

ITEM 6A

03/09/12 LETTING

TOTAL SHEETS: 38

MH022

MT. HAWLEY AUXILIARY AIRPORT

PEORIA, ILLINOIS

IL. PROJ. NO. 3MY-4168

AIP PROJ. NO. 3-17-0079-B11

EXTEND RUNWAY 18/36 (400' x 60'); CONSTRUCT TAXIWAY TURNAROUND; INCLUDING LIGHTING AND MARKING

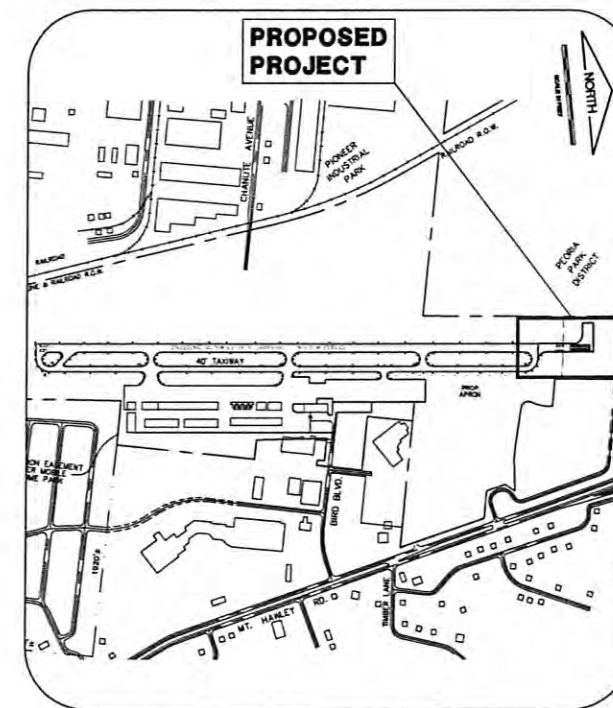
2/3/2012

SUMMARY OF QUANTITIES			
Item Number	DESCRIPTION	UNIT	QUANTITY
AR150510	ENGINEER'S FIELD OFFICE	LS	1
AR150560	TEMPORARY THRESHOLD	LS	1
AR152410	UNCLASSIFIED EXCAVATION	CY	22365
AR156510	SILT FENCE	LF	2100
AR156511	DITCH CHECK	EA	2
AR156520	INLET PROTECTION	EA	1
AR156543	RIPRAP - GRADATION NO. 3	SY	20
AR209604	CRUSHED AGG. BASE COURSE - 4"	SY	69
AR209608	CRUSHED AGG. BASE COURSE - 8"	SY	4414
AR209612	CRUSHED AGG. BASE COURSE - 12"	SY	1100
AR401613	BIT. SURF. CSE. - METHOD I, SUPERPAVE	TON	550
AR401900	REMOVE BITUMINOUS PAVEMENT	SY	28
AR402620	POROUS FRICTION COURSE 5/8"	SY	4175
AR403613	BIT. BASE CSE. - METHOD I, SUPERPAVE	TON	1106
AR602510	BITUMINOUS PRIME COAT	GAL	1766
AR603510	BITUMINOUS TACK COAT	GAL	835
AR620510	PAVEMENT MARKING	SF	11664
AR620900	PAVEMENT MARKING REMOVAL	SF	6665
AR801228	PAVEMENT INSERTS	EA	5
AR801229	GAS LINE PROTECTION	SY	72
AR161510	CLASS C FENCE	LF	529
AR161624	CLASS C GATE - 24'	EA	2
AR161900	REMOVE CLASS C FENCE	LF	453
AR701212	12" CMP	LF	24
AR701512	12" RCP, CLASS IV	LF	92
AR705504	4" PERFORATED UNDERDRAIN	LF	1092
AR705640	UNDERDRAIN CLEANOUT	EA	3
AR752212	METAL END SECTION 12"	EA	2
AR752412	PRECAST REINFORCED CONC. FES 12"	EA	2
AR752850	SPECIAL STRUCTURE	EA	1
AR901510	SEEDING	AC	10
AR904510	SODDING	SY	800
AR908510	MULCHING	AC	10
AR908520	EXCELSIOR BLANKET	SY	8300
AR801226	2-1/2" #10 USE, UG CABLE IN UD	LF	1750
AR125410	MITL - STAKE MOUNTED	EA	12
AR125445	TAXI GUIDANCE SIGN, 5 CHARACTER	EA	1
AR125505	MIRL, STAKE MOUNTED	EA	3
AR125540	MI THRESHOLD LIGHT STAKE MTD	EA	6
AR125964	RELOCATE TAXI GUIDANCE SIGN	EA	1

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08	PROPOSED PLAN AND PROFILE 2
09	TYPICAL SECTIONS 1
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34	TURNAROUND CROSS SECTION 2
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38	DITCH B CROSS SECTION 1



LOCATION MAP



SITE PLAN

DESIGN INFORMATION
 GEOMETRIC DESIGN
 DESIGN GROUP - GROUP 1
 WINGSPAN - UP TO BUT NOT INCLUDING 49 FT.
 DESIGN APPROACH CATEGORY - B
 PAVEMENT STRUCTURE DESIGN DATA
 DEPARTURE WEIGHT = 12,500 LBS.
 SINGLE WHEEL GEAR
 CALL J.U.L.I.E.
 BEFORE EXCAVATING
 1-800-892-0123
 TOWNSHIP: 9 & 10 NORTH
 RANGE: 8 EAST OF 4TH P.M.
 SECTION: 9 & 10 NORTH
 COUNTY: PEORIA
 CIVIL TOWNSHIP: MEDINA RICHWOODS
 WEST SIDE OF MT. HAWLEY RD. 2/3 MI. SOUTH OF
 THE ROUTE 6/88 (MT. HAWLEY RD.) INTERCHANGE

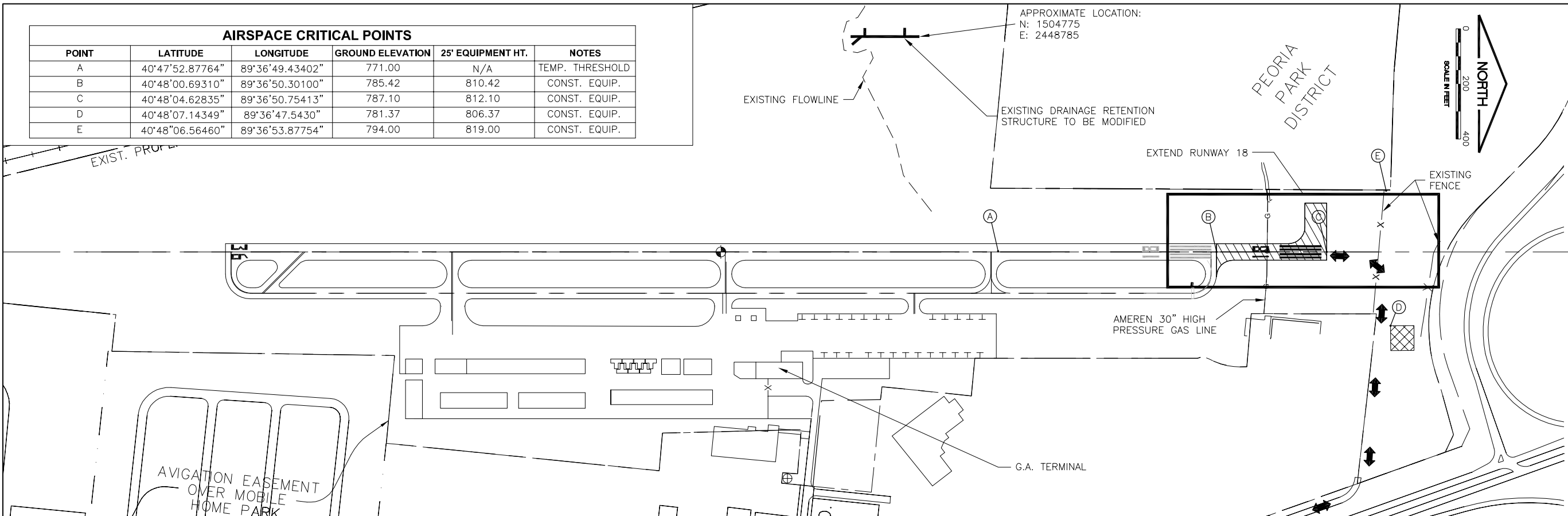


MT. HAWLEY AUXILIARY AIRPORT
 METROPOLITAN AIRPORT AUTHORITY
 OF PEORIA
 APPROVED *Harold Olson*
 DATE January 23, 2012

CMT
 CRAWFORD MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 ■ SPRINGFIELD, IL ■ AURORA, IL ■ ST. LOUIS, MO
 SUBMITTED BY _____
 DATE _____
 CMT JOB NUMBER: 11061-04-00

K:\Mh022\03\1106104\Draw\Sheets
 FILE: COVER.dwg
 UPDATE BY: Jeremy Wasilowski
 PLOT DATE: 2/13/2012 2:03 PM

AIRSPACE CRITICAL POINTS					
POINT	LATITUDE	LONGITUDE	GROUND ELEVATION	25' EQUIPMENT HT.	NOTES
A	40°47'52.87764"	89°36'49.43402"	771.00	N/A	TEMP. THRESHOLD
B	40°48'00.69310"	89°36'50.30100"	785.42	810.42	CONST. EQUIP.
C	40°48'04.62835"	89°36'50.75413"	787.10	812.10	CONST. EQUIP.
D	40°48'07.14349"	89°36'47.5430"	781.37	806.37	CONST. EQUIP.
E	40°48'06.56460"	89°36'53.87754"	794.00	819.00	CONST. EQUIP.



APPROXIMATE LOCATION:
N: 1504775
E: 2448785

K:\MHowley\1106104\Draw\Sheets
FILE: SITE PLAN.dwg
UPDATE BY: Jeremy Wasilewski
PLOT DATE: 2/13/2012 2:05 PM
BASE: MT HAWLEY
Proposed Geometry
ALPSA
1106104-V-VF2D
BASE

MH022

REVISIONS		
NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

GENERAL NOTES

- ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
- THE CONTRACTOR SHALL CONFIRM WITH THE AIRPORT THROUGH THE RESIDENT ENGINEER THAT ALL RUNWAYS, TAXIWAYS, AND APRONS WITHIN THE CONSTRUCTION LIMITS HAVE BEEN CLOSED TO AIRCRAFT TRAFFIC PRIOR TO THE START OF ANY CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL HAVE PERSONNEL CAPABLE OF MONITORING AIRCRAFT COMMUNICATIONS. THE CONTRACTOR SHALL PROVIDE HIS OWN RADIOS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY COMPLETED RADIO TRAINING TO MONITOR THESE RADIOS.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS AND STOCKPILES SHALL BE 25' ABOVE GROUND ELEVATION.
- BROKEN ASPHALT, PCC AND OTHER CONSTRUCTION DEBRIS SHALL BE DISPOSED OF BY THE CONTRACTOR OFF OF AIRPORT PROPERTY UNLESS OTHERWISE DIRECTED BY THE AIRPORT.
- VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 45' FROM THE CENTERLINE OF ACTIVE TAXIWAYS OR 125' FROM THE CENTERLINE OF ACTIVE RUNWAYS.
- ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS OUTSIDE OF THE DESIGNATED GRADING LIMITS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT.
- THE CONTRACTOR SHALL CONTINUOUSLY CLEAN CONSTRUCTION AREAS WHICH WILL BE OPENED TO AIR TRAFFIC.
- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
- CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
 - THE CONTRACTOR'S ACCESS TO WORK SHALL BE AS SHOWN IN THE PLANS.
 - THE CONTRACTOR SHALL USE AN EXISTING GATE(S) FOR ACCESS TO THE AIRFIELD. THE CONTRACTOR SHALL INSTALL AND MAINTAIN A HEAVY-DUTY PADLOCK ON THE ACCESS GATE. HE SHALL PROVIDE KEYS FOR HIS PADLOCK TO THE RESIDENT ENGINEER, AND AIRPORT MANAGER. NO ADDITIONAL KEYS ARE TO BE DISTRIBUTED UNLESS AUTHORIZED BY THE RESIDENT ENGINEER.
 - CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
 - DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS.
 - THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE(S) UPON LEAVING THE SITE.
 - ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS NOT TO VIOLATE AIRPORT PART 77 SURFACES, OR RUNWAY AND TAXIWAY SAFETY AREAS.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE USED BY HIS VEHICLES ACCESSING THE WORK SITE OR DEPARTING THE WORK SITE IMMEDIATELY FOLLOWING SAID VEHICLE.

LEGEND

- CONTRACTOR'S ACCESS ROUTE
- PROPOSED WORK AREA
- NEW RUNWAY EXTENSION
- CONTRACTOR'S STAGING, STORAGE AND PARKING AREA
- EXISTING GAS LINE
- EXISTING FENCE LINE

BASELINE INFORMATION

BASELINE	STATION	NORTHING	EASTING
RWY. 18/36	25+00	1502213.4120	2449938.1501
RWY. 18/36	55+00	1505200.2852	2449657.8133
RWY. 18/36	69+50	1506643.9405	2449522.3171
TURNAROUND	200+00	1506225.7783	2449561.5643
TURNAROUND	203+20	1506195.8757	2449242.9645
TXY	125+00	1502227.4289	2450087.4938
TXY	169+50	1506657.9574	2449671.6608

CONTROL POINTS

MARKER	ELEVATION	NORTHING	EASTING
IRON PIN	781.486	1505774.480	2449684.210
IRON PIN	768.184	1504992.677	2449757.697
IRON PIN	766.750	1504023.520	2449871.277
IRON PIN	768.039	1503024.277	2449977.925

**UNICOM FREQUENCY - 122.7
MAXIMUM HEIGHT OF EQUIPMENT - 25'**

MT. HAWLEY AUXILIARY AIRPORT
PEORIA, ILLINOIS

RUNWAY 18 EXTENSION
SITE PLAN

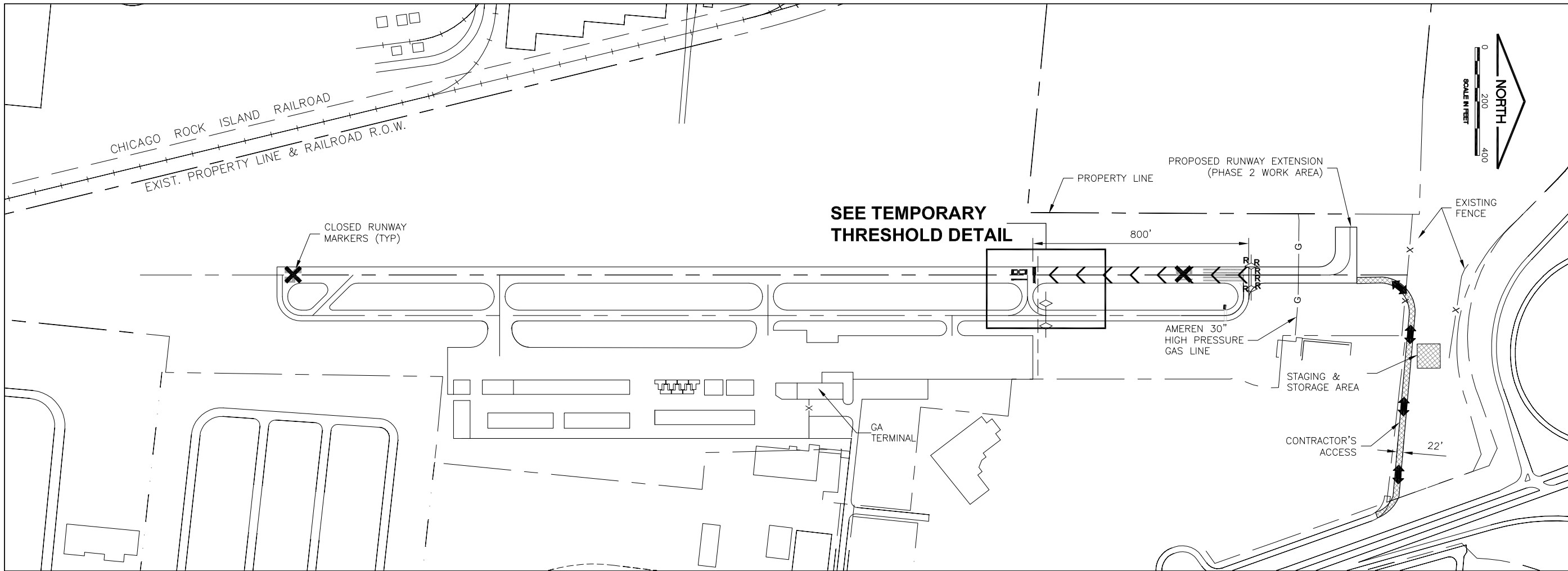
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DATE:	2/3/2012
JOB No:	11061-04-00
IL. PROJ. NO.	3MY-4168
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SHEET	02 OF 38 SHEETS

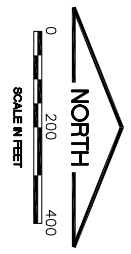
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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



SEE TEMPORARY THRESHOLD DETAIL



**MT. HAWLEY AUXILIARY AIRPORT
 PEORIA, ILLINOIS**
RUNWAY 18 EXTENSION
CONSTRUCTION ACTIVITY PLAN PHASE 1

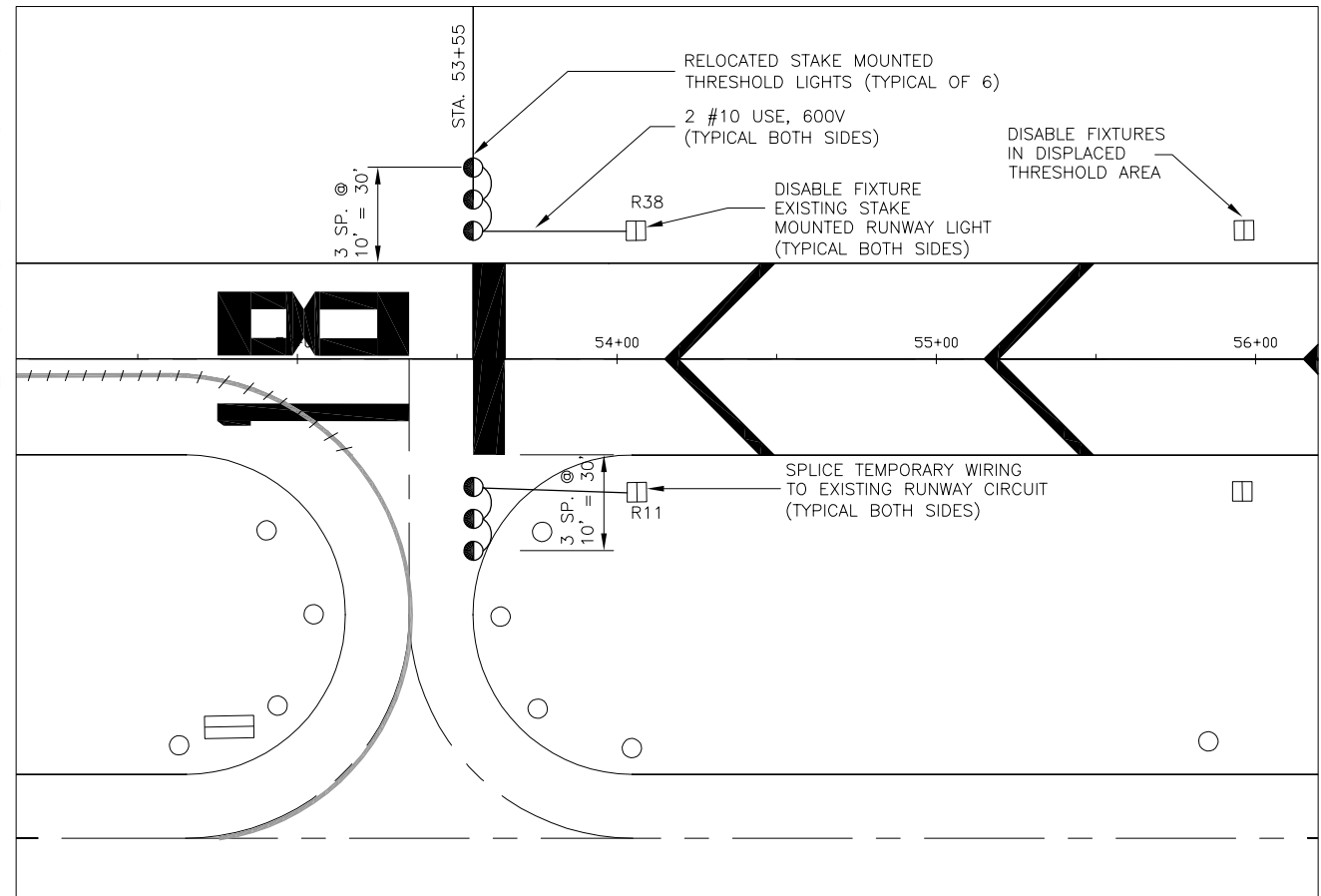
PHASE 1 NOTES:

- BEFORE ANY WORK CAN BE STARTED WITHIN THE EXISTING RUNWAY SAFETY AREA, RUNWAY SHOULD BE CLOSED AND THE TEMPORARY THRESHOLD SHALL BE INSTALLED AT STA. 53+55 AS DETAILED ON THIS SHEET.
- THE FOLLOWING MARKINGS AT THE NORTH END OF RUNWAY 18/36 SHALL BE REMOVED, BY SANDBLASTING, WATERBLASTING OR OTHER APPROVED NON-DESTRUCTIVE METHOD, SIMULTANEOUSLY WITH THE INSTALLATION OF THE TEMPORARY THRESHOLD:
 - NUMERAL 18
 - THRESHOLD MARKINGS
 - THREE NORTHERN MOST CENTERLINE STRIPES
- THE COST OF REMOVING THE EXISTING MARKINGS SHALL BE MEASURED AND PAID UNDER ITEM AR620900 - PAVEMENT MARKING REMOVAL AND SHALL NOT BE CONSIDERED PART OF THE TEMPORARY THRESHOLD ITEM.
- THE TEMPORARY MARKINGS SHALL MEET THE FOLLOWING REQUIREMENTS:
 - MARKING MATERIALS SHALL BE ONE COAT OF PAINTED MARKINGS WITH GLASS BEADS.
 - COLORS FOR TEMPORARY MARKINGS SHALL BE AS FOLLOWS:
 - WHITE FOR NUMERAL 18 AND THRESHOLD BAR.
 - YELLOW FOR CHEVRONS
- EXISTING THRESHOLD LIGHTING SHALL BE RELOCATED TO LOCATION OF THE TEMPORARY THRESHOLD. THE LIGHTS SHALL HAVE THE STAKES REMOVED AND BE MOUNTED ON A 2"x12" AND MOUNTED TO THE PAVEMENT WITH A NON DESTRUCTIVE METHOD.
- PLACEMENT AND REMOVAL OF TEMPORARY THRESHOLD MARKINGS AND LIGHTING SHALL ONLY BE DONE WHEN THE RUNWAY IS CLOSED.
- AIRPORT SHALL APPROVE DATE FOR RUNWAY CLOSURES TO BOTH CONSTRUCT AND REMOVE TEMPORARY THRESHOLD.
- THE CONTRACTOR SHALL PROVIDE 10 WORKING DAYS NOTICE TO AIRPORT PRIOR TO CLOSURE.
- PUBLISHED APPROACH TO RUNWAY 18, NOT AVAILABLE TO AIR TRAFFIC WHEN TEMPORARY THRESHOLD IS IN PLACE.

- PRIOR TO INSTALLING THE TEMPORARY THRESHOLD, THE CONTRACTOR SHALL CLOSE THE RUNWAY BY PLACING "CLOSED RUNWAY MARKER" AT EACH RUNWAY END.
- THE CONTRACTOR SHALL HAVE 36 CONSECUTIVE HOURS TO INSTALL THE TEMPORARY THRESHOLD INCLUDING LIGHTING, MARKING, AND REMOVAL OF THE EXISTING MARKINGS.
- AFTER ALL THE EXISTING MARKINGS HAVE BEEN REMOVED AS SPECIFIED AND THE TEMPORARY MARKINGS AND LIGHTS ARE IN PLACE, RUNWAY 18/36 SHALL BE RE-OPENED TO AIR TRAFFIC.
- ALL MARKING REMOVALS, TEMPORARY MARKING, LIGHTING RELOCATION, AND CABLING AND ANY OTHER INCIDENTALS NECESSARY TO COMPLETE THE CONSTRUCTION OF THE TEMPORARY THRESHOLD SHALL BE CONSIDERED INCIDENTAL TO THE TEMPORARY THRESHOLD ITEM AND SHALL NOT BE MEASURED SEPARATELY FOR PAYMENT.
- RELOCATE AMBER LENSES, IF THERE, TO ONE-HALF THE OPEN RUNWAY LENGTH.

LEGEND

	CONSTRUCTION SETBACK LINE
	CONTRACTOR'S ACCESS ROUTE
	TYPE 1 BARRICADES WITH OMNIDIRECTIONAL FLASHING RED LIGHTS & 20"x20" ORANGE FLAGS
R	RELOCATE THRESHOLD LIGHTS TEMPORARILY
	CLOSED RUNWAY MARKERS
	EXISTING STAKE MOUNTED RUNWAY LIGHT
	PROPOSED TEMPORARY THRESHOLD LIGHT
X	EXISTING FENCE LINE
G	EXISTING GAS LINE
	NEW CONTRACTOR'S ACCESS ROAD AND STAGING/STORAGE AREAS



TEMPORARY THRESHOLD DETAIL

SCALE: 1" = 30'-0"

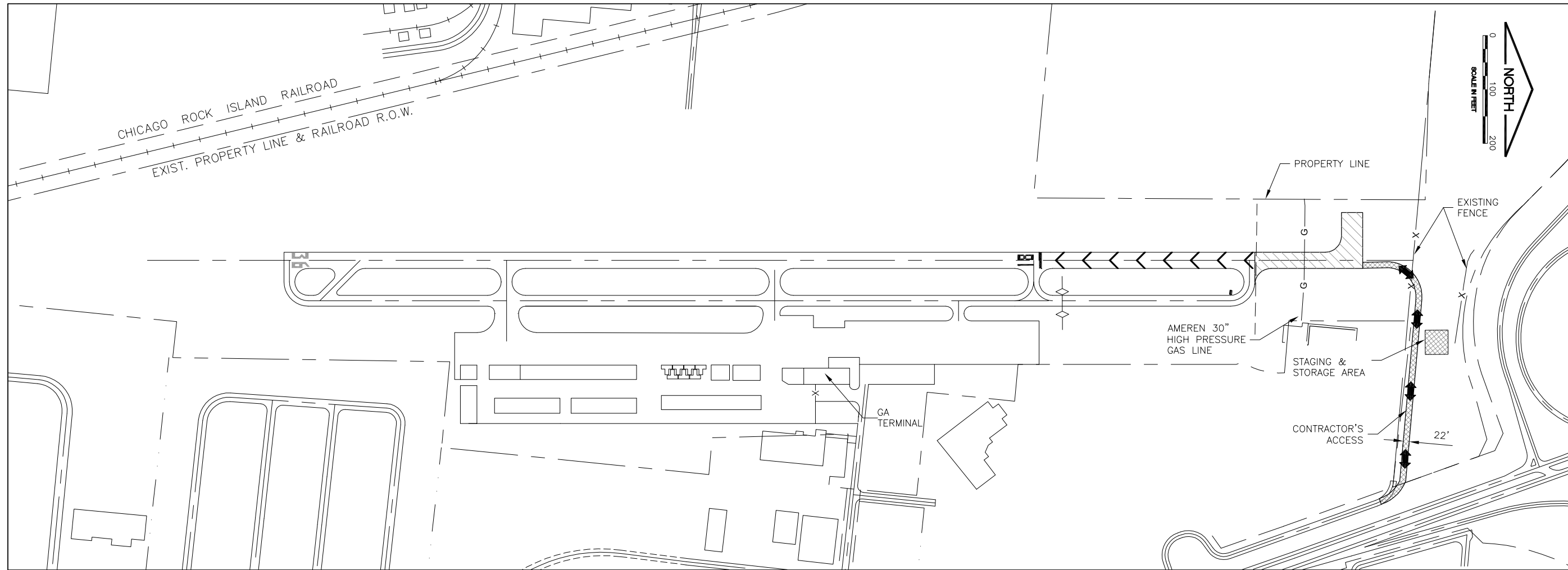
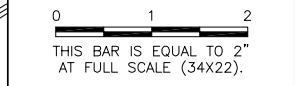
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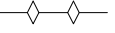
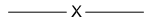
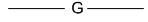

REVISIONS		
NUMBER	BY	DATE



PHASE 2 NOTES:


1. PHASE 2 CONSTRUCTION SHALL CONSIST OF THE FOLLOWING ITEMS:
 - 1.1. REMOVAL OF EXISTING FENCE AND INSTALLATION OF TEMPORARY AND NEW PERMANENT FENCE.
 - 1.2. CONSTRUCTION OF RUNWAY 18 EXTENSION AND TURNAROUND.
 - 1.3. RELOCATE EXISTING "18" TAXIWAY GUIDANCE SIGN TO NEW LOCATION ON TURNAROUND.
 - 1.4. INSTALLATION OF NEW "36-18" TAXIWAY GUIDANCE SIGN TO BE LOCATED AT EXISTING "18" LOCATION ON TAXIWAY.
 - 1.5. INSTALLATION OF NEW LIGHTS ON RUNWAY EXTENSION AND TURNAROUND; INCLUDING RUNWAY EDGE LIGHTS, TAXIWAY EDGE LIGHTS, AND THRESHOLD LIGHTS.
2. PRIOR TO THE COMMENCEMENT OF CONSTRUCTION, THE CONTRACTOR SHALL REVIEW THE LOCATION OF THE DESIGNATED ACCESS ROUTE AND STAGING AREAS. NO TIME EXTENSION OR DELAYS TO THE PROJECT WILL BE ACCEPTED FOR LACK OF ADEQUATE ACCESS TO THE SITE. IF NECESSARY, THE CONTRACTOR SHALL CONSTRUCT A TEMPORARY HAUL ROAD TO PROVIDE SUFFICIENT ACCESS TO THE SITE EVEN DURING ADVERSE WEATHER.
3. THE CONSTRUCTION AND REMOVAL OF ALL TEMPORARY HAUL ROUTES/ACCESS ROUTES SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
4. A TEMPORARY METAL PIPE SHALL BE PLACED AS DETAILED ON SHEET 12.

