03-09-2018 LETTING ITEM 005

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	1
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 60V22		

D-91-529-12

FOR INDEX OF SHEETS, SEE SHEET NO. 2

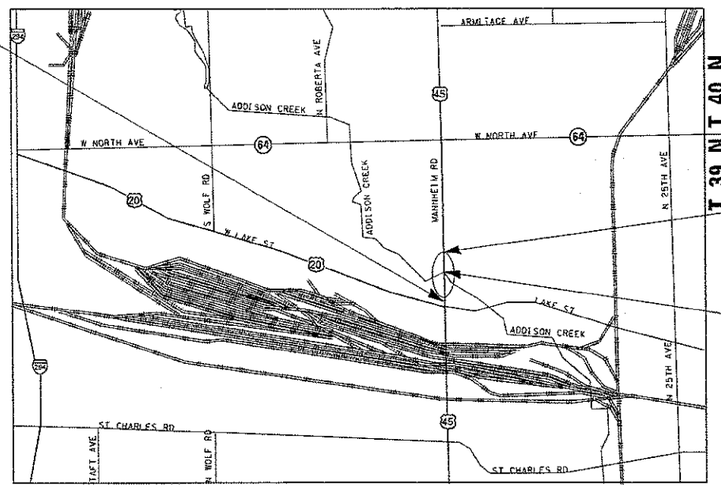
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED
HIGHWAY PLANS
F.A.P. 330: US12/45 (MANNHEIM ROAD)
OVER ADDISON CREEK
SECTION: 464-B
PROJECT NO.: NHPP-ZQVB(017)
CULVERT REPLACEMENT
COOK COUNTY
C-91-529-12

IMPROVEMENT LOCATED WITHIN THE VILLAGE OF STONE PARK

TRAFFIC DATA
MANNHEIM ROAD
2017 ADT = 41,900
SPEED LIMIT = 30 MPH

BEGIN PROJECT:
MANNHEIM ROAD
64 + 10.00

R 12 E



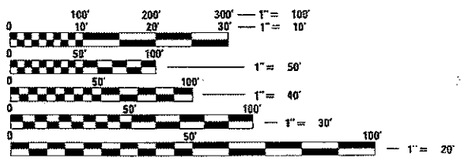
N

END PROJECT:
MANNHEIM ROAD
78 + 50.00

BOX CULVERT STA. 77 + 45
EXISTING STRUCTURE NO. 061-1036
PROPOSED STRUCTURE NO. 016-1351

LOCATION MAP
NOT TO SCALE
PROVISO TOWNSHIP

GROSS LENGTH OF PROJECT = 1440 FT = 0.272 MILE
NET LENGTH OF PROJECT = 1440 FT = 0.272 MILE



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

LOCATION OF SECTION INDICATED THUS: —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED DEC. 12 20 17
Anthony J. Quigley
REGIONAL ENGINEER
Feb 2 20 18
ENGINEER OF DESIGN AND ENVIRONMENT
Feb 2 20 18
DIRECTOR OF PROGRAM DEVELOPMENT

2

HBM ENGINEERING GROUP, LLC.
MOUSSA A. ISSA, S.E.
*081-005738
Mousa A. Issa
DATE: 12/08/2017
SIGNATURE AND SEAL APPLIES TO DRWG NO 51 TO 530 (SHT 45-74)

LICENSED STRUCTURAL ENGINEER
MOUSSA A. ISSA
081-5738
CHICAGO
ILLINOIS
11/30/18

2600 Warrenville Road, Suite 203
Downers Grove, Illinois 60515
630.705.0110 voice, 630.839.2566 fax
www.mps-il.com
MILLENNIA PROFESSIONAL SERVICES

PROJECT MANAGER: FAWAD AQUEEL, P.E. (847) 705-4247
PROJECT ENGINEER: RAGHAD ADEIS-DAHHAN, P.E. (847) 705-4237

CONTRACT NO. 60V22

GANDHI AND ASSOCIATE, INC
PETER A. VANDEMOTTER
* 062-043463
Peter VandeMotter
DATE: 12/8/17
SIGNATURE AND SEAL APPLIES TO DRWG NO 42 TO 44

PETER A. VANDEMOTTER
062-043463
REGISTERED PROFESSIONAL ENGINEER
OF
ILLINOIS

ATLAS ENGINEERING GROUP, Ltd.
YEMI O. OYEWOLE, P.E.
* 062-058164
DATE: 12/8/2017
SIGNATURE AND SEAL APPLIES TO DRWG NO 22 TO 25

YEMI O. OYEWOLE
062-058164
LICENSED PROFESSIONAL ENGINEER
STATE OF ILLINOIS

MILLENNIA PROFESSIONAL SERVICES
THOMAS V. NGO, P.E.
* 062-058379
Thomas V Ngo
DATE: 12/08/17
SIGNATURE AND SEAL APPLIES TO DRWG NO 1 TO 21, 26-41

THOMAS V. NGO
062-058379
REGISTERED PROFESSIONAL ENGINEER
OF
ILLINOIS

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

INDEX OF SHEETS

LIST OF ILLINOIS DOT HIGHWAY STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
001006	DECIMAL OF AN INCH AND OF A FOOT
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
420406 ..	PAVEMENT CONNECTOR (HMA) FOR BRIDGE APPROACH SLAB
424026-02	ENTRANCE / ALLEY PEDESTRIAN CROSSINGS
442201-03	CLASS C AND D PATCHES
515001-03	NAME PLATE FOR BRIDGES
542001-06	CONCRETE END SECTIONS FOR PIPE CULVERTS 15" (375 mm) THRU 84" (2100 mm) DIAMETER
601001-05	PIPE UNDERDRAINS TYPE 2 4"
601101-02	CONCRETE HEADWALL FOR PIPE UNDERDRAIN
602406-08	PRECAST MANHOLE TYPE A 6' (1.83 m) DIAMETER
604001-04	FRAME AND LIDS, TYPE 1
604091-03	FRAME AND GRATE TYPE 24
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
606301-04	PC CONCRETE ISLANDS AND MEDIANS
664001-02	CHAIN LINK FENCE
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS < 45 MPH
701502-08	URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NON TRAVERSABLE MEDIAN
701602-09	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701606-10	URBAN LANE CLOSURE,MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-07	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
780001-05	TYPICAL PAVEMENT MARKINGS
781001-04	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
782006	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS
812001	RACEWAY EMBEDDED IN STRUCTURE
814001-03	HANDHOLES
880001-01	SPAN WIRE MOUNTED SIGNALS AND FLASHING BEACON INSTALLATION

GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES 48 HOUR NOTIFICATION IS REQUIRED.

2. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE UTILITY COMPANIES, AND THE VILLAGE OF STONE PARK.

3. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

4. ALL DAMAGE TO EXISTING PAVEMENT MARKING OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTORS EXPENSE. NO ADDITIONAL COST TO THE DEPARTMENT.

5. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCES, ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL STRIPING SHALL BE AS DIRECTED BY THE ENGINEER.

6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING MATERIALS.

GENERAL NOTES (CONTINUED)

7. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

8. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

9. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

10. DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN ON THE PLANS.

11. FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.

12. THE CONTRACTOR SHALL PLACE PROPOSED PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT 1 TYPICAL PAVEMENT MARKINGS DETAIL (TC-13).

13. THE CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL LOGS, SHRUBS, BUSHES, SAPLINGS, UNDERBRUSH OR DEBRIS ACCORDING TO SECTION 201 OF THE STANDARD SPECIFICATIONS AT LOCATIONS REQUIRING ACCESS TO THE SUBSTRUCTURE. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT THE COST SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

14. ANY ABANDONED UTILITY OR SEWER ENCOUTERED DURING CONSTRUCTION SHALL BE PLUGGED AS DIRECTED BY THE ENGINEER AND ABANDONED IN PLACE. THIS WORK SHALL BE INCIDENTAL TO THE COST OF THE CONTRACT.

15. DURING CONSTRUCTION OPERATIONS, IF ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, THE MATERIAL SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE OF DUST AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOTE BE PAID FOR SEPERATELY BUT SHALL BE INCIDENTAL IN THE COST OF THE CONTRACT.

16. IF CONFLICTS OCCUR AND RELOCATION OF THE NEW FACILITIES IS NOT FEASIBLE, THE CONTRACTOR SHALL WORK WITH THE ENGINEER TO MAKE ARRANGEMENTS WITH THE UTILITY COMPANIES TO HAVE THE AFFECTED UTILITIES PROTECTED OR RELOCATED. NO ADDITIONAL COMPENSATION WILL BE ALLOWED AS A RESULT OF ANY ADDITIONAL COSTS.

17. THIS PROJECT REQUIRES A US ARMY CORPS OF ENGINEERS (USACE) 404 PERMIT THAT WILL BE SECURED BY THE DEPARTMENT. AS A CONDITION OF THIS PERMIT, THE CONTRACTOR WILL NEED TO SUBMIT AN IN-STREAM WORK PLAN TO THE DEPARTMENT FOR APPROVAL. GUIDELINES ON ACCEPTABLE IN-STREAM WORK TECHNIQUES CAN BE FOUND ON THE USACE WEBSITE. THE USACE DEFINES AND DETERMINES IN-STREAM WORK. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT AN IN-STREAM WORK PLAN WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

18 THE ENGINEER SHALL CONTACT PATRICE HARRIS, AREA TRAFFIC FIELD TECHNICIAN, VIA EMAIL AT PATRICE.HARRIS@ILLINOIS.GOV TWO (2) WEEKS PRIOR TO THE PLACING PERMANENT PAVEMENT MARKINGS.

19 CULVERT REMOVAL MAY NOT START UNTIL LIGHTING CABLES ARE RELOCATED TO TEMPORARY POLES. LIGHTING IS TO BE MAINTAINED THROUGHOUT PROJECT. LIGHTING IS POWERED FROM VILLAGE CONTROLLER ON DIVISION STREET EAST OF MANNHEIM ROAD.
IF ACCESS IS REQUIRED CONTACT PUBLIC WORKS.
TEMPORARY POLES AND WIRING MAY BE REMOVED AFTER PROPOSED LIGHTING IS FUNCTIONAL.

2600 Warrenville Road, Suite 203, Downers Grove, IL 60515
630.705.0110 voice, 630.639.2566 fax
www.mps-il.com
MILLENNIA PROFESSIONAL SERVICES

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DRAWN -	JP	REVISED -
CHECKED -	TVN	REVISED -
DATE -	1/18/2018	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US12/45 (MANNHEIM RD) OVER ADDISON CREEK
INDEX OF SHEETS, LIST OF IDOT HIGHWAY STANDARDS, GENERAL NOTES AND COMMITMENTS

SCALE: N/A | SHEET OF SHEETS | STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	2
		CONTRACT NO. 60V22		
	ILLINOIS	FED. AID PROJECT		

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PLOT SCALE = 2.0000 '/ In.
USER NAME = Millennia Professional Services

URBAN

CODE NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
				ROADWAY 80% FED 20% STATE 0004	STRUCTURE 016-1351 80% FED 20% STATE 0010	LIGHTING 100% VILLAGE 0021	TRAFFIC SIGNAL INTERCONNECTION 80% FED 20% STATE 0021	WATER MAIN 100% VILLAGE 0043
20101000	TEMPORARY FENCE	FOOT	77	77				
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	37.9	37.9				
20200100	EARTH EXCAVATION	CU YD	55	55				
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	15.4	15.4				
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	10	10				
20300100	CHANNEL EXCAVATION	CU YD	220	220				
20800150	TRENCH BACKFILL	CU YD	279	210				69
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	118	118				
25000210	SEEDING, CLASS 2A	ACRE	0.02	0.02				
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	2	2				
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	2	2				
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	2	2				
25100635	HEAVY DUTY EROSION CONTROL BLANKET	SQ YD	118	118				
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	2.4	2.4				
28000400	PERIMETER EROSION BARRIER	FOOT	105	105				
28000510	INLET FILTERS	EACH	6	6				
28001200	TEMPORARY HEAVY DUTY EROSION CONTROL BLANKET	SQ YD	118	118				
28100107	STONE RIPRAP, CLASS A4	SQ YD	412	83	329			
28200200	FILTER FABRIC	SQ YD	485	156	329			
30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	2455	2455				
35501327	HOT-MIX ASPHALT BASE COURSE, 10 3/4"	SQ YD	1384	1384				
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	1690	1690				
40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	59.5	59.5				
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	19					19
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	50	50				
40603565	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70	TON	296	296				
42000070	PAVEMENT CONNECTOR (HMA) FOR BRIDGE APPROACH SLAB	SQ YD	177	177				
42001300	PROTECTIVE COAT	SQ YD	1331	1331				
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	107	107				
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	3134	3134				
44000100	PAVEMENT REMOVAL	SQ YD	1888	1888				
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	439	439				

* SPECIALTY ITEM

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USER NAME = Millennia Professional Services

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630.785.8110 voice, 630.839.2566 fax
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DATE	12/12/2017	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US12/45 (MANNHEIM RD) OVER ADDISON CREEK
SUMMARY OF QUANTITIES

SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	3
		CONTRACT NO. 60V22		
	ILLINOIS	FED. AID PROJECT		

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URBAN

CODE NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
				ROADWAY 80% FED 20% STATE 0004	STRUCTURE 016-1351 80% FED 20% STATE 0010	LIGHTING 100% VILLAGE 0021	TRAFFIC SIGNAL INTERCONNECTION 80% FED 20% STATE 0021	WATER MAIN 100% VILLAGE 0043
44000160	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"	SQ YD	1306	1306				
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	199	199				
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1970	1970				
44000600	SIDEWALK REMOVAL	SQ FT	3290	3290				
44002212	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 3"	SQ YD	110					110
44003100	MEDIAN REMOVAL	SQ FT	8990	8990				
44201789	CLASS D PATCHES, TYPE II, 12 INCH	SQ YD	110					110
44213200	SAW CUTS	FOOT	2065	2065				
50100100	REMOVAL OF EXISTING STRUCTURES	EACH	1		1			
50200100	STRUCTURE EXCAVATION	CU YD	478		478			
50300100	FLOOR DRAINS	EACH	6		6			
50300225	CONCRETE STRUCTURES	CU YD	187.3		187.3			
50300255	CONCRETE SUPERSTRUCTURE	CU YD	382.5		382.5			
50300260	BRIDGE DECK GROOVING	SQ YD	912		912			
50300300	PROTECTIVE COAT	SQ YD	1173		1173			
50301350	CONCRETE SUPERSTRUCTURE (APPROACH SLAB)	CU YD	259		259			
50500505	STUD SHEAR CONNECTORS	EACH	608		608			
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	234,580		234,580			
50800515	BAR SPLICERS	EACH	952		952			
50901720	BICYCLE RAILING	FOOT	7		7			
50900105	ALUMINUM RAILING, TYPE L	FOOT	103		103			
51201800	FURNISHING STEEL PILES HP14X73	FOOT	624		624			
51202305	DRIVING PILES	FOOT	624		624			
51203800	TEST PILE STEEL HP14X73	EACH	2		2			
51204650	PILE SHOES	EACH	28		28			
51500100	NAME PLATES	EACH	1		1			
52200010	TEMPORARY SHEET PILING	SQ FT	650		650			
52200015	PERMANENT SHEET PILING	SQ FT	4020		4,020			
54215436	CAST-IN-PLACE REINFORCED CONCRETE END SECTIONS 36"	EACH	1	1				
550A0140	STORM SEWERS, CLASS A, TYPE 1 30"	FOOT	104	104				

* SPECIALTY ITEM

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US12/45 (MANNHEIM RD) OVER ADDISON CREEK
SUMMARY OF QUANTITIES

SCALE: · SHEET OF SHEETS STA. · TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	4
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

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URBAN

CODE NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
				ROADWAY 80% FED 20% STATE	STRUCTURE 016-1351 80% FED 20% STATE	LIGHTING 100% VILLAGE	TRAFFIC SIGNAL INTERCONNECTION 80% FED 20% STATE	WATER MAIN 100% VILLAGE
				0004	0010	0021	0021	0043
55000160	STORM SEWERS, CLASS A, TYPE 1 36"	FOOT	41	41	"	"	"	"
55000360	STORM SEWERS, CLASS A, TYPE 2 15"	FOOT	10	10				
55100700	STORM SEWER REMOVAL 15"	FOOT	49	49				
55101400	STORM SEWER REMOVAL 30"	FOOT	50	50				
56104900	WATER VALVES 6"	EACH	2					2
56400300	FIRE HYDRANTS TO BE ADJUSTED	EACH	1					1
56400510	FIRE HYDRANTS TO BE REMOVED AND REPLACED	EACH	1					1
56500200	DOMESTIC WATER SERVICE BOXES TO BE MOVED	EACH	1					1
56500600	DOMESTIC WATER SERVICE BOXES TO BE ADJUSTED	EACH	4					4
59100100	GEOCOMPOSITE WALL DRAIN	SQ YD	76		76			
60108100	PIPE UNDERDRAINS 4" (SPECIAL)	FOOT	76	76				
60223800	MANHOLES, TYPE A, 6'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	3	3				
60255800	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	5	5				
60261540	INLETS TO BE ADJUSTED WITH NEW TYPE 24 FRAME AND GRATE	EACH	8	8				
60266600	VALVE BOXES TO BE ADJUSTED	EACH	1					1
60500105	FILLING MANHOLES	EACH	1	1				
60604400	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18	FOOT	667	667				
60605300	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (MODIFIED)	FOOT	388	388				
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SQ FT	3036	3036				
60619200	CONCRETE MEDIAN, TYPE SB-6.06	SQ FT	3374	3374				
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	55	55				
* 66900450	SPECIAL WASTE PLANS AND REPORTS	LSUM	1	1				
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1				
* 66900665	TCL SOIL ANALYSIS	EACH	1	1				
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	12	12				
67100100	MOBILIZATION	LSUM	1	1				
70300510	PAVEMENT MARKING TAPE, TYPE III - LETTERS AND SYMBOLS	SQ FT	500	500				
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	22645	22645				
70300540	PAVEMENT MARKING TAPE, TYPE III 6"	FOOT	2406	2406				
70300570	PAVEMENT MARKING TAPE, TYPE III 24"	FOOT	146	146				

* SPECIALTY ITEM

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CHECKED - TVN		REVISED -
DATE - 12/14/2017		REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US1245 (MANNHEIM RD) OVER ADDISON CREEK
SUMMARY OF QUANTITIES

SCALE: · | SHEET OF SHEETS | STA. · | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	5
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

#FILE#

REV

URBAN

	CODE NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
					ROADWAY 80% FED 20% STATE 0004	STRUCTURE 016-1351 80% FED 20% STATE 0010	LIGHTING 100% VILLAGE 0021	TRAFFIC SIGNAL INTERCONNECTION 80% FED 20% STATE 0021	WATER MAIN 100% VILLAGE 0043
	70400100	TEMPORARY CONCRETE BARRIER	FOOT	942	942	"	"	"	"
	70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	1057	1057				
	70600255	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	3	3				
	70600322	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	3	3				
	72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	2	2				
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	463	463				
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	4051	4051				
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	3049	3049				
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	171	171				
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	310	310				
*	78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	235	235				
*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	247	247				
*	78200011	BARRIER WALL REFLECTORS, TYPE C	EACH	75	75				
	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	247	247				
*	81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	323				323	
*	81028350	UNDERGROUND CONDUIT, PVC, 2" DIA.	FOOT	121			121		
*	81200230	CONDUIT EMBEDDED IN STRUCTURE, 2" DIA., PVC	FOOT	125			85	40	
*	81301290	JUNCTION BOX, STAINLESS STEEL, EMBEDDED IN STRUCTURE, 12" X 12" X 6"	EACH	6			4	2	
*	81400100	HANDHOLE	EACH	2				2	
*	81603035	UNIT DUCT, 600V, 2-1C NO.6, 1/C NO.6 GROUND, (XLP-TYPE USE), 1" DIA. POLYETHYLENE	FOOT	550			550		
*	81800230	AERIAL CABLE, 2-1/C NO. 6 WITH MESSENGER WIRE	FOOT	500			500		
*	82102400	LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, 400 WATT	EACH	2			2		
*	83050760	LIGHT POLE, ALUMINUM, 47.5 FT. M.H., 8 FT. MAST ARM	EACH	1			1		
*	83600200	LIGHT POLE FOUNDATION, 24" DIAMETER	FOOT	7			7		
*	83800205	BREAKAWAY DEVICE, TRANSFORMER BASE, 15 INCH BOLT CIRCLE	EACH	1			1		
	84200500	REMOVAL OF LIGHTING UNIT, SALVAGE	EACH	1			1		
	84200804	REMOVAL OF POLE FOUNDATION	EACH	1			1		
*	85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	3				3	
*	87300925	ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	2419				2419	
*	89000100	TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1				1	

