

PROGRAM AND OFFICE ENGINEER CHARLES F. RIDDLE, P.E. 847-705-4406, SCHAUMBURG  
CONSULTING ENGINEER: ENGINEERING ENTERPRISES, INC. CONTACT: JAMES R. LENZINI 630-466-6700

03-09-12 LETTING ITEM 067

**FOR INDEX OF SHEETS  
AND HIGHWAY STANDARDS  
SEE SHEET NO. 2**

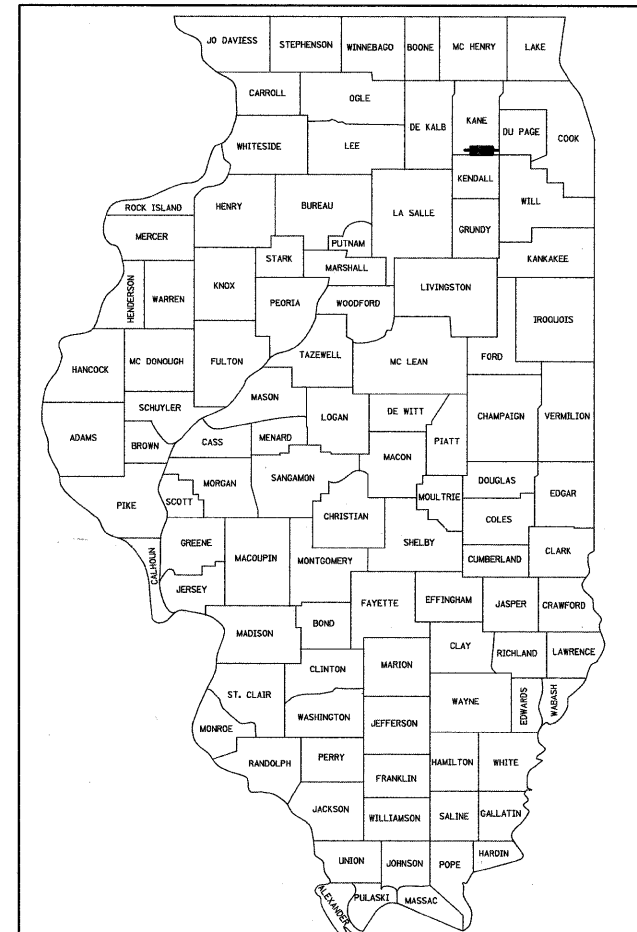
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS**

**PLANS FOR PROPOSED  
FEDERAL AID HIGHWAY**

**FAU 3895 (PRAIRIE STREET)  
OVER BLACKBERRY CREEK  
BRIDGE REHABILITATION  
SECTION: 08-15104-01-BR  
PROJECT NUMBER: BHOS-0089(134)  
SUGAR GROVE TOWNSHIP  
KANE COUNTY  
JOB NUMBER: C-91-167-09**

CONTRACT NO. 63661

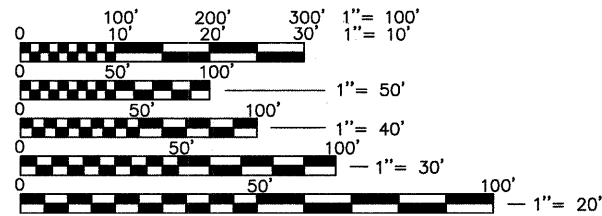
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3895	08-15104-01-BR	KANE	38	1



LOCATION OF SECTION INDICATED THUS: - ■ -


PROJECT LOCATED IN THE  
SUGAR GROVE TOWNSHIP  
ROAD DISTRICT,  
KANE COUNTY, IL

**DESIGN DESIGNATION**  
COLLECTOR  
ADT: 2,050 (2010)  
ADT: 5,700 (2022)  
POSTED SPEED: 40 MPH  
DESIGN SPEED: 40 MPH  
STRUCTURE NO: 045-3094 (EXISTING)  
STRUCTURE NO: 045-3094 (PROPOSED)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

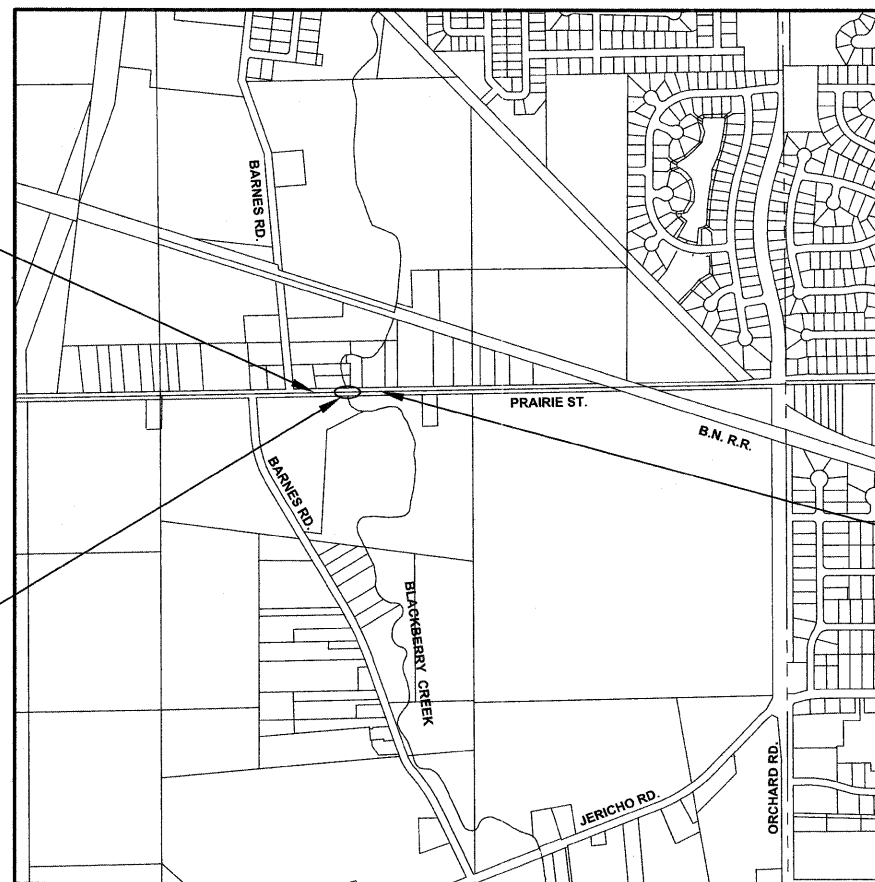
**JULIE**  
JOINT  
UTILITY  
LOCATION  
INFORMATION FOR  
EXCAVATION  
CALL 811



Know what's below.  
Call before you dig.

PROJECT BEGINS  
STA. 162+00  
PRAIRIE ST.

SN 045-3094



SW 1/4 SECT. 24, NW 1/4 SECT. 25  
T38N, R7E, 3RD PM, SUGAR GROVE TOWNSHIP  
**LOCATION MAP**  
TOTAL LENGTH OF PROJECT = 565 FEET (0.107 MILES)

PROJECT ENDS  
STA. 167+65  
PRAIRIE ST.

DATE: 12/15/2011  
BY: James R. Lenzini  
JAMES R. LENZINI  
LICENSE EXPIRES: NOVEMBER 30, 2013  
SHEETS: 1-2, 8-13, & 31-38



SEAL

DATE: 12/15/2011  
BY: David S. Bronars  
DAVID S. BRONARS  
LICENSE EXPIRES: NOVEMBER 30, 2012  
SHEETS: 3-5, 14-30 ONLY



SEAL

**Engineering Enterprises, Inc.**  
CONSULTING ENGINEERS  
52 Wheeler Road  
Sugar Grove, Illinois 60554  
630.466.6700 - www.eeiweb.com



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Plotted: December 15, 2011 @ 2:31 PM By: Kris Pung - Tab: 01\_CVR (23x34)

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SPECIALTY ITEM	SPECIAL PROVISION	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE			
						0004	0014	0042	
		20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	120	120			
	*	20200100	EARTH EXCAVATION	CU YD	515	515			
		20800150	TRENCH BACKFILL	CU YD	68	68			
		21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	710	710			
		21301072	EXPLORATION TRENCH 72" DEPTH	FOOT	100	100			
	*	25000210	SEEDING, CLASS 2A	ACRE	0.2	0.2			
	*	25000314	SEEDING, CLASS 4B	ACRE	0.1	0.1			
	*	25000400	NITROGEN FERTILIZER NUTRIENT	POUND	27	27			
	*	25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	27	27			
	*	25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	27	27			
	*	25100630	EROSION CONTROL BLANKET	SQ YD	1000	1000			
	*	25100900	TURF REINFORCEMENT MAT	SQ YD	168	168			
	*	25301800	SEEDLINGS	UNIT	0.4	0.4			
		28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	60	60			
		28000305	TEMPORARY DITCH CHECKS	FOOT	42	42			
		28000315	AGGREGATE DITCH CHECKS	TON	8	8			
		28000400	PERIMETER EROSION BARRIER	FOOT	377	377			
		28000500	INLET AND PIPE PROTECTION	EACH	3	3			
		28000510	INLET FILTERS	EACH	4	4			
		28100105	STONE RIPRAP, CLASS A3	SQ YD	133	133			
		28200200	FILTER FABRIC	SQ YD	161	161			
	*	40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	170	170			
		40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	625	625			
		40600300	AGGREGATE (PRIME COAT)	TON	6	6			
		40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	22	22			
		40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	335	335			
		40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	105	105			
		44000100	PAVEMENT REMOVAL	SQ YD	682	682			
		44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	222	222			
		48101200	AGGREGATE SHOULDERS, TYPE B	TON	30	30			
		48203021	HOT-MIX ASPHALT SHOULDERS, 6"	SQ YD	148	148			
		48301000	PROTECTIVE COAT	SQ YD	81	81			

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DESIGNED - LM	REVISED - _____
CHECKED - GG	REVISED - _____
DRAWN - LM	REVISED - _____
CHECKED - DSB	REVISED - _____

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SHEET NO. 1 OF 3 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3895	08-15104-01-BR	KANE	38	3
			CONTRACT NO. 636b1	
ILLINOIS FED. AID PROJECT				

SPECIALTY ITEM	SPECIAL PROVISION	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE			
						0004	0014	0042	
		50101500	REMOVAL OF EXISTING SUPERSTRUCTURES	EACH	1		1		
		50102400	CONCRETE REMOVAL	CU YD	1.9		1.9		
		50104650	SLOPE WALL REMOVAL	SQ YD	1		1		
		50105220	PIPE CULVERT REMOVAL	FOOT	49	49			
		50300225	CONCRETE STRUCTURES	CU YD	3.9		3.9		
		50300300	PROTECTIVE COAT	SQ YD	426		426		
	*	50400405	PRECAST PRESTRESSED CONCRETE DECK BEAMS (21" DEPTH)	SQ FT	3780		3780		
		50500405	FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	2220		2220		
		50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	5910		5910		
*		50901050	STEEL RAILING, TYPE SM	FOOT	271		271		
		51100300	SLOPE WALL 6 INCH	SQ YD	1		1		
		51500100	NAME PLATES	EACH	1		1		
		54010302	PRECAST CONCRETE BOX CULVERTS 3' X 2'	FOOT	104	104			
		54213663	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 18"	EACH	1	1			
		54213669	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 24"	EACH	1	1			
		54247110	GRATING FOR CONCRETE FLARED END SECTION 18"	EACH	1	1			
		54247130	GRATING FOR CONCRETE FLARED END SECTION 24"	EACH	1	1			
		550A0070	STORM SEWERS, CLASS A, TYPE 1 15"	FOOT	28	28			
		550A0090	STORM SEWERS, CLASS A, TYPE 1 18"	FOOT	161	161			
		550A0120	STORM SEWERS, CLASS A, TYPE 1 24"	FOOT	9	9			
		550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	32	32			
		550A0360	STORM SEWERS, CLASS A, TYPE 2 15"	FOOT	42	42			
		59000200	EPOXY CRACK INJECTION	FOOT	99		99		
		60200805	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 8 GRATE	EACH	1	1			
		60207605	CATCH BASINS, TYPE C, TYPE 8 GRATE	EACH	2	2			
		60207915	CATCH BASINS, TYPE C, TYPE IIV FRAME AND GRATE	EACH	1	1			
		60223800	MANHOLES, TYPE A, 6'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	2	2			
		60236825	INLETS, TYPE A, TYPE IIV FRAME AND GRATE	EACH	1	1			
		60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	460	460			
*		63000005	STEEL PLATE BEAM GUARDRAIL, TYPE B	FOOT	37.5	37.5			
*		63100087	TRAFFIC BARRIER TERMINAL, TYPE 6A	EACH	3	3			
*		63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	2	2			

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CHECKED - DSB	REVISED - _____

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

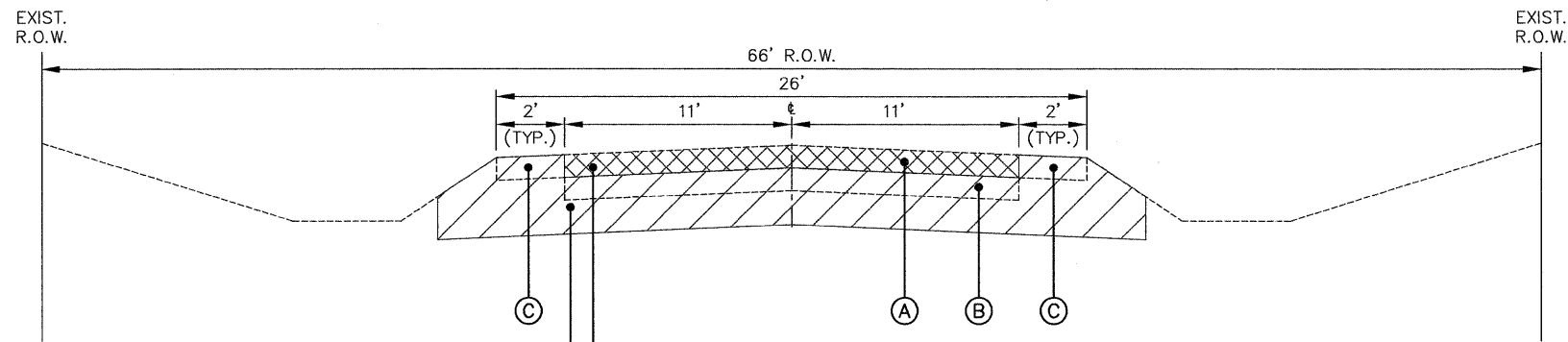
**SUMMARY OF QUANTITIES**

SHEET NO. 2 OF 3 SHEETS

F.A.U. RTE. 3895	SECTION 08-15104-01-BR	COUNTY KANE	TOTAL SHEETS 38	SHEET NO. 4
ILLINOIS FED. AID PROJECT CONTRACT NO. 63661				



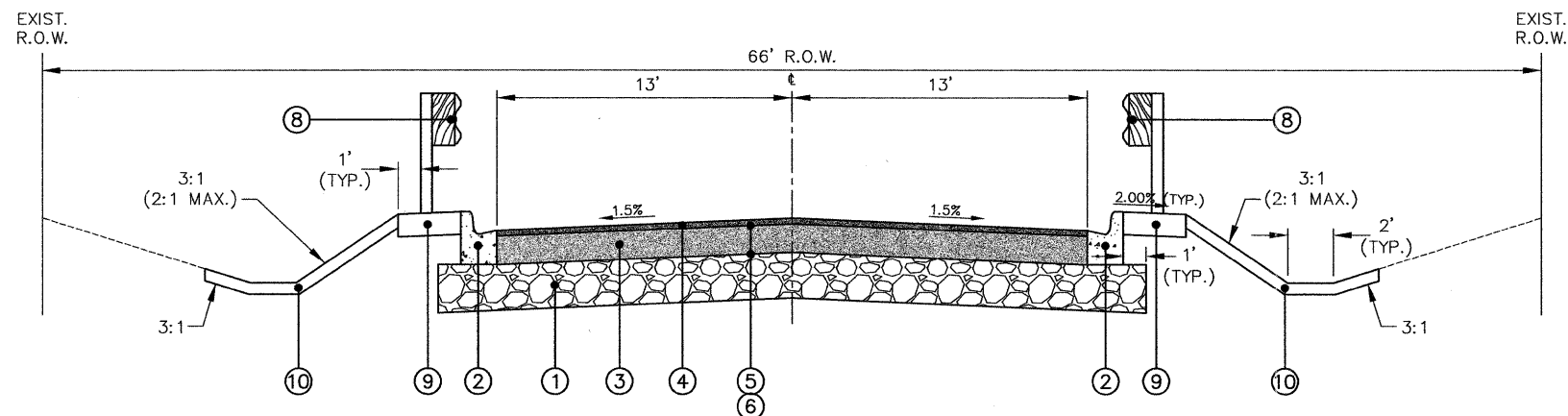




**PRAIRIE STREET  
EXISTING TYPICAL SECTION**

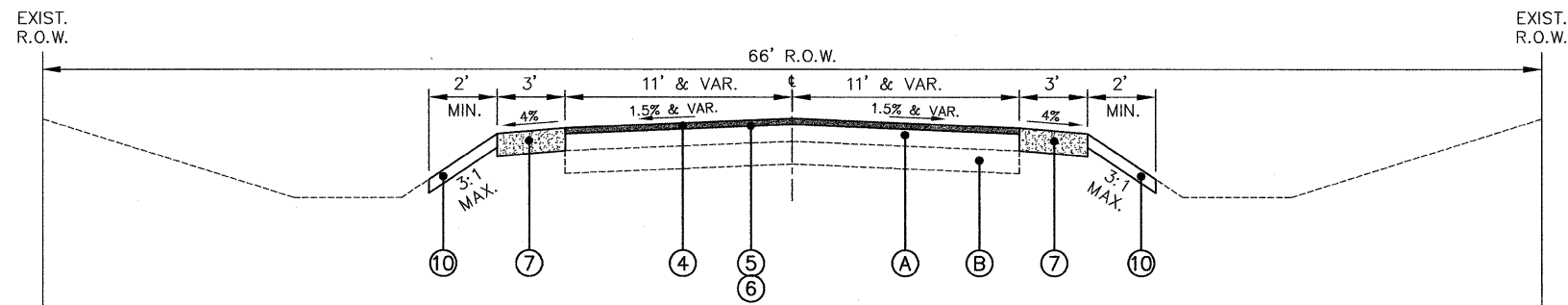
STA 162+00 TO STA 164+07.67  
STA 165+44.33 TO STA 167+65  
(N.T.S.)

- (D) STA 162+00 TO 162+50 & 166+50 TO 167+65
- (E) STA 162+50 TO 164+07.67 & 165+44.33 TO 166+50
- (F) STA 162+50 TO 164+07.67 & 165+44.33 TO 166+50



**PRAIRIE STREET  
PROPOSED TYPICAL SECTION**

STA 162+50 TO STA 164+07.67  
STA 165+44.33 TO STA 166+50  
(N.T.S.)



**PRAIRIE STREET  
PROPOSED TYPICAL SECTION**

STA 162+00 TO STA 162+50  
STA 166+50 TO STA 167+65  
(N.T.S.)

EXISTING LEGEND	
(A)	EXISTING ASPHALT PAVEMENT
(B)	EXISTING AGGREGATE BASE
(C)	EXISTING AGGREGATE SHOULDER
(D)	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (0" TO 2")
(E)	PAVEMENT REMOVAL (6" ASPHALT ASSUMED)
(F)	EARTH EXCAVATION

PROPOSED LEGEND	
(1)	AGGREGATE SUBGRADE 12"
(2)	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
(3)	HOX-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 7.5"
(4)	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1.5"
(5)	BITUMINOUS MATERIALS (PRIME COAT)
(6)	AGGREGATE (PRIME COAT)
(7)	AGGREGATE SHOULDERS, TYPE B
(8)	STEEL PLATE BEAM GUARDRAIL
(9)	HOT-MIX ASPHALT SHOULDERS 6" AT STEEL PLATE BEAM GUARD RAIL
(10)	TOPSOIL 4", SEEDING, FERTILIZER AND EROSION CONTROL BLANKET

PAVEMENT DESIGN INFORMATION	
PRAIRIE STREET	TF = 0.27 (ACTUAL)
HOT-MIX ASPHALT PAVEMENT	TF = 0.50 (USED)
CLASS II	SSR POOR
80,000 LB	AC MIX TEMP 78°
TWO LANE RURAL	PG 64-22
2022 ADT 5,700	MODULUS 600 KSI
PV 5,506 (96.6%)	THICKNESS REQUIRED = 8.75"
SU 177 (3.1%)	THICKNESS PROVIDED = 9"
MU 17 (0.3%)	

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
OPERATION	MIXTURE TYPE	AIR VOIDS @ N <sub>des</sub>
ROADWAY RECONSTRUCTION	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 7.5" (IN 2 LIFTS)	4% @ 50 Gyr
ROADWAY RESURFACING	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5 MM), 1.5"	4% @ 50 Gyr
DRIVEWAY RECONSTRUCTION	STABILIZED DRIVEWAYS, 10"	
SHOULDER	HMA SURFACE COURSE, MIX "D", N50 (IL-9.5 MM), 3"	4% @ 50 Gyr
	HOT-MIX ASPHALT SHOULDERS 6"	
	HMA BINDER, IL-19.0, 6" (IN 2 LIFTS)	4% @ 50 Gyr

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE SPECIAL PROVISIONS.

Plotted: January 3, 2012 @ 2:08 PM By: Kris Pung - Job: 06 TYP SEC (22-34)  
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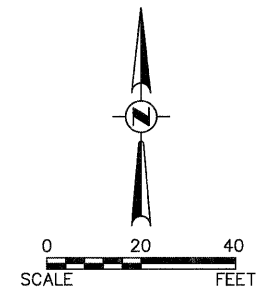
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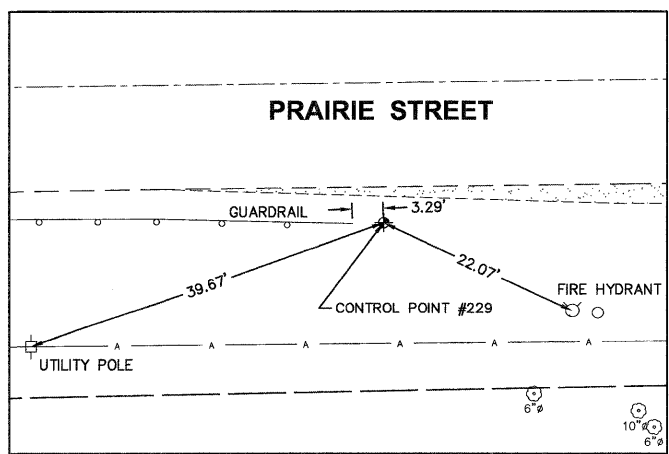
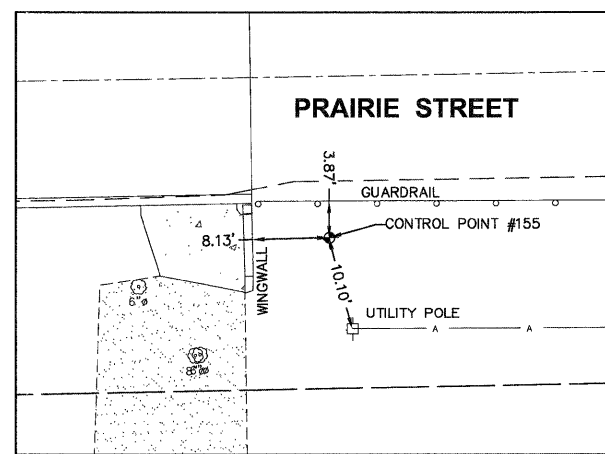
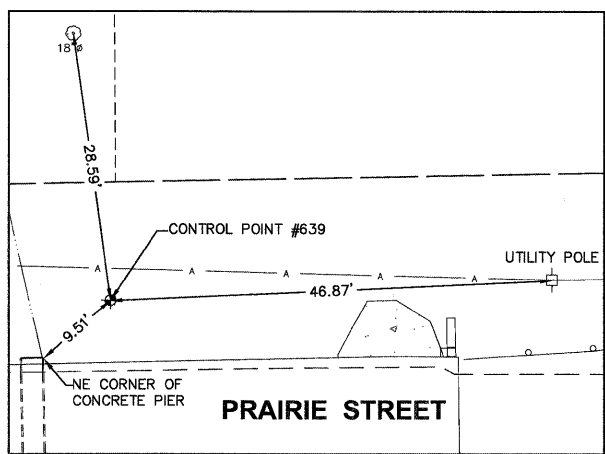
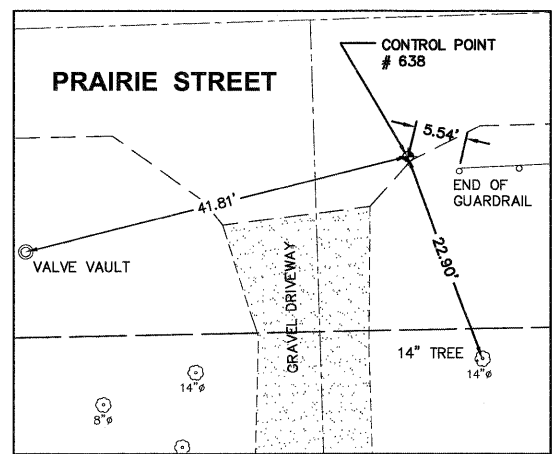
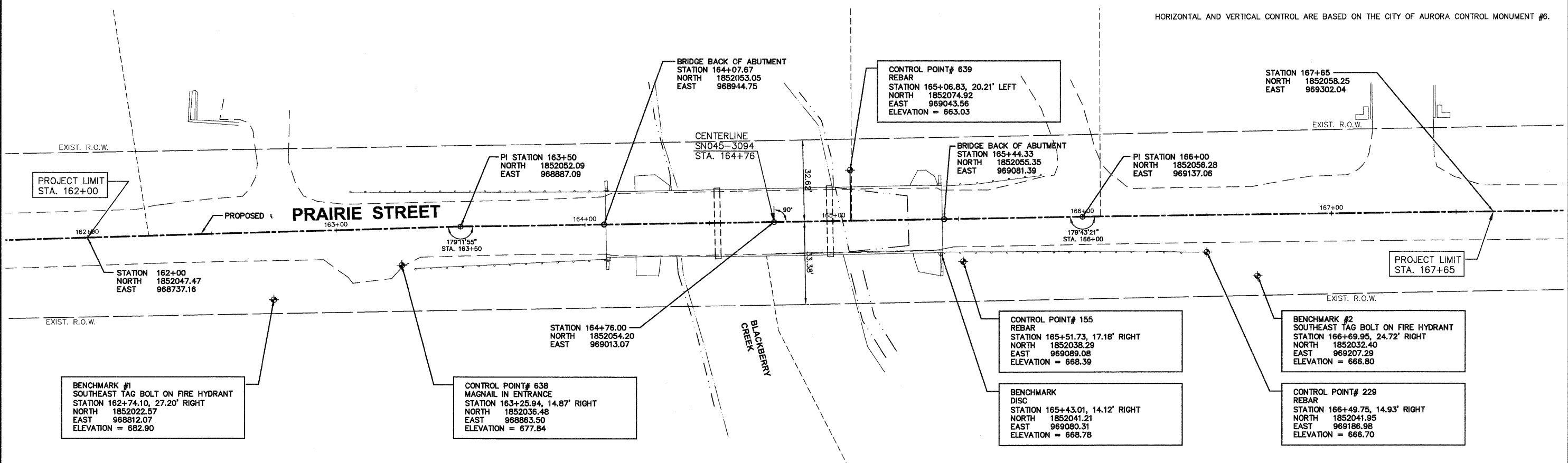
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS	
PROJECT NO. BH05-0089(134)	JOB NO. C-91-167-09
SCALE: N/A	SHEET NO. 1 OF 1 SHEETS STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3895	08-15104-01-BR	KANE	38	6
CONTRACT NO. 63661				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



HORIZONTAL AND VERTICAL CONTROL ARE BASED ON THE CITY OF AURORA CONTROL MONUMENT #6.



