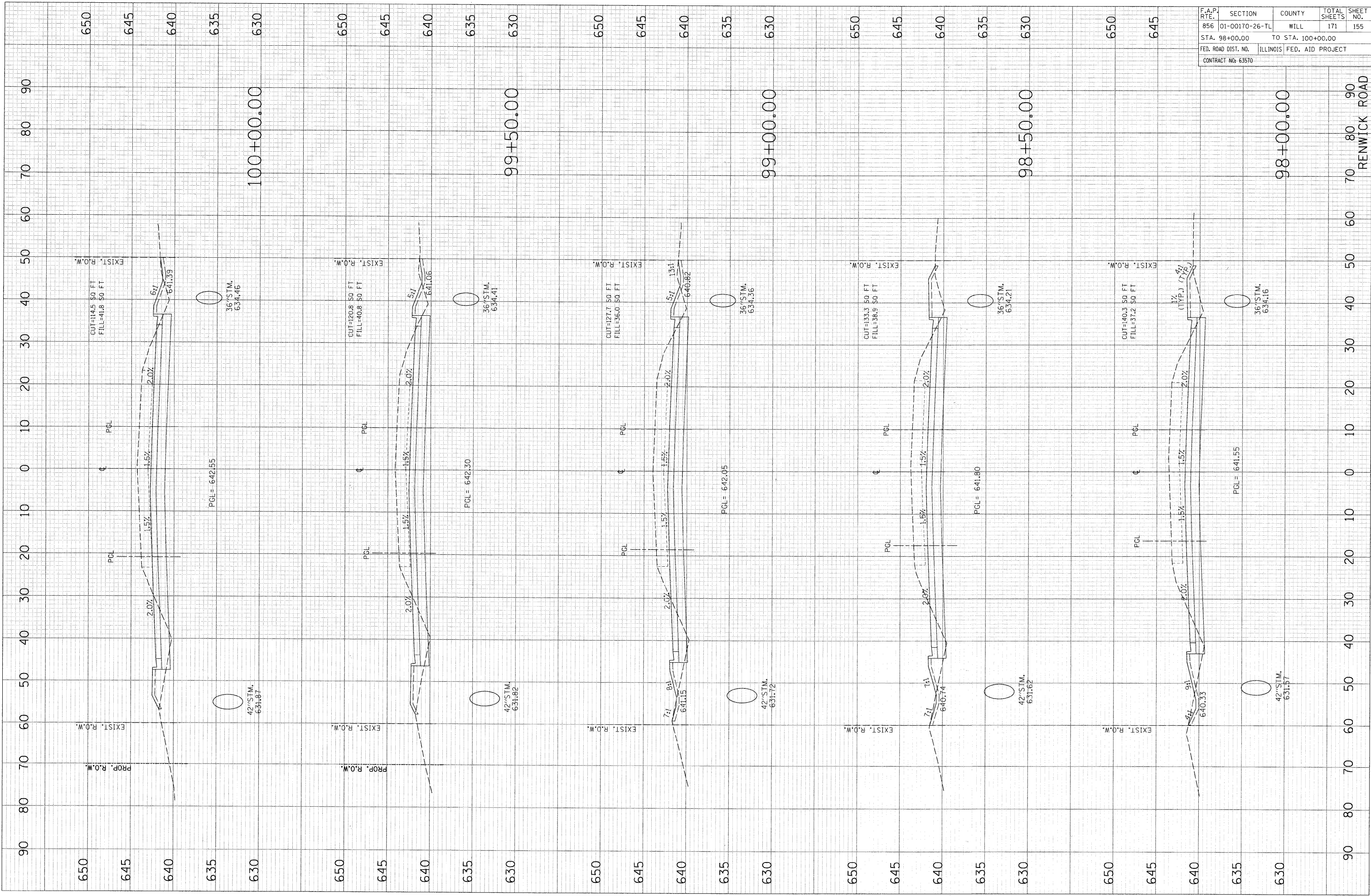
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	154
STA. 95+50.00		TO STA. 97+50.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

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RENWICK ROAD

PLOT DATE = 8/22/2011
 FILE NAME = P:\79766\cadd\dr-wrings\150.ssh\20110822\150.dwg
 PLOT SCALE = 1/8" = 1' / 1" / 1" / 1"
 USER NAME = pavetibz

ORIGINAL	BY	DATE
SURVEY		
FLIPPED		
PLATTED		
AREAS CHECKED		
NO.		

FINI	BY	DATE
SURVEY		
FLIPPED		
PLATTED		
AREAS CHECKED		
NO.		



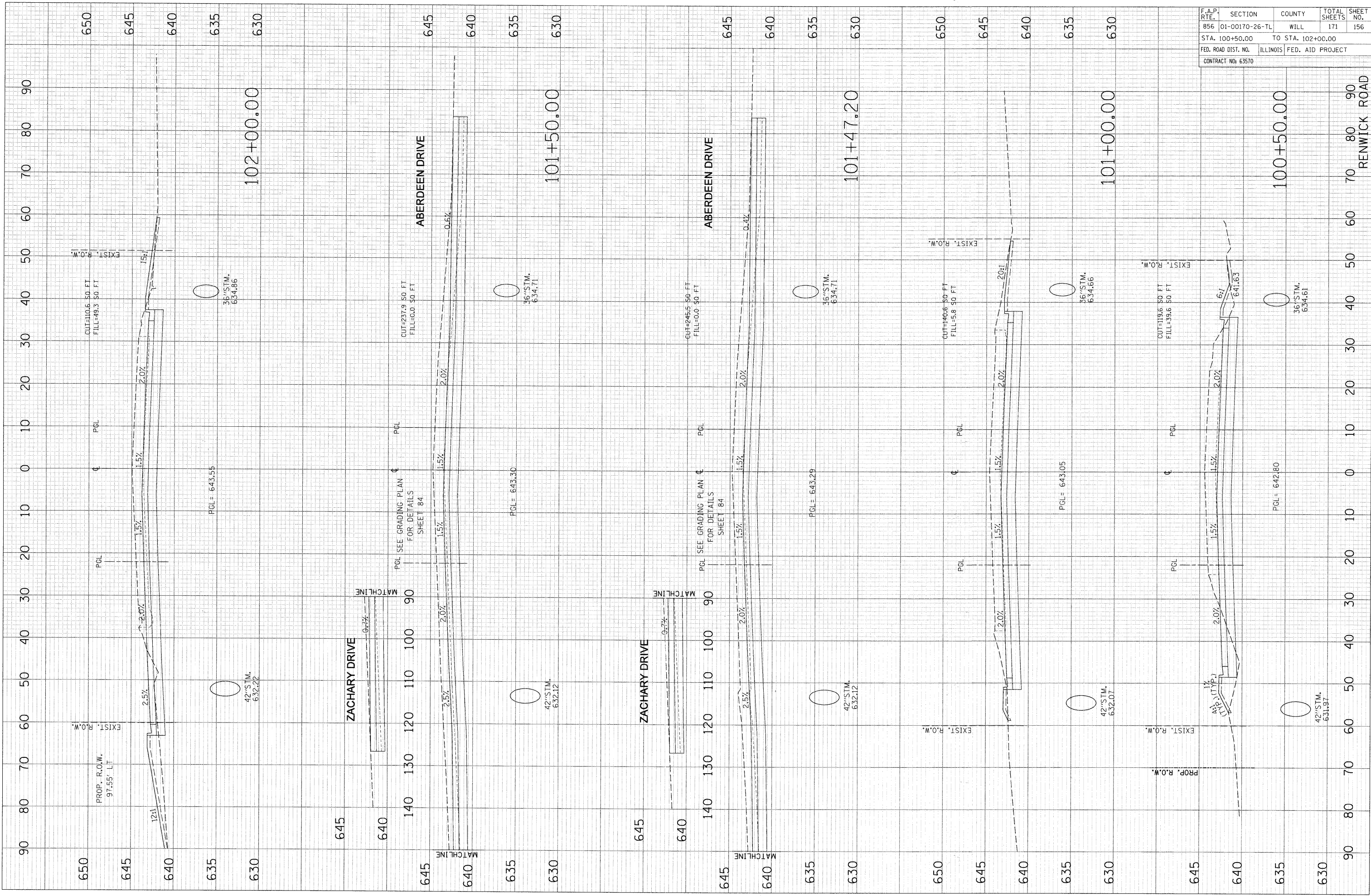
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	155
STA. 98+00.00		TO STA. 100+00.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