LEGEND

-  WORK AREA
-  CONSTRUCTION SETBACK LINE
-  CONTRACTOR'S ACCESS ROUTE
-  TYPE 1 BARRICADES WITH OMNIDIRECTIONAL FLASHING RED LIGHTS & 20"x20" ORANGE FLAGS
-  EXISTING FENCE LINE
-  EXISTING GAS LINE
-  NEW CONTRACTOR'S ACCESS ROAD AND STAGING/STORAGE AREAS

**MT. HAWLEY AUXILIARY AIRPORT
 PEORIA, ILLINOIS**

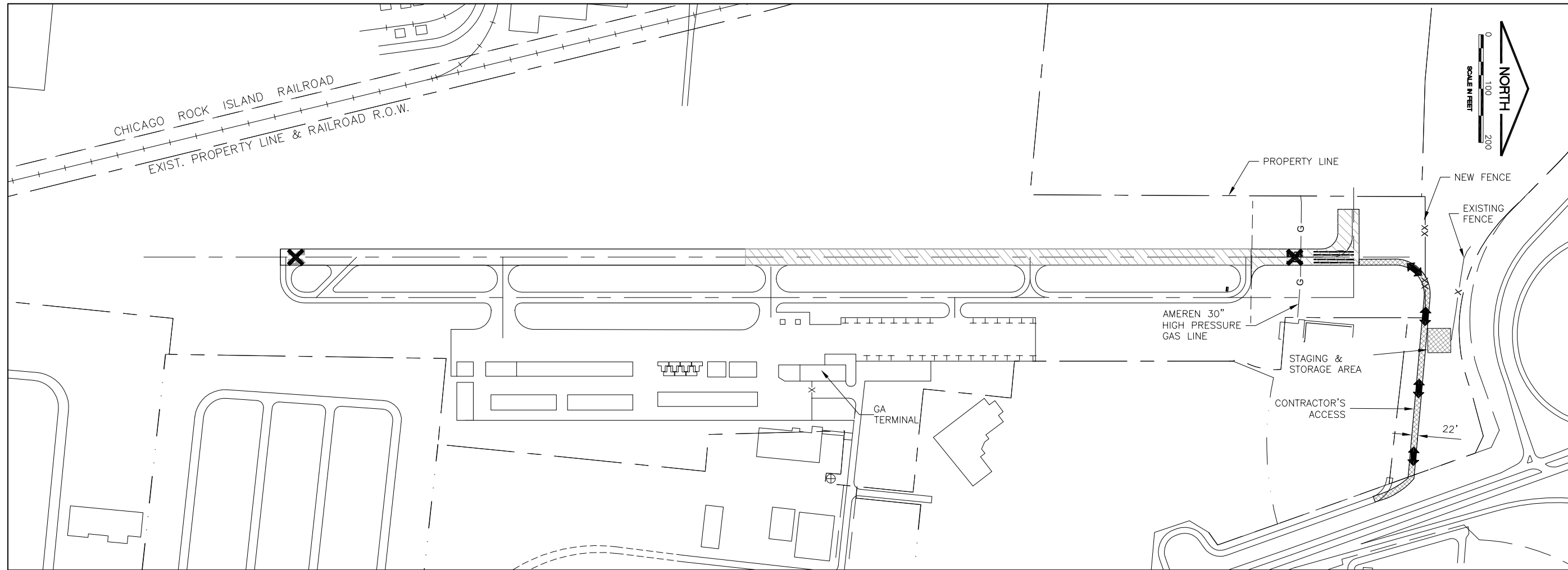
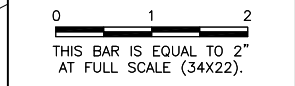
**RUNWAY 18 EXTENSION
 CONSTRUCTION ACTIVITY PLAN PHASE 2**

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SHEET	04 OF 38 SHEETS

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PHASE 3 NOTES:

- AIRPORT SHALL APPROVE DATE FOR RUNWAY CLOSURE TO REMOVE TEMPORARY THRESHOLD.
- THE CONTRACTOR SHALL PROVIDE 10 WORKING DAYS NOTICE TO AIRPORT PRIOR TO CLOSURE.
- THE CONTRACTOR SHALL CLOSE THE RUNWAY BY PLACING "CLOSED RUNWAY MARKER" AT EACH RUNWAY END.
- THE CONTRACTOR SHALL HAVE 36 CONSECUTIVE HOURS TO REMOVE THE TEMPORARY THRESHOLD AND OPEN THE RUNWAY.
- THE COST OF FURNISHING, INSTALLING, MAINTAINING, AND REMOVING THE TEMPORARY THRESHOLD SHALL BE PAID FOR UNDER ITEM 150560 TEMPORARY THRESHOLD.
- ALL EDGE LIGHTING AND GUIDANCE SIGNS WILL BE OPERATIONAL BEFORE THE RUNWAY IS OPENED.
- RELOCATION OF THE THRESHOLD LIGHTS SHALL BE PAID FOR ONLY ONCE, ANY EXTRA WORK ASSOCIATED WITH USING THE EXISTING THRESHOLD LIGHTS FOR THE TEMPORARY THRESHOLD SHALL BE CONSIDERED INCIDENTAL TO ITEM 150560 TEMPORARY THRESHOLD.
- RUNWAY 18-36 SHALL BE CLOSED FOR THE DURATION OF THIS PHASE. UPON COMPLETION OF THE PROPOSED PAVEMENT MARKING AND THE REMOVAL OF THE TEMPORARY THRESHOLD, THE RUNWAY SHALL BE REOPENED TO AIR TRAFFIC.

LEGEND

	WORK AREA
	CONSTRUCTION SETBACK LINE
	CONTRACTOR'S ACCESS ROUTE
	TYPE 1 BARRICADES WITH OMNIDIRECTIONAL FLASHING RED LIGHTS & 20"x20" ORANGE FLAGS
	CLOSED RUNWAY MARKERS
	FENCE LINE
	GAS LINE
	NEW CONTRACTOR'S ACCESS ROAD

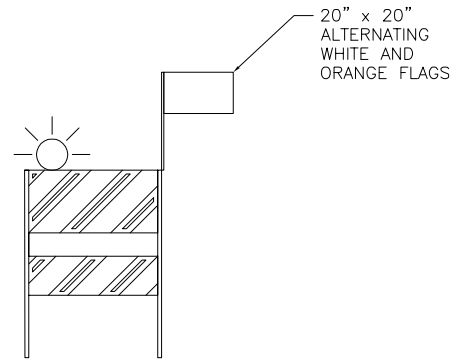
**MT. HAWLEY AUXILIARY AIRPORT
 PEORIA, ILLINOIS**

RUNWAY 18 EXTENSION

CONSTRUCTION ACTIVITY PLAN PHASE 3

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SHEET	05 OF 38 SHEETS

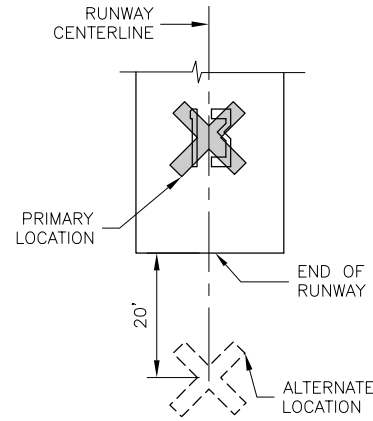


LIGHT BARRICADE DETAIL

N.T.S.

NOTES:

1. LIGHTS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
2. SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
4. PLACE AT 10' INTERVALS.

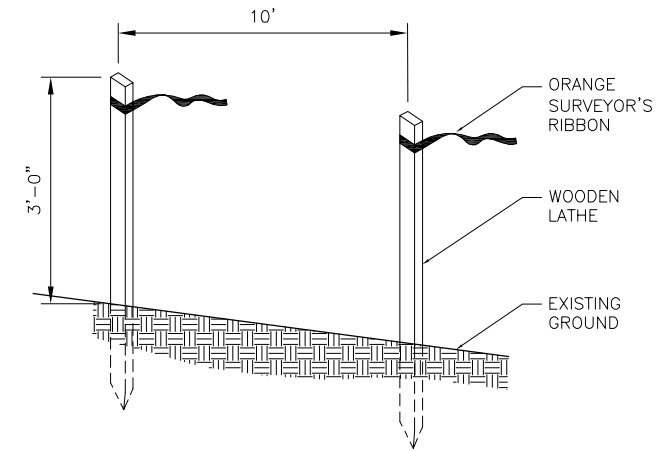


CLOSED RUNWAY MARKER DETAIL

N.T.S.

NOTES:

1. MARKERS SHALL BE PORTABLE, LIGHTED RUNWAY CLOSURE MARKERS IN ACCORDANCE WITH FAA AC150/5345-55 FOR FAA-L893 AND MANUFACTURER MUST BE ON THE FAA APPROVED LIST.
2. AIRPORT SHALL PROVIDE AND MAINTAIN MARKERS THROUGHOUT THE DURATION OF CLOSURE.
3. COST OF INSTALLING, MAINTAINING, FUELING, RELOCATING, AND REMOVING MARKERS SHALL BE INCIDENTAL TO THE RUNWAY CLOSURE MARKER ITEM.
4. MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS. WHEN CONSTRUCTION OPERATIONS CONFLICT, THE CLOSURE MARKERS SHALL BE MOVED TO THE ALTERNATE LOCATION.



CONSTRUCTION SETBACK LINE DETAIL

N.T.S.

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NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

**MT. HAWLEY AUXILIARY AIRPORT
 PEORIA, ILLINOIS**

RUNWAY 18 EXTENSION

CONSTRUCTION ACTIVITY NOTES AND DETAILS

CONSTRUCTION ACTIVITY PLAN GENERAL NOTES

1. THE CONTRACTOR SHALL PLAN AND CONDUCT HIS WORK SO AS TO NOT INTERFERE OR HINDER THE PROGRESS OR WORK BEING PERFORMED BY OTHER CONTRACTORS.
2. THE TIMELY PROSECUTION OF THE OVERALL PROJECT IS DEPENDENT UPON THE PROPER COORDINATION BETWEEN CONTRACTORS.
3. IT SHALL BE FULLY UNDERSTOOD BY THE CONTRACTOR THAT THE PROSECUTION OF THE OVERALL PROJECT ARE THE GOVERNING CRITERIA FOR RESOLVING CONFLICTS WHICH MAY ARISE BETWEEN HIS SCHEDULE AND THE SCHEDULE OF OTHER CONTRACTORS.
4. WHEN CONFLICTS ARISE, RESOLUTION OF SUCH CONFLICTS WILL BE MADE BY THE AIRPORT THROUGH THE RESIDENT ENGINEER IN THE BEST INTEREST OF THE AIRPORT.
5. DELAYS, CHANGES IN SCHEDULING OR THE EXPEDITION OF WORK UNDER THIS CONTRACT TO PROVIDE FOR THE TIMELY PROSECUTION OF THE PROJECT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
6. CONTRACTOR'S STAGING, STORAGE, AND PARKING WILL BE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLANS.
7. THE CONTRACTOR SHALL PLACE ALL BARRICADES, CONSTRUCTION SETBACK LINES, AND EROSION CONTROL ITEMS AS SHOWN PRIOR TO INITIATING WORK IN EACH PHASE. ALL COSTS TO FURNISH, INSTALL, AND MAINTAIN THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
8. CONSTRUCTION PHASING IS OF CRITICAL IMPORTANCE TO THE AIRPORT FOR THIS PROJECT.
9. CONSTRUCTION RELATED ITEMS REQUIRING THE CLOSURE OF RUNWAYS AND/OR TAXIWAYS SHALL REQUIRE CLOSE COORDINATION WITH THE AIRPORT AND FAA. NO EXTENSION TO CONTRACT TIME WILL BE GIVEN FOR DELAYS CAUSED BY LACK OF ADEQUATE COORDINATION. THE AIRPORT SHALL REQUIRE 7 DAYS NOTIFICATIONS PRIOR TO THE CLOSURE OF RUNWAY AND/OR TAXIWAYS.

SECURITY NOTES

1. MAINTAINING THE SECURITY REQUIREMENTS OF THE AIRPORT SHALL BE A PRIMARY CONCERN FOR THE CONTRACTOR.
2. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTAINING AIRPORT SECURITY BY SUPERVISING OPENINGS OR MAINTAINING THE AIRPORT PERIMETER FENCE LINE AT ALL TIMES DURING THE COURSE OF THE WORK.
3. FINES CAN BE LEVIED AGAINST THE CONTRACTOR BY THE TRANSPORTATION SECURITY ADMINISTRATION (TSA) FOR NEGLIGENCE IF THE AIRPORT SECURITY IS COMPROMISED AND THE AIRPORT PERIMETER FENCE LINE IS NOT MAINTAINED AS SPECIFIED ABOVE. FINES CAN ALSO BE LEVIED AGAINST THE CONTRACTOR FOR FAILURE TO COOPERATE WITH THE AIRPORT MANAGEMENT AS REQUIRED TO MAINTAIN AIRPORT SECURITY.

CONTRACTOR'S RESPONSIBILITY FOR UTILITY SERVICE

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

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JOB No: 11061-04-00

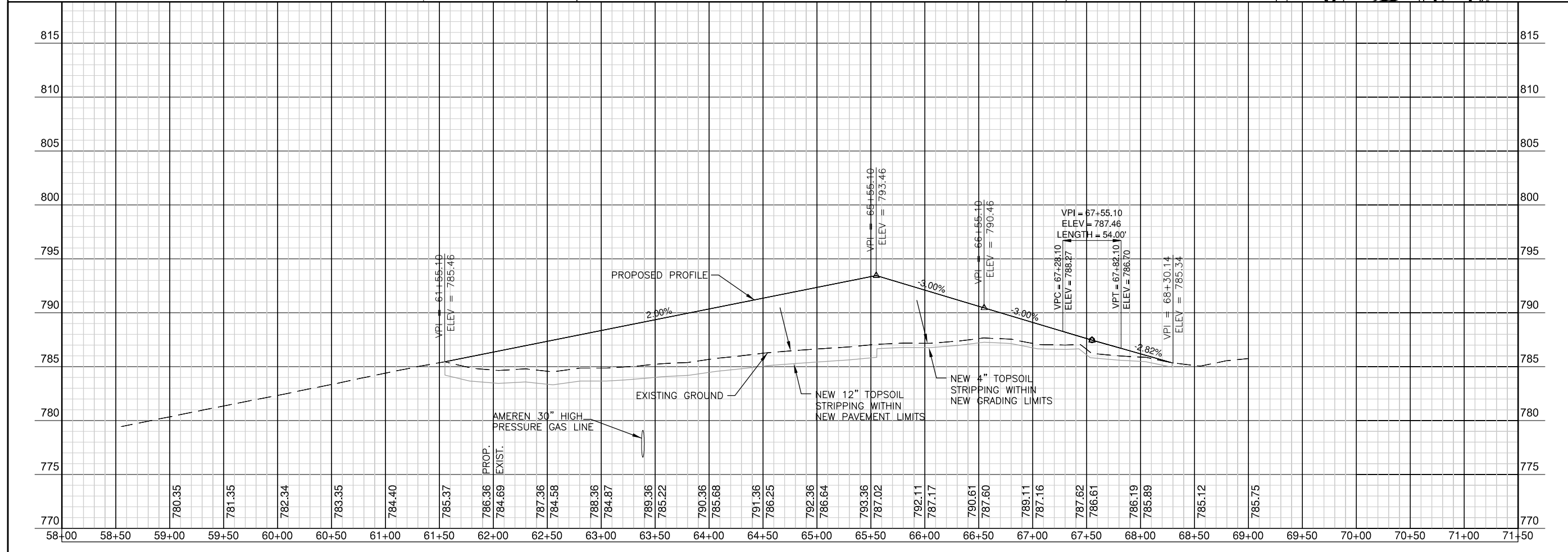
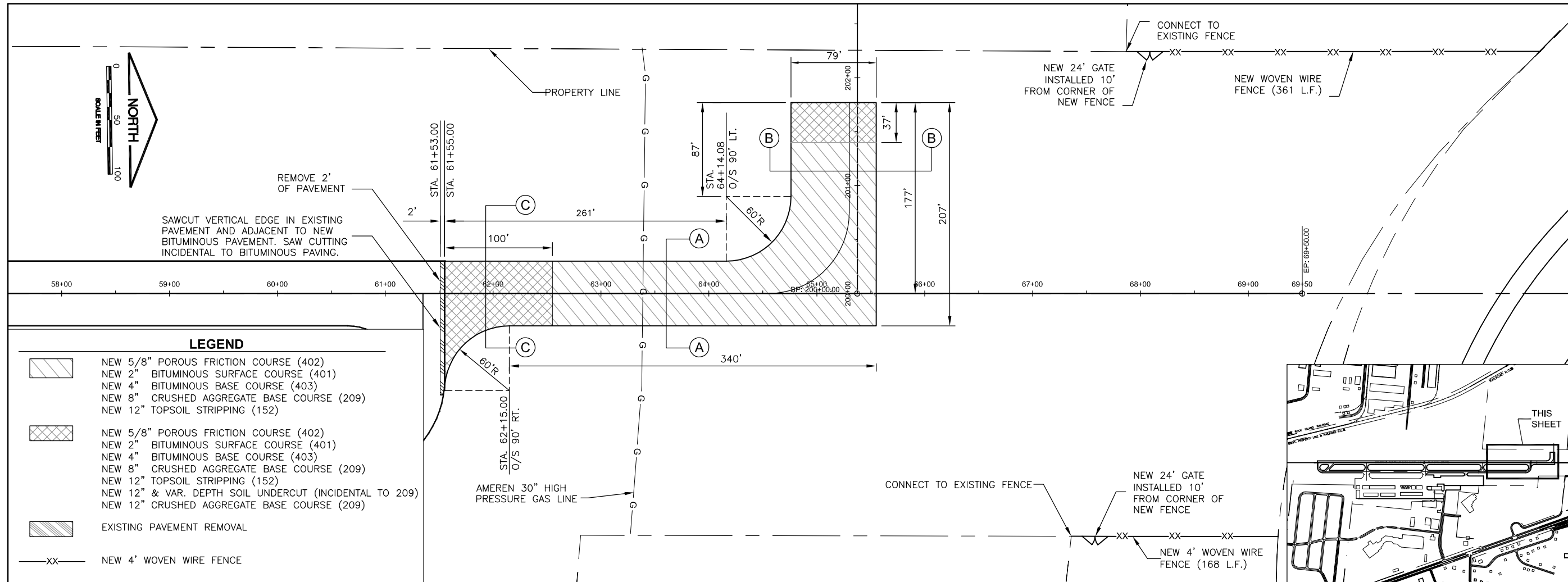
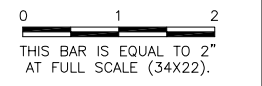
IL PROJ. NO. 3MY-4168

AIP PROJ. NO. 3-17-0079-B11

SHEET 06 OF 38 SHEETS

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**MT. HAWLEY AUXILIARY AIRPORT
 PEORIA, ILLINOIS**

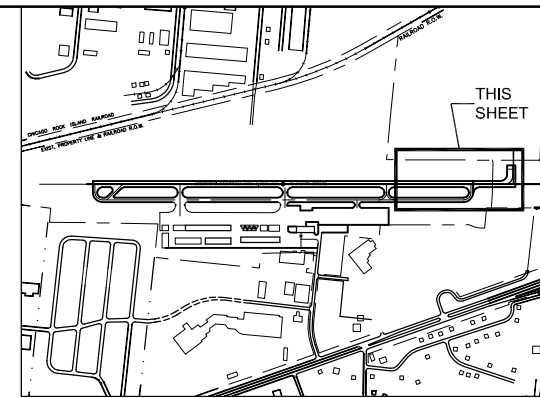
RUNWAY 18 EXTENSION

PROPOSED PLAN AND PROFILE 1

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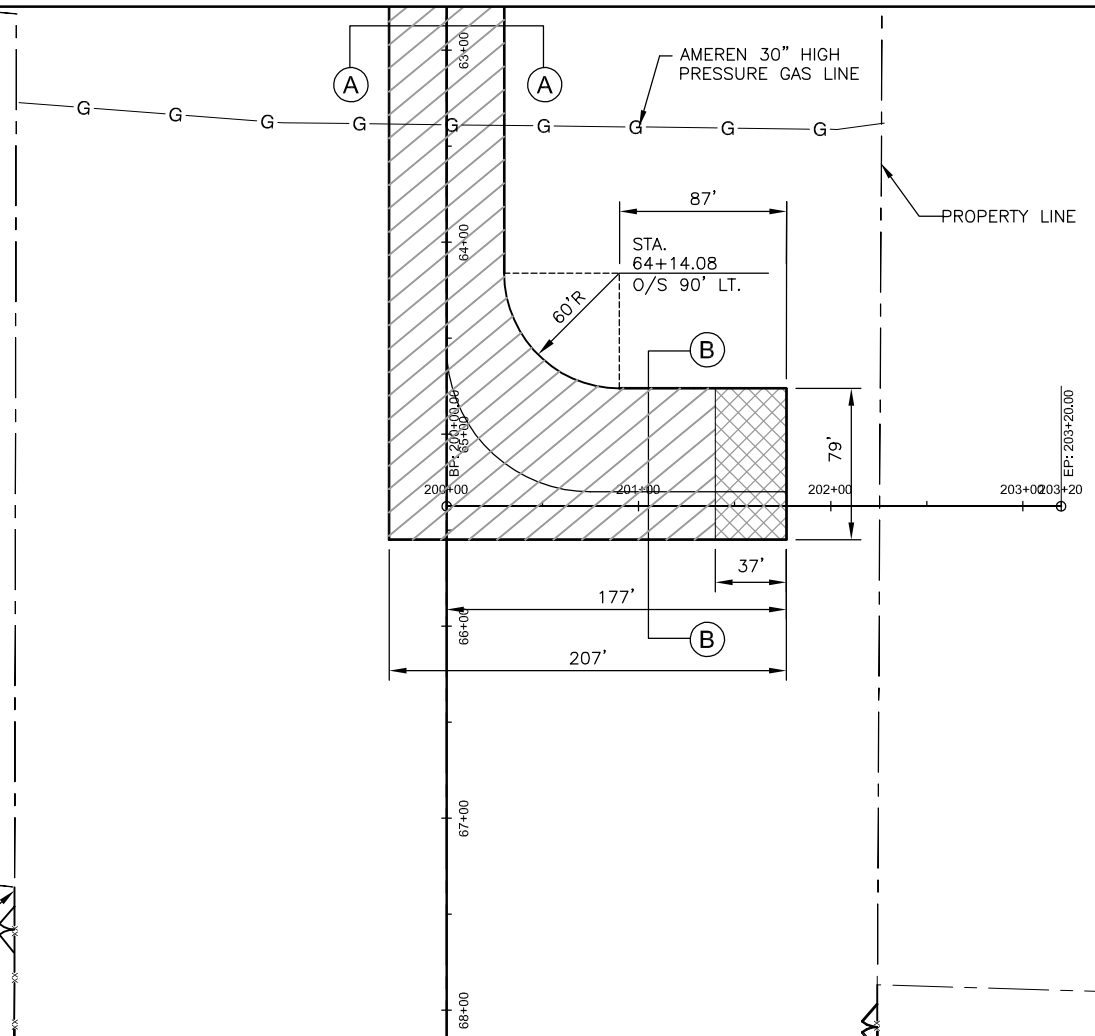
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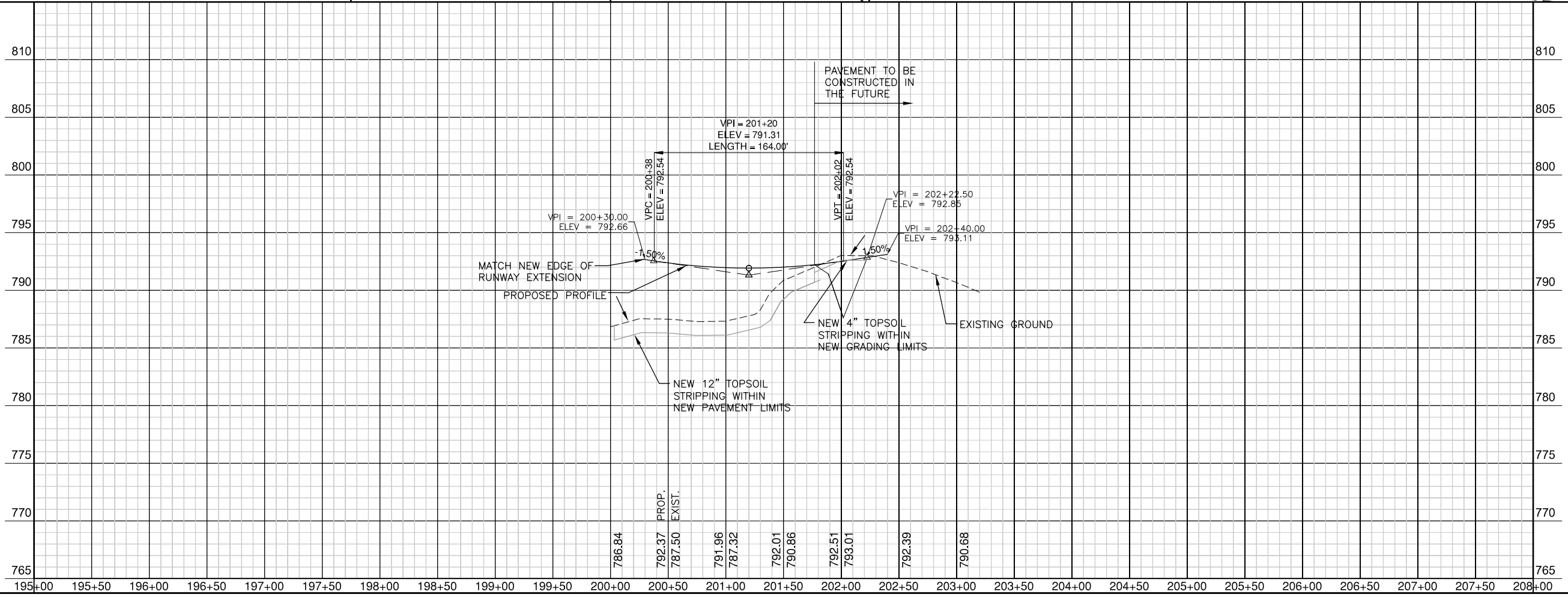
- LEGEND**
- NEW 5/8" POROUS FRICTION COURSE (402)
 - NEW 2" BITUMINOUS SURFACE COURSE (401)
 - NEW 4" BITUMINOUS BASE COURSE (403)
 - NEW 8" CRUSHED AGGREGATE BASE COURSE (209)
 - NEW 12" TOPSOIL STRIPPING (152)
 - NEW 5/8" POROUS FRICTION COURSE (402)
 - NEW 2" BITUMINOUS SURFACE COURSE (401)
 - NEW 4" BITUMINOUS BASE COURSE (403)
 - NEW 8" CRUSHED AGGREGATE BASE COURSE (209)
 - NEW 12" TOPSOIL STRIPPING (152)
 - NEW 12" & VAR. DEPTH SOIL UNDERCUT (INCIDENTAL TO 209)
 - NEW 12" CRUSHED AGGREGATE BASE COURSE (209)
 - EXISTING PAVEMENT REMOVAL
 - NEW 4' WOVEN WIRE FENCE

**MT. HAWLEY AUXILIARY AIRPORT
 PEORIA, ILLINOIS**

RUNWAY 18 EXTENSION

PROPOSED PLAN AND PROFILE 2

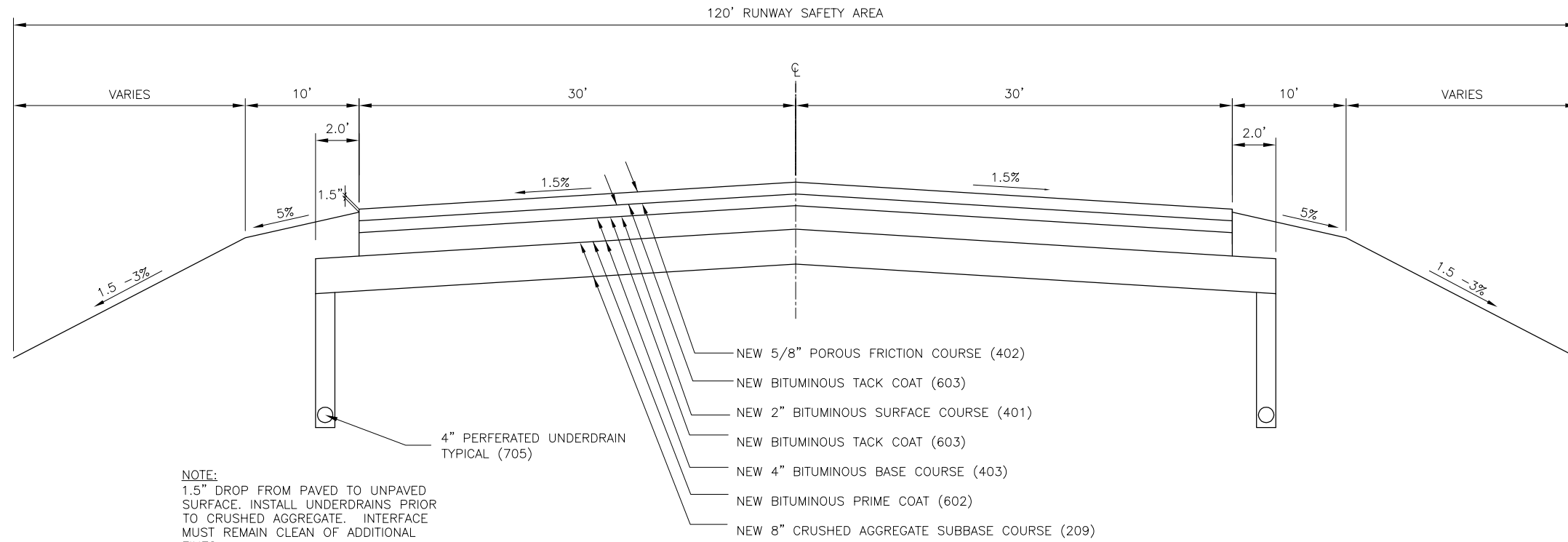
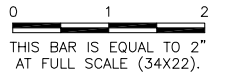
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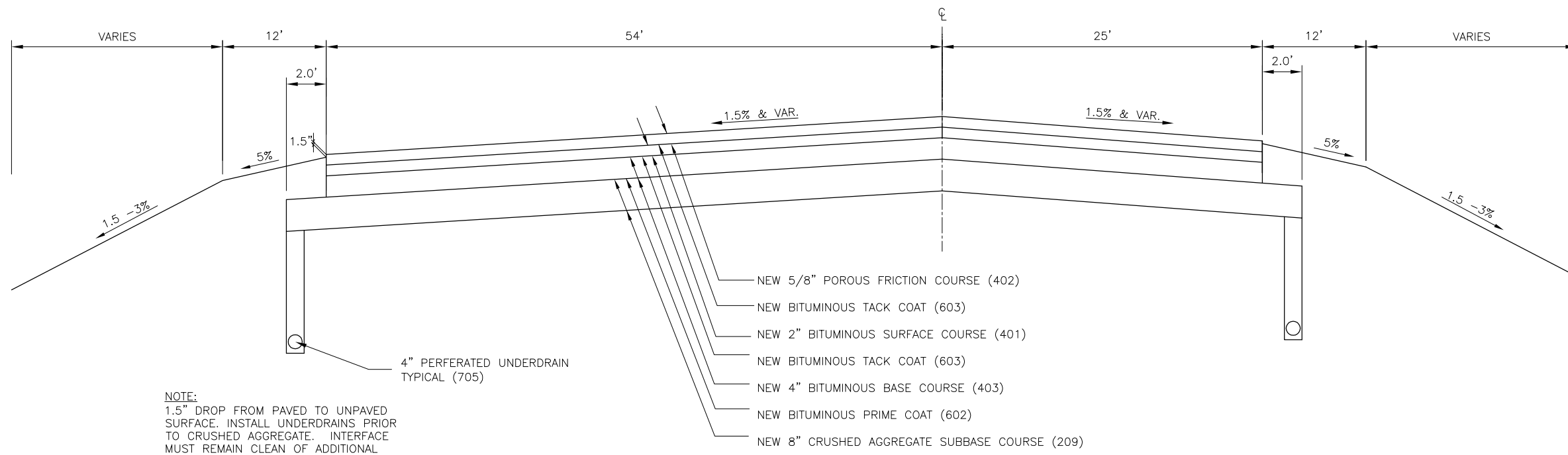
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NOTE:
 1.5" DROP FROM PAVED TO UNPAVED SURFACE. INSTALL UNDERDRAINS PRIOR TO CRUSHED AGGREGATE. INTERFACE MUST REMAIN CLEAN OF ADDITIONAL FINES.