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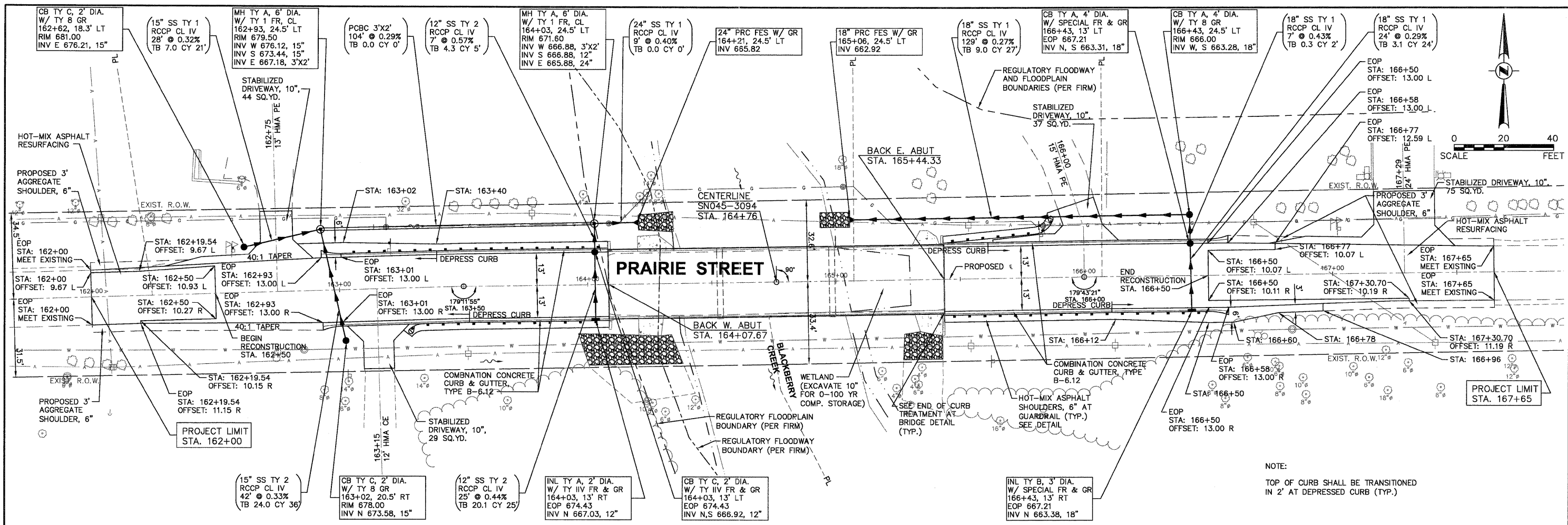
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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

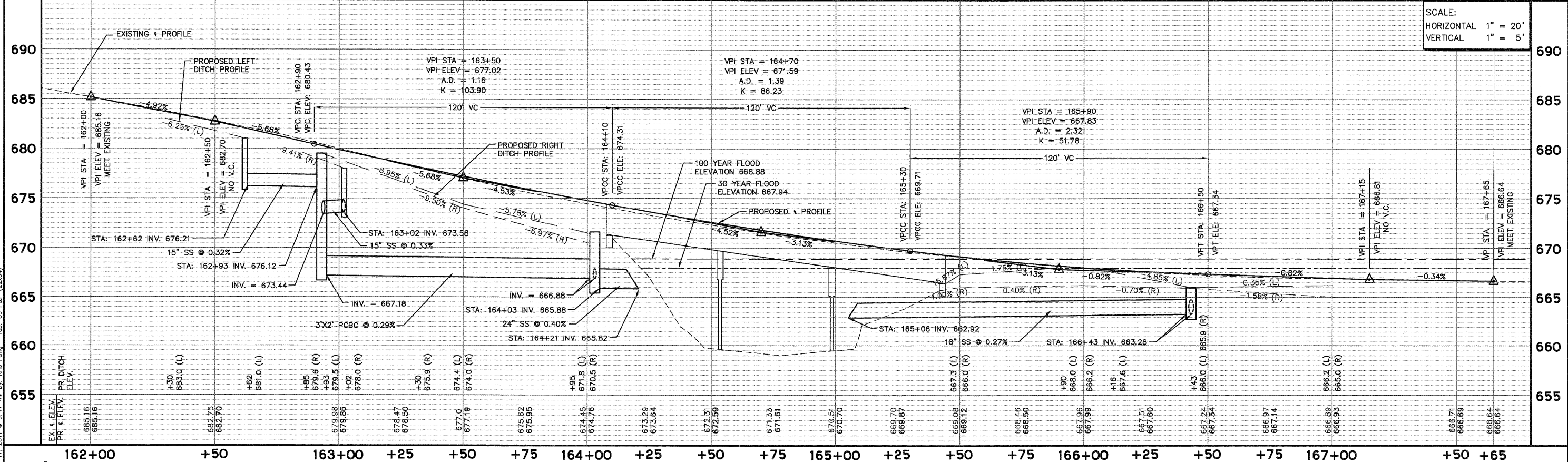
ALIGNMENT, TIES AND BENCHMARKS	
PROJECT NO. BH05-0089 (134)	JOB NO. C-91-167-09
SHEET NO. 1 OF 1 SHEETS	STA. 161+80 TO STA. 167+80

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3895	08-15104-01-BR	KANE	38	7
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63661	





NOTE:  
TOP OF CURB SHALL BE TRANSITIONED  
IN 2' AT DEPRESSED CURB (TYP.)



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 FILE NAME: BHOS-0089-134-01-167-09  
 FILE#: 08-15104-01-BR  
 PLOT SCALE = 1"=20'  
 PLOT DATE = 11/17/11

DESIGNED -	REVISOR -
DRAWN -	REVISOR -
CHECKED -	REVISOR -
DATE -	REVISOR -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

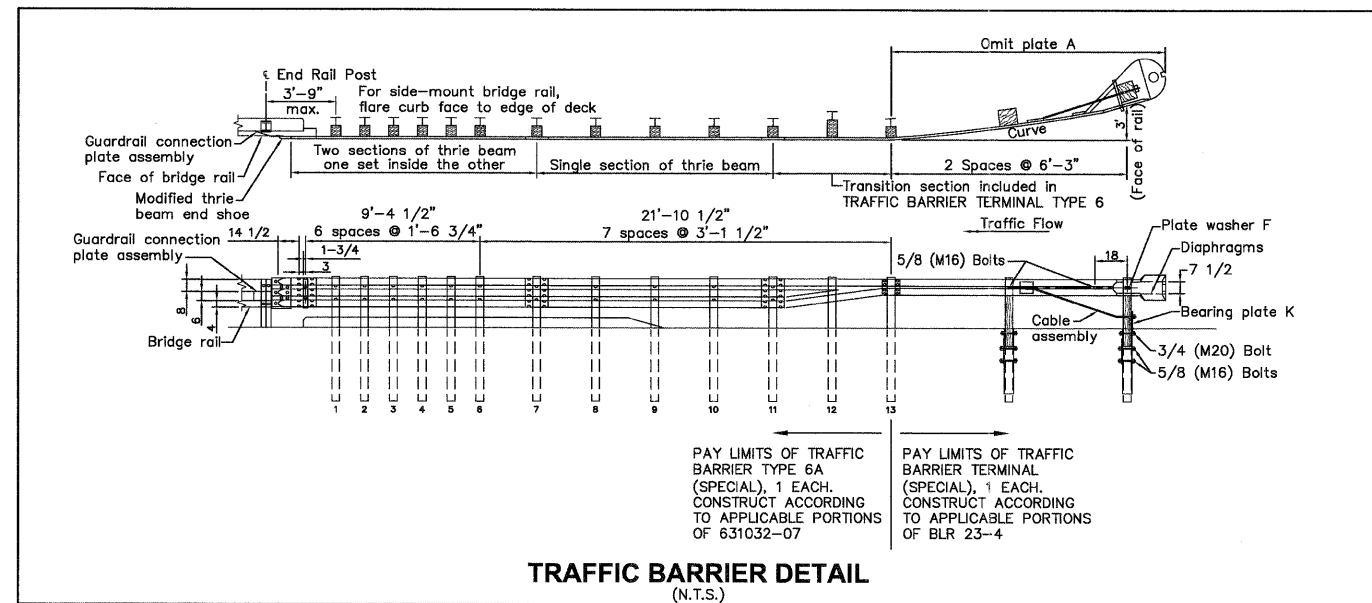
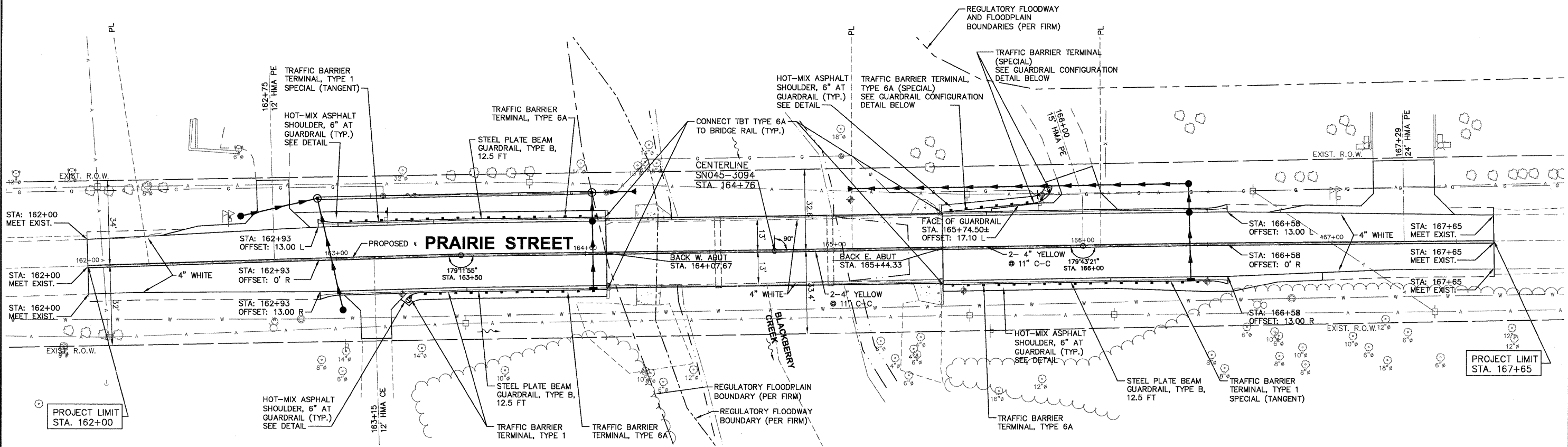
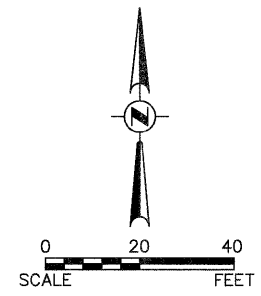
<b>PLAN AND PROFILE</b>	
PROJECT NO. BHOS-0089 (134)	JOB NO. C-91-167-09
SHEET NO. 1 OF 1 SHEETS	STA. 161+80 TO STA. 167+80

F.A.U. R.T.E. 3895	SECTION 08-15104-01-BR	COUNTY KANE	TOTAL SHEETS 38	SHEET NO. 9
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63661	

Printed: November 17, 2011 @ 9:47 AM By: Kris Pung - Job: 09 P&E (22-34)  
 PLOT DATE = 11/17/11

PLOT DATE = 11/17/11  
 PLOT SCALE = 1"=20'





**NOTE:**  
ALL PAVEMENT MARKING  
TO BE URETHANE

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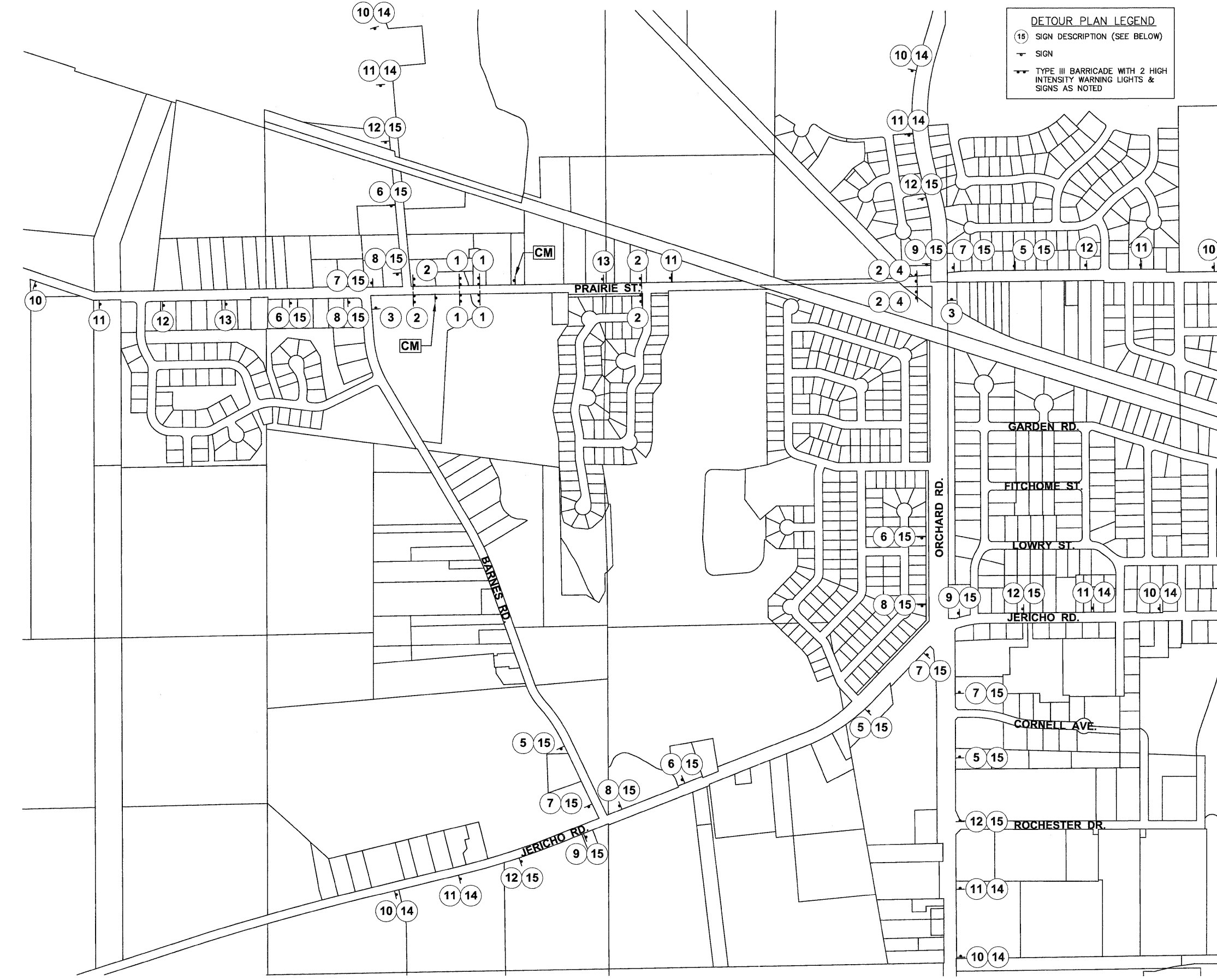
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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**GUARDRAIL AND PAVEMENT MARKING PLAN**

PROJECT NO. BHOS-0089 (134)	JOB NO. C-91-167-09
SHEET NO. 1 OF 1 SHEETS	STA. 161+80 TO STA. 167+80

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3895	08-15104-01-BR	KANE	38	10
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63661	



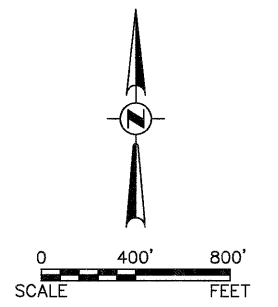
**DETOUR PLAN LEGEND**

15 SIGN DESCRIPTION (SEE BELOW)

— SIGN

— TYPE III BARRICADE WITH 2 HIGH INTENSITY WARNING LIGHTS & SIGNS AS NOTED

- NOTES:
1. LOCATIONS OF ALL SIGNS ARE APPROXIMATE. FINAL LOCATION SHALL BE BASED ON FIELD CONDITIONS AND AS APPROVED BY THE ENGINEER. COORDINATION WITH THE ORCHARD ROAD WIDENING IMPROVEMENTS SOUTH OF JERICO ROAD MAY BE REQUIRED.
  2. SIGN SPACING = 500 FEET (TYP.), UNLESS NOTED OTHERWISE OR AS DIRECTED BY THE ENGINEER.
  3. STREET NAME SIGN (15) TO BE PLACED ABOVE DETOUR SIGN.
  4. STREET SIGN (14) TO BE PLACED BELOW WARNING SIGN.
  5. CHANGEABLE MESSAGE SIGNS (CM) TO BE INSTALLED A MINIMUM OF TWO WEEKS IN ADVANCE OF ANY CONSTRUCTION WORK NOTIFYING PUBLIC OF PENDING ROAD CLOSURE.











1	<b>ROAD CLOSED</b> R11-2 48X30 ON TYPE III BARRICADE W/ FLASHING LIGHT (3 ACROSS ROAD)	9	<b>DETOUR</b> M4-9 30X30
2	<b>ROAD CLOSED TO THRU TRAFFIC</b> R11-4 60X30 ON TYPE III BARRICADE W/ FLASHING LIGHT (2 STAGGERED ACROSS ROAD)	10	<b>ROAD CONSTRUCTION AHEAD</b> W20-1 36X36
3	<b>END DETOUR</b> M4-8A 24X18	11	<b>ROAD CLOSED AHEAD</b> W20-3 36X36
4	<b>DETOUR</b> M4-10L 48X18	12	<b>DETOUR AHEAD</b> W20-2 36X36
5	<b>DETOUR</b> M4-9L 30X30	13	<b>ROAD CLOSED 1000 FT</b> W20-3 36X36
6	<b>DETOUR</b> M4-9R 30X30	14	<b>PRAIRIE ST. BARNES RD. TO ORCHARD RD.</b> 36X24
7	<b>DETOUR</b> M4-9L 30X24	15	<b>PRAIRIE ST</b> 30X18
8	<b>DETOUR</b> M4-9R 30X24		<b>CHANGEABLE MESSAGE SIGN</b> CM

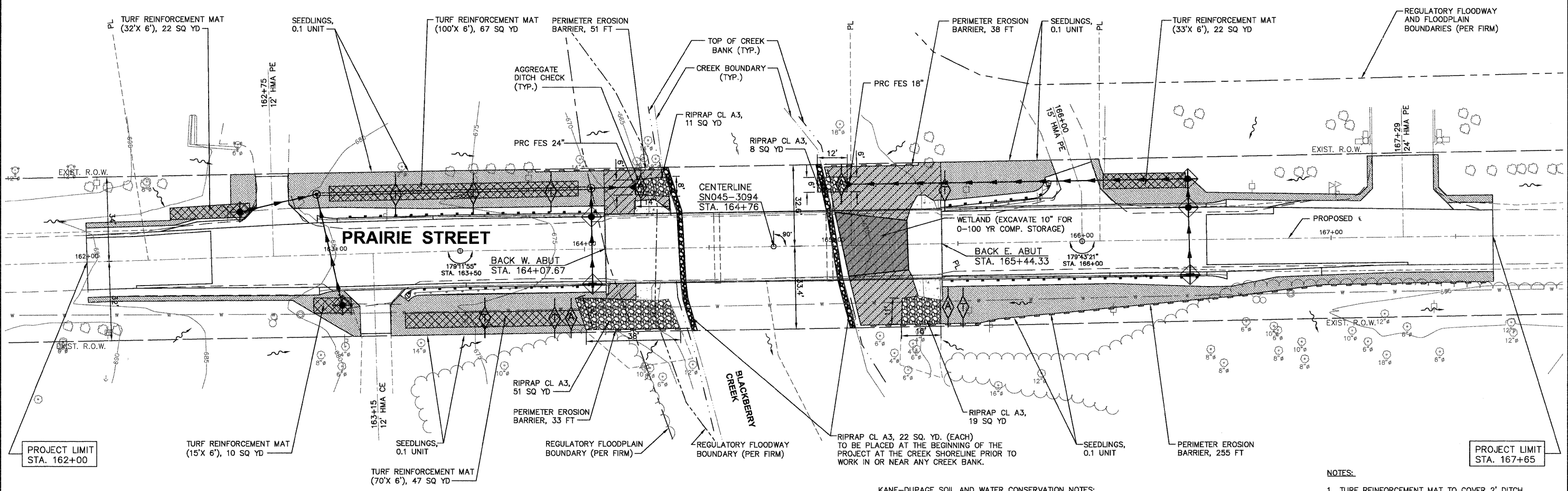
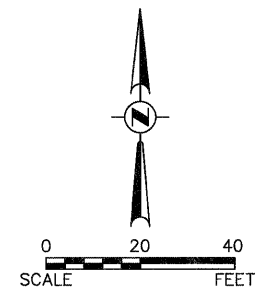
Printed: November 21, 2011 @ 1:49 PM By: Kris Pung - Tab: 11 Detour (22x34)  
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PLOT SCALE = #SCALE#				CHECKED -				REVISED -				PROJECT NO. BHOS-0089(134)												JOB NO. C-91-167-09				CONTRACT NO. 63661					
PLOT DATE = #DATE#				DATE -				REVISED -				SCALE: 1"=400'												SHEET NO. 1 OF 1 SHEETS				STA. N/A TO STA. N/A				FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT	

Path: H:\S\SR\PROJ\K0812\DWG\DWG\_FINAL\ENG\K0812-DETOUR

**LEGEND**

-  SEEDING - CLASS 2A, FERTILIZER & EROSION CONTROL BLANKET
-  SEEDING - CLASS 4B, FERTILIZER & EROSION CONTROL BLANKET
-  SEEDING - CLASS 2A, FERTILIZER & TURF REINFORCEMENT MAT
-  PROPOSED RIPRAP
-  PERIMETER EROSION BARRIER
-  TEMPORARY DITCH CHECK
-  AGGREGATE DITCH CHECK W/ FILTER FABRIC
-  INLET AND PIPE PROTECTION



**NOTES:**

1. TURF REINFORCEMENT MAT TO COVER 2' DITCH BOTTOM AND 2' UP EACH SIDE SLOPE (TYP.).
2. FINAL PLACEMENT OF SEEDLINGS TO BE VERIFIED/DETERMINED BY ENGINEER.
3. SEEDLINGS SHALL BE OF THE OAK SPECIES.
4. TEMPORARY DITCH CHECKS SHALL BE ROLLED EXCELSIOR AND PLACED AFTER INSTALLATION OF TURF REINFORCEMENT MAT, SEEDING, AND EROSION CONTROL BLANKET.
5. INLET FILTERS SHALL BE USED AT CURB STRUCTURES FOR INLET AND PIPE PROTECTION.
6. INLET AND PIPE PROTECTION SHALL BE SILT FILTER FABRIC TYPE.
7. FILTER FABRIC SHALL BE USED AT ALL RIP RAP AREAS.
8. NO STOCKPILES SHALL BE ALLOWED IN THE FLOODPLAIN.
9. AGGREGATE DITCH CHECKS WITH FILTER FABRIC SHALL BE INSTALLED IMMEDIATELY AFTER INITIAL GRADING ACTIVITIES. FILTER FABRIC WILL BE PAID FOR SEPARATELY.

**KANE-DUPAGE SOIL AND WATER CONSERVATION NOTES:**

UNLESS OTHERWISE INDICATED, ALL VEGETATIVE AND STRUCTURAL EROSION AND SEDIMENT CONTROL PRACTICES WILL BE CONSTRUCTED ACCORDING TO MINIMUM STANDARDS AND SPECIFICATIONS IN THE ILLINOIS URBAN MANUAL, LATEST EDITION.

THE KANE-DUPAGE SOIL AND WATER CONSERVATION DISTRICT (KDSWCD) MUST BE NOTIFIED ONE WEEK PRIOR TO THE PRE-CONSTRUCTION CONFERENCE, ONE WEEK PRIOR TO THE COMMENCEMENT OF LAND DISTURBING ACTIVITIES, AND ONE WEEK PRIOR TO THE FINAL INSPECTION. A COPY OF THE APPROVED EROSION AND SEDIMENT CONTROL PLAN SHALL BE MAINTAINED ON THE SITE AT ALL TIMES.

PRIOR TO COMMENCING LAND-DISTURBING ACTIVITIES IN AREAS OTHER THAN INDICATED ON THESE PLANS (INCLUDING BUT NOT LIMITED TO, ADDITIONAL PHASES OF DEVELOPMENT AND OFF-SITE BORROW OR WASTE AREAS) A SUPPLEMENTARY EROSION CONTROL PLAN SHALL BE SUBMITTED TO THE OWNER FOR REVIEW BY THE KDSWCD.

THE CONTRACTOR IS RESPONSIBLE FOR INSTALLATION OF ANY ADDITIONAL EROSION CONTROL MEASURES NECESSARY TO PREVENT EROSION AND SEDIMENTATION AS DETERMINED BY THE KDSWCD.

DURING DEWATERING OPERATIONS, WATER WILL BE PUMPED INTO SEDIMENT BASINS OR SILT TRAPS. DEWATERING DIRECTLY INTO FIELD TILES OR STORMWATER STRUCTURES IS PROHIBITED.

IT IS THE RESPONSIBILITY OF THE LANDOWNER AND/OR GENERAL CONTRACTOR TO INFORM ANY SUB-CONTRACTOR(S) WHO MAY PERFORM WORK ON THIS PROJECT, OF THE REQUIREMENTS IN IMPLEMENTING AND MAINTAINING THESE EROSION CONTROL PLANS AND THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT REQUIREMENTS SET FORTH BY THE ILLINOIS EPA.

SEDIMENT CONTROL MEASURES WILL BE SELECTED BY CONTRACTOR, METHODS APPROVED BY ENGINEER AND THE KDSWCD.

SEDIMENT CONTROL IS PLACED AT THE SHORELINE OF BLACKBERRY CREEK AT THE LOCATION OF THAT DAYS CONSTRUCTION AND INSTALLED ACCORDING TO THE DETAIL.

SEDIMENT CONTROL MEASURE WILL BE INSPECTED FREQUENTLY AND MAINTAINED WHEN NECESSARY. ADDITIONAL SEDIMENT CONTROL WILL BE ADDED IF CREEK FLOW APPEARS TURBID.

ALL DISTURBED AREAS AND WORK AREAS MUST BE ISOLATED FROM CREEK FLOWS AT ALL TIMES. THE DIVERSION/ISOLATION OF THE CREEK FLOWS MUST BE CONSTRUCTED FROM NON-ERODIBLE MATERIALS. THE KDSWCD MUST BE IN AGREEMENT WITH OVERALL EXACT METHOD OF DIVERSION/ISOLATION PRIOR TO THE COMMENCEMENT OF CONSTRUCTION.

WORK MUST BE TIMED TO TAKE PLACE DURING LOW OR NO-FLOW CONDITIONS. EQUIPMENT SHOULD WORK FROM THE BANK. NO WORK SHALL BE PERFORMED IN FLOWING WATER. ONCE WORK IN THESE AREAS BEGINS, PRIORITY SHALL BE GIVEN TO THE COMPLETION OF THE WORK AND FINAL STABILIZATION OF ALL DISTURBED AREAS.