* SPECIALTY ITEM

FILE NAME = #FILE#
PLOT SCALE = #SCALE# / in.
USER NAME = #USERNAME#

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DATE	12/14/2017	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US12/45 (MANNHEIM RD) OVER ADDISON CREEK
SUMMARY OF QUANTITIES

SCALE: · | SHEET OF SHEETS | STA. · | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	6
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

#FILE#

REV

URBAN

	CODE NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
					ROADWAY 80% FED 20% STATE 0004	STRUCTURE 016-1351 80% FED 20% STATE 0010	LIGHTING 100% VILLAGE 0021	TRAFFIC SIGNAL INTERCONNECTION 80% FED 20% STATE 0021	WATER MAIN 100% VILLAGE 0043
*	89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	2096	·	·	·	2096	·
*	89502380	REMOVE EXISTING HANDHOLE	EACH	2				2	
*	X0324599	ROD AND CLEAN EXISTING CONDUIT	FOOT	500				500	
*	X0326148	TEMPORARY WOOD POLE, 60 FT., CLASS 4, 15 FT. MAST ARM	EACH	1			1		
*	X0327004	TEMPORARY WOOD POLE, 60 FT., CLASS 4	EACH	3			3		
	X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	4239	4239				
	X2070304	POROUS GRANULAR EMBANKMENT, SPECIAL	CU YD	470	72	398			
	X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	5	5				
	X5509900	ABANDON AND FILL EXISTING STORM SEWER	FOOT	45	45				
□	X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	94	94				
□	X5537900	STORM SEWERS TO BE CLEANED 15"	FOOT	135	135				
□	X5538000	STORM SEWERS TO BE CLEANED 18"	FOOT	225	225				
□	X5538400	STORM SEWERS TO BE CLEANED 30"	FOOT	196	196				
	X5860110	GRANULAR BACKFILL FOR STRUCTURES	CU YD	164		164			
	X6640304	CHAIN LINK FENCE TO BE REMOVED AND RE-ERECTED	FOOT	65	65				
	X6640312	CHAIN LINK GATES TO BE REMOVED AND RE-ERECTED	EACH	1	1				
	X7010216	TRAFFIC CONTROL AND PROTECTION (SPECIAL)	L SUM	1	1				
	X7015005	CHANGEABLE MESSAGE SIGN	CAL DAY	368	368				
•	X8100105	CONDUIT SPLICE	EACH	2				2	
•	X8710024	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM24F	FOOT	2419				2419	
	X1700062	BRICK PAVER REMOVAL AND REPLACEMENT	SQ FT	924	924				
	X1200018	DUCTILE IRON WATER MAIN 6" RESTRAINED JOINT TYPE	FOOT	164					164
	Z0004538	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 10"	SQ YD	58	58				
	Z0013798	CONSTRUCTION LAYOUT	LSUM	1	1				
□	Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	6	6				
	Z0018911	DRILL AND GROUT #6 TIE BARS	EACH	949	949				
	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	132	132				
*	Z0033020	LUMINAIRE SAFETY CABLE ASSEMBLY	EACH	1			1		
*	Z0033024	MAINTAIN EXISTING LIGHTING SYSTEM	L SUM	1			1		
*	Z0033044	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	3				3	
	Z0046304	PIPE UNDERDRAINS FOR STRUCTURES 4"	FOOT	232		232			

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IF NON-PARTICIPATING

* SPECIALTY ITEM

FILE NAME = $FILE$
PLOT SCALE = $PLOTSCALE$ / in.
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US12/45 (MANNHEIM RD) OVER ADDISON CREEK
SUMMARY OF QUANTITIES

SCALE: · | SHEET OF SHEETS | STA. · | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	7
		CONTRACT NO. 60V22		
		ILLINOIS FED. AID PROJECT		

$FILE$

REV

URBAN

CODE NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
				ROADWAY 80% FED 20% STATE 0004	STRUCTURE 016-1351 80% FED 20% STATE 0010	LIGHTING 100% VILLAGE 0021	TRAFFIC SIGNAL INTERCONNECTION 80% FED 20% STATE 0021	WATER MAIN 100% VILLAGE 0043
Z0056616	STORM SEWER (WATER MAIN REQUIREMENTS) 24 INCH	FOOT	20	*	*	*	*	20
Z0062456	TEMPORARY PAVEMENT	SQ YD	4619	4619				
Z0073510	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1				1	
Ø Z0076600	TRAINEES	HOUR	1000	1000				
Z0077700	WOOD FENCE TO BE REMOVED AND RE-ERECTED	FOOT	15	15				
Ø Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOUR	1000	1000				
X1200187	DUCTILE IRON WATER MAIN FITTINGS RESTRAINED JOINT 6" 90.00 DEGREE BEND	EACH	4					4
* X2700004	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 7"	FOOT	94	94				
X2700003	GROOVING FOR RECESSED PAVEMENT MARKING 8"	FOOT	94	94				

* SPECIALTY ITEM Ø 0042

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US12/45 (MANNHEIM RD) OVER ADDISON CREEK
SUMMARY OF QUANTITIES

SCALE: · | SHEET OF SHEETS | STA. · | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	8
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

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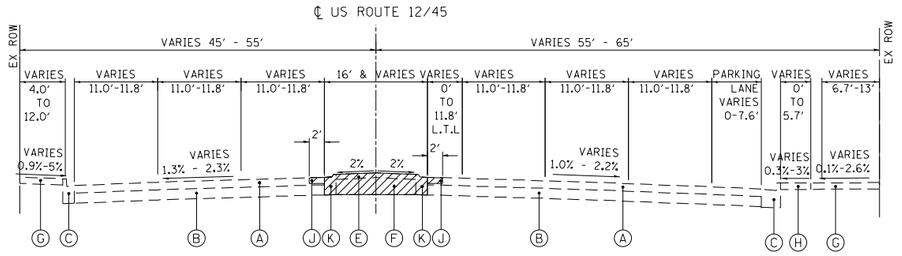
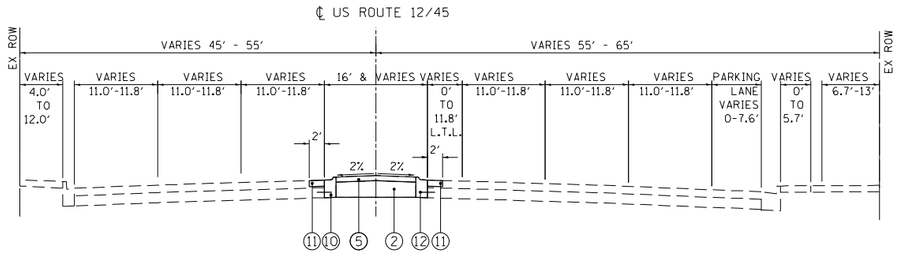


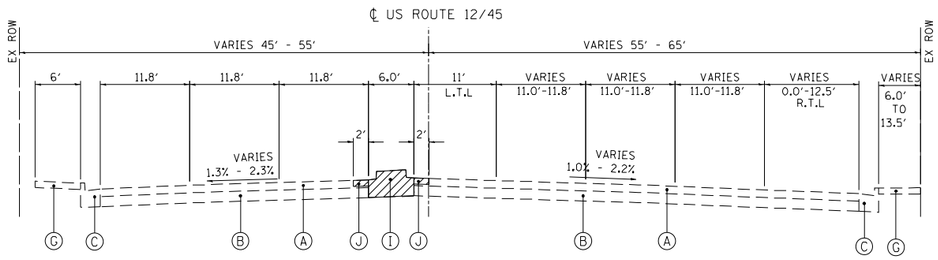
EXISTING TYPICAL SECTION

STA 64+10 TO STA 67+43.20
(LOOKING NORTH)



PROPOSED TYPICAL SECTION

STA 64+10 TO STA 67+43.20
(LOOKING NORTH)



EXISTING TYPICAL SECTION

STA 67+43 TO STA 70+10
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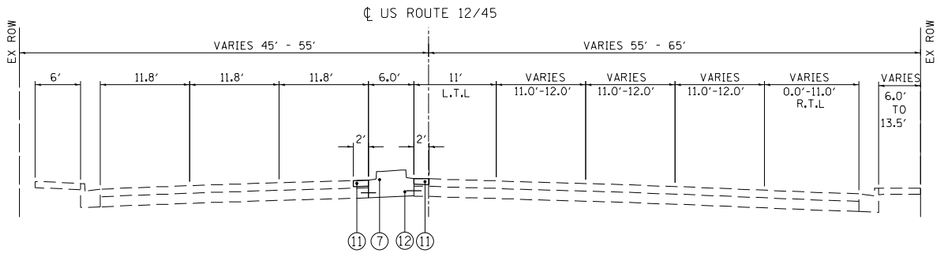


PROPOSED TYPICAL SECTION

STA 67+43 TO STA 70+10
(LOOKING NORTH)

EXISTING LEGEND:

- (A) BITUMINOUS OVERLAY, VARIES 3 1/2''-8 1/2''
- (B) PCC PAVEMENT, VARIES 6''-10''
- (C) COMBINATION CONCRETE CURB AND GUTTER, B-6.24
- (D) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12
- (E) BITUMINOUS MEDIAN SURFACE, 4''
- (F) AGGREGATE FILL
- (G) PCC SIDEWALK
- (H) BRICK PAVERS
- (I) CONCRETE MEDIAN
- (J) HMA SURFACE REMOVAL 2''
- (K) COMBINATION CONCRETE CURB AND GUTTER, B-6.18
- (L) HMA SURFACE REMOVAL 2 3/4''

REMOVAL

HMA MIXTURES REQUIREMENT

MIXTURE USES	AIR VOIDS @Ndes	QMP TYPE
RECONSTRUCTION		
POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, (IL 9.5mm), 2"	4% @ 70 GYR.	QC/QA
HMA BASE COURSE, 10 3/4" (HMA BINDER IL-19)	4% @ 90 GYR.	QC/QA
RESURFACING		
POLYMERIZED HMA SURFACE COURSE, MIX "E", N70 (IL 9.5mm), 2"	4% @ 70 GYR.	QC/QA
LEVELING BINDER (MACHINE METHOD), N70 (IL 9.5mm), 3/4"	4% @ 70 GYR.	QC/QA
HMA DRIVEWAY 10"		
HMA SURFACE COURSE, MIX D, N50 (IL 9.5mm), 2"	4% @ 50 GYR.	QC/QA
HMA BASE COURSE (HMA BINDER IL-19mm); CE-8"	4% @ 50 GYR.	QC/QA
** **TEMPORARY PAVEMENT**		
TEMP PAVEMENT (HMA BINDER IL-19 mm); 9"	4% @ 50 GYR.	QC/QA
HMA SURFACE COURSE, MIX D, N50 (IL 9.5mm); 2"	4% @ 50 GYR.	QC/QA
RESURFACING ALONG IMPROVED MEDIAN		
HMA SURFACE COURSE, MIX D, N70 (IL-9.5mm), 2"	4% @ 70 GYR.	QC/QA
PATCHING		
CLASS D PATCH (HMA BINDER IL-19mm)	4% @ 70 GYR.	QC/QA
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19mm)	4% @ 70 GYR.	QC/QA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA)		

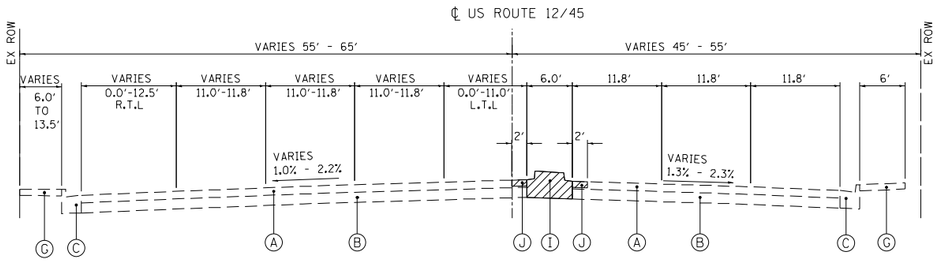
** THE CONTRACTOR SHALL HAVE THE OPTION OF CONSTRUCTING A TEMPORARY HMA PAVEMENT OR TEMPORARY PCC PAVEMENT. IF A TEMPORARY PCC PAVEMENT IS CONSTRUCTED IT SHALL HAVE A THICKNESS OF 11". PCC TEMPORARY PAVEMENT SHALL CONSIST OF CLASS PV CONCRETE MEETING THE REQUIREMENTS OF ART. 1020 OF THE STANDARD SPECIFICATIONS. TEMPORARY PCC PAVEMENT DOES NOT REQUIRE DOWEL BARS.

MIXTURE NOTES:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN

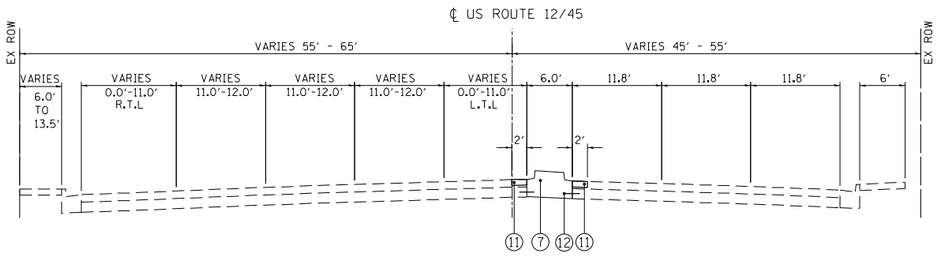
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISIONS.

3. QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.



EXISTING TYPICAL SECTION

STA 71+67.5 TO STA 74+62.5
(LOOKING NORTH)



PROPOSED TYPICAL SECTION

STA 71+67.5 TO STA 74+62.5
(LOOKING NORTH)

PROPOSED LEGEND:

1. POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, 2"
2. AGGREGATE SUBGRADE IMPROVEMENT 12"
3. COMBINATION CONCRETE CURB AND GUTTER, B-6.24 (MODIFIED)
4. HMA BASE COURSE, 10 3/4"
5. CONCRETE MEDIAN SURFACE, 4 INCH
6. PCC SIDEWALK 5 INCH
7. CONCRETE MEDIAN, TYPE SB-6.06
8. LEVELING BINDER (MACHINE METHOD), N70, 3/4"
9. BRICK PAVER REMOVAL AND REPLACEMENT (SEE SPECIAL PROVISIONS)
10. COMBINATION CONCRETE CURB AND GUTTER, B-6.18
11. HMA SURFACE COURSE, MIX "D", N70, 2"
12. DRILL AND GROUT #6 TIE BARS AT 24" C-C

TYP-01

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PLOT SCALE = 20.0000 '/ in.
USER NAME = Millennia Professional Services

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US12/45 (MANNHEIM RD) OVER ADDISON CREEK
MAINLINE TYPICAL SECTIONS

SCALE: NTS | SHEET OF SHEETS | STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	9
		CONTRACT NO. 60V22		
	ILLINOIS	FED. AID PROJECT		

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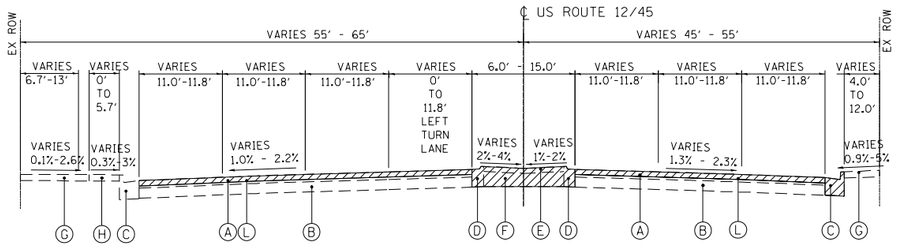
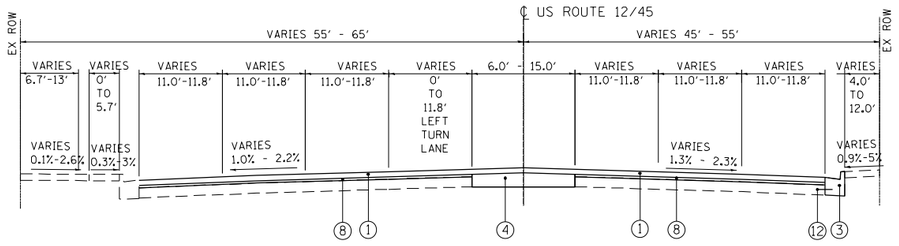


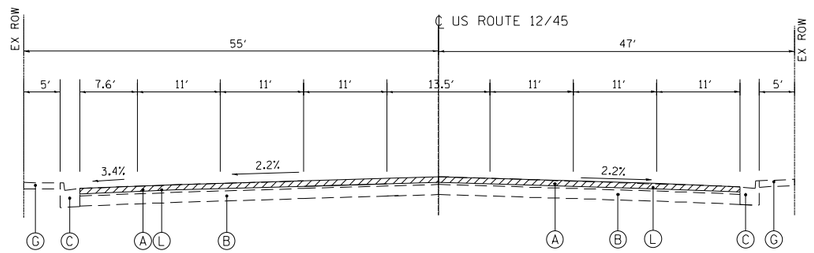
EXISTING TYPICAL SECTION

STA 74+62.50 TO STA 75+71.00
(LOOKING NORTH)



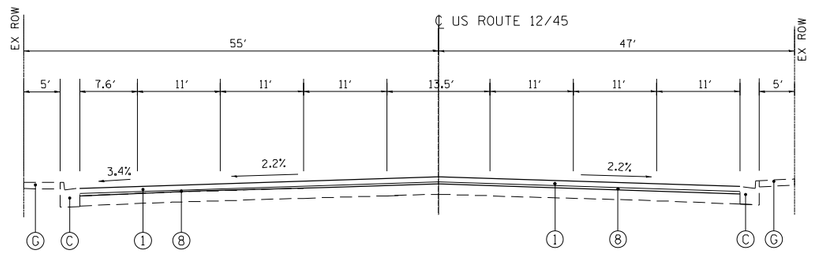
PROPOSED TYPICAL SECTION

STA 74+62.50 TO STA 75+71.00
(LOOKING NORTH)



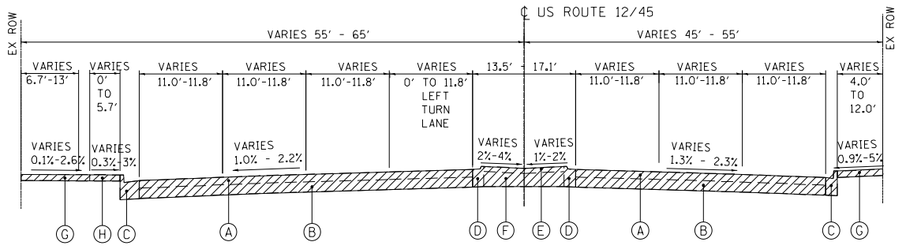
EXISTING TYPICAL SECTION

STA 78+18.00 TO STA 78+50.00
(LOOKING NORTH)



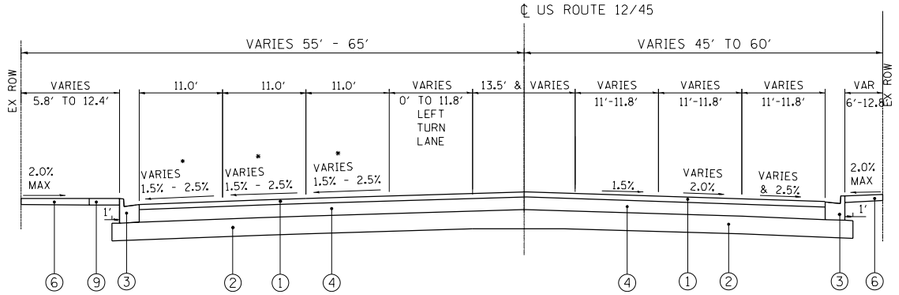
PROPOSED TYPICAL SECTION

STA 78+18.00 TO STA 78+50.00
(LOOKING NORTH)



EXISTING TYPICAL SECTION

STA 75+71.00 TO STA 76+97.94
STA 77+92.45 TO STA 78+18.00
(LOOKING NORTH)



PROPOSED TYPICAL SECTION

** STA 75+71 TO STA 76+87.94
STA 78+02.45 TO STA 78+18.00
(LOOKING NORTH)

* CROSS SLOPE VARIES DUE TO WARPING OF GUTTER AT OUTSIDE EDGE OF PAVEMENT TO PROVIDE A MINIMUM 0.3% LONGITUDINAL SLOPE AT THE OUTER EDGE OF PAVEMENT