A PROPOSED RUNWAY TYPICAL SECTION **A**
 N.T.S.



NOTE:
 1.5" DROP FROM PAVED TO UNPAVED SURFACE. INSTALL UNDERDRAINS PRIOR TO CRUSHED AGGREGATE. INTERFACE MUST REMAIN CLEAN OF ADDITIONAL FINES.

B PROPOSED TURNAROUND TYPICAL SECTION **B**
 N.T.S.

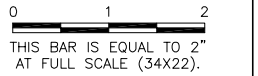
**MT. HAWLEY AUXILIARY AIRPORT
 PEORIA, ILLINOIS**
**RUNWAY 18 EXTENSION
 TYPICAL SECTIONS 1**

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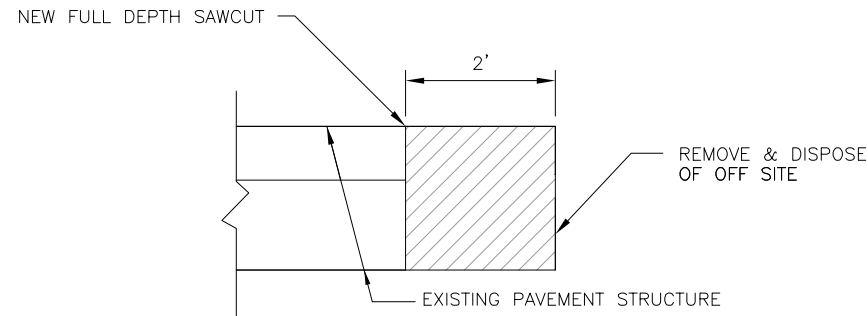
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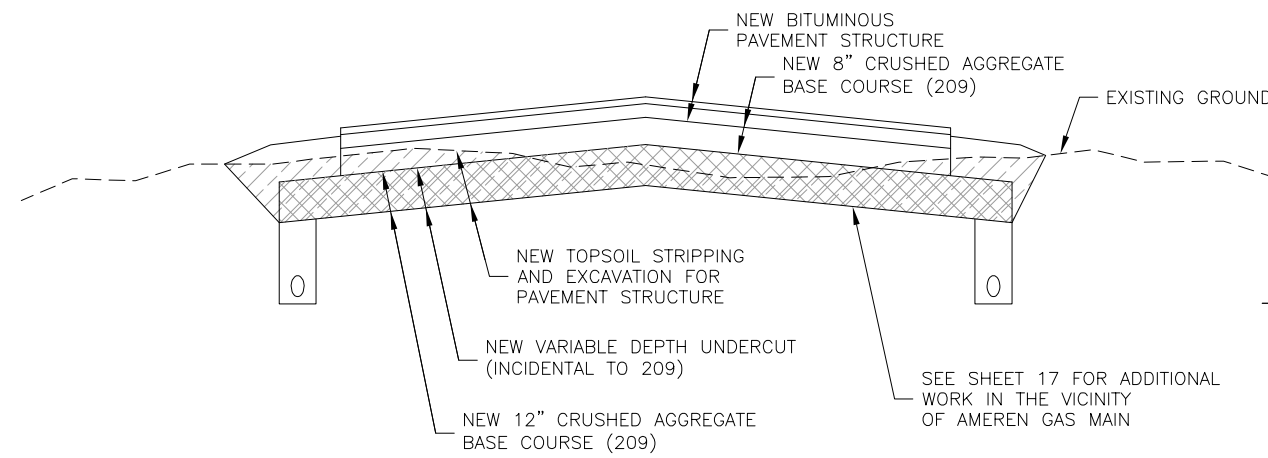
UNDERCUT NOTES:

1. LIMITS OF UNDERCUT ARE AS SHOWN OR AS DIRECTED BY THE RESIDENT ENGINEER.
2. WITHIN THE LIMITS SHOWN FOR THE PAVEMENT UNDERCUT, THE PROPOSED STRUCTURE SHALL BE A TOTAL THICKNESS OF 26.625".
3. WHERE ADDITIONAL EXCAVATION IS REQUIRED BEYOND THE 12" TOPSOIL STRIPPING FOR THE CONSTRUCTION OF THE PROPOSED UNDERCUT PAVEMENT SECTION, THAT THAT EXCAVATION SHALL BE CONSIDERED INCIDENTAL TO THE 12" CRUSHED AGGREGATE BASE COURSE ITEM.



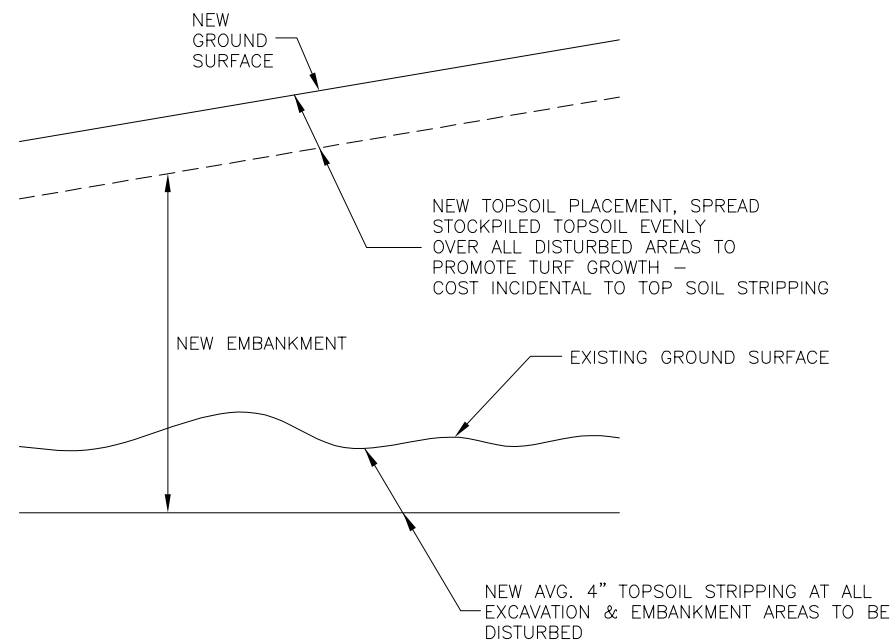
PAVEMENT REMOVAL SAWING DETAIL

N.T.S.



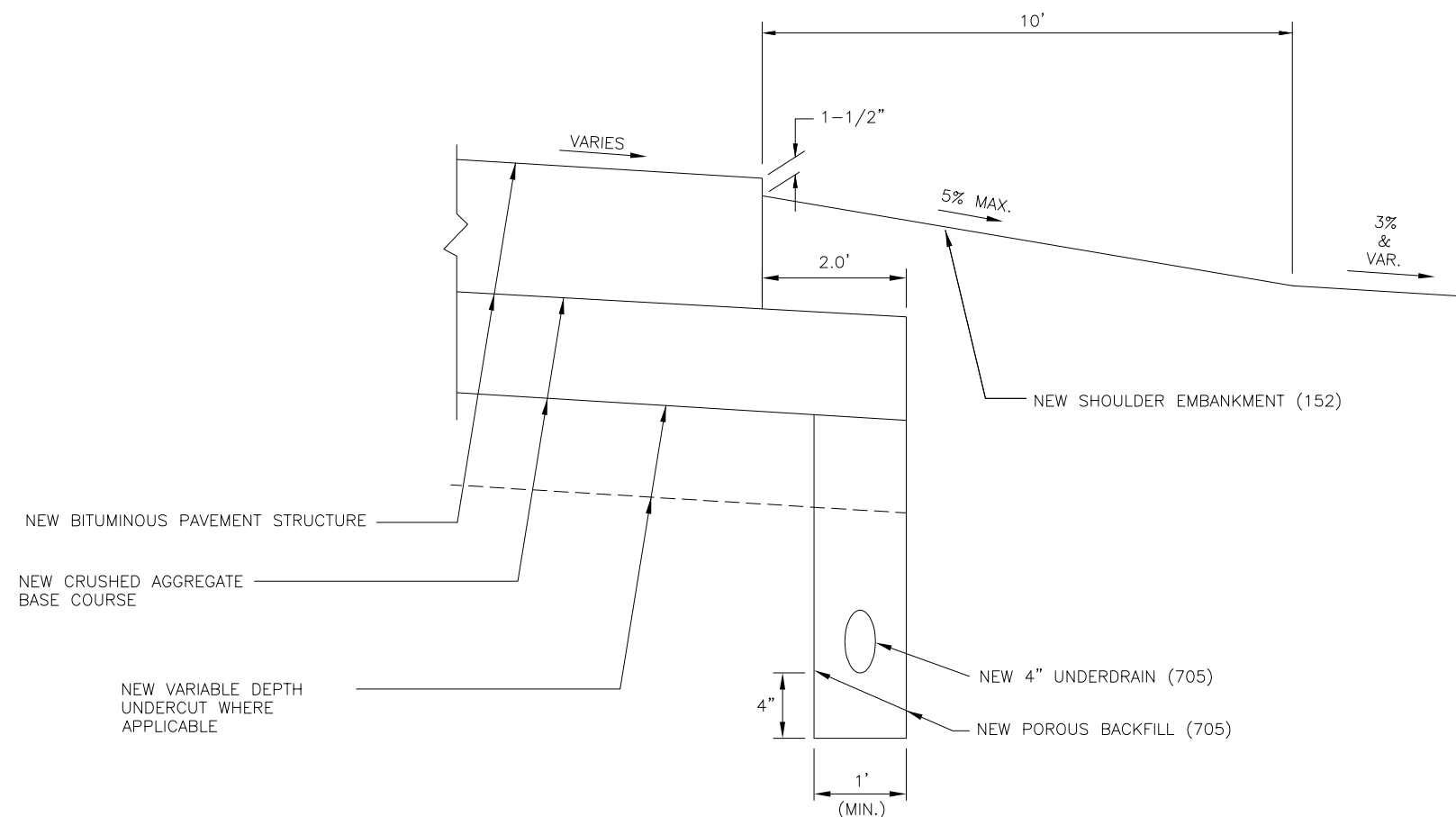
PROPOSED PAVEMENT UNDERCUT SECTION

N.T.S.



TOPSOIL STRIPPING DETAIL

N.T.S.



NEW TURF SHOULDER TYPICAL SECTION

N.T.S.

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 PEORIA, ILLINOIS**

**RUNWAY 18 EXTENSION
 TYPICAL SECTIONS 2**

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SHEET	10 OF 38 SHEETS

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**MT. HAWLEY AUXILIARY AIRPORT
 PEORIA, ILLINOIS**

RUNWAY 18 EXTENSION

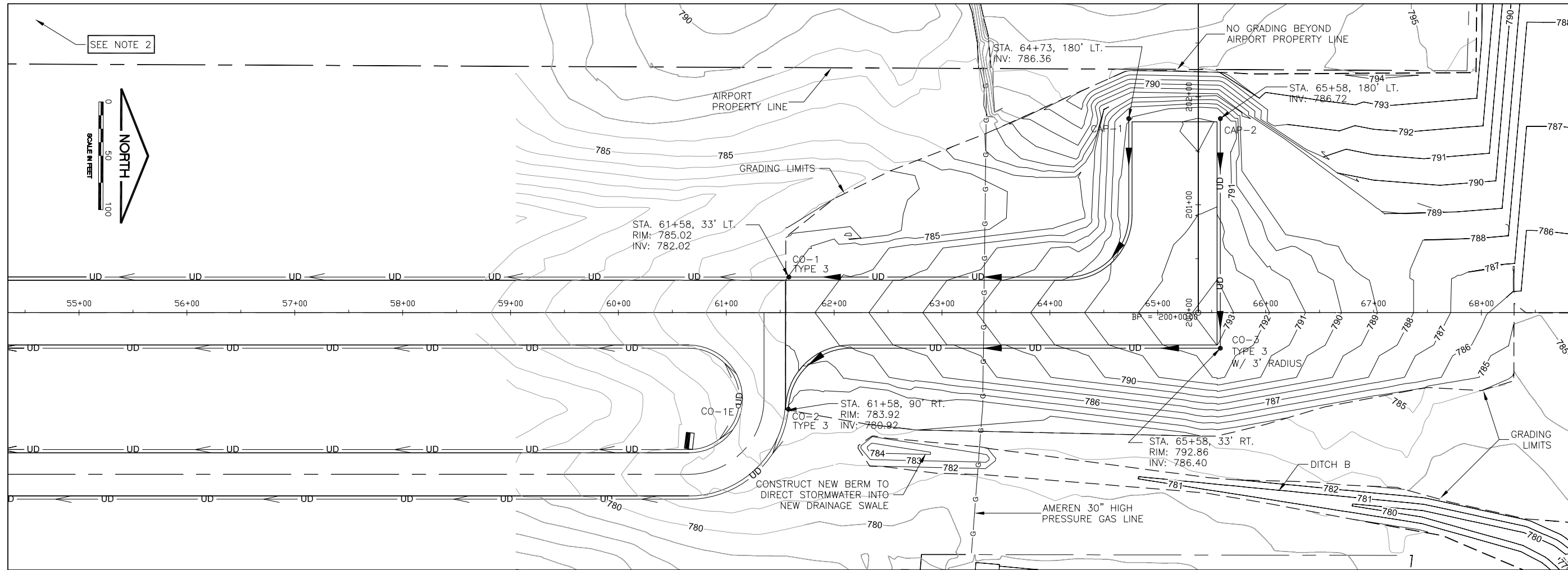
GRADING AND DRAINAGE PLAN 1

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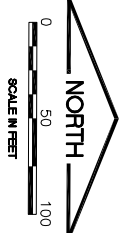
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SEE NOTE 2



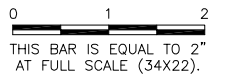
PIPE UNDERDRAIN SCHEDULE			
LINE	PIPE	LENGTH (FT.)	SLOPE %
CAP-1 TO CO-1	4" PERFORATED	438	0.15
CAP-2 TO CO-3	4" PERFORATED	213	0.15
CO-3 TO CO-2	4" PERFORATED	434	0.15

- NOTES:**
- SEE GRADING AND DRAINAGE PLAN 2 BORROW AREA GRADING AND GRADING OF PROPOSED SWALES.
 - THE APPROXIMATE LOCATION OF THE EXISTING RETENTION WALL STRUCTURE IS SHOWN ON THE SITE PLAN SHEET. THE CONTRACTOR SHALL LOCATE EXISTING STRUCTURE AND ATTACH THE NEW SPECIAL DRAINAGE STRUCTURE AS DETAILED ON SHEET DRAINAGE DETAILS 2. ALL GRADING, SEEDING, AND EXCELSIOR BLANKET REQUIRED TO RESTORE THE AREA TO ITS ORIGINAL CONDITION SHALL BE CONSIDERED INCIDENTAL TO THE SPECIAL STRUCTURE PAY ITEM.

LEGEND	
○ CO	EXISTING CLEANOUT
● CO	NEW CLEANOUT
● CAP	NEW UNDERDRAIN END CAP
●	NEW MANHOLE
← UD	EXISTING UNDERDRAIN
→ UD	NEW UNDERDRAIN
— G	GAS LINE
— 790	NEW CONTOUR
- - - 790	EXISTING CONTOUR
- - -	AIRPORT PROPERTY LINE
- - -	NEW GRADING LIMITS

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**MT. HAWLEY AUXILIARY AIRPORT
 PEORIA, ILLINOIS**

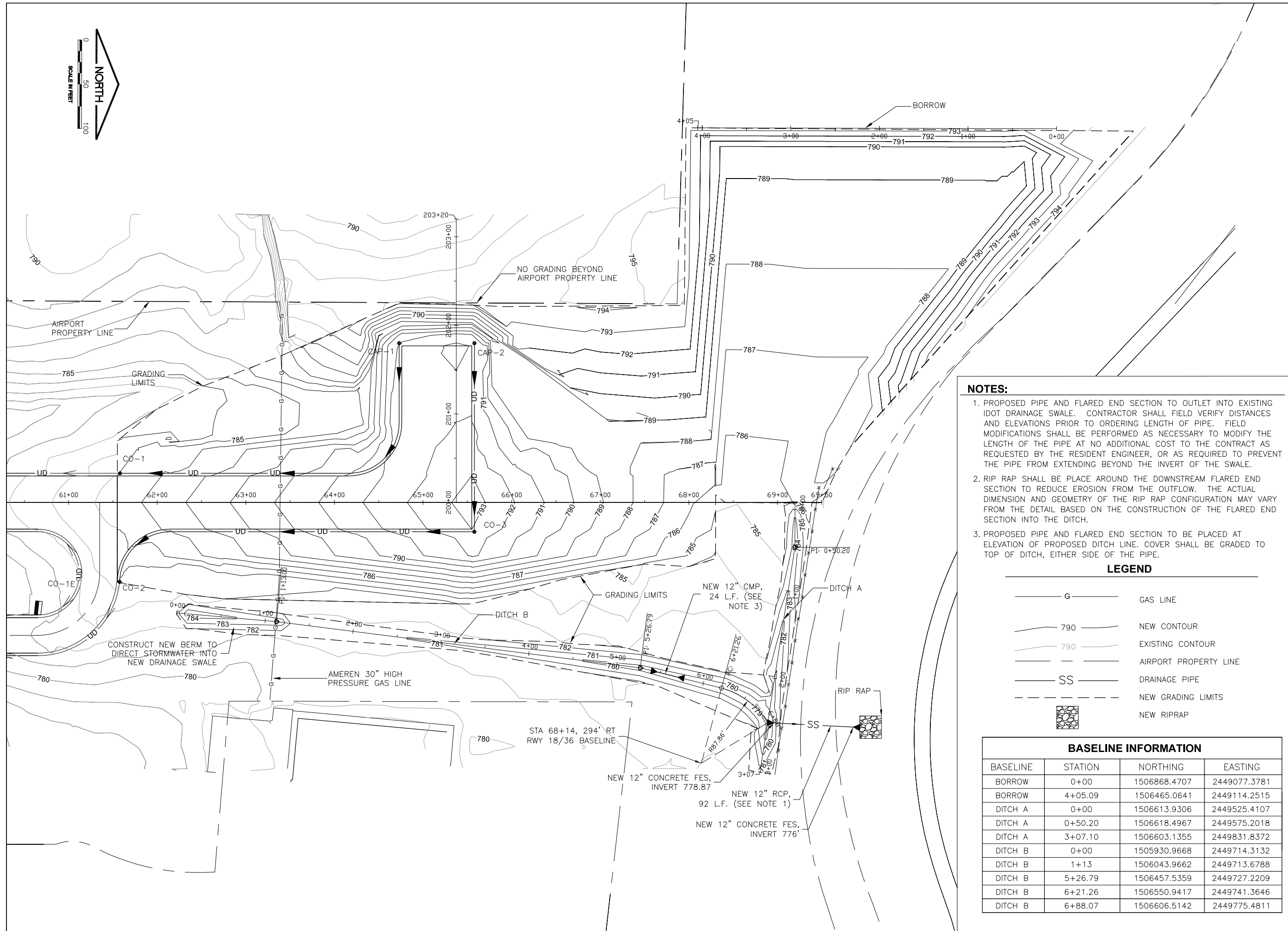
**RUNWAY 18 EXTENSION
 GRADING AND DRAINAGE PLAN 2**

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NOTES:

1. PROPOSED PIPE AND FLARED END SECTION TO OUTLET INTO EXISTING IDOT DRAINAGE SWALE. CONTRACTOR SHALL FIELD VERIFY DISTANCES AND ELEVATIONS PRIOR TO ORDERING LENGTH OF PIPE. FIELD MODIFICATIONS SHALL BE PERFORMED AS NECESSARY TO MODIFY THE LENGTH OF THE PIPE AT NO ADDITIONAL COST TO THE CONTRACT AS REQUESTED BY THE RESIDENT ENGINEER, OR AS REQUIRED TO PREVENT THE PIPE FROM EXTENDING BEYOND THE INVERT OF THE SWALE.
2. RIP RAP SHALL BE PLACE AROUND THE DOWNSTREAM FLARED END SECTION TO REDUCE EROSION FROM THE OUTFLOW. THE ACTUAL DIMENSION AND GEOMETRY OF THE RIP RAP CONFIGURATION MAY VARY FROM THE DETAIL BASED ON THE CONSTRUCTION OF THE FLARED END SECTION INTO THE DITCH.
3. PROPOSED PIPE AND FLARED END SECTION TO BE PLACED AT ELEVATION OF PROPOSED DITCH LINE. COVER SHALL BE GRADED TO TOP OF DITCH, EITHER SIDE OF THE PIPE.

LEGEND

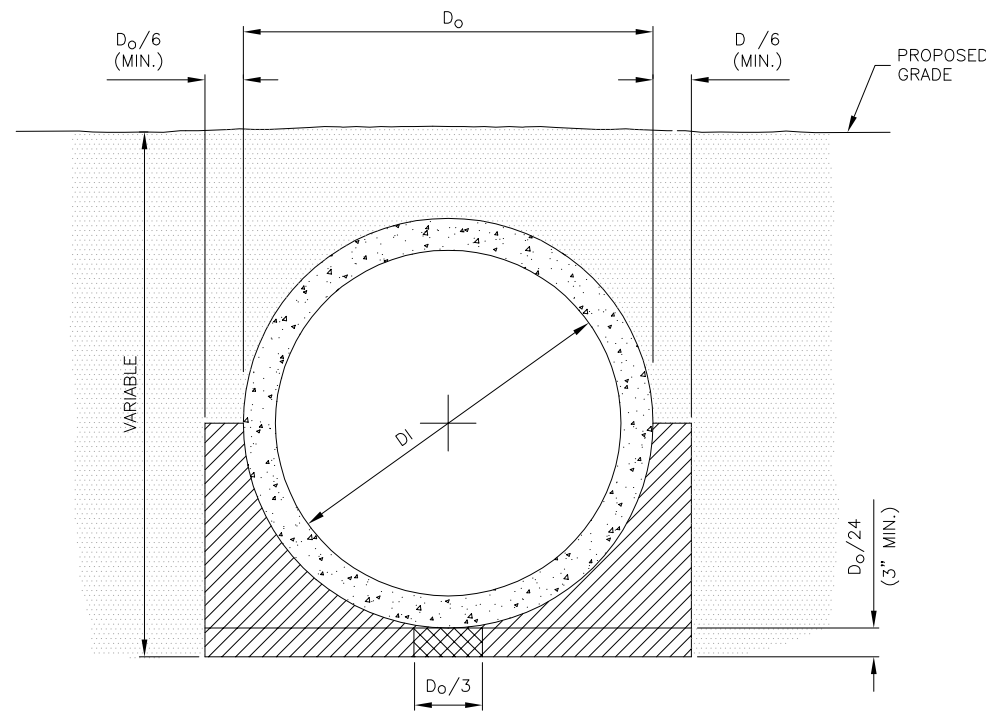
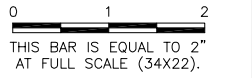
	GAS LINE
	NEW CONTOUR
	EXISTING CONTOUR
	AIRPORT PROPERTY LINE
	DRAINAGE PIPE
	NEW GRADING LIMITS
	NEW RIPRAP

BASELINE INFORMATION

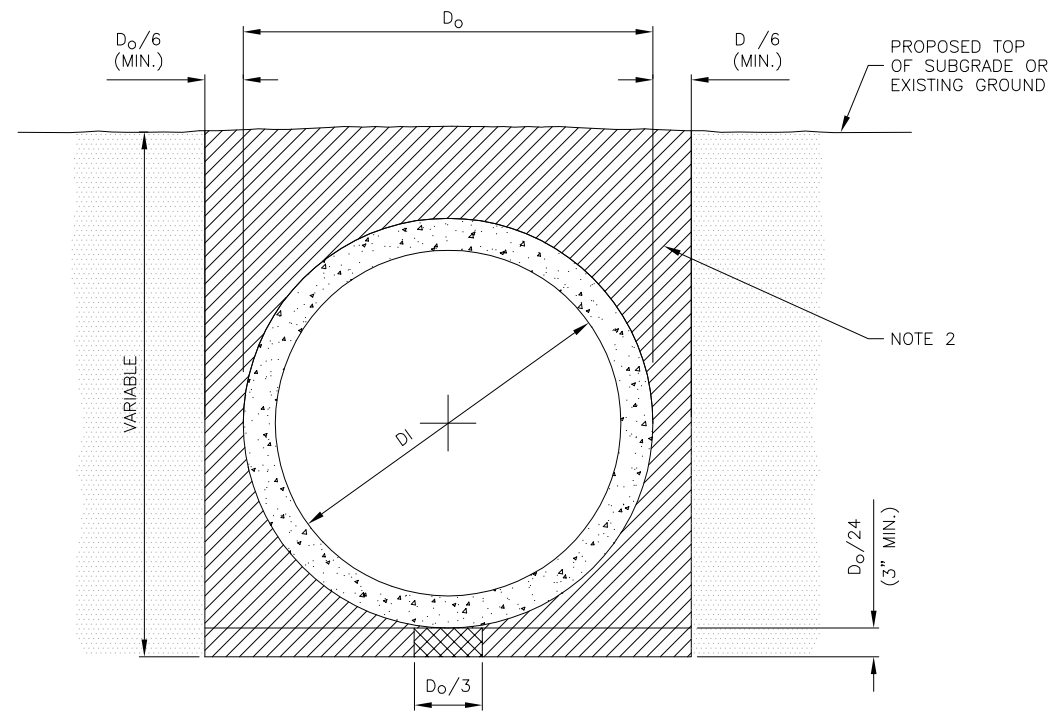
BASELINE	STATION	NORTHING	EASTING
BORROW	0+00	1506868.4707	2449077.3781
BORROW	4+05.09	1506465.0641	2449114.2515
DITCH A	0+00	1506613.9306	2449525.4107
DITCH A	0+50.20	1506618.4967	2449575.2018
DITCH A	3+07.10	1506603.1355	2449831.8372
DITCH B	0+00	1505930.9668	2449714.3132
DITCH B	1+13	1506043.9662	2449713.6788
DITCH B	5+26.79	1506457.5359	2449727.2209
DITCH B	6+21.26	1506550.9417	2449741.3646
DITCH B	6+88.07	1506606.5142	2449775.4811

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**STANDARD TRENCH INSTALLATION
 NON-PAVED AREA**
 N.T.S.



**STANDARD TRENCH INSTALLATION
 PROPOSED PAVED AREA**
 N.T.S.

TRENCH INSTALLATION LEGEND

- DRAINAGE CONDUIT MATERIAL—CONCRETE
- MIDDLE BEDDING LOOSELY PLACED UNCOMPACTED BEDDING
- HAUNCH AND OUTER BEDDING COMPACTION— TO ENGINEER'S SATISFACTION OR 95% STANDARD PROCTOR
- LOWER SIDE AND OVERFILL COMPACTION— SAME AS EMBANKMENT REQUIREMENTS
- D_o PIPE OUTSIDE DIAMETER
- D_i PIPE INSIDE DIAMETER

TRENCH INSTALLATION NOTES

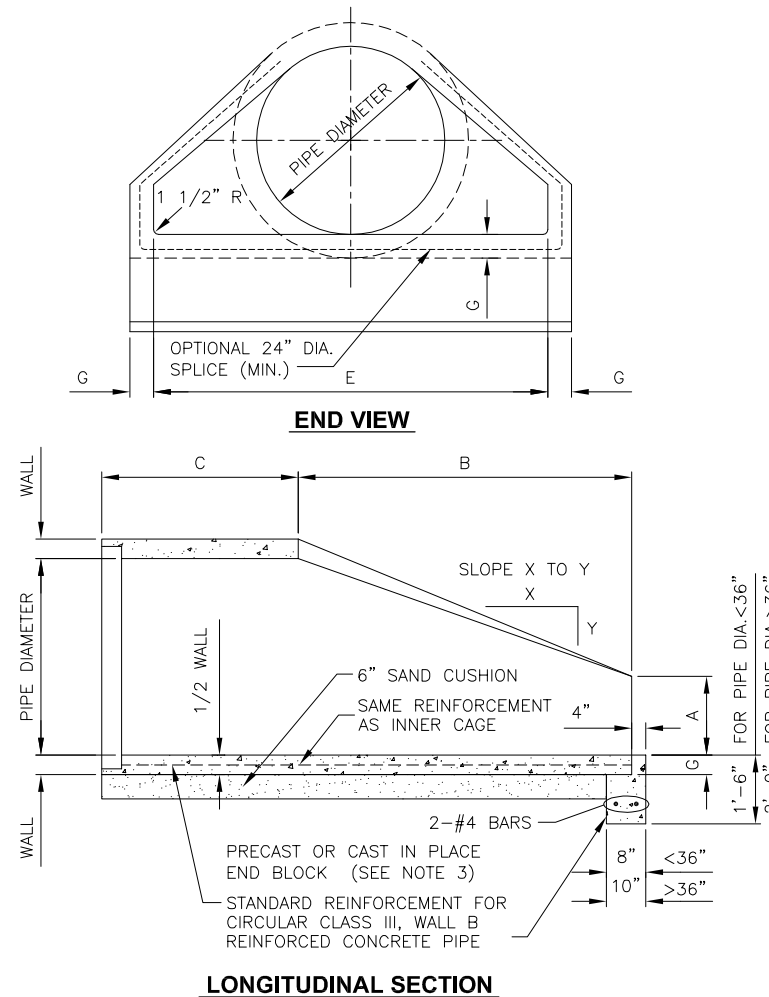
- BEDDING SHOWN IS IN ACCORDANCE WITH "STANDARD EMBANKMENT INSTALLATIONS", STANDARD INSTALLATION & BEDDING FACTORS FOR THE INDIRECT DESIGN METHOD (DESIGN DATA 40), AMERICAN CONCRETE PIPE ASSOCIATION.
- BACKFILL TO EXTEND 3' BEYOND EDGES OF PROPOSED PAVEMENT.