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FILE#		DRAWN -	REVISED -
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PLOT DATE = #DATE#		DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

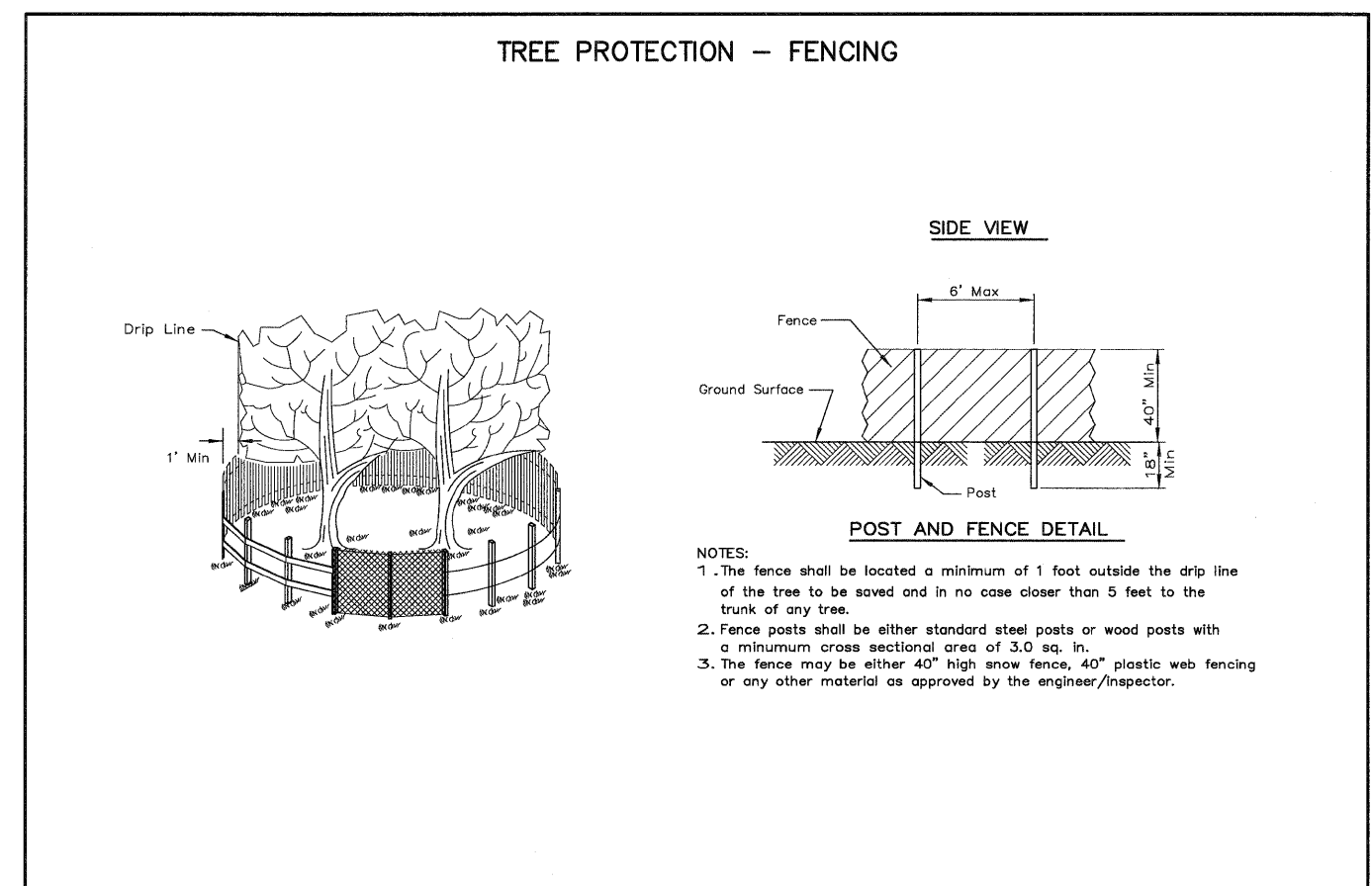
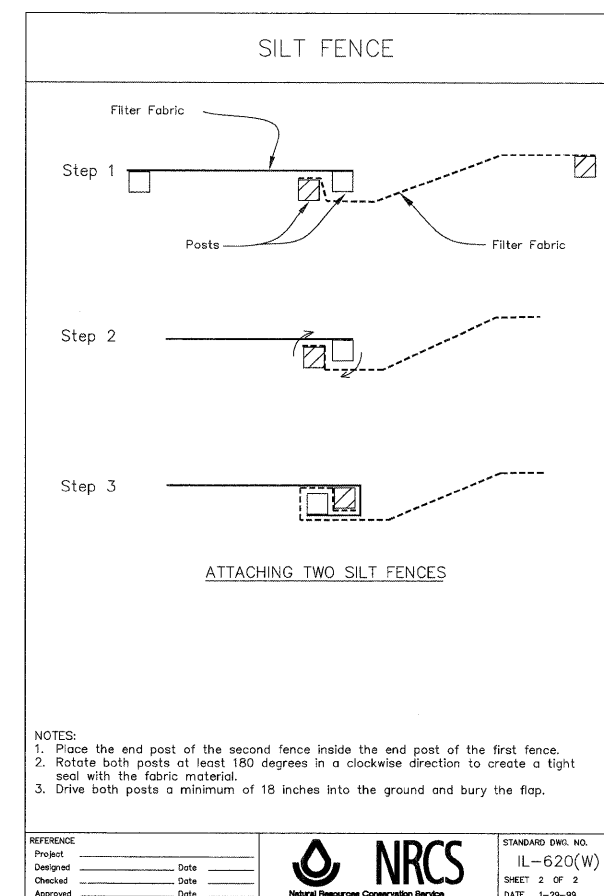
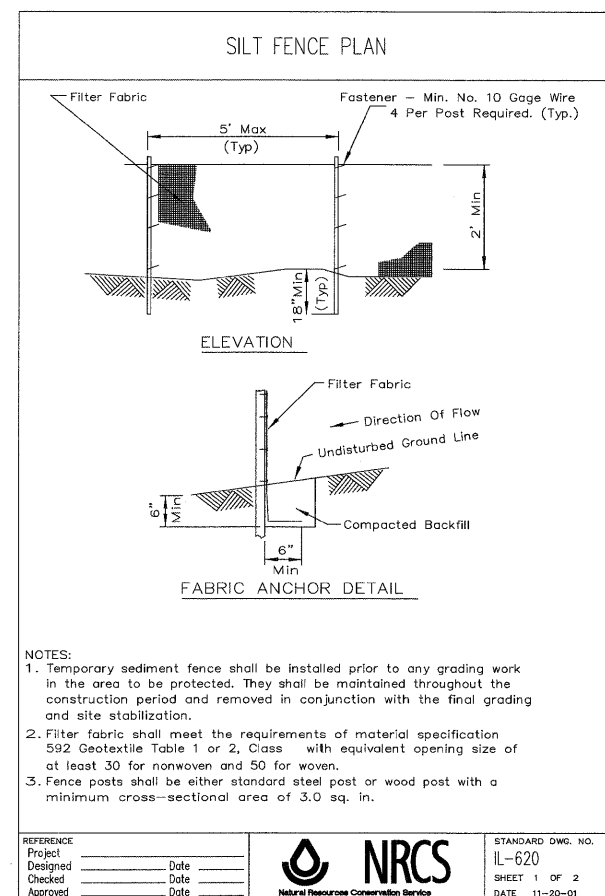
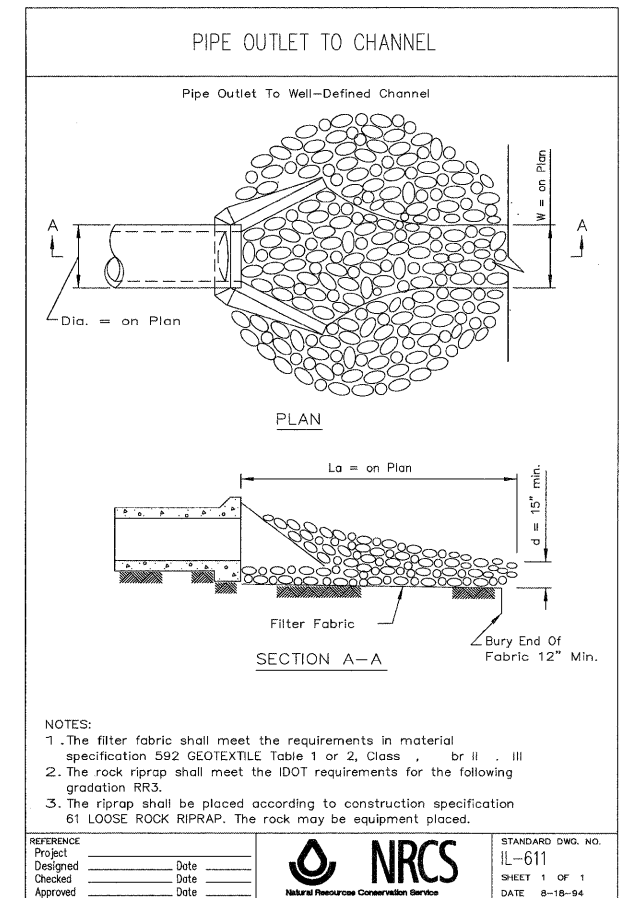
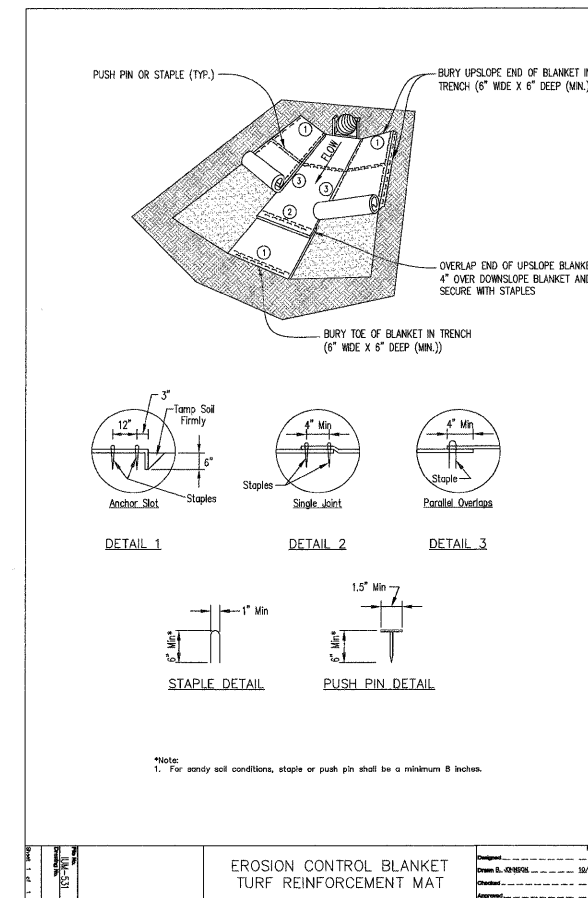
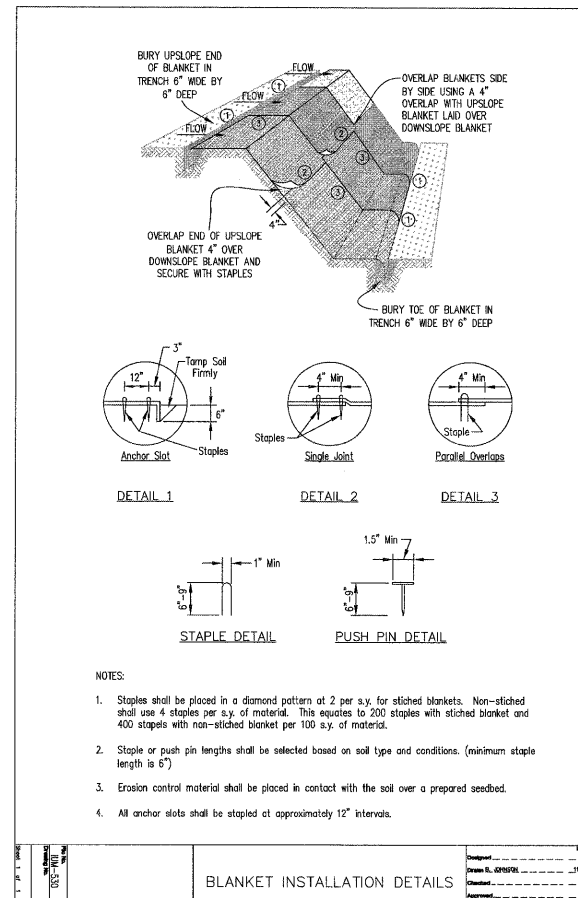
**EROSION CONTROL AND LANDSCAPE PLAN**

PROJECT NO. BHOS-0089 (134)	JOB NO. C-91-167-09
SHEET NO. 1 OF 2 SHEETS	STA. 161+80 TO STA. 167+80

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3895	08-15104-01-BR	KANE	38	12
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63661	

Plotted: December 20, 2011 @ 11:25 AM By: Kris Pung - Tab: 12 Erosion (22x34)  
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Path: H:\SUSKPROJ\K0812\DWG\DWG\_FINAL\ENG\K0812-EROSION



Plotted: December 21, 2011 @ 12:49 PM By: Kris Pung - Tab: 13 Erosion Details (22-34)  
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REFERENCE Project	_____ Date _____	 STANDARD DWG. NO. IL-620 SHEET 1 OF 2 DATE 11-20-01
Designed	_____ Date _____	
Checked	_____ Date _____	
Approved	_____ Date _____	

REFERENCE Project	_____ Date _____	 STANDARD DWG. NO. IL-620(W) SHEET 2 OF 2 DATE 1-29-99
Designed	_____ Date _____	
Checked	_____ Date _____	
Approved	_____ Date _____	

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

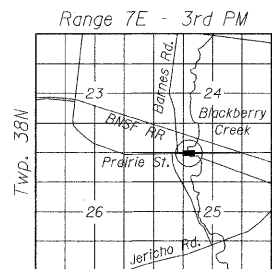
<b>EROSION CONTROL AND LANDSCAPE PLAN DETAILS</b>		F.A.J. RTE. 3895	SECTION 08-15104-01-BR	COUNTY KANE	TOTAL SHEETS 38	SHEET NO. 13
PROJECT NO. BHOS-0089(134)	JOB NO. C-91-167-09	SCALE: N/A		CONTRACT NO. 63661		
SHEET NO. 2 OF 2 SHEETS	STA. N/A TO STA. N/A	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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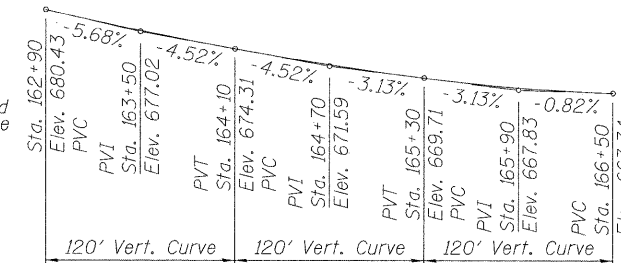
Bench Mark: Disc at South East Abutment Wingwall, Elev. 668.78

Existing Structure: Structure No. 045-3094, Built in 1972 as Section 104B-TR. The existing superstructure consists of 3 spans of 9 PPC Deck Beams with a back-to-back of abutments length of 136'-8", an out-to-out width of 27'-0", an HMA Wearing Surface and Type N Bridge Railing. The existing substructure consists of Bent Abutments on Timber Piles and Bent Piers on Precast Concrete Piles. The existing superstructure is to be removed and replaced with 7 PPC Deck Beams with an out-to-out width of 28'-0", a Concrete Wearing Surface and Type SM Bridge Railing. Traffic will be detoured during construction.

Salvage: No salvage.



LOCATION SKETCH



PROFILE GRADE  
(Along Centerline of Prairie St.)

LOADING HL-93 (NEW CONST.)

LOADING HS20-44 (EXIST. CONST.)

Allow 25#/sq. ft. for future wearing surface.

DESIGN SPECIFICATIONS

NEW CONSTRUCTION

2010 Interim Revisions to AASHTO LRFD Bridge Design Specifications, 5th Edition

EXISTING CONSTRUCTION

2002 AASHTO Standard Specifications for Highway Bridges

DESIGN STRESSES

FIELD UNITS (NEW CONSTRUCTION)

f'c = 3,500 psi  
fy = 36,000 psi  
fy = 60,000 psi (Reinforcement)

PRECAST PRESTRESSED UNITS

f'c = 6,000 psi  
f'ci = 5,000 psi  
fpu = 270,000 psi (1/2" Low Lax Strands)  
fpbt = 201,960 psi (1/2" Low Lax Strands)

FIELD UNITS (EXIST. CONSTRUCTION)

f'c = 3,500 psi  
fy = 40,000 psi (Reinforcement)

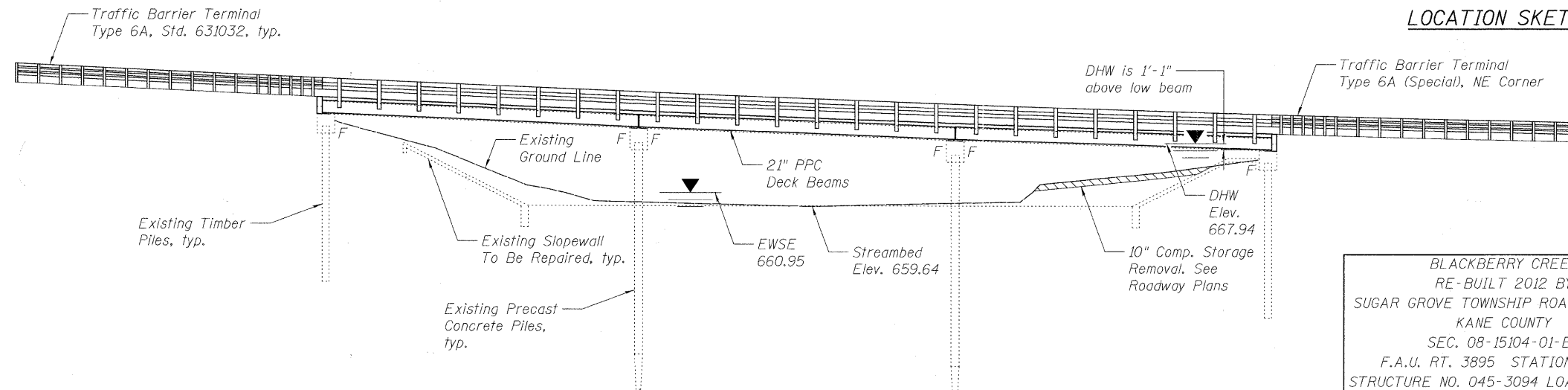
SEISMIC DATA

LRFD - NEW CONSTRUCTION

Seismic Performance Zone (SPZ) = 1  
Design Spectral Acceleration at 1.0 sec. (Sp1) = 0.096 g  
Design Spectral Acceleration at 0.2 sec. (Sps) = 0.192 g  
Soil Site Class = D

LFD - EXISTING CONSTRUCTION

Seismic Performance Category (SPC) = A  
Bedrock Acceleration Coefficient (A) = 0.04  
Site Coefficient (S) = 1.0

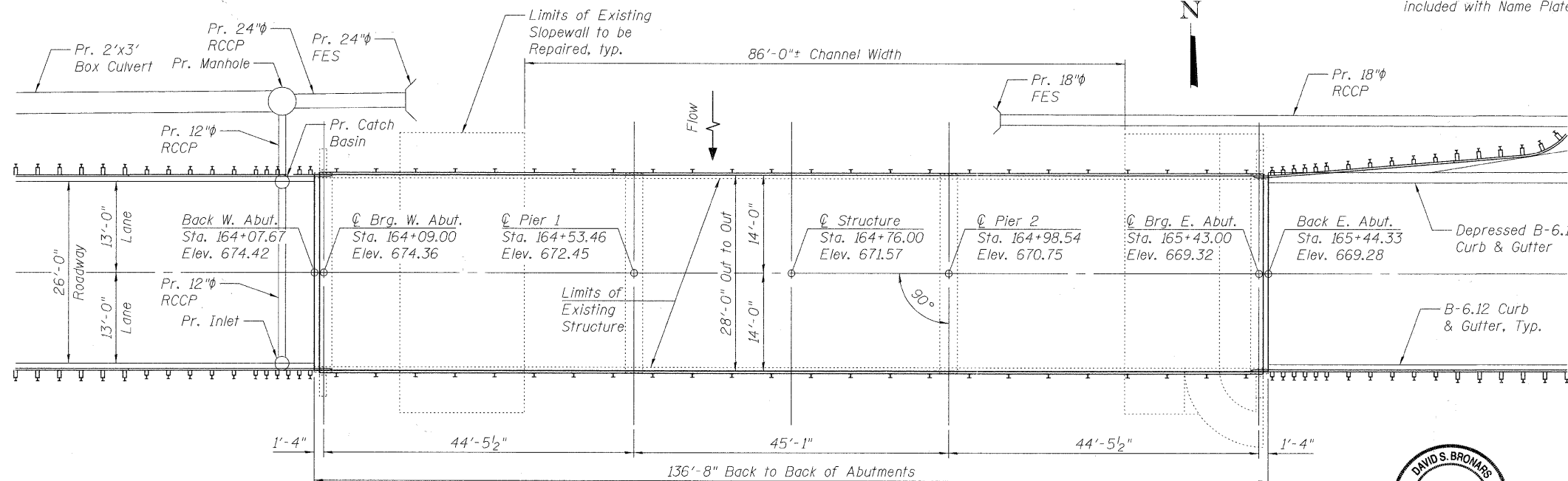


ELEVATION

BLACKBERRY CREEK  
RE-BUILT 2012 BY  
SUGAR GROVE TOWNSHIP ROAD DISTRICT  
KANE COUNTY  
SEC. 08-15104-01-BR  
F.A.U. RT. 3895 STATION 164+76  
STRUCTURE NO. 045-3094 LOADING HL-93

NAME PLATE

See Std. 515001  
Existing Name Plate shall be cleaned and relocated next to new Name Plate. Cost included with Name Plates



PLAN

WATERWAY INFORMATION

Drainage Area = 55.3 Sq. Miles		Low Grade Elev. 666.38 @ Sta. 168+48.06									
Flood Yr.	Freq.	Q, C.F.S.	Opening Sq. Ft.		Nat. H.W.E.	Head - Ft.		Headwater El.		Exist.	Prop.
			Exist.	Prop.		Exist.	Prop.	Exist.	Prop.		
10	1,496	640.15	640.15	667.15	0.03	0.03	667.18	667.18			
Design	30	2,109	707.53	707.53	667.88	0.06	0.06	667.94	667.94		
Base	100	2,952	772.15	772.15	668.78	0.10	0.10	668.88	668.88		
Overtopping	2.8	801	550.45	550.45	666.01	0.37	0.37	666.38	666.38		
Max. Calc.	500	4,287	853.13	853.13	670.09	0.10	0.10	670.19	670.19		

10 Year Velocity Through Existing Bridge = 2 ft/s  
10 Year Velocity Through Proposed Bridge = 2 ft/s

DESIGN SCOUR ELEVATION TABLE

Design Scour Elevation (ft.)	W. Abut.	Pier 1	Pier 2	E. Abut.
	-	653.27	654.33	658.37



David Bronars 12/15/2011  
Exp 11/30/2012

I certify that to the best of my knowledge, information and belief, this bridge/box culvert is designed using the loads shown on the plans and consistent with that degree of care and skill ordinarily exercised by members of the same profession. The design is reasonably-priced for the style of structure and complies with requirements of the current AASHTO Standard Specifications for Highway Bridges.

GENERAL PLAN & ELEVATION  
PRAIRIE ST. OVER BLACKBERRY CREEK

F.A.U. ROUTE 3895

SECTION 08-15104-01-BR

KANE COUNTY

STATION 164+76.00

STRUCTURE NO. 045-3094

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN & ELEVATION  
STRUCTURE NO. 045-3094

SHEET NO. 1 OF 17 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3895	08-15104-01-BR	KANE	38	14
CONTRACT NO. 63661			ILLINOIS FED. AID PROJECT	

PLOT DATE = 12/15/2011  
FILE NAME = \\A:\113-2245-008 Prairie Street\CAD\SS\Sheets\IP\_Plotter - Prairie 81 - General Plan Elevation.dgn  
USER NAME = mst-utl



DESIGNED - LM	REVISED -
CHECKED - GC	REVISED -
DRAWN - LM	REVISED -
CHECKED - DSB	REVISED -



**GENERAL NOTES**

1. Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.
2. Reinforcement bars designated (E) shall be epoxy coated.
3. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
4. Existing dowel bars shall be burned off flush with the existing substructure. Grind existing dowel bars smooth and seal with epoxy. Cost included with Removal of Existing Superstructure.
5. Removal of the existing bridge rail shall be included in the cost of Removal of Existing Superstructure.
6. Removal of any existing pavement and base material located above the existing abutments shall be included with the cost of Removal of Existing Superstructures.

**INDEX OF SHEETS**

1. General Plan & Elevation
2. General Data
- 3.-4. Top of Deck Elevations
5. Superstructure
6. Superstructure Details
7. Steel Railing Type SM
8. PPC Deck Beam
9. PPC Deck Beam Details
10. Abutments and Wingwalls
11. Pier Repairs
12. Slopewall Repairs
- 13.-17. Existing Plans

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	SUPER	SUB	TOTAL
Removal of Existing Superstructures	EACH	1		1
Concrete Removal	CU YD		1.9	1.9
Slope Wall Removal	SQ YD		1	1
Concrete Structures	CU YD		3.9	3.9
Protective Coat	SQ YD	426		426
Precast Prestressed Concrete Deck Beams (21" Depth)	SQ FT	3,780		3,780
Furnishing and Erecting Structural Steel	POUND	2,220		2,220
Reinforcement Bars, Epoxy Coated	POUND	5,670	240	5,910
Steel Railing, Type SM	FOOT	271		271
Slope Wall 6 inch	SQ YD		1	1
Name Plates	EACH	1		1
Epoxy Crack Injection	FOOT		99	99
Concrete Wearing Surface, 5"	SQ YD	421		421
Asbestos Bearing Pad Removal	EACH	36		36
Structural Repair of Concrete (Depth Equal To or Less Than 5 in.)	SQ FT		10	10

PLOT DATE = 12/6/2011  
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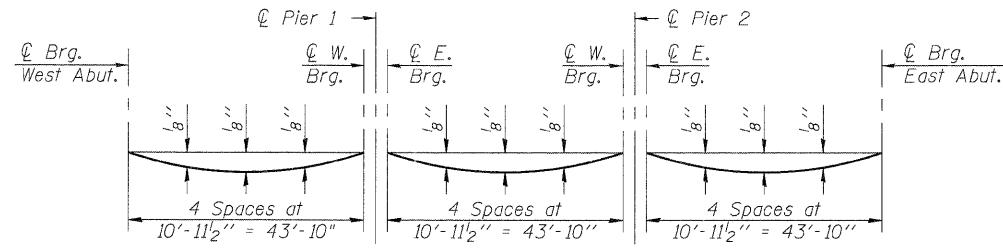
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CHECKED - GG	REVISED - _____
DRAWN - LM	REVISED - _____
CHECKED - DSB	REVISED - _____

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**GENERAL DATA**  
**STRUCTURE NO. 045-3094**

SHEET NO. 2 OF 17 SHEETS

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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			CONTRACT NO. 63661	
ILLINOIS FED. AID PROJECT				

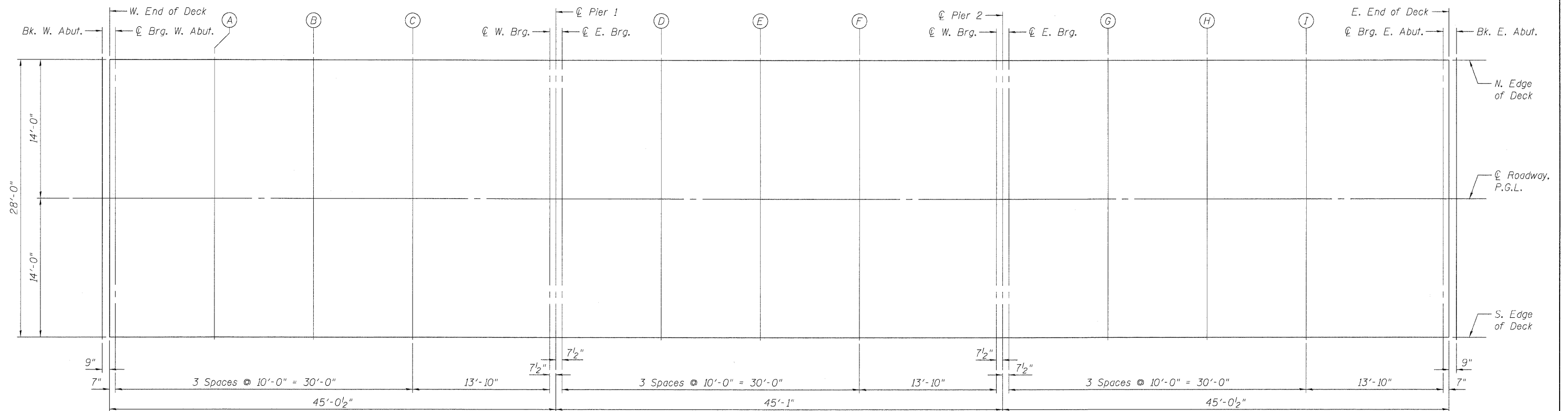


**DEAD LOAD DEFLECTION DIAGRAM**

(Includes weight of concrete, excluding beams).

Note:

The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown on sheet 4 of 17.



**PLAN**

PLOT DATE = 12/2/2011  
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 USER NAME = merrin



DESIGNED - LM	REVISED -
CHECKED - GG	REVISED -
DRAWN - LM	REVISED -
CHECKED - DSB	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TOP OF DECK ELEVATIONS (1 OF 2)  
STRUCTURE NO. 045-3094**

SHEET NO. 3 OF 17 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3895	08-15104-01-BR	KANE	38	16
CONTRACT NO. 63661			ILLINOIS FED. AID PROJECT	

NORTH EDGE OF DECK

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Bk. W. Abut.	164+07.67	-14.00	674.20	674.20
W. End of Deck	164+08.42	-14.00	674.16	674.16
CL Brg. W. Abut	164+09.00	-14.00	674.14	674.14
A	164+19.00	-14.00	673.69	673.70
B	164+29.00	-14.00	673.25	673.26
C	164+39.00	-14.00	672.83	672.84
CL W. Brg.	164+52.83	-14.00	672.26	672.26
CL Pier 1	164+53.46	-14.00	672.23	672.23
CL E. Brg.	164+54.08	-14.00	672.21	672.21
D	164+64.08	-14.00	671.81	671.82
E	164+74.08	-14.00	671.43	671.44
F	164+84.08	-14.00	671.05	671.06
CL W. Brg.	164+97.92	-14.00	670.56	670.56
CL Pier 2	164+98.54	-14.00	670.53	670.53
CL E. Brg.	164+99.17	-14.00	670.51	670.51
G	165+09.17	-14.00	670.17	670.18
H	165+19.17	-14.00	669.84	669.85
I	165+29.17	-14.00	669.52	669.53
CL Brg. E. Abut.	165+43.00	-14.00	669.10	669.10
E. End of Deck	165+43.58	-14.00	669.08	669.08
Bk. E. Abut.	165+44.33	-14.00	669.06	669.06

☉ ROADWAY, P.G.L.

