

BRIDGE 121 GENERAL NOTES

1. Refer to "BRIDGE GENERAL NOTES" Sheet for additional Timber Bridge GENERAL NOTES.
2. The Contractor shall remove all elements of the existing Timber Decking and properly dispose of it off-site. This includes: Transverse Railroad Ties, Timber Walkway, Steel Anchors, Excess Limestone Blocks, and any other items as applicable to the existing Timber Decking which is necessary to set new Timber Stringers. All items to be removed shall be disposed of in conformance with the requirements of Section 202.03 of the IDOT Standard Specifications. All material and labor necessary to complete this item of work shall be included in the Contract Unit Price for REMOVAL OF EXISTING SUPERSTRUCTURE with no additional compensation.
3. The Contractor shall remove any loose or deteriorated mortar from the existing masonry limestone abutment joints. He shall then clean and tuck point in accordance with the Contract Special Provisions. This work will be paid for as Lump Sum at the Contract Unit Price for MASONRY CLEANING AND TUCK POINTING.
4. All tree removal and selective brush clearing shall be in accordance with the Plans and Special Provisions or as directed by the Engineer. TREE REMOVAL AND SELECTIVE CLEARING will be measured and paid for at the Contract Unit Price for the respective individual items.
5. The Contractor shall furnish and install a brass Name Plate in accordance with the Section 515 of the IDOT Standard Specifications except that it shall be installed with four (4) tamper resistant screws to the top timber bridge rail on the right hand side of the approach end while looking in the direction of increasing Stationing. The plate shall be made of solid brass 3mm thick with imprinted stamp lettering 6mm high. This item will be measured and paid at the Contract Unit Price EACH for NAME PLATE.
6. The existing Steel Bridge shall remain in place including bearings, cross-bracing, and steel stringers. Any cleaning that is necessary to attach the new Timber Decking shall be done in place. A waterproof membrane barrier conforming to the material requirements of Section 1060.09 of the Standard Specifications shall be placed between the timber stringers and steel supports. The cost shall be included in the Unit cost for TREATED TIMBER.
7. Flat washers shall be placed between nuts and treated wood surfaces, and shall be either galvanized or stainless steel. When fastening to plate steel, lock washers shall be used instead of flat washers.
8. The Southwest Wing Wall has loose and dislodged stones. The Contractor shall remove the loose stones and clean the affected area of loose material. He shall then form a repair as shown on the plans. This Repair will be paid as STRUCTURAL REPAIR OF CONCRETE OF THE DEPTH REQUIRED.
9. Two trees approximately 200mm (Unit Diameter) growing from the limestone shall be removed as part of this contract. The cost of the removal shall be incidental to MASONRY CLEANING AND TUCK POINTING.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

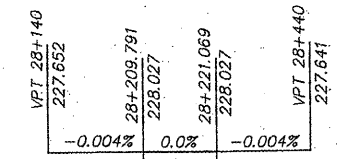
DESIGN LOADING
Pedestrian/Bicycle = 4.07KN/m² (85 psf)
Vehicular = H-10

HIGHWAY CLASSIFICATION
Pecatonica Prairie Path
Functional Class: Multi-Use Path

DESIGN SPECIFICATIONS
2002 AASHTO "Standard Specifications for Highway Bridges" - 17th Edition
1997 AASHTO "Guide Specifications for Design of Pedestrian Bridges"

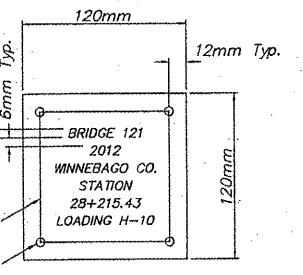
DESIGN STRESSES
FIELD UNITS
f_c = 24 MPa (3,500 psi) - Cast-in-Place Concrete
f_y = 400 MPa (60,000 psi) - Reinforcement
f_t = 250 MPa (36,000 psi) - Fasteners
F_b = 5.9 MPa (850 psi) - Timber Stringers
F_v = 0.7 MPa (100 psi) - Timber Stringers
F_b = 6.7 MPa (975 psi) - Timber Decking

SEISMIC DATA
Seismic Performance Category (SPC) = A
Bedrock Acceleration Coefficient (A) = 0.0325g
Site Coefficient (s) = 1.0

