

03-09-12 LETTING ITEM 072

FOR INDEX OF SHEETS SEE SHEET NO. 2

F.A.P. ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0524	11-00080-00-RS	KANE	15	1

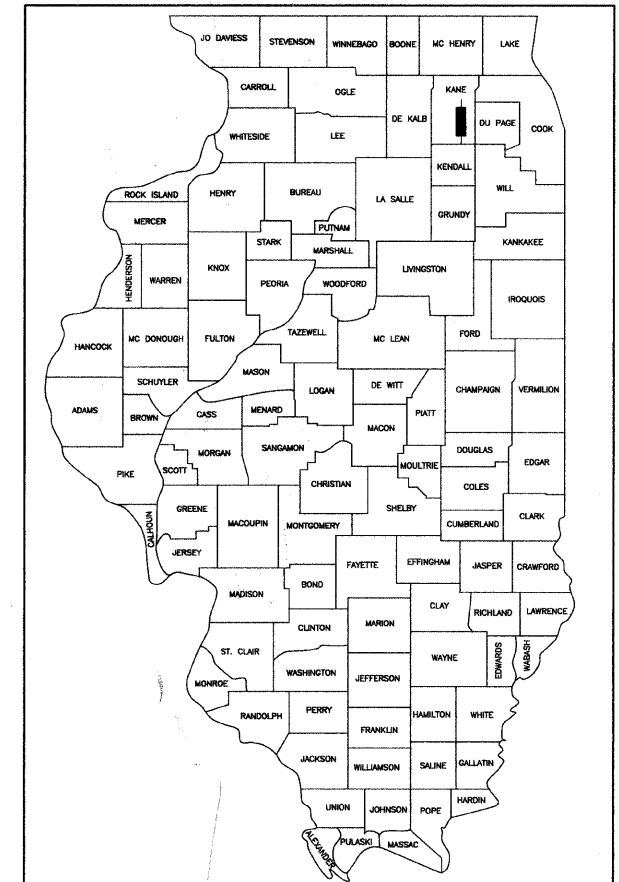
CONTRACT NO. 63670

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
**DIVISION OF HIGHWAYS**  
**PLANS FOR PROPOSED FEDERAL AID HIGHWAY**  
**F.A.P. ROUTE 0524 (MAIN STREET)**  
**RANDALL ROAD TO VAN NORTWICK AVENUE**  
**RESURFACING**  
**SECTION: 11-00080-00-RS**  
**PROJECT NO. M-9003(879)**  
**CITY OF BATAVIA**  
**KANE COUNTY**  
**C-91-117-12**

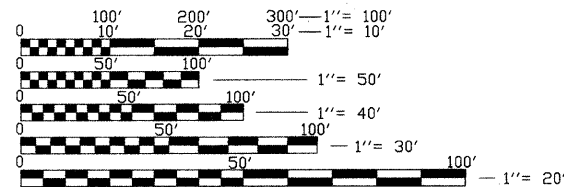
**TRAFFIC DATA**

ADT (2005) = 8,900-11,500  
 POSTED SPEED 30 MPH  
 DESIGN SPEED 35 MPH

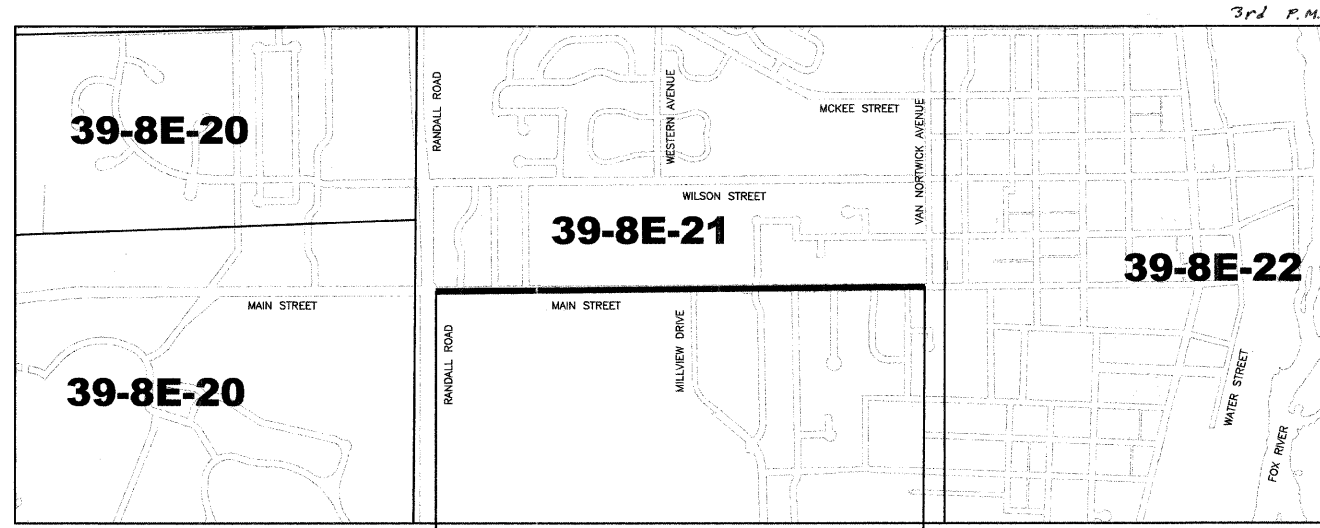
FUNCTIONAL CLASSIFICATION: COLLECTOR



LOCATION OF SECTION INDICATED THUS: — —



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.



BATAVIA TOWNSHIP

PROJECT BEGINS  
 STA. 10+49.73  
 MAIN STREET

PROJECT ENDS  
 STA. 59+83.11  
 MAIN STREET

J.U.L.I.E.  
 JOINT UTILITY LOCATION FOR EXCAVATION  
 1-800-892-0123 or 811  
  
 I.D.O.T. BUREAU OF TRAFFIC  
 MEADE ELECTRIC  
 1-773-287-7672  
  
 KANE COUNTY DEPARTMENT OF TRANSPORTATION  
 1-630-208-3130

LOCATION MAP  
 N.T.S.

PROJECT LOCATION = TOWNSHIP 39 NORTH,  
 RANGE 8 EAST, BATAVIA TOWNSHIP, 3RD P.M.  
 GROSS LENGTH OF PROJECT = 4,933.39 FEET (0.93 MILES)  
 NET LENGTH OF PROJECT = 4,933.39 FEET (0.93 MILES)

PROJECT LOCATED IN  
 THE CITY OF BATAVIA

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 DIVISION OF HIGHWAYS

APPROVED: Noel Basquin, P.E.  
 Noel Bz  
 CITY OF BATAVIA, CITY ENGINEER

PASSED: JANUARY 5, 2012  
C. Holt  
 BUREAU CHIEF OF LOCAL ROADS AND STREETS

RELEASING FOR BID  
 BASED ON LIMITED  
 REVIEW: JANUARY 10, 2012  
Diana M. O'Hara  
 DEPUTY DIRECTOR OF HIGHWAYS, REGION #1 ENGINEER

DATE: 12/14/11

BY: Karen R. Young, P.E.  
 KAREN R. YOUNG, P.E.  
 REGISTERED P.E. STATE OF ILLINOIS

LICENSE EXPIRES: NOVEMBER 30, 2013

SEAL

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 OF THE STATE OF ILLINOIS**

PROGRAM AND OFFICE ENGINEER CHARLES F. RIDDLE (847) 705-4406 SCHAUMBURG

..Streets\STR\_11\_005\_Min\_LAPP\Cad 09\Plan Sheets\01\_COR.dwg(01\_COR\_VR\_MAP)  
 PLOTTED: Wednesday, 14 Dec 2011 - 3:40pm  
 SAVER: Wednesday, 14 Dec 2011 - 11:02am