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Bk. W. Abut.	164+07.67	0.00	674.42	674.42
W. End of Deck	164+08.42	0.00	674.38	674.38
CL Brg. W. Abut	164+09.00	0.00	674.36	674.36
A	164+19.00	0.00	673.91	673.92
B	164+29.00	0.00	673.47	673.48
C	164+39.00	0.00	673.04	673.05
CL W. Brg.	164+52.83	0.00	672.48	672.48
CL Pier 1	164+53.46	0.00	672.45	672.45
CL E. Brg.	164+54.08	0.00	672.42	672.42
D	164+64.08	0.00	672.03	672.04
E	164+74.08	0.00	671.64	671.65
F	164+84.08	0.00	671.27	671.28
CL W. Brg.	164+97.92	0.00	670.78	670.78
CL Pier 2	164+98.54	0.00	670.75	670.75
CL E. Brg.	164+99.17	0.00	670.73	670.73
G	165+09.17	0.00	670.39	670.40
H	165+19.17	0.00	670.06	670.07
I	165+29.17	0.00	669.74	669.75
CL Brg. E. Abut.	165+43.00	0.00	669.32	669.32
E. End of Deck	165+43.58	0.00	669.30	669.30
Bk. E. Abut.	165+44.33	0.00	669.28	669.28

SOUTH EDGE OF DECK

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Bk. W. Abut.	164+07.67	14.00	674.20	674.20
W. End of Deck	164+08.42	14.00	674.16	674.16
CL Brg. W. Abut	164+09.00	14.00	674.14	674.14
A	164+19.00	14.00	673.69	673.70
B	164+29.00	14.00	673.25	673.26
C	164+39.00	14.00	672.83	672.84
CL W. Brg.	164+52.83	14.00	672.26	672.26
CL Pier 1	164+53.46	14.00	672.23	672.23
CL E. Brg.	164+54.08	14.00	672.21	672.21
D	164+64.08	14.00	671.81	671.82
E	164+74.08	14.00	671.43	671.44
F	164+84.08	14.00	671.05	671.06
CL W. Brg.	164+97.92	14.00	670.56	670.56
CL Pier 2	164+98.54	14.00	670.53	670.53
CL E. Brg.	164+99.17	14.00	670.51	670.51
G	165+09.17	14.00	670.17	670.18
H	165+19.17	14.00	669.84	669.85
I	165+29.17	14.00	669.52	669.53
CL Brg. E. Abut.	165+43.00	14.00	669.10	669.10
E. End of Deck	165+43.58	14.00	669.08	669.08
Bk. E. Abut.	165+44.33	14.00	669.06	669.06

USER NAME = marston

PLT DATE = 12/16/2011  
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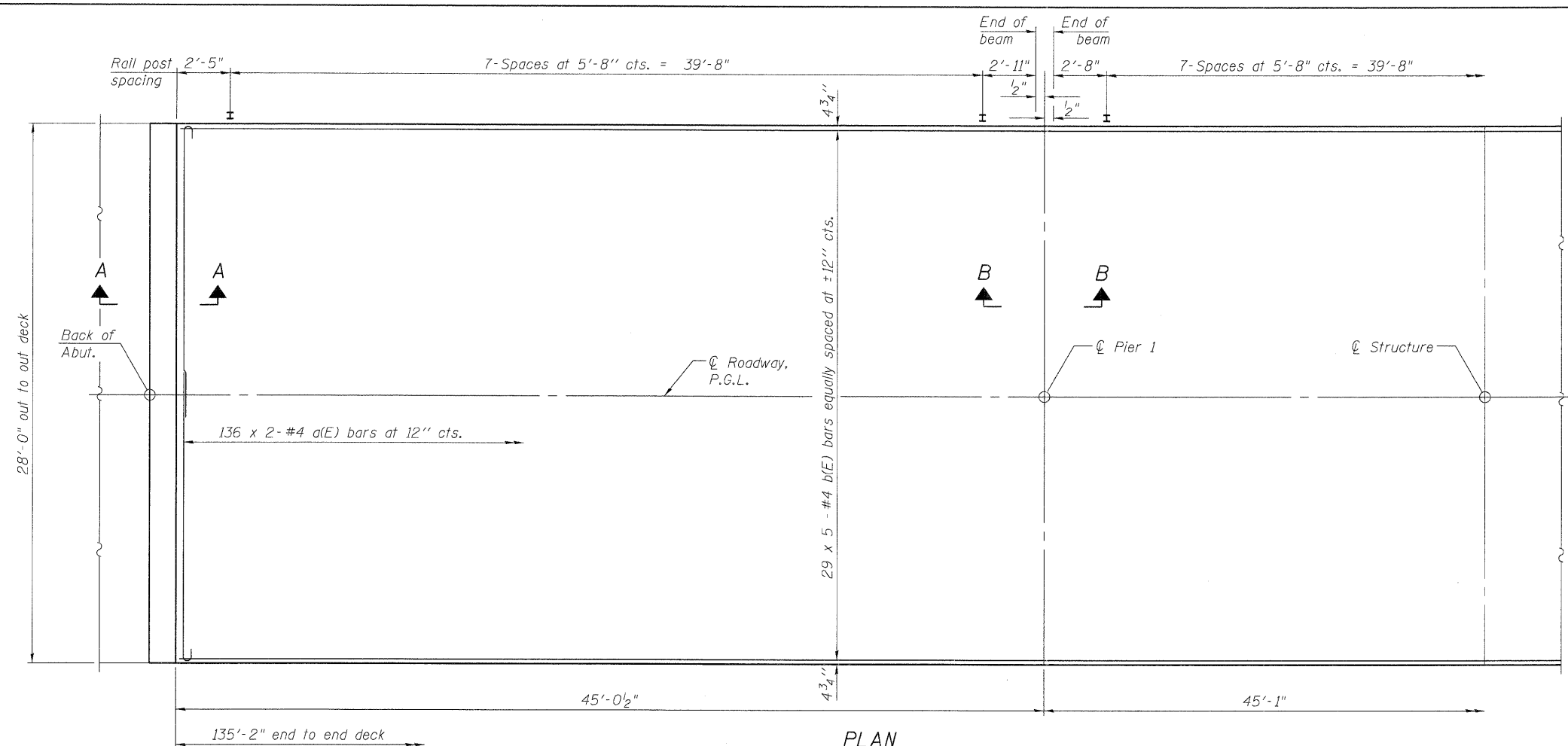
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CHECKED - GG	REVISED - _____
DRAWN - LM	REVISED - _____
CHECKED - DSB	REVISED - _____

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

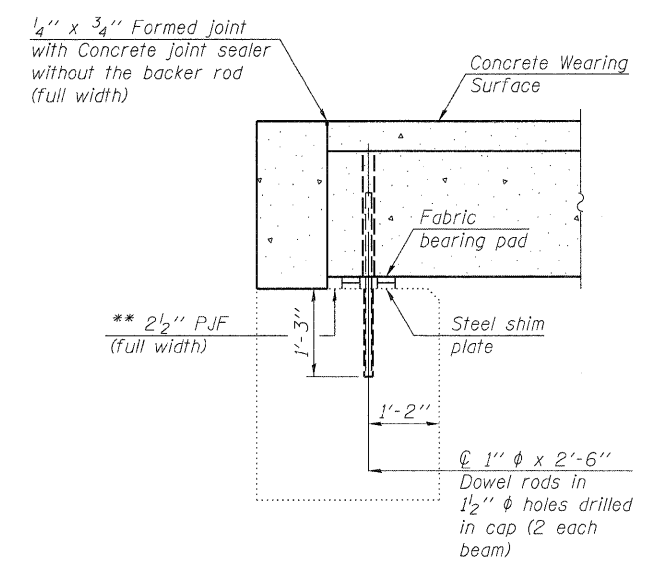
TOP OF DECK ELEVATIONS (2 OF 2)  
 STRUCTURE NO. 045-3094

SHEET NO. 4 OF 17 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3895	08-15104-01-BR	KANE	38	17
CONTRACT NO. 63661			ILLINOIS FED. AID PROJECT	

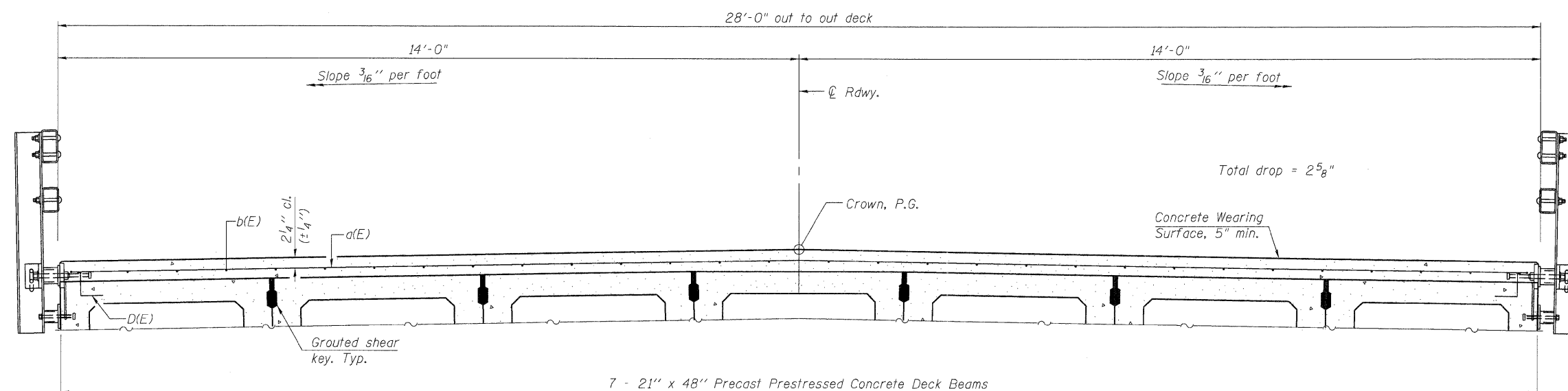


**PLAN**  
(Symmetrical about  $\text{C}_{\text{Structure}}$ )



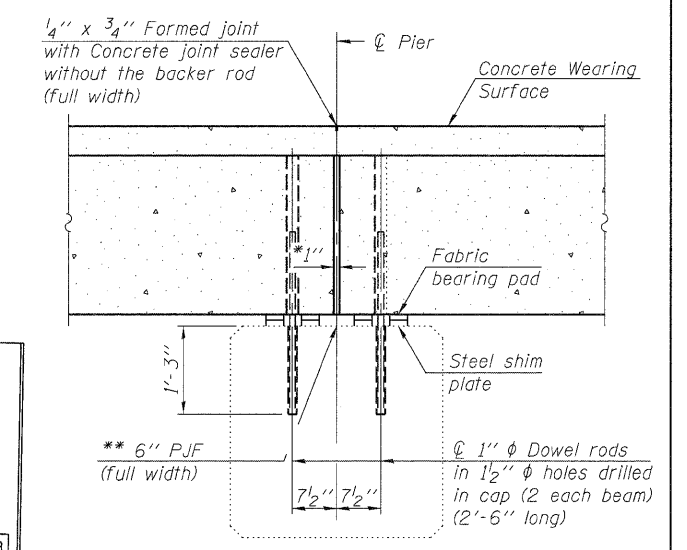
**SECTION A-A**

Notes:  
All concrete wearing surfaces shall be placed prior to casting a backwall and/or approach slab.  
See sheet 9 of 17 for fabric bearing pad and steel shim plate details.



**CROSS SECTION**  
(Looking East)

Notes:  
See sheet 6 of 17 for Superstructure Details and Bill of Material.  
Bars indicated thus 29 x 5-#4 etc. indicates 29 lines of bars with 5 lengths per line.



**SECTION B-B**

\* 1" Jt. shall be filled with non-shrink grout. 1" dimension may vary to accommodate tolerance in beam lengths.  
\*\* The height of the joint filling material shall vary across the length of the substructure to meet the height of the Fabric bearing pad and Steel shim plate. At the piers, where two bearings are present, the PJF height shall match the taller of the two.

**MINIMUM BAR LAP**  
#4 bar = 2'-7"

USER NAME = meertol  
 PLOT DATE = 12/2/2011  
 FILE NAME = X:\N\13-2245-9081 Prairie Street\CAD\N\Sheets\Prairie BS - Superstructure.dgn



DESIGNED - LM	REVISÉ -
CHECKED - GG	REVISÉ -
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CHECKED - DSB	REVISÉ -

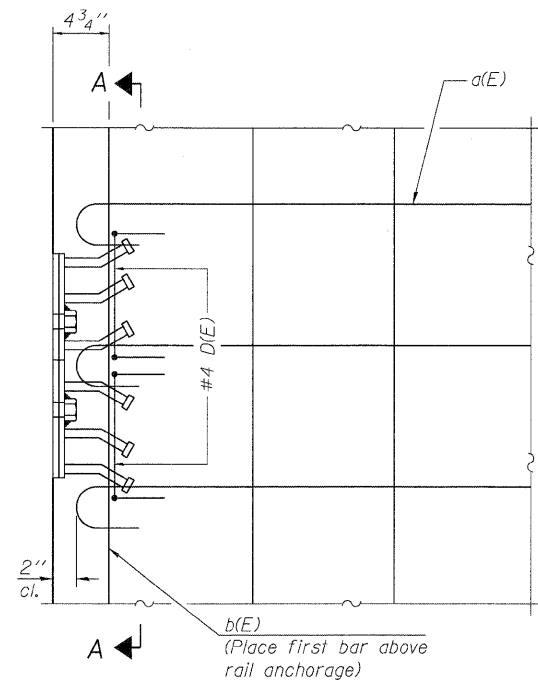
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**SUPERSTRUCTURE**  
**STRUCTURE NO. 045-3094**

SHEET NO. 5 OF 17 SHEETS

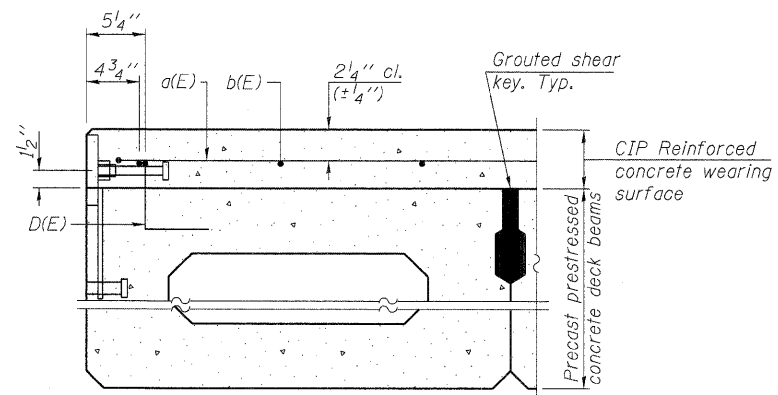
F.A.U. RTE. 3895	SECTION 08-15104-01-BR	COUNTY KANE	TOTAL SHEETS 38	SHEET NO. 18
CONTRACT NO. 63661			ILLINOIS FED. AID PROJECT	

PLOT DATE = 12/12/2011  
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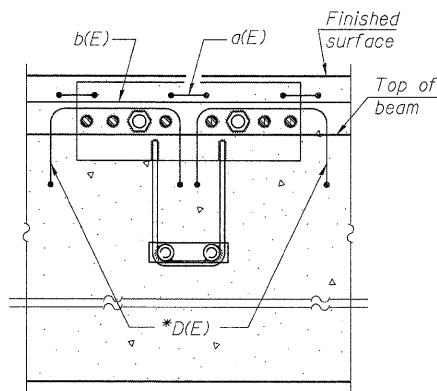


**PLAN**

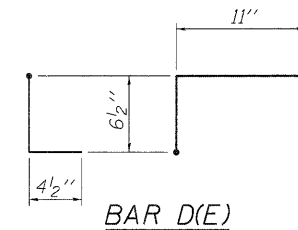
Notes:  
 Formwork necessary for the wearing surface may be secured utilizing the bottom rail anchorage inserts and/or additional inserts cast into the beam.



**SECTION THRU FASCIA BEAM**

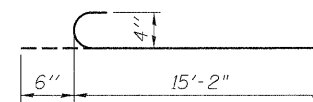


**SECTION A-A**



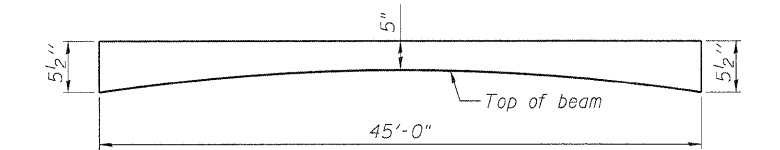
**BAR D(E)**

\* Place 2-#4 D(E) bars in beam at each post location as shown. D(E) bar included in cost of beam.



**BAR a(E)**

**MINIMUM BAR LAP**  
 #4 bar = 2'-7"



**ANTICIPATED CONCRETE WEARING SURFACE PROFILE**  
 (For information only)

**SUPERSTRUCTURE BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a(E)	272	#4	15'-8"	C
b(E)	145	#4	29'-1"	—
Reinforcement Bars, Epoxy Coated		Pound	5,670	
Concrete Wearing Surface, 5"		Sq. Yd.	421	

Bars indicated thus 29 x 5-#4 etc. indicates 29 lines of bars with 5 lengths per line.



DESIGNED - LM	REVISED -
CHECKED - GG	REVISED -
DRAWN - LM	REVISED -
CHECKED - DSB	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

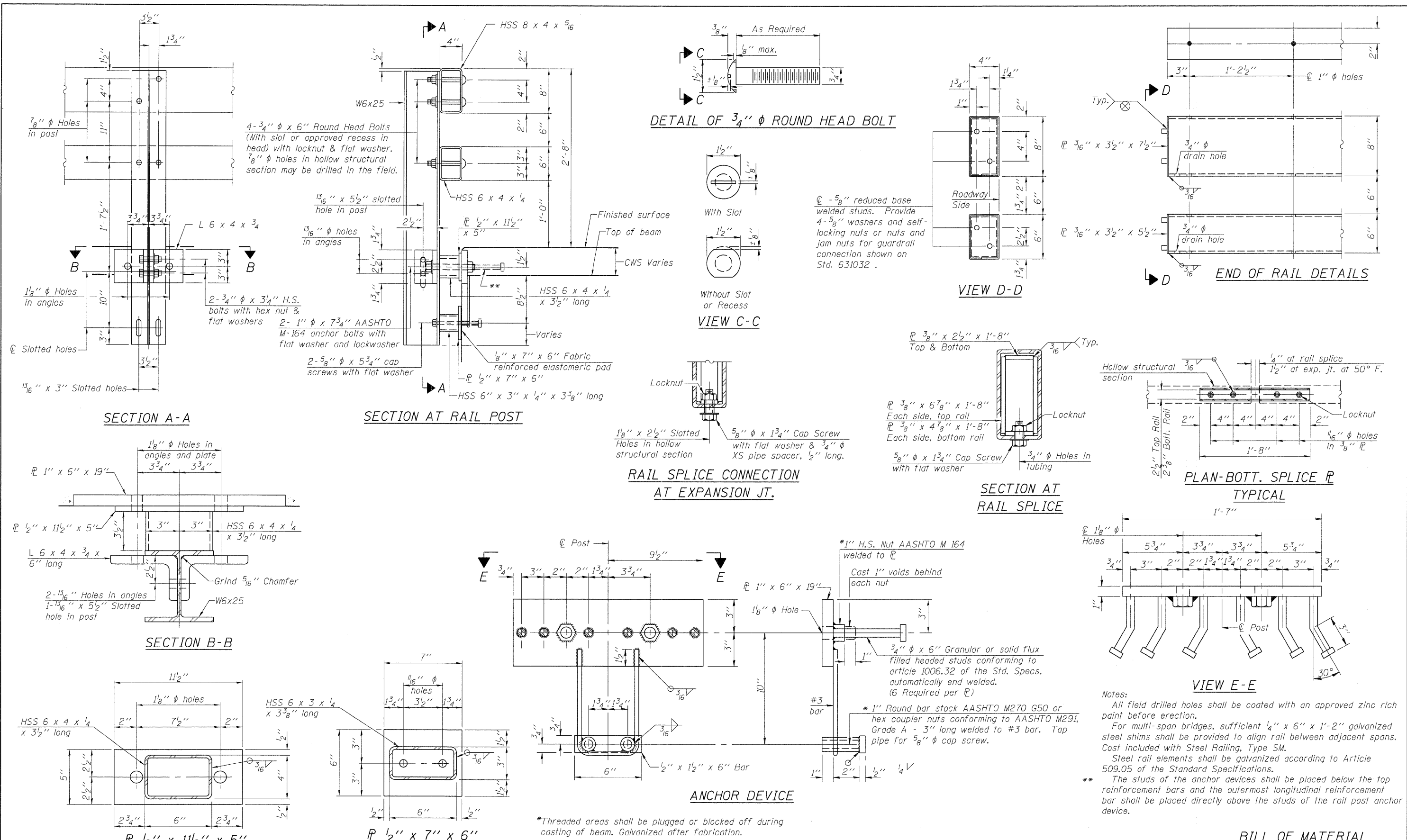
**SUPERSTRUCTURE DETAILS  
 STRUCTURE NO. 045-3094**

SHEET NO. 6 OF 17 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3895	08-15104-01-BR	KANE	38	19
			CONTRACT NO. 63661	
ILLINOIS FED. AID PROJECT				



USER NAME = me-rtm  
 PLOT DATE = 12/26/2011  
 FILE NAME = X:\S\13-2245-2009\Project Sheets\0815104-01-Steel Railing Type SM.dgn



R-34CWS 7-1-10 (6'-3" Maximum Post Spacing) (5" minimum to 7 1/8" maximum CWS thickness)



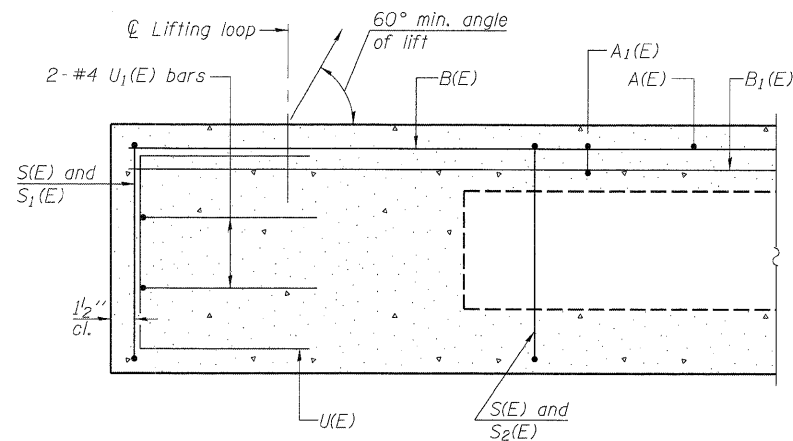
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DRAWN - LM	REVISED -
CHECKED - DSB	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

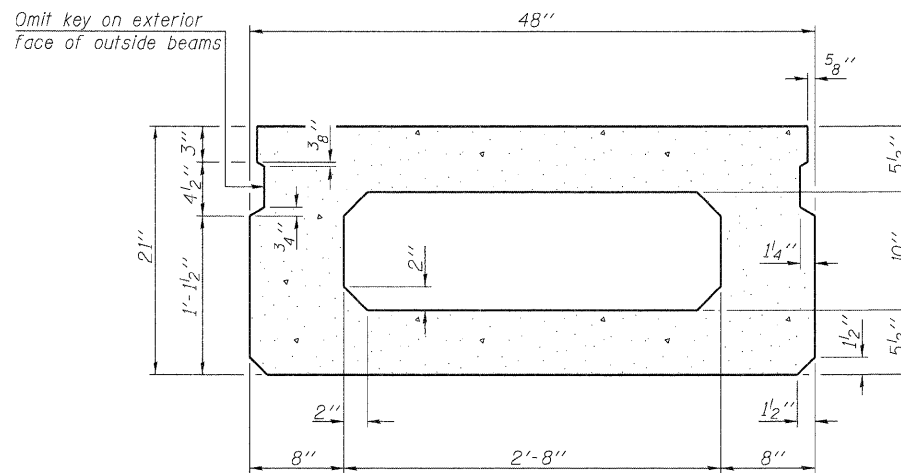
STEEL RAILING, TYPE SM WITH CONCRETE WEARING SURFACE  
STRUCTURE NO. 045-3094

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3895	08-15104-01-BR	KANE	38	20
			CONTRACT NO. 63661	
ILLINOIS FED. AID PROJECT				

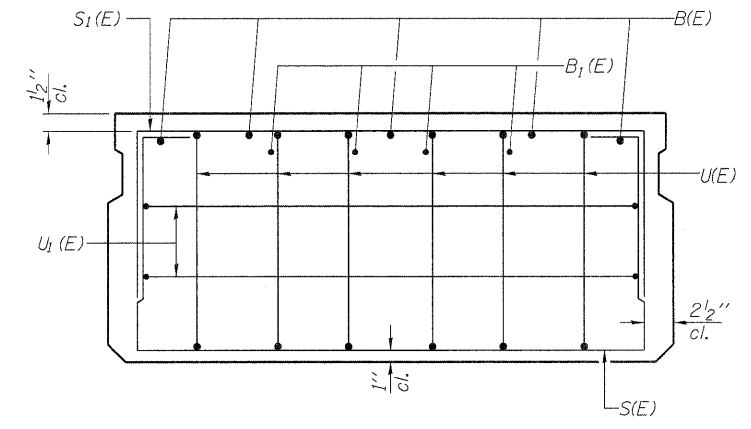
SHEET NO. 7 OF 17 SHEETS



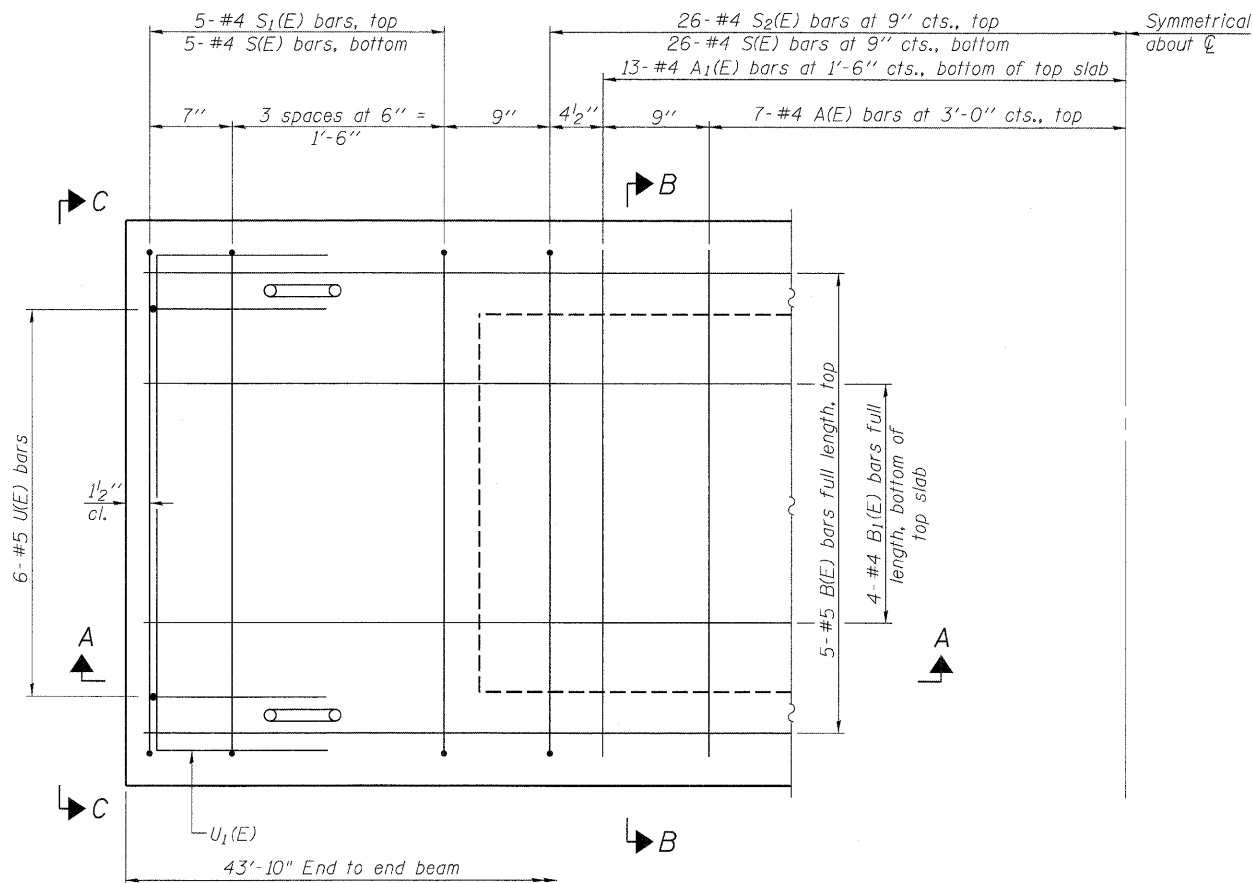
SECTION A-A



SECTION B-B  
(Showing dimensions)

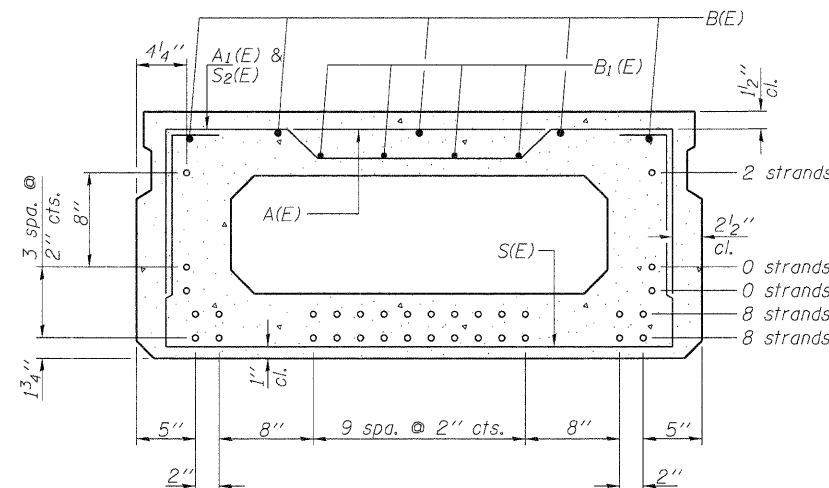


VIEW C-C



PLAN VIEW

Note: Spacing of S(E) and S2(E) bars may be adjusted up to 4" in the immediate area of the transverse tie diaphragms to miss the block outs for the transverse ties.