PIPE DIA.	APPROX WT.(lbs.)	WALL	A	B	C	D	E	G	R	SLOPE
12"	530	2"	4"	2'-0"	4'-0 7/8"	6'-0 7/8"	2'-0"	2"	9"	3:1
15"	740	2 1/4"	6"	2'-3"	3'-10"	6'-1"	2'-6"	2 1/4"	11"	3:1
18"	990	2 1/2"	9"	2'-3"	3'-10"	6'-1"	3'-0"	2 1/2"	12"	3:1
21"	1280	2 3/4"	9"	2'-11"	3'-2"	6'-1"	3'-6"	2 3/4"	13"	3:1
24"	1520	3"	9 1/2"	3'-7 1/2"	2'-6"	6'-1 1/2"	4'-0"	3"	14"	3:1
27"	1930	3 1/4"	10 1/2"	4'-0"	2'-1 1/2"	6'-1 1/2"	4'-6"	3 1/4"	14 1/2"	3:1
30"	2190	3 1/2"	1'-0"	4'-6"	1'-7 3/4"	6'-1 3/4"	5'-0"	3 1/2"	15"	3:1
33"	3200	3 3/4"	1-1 1/2"	4'-10 1/2"	3'-3 1/4"	8'-1 3/4"	5'-6"	3 3/4"	17 1/2"	3:1
36"	4100	4"	1'-3"	5'-3"	2'-10 3/4"	8'-1 3/4"	6'-0"	4"	20"	3:1
42"	5380	4 1/2"	1'-9"	5'-3"	2'-11"	8'-2"	6'-6"	4 1/2"	22"	3:1
48"	6550	5"	2'-0"	6'-0"	2'-2"	8'-2"	7'-0"	5"	22"	3:1
54"	8240	5 1/2"	2'-3"	5'-5"	2'-11"	8'-4"	7'-6"	5 1/2"	24"	2.4:1
60"	8730	6"	2'-11"	5'-0"	3'-3"	8'-3"	8'-0"	5"	*	2:1
66"	10710	6 1/2"	2'-6"	6'-0"	2'-3"	8'-3"	8'-6"	5 1/2"	*	2:1
72"	12520	7"	3'-0"	6'-6"	1'-9"	8'-3"	9'-0"	6"	*	1.86:1
78"	14770	7 1/2"	3'-0"	7'-6"	1'-9"	9'-3"	9'-6"	6 1/2"	*	1.82:1
84"	18160	8"	3'-0"	7'-6 1/2"	1'-9"	9'-3 1/2"	10'-0"	6 1/2"	*	1.5:1

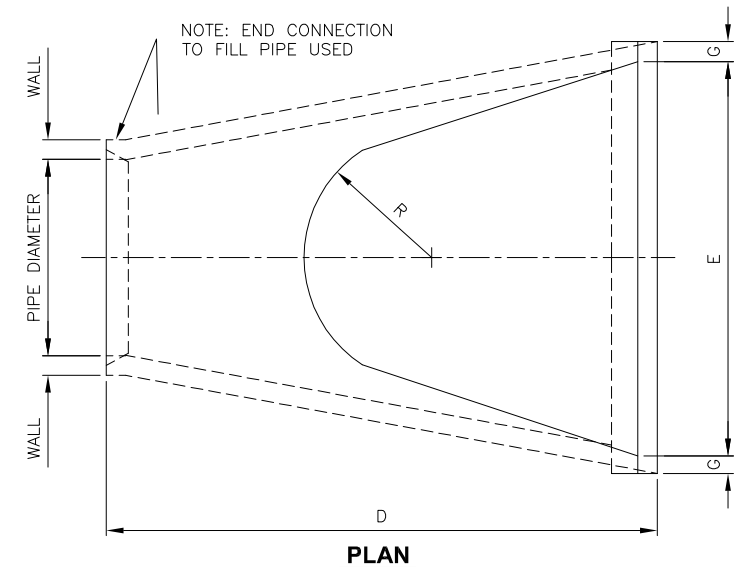
* RADIUS AS FURNISHED BY MANUFACTURER.

PRECAST FLARE SECTION NOTES

- PRECAST CONCRETE FLARED END SECTIONS SHALL CONFORM TO THE APPLICABLE REQUIREMENTS OF AASHTO M-170 CLASS III, WALL B REINFORCED CONCRETE PIPE.
- PRECAST CONCRETE FLARED END SECTION FOR PIPE DIAMETER REQUIRED SHALL BE AS INDICATED ON DETAIL PLAN FOR EACH INDIVIDUAL INSTALLATION.
- THE END BLOCK SHALL BE PLACED PRIOR TO THE INSTALLATION OF THE FLARED END SECTION. THE END BLOCK SHALL BE BACKFILLED IN ACCORDANCE WITH ITEM 701.



PRECAST REINFORCED CONCRETE FLARED END SECTION
 (I.D.O.T. STD. NO. 542301)
 N.T.S.



PLAN

MT. HAWLEY AUXILIARY AIRPORT
 PEORIA, ILLINOIS

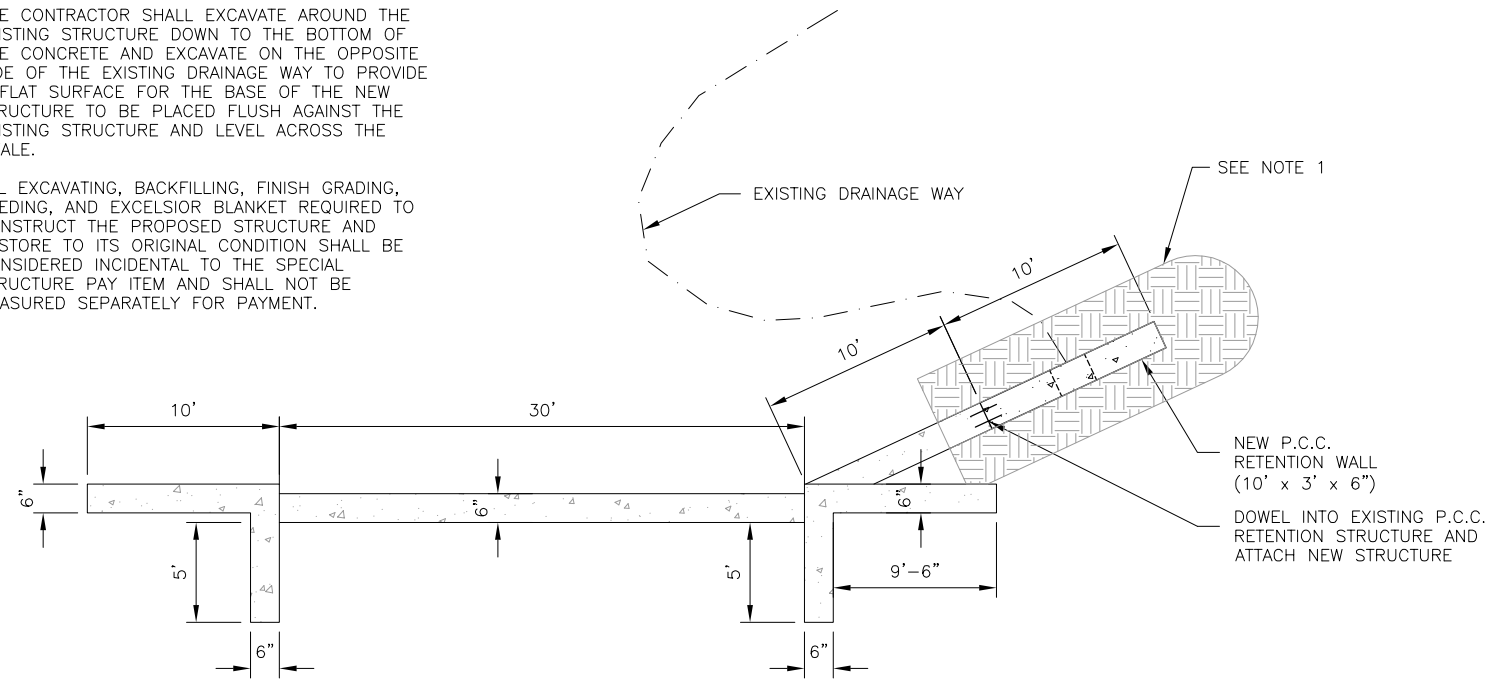
RUNWAY 18 EXTENSION
 DRAINAGE DETAILS 1

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AIP PROJ. NO.	3-17-0079-B11
SHEET	13 OF 38 SHEETS

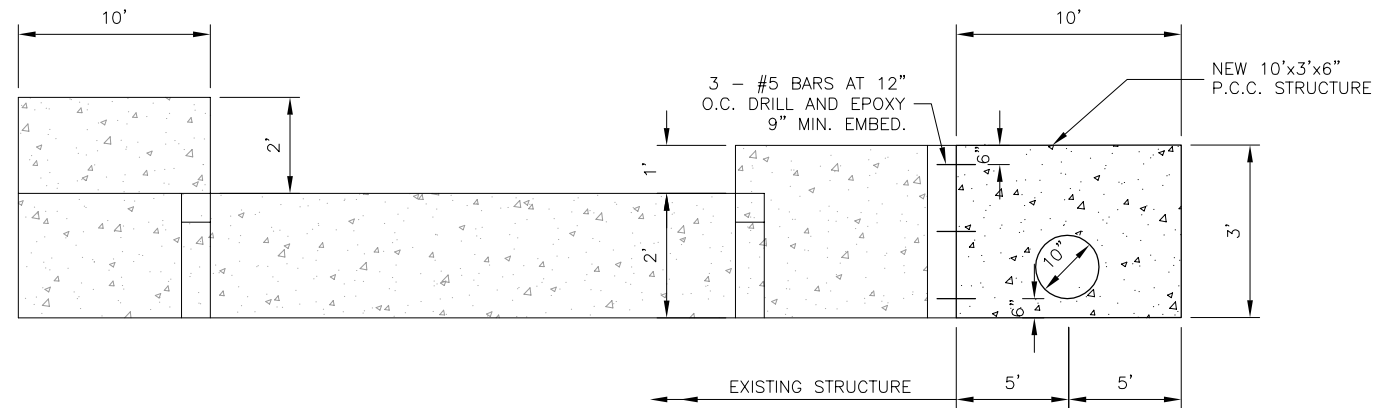
SPECIAL STRUCTURE GRADING NOTES

1. THE CONTRACTOR SHALL EXCAVATE AROUND THE EXISTING STRUCTURE DOWN TO THE BOTTOM OF THE CONCRETE AND EXCAVATE ON THE OPPOSITE SIDE OF THE EXISTING DRAINAGE WAY TO PROVIDE A FLAT SURFACE FOR THE BASE OF THE NEW STRUCTURE TO BE PLACED FLUSH AGAINST THE EXISTING STRUCTURE AND LEVEL ACROSS THE SWALE.
2. ALL EXCAVATING, BACKFILLING, FINISH GRADING, SEEDING, AND EXCELSIOR BLANKET REQUIRED TO CONSTRUCT THE PROPOSED STRUCTURE AND RESTORE TO ITS ORIGINAL CONDITION SHALL BE CONSIDERED INCIDENTAL TO THE SPECIAL STRUCTURE PAY ITEM AND SHALL NOT BE MEASURED SEPARATELY FOR PAYMENT.



EXISTING RETENTION STRUCTURE ADJUSTMENT - PLAN VIEW

NTS

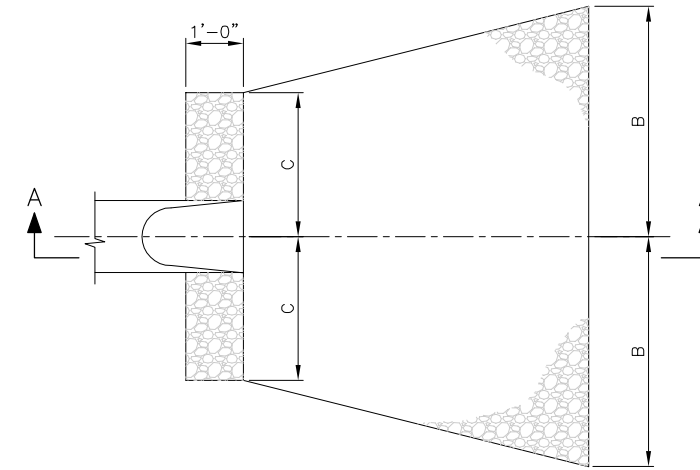


EXISTING RETENTION STRUCTURE ADJUSTMENT - SECTION VIEW

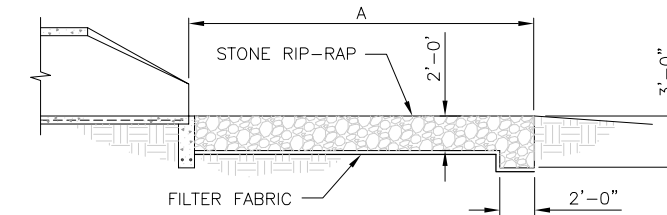
NTS

SPECIAL STRUCTURE NOTES

1. THE PROPOSED STRUCTURE SHALL BE REINFORCED WITH #5 BARS HORIZONTALLY SPACED AT 12" O.C. AND VERTICALLY SPACED 1' - 6" O.C. REINFORCING BARS SHALL ALSO BE PLACED AROUND THE 10" HOLE ON 4 SIDES.
2. THE PROPOSED STRUCTURE SHALL BE DOWELED TO THE EXISTING STRUCTURE IN ACCORDANCE WITH THE MATERIALS AND CONSTRUCTION METHODS SPECIFIED FOR ITEM 501 OF THE STANDARD SPECIFICATIONS.
3. THE BOTTOM 6" OF THE PROPOSED STRUCTURE SHALL BE IMBEDDED IN THE GROUND SO THAT THE INVERT OF THE 10" DIAMETER HOLE SHALL BE PLACED AT THE BOTTOM OF THE EXISTING SWALE.



PLAN



SECTION A-A

RIP-RAP DETAILS

N.T.S.

RIP-RAP NOTES

1. THE ENGINEER SHALL DETERMINE THE FINAL RIP-RAP CONFIGURATION IN THE FIELD.
2. PLACE AT EXISTING PIPE LOCATIONS AS DIRECTED BY THE ENGINEER.

DIMENSIONS - TABLE 1

INSIDE DIAMETER STORM SEWER (IN.)	OUTLET DIMENSION (FT.)			INLET DIMENSION (FT.)		
	A	B	C	A	B	C
12" THRU 24"	15	9	2	9	6	2

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0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

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**RUNWAY 18 EXTENSION
DRAINAGE DETAILS 2**

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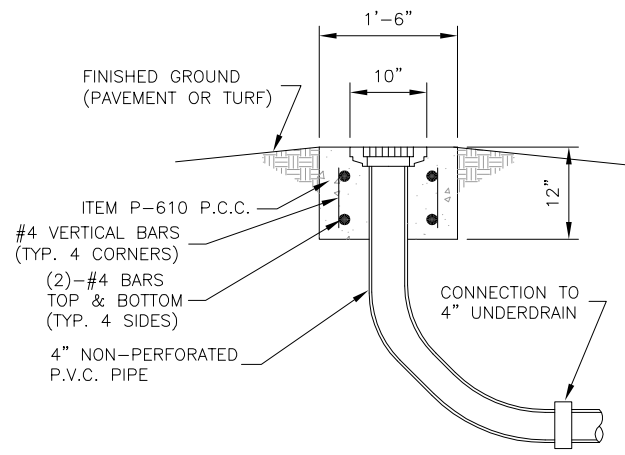
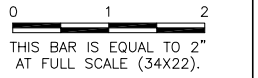
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AIP PROJ. NO. 3-17-0079-B11

SHEET 14 OF 38 SHEETS

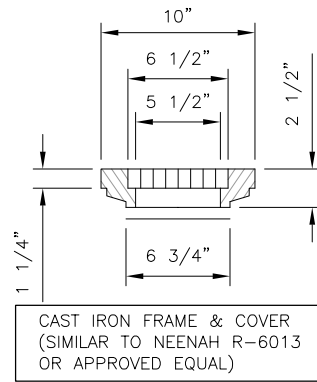
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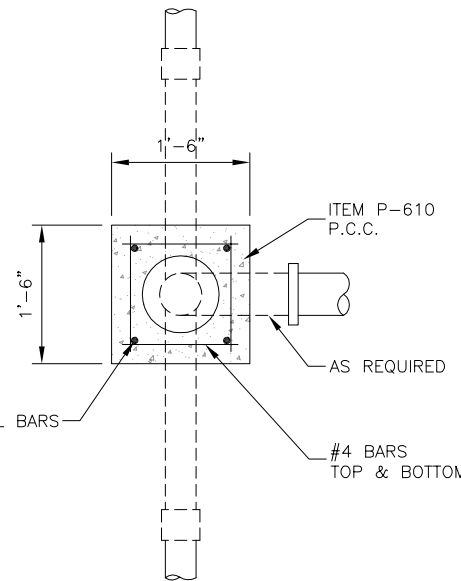


SIDE VIEW

UNDERDRAIN CLEAN-OUT DETAIL - TYPE 1
 N.T.S.

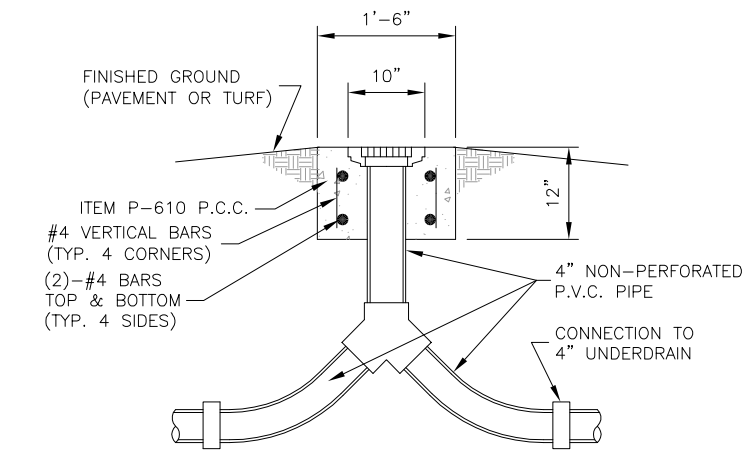


FRAME AND COVER



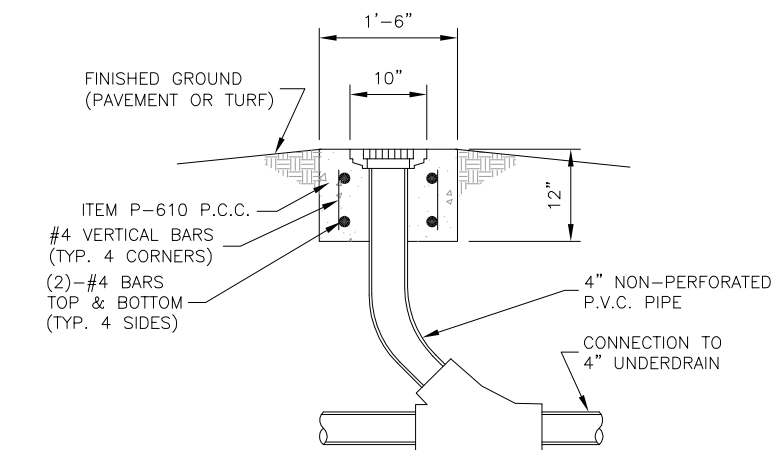
PLAN

UNDERDRAIN CLEAN-OUT DETAILS
 N.T.S.



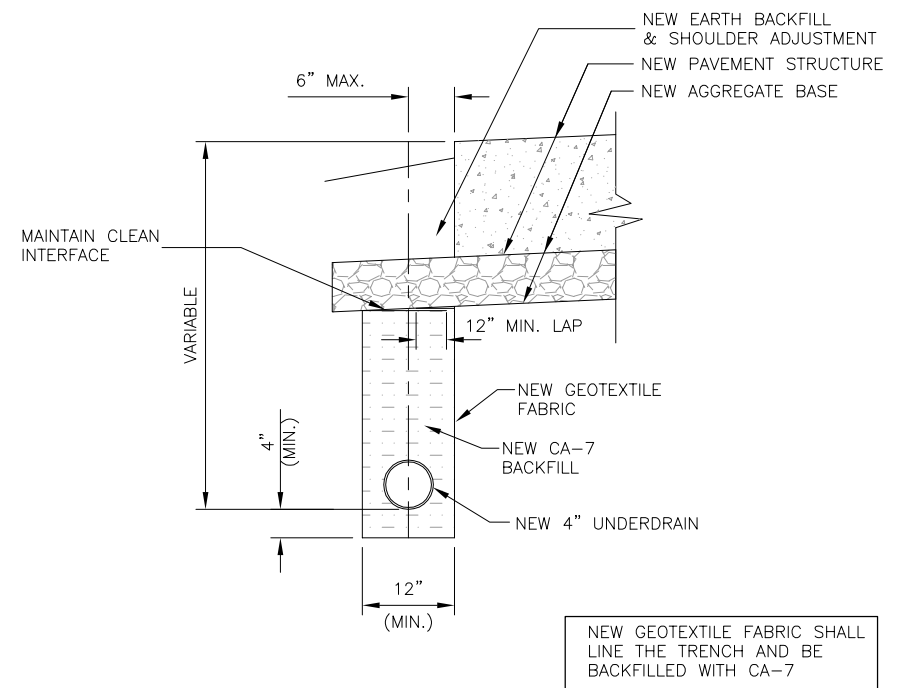
SIDE VIEW

UNDERDRAIN CLEAN-OUT DETAIL - TYPE 2
 N.T.S.



SIDE VIEW

UNDERDRAIN CLEAN-OUT DETAIL - TYPE 3
 N.T.S.



TYPICAL UNDERDRAIN DETAIL
 PAVEMENT EDGE
 N.T.S.

NOTES

DURING UNDERDRAIN CONSTRUCTION, CARE SHALL BE TAKEN TO ENSURE GOOD DRAINAGE BETWEEN THE SUB-BASE AND THE POROUS BACKFILL MATERIAL WHICH WILL AFFECT DRAINAGE TO THE UNDERDRAIN SHALL BE REMOVED PRIOR TO THE PLACEMENT OF PROPOSED POROUS BACKFILL. ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL. UNDERDRAIN END CAP SHALL BE CONSIDERED INCIDENTAL TO UNDERDRAIN PIPE.

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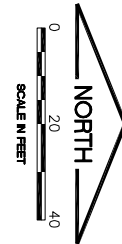
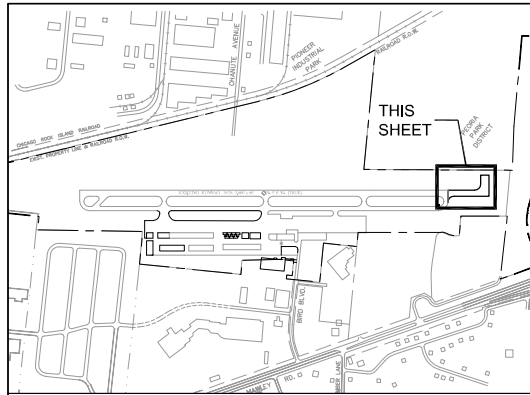
RUNWAY 18 EXTENSION
 UNDERDRAIN DETAILS

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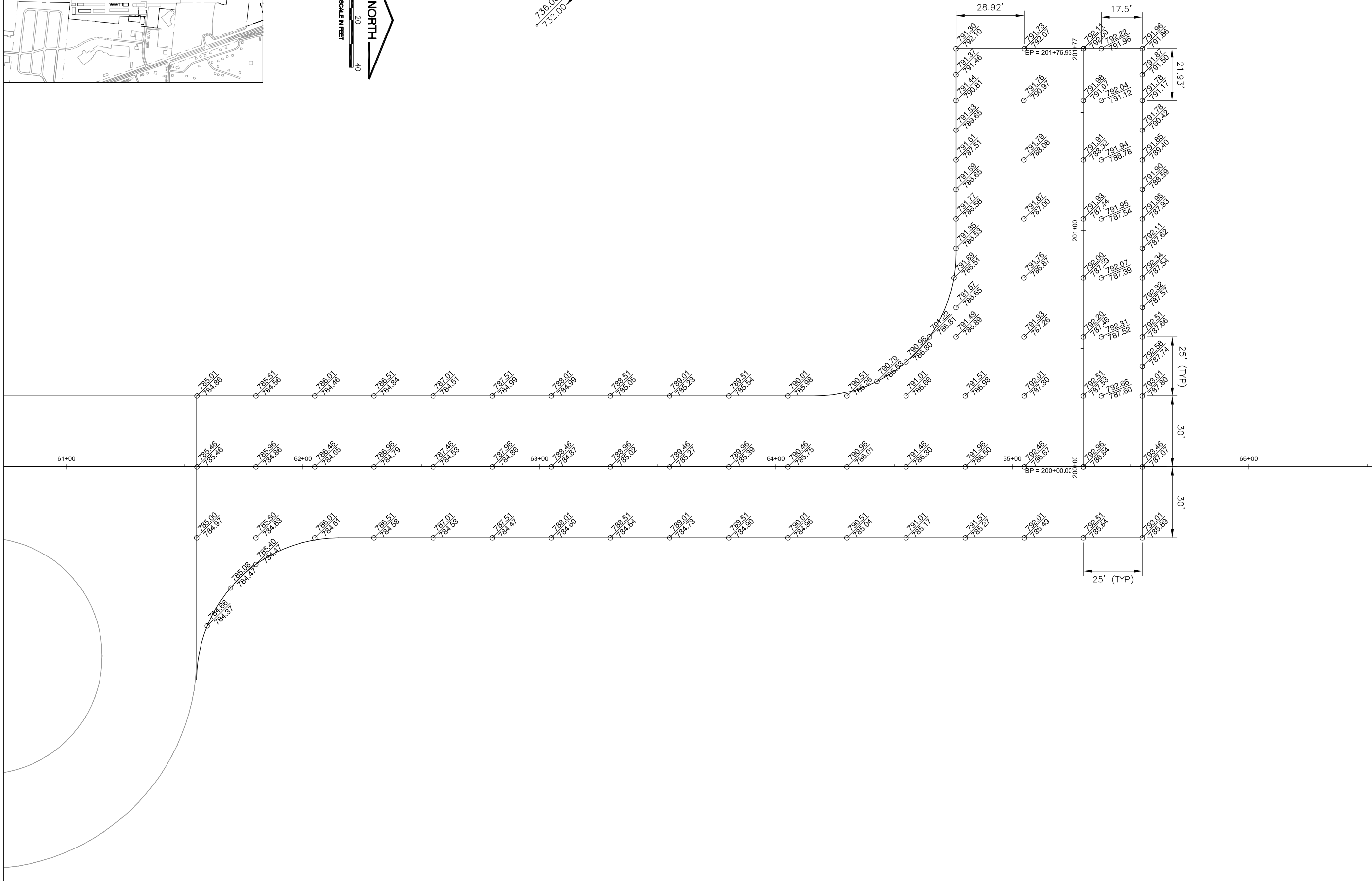
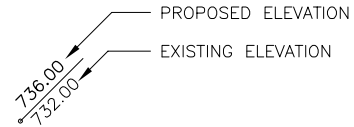
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SHEET	15 OF 38 SHEETS

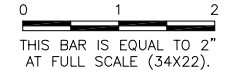


LEGEND



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**MT. HAWLEY AUXILIARY AIRPORT
 PEORIA, ILLINOIS
 RUNWAY 18 EXTENSION
 STAKING PLAN**

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SHEET 16 OF 38 SHEETS	

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0 1 2
 THIS BAR IS EQUAL TO 2"
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 PEORIA, ILLINOIS