** PAVEMENT CONNECTOR (HMA)
STA 76+87.9 TO STA 76+97.9
STA 77+92.4 TO STA 78+02.4

EXISTING LEGEND:

- (A) BITUMINOUS OVERLAY, VARIES 3 1/2″-8 1/2″
- (B) PCC PAVEMENT, VARIES 6″-10″
- (C) COMBINATION CONCRETE CURB AND GUTTER, B-6.24
- (D) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12
- (E) BITUMINOUS MEDIAN SURFACE, 4″
- (F) AGGREGATE FILL
- (G) PCC SIDEWALK
- (H) BRICK PAVERS
- (I) CONCRETE MEDIAN
- (J) HMA SURFACE REMOVAL 2″
- (K) COMBINATION CONCRETE CURB AND GUTTER, B-6.18
- (L) HMA SURFACE REMOVAL 2 3/4″

REMOVAL

PROPOSED LEGEND:

1. POLYMERIZED HMA SURFACE COURSE, MIX ″E″, N70, 2″
2. AGGREGATE SUBGRADE IMPROVEMENT 12″
3. COMBINATION CONCRETE CURB AND GUTTER, B-6.24 (MODIFIED)
4. HMA BASE COURSE, 10 3/4″
5. CONCRETE MEDIAN SURFACE, 4 INCH
6. PCC SIDEWALK 5 INCH
7. CONCRETE MEDIAN, TYPE SB-6.06
8. LEVELING BINDER (MACHINE METHOD), N70, 3/4″
9. BRICK PAVER REMOVAL AND REPLACEMENT (SEE SPECIAL PROVISIONS)
10. COMBINATION CONCRETE CURB AND GUTTER, B-6.18
11. HMA SURFACE COURSE, MIX ″D″, N70, 2″
12. DRILL AND GROUT #6 TIE BARS AT 24″ C-C

TYP-02

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DATE - $DATE$	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US12/45 (MANNHEIM RD) OVER ADDISON CREEK
MAINLINE TYPICAL SECTIONS

SCALE: NTS | SHEET OF SHEETS | STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	10
		CONTRACT NO. 60V22		
	ILLINOIS	FED. AID PROJECT		

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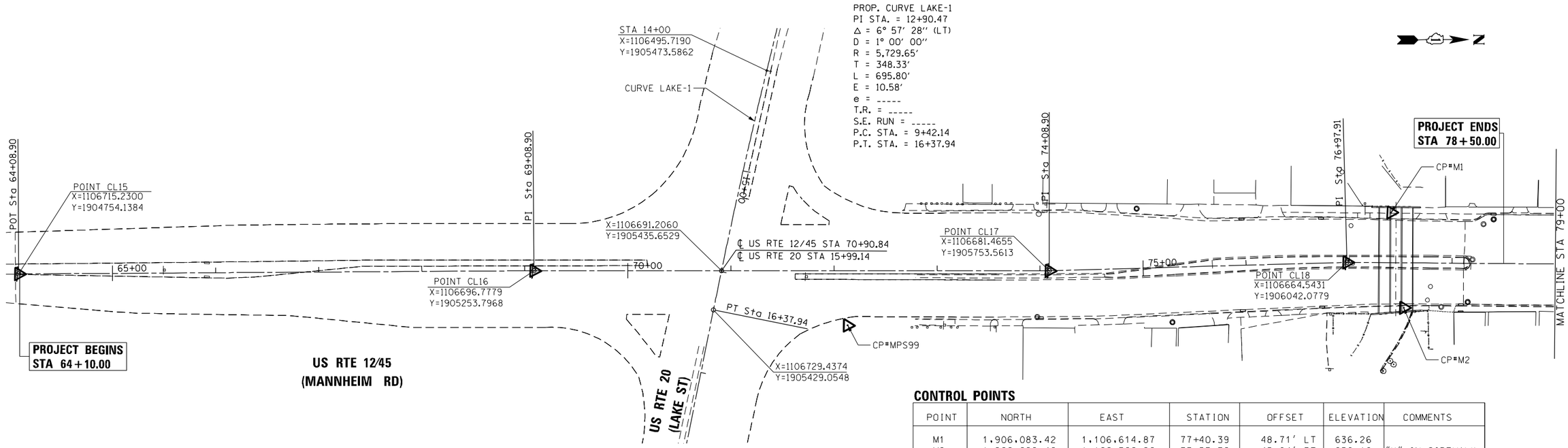
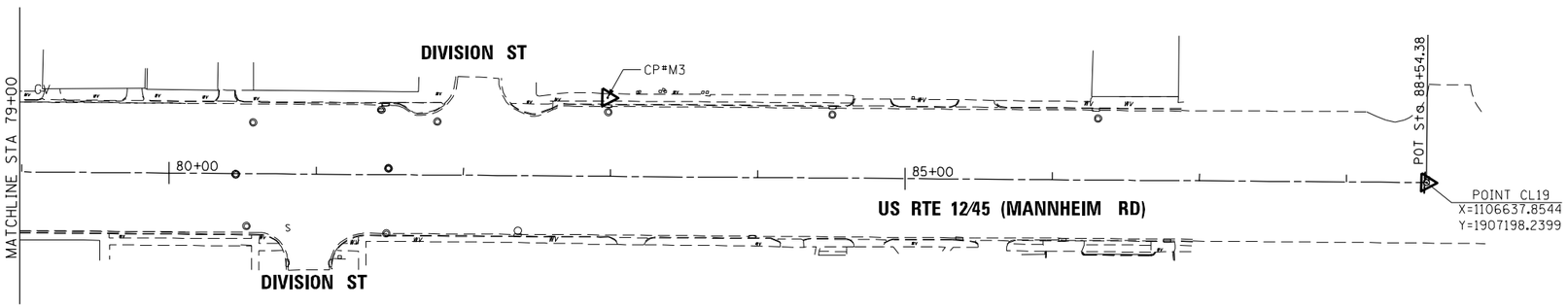


CONTROL POINTS

POINT	NORTH	EAST	STATION	OFFSET	ELEVATION	COMMENTS
M1	1,906,083.42	1,106,614.87	77+40.39	48.71′ LT	636.26	
M2	1,906,098.46	1,106,706.90	77+53.30	43.64′ RT	636.16	"X" IN SIDEWALK
M3	1,906,640.37	1,106,597.22	82+97.59	53.50′ LT	636.06	"X" IN SIDEWALK
MPS99	1,905,559.65	1,106,740.41	72+13.27	52.98′ RT	638.61	"X" IN SIDEWALK



US 12/45(MAINNHEIM RD) ALIGNMENT

==========

POINT CL15 N 1904754.1384 E 1106715.2300 STA 64+08.90

COURSE FROM CL15 TO CL16 N2°06′53.79″W DIST 500′

POINT CL16 N 1905253.7968 E 1106696.7779 STA 69+08.90

COURSE FROM CL16 TO CL17 N1°45′17.79″W DIST 500′

POINT CL17 N 1,905753.5613 E 1106.681.4655 STA 74+08.90

COURSE FROM CL17 TO CL18 N3°21′24.26″W DIST 289.01

POINT CL18 N 1906042.0779 E 1106664.5431 STA 76+97.91

COURSE FROM CL18 TO CL19 N1°19′20.54″W DIST 1,156.47

POINT CL19 N 1907198.2399 E 1106637.8544 STA 88+54.38

==========

ENDING ALIGNMENT

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PLOT SCALE = 100.0000 ' / in.
USER NAME = Millennia Professional Services

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US12/45 (MANNHEIM RD) OVER ADDISON CREEK
ALIGNMENT, TIES AND BENCHMARKS

SCALE: · SHEET OF SHEETS STA. · TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	11
		CONTRACT NO. 60V22		
	ILLINOIS	FED. AID PROJECT		

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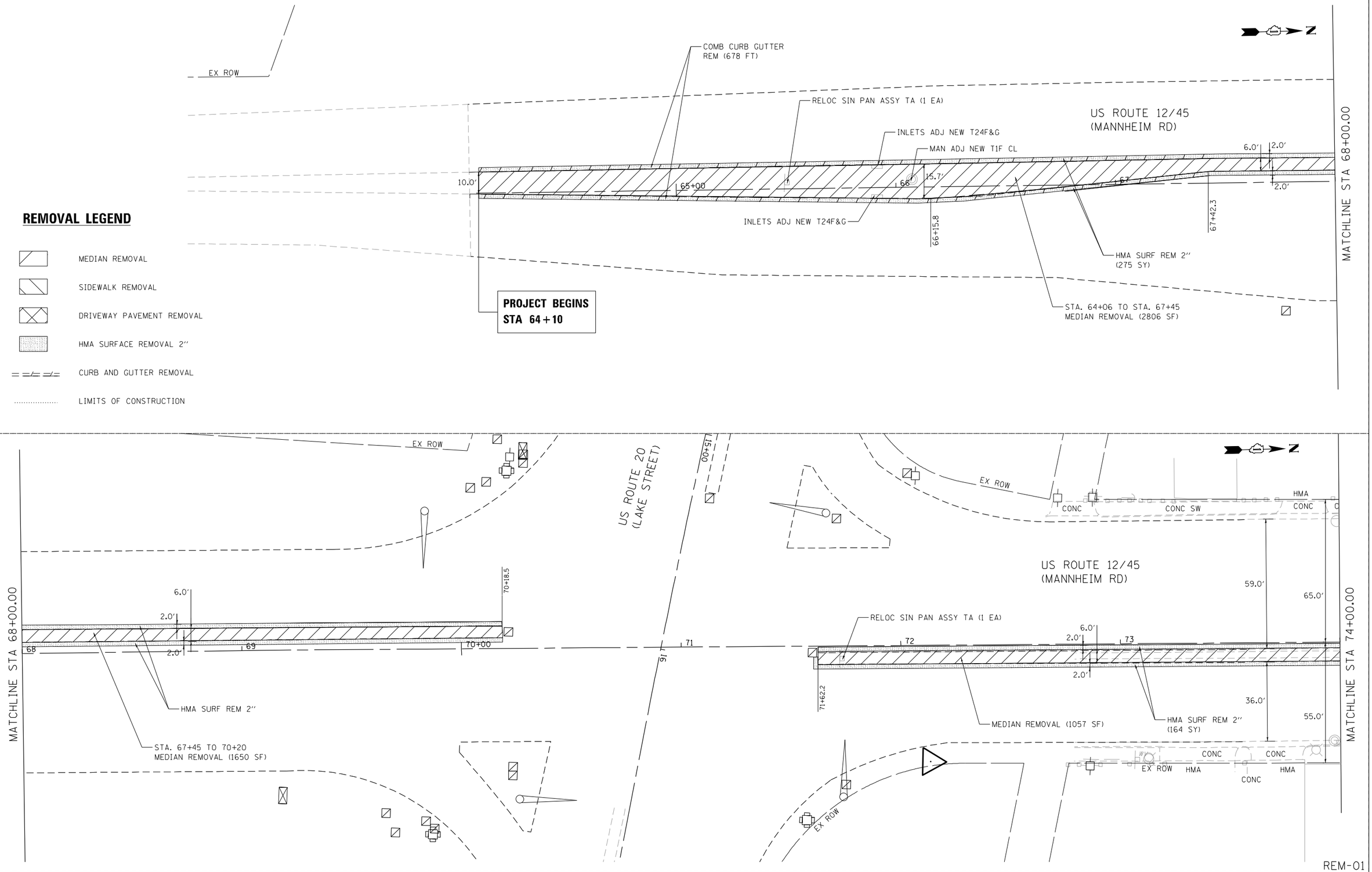

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USER NAME = Millennia Professional Services

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CHECKED - TVN	REVISED -	
DATE - 12/13/2017	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US12/45 (MANNHEIM RD) OVER ADDISON CREEK
EXISTING PLAN AND REMOVALS

SCALE: 1"=20' | SHEET OF SHEETS | STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	12
		CONTRACT NO. 60V22		
	ILLINOIS	FED. AID PROJECT		

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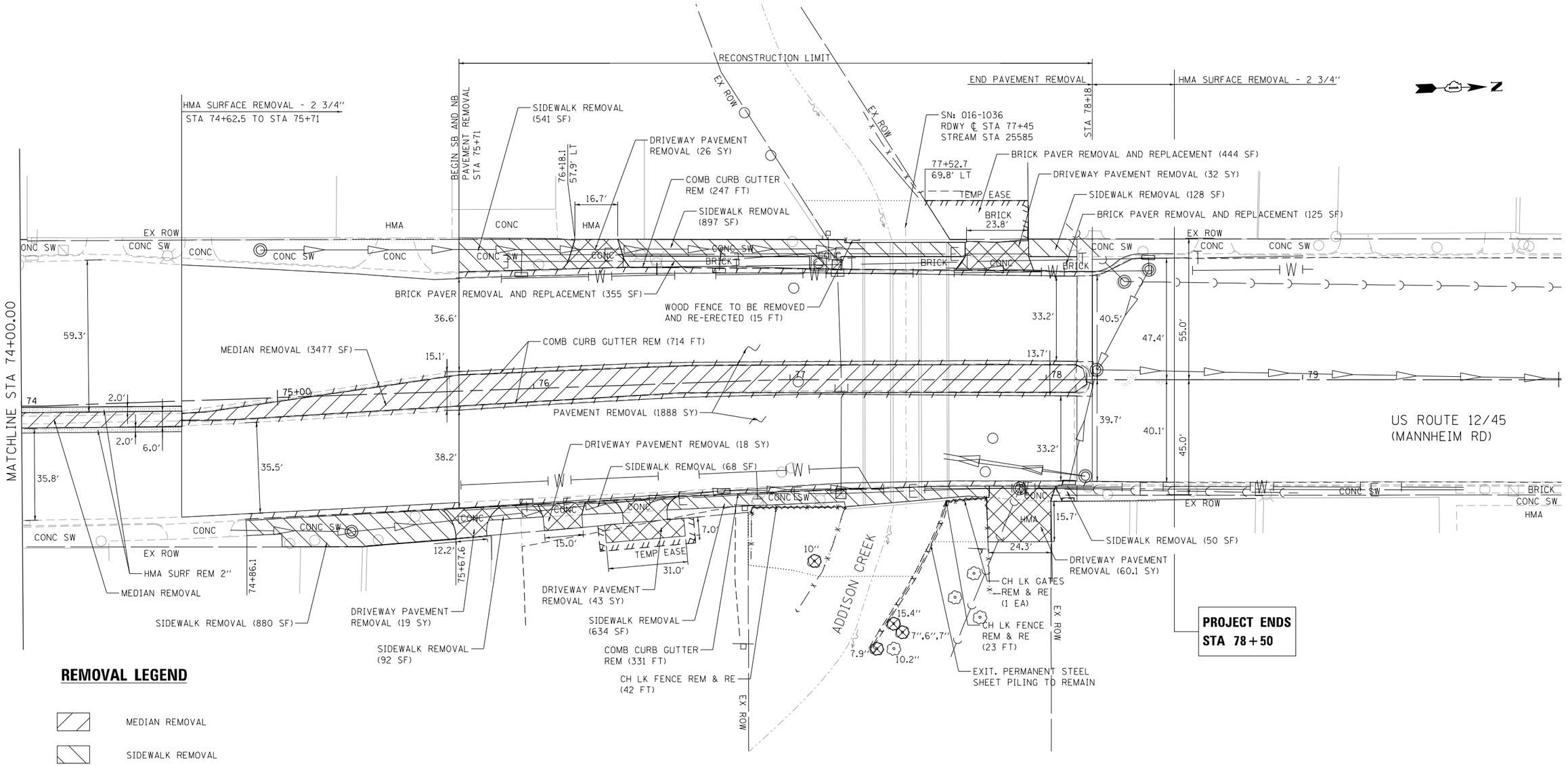

REMOVAL LEGEND

- MEDIAN REMOVAL
- SIDEWALK REMOVAL
- DRIVEWAY PAVEMENT REMOVAL
- HMA SURFACE REMOVAL 2"
- CURB AND GUTTER REMOVAL
- LIMITS OF CONSTRUCTION
- TREE REMOVAL

REM-02

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US12/45 (MANNHEIM RD) OVER ADDISON CREEK
EXISTING PLAN AND REMOVALS

SCALE: 1"=20' | SHEET OF SHEETS | STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	13
		CONTRACT NO. 60V22		
	ILLINOIS	FED. AID PROJECT		

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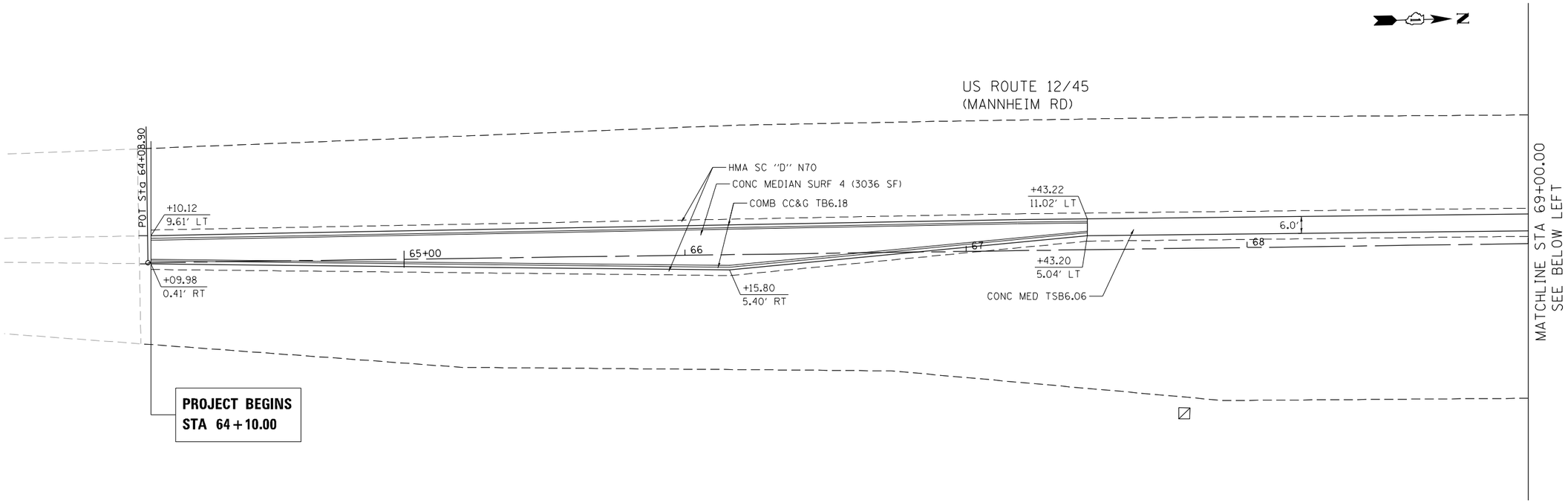

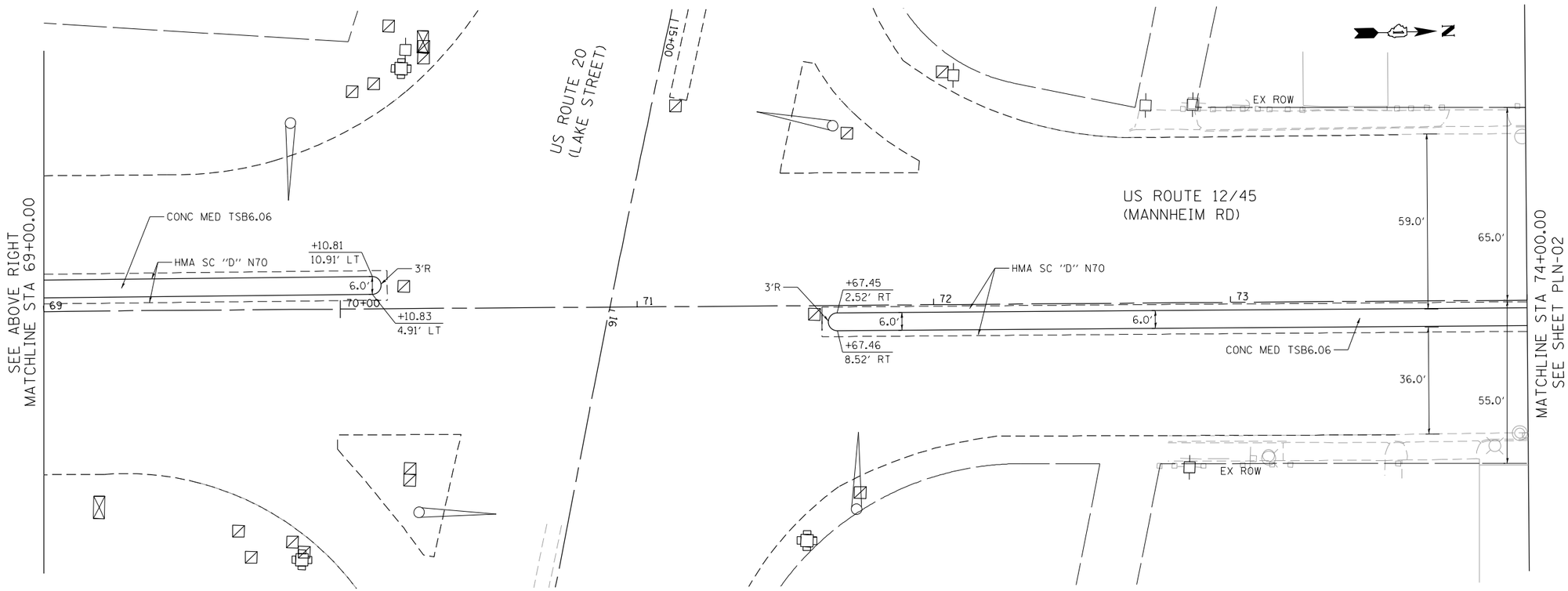

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US12/45 (MANNHEIM RD) OVER ADDISON CREEK
PROPOSED PLAN

SCALE: 1"=20' | SHEET OF SHEETS | STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	14
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