SECTION B-B

(Showing reinforcement and permissible strand locations)

Note: Place the number of strands specified in each row symmetrically about the centerline of beam in the permissible strand locations shown.

BAR LIST  
ONE BEAM ONLY  
(For Information Only)

Bar	No.	Size	Length	Shape
A(E)	14	#4	3'-7"	—
A1(E)	26	#4	3'-10"	—
B(E)	5	#5	43'-7"	—
B1(E)	4	#4	43'-7"	—
S(E)	52	#4	7'-5"	U
S1(E)	10	#4	5'-11"	U
S2(E)	52	#4	6'-2"	U
U(E)	12	#5	4'-0"	U
U1(E)	4	#4	6'-0"	U

Note: See sheet 9 of 17 for additional details and Bill of Material.

MINIMUM BAR LAP

#4 bar = 2'-0"  
#5 bar = 2'-6"

Notes: Compressive strength of prestressed concrete, f'c, shall be 6000 psi.  
Compressive strength of prestressed concrete at release, f'ci, shall be 5000 psi.

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

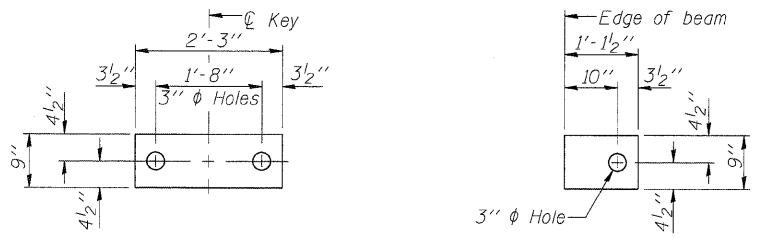
21" x 48" PPC DECK BEAM  
STRUCTURE NO. 045-3094

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3895	08-15104-01-BR	KANE	38	21
CONTRACT NO. 63661				

SHEET NO. 8 OF 17 SHEETS

ILLINOIS FED. AID PROJECT

DESIGNED	- LM	REVISED	-
CHECKED	- GG	REVISED	-
DRAWN	- LM	REVISED	-
CHECKED	- DSB	REVISED	-

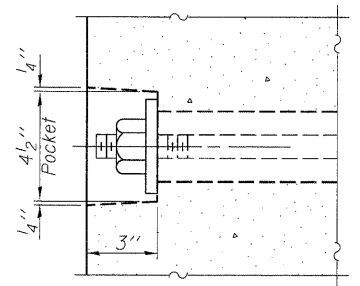


FABRIC BEARING PAD  
(Interior)

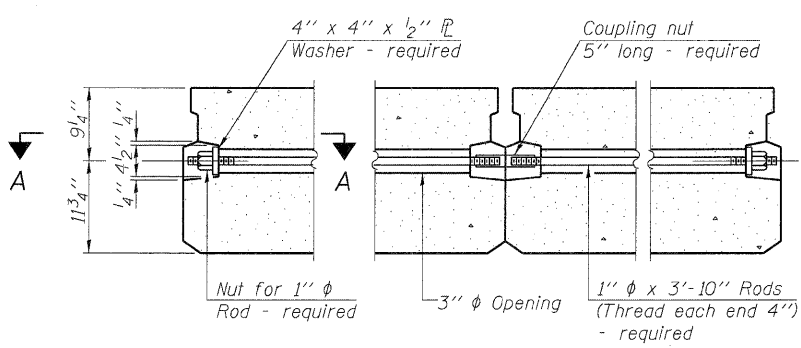
FABRIC BEARING PAD  
(Exterior)

FIXED

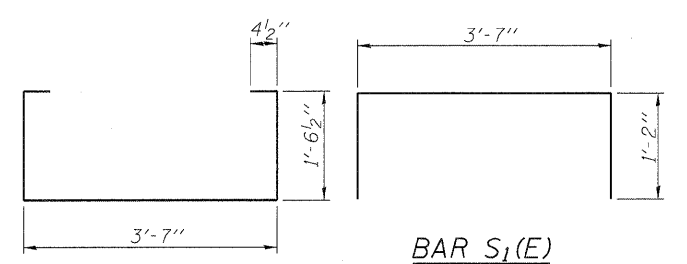
Notes:  
All bearing pads shall be 1" thick.  
See table on this sheet for steel shim plate thickness.



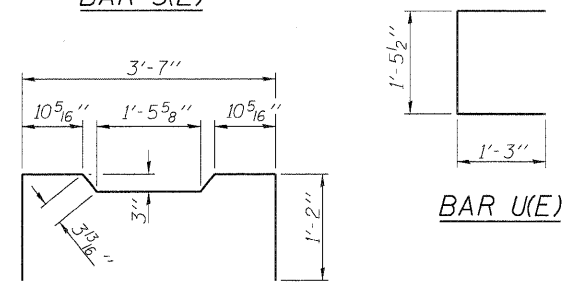
SECTION A-A



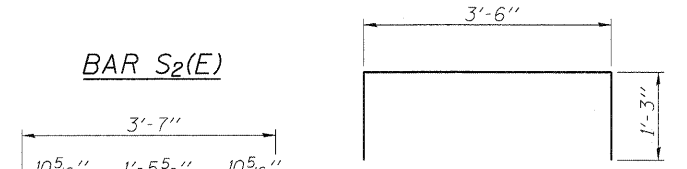
TYPICAL TRANSVERSE TIE ASSEMBLY



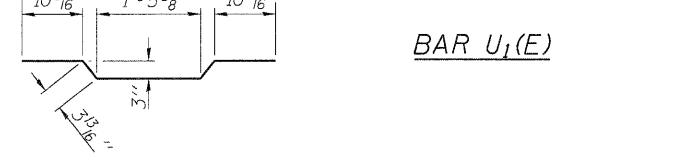
BAR S(E)



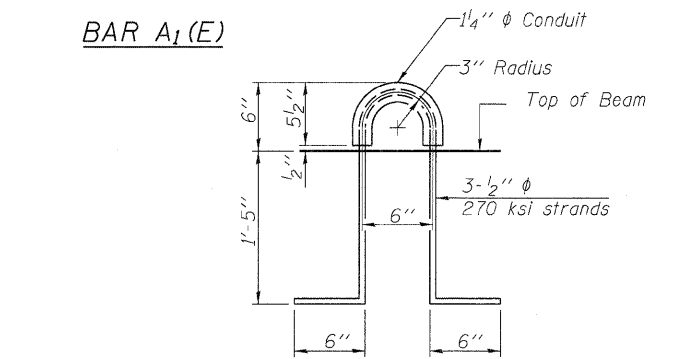
BAR U(E)



BAR S2(E)

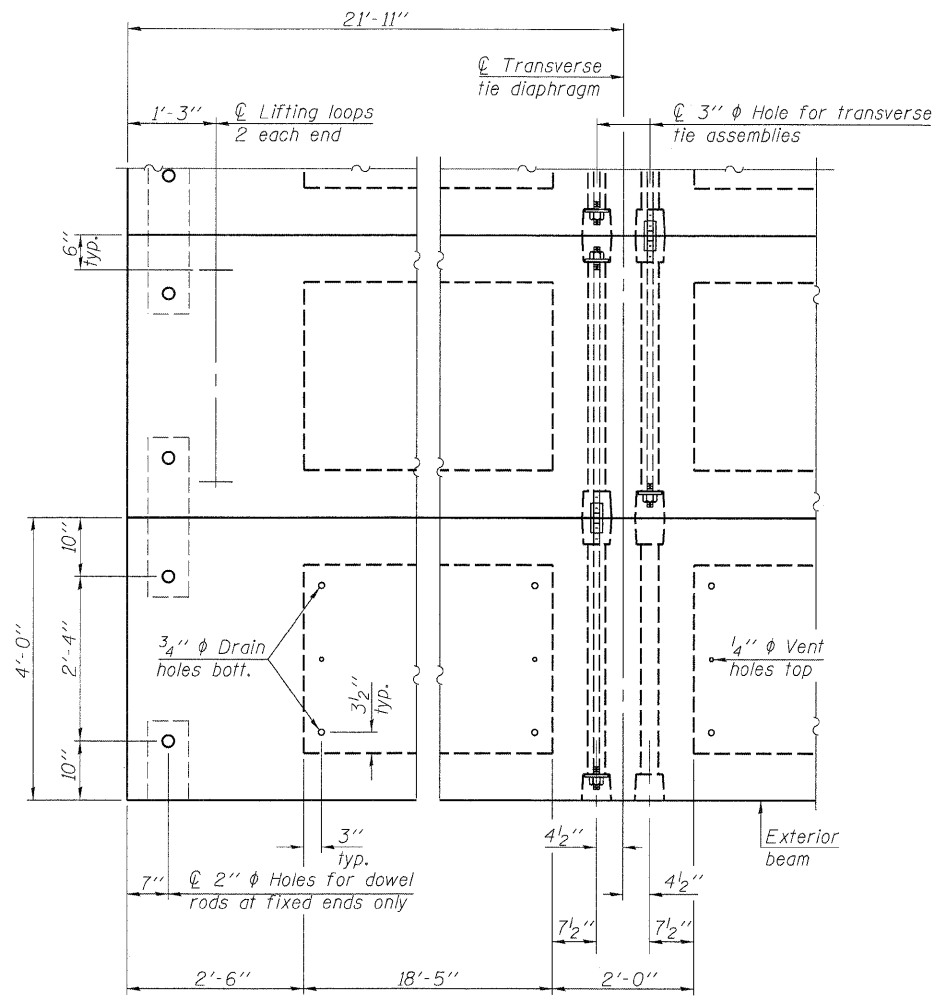


BAR U1(E)



BAR A1(E)

LIFTING LOOP DETAIL



PLAN VIEW

STEEL SHIM PLATE THICKNESS

	End Bm. 1	Bms. 1-2 Key	Bms. 2-3 Key	Bms. 3-4 Key	Bms. 4-5 Key	Bms. 5-6 Key	Bms. 6-7 Key	End Bm. 7
⊘ Brg. W. Abut.	1/4"	3/8"	1/2"	5/8"	5/8"	1/2"	3/8"	1/4"
⊘ W. Brg. Pier 1	3/4"	7/8"	1"	1 1/8"	1 1/8"	1"	7/8"	3/4"
⊘ E. Brg. Pier 1	3/4"	7/8"	1"	1 1/8"	1 1/8"	1"	7/8"	3/4"
⊘ W. Brg. Pier 2	3/4"	7/8"	1"	1 1/8"	1 1/8"	1"	7/8"	3/4"
⊘ E. Brg. Pier 2	3/4"	7/8"	1"	1 1/8"	1 1/8"	1"	7/8"	3/4"
⊘ Brg. E. Abut.	1/4"	3/8"	1/2"	5/8"	5/8"	1/2"	3/8"	1/4"

Notes:  
Steel shim plates of the dimensions of the fabric bearing pads and the thickness of the above table shall be provided beneath the fabric bearing pads.  
Steel shim plates shall be galvanized according to AASHTO M111.

NOTES

Prestressing steel shall be uncoated high strength, low relaxation 7-wire strand, Grade 270. The nominal diameter shall be 1/2" and the nominal cross-sectional area shall be 0.153 sq. in.  
The 1" rods in the transverse tie assembly shall be tightened to a snug fit and the threads set. Pockets on exterior faces of bridge shall be filled with grout after transverse tie assembly is in place.  
Reinforcement bars shall conform to ASTM A 706, Grade 60. (See Special Provisions).  
Two 1/8" fabric adjusting shims of the dimensions of the exterior bearing pad shall be provided for each bearing pad location.  
A minimum 2 1/2" diameter lifting pin shall be used to engage the lifting loops during handling.  
Corrosion Inhibitor, per Article 1020.05(b)(12) and 1021.06 of the Standard Specifications, shall be used in the concrete for precast prestressed concrete deck beams.  
Compressive strength of prestressed concrete, f'c, shall be 6000 psi.  
Compressive strength of prestressed concrete at release, f'ci, shall be 5000 psi.

Note: Connect beams in pairs with the transverse tie configuration shown.

BILL OF MATERIAL

Material	Quantity	Unit
Precast Prestressed Conc. Deck Bms. (21' depth)	Sq. Ft.	3,780
Furnishing and Erecting Structural Steel	Pound	2,220

PLOT DATE: 12/16/2011  
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 USER NAME: smartin



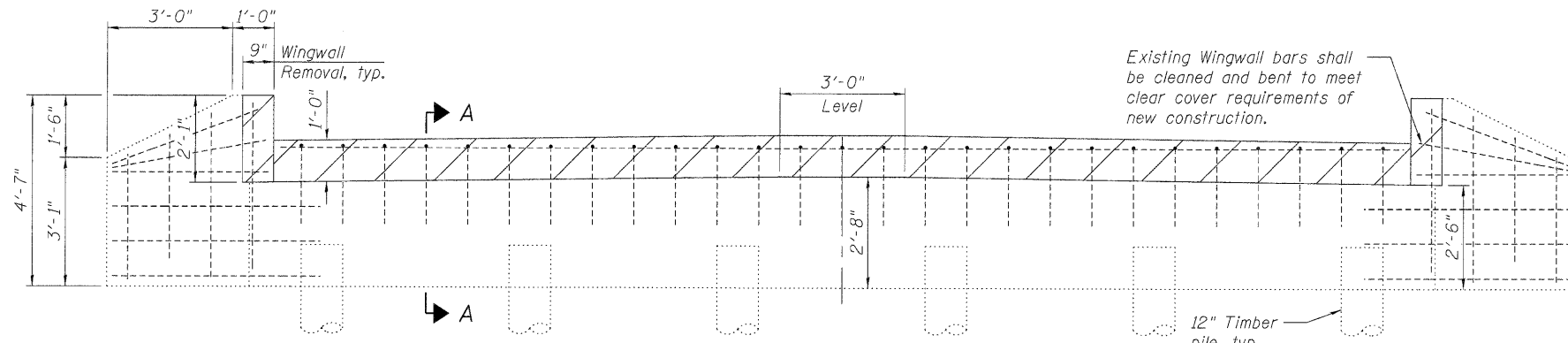
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CHECKED - GG	REVISED -
DRAWN - LM	REVISED -
CHECKED - DSB	REVISED -

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DEPARTMENT OF TRANSPORTATION

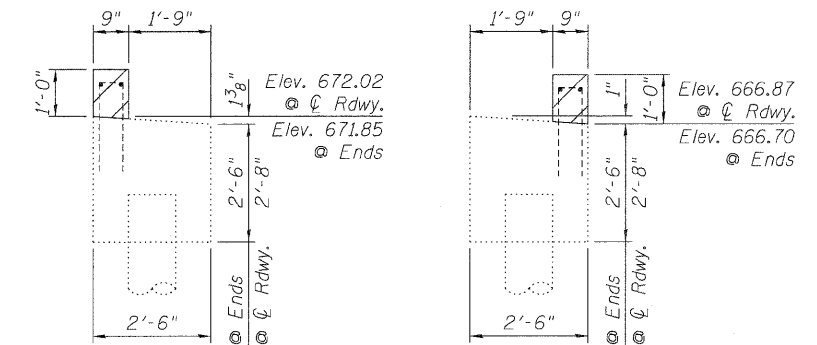
21" x 48" PPC DECK BEAM DETAILS  
STRUCTURE NO. 045-3094

SHEET NO. 9 OF 17 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3895	08-15104-01-BR	KANE	38	22
CONTRACT NO. 63661				ILLINOIS FED. AID PROJECT

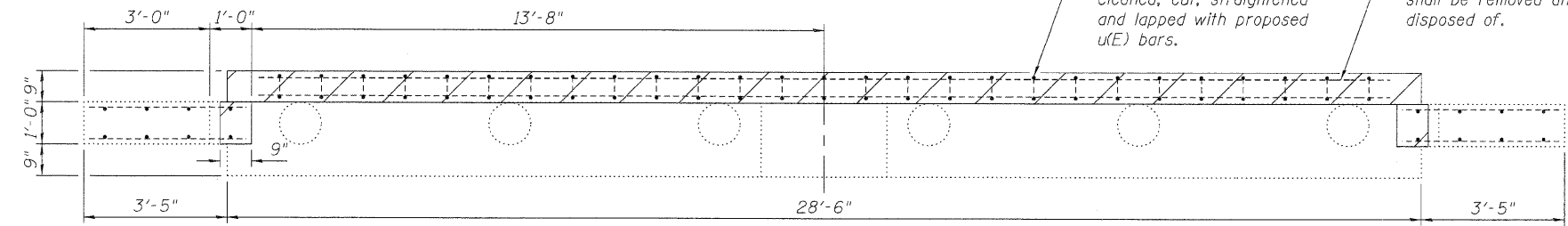
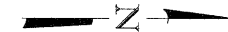


**CONCRETE REMOVAL - ELEVATION**  
West Abutment looking West. East Abutment similar.



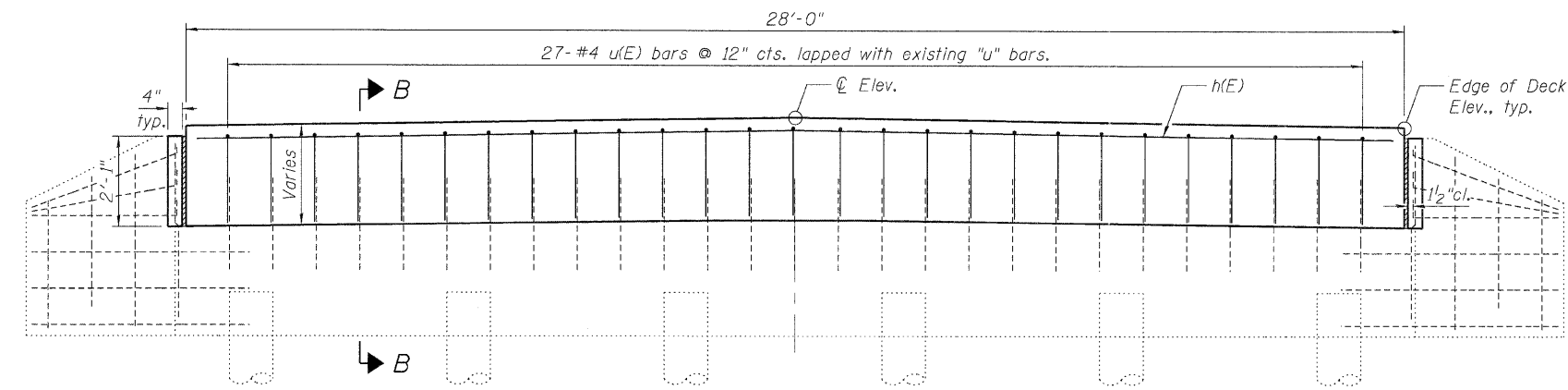
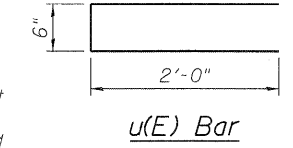
**West Abutment**      **East Abutment**

**SECTION A-A**

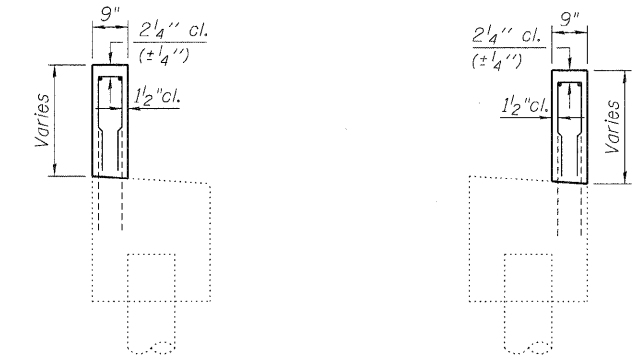


**CONCRETE REMOVAL - PLAN**  
West Abutment shown. East Abutment similar.

- Notes:
- Existing reinforcement shall be cleaned and incorporated into the new construction. Cost included with Concrete Removal.
  - The concrete shown on this sheet shall not be poured until after the PPC Deck Beams are in place and the concrete wearing surface has been constructed.

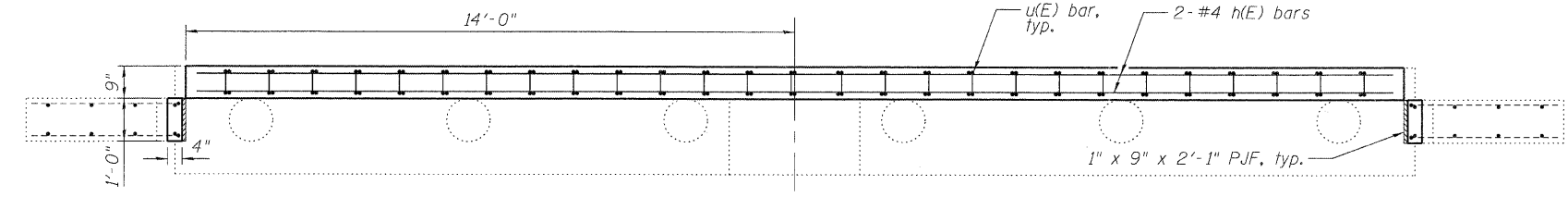
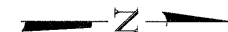


**ELEVATION**  
West Abutment looking West. East Abutment similar.  
See Table for Backwall Elevations



**West Abutment**      **East Abutment**

**SECTION B-B**



**PLAN**  
West Abutment shown. East Abutment similar.

**BACKWALL ELEVATIONS**

Location	℄	Edge of Deck
Back of W. Abutment	674.42	674.20
W. End of Deck	674.38	671.16
E. End of Deck	669.30	669.08
Back of E. Abutment	669.28	669.06

**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape	
h(E)	4	#4	27'-9"	—	
u(E)	54	#4	4'-6"	—	
Reinforcement Bars, Epoxy Coated				Pound	240
Concrete Structures				Cu. Yd.	3.9
Concrete Removal				Cu. Yd.	1.9

PLOT DATE = 12/6/2011  
 FILE NAME = K:\P\13-2245-2009 Peoria Street\GIS\Sheets\Parts 10 - Abutments and Wingwalls.dwg  
 USER NAME = mment



DESIGNED - LM	REVISED -
CHECKED - GG	REVISED -
DRAWN - LM	REVISED -
CHECKED - DSB	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

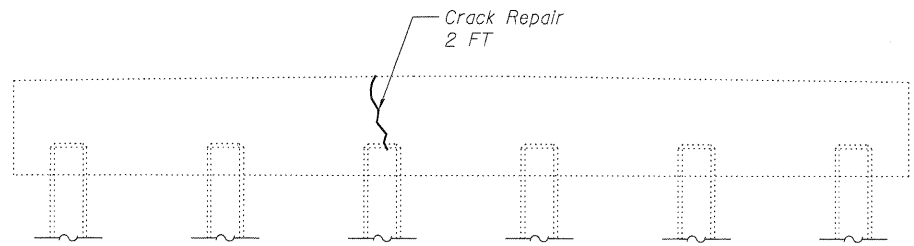
**ABUTMENTS AND WINGWALLS**  
**STRUCTURE NO. 045-3094**

SHEET NO. 10 OF 17 SHEETS

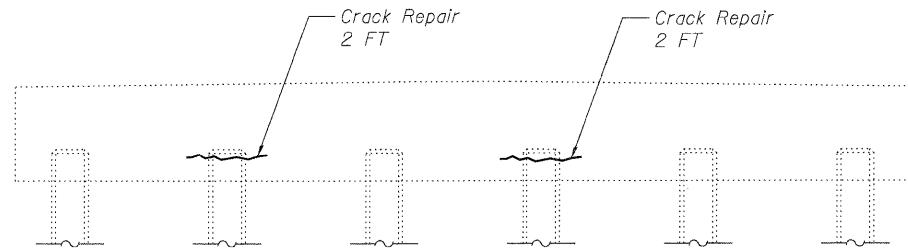
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3895	08-15104-01-BR	KANE	38	23
				CONTRACT NO. 63661
ILLINOIS FED. AID PROJECT				

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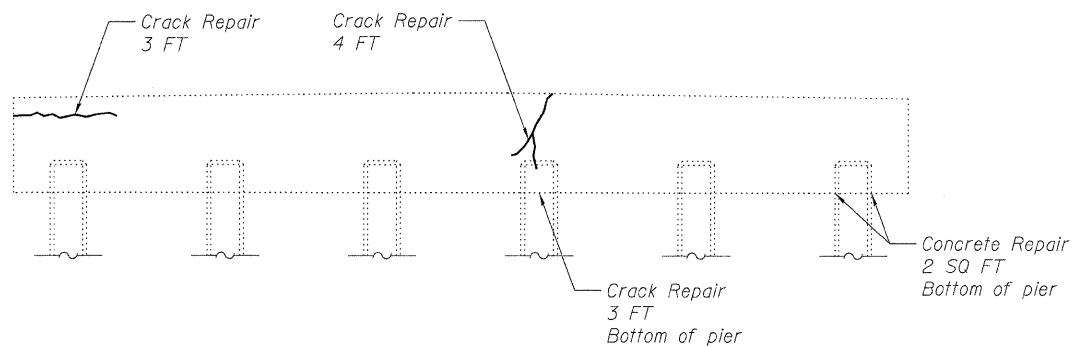
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**ELEVATION**  
Looking West

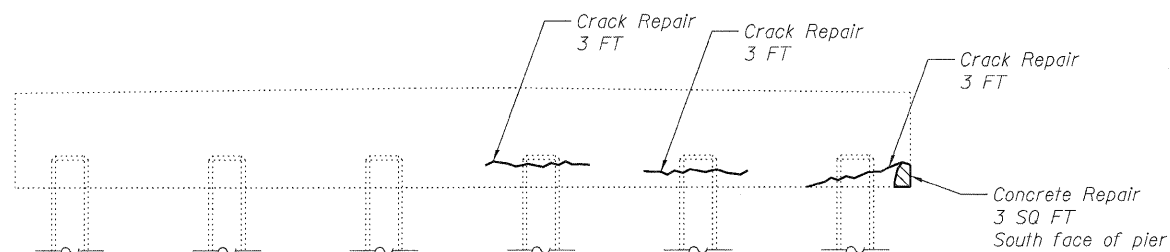


**ELEVATION**  
Looking West



**ELEVATION**  
Looking East

**PIER 1 (WEST PIER)**



**ELEVATION**  
Looking East

**PIER 2 (EAST PIER)**

**BILL OF MATERIAL**

Item	Unit	Quantity
Epoxy Crack Injection	Foot	25
Structural Repair of Concrete (Depth Equal to or Less Than 5 in.)	Sq. Ft.	5



DESIGNED - LM	REVISED -
CHECKED - GG	REVISED -
DRAWN - LM	REVISED -
CHECKED - DSB	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

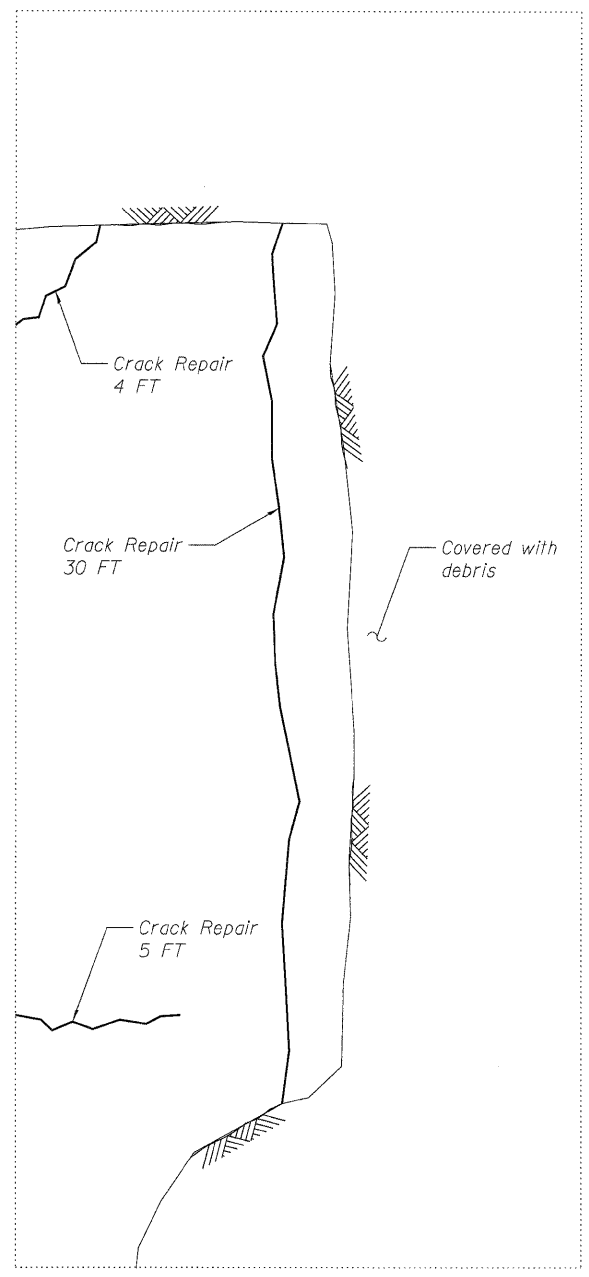
**PIER REPAIRS  
 STRUCTURE NO. 045-3094**

