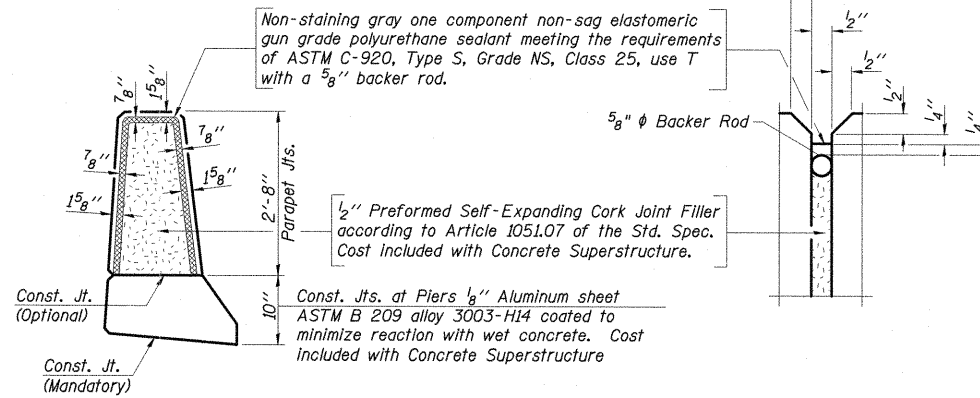


INSIDE ELEVATION OF PARAPET
(North Parapet shown, South opposite)



PARAPET JOINT DETAILS

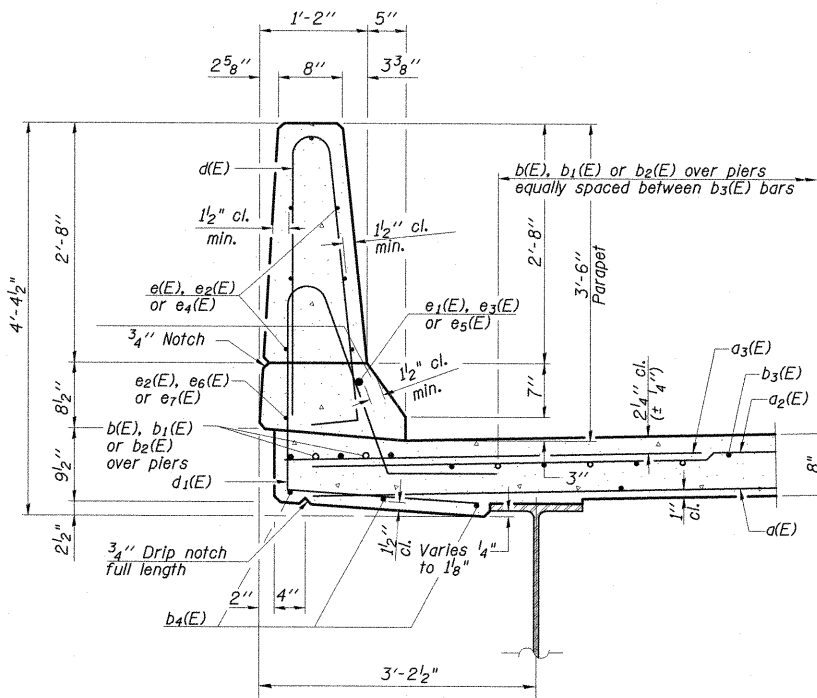
MINIMUM BAR LAP
#4 bar = 2'-7"
#8 bar = 6'-9"

**S.N. 100-0093 (W.B.)
SUPERSTRUCTURE
BILL OF MATERIAL**

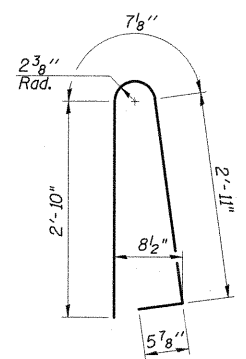
Bar	No.	Size	Length	Shape
a(E)	813	#5	19'-1"	
a ₁ (E)	405	#5	24'-6"	
a ₂ (E)	1327	#5	29'-11"	
a ₃ (E)	1237	#6	6'-6"	
a ₄ (E)	12	#6	26'-6"	
b(E)	112	#6	30'-0"	
b ₁ (E)	112	#6	36'-0"	
b ₂ (E)	112	#6	18'-4"	
b ₃ (E)	684	#5	28'-6"	
b ₄ (E)	825	#5	23'-5"	
d(E)	668	#5	6'-10"	
d ₁ (E)	668	#5	7'-11"	
e(E)	112	#4	15'-2"	
e ₁ (E)	8	#8	34'-3"	
e ₂ (E)	64	#4	19'-8"	
e ₃ (E)	8	#8	19'-8"	
e ₄ (E)	84	#4	16'-8"	
e ₅ (E)	8	#8	30'-6"	
e ₆ (E)	8	#4	27'-5"	
e ₇ (E)	12	#4	22'-4"	
m(E)	12	#6	26'-4"	
m ₁ (E)	32	#6	13'-2"	
m ₂ (E)	14	#6	9'-0"	
m ₃ (E)	4	#6	2'-10"	
m ₄ (E)	24	#8	28'-3"	
s(E)	138	#5	6'-10"	┘
s ₁ (E)	138	#5	12'-8"	┘
v(E)	114	#5	3'-6"	┘
Reinforcement Bars, Epoxy Coated		Pound	156,850	
Concrete Superstructure		Cu. Yd.	591.8	
Bridge Deck Grooving		Sq. Yd.	1,768	
Protective Coat		Sq. Yd.	2,125	
Bar Splicers		Each	114	