RENWICK ROAD

PLOT DATE = 8/22/2011
 FILE NAME = F:\79766\cadd\p\surveys\158\cadd\p\plan\158\plan\158.dwg
 PLOT SCALE = 1/8" = 1' / in.
 USER NAME = pavelipz

ORIGINAL SURVEY NO.	SURVEY DATE	BY

FINAL SURVEY NO.	SURVEY DATE	BY



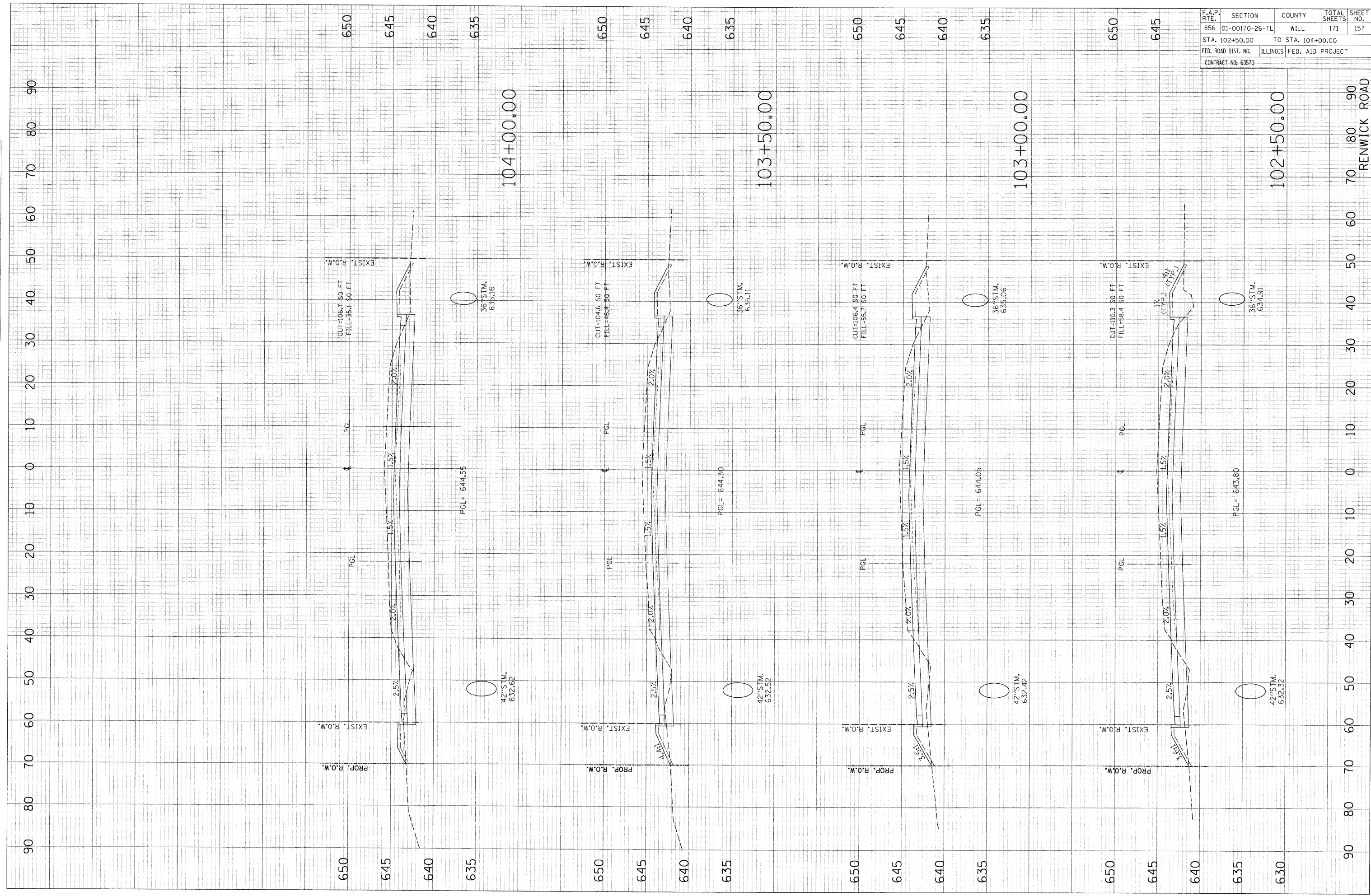
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	156
STA. 100+50.00		TO STA. 102+00.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

FINAL SURVEY
 SURVEYED BY
 DATE
 NO. CHECKED

ORIGINAL SURVEY
 SURVEYED BY
 DATE
 NO. CHECKED

PLOT DATE = 8/22/2011
 FILE NAME = P:\79766\add\p\avg\1E01\01-00170-26-TL.dwg
 PLOT SCALE = 10.0000' / 1" = 100.0000
 USER NAME = pavelip2

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	157
STA. 102+50.00 TO STA. 104+00.00				
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				



PLOT DATE = 8/22/2011
 FILE NAME = P:\79766\aved\dr-avg\158.as.sh
 PLOT SCALE = 10/000 / in.
 USER NAME = pavel62