SHEET NO. 11 OF 17 SHEETS

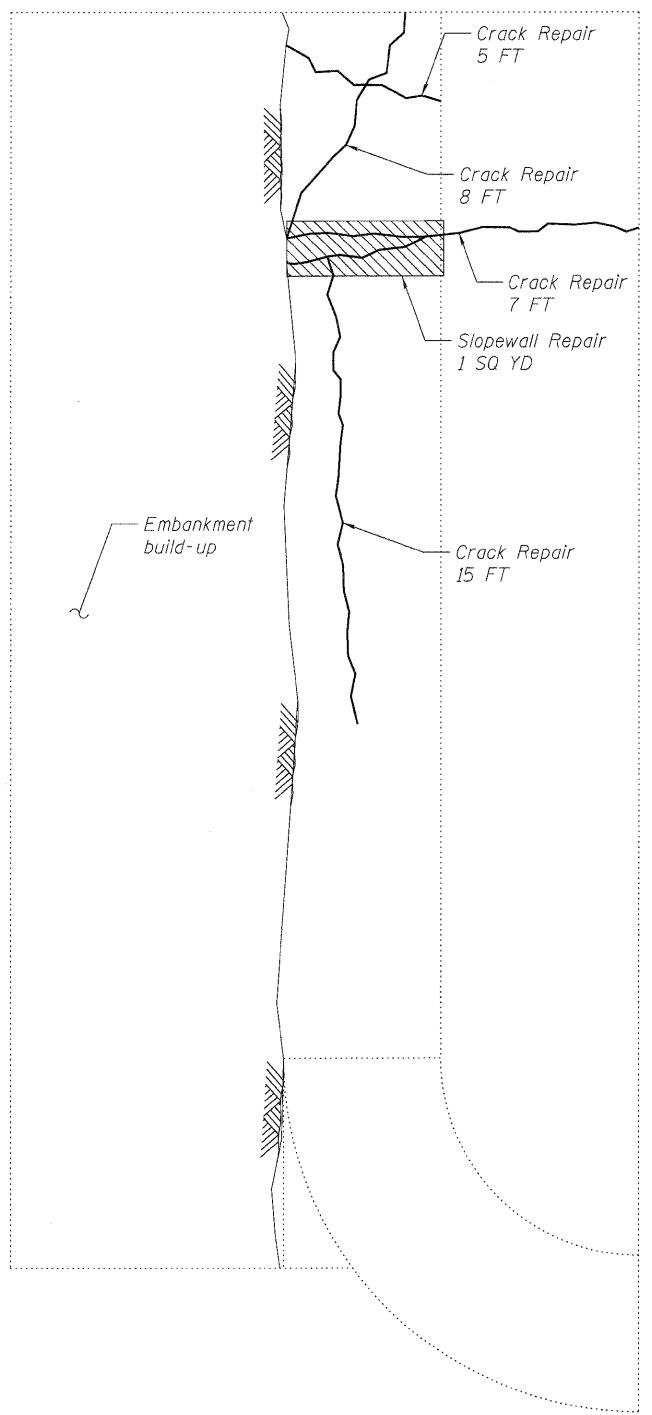
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3895	08-15104-01-BR	KANE	38	24
ILLINOIS FED. AID PROJECT			CONTRACT NO. 63661	



USER NAME = nes-tdl  
 PLOT DATE = 12/16/2011  
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WEST SLOPEWALL PLAN



EAST SLOPEWALL PLAN



**NOTES:**

1. Slopewall shall be reinforced with welded wire fabric, 6" x 6" - W4.0 x W4.0, weighing 58 lbs. per 100 sq. ft. This fabric shall be lapped with the existing wire fabric a length of one mesh unit (6").
2. Existing wire mesh shall be cleaned and incorporated into the new construction. Cost included with Slope Wall Removal.

**BILL OF MATERIAL**

Item	Unit	Quantity
Epoxy Crack Injection	Foot	74
Slope Wall Removal	Sq. Yd.	1
Slope Wall 6 Inch	Sq. Yd.	1



DESIGNED - LM	REVISED -
CHECKED - GG	REVISED -
DRAWN - LM	REVISED -
CHECKED - DSB	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

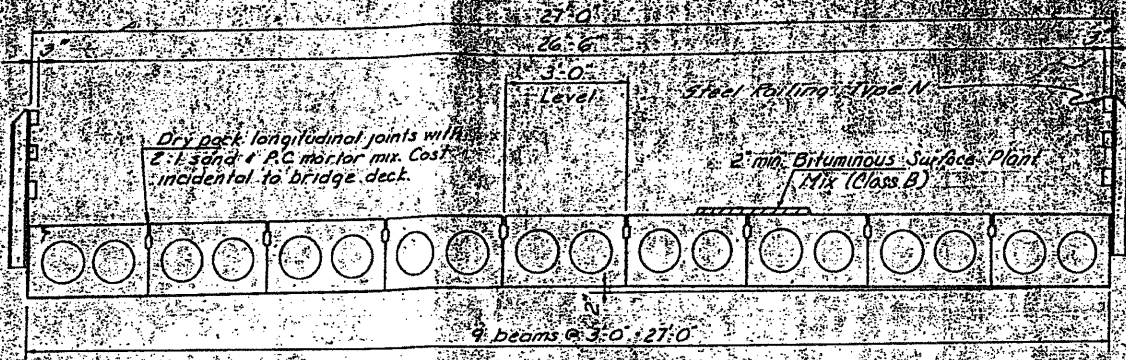
SLOPEWALL REPAIRS  
STRUCTURE NO. 045-3094

SHEET NO. 12 OF 17 SHEETS

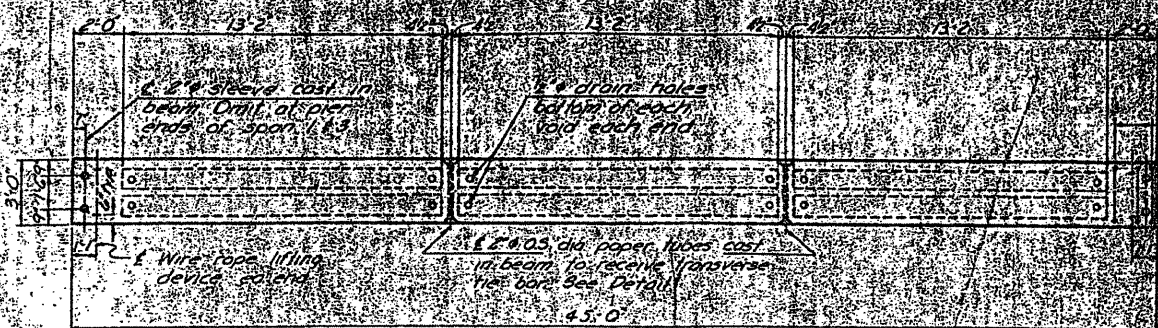
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3895	08-15104-01-BR	KANE	38	25
			CONTRACT NO. 63661	
ILLINOIS FED. AID PROJECT				



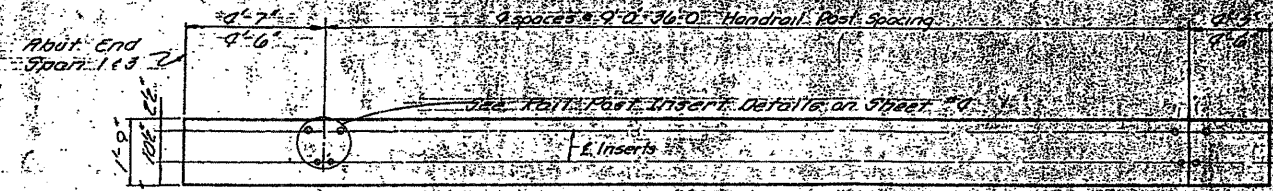




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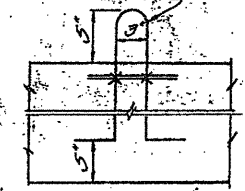


TYPICAL PLAN OF BEAM

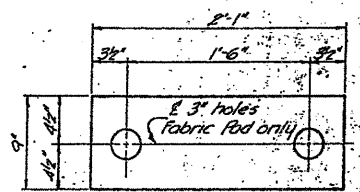


ELEVATION OF OUTSIDE BEAMS

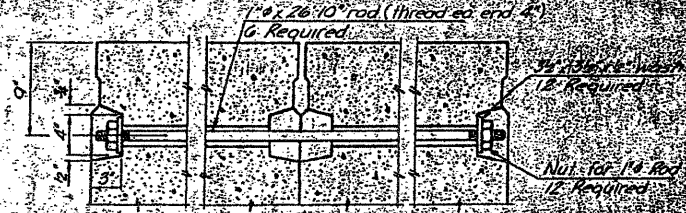
2-7/8" x 5/16" Strand 1000 Coags shall be burned off for beams have been erected



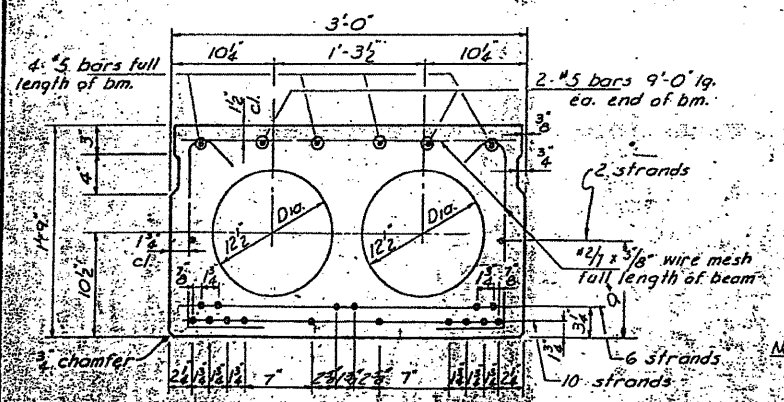
LIFTING LOOP DETAILS



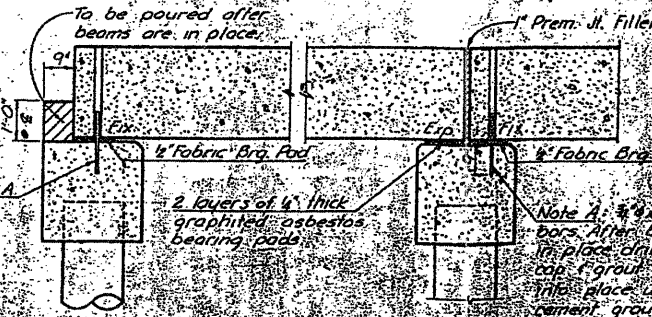
PLAN OF BEARING PADS



TYPICAL TRANSVERSE TIE ASSEMBLY



TYPICAL SECTION THRU BEAM



SECTION AT ABUTS

SECTION AT PIERS

BILL OF MATERIAL SUPERSTRUCTURE

Item	Quantity
Precast Prestressed Concrete Deck Beams	38
Steel Rail Ties	38
Bituminous Materials (Prime Coat, Guss Coat, Bituminous Mixture - Class B)	12

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
08-15104-01-BR	KANE	38	27
CONTRACT NO. 63661			

FOR INFORMATION ONLY

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

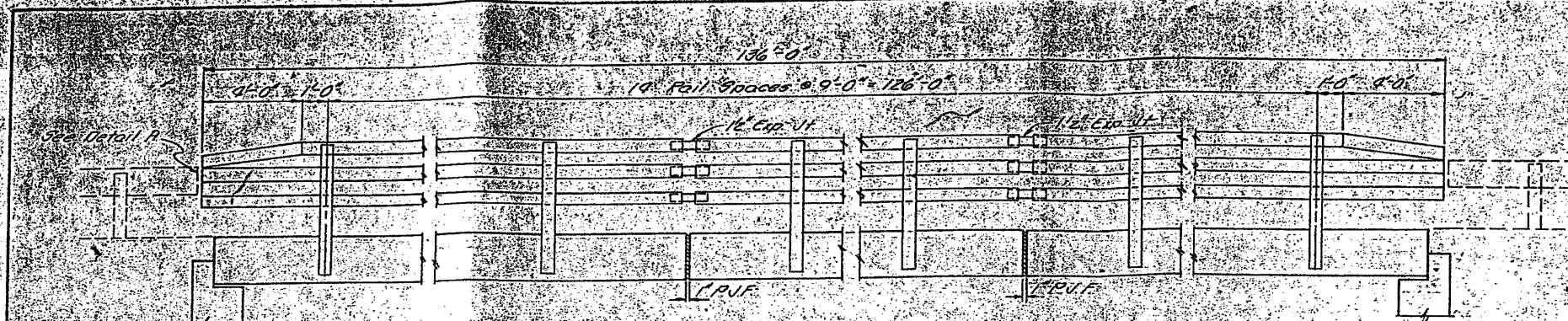
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STRUCTURE NO. 045-3094

SHEET NO. 14 OF 17 SHEETS

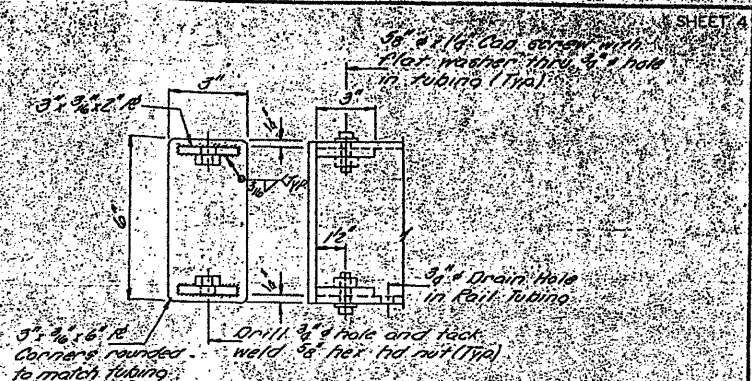
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3895	08-15104-01-BR	KANE	38	27
CONTRACT NO. 63661				

ILLINOIS FED. AID PROJECT

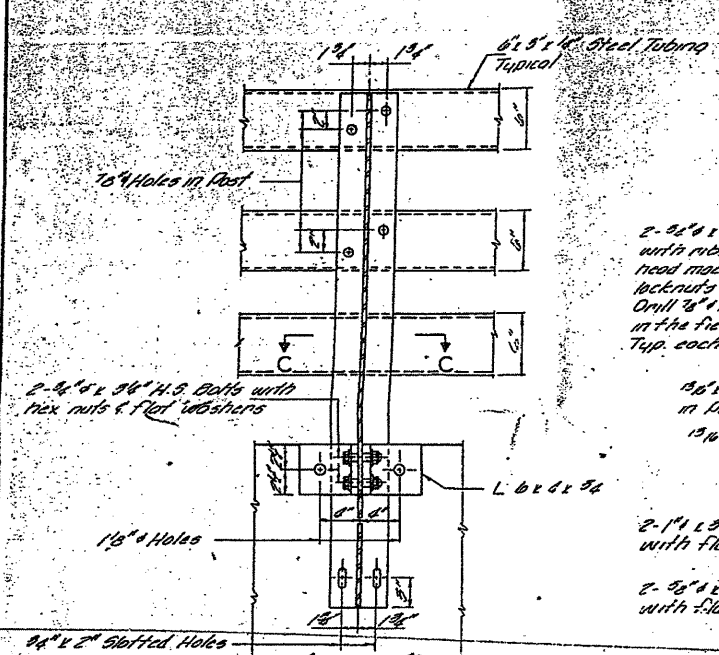




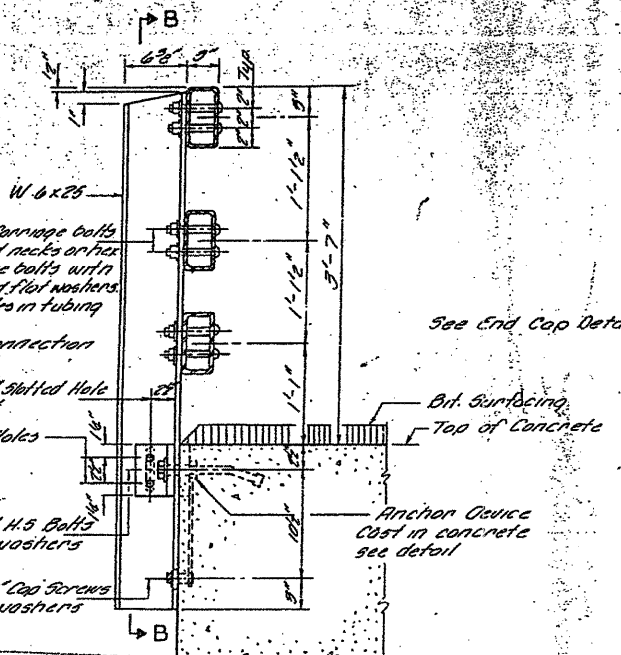
**ELEVATION**  
Showing Outside Face of railing



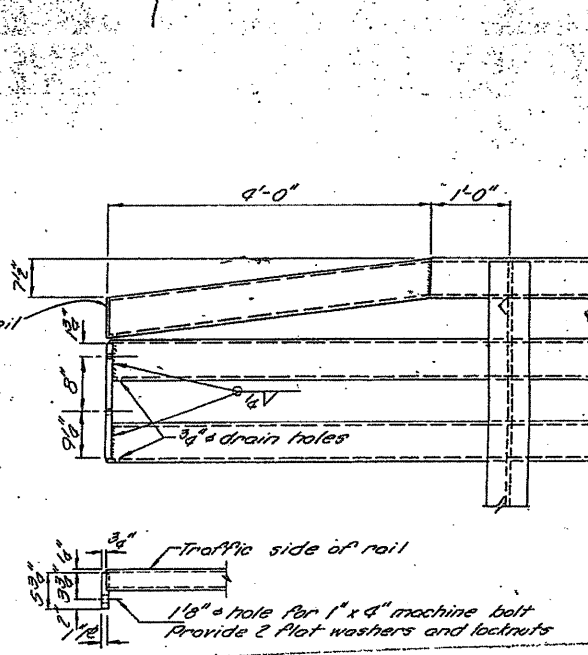
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As Required



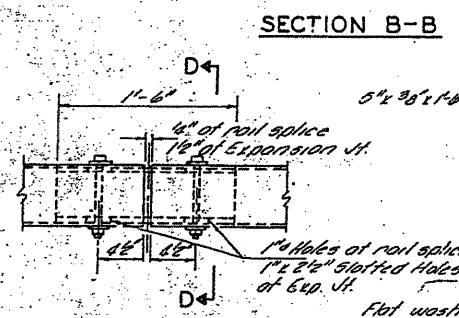
**SECTION B-B**



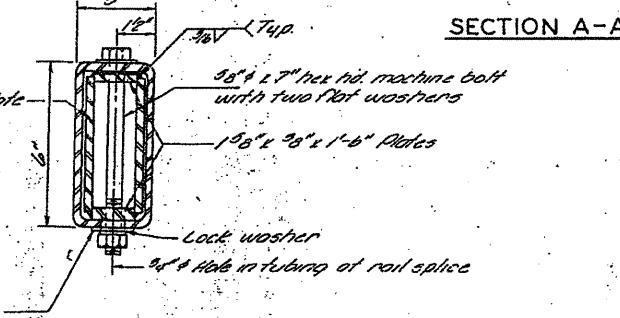
**SECTION A-A**



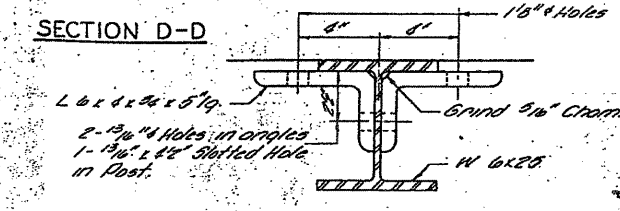
**DETAIL A**



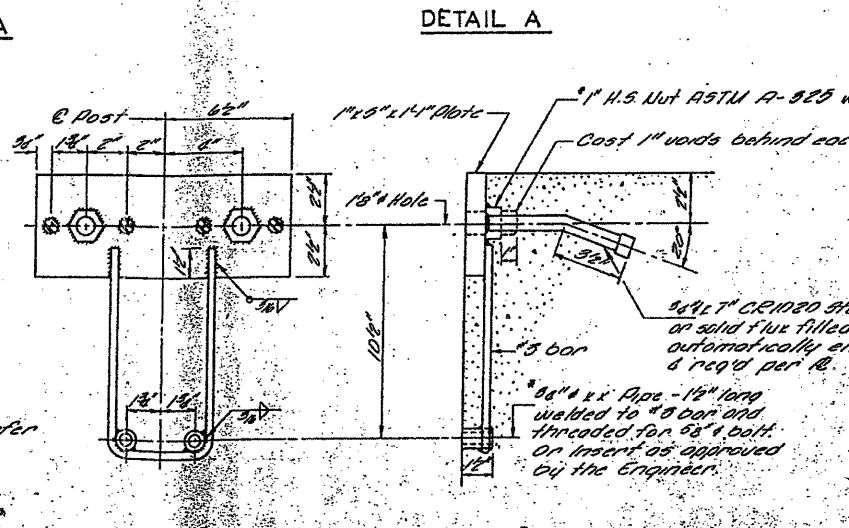
**RAIL SPLICE**



**SECTION D-D**



**SECTION C-C**



**ANCHOR DEVICE**

**NOTES**

Hollow structural steel tubing shall conform to the requirements of ASTM designation A-501, Hot Formed Welded and Seamless Carbon Steel Structural Tubing.

All other steel shapes and plates shall conform to the requirements of ASTM designation A-36 except posts shall conform to ASTM A-141.

Bolts, cap screws, and nuts shall conform to the requirements of ASTM designation A-307 except for high strength bolts, nuts and washers noted which shall conform to ASTM designation A-325.

All bolts, nuts, cap screws, washers and lock washers shall be galvanized in accordance with ASTM designation F-153.

All posts, railing, rail splices, anchor devices and angles shall be galvanized after shop fabrication in accordance with ASTM designation A-123 and A-388. Galvanized rail shall not be painted.

Railing shall be in accordance with Section 609 of the Standard Specifications, except as noted and shall be paid for at the contract unit price per linear foot for STEEL RAILING, TYPE 'N'.

All field drilled holes shall be coated with an approved zinc rich paint before erection.

The lower portion of the post flange in contact with concrete shall receive two coats of asphalt paint conforming to Section 714.08 Type B or place 1/2" fabric bearing pad between the post and concrete.

The 3/4" high strength bolts used to connect the 6" x 3" angles to the post shall be tightened in accordance with Article 70.11 of Standard Specifications. The 1" high strength bolts connecting the angles to the concrete beam shall be tightened to snug fit and given an additional 1/2 turn.

For multi-span bridges, sufficient 4" x 6" x 1/8" galvanized steel shims shall be provided to align rail between adjacent spans. Cast incidental to steel railing.

Provide galvanized washers as required to make railing post vertical. Cast incidental to steel railing.

**BILL OF MATERIAL**

ITEM	QUANTITY
Steel Railing Type 'N'	272 Lin. Ft.

**STEEL RAILING TYPE 'N' MODIFIED  
BARNES BRIDGE  
SEC. 104B-TR SUGAR GROVE RD. DIST.  
KANE COUNTY  
STATION 16476**

**COLLINS & RICE  
CONSULTING ENGINEERS**

**F.S. M.D.  
J.F. M.D.**

**FOR INFORMATION ONLY**



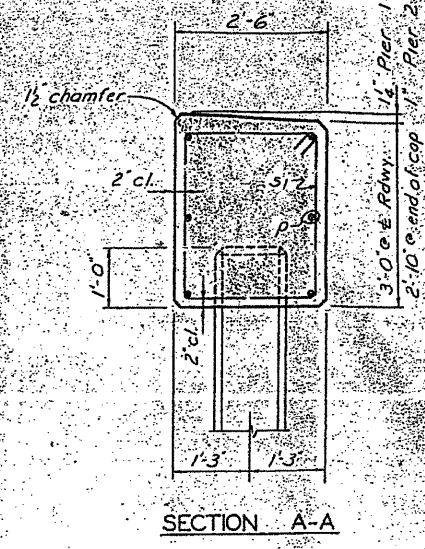
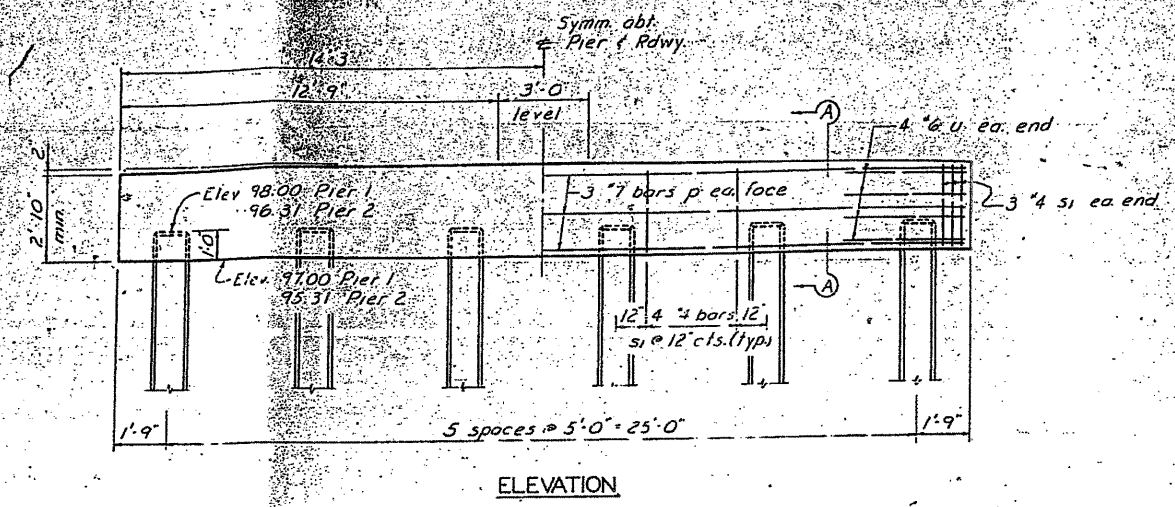
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

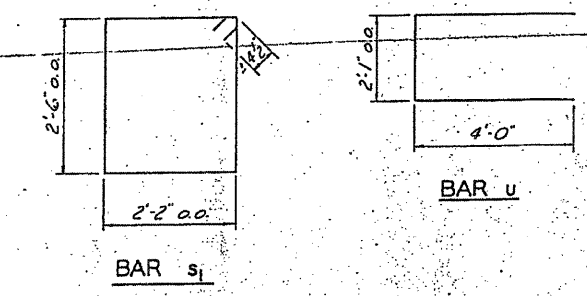
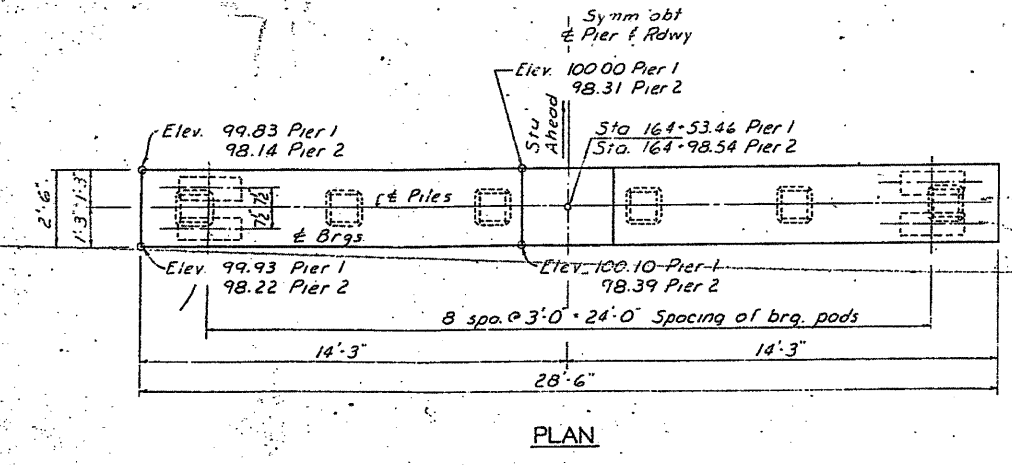
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STRUCTURE NO. 045-3094**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3895	08-15104-01-BR	KANE	38	28
CONTRACT NO. 63661			ILLINOIS FED. AID PROJECT	

