

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED
HIGHWAY IMPROVEMENT

| | | | | |
|---------------------|----------|--------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 327 | 20BR | CLINTON | 51 | 1 |
| FED. ROAD DIST. NO. | ILLINOIS | CONTRACT NO. 76B43 | | |

FOR INDEX OF SHEETS, SEE SHEET NO. 2

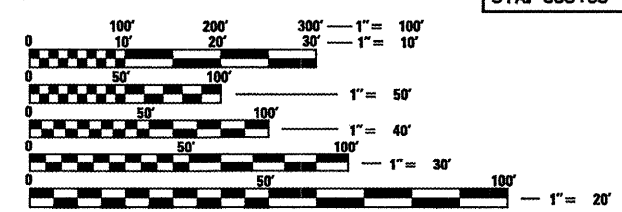
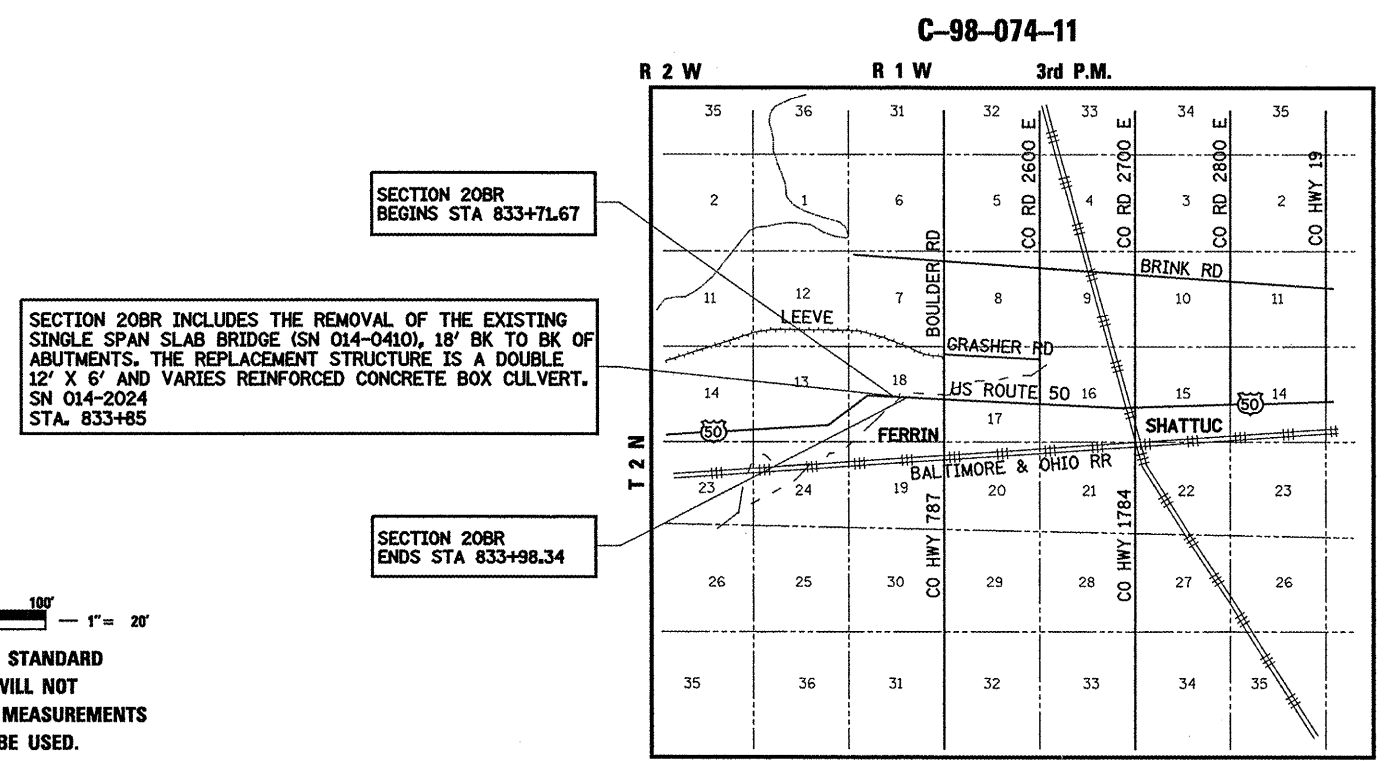
FAP ROUTE 327 (US ROUTE 50)

SECTION 20BR

PROJECT ACF-0327(057)

CLINTON COUNTY

BRIDGE REPLACEMENT



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

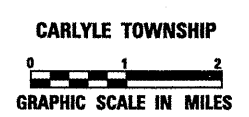
J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123
 OR 811

PROJECT ENGINEER: PATTI LEBEAU (618) 346-3179
PROJECT MANAGER: REBECCA THARP (618) 346-3323
CONTRACT NO. 76B43

DESIGN DESIGNATION

NA
 ADT = 3850 (2009-ACTUAL)
 ADT = 4000 (2012-ESTIMATED)
 ADT = 4900 (2032-ESTIMATED)
 SU = 3.2%
 MU = 11.7%

LOCATION MAP



GROSS SECTION LENGTH = 26.67 FEET = 0.005 MILES
 NET SECTION LENGTH = 26.67 FEET = 0.005 MILES
 LATITUDE 38.61739°
 LONGITUDE 89.24409°
 SN 014-0410



Brian R. Mueller

ILLINOIS PROFESSIONAL ENGINEER NO. 062-052018 DATE 11-18-11
 EXP. 11-30-2013

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

SUBMITTED *December 16 20 11*
John M. Osmer
 DEPUTY DIRECTOR OF HIGHWAYS, REGION 5 ENGINEER

Feb 3 20 12
John D. Cavamelli, P.E./B.L.
 acting ENGINEER OF DESIGN AND ENVIRONMENT

Feb 3 20 12
William R. Fienler
 acting DIRECTOR OF HIGHWAYS, CHIEF ENGINEER



BERNARDIN * LOCHMUELLER & ASSOCIATES, INC.
 3 OAK DRIVE
 MARYVILLE, ILLINOIS 62062
 PHONE (618) 288-4666
 FAX (618) 288-4666

**PRINTED BY THE AUTHORITY
 OF THE STATE OF ILLINOIS**

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | CONSTRUCTION CODE | | | |
|------------|---|-------|----------------|-------------------------------|--|--|--|
| | | | | 80% FED. 20% STATE 0011 | | | |
| 20200100 | EARTH EXCAVATION | CU YD | 1880 | 1880 | | | |
| * 25000200 | SEEDING, CLASS 2 | ACRE | 1 | 1 | | | |
| * 25000400 | NITROGEN FERTILIZER NUTRIENT | POUND | 90 | 90 | | | |
| * 25000500 | PHOSPHORUS FERTILIZER NUTRIENT | POUND | 90 | 90 | | | |
| * 25000600 | POTASSIUM FERTILIZER NUTRIENT | POUND | 90 | 90 | | | |
| * 25100115 | MULCH, METHOD 2 | ACRE | 3 | 3 | | | |
| 28000200 | EARTH EXCAVATION FOR EROSION CONTROL | CU YD | 8 | 8 | | | |
| 28000250 | TEMPORARY EROSION CONTROL SEEDING | POUND | 200 | 200 | | | |
| 28000305 | TEMPORARY DITCH CHECKS | FOOT | 153 | 153 | | | |
| 28000500 | INLET AND PIPE PROTECTION | EACH | 2 | 2 | | | |
| 28001000 | AGGREGATE (EROSION CONTROL) | TON | 7 | 7 | | | |
| 28100107 | STONE RIPRAP, CLASS A4 | SQ YD | 1245 | 1245 | | | |
| 28200200 | FILTER FABRIC | SQ YD | 1245 | 1245 | | | |
| 35600724 | HOT-MIX ASPHALT BASE COURSE WIDENING, 12" | SQ YD | 413 | 413 | | | |

*SPECIALTY ITEM

| | | | | | | | | | | | | |
|------------------------|-----------------------------|----------------|-----------|---|---|-------------------------|------|-----------------------|--------------------|-------------------|-----------------------|-------------------|
| FILE NAME = #FILEL# | USER NAME = Paul | DESIGNED - JLS | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SUMMARY OF QUANTITIES | | | F.A.P. RTE. 327 | SECTION 20BR | COUNTY CLINTON | TOTAL SHEETS 51 | SHEET NO. 3 |
| | PLOT SCALE = 40.0000' / IN. | DRAWN - PDB | REVISED - | | SCALE: | SHEET NO. 1 OF 5 SHEETS | STA. | TO STA. | CONTRACT NO. 76B43 | | | |
| | PLOT DATE = 11/17/2011 | CHECKED - BRM | REVISED - | | FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT | | | | | | | |

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | CONSTRUCTION CODE | | | |
|----------|---|-------|----------------|------------------------------|--|--|--|
| | | | | 80% FED 20% STATE 0011 | | | |
| 40200800 | AGGREGATE SURFACE COURSE, TYPE B | TON | 14 | 14 | | | |
| 40600200 | BITUMINOUS MATERIALS (PRIME COAT) | TON | 0.5 | 0.5 | | | |
| 40600300 | AGGREGATE (PRIME COAT) | TON | 3 | 3 | | | |
| 40600990 | TEMPORARY RAMP | SQ YD | 54 | 54 | | | |
| 40603345 | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90 | TON | 93 | 93 | | | |
| 42001430 | BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE) | SQ YD | 48 | 48 | | | |
| 44000100 | PAVEMENT REMOVAL | SQ YD | 269 | 269 | | | |
| 44000155 | HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2" | SQ YD | 1527 | 1527 | | | |
| 48101500 | AGGREGATE SHOULDERS, TYPE B 6" | SQ YD | 674 | 674 | | | |
| 48203100 | HOT-MIX ASPHALT SHOULDERS | TON | 35 | 35 | | | |
| 50100100 | REMOVAL OF EXISTING STRUCTURES | EACH | 1 | 1 | | | |
| 50105220 | PIPE CULVERT REMOVAL | FOOT | 18 | 18 | | | |
| 50300225 | CONCRETE STRUCTURES | CU YD | 22.8 | 22.8 | | | |
| 50300255 | CONCRETE SUPERSTRUCTURE | CU YD | 112 | 112 | | | |

| | | | |
|------------------------|-----------------------------|-----------------|-----------|
| FILE NAME = #FILEL# | USER NAME = Paul | DESIGNED - JLS | REVISED - |
| | | DRAWN - PDB | REVISED - |
| | PLOT SCALE = 48,0000' / IN. | CHECKED - BRM | REVISED - |
| | PLOT DATE = 12/8/2011 | DATE - 02-08-11 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: SHEET NO. 2 OF 5 SHEETS STA. TO STA.

| | | | | |
|---|---------|---------|--------------------|--------------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 327 | 20BR | CLINTON | 51 | 4 |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 76B43 | |

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | CONSTRUCTION CODE | | | |
|------------|--|--------|----------------|------------------------------|--|--|--|
| | | | | 80% FED 20% STATE 0011 | | | |
| 50300260 | BRIDGE DECK GROOVING | SQ YD | 324 | 324 | | | |
| 50800105 | REINFORCEMENT BARS | POUND | 15910 | 15910 | | | |
| 50800205 | REINFORCEMENT BARS, EPOXY COATED | POUND | 40510 | 40510 | | | |
| 50800515 | BAR SPLICERS | EACH | 350 | 350 | | | |
| * 50900200 | STEEL RAILING, TYPE 2399 | FOOT | 54 | 54 | | | |
| 51500100 | NAME PLATES | EACH | 1 | 1 | | | |
| 54003000 | CONCRETE BOX CULVERTS | CU YD | 110.5 | 110.5 | | | |
| 54200229 | PIPE CULVERTS, CLASS D, TYPE 1 24" | FOOT | 30 | 30 | | | |
| * 63000003 | STEEL PLATE BEAM GUARD RAIL, TYPE A, 9 FOOT POSTS | FOOT | 425 | 425 | | | |
| * 63100087 | TRAFFIC BARRIER TERMINAL, TYPE 6A | EACH | 4 | 4 | | | |
| * 63100167 | TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT | EACH | 4 | 4 | | | |
| 63200310 | GUARDRAIL REMOVAL | FOOT | 360 | 360 | | | |
| 67000400 | ENGINEER'S FIELD OFFICE, TYPE A | CAL MO | 7 | 7 | | | |
| 67100100 | MOBILIZATION | L SUM | 1 | 1 | | | |

* SPECIALTY ITEM

| | | | | | | | | | | | | |
|-------------|-----------------------------|-----------------|----------------------|---|------------------------------|--|--------------------|---|---------|---------|---|-----------|
| FILE NAME = | USER NAME = baeibel | DESIGNED - JLS | REVISED - 12-14-2011 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SUMMARY OF QUANTITIES | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| #FILEL# | PLOT SCALE = 40.0000' / IN. | DRAWN - PDB | REVISED - | | | | | 327 | 20BR | CLINTON | 51 | 5 |
| | PLOT DATE = 12/14/2011 | CHECKED - BRM | REVISED - | | | | | SCALE: SHEET NO. 3 OF 5 SHEETS STA. TO STA. | | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | |
| | | DATE - 02-08-11 | REVISED - | | | | CONTRACT NO. 76B43 | | | | | |

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | CONSTRUCTION CODE | | | |
|------------|---|-------|----------------|------------------------------|--|--|--|
| | | | | 80% FED 20% STATE 0011 | | | |
| 70100405 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701321 | EACH | 1 | 1 | | | |
| 70100450 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701201 | L SUM | 1 | 1 | | | |
| 70100460 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701306 | L SUM | 1 | 1 | | | |
| 70100500 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701326 | L SUM | 1 | 1 | | | |
| 70106500 | TEMPORARY BRIDGE TRAFFIC SIGNALS | EACH | 1 | 1 | | | |
| 70106700 | TEMPORARY RUMBLE STRIPS | EACH | 6 | 6 | | | |
| 70300100 | SHORT-TERM PAVEMENT MARKING | FOOT | 266 | 266 | | | |
| 70300220 | TEMPORARY PAVEMENT MARKING - LINE 4" | FOOT | 183 | 183 | | | |
| 70300240 | TEMPORARY PAVEMENT MARKING - LINE 6" | FOOT | 738 | 738 | | | |
| 70301000 | WORK ZONE PAVEMENT MARKING REMOVAL | SQ FT | 83 | 83 | | | |
| 70400100 | TEMPORARY CONCRETE BARRIER | FOOT | 412.5 | 412.5 | | | |
| 70400200 | RELOCATE TEMPORARY CONCRETE BARRIER | FOOT | 325 | 325 | | | |
| * 78100100 | RAISED REFLECTIVE PAVEMENT MARKER | EACH | 8 | 8 | | | |
| * 78100105 | RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE) | EACH | 1 | 1 | | | |

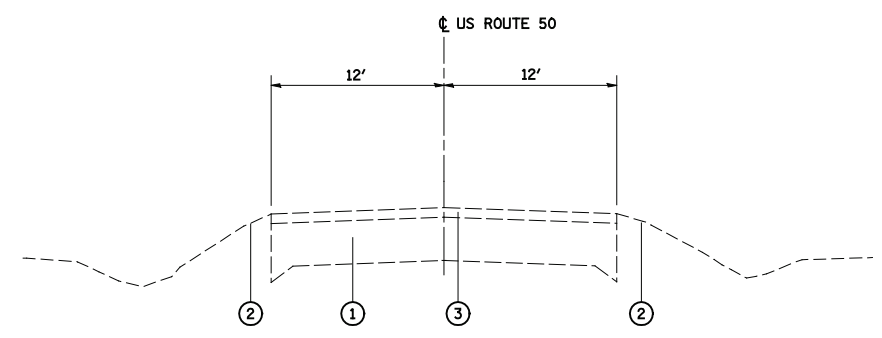
* SPECIALTY ITEM

| | | | | | | | | | | | | |
|-----------------------|-----------------------------|-----------------|-----------|---|---|-------------------------|------|-----------------------|--------------------|-------------------|-----------------------|-------------------|
| FILE NAME = #FILE# | USER NAME = Paul | DESIGNED - JLS | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SUMMARY OF QUANTITIES | | | F.A.P. RTE. 327 | SECTION 20BR | COUNTY CLINTON | TOTAL SHEETS 51 | SHEET NO. 6 |
| | PLOT SCALE = 40.0000' / IN. | DRAWN - PDB | REVISED - | | SCALE: | SHEET NO. 4 OF 5 SHEETS | STA. | TO STA. | CONTRACT NO. 76B43 | | | |
| | PLOT DATE = 11/17/2011 | CHECKED - BRM | REVISED - | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | | | | |
| | | DATE - 02-08-11 | REVISED - | | | | | | | | | |

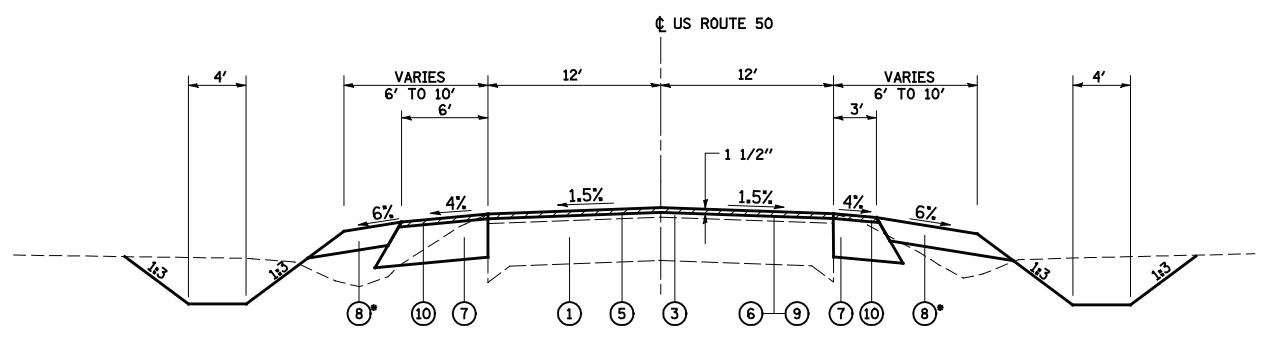
| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | CONSTRUCTION CODE | | | | | |
|------------|---|-------|----------------|------------------------------|--|--|--|--|--|
| | | | | 80% FED 20% STATE 0011 | | | | | |
| * 78200410 | GUARDRAIL MARKERS, TYPE A | EACH | 16 | 16 | | | | | |
| * 78201000 | TERMINAL MARKER - DIRECT APPLIED | EACH | 4 | 4 | | | | | |
| 78300100 | PAVEMENT MARKING REMOVAL | SQ FT | 547 | 547 | | | | | |
| 78300200 | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL | EACH | 9 | 9 | | | | | |
| X4400100 | PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH) | SQ YD | 47 | 47 | | | | | |
| X6660445 | RIGHT OF WAY AND PROPERTY CORNERS | EACH | 1 | 1 | | | | | |
| * X7200200 | WIDE LOAD SIGNING | L SUM | 1 | 1 | | | | | |
| * X7800610 | URETHANE PAVEMENT MARKING - LINE 4" | FOOT | 1641 | 1641 | | | | | |
| Z0026407 | TEMPORARY SHEET PILING | SQ FT | 332.4 | 332.4 | | | | | |
| Z0030250 | IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3 | EACH | 2 | 2 | | | | | |
| Z0030350 | IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3 | EACH | 2 | 2 | | | | | |
| Z0064505 | SECTION CORNER MARKERS | EACH | 1 | 1 | | | | | |
| Z0073002 | TEMPORARY SOIL RETENTION SYSTEM | SQ FT | 156.9 | 156.9 | | | | | |
| + Z0076600 | TRAINEES | HR | 500 | 500 | | | | | |

+ 0042

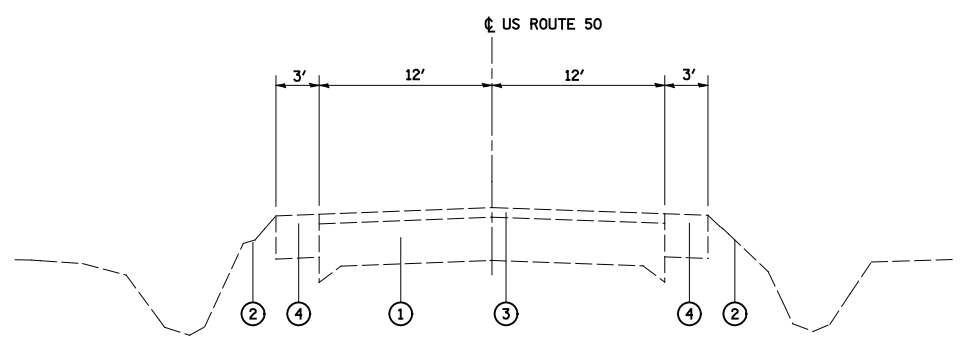
* SPECIALTY ITEM



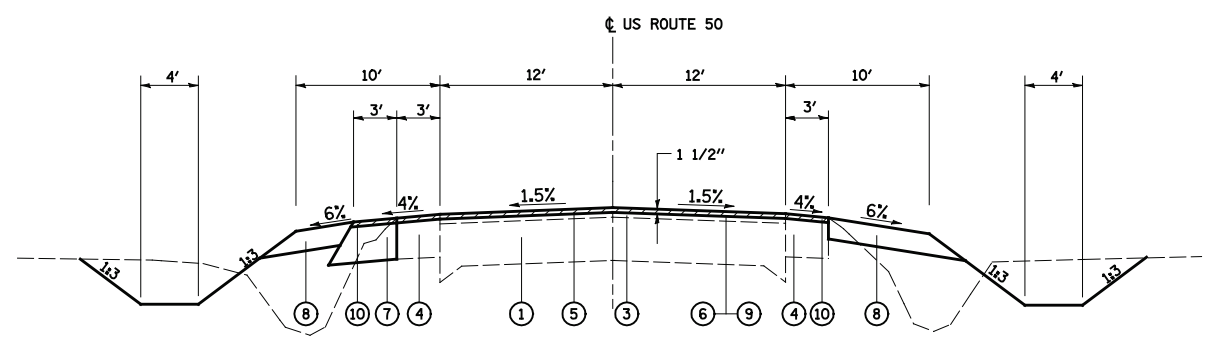
EXISTING TYPICAL SECTION
 STA 831+30.00 TO STA 833+03.95 LT
 STA 834+67.25 TO STA 836+40.00 LT



PROPOSED TYPICAL SECTION
 STA 831+30.00 TO STA 833+03.95 LT
 STA 834+67.25 TO STA 836+40.00 LT
 *6' AGGREGATE SHOULDER STA 830+75 TO STA 831+30
 AND STA 836+40 TO STA 836+95



EXISTING TYPICAL SECTION
 STA 833+03.95 TO STA 833+76.28 LT
 STA 833+94.24 TO STA 834+67.25 LT
 STA 833+05.40 TO STA 833+76.34 RT
 STA 833+94.15 TO STA 834+67.17 RT



PROPOSED TYPICAL SECTION
 STA 833+03.95 TO STA 833+36.17 LT
 STA 834+33.83 TO STA 834+67.25 LT
 STA 833+05.40 TO STA 833+36.17 RT
 STA 834+33.83 TO STA 834+67.17 RT
 STRUCTURE OMISSION: STA 833+36.17 TO STA 834+33.83

LEGEND

- ① EXISTING 9"-7"-9" CONCRETE PAVEMENT
- ② EXISTING SHOULDER
- ③ EXISTING RESURFACING VARIABLE DEPTH (1 1/2" AND VARIES)
- ④ EXISTING CONCRETE SHOULDER
- ⑤ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90 - 1 1/2"
- ⑥ PROPOSED BITUMINOUS MATERIALS (PRIME COAT)
- ⑦ PROPOSED HOT-MIX ASPHALT BASE COURSE WIDENING 12"
- ⑧ PROPOSED AGGREGATE SHOULDERS, TYPE B 6"
- ⑨ PROPOSED AGGREGATE (PRIME COAT)
- ⑩ PROPOSED HOT-MIX ASPHALT SHOULDERS, 1 1/2" AND VARIES

HOT-MIX ASPHALT SURFACE REMOVAL OR PORTLAND CEMENT CONCRETE SURFACE REMOVAL

| | | | | | | | | | | | | |
|-------------|------------------|-----------------|-----------|---|---|-------------------------|--------------------|-----------------|---------------------------|--------------------|----------------|--|
| FILE NAME = | USER NAME = Paul | DESIGNED - JLS | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL SECTIONS - FAP 327 (US ROUTE 50) | | F.A.P. RTE. 327 | SECTION 20BR | COUNTY CLINTON | TOTAL SHEETS 51 | SHEET NO. 8 | |
| | | DRAWN - PDB | REVISED - | | SCALE: 20 | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | CONTRACT NO. 76B43 | | | |
| | | CHECKED - BRM | REVISED - | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | | | | |
| | | DATE - 02-08-11 | REVISED - | | | | | | | | | |

EARTHWORK SCHEDULE

| LOCATION | EARTH EXCAVATION (CU YD) | FOR INFORMATION ONLY | | |
|--------------------------------|--------------------------|---|--------------------|---|
| | | EARTH EXCAVATION ADJUSTED FOR SHRINKAGE 25% (CU YD) | EMBANKMENT (CU YD) | EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) (CU YD) |
| STATION TO STATION | | | | |
| STA 830+75.00 TO STA 836+95.00 | 1880 | 1410 | 1105 | 305 |
| PAY TOTAL | 1880 | 1410 | 1105 | 305 |

PAVEMENT SCHEDULE

| LOCATION | BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE) (SQ YD) | HOT-MIX ASPHALT BASE COURSE WIDENING 12" (SQ YD) | BITUMINOUS MATERIALS (PRIME COAT) (TON) | AGGREGATE (PRIME COAT) (TON) | TEMPORARY RAMP (SQ YD) | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (TON) | HOT-MIX ASPHALT SHOULDERS (TON) | AGGREGATE SHOULDERS, TYPE B 6" (SQ YD) |
|--------------------------------|---|--|---|------------------------------|------------------------|--|---------------------------------|--|
| STATION TO STATION | | | | | | | | |
| STA 830+75.00 TO STA 833+36.17 | | 206.1 | 0.23 | 1.13 | 26.6 | 46.18 | 17.32 | 335.8 |
| STA 833+36.17 TO STA 833+42.17 | 24.0 | | | | | | | |
| STA 834+27.83 TO STA 834+33.83 | 24.0 | | | | | | | |
| STA 834+33.83 TO STA 836+95.00 | | 206.1 | 0.23 | 1.13 | 26.6 | 46.18 | 17.32 | 337.8 |
| TOTAL | 48.0 | 412.2 | 0.46 | 2.26 | 53.2 | 92.36 | 34.64 | 673.6 |
| PAY TOTAL | 48 | 413 | 0.5 | 3 | 54 | 93 | 35 | 674 |

RIPRAP SCHEDULE

| LOCATION | STONE RIPRAP, CLASS A4 (SQ YD) | FILTER FABRIC (SQ YD) |
|--------------------------------|--------------------------------|-----------------------|
| STATION TO STATION | | |
| STA 833+64.00 TO STA 836+95.00 | 1121.7 | 1121.7 |
| TOTAL | 1121.7 | 1121.7 |
| PAY TOTAL | 1122* | 1122* |

* NOT A TOTAL QUANTITY

EROSION CONTROL SCHEDULE

| LOCATION | INLET AND PIPE PROTECTION (EACH) | EARTH EXCAVATION FOR EROSION CONTROL (CU YD) | AGGREGATE (EROSION CONTROL) (TON) | TEMPORARY EROSION CONTROL SEEDING (POUNDS) |
|--------------------------------|----------------------------------|--|-----------------------------------|--|
| STATION | OFFSET | SIDE | | |
| STA 829+95.00 | 26' | LT | 1 | |
| STA 830+75.00 TO STA 836+95.00 | | | | 200 |
| STA 833+85.00 | 23' | LT | 1 | |
| STA 833+85.00 | 52' | RT | 8 | 7 |
| PAY TOTAL | | | 2 | 8 |

REMOVAL SCHEDULE

| LOCATION | REMOVAL OF EXISTING STRUCTURES (EACH) | PIPE CULVERT REMOVAL (FOOT) | GUARDRAIL REMOVAL (FOOT) | PAVEMENT REMOVAL (SQ YD) | PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH) (SQ YD) | HOT-MIX ASPHALT SURFACE REMOVAL 1/2" (SQ YD) |
|--------------------------------|---------------------------------------|-----------------------------|--------------------------|--------------------------|---|--|
| STATION TO STATION | | | | | | |
| STA 830+15.80 | | 18 | | | | |
| STA 831+30.00 TO STA 833+36.17 | | | | | | 767.9 |
| STA 832+74.81 TO STA 834+45.58 | | | 169.9 | | | |
| STA 833+03.84 TO STA 833+36.17 | | | | | 10.9 | |
| STA 833+05.40 TO STA 833+36.17 | | | | | 10.7 | |
| STA 833+36.17 TO STA 833+76.24 | | | | 134.8 | | |
| STA 833+47.79 TO STA 835+37.78 | | | 189.5 | | | |
| STA 833+85.00 | 1 | | | | | |
| STA 833+94.17 TO STA 834+33.83 | | | | 133.8 | | |
| STA 834+33.83 TO STA 836+40.00 | | | | | | 758.8 |
| STA 834+33.83 TO STA 834+67.26 | | | | | 12.0 | |
| STA 834+33.83 TO STA 834+67.17 | | | | | 12.5 | |
| TOTAL | 1 | 18 | 359.4 | 268.6 | 46.1 | 1526.7 |
| PAY TOTAL | 1 | 18 | 360 | 269 | 47 | 1527 |

SEEDING SCHEDULE

| LOCATION | SEEDING, CLASS 2 (ACRE) | NITROGEN FERTILIZER NUTRIENT (POUND) | PHOSPHORUS FERTILIZER NUTRIENT (POUND) | POTASSIUM FERTILIZER NUTRIENT (POUND) | MULCH METHOD 2 (ACRE) |
|--------------------------------|-------------------------|--------------------------------------|--|---------------------------------------|-----------------------|
| STATION TO STATION | | | | | |
| STA 830+75.00 TO STA 836+95.00 | 1 | 90 | 90 | 90 | 3 |
| PAY TOTAL | 1 | 90 | 90 | 90 | 3 |

TEMPORARY PAVEMENT MARKING SCHEDULE

| LOCATION | SHORT-TERM PAVEMENT MARKING (FOOT) | TEMPORARY PAVEMENT MARKING-LINE 4" (FOOT) | WORK ZONE PAVEMENT MARKING REMOVAL (SQ FT) |
|--------------------------------|------------------------------------|---|--|
| STATION TO STATION | | | |
| STA 831+30.00 TO STA 836+40.00 | 265.2 | 182.3 | 82.9 |
| SUBTOTAL | 265.2 | 182.3 | 82.9 |
| PAY TOTAL | 266 | 183 | 83 |

TEMPORARY DITCH CHECK SCHEDULE

| LOCATION | TEMPORARY DITCH CHECKS (FOOT) | | |
|------------------|-------------------------------|------|------------|
| STATION | OFFSET | SIDE | |
| 832+00.00 | 37.6 | LT | 17 |
| 832+00.00 | 36.3 | RT | 17 |
| 832+90.00 | 39.5 | RT | 17 |
| 833+50.00 | 42.0 | LT | 17 |
| 833+50.00 | 41.9 | RT | 17 |
| 834+25.00 | 42.3 | LT | 17 |
| 834+25.00 | 42.3 | RT | 17 |
| 835+75.00 | 40.5 | LT | 17 |
| 835+75.00 | 38.6 | RT | 17 |
| PAY TOTAL | | | 153 |

ENTRANCE SCHEDULE

| LOCATION | PIPE CULVERTS, CLASS D, TYPE 1 24" (FOOT) | AGGREGATE SURFACE COURSE, TYPE B (TON) |
|------------------|---|--|
| STATION | SIDE | |
| STA 830+15.80 | LT | 30.0 |
| TOTAL | | 30.0 |
| PAY TOTAL | | 14 |

TRAFFIC CONTROL SCHEDULE

| LOCATION | TEMPORARY RUMBLE STRIPS (EACH) | TEMPORARY CONCRETE BARRIER (FOOT) | RELOCATE TEMPORARY CONCRETE BARRIER (FOOT) | IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3 (EACH) | IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3 (EACH) |
|--------------------------|--------------------------------|-----------------------------------|--|--|---|
| STATION | SIDE | | | | |
| STA 813+25 | RT | 1 | | | |
| STA 818+25 | RT | 1 | | | |
| STA 823+25 | RT | 1 | | | |
| STA 831+83 TO STA 835+90 | LT | | 87.5 | 325 | 2 |
| STA 832+23 TO STA 835+47 | RT | | 325 | | 2 |
| STA 844+54 | LT | 1 | | | |
| STA 849+54 | LT | 1 | | | |
| STA 854+54 | LT | 1 | | | |
| PAY TOTAL | | 6 | 412.5 | 325 | 2 |

PERMANENT PAVEMENT MARKING SCHEDULE

| LOCATION | URETHANE PAVEMENT MARKING - LINE 4" | | RAISED REFLECTIVE PAVEMENT MARKER (EACH) | RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE) (EACH) | PAVEMENT MARKING REMOVAL (SQ FT) | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL (EACH) |
|--------------------------------|-------------------------------------|-------------------------|--|---|----------------------------------|--|
| | SOLID WHITE (FOOT) | SKIP DASH YELLOW (FOOT) | | | | |
| STATION TO STATION | | | | | | |
| STA 830+25.00 TO STA 833+36.17 | 622.4 | 77.8 | 4 | | 233.3 | 4 |
| STA 833+36.17 TO STA 834+33.83 | 195.4 | 24.4 | | 1 | 73.3 | 1 |
| STA 834+33.83 TO STA 837+54.00 | 640.4 | 80 | 4 | | 240.1 | 4 |
| SUBTOTAL | 1458.2 | 182.2 | 8 | 1 | 546.7 | 9 |
| PAY TOTAL | | 1641 | 8 | 1 | 547 | 9 |

GUARDRAIL SCHEDULE

| LOCATION | TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT (EACH) | TRAFFIC BARRIER TERMINAL, TYPE 6A (EACH) | STEEL PLATE BEAM GUARD RAIL, TYPE A, 9 FOOT POSTS (FOOT) | TERMINAL MARKER - DIRECT APPLIED (EACH) |
|--------------------------------|---|--|--|---|
| STATION TO STATION | SIDE | | | |
| STA 831+27.92 TO STA 833+71.67 | RT | 1 | 150 | 1 |
| STA 832+15.42 TO STA 833+71.67 | LT | 1 | 62.5 | 1 |
| STA 833+98.33 TO STA 835+54.58 | RT | 1 | 62.5 | 1 |
| STA 833+98.33 TO STA 836+42.08 | LT | 1 | 150 | 1 |
| PAY TOTAL | | 4 | 425.0 | 4 |

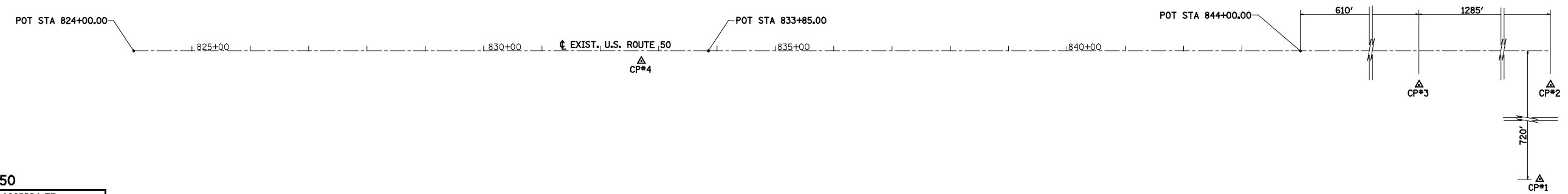
GUARDRAIL MARKERS SCHEDULE

| LOCATION | GUARDRAIL MARKERS, TYPE A (EACH) |
|--------------------------------|----------------------------------|
| STATION TO STATION | SIDE |
| STA 831+27.92 TO STA 835+54.58 | RT |
| STA 832+15.42 TO STA 836+42.08 | LT |
| PAY TOTAL | 16 |

RIGHT OF WAY SCHEDULE

| LOCATION | RIGHT OF WAY AND PROPERTY CORNERS (EACH) | SECTION CORNER MARKERS (EACH) |
|------------------|--|-------------------------------|
| STATION | OFFSET | SIDE |
| 830+00.00 | 33.0 | LT |
| 836+60.24* | 0.2 | RT |
| PAY TOTAL | 1 | 1 |

* CENTER OF SECTION 18, T. 2 N., R. 1 W.