ORIGINAL SURVEY SUBMITTED
 SURVEY SUBMITTED
 PLOTTED
 TEMPLATE AREAS CHECKED
 NO. AREAS CHECKED

FINAL SURVEY SUBMITTED
 SURVEY SUBMITTED
 NOTE BOOK AREAS CHECKED
 NO. AREAS CHECKED

BY DATE

655
650
645
640
635

655
650
645
640
635

ROSE SUBDIVISION
 WEST ENTRANCE
 SEE GRADING PLAN FOR DETAILS
 SHEET 84

CUT=149.4 SO FT
 FILL=9.1 SO FT

42"STM.
637.53

36"STM.
638.52

PCL = 646.25

106+50.00

655
650
645
640
635

655
650
645
640
635

CUT=108.7 SO FT
 FILL=34.9 SO FT

42"STM.
637.48

36"STM.
638.47

PCL = 645.77

106+00.00

650
645
640
635

650
645
640
635

CUT=109.3 SO FT
 FILL=32.6 SO FT

42"STM.
637.42

36"STM.
638.41

PCL = 645.37

105+50.00

650
645
640
635

650
645
640
635

CUT=113.4 SO FT
 FILL=24.4 SO FT

42"STM.
632.77

36"STM.
638.36

PCL = 645.05

105+00.00

650
645
640
635

650
645
640
635

CUT=112.1 SO FT
 FILL=48.0 SO FT

42"STM.
632.72

36"STM.
635.21

PCL = 644.80

104+50.00

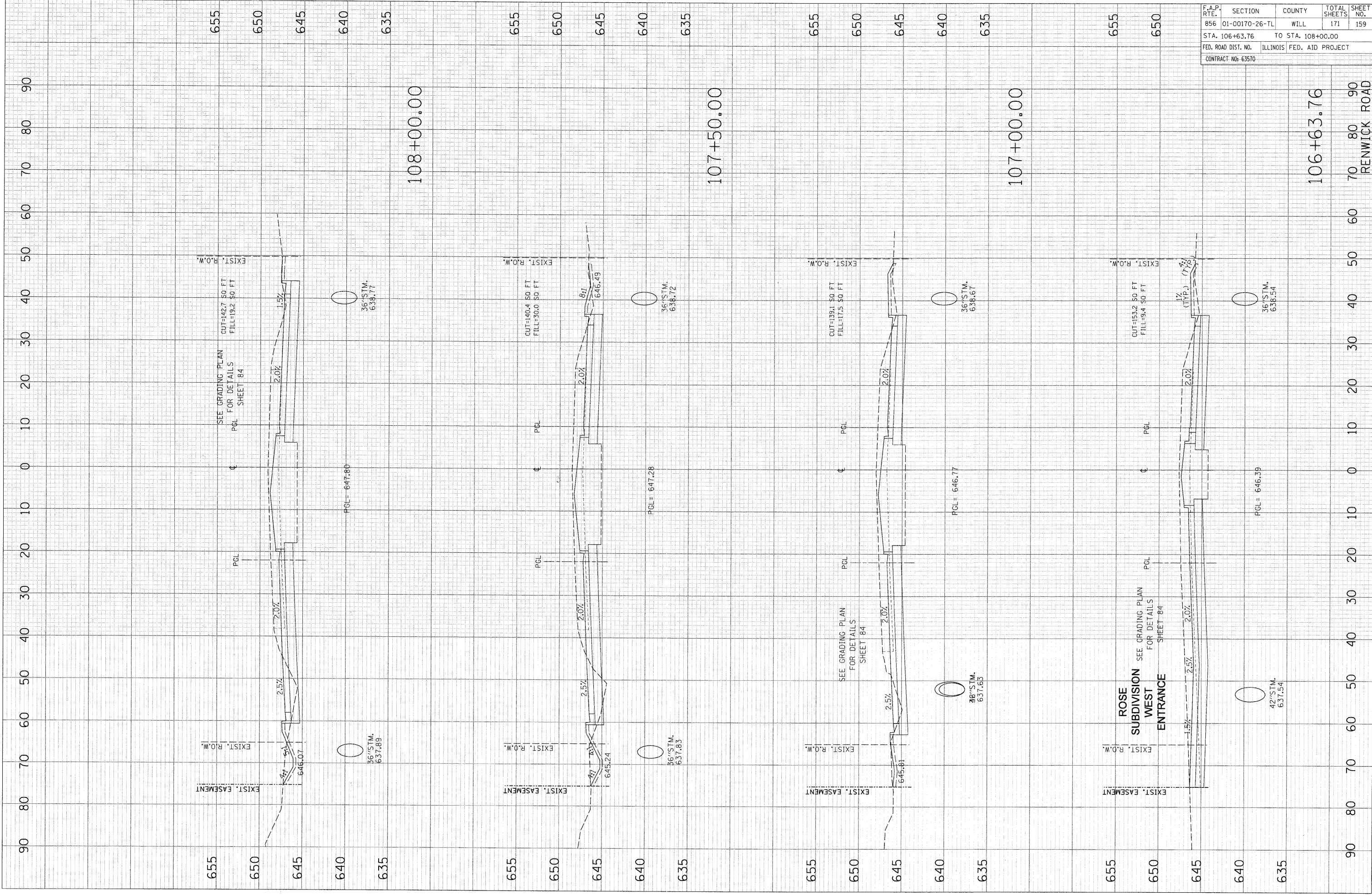
90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90
 RENWICK ROAD

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	158
STA. 104+50.00		TO STA. 106+50.00		
FED. ROAD DIST. NO.	ILLINOIS		FED. AID PROJECT	
CONTRACT NO: 63570				

PLOT DATE = 8/22/2011
 FILE NAME = P:\79766\road\108\108.dwg
 PLOT SCALE = 10.0000 / in.
 USER NAME = potvlp2

ORIGINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
TEMP. DATE		
AREAS CHECKED		
NO.		

FINAL SURVEY	BY	DATE
SURVEYED		
NOTE BOOK		
TEMP. DATE		
AREAS CHECKED		
NO.		



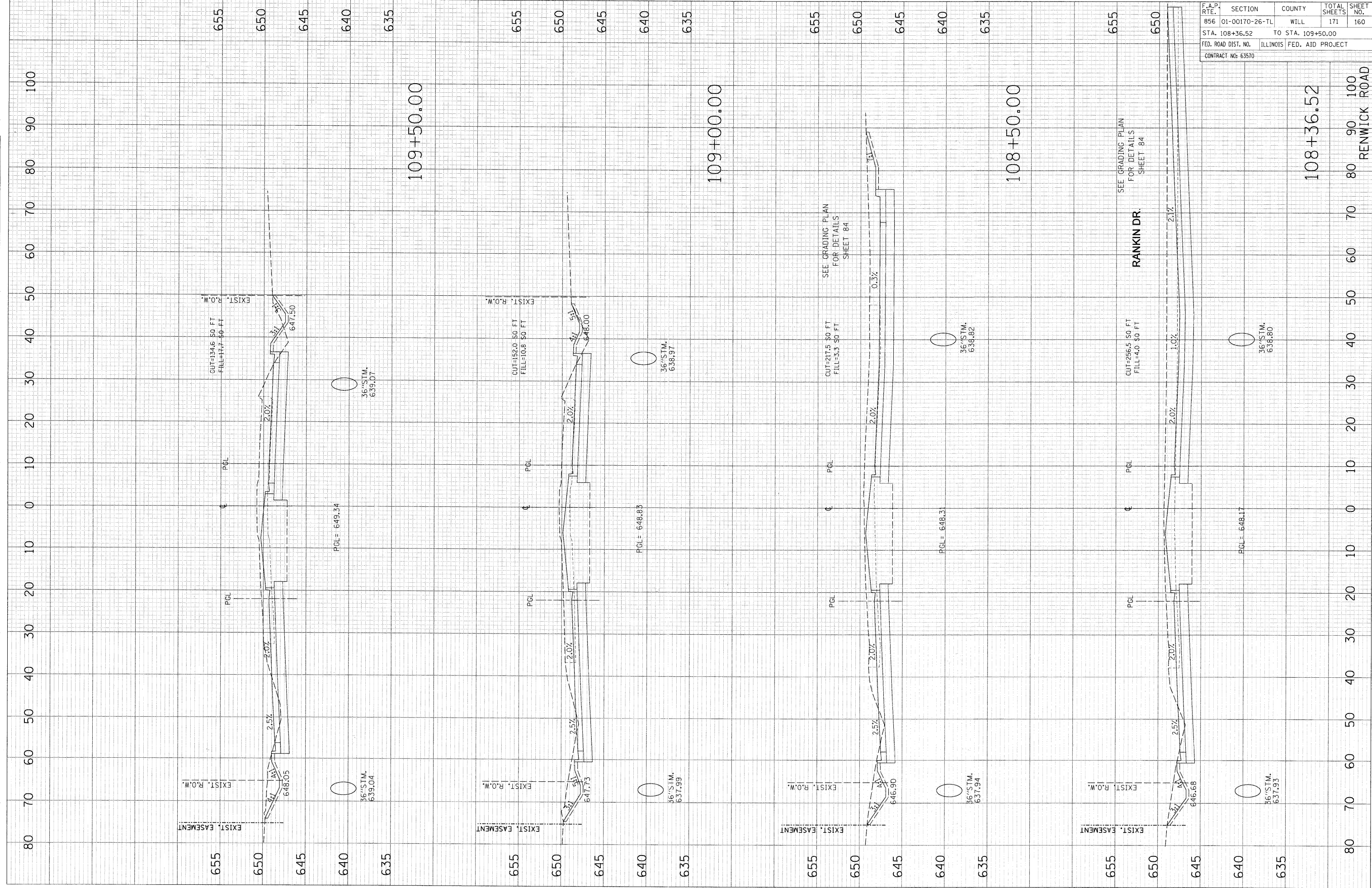
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	159
STA. 106+63.76		TO STA. 108+00.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

FINAL SURVEY	BY	DATE
SURVEYED		
NOTED BOOK		
AREAS CHECKED		
NO.		

ORIGINAL SURVEY	BY	DATE
PLOTTED		
AREAS CHECKED		
NO.		