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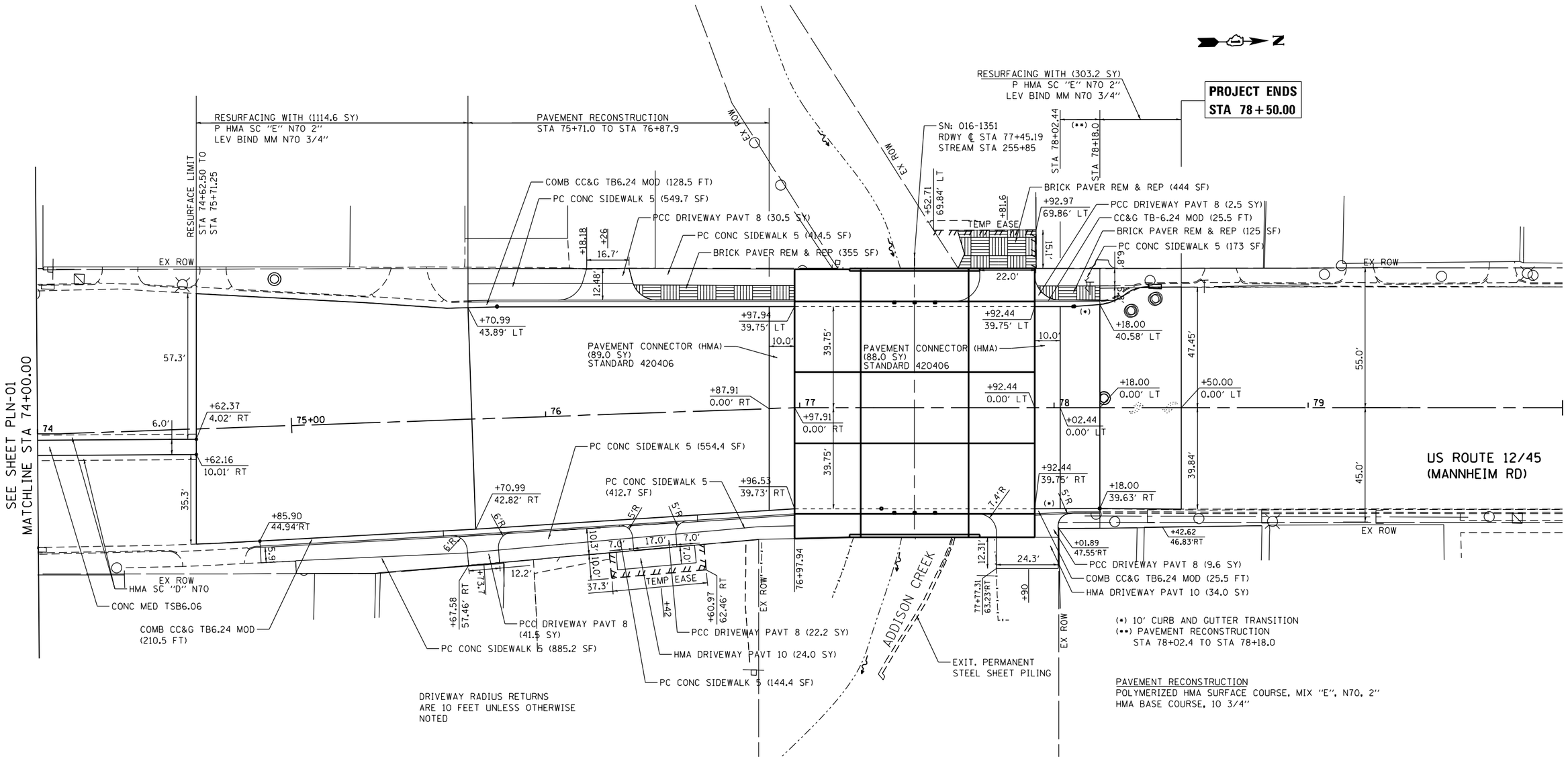

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CHECKED - TVN	REVISED -	
DATE - 1/15/2018	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US12/45 (MANNHEIM RD) OVER ADDISON CREEK
PROPOSED PLAN

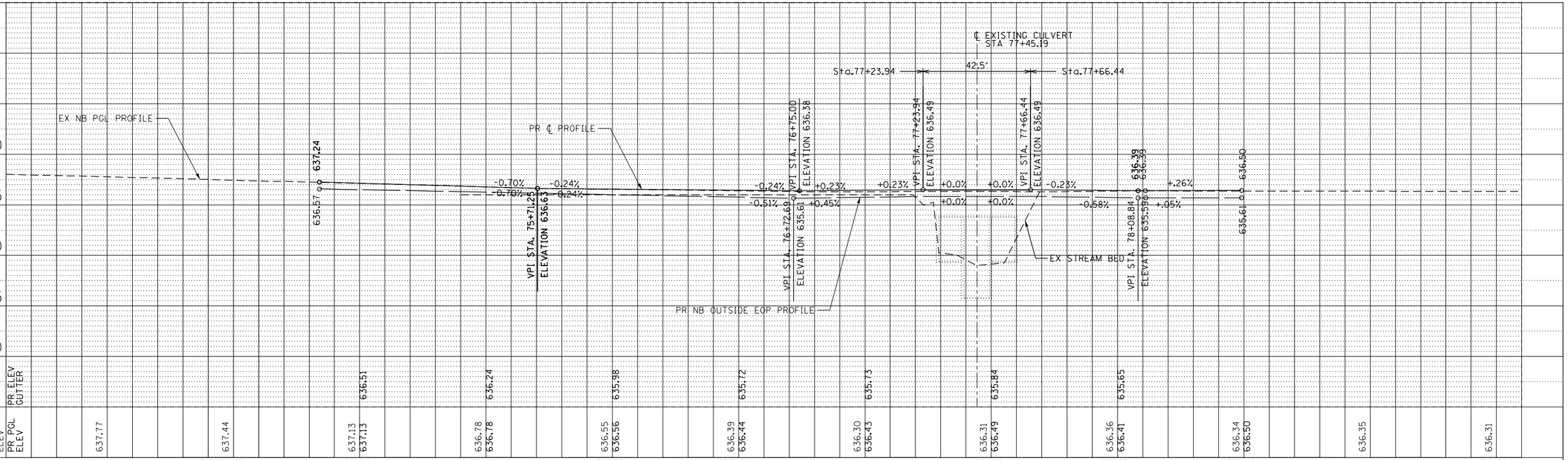
SCALE: 1"=20' | SHEET OF SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	15
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

$FILE$

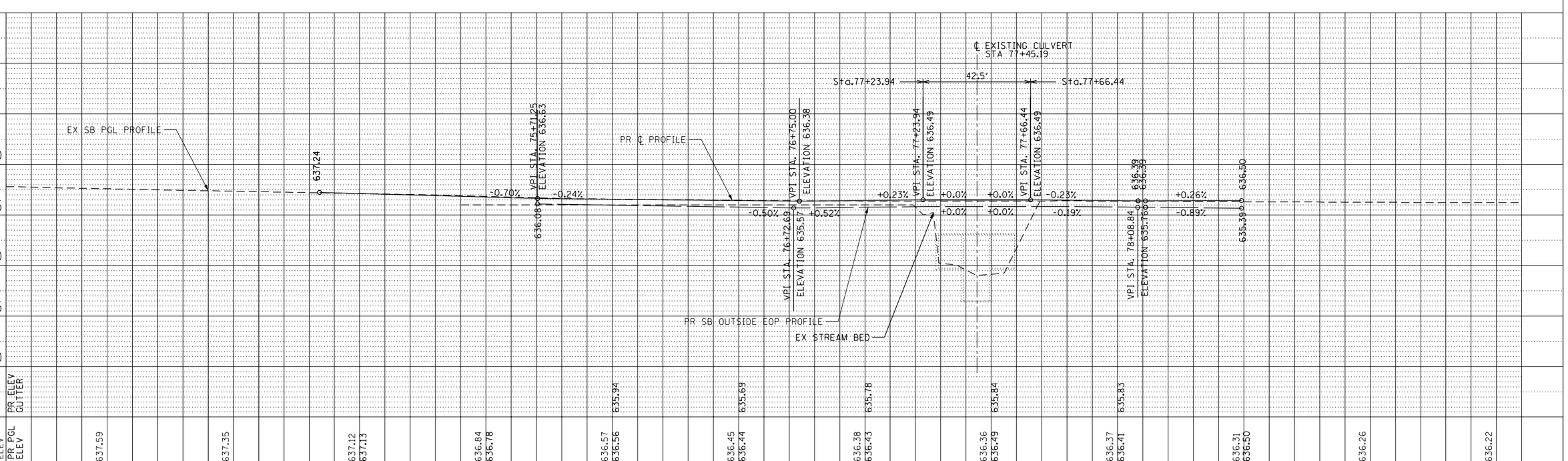
PLAN

	BY	DATE
SURVEYED		
PLOTTED		
ALIGNMENT CHECKED		
RT. OF WAY CHECKED		
CADD FILE NAME		
NOTE BOOK		
NO.		



PROFILE

	BY	DATE
SURVEYED		
PLOTTED		
DRAWN		
CHECKED		
NOTE BOOK		
NO.		



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CHECKED	- TVN	REVISED -
DATE	- $DATE$	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US 12/45 (MANNHEIM ROAD) OVER ADDISON CREEK
PROFILE

SCALE:1"=20'H;1"=10'V SHEET OF SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	16
		CONTRACT NO. 60V22		
		ILLINOIS FED. AID PROJECT		

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MAINTENANCE OF TRAFFIC GENERAL NOTES

1 THE SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLANS SHALL SERVE AS A GUIDE FOR SAFE DIVERSION OF TRAFFIC DURING EXECUTION OF THIS CONTRACT. HOWEVER, THE CONTRACTOR MAY IMPROVE OR MODIFY THE TRAFFIC CONTROL PLANS TO MEET CONSTRUCTION NEEDS BUT NOT AT THE EXPENSE OF PUBLIC SAFETY OR CONVENIENCE. ANY CHANGES TO THE TRAFFIC CONTROL PLAN SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

2 THE CONTRACTOR SHALL BE REQUIRED TO MAINTAIN TRAFFIC IN ACCORDANCE WITH THE TRAFFIC CONTROL PLANS, SPECIAL PROVISIONS, APPLICABLE STATE STANDARDS, AND AS DIRECTED BY THE ENGINEER.

3 CONTRACTOR SHALL MAINTAIN A MINIMUM OF TWO THROUGH LANES IN EACH DIRECTION THROUGH OUT THE PROJECT AREA AT ALL TIMES.

4 UNLESS OTHERWISE NOTED IN THE PLANS, THE CONTRACTOR SHALL BE REQUIRED TO MAINTAIN ACCESS TO ALL ENTRANCES, APPROACHES WITHIN THE PROJECT LIMITS. THIS WORK IS TO BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH AS, "TEMPORARY ACCESS (COMMERCIAL ENTRANCE)".

5 THE TRAFFIC SIGNAL ENGINEER SHALL BE INFORMED 48 HOURS IN ADVANCE OF ANY CHANGE TO THE SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLANS.

6 DRUMS/TYPE II BARRICADES SHALL BE PROVIDED AS SHOWN IN THE PLANS AND SPACED 50 FEET CENTER TO CENTER ON TANGENT, AND 20 FEET CENTER TO CENTER ON TAPERS AND CURVES.

7 THE CONTRACTOR SHALL PROVIDE ADEQUATE TEMPORARY DRAINAGE AND EROSION & SEDIMENT CONTROL PLAN PROTECTION DURING ALL PHASES OF CONSTRUCTION.

8 ALL EXISTING SIGNS THAT CONFLICT WITH THE TRAFFIC CONTROL PLAN SHALL BE COVERED OR REMOVED IN ACCORDANCE WITH ARTICLE 107.25 OF THE STANDARD SPECIFICATIONS.

9 THE CONTRACTOR SHALL PROVIDE, INSTALL, MAINTAIN AND REMOVE ALL SIGNS AND SIGN SUPPORTS REQUIRED FOR TRAFFIC CONTROL AND PROTECTION.

10 THE CONTRACTOR SHALL PLACE A CHANGEABLE MESSAGE SIGN AT EACH END OF THE PROJECT AND/OR AS DIRECTED BY THE ENGINEER TO INFORM MOTORISTS OF UPCOMING CONSTRUCTION ACTIVITIES. THE MESSAGE SIGNS WITH THE APPROPRIATE INFORMATION SHALL BE IN PLACE TWO WEEKS BEFORE START OF CONSTRUCTION ACTIVITY. THIS WORK IS TO BE PAID FOR AT THE CONTRACT UNIT PRICE PER CALENDAR MONTH, "CHANGEABLE MESSAGE SIGN".

11 THE CONTRACTOR SHALL PLACE "DRIVEWAY ENTRANCE" SIGNS AT EVERY COMMERCIAL ENTRANCE WITHIN THE PROJECT LMITS WHERE ENTRANCE IS OBSTRUCTED DUE TO CONSTRUCTION AND/OR AS DIRECTED BY THE ENGINEER. SEE TC-26.

12 THE CONTRACTOR SHALL MAINTAIN SIDEWALK ON ONE SIDE OF THE ROAD AT ALL TIMES.

13 THE CONTRACTOR SHALL CONTACT THE IDOT D1 TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

SUGGESTED CONSTRUCTION STAGING

PRESTAGE
CONSTRUCT WATER MAIN RELOCATION. OPEN CUT PAVEMENT ADJACENT TO THE TRAVELED LANE SHALL NOT BE LEFT OPEN OVERNIGHT OR WHEN THE CONTRACTOR STOPS WORKING AT THE END OF THE WORK DAY. OPEN CUT HOLES SHALL BE BACKFILLED WITH COMPACTED TRENCH BACKFILL UP TO THE RIDING SURFACE OF ADJACENT TRAVEL LANES. IF NECESSARY THE CONTRACT MAY USE STEEL PLATES MEETING THE RESIDENT ENGINEER'S APPROVAL TO COVER OPEN CUTS IN THE PAVEMENT.

MAINTAIN NORTHBOUND AND SOUTHBOUND TRAFFIC ALONG US ROUTE 12/45. UTILIZE THE FOLLOWING IDOT HIGHWAY TRAFFIC CONTROL STANDARDS: 701427, 701502, 701601, 701602, 701606, 701701, 701801, AND 701901

STAGE I
ESTABLISH TRAFFIC CONTROL AND PROTECTION IN ACCORDANCE WITH THE SUGGESTED MAINTENANCE OF TRAFFIC CONTROL PLANS. ESTABLISH EROSION CONTROL AND PROTECTION IN ACCORDANCE WITH THE EROSION CONTROL PLANS. REMOVE THE PORTION OF THE EXISTING CULVERT STRUCTURE AS INDICATED IN THE STRUCTURAL PLANS. CONSTRUCT THE PROPOSED STRUCTURE AS INDICATED IN THE STRUCTURAL PLANS. REMOVE MEDIAN AND CONSTRUCT THE TEMPORARY PAVEMENT FROM STATION 71+62 TO STATION 74+62. TEMPORARY PAVEMENT FROM STATION 64+06 TO STATION 70+18 SHALL BE CONSTRUCTED IN STAGE III. REMOVE MEDIAN AND CONSTRUCT THE HMA PAVEMENT AND FILL IN MEDIAN FROM STATION 74+62 TO STATION 76+97.9 AND FROM STATION 77+92.4 TO STATION 78+18.

STAGE II
ESTABLISH TRAFFIC CONTROL AND PROTECTION IN ACCORDANCE WITH THE SUGGESTED MAINTENANCE OF TRAFFIC CONTROL PLANS. ESTABLISH EROSION CONTROL AND PROTECTION IN ACCORANCE WITH THE EROSION CONTROL PLANS. REMOVE THE WEST SIDE OF THE EXISTING CULVERT AS INDICATED IN THE STRUCTURAL PLANS. CONSTRUCT THE WEST SIDE OF THE PROPOSED STRUCTURE AS INDICATED IN THE STRUCTURAL PLANS. CONSTRUCT THE HMA PAVEMENT, COMBINATION CURB AND GUTTER, DRIVEWAY PAVEMENTS, BRICK PAVERS AND PCC SIDEWALK.

STAGE III
CONSTRUCT TEMPORARY PAVEMENT SOUTH OF US 20 (LAKE STREET). INSTALL THE TEMPORARY TRAFFIC SIGNAL AT THE INTERSECTION OF US 12/45 AND US 20. ESTABLISH TRAFFIC CONTROL AND PROTECTION IN ACCORDANCE WITH THE SUGGESTED MAINTENANCE OF TRAFFIC CONTROL PLANS. ESTABLISH EROSION CONTROL AND PROTECTION IN ACCORANCE WITH THE EROSION CONTROL PLANS. REMOVE MEDIAN AND CONSTRUCT THE TEMPORARY PAVEMENT FROM STATION 65+06 TO STATION 70+18. REMOVE THE EAST SIDE OF EXISTING CULVERT AS INDICATED IN THE STRUCTURAL PLANS. CONSTRUCT THE EAST SIDE OF THE PROPOSED STRUCTURE AS INDICATED IN THE STRUCTURAL PLANS. CONSTRUCT THE HMA PAVEMENT, COMBINATION CURB AND GUTTER, DRIVEWAY PAVEMENTS, LIGHT POLE RELOCATION, AND PCC SIDEWALK.

STAGE IV
ESTABLISH TRAFFIC CONTROL AND PROTECTION IN ACCORDANCE WITH THE SUGGESTED MAINTENANCE OF TRAFFIC CONTROL PLANS. ESTABLISH EROSION CONTROL AND PROTECTION IN ACCORANCE WITH THE EROSION CONTROL PLANS. REMOVE THE TEMPORARY PAVEMENT CONSTRUCTED IN THE STAGE I AND STAGE III. CONSTRUCT COMBINATION CURB AND GUTTER, CONCRETE MEDIAN SURFACE 4IN, AND CONCRETE MEDIAN TYPE SB-6.06

POST-STAGE
HMA SURFACE COURSE RE-ESTABLISH THE PERMANENT TRAFFIC SIGNAL AT THE US 12/45 AND US 20 INTERSECTION, INSTALL PERMANENT PAVEMENT MARKINGS, PERFORM CLEAN UP AND PUNCH LIST ITEMS.