US ROUTE 50

| DESCRIPTION | COORDINATE | |
|-------------------|------------|-----------|
| | NORTHING | EASTING |
| POT STA 824+00.00 | 711654.52 | 559303.43 |
| POT STA 833+85.00 | 711610.42 | 560287.44 |
| POT STA 844+00.00 | 711564.99 | 561301.43 |

BENCHMARK * 10

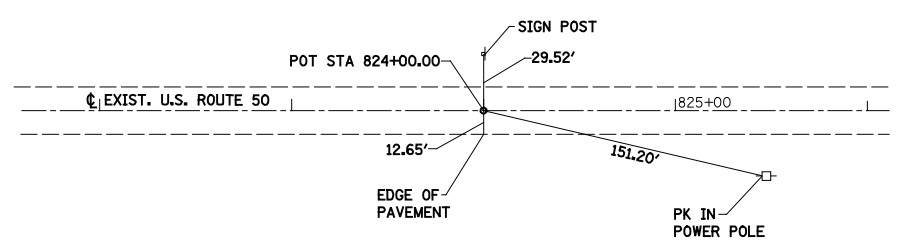
RAILROAD SPIKE IN POWER POLE AT SOUTHEAST CORNER OF US 50 AND CREEK ROAD
ELEV 458.933

BENCHMARK * 11

RAILROAD SPIKE IN POWER POLE AT SOUTH SIDE OF US 50, JUST EAST OF SN 014-0410.
STA 835+23, 34' RT
ELEV 461.081

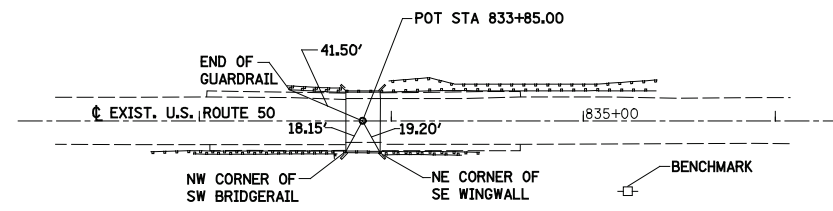
BENCHMARK * 12

RAILROAD SPIKE IN LIGHT POLE AT SOUTHEAST CORNER OF US 50 AND FERRIN / BOULDER ROAD
ELEV 464.116



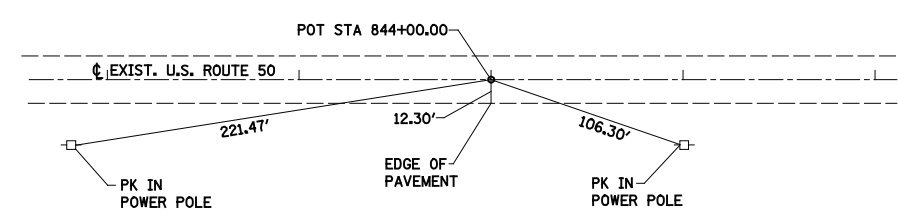
POT STATION 824+00.00

N 711654.5180
E 559303.4310



POT STATION 833+85.00

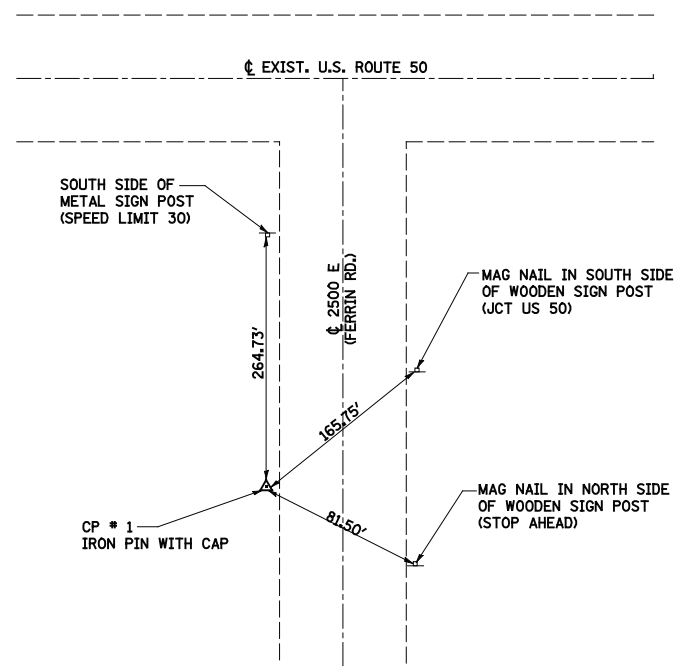
SN 014-0410
N 711610.4230
E 560287.4440



POT STATION 844+00.00

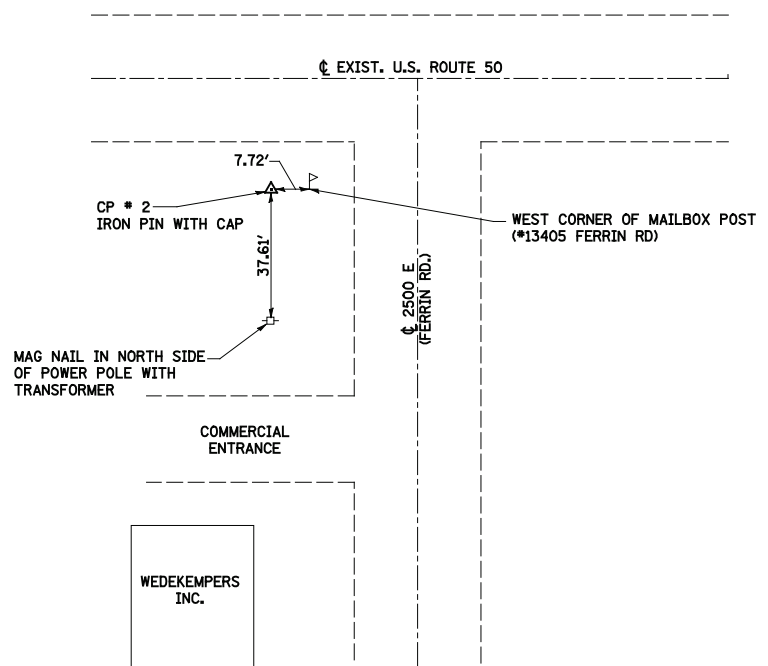
N 711564.9860
E 561301.4260

| | | | | | | | | | | | | |
|---|------------------|-----------------|-----------|---|---|-------------------------|--------------|---------------------------|--------------|----------------|-----------------|--------------|
| FILE NAME = | USER NAME = Paul | DESIGNED - JLS | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | HORIZONTAL CONTROL & TIE POINTS, US ROUTE 50 | | | F.A.P. RTE. 327 | SECTION 20BR | COUNTY CLINTON | TOTAL SHEETS 51 | SHEET NO. 10 |
| S:\Projects\40-0022-3HY US 50 Final Plans\dgn\CADD Sheets\0876843-ah1-ATB.dgn | | DRAWN - PDB | REVISED - | | SCALE: 1"=100' | SHEET NO. 1 OF 2 SHEETS | STA. TO STA. | CONTRACT NO. 76B43 | | | | |
| | | CHECKED - BRM | REVISED - | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | | | | |
| | | DATE - 02-08-11 | REVISED - | | | | | | | | | |



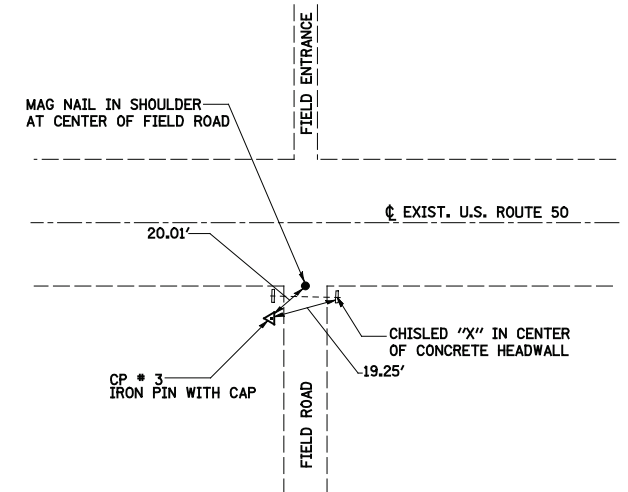
**CONTROL POINT NO. 1
(IRON PIN WITH CAP)**

N 710718.7822
E 563206.0878



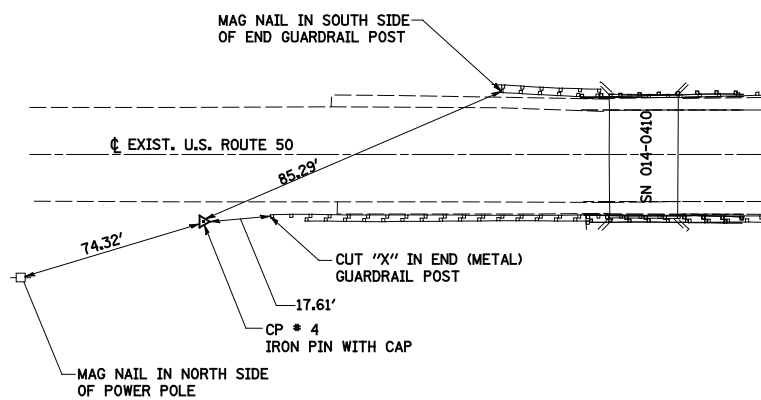
**CONTROL POINT NO. 2
(IRON PIN WITH CAP)**

N 711440.5865
E 563193.5746



**CONTROL POINT NO. 3
(IRON PIN WITH CAP)**

N 711507.8022
E 561908.4872



**CONTROL POINT NO. 4
(IRON PIN WITH CAP)**

N 711598.1155
E 560172.2072

NOTE:
ALL DISTANCES MEASURED DIRECT.

| | | | |
|--|------------------|-----------------|-----------|
| FILE NAME = | USER NAME = Paul | DESIGNED - JLS | REVISED - |
| S:\Projects\410-0022-3HY US 50 Final Plans\dgn\CADD Sheets\0876843-ah1-ATB.dgn | | DRAWN - PDB | REVISED - |
| | | CHECKED - BRM | REVISED - |
| | | DATE - 02-08-11 | REVISED - |
| | | | |

| | | | |
|--|------------------|-----------------|-----------|
| FILE NAME = | USER NAME = Paul | DESIGNED - JLS | REVISED - |
| S:\Projects\410-0022-3HY US 50 Final Plans\dgn\CADD Sheets\0876843-ah1-ATB.dgn | | DRAWN - PDB | REVISED - |
| | | CHECKED - BRM | REVISED - |
| | | DATE - 02-08-11 | REVISED - |
| | | | |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

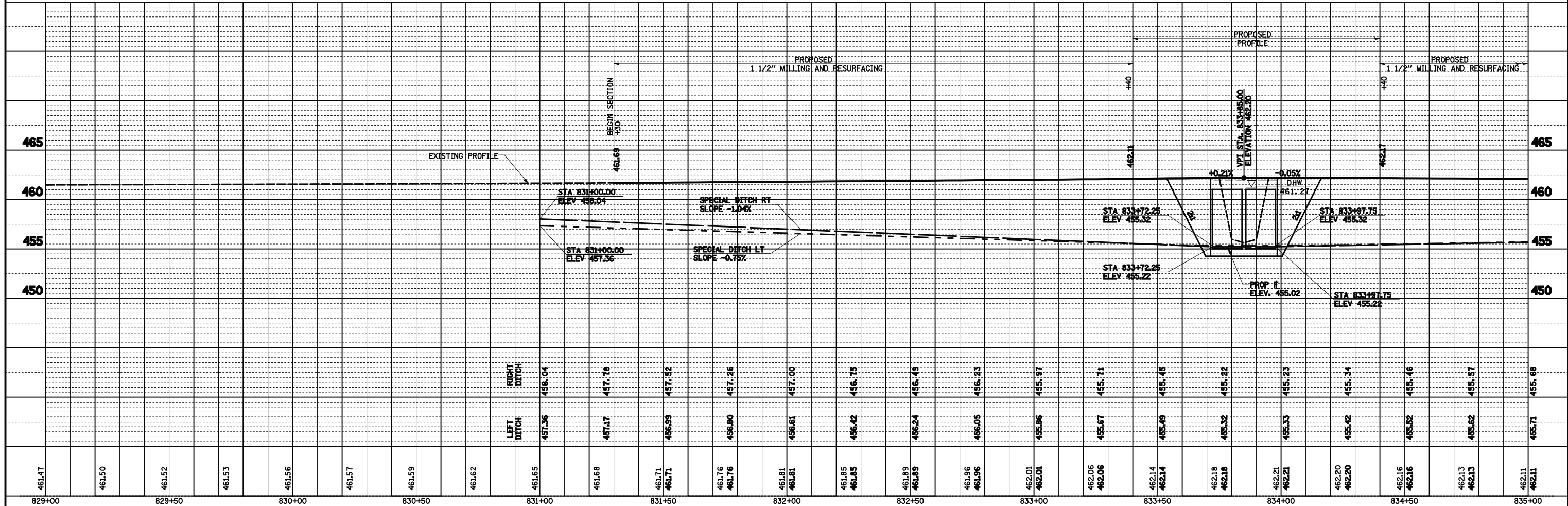
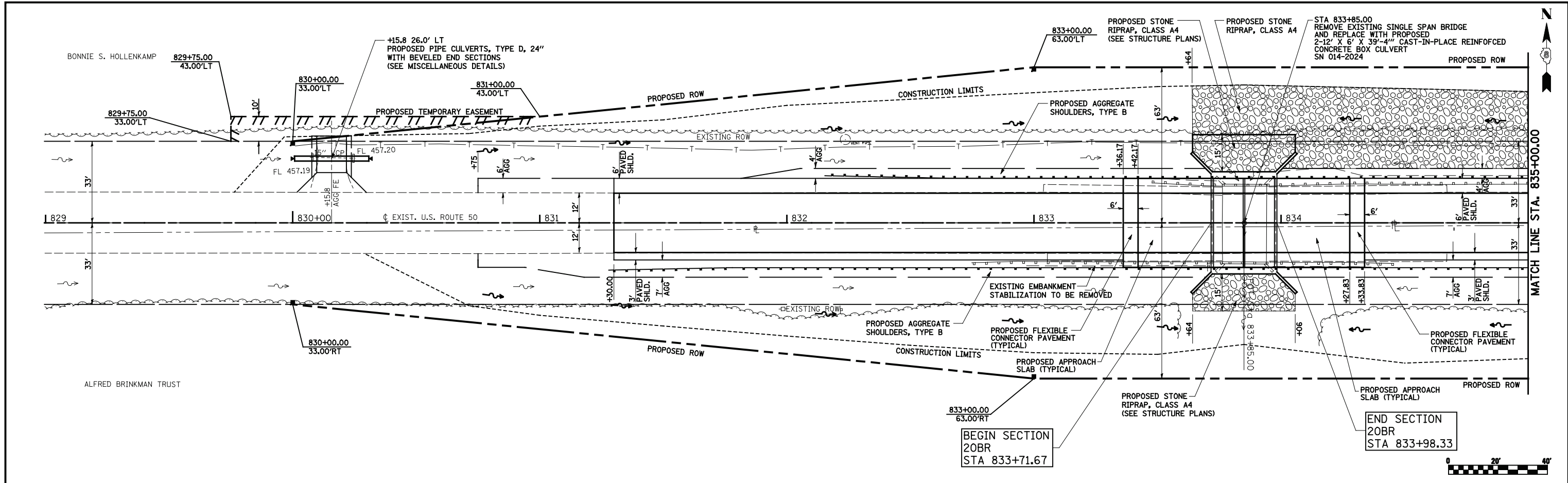
HORIZONTAL CONTROL & TIE POINTS, US ROUTE 50

SCALE: SHEET NO. 2 OF 2 SHEETS STA. TO STA.

| | | | | |
|---------------------------|---------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 327 | 20BR | CLINTON | 51 | 11 |
| CONTRACT NO. 76B43 | | | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

| | | | |
|------|----------------|----|------|
| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | CHECKED | | |
| | APPROVED | | |
| | NO. _____ | | |
| | CADD FILE NAME | | |

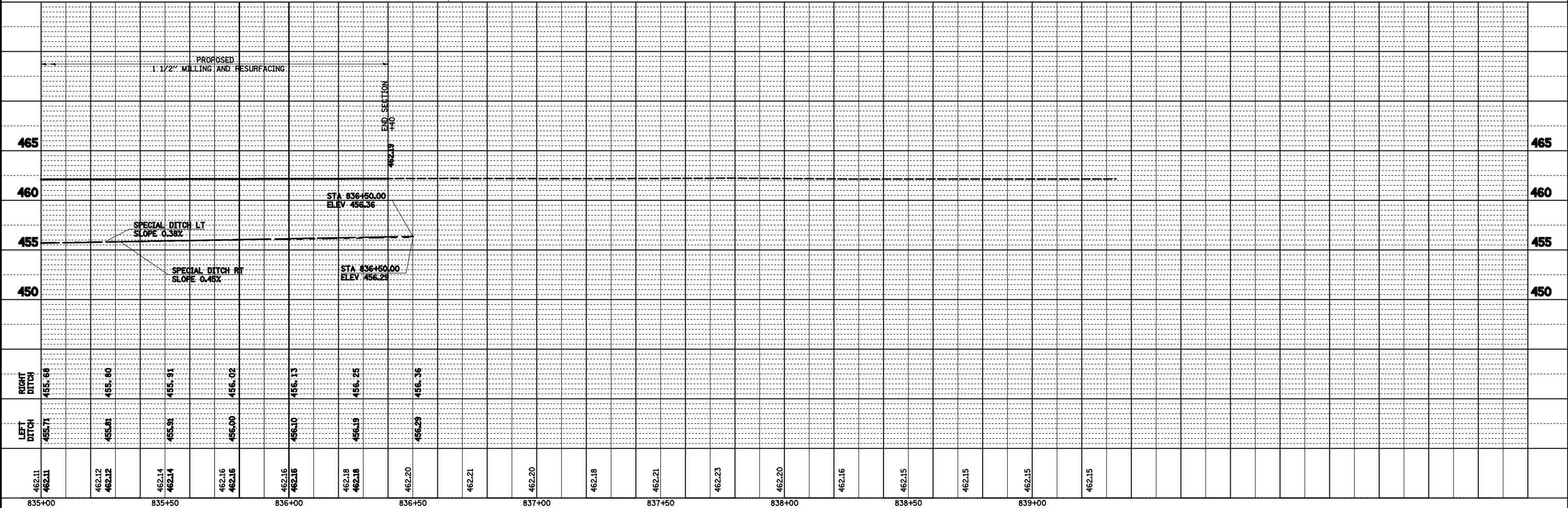
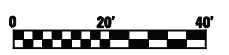
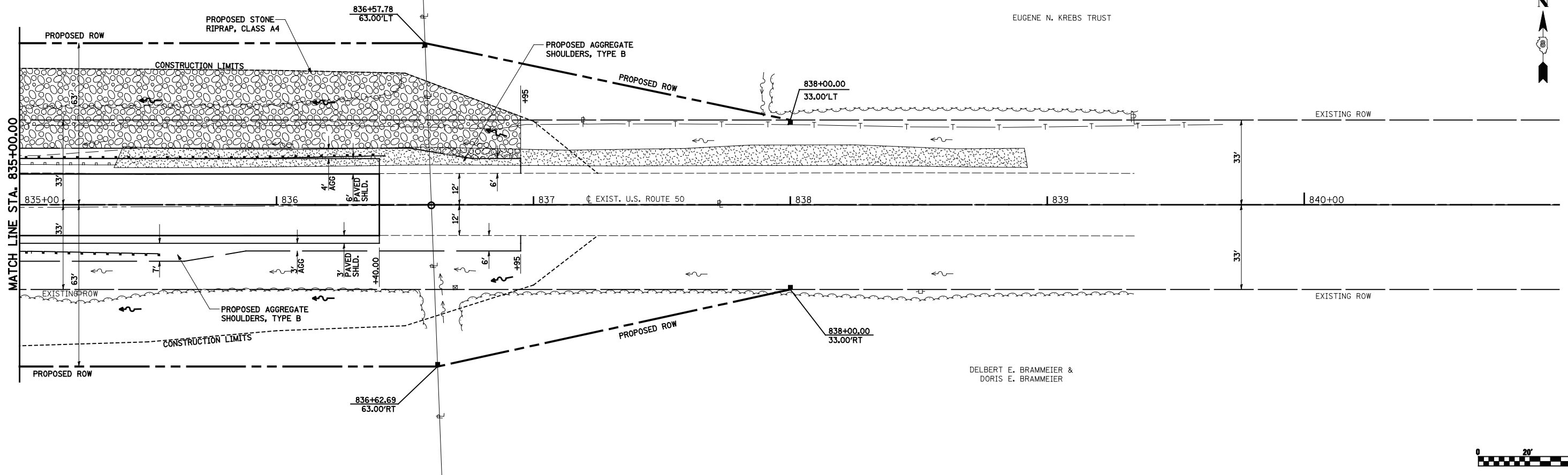
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|---------|--------------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | CHECKED | | |
| | APPROVED | | |
| | NO. _____ | | |
| | NOTATIS CHRD | | |



| | | | | | | | | | | | |
|---------------------------|-----------------------------|----------------|-----------|---|---|--------------------|--------------|-------------------------|-----------------|----------------------------------|--|
| FILE NAME = | USER NAME = Paul | DESIGNED - JWS | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PLAN AND PROFILE - FAP 327 (US ROUTE 50) | F.A.P. RTE. 327 | SECTION 20BR | COUNTY CLINTON | TOTAL SHEETS 51 | SHEET NO. 12 | |
| *FILEL\$ | PLOT SCALE = 40.0000' / IN. | CHECKED - BRM | REVISED - | | | SCALE: 1"=20' | | SHEET NO. 1 OF 2 SHEETS | | STA. 829+00.00 TO STA. 835+00.00 | |
| | PLOT DATE = 11/16/2011 | DATE - 12-2-10 | REVISED - | | | CONTRACT NO. 76B43 | | | | | |
| ILLINOIS FED. AID PROJECT | | | | | | | | | | | |

| | | | |
|---------------|---------------|----|------|
| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | CHECKED | | |
| | ALIGNED | | |
| | CAD FILE NAME | | |
| NOTE BOOK NO. | | | |

| | | | |
|---------------|--------------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | CHECKED | | |
| | STRUCTURE | | |
| NOTE BOOK NO. | | | |
| | NOTATIS CHRD | | |

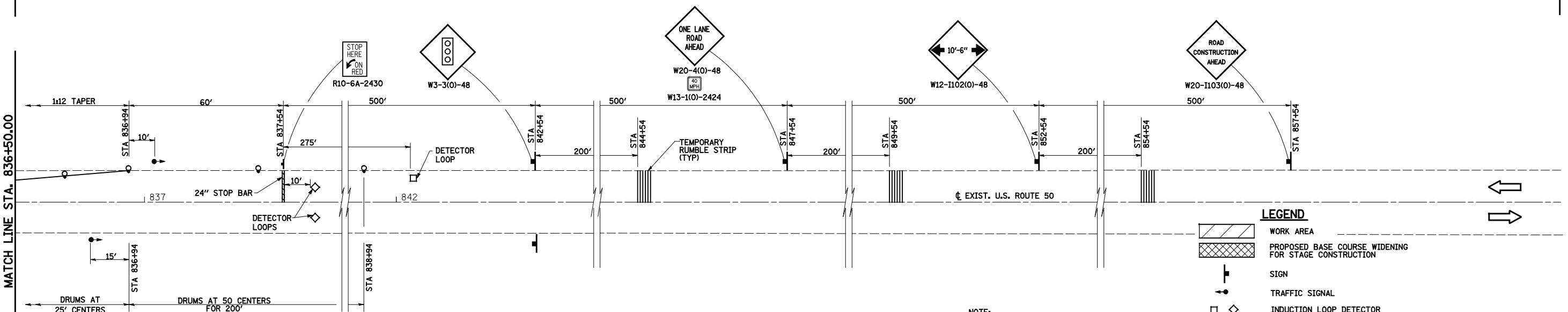
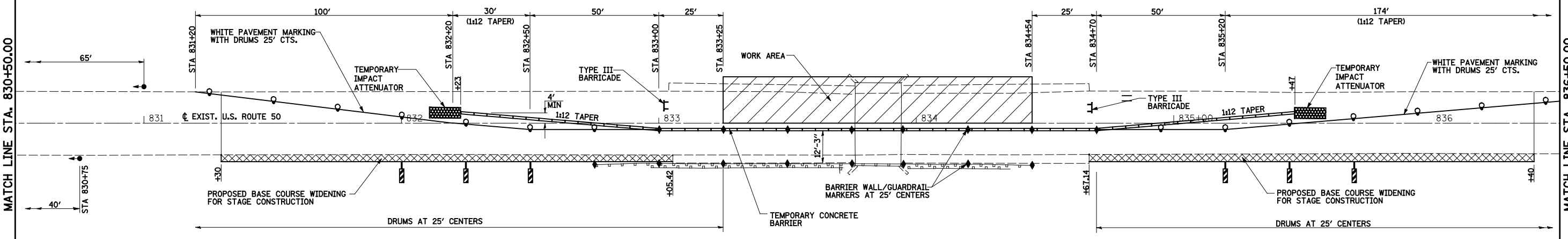
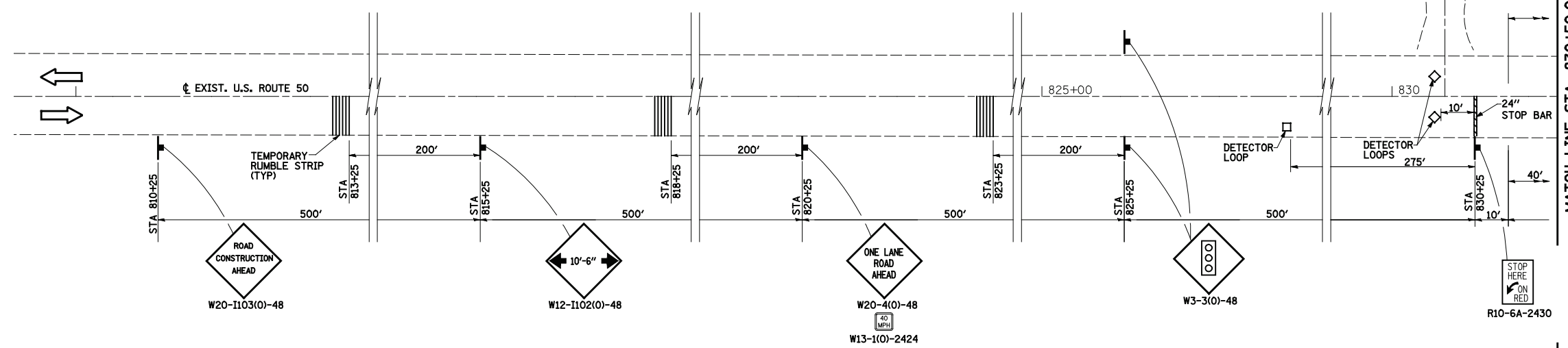
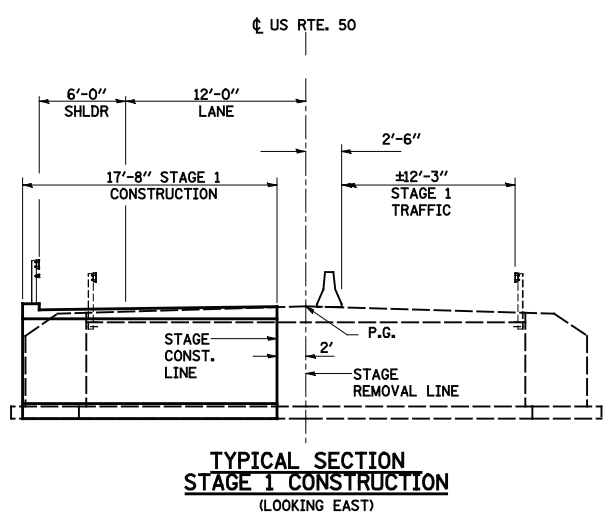


| | | | |
|-------------|-----------------------------|----------------|-----------|
| FILE NAME = | USER NAME = Paul | DESIGNED - JWS | REVISED - |
| *FILEL* | | DRAWN - PDB | REVISED - |
| | PLOT SCALE = 40.0000' / IN. | CHECKED - BRM | REVISED - |
| | PLOT DATE = 11/16/2011 | DATE - 12-2-10 | REVISED - |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE - FAP 327 (US ROUTE 50)
 SCALE: 1"=20' SHEET NO. 2 OF 2 SHEETS STA. 835+00.00 TO STA. 839+50.00

| | | | | |
|---------------------------|---------|---------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 327 | 20BR | CLINTON | 51 | 13 |
| CONTRACT NO. 76B43 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



- LEGEND**
- WORK AREA
 - PROPOSED BASE COURSE WIDENING FOR STAGE CONSTRUCTION
 - SIGN
 - TRAFFIC SIGNAL
 - INDUCTION LOOP DETECTOR
 - DOUBLE VERTICAL PANEL
 - DRUMS WITH BI-DIRECTIONAL STEADY BURNING LIGHTS
 - TYPE C BIDIRECTIONAL REFLECTORS

NOTE:
SEE STANDARD 701321 FOR DETAILS NOT SHOWN ON PLANS.
SEE MISCELLANEOUS DETAIL SHEET FOR SAND MODULE IMPACT ATTENUATOR LAYOUT.