**CONTRACT NO. 63670**

**GENERAL NOTES**

1. ALL REFERENCES TO THE STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE JANUARY 1, 2012 VERSION OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AS PREPARED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION.
2. EXISTING UTILITIES ARE SHOWN ON THE PLANS ACCORDING TO RECORD INFORMATION OBTAINED FROM THE UTILITY COMPANIES, CITY OF BATAVIA, ILLINOIS DEPARTMENT OF TRANSPORTATION AND/OR OTHER OFFICES AND AGENCIES ASSOCIATED WITH THE DEVELOPMENT OF THESE PLANS. THESE SOURCES DO NOT GUARANTEE THEIR ACCURACY OR COMPLETENESS. THE CONTRACTOR WILL BE REQUIRED TO VERIFY THE LOCATION, EXISTENCE, AND NATURE OF ALL UTILITIES AND SHALL TAKE DUE CARE DURING ALL PHASES OF THE CONSTRUCTION TO PROTECT EXISTING UTILITY FACILITIES FROM DAMAGE THAT MAY RESULT FROM THE WORK. DAMAGED UTILITIES SHALL BE REPAIRED AT THE CONTRACTORS EXPENSE IN ACCORDANCE WITH SPECIAL PROVISION LR105.
3. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, THE CITY OF BATAVIA AND KANE COUNTY DEPARTMENT OF TRANSPORTATION.
4. BEFORE STARTING EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123, THE CITY OF BATAVIA, KANE COUNTY (630-208-3130), AND IDOT BUREAU OF TRAFFIC (MEADE ELECTRIC) (773-287-7672) FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION REQUIRED).
5. NO WORK SHALL COMMENCE UNTIL TRAFFIC CONTROL REQUIREMENTS ARE MET.
6. THE ENGINEER SHALL BE NOTIFIED BY THE CONTRACTOR PRIOR TO THE START OF CONSTRUCTION IN ACCORDANCE WITH ARTICLE 108.02.
7. UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER, ALL EXISTING ACCESS POINTS SHALL BE MAINTAINED AT ALL TIMES BY THE CONTRACTOR.
8. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE WITHOUT WRITTEN CONSENT FROM THE ENGINEER.
9. FULL-DEPTH SAW CUTS SHALL BE USED TO REMOVE EXISTING PAVEMENT, CURB AND GUTTER, SIDEWALK, DRIVEWAYS, BUTT JOINTS AND APPURTENANCES FROM MATERIAL TO REMAIN, IN ACCORDANCE WITH SECTION 440 OF THE "STANDARD SPECIFICATIONS". THE COST OF THE SAWING SHALL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED. IT IS THE CONTRACTORS RESPONSIBILITY TO DETERMINE THE THICKNESS OF THE EXISTING PAVEMENT, SIDEWALK, CURB/GUTTER AND WHETHER OR NOT IT CONTAINS REINFORCEMENT. ALL REINFORCEMENT SHALL BE ADEQUATELY SAW CUT AS TO NOT CAUSE DAMAGE TO THE ADJACENT MATERIAL DURING REMOVAL. FOR SIDEWALK REMOVAL ADJACENT TO AN EXISTING ASPHALT/CONCRETE DRIVEWAY NOT CALLED OUT FOR REMOVAL, THE CONTRACTOR MUST REMOVE THE SIDEWALK BY HAND AS TO NOT DAMAGE THE ADJACENT PAVEMENT. ANY DAMAGE TO THE ADJACENT PAVEMENT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO FIX.
10. ALL DIMENSIONS, INCLUDING RADII, ARE GIVEN TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
11. THE CONTRACTOR IS RESPONSIBLE TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.
12. BASE COURSE SHALL NOT BE PLACED ADJACENT TO CURB AND GUTTER UNTIL AFTER THE CURB AND GUTTER HAS BEEN PROPERLY BACKFILLED TO THE SATISFACTION OF THE ENGINEER.
13. PRIOR TO PLACING HOT-MIX ASPHALT ADJACENT TO EXISTING PAVEMENT TO REMAIN, THE EXPOSED EDGE SHALL BE CLEANED OF LOOSE MATERIAL TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE HOT-MIX ASPHALT BEING PLACED.
14. HOT-MIX ASPHALT SURFACE COURSE SHALL NOT BE PLACED UNTIL ALL WORK INCLUDING TOP SOIL PLACEMENT, AND HOT-MIX ASPHALT BINDER COURSE HAS BEEN COMPLETED TO THE SATISFACTION OF THE ENGINEER.
15. TEN FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED ITEMS OF WORK TO EXISTING ITEMS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEM OF WORK SPECIFIED.
16. THE CONTRACTOR SHALL NOT CROSS COMPLETED BASE COURSE, OR EXISTING PAVEMENT NOT SCHEDULED TO BE REMOVED, WITH TRACK EQUIPMENT.
17. THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SAND BAGS ON EACH TYPE I OR II BARRICADE USED, ONE (1) SAND BAG ACROSS EACH BOTTOM RAIL. TYPE III BARRICADES SHALL HAVE FOUR (4) WEIGHTED SANDBAGS.
18. ANY SIGN LOCATED IN THE PUBLIC RIGHT-OF-WAY WHICH INTERFERES WITH CONSTRUCTION OF THE PROPOSED ROADWAY WORK OR LIGHTING SYSTEM, THAT IS INTENDED TO BE MAINTAINED SHALL BE RELOCATED.
19. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY SIGNS DAMAGED BY HIS/HER CONSTRUCTION ACTIVITIES AND WILL REPLACE THEM AT NO ADDITIONAL COST.
20. ON ALL IMPROVEMENTS, THE FRAMES AND LIDS OF EXISTING CATCH BASINS, INLETS, MANHOLES, AND VALVE VAULTS WHICH ARE TO BE ABANDONED DUE TO CONSTRUCTION OF THIS IMPROVEMENT ARE TO REMAIN THE PROPERTY OF THE CITY OF BATAVIA AND BE SALVAGED. THESE ITEMS SHALL BE DELIVERED TO THE CITY OF BATAVIA PUBLIC WORKS YARD LOCATED AT 200 NORTH RADDANT ROAD BATAVIA IL 60510
21. **MAINTENANCE OF SEWER FLOWS** - THE CONTRACTOR SHALL CONDUCT HIS OPERATIONS TO MAINTAIN AT ALL TIMES FLOW THROUGH EXISTING STORM AND SANITARY SEWER SYSTEMS. HE/SHE SHALL PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT IF NECESSARY AND A TEMPORARY OUTLET AND BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER COLLECTED IN A SAFE MANNER WITHOUT DAMAGE OF ANY KIND TO ADJACENT PROPERTIES. THE ENDS OF EXISTING DRAINAGE LINES WHICH ARE NOT TO BE INCORPORATED INTO THE PROJECT ARE TO BE SEALED AS SPECIFIED IN THE SPECIAL PROVISIONS. EXISTING STRUCTURES ARE TO BE INSPECTED BEFORE CONSTRUCTION STARTS - ALL ACCUMULATION OF MATERIAL SHALL BE REMOVED IN THE STRUCTURES DUE TO THE CONSTRUCTION OPERATIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF FRAMES AND GRATES TO BE ADJUSTED.
22. FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EPOXY COATED, UNLESS OTHERWISE NOTED ON THE PLAN.
23. **PRIME COAT** - PRIME COAT MUST BE INSTALLED NO EARLIER THAN TWENTY-FOUR (24) HOURS PRIOR TO PLACEMENT OF HOT-MIX ASPHALT.
24. **BUTT JOINTS** - BUTT JOINT WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT). IN ACCORDANCE WITH THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
25. **MILLED PAVEMENT OPEN TO TRAFFIC** - WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 40 MM (1.5 INCHES) WHERE THE SPEED LIMIT IS 80 KM/H (45 MPH). A MAXIMUM GRADE DIFFERENTIAL OF 75 MM (3 INCHES) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
26. **PORTLAND CEMENT CONCRETE SIDEWALK** - IF UPON REMOVAL OF THE EXISTING SIDEWALK A SUITABLE SUB-BASE MATERIAL IS ENCOUNTERED, THE NEW CONCRETE MAY BE CONSTRUCTED OVER THAT EXISTING SUB-BASE. HOWEVER, IF A SOFT OF UNSTABLE SUB-BASE IS ENCOUNTERED, THIS MATERIAL SHALL BE EXCAVATED AND REPLACED WITH A MINIMUM OF 4" AGGREGATE BASE COURSE, TYPE B, CA6. SIDEWALK TO BE INSTALLED AT NEW LOCATIONS WILL REQUIRE THE INSTALLATION OF 4" AGGREGATE BASE COURSE, TYPE B, CA6. THE EXCAVATION AND STONE SHALL BE INCIDENTAL TO THE ITEM BEING INSTALLED. AT LOCATIONS WHERE THE SIDEWALK IS ADJACENT TO DRIVEWAYS OR CURB AND GUTTER, ALL VOIDS FROM THE TOP OF SUB-BASE TO THE BOTTOM OF SIDEWALK SHALL BE FILLED WITH AGGREGATE BASE COURSE, TYPE B. AT LOCATIONS WHERE SIDEWALK IS TO BE CONSTRUCTED ACROSS TRENCHES, THREE (3) #10 TEN FOOT LONG REINFORCEMENT BARS SHALL BE PLACED IN THE SIDEWALK CENTERED OVER THE TRENCH. THESE REINFORCEMENT BARS SHALL NOT BE CONTINUOUS THROUGH TRAVERSE EXPANSION JOINTS BUT SHALL BE STOPPED 3 INCHES SHORT OF SAME. THE COST OF THESE REINFORCEMENT BARS, COMPLETE IN PLACE SHALL BE INCIDENTAL TO THE ITEM BEING INSTALLED.
27. **DETECTABLE WARNINGS** - THE CONTRACTOR SHALL UTILIZE THE VITRIFIED POLYMER COMPOSITE NON-REMOVABLE DETECTABLE TACTILE WARNING SYSTEM IN THE RED COLOR AS PRODUCED BY ARMOR-TILE OR APPROVED EQUAL IN CONFORMANCE WITH ADAAG. CURB RAMPS SHALL BE CONSTRUCTED TO THE SAME THICKNESS AS THE ADJACENT SIDEWALK WITH A MINIMUM THICKNESS OF 6 IN.
28. **HOT-MIX ASPHALT SURFACE REMOVAL** - STREETS THAT HAVE CRACK SEAL MATERIAL AT THE JOINT ALONG THE CURB AND THE PAVEMENT EDGE WILL REQUIRE THE REMOVAL OF THIS MATERIAL PRIOR TO THE PAVING OPERATIONS. THE CRACK SEAL MATERIAL SHALL BE COMPLETELY REMOVED TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE INCIDENTAL TO THE ITEM BEING REMOVED. WHEN THE MILLED PAVEMENT SURFACES ARE OPEN TO TRAFFIC THE FOLLOWING WILL BE REQUIRED: THE FIRST LIFT OF RESURFACING OR LEVEL BINDER SHALL BE PLACED WITHIN 2 WORKING DAYS AFTER THE PAVEMENT SURFACE HAS BEEN MILLED. IF IT IS NOT RESURFACED WITHIN FIVE CALENDAR DAYS, THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN THE PAVEMENT AT HIS EXPENSE. UNDER NO CIRCUMSTANCES SHALL MILLED PAVEMENT BE ALLOWED TO REMAIN OVER WINTER.

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





- 1 COVER SHEET
- 2 INDEX OF SHEETS, LEGEND  
GENERAL NOTES, I.D.O.T. STANDARD DRAWINGS
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- 4 EXISTING & PROPOSED TYPICAL CROSS SECTIONS
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- 15 DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

**LIST OF STANDARD DRAWINGS**

- |           |  |
|-----------|--|
| 000001-06 | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS                   |
| 424001-06 | PERPENDICULAR CURB RAMPS FOR SIDEWALKS                         |
| 424006    | DIAGONAL CURB RAMPS FOR SIDEWALKS                              |
| 424011    | CORNER PARALLEL CURB RAMPS FOR SIDEWALKS                       |
| 424021    | DEPRESSED CORNER FOR SIDEWALKS                                 |
| 442201-03 | CLASS C AND D PATCHES  |
| 606001-04 | CONCRETE CURB AND COMBINATION CONCRETE CURB AND GUTTER         |
| 701501-06 | URBAN LANE CLOSURE, 2L, 2W UNDIVIDED                           |
| 701601-07 | URBAN LANE CLOSURE, MULTILANE, 1W OR 2W, NONTRAVERSABLE MEDIAN |
| 701701-08 | URBAN LANE CLOSURE, MULTILANE INTERSECTION                     |
| 701801-05 | LANE CLOSURE, MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE |
| 701901-02 | TRAFFIC CONTROL DEVICES  |
| 780001-03 | TYPICAL PAVEMENT MARKINGS                                      |

NOTE: ALL BOXED GENERAL NOTES ARE INCLUDED IN THE COST OF OTHER ITEMS.

**LEGEND OF SYMBOLS**

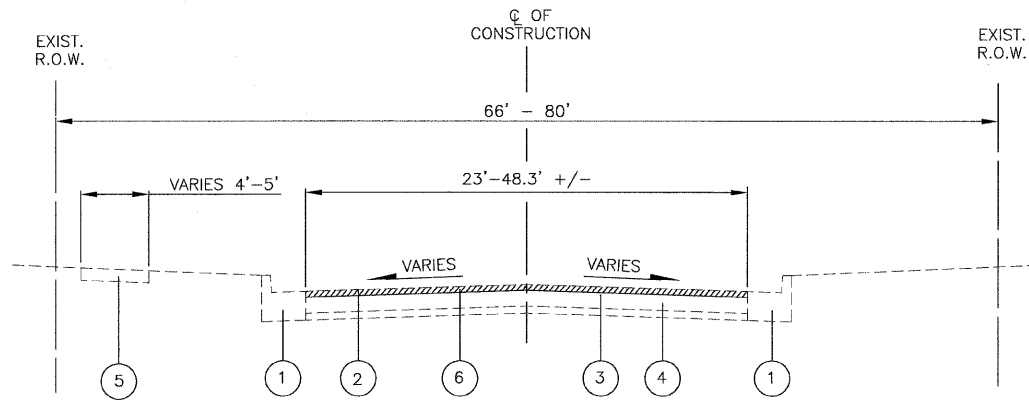
- |   |   |
|---|---|
|  | DRIVEWAY PAVEMENT REMOVAL                         |
|  | HOT-MIX ASPHALT DRIVEWAY PAVEMENT                 |
|  | SIDEWALK REMOVAL                                  |
|  | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT      |
|  | AGGREGATE SHOULDERS, TYPE B                       |
|  | COMBINATION CONCRETE CURB AND GUTTER REMOVAL      |
|   | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 |
| <b>A</b>  | ADJUST STRUCTURE OR FRAME & GRATE                 |
| <b>P</b>  | PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH          |
| <b>R</b>  | SIDEWALK REMOVAL                                  |

