

GENERAL NOTES

WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKERS AND MONUMENTS UNTIL THE OWNER, AND AUTHORIZED SURVEYOR OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION.

THE AREA TO BE SEEDED SHALL CONSIST OF ALL DISTURBED EARTH SURFACES WITHIN THE RIGHT OF WAY, AS DIRECTED BY THE ENGINEER.

SEEDING, CLASS 2 (SPECIAL) = 3.05 ACRE

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE USE(S)	SURFACE	BINDER
PG:	PG 58-22	PG 58-22
DESIGN AIR VOIDS:	3.0 @ N50	3.0 @ N50
MIXTURE COMPOSITION: (GRADATION MIXTURE)	IL 9.5 OR 12.5	IL 19.0
FRICTION AGGREGATE	C	N/A
20 YEAR ESAL	0.1	0.1
MIX UNIT WEIGHT	112 LBS/SY/IN	

PAVEMENT DESIGN

Structural Design Traffic (S.D.T.) : Year 2021; P.V. = 662, S.U. = 14, M.U. = 14
Class III Road

Minimum Soil Support: I.B.R. = 3.0 (Assumed) (> 3 k.s.i.)

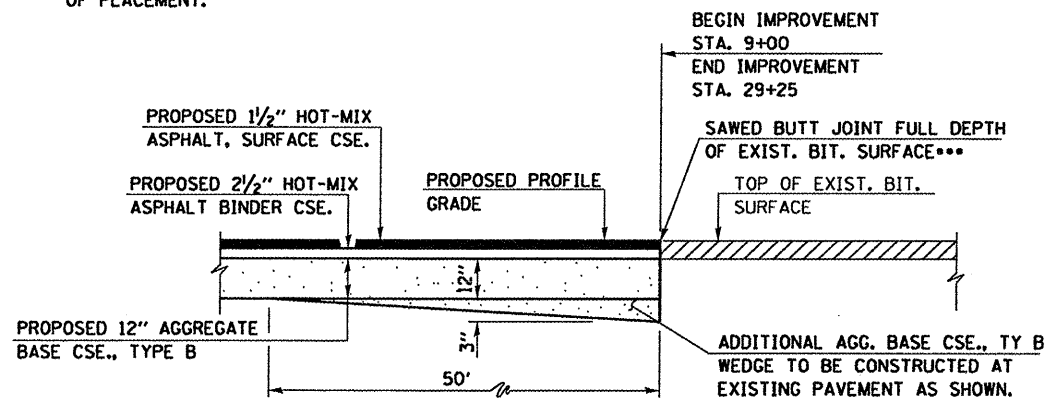
Percent of S.D.T. in Design Lane: P = 50%, S = 50%, M = 50%
T.F. = 0.118

Temp.=72° F.; E_{ac}=656; Design Strain=327

APPLICATION RATES USED IN QUANTITY CALCULATIONS

STONE RIPRAP, CLASS A5	1.65 TON/CU YD
AGGREGATE DITCH (SPECIAL)	1.65 TON/CU YD
AGGREGATE BASE COURSE AND SURFACE COURSE	2.05 TON/CU YD
BITUMINOUS MATERIALS (PRIME COAT) - (AGG.)	0.35 GAL/SQ YD
BITUMINOUS MATERIALS (PRIME COAT) - (HMA)	0.10 GAL/SQ YD
HOT-MIX ASPHALT (BINDER & SURFACE COURSE)	112*/SQ YD/IN

THE ABOVE NOTED APPLICATION RATES FOR BITUMINOUS MATERIALS (PRIME COAT) ARE FOR QUANTITY CALCULATIONS ONLY. THE APPLICATION RATE TO BE APPLIED WILL BE DETERMINED BY THE ENGINEER AT THE TIME OF PLACEMENT.



ELEVATION AT BEGINNING AND END OF IMPROVEMENT

JOINT DETAILS

***COST INCLUDED IN "PAVEMENT REMOVAL".

TREE REMOVAL

TREES THAT INTERFERE WITH CONSTRUCTION WITHIN THE LIMITS OF THE ROW LINES SHALL BE REMOVED AT THE DIRECTION OF THE ENGINEER.