MAINTAIN NORTHBOUND AND SOUTHBOUND TRAFFIC ALONG US ROUTE 12/45. UTILIZE THE FOLLOWING IDOT HIGHWAY TRAFFIC CONTROL STANDARDS: 701427, 701502, 701601, 701602, 701606, 701701, 701801, AND 701901

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US12/45 (MANNHEIM RD) OVER ADDISON CREEK
SUGGESTED STAGES OF CONSTRUCTION
AND TRAFFIC CONTROL GENERAL NOTES AND DESCRIPTION

SCALE: N/A | SHEET 1 OF 1 SHEETS | STA. - TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	17
		CONTRACT NO. 60V22		
	ILLINOIS	FED. AID PROJECT		

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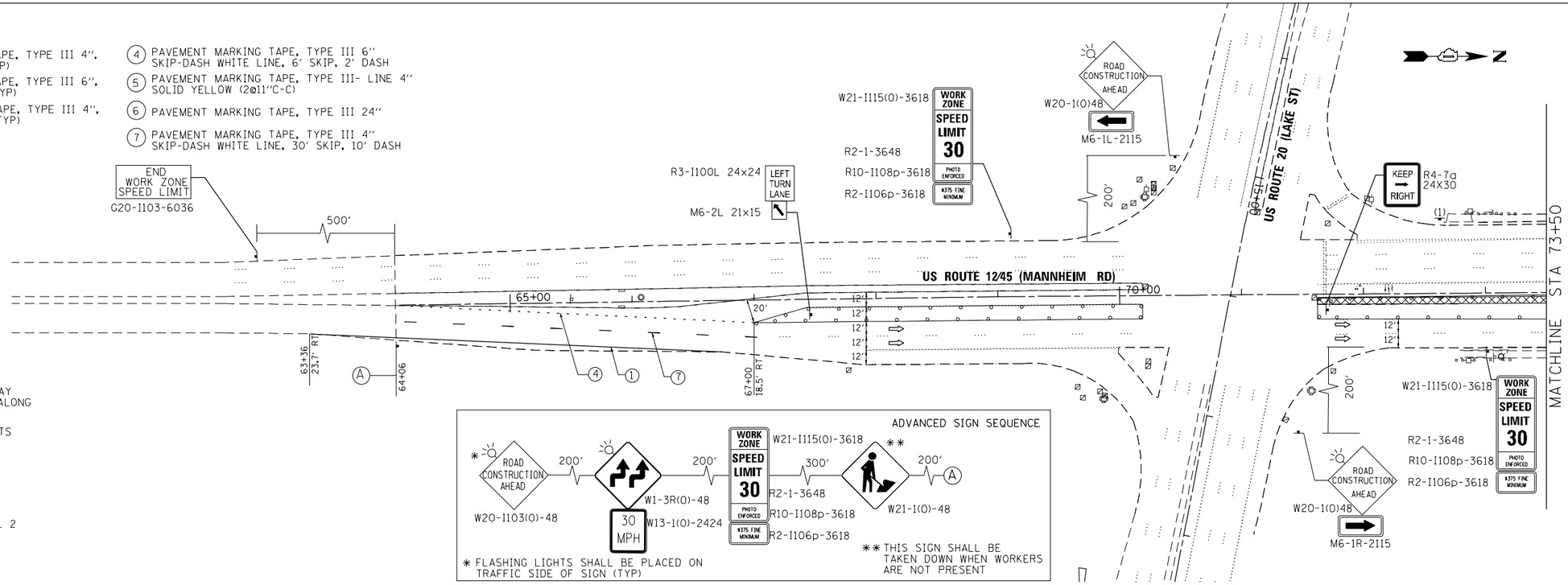
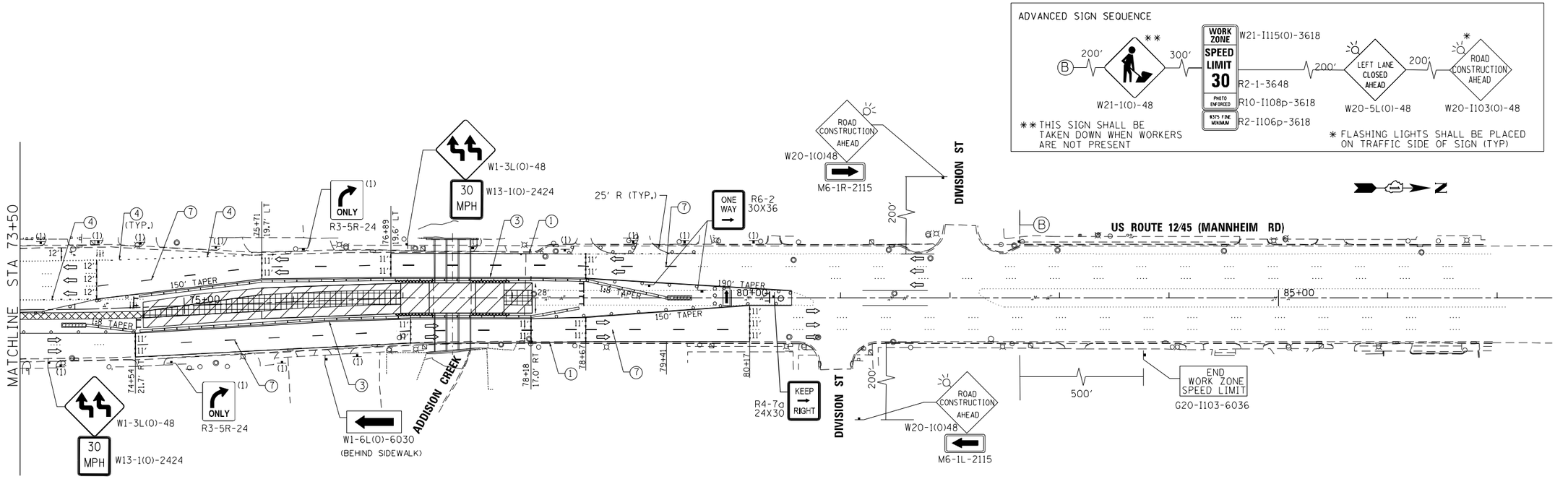
2. PAVEMENT MARKING TAPE, TYPE III 6", SOLID WHITE LINE (TYP)
3. PAVEMENT MARKING TAPE, TYPE III 4", SOLID YELLOW LINE (TYP)
4. PAVEMENT MARKING TAPE, TYPE III 6" SKIP-DASH WHITE LINE, 6' SKIP, 2' DASH
5. PAVEMENT MARKING TAPE, TYPE III- LINE 4" SOLID YELLOW (2@11"C-C)
6. PAVEMENT MARKING TAPE, TYPE III 24"
7. PAVEMENT MARKING TAPE, TYPE III 4" SKIP-DASH WHITE LINE, 30' SKIP, 10' DASH

LEGEND

- DRUMS OR TYPE II BARRICADES @ 50 FT CENTERS ALONG ROADWAY (TANGENT) AND 20 FT CENTERS ALONG TAPERS AND RADIUS RETURNS.
- TYPE III BARRICADES WITH LIGHTS
- ARROW BOARD
- WORK ZONE
- TEMPORARY PAVEMENT
- IMPACT ATTENUATOR TEST LEVEL 2
- TEMPORARY CONCRETE BARRIER
- HMA PAVEMENT





STG1-1

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US12/45 (MANNHEIM RD) OVER ADDISON CREEK
SUGGESTED STAGES CONSTRUCTION & TRAFFIC CONTROL PLAN STAGE-I

SCALE: 1"=50' | SHEET OF SHEETS | STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	18
		CONTRACT NO. 60V22		
	ILLINOIS	FED. AID PROJECT		

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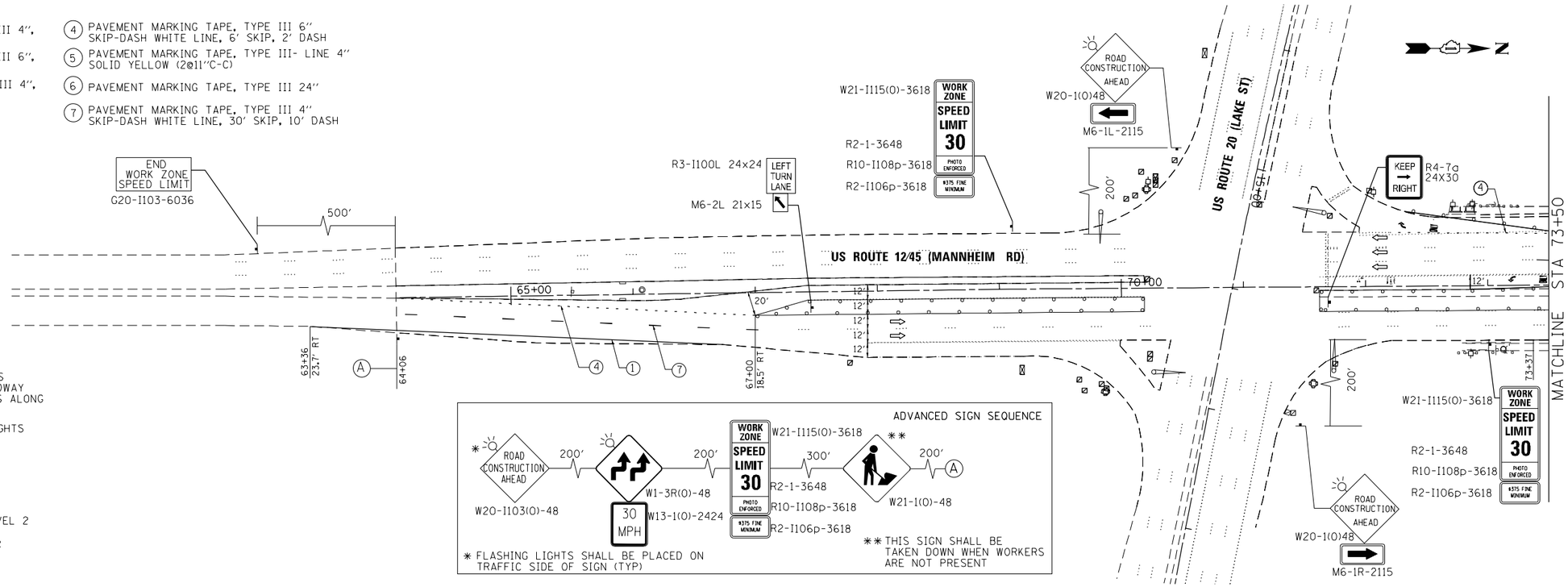
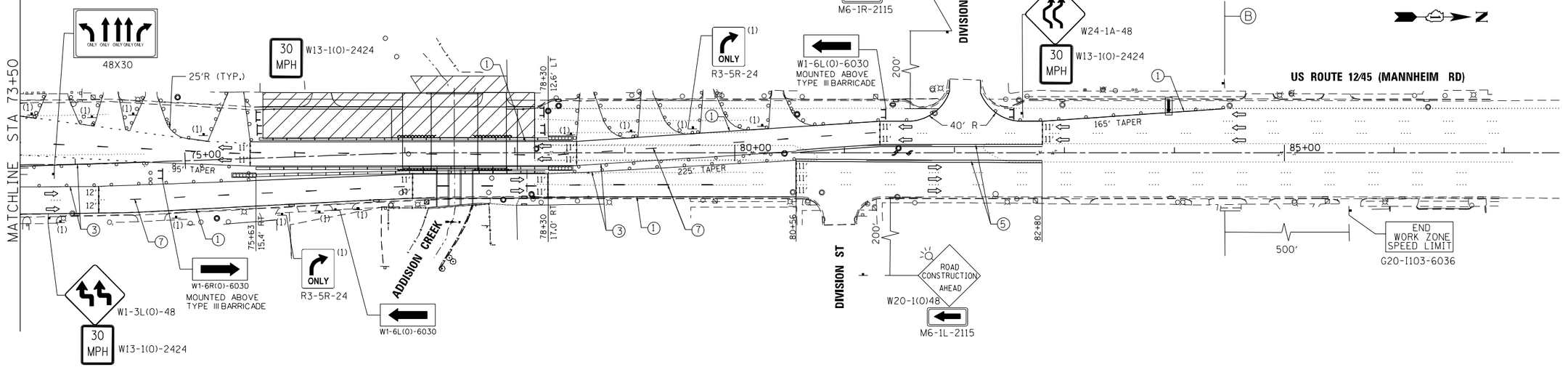
2. PAVEMENT MARKING TAPE, TYPE III 6", SOLID WHITE LINE (TYP)
3. PAVEMENT MARKING TAPE, TYPE III 4", SOLID YELLOW LINE (TYP)
4. PAVEMENT MARKING TAPE, TYPE III 6" SKIP-DASH WHITE LINE, 6' SKIP, 2' DASH
5. PAVEMENT MARKING TAPE, TYPE III- LINE 4" SOLID YELLOW (2@11"C-C)
6. PAVEMENT MARKING TAPE, TYPE III 24"
7. PAVEMENT MARKING TAPE, TYPE III 4" SKIP-DASH WHITE LINE, 30' SKIP, 10' DASH

LEGEND

- DRUMS OR TYPE II BARRICADES @ 50 FT CENTERS ALONG ROADWAY (TANGENT) AND 20 FT CENTERS ALONG TAPERS AND RADIUS RETURNS.
- TYPE III BARRICADES WITH LIGHTS
- ARROW BOARD
- WORK ZONE
- TEMPORARY PAVEMENT
- IMPACT ATTENUATOR TEST LEVEL 2
- TEMPORARY CONCRETE BARRIER
- HMA PAVEMENT





STG2-1

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CHECKED	TVN	REVISED	-
DATE	1/18/2018	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US12/45 (MANNHEIM RD) OVER ADDISON CREEK
SUGGESTED STAGES CONSTRUCTION & TRAFFIC CONTROL PLAN STAGE-II

SCALE: 1"=50' | SHEET OF SHEETS | STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	19
		CONTRACT NO. 60V22		
	ILLINOIS	FED. AID PROJECT		

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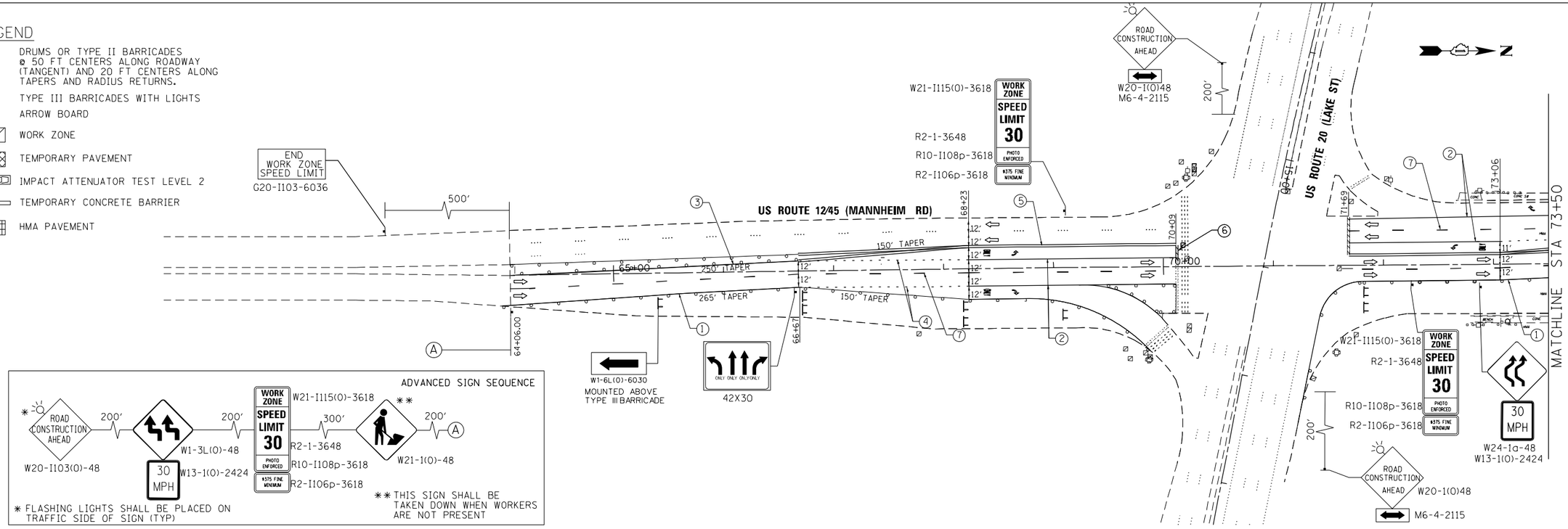
LEGEND

- DRUMS OR TYPE II BARRICADES @ 50 FT CENTERS ALONG ROADWAY (TANGENT) AND 20 FT CENTERS ALONG TAPERS AND RADIUS RETURNS.
- TYPE III BARRICADES WITH LIGHTS
- ARROW BOARD
- WORK ZONE
- TEMPORARY PAVEMENT
- IMPACT ATTENUATOR TEST LEVEL 2
- TEMPORARY CONCRETE BARRIER
- HMA PAVEMENT



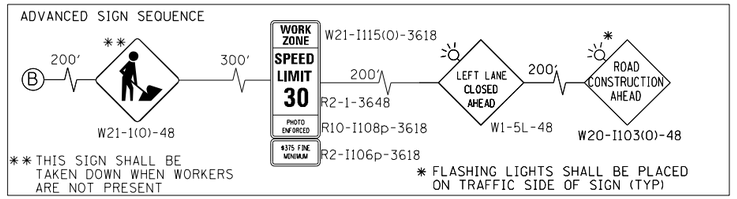
ADVANCED SIGN SEQUENCE

* FLASHING LIGHTS SHALL BE PLACED ON TRAFFIC SIDE OF SIGN (TYP)

** THIS SIGN SHALL BE TAKEN DOWN WHEN WORKERS ARE NOT PRESENT

1. PAVEMENT MARKING TAPE, TYPE III 4", SOLID WHITE LINE (TYP)
2. PAVEMENT MARKING TAPE, TYPE III 6", SOLID WHITE LINE (TYP)
3. PAVEMENT MARKING TAPE, TYPE III 4", SOLID YELLOW LINE (TYP)
4. PAVEMENT MARKING TAPE, TYPE III 6" SKIP-DASH WHITE LINE, 6' SKIP, 2' DASH
5. PAVEMENT MARKING TAPE, TYPE III- LINE 4" SOLID YELLOW (2@11"C-C)
6. PAVEMENT MARKING TAPE, TYPE III 24"
7. PAVEMENT MARKING TAPE, TYPE III 4" SKIP-DASH WHITE LINE, 30' SKIP, 10' DASH

ADVANCED SIGN SEQUENCE



** THIS SIGN SHALL BE TAKEN DOWN WHEN WORKERS ARE NOT PRESENT

* FLASHING LIGHTS SHALL BE PLACED ON TRAFFIC SIDE OF SIGN (TYP)

STG3-1

2600 Warrenville Road, Suite 203, Downers Grove, IL 60515
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MILLENNIA PROFESSIONAL SERVICES

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DRAWN - JP		REVISED -
CHECKED - TVN		REVISED -
DATE - 1/18/2018		REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US12/45 (MANNHEIM RD) OVER ADDISON CREEK
SUGGESTED STAGES CONSTRUCTION & TRAFFIC CONTROL PLAN STAGE-III

SCALE: 1"=50' | SHEET OF SHEETS | STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	20
		CONTRACT NO. 60V22		
	ILLINOIS	FED. AID PROJECT		

FILE NAME = P:\2011\ME11007_VarVar_PhII\CADD\W011.US12-45\Shts\D160V22-sht-Stage3.dgn
PLOT SCALE = 100.0000 ' / in.
USER NAME = Millennia Professional Services

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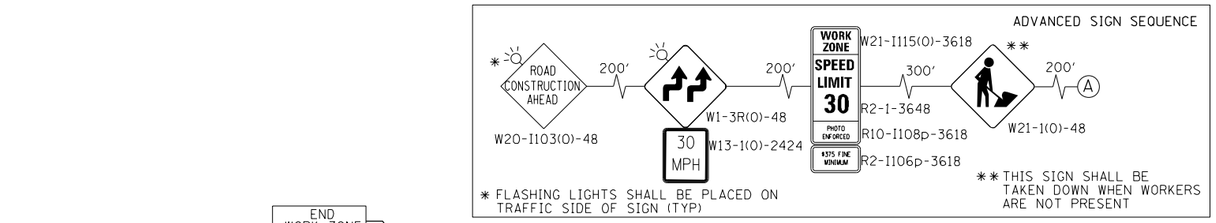
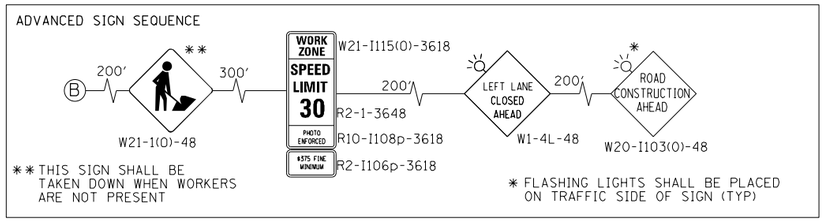
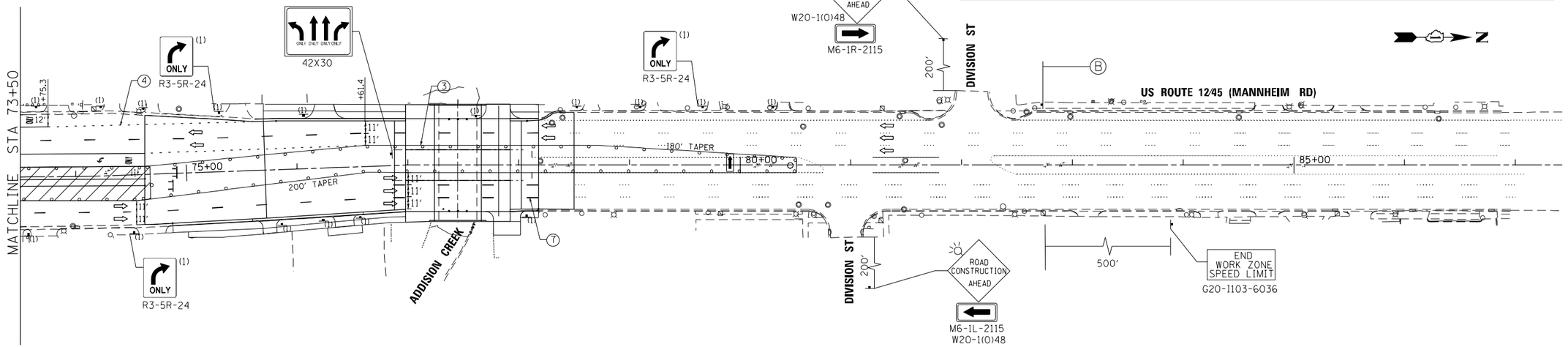


1. PAVEMENT MARKING TAPE, TYPE III 4", SOLID WHITE LINE (TYP)
2. PAVEMENT MARKING TAPE, TYPE III 6", SOLID WHITE LINE (TYP)
3. PAVEMENT MARKING TAPE, TYPE III 4", SOLID YELLOW LINE (TYP)
4. PAVEMENT MARKING TAPE, TYPE III 6" SKIP-DASH WHITE LINE, 6' SKIP, 2' DASH
5. PAVEMENT MARKING TAPE, TYPE III- LINE 4" SOLID YELLOW (2@11"C-C)
6. PAVEMENT MARKING TAPE, TYPE III 24"
7. PAVEMENT MARKING TAPE, TYPE III 4" SKIP-DASH WHITE LINE, 30' SKIP, 10' DASH





STG4-1

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DATE - 1/18/2018	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US12/45 (MANNHEIM RD) OVER ADDISON CREEK
SUGGESTED STAGES CONSTRUCTION & TRAFFIC CONTROL PLAN STAGE-IV

SCALE: 1"=50' | SHEET OF SHEETS | STA. - TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	21
		CONTRACT NO. 60V22		
	ILLINOIS	FED. AID PROJECT		

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PLOT SCALE = 100.0000 ' / in.
USER NAME = Millennia Professional Services

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DRAINAGE GENERAL NOTES

THE CONTRACTOR SHALL NOTIFY THE AGENCIES AND UTILITIES AT LEAST 10 DAYS PRIOR TO ANY CONSTRUCTION IN THE AREA AND SHALL COMPLY WITH ALL RESTRICTIONS FOR EQUIPMENT MOVEMENTS AND CLEARANCES IN REGARDS TO THEIR FACILITIES.