FILE NAME =	USER NAME =	DESIGNED -- KRY	REVISED -- 1/9/12	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MAIN STREET LAPP GENERAL NOTES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -- DMO	REVISED --			0524	11-00080-00-RS	KANE	15	2	
		CHECKED -- NAB	REVISED --			CONTRACT NO. 63670					
		DATE -- 10/21/11	REVISED --			SCALE: N.T.S.	SHEET NO. 2 OF 15 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	

**SUMMARY OF QUANTITIES  
CONSTRUCTION TYPE CODE 0005**

CODE	PAY ITEM DESCRIPTION	UNIT	TOTAL QUANTITY
	40300100 BITUMINOUS MATERIALS (PRIME COAT)	GAL	4008
	40600625 LEVELING BINDER (MACHINE METHOD), N50	TON	672.7
	40600895 CONSTRUCTION TEST STRIP	EACH	1
	40600982 HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	312.2
	40603335 HOT-MIX ASPHALT SURFACE COURSE, MIX 'D', N50	TON	1571.2
S	42400200 PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	195.6
	42400800 DETECTABLE WARNINGS	SQ FT	86
	44000159 HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	16032.5
S	44000200 DRIVEWAY PAVEMENT REMOVAL	SQ YD	317.1
	44000500 COMBINATION CURB AND GUTTER REMOVAL	FOOT	87
	44000600 SIDEWALK REMOVAL	SQ FT	261.6
S	44201757 CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	100
S	44201843 CLASS D PATCHES, TYPE III, 16 INCH	SQ YD	100
S	44201879 CLASS D PATCHES, TYPE III 19 INCH	SQ YD	100
	44300200 STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	2500
	48101200 AGGREGATE SHOULDERS, TYPE B	TON	296.6
S	54213657 PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12"	EACH	10
	54247090 GRATING FOR FLARED END SECTION 12"	EACH	10
S	550A0050 STORM SEWERS, CLASS A TYPE 1 12"	FOOT	237
	55100500 STORM SEWER REMOVAL 12"	FOOT	237
S	60603800 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	87
	67100100 MOBILIZATION	L SUM	1
S	70102620 TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1
S	70102630 TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1
S	70102635 TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1
S	70102640 TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1
*	78000100 THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	190
*	78000200 THERMOPLASTIC PAVEMENT MARKING - LINE 4'	FOOT	11890
*	78000400 THERMOPLASTIC PAVEMENT MARKING - LINE 6'	FOOT	1374
*	78000600 THERMOPLASTIC PAVEMENT MARKING - LINE 12'	FOOT	348
*	78000650 THERMOPLASTIC PAVEMENT MARKING - LINE 24'	FOOT	94.5
S *	88600600 DETECTOR LOOP REPLACEMENT	FOOT	322
S	Z0004510 HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3'	SQ YD	317.1

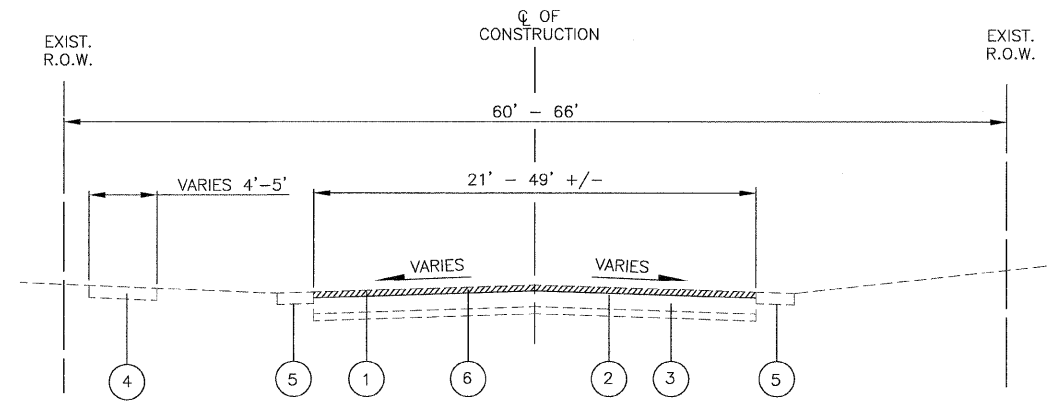
\* = DENOTES SPECIALTY ITEM  
S = SPECIAL PROVISION



- ① EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12

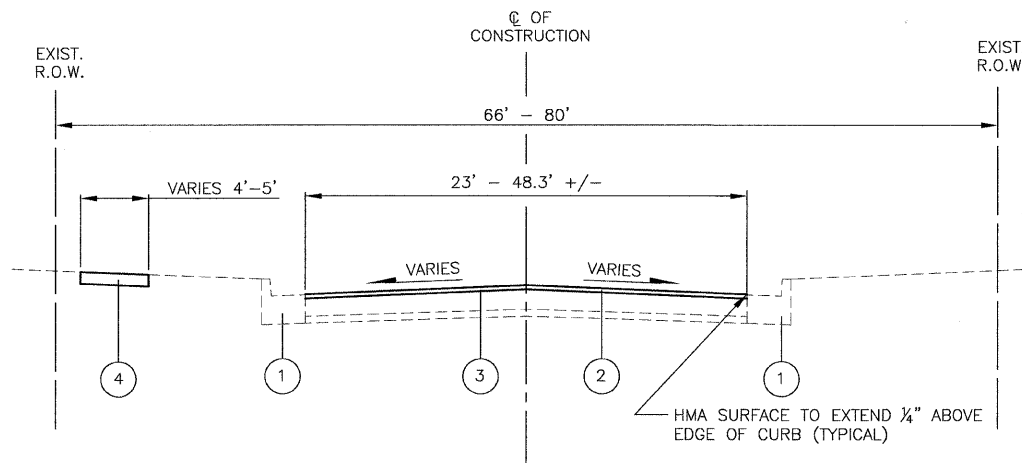
	10+49.73 - 16+00	16+00 - 20+45.5
② EXISTING HOT-MIX ASPHALT SURFACE	14" - 16"	9" - 9.5"
③ EXISTING PORTLAND CEMENT CONCRETE PAVEMENT	0"	7" - 9"
④ EXISTING AGGREGATE BASE	2" - 8"	0"
⑤ EXISTING PORTLAND CEMENT CONCRETE SIDEWALK		
⑥ HOT-MIX ASPHALT SURFACE REMOVAL - 2.5"		

EXISTING TYPICAL SECTION  
MAIN STREET  
STA. 10+49.73 TO STA. 20+24.5



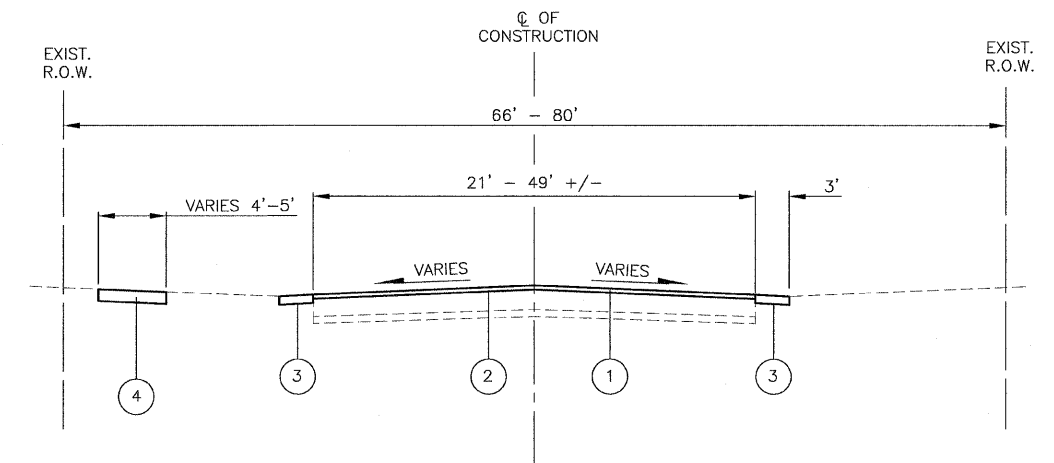
- ① EXISTING HOT-MIX ASPHALT SURFACE 6" - 9"
- ② EXISTING PORTLAND CONCRETE PAVEMENT 6.25" - 9"
- ③ EXISTING AGGREGATE BASE 0" - 3.5"
- ④ EXISTING PORTLAND CEMENT CONCRETE SIDEWALK
- ⑤ EXISTING AGGREGATE SHOULDER
- ⑥ HOT-MIX ASPHALT SURFACE REMOVAL - 2.5"

EXISTING TYPICAL SECTION  
MAIN STREET  
STA. 20+24.5 TO STA. 59+83.11



- ① COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
- ② HOT-MIX SURFACE COURSE, MIX "D", N50, 1.75"
- ③ LEVELING BINDER (MACHINE METHOD), N50 - 0.75"
- ④ PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH - 2% MAX CROSS-SLOPE

PROPOSED TYPICAL SECTION  
MAIN STREET  
STA. 10+49.73 TO STA. 20+24.5



- ① HOT-MIX SURFACE COURSE, MIX "D", N50, 1.75"
- ② LEVELING BINDER (MACHINE METHOD), N50 - 0.75"
- ③ AGGREGATE SHOULDERS, TYPE B, TOP DRESS ONLY
- ④ PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH - 2% MAX CROSS-SLOPE

PROPOSED TYPICAL SECTION  
MAIN STREET  
STA. 20+24.5 TO STA. 59+83.11

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

PAY ITEM DESCRIPTION	VOIDS
RESURFACING - HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 1.75" (IL-9.5MM)	4% @ 50
RESURFACING - LEVELING BINDER (MACHINE METHOD), N50 (IL -9.5 MM) - 0.75"	4% @ 50
PATCHING - CLASS D PATCHES (HMA BINDER IL-19MM) (SPECIAL)	4% @ 70
DRIVEWAY - HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 3"	4% @ 50

NOTES:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITY IS 112 LBS/SY/IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA, THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP", SEE DISTRICT ONE SPECIAL PROVISIONS.
3. THE CONTRACTOR SHALL MILL BEFORE PATCHING.
4. PATCHING - MAXIMUM LIFT OF 3".