PLOT DATE = 8/22/2011
 FILE NAME = P:\79766\add\of-strings\180-s.dwg
 PLOT SCALE = 10.000' / 1"
 USER NAME = pavelcz

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	160
STA. 108+36.52		TO STA. 109+50.00		
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			
CONTRACT NO: 63570				

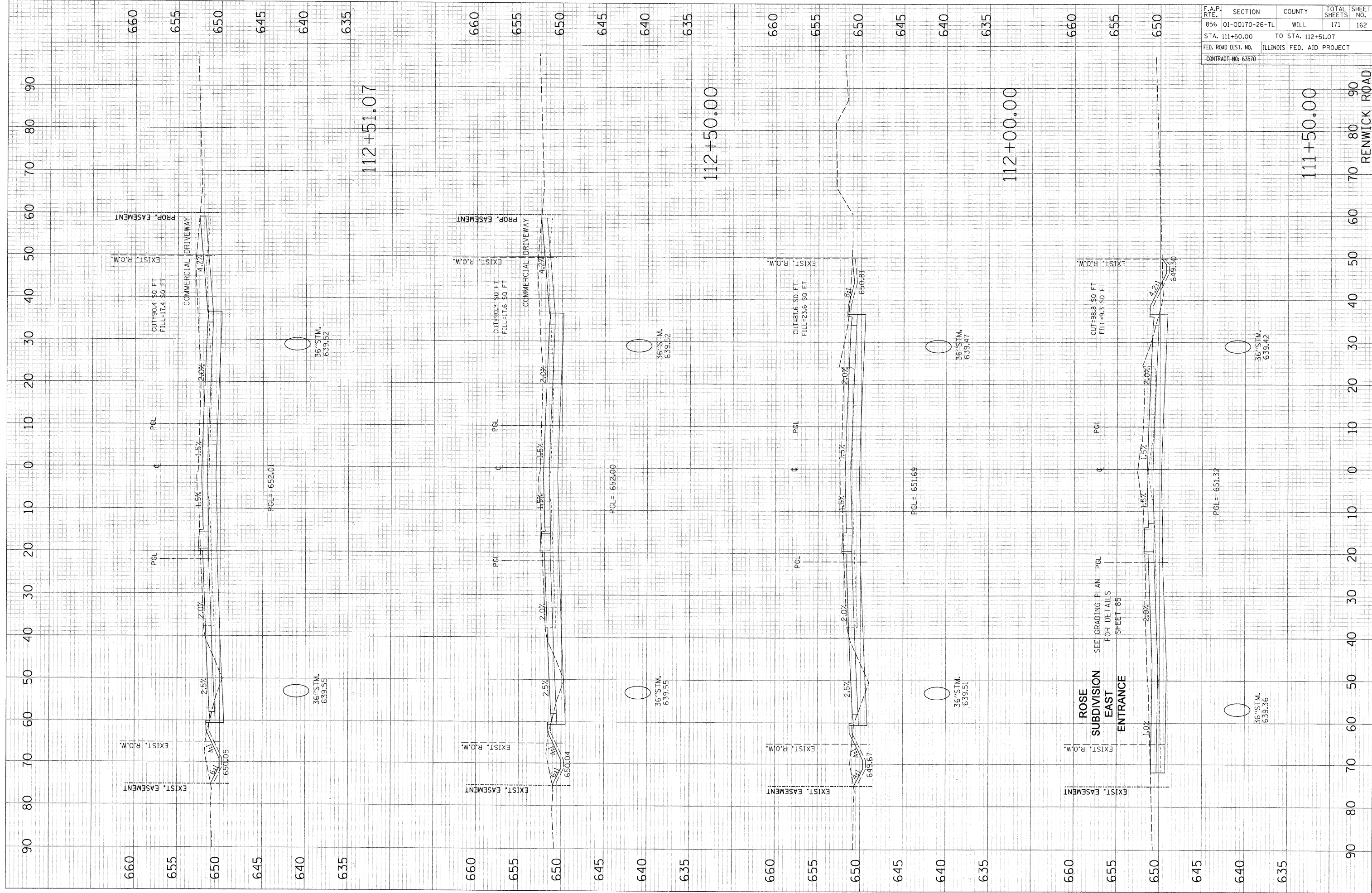


RENWICK ROAD

PLOT DATE = 8/22/2011
 FILE NAME = P:\79756\add\158\158.dwg
 PLOT SCALE = 10.000' / 1"
 USER NAME = pavelpz

ORIGINAL SURVEY PLOTTED AREAS CHECKED

FINAL SURVEY SURVEYED SURVEYED NOTE BOOK AREAS CHECKED



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	162
STA. 111+50.00		TO STA. 112+51.07		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63670				

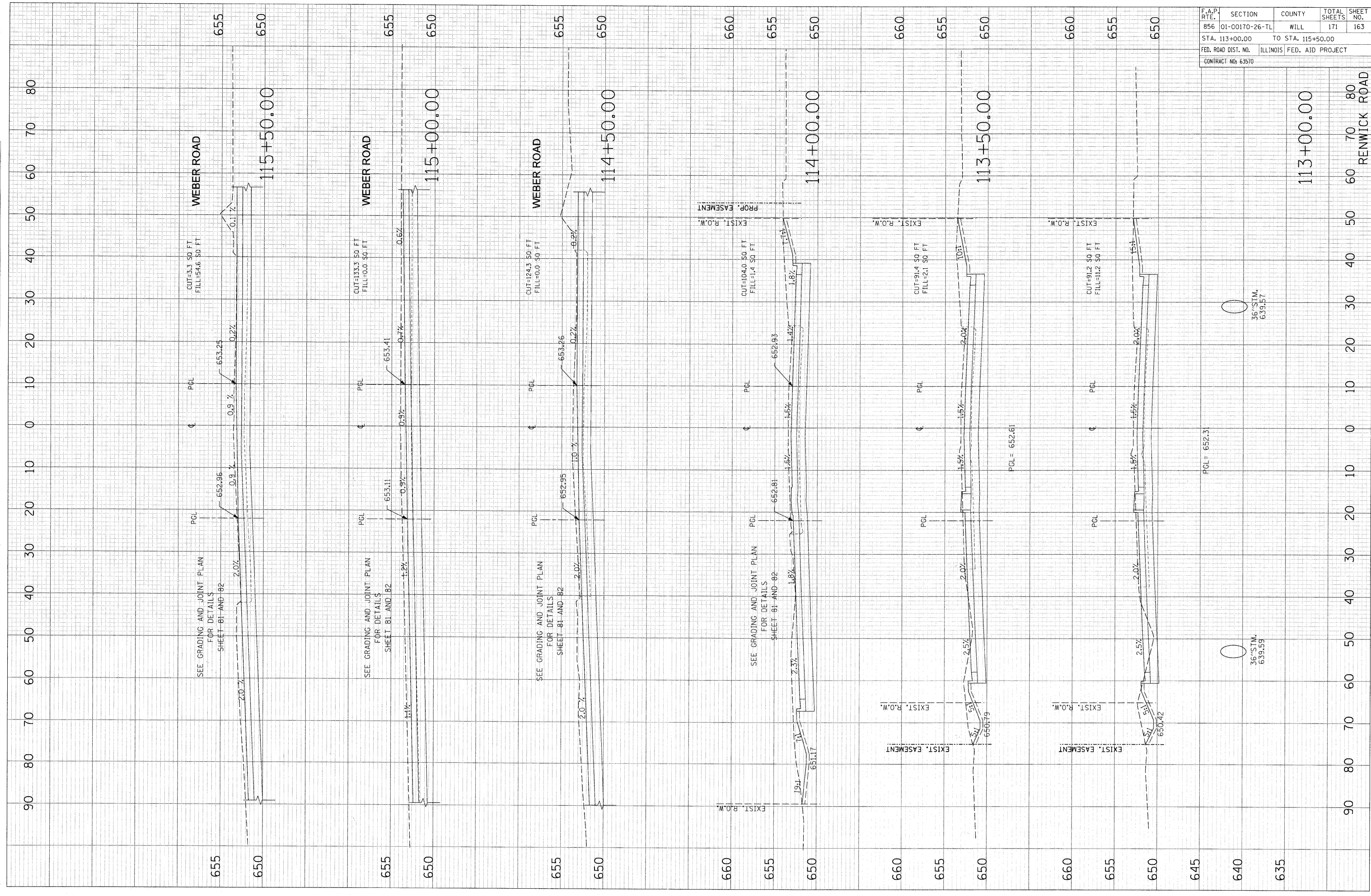
RENWICK ROAD

ROSE SUBDIVISION EAST ENTRANCE
 SEE GRADING PLAN FOR DETAILS
 SHEET 85

PLOT DATE = 8/22/2011
 FILE NAME = F:\79766\cadd\dr-avrgn\150-s.sh
 PLOT SCALE = 10.000 / in.
 USER NAME = pavelbz

ORIGINAL SURVEY PLOTTED
 SURVEYED BY DATE
 NO. AREAS CHECKED

FINAL SURVEY NOTE BOOK
 SURVEYED BY DATE
 NO. AREAS CHECKED



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	163
STA. 113+00.00		TO STA. 115+50.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