MAINTAINING DRAINAGE: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN DRAINAGE FLOWS AT ALL TIMES DURING THE PERFORMANCE FOR THE WORK. THE CONTRACTOR SHALL SUPPLY A PLAN THAT WILL MAINTAIN FLOWS THAT MEET ALL LOCAL, STATE AND FEDERAL REGULATIONS AND NOT CAUSE ANY DAMAGES UPSTREAM OR TO ANY ADJACENT DRAINAGE WATERSHED. THE PLAN SHALL BE SEALED BY A REGISTERED PROFESSIONAL ENGINEER IN THE STATE OF ILLINOIS. THIS PLAN MUST BE SUBMITTED AT LEAST TWO WEEKS PRIOR TO THE START OF THE PROJECT. COST OF MAINTAINING DRAINAGE FLOWS SHALL BE INCIDENTAL TO THE CONTRACT.

CONTRACTOR SHALL NOT SCALE DIMENSIONS FROM THE CONTRACT PLANS FOR CONSTRUCTION PURPOSES. SCALES, IF SHOWN, ARE FOR INFORMATION ONLY.

THE HORIZONTAL CONTROLS SHALL BE BASED ON STATE PLANE COORDINATES (NAD83). THE VERTICAL CONTROLS ARE BASED ON NORTH AMERICAN DATUM 1988 (NAVD88).

RESTORATION AREAS WILL INCLUDE ALL DISTURBED AREAS INCLUDING AREA FOR GRADING AND SHAPING DITCHES AND ESTABLISHING SLOPES AND THE STABILIZED CONSTRUCTION ENTRANCE WITH PLACING TOPSOIL, SEEDING CLASS 2A, NITROGEN FERTILIZER, POTASSIUM FERTILIZER, PHOSPHOROUS FERTILIZER AND EROSION CONTROL BLANKET. ITEMS LISTED ABOVE WILL BE PAID FOR SEPARATELY.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E" AT 1-800-892-0123 FOR FIELD LOCATION OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES AT LEAST 48 HOURS PRIOR TO BEGINNING CONSTRUCTION.

ALL INVERT ELEVATIONS FOR EXISTING AND PROPOSED PIPES AND HEADWALL TO BE CONSTRUCTED SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION.

STORM SEWER GENERAL NOTES

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING STRUCTURE HAVE BEEN TAKEN FROM FIELD MEASUREMENTS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND DETAILS IN THE FIELD AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE JUSTIFICATION FOR SCHEDULING DELAYS OR CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE FOR THE WORK.

ALL EXCAVATION AND BACKFILL REQUIRED FOR THE INSTALLATION OF CONCRETE END SECTION SHALL BE INCLUDED IN THE COST OF "END SECTIONS" OR "CAST-IN-PLACE REINFORCED CONCRETE END SECTIONS" OF THE DIAMETER SPECIFIED.

BEFORE ORDERING PIPE CULVERTS OR PIPE DRAINS, THE CONTRACTOR SHALL CONSULT THE ENGINEER FOR EXACT LENGTHS.

THE WORK REQUIRED TO CONNECT ANY SEWER TO AN EXISTING DRAINAGE STRUCTURE OR PIPE WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT UNIT PRICE BID FOR THE SEWER ITEMS.

ABANDONED UNDERGROUND UTILITIES THAT CONFLICT WITH CONSTRUCTION SHALL BE DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY ACCORDING TO ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF EARTH EXCAVATION.

ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUBNUMBER SHOWN IN THE LIST OF STANDARDS OR THE COPY INCLUDED IN THESE PLANS.

THE CONTRACTOR SHALL CONTACT J.U.L.I.E. AT LEAST 48 HOURS PRIOR TO EXCAVATION TO DETERMINE WHICH UTILITIES ARE WITHIN THE AREA.

ANY TURF AREAS OUTSIDE THE CONSTRUCTION SEEDING LIMITS WHICH ARE DISTURBED SHALL BE REPAIRED, RESEEDED AND COVERED WITH EROSION BLANKET TO THE SATISFACTION OF AND AS DIRECTED BY THE ENGINEER AT THE CONTRACTORS EXPENSE.

THIS PROJECT REQUIRES A U.S. ARMY CORPS OF ENGINEERS (USACE) 404 PERMIT FOR WORK IN WATERS OF THE US THAT WILL BE SECURED BY THE DEPARTMENT. ALL CONDITIONS OF THE 404 PERMIT, FOUND IN THE SPECIAL PROVISIONS, MUST BE FOLLOWED. WHEN AN IN-STREAM WORK AREA IS REQUIRED THE CONTRACTOR WILL NEED TO SUBMIT AN IN-STREAM WORK PLAN TO THE DEPARTMENT FOR APPROVAL. GUIDELINES ON ACCEPTABLE IN-STREAM WORK TECHNIQUES CAN BE FOUND ON THE USACE WEBSITE. THE USACE DEFINES AND DETERMINES IN-STREAM WORK IN WETLANDS AND WATERWAYS. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT AN IN-STREAM WORK PLAN WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

LEGEND

	EXISTING	PROPOSED
STORM SEWER		
PIPE UNDERDRAIN		
MANHOLE		
CATCH BASIN		
END SECTION		
HEADWALL		
STORM INLET		
WATER MAIN	W	W
TRAFFIC SIGNAL INTERCONNECT		FO
ELECTRICAL LINE	E	
COMMUNICATION LINE	T	
WIRE FENCE	X X X	
SANITARY SEWER		
ABANDON AND FILL EXISTING STORM SEWER		
STORM SEWER REMOVAL		
DRAINAGE STRUCTURES TO BE CLEANED		C
CREEK FLOW		

FILE NAME = D1601V22-sht-gnotes.dgn

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DATE - 3/08/2017		REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

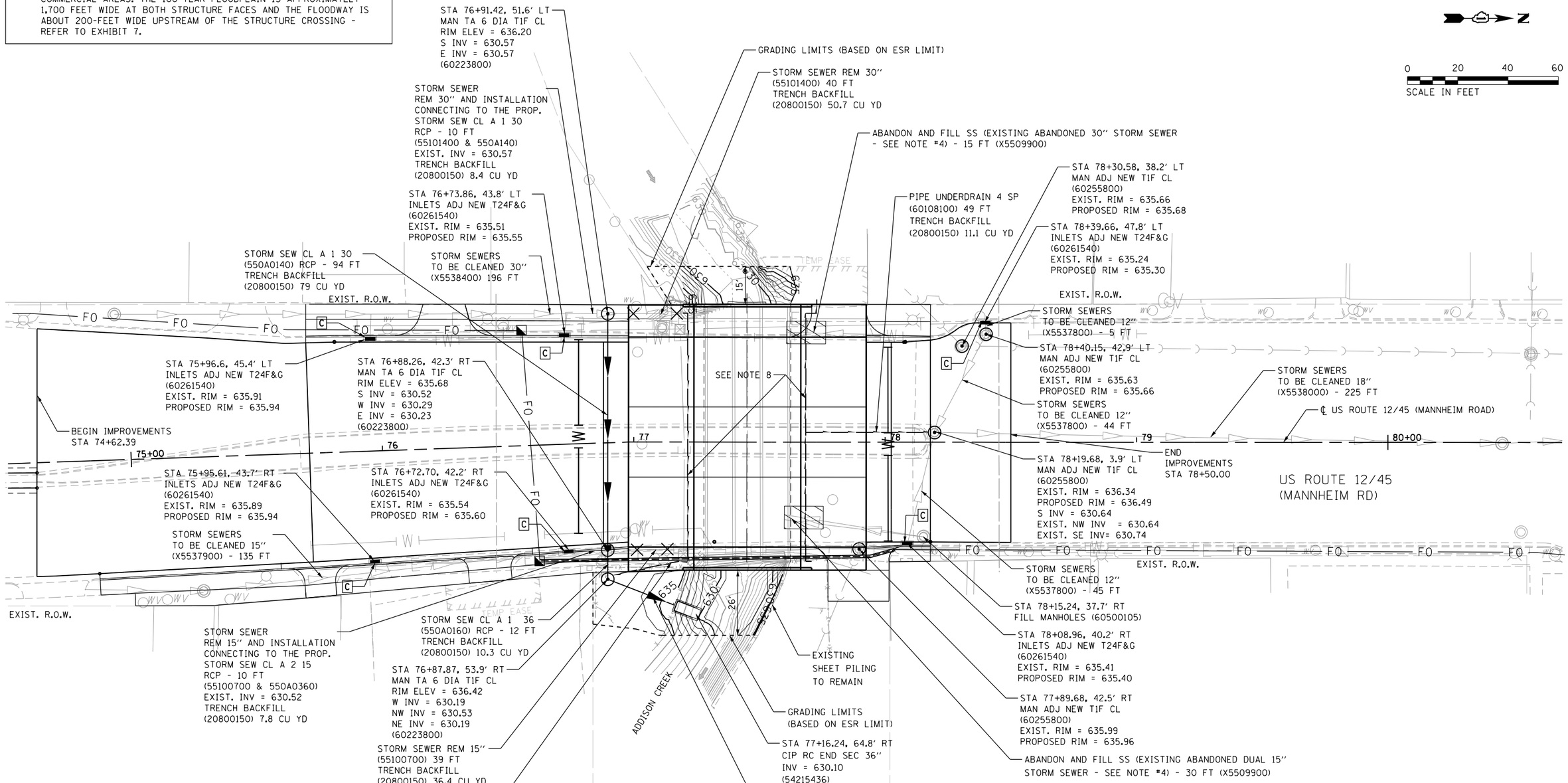
US 12/45 (MANNHEIM RD) OVER ADDISON CREEK

DRAINAGE GENERAL NOTES

SCALE: N.T.S. | SHEET OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	22
		CONTRACT NO. 60V22		
	ILLINOIS	FED. AID PROJECT		

NOTE: BASED ON THE HYDRAULIC REPORT, THE ADDISON CREEK FLOODPLAIN EXPANDS OUT TO INCLUDE A LARGE AREA OF RESIDENTIAL AND COMMERCIAL AREAS. THE 100-YEAR FLOODPLAIN IS APPROXIMATELY 1,700 FEET WIDE AT BOTH STRUCTURE FACES AND THE FLOODWAY IS ABOUT 200-FEET WIDE UPSTREAM OF THE STRUCTURE CROSSING - REFER TO EXHIBIT 7.



NOTES:

1) EXISTING DRAINAGE STRUCTURES ALONG US ROUTE 12/45 (MANNHEIM ROAD) SHALL BE CLEANED DURING PRE- STAGE PRIOR TO THE SHIFT OF TRAFFIC, REGARDLESS OF WHEN THE ADJUSTMENT WORK SHALL TAKE PLACE. THE COST OF THIS CLEANING IS INCLUDED IN THE COST OF NEW FRAMES AND GRATES.

2) ALL DRAINAGE STRUCTURES TO BE ADJUSTED ALONG US ROUTE 12/45 (MANNHEIM ROAD) MUST BE CLEANED AFTER ADJUSTMENTS ARE COMPLETE. THE COST OF THIS CLEANING IS INCLUDED IN THE COST OF NEW FRAMES AND GRATES.

3) ALL THE WATER VALVE VAULTS (BOXES) AND WATER MAIN ADJUSTMENTS ARE SHOWN ON WATER MAIN IMPROVEMENT SHEETS.

4) THE CONTRACTOR SHALL FIELD VERIFY THE EXISTING FIELD CONDITIONS AND THE LOCATION OF ALL UTILITIES BOTH PUBLIC AND PRIVATELY OWNED PRIOR TO COMMENCING WORK ON THE PROJECT. THERE WILL BE NO ADDITIONAL COMPENSATION FOR CLAIMS THAT ARISE FROM FAILURE TO INVESTIGATE THE EXISTING CONDITIONS.

5) FOR PROPOSED WATER MAIN DETAILS SEE WATER MAIN PLAN.

6) THE CONTRACTOR SHALL MAINTAIN EXISTING 15" AND 30" STORM SEWER OUTFALLS (SOUTH OF THE BRIDGE) UNTIL THE COMPLETION OF THE NEW STORM SEWER (SEE MOT STAGING FOR DETAILS).

7) FOR DRAINAGE LEGEND SEE DRAINAGE GENERAL NOTES.

8) SEE STRUCTURAL PLANS FOR DETAILS.

9) SEE WATER MAIN PLANS FOR ADDITIONAL DETAILS.

FILE NAME = D160V22-sht-drain 01.dgn

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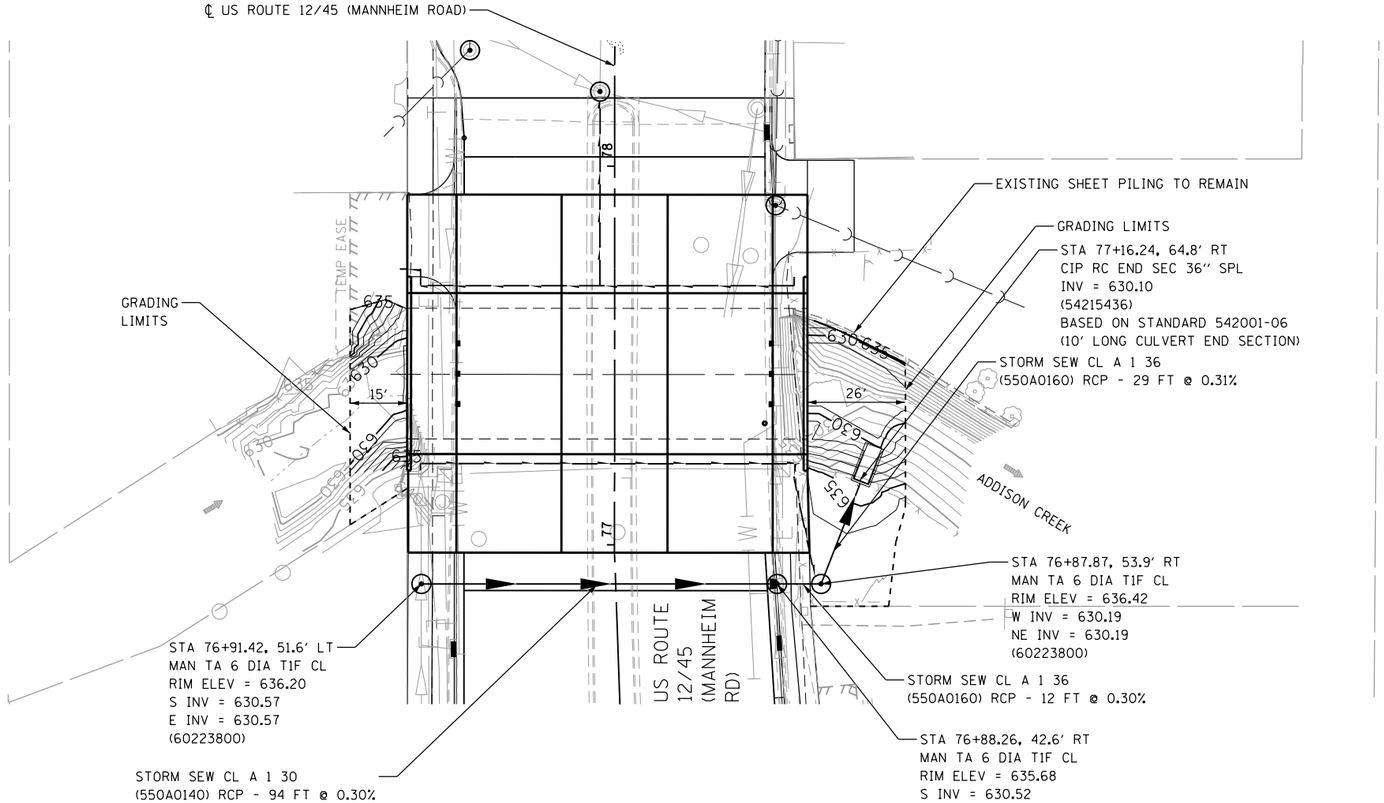
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US 12/45 (MANNHEIM RD) OVER ADDISON CREEK

SCALE: 1"=20' | SHEET OF SHEETS | STA. | TO STA.

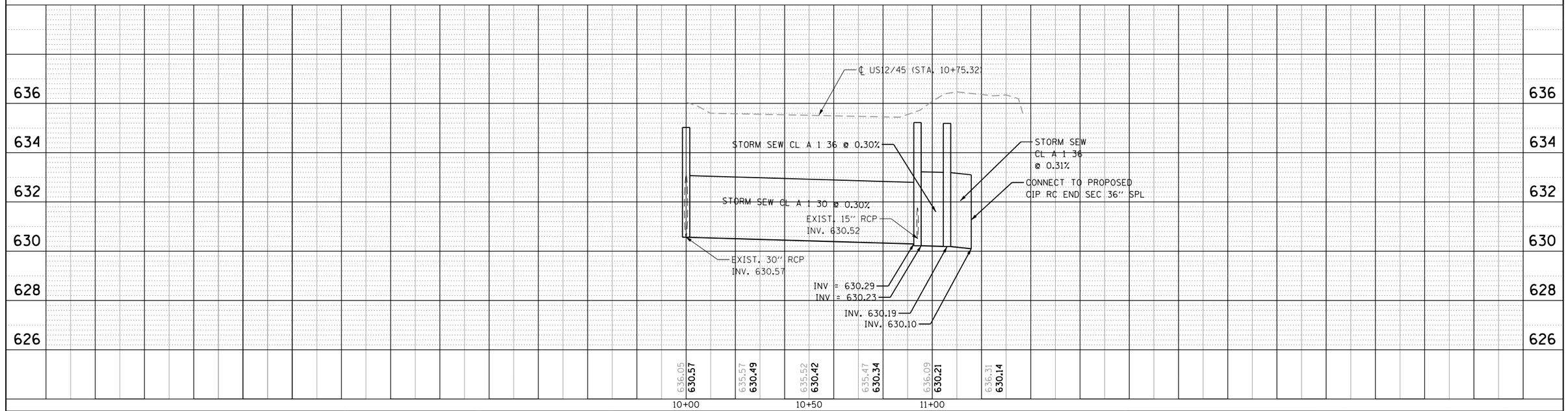
DRAINAGE PLAN

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	23
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			



NOTES:

1) FOR PROPOSED DRAINAGE ITEMS SEE DRAINAGE PLAN SHEET



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US 12/45 (MANNHEIM RD) OVER ADDISON CREEK

SCALE:1'=20'H 1'=2'V | SHEET OF SHEETS | STA. TO STA.