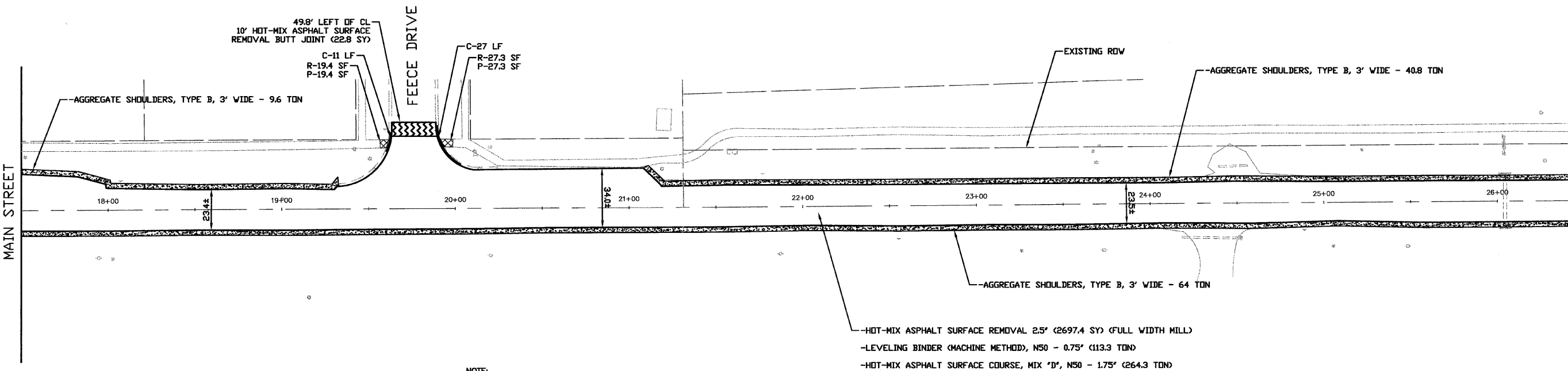
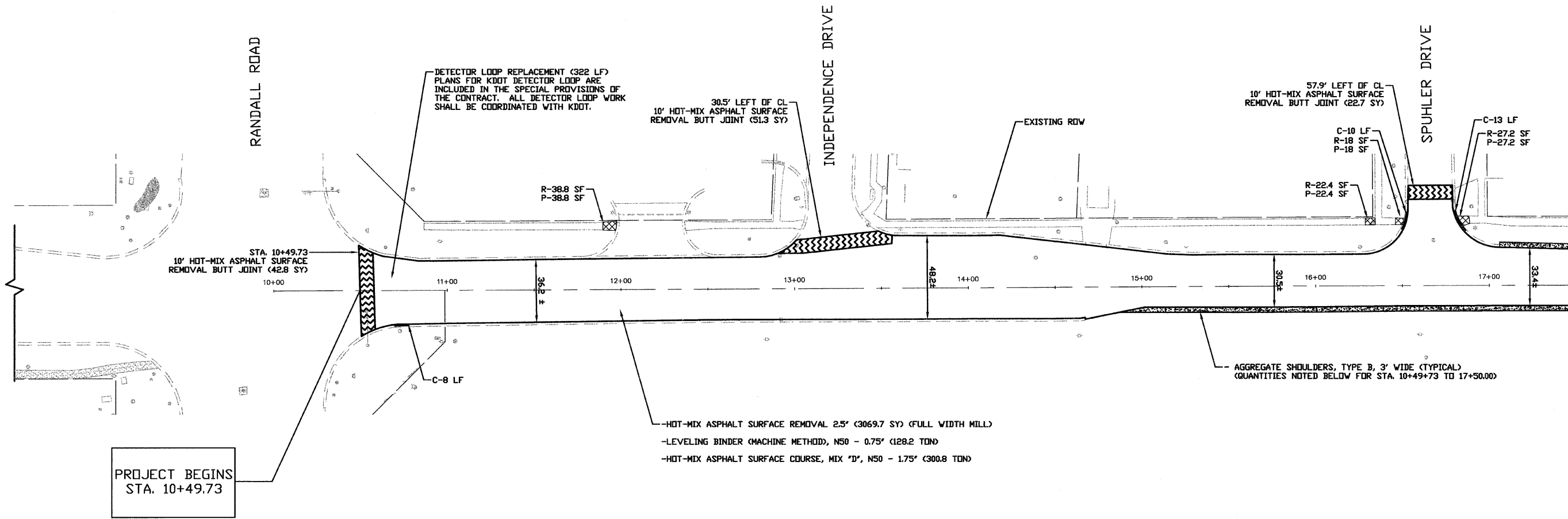
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		CHECKED - NAB	REVISED -
		DATE - 10/21/11	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

MAIN STREET LAPP  
TYPICAL SECTIONS

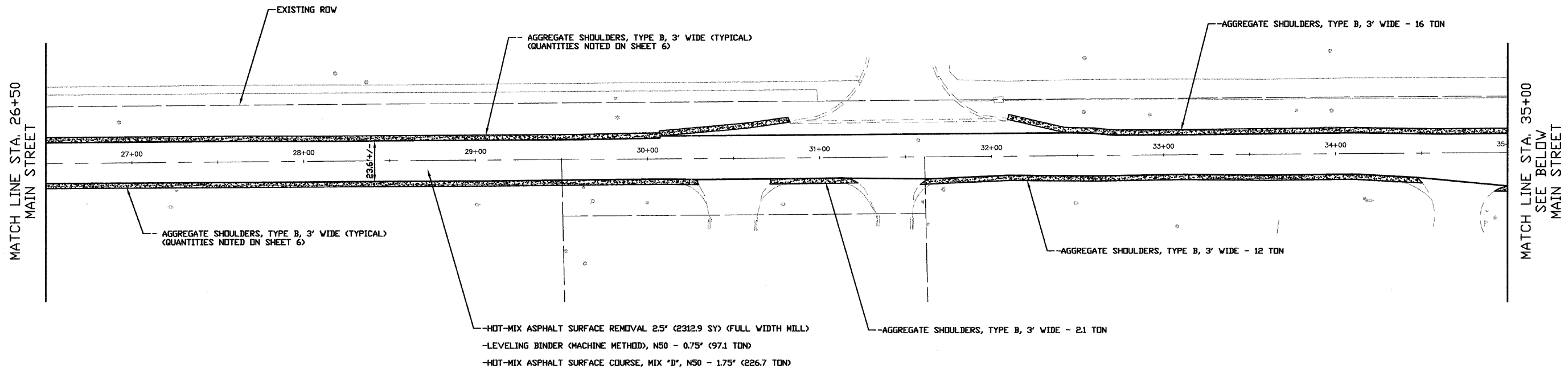
SCALE: N.T.S. SHEET NO. 4 OF 15 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0524	11-00080-00-RS	KANE	15	4
CONTRACT NO. 63670			ILLINOIS FED. AID PROJECT	

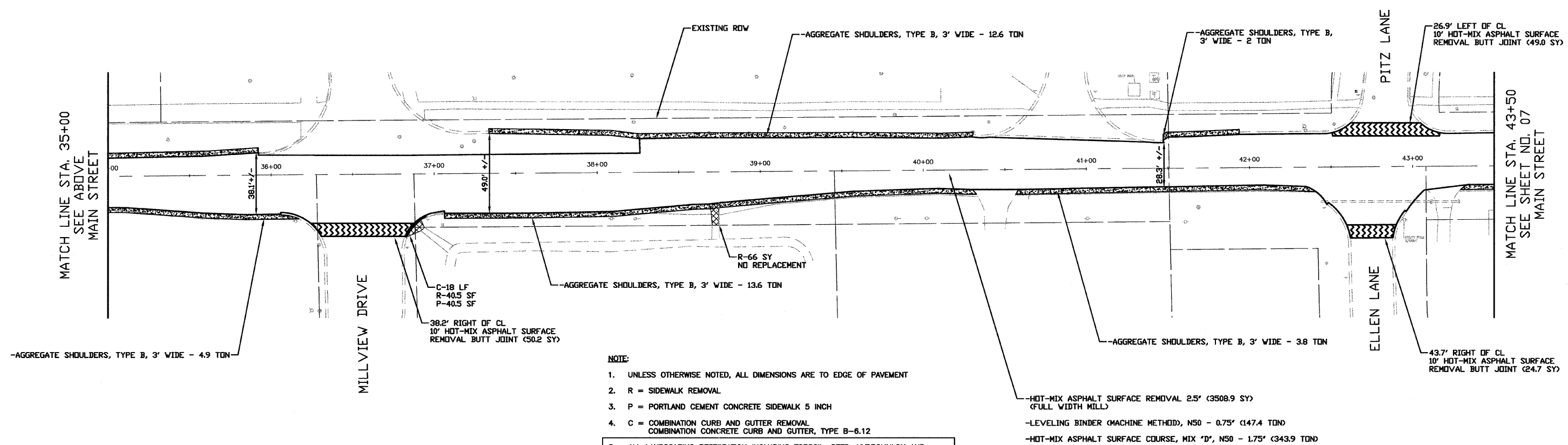


- NOTE:**
- UNLESS OTHERWISE NOTED, ALL DIMENSIONS ARE TO EDGE OF PAVEMENT
  - R = SIDEWALK REMOVAL
  - P = PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
  - C = COMBINATION CURB AND GUTTER REMOVAL  
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
  - ALL LANDSCAPING RESTORATION INCLUDING TOPSOIL, SEED, HYDROMULCH AND NUTRIENTS IS INCLUDED IN THE COST OF THE ITEM BEING REMOVED OR REPLACED.
  - ALL FRAMES & GRATES/LIDS SHALL REMAIN IN PLACE CONTRACTOR TO MILL AROUND.

FILE NAME =	USER NAME =	DESIGNED — KRY	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MAIN STREET LAPP PLAN SHEETS		F.A.P. RTE. 0524	SECTION 11-00080-00-RS	COUNTY KANE	TOTAL SHEETS 15	SHEET NO. 5	CONTRACT NO. 636670 ILLINOIS FED. AID PROJECT	
		DRAWN — DMO	REVISED —		SCALE: 1:30	SHEET NO. 05 OF 17 SHEETS	STA. 10+49.73 TO STA. 26+50.00						
		PLOT SCALE = 1:30	CHECKED — NAB										
		PLOT DATE = 12/14/2011	DATE — 10/21/11										