FILE NAME =
#FILE#

USER NAME = Paul
DRAWN - PDB
CHECKED - BRM
DATE - 02-08-11

DESIGNED - JLS
REVISIONS -

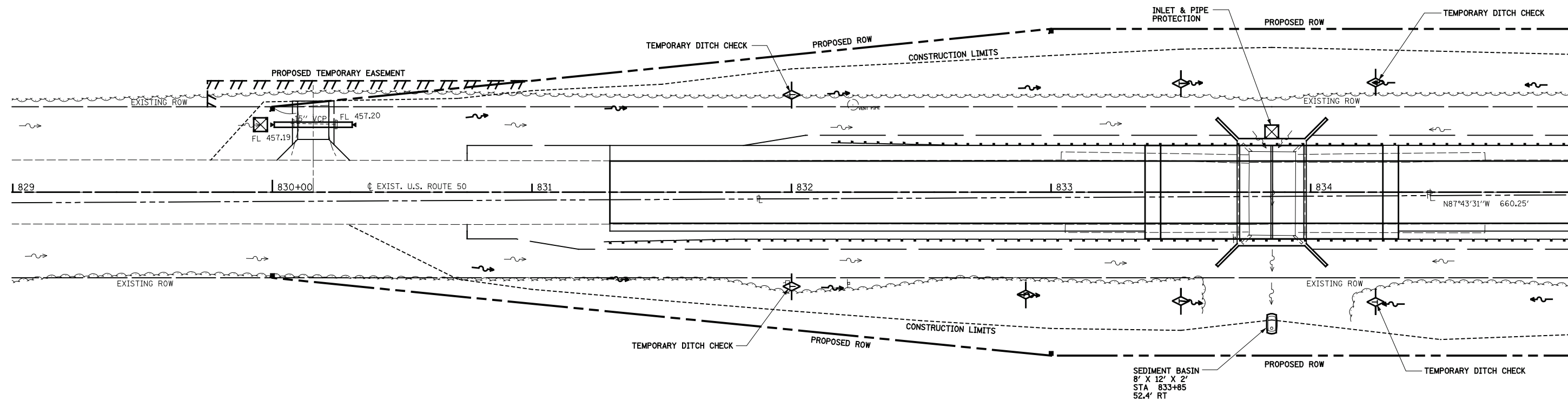
REVISED -
REVISED -
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**STAGE CONSTRUCTION DETAILS
STAGE 1, US ROUTE 50**

SCALE: 1"=20' SHEET NO. 1 OF 2 SHEETS STA. 810+25.00 TO STA. 857+52.00

| | | | | |
|---------------------------|---------|---------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 327 | 20BR | CLINTON | 51 | 14 |
| CONTRACT NO. 76B43 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



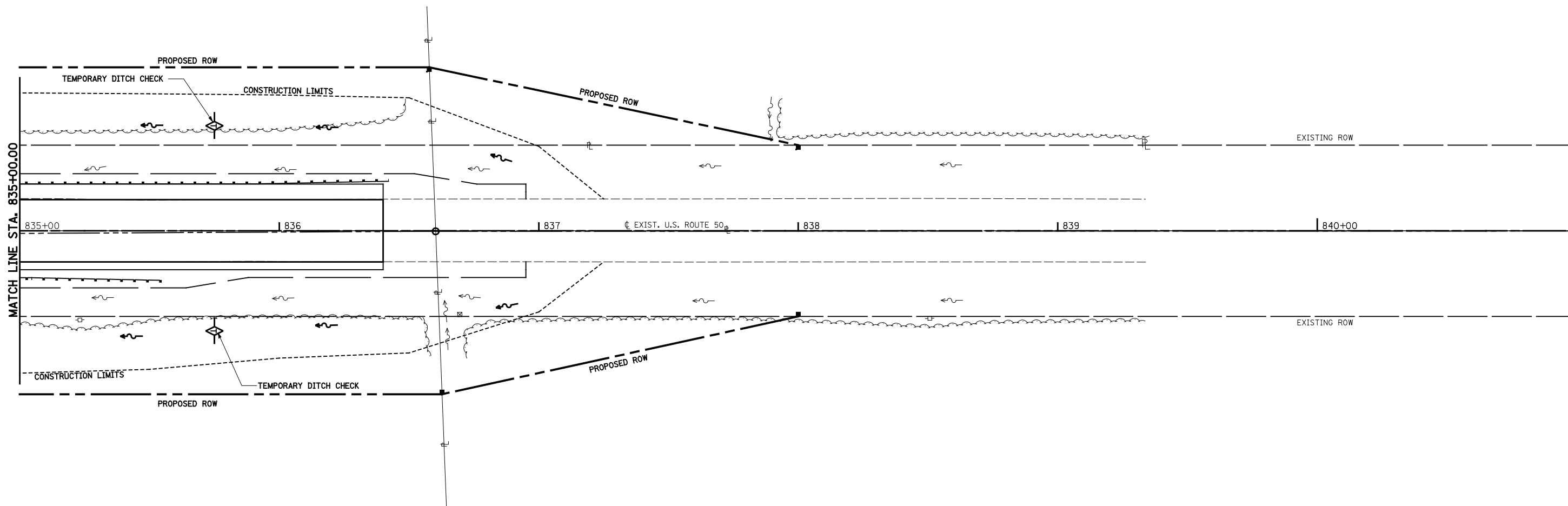
MATCH LINE STA. 835+00.00

LEGEND

| ITEM | SYMBOL |
|---------------------------|--------|
| TEMPORARY DITCH CHECKS | |
| ROADWAY DITCH FLOW | |
| INLET AND PIPE PROTECTION | |
| SEDIMENT BASIN | |



| | | | | | | | | | | | | |
|------------------------|-----------------------------|-----------------|-----------|---|--|-------------------------|----------------------------------|---------------------------|-----------------|-------------------|-----------------------|--------------------|
| FILE NAME = #FILEL# | USER NAME = Paul | DESIGNED - JLS | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | EROSION CONTROL - FAP 327 (US ROUTE 50) | | | F.A.P. RTE. 327 | SECTION 20BR | COUNTY CLINTON | TOTAL SHEETS 51 | SHEET NO. 17 |
| | PLOT SCALE = 40.0000' / IN. | DRAWN - PDB | REVISED - | | SCALE: 1"=20' | SHEET NO. 1 OF 2 SHEETS | STA. 829+00.00 TO STA. 835+00.00 | CONTRACT NO. 76B43 | | | | |
| | PLOT DATE = 11/16/2011 | CHECKED - BRM | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | |
| | | DATE - 02-10-11 | REVISED - | | | | | | | | | |



LEGEND

| ITEM | SYMBOL |
|---------------------------|--------|
| TEMPORARY DITCH CHECKS | |
| ROADWAY DITCH FLOW | |
| INLET AND PIPE PROTECTION | |
| SEDIMENT BASIN | |



| | | | | | | | | | | | | | |
|------------------------|------------------|----------------|-----------|---|--|-------------------------|----------------------------------|---------------------------|-----------------|-------------------|-----------------------|--------------------|--|
| FILE NAME = #FILEL# | USER NAME = Paul | DESIGNED - JLS | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | EROSION CONTROL - FAP 327 (US ROUTE 50) | | | F.A.P. RTE. 327 | SECTION 20BR | COUNTY CLINTON | TOTAL SHEETS 51 | SHEET NO. 18 | |
| | | | | | SCALE: 1"=20' | SHEET NO. 2 OF 2 SHEETS | STA. 835+00.00 TO STA. 839+50.00 | CONTRACT NO. 76B43 | | | | | |
| | | | | | ILLINOIS FED. AID PROJECT | | | | | | | | |
| | | | | | | | | | | | | | |

PART OF SECTION 18, T2N, R1W, OF THE 3RD PM, CLINTON COUNTY, ILLINOIS

SEE BASIS OF COORDINATES AND BEARINGS STATEMENT ON SHEET 2



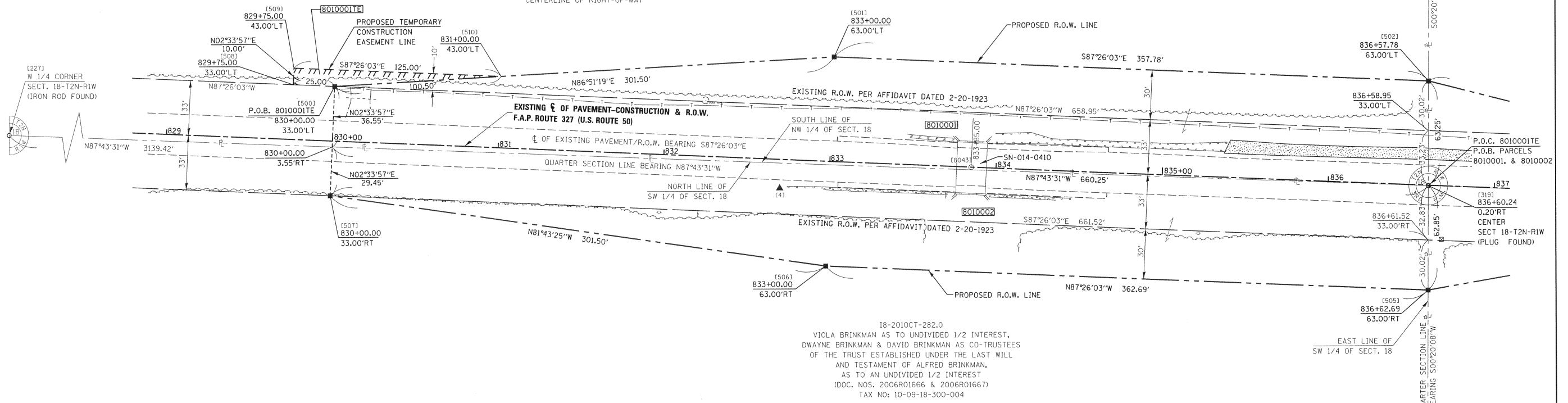
SPACE RESERVED FOR RECORDING OFFICER

| COORDINATE TABLE | | | | |
|---|-----------|----------|--------------|--------------|
| EPNT #1/DESCRIPTION | STATION | OFFSET | NORTH | EAST |
| [227] W 1/4 CORNER IRON ROD FOUND | | | 711,722.5060 | 557,425.4500 |
| [264] N 1/4 CORNER IRON ROD W/CAP FOUND | | | 714,267.2460 | 560,578.0340 |
| [319] CENTER SECT. 18 PLUG FOUND | 836+60.24 | 0.20'RT | 711,597.9040 | 560,562.4010 |
| [500] /PROP. ROW | 830+00.00 | 33.00'LT | 711,660.6253 | 559,904.3068 |
| [501] /PROP. ROW | 833+00.00 | 63.00'LT | 711,677.1654 | 560,205.3490 |
| [502] /PROP. ROW | 836+57.78 | 63.00'LT | 711,661.1490 | 560,562.7714 |
| [505] /PROP. ROW | 836+62.69 | 63.00'RT | 711,535.0557 | 560,562.0329 |
| [506] /PROP. ROW | 833+00.00 | 63.00'RT | 711,551.2918 | 560,199.7085 |
| [507] /PROP. ROW | 830+00.00 | 33.00'RT | 711,594.6915 | 559,901.3522 |
| [508] TE | 829+75.00 | 33.00'LT | 711,661.7445 | 559,879.3318 |
| [509] TE | 829+75.00 | 43.00'LT | 711,671.7344 | 559,879.7795 |
| [510] TE | 831+00.00 | 43.00'LT | 711,666.1387 | 560,004.6542 |
| [8042] POT | 824+00.00 | | 711,654.5180 | 559,303.4310 |
| [8043] POL | 833+85.00 | | 711,610.4230 | 560,287.4440 |

LOT 1 BEING
EAST 80 ACRES OF NW 1/4
SECTION 18, T2N, R1W

18-2010CT-284.0
BONNIE S. HOLLENKAMP
TAX NO. 10-09-18-100-012
(DOC. NO. 2001R0046 & 2001R0043)

NOTE: CENTERLINE OF PAVEMENT IS ALSO CENTERLINE OF RIGHT-OF-WAY



18-2010CT-282.0
VIOLA BRINKMAN AS TO UNDIVIDED 1/2 INTEREST,
DWAYNE BRINKMAN & DAVID BRINKMAN AS CO-TRUSTEES
OF THE TRUST ESTABLISHED UNDER THE LAST WILL
AND TESTAMENT OF ALFRED BRINKMAN,
AS TO AN UNDIVIDED 1/2 INTEREST
(DOC. NOS. 2006R01666 & 2006R01667)
TAX NO: 10-09-18-300-004

SW 1/4
SECTION 18, T2N, R1W

ASSOCIATED PROFESSIONALS, INC.
17625 Mockingbird Road, P.O. Box 311
Nashville, Illinois 62263
Ph. 618-478-9000 Fax 618-478-9001
web: www.apisurvey.com
IDPR Design Firm License No. 184-001303

ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS
FAP ROUTE 327 (US 50)
SECTION 20BR
CLINTON COUNTY
JOB NO. R-98-010-10
STATION 830+00.00 TO STATION 836+60.24

SCALE: 1" = 30'
SHEET 3 OF 5

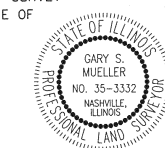
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|---------|---------|--------------|-----------|
| 327 | 20BR | CLINTON | 51 | 29 |

CONTRACT NO. 76B43

STATE OF ILLINOIS)
) SS
COUNTY OF WASHINGTON)

I, GARY S. MUELLER, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, CERTIFY THAT I HAVE SURVEYED THE PLAT OF HIGHWAY SHOWN HEREON AND THAT THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY FOR THE PROPOSED PARCEL(S) TO BE ACQUIRED BY THE STATE OF ILLINOIS, DEPARTMENT OF TRANSPORTATION, SHOWN HEREON.

DATED 7/5/2011
Gary S. Mueller
GARY S. MUELLER, PLS NO. 3332
LICENSE EXPIRATION DATE: 11/30/2012



COMPLETION DATE OF FIELD WORK PERFORMED
LAND SURVEY: 10/29/2010 ROW STAKING: 5/5/2011

* SEE TOTAL HOLDING AREA SOURCE TABLE ON SHEET 2

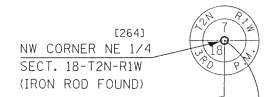
| PARCEL NO. | OWNER | TOTAL HOLDING ACRES | FEE SIMPLE ACQUISITION | | | | | | REMAINDER ACRES | EASEMENTS | | PERMANENT TAX NUMBER | PROPERTY ACQUIRED BY |
|------------|---|---------------------|------------------------|---------|---------------------------------|---------|-----------|---------|-----------------|-------------------------------------|---------|----------------------|----------------------|
| | | | GROSS ACRES | SO. FT. | PREVIOUSLY DEDICATED/USED ACRES | SO. FT. | NET ACRES | SO. FT. | | PE = PERMANENT TE = TEMPORARY ACRES | SO. FT. | | |
| 8010001 | BONNIE S. HOLLENKAMP, A MARRIED PERSON TITLE REPORT NO. CT-284.0 | 193.7605 | 0.8782 | 38,255 | 0.5281 | 23,004 | 0.3501 | 15,251 | 192.8823 | TE 0.0172 | TE 750 | 10-09-18-100-012 | |
| 8010002 | VIOLA BRINKMAN AS TO AN UNDIVIDED 1/2 INTEREST AND DWAYNE BRINKMAN AND DAVID BRINKMAN AS CO-TRUSTEES OF THE TRUST ESTABLISHED UNDER THE LAST WILL AND TESTAMENT OF ALFRED BRINKMAN, AS TO AN UNDIVIDED 1/2 INTEREST | 175.600 | 0.8249 | 35,933 | 0.4722 | 20,570 | 0.3527 | 15,363 | 174.7751 | | | 10-09-18-300-004 | |

PART OF SECTION 18, T2N, R1W, OF THE 3RD PM, CLINTON COUNTY, ILLINOIS

SEE BASIS OF COORDINATES AND BEARINGS STATEMENT ON SHEET 2



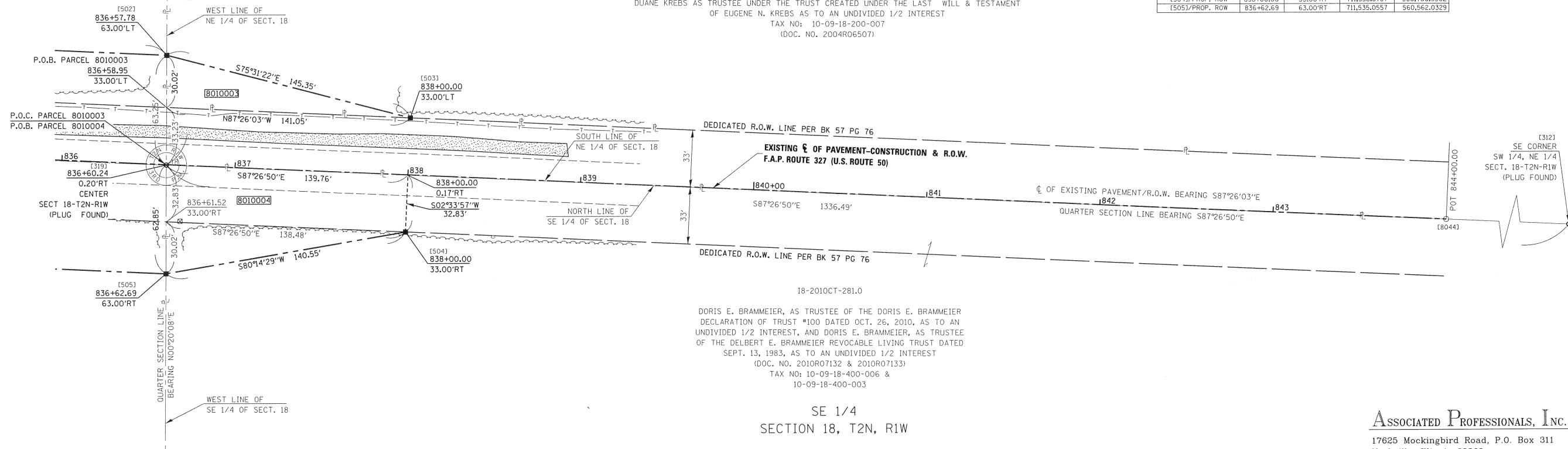
SPACE RESERVED FOR RECORDING OFFICER



NE 1/4
SECTION 18, T2N, R1W

18-2010CT-283.0
LIFE ESTATE IN AUDREY L. KREBS AKA AUDREY KREBS,
REMAINDER INTEREST IN AUDREY L. KREBS, TRUSTEE OF THE AUDREY L. KREBS REVOCABLE
LIVING TRUST DATED 8-25-04 AS TO AN UNDIVIDED 1/2 INTEREST AND
DUANE KREBS AS TRUSTEE UNDER THE TRUST CREATED UNDER THE LAST WILL & TESTAMENT
OF EUGENE N. KREBS AS TO AN UNDIVIDED 1/2 INTEREST
TAX NO: 10-09-18-200-007
(DOC. NO. 2004R06507)

| COORDINATE TABLE | | | | |
|---|-----------|----------|--------------|--------------|
| (PNT #)/DESCRIPTION | STATION | OFFSET | NORTH | EAST |
| {264}/N 1/4 CORNER IRON ROD W/CAP FOUND | | | 714,267.2460 | 560,578.0340 |
| {312}/SE CORNER SW 1/4, NE 1/4 PLUG FOUND | | | 711,538.3800 | 561,897.5640 |
| {319}/CENTER SECT. 18 PLUG FOUND | 836+60.24 | 0.20'RT | 711,597.9040 | 560,562.4010 |
| {502}/PROP. ROW | 836+57.78 | 63.00'LT | 711,661.1490 | 560,562.7714 |
| {503}/PROP. ROW | 838+00.00 | 33.00'LT | 711,624.8125 | 560,703.5048 |
| {504}/PROP. ROW | 838+00.00 | 33.00'RT | 711,558.8787 | 560,700.5502 |
| {505}/PROP. ROW | 836+62.69 | 63.00'RT | 711,538.0557 | 560,562.0329 |



18-2010CT-281.0
DORIS E. BRAMMEIER, AS TRUSTEE OF THE DORIS E. BRAMMEIER
DECLARATION OF TRUST #100 DATED OCT. 26, 2010, AS TO AN
UNDIVIDED 1/2 INTEREST, AND DORIS E. BRAMMEIER, AS TRUSTEE
OF THE DELBERT E. BRAMMEIER REVOCABLE LIVING TRUST DATED
SEPT. 13, 1983, AS TO AN UNDIVIDED 1/2 INTEREST
(DOC. NO. 2010R07132 & 2010R07133)
TAX NO: 10-09-18-400-006 &
10-09-18-400-003

SE 1/4
SECTION 18, T2N, R1W

* SEE TOTAL HOLDING AREA
SOURCE TABLE ON SHEET 2

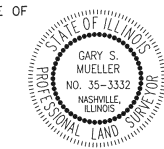
| PARCEL NO. | OWNER | TOTAL HOLDING ACRES | FEE SIMPLE ACQUISITION | | | | REMAINDER ACRES | PERMANENT TAX NUMBER | PROPERTY ACQUIRED BY | | |
|------------|--|---------------------|------------------------|---------------|---------------------------------|-------------|-----------------|----------------------|----------------------|-------------------------------------|--|
| | | | GROSS ACRES | GROSS SQ. FT. | PREVIOUSLY DEDICATED/USED ACRES | NET SQ. FT. | | | | | |
| 8010003 | A LIFE ESTATE IN AUDREY L. KREBS AKA AUDREY KREBS, REMAINDER INTEREST IN AUDREY L. KREBS, TRUSTEE OF THE AUDREY L. KREBS REVOCABLE LIVING TRUST DATED AUGUST 25, 2004 AS TO AN UNDIVIDED 1/2 INTEREST AND DUANE KREBS AS TRUSTEE UNDER THE TRUST CREATED UNDER THE LAST WILL AND TESTAMENT OF EUGENE N. KREBS AS TO AN UNDIVIDED 1/2 INTEREST TITLE REPORT NO. CT-283.0 | 148.770 | 0.0485 | 2,113 | N/A | N/A | 0.0485 | 2,113 | 148.7215 | 10-09-18-200-007 | |
| 8010004 | DORIS E. BRAMMEIER, AS TRUSTEE OF THE DORIS E. BRAMMEIER DECLARATION OF TRUST #100 DATED OCT. 26, 2010, AS TO AN UNDIVIDED 1/2 INTEREST, AND DORIS E. BRAMMEIER, AS TRUSTEE OF THE DELBERT E. BRAMMEIER REVOCABLE LIVING TRUST DATED SEPT. 13, 1983, AS TO AN UNDIVIDED 1/2 INTEREST TITLE REPORT NO. CT-281.0 | 133.960 | 0.1524 | 6,639 | 0.1048 | 4,565 | 0.0476 | 2,074 | 133.8076 | 10-09-18-400-003 & 10-09-18-400-006 | |

ASSOCIATED PROFESSIONALS, INC.
17625 Mockingbird Road, P.O. Box 311
Nashville, Illinois 62263
Ph. 618-478-9000 Fax 618-478-9001
web: www.apisurvey.com
IDPR Design Firm License No. 184-001303

STATE OF ILLINOIS)
COUNTY OF WASHINGTON) SS

I, GARY S. MUELLER, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, CERTIFY THAT I HAVE SURVEYED THE PLAT OF HIGHWAY SHOWN HEREON AND THAT THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY FOR THE PROPOSED PARCEL(S) TO BE ACQUIRED BY THE STATE OF ILLINOIS, DEPARTMENT OF TRANSPORTATION, SHOWN HEREON.

DATED 7/5/2011
Gary S. Mueller
GARY S. MUELLER, PLS NO. 3332
LICENSE EXPIRATION DATE: 11/30/2012



| COMPLETION DATE OF FIELD WORK PERFORMED | |
|---|-----------------------|
| LAND SURVEY: 10/29/2010 | ROW STAKING: 5/5/2011 |

ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS
FAP ROUTE 327 (US 50)
SECTION 20BR
CLINTON COUNTY
JOB NO. R-98-010-10
STATION 836+60.24 TO STATION 838+00.00

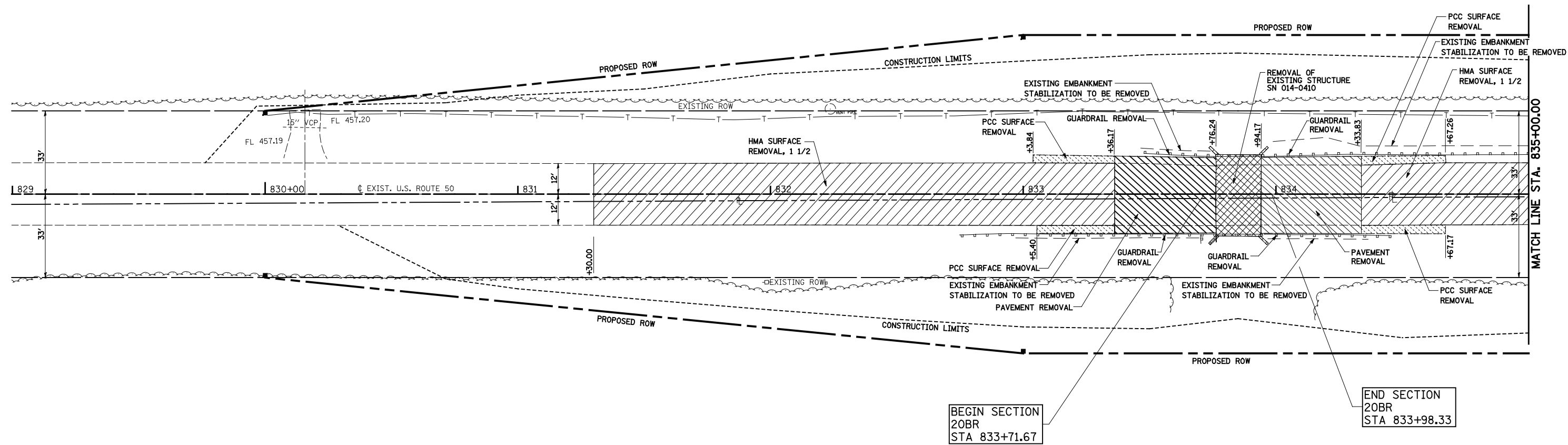
SCALE: 1" = 30'

ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS/REGION 5/DISTRICT 8
1102 EASTPORT PLAZA DRIVE
COLLINGSVILLE, ILLINOIS 62234-6198

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|---------|---------|--------------|-----------|
| 327 | 20BR | CLINTON | 51 | 21 |

CONTRACT NO. 76B43

FED. ROAD DIST. NO. 8 ILLINOIS FED. AID PROJECT



LEGEND

- HMA SURFACE REMOVAL, 1 1/2"
- PAVEMENT REMOVAL
- REMOVAL OF EXISTING STRUCTURE
- PCC SURFACE REMOVAL (VARIABLE DEPTH)

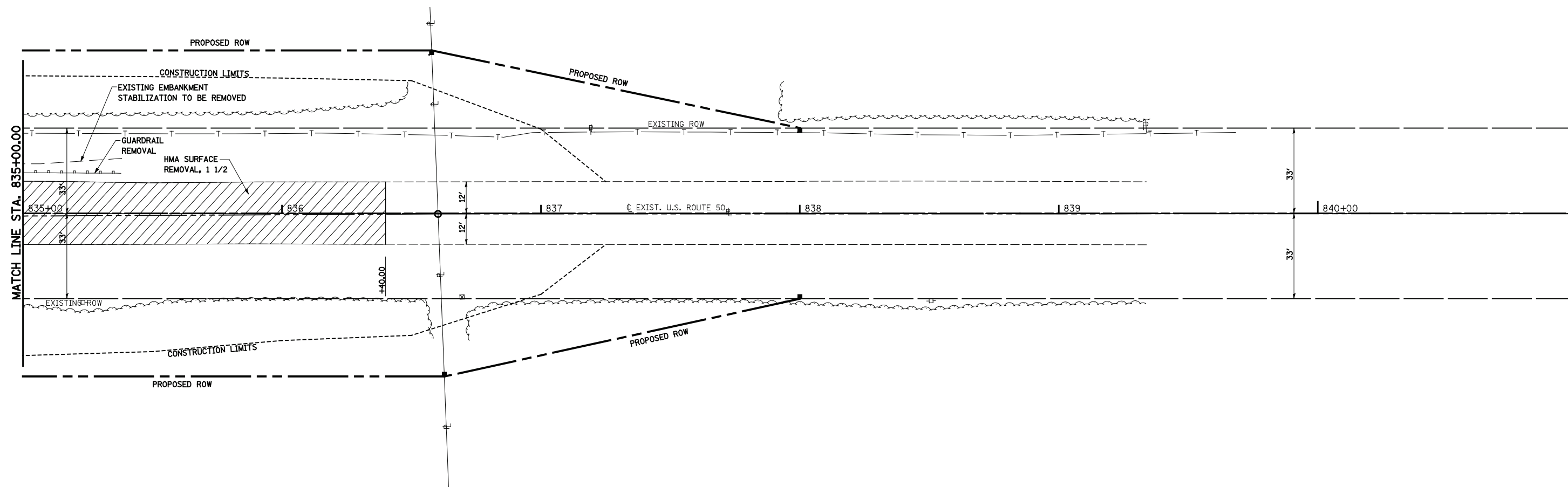


| | | | | | | | | | | | |
|-----------------------------|------------------|-----------------|-----------|---|--|--------------------|--------------|-------------------------|-----------------|----------------------------------|--|
| FILE NAME = #FILEL# | USER NAME = Paul | DESIGNED - JLS | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REMOVAL - FAP 327 (US ROUTE 50) | F.A.P. RTE. 327 | SECTION 20BR | COUNTY CLINTON | TOTAL SHEETS 51 | SHEET NO. 23 | |
| PLOT SCALE = 40.0000' / IN. | | CHECKED - BRM | REVISED - | | | SCALE: 1"=20' | | SHEET NO. 1 OF 2 SHEETS | | STA. 829+00.00 TO STA. 835+00.00 | |
| PLOT DATE = 11/17/2011 | | DATE - 02-10-11 | REVISED - | | | CONTRACT NO. 76B43 | | | | | |
| ILLINOIS FED. AID PROJECT | | | | | | | | | | | |

BEGIN SECTION
20BR
STA 833+71.67

END SECTION
20BR
STA 833+98.33

MATCH LINE STA. 835+00.00

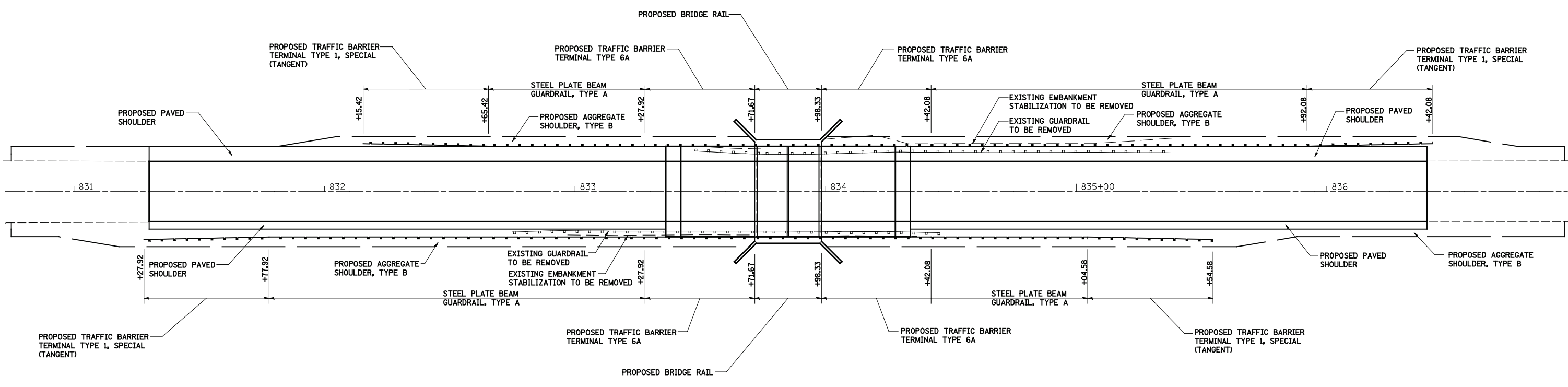


LEGEND

- HMA SURFACE REMOVAL, 1 1/2"
- PAVEMENT REMOVAL
- REMOVAL OF EXISTING STRUCTURE
- PCC SURFACE REMOVAL (VARIABLE DEPTH)



| | | | | | | | | | | | | | | |
|-----------------------------|------------------|-----------------|-----------|---|--|--|--|-------------------------|-----------------|-------------------|----------------------------------|--------------------|--------------------|--|
| FILE NAME = #FILEL# | USER NAME = Paul | DESIGNED - JLS | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REMOVAL - FAP 327 (US ROUTE 50) | | | F.A.P. RTE. 327 | SECTION 20BR | COUNTY CLINTON | TOTAL SHEETS 51 | SHEET NO. 24 | | |
| PLOT SCALE = 40.0000' / IN. | | CHECKED - BRM | REVISED - | | SCALE: 1"=20' | | | SHEET NO. 2 OF 2 SHEETS | | | STA. 835+00.00 TO STA. 839+50.00 | | CONTRACT NO. 76B43 | |
| PLOT DATE = 11/16/2011 | | DATE - 02-10-11 | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | | | |
| | | | | | | | | | | | | | | |



**US ROUTE 50
GUARDRAIL LOCATION LEFT**

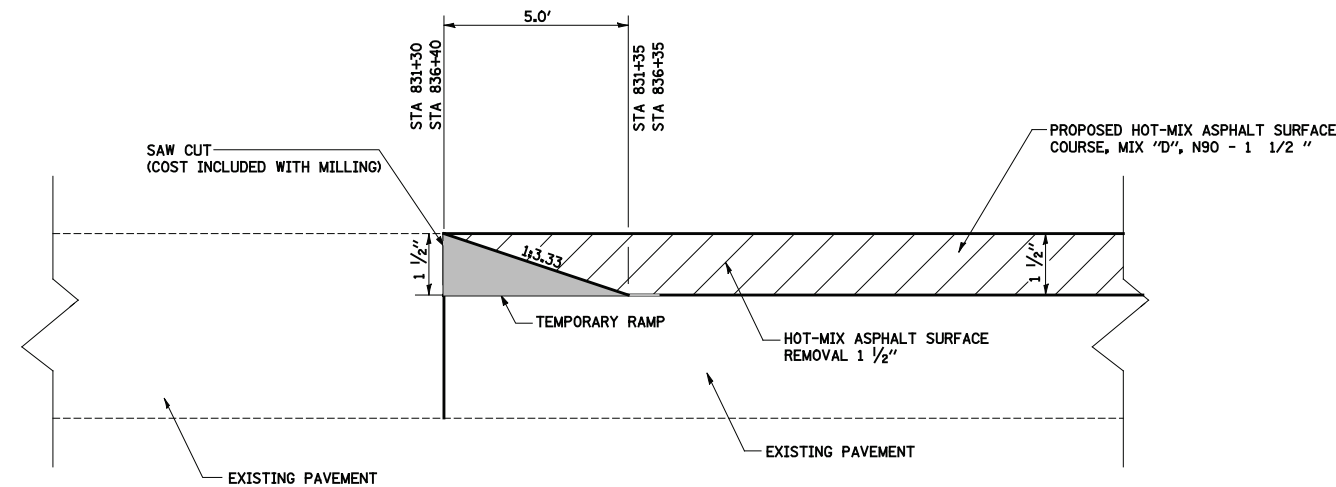
STA 832+15.42 TO STA 833+71.67
STA 833+98.33 TO STA 836+42.08

**US ROUTE 50
GUARDRAIL LOCATION RIGHT**

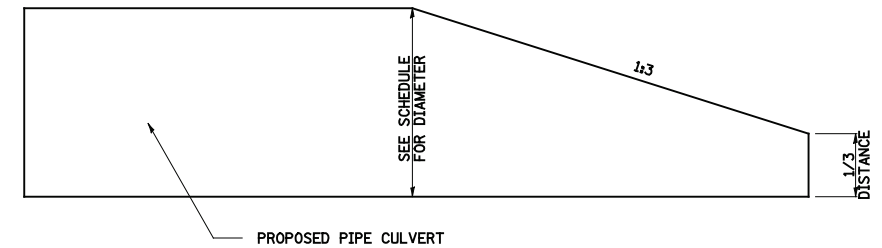
STA 831+27.92 TO STA 833+71.67
STA 833+98.33 TO STA 835+54.08



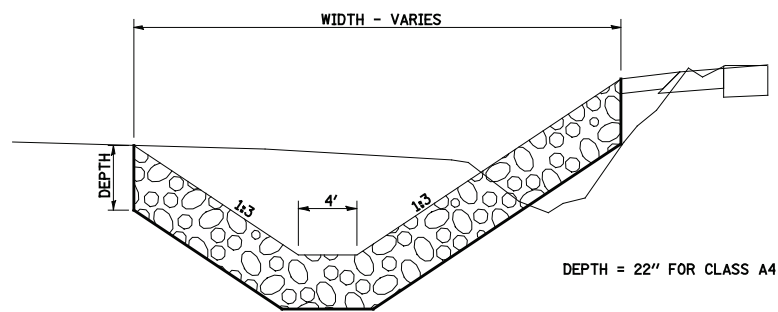
| | | | | | | | | | | | | |
|------------------------|-----------------------------|----------------|--|---|--|--------------------|---------|----------------|---------|--------|-----------------|--------------|
| FILE NAME = #FILEL# | USER NAME = Paul | DESIGNED - JLS | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | GUARDRAIL PLAN DETAILS FAP 327, US ROUTE 50 | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | PLOT SCALE = 40.0000' / IN. | DRAWN - PDB | REVISED - | | 327 | 20BR | CLINTON | 51 | 25 | | | |
| PLOT DATE = 11/17/2011 | CHECKED - BRM | REVISED - | SCALE: 1"=20' SHEET NO. 1 OF 1 SHEETS STA. 825+00.00 TO STA. 842+00.00 | | | CONTRACT NO. 76B43 | | | | | | |
| | DATE - 1-19-11 | REVISED - | ILLINOIS FED. AID PROJECT | | | | | | | | | |



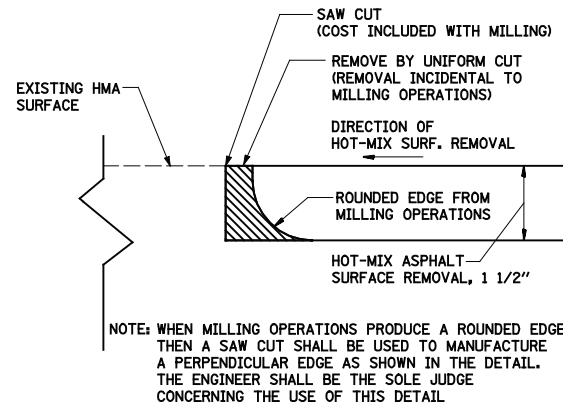
DETAIL OF SURFACE TRANSITION
NOT TO SCALE



BEVELED END DETAIL
THE BEVELED END PORTION WILL BE PAID FOR AT THE SAME UNIT PRICE PER FOOT AS THE PIPE CULVERT.