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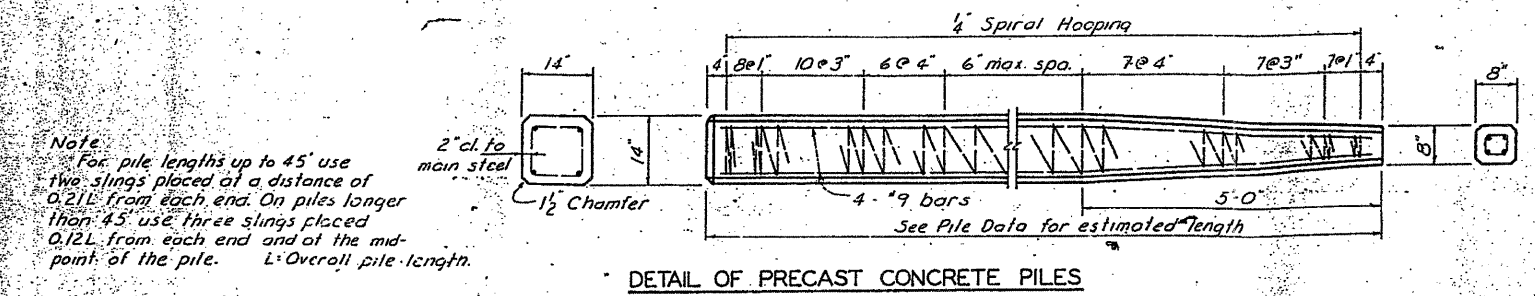


**PILE DATA**  
 Type: Precast Concrete (14")  
 No. Req'd (2 Piers): 12  
 Min. Capacity: 39 Tons/Pile  
 Est. Length: 35 Feet  
 \*Includes one test pile to be driven in Pier 1.  
 Note: Minimum tip elevation of all piles in the piers shall be 63.0



**BILL OF MATERIAL - 2 PIERS**

BAR	NO.	SIZE	LENGTH	SHAPE
p	12	7"	28'-0"	—
s1	52	4"	10'-1"	□
u	16	6"	10'-1"	□
Class X Concrete		Cu. Yds.		15.4
Reinforcement Bars		Lbs.		1,280
Precast Concrete Piles		Lin. Ft.		385
Test Piles (Concrete)		Each		1



**PIERS**  
 BARNES BRIDGE  
 SEC. 104B-TR SUGAR GROVE RD. DIST.  
 KANE COUNTY  
 STATION 164+76  
 COLLINS AND RICE  
 CONSULTING ENGINEERS  
 M.J.R. REG. 10-6-64-307

**FOR INFORMATION ONLY**

PLOT DATE = 1/2/2012  
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

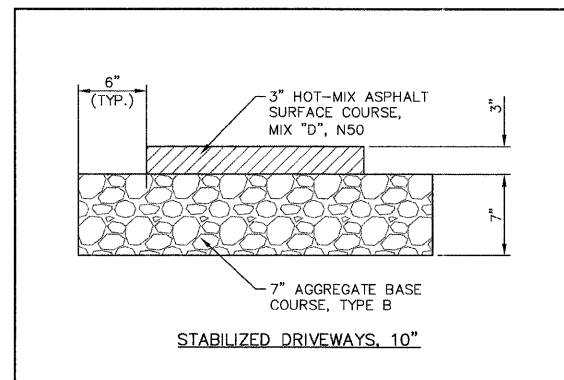
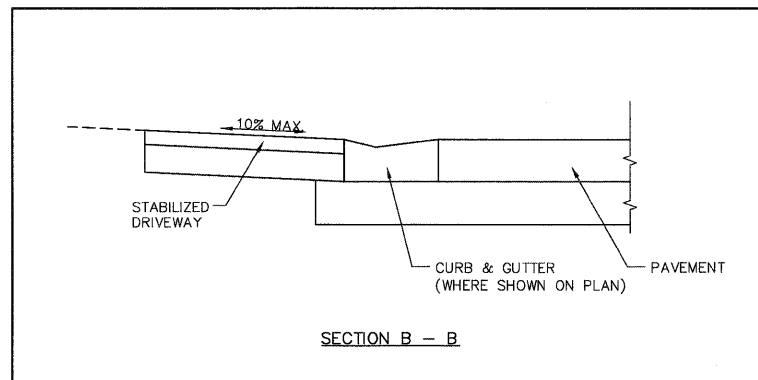
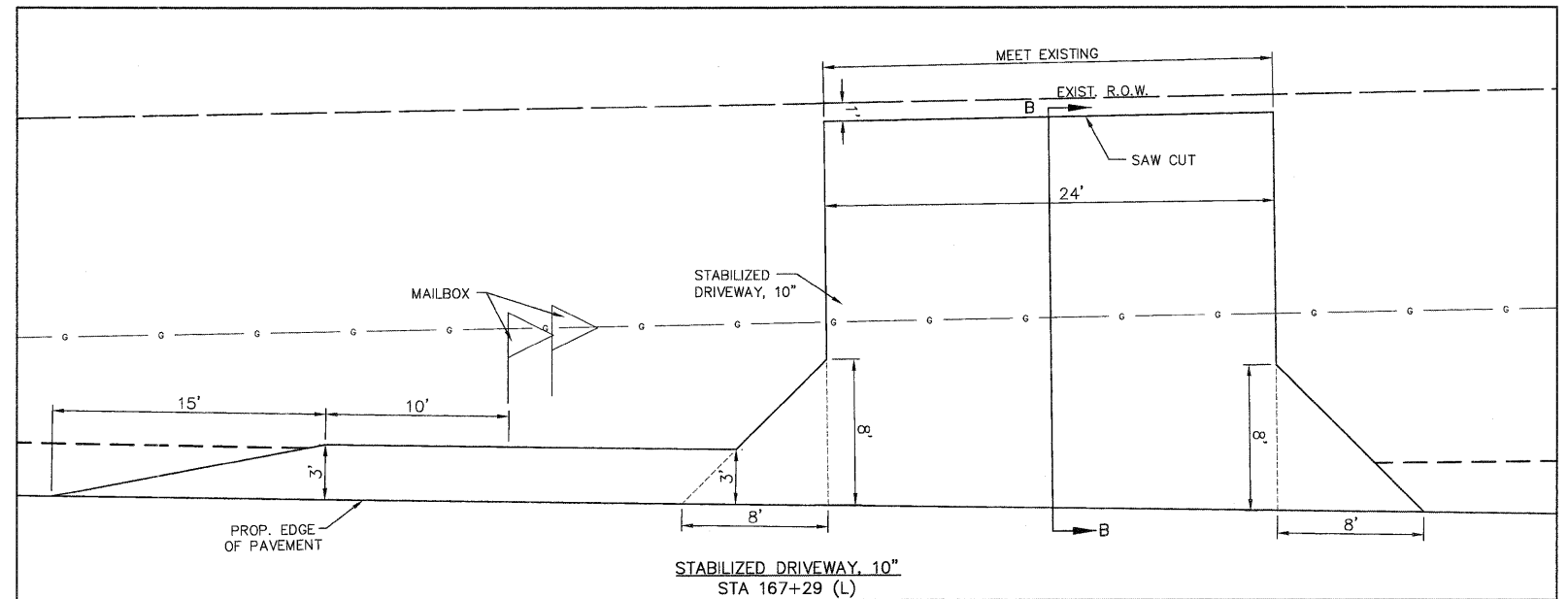
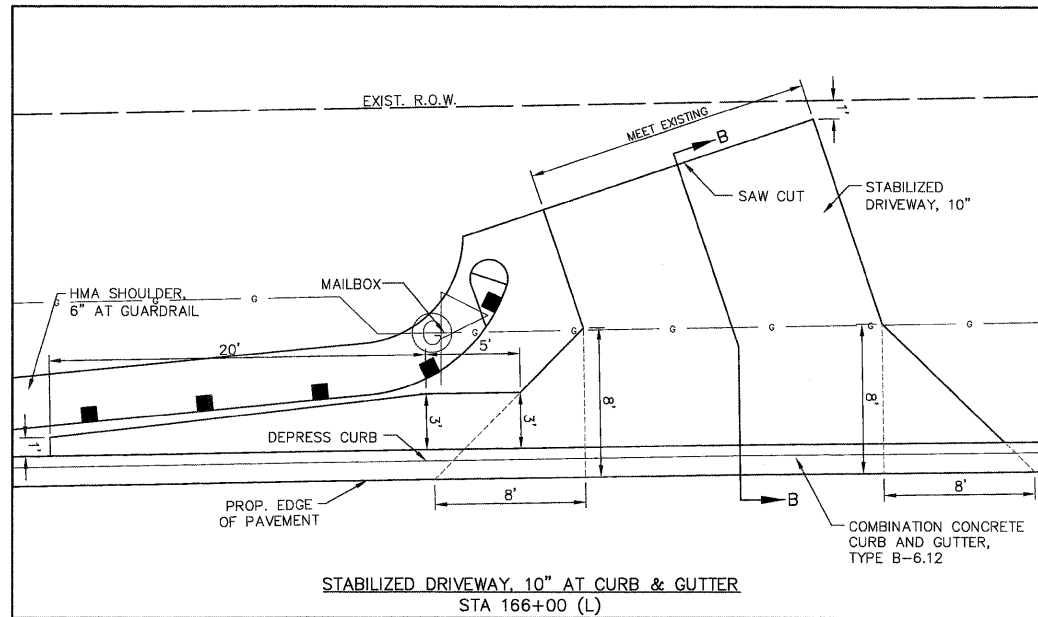
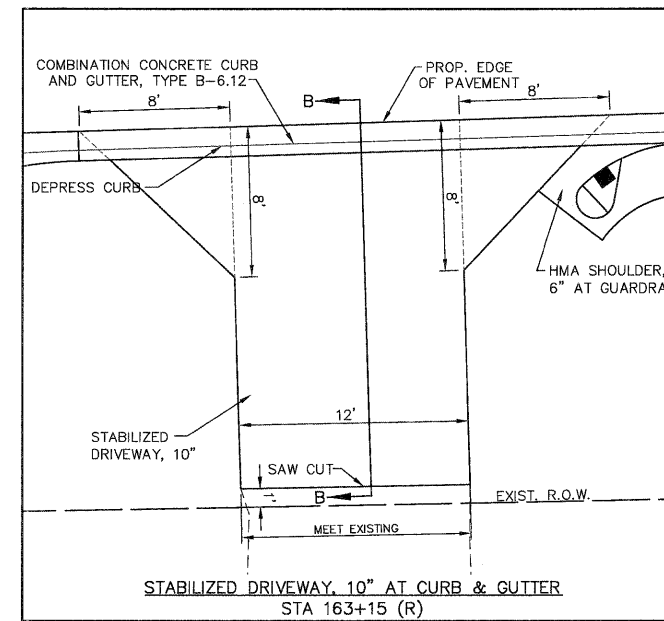
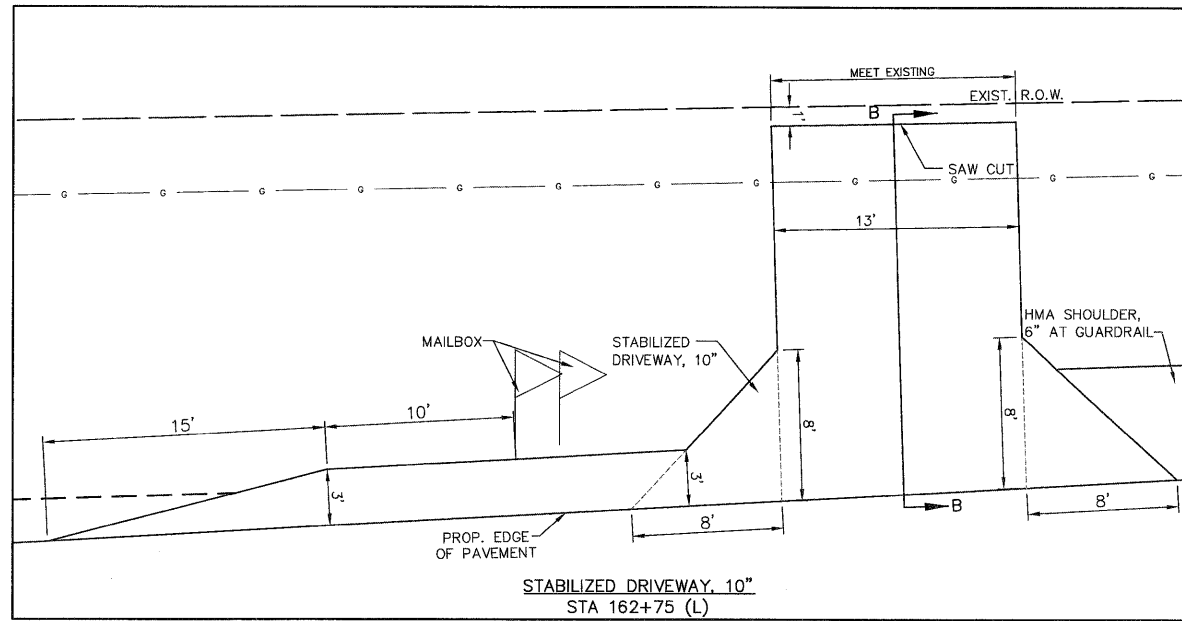
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 STRUCTURE NO. 045-3094  
 SHEET NO. 16 OF 17 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3895	08-15104-01-BR	KANE	38	29
CONTRACT NO. 63661				

ILLINOIS FED. AID PROJECT







Plotted: November 17, 2011 @ 9:35 AM By: Kris Pump - Tab: 31 Details (22x34)

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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

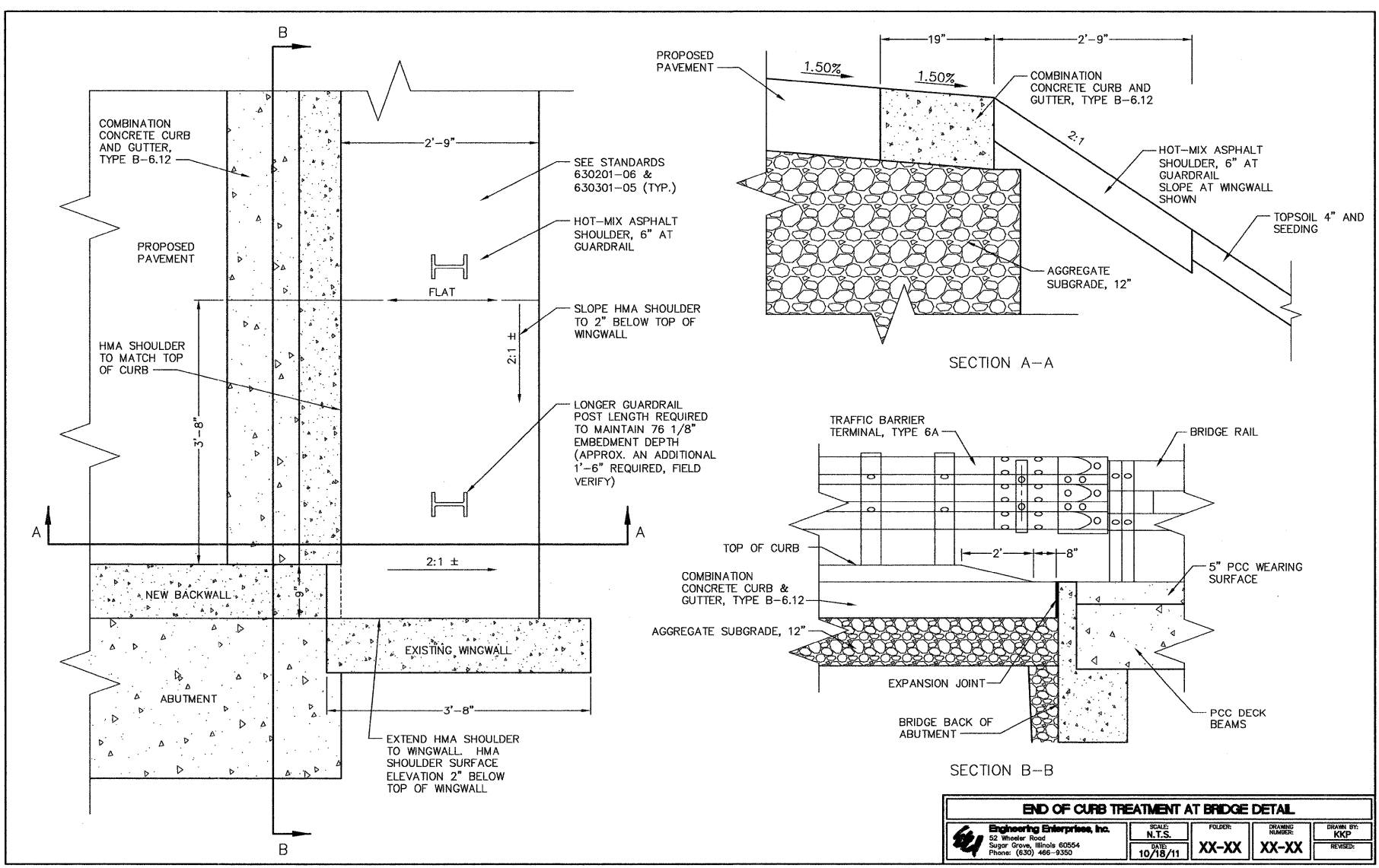
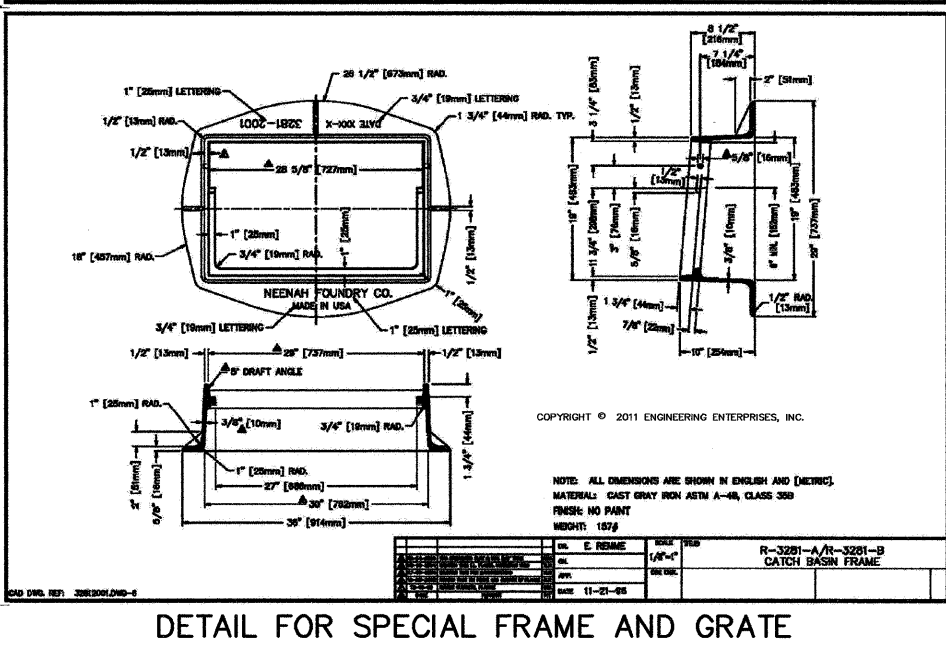
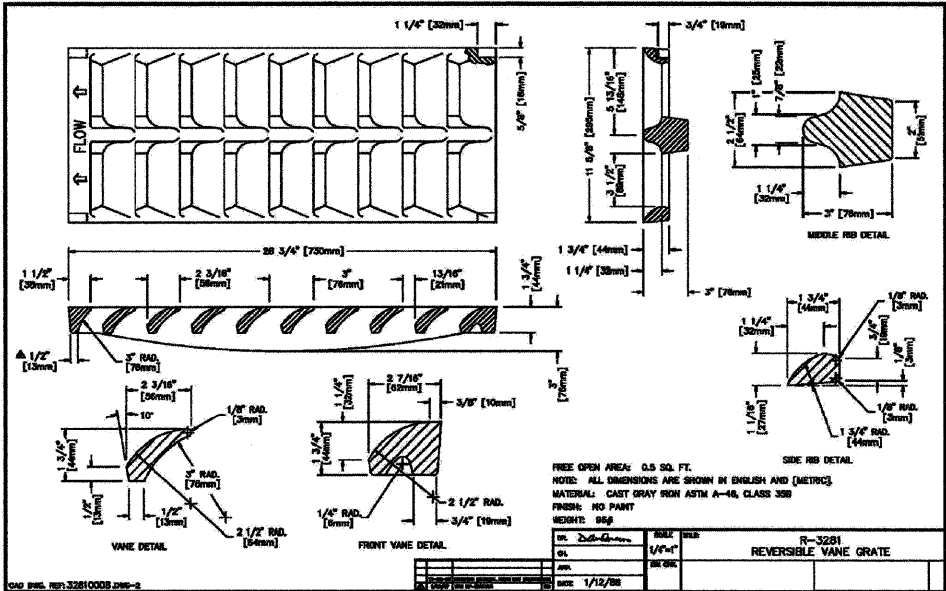
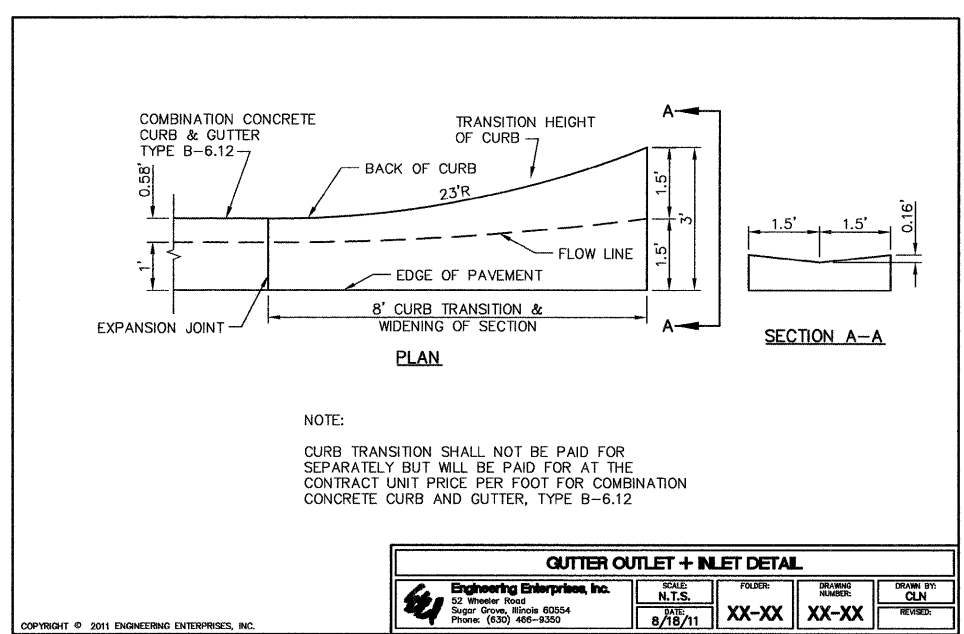
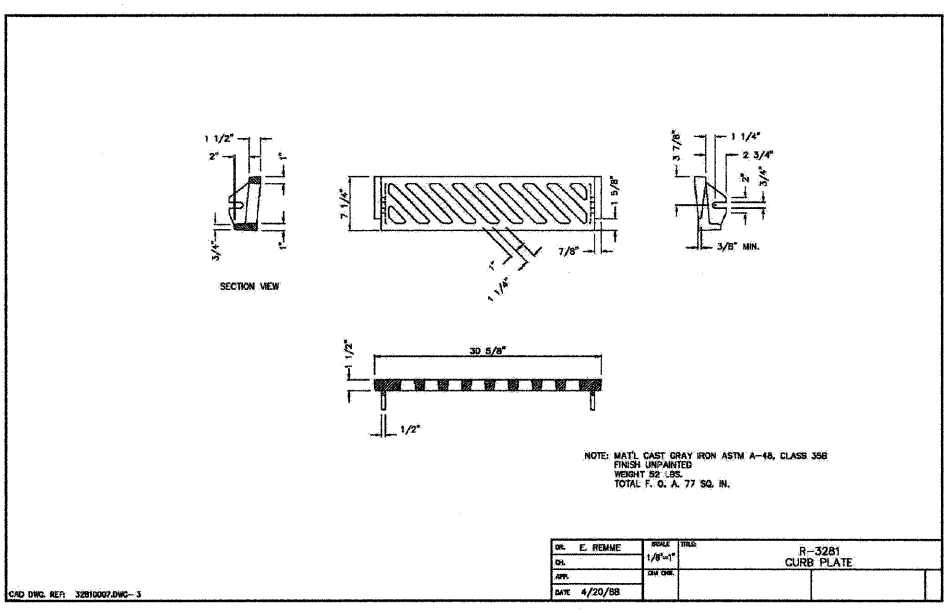
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F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT			CONTRACT NO. 63661	

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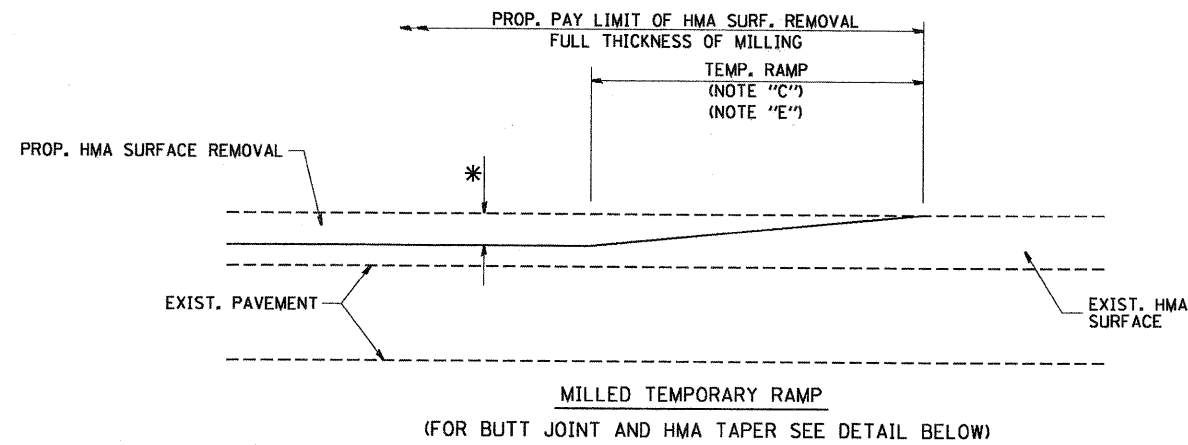
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SPECIAL DETAILS**

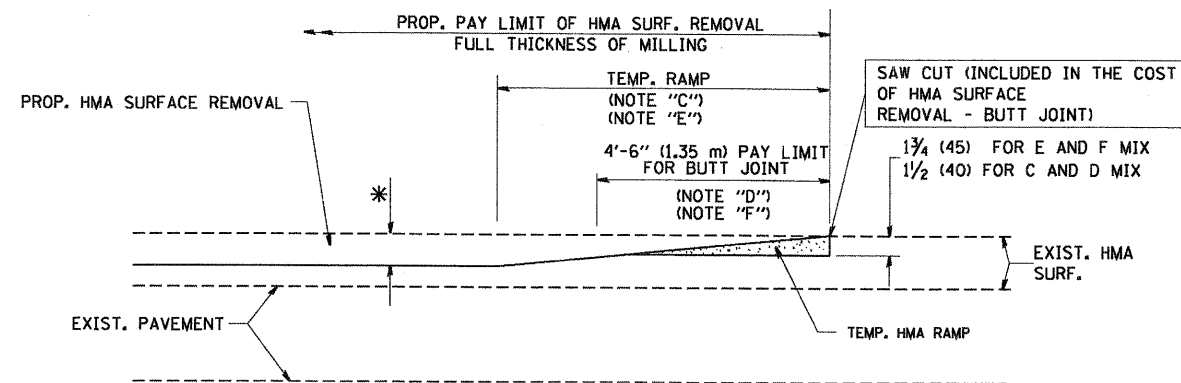
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STA. N/A	TO STA. N/A

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CONTRACT NO. 63661				
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT				