DRAINAGE PLAN & PROFILE

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	24
		CONTRACT NO. 60V22		
		ILLINOIS FED. AID PROJECT		

FILE NAME = D160V22-sht-dr-prof 01.dgn

EROSION CONTROL GENERAL NOTES

1. THE CONTRACTOR SHALL BE REQUIRED TO INSTALL AND MAINTAIN SILT FENCE AS INDICATED IN THE PLAN PRIOR TO BEGINNING ANY CONSTRUCTION ACTIVITIES WHICH WILL POTENTIALLY CREATE EROSION CONDITIONS. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COST WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.

2. PERIMETER EROSION BARRIER SHALL BE ERECTED ADJACENT TO THE CONSTRUCTION LIMITS. THE RESIDENT ENGINEER SHALL MAKE THE FINAL DETERMINATION ON THE PLACEMENT AND LOCATION OF THE PERIMETER EROSION BARRIER. IN ADDITION TO THE LOCATIONS INDICATED ON THE PLAN, SILT FENCE SHALL BE PLACED AROUND THE PERIMETER OF THE EARTH STOCKPILES.

3. THE TOTAL DISTURBED PROJECT AREA IS 42,207.73 SQ. FT. (LESS THAN 1 ACRE). NO SWPPP (STORM WATER POLLUTION PREVENTION PLAN) WILL BE REQUIRED FOR THE PROJECT.

4. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF THE YEAR, SITE CONDITIONS AND THE USE OF TEMPORARY AND PERMANENT MEASURES.

5. EXISTING CULVERTS WITHIN THE DISTURBED LIMIT SHALL BE PROTECTED PRIOR TO ANY CONSTRUCTION ACTIVITIES COMMENCING UPSTREAM.

6. INLETS EXPOSED TO TRAFFIC, AND ALONG CURB & GUTTER WITHIN 4' OF STAGED TRAFFIC, WITH INLET FILTER PROTECTION SHALL HAVE FILTER BASKETS WITH OVERFLOW TO ALLOW FOR THE POSITIVE DRAINAGE OF WATER OFF THE ROADWAY. THESE INLET FILTERS SHALL BE INSPECTED AND CLEANED UTILIZING OFF-PEAK LANE CLOSURES AS APPROVED BY THE ENGINEER. THE COST OF TRAFFIC CONTROL FOR INSPECTION AND MAINTENANCE OF EROSION CONTROL SYSTEMS SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT BID PRICE FOR "TRAFFIC CONTROL AND PROTECTION".

7. ALL STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.

8. THE FOLLOWING EROSION AND SEDIMENT CONTROL MEASURES, BUT NOT LIMITED TO, WILL BE CHECKED WEEKLY AND AFTER EACH SIGNIFICANT RAINFALL (0.5 INCHES OR GREATER IN A 24-HOUR PERIOD):
   A. SEEDING - ALL ERODIBLE BARE EARTH AREAS WILL BE TEMPORARILY SEEDED ON A WEEKLY BASIS.
   B. PERIMETER EROSION BARRIER - SEDIMENT WILL BE REMOVED WHEN IT EXCEEDS HALF THE HEIGHT OF THE FENCE AND FENCE THAT COLLAPSES WILL BE REPLACED IMMEDIATELY.
   C. EROSION CONTROL - ANY AREAS WHICH FAIL WILL BE REPAIRED IMMEDIATELY.
   D. EROSION CONTROL BLANKET AND OTHER EROSION CONTROLS WILL BE INSPECTED AFTER EVERY RUNOFF EVENT AND MAINTAINED AS NEEDED.
   E. INLET FILTERS WILL BE INSPECTED AFTER EVERY RUNOFF EVENT AND MAINTAINED AS NEEDED.

ALL MAINTENANCE OF EROSION CONTROL SYSTEMS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR. ALL LOCATIONS WHERE VEHICLES ENTER OR EXIT THE CONSTRUCTION SITE AND ALL OTHER AREAS SUBJECT TO EROSION SHOULD ALSO BE INSPECTED PERIODICALLY.
ALL ESC MEASURES WILL BE MAINTAINED IN ACCORDANCE WITH THE IDOT EROSION AND SEDIMENT CONTROL FIELD GUIDE FOR CONSTRUCTION INSPECTION AND IDOT's BEST MANAGEMENT PRACTICES -MAINTENANCE GUIDE: (http://www.idot.illinois.gov/transportation-system/environment/erosion-and-sediment-control).

9. ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED. AS APPROVED BY THE ENGINEER.

10. CONSTRUCTION ACTIVITIES SHALL BE SCHEDULED TO MINIMIZE THE TIME SOIL IS EXPOSED AND UNPROTECTED. IN NO CASE SHALL THE EXISTING VEGETATION BE DESTROYED, REMOVED, OR DISTURBED MORE THAN FOURTEEN (14) CALENDAR DAYS PRIOR TO THE INITIATION OF IMPROVEMENTS.

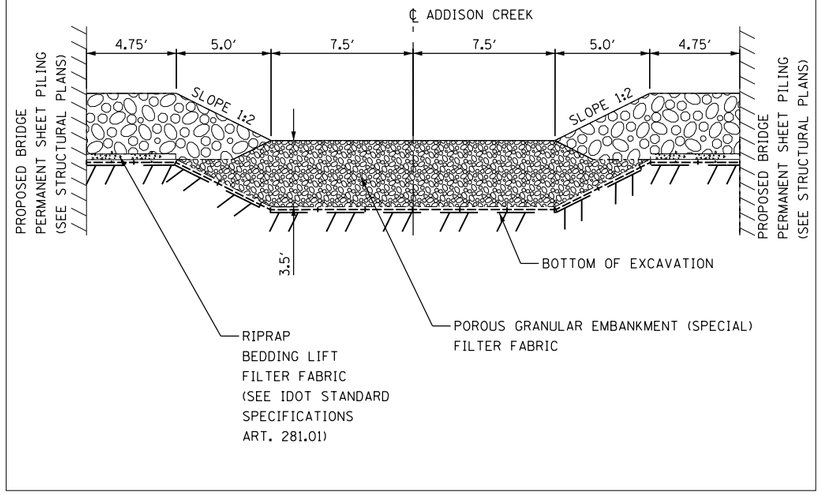
11. IN AREAS WHERE WORK IS COMPLETED, PERMANENT STABILIZATION SHALL BE INITIATED IMMEDIATELY UPON COMPLETION OR CESSATION OF DISTURBANCE FOR 14 DAYS, AND THE INSTALLATION SHALL BE COMPLETED BY THE 14TH DAY AFTER NO DISTURBANCE.

12. THE CONTRACTOR IS RESPONSIBLE FOR INSTALLATION OF ANY ADDITIONAL EROSION CONTROL MEASURES NECESSARY TO PREVENT EROSION AND SEDIMENTATION AS DETERMINED BY THE ENGINEER.

LEGEND

EXISTING		PROPOSED
——— ———	R.O.W.	
	INLET FILTERS (28000510) AT OPEN GRATE OF INLET, CB OR MH	◈
	STONE RIPRAP, CLASS A4 (28100107) (SEE STRUCTURAL PLANS FOR RR TYPICAL SECTION)	▦
——▷——	STORM SEWER OR PIPE CULVERT	——▶——
	PERIMETER EROSION BARRIER (28000400)	- - - -
	HEAVY DUTY EROSION CONTROL BLANKET (25100635) CLASS 2A MIXTURE SEEDING (25000210) TOPSOIL FURNISH AND PLACE 4" (21101615)	▦
⇨	CREEK FLOW	
	TEMPORARY FENCING (20101000)	— x — x —
	POROUS GRANULAR EMBANKMENT, SPECIAL (X2070304) (SEE STRUCTURAL PLANS FOR PGE TYPICAL SECTION)	▩

PROPOSED CHANNEL DETAIL



FILE NAME = D:\60V22-sht-eros notes.dgn

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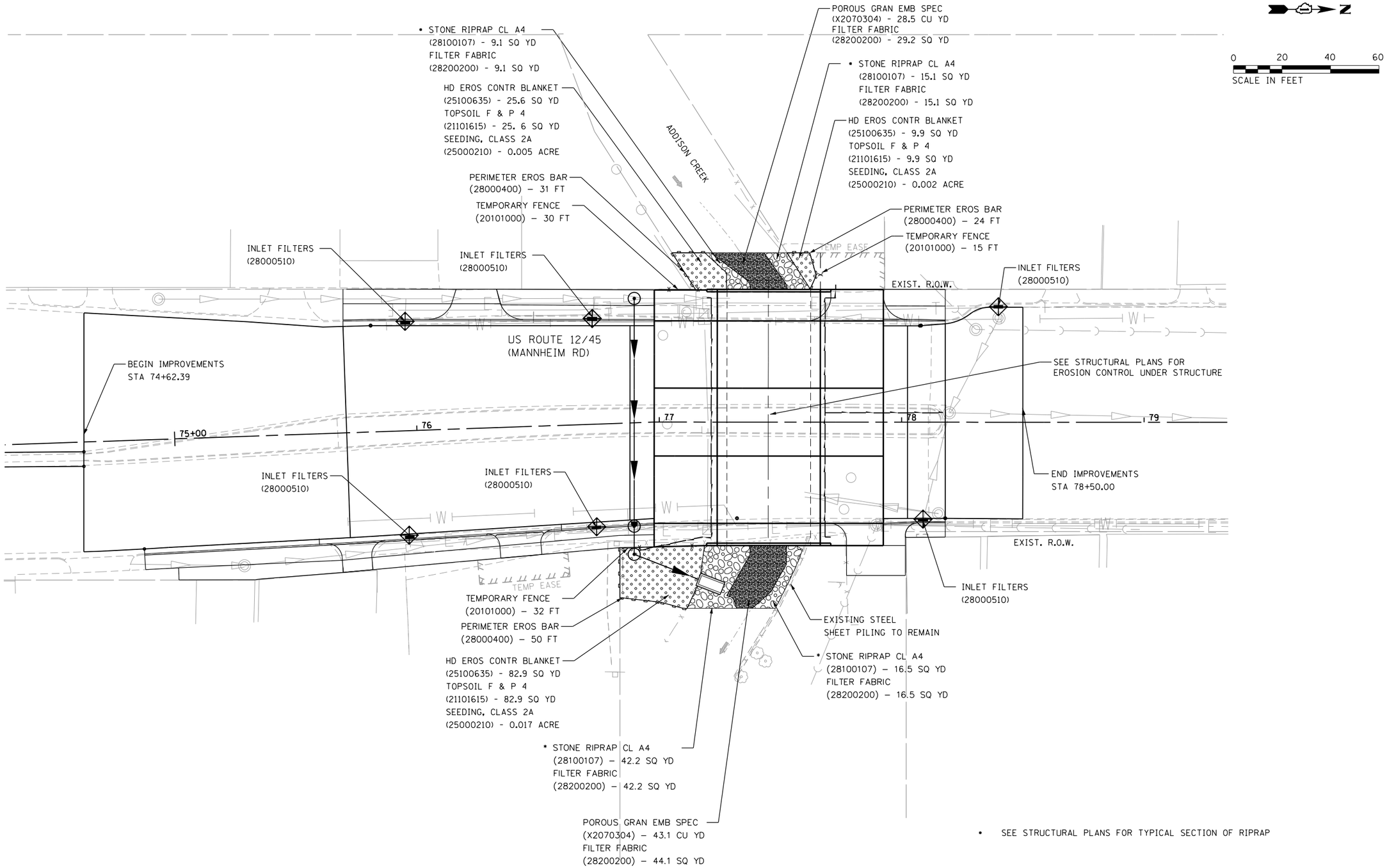
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US 12/45 (MANNHEIM RD) OVER ADDISON CREEK

SCALE: N.T.S. | SHEET OF SHEETS | STA. TO STA.

EROSION CONTROL GENERAL NOTES

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	25
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			



• SEE STRUCTURAL PLANS FOR TYPICAL SECTION OF RIPRAP

FILE NAME = D16OV22-sht-eros 01.dgn

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US 12/45 (MANNHEIM RD) OVER ADDISON CREEK

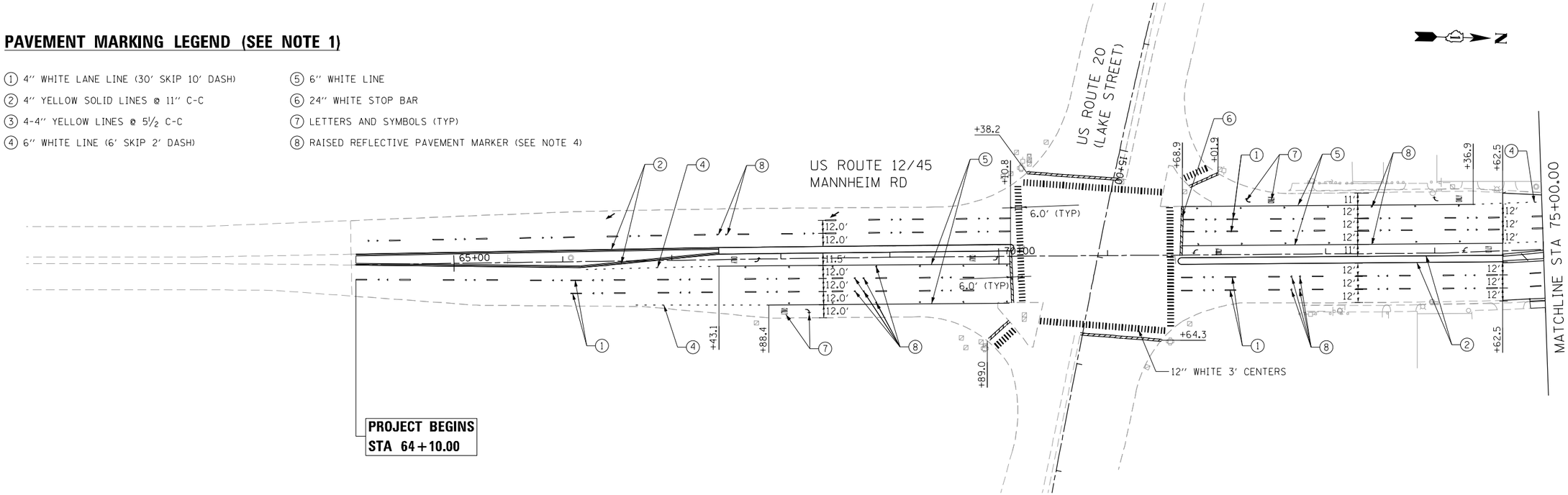
SCALE: 1"=20' | SHEET OF SHEETS | STA. TO STA.

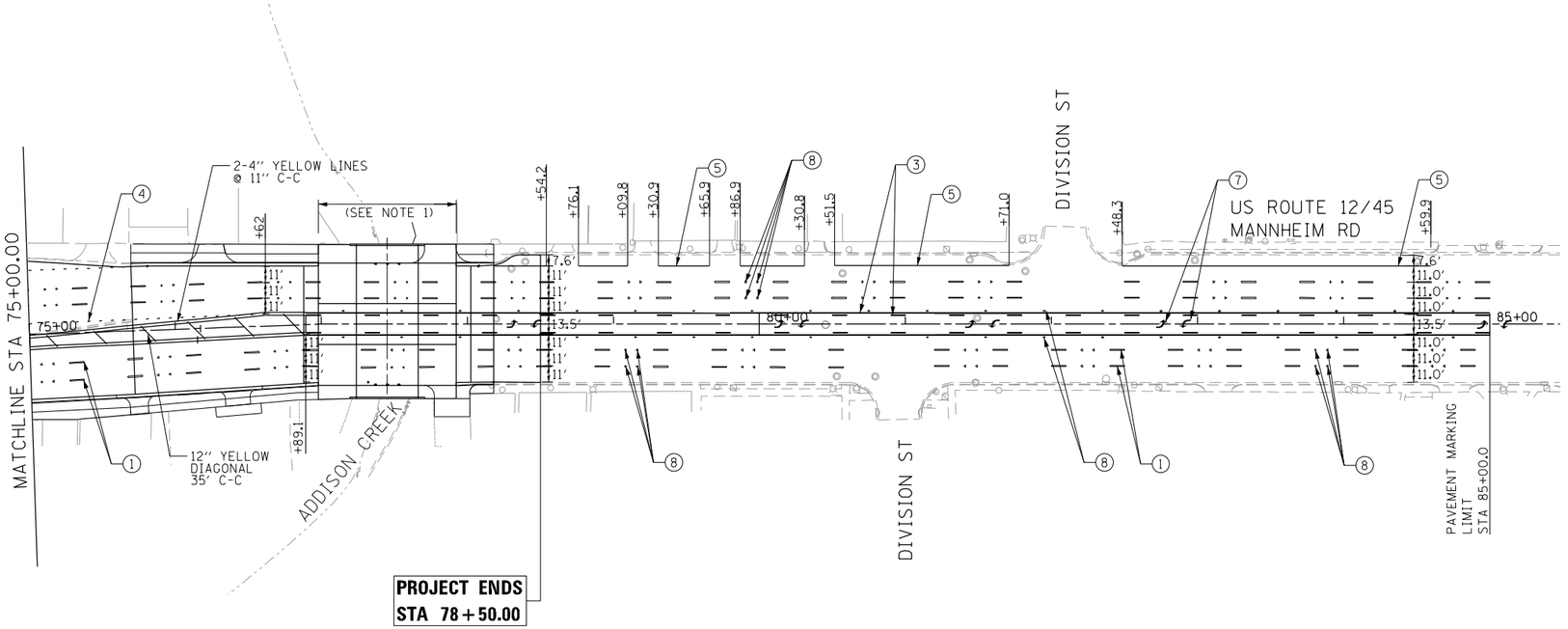
SOIL EROSION CONTROL PLAN

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	26
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

PAVEMENT MARKING LEGEND (SEE NOTE 1)

1. 4" WHITE LANE LINE (30' SKIP 10' DASH)
2. 4" YELLOW SOLID LINES @ 11" C-C
3. 4-4" YELLOW LINES @ 5½" C-C
4. 6" WHITE LINE (6' SKIP 2' DASH)
5. 6" WHITE LINE
6. 24" WHITE STOP BAR
7. LETTERS AND SYMBOLS (TYP)
8. RAISED REFLECTIVE PAVEMENT MARKER (SEE NOTE 4)





NOTES

1. LANE LINE PAVEMENT MARKINGS WITHIN THE PCC BRIDGE DECK AND APPROACH PAVEMENT SHALL BE PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 7".
   LANE LINE SHALL CONSIST OF 4" WIDTH WHITE AND 1.5" WIDTH BLACK CONTRAST PAVEMENT MARKING ON EACH SIDE OF THE WHITE LINE (SEE SPECIAL PROVISIONS).
   THE LANE LINE SHALL BE RECESSED AND PAID FOR AS GROOVING FOR RECESSED PAVEMENT MARKING 8" (SEE SPECIAL PROVISIONS).
   ALL OTHER PAVEMENT MARKINGS WITHIN THE PCC BRIDGE DECK AND APPROACH PAVEMENT SHALL BE POLYUREA.
2. PAVEMENT MARKINGS OUTSIDE OF THE PCC BRIDGE DECK AND APPROACH PAVEMENT SHALL BE THERMOPLASTIC.
3. THE CONTRACTOR SHALL PLACE PROPOSED PAVEMENT MARKINGS IN ACCORDANCE WITH DISTRICT 1 TYPICAL PAVEMENT MARKINGS DETAIL (TC-13).
4. REFER TO DISTRICT 1 RAISED REFLECTIVE PAVEMENT MARKERS (TC-11) FOR ADDITIONAL INFORMATION.