36" STIM.
639.517

36" STIM.
639.59

PGL = 652.31

PGL = 652.61

113+00.00

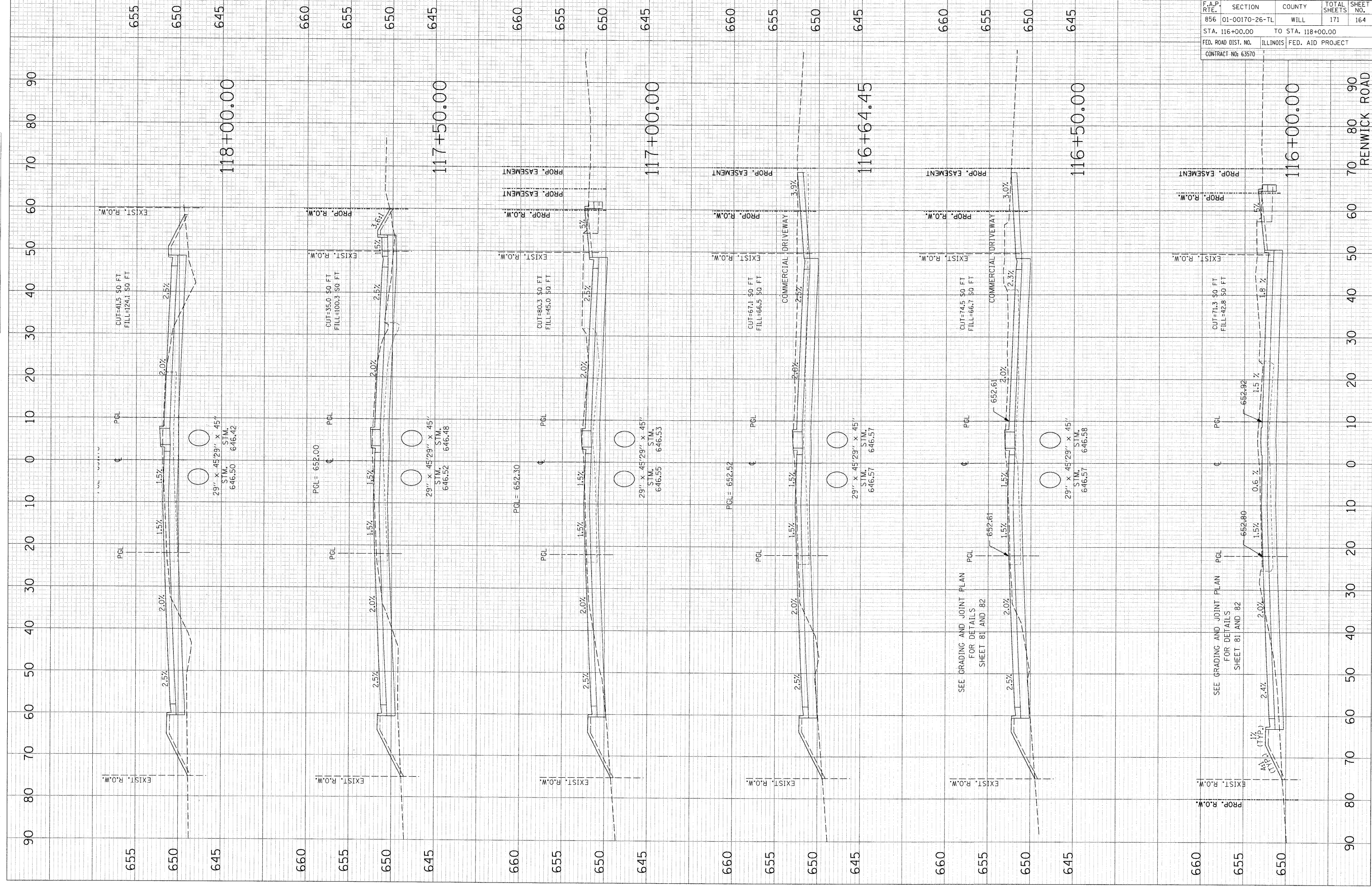
RENWICK ROAD

FINAL SURVEYED SURVEY PLOTTED TEMPLATE AREAS CHECKED NO. BY DATE

ORIGINAL SURVEY PLOTTED TEMPLATE AREAS CHECKED NO. BY DATE

PLOT DATE = 6/22/2011
 FILE NAME = P:\79766\cadd\158\158.dwg
 PLOT SCALE = 10.0000' / 1" /
 USER NAME = pavelipz

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	164
STA. 116+00.00		TO STA. 118+00.00		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO: 63570				



90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90
 RENWICK ROAD

SEE GRADING AND JOINT PLAN FOR DETAILS SHEET 81 AND 82

SEE GRADING AND JOINT PLAN FOR DETAILS SHEET 81 AND 82

CUT=41.5 50 FT
 FILL=124.1 50 FT

CUT=35.0 50 FT
 FILL=100.3 50 FT

CUT=80.3 50 FT
 FILL=45.0 50 FT

CUT=67.1 50 FT
 FILL=66.5 50 FT

CUT=74.5 50 FT
 FILL=66.7 50 FT

CUT=71.3 50 FT
 FILL=42.8 50 FT

118+00.00

117+50.00

117+00.00

116+64.45

116+50.00

116+00.00

655

650

645

660

655

650

645

660

655

650

645

660

655

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660

655

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655

650

645

EXIST. R.O.W.

EXIST. R.O.W.

EXIST. R.O.W.

EXIST. R.O.W.

EXIST. R.O.W.

EXIST. R.O.W.

PROP. EASEMENT

PROP. EASEMENT

PROP. EASEMENT

PROP. EASEMENT

PGL

PGL

PGL

PGL

PGL

PGL

29' x 45' 29' x 45'
 STM. 646.50
 646.42

29' x 45' 29' x 45'
 STM. 646.52
 646.48

29' x 45' 29' x 45'
 STM. 646.55
 646.53

29' x 45' 29' x 45'
 STM. 646.57
 646.57

29' x 45' 29' x 45'
 STM. 646.57
 646.58

PGL = 652.30

PGL = 652.52

PGL = 652.61

PGL = 652.86

PGL = 652.00

652.61

652.86

1% (TYP.)