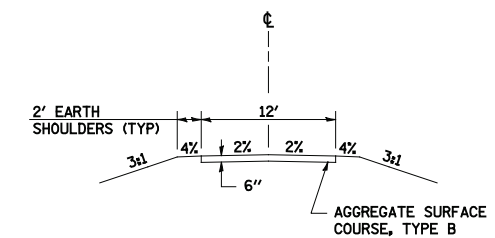
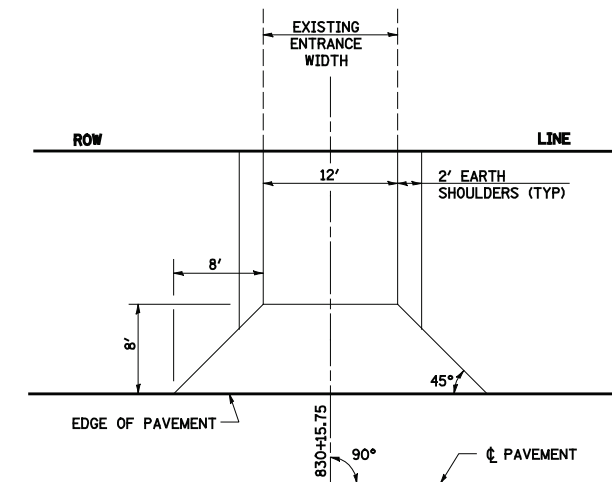


RIPRAP TYPICAL APPLICATION - CLASS A4



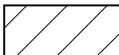

MILLING DETAIL AT SURFACE TRANSITION
NOT TO SCALE

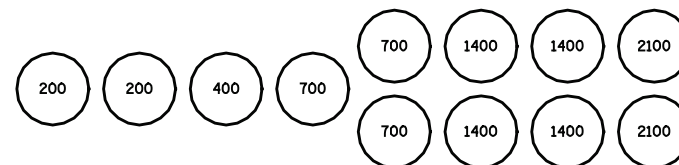
NOTE: WHEN MILLING OPERATIONS PRODUCE A ROUNDED EDGE, THEN A SAW CUT SHALL BE USED TO MANUFACTURE A PERPENDICULAR EDGE AS SHOWN IN THE DETAIL. THE ENGINEER SHALL BE THE SOLE JUDGE CONCERNING THE USE OF THIS DETAIL.



FIELD ENTRANCE DETAIL

LEGEND

| | |
|---|--|
|  | HOT-MIX ASPHALT SURFACE REMOVAL 1 1/2" |
|  | TEMPORARY RAMP |



SAND MODULE IMPACT ATTENUATOR LAYOUT
(IF OPTION USED)

| | | | |
|---|-----------------------------|----------------|-----------|
| FILE NAME = | USER NAME = Paul | DESIGNED - ESW | REVISED - |
| S:\Projects\410-0022-3HY US 50 Final Plans\dgn\CADD Sheets\0876843-ah1-misc.dgn | | DRAWN - PDB | REVISED - |
| | PLOT SCALE = 40.0000' / IN. | CHECKED - BRM | REVISED - |
| | PLOT DATE = 11/16/2011 | DATE - 11-1-10 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| | |
|------------------------------|--------------------------------------|
| MISCELLANEOUS DETAILS | |
| SCALE: 1"=20' | SHEET NO. 1 OF 1 SHEETS STA. TO STA. |

| | | | | |
|---|--------------|----------------|-----------------|--------------|
| F.A.P. RTE. 327 | SECTION 20BR | COUNTY CLINTON | TOTAL SHEETS 51 | SHEET NO. 26 |
| CONTRACT NO. 76843 | | | | |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |

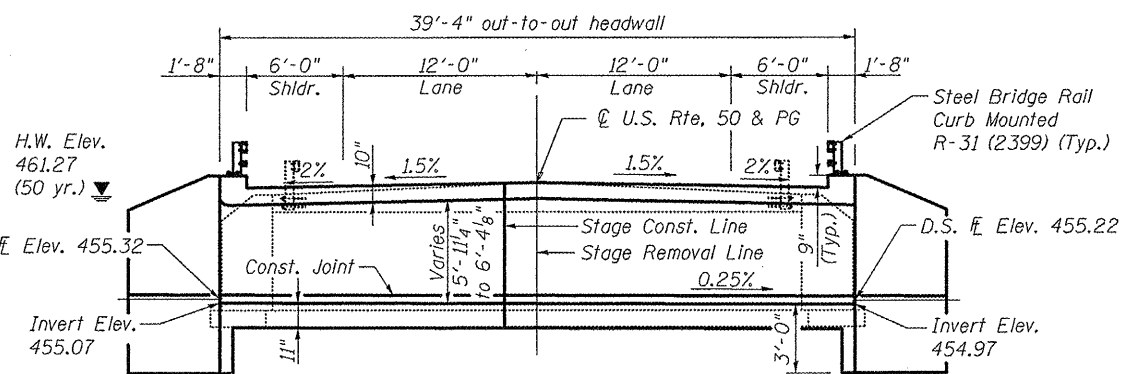
Bench Mark: Railroad Spike in Power Pole: Sta. 835+23.14, 34.37' Rt., Elev. 461.06

Existing Structure: S.N. 014-0410 originally constructed in 1922 as SBI Rte. 12, Section 20, with repairs in 2008 is a single span slab bridge that carries US Rte. 50 over an unnamed stream. The structure is on a 0 degree skew. The existing superstructure consists of an 11 1/2" thick concrete deck slab and a 10" reinforced concrete overlay. The span length is 16'-0" with a back-to-back abutment length of 18'-0". The out-to-out width is 32'-0" with a curb-to-curb width of 30'-0". The existing substructure consists of concrete abutments founded on spread footings. Traffic to be maintained utilizing stage construction with the use of signalized Traffic Lights.

No Salvage.

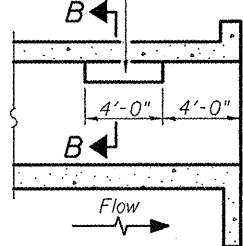
GENERAL NOTES

Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. Reinforcement bars designated (E) shall be epoxy coated. Precast alternative is not allowed. Excavation behind existing abutment walls shall be performed to balance front and back soil pressure before removing the existing superstructure. The Contractor shall sawcut the upper portion of the existing abutment at the stage removal line before Stage I removal to ensure the remaining portion will not be prematurely damaged.



LONGITUDINAL SECTION

Notch formed by rough finished board attached to and removed with form work, each interior wall. (Do not chamfer).



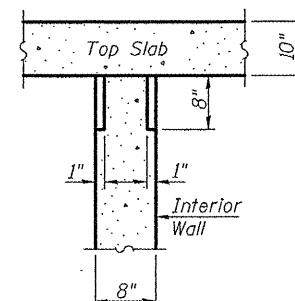
LONGITUDINAL SECTION

INDEX OF SHEETS

1. General Plan & Elevation
2. Stage Construction Details
3. Temporary Concrete Barrier for Stage Construction
4. Culvert Top Slab Elevations
5. Top of West Approach Slab Elevations
6. Top of East Approach Slab Elevations
- 7.-8. Bridge Approach Slab Details
9. Culvert Details-Top Slab
10. Culvert Details-Bottom Slab
11. Culvert Details
12. Steel Railing, Type 2399
13. Bar Splicer Details
- 14.-15. Soil Borings

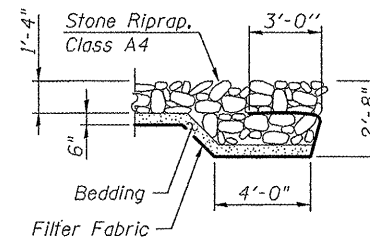
TOTAL BILL OF MATERIAL

| ITEM | UNIT | TOTAL |
|----------------------------------|---------|--------|
| Stone RipRap, Class A4 | Sq. Yd. | 123 |
| Filter Fabric | Sq. Yd. | 123 |
| Removal of Existing Structures | Each | 1 |
| Concrete Structures | Cu. Yd. | 22.8 |
| Concrete Superstructure | Cu. Yd. | 112.0 |
| Bridge Deck Grooving | Sq. Yd. | 324 |
| Concrete Box Culverts | Cu. Yd. | 110.5 |
| Reinforcement Bars | Pound | 15,910 |
| Reinforcement Bars, Epoxy Coated | Pound | 40,510 |
| Temporary Sheet Piling | Sq. Ft. | 332.4 |
| Temporary Soil Retention System | Sq. Ft. | 156.9 |
| Bar Splicers | Each | 350 |
| Name Plates | Each | 1 |
| Steel Railing, Type 2399 | Foot | 54 |

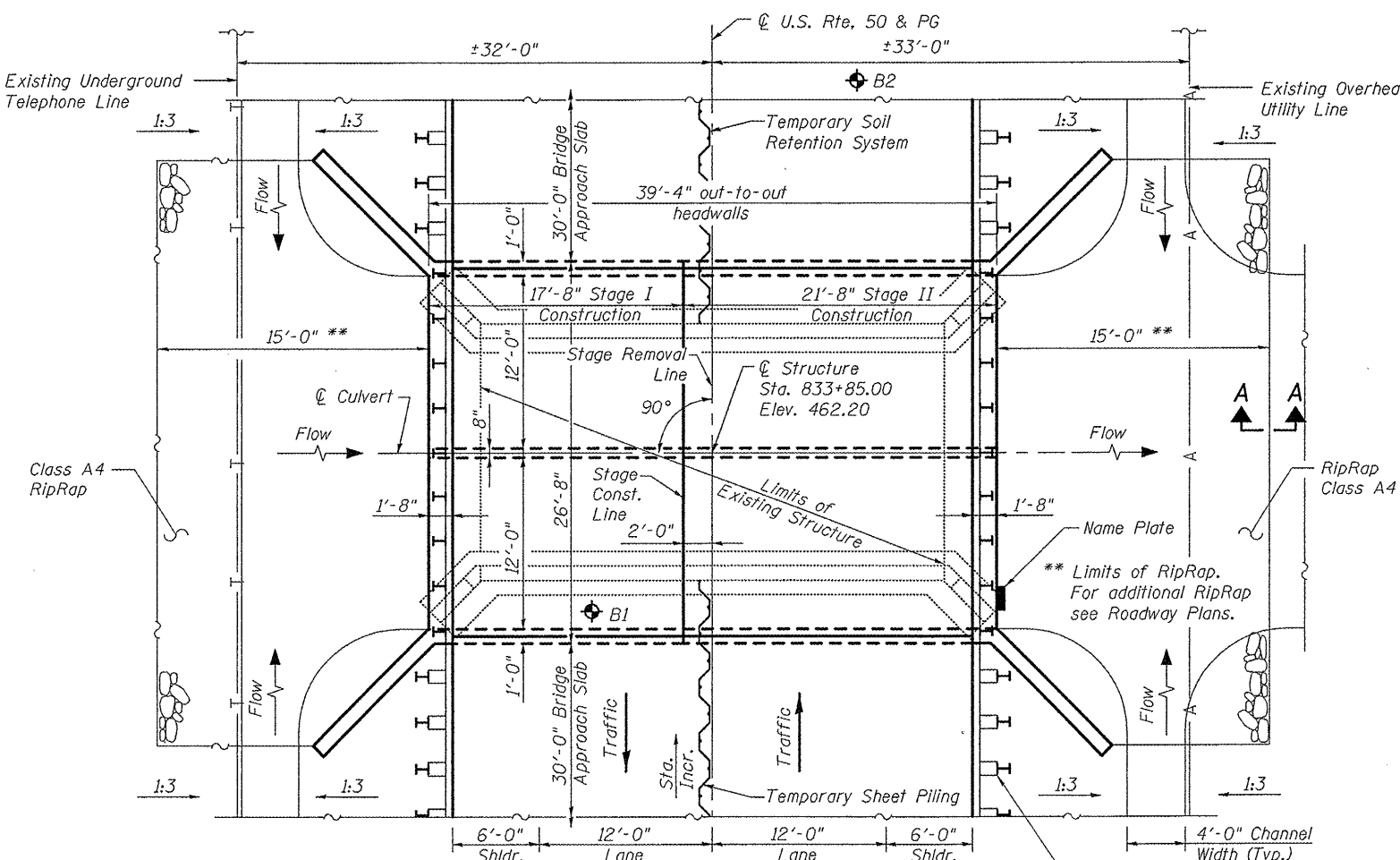


SECTION B-B

PHOEBE NESTING SITE DETAILS (Downstream End Only)



SECTION A-A



PLAN VIEW

DESIGN SCOUR ELEVATION TABLE

| Design Scour Elevation (ft.) | U.S. Invert | D.S. Invert |
|------------------------------|-------------|-------------|
| | 452.07 | 451.97 |

WATERWAY INFORMATION

| Flood | Freq. Yr. | Q C.F.S. | Opening Sq. Ft.* | | Head - Ft. | | Headwater El. | |
|-----------------|-----------|----------|------------------|-------|------------|-------|---------------|--------|
| | | | Exist. | Prop. | Exist. | Prop. | Exist. | Prop. |
| Ex. Overtopping | 9 | 363 | 81.6 | 134.9 | 460.59 | 0.78 | - | 461.37 |
| | 10 | 381 | 81.6 | 136.3 | 460.65 | 0.85 | 0.00 | 461.50 |
| Design | 50 | 612 | 81.6 | 142.6 | 461.27 | 2.17 | 0.00 | 463.44 |
| Pr. Overtopping | 80 | 676 | 81.6 | 142.6 | 461.29 | - | 0.08 | 461.37 |
| Base | 100 | 715 | 81.6 | 142.6 | 461.49 | 2.21 | 0.01 | 463.70 |

* Waterway opening is calculated at the downstream face of the culvert.

DESIGN SPECIFICATIONS

2002 AASHTO Standard Specifications for Highway Bridges

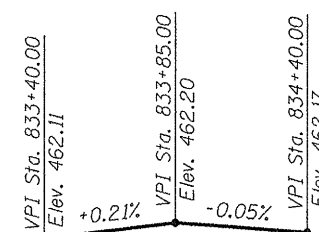
LOADING HS 20-44

Allow 50#/sq. ft. for future wearing surface.

DESIGN STRESSES

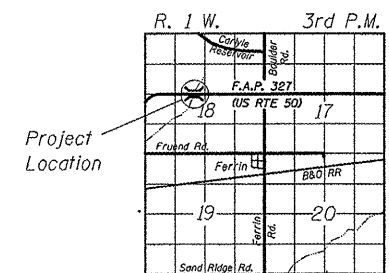
FIELD UNITS

f'c = 3,500 psi
fy = 60,000 psi (Reinforcement)



PROFILE GRADE

GENERAL PLAN & ELEVATION
U.S. ROUTE 50 OVER UNNAMED STREAM
F.A.P. RTE. 327 - SEC. 20BR
CLINTON COUNTY
STATION 833+85.00
STRUCTURE NUMBER 014-2024



LOCATION SKETCH

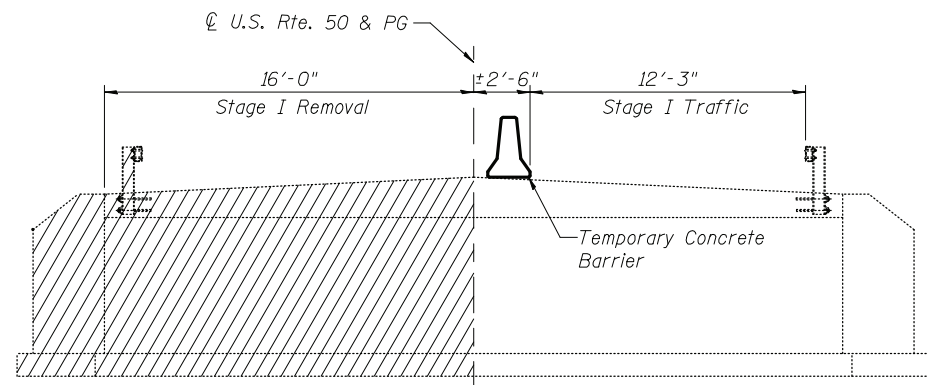
STATION 833+85.00
 BUILT 20... BY
 STATE OF ILLINOIS
 F.A.P. RT. 327, SEC. 20BR
 LOADING HS 20-44
 STR. NO 014-2024

NAME PLATE
(See Std. 515001)

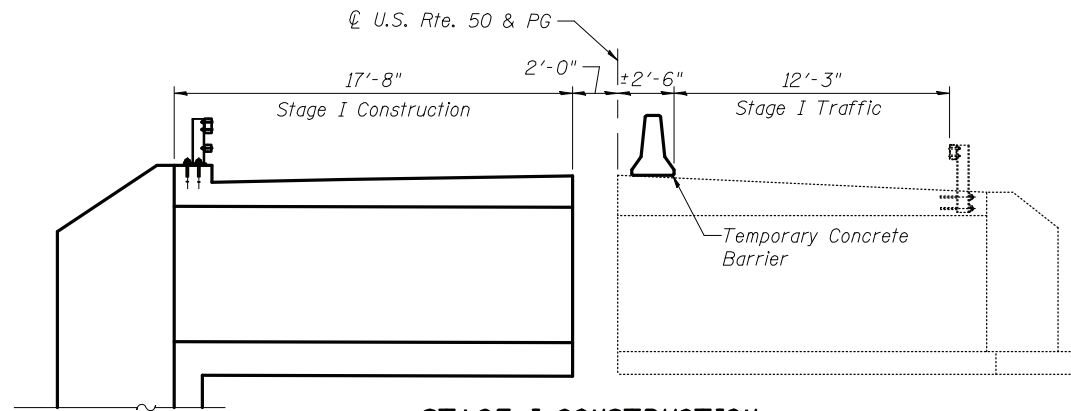


APPROVED
 For Structural Adequacy Only

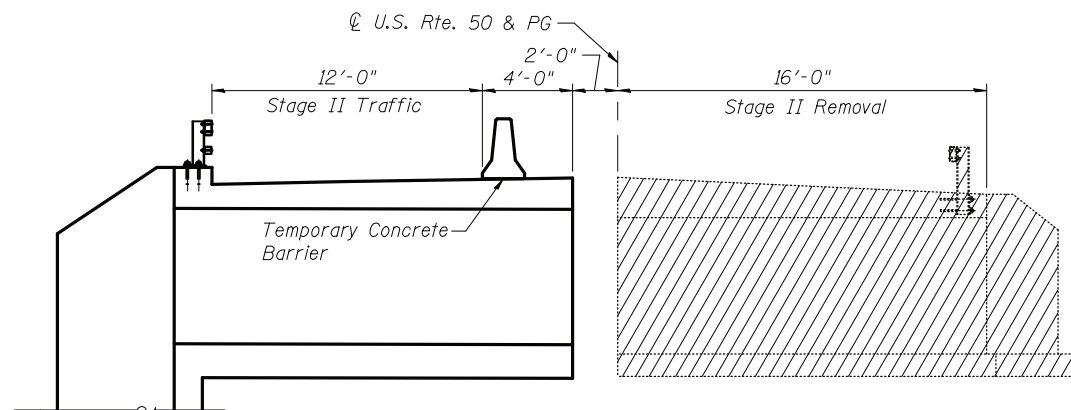
Chadwick J. Fuesting
 Engineer of Bridges & Structures



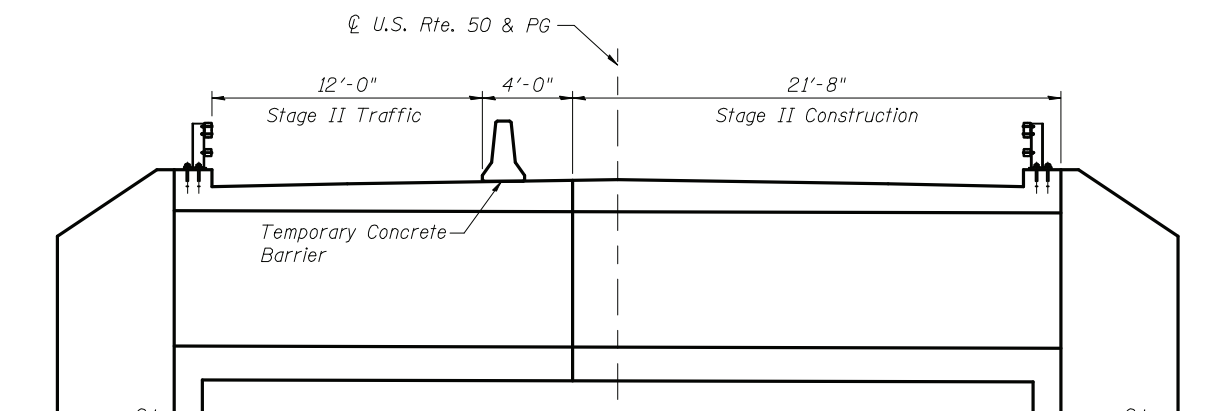
STAGE I REMOVAL



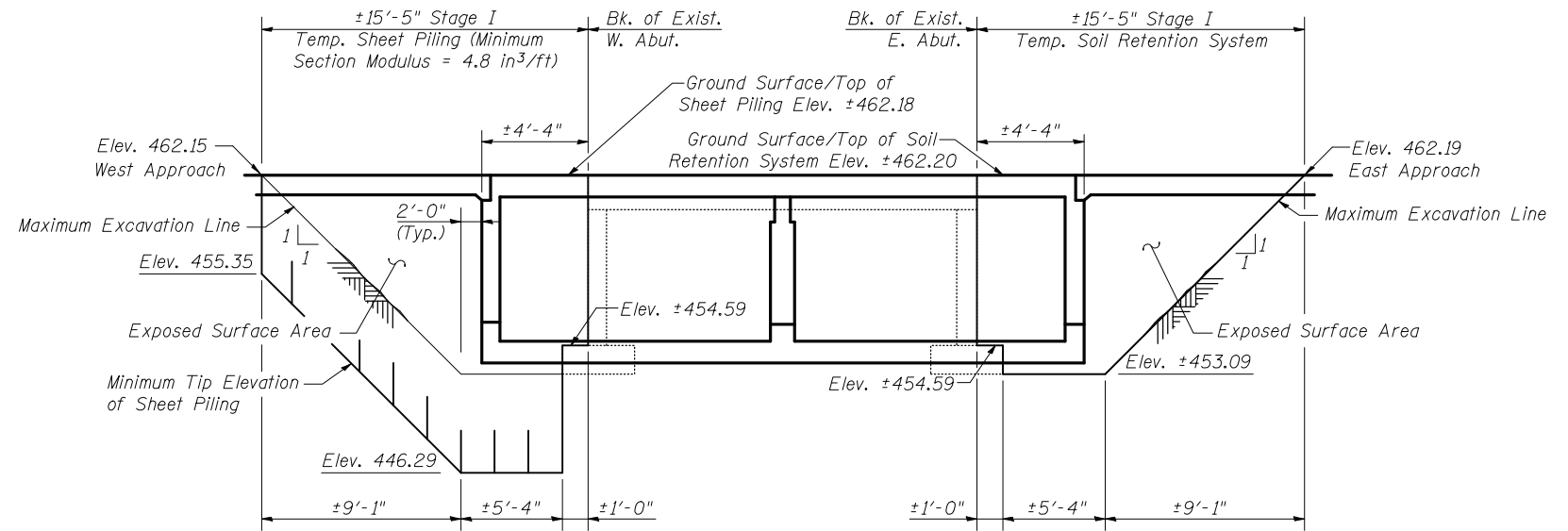
STAGE I CONSTRUCTION



STAGE II REMOVAL



STAGE II CONSTRUCTION

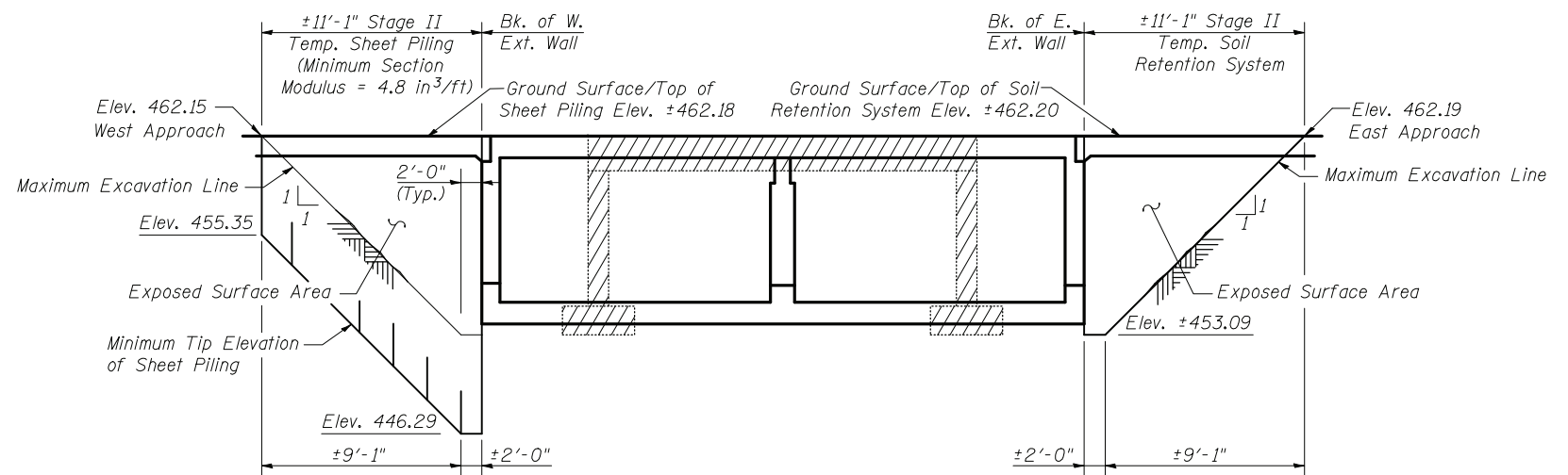


STAGE I TEMPORARY SOIL RETENTION DETAILS

(Looking Upstream)

STAGE I SOIL RETENTION

| | | |
|---------------------------------|---------|-------|
| Temporary Sheet Piling | Sq. Ft. | 198.4 |
| Temporary Soil Retention System | Sq. Ft. | 97.4 |



STAGE II TEMPORARY SOIL RETENTION DETAILS

(Looking Upstream)

STAGE II SOIL RETENTION

| | | |
|---------------------------------|---------|-------|
| Temporary Sheet Piling | Sq. Ft. | 134.0 |
| Temporary Soil Retention System | Sq. Ft. | 59.5 |

Notes:

Stage removal & construction sections shown are looking East.
 Hatched areas indicate Removal of Existing Structure.
 A cantilevered sheet piling design does not appear feasible and additional members or other retention systems may be necessary. The Contractor shall submit a Temporary Soil Retention System design including plan details and calculations for review and acceptance by the Engineer.
 If the Contractor chooses to alter the temporary cantilevered sheet piling design requirements shown on the plans, a design submittal including plan details and calculations will be required for review and acceptance by the Engineer.
 For quantity of Temporary Concrete Barrier, see Roadway Plans.

| | | | |
|---|---------------|--|-----------|
| FILE NAME = \014202476B43-002-Stage Construction | DRAWN - JG | DESIGNED - ACS | REVISIONS |
| BERNHARDT LOGGEMULLER & ASSOCIATES, INC. 3 OAK DRIVE MARTINEVILLE, ILLINOIS 62458 PHONE (618) 262-4000 FAX (618) 262-4000 | CHECKED - BB | ILLINOIS Design Firm Number 184,001670 | REVISIONS |
| | DRAWN - JG | PLOT SCALE = | REVISIONS |
| | CHECKED - CJF | PLOT DATE = 8:51:31 AM 12/14/2011 | REVISIONS |

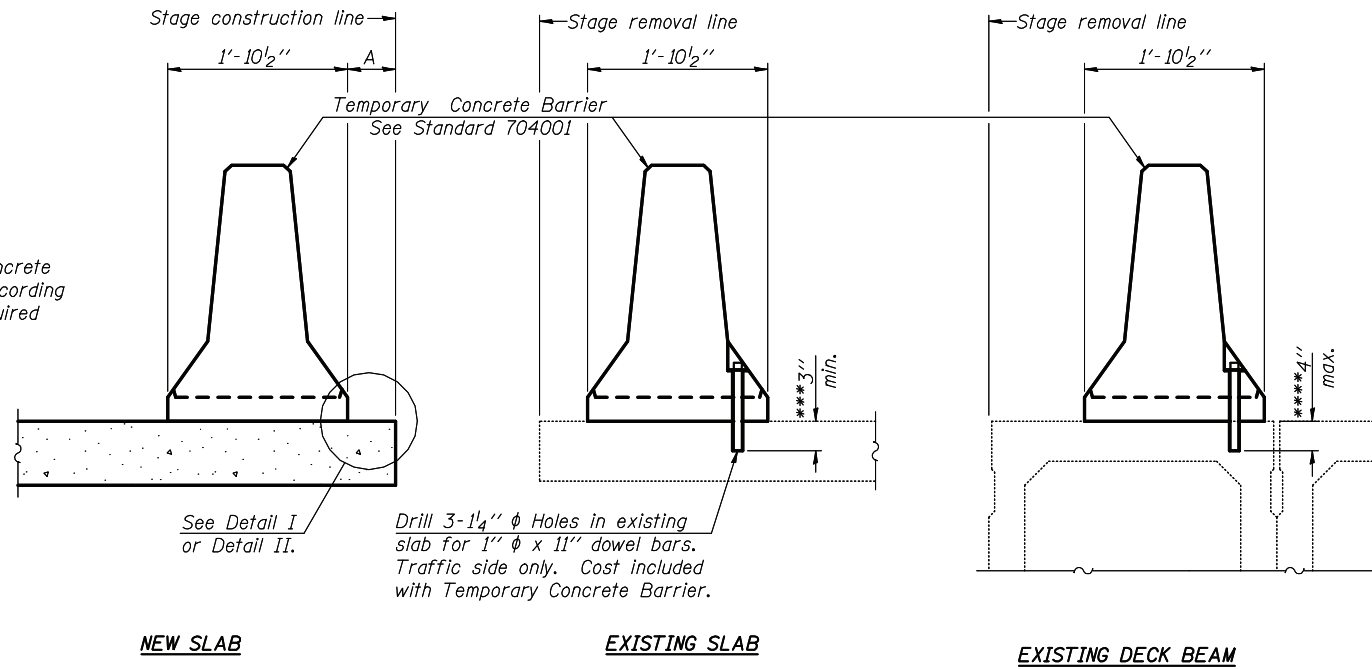
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**STAGE CONSTRUCTION DETAILS
STRUCTURE NO. 014-2024**

SHEET NO. 2 OF 15 SHEETS

| | | | | |
|---|---------|---------|--------------|-----------|
| F.A.P. R.T.E. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 327 | 20BR | CLINTON | 51 | 28 |
| CONTRACT NO. 76B43 | | | | |
| FED. ROAD DIST. NO. - [ILLINOIS] FED. AID PROJECT | | | | |

When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".



SECTIONS THRU SLAB OR DECK BEAM

NOTES

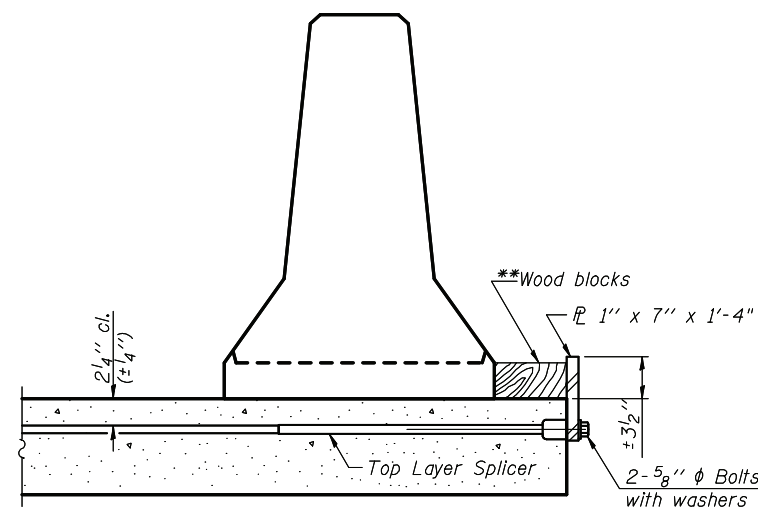
Detail I - With Bar Splicer or Couplers:
Connect one (1) 1" x 7" x 1'-4" steel \bar{P} to the top layer of couplers with 2-5/8" ϕ bolts screwed to coupler at approximate \bar{C} of each barrier panel.

Detail II - With Extended Reinforcement Bars:
Connect one (1) 1" x 7" x 1'-4" steel \bar{P} to the concrete slab or concrete wearing surface with 2-5/8" ϕ Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate \bar{C} of each barrier panel.