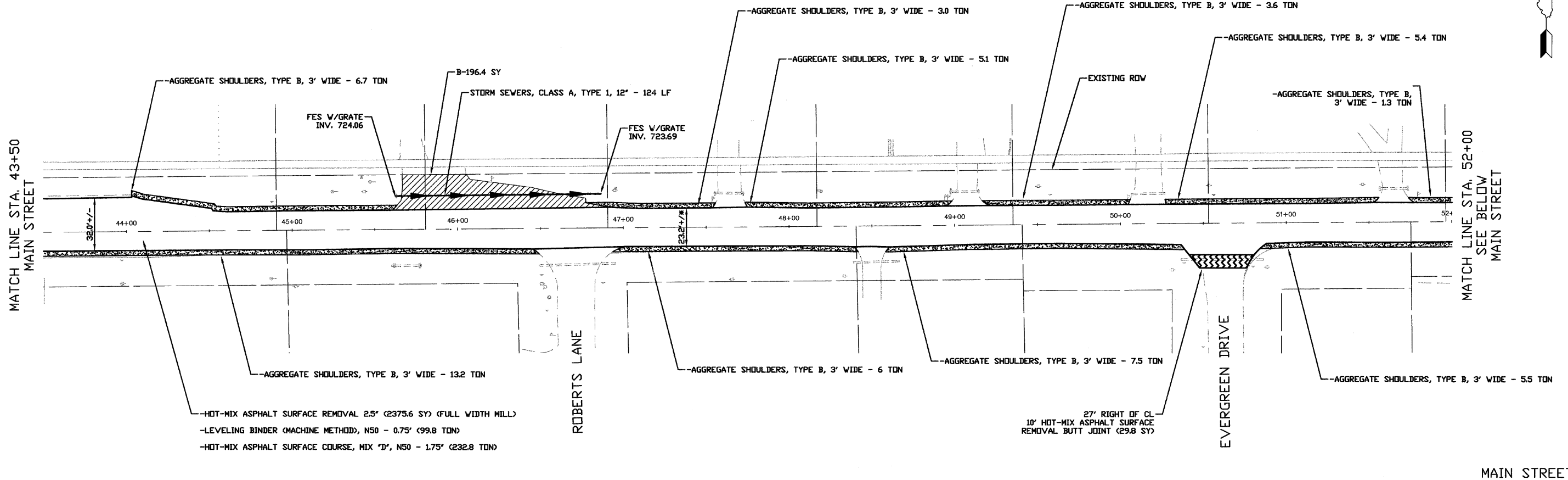
MAIN STREET



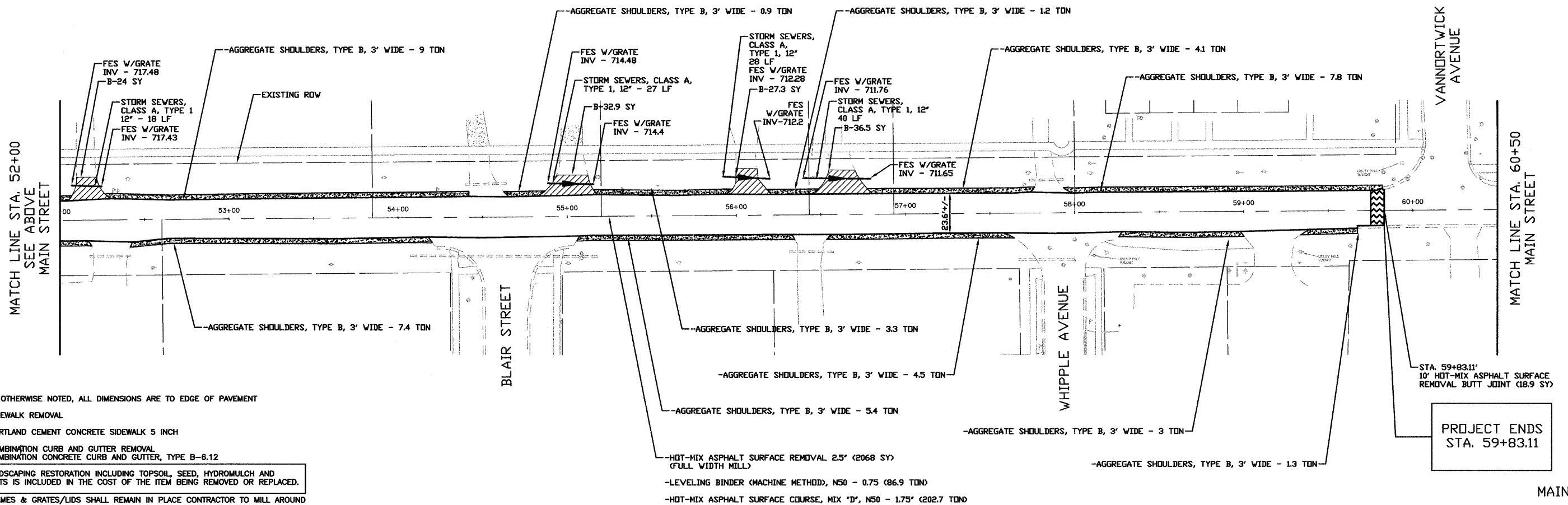
- NOTE:**
1. UNLESS OTHERWISE NOTED, ALL DIMENSIONS ARE TO EDGE OF PAVEMENT
  2. R = SIDEWALK REMOVAL
  3. P = PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
  4. C = COMBINATION CURB AND GUTTER REMOVAL COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
  5. ALL LANDSCAPING RESTORATION INCLUDING TOPSOIL, SEED, HYDROMULCH AND NUTRIENTS IS INCLUDED IN THE COST OF THE ITEM BEING REMOVED OR REPLACED.
  6. ALL FRAMES & GRATES/LIDS SHALL REMAIN IN PLACE CONTRACTOR TO MILL AROUND

MAIN STREET

FILE NAME =	USER NAME =	DESIGNED - KRY	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MAIN STREET LAPP PLAN SHEETS		F.A.P. RTE. 0524	SECTION 11-00080-00-RS	COUNTY KANE	TOTAL SHEETS 15	SHEET NO. 6
	PLOT SCALE = 1:30	DRAWN - DMO	REVISED -		SCALE: 1:30	SHEET NO. 06 OF 17 SHEETS	STA. 26+50.00 TO STA. 43+50.00	CONTRACT NO. 63670		ILLINOIS FED. AID PROJECT	
	PLOT DATE = 12/14/2011	CHECKED - NAB	REVISED -								
		DATE - 10/21/11	REVISED -								



MAIN STREET



MAIN STREET

**NOTE:**

1. UNLESS OTHERWISE NOTED, ALL DIMENSIONS ARE TO EDGE OF PAVEMENT
2. R = SIDEWALK REMOVAL
3. P = PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
4. C = COMBINATION CURB AND GUTTER REMOVAL  
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
5. ALL LANDSCAPING RESTORATION INCLUDING TOPSOIL, SEED, HYDROMULCH AND NUTRIENTS IS INCLUDED IN THE COST OF THE ITEM BEING REMOVED OR REPLACED.
6. ALL FRAMES & GRATES/LIDS SHALL REMAIN IN PLACE CONTRACTOR TO MILL AROUND

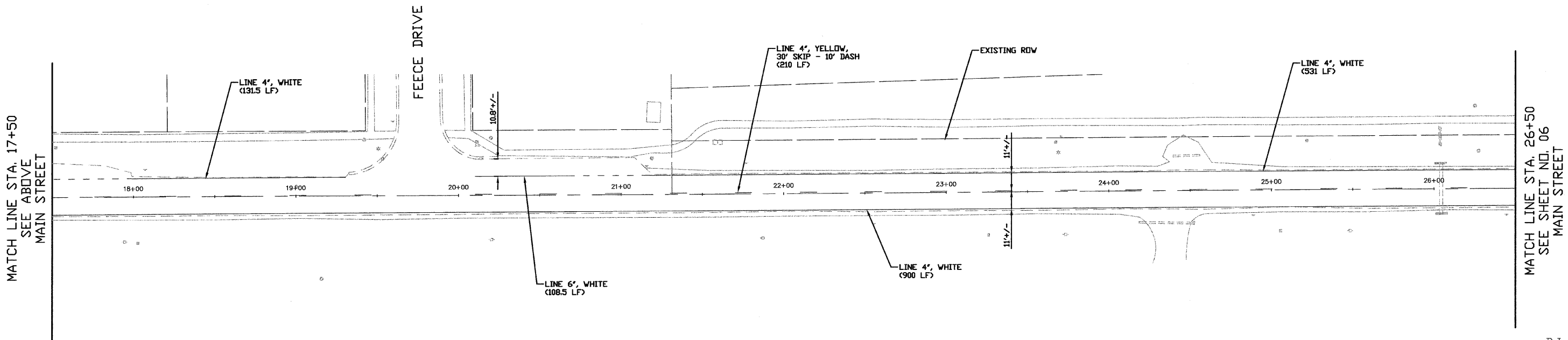
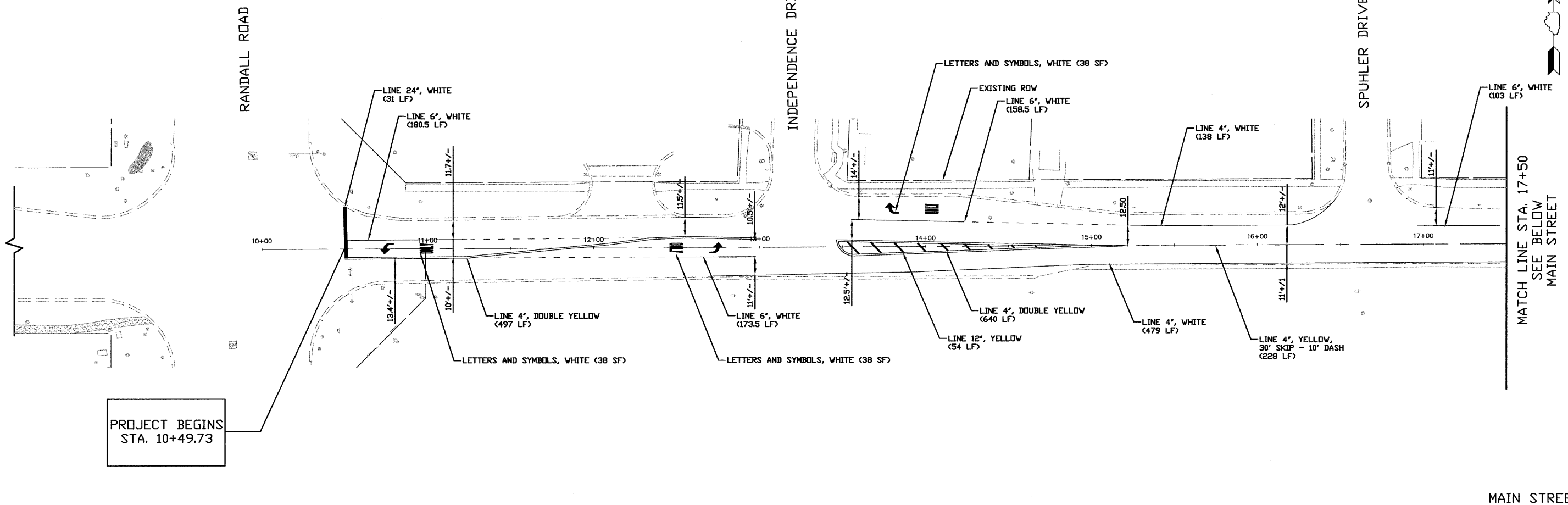
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

MAIN STREET LAPP  
PLAN SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0524	11-00080-00-RS	KANE	15	7
CONTRACT NO.			63670	
ILLINOIS FED. AID PROJECT				

FILE NAME =	USER NAME =	DESIGNED --	REVISED --
		DRAWN -- DMO	REVISED --
		CHECKED -- NAB	REVISED --
		DATE -- 10/21/11	REVISED --
PLOT SCALE = 1:30			
PLOT DATE = 12/14/2011			

SCALE: 1:30 SHEET NO. 07 OF 17 SHEETS STA. 60+50 TO STA. 43+50



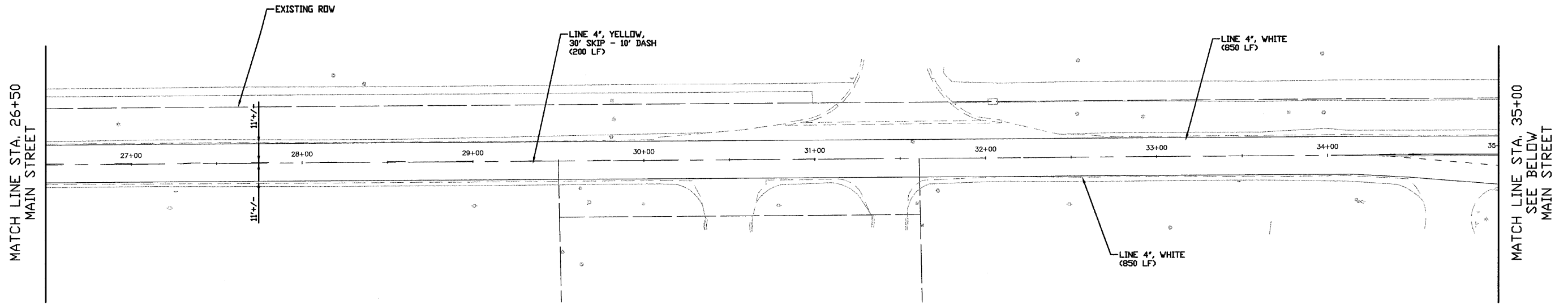
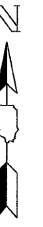
**NOTE:**  
 1. UNLESS OTHERWISE NOTED, ALL DIMENSIONS ARE TO EDGE OF PAVEMENT