COMMERCIAL DRIVEWAY

COMMERCIAL DRIVEWAY

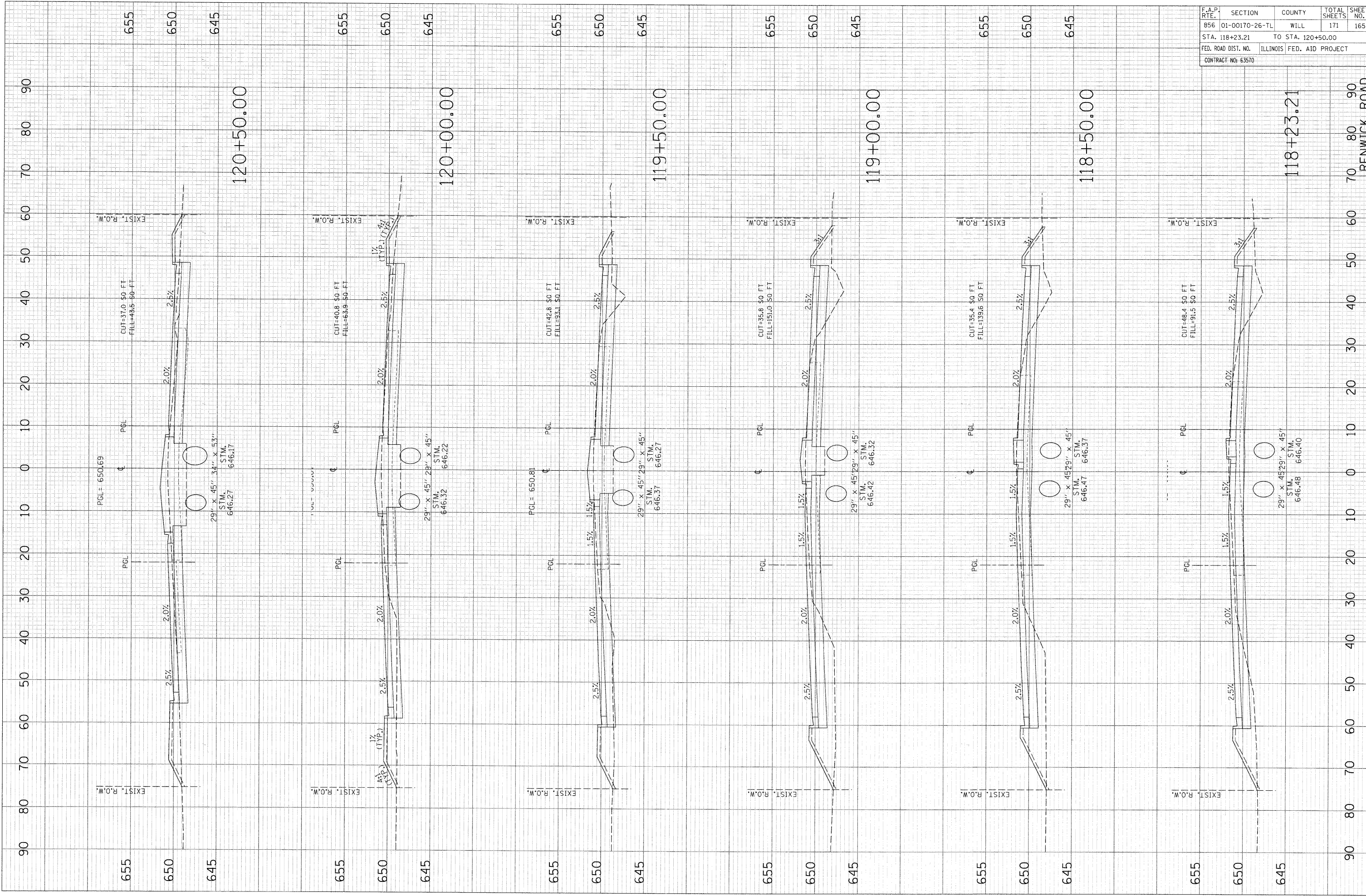
COMMERCIAL DRIVEWAY

PLOT DATE = 8/22/2011
 FILE NAME = P:\79766\cadd\drainage\150.ssh\150.dwg
 PLOT SCALE = 10/000 / in.
 USER NAME = potvibz

ORIGINAL SURVEY PLOTTED
 SURVEY PLOTTED
 TEMPLATE AREAS CHECKED

FINAL SURVEY PLOTTED
 SURVEY PLOTTED
 TEMPLATE AREAS CHECKED

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	165
STA. 118+23.21		TO STA. 120+50.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

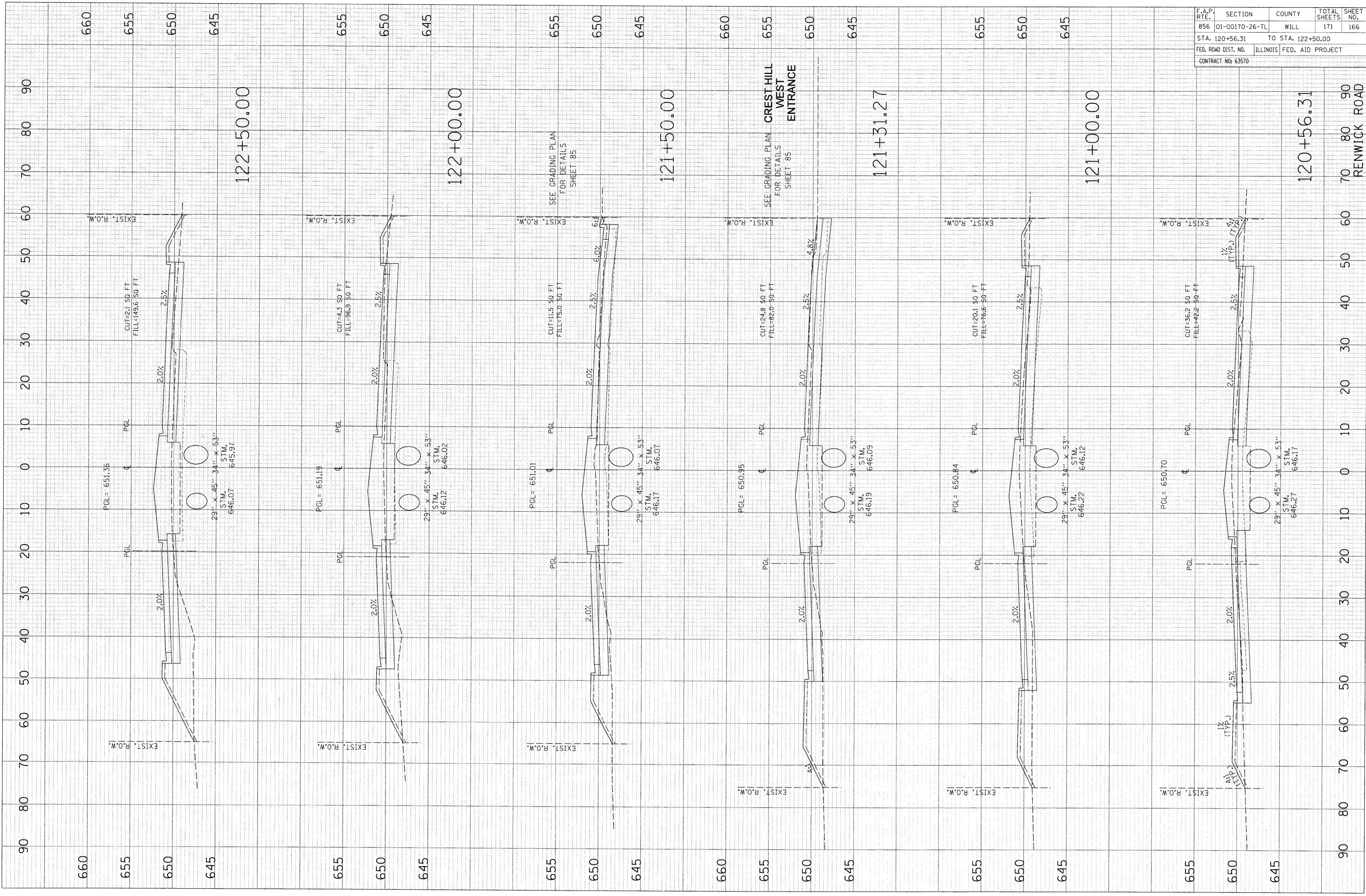


90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90
 RENWICK ROAD

PLOT DATE = 8/22/2011
 FILE NAME = P:\79766\cadd\cadd\overings\12011\12011.dwg
 PLOT SCALE = 10.0000 / 1"
 USER NAME = pavalicz

ORIGINAL SURVEY PLOTTED BY DATE
 SURVEY PLOTTED BY DATE
 AREAS CHECKED AREAS CHECKED

FINAL SURVEY PLOTTED BY DATE
 SURVEY PLOTTED BY DATE
 NOTE BOOK NO. AREAS CHECKED AREAS CHECKED



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	166
STA. 120+56.31		TO STA. 122+50.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