RUNWAY 18 EXTENSION
 PAVING AND MISCELLANEOUS DETAILS

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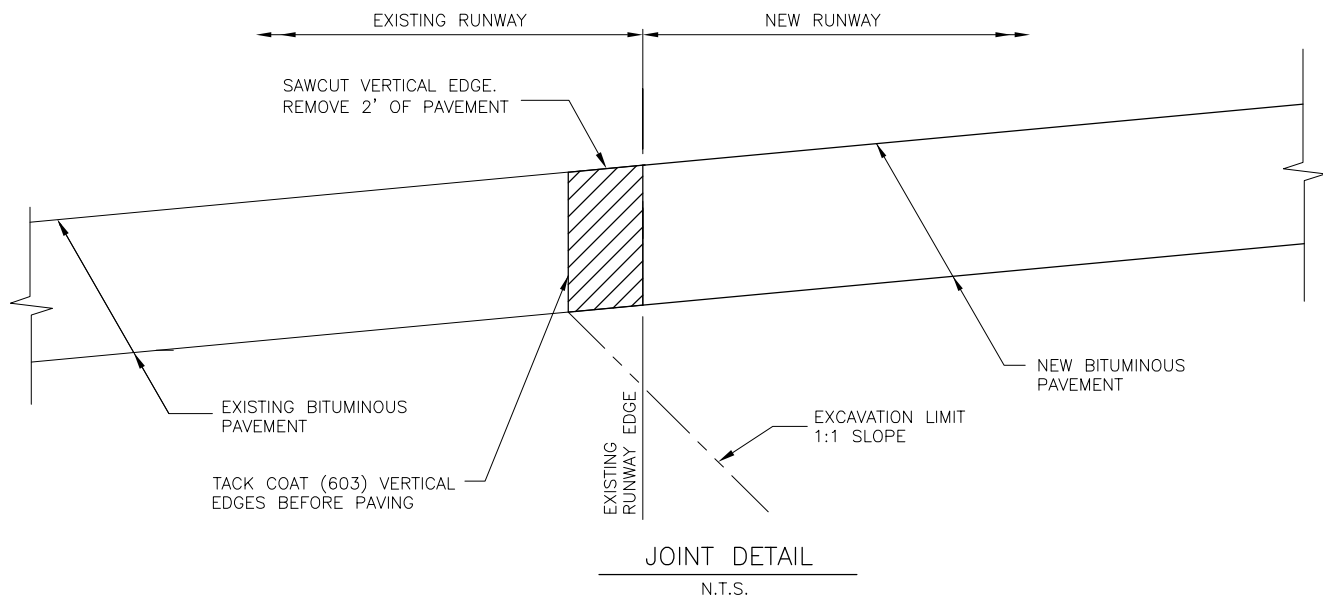
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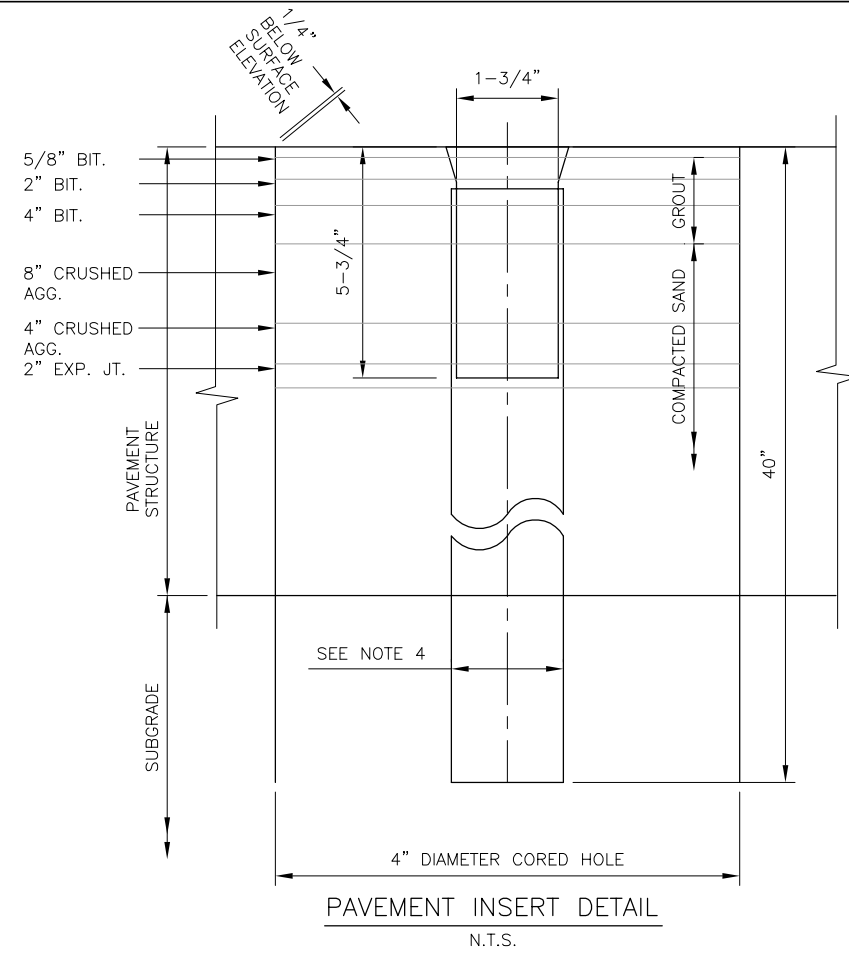
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SHEET 17 OF 38 SHEETS



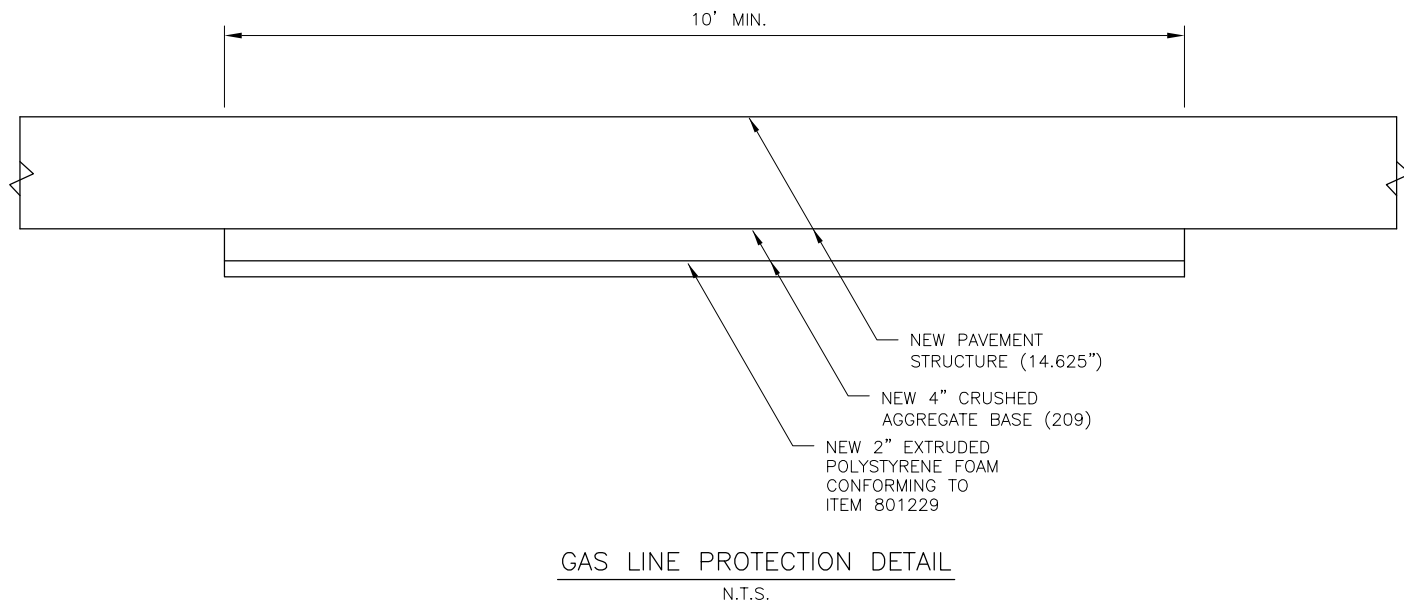
NOTES:

- CONTRACTOR SHALL USE A ROLLER TO COMPACT AGAINST ADJACENT STRUCTURES AND TO ROLL PERPENDICULAR TO THE PAVING DIRECTION.
- CONTRACTOR SHALL EXCAVATE THE SUBGRADE AS SHOWN AGAINST THE EXISTING PAVEMENT BASE TO KEEP THE SUBGRADE COMPACTED UNDER THE EXISTING PAVEMENT STRUCTURE.
- PRIOR TO CONSTRUCTING THE RUNWAY EXTENSION A MINIMUM OF 2' OF EXISTING PAVEMENT SHALL BE REMOVED. PROVIDING A SMOOTH SAW CUT VERTICAL FACE TO MATCH THE PROPOSED PAVEMENT STRUCTURE TO THE EXISTING PAVEMENT STRUCTURE.
- TACK COAT SHALL BE PLACED ON ALL VERTICAL EDGES AND BETWEEN ANY LIFTS.



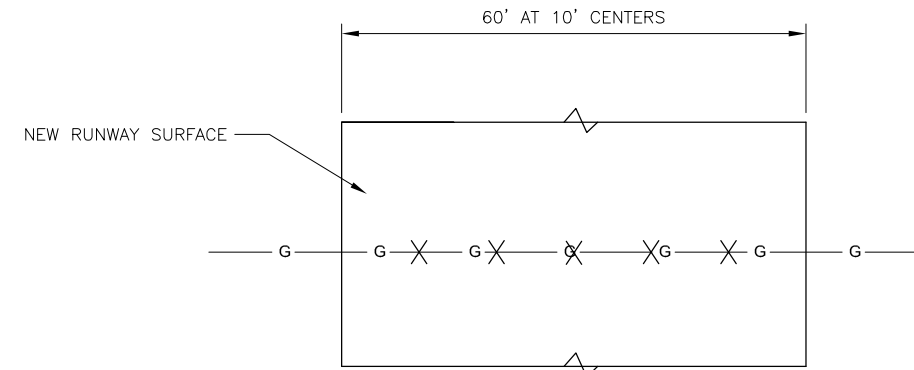
NOTES:

- AFTER PAVEMENT IS FINISHED AND IN PLACE, THE PAVEMENT INSERTS FOR REFERENCE ELECTRODE ACCESS ARE TO BE INSTALLED AS DIRECTED BY AMEREN. CONTRACTOR SHALL COORDINATE THIS WORK WITH AMEREN.
- METHOD OF INSTALLATION WILL REQUIRE A 4" HOLE TO BE CORED THROUGH THE ENTIRE PAVEMENT THICKNESS, AT THE SHOWN LOCATIONS.
- THE CONTRACTOR SHALL PROVIDE HDPE PIPE THAT SHALL BE INSERTED TO REACH A DEPTH OF 40" BELOW THE PAVEMENT SURFACE.
- THE HDPE PIPE SHALL BE LARGE ENOUGH TO ACCOMMODATE THE PAVEMENT INSERTS BUT SHALL FIT SECURELY TO PREVENT THE INSERTS FROM MOVING. THE TOP OF THE PAVEMENT INSERTS SHALL BE FLUSH WITH THE FINISHED PAVEMENT SURFACE.
- ONCE THE HDPE PIPE AND PAVEMENT INSERTS ARE IN PLACE, THE 4" CORED HOLE SHALL BE FILLED WITH COMPACTED SAND AND GROUT AS DETAILED ABOVE. THE INSIDE OF THE PAVEMENT INSERT SHALL REMAIN UNCOVERED FOR ACCESS TO THE SOIL BENEATH THE PAVEMENT.
- THE PAVEMENT INSERTS SHALL BE SUPPLIED BY AMEREN. THE HOLES SHALL NOT BE CORED UNTIL MATERIALS AND AMEREN PERSONNEL ARE ON SITE.



NOTES:

- DETAIL SHALL BE CONSTRUCTED OVERTOP AND CENTERED ON EXISTING HIGH PRESSURE GAS LINE LOCATION.
- METHOD OF CONSTRUCTION SHALL CONFORM TO THE DETAILS OUTLINE IN THE SPECIAL PROVISIONS FOR ITEM 801229.
- STYROFOAM EXTRUDED POLYSTYRENE FOAM MATERIAL SHALL BE USED AND MEASURED IN SQUARE FEET AREA.
- THE STRUCTURE OVER THE GAS LINE SHALL BE PLACED AS DETAILED: NEW PAVEMENT STRUCTURE, NEW 4" CRUSHED AGGREGATE BASE, NEW 2" EXTRUDED POLYSTYRENE FOAM.



NOTES:

- LOCATIONS OF PAVEMENT INSERTS FOR REFERENCE ELECTRODE ACCESS IS AS SHOWN, EVENLY SPACED EVERY 10 FEET.
- PAVEMENT INSERTS ARE TO BE PLACED DIRECTLY OVER THE 30" GASE LINE.

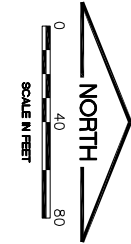
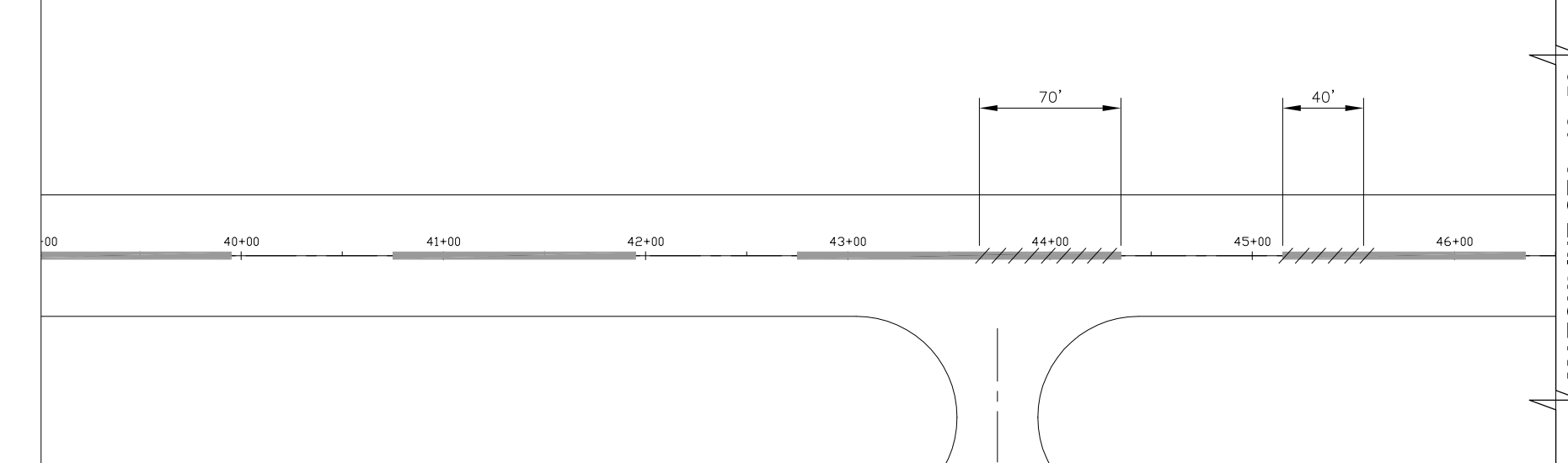
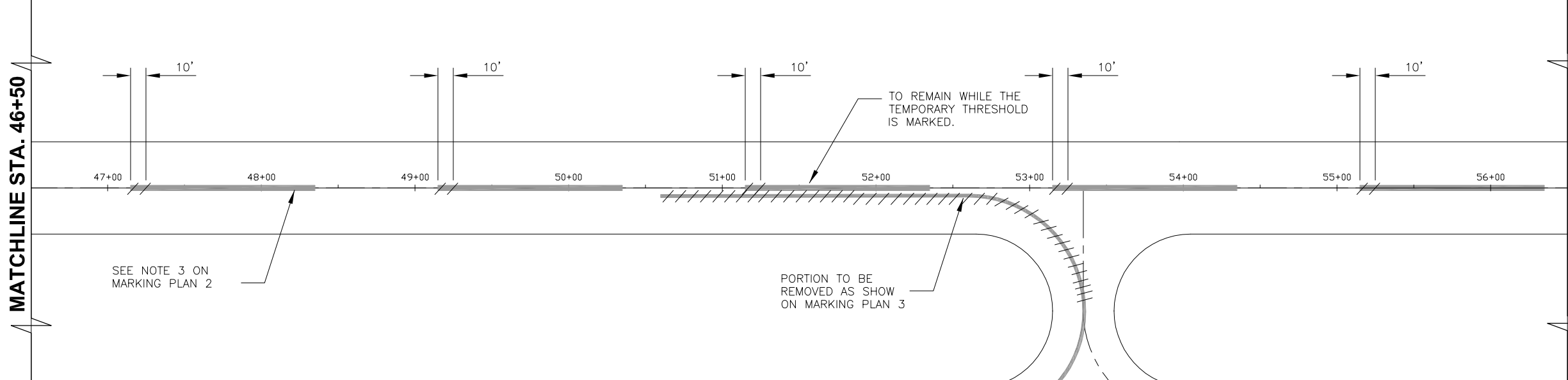
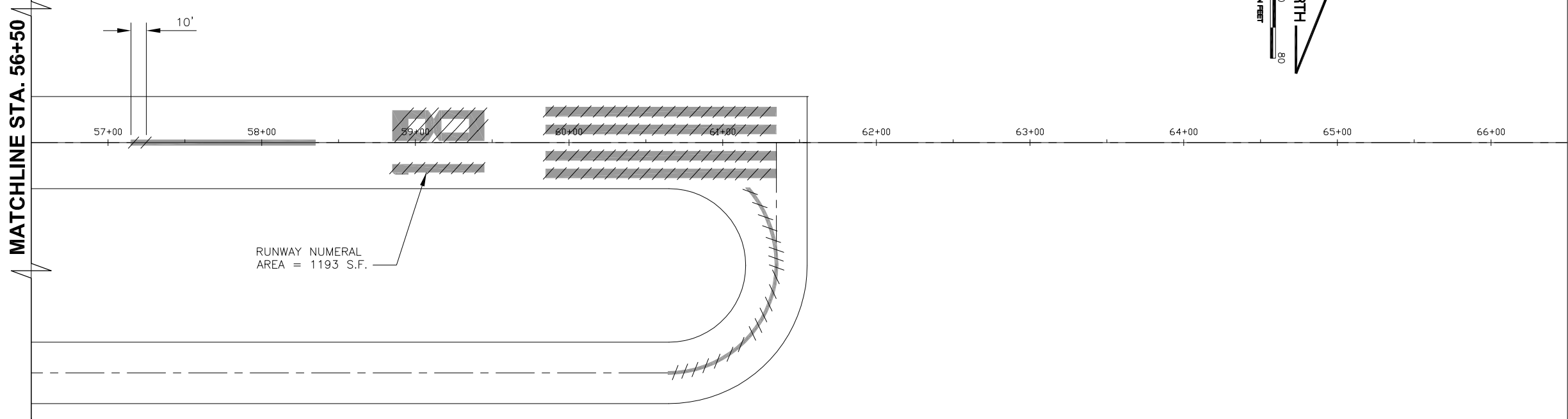
EXISTING RUNWAY 18 MARKINGS

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 Proposed Geometry
 BASE MT HAWLEY

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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



LEGEND

	EXISTING MARKING
	PAVEMENT MARKING REMOVAL

- NOTE:**
- EXISTING MARKINGS WILL BE REMOVED AT DIFFERENT TIMES OF THE PROJECT.
 - ONLY EXISTING MARKINGS AFFECTED BY THE PLACEMENT OF THE TEMPORARY THRESHOLD WILL BE REMOVED FIRST.
 - ONCE TEMPORARY MARKINGS ARE REMOVED, THE REMAINING MARKINGS WILL BE REMOVED ACCORDING TO PLAN AND THEN REMARKED TO NEW LAYOUT.
 - ONLY THE PORTION SHOWN OF THE EXISTING MARKINGS TO BE REMOVED.

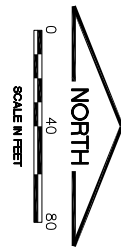
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 PEORIA, ILLINOIS**

**RUNWAY 18 EXTENSION
 MARKING PLAN 1**

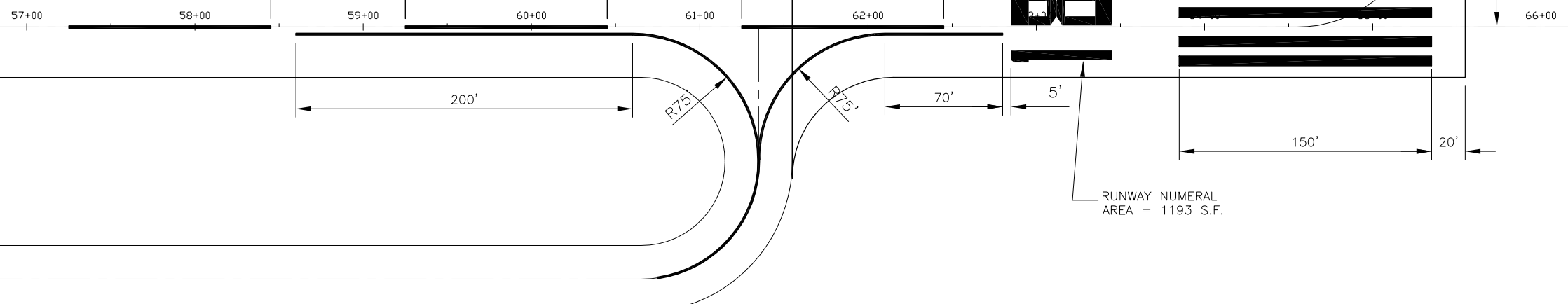
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SHEET	18 OF 38 SHEETS

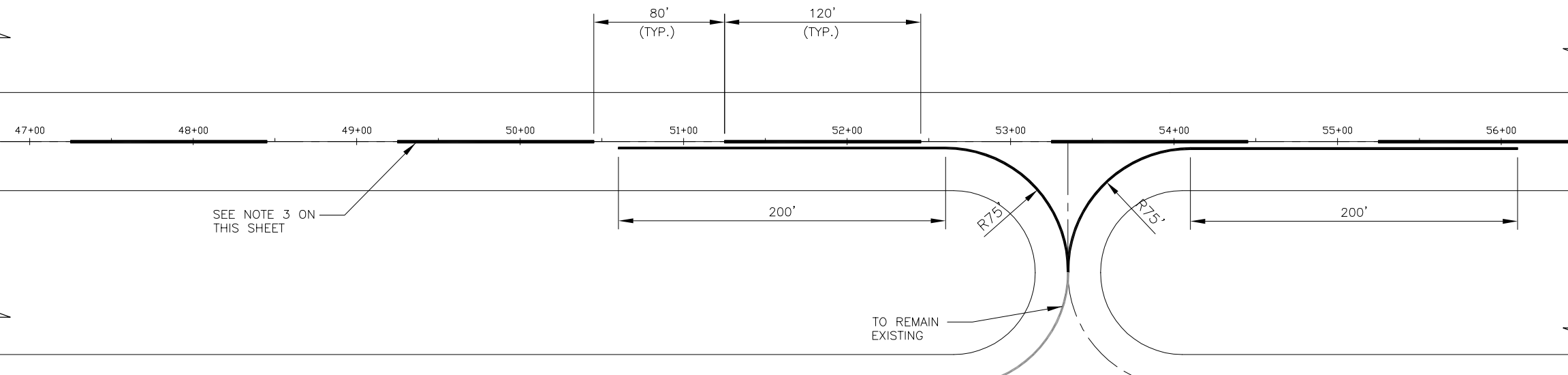
PROPOSED RUNWAY 18 MARKINGS



MATCHLINE STA. 56+50

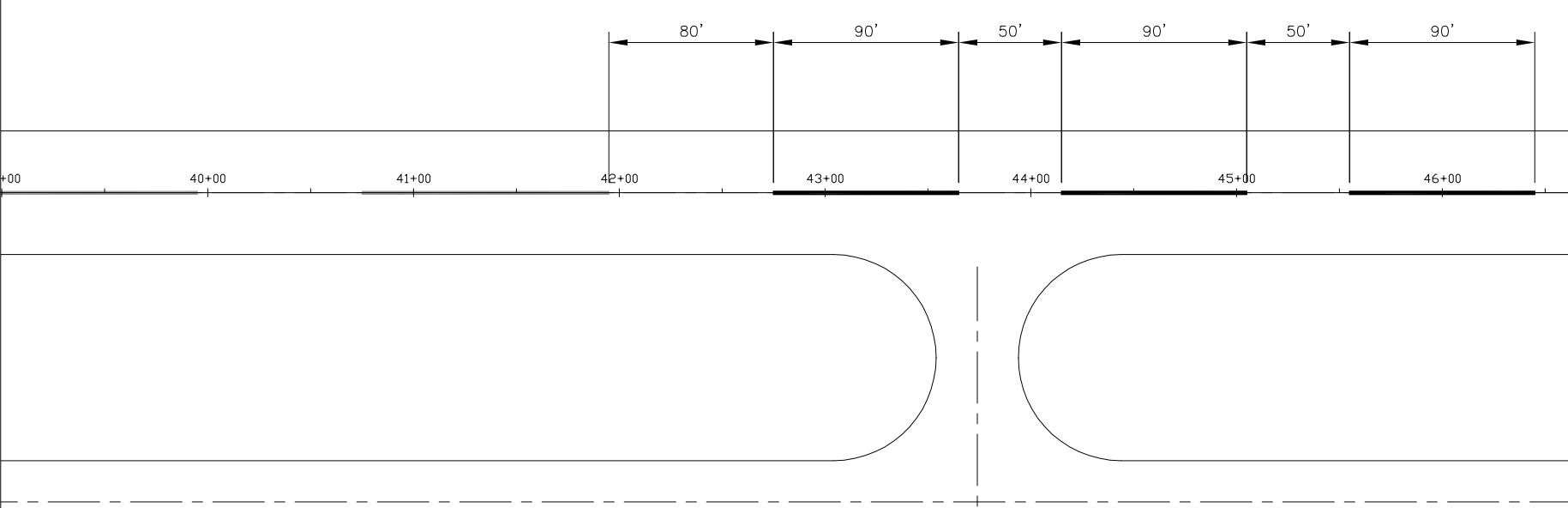


MATCHLINE STA. 46+75



MATCHLINE STA. 56+50

MATCHLINE STA. 46+75



LEGEND

- NEW MARKING
- EXISTING MARKING

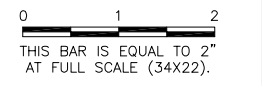
NOTE:

1. ALL NEW MARKINGS WILL CONSIST OF TWO COATS OF PAINT INCLUDING GLASS BEAD.
2. NO BLACK BORDER ON TAXIWAY CENTERLINE.
3. NEW MARKINGS WILL BE MARKED AT THE SAME TIME ONCE THE TEMPORARY THRESHOLD IS REMOVED.
4. RUNWAY CENTERLINE MARKINGS SHALL HAVE 10', 40', OR 70' REMOVED FROM THE END OF THE STRIPES AS SHOWN ON MARKING PLAN 1. THE EXISTING MARKINGS SHALL BE REMARKED INCLUDING ANY ADDITIONAL MARKINGS REQUIRED TO STRIPE THE RUNWAY AS DETAILED ON THIS SHEET.

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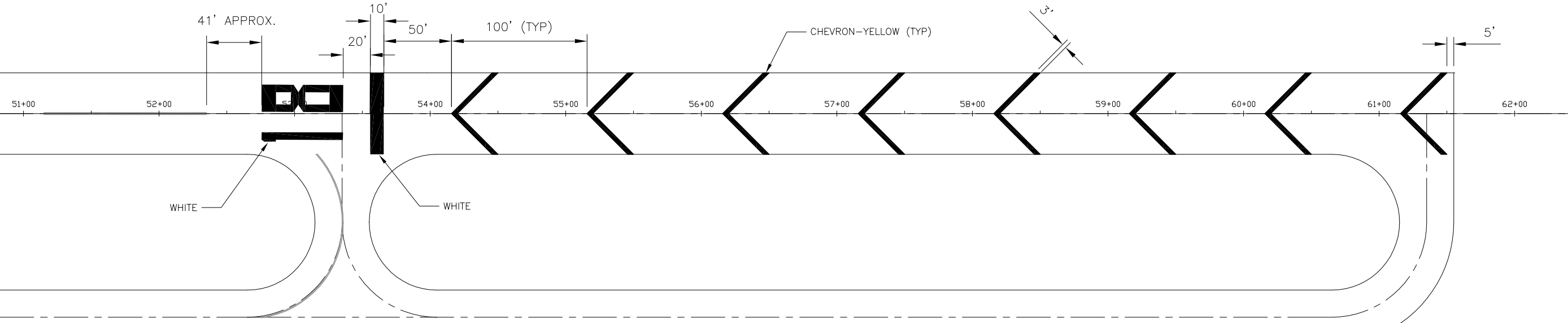
RUNWAY 18 EXTENSION
 MARKING PLAN 2

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SHEET 19 OF 38 SHEETS	

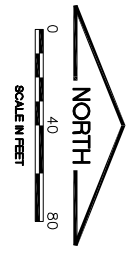
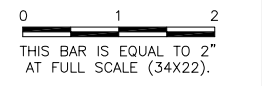
TEMPORARY THRESHOLD MARKINGS

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 Proposed Geometry
 BASE MT HAWLEY



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LEGEND

- TEMPORARY MARKING
- EXISTING MARKING

NOTE:

1. ONLY EXISTING MARKINGS AFFECTED BY THE PLACEMENT OF THE TEMPORARY MARKINGS SHALL BE REMOVED PRIOR TO THE CONSTRUCTION OF THE TEMPORARY THRESHOLD.
2. ONCE TEMPORARY THRESHOLD IS REMOVED, THE REMAINING EXISTING MARKINGS SHOWN FOR REMOVAL ON MARKING PLAN 1 SHALL BE REMOVED AND REPAINTED ACCORDING TO THE NEW DIMENSIONS ON MARKING PLAN 2.
3. NO BLACK BORDER ON YELLOW CHEVRON MARKINGS.

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**RUNWAY 18 EXTENSION
 MARKING PLAN 3**

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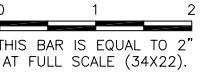
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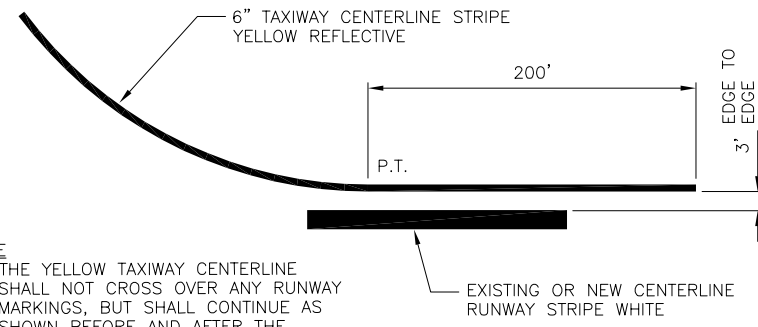
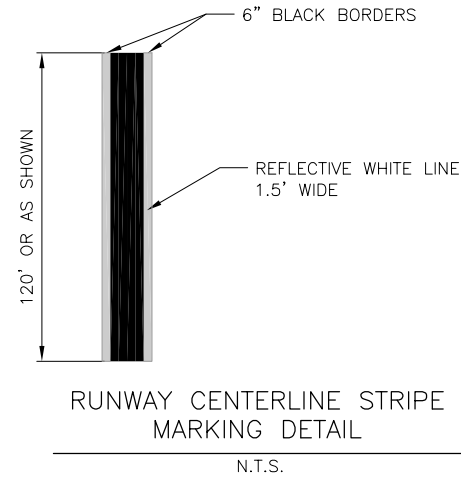
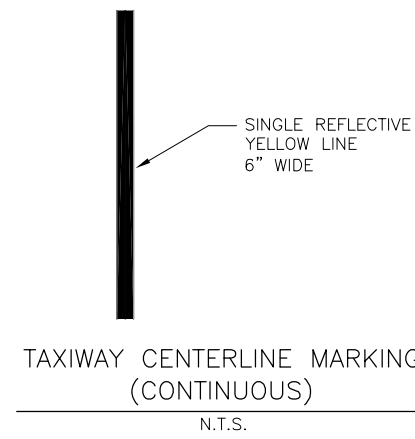
REVISIONS

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MARKING NOTES

1. ALL NEW AIRFIELD MARKING SHALL HAVE REFLECTIVE BEADS & 6" BLACK BORDER, AS NOTED.
2. BLACK BORDER DOES NOT INCLUDE REFLECTIVE BEADS.