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DESIGNED	TVN	REVISED	-
DRAWN	JP	REVISED	-
CHECKED	TVN	REVISED	-
DATE	1/15/2018	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US12/45 (MANNHEIM RD) OVER ADDISON CREEK
PROPOSED PAVEMENT MARKING

SCALE: 1"=50' | SHEET OF SHEETS | STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	27
		CONTRACT NO. 60V22		
	ILLINOIS	FED. AID PROJECT		

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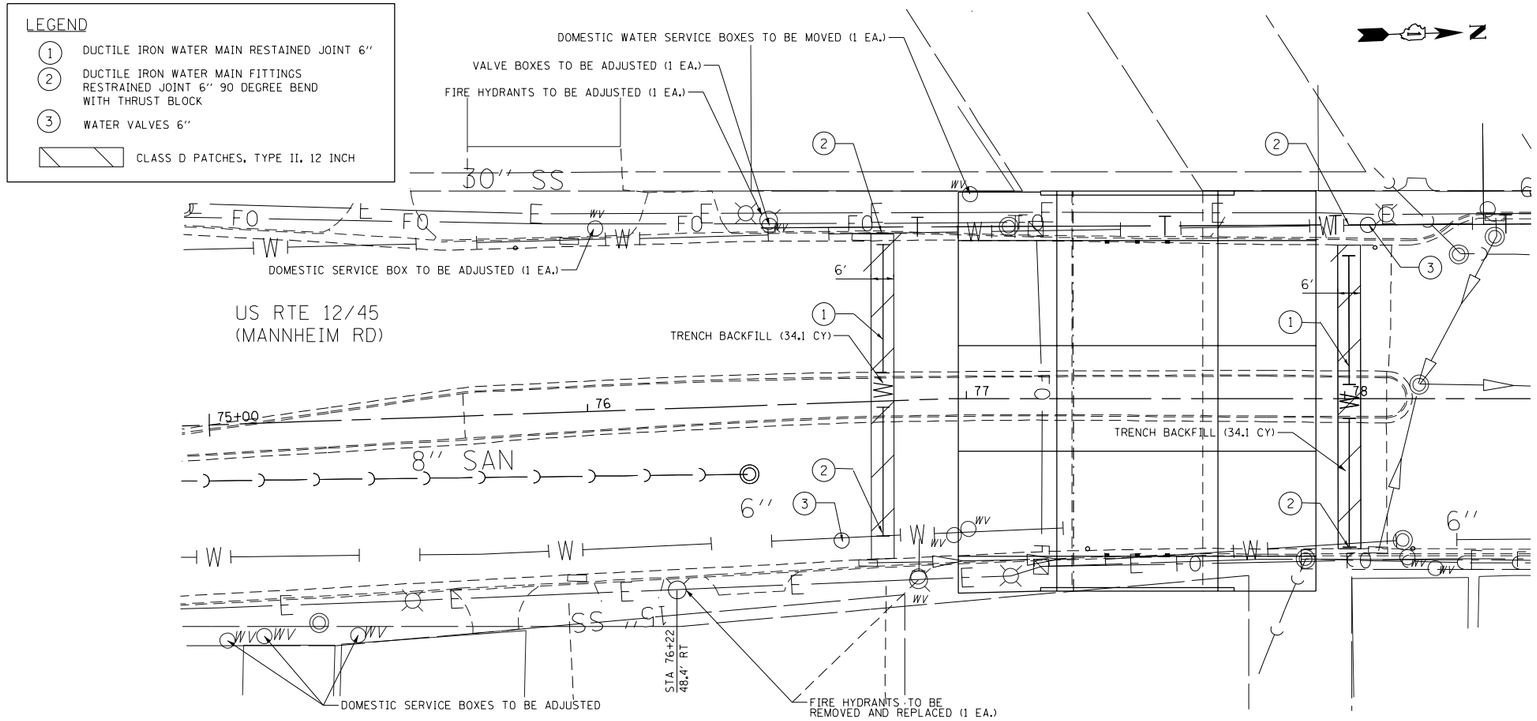
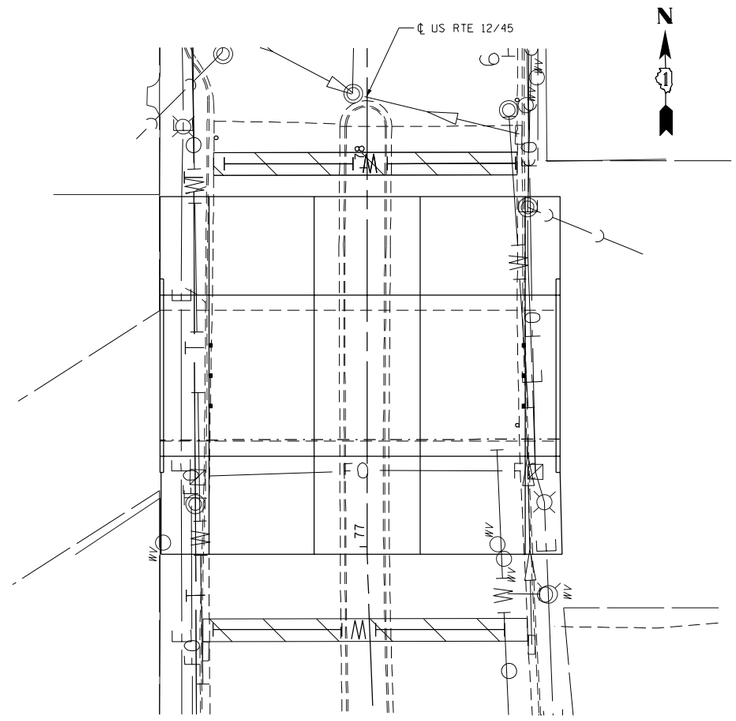
PLOT SCALE = 100.0000 ' / in.
USER NAME = Millennia Professional Services

LEGEND

1. DUCTILE IRON WATER MAIN RESTAINED JOINT 6"
2. DUCTILE IRON WATER MAIN FITTINGS RESTRAINED JOINT 6" 90 DEGREE BEND WITH THRUST BLOCK
3. WATER VALVES 6"

CLASS D PATCHES, TYPE II, 12 INCH





WATER MAIN - GENERAL NOTES

1. WORK SHALL BE DONE IN ACCORDANCE WITH APPLICABLE SECTIONS OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION ADOPTED JANUARY 1, 2013 AND IN ACCORDANCE WITH APPLICABLE SECTIONS OF THE STANDARD SPECIFICATIONS FOR WATER & SEWER CONSTRUCTION IN ILLINOIS.

2. THE CONTRACTOR SHALL NOTIFY THE VILLAGE OF STONE PARK, PUBLIC WORKS DEPARTMENT, AT LEAST 1 WEEK IN ADVANCE OF COMMENCING WORK. THE VILLAGE SHALL BE CONTACTED AT (708) 450-3208 DURING THEIR NORMAL BUSINESS HOURS OF 8 AM TO 4 PM, MONDAY THROUGH FRIDAY.

3. RESTRAINED JOINT DUCTILE IRON PIPE AND FITTINGS SHALL BE USED FOR THE NEW MAIN CONSTRUCTION.

4. CONNECTIONS TO EXISTING MAINS ARE REQUIRED. THIS WORK SHALL BE DONE IN ACCORDANCE WITH APPLICATION PORTIONS OF SECTION 41 OF THE STANDARD SPECIFICATIONS FOR WATER & SEWER CONSTRUCTION IN ILLINOIS.

5. NECESSARY SECTIONS OF THE EXISTING WATER MAIN SHALL BE REMOVED FROM SERVICE SO THE CONNECTION TO EXISTING MAINS CAN BE COMPLETED. THIS SHALL RESULT IN AN INTERRUPTION OF SERVICE TO ADJACENT PROPERTIES SERVICED BY THE SECTION OF MAINS THAT ARE TO BE TEMPORARILY REMOVED FROM SERVICE.

6. PROPOSED WATER MAIN PIPE AND FITTINGS SHALL BE INSTALLED BY MEANS OF OPEN CUTTING AND EXCAVATION AS NEEDED.

7. THE PAVEMENT SHALL NOT BE LEFT OPEN DURING NON-WORKING HOURS. THE CONTRACTOR SHALL BACKFILL OPEN CUTS IN THE PAVEMENT PRIOR TO THE END OF DAILY OPERATIONS.

8. IF NECESSARY METAL PLATES MEETING THE RESIDENT ENGINEER'S APPROVAL SHALL BE INSTALLED TO ELMINATE OPEN SECTIONS OF PAVEMENT DURING NON-WORK HOURS.

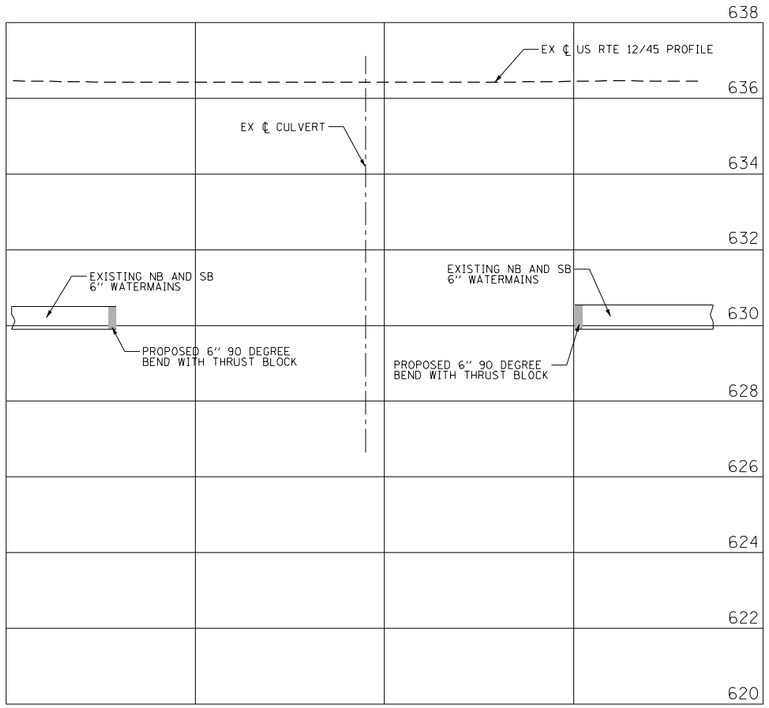
9. THRUST BLOCKS SHALL BE INSTALLED AT LOCATIONS WHERE PROPOSED 90 DEGREE FITTINGS ARE SPECIFIED.

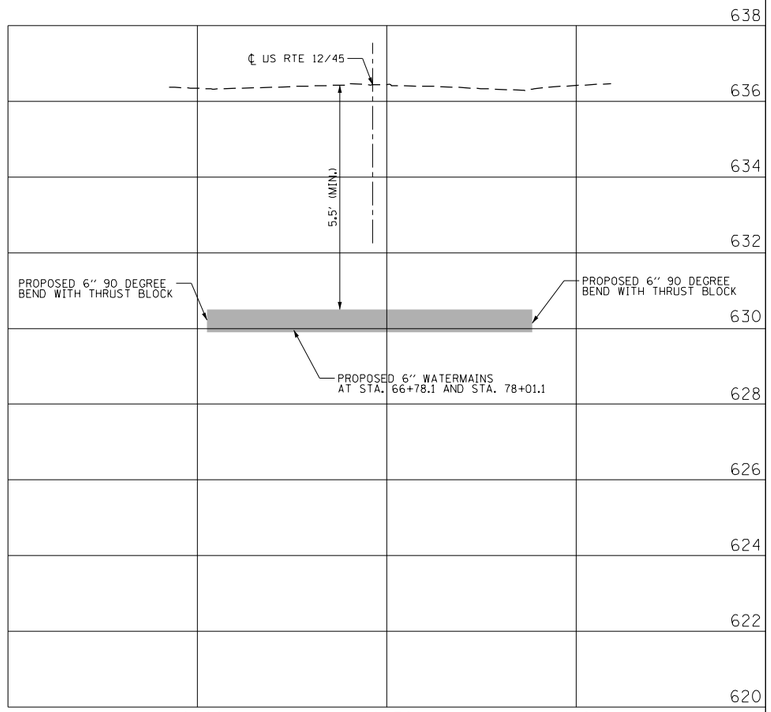
10. PRESSURE TESTING AND DISINFECTION OF THE WATER MAIN SHALL BE DONE IN ACCORDANCE WITH APPLICABLE PORTIONS OF SECTION 41 OF THE STANDARD SPECIFICATIONS FOR WATER & SEWER CONSTRUCTION IN ILLINOIS.

11. ALL WORK SHALL BE DONE AND COMPLETED IN A MANNER ACCEPTABLE TO THE RESIDENT ENGINEER AND THE VILLAGE OF STONE PARK.

12. THE VILLAGE OF STONE PARK SHALL HAVE THE FINAL APPROVAL AND ACCEPTANCE OF THE COMPLETED WORK.

13. THE SECTION OF EXISTING MAIN BETWEEN THE NEW MAIN CROSSINGS SHALL BE ABANDONED IN PLACE. EACH END OF THE EXISTING MAIN TO BE ABANDONED SHALL BE CAPPED.





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DESIGNED - TVN	REVISED -	STATE OF ILLINOIS	US12/45 (MANNHEIM RD) OVER ADDISON CREEK
DRAWN - JP	REVISED -	DEPARTMENT OF TRANSPORTATION	VILLAGE OF STONE PARK – WATERMAIN
CHECKED - TVN	REVISED -		
DATE - 1/18/2018	REVISED -		

SCALE: · SHEET OF SHEETS STA. · TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	28
		CONTRACT NO. 60V22		
	ILLINOIS	FED. AID PROJECT		

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USER NAME = Millennia Professional Services

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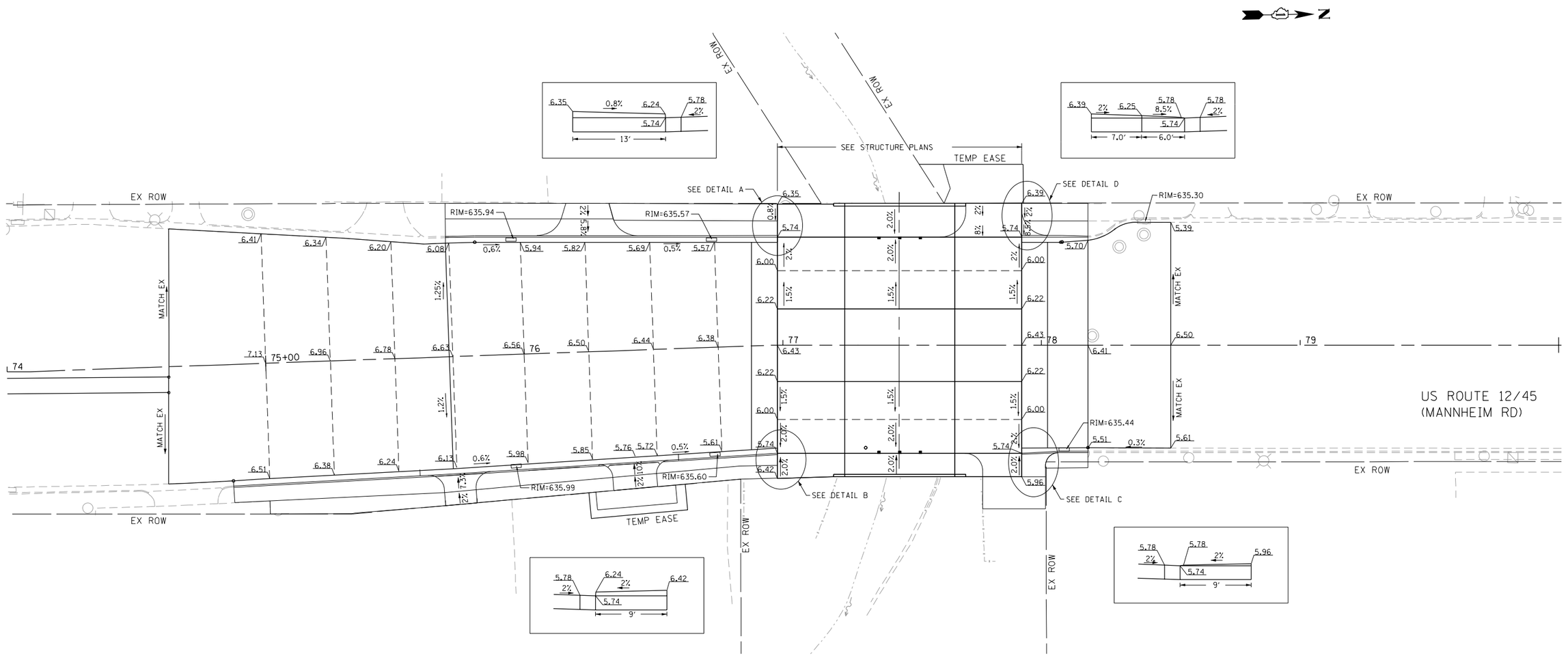

US ROUTE 12/45
(MANNHEIM RD)

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USER NAME = Millennia Professional Services

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DRAWN - JP	REVISED -	
CHECKED - TVN	REVISED -	
DATE - 1/10/2018	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US12/45 (MANNHEIM RD) OVER ADDISON CREEK
PAVEMENT ELEVATIONS

SCALE: 1"=20' | SHEET 1 OF 1 SHEETS | STA. - TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	29
		CONTRACT NO. 60V22		
	ILLINOIS	FED. AID PROJECT		

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TRAFFIC SIGNAL LEGEND

(NOT TO SCALE)

ITEM	EXISTING	PROPOSED
CONTROLLER CABINET		
COMMUNICATION CABINET	CC	CC
MASTER CONTROLLER	MC	MC
MASTER MASTER CONTROLLER	MMC	MMC
UNINTERRUPTABLE POWER SUPPLY		
SERVICE INSTALLATION -(P) POLE MOUNTED	P	P
SERVICE INSTALLATION -(G) GROUND MOUNTED -(GM) GROUND MOUNTED METERED	G GM	G GM
TELEPHONE CONNECTION	ET	T
STEEL MAST ARM ASSEMBLY AND POLE		
ALUMINUM MAST ARM ASSEMBLY AND POLE		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE		
SIGNAL POST -(BM) BARREL MOUNTED - TEMPORARY		BM
WOOD POLE		
GUY WIRE		
SIGNAL HEAD		
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD OPTICALLY PROGRAMMED	P P	P P
FLASHER INSTALLATION -(FS) SOLAR POWERED	F FS F FS	F FS F FS
PEDESTRIAN SIGNAL HEAD		
PEDESTRIAN PUSH BUTTON -(APS) ACCESSIBLE PEDESTRIAN PUSH BUTTON	APS	APS
RADAR DETECTION SENSOR	R	R
VIDEO DETECTION CAMERA	V	V
RADAR/VIDEO DETECTION ZONE		
PAN, TILT, ZOOM (PTZ) CAMERA	PTZ	PTZ
EMERGENCY VEHICLE LIGHT DETECTOR		
CONFIMATION BEACON		
WIRELESS INTERCONNECT		
WIRELESS INTERCONNECT RADIO REPEATER	ERR	RR

ITEM	EXISTING	PROPOSED
HANDHOLE -SQUARE -ROUND		
HEAVY DUTY HANDHOLE -SQUARE -ROUND	H	H
DOUBLE HANDHOLE		
JUNCTION BOX	J	J
RAILROAD CANTILEVER MAST ARM		
RAILROAD FLASHING SIGNAL		
RAILROAD CROSSING GATE		
RAILROAD CROSSBUCK		
RAILROAD CONTROLLER CABINET	RXR	RXR
UNDERGROUND CONDUIT (UC), GALVANIZED STEEL		
TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE		
SYSTEM ITEM	S	SP
INTERSECTION ITEM	I	IP
REMOVE ITEM		R
RELOCATE ITEM		RL
ABANDON ITEM		A
CONTROLLER CABINET AND FOUNDATION TO BE REMOVED		RCF
MAST ARM POLE AND FOUNDATION TO BE REMOVED		RMF
SIGNAL POST AND FOUNDATION TO BE REMOVED		RPF
DETECTOR LOOP, TYPE I		
PREFORMED DETECTOR LOOP	P P	P P
SAMPLING (SYSTEM) DETECTOR	S S	S S
INTERSECTION AND SAMPLING (SYSTEM) DETECTOR	IS IS	IS IS
QUEUE AND SAMPLING (SYSTEM) DETECTOR	QS QS	QS QS
WIRELESS DETECTOR SENSOR	W	W
WIRELESS ACCESS POINT		

ITEM	EXISTING	PROPOSED
SIGNAL HEAD -(P) PROGRAMMABLE SIGNAL HEAD	P	P
SIGNAL HEAD WITH BACKPLATE -(P) PROGRAMMABLE SIGNAL HEAD -(RB) RETROREFLECTIVE BACKPLATE	