**S.N. 100-0094 (E.B.)
SUPERSTRUCTURE
BILL OF MATERIAL**

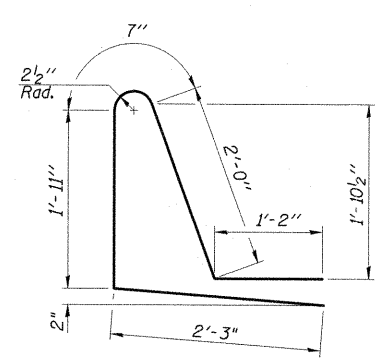
Bar	No.	Size	Length	Shape
a(E)	813	#5	19'-1"	
a ₁ (E)	405	#5	24'-6"	
a ₂ (E)	1327	#5	29'-11"	
a ₃ (E)	1237	#6	6'-6"	
a ₄ (E)	12	#6	26'-6"	
b(E)	112	#6	30'-0"	
b ₁ (E)	112	#6	36'-0"	
b ₂ (E)	112	#6	18'-4"	
b ₃ (E)	684	#5	28'-6"	
b ₄ (E)	825	#5	23'-5"	
d(E)	668	#5	6'-10"	
d ₁ (E)	668	#5	7'-11"	
e(E)	112	#4	15'-2"	
e ₁ (E)	8	#8	34'-3"	
e ₂ (E)	64	#4	19'-8"	
e ₃ (E)	8	#8	19'-8"	
e ₄ (E)	84	#4	16'-8"	
e ₅ (E)	8	#8	30'-6"	
e ₆ (E)	8	#4	27'-5"	
e ₇ (E)	12	#4	22'-4"	
m(E)	12	#6	26'-4"	
m ₁ (E)	32	#6	13'-2"	
m ₂ (E)	14	#6	9'-0"	
m ₃ (E)	4	#6	2'-10"	
m ₄ (E)	24	#8	28'-3"	
s(E)	138	#5	6'-10"	┘
s ₁ (E)	138	#5	12'-8"	┘
v(E)	114	#5	3'-6"	┘
Reinforcement Bars, Epoxy Coated		Pound	156,850	
Concrete Superstructure		Cu. Yd.	592.0	
Bridge Deck Grooving		Sq. Yd.	1,768	
Protective Coat		Sq. Yd.	2,125	
Bar Splicers		Each	114	



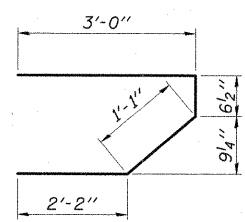
SECTION THRU PARAPET



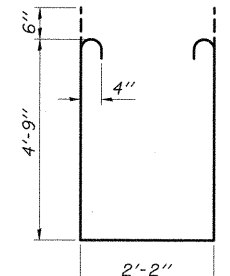
BAR d(E)



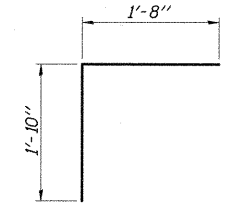
BAR d₁(E)



BAR s(E)



BAR s₁(E)



BAR v(E)

Note:
Slipforming of the parapet is not allowed.

CMT
CRAWFORD MURPHY & TILLY, INC.
CONSULTING ENGINEERS
SPRINGFIELD, IL ■ AURORA, IL ■ ST. LOUIS, MO
ROCKFORD, IL ■ PEORIA, IL ■ CHICAGO, IL

USER NAME = Gary Davis	DESIGNED - MCC	REVISED -
PLOT SCALE =	CHECKED - ATI	REVISED -
PLOT DATE = 1/10/2012	DRAWN - GLD	REVISED -
	CHECKED - ATI	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PARAPET ELEVATION AND DETAILS
STRUCTURE NO. 100-0093 (W.B.) & STRUCTURE NO. 100-0094 (E.B.)**

SHEET NO. S-17 OF S-38 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
331	(IX-1)VB-1	WILLIAMSON	367	183
CONTRACT NO. 98859				

ILLINOIS FED. AID PROJECT

FILE NAME: I:\v\d\0986603\drw\cadd\sheet\structural\plans\shaf - bridge\PARAPET ELEV & DETAIL.dwg