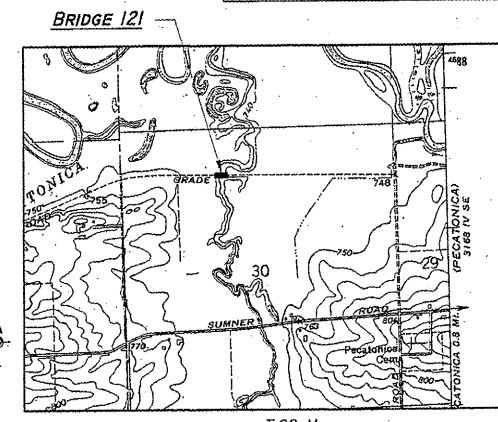


INDEX OF BRIDGE SHEETS

1. Bridge NO. 121 GP&E
2. Bridge Details
3. Bridge Details
4. Bridge Details



NAME PLATE



LOCATION SKETCH



Signature: *A.J.A. Gharami*
Date: 12/14/11
Exp. Date: 11/30/17

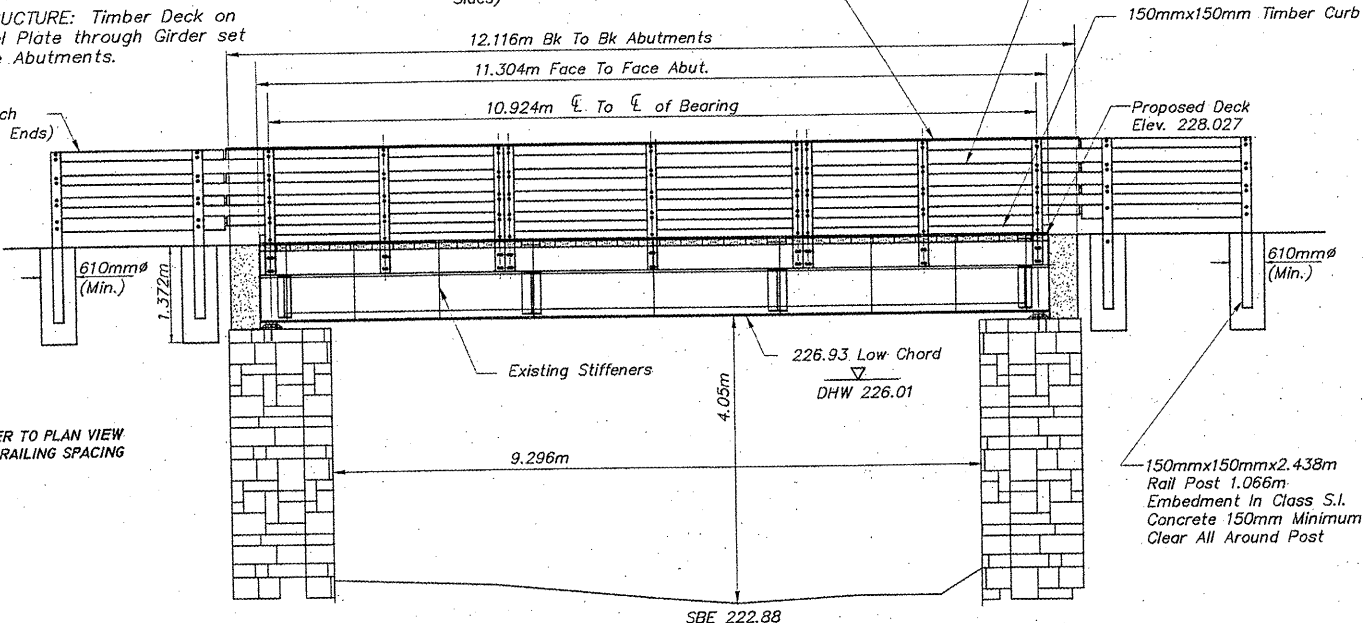
GENERAL PLAN & ELEVATION
BRIDGE 121
OVER THE SUMNER CREEK
WINNEBAGO COUNTY
SECTION NO. 94-00267-00-BT
STATION 28+215.43

SHEET 1 OF 4

BENCHMARK		
NO.	DESCRIPTION	ELEVATION
323	North Bolt on the Southeast corner of Bridge No. 121 Station 28+221.124, 1.542m Rt.	228.024