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

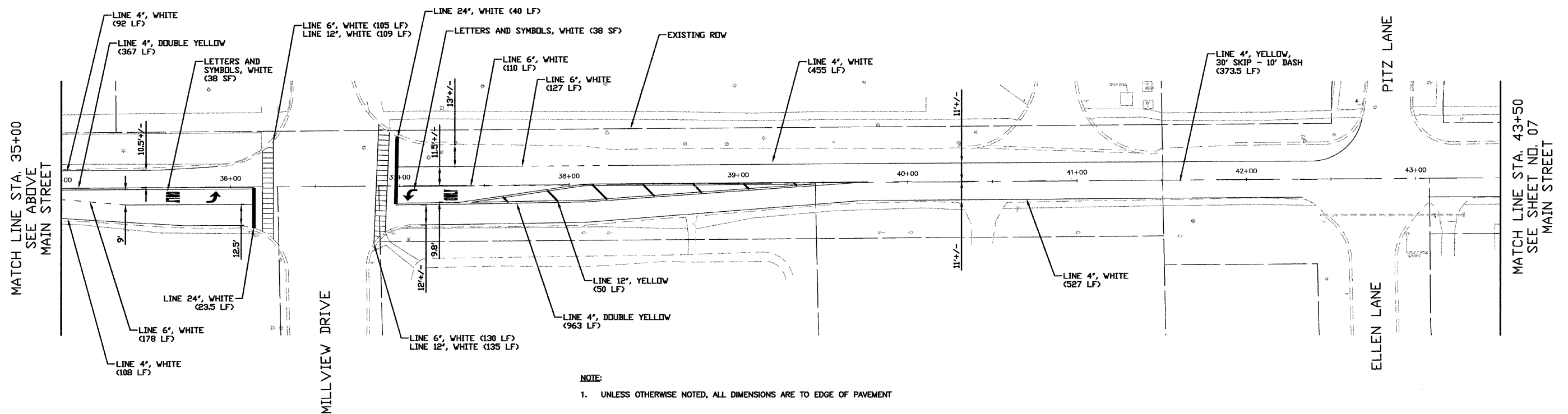
MAIN STREET LAPP  
 PAVEMENT MARKING

FILE NAME =	USER NAME =	DESIGNED — KRY	REVISED —	SCALE: 1:30	SHEET NO. 08 OF 17 SHEETS	STA. 10+49.73 TO STA. 26+50.00	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
		DRAWN — DMO	REVISED —				0524	11-00080-00-RS	KANE	15	8		
		PLOT SCALE = 1:30	REVISED —				CONTRACT NO. 63670						
		PLOT DATE = 12/14/2011	REVISED —				ILLINOIS FED. AID PROJECT						





MAIN STREET

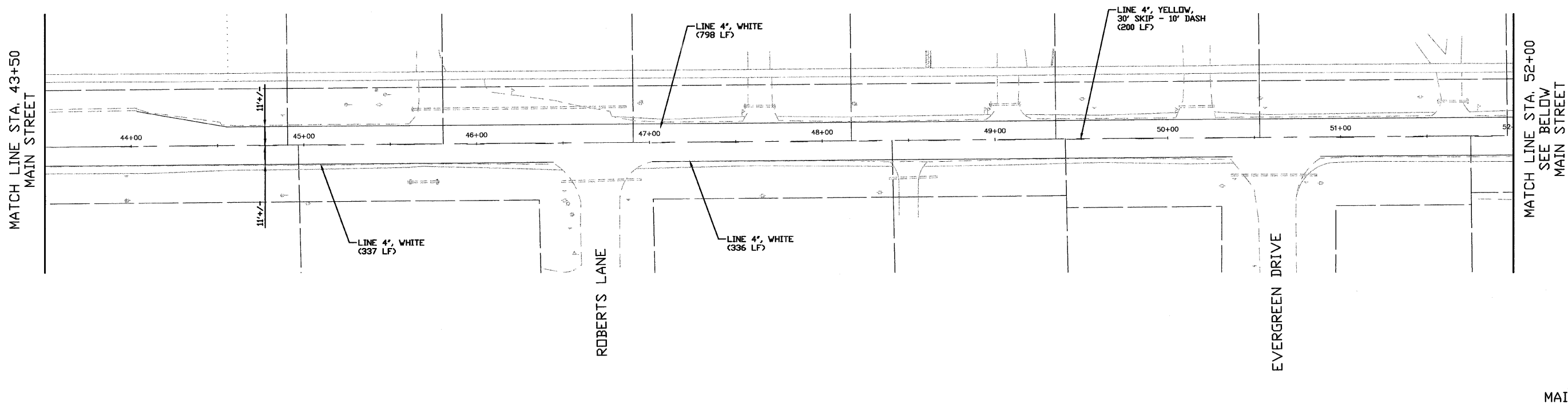


NOTE:  
1. UNLESS OTHERWISE NOTED, ALL DIMENSIONS ARE TO EDGE OF PAVEMENT

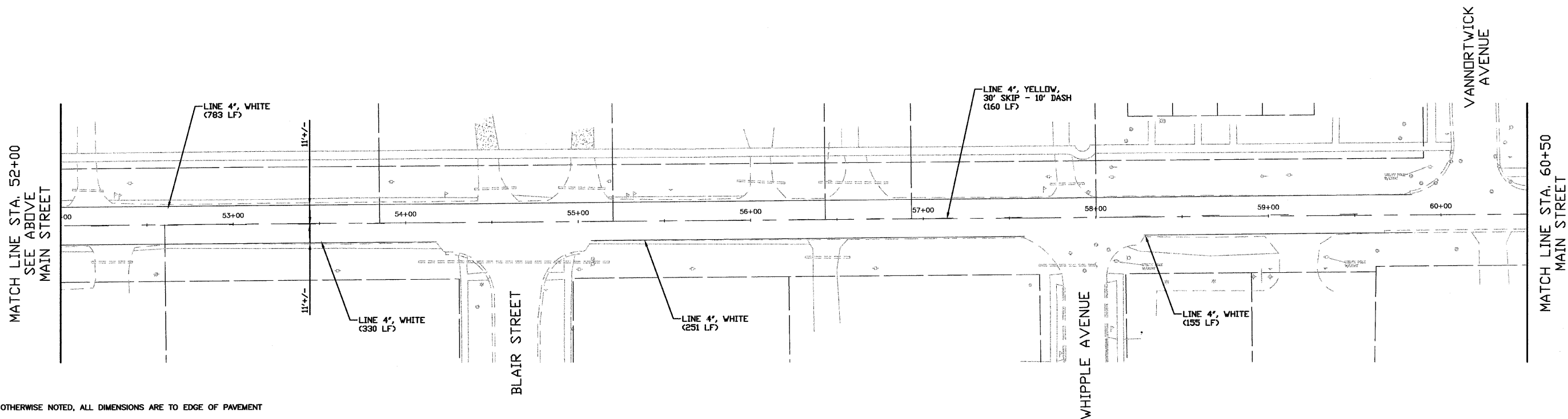
MAIN STREET



FILE NAME =	USER NAME =	DESIGNED — KRY	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MAIN STREET LAPP PAVMENT MARKING SHEETS		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 1:30	DRAWN — DMO	REVISED —				0524	11-00080-00-RS	KANE	15	9
	PLOT DATE = 12/14/2011	CHECKED — NAB	REVISED —				CONTRACT NO. 63670				
	DATE — 10/21/11	REVISED —		SCALE: 1:30	SHEET NO. 9 OF 17 SHEETS	STA. 26+50.00 TO STA. 43+50.00	ILLINOIS FED. AID PROJECT				



MAIN STREET

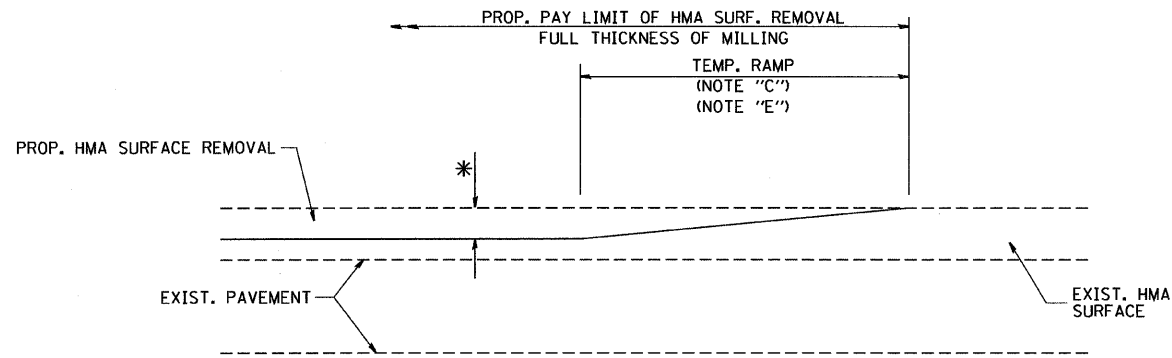


MAIN STREET



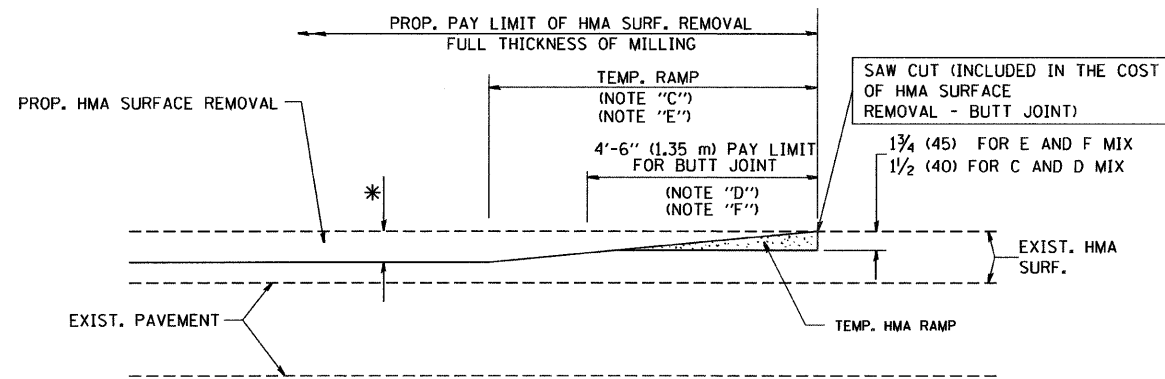
**NOTE:**  
 1. UNLESS OTHERWISE NOTED, ALL DIMENSIONS ARE TO EDGE OF PAVEMENT

FILE NAME =	USER NAME =	DESIGNED - NAB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MAIN STREET LAPP PAVEMENT MARKING SHEETS		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT DATE = 12/14/2011	CHECKED - KRY	REVISED -		SCALE: 1:30		SHEET NO. 10 OF 17 SHEETS	STA. 43+50	TO STA. 60+50	CONTRACT NO. 63670	ILLINOIS FED. AID PROJECT
	DATE - 10/21/11	REVISED -									