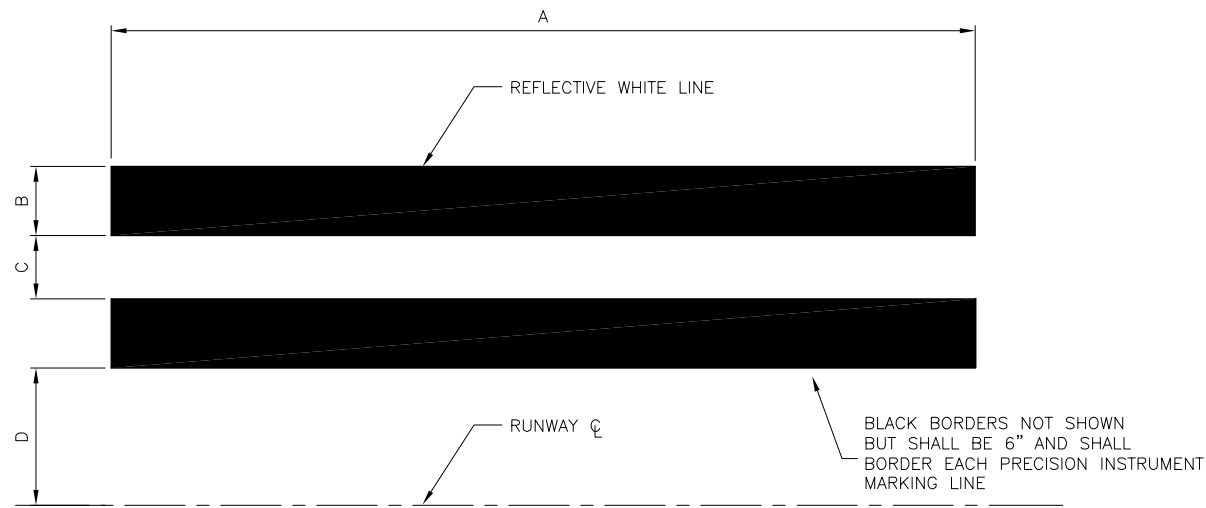


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RUNWAY 18 EXTENSION
 MARKING DETAILS 1

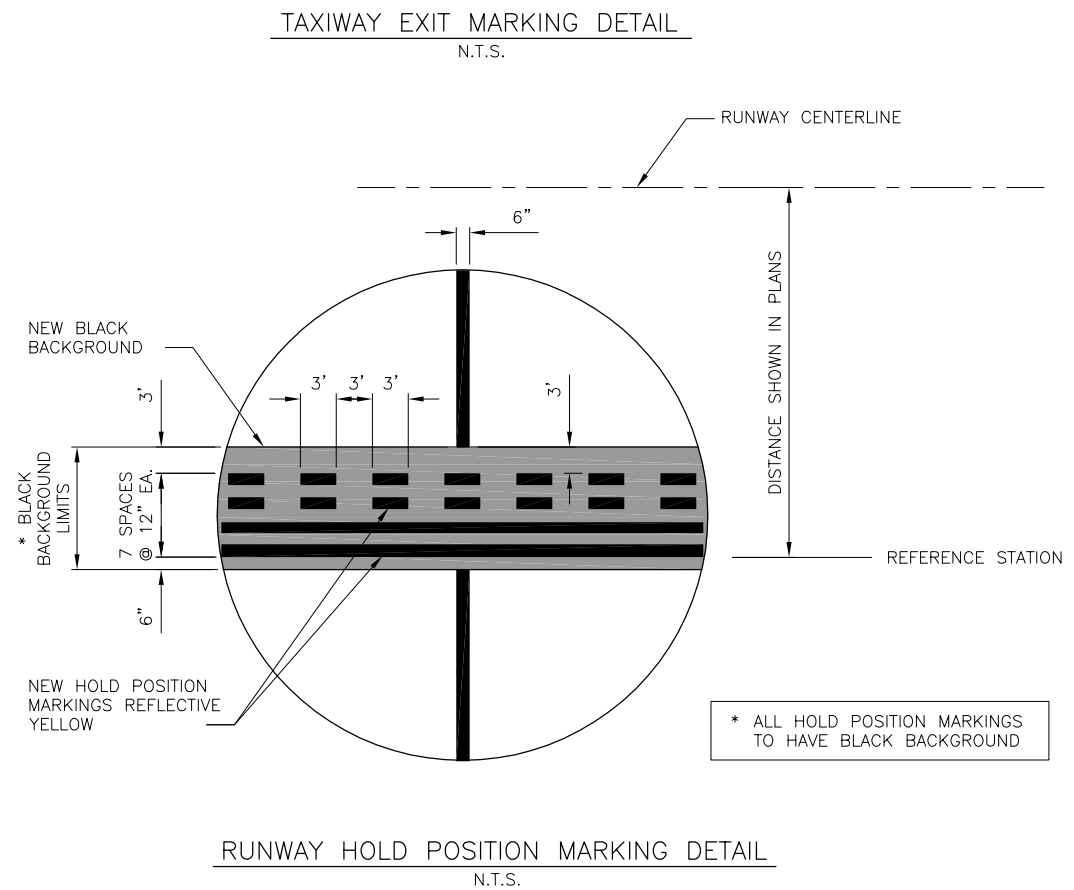
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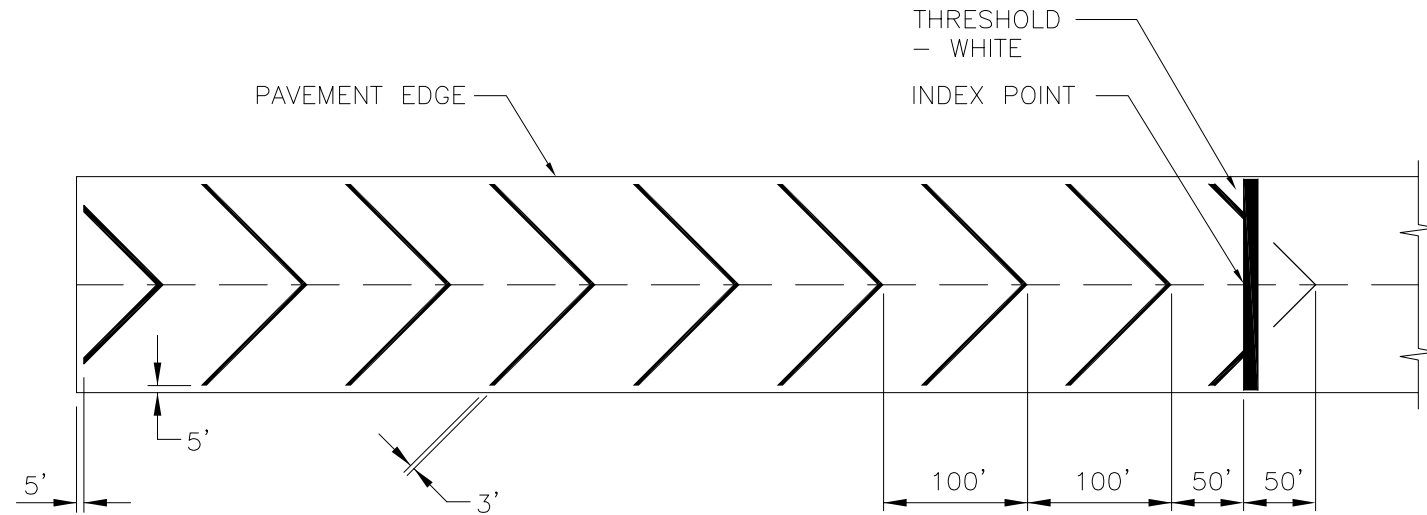
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SHEET	21 OF 38 SHEETS



150' RUNWAY				
MARKING (PER RUNWAY END)	DIMENSION *			
	A	B	C	D
THRESHOLD MARKER	150'	5.75'	5.75'	5.75'

PRECISION INSTRUMENT MARKING DETAIL
 N.T.S.

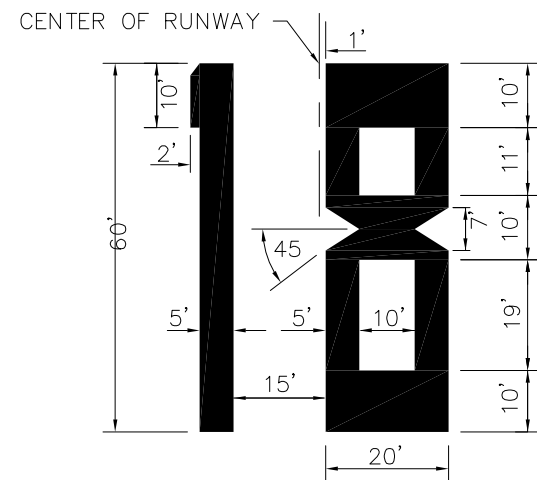




MARKINGS FOR TEMPORARY THRESHOLD
 N.T.S.

NOTES

- CHEVRON ARE YELLOW AND AT AN ANGLE OF 45 DEGREES TO THE RUNWAY CENTERLINE.



318 SQ. FT. 875 SQ. FT.

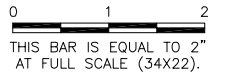
DETAIL - RUNWAY NUMERALS
 N.T.S.

MARKING NOTES

- ALL NEW AIRFIELD MARKING SHALL HAVE REFLECTIVE BEADS & 6" BLACK BORDER, WITH THE EXCEPTION OF CHEVRONS AND TAXIWAY CENTERLINE DOES NOT HAVE A BLACK BORDER.
- BLACK BORDER DOES NOT INCLUDE REFLECTIVE BEADS.
- AREA SHOWN FOR THE NUMERALS INCLUDES THE AREA OF THE BLACK BORDER TO BE PAINTED.

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**MT. HAWLEY AUXILIARY AIRPORT
 PEORIA, ILLINOIS**

**RUNWAY 18 EXTENSION
 MARKING DETAILS 2**

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APPROVED BY:	CET
DATE:	2/3/2012
JOB No:	11061-04-00
IL PROJ. NO.	3MY-4168
AIP PROJ. NO.	3-17-0079-B11
SHEET	22 OF 38 SHEETS

MH022

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NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

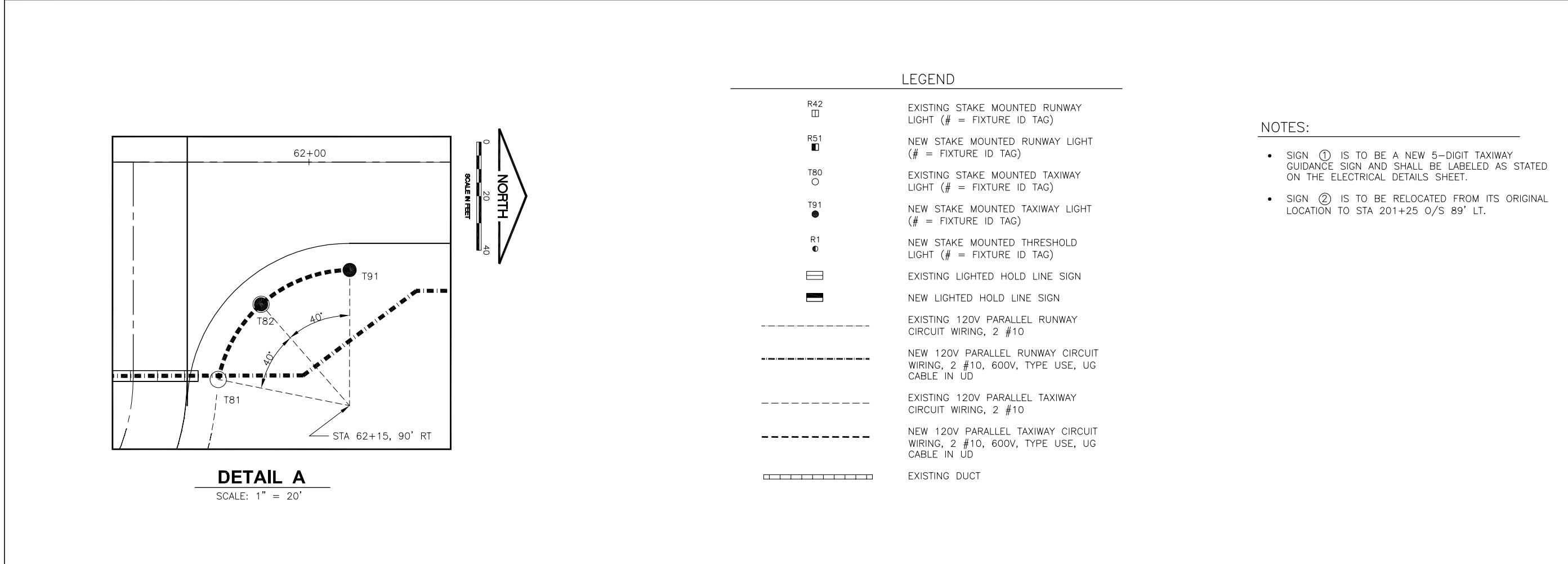
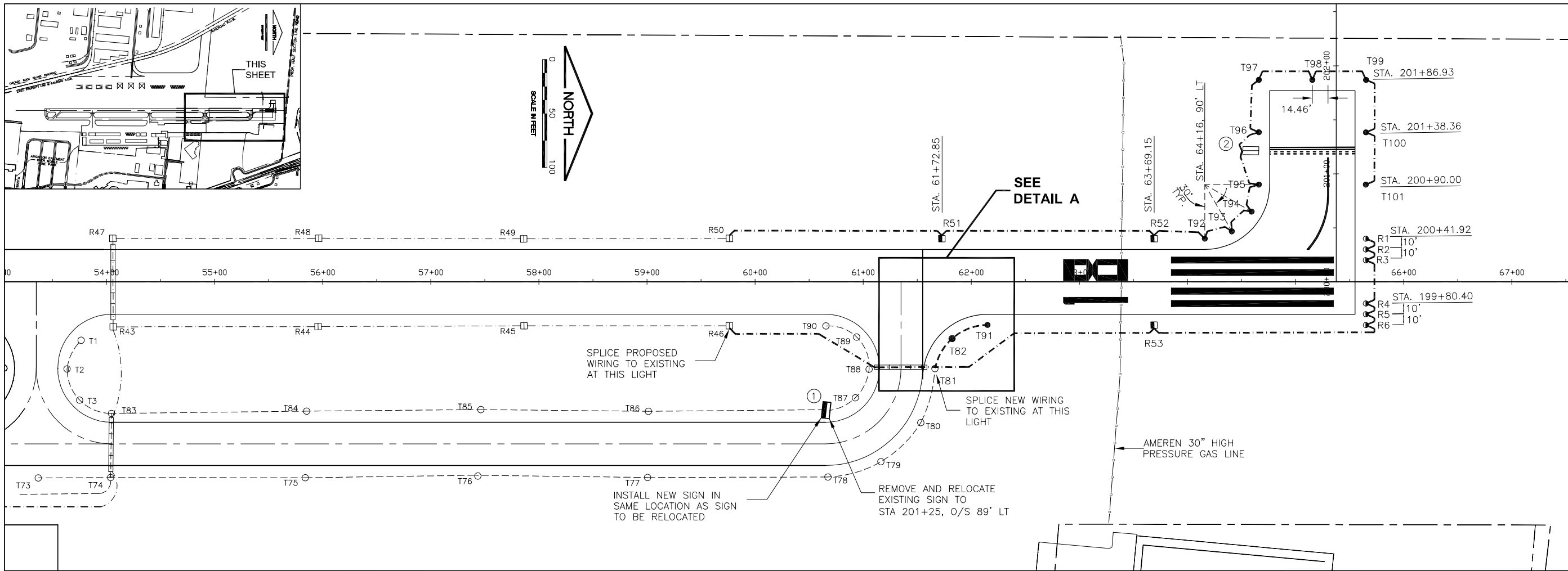
**MT. HAWLEY AUXILIARY AIRPORT
 PEORIA, ILLINOIS**
**RUNWAY 18 EXTENSION
 ELECTRICAL AND LIGHTING PLAN**

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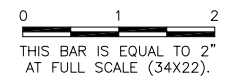


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SHEET	23 OF 38 SHEETS



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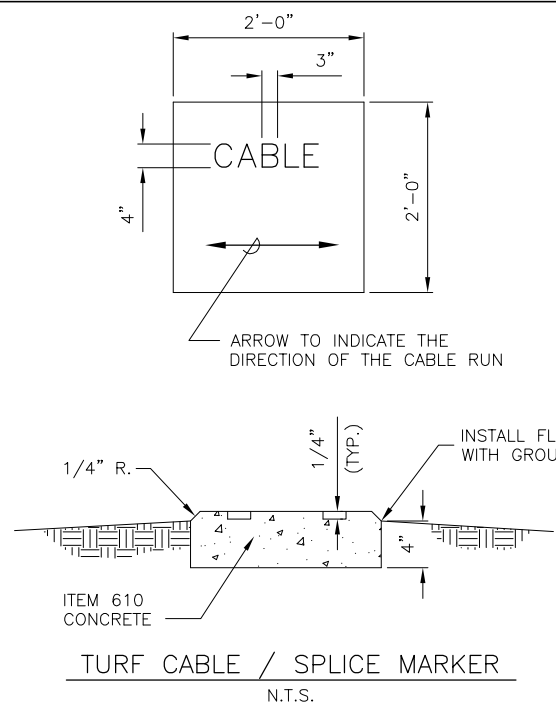
**MT. HAWLEY AUXILIARY AIRPORT
 PEORIA, ILLINOIS**
**RUNWAY 18 EXTENSION
 ELECTRICAL DETAILS**

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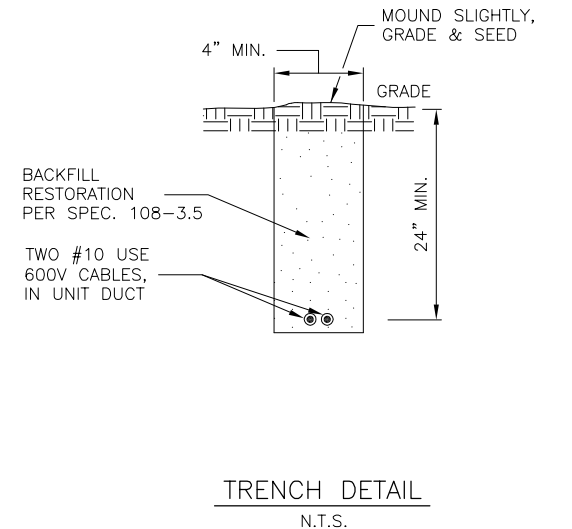
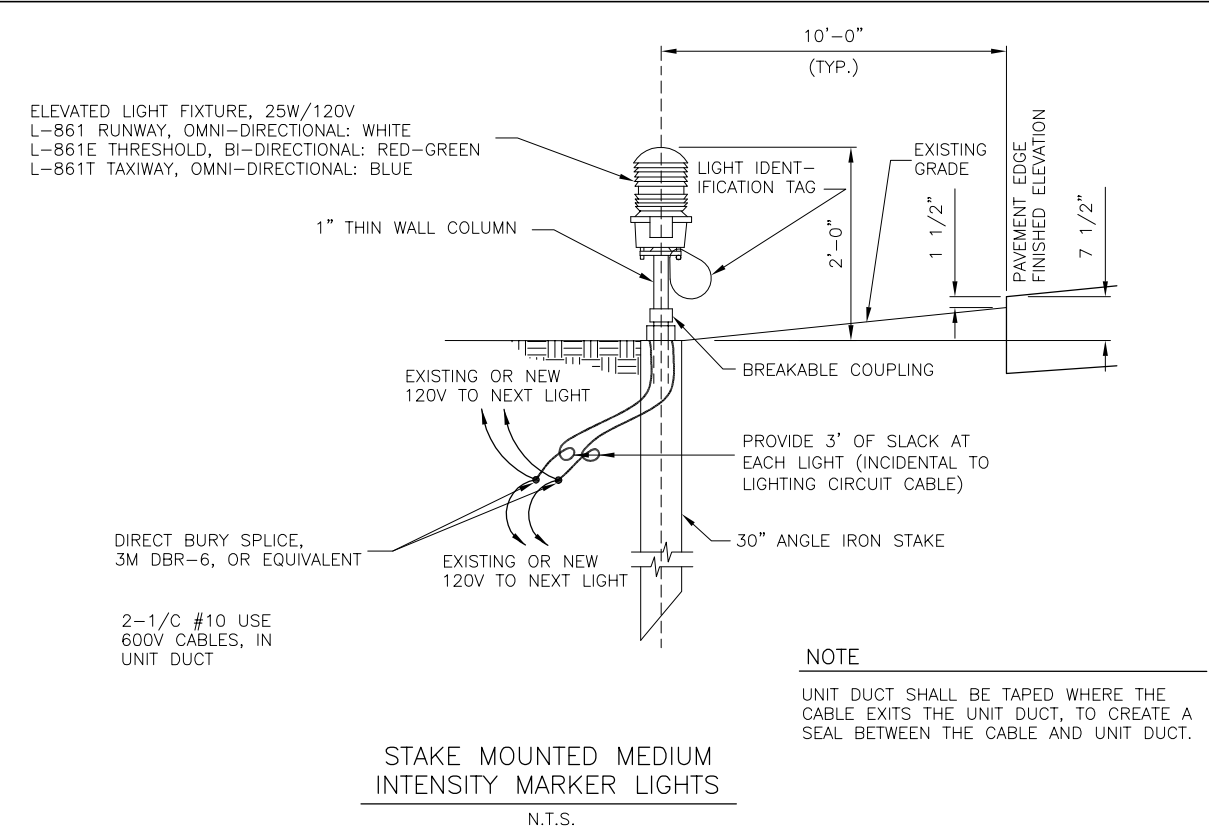
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JOB No:	11061-04-00
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AIP PROJ. NO.	3-17-0079-B11
SHEET	24 OF 38 SHEETS



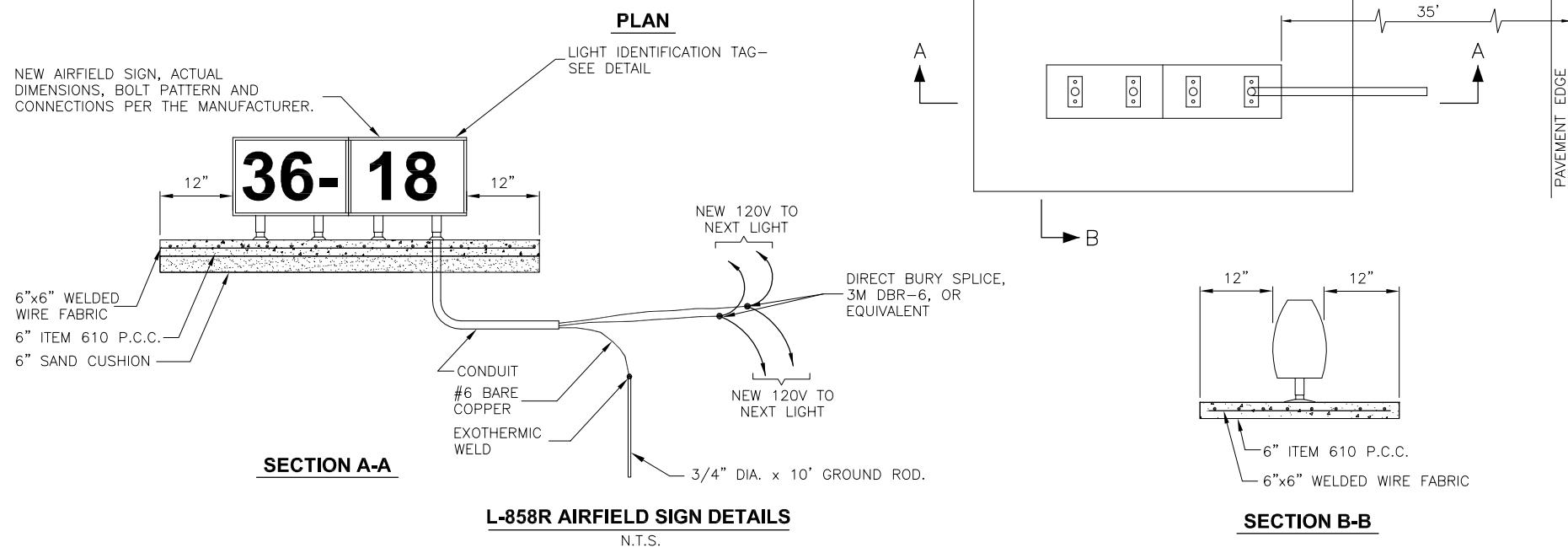
- NOTES**
- CABLE MARKERS SHALL BE INSTALLED AT ALL BENDS AND EVERY 200' ALONG THE HOMERUN.
 - ITEM 610 CONCRETE SHALL BE USED.
 - ALL EXPOSED EDGES SHALL BE EDGED WITH A 1/4" RADIUS TOOL.
 - THE COST OF FURNISHING AND INSTALLING NEW MARKERS SHALL BE INCIDENTAL TO THE ASSOCIATED ITEMS.
 - 0.049 CU. YD. CONCRETE PER MARKER.
 - A MARKER CONFORMING TO THIS DETAIL MARKED "SPLICE" SHALL BE INSTALLED AT ALL SPLICE LOCATIONS NOT IN LIGHT CANS OR MANHOLES.

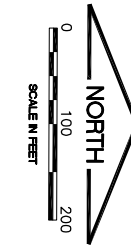
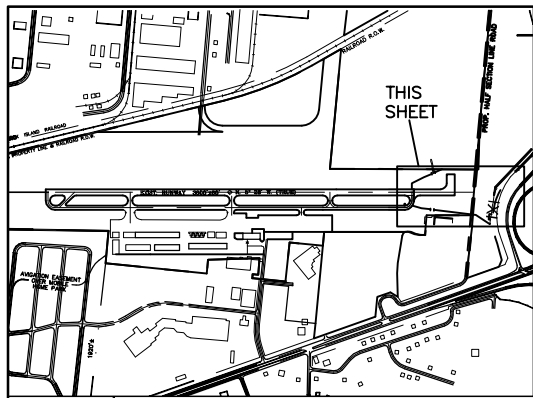


- NOTES**
- DEPTH OF TRENCHES SHALL BE AS SHOWN ABOVE UNLESS OTHERWISE SPECIFIED ON THE PLANS.
 - SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
 - ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO ITEM 108.

SIGNAGE SCHEDULE

SIGN #	SIDE	NEW SIGN LEGEND	WHITE WITH BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
1	N							
	S	36-18	36-18			5	TXY CKT.	NEW SIGN
2	W	18	18			2	RWY CKT.	RELOCATE EXISTING SIGN
	E							

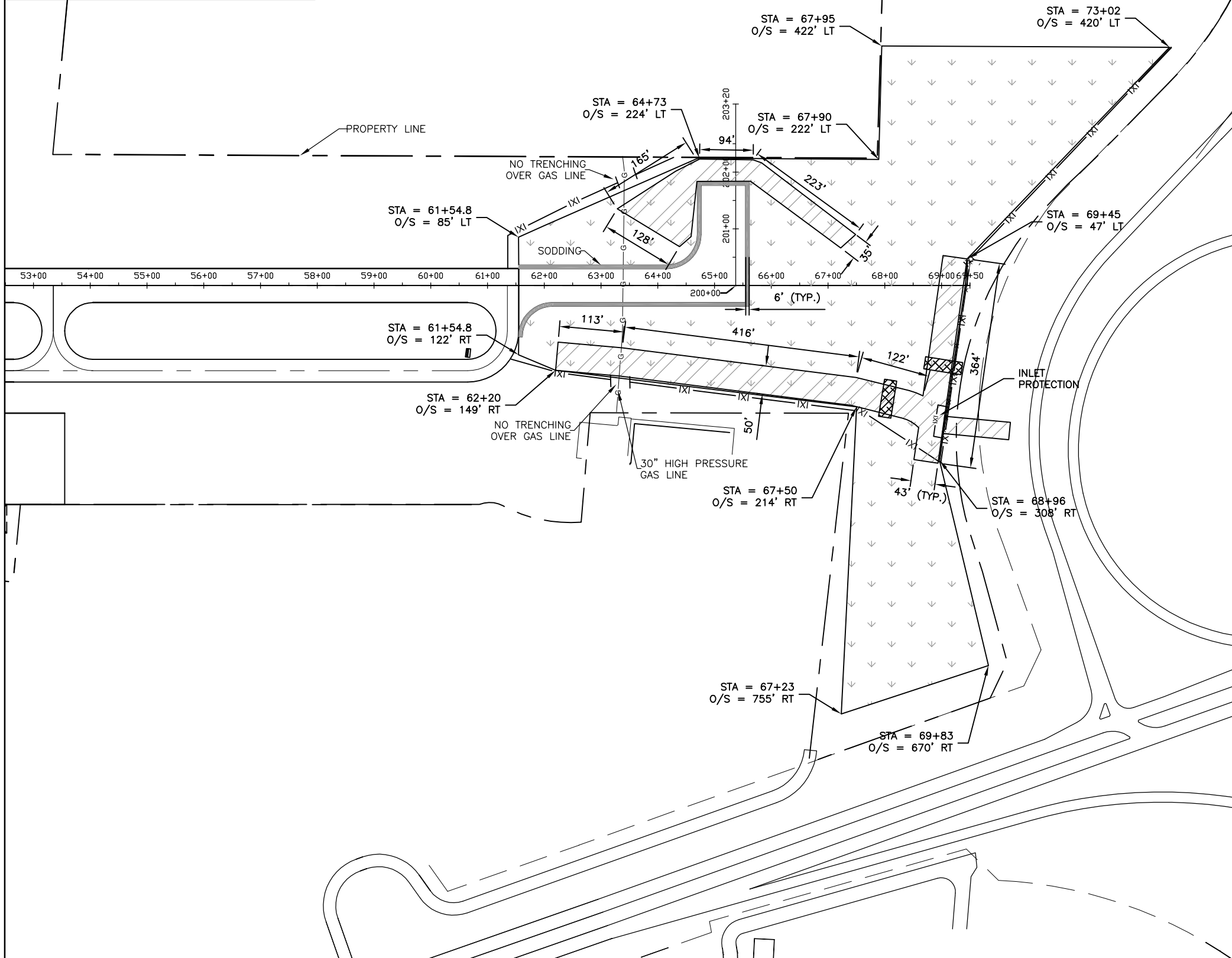
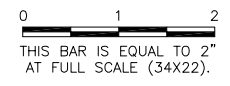




K:\MHowley\1106104\Draw\Sheets
 FILE: EROSION CONTROL AND TURFING
 UPDATE BY: Jeremy Wasilewski
 PLOT DATE: 2/13/2012 2:44 PM
 BASE: MT HAWLEY
 Proposed Geometry
 1106104-V-VF2D

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NUMBER	BY	DATE



NOTES:

- EXCELSIOR BLANKET TO BE PLACED ON ALL 4:1 SLOPES OR ANY OTHER LOCATIONS AS SHOWN ON THE PLANS.
- GRADING, SEEDING, AND EXCELSIOR BLANKET AROUND THE PROPOSED DRAINAGE STRUCTURES SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED DRAINAGE STRUCTURE AND SHALL BE COMPLETED IN A MANNER THAT RESTORES THE SURROUNDING AREA TO ITS ORIGINAL CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER.