TREE REMOVAL (6 TO 15 UNITS DIAMETER)

22' RT. STA. 23+14	=	6 UNIT
22' RT. STA. 23+14	=	6 UNIT
3' RT. STA. 23+25	=	8 UNIT
3' RT. STA. 23+25	=	8 UNIT
10' RT. STA. 23+31	=	6 UNIT
0.5' LT. STA. 23+41	=	10 UNIT
0.5' LT. STA. 23+41	=	10 UNIT
0.5' LT. STA. 23+41	=	10 UNIT
0.5' LT. STA. 23+41	=	10 UNIT
0.5' LT. STA. 23+41	=	10 UNIT
1' LT. STA. 23+44	=	10 UNIT
1' LT. STA. 23+44	=	10 UNIT
14' RT. STA. 23+54	=	8 UNIT
TOTAL	=	122 UNIT

TREE REMOVAL, ACRES

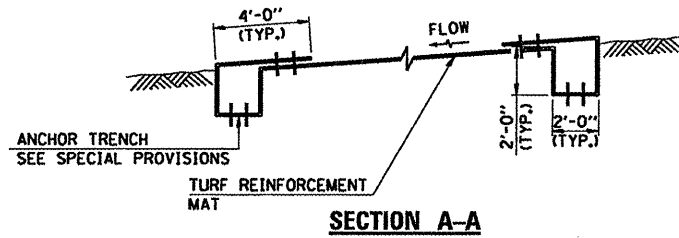
LT. STA. 9+00 TO LT. STA. 16+50	
RT. STA. 14+50 TO RT. STA. 16+50	
LT. STA. 16+50 TO LT. STA. 21+29	
RT. STA. 16+50 TO RT. STA. 20+86	
RT. STA. 22+71 TO RT. STA. 23+07	
LT. STA. 22+81 TO LT. STA. 23+10	
LT. STA. 24+84 TO LT. STA. 26+88	
RT. STA. 23+75 TO RT. STA. 27+50	
TOTAL	= 2.2 ACRES

TREE REMOVAL (OVER 15 UNITS DIAMETER)

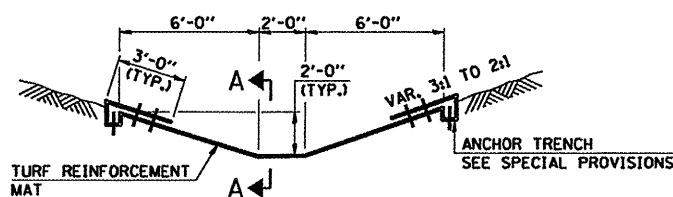
5' LT. STA. 23+61	=	18 UNIT
23.5' LT. STA. 24+68	=	18 UNIT
TOTAL	=	36 UNIT

TREE REPLACEMENT

FOR EACH REMOVED TREE HAVING A DIAMETER OF SIX (6) INCHES OR GREATER, THE CONTRACTOR SHALL PLANT ONE (1) CONTAINER GROWN TREE (SEE SPECIAL PROVISIONS). IN AREAS WHERE TREE REMOVAL IS MEASURED FOR PAYMENT IN ACRES, THE CONTRACTOR SHALL MARK TREES TO BE REMOVED THAT HAVE A DIAMETER GREATER THAN SIX (6) INCHES. PRIOR TO REPLACEMENT TREES BEING ORDERED, THE CONTRACTOR SHALL RECEIVE CONCURRENCE FROM THE ENGINEER IN REGARD TO THE NUMBER OF REPLACEMENT TREES REQUIRED FOR TREE REMOVAL IN AREAS MEASURED FOR PAYMENT IN ACRES.