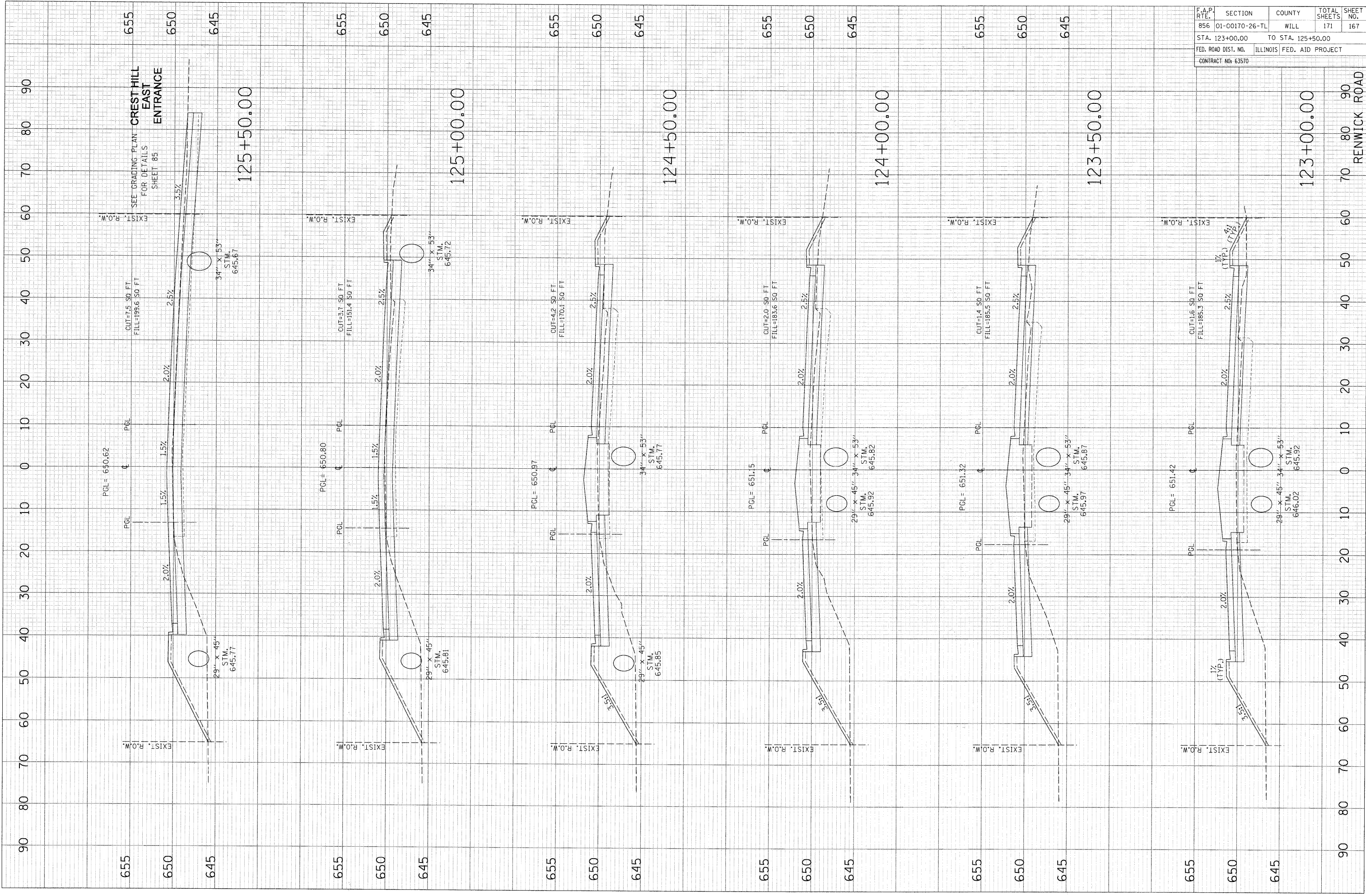
120+56.31
 70 80 90
 RENWICK ROAD

PLOT DATE = 8/22/2011
 FILE NAME = F:\79766\road\dr-wrings\189-0-4-12\crescent\189-0-4-12.dwg
 PLOT SCALE = 10.0000 / in.
 USER NAME = bowerbc

ORIGINAL SURVEY
 SURVEY PLOTTED
 AREAS CHECKED

FINAL SURVEY
 SURVEY PLOTTED
 NOTE BOOK AREAS CHECKED

BY: _____ DATE: _____



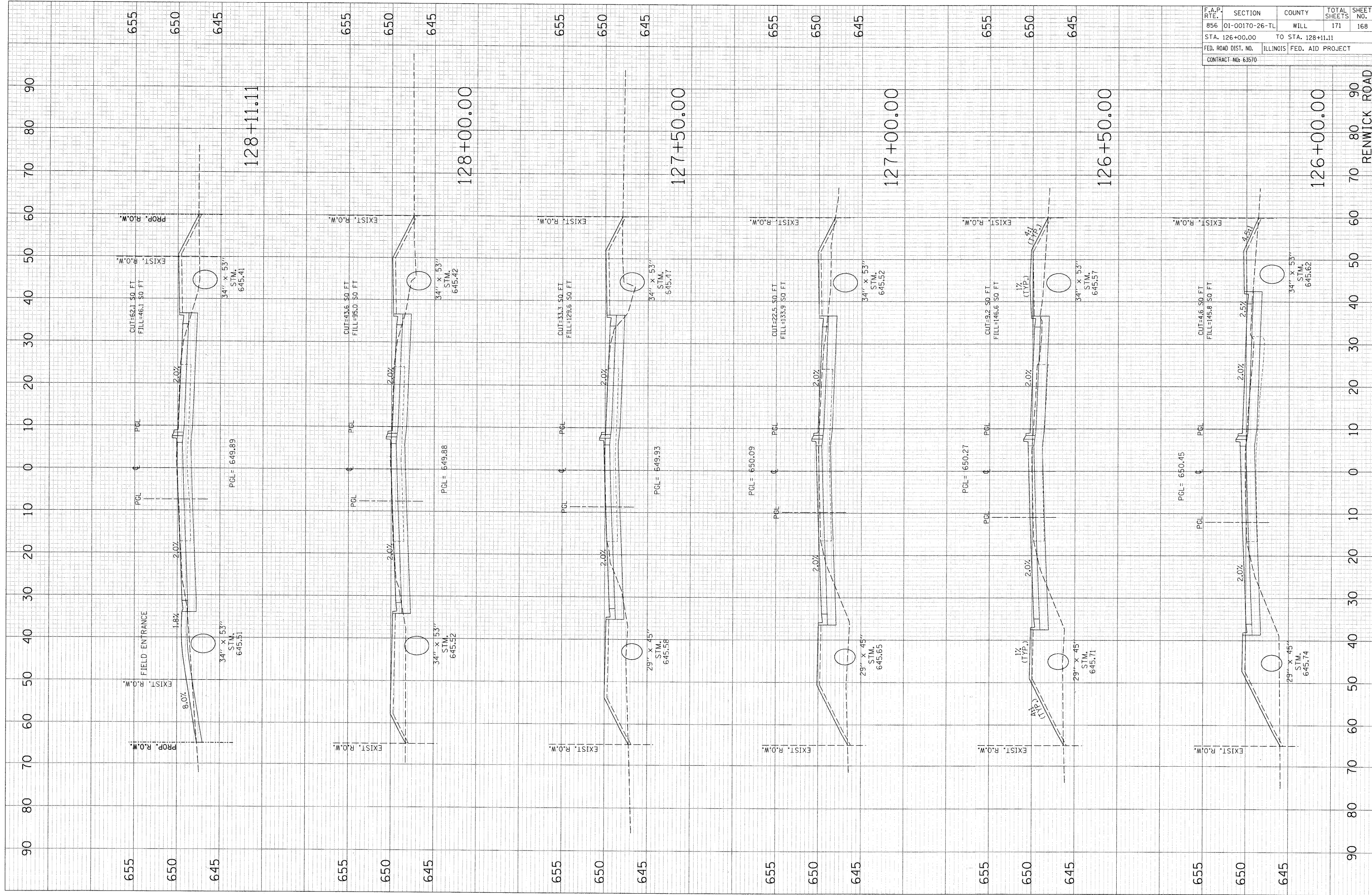
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	167
STA. 123+00.00		TO STA. 125+50.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

RENWICK ROAD

PLOT DATE = 8/22/2011
 FILE NAME = P:\79766\Cadd\126+00.rwg\126+00.dwg
 PLOT SCALE = 10.000' / 1" = 1000
 USER NAME = potolbz

ORIGINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
TEMPLATE		
AREAS		
AREAS CHECKED		

FINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
TEMPLATE		
AREAS		
AREAS CHECKED		



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	168
STA. 126+00.00		TO STA. 128+11.11		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