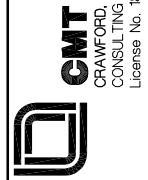
LEGEND

	NEW GRADING AND SEEDING LIMITS
	NEW EXCELSIOR BLANKET
	NEW SOD 6' STRIP
	NEW SILT FENCE
	NEW DITCH CHECK

**MT. HAWLEY AUXILIARY AIRPORT
 PEORIA, ILLINOIS**

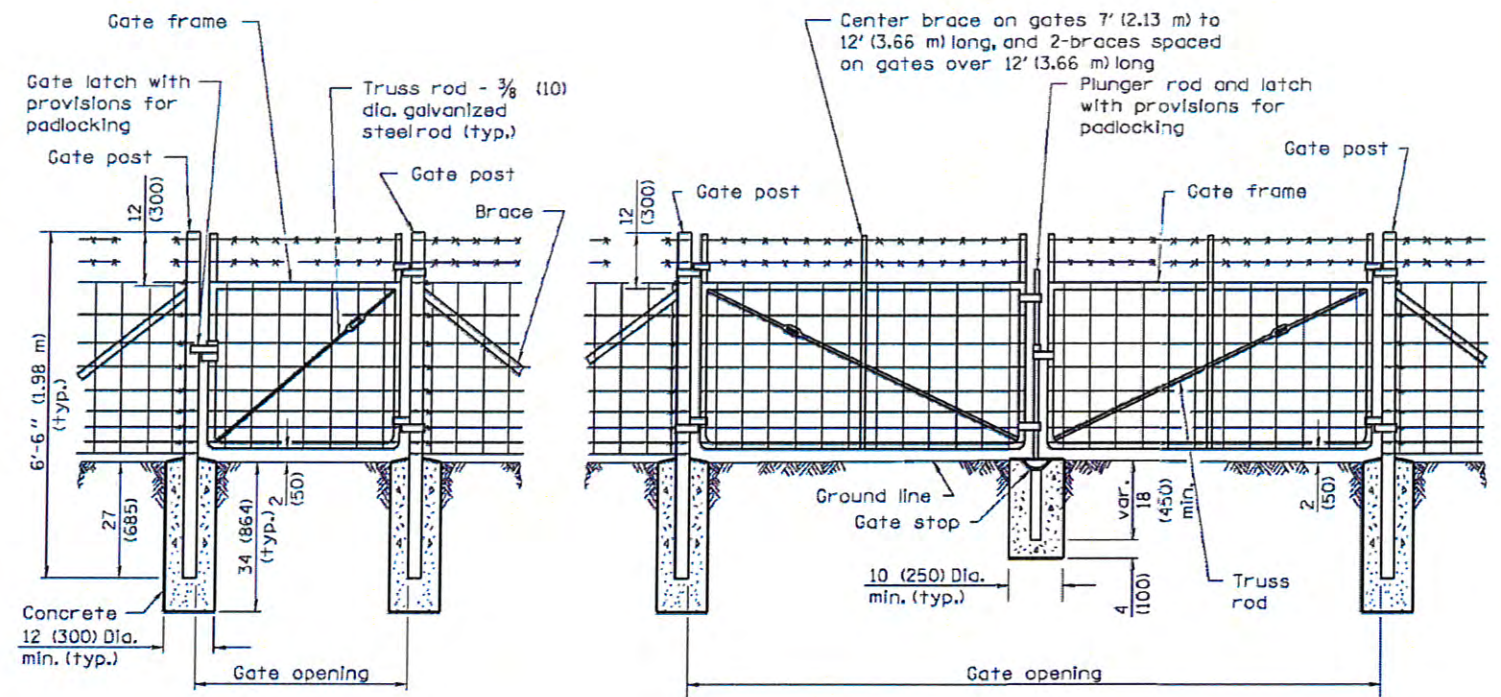
**RUNWAY 18 EXTENSION
 EROSION CONTROL AND TURFING PLAN**

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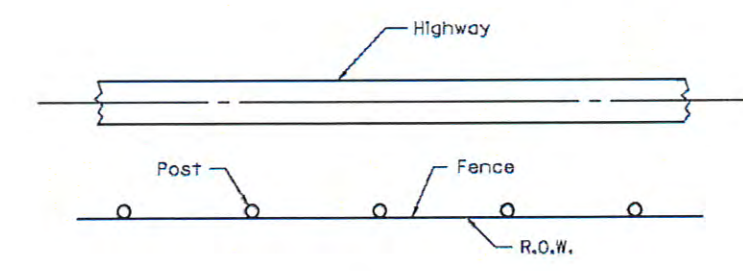
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DATE:	2/3/2012
JOB No:	11061-04-00
IL PROJ. NO.	3MY-4168
AIP PROJ. NO.	3-17-0079-B11
SHEET	25 OF 38 SHEETS

FENCE USING METAL POSTS



SINGLE GATE

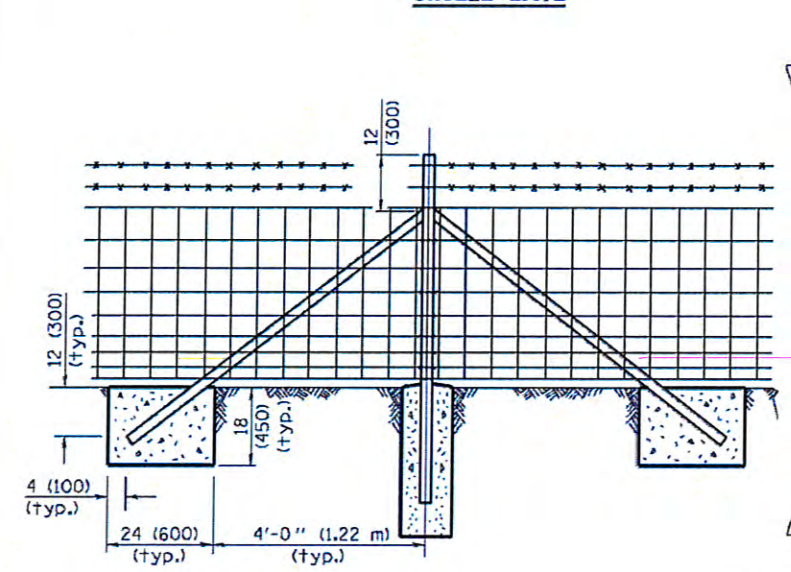
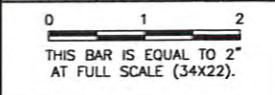
DOUBLE GATE



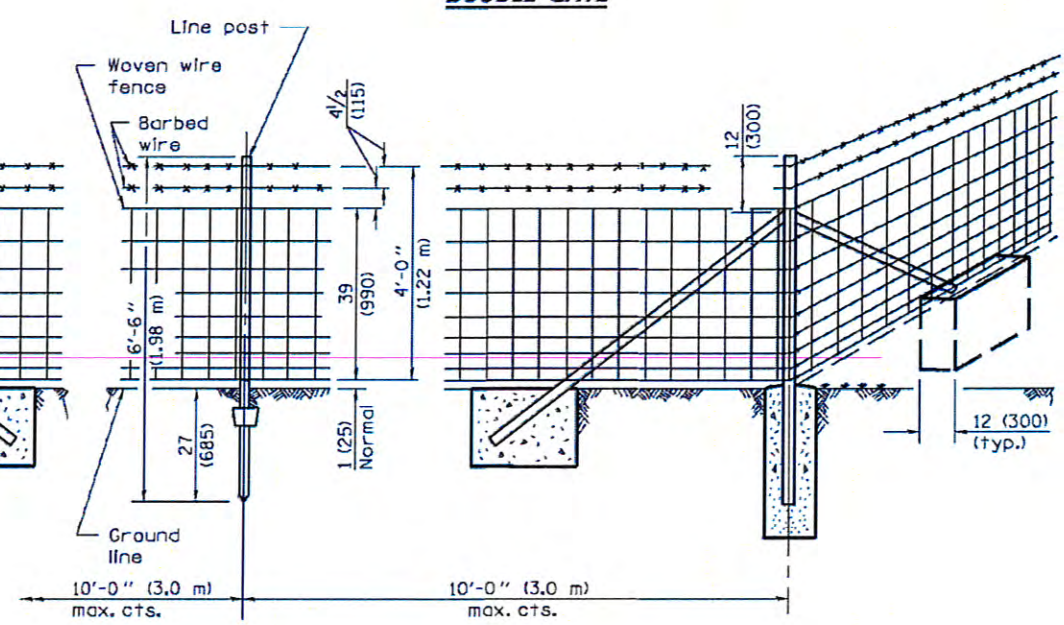
PLAN

MH022

REVISIONS		
NUMBER	BY	DATE



PULL POST



LINE POST

CORNER OR END POST

NOTES

Barbed wires shall be tied to each post. Top and bottom wires of woven fence shall be tied to each post. Tie every other wire between, alternating on successive posts.

Barbed wires and line wires of woven fence shall be fastened to the corner, end, pull, and gate posts by wrapping the wires around the post and tying back on itself with not less than 3 twists tightly wrapped.

GENERAL NOTES

Pullposts shall be placed at the locations determined by the Engineer. They shall be placed at 660' (200 m) intervals between posts to which the ends of the fabric and barbed wires are fastened or midway between such posts when the distance is less than 1320' (400 m) and greater than 660' (200 m).

Bracing for gate posts shall be the same type used for end posts.

The clearance between the bottom fence wire and the ground may be up to 3 (75) for a maximum distance of 8' (2.4 m) when uneven ground is encountered.

All dimensions are in inches (millimeters) unless otherwise shown.

Illinois Department of Transportation

PASSED January 1, 2009
 ENGINEER OF POLICY AND PROCEDURES

APPROVED January 1, 2009
 ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED 1-1-1

DATE	REVISIONS
1-1-09	Switched units to English (metric).
1-1-02	Corrected dimensions on sheet 3 and 4.

WOVEN WIRE FENCE

STANDARD 665001-02

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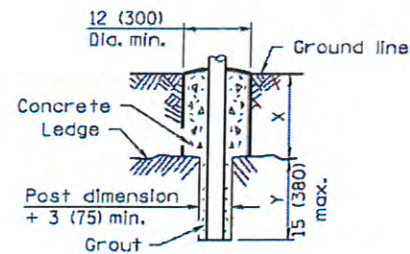
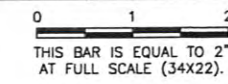
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APPROVED BY:	CET
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AIP PROJ. NO.	3-17-0079-B11
SHEET	26 OF 38 SHEETS

MT. HAWLEY AUXILIARY AIRPORT
PEORIA, ILLINOIS
RUNWAY 18 EXTENSION
FENCE DETAILS 1

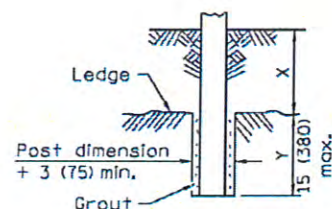
(Sheet 1 of 4)

MH022

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NUMBER	BY	DATE



METAL POST

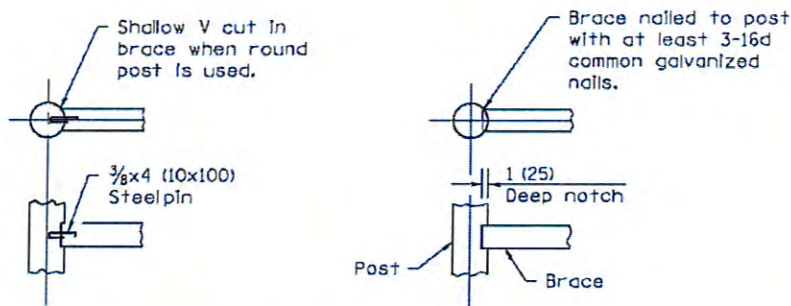


WOOD POST

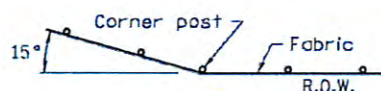
NOTE

X + Y shall not exceed 27 (685), 33 (840), or 3'-9" (1.14 m) as applicable. When X is 0 to 12 (300), 18 (450), or 30 (760), Y = 15 (380), and the post shall be shortened as required. When X exceeds 12 (300), 18 (450), or 30 (760), Y shall be decreased correspondingly.

**FOOTING FOR POSTS
 WHEN ROCK LEDGE IS ENCOUNTERED**



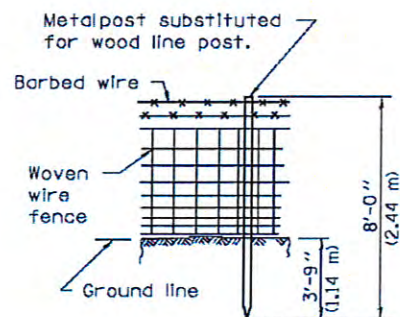
**ALTERNATE DETAILS FOR FASTENING
 WOOD BRACE TO WOOD POST**



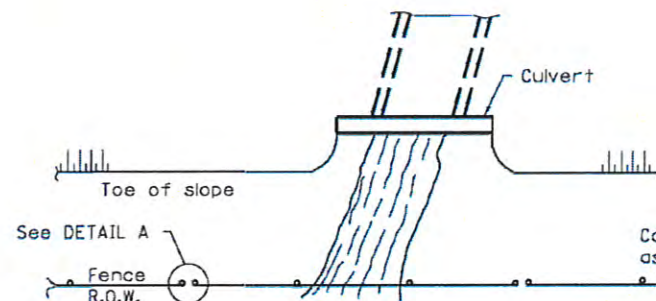
NOTE

Where fence line has a change in direction of 15° or more, a corner post with bracing as required shall be placed as shown above. Where angle is less than 15° and existing conditions require a corner post, they shall be placed as directed by the Engineer.

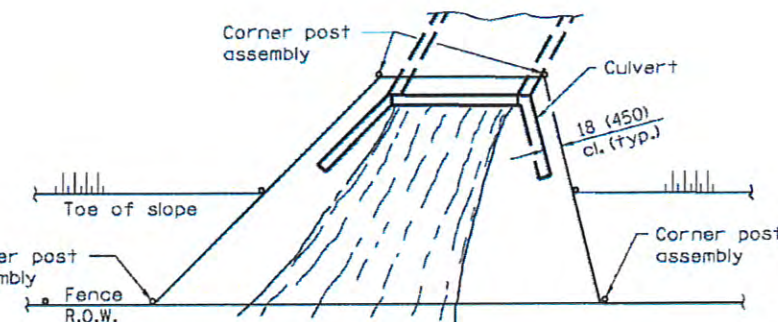
INSTALLATION AT CORNERS



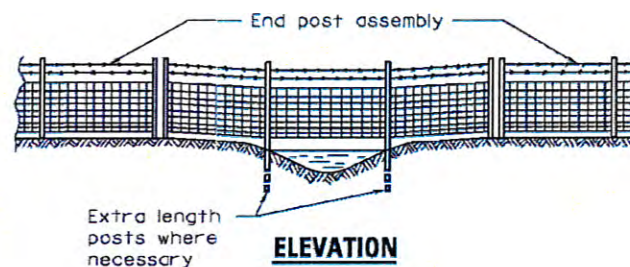
**PROTECTIVE ELECTRICAL GROUNDING
 FOR WOOD POST FENCE INSTALLATION**



PLAN AT STREAM CROSSING



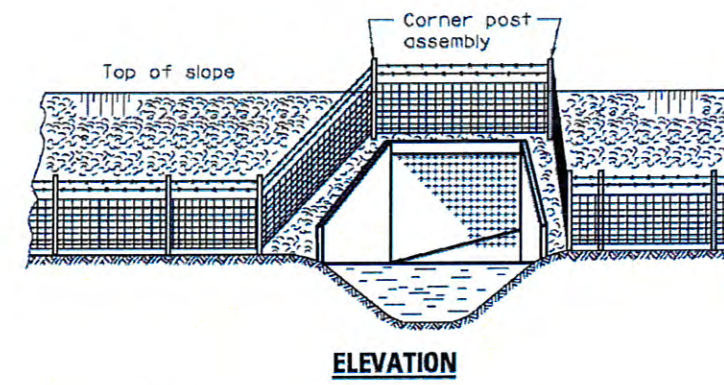
PLAN AT HEADWALL



NOTE

The woven wire fabric shall be replaced by barbed wire strands at 12 (300) maximum centers between the double posts shown on DETAIL A when shown on the plans.

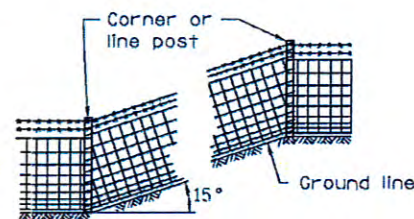
INSTALLATION OVER STREAM



NOTE

When the width of the culvert makes it necessary to anchor a post to the top of the culvert, a cast iron shoe or other device approved by the Engineer shall be used.

INSTALLATION AROUND HEADWALL

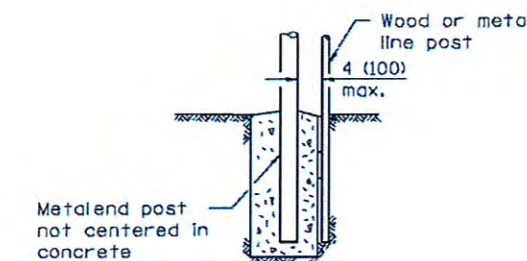


NOTE

Where grade line has a change in slope of 15° or more, a corner post with bracing as required shall be placed as shown above. Where angle is less than 15° line posts may be used.

When the tension of the fence tends to pull the posts from the ground, the line posts shall be anchored with the applicable concrete or wood anchorage specified for corner posts.

INSTALLATION ON SLOPES



DETAIL A

WOVEN WIRE FENCE

(Sheet 4 of 4)

STANDARD 665001-02

Illinois Department of Transportation

PASSED January 1, 2009

ENGINEER OF POLICY AND PROCEDURES

APPROVED January 1, 2009

ENGINEER OF DESIGN AND ENVIRONMENT

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 PEORIA, ILLINOIS

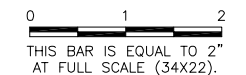
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 FENCE DETAILS 2

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SHEET	27 OF 38 SHEETS

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NUMBER	BY	DATE



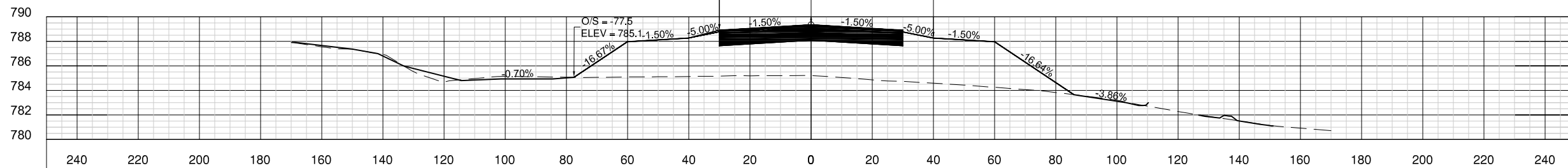
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 PEORIA, ILLINOIS**

RUNWAY 18 EXTENSION

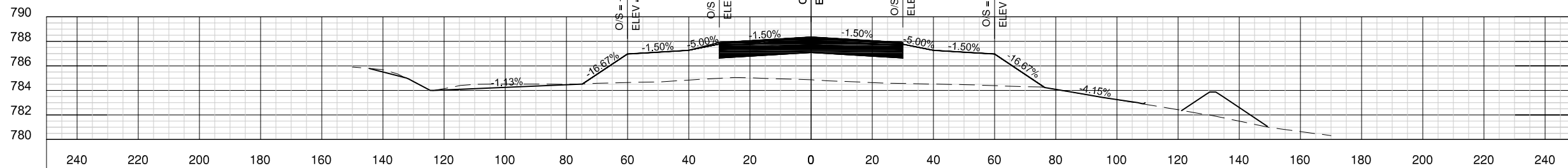
RUNWAY 18 CROSS SECTION 1

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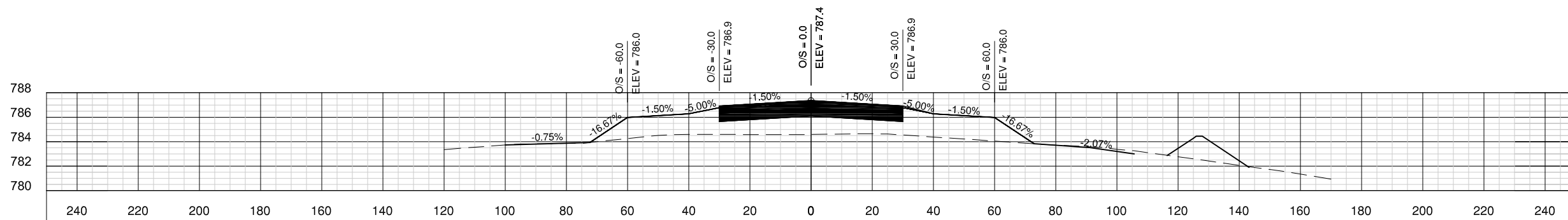
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APPROVED BY:	CET
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AIP PROJ. NO.	3-17-0079-B11
SHEET	29 OF 38 SHEETS



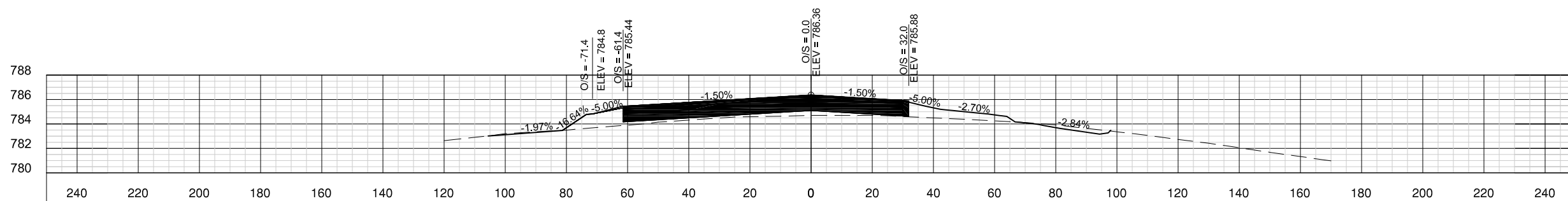
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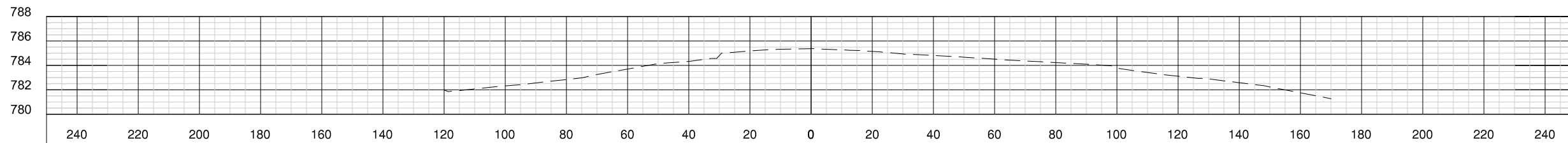
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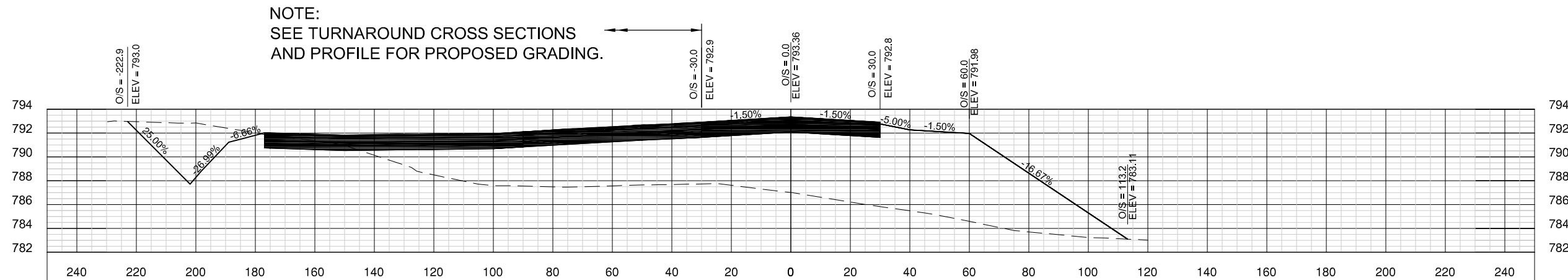
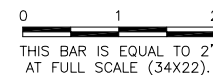


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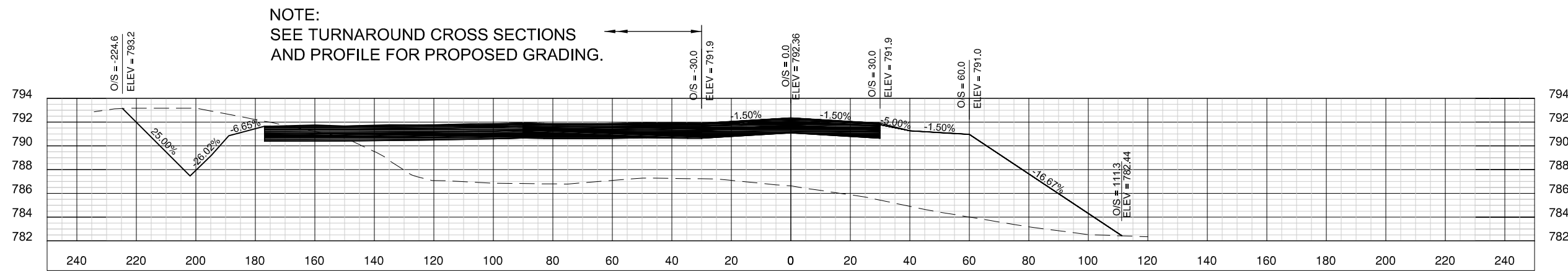
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REVISIONS

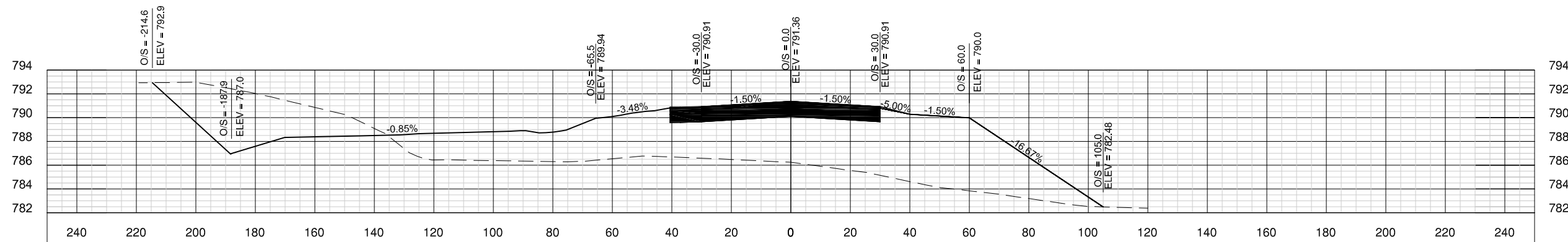
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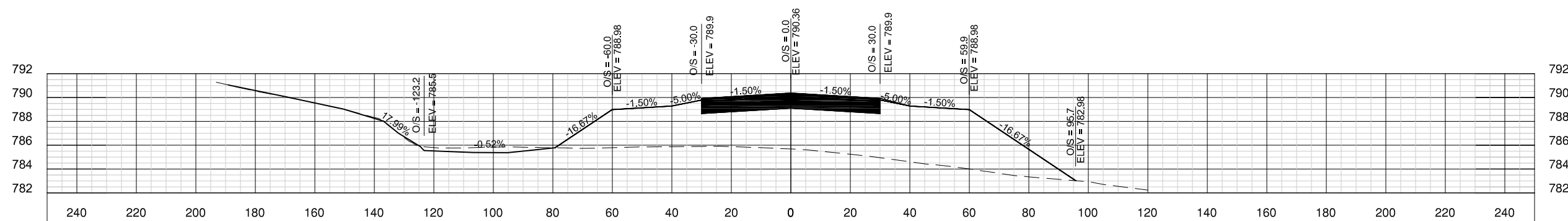
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**MT. HAWLEY AUXILIARY AIRPORT
 PEORIA, ILLINOIS**

**RUNWAY 18 EXTENSION
 RUNWAY 18 CROSS SECTION 2**

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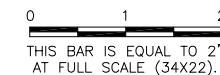
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DRAWN BY:	CMT
CHECKED BY:	CET
APPROVED BY:	CET
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SHEET	30 OF 38 SHEETS

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REVISIONS

NUMBER	BY	DATE



**MT. HAWLEY AUXILIARY AIRPORT
 PEORIA, ILLINOIS**

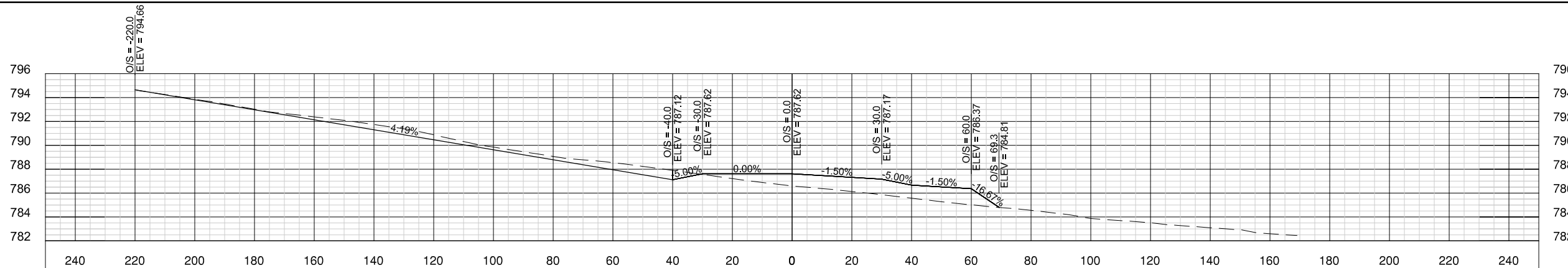
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 RUNWAY 18 CROSS SECTION 3**

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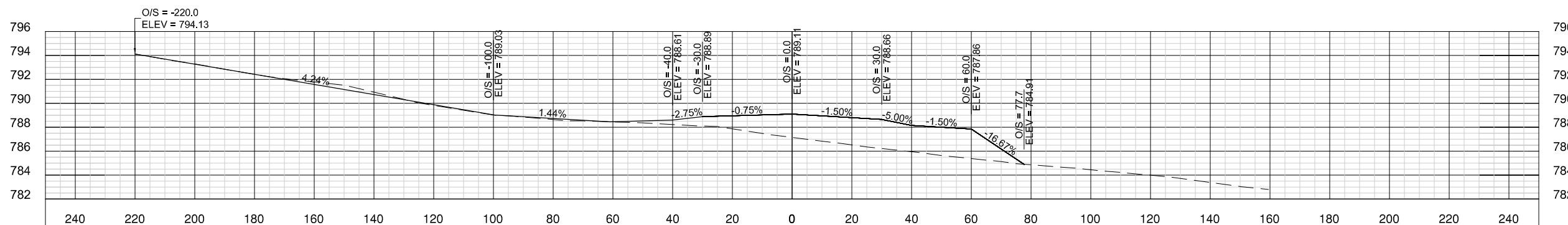