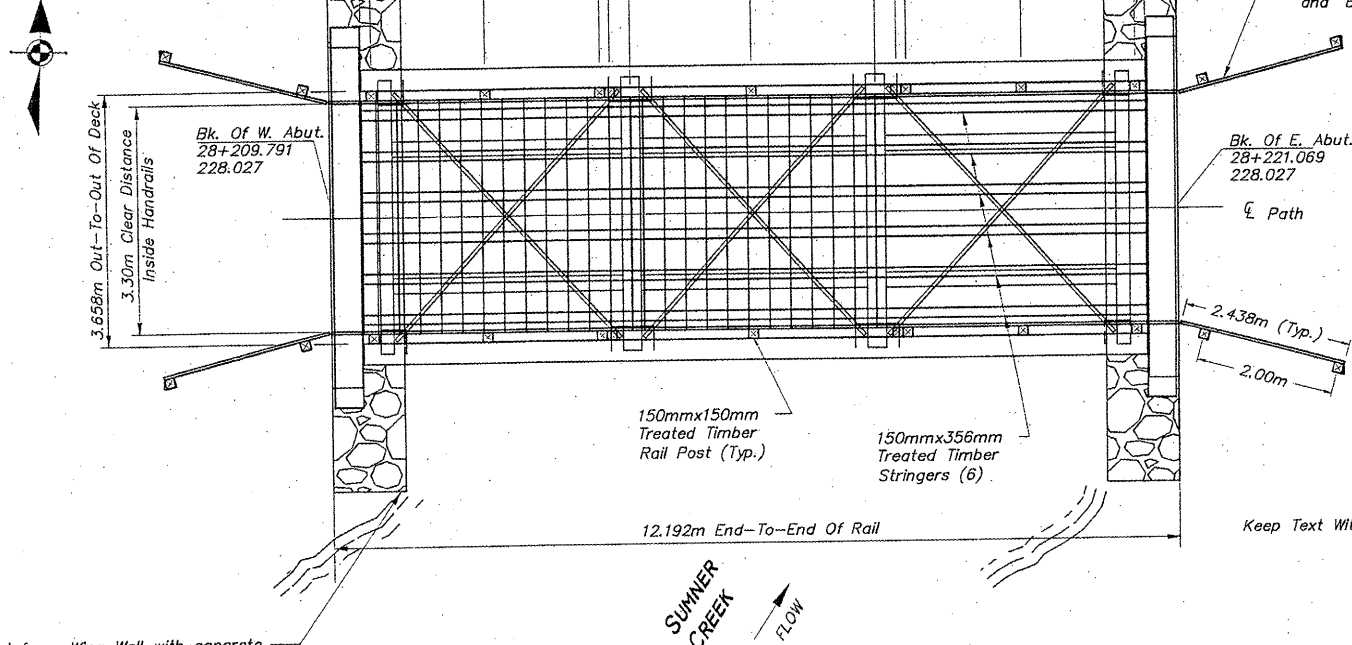
EXISTING STRUCTURE: Timber Deck on Railroad Steel Plate through Girder set on Limestone Abutments.

Timber Approach Handrail (Both Ends)



ELEVATION NOT TO SCALE

CAUTION
OVERHEAD WIRES



PLAN NOT TO SCALE

Reinforce Wing Wall with concrete. This work will be paid as STRUCTURAL REPAIR OF CONCRETE. SEE DETAILS on Sheets 85 and 87 and Special Provisions.

SHEET REVIEW		REVISIONS	
AGENCY	DATE	NO.	ITEM

AGENCY	DATE

NO.	ITEM	DATE

SCALE:	N/A
DRAWN BY:	REK
CHECKED BY:	JJM
DATE:	DECEMBER 12, 2011

McClure
Engineering Associates, Inc.
7282 Argus Drive
Rockford, Illinois 61107-5937
Tel: (815) 398-2332 Fax: (815) 398-2496
Design Firm License: Illinois 184-000816
Copyright 2011 By McClure Engineering Associates, Inc.

BRIDGE NO. 121 GP&E
PECATONICA PRAIRIE PATH
WINNEBAGO COUNTY HIGHWAY DEPARTMENT
SECTION 94-00267-00-BT
FILE:H:\10-042 WINN CO PEC PATH\DESIGN\DRAWINGS\BRIDGES\10-042 B121.DWG
JOB:04-30-10-042

SHEET NO.	84
OF	107