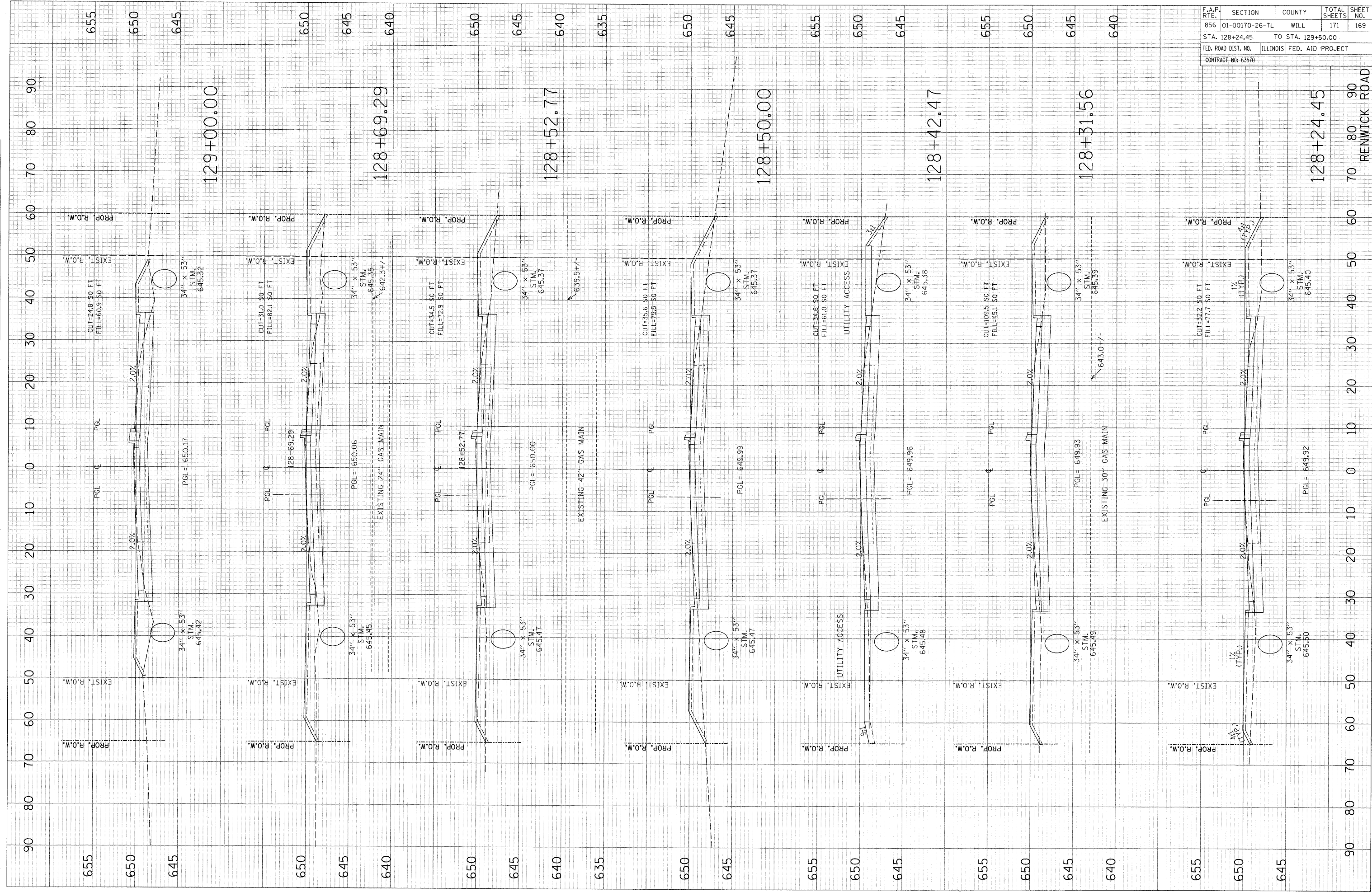
126+00.00
 127+00.00
 127+50.00
 128+00.00
 128+11.11
 RENWICK ROAD

FINAL SURVEY	BY	DATE
SURVEYED		
NOTED BOOK		
AREAS CHECKED		
NO.		

ORIGINAL SURVEY	BY	DATE
SURVEYED		
NOTED BOOK		
AREAS CHECKED		
NO.		

PLOT DATE = 8/22/2011
 FILE NAME = P:\79766\cadd\dwg\wings\156.dwg
 PLOT SCALE = 10.0000 / 1"
 USER NAME = pavelcp

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	169
STA. 128+24.45		TO STA. 129+50.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				



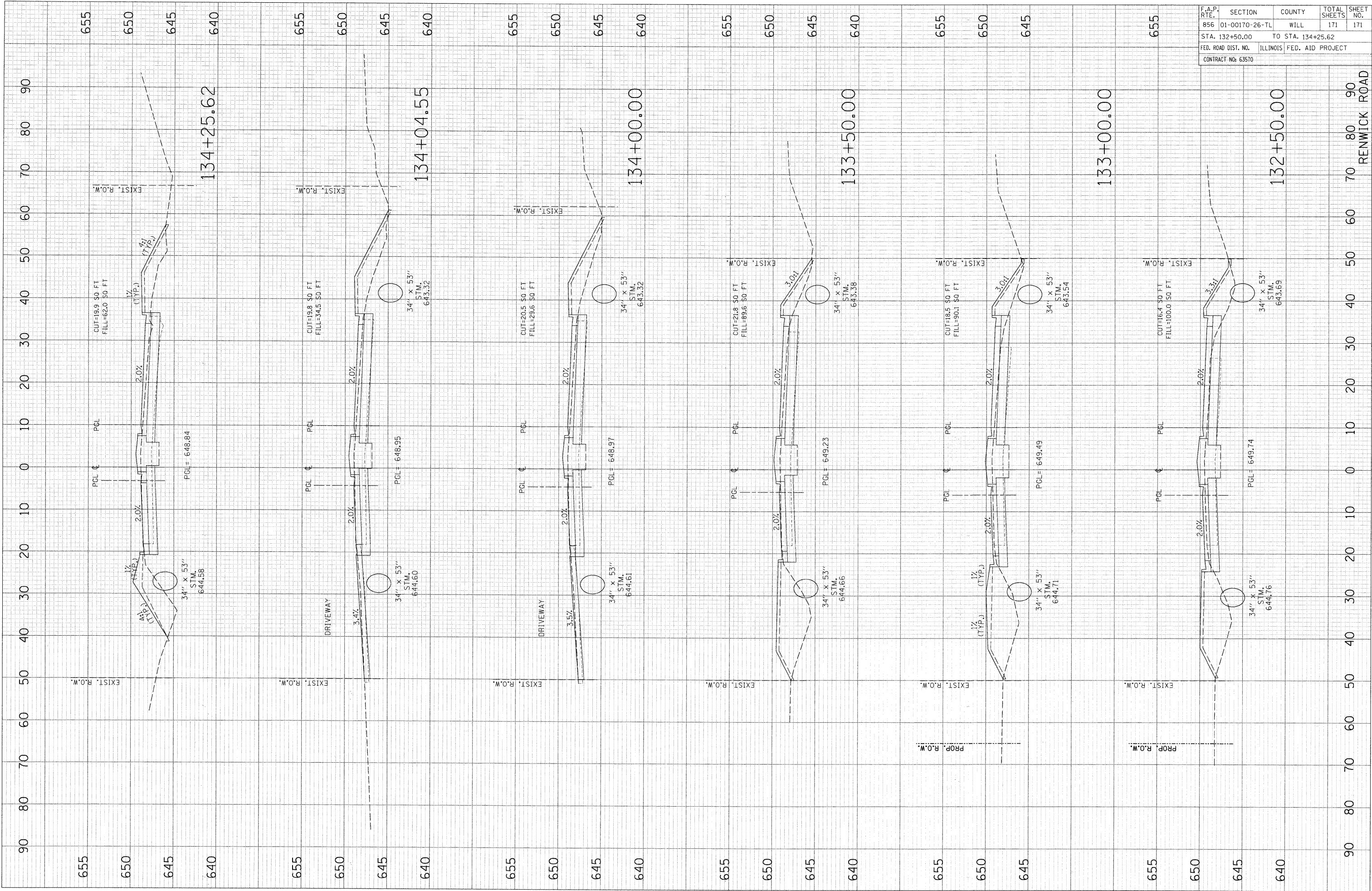
RENWICK ROAD

PLOT DATE = 8/22/2011
 FILE NAME = P:\179765\cadd\drwg\150s.shp
 PLOT SCALE = 10.0000 / in.
 USER NAME = ppatilp2

ORIGINAL SURVEY SURVEY PLOTTED
 SURVEY PLOTTED
 NO. AREAS CHECKED

FINAL SURVEY SURVEY PLOTTED
 SURVEY PLOTTED
 NO. AREAS CHECKED

BY DATE



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
856	01-00170-26-TL	WILL	171	171
STA. 132+50.00		TO STA. 134+25.62		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO: 63570				

RENWICK ROAD