MILLED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

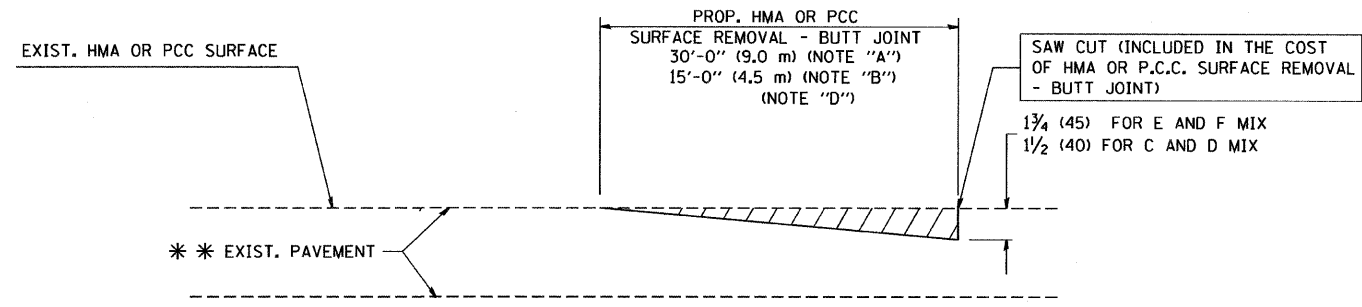
OPTION 1



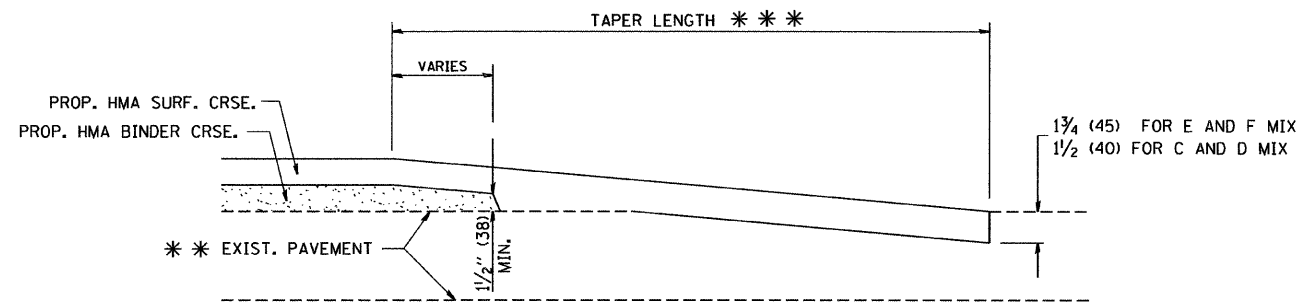
HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

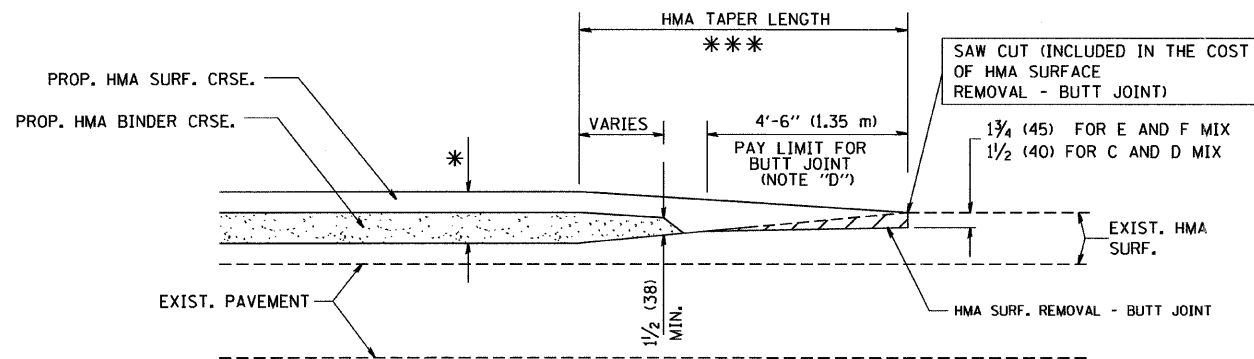
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND  
HMA TAPER

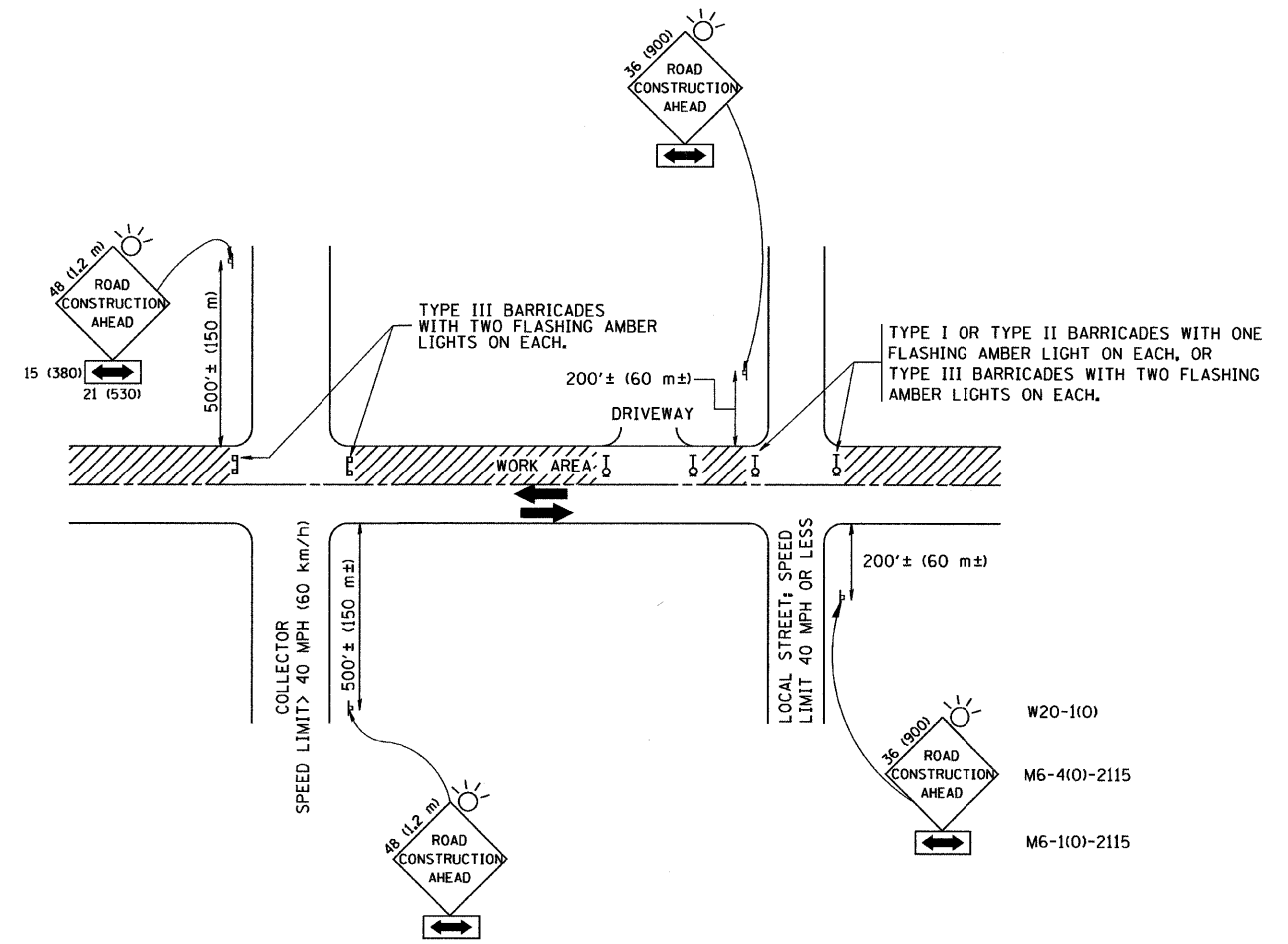
TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING

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		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.B. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0524	11-00080-00-RS	KANE	15	11
BD400-05 BD32		CONTRACT NO. 63670		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

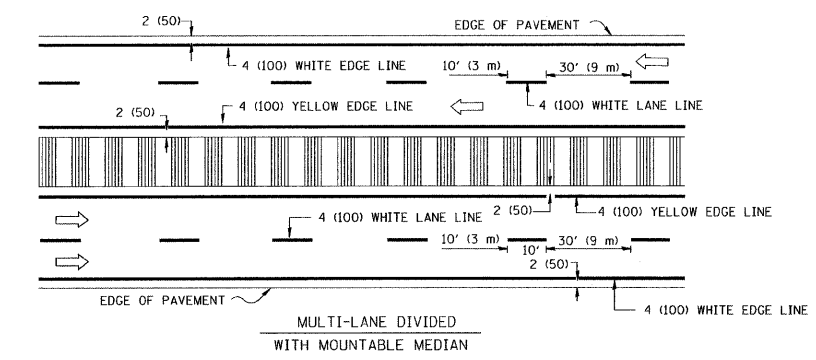
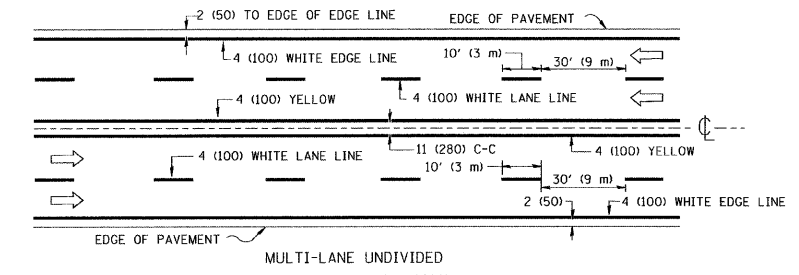
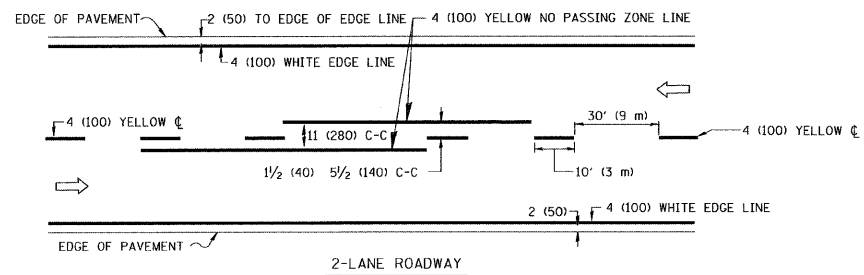
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

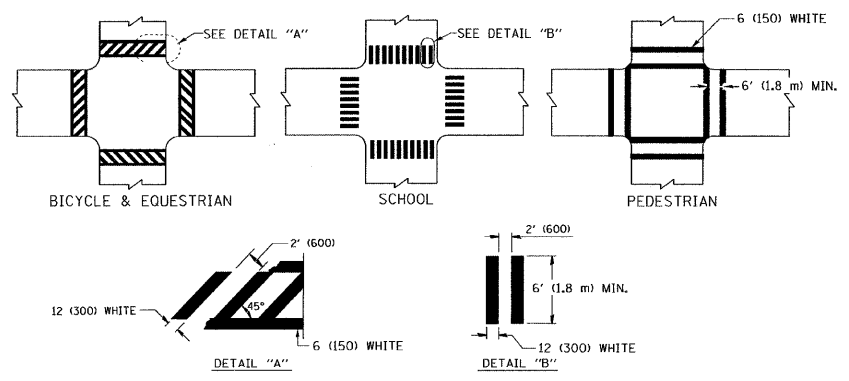
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		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