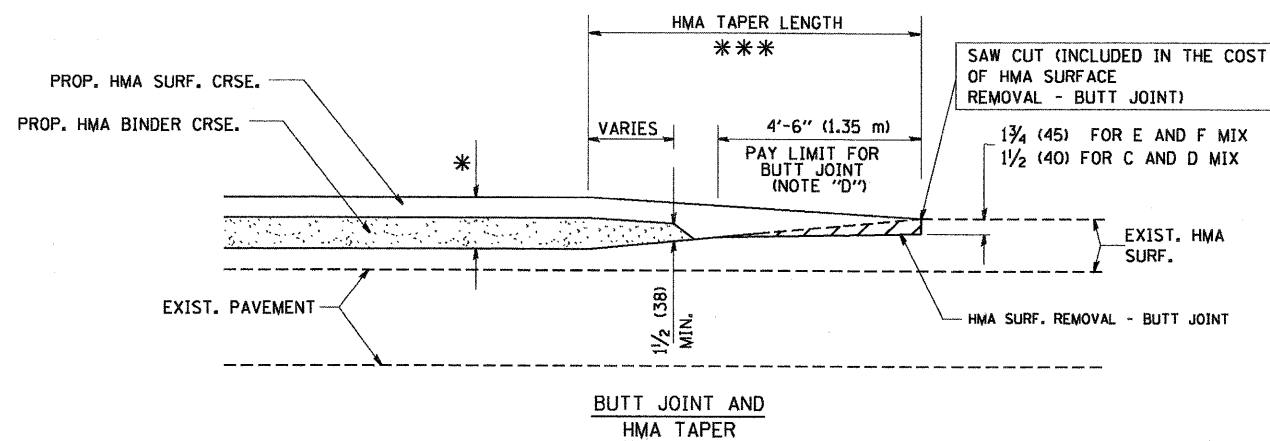
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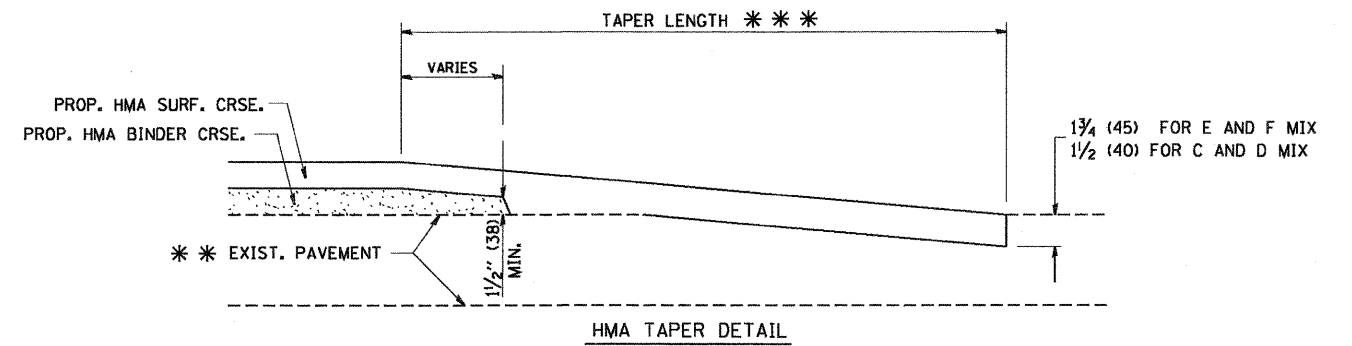
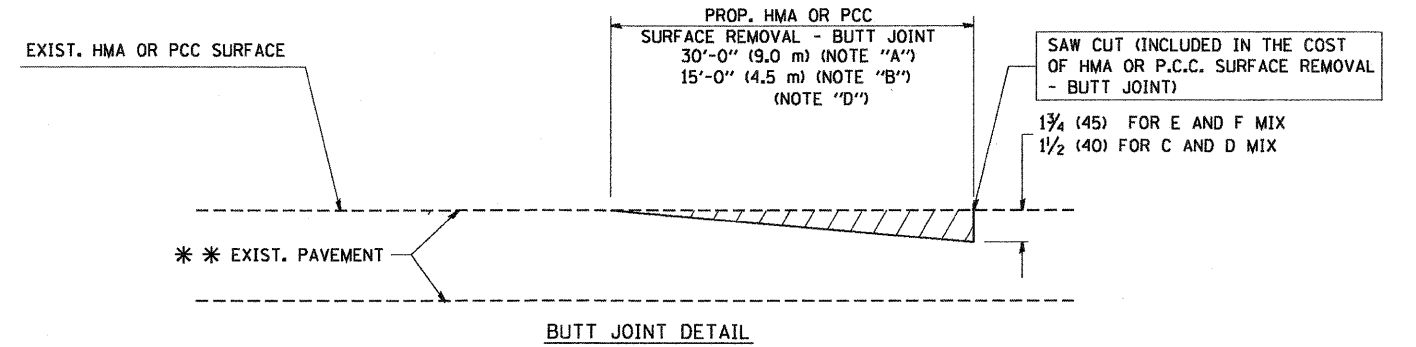
**OPTION 1**



**OPTION 2**  
**TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**



**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

Plotted: November 22, 2011 @ 10:38 AM By: Kris Pung - Tab: 33 Details (22x34)  
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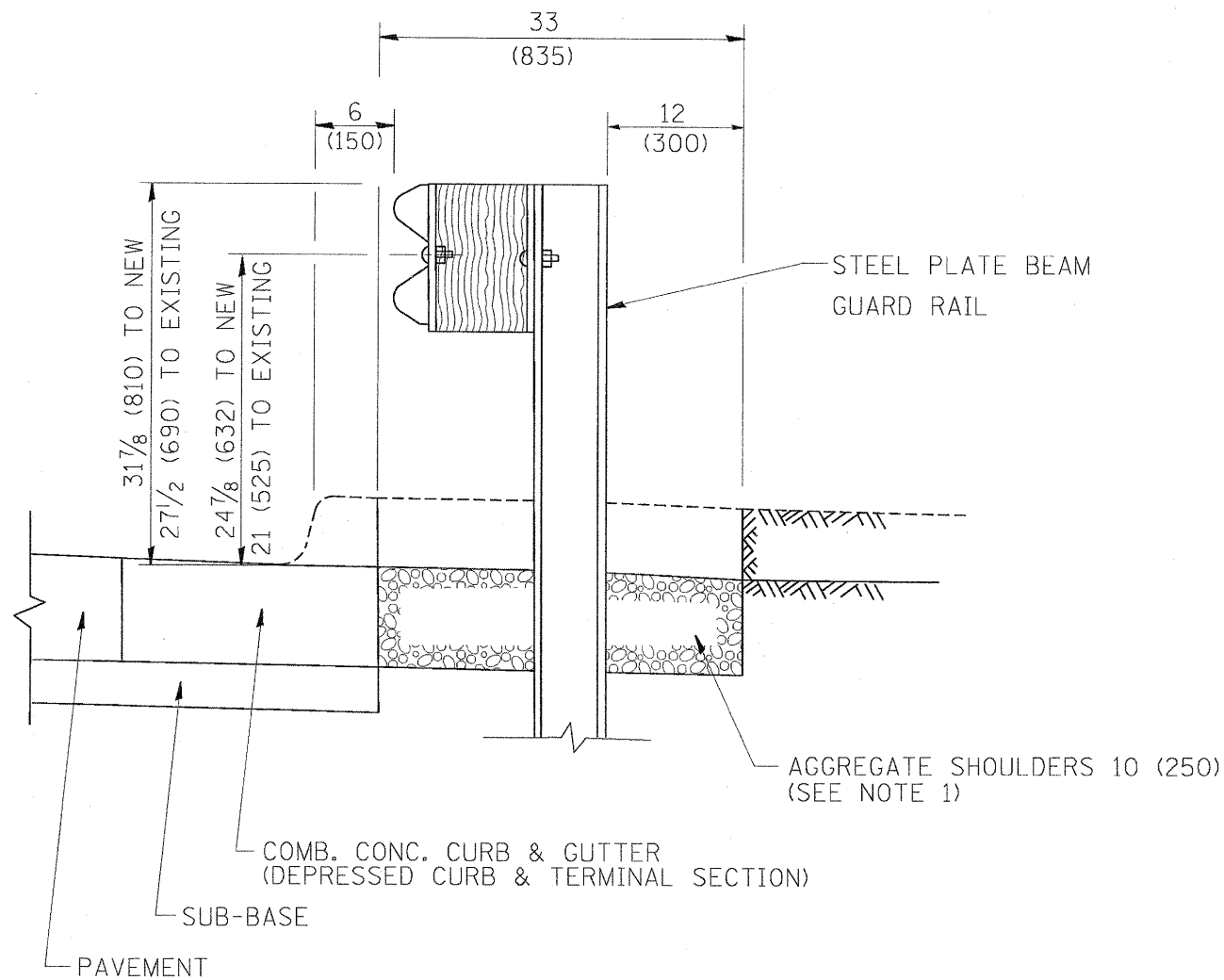
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE BUTT JOINTS AND HMA TAPER**

PROJECT NO. BH05-0089(134)	JOB NO. C-91-167-09
SCALE: N/A	SHEET NO. 1 OF 1 SHEETS
STA. N/A	TO STA. N/A

F.A.U. RTE. 3895	SECTION 08-15104-01-BR	COUNTY KANE	TOTAL SHEETS 38	SHEET NO. 33
BD400-05 BD32		CONTRACT NO. 63661		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

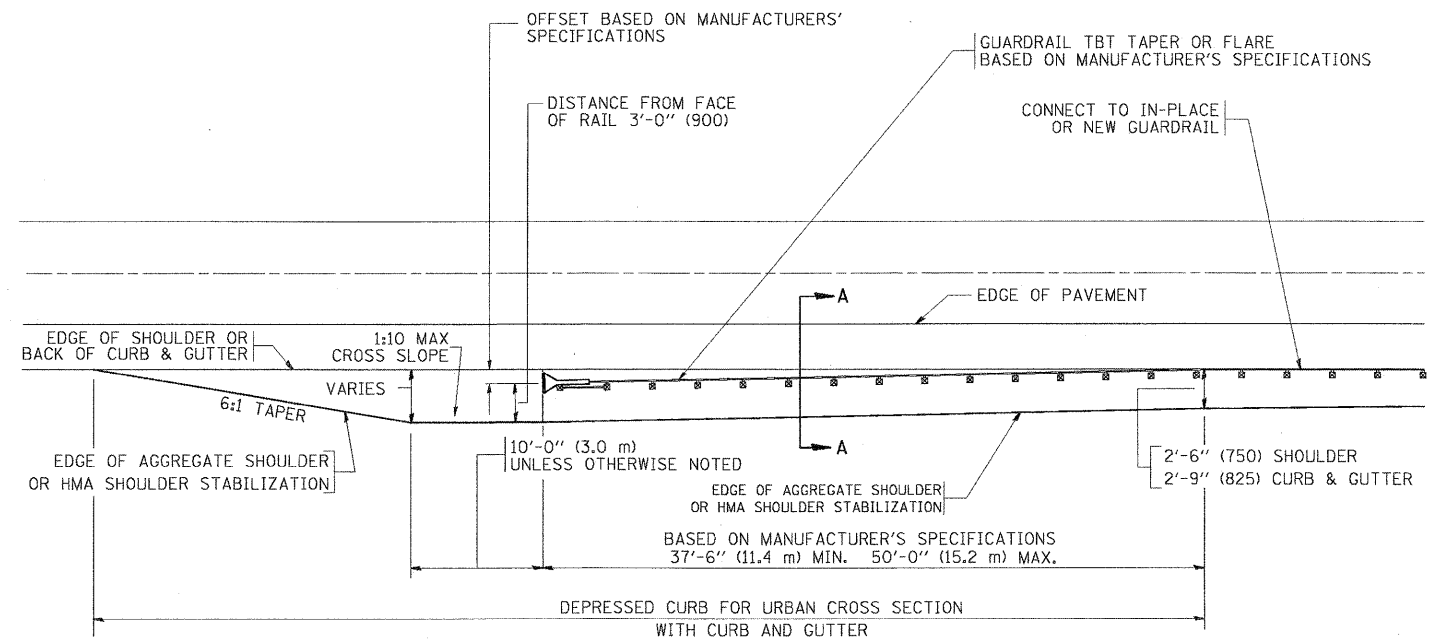
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**SECTION A-A**

- NOTES: 1. THE AGGREGATE SHOULDER, 10" OR HMA SHOULDER, 6" (IF REQUIRED) SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL.
2. "EXISTING" GUARDRAIL REFERS TO CONNECTING TERMINAL SECTION TO GUARD RAILING PRIOR TO THE MIDWEST GUARDRAIL SYSTEM.
3. THE CONTRACTOR SHALL VERIFY THE TYPE/HEIGHT OF GUARDRAIL IN-PLACE BEFORE ORDERING THE NEW TERMINAL SECTION. COST INCLUDED WITH THE COST OF THE TERMINAL. THE TERMINAL SECTION HEIGHT TO BE PLACED MUST MATCH THE HEIGHT OF THE IN-PLACE GUARDRAIL.

**DETAILS FOR STEEL PLATE BEAM  
GUARD RAIL ADJACENT TO CURB AND GUTTER  
[FOR ROADWAY SPEED 35 MPH (60 kmh) TO 45 MPH (70 kmh)]**



**DEPRESSED CURB AND GUTTER AND  
SHOULDER TREATMENT AT TBT TY. 1 SPL.**

BASIS OF PAYMENT: HMA SHOULDERS 6 (150) (IF REQUIRED) WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SHOULDERS 6" (150 mm)".

STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

TBT = TRAFFIC BARRIER TERMINAL  
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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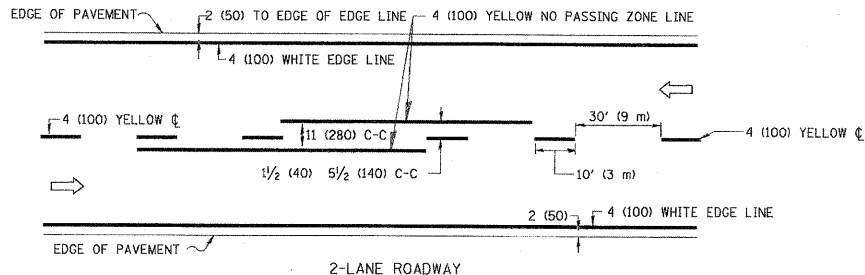
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

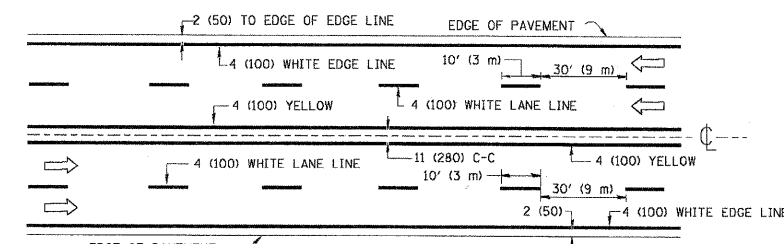
**DISTRICT ONE DETAILS FOR DEPRESSED CURB & GUTTER  
AND SHOULDER TREATMENT AT TBT 1 SPL.**

PROJECT NO. BHOS-0089(134)	JOB NO. C-91-167-09	F.A.U. RTE. 3895	SECTION 08-15104-01-BR	COUNTY KANE	TOTAL SHEETS 38	SHEET NO. 34
SCALE: N/A	SHEET NO. 1 OF 1 SHEETS	STA. N/A	TO STA. N/A	BD600-10 (BD 34) CONTRACT NO. 63661		
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT						

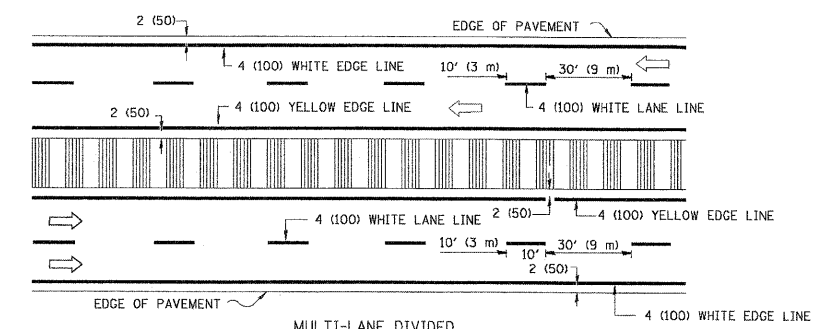
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2-LANE ROADWAY



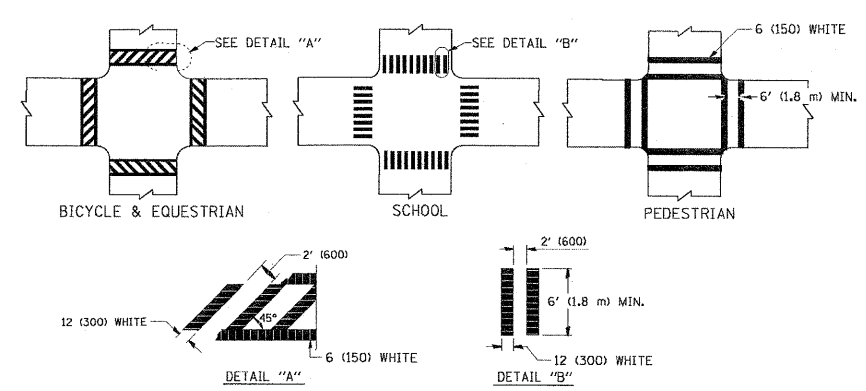
MULTI-LANE UNDIVIDED



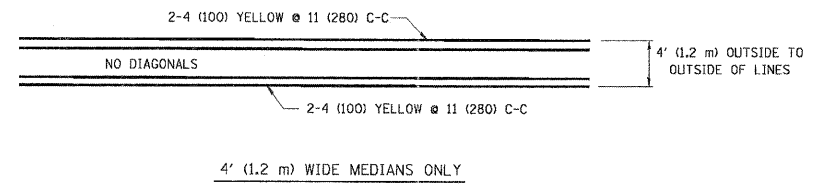
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

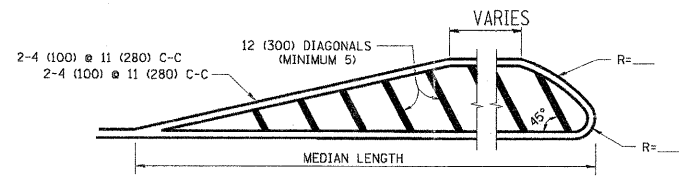
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

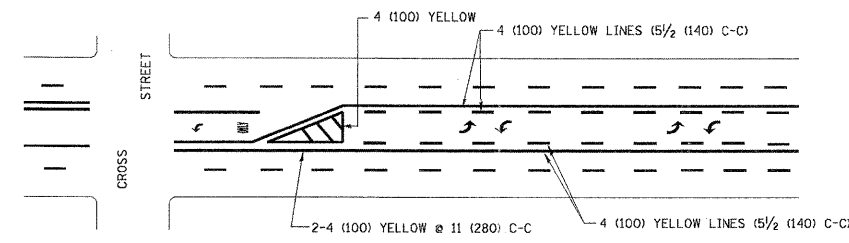


4' (1.2 m) WIDE MEDIANS ONLY

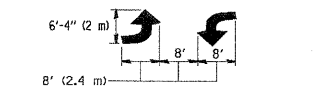


MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

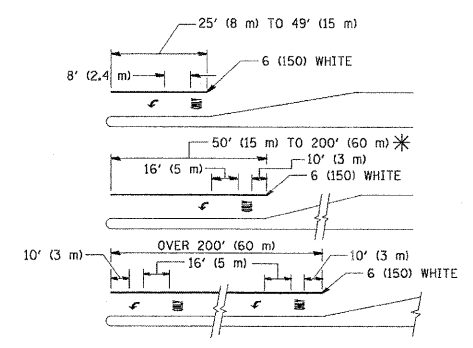


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

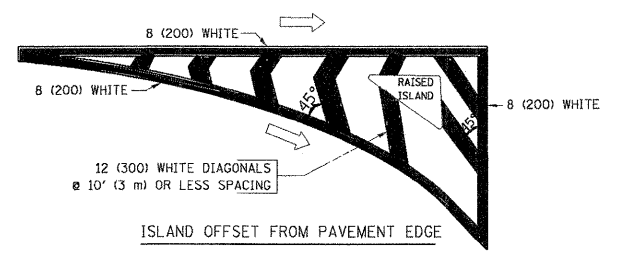


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

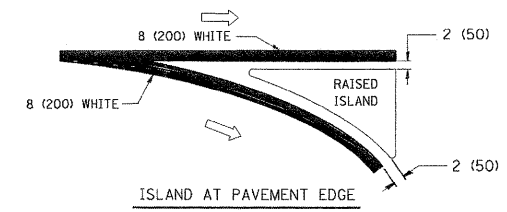
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

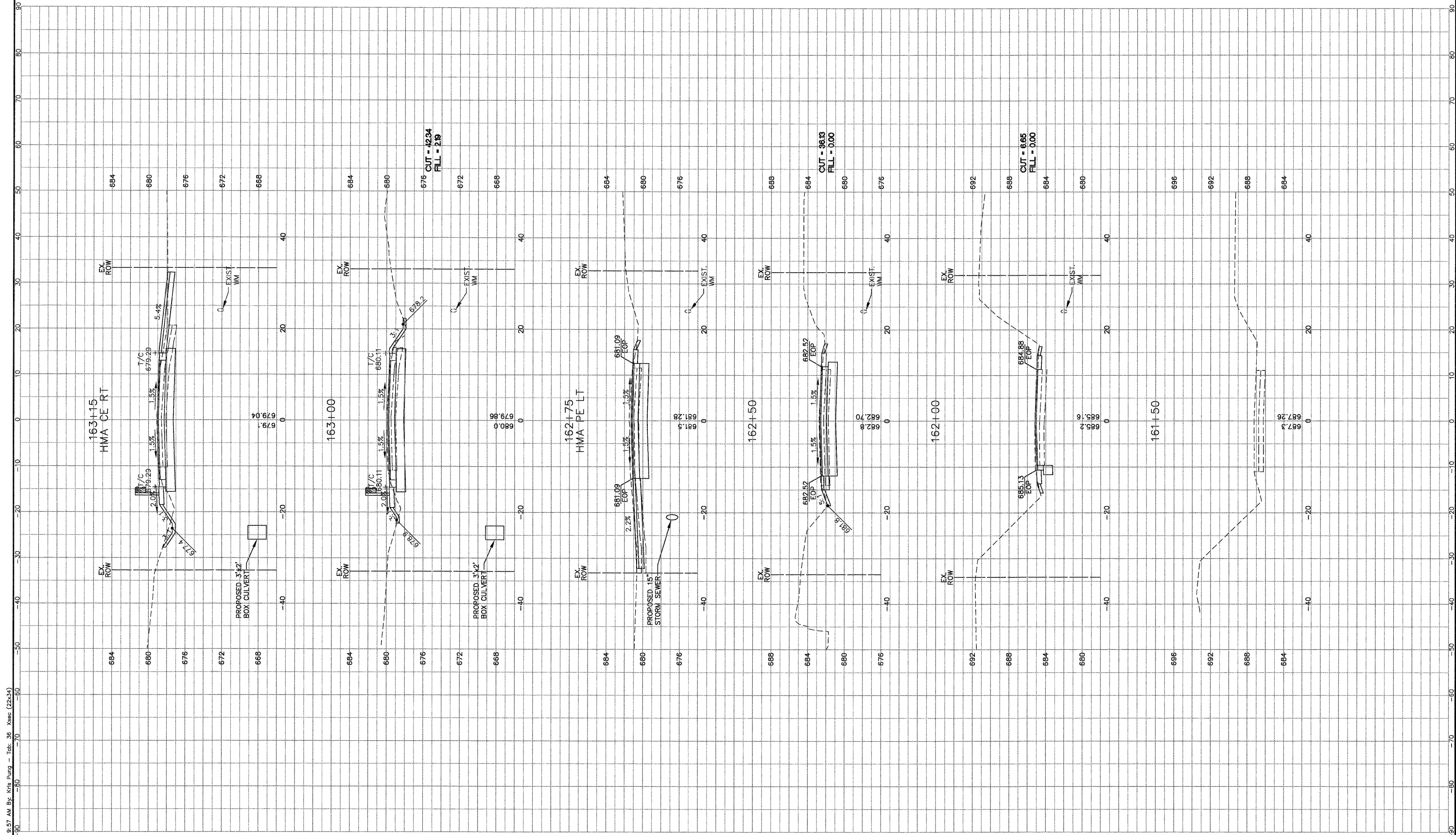
DISTRICT ONE			
TYPICAL PAVEMENT MARKINGS			
PROJECT NO. BH05-0089(134)	JOB NO. C-91-167-09	F.A.U. RTE. 3895	SECTION 08-15104-01-BR
SCALE: N/A	SHEET NO. 1 OF 1 SHEETS	STA. N/A	TO STA. N/A

TOTAL SHEETS 38	SHEET NO. 35
CONTRACT NO. 63661	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS**

PROJECT NO. BHOS-0089-(134)	JOB NO. C-91-167-09
SHEET NO. 1 OF 3 SHEETS	STA. 161+50 TO STA. 163+15

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3895	08-15104-01-BR	KANE	38	36
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63661	

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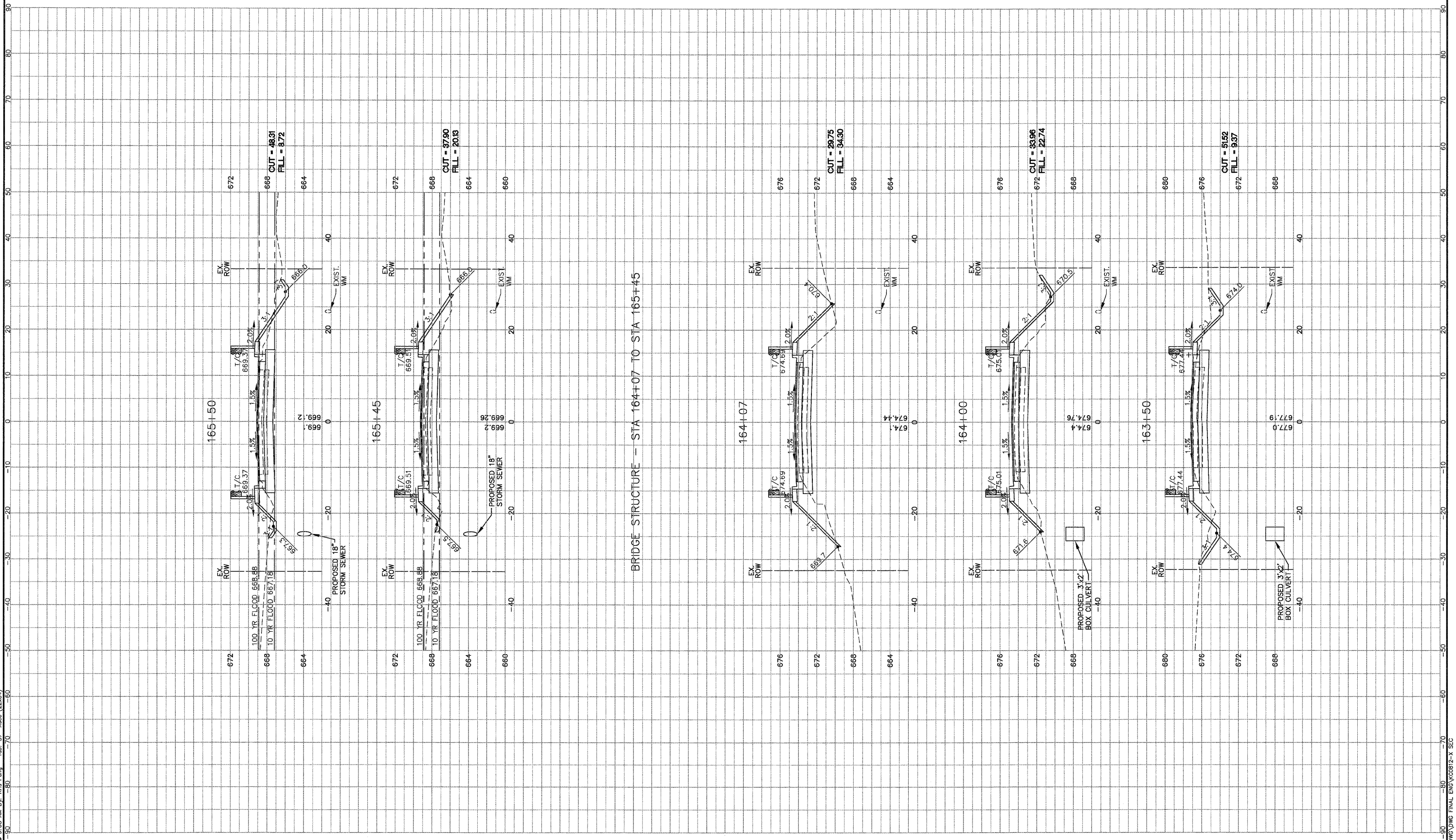
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS

PROJECT NO. BHOS-0089-(134) JOB NO. C-91-167-09  
SCALE: 1"=10'H, 1"=5'V SHEET NO. 2 OF 3 SHEETS STA. 163+50 TO STA. 165+50

F.A.U. RTE. 3895	SECTION 08-15104-01-BR	COUNTY KANE	TOTAL SHEETS 38	SHEET NO. 37
CONTRACT NO. 63661				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS

PROJECT NO. BHOS-0089-(134) JOB NO. P-91-167-09  
SCALE: 1"=10'H, 1"=5'V SHEET NO. 3 OF 3 SHEETS STA. 166+00 TO STA. 168+00

F.A.U. RTE. 3895	SECTION 08-15104-01-BR	COUNTY KANE	TOTAL SHEETS 38	SHEET NO. 38
CONTRACT NO. 63661			FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT	