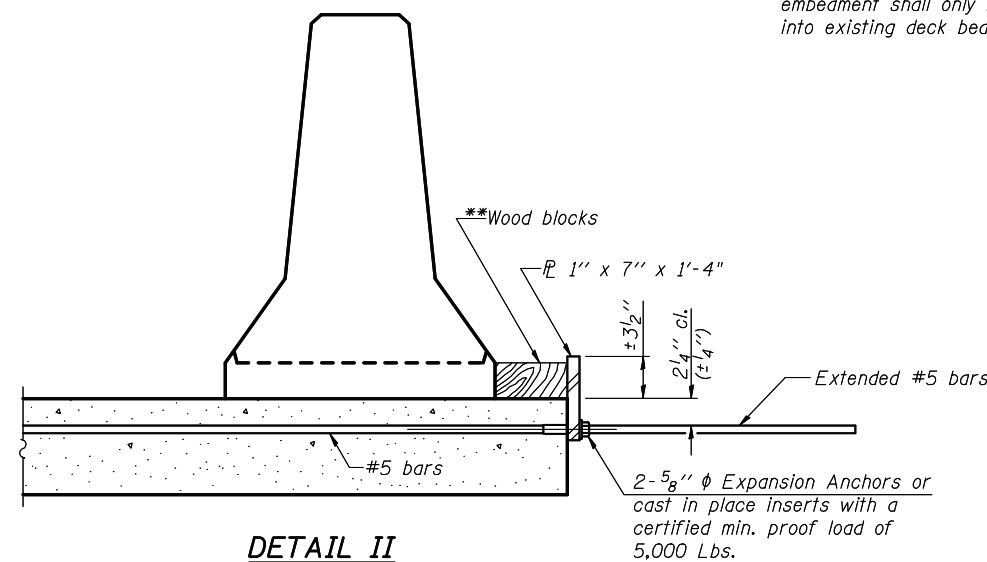
Cost of anchorage is included with Temporary Concrete Barrier. The 1" x 7" x 1'-4" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

*** Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

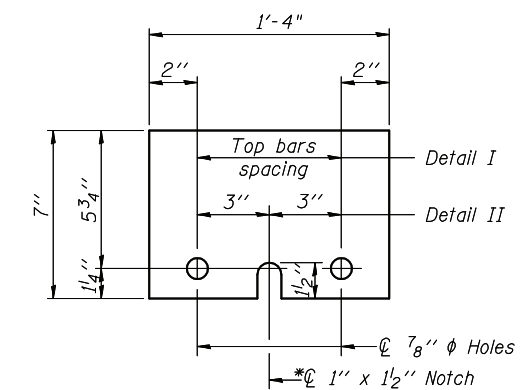
**** If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



DETAIL I



DETAIL II



STEEL RETAINER \bar{P} 1" x 7" x 1'-4"

* Required only with Detail II

** Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

R-27 7-1-10

| | | | | | | | | | |
|---|---------------|-----------|---|---|---|---------|---------|--------------|-----------|
| FILE NAME = _014202476B43-003-Temp Conc | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION STRUCTURE NO. 014-2024 | F.A.P. R.T.E. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| DESIGNED BY = | CHECKED - | REVISED - | | | 327 | 20BR | CLINTON | 51 | 29 |
| Illinois Design Firm Number 184.001670 | DRAWN - JG | REVISED - | | | CONTRACT NO. 76B43 | | | | |
| PLOT SCALE = | CHECKED - CJF | REVISED - | | | FED. ROAD DIST. NO. - [ILLINOIS] FED. AID PROJECT | | | | |
| PLOT DATE = 8:52:12 AM 12/14/2011 | | | SHEET NO. 3 OF 15 SHEETS | | | | | | |

NORTH END OF HEADWALL

| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|-----------|--------|------------------------------|
| ℄ of West Exterior Wall | 833+72.17 | -19.67 | 462.63 |
| ℄ of Culvert | 833+85.00 | -19.67 | 462.65 |
| ℄ of East Exterior Wall | 833+97.83 | -19.67 | 462.64 |

NORTH BEGIN OF HEADWALL

| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|-----------|--------|------------------------------|
| ℄ of West Exterior Wall | 833+72.17 | -18.00 | 462.63 |
| ℄ of Culvert | 833+85.00 | -18.00 | 462.65 |
| ℄ of East Exterior Wall | 833+97.83 | -18.00 | 462.64 |

NORTH EDGE OF SHOULDER

| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|-----------|--------|------------------------------|
| ℄ of West Exterior Wall | 833+72.17 | -18.00 | 461.88 |
| ℄ of Culvert | 833+85.00 | -18.00 | 461.90 |
| ℄ of East Exterior Wall | 833+97.83 | -18.00 | 461.89 |

NORTH EDGE OF PAVEMENT

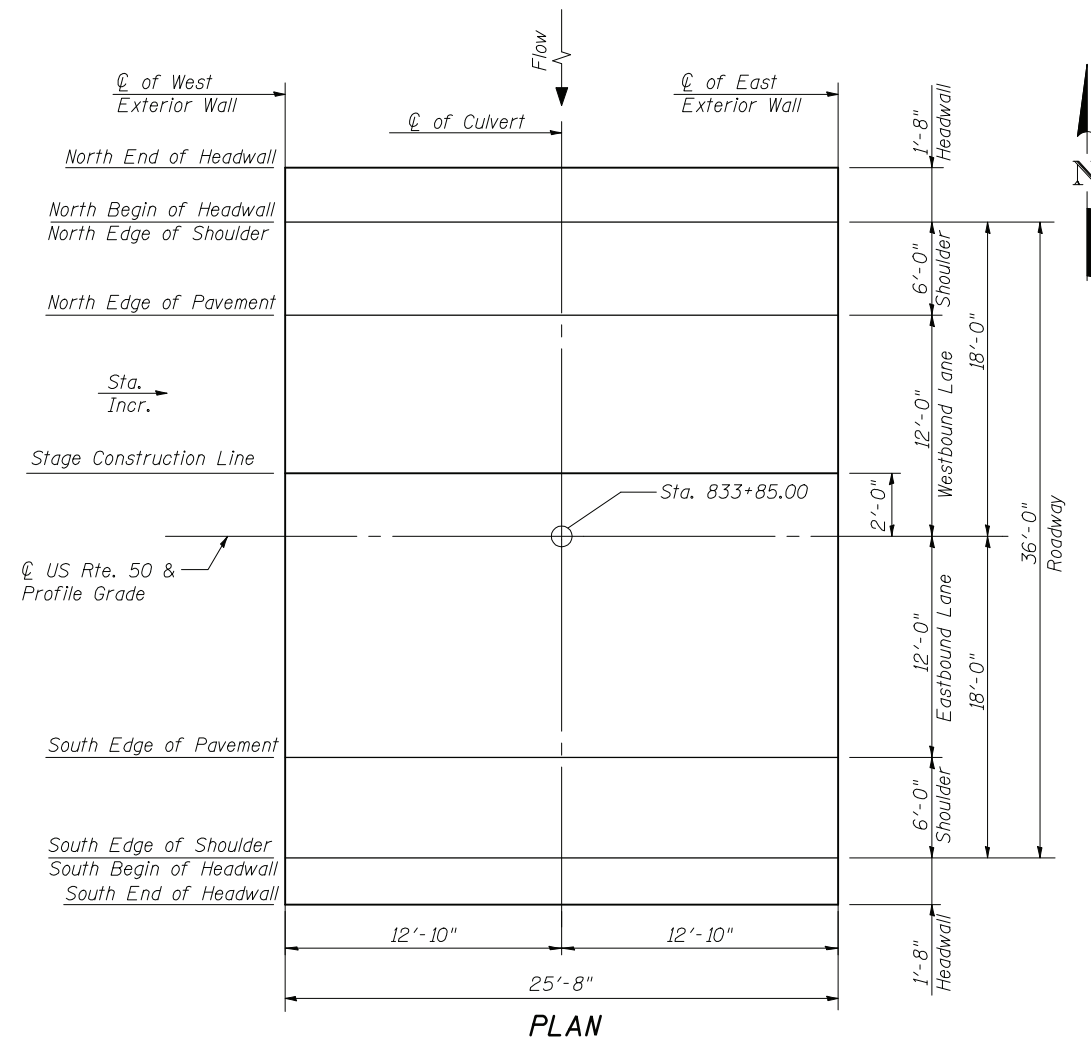
| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|-----------|--------|------------------------------|
| ℄ of West Exterior Wall | 833+72.17 | -12.00 | 462.00 |
| ℄ of Culvert | 833+85.00 | -12.00 | 462.02 |
| ℄ of East Exterior Wall | 833+97.83 | -12.00 | 462.01 |

STAGE CONSTRUCTION LINE

| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|-----------|--------|------------------------------|
| ℄ of West Exterior Wall | 833+72.17 | -2.00 | 462.15 |
| ℄ of Culvert | 833+85.00 | -2.00 | 462.17 |
| ℄ of East Exterior Wall | 833+97.83 | -2.00 | 462.16 |

℄ US RTE. 50 & PROFILE GRADE

| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|-----------|--------|------------------------------|
| ℄ of West Exterior Wall | 833+72.17 | 0.00 | 462.18 |
| ℄ of Culvert | 833+85.00 | 0.00 | 462.20 |
| ℄ of East Exterior Wall | 833+97.83 | 0.00 | 462.19 |



SOUTH EDGE OF PAVEMENT

| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|-----------|--------|------------------------------|
| ℄ of West Exterior Wall | 833+72.17 | 12.00 | 462.00 |
| ℄ of Culvert | 833+85.00 | 12.00 | 462.02 |
| ℄ of East Exterior Wall | 833+97.83 | 12.00 | 462.01 |

SOUTH EDGE OF SHOULDER

| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|-----------|--------|------------------------------|
| ℄ of West Exterior Wall | 833+72.17 | 18.00 | 461.88 |
| ℄ of Culvert | 833+85.00 | 18.00 | 461.90 |
| ℄ of East Exterior Wall | 833+97.83 | 18.00 | 461.89 |

SOUTH BEGIN OF HEADWALL

| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|-----------|--------|------------------------------|
| ℄ of West Exterior Wall | 833+72.17 | 18.00 | 462.63 |
| ℄ of Culvert | 833+85.00 | 18.00 | 462.65 |
| ℄ of East Exterior Wall | 833+97.83 | 18.00 | 462.64 |

SOUTH END OF HEADWALL

| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|-----------|--------|------------------------------|
| ℄ of West Exterior Wall | 833+72.17 | 19.67 | 462.63 |
| ℄ of Culvert | 833+85.00 | 19.67 | 462.65 |
| ℄ of East Exterior Wall | 833+97.83 | 19.67 | 462.64 |

| | | |
|--|----------------|-----------|
| FILE NAME = \014202476B43-004-Culvert Slab | DESIGNED - ACS | REVISED - |
| DESIGNER NAME = Illinois Design Firm Number 184,001670 | CHECKED - BB | REVISED - |
| PLOT SCALE = | DRAWN - JG | REVISED - |
| PLOT DATE = 8:52:59 AM 12/14/2011 | CHECKED - CJF | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CULVERT TOP SLAB ELEVATIONS
STRUCTURE NO. 014-2024**

SHEET NO. 4 OF 15 SHEET

| | | | | |
|---|---------|---------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 327 | 20BR | CLINTON | 51 | 30 |
| CONTRACT NO. 76B43 | | | | |
| FED. ROAD DIST. NO. - [ILLINOIS] FED. AID PROJECT | | | | |

NORTH EDGE OF SHOULDER

| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|-----------|--------|------------------------------|
| End of West Approach | 833+42.17 | -18.00 | 461.81 |
| A | 833+52.17 | -18.00 | 461.84 |
| B | 833+62.17 | -18.00 | 461.86 |
| ☉ of West Exterior Wall | 833+72.17 | -18.00 | 461.88 |

NORTH EDGE OF PAVEMENT

| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|-----------|--------|------------------------------|
| End of West Approach | 833+42.17 | -12.00 | 461.93 |
| A | 833+52.17 | -12.00 | 461.96 |
| B | 833+62.17 | -12.00 | 461.98 |
| ☉ of West Exterior Wall | 833+72.17 | -12.00 | 462.00 |

STAGE CONSTRUCTION LINE

| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|-----------|--------|------------------------------|
| End of West Approach | 833+42.17 | -2.00 | 462.08 |
| A | 833+52.17 | -2.00 | 462.11 |
| B | 833+62.17 | -2.00 | 462.13 |
| ☉ of West Exterior Wall | 833+72.17 | -2.00 | 462.15 |

☉ US RTE. 50 & PROFILE GRADE

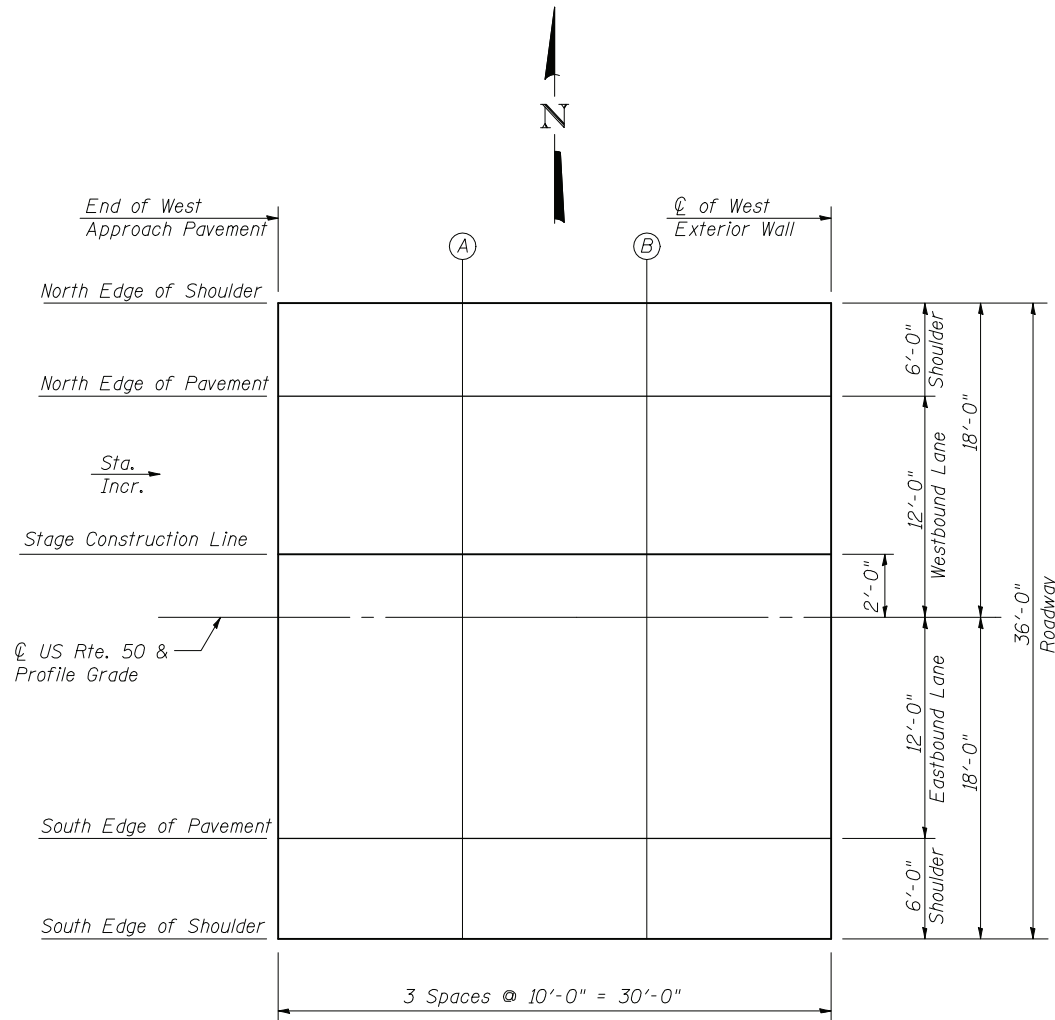
| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|-----------|--------|------------------------------|
| End of West Approach | 833+42.17 | 0.00 | 462.11 |
| A | 833+52.17 | 0.00 | 462.14 |
| B | 833+62.17 | 0.00 | 462.16 |
| ☉ of West Exterior Wall | 833+72.17 | 0.00 | 462.18 |

SOUTH EDGE OF PAVEMENT

| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|-----------|--------|------------------------------|
| End of West Approach | 833+42.17 | 12.00 | 461.93 |
| A | 833+52.17 | 12.00 | 461.96 |
| B | 833+62.17 | 12.00 | 461.98 |
| ☉ of West Exterior Wall | 833+72.17 | 12.00 | 462.00 |

SOUTH EDGE OF SHOULDER

| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|-----------|--------|------------------------------|
| End of West Approach | 833+42.17 | 18.00 | 461.81 |
| A | 833+52.17 | 18.00 | 461.84 |
| B | 833+62.17 | 18.00 | 461.86 |
| ☉ of West Exterior Wall | 833+72.17 | 18.00 | 461.88 |



PLAN

NORTH EDGE OF SHOULDER

| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|-----------|--------|------------------------------|
| ☉ of East Exterior Wall | 833+97.83 | -18.00 | 461.89 |
| A | 834+07.83 | -18.00 | 461.89 |
| B | 834+17.83 | -18.00 | 461.88 |
| End of East Approach | 834+27.83 | -18.00 | 461.88 |

NORTH EDGE OF PAVEMENT

| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|-----------|--------|------------------------------|
| ☉ of East Exterior Wall | 833+97.83 | -12.00 | 462.01 |
| A | 834+07.83 | -12.00 | 462.01 |
| B | 834+17.83 | -12.00 | 462.00 |
| End of East Approach | 834+27.83 | -12.00 | 462.00 |

STAGE CONSTRUCTION LINE

| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|-----------|--------|------------------------------|
| ☉ of East Exterior Wall | 833+97.83 | -2.00 | 462.16 |
| A | 834+07.83 | -2.00 | 462.16 |
| B | 834+17.83 | -2.00 | 462.15 |
| End of East Approach | 834+27.83 | -2.00 | 462.15 |

☉ US RTE. 50 & PROFILE GRADE

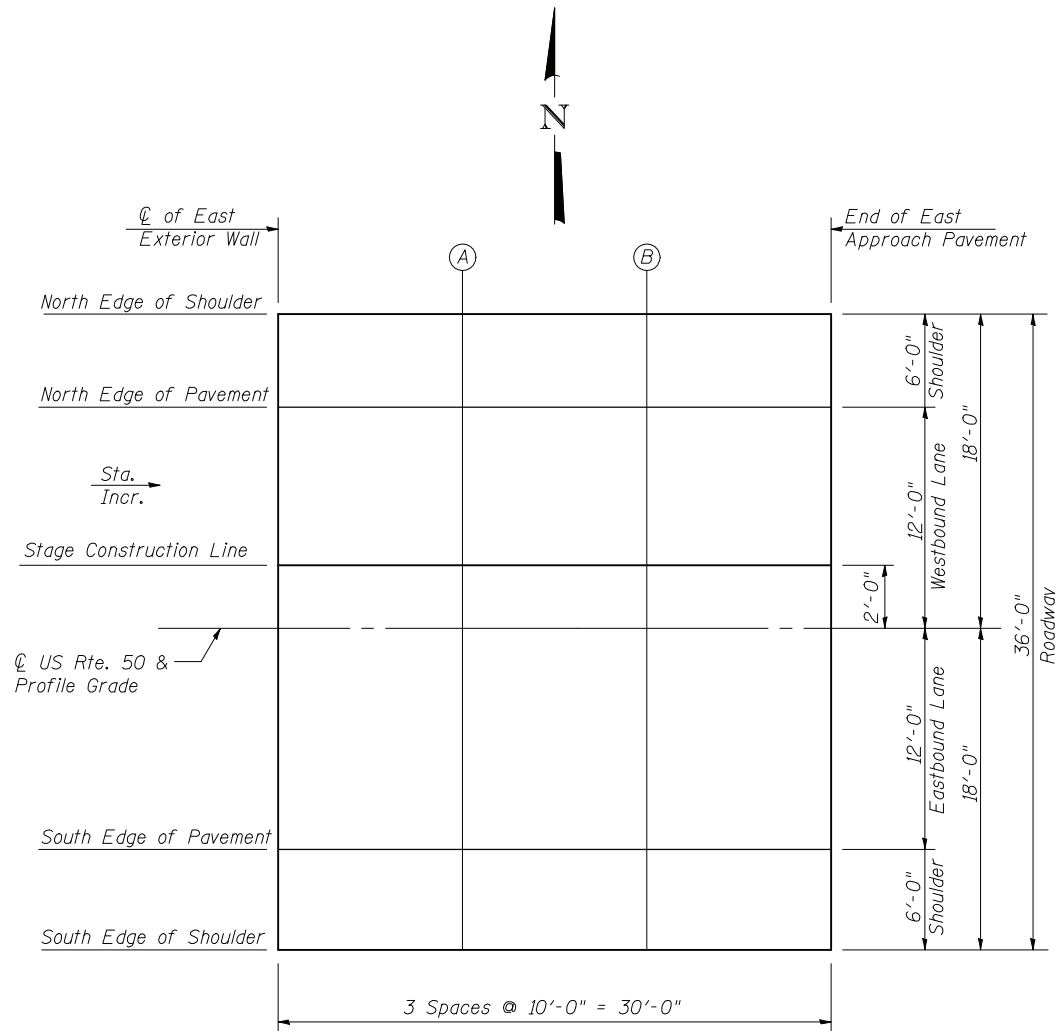
| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|-----------|--------|------------------------------|
| ☉ of East Exterior Wall | 833+97.83 | 0.00 | 462.19 |
| A | 834+07.83 | 0.00 | 462.19 |
| B | 834+17.83 | 0.00 | 462.18 |
| End of East Approach | 834+27.83 | 0.00 | 462.18 |

SOUTH EDGE OF PAVEMENT

| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|-----------|--------|------------------------------|
| ☉ of East Exterior Wall | 833+97.83 | 12.00 | 462.01 |
| A | 834+07.83 | 12.00 | 462.01 |
| B | 834+17.83 | 12.00 | 462.00 |
| End of East Approach | 834+27.83 | 12.00 | 462.00 |

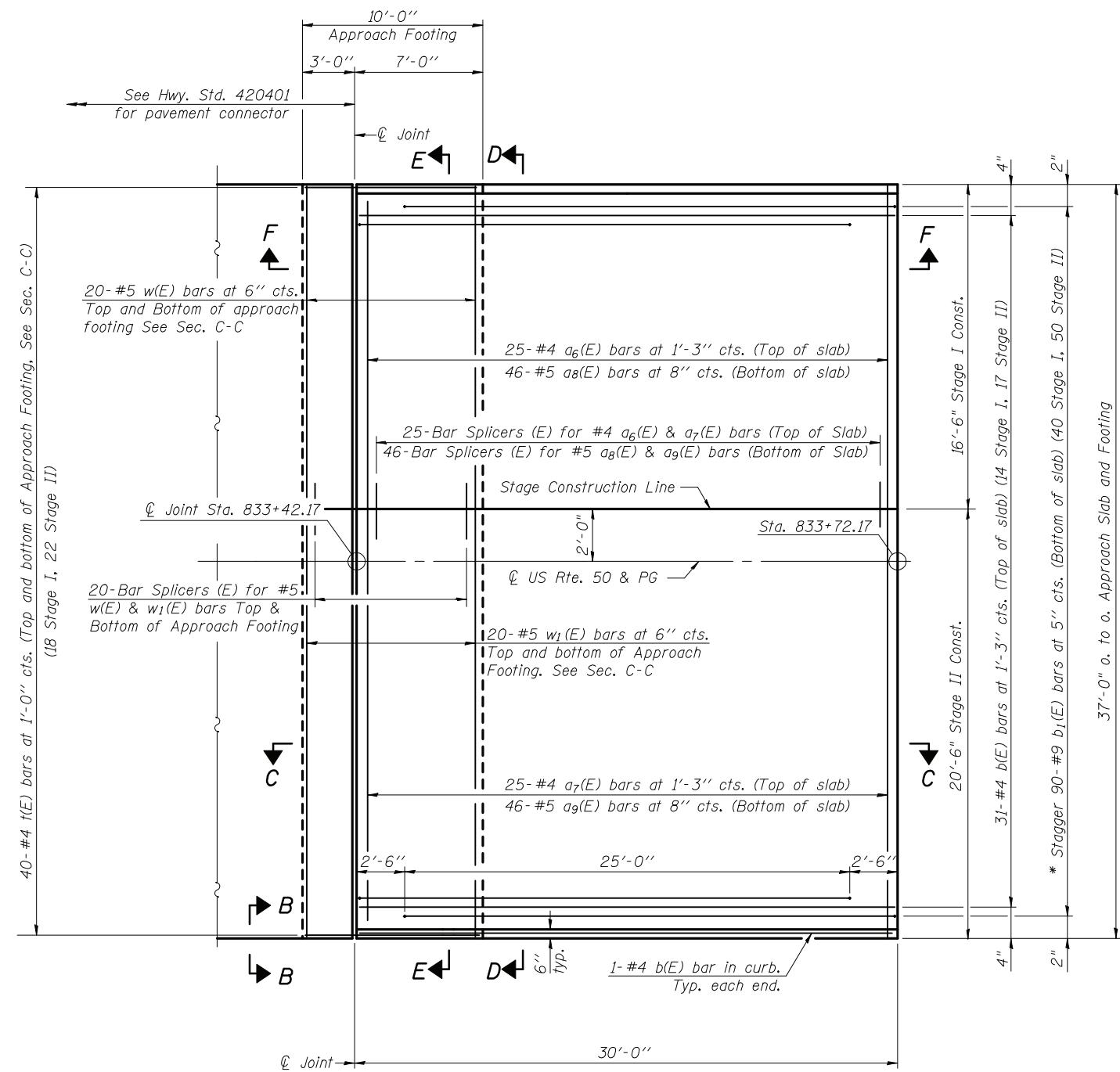
SOUTH EDGE OF SHOULDER

| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|-----------|--------|------------------------------|
| ☉ of East Exterior Wall | 833+97.83 | 18.00 | 461.89 |
| A | 834+07.83 | 18.00 | 461.89 |
| B | 834+17.83 | 18.00 | 461.88 |
| End of East Approach | 834+27.83 | 18.00 | 461.88 |

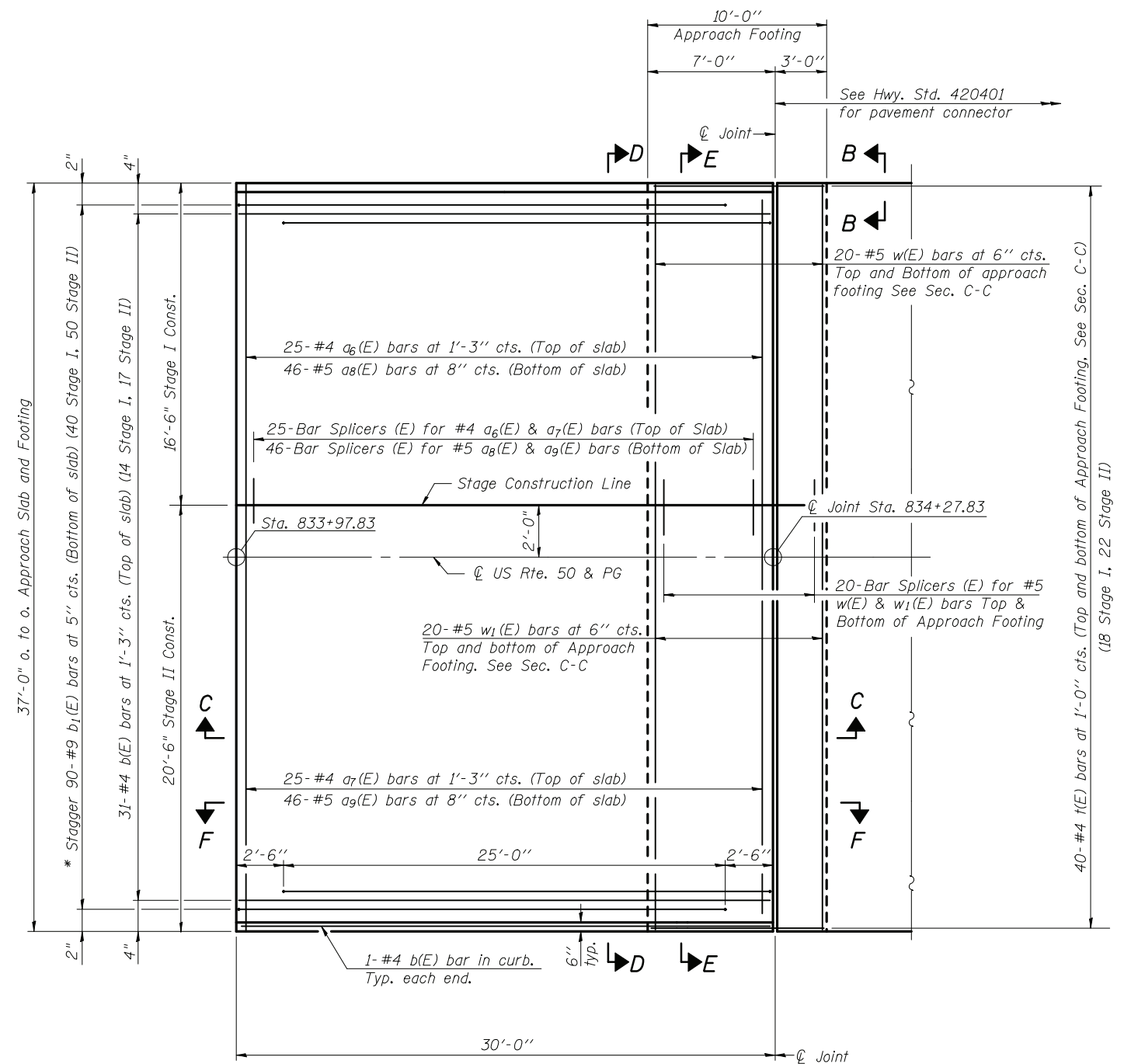


PLAN

Notes:
See sheet 8 of 15 for Sections C-C, D-D & E-E and View B-B and F-F.
a₆(E) thru a₉(E) bar spacings measured along \varnothing Rdwy.



PLAN
(West Approach)



PLAN
(East Approach)

* Tilt #9 b₁(E) bars as required to maintain clearance.

FILE NAME = \014202476B43-007-App Plan.dwg
DESIGNED - ACS
CHECKED - BB
DRAWN - JG
CHECKED - CJF
PLOT SCALE =
PLOT DATE = 8:54:41 AM 12/14/2011

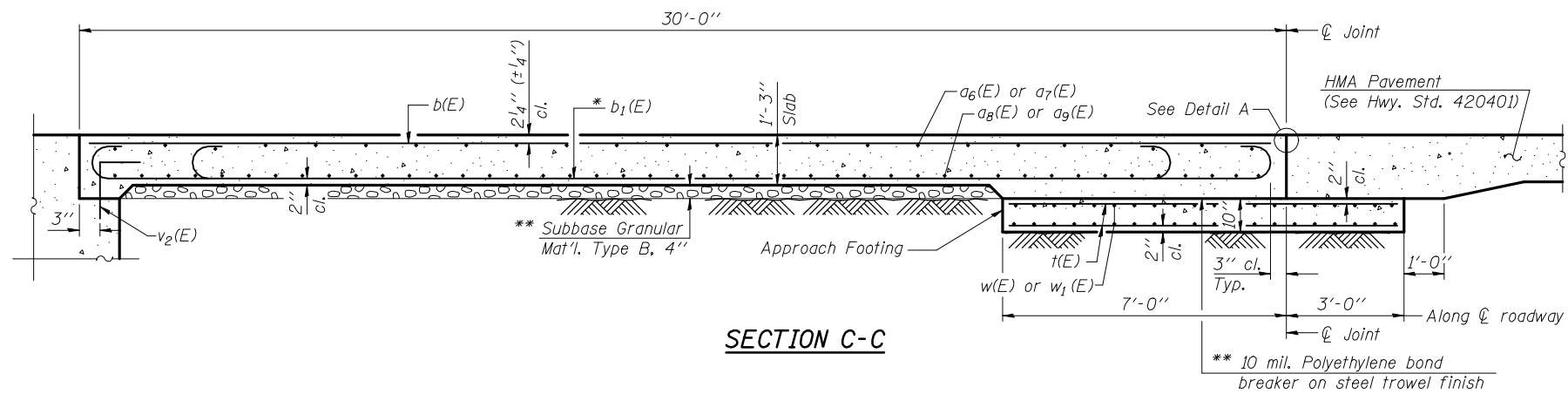
DESIGNED - ACS
CHECKED - BB
DRAWN - JG
CHECKED - CJF
REVISOR -
REVISOR -
REVISOR -
REVISOR -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

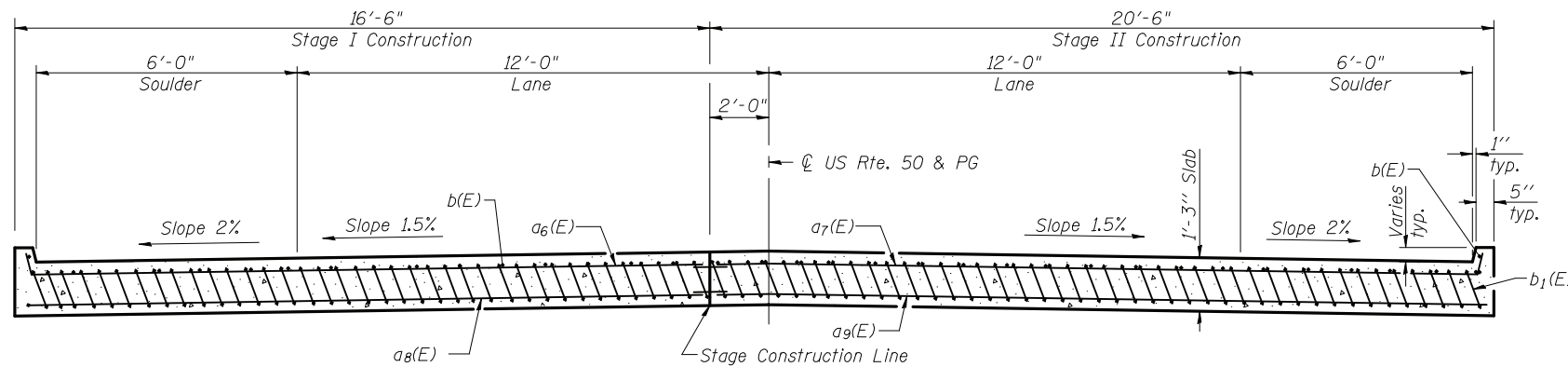
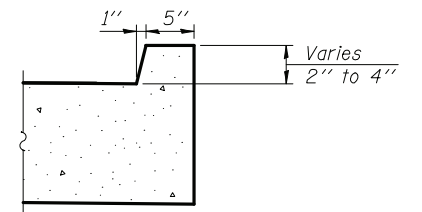
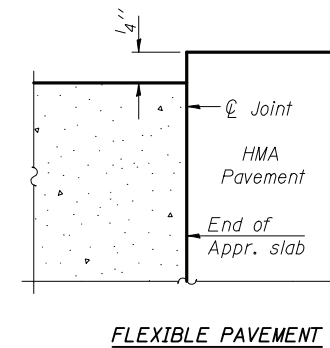
BRIDGE APPROACH SLAB DETAILS
STRUCTURE NO. 014-2024

SHEET NO. 7 OF 15 SHEETS

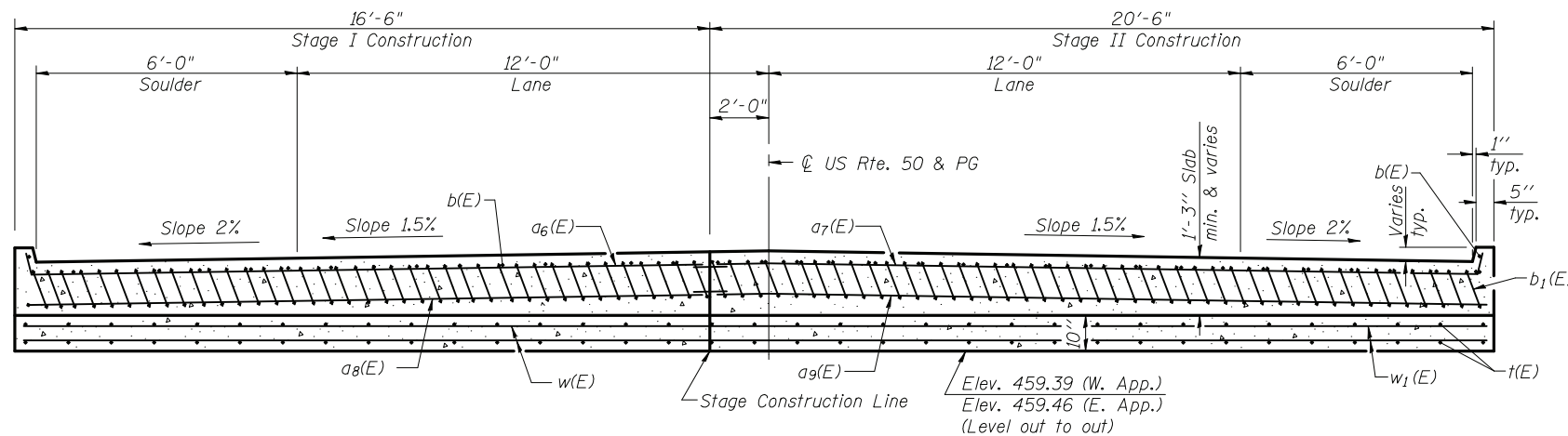
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|---------|--------------|-----------|
| 327 | 20BR | CLINTON | 51 | 33 |
| CONTRACT NO. 76B43 | | | | |
| FED. ROAD DIST. NO. - [ILLINOIS] FED. AID PROJECT | | | | |



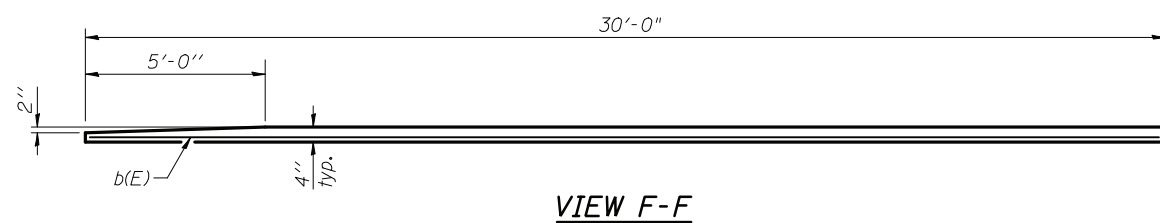
Notes:
 Approach slab concrete shall be paid for as Concrete Superstructure.
 Approach footing concrete shall be paid for as Concrete Structures.
 Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated.
 For $v_2(E)$ bar details, see sheet 11 of 15.
 The approach footing maximum applied service bearing pressure (Q_{max}) = 2.0 ksf.
 For bar splicer details, see sheet 13 of 15.
 Cost of excavation for approach footing included with Concrete Structures.



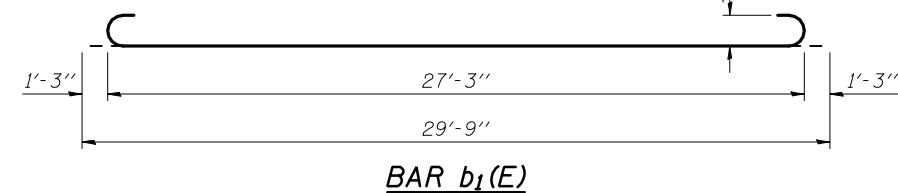
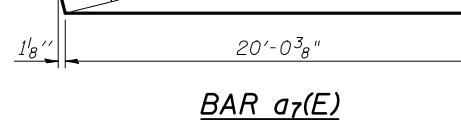
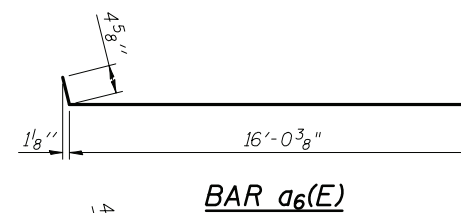
SECTION D-D



SECTION E-E



VIEW F-F

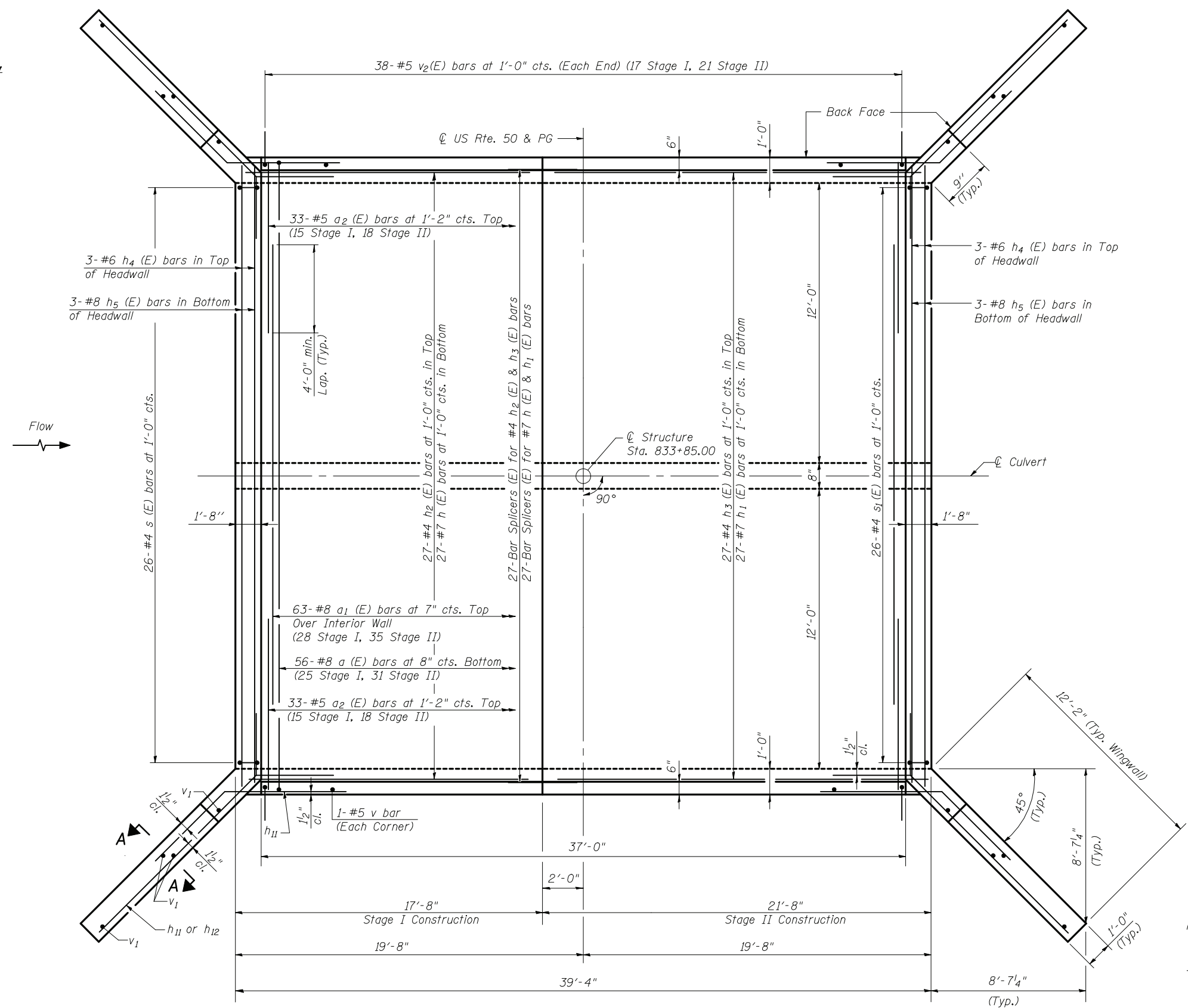
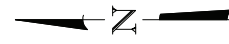


* Tilt #9 $b_1(E)$ bars as required to maintain clearance.

** Cost included with Concrete Superstructure.

**TWO APPROACHES
 BILL OF MATERIAL**

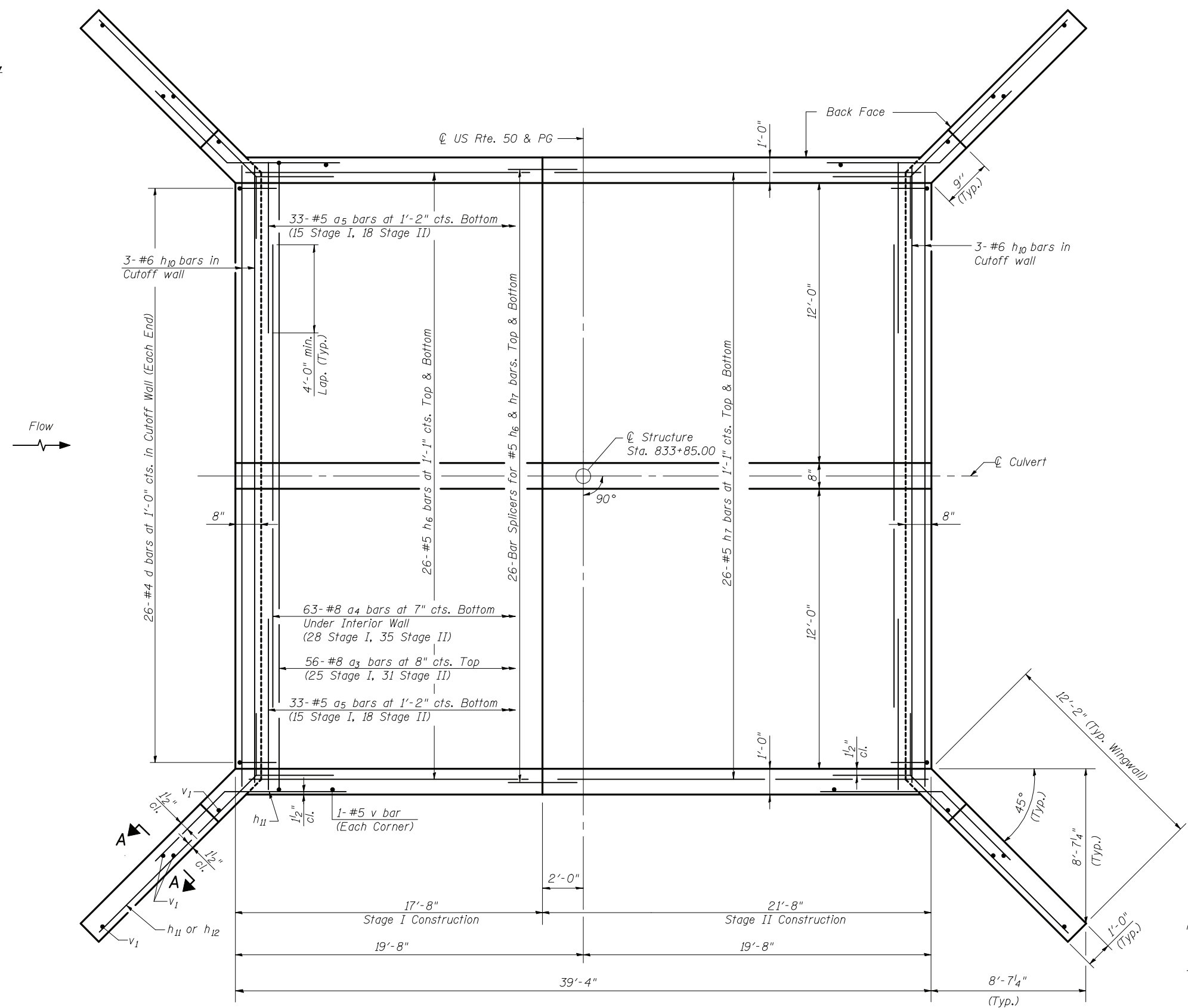
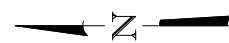
| Bar | No. | Size | Length | Shape | |
|----------------------------------|-----|------|--------|---------|--------|
| $a_6(E)$ | 50 | #4 | 16'-5" | ┌───┐ | |
| $a_7(E)$ | 50 | #4 | 20'-5" | ┌───┐ | |
| $a_8(E)$ | 92 | #5 | 16'-2" | ┌───┐ | |
| $a_9(E)$ | 92 | #5 | 20'-2" | ┌───┐ | |
| $b(E)$ | 66 | #4 | 29'-8" | ┌───┐ | |
| $b_1(E)$ | 180 | #9 | 29'-9" | ┌───┐ | |
| $t(E)$ | 160 | #4 | 9'-8" | ┌───┐ | |
| $w(E)$ | 80 | #5 | 16'-2" | ┌───┐ | |
| $w_1(E)$ | 80 | #5 | 20'-2" | ┌───┐ | |
| Concrete Superstructure | | | | Cu. Yd. | 112.0 |
| Concrete Structures | | | | Cu. Yd. | 22.8 |
| Reinforcement Bars, Epoxy Coated | | | | Pound | 28,300 |
| Bar Splicers | | | | Each | 222 |



TOP SLAB PLAN

Notes:
 For details of bar splicers, see sheet 13 of 15.
 For Section A-A and Bill of Material, see sheet 11 of 15.

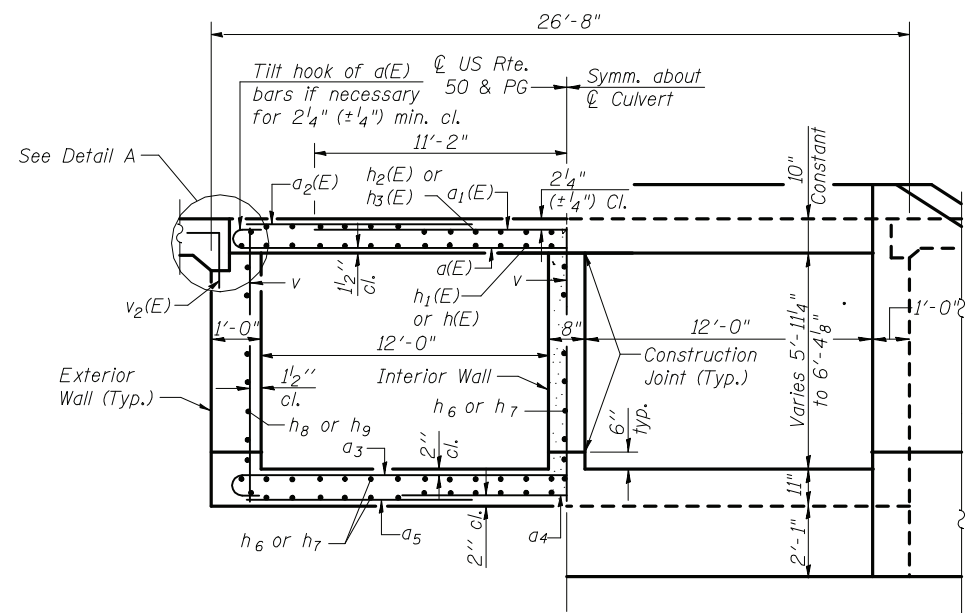
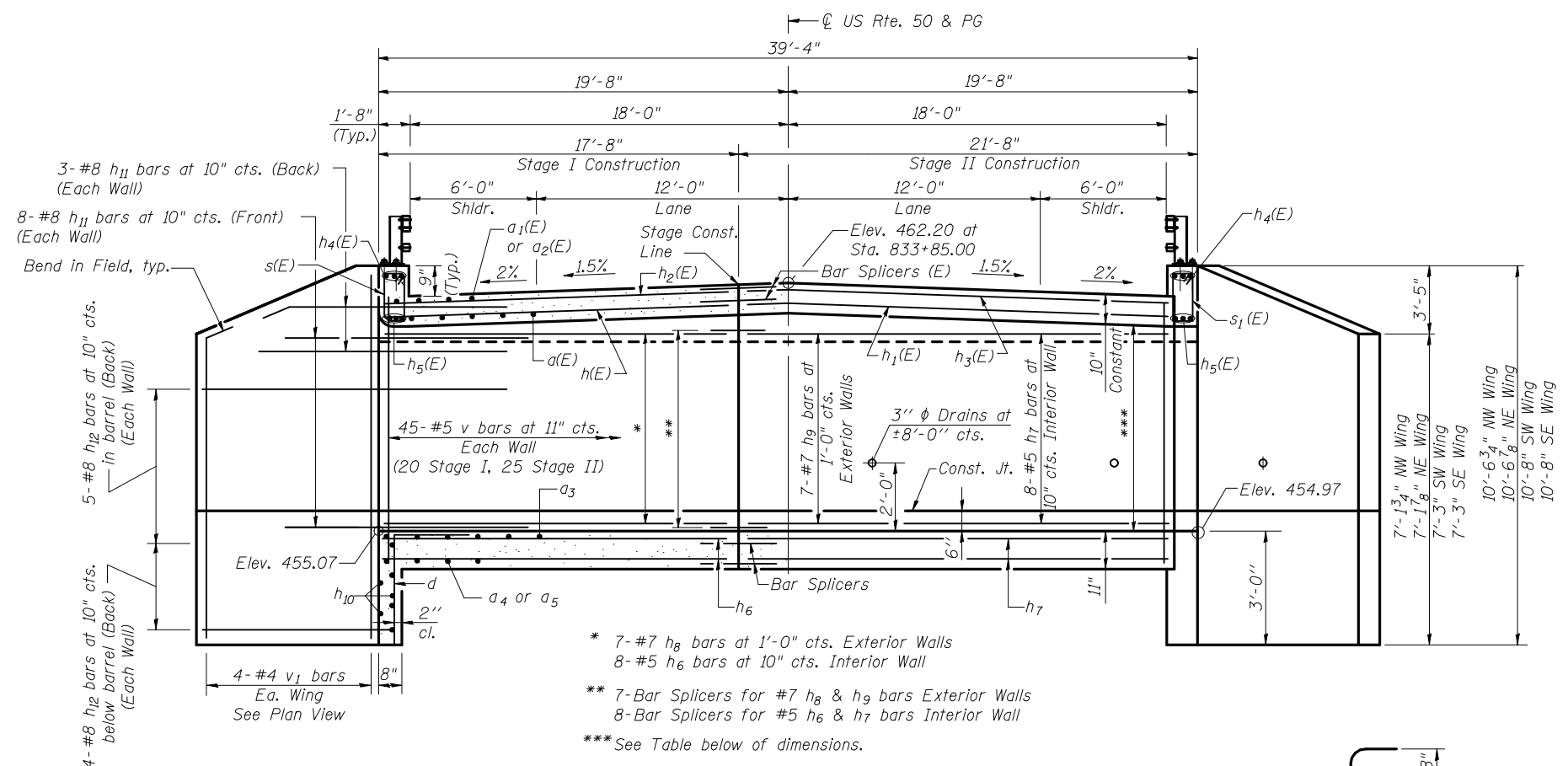
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|--|---|--|---|---|--|
| FILE NAME = _014202476B43-009-Top Slab DESIGNER NAME = Illinois Design Firm Number 184,001670 PLOT SCALE = PLOT DATE = 8:55:52 AM 12/14/2011 | DESIGNED - ACS CHECKED - BB DRAWN - JG CHECKED - CJF | REVISED - REVISED - REVISED - REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | CULVERT DETAILS - TOP SLAB STRUCTURE NO. 014-2024 SHEET NO. 9 OF 15 SHEET | F.A.P. RTE. 327 SECTION 20BR COUNTY CLINTON TOTAL SHEETS 51 SHEET NO. 35 CONTRACT NO. 76B43 |
| | | | | | FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT |



Notes:
 For details of bar splicers, see sheet 13 of 15.
 For Section A-A and Bill of Material, see sheet 11 of 15.

BOTTOM SLAB PLAN

| | | | | | |
|--|---|--|--|--|--|
| FILE NAME = \014202476B43-010-Bottom Slab Plan.dwg DESIGNER NAME = Illinois Design Firm Number 184,001670 PLOT SCALE = PLOT DATE = 8:56:28 AM 12/14/2011 | DESIGNED - ACS CHECKED - BB DRAWN - JG CHECKED - CJF | REVISED - REVISED - REVISED - REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | CULVERT DETAILS - BOTTOM SLAB STRUCTURE NO. 014-2024 | F.A.P. RTE. 327 SECTION 20BR COUNTY CLINTON TOTAL SHEETS 51 SHEET NO. 36 |
| | | | | | SHEET NO. 10 OF 15 SHEET |

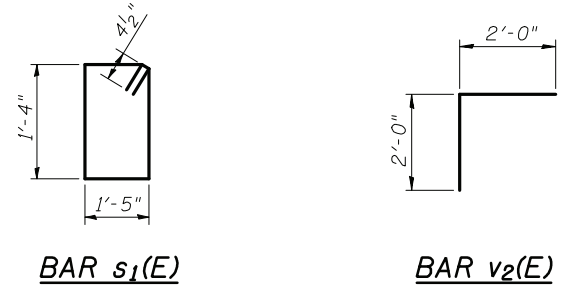
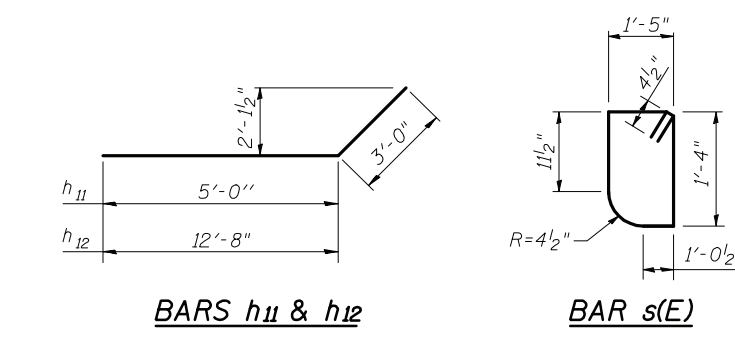
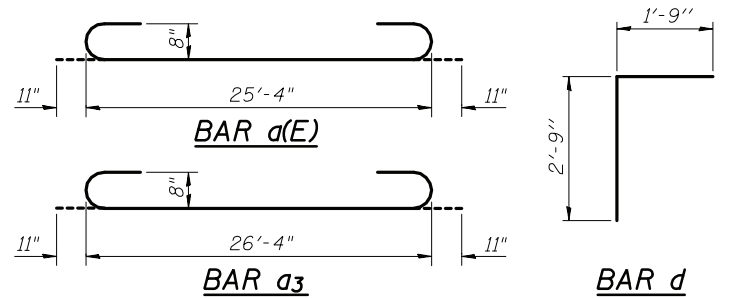


HALF SECTION THRU BARREL

HALF END ELEVATION

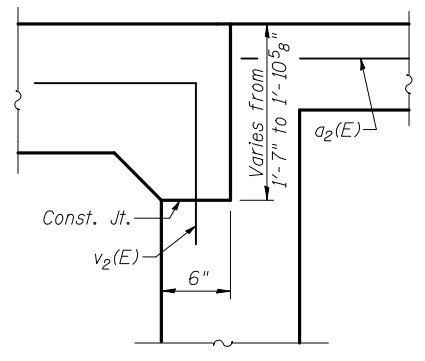
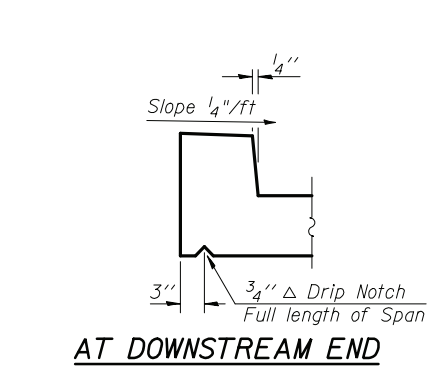
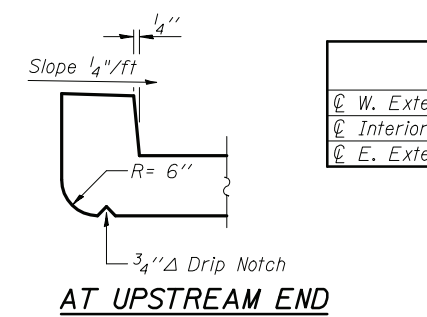
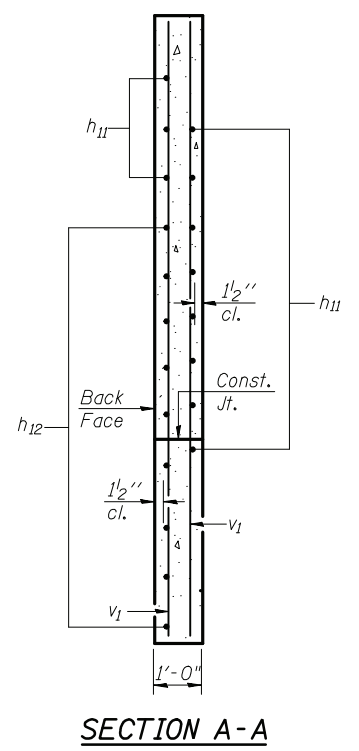
- * 7-#7 h₈ bars at 1'-0" cts. Exterior Walls
- 8-#5 h₆ bars at 10" cts. Interior Wall
- ** 7-Bar Splicers for #7 h₈ & h₉ bars Exterior Walls
- 8-Bar Splicers for #5 h₆ & h₇ bars Interior Wall
- *** See Table below of dimensions.

ELEVATION
(Looking East)



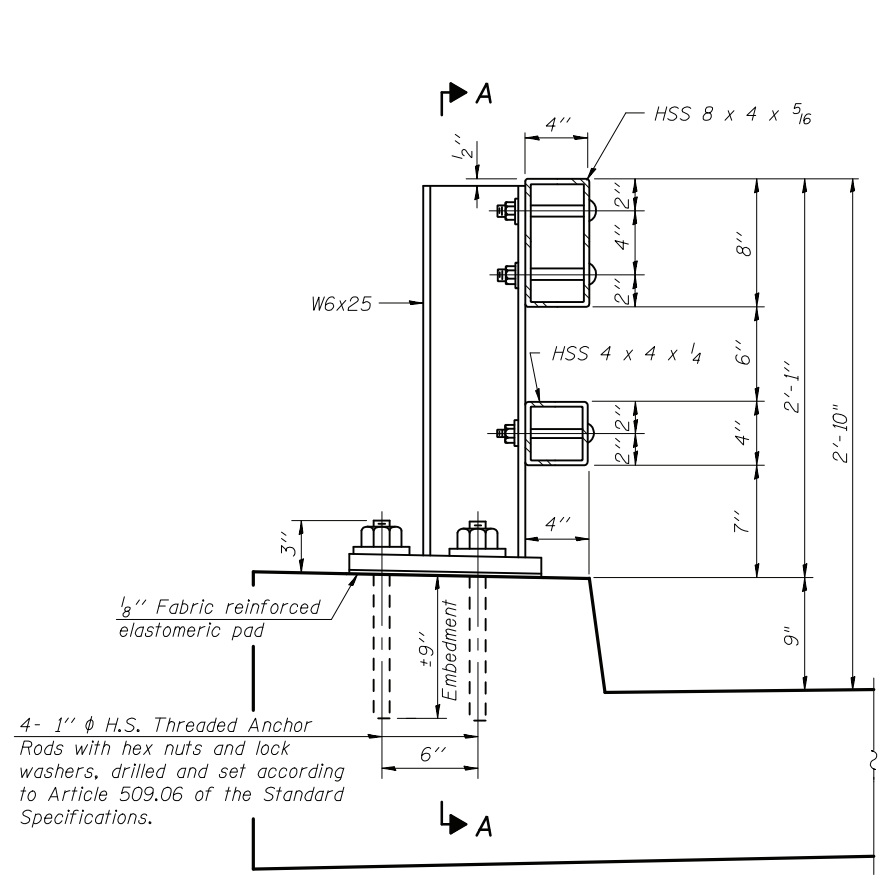
BILL OF MATERIAL

| Bar | No. | Size | Length | Shape |
|----------------------------------|-----|---------|--------|-------|
| a(E) | 56 | #8 | 27'-2" | U |
| a ₁ (E) | 63 | #8 | 22'-4" | — |
| a ₂ (E) | 66 | #5 | 5'-7" | — |
| a ₃ | 56 | #8 | 28'-2" | U |
| a ₄ | 63 | #8 | 22'-4" | — |
| a ₅ | 66 | #5 | 5'-7" | — |
| d | 52 | #4 | 4'-6" | L |
| h(E) | 27 | #7 | 17'-5" | — |
| h ₁ (E) | 27 | #7 | 21'-5" | — |
| h ₂ (E) | 27 | #4 | 17'-5" | — |
| h ₃ (E) | 27 | #4 | 21'-5" | — |
| h ₄ (E) | 6 | #6 | 24'-0" | — |
| h ₅ (E) | 6 | #8 | 25'-5" | — |
| h ₆ | 60 | #5 | 17'-5" | — |
| h ₇ | 60 | #5 | 21'-5" | — |
| h ₈ | 14 | #7 | 17'-5" | — |
| h ₉ | 14 | #7 | 21'-5" | — |
| h ₁₀ | 6 | #6 | 26'-0" | — |
| h ₁₁ | 44 | #8 | 8'-0" | — |
| h ₁₂ | 36 | #8 | 15'-8" | — |
| s(E) | 26 | #4 | 6'-1" | D |
| s ₁ (E) | 26 | #4 | 6'-3" | D |
| v | 139 | #5 | 7'-4" | — |
| v ₁ | 16 | #4 | 10'-3" | — |
| v ₂ (E) | 76 | #5 | 4'-0" | L |
| Concrete Box Culverts | | Cu. Yd. | 110.5 | |
| Reinforcement Bars | | Pound | 15,910 | |
| Reinforcement Bars, Epoxy Coated | | Pound | 12,210 | |
| Bar Splicers | | Each | 128 | |

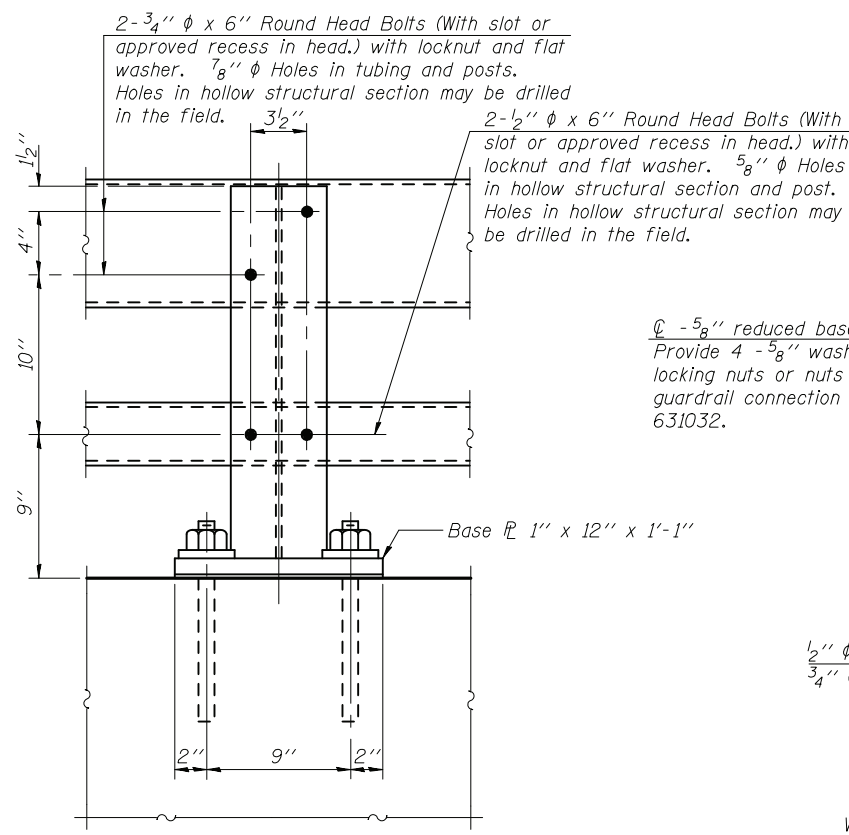


| | Upstream Invert | @ US Rte. 50 & PG | Downstream Invert |
|--------------------|-----------------|-------------------|-------------------|
| @ W. Exterior Wall | 5'-11 1/2" | 6'-4" | 6'-0 5/8" |
| @ Interior Wall | 5'-11 1/2" | 6'-4 1/8" | 6'-0 3/4" |
| @ E. Exterior Wall | 5'-11 1/4" | 6'-3 3/4" | 6'-0 3/8" |

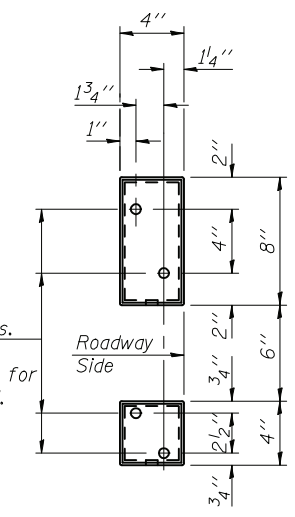
Notes: A distance of half the length of the wingwall but not less than 6'-0" of the barrel shall be poured monolithically with the wingwalls.



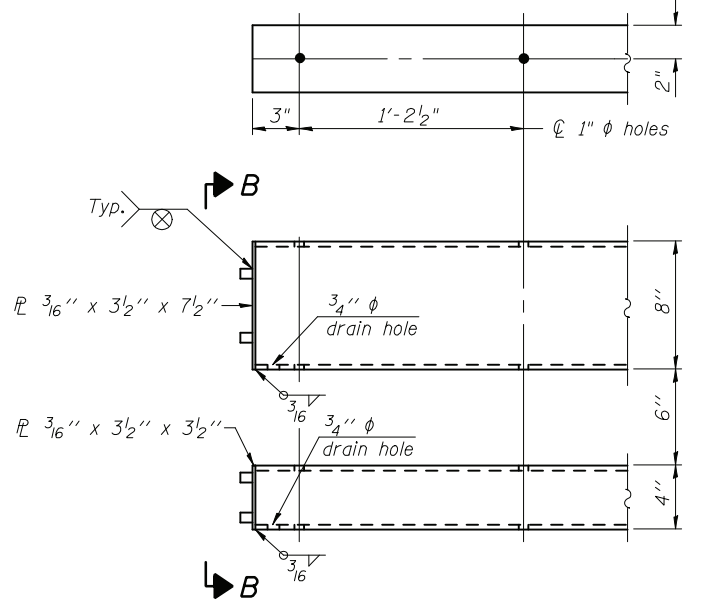
SECTION AT RAIL POST



SECTION A-A



VIEW B-B



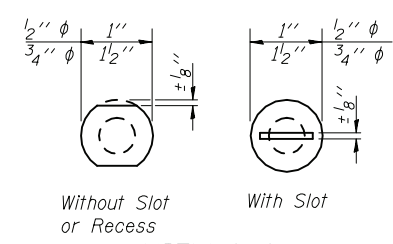
END OF RAIL DETAILS

4- 1" ϕ H.S. Threaded Anchor Rods with hex nuts and lock washers, drilled and set according to Article 509.06 of the Standard Specifications.

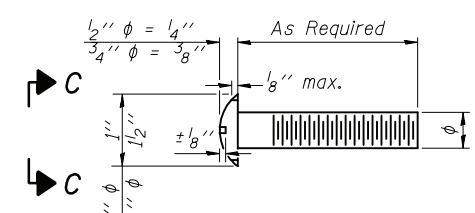
2- 3/4" ϕ x 6" Round Head Bolts (With slot or approved recess in head.) with locknut and flat washer. 7/8" ϕ Holes in tubing and posts. Holes in hollow structural section may be drilled in the field.

2- 1/2" ϕ x 6" Round Head Bolts (With slot or approved recess in head.) with locknut and flat washer. 5/8" ϕ Holes in hollow structural section and post. Holes in hollow structural section may be drilled in the field.

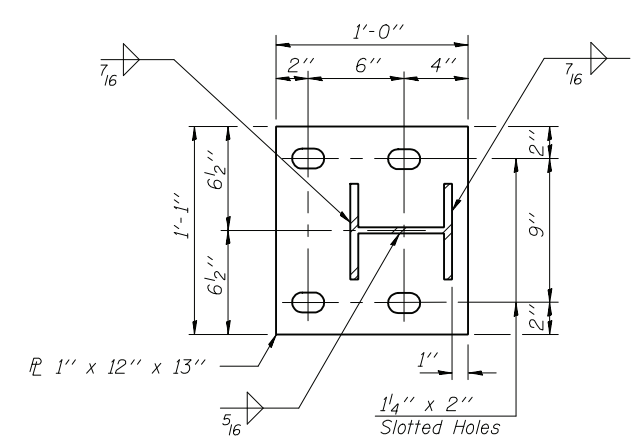
4 - 5/8" reduced base welded studs. Provide 4 - 5/8" washers and self-locking nuts or nuts and jam nuts for guardrail connection shown on Std. 631032.



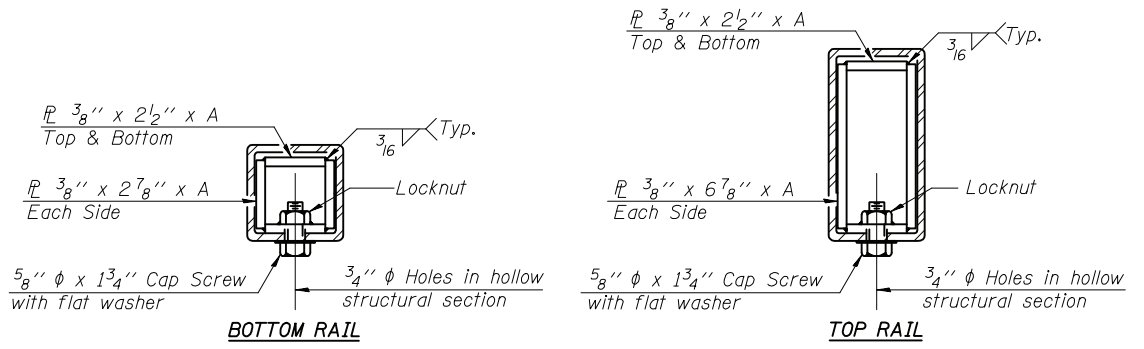
VIEW C-C



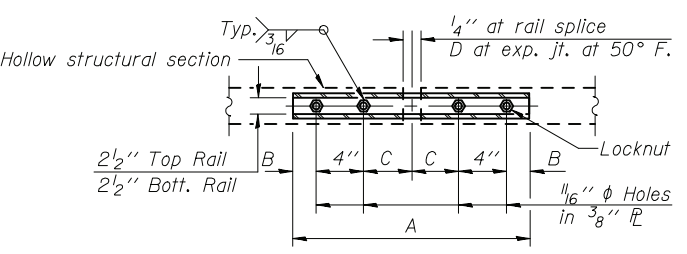
DETAIL OF 1/2" ϕ & 3/4" ϕ ROUND HEAD BOLTS



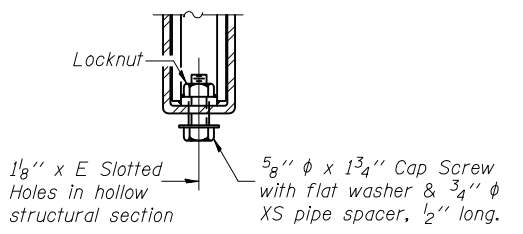
BASE PLATE DETAIL



SECTIONS AT RAIL SPLICE



PLAN-BOTT. SPLICE R TYPICAL



RAIL SPLICE CONNECTION AT EXPANSION JT.

Notes:
 All field drilled holes shall be coated with an approved zinc rich paint before erection.
 Posts shall not be located closer than 1'-3" to end of culvert.
 Provide one 1/8" and two 1/16" steel shims for 25% of the posts.
 Shims shall be similar to base plates in size and holes.
 All steel rail elements shall be galvanized according to Article 509.05 of the Standard Specifications.

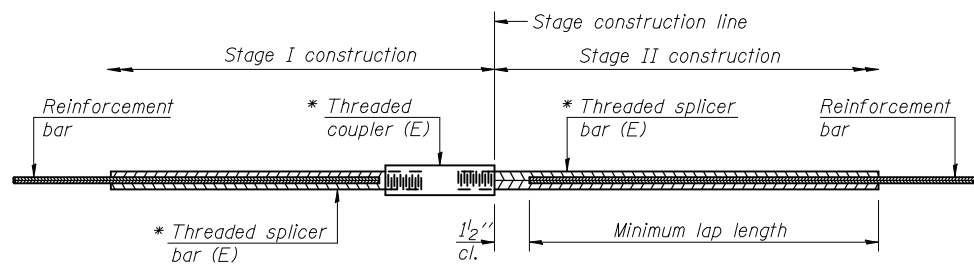
SPLICE DIMENSIONS

| T | D | A | B | C | E |
|---------------|--------|--------|--------|--------|--------|
| ≤ 4" | 2 1/2" | 1'-8" | 2" | 4" | 2 1/2" |
| > 4" ≤ 6 1/2" | 3 3/4" | 2'-0" | 2 1/2" | 5 1/2" | 3 1/2" |
| > 6 1/2" ≤ 9" | 5" | 2'-4" | 3 1/2" | 6 1/2" | 9" |
| > 9" ≤ 13" | 7" | 2'-10" | 4 1/2" | 8 1/2" | 11" |
| Rail Splice | 1/4" | 1'-8" | 2" | 4" | — |

T = Total movement at expansion joint as shown on the design plans.

BILL OF MATERIAL

| Item | Unit | Quantity |
|--------------------------|------|----------|
| Steel Railing, Type 2399 | Foot | 54 |



STANDARD BAR SPLICER ASSEMBLY

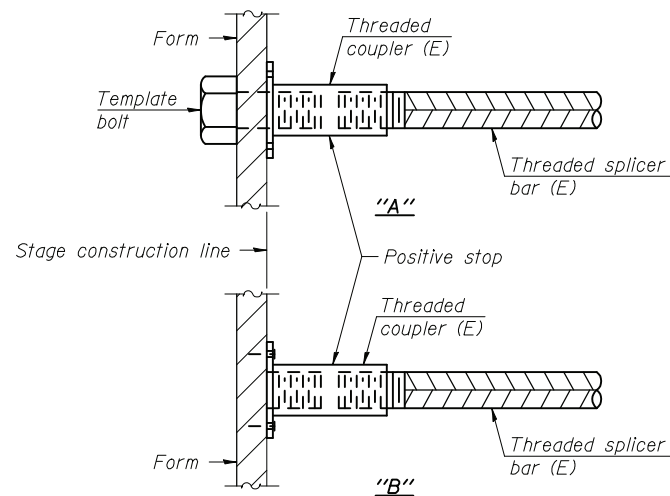
| Minimum Lap Lengths | | | | | |
|------------------------|---------|---------|---------|---------|---------|
| Bar size to be spliced | Table 1 | Table 2 | Table 3 | Table 4 | Table 5 |
| 3, 4 | 1'-5" | 1'-11" | 2'-1" | 2'-4" | 2'-3" |
| 5 | 1'-9" | 2'-5" | 2'-7" | 2'-11" | 2'-10" |
| 6 | 2'-1" | 2'-11" | 3'-1" | 3'-6" | 3'-4" |
| 7 | 2'-9" | 3'-10" | 4'-2" | 4'-8" | 4'-6" |
| 8 | 3'-8" | 5'-1" | 5'-5" | 6'-2" | 5'-10" |
| 9 | 4'-7" | 6'-5" | 6'-10" | 7'-9" | 7'-5" |

- Table 1: Black bar, 0.8 Class C
- Table 2: Black bar, Top bar lap, 0.8 Class C
- Table 3: Epoxy bar, 0.8 Class C
- Table 4: Epoxy bar, Top bar lap, 0.8 Class C
- Table 5: Epoxy bar, Top bar lap, Class B

Threaded splicer bar length = min. lap length + 1/2" + thread length

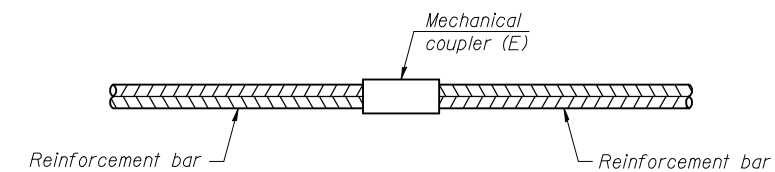
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

| Location | Bar size | No. assemblies required | Table for minimum lap length |
|-----------------------|----------|-------------------------|------------------------------|
| Top Slab | #4(E) | 27 | Table 3 |
| Approach Slab | #4(E) | 50 | Table 3 |
| Bottom Slab | #5 | 52 | Table 1 |
| Interior Wall | #5 | 8 | Table 1 |
| Approach Slab | #5(E) | 92 | Table 3 |
| Approach Slab Footing | #5(E) | 80 | Table 3 |
| Exterior Side Walls | #7 | 14 | Table 1 |
| Top Slab | #7(E) | 27 | Table 3 |



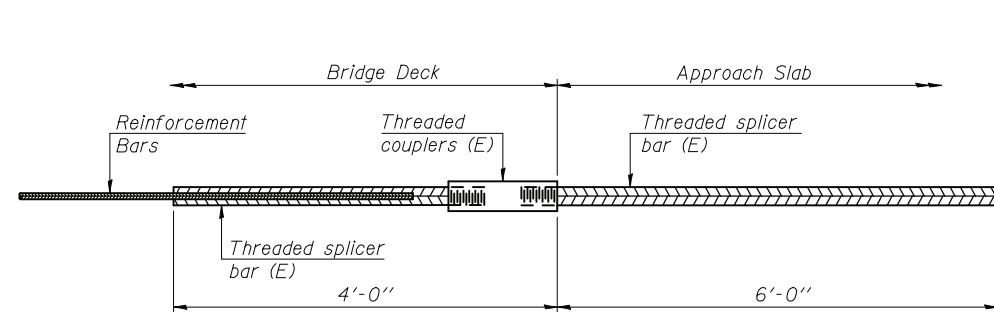
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



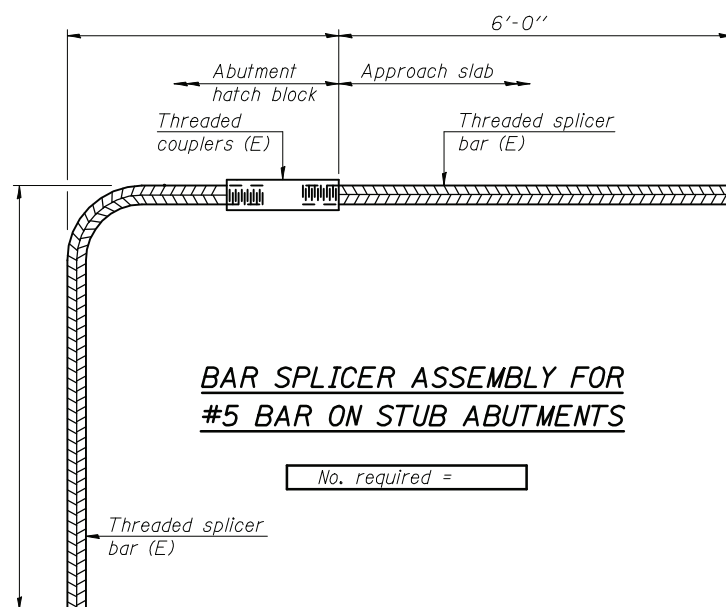
STANDARD MECHANICAL SPLICER

| Location | Bar size | No. assemblies required |
|----------|----------|-------------------------|
| | | |
| | | |
| | | |
| | | |



BAR SPLICER ASSEMBLY FOR #5 BAR ON INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

No. required =



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies for alternatives.

BSD-1

7-1-10



Illinois Department of Transportation
Division of Highways
Illinois Department of Transportation

SOIL BORING LOG

Page 1 of 2

Date 3/10/11

ROUTE FAP 327 DESCRIPTION US 50 over Unnamed Stream LOGGED BY JAS (TSI)

SECTION 20BR LOCATION SEC. 18, TWP. 2N, RNG. 1W, 3 PM

COUNTY Clinton DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic SPT

STRUCT. NO. 014-0410 (E) / 014-2024 (P)
Station 833+85

BORING NO. 1 North End
Station 833+74
Offset 9.00ft LT
Ground Surface Elev. 462.07 ft

| D E P T H | B L O W S | U C S | M O I S T | Surface Water Elev. _____ ft | D E P T H | B L O W S | U C S | M O I S T |
|-----------------------|-----------------------|-------------|-----------------------|------------------------------|-----------------------|-----------------------|-------------|-----------------------|
| (ft) | (/6") | (tsf) | (%) | Stream Bed Elev. _____ ft | (ft) | (/6") | (tsf) | (%) |

Groundwater Elev.:
First Encounter 444.6 ft
Upon Completion _____ ft
After _____ Hrs. _____ ft

| | | | | | | | | |
|---|-------|-------|------|----|--|-------|------|----|
| Asphalt Concrete (9"), Portland Cement Concrete (4"), & Crushed Limestone Base (2") | 460.8 | 1 | | | Gray (Hard, Moist) Silty LOAM (Till) (continued) | 10 | | |
| Dark Gray (Soft, Moist) Silty Clay LOAM (Fill) A-6(18) See Classification @ 1.5 ft | 458.6 | 1 | 0.36 | 28 | Trace Gravel | 20 | 5.60 | 11 |
| | | 2 | B | | | 36 | S | |
| Dark Gray (Medium Stiff, Moist) Silty CLAY (Possible Fill) | 456.6 | 1 | | | Gray, Shale Pieces | 26 | | |
| | | 2 | 1.56 | 27 | | 50/5" | 5.24 | 11 |
| | | 4 | S | | | -25 | - | S |
| Dark Gray (Medium Stiff, Moist) Silty CLAY (Loess) | 454.1 | 2 | | | Dark Gray (Hard, Moist) Silty CLAY | 25 | | |
| | | 3 | 1.54 | 22 | Trace Gravel | 40 | 6.20 | 11 |
| | | 4 | S | | Trace Limestone Pieces | 39 | S | |
| Gray / Gray to Brown (Medium Stiff, Moist) Silty CLAY (Alluvial) | 451.6 | 2 | | | Dark Gray (Hard, Moist) Silty LOAM | 19 | | |
| | | 2 | 1.27 | 23 | Trace Gravel | 33 | 9.77 | 10 |
| | | 3 | B | | | -30 | 30 | B |
| Gray/Brown (Medium Stiff, Moist) CLAY (Alluvial) A-7-6(22) See Classification @ 11.5 ft | 446.6 | 1 | | | Dark Gray (Hard, Moist) Silty CLAY | 9 | | |
| | | 3 | 0.58 | 24 | Limestone, Shale Pieces | 50/5" | -- | 19 |
| | | 3 | S | | Sample Too Small Too Test | -35 | - | |
| Brown (Medium Stiff) Clay LOAM (Alluvial) A-6(9) See Classification @ 16.5 ft | 444.6 | 1 | | | Dark Gray (Hard, Moist) Silty LOAM | 17 | | |
| | | 2 | 0.50 | 18 | Gravel | 50/5" | 3.69 | 12 |
| | | 4 | B | | | -40 | - | S |
| Gray (Hard, Moist) Silty LOAM (Till) | | 27 | | | | | | |
| Trace Gravel | | 31 | 2.32 | 11 | | | | |
| | | 50/4" | S | | | | | |
| | | -20 | | | | | | |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, from 137 (Rev. 8-99)



Illinois Department of Transportation
Division of Highways
Illinois Department of Transportation

SOIL BORING LOG

Page 2 of 2

Date 3/10/11

ROUTE FAP 327 DESCRIPTION US 50 over Unnamed Stream LOGGED BY JAS (TSI)

SECTION 20BR LOCATION SEC. 18, TWP. 2N, RNG. 1W, 3 PM

COUNTY Clinton DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic SPT

STRUCT. NO. 014-0410 (E) / 014-2024 (P)
Station 833+85

BORING NO. 1 North End
Station 833+74
Offset 9.00ft LT
Ground Surface Elev. 462.07 ft

| D E P T H | B L O W S | U C S | M O I S T | Surface Water Elev. _____ ft | D E P T H | B L O W S | U C S | M O I S T |
|-----------------------|-----------------------|-------------|-----------------------|------------------------------|-----------------------|-----------------------|-------------|-----------------------|
| (ft) | (/6") | (tsf) | (%) | Stream Bed Elev. _____ ft | (ft) | (/6") | (tsf) | (%) |

Groundwater Elev.:
First Encounter 444.6 ft
Upon Completion _____ ft
After _____ Hrs. _____ ft

| | | | | | | | | | |
|--|-------|-------|-------|----|---------------------------------------|-------|-------|----|----|
| Dark Gray (Hard, Moist) Silty LOAM (continued) | 419.1 | | | | Brown Weathered SANDSTONE (continued) | 401.6 | 50/1" | NC | 17 |
| | | | | | Auger Refusal - END OF BORING | | | | |
| Brown (Hard, Moist) Sandy Clay LOAM | 416.1 | 19 | | | | | | | |
| | | 45 | NC | 21 | | | | | |
| | | -45 | 50/5" | | | | | | |
| Dark Gray (Hard, Moist) Silty LOAM with Gravel | 413.1 | 12 | | | | | | | |
| | | 45 | -- | 12 | | | | | |
| Dark Gray (Hard, Moist) Sandy CLAY with Gravel | 410.1 | 5 | | | | | | | |
| Hard Drilling | | 6 | 3.76 | 24 | | | | | |
| | | -55 | 10 | B | | | | | |
| Gray and Brown (Hard) Silty CLAY with Gravel | 404.6 | 5 | | | | | | | |
| | | 6 | 3.76 | 24 | | | | | |
| | | -55 | 10 | B | | | | | |
| Hard Drilling | | | | | | | | | |
| Brown Weathered SANDSTONE (Hard, Moist) | | 50/3" | NC | 19 | | | | | |
| | | -60 | | | | | | | |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, from 137 (Rev. 8-99)



Illinois Department of Transportation
Division of Highways
Illinois Department of Transportation

SOIL BORING LOG

Page 1 of 2

Date 3/11/11

ROUTE FAP 327 DESCRIPTION US 50 over Unnamed Stream LOGGED BY JAS (TSI)

SECTION 20BR LOCATION SEC. 18, TWP. 2N, RNG. 1W, 3 PM

COUNTY Clinton DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic SPT

STRUCT. NO. 014-0410 (E) / 014-2024 (P) Station 833+85
BORING NO. 2 South End Station 834+09 Offset 9.00ft RT Ground Surface Elev. 462.08 ft

| DEPTH (ft) | BLOW COUNT (blows/6") | UCS FAILURE MODE | MOISTURE (%) | SOIL DESCRIPTION | DEPTH (ft) | BLOW COUNT (blows/6") | UCS FAILURE MODE | MOISTURE (%) |
|------------|-----------------------|------------------|--------------|------------------|------------|-----------------------|------------------|--------------|
|------------|-----------------------|------------------|--------------|------------------|------------|-----------------------|------------------|--------------|

| | | | | | | | | |
|--|-------|----|------|----|---|-------|-------|----|
| Asphalt Concrete (12"), Portland Cement Concrete (7") | 460.6 | 1 | | | Dark Gray (Hard, Moist) SILT (Till) | 24 | | |
| | | 2 | 1.44 | 36 | | 36 | 20.64 | 10 |
| Dark Gray (Soft, Moist) Silty CLAY (Fill) | | 2 | B | | Began Mud Rotary Drilling | 50/2" | S | |
| | | 2 | | | Trace Gravel | 30 | | |
| Medium Stiff, More Clayey | | 3 | 3.68 | 28 | | 50/5" | 25.15 | 10 |
| | 456.6 | 4 | S | | | -25 | - | |
| Dark Gray (Soft, Moist) Silty CLAY (Alluvial) | | 1 | | | Dark Gray (Hard, Moist) Silty CLAY (Till) | 30 | | |
| | | 2 | 3.28 | 26 | Trace Wood | 30 | 18.30 | 12 |
| | | 2 | B | | | 50/5" | S | |
| Medium Stiff, Gray to Brown | | 1 | | | Dark Gray (Hard, Moist) Silty Clay LOAM | 24 | | |
| | 451.6 | 2 | 4.60 | 22 | Trace Gravel | 28 | 39.55 | 10 |
| | | 3 | S | | | 50/5" | S | |
| Gray to Brown (Medium Stiff, Moist) CLAY (Alluvial) | | 1 | | | | -30 | | |
| | 449.1 | 3 | 5.65 | 20 | Dark Brown/Gray (Very Stiff, Moist) CLAY (Till) | 4 | | |
| | | 3 | S | | See Classification @ 34 ft | 6 | 12.48 | 23 |
| Gray/Brown (Medium Stiff, Moist) Clay LOAM | | 3 | 4.86 | 19 | | 12 | B | |
| | 446.6 | 3 | S | | | -35 | | |
| Gray/Brown (Very Stiff, Moist) Silty LOAM (Alluvial) | | 1 | | | Dark Gray (Hard, Moist) Silty Clay LOAM (Till) | 17 | | |
| | 444.6 | 6 | 3.77 | 17 | | 27 | 27.94 | 12 |
| | | 11 | S | | | 32 | S | |
| Brown (Medium Dense, Wet) Gravelly SAND (Alluvial) See Gradation @ 19 ft | | 1 | | | | 17 | | |
| | 442.1 | 7 | NC | -- | | 27 | 27.94 | 12 |
| | | 12 | | | | 32 | S | |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, from 137 (Rev. 8-99)



Illinois Department of Transportation
Division of Highways
Illinois Department of Transportation

SOIL BORING LOG

Page 2 of 2

Date 3/11/11

ROUTE FAP 327 DESCRIPTION US 50 over Unnamed Stream LOGGED BY JAS (TSI)

SECTION 20BR LOCATION SEC. 18, TWP. 2N, RNG. 1W, 3 PM

COUNTY Clinton DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic SPT

STRUCT. NO. 014-0410 (E) / 014-2024 (P) Station 833+85
BORING NO. 2 South End Station 834+09 Offset 9.00ft RT Ground Surface Elev. 462.08 ft

| DEPTH (ft) | BLOW COUNT (blows/6") | UCS FAILURE MODE | MOISTURE (%) | SOIL DESCRIPTION | DEPTH (ft) | BLOW COUNT (blows/6") | UCS FAILURE MODE | MOISTURE (%) |
|------------|-----------------------|------------------|--------------|------------------|------------|-----------------------|------------------|--------------|
|------------|-----------------------|------------------|--------------|------------------|------------|-----------------------|------------------|--------------|

| | | | | | | | | |
|--|--|--------|-------|----|------------------------------|--|--|--|
| Dark Gray (Hard, Moist) Silty Clay LOAM (Till) (continued) | | | | | Surface Water Elev. _____ ft | | | |
| | | | | | Stream Bed Elev. _____ ft | | | |
| Trace Limestone Pieces | | 11 | | | Groundwater Elev.: | | | |
| Trace Shale Pieces | | 17 | 24.70 | 14 | First Encounter _____ ft | | | |
| | | 21 | B | | Upon Completion _____ ft | | | |
| | | -45 | | | After _____ Hrs. _____ ft | | | |
| Weathered Limestone Cobble | | 100/1" | | | | | | |
| Dark Brown | | - | NC | 7 | | | | |
| | | -50 | | | | | | |
| Brown (Very Stiff, Moist) Silty CLAY (Till) | | 3 | | | | | | |
| Trace Shale Pieces | | 6 | 3.68 | 24 | | | | |
| | | 11 | S | | | | | |
| END OF BORING | | | | | | | | |

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, from 137 (Rev. 8-99)

FILE NAME = \014202476B43-015-Borings 2.dgn
DESIGNED -
CHECKED -
DRAWN - JG
CHECKED - CJF

USER NAME =
Illinois Design Firm Number 184.001670
PLOT SCALE =
PLOT DATE = 8:59:23 AM/12/14/2011

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SOIL BORINGS
STRUCTURE NO. 014-2024**

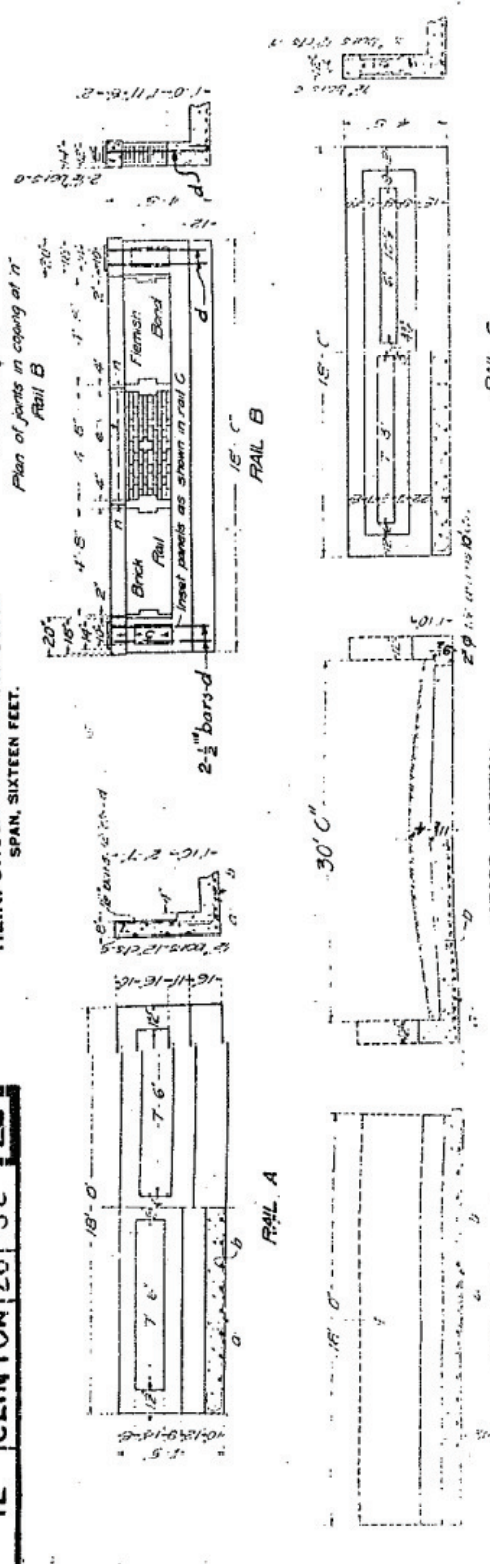
SHEET NO. 15 OF 15 SHEET

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|---------|--------------|-----------|
| 327 | 20BR | CLINTON | 51 | 41 |
| CONTRACT NO. 76B43 | | | | |
| FED. ROAD DIST. NO. - [ILLINOIS] FED. AID PROJECT | | | | |

| | | | | |
|----------------------|---------|------|--------------|-----------|
| BOND ISSUE ROUTE NO. | COUNTY | SEC. | TOTAL SHEETS | SHEET NO. |
| 12 | CLINTON | 20 | 32 | 26 |

STATE OF ILLINOIS
STATE HIGHWAY DEPARTMENT
REINFORCED CONCRETE SLAB
SPAN, SIXTEEN FEET.

SHEET NO. 1
SHEETS



BILL OF MATERIAL

| NO. | DESCRIPTION | QTY. | UNIT | PRICE | TOTAL |
|-----|--------------------|------|-------|--------|--------|
| 1 | CONCRETE | 17.0 | CU YD | 12.00 | 204.00 |
| 2 | REINFORCEMENT BARS | 1.2 | TONS | 150.00 | 180.00 |
| 3 | FORMWORK | 16.0 | SQ YD | 10.00 | 160.00 |
| 4 | BRICK | 1.0 | 1000 | 10.00 | 10.00 |
| 5 | RAIL B | 1.0 | 1000 | 10.00 | 10.00 |
| 6 | RAIL C | 1.0 | 1000 | 10.00 | 10.00 |

USE RAIL B .30 FOOT ROADWAY

STATION 833+85
STATE BOND ISSUE - ROUTE 12
SECTION 20 - CLINTON CO.

Class A concrete to be used throughout
Proportions: 1-3-4

APPROVED
R. F. Burch
ENGINEER OF DESIGN

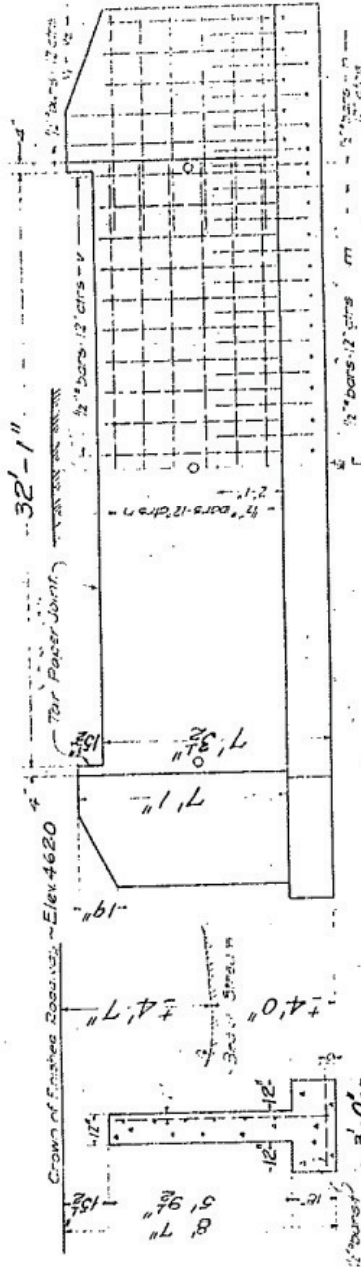
PLAN

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STATE OF ILLINOIS
STATE HIGHWAY DEPARTMENT
R. C. ABUTMENTS FOR SLAB BRIDGE
HEIGHT OVER ALL 8'-7"

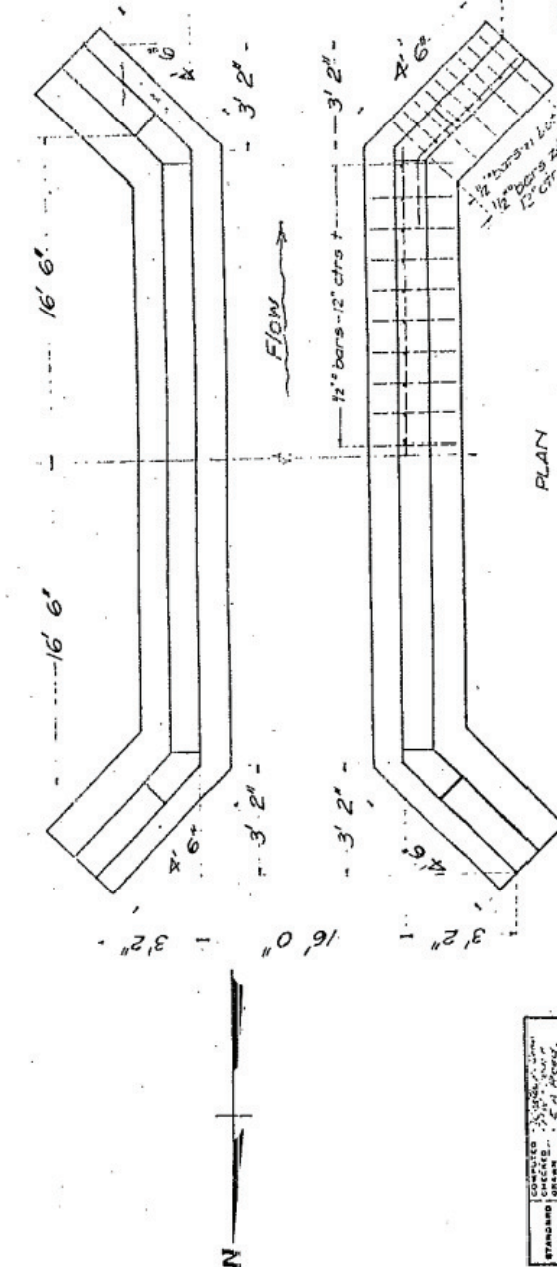
| | | | | |
|----------------------|---------|------|--------------|-----------|
| BOND ISSUE ROUTE NO. | COUNTY | SEC. | TOTAL SHEETS | SHEET NO. |
| 12 | CLINTON | 20 | 32 | 26 |

B.M. - N. & W. in rear of 1st. Pole at right
of station 832+10. Elev. 460.35



SHOWING OUTLINES - FRONT ELEVATION - SHOWING REINFORCEMENT

SECTION OF ABUTMENT



SHOWING OUTLINES

SHOWING REINFORCEMENT

STATION 833+85
STATE BOND ISSUE - ROUTE 12
SECTION 20 - CLINTON CO.

APPROVED
R. F. Burch
ENGINEER OF DESIGN

SUMMARY OF MATERIAL

| NO. | DESCRIPTION | QTY. | UNIT | PRICE | TOTAL |
|-----|--------------------|------|-------|--------|--------|
| 1 | CONCRETE | 17.0 | CU YD | 12.00 | 204.00 |
| 2 | REINFORCEMENT BARS | 1.2 | TONS | 150.00 | 180.00 |
| 3 | FORMWORK | 16.0 | SQ YD | 10.00 | 160.00 |
| 4 | BRICK | 1.0 | 1000 | 10.00 | 10.00 |
| 5 | RAIL B | 1.0 | 1000 | 10.00 | 10.00 |
| 6 | RAIL C | 1.0 | 1000 | 10.00 | 10.00 |

FILE NAME =
USER NAME = Paul
DESIGNED - ESW
DRAWN - PDB
CHECKED - BRM
DATE - 11-9-11

REVISIONS
REVISED -
REVISED -
REVISED -
REVISED -

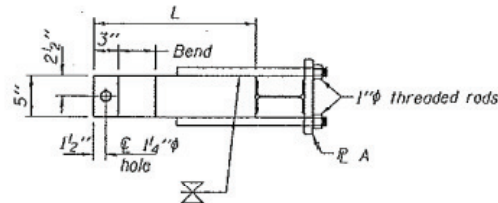
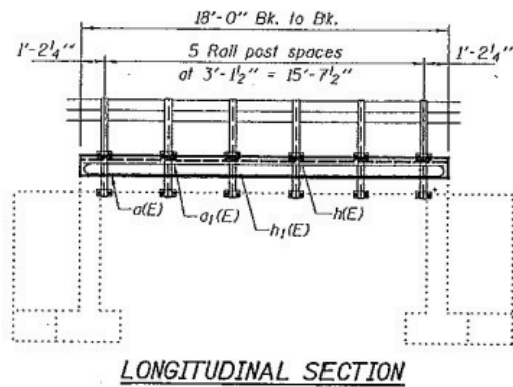
EXISTING STRUCTURE PLANS
FOR INFORMATION ONLY
SCALE: SHEET NO. 1 OF 3 SHEETS STA. TO STA.

F.A.P. RTE. 327 SECTION 20BR COUNTY CLINTON TOTAL SHEETS 51 SHEET NO. 42
CONTRACT NO. 76B43
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | | | |
|-----------------------|---------|---------|------|------|---------------------------|
| ROUTE NO. | SECTION | EMPH. | DATE | POST | SHEET NO. 1 |
| US 50 | | CLINTON | | | 2 SHEETS |
| FED. ROAD DIST. NO. 7 | | | | | ILLINOIS FED. AID PROJECT |

Contract Number:



SECTION A-A

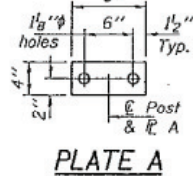
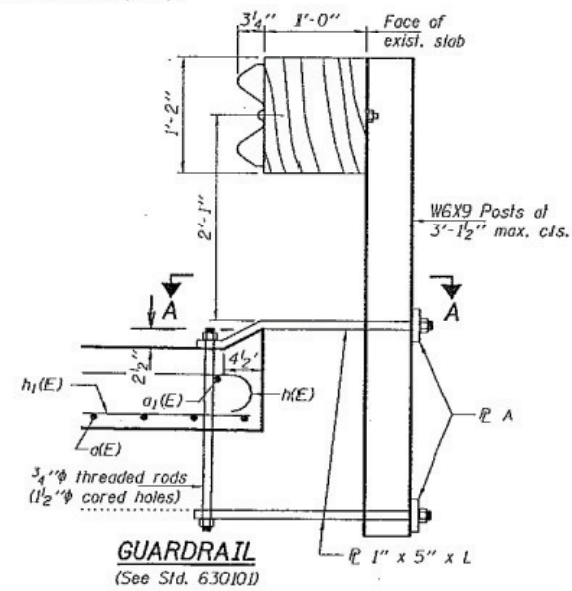


PLATE A



GUARDRAIL
(See Std. 630101)

GENERAL NOTES

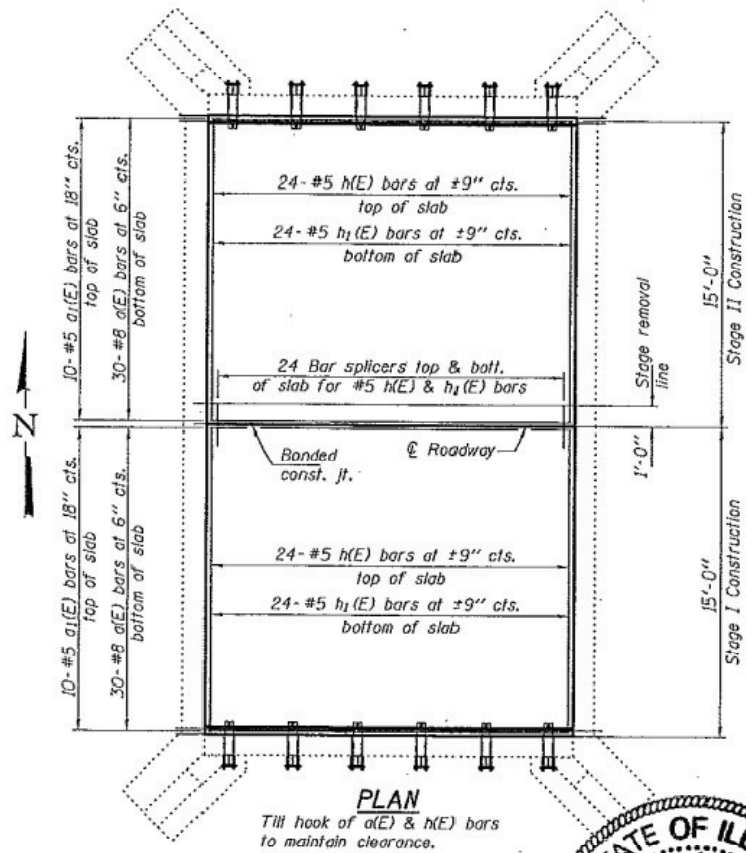
Plan dimensions and details relative to existing plans are subject to routine variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished based upon the unit price bid for the work.

Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60 (IL Modified). See Special Provisions.

Reinforcement bars designated (E) shall be epoxy coated.

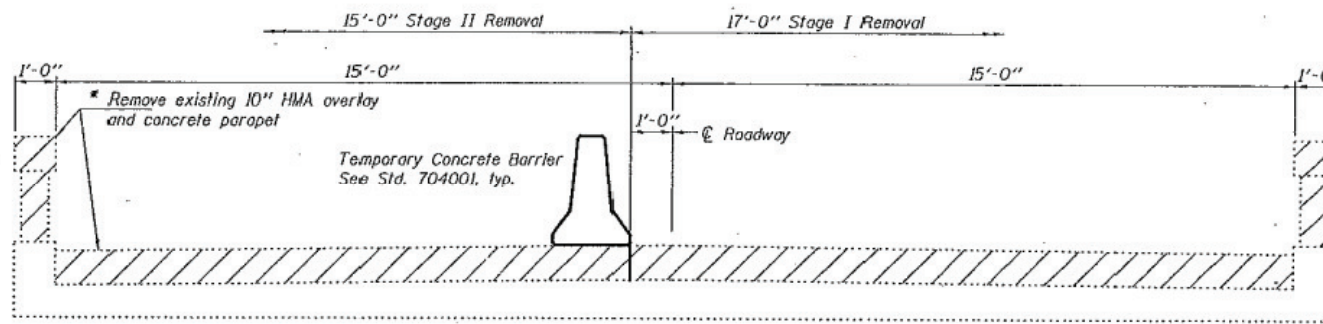
The deck surface shall have its final finish lined according to Article 420.09(e)(1) of the Standard Specifications. Cost included with Concrete Superstructure.

All plates, threaded rods and nuts shall be galvanized according to Article 509.05 of the Standard Specifications.



PLAN

Till hook of a(E) & h(E) bars to maintain clearance.

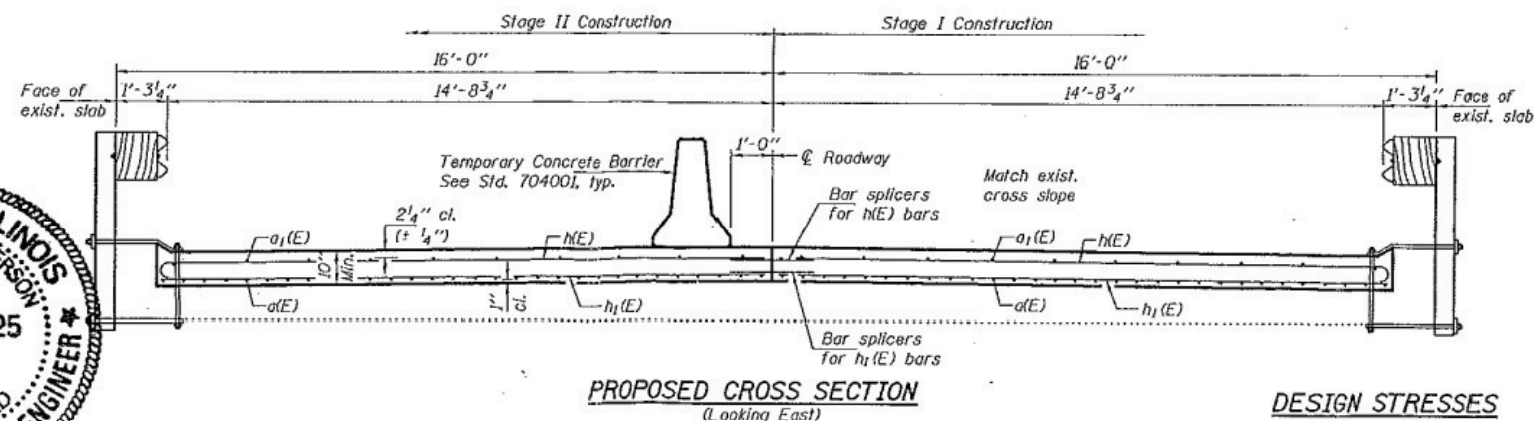


EXISTING CROSS SECTION

(Looking East)

Hatched areas indicate removal

* Remove the existing wearing surface and concrete parapet to the top of the existing slab and replace as shown. Slope to match roadway. Cost of all removal is included with Hot-Mix Asphalt Surface Removal.



PROPOSED CROSS SECTION

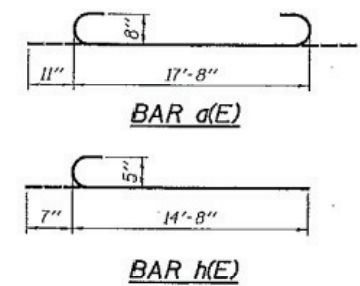
(Looking East)

Note:
Extend existing deck drains through new deck. Place reinforcement to clear drains by 3".

DESIGN STRESSES

FIELD UNITS

f'c = 3,500 psi
fy = 60,000 psi (Reinforcement)



BAR a(E)

BAR h(E)

BILL OF MATERIAL

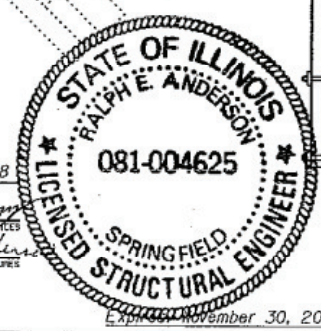
| Bar | No. | Size | Length | Shape |
|----------------------------------|----------|------|--------|-------|
| a(E) | 60 | #8 | 19'-6" | U |
| a1(E) | 20 | #5 | 17'-8" | — |
| h(E) | 48 | #5 | 15'-3" | U |
| h1(E) | 48 | #5 | 14'-8" | — |
| Bar Splicers | Each | | 48 | |
| Hot-Mix Asphalt Surface Removal | Sq. Yds. | | 60.4 | |
| Reinforcement Bars, Epoxy Coated | Pound | | 4990 | |
| Concrete Superstructure | Cu. Yds. | | 16.7 | |
| SPBGR Attached to Structure | Foot | | 36 | |

SLAB DETAILS

US 50
CLINTON COUNTY
SN 014-0410

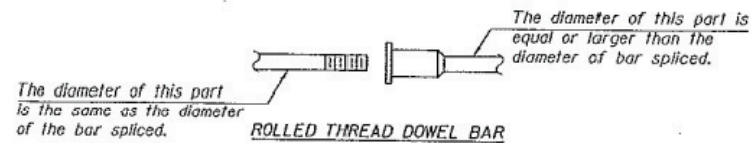
| | |
|----------|-----------------|
| DESIGNED | Victor H. Volpi |
| CHECKED | Paul Boliva |
| DRAWN | boliva |
| CHECKED | VHV |

MARCH 19, 2008
EXAMINED
PASSED



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | | | |
|------------------|---------|---------|--------|------|-------------|
| PROJECT NO. | SECTION | COUNTY | SHEETS | POST | SHEET NO. 2 |
| US 50 | | CLINTON | | | 2 SHEETS |
| CONTRACT NUMBER: | | | | | |



ROLLED THREAD DOWEL BAR



ONE PIECE

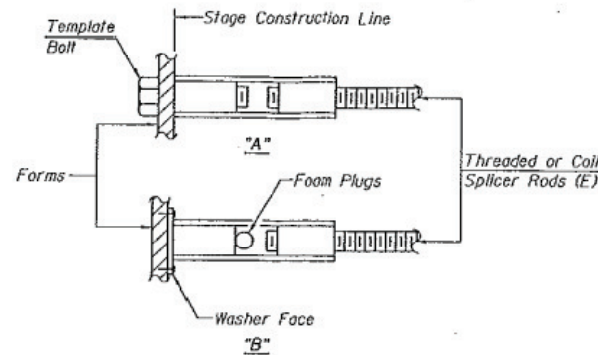
Wire Connector



WELDED SECTIONS

BAR SPLICER ASSEMBLY ALTERNATIVES

**Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.



INSTALLATION AND SETTING METHODS

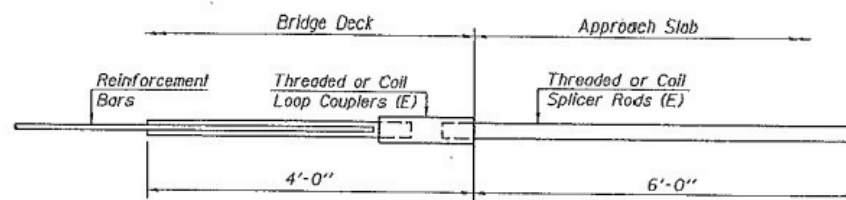
"A" : Set bar splicer assembly by means of a template bolt.
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
(E) : Indicates epoxy coating.

NOTES

Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.
Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.
All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.
Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

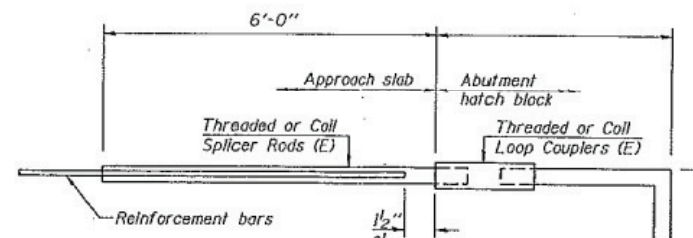
- Minimum Capacity = $1.25 \times f_y \times A_s$
(Tension in kips)
 - Minimum *Pull-out Strength = $0.66 \times f_y \times A_s$
(Tension in kips)
- Where f_y = Yield strength of lapped reinforcement bars in ksi.
 A_s = Tensile stress area of lapped reinforcement bars.
* = 28 day concrete

| Bar Size to be Spliced | Splicer Rod or Dowel Bar Length | Strength Requirements | |
|------------------------|---------------------------------|------------------------------|---------------------------------------|
| | | Min. Capacity kips - tension | Min. Pull-Out Strength kips - tension |
| #4 | 1'-8" | 14.7 | 7.9 |
| #5 | 2'-0" | 23.0 | 12.3 |
| #6 | 2'-7" | 33.1 | 17.4 |
| #7 | 3'-5" | 45.1 | 23.8 |
| #8 | 4'-6" | 58.9 | 31.3 |
| #9 | 5'-9" | 75.0 | 39.6 |
| #10 | 7'-3" | 95.0 | 50.3 |
| #11 | 9'-0" | 117.4 | 61.8 |



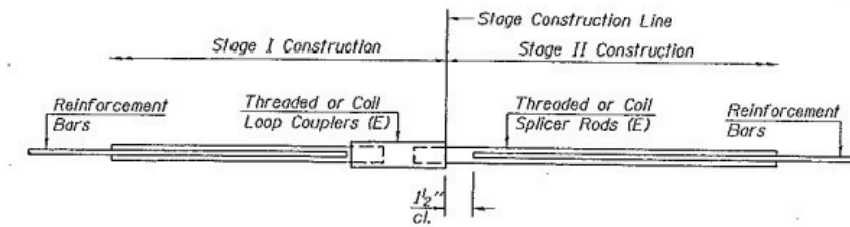
FOR INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

| | |
|--|--|
| Bar Splicer for #5 bar | |
| Min. Capacity = 23.0 kips - tension | |
| Min. Pull-out Strength = 12.3 kips - tension | |
| No. Required = | |



FOR STUB ABUTMENTS

| | |
|--|--|
| Bar Splicer for #5 bar | |
| Min. Capacity = 23.0 kips - tension | |
| Min. Pull-out Strength = 12.3 kips - tension | |
| No. Required = | |



STANDARD

| Bar Size | No. Assemblies Required | Location |
|----------|-------------------------|----------------|
| #5 | 24 | Top of slab |
| #5 | 24 | Bottom of slab |
| | | |
| | | |

REPAIR DETAILS
US 50
CLINTON COUNTY
SN 014-0410

| | | | |
|----------|---------|----------|--------------------------|
| DESIGNED | VHV | EXAMINED | <i>Carl Perry</i> |
| CHECKED | DAB | PASSED | <i>Ralph E. Anderson</i> |
| DRAWN | baliva | | |
| CHECKED | VHV DAB | | |

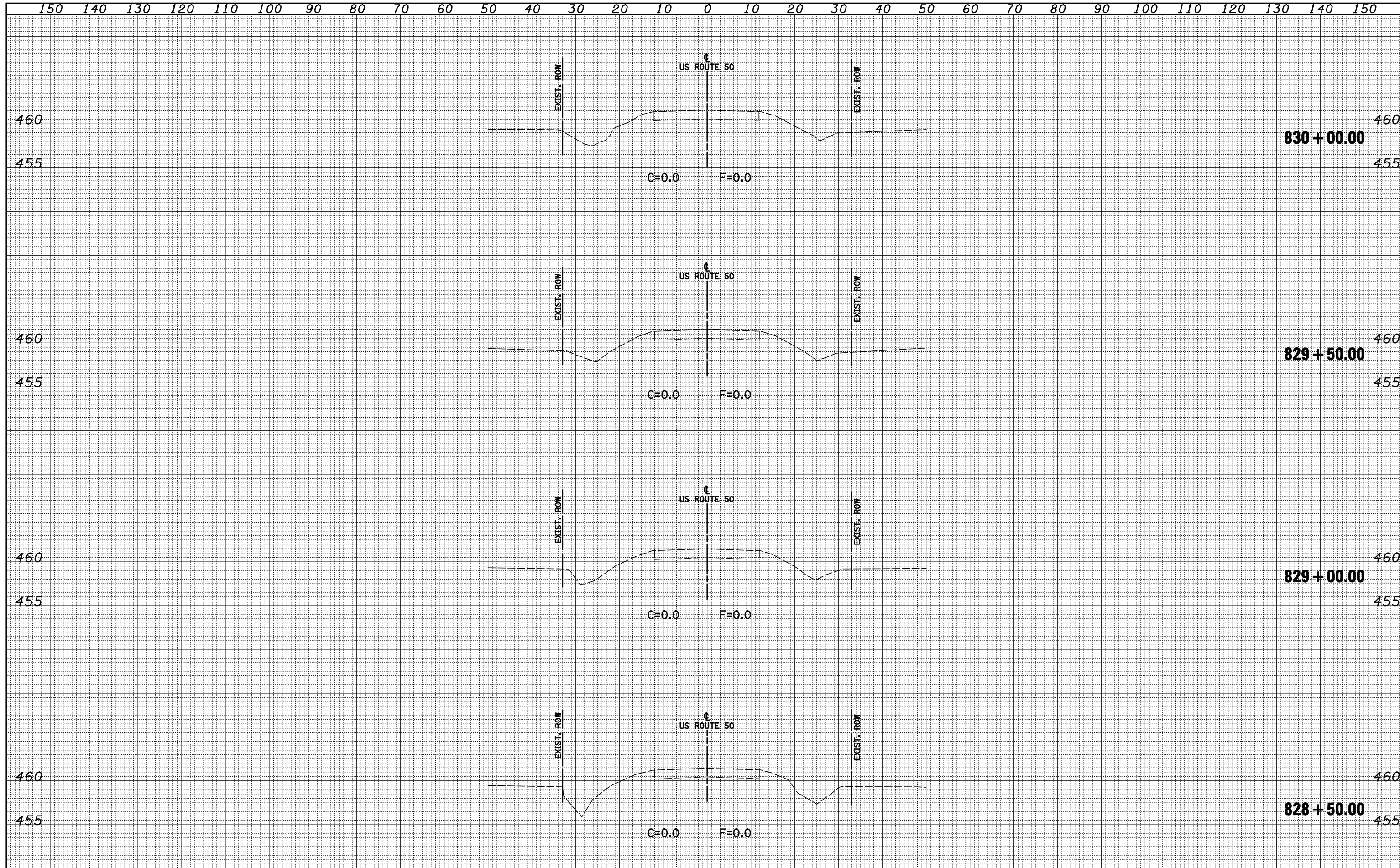
MARCH 19, 2008
ENGINEER OF STRUCTURAL SERVICES
ENGINEER OF BRIDGES AND STRUCTURES

BSD-1 11-1-06

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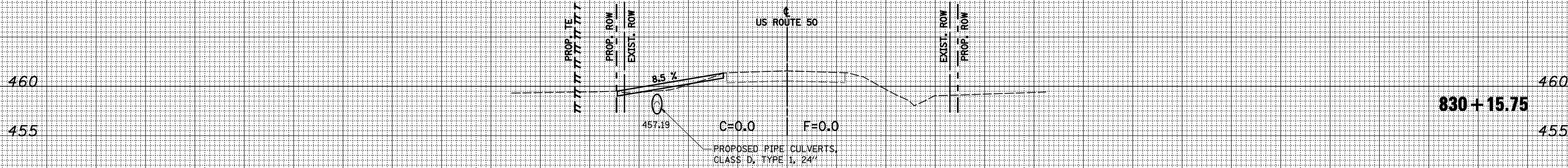
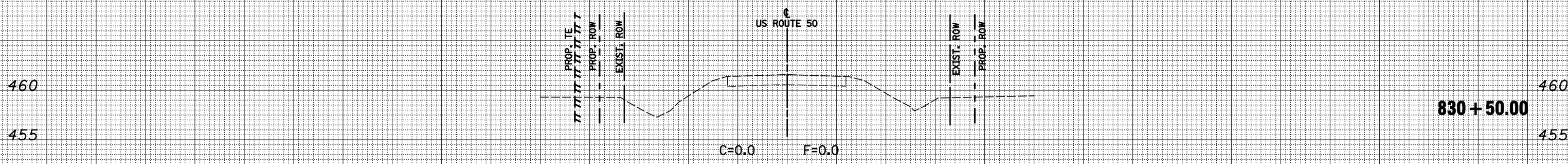
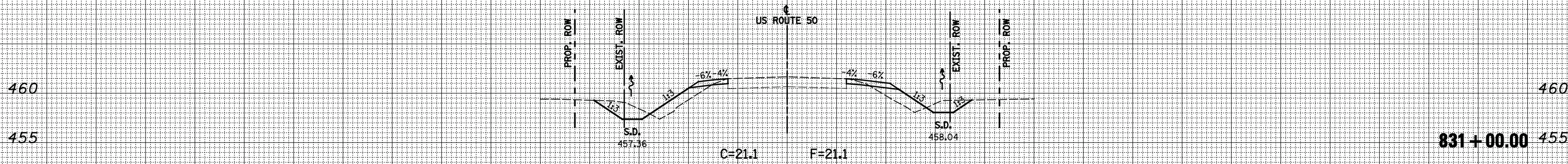
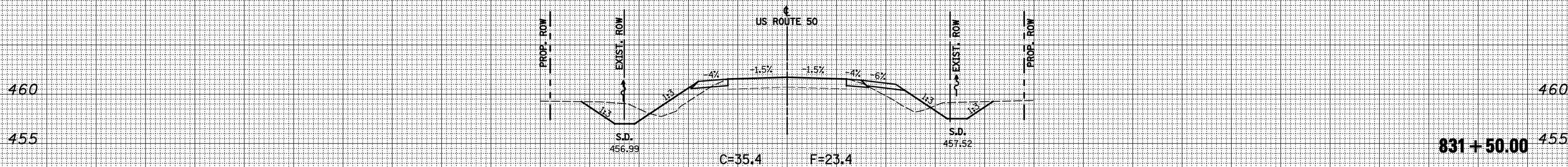
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| S:\Projects\40-0022-3HY US 50 Final Plans\dgn\CADD Sheets\081643-xshf.dgn | | DRAWN - LEC | REVISED - | | 327 | 20BR | CLINTON | 51 | 45 | | | |
| PLOT SCALE = 20.0000' / IN. | | CHECKED - JWS | REVISED - | | SCALE: 1"=10' | | | SHEET NO. 1 OF 7 SHEETS | | STA. 828+50.00 TO STA. 830+00.00 | CONTRACT NO. 76B43 | |
| PLOT DATE = 11/16/2011 | | DATE - 12-2-10 | REVISED - | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | | | | |

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150



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| CHECKED | |
| NO. | |

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USER NAME = Paul
 DESIGNED - JLS
 DRAWN - LEC
 CHECKED - JWS
 DATE - 12-2-10

REVISED -
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 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

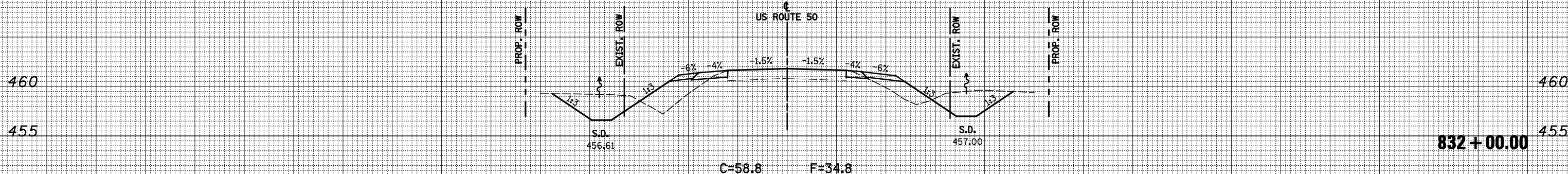
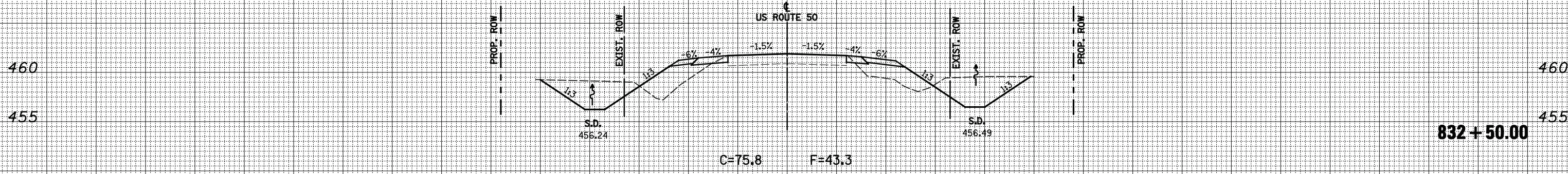
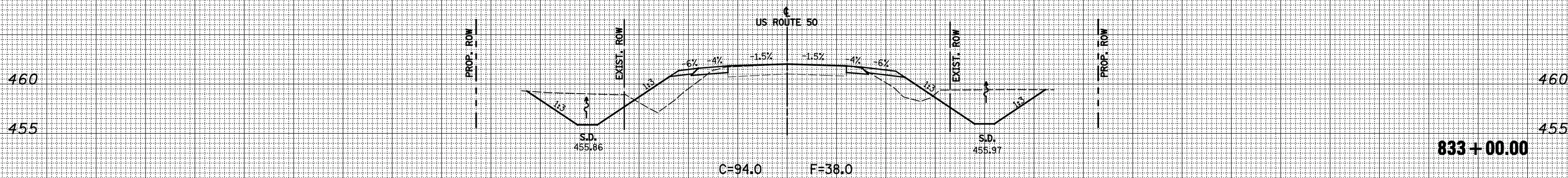
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| CONTRACT NO. 76B43 | | | | |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |

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| BY | |
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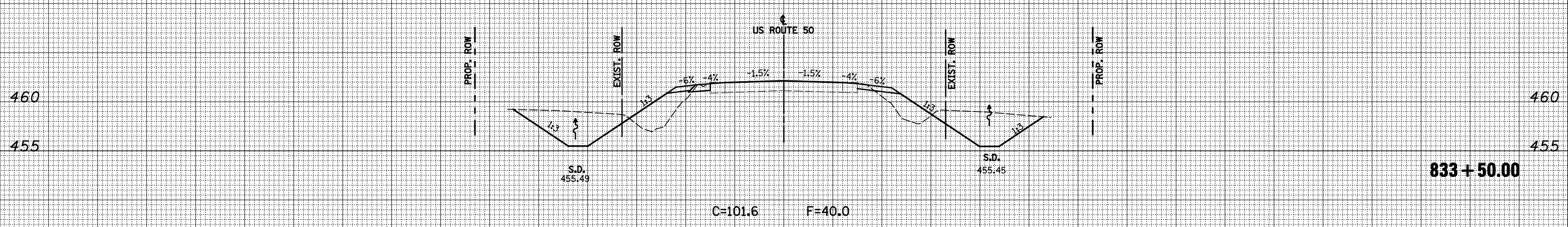
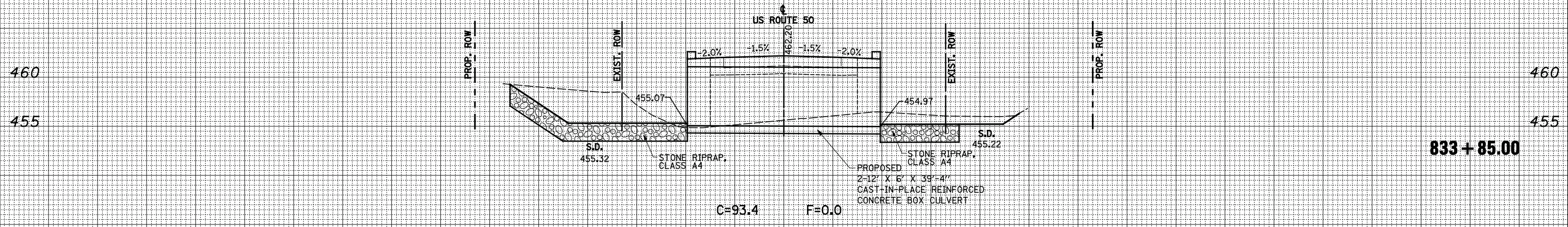
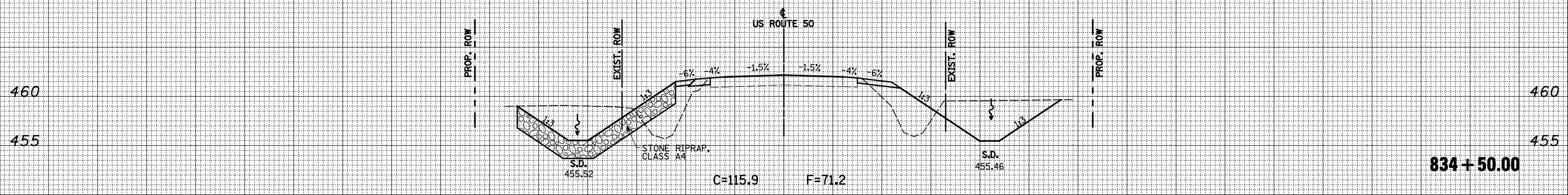
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| PLOT DATE = 11/16/2011 | | DATE - 12-2-10 | REVISED - | | | | | | | | | |

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| FINAL SURVEY | |
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| NOTE BOOK | |
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| NOTE BOOK | |
| NO. | |
| AREAS CHECKED | |
| AREAS CHECKED | |



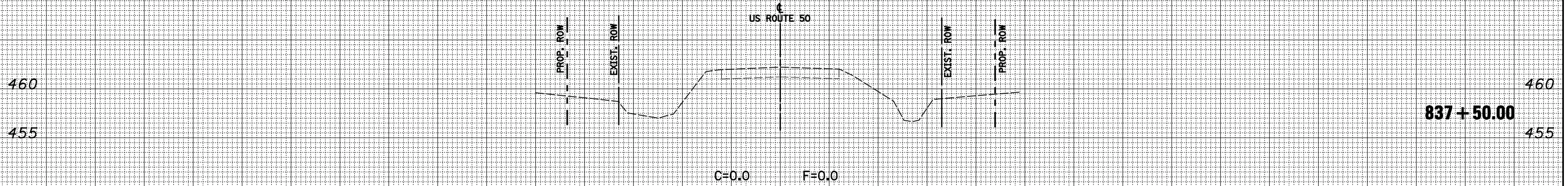
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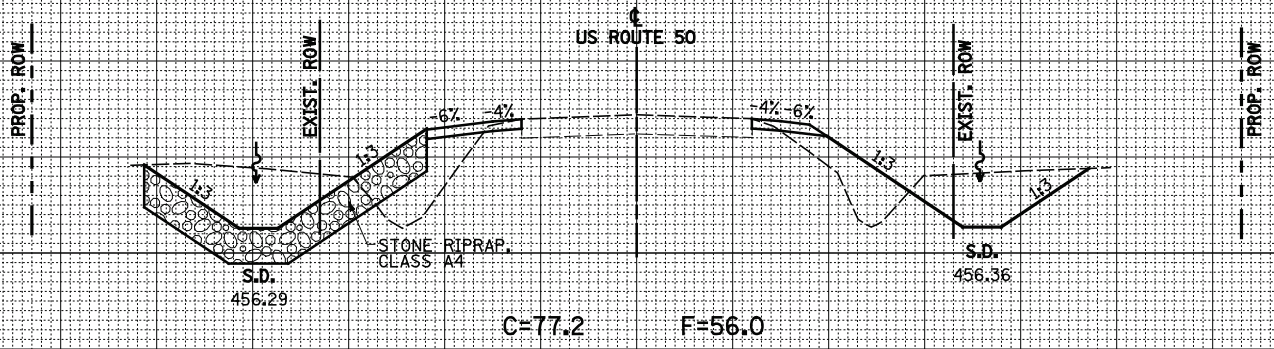
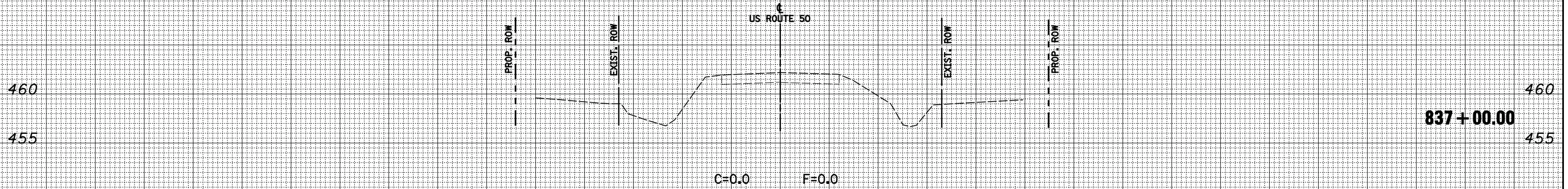
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USER NAME = Paul
 DESIGNED - JLS
 DRAWN - LEC
 CHECKED - JWS
 DATE - 12-2-10

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS - FAP 327 (US ROUTE 50)

SCALE: 1"=10' SHEET NO. 6 OF 7 SHEETS STA. 836+50.00 TO STA. 838+00.00

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
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| CONTRACT NO. 76B43 | | | | |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |