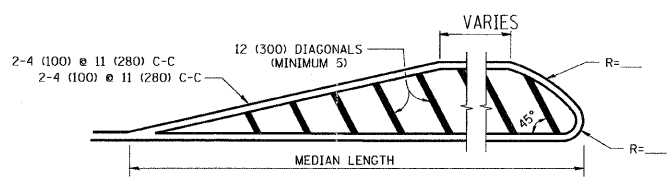
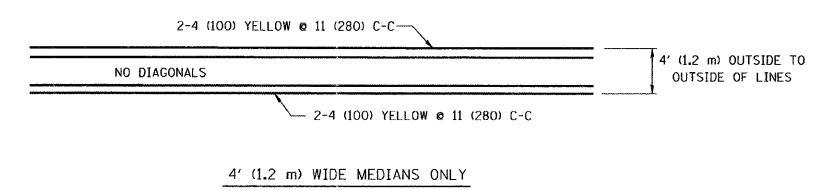
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	0524	11-00080-00-RS	KANE	15	12
STA. TO STA.		TC-10		CONTRACT NO. 63670		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						



NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE  
**TYPICAL LANE AND EDGE LINE MARKING**

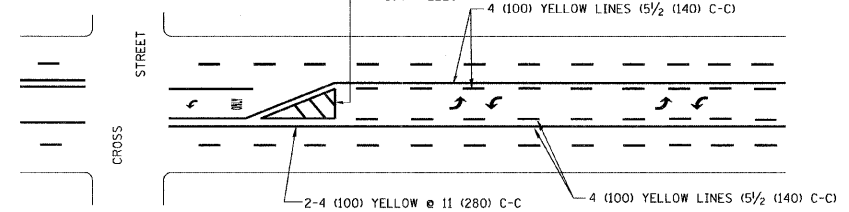


**TYPICAL CROSSWALK MARKING**

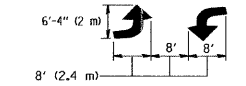


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.  
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

**MEDIANS OVER 4' (1.2 m) WIDE**

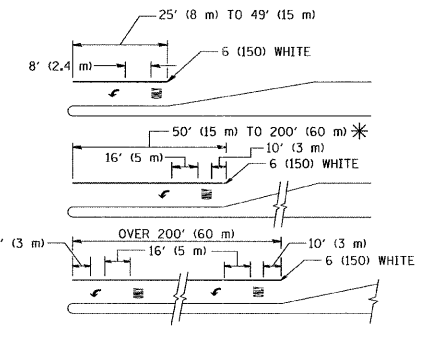


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



**MEDIAN WITH TWO-WAY LEFT TURN LANE**

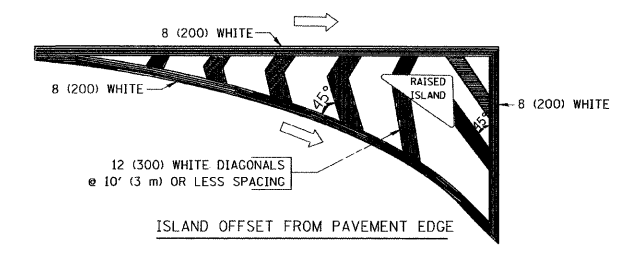
**TYPICAL PAINTED MEDIAN MARKING**



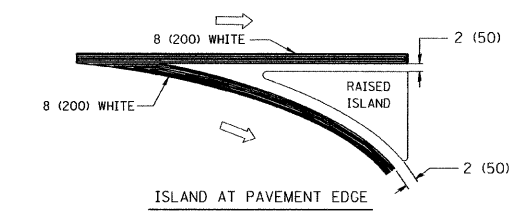
FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED.  
 AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
 \* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**



**ISLAND OFFSET FROM PAVEMENT EDGE**



**ISLAND AT PAVEMENT EDGE**

**TYPICAL ISLAND MARKING**

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

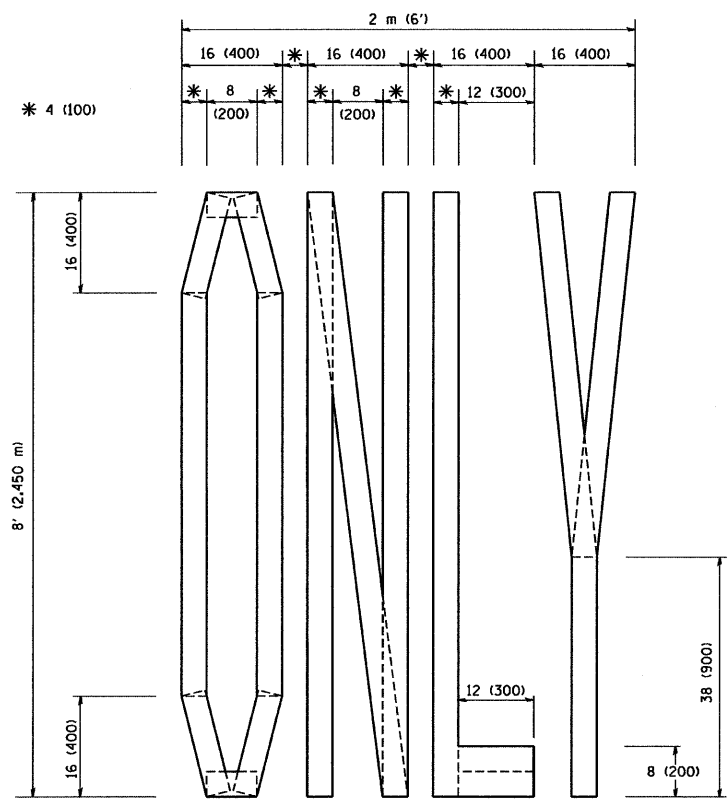
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

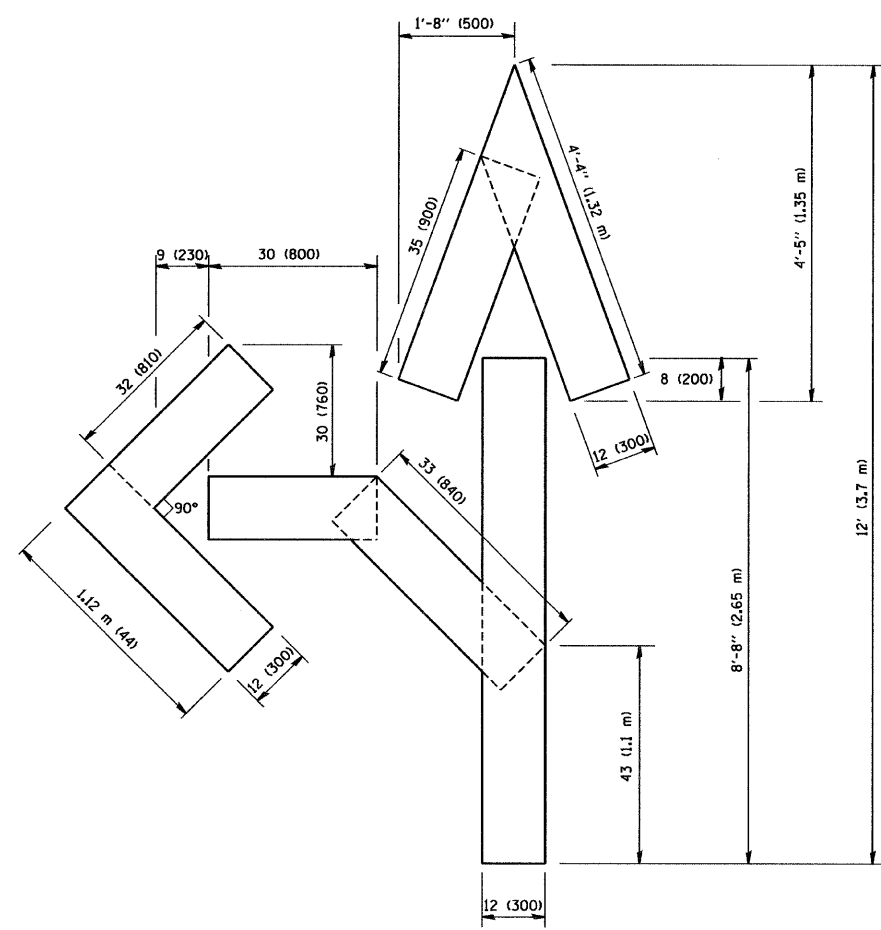
FILE NAME =	USER NAME = drivakosgn	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
ca:\pwork\pavdot\drivakosgn\d0108315\13.dgn		DRAWN -	REVISED - C. JUCIUS 09-09-09
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

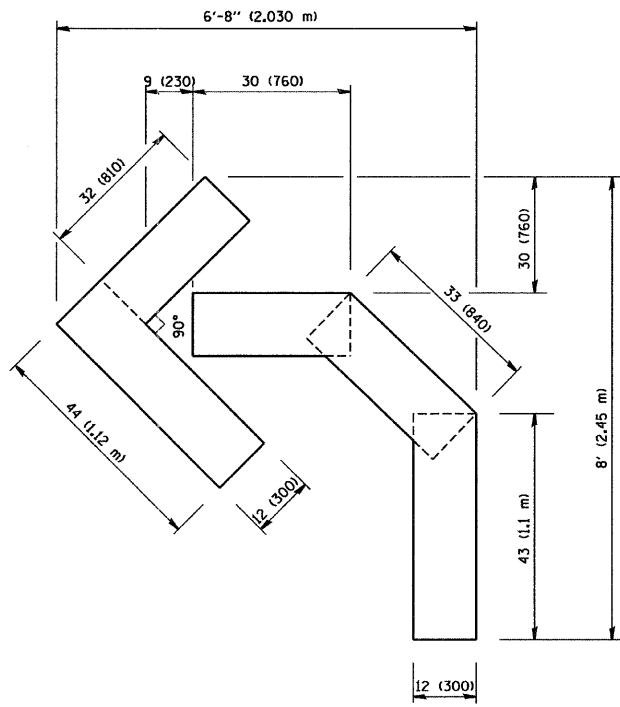
<b>DISTRICT ONE</b>		F.A.B. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
<b>TYPICAL PAVEMENT MARKINGS</b>		0524	11-00080-00-RS	KANE	15	13
SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.	CONTRACT NO. 63670	
		FED. ROAD DIST. NO. 1 ILLINOIS		FED. AID PROJECT		



QUANTITY  
 4 (100) LINE = 64.1 ft. (19.7 m)  
 21.1 sq. ft. (1.97 sq. m)



QUANTITY  
 4 (100) LINE = 82.5 ft. (25.3 m)  
 27.5 sq. ft. (2.53 sq. m)



QUANTITY  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\diststd\22x34\to16.dgn	USER NAME = geglennobt	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
		DRAWN -	REVISED -T. RAMMACHER 11-04-97
	PLOT SCALE = 50.0000 / IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 1/4/2008	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS  
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE. 0524	SECTION 11-00080-00-RS	COUNTY KANB	TOTAL SHEETS 15	SHEET NO. 14
TC-16			CONTRACT NO. 63670	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