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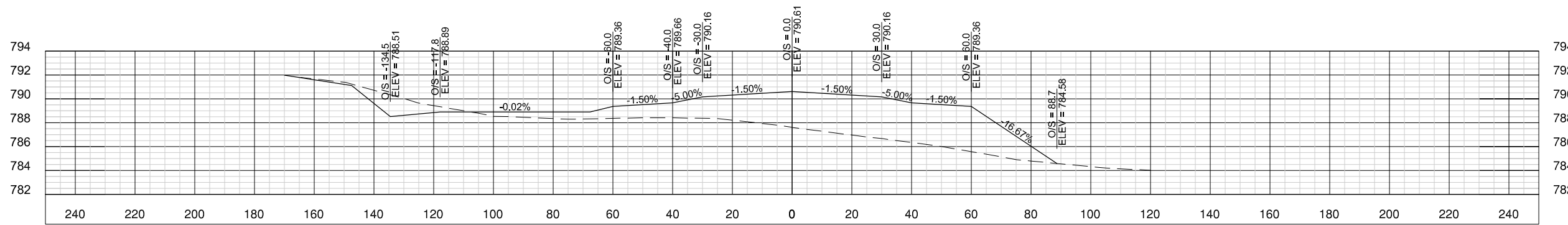
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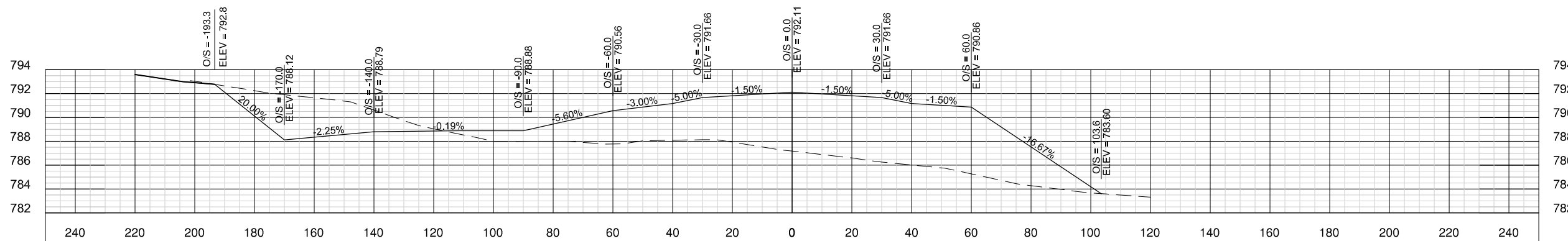
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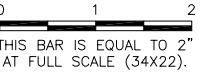


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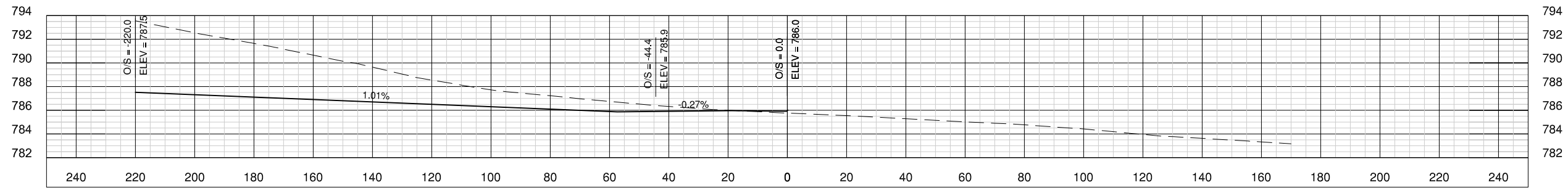
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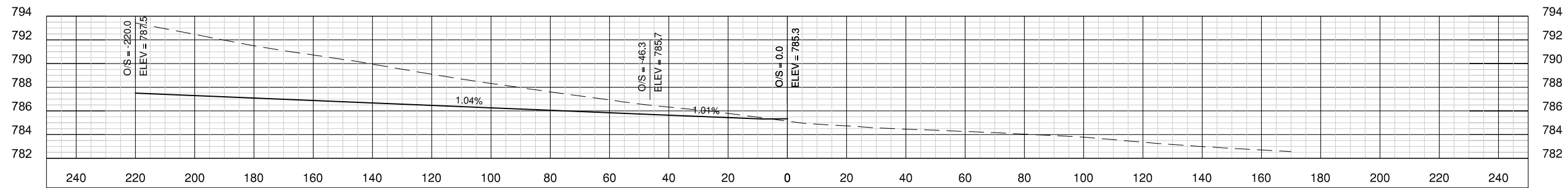
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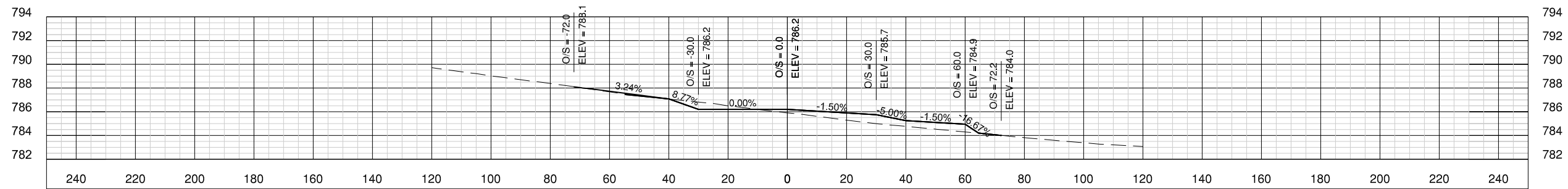
NOTE:
 CROSS SECTION 68+50 AND 69+00 ARE USED FOR
 REFERENCE ONLY. TO SEE PROPOSED GRADING
 REFER TO BORROW CROSS SECTIONS.



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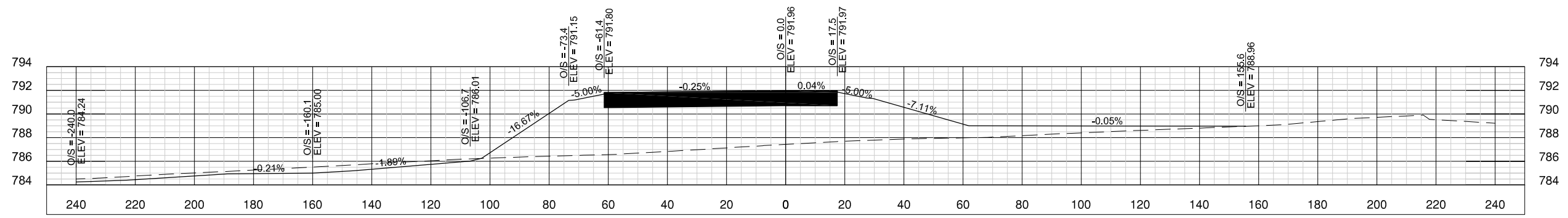
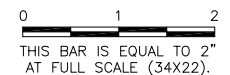
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 PEORIA, ILLINOIS**
RUNWAY 18 EXTENSION
RUNWAY 18 CROSS SECTION 4

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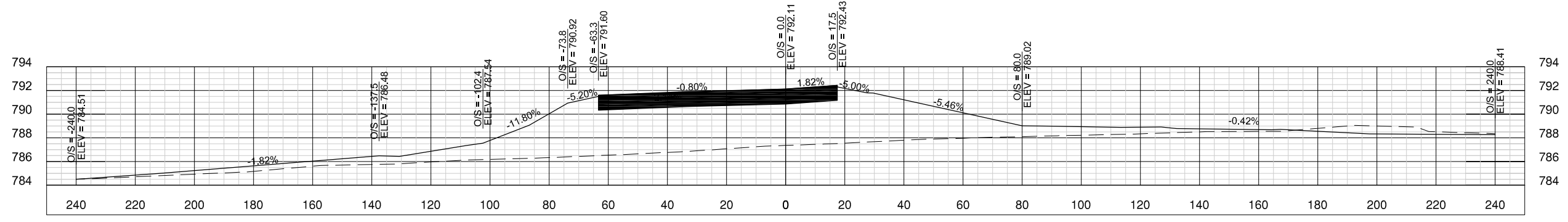
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DRAWN BY:	CMT
CHECKED BY:	CET
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DATE:	2/3/2012
JOB No:	11061-04-00
IL PROJ. NO.	3MY-4168
AIP PROJ. NO.	3-17-0079-B11
SHEET	32 OF 38 SHEETS

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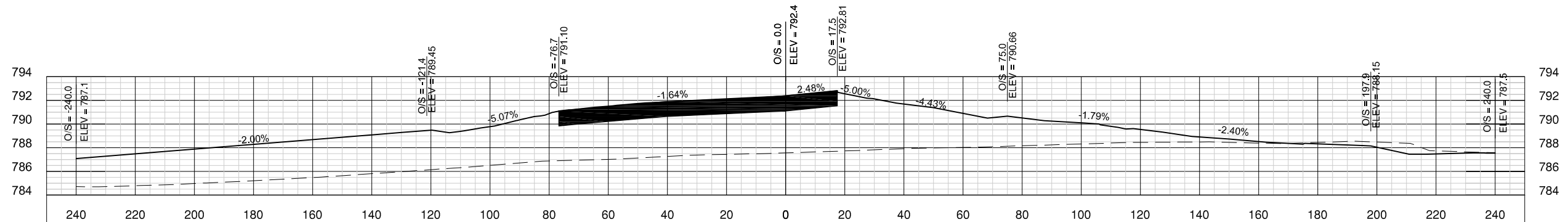
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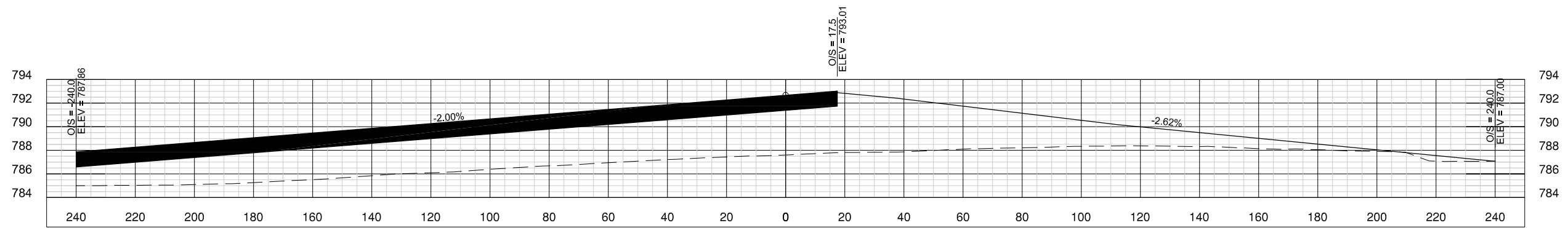
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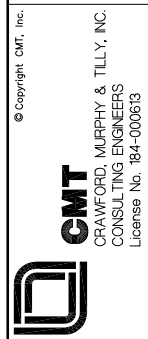


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**MT. HAWLEY AUXILIARY AIRPORT
 PEORIA, ILLINOIS**
**RUNWAY 18 EXTENSION
 TURNAROUND CROSS SECTION 1**

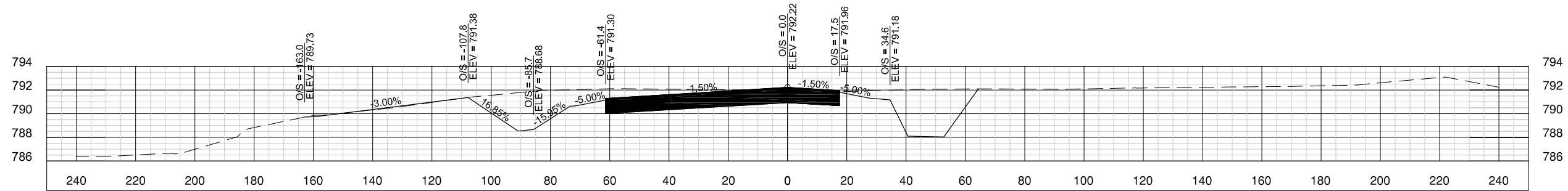
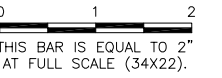


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IL PROJ. NO.	3MY-4168
AIP PROJ. NO.	3-17-0079-B11
SHEET	33 OF 38 SHEETS

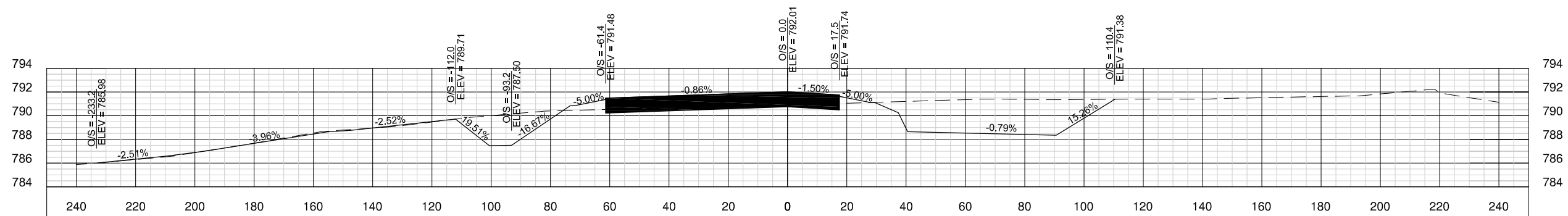
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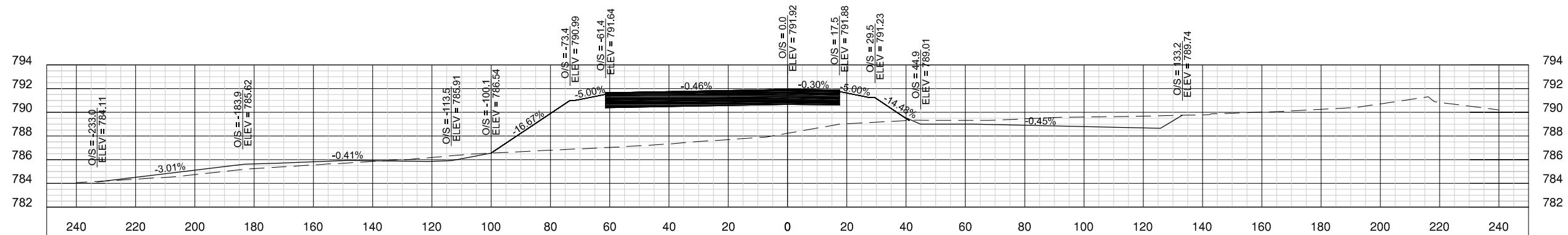
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**MT. HAWLEY AUXILIARY AIRPORT
 PEORIA, ILLINOIS**
**RUNWAY 18 EXTENSION
 TURNAROUND CROSS SECTION 2**

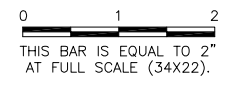
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 DATE: 2/3/2012
 JOB No: 11061-04-00
 IL. PROJ. NO. 3MY-4168
 AIP PROJ. NO. 3-17-0079-B11
 SHEET 34 OF 38 SHEETS

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 PEORIA, ILLINOIS**

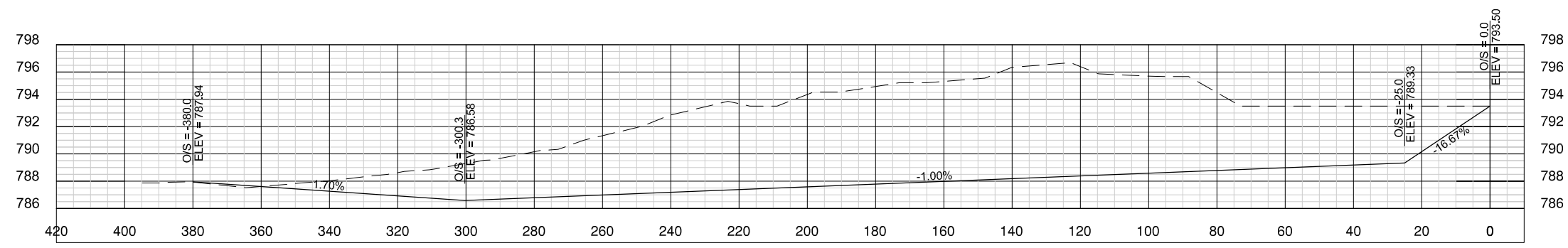
**RUNWAY 18 EXTENSION
 BORROW CROSS SECTION 1**

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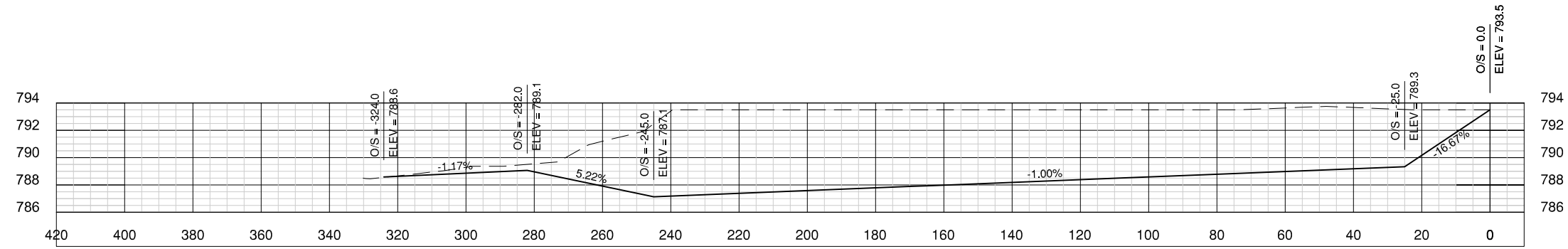
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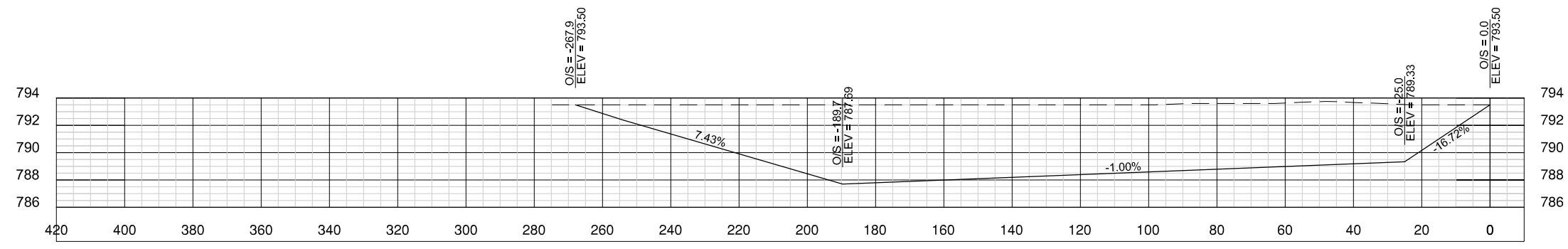
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JOB No:	11061-04-00
IL PROJ. NO.	3MY-4168
AIP PROJ. NO.	3-17-0079-B11
SHEET	35 OF 38 SHEETS



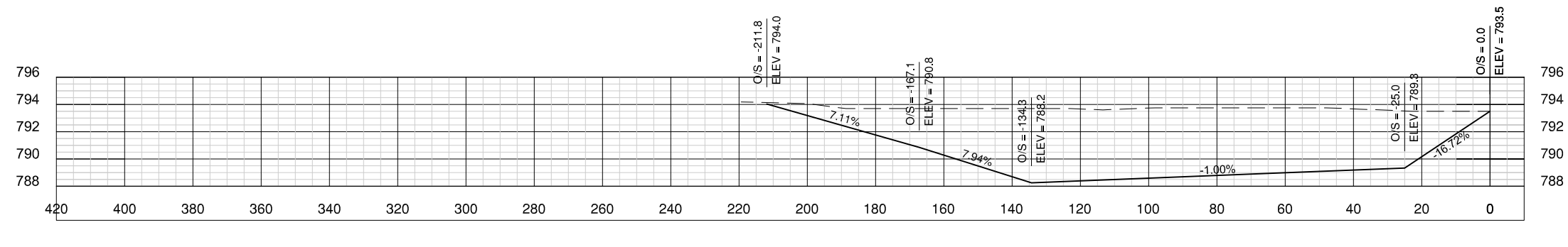
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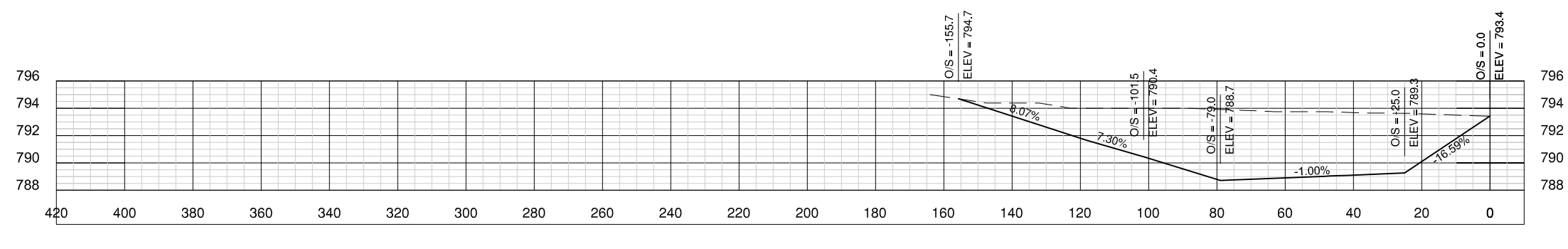
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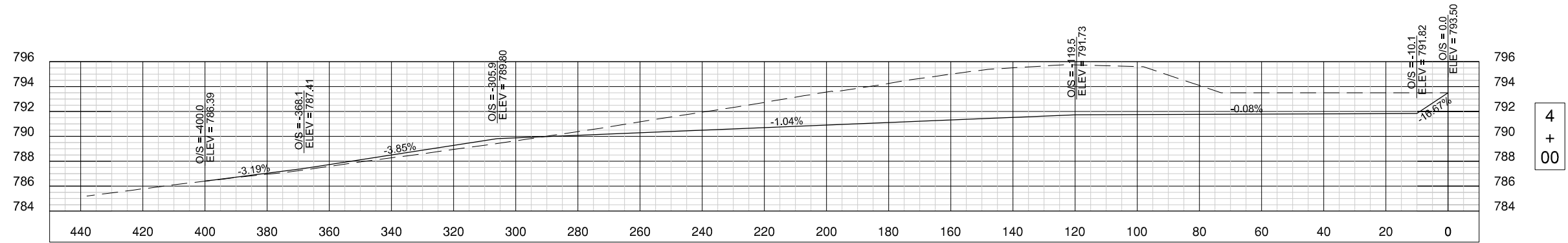
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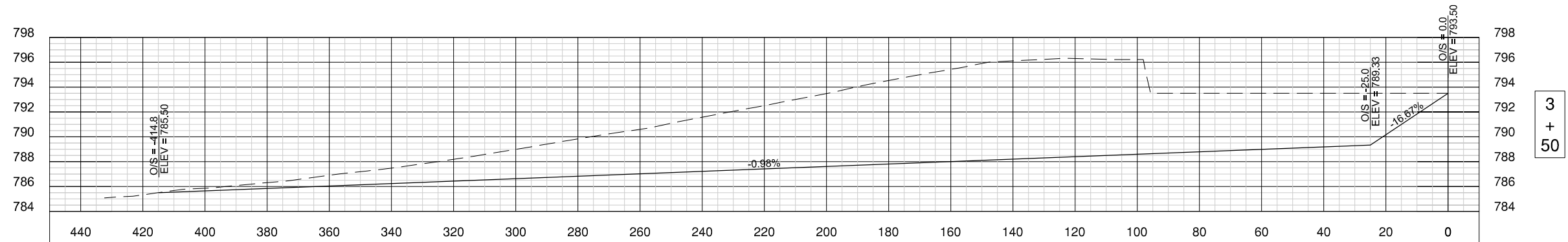
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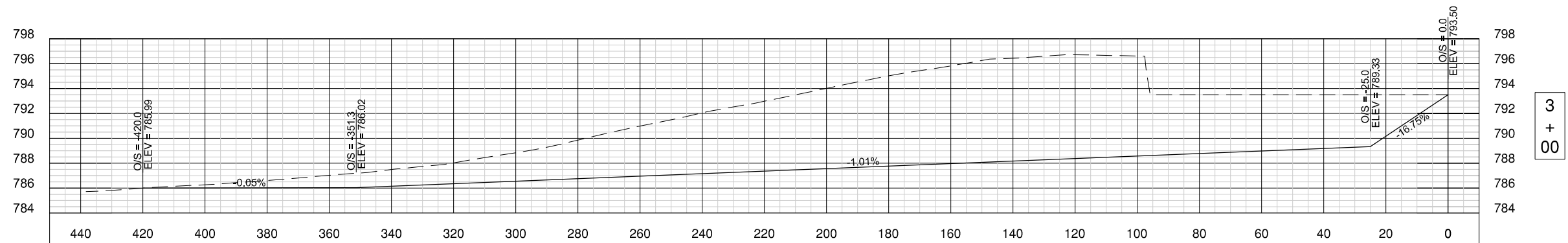
0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



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3
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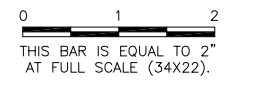
**MT. HAWLEY AUXILIARY AIRPORT
 PEORIA, ILLINOIS**
**RUNWAY 18 EXTENSION
 BORROW CROSS SECTION 2**

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AIP PROJ. NO.	3-17-0079-B11
SHEET	36 OF 38 SHEETS

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**MT. HAWLEY AUXILIARY AIRPORT
 PEORIA, ILLINOIS**

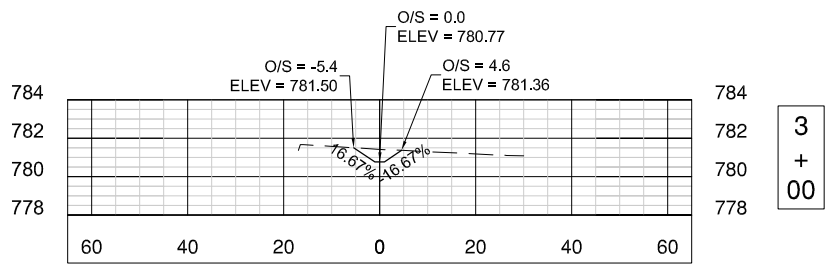
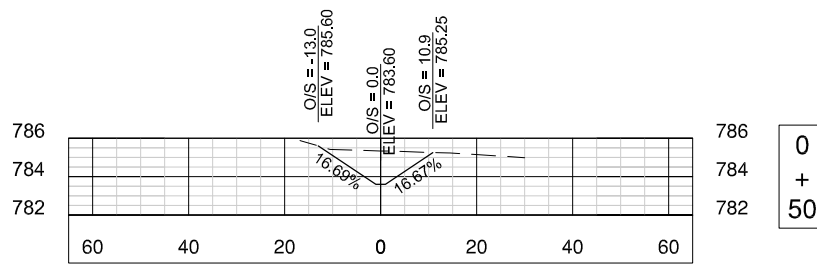
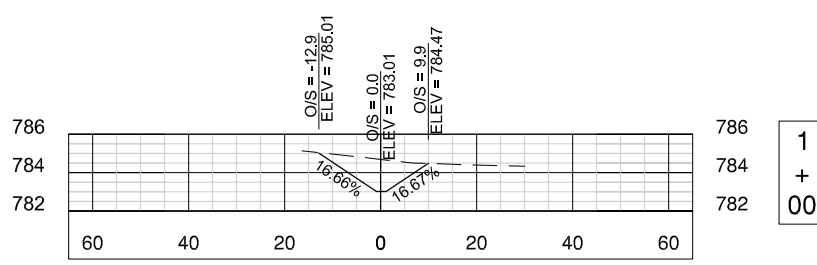
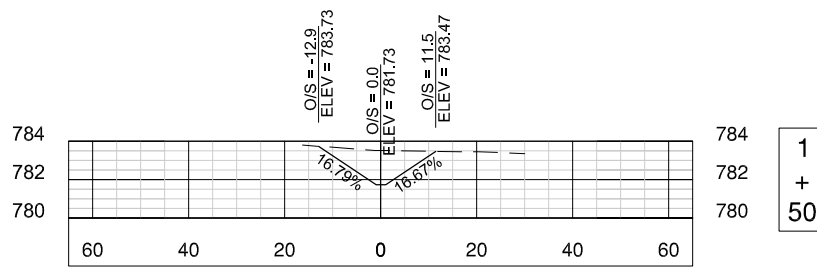
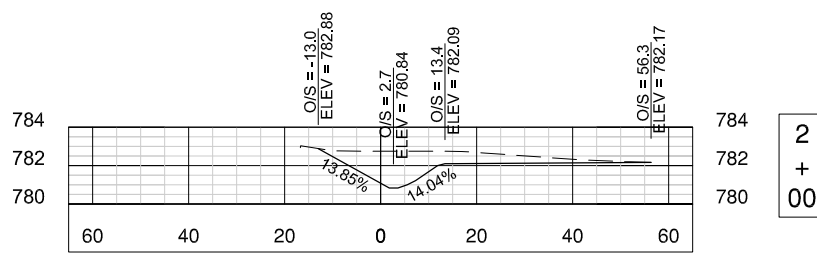
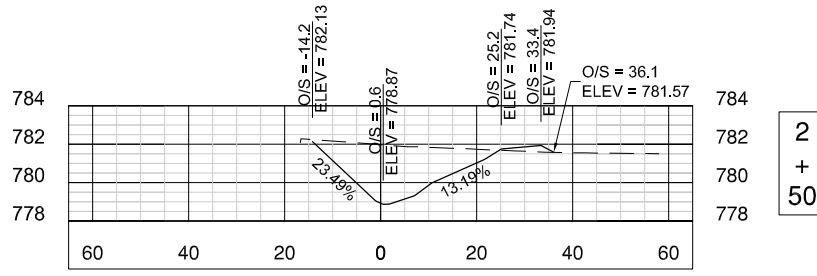
**RUNWAY 18 EXTENSION
 DITCH A CROSS SECTION 1**

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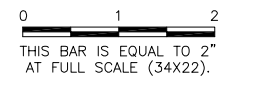


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**MT. HAWLEY AUXILIARY AIRPORT
 PEORIA, ILLINOIS**

**RUNWAY 18 EXTENSION
 DITCH B CROSS SECTION 1**

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SHEET 38 OF 38 SHEETS	