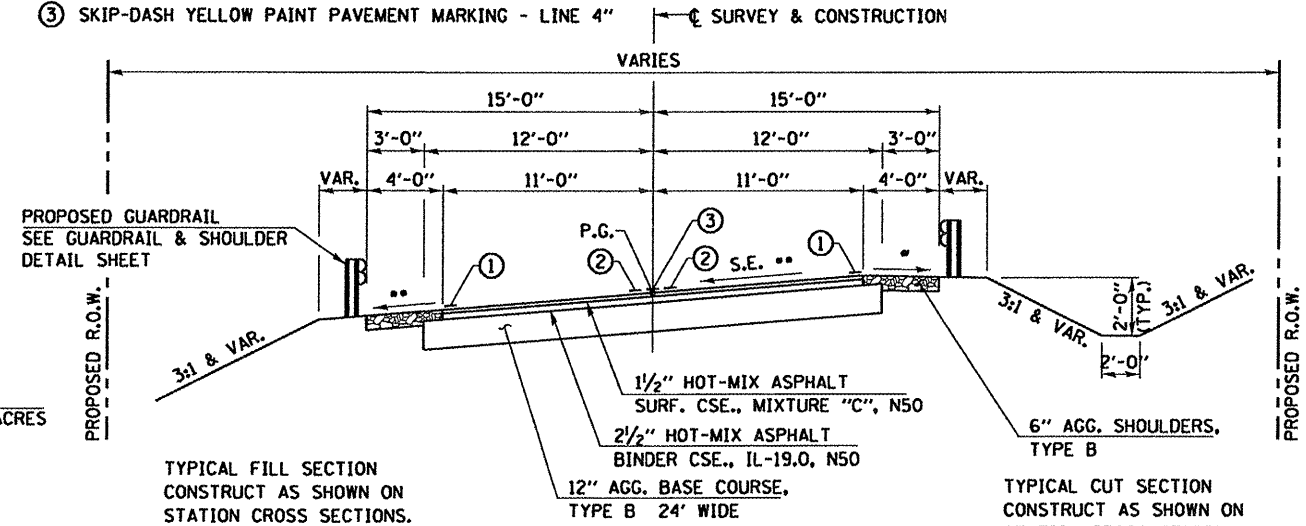
SECTION A-A



TURF REINFORCEMENT MAT

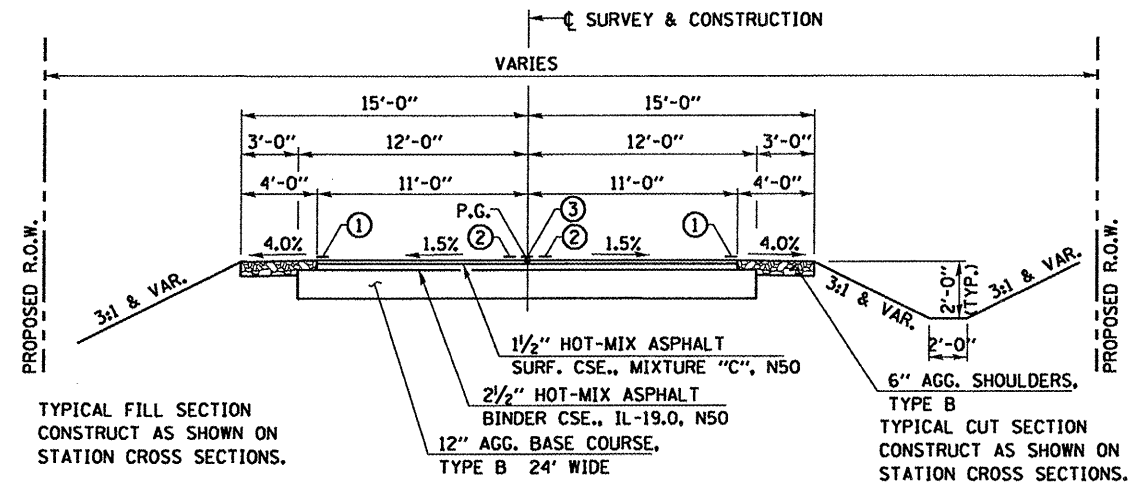
LT. STA. 11+00 TO LT. STA. 14+00	=	935 SQ. YD.
LT. STA. 15+00 TO LT. STA. 15+50	=	195 SQ. YD.
RT. STA. 13+00 TO RT. STA. 15+72	=	852 SQ. YD.
RT. STA. 16+28 TO RT. STA. 20+00	=	1,148 SQ. YD.
LT. STA. 23+50 TO LT. STA. 24+50	=	275 SQ. YD.
LT. STA. 24+50 TO LT. STA. 26+50	=	551 SQ. YD.
LT. STA. 26+50 TO LT. STA. 27+47	=	311 SQ. YD.
LT. STA. 27+87 TO LT. STA. 28+00	=	86 SQ. YD.
TOTAL	=	4,353 SQ. YD.

- ① SOLID WHITE PAINT PAVEMENT MARKING - LINE 4"
- ② SOLID YELLOW PAINT PAVEMENT MARKING - LINE 4"
- ③ SKIP-DASH YELLOW PAINT PAVEMENT MARKING - LINE 4"



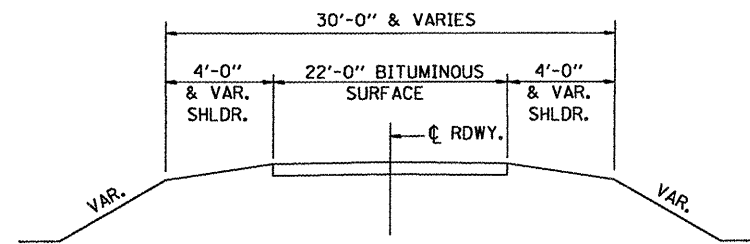
PROPOSED TYPICAL CROSS SECTION

- 2.0% STA. 12+47 TO STA. 18+77
- 8% STA. 12+47 TO STA. 18+77
- S.E. TRANSITION FROM STA. 10+38 TO STA. 12+47 AND FROM STA. 18+77 TO STA. 20+86
- 4.0% STA. 24+94 TO STA. 26+97
- 5.5% STA. 24+94 TO STA. 26+97
- S.E. TRANSITION FROM STA. 23+40 TO STA. 24+94 AND FROM STA. 26+97 TO STA. 28+51



PROPOSED TYPICAL CROSS SECTION

- STA. 9+50 TO STA. 10+38
- STA. 28+51 TO STA. 28+75
- TRANSITION FROM EXISTING ROADWAY TO PROPOSED ROADWAY STA. 9+00 TO STA. 9+50
- TRANSITION FROM PROPOSED ROADWAY TO EXISTING ROADWAY 28+75 TO 29+25



EXISTING TYPICAL CROSS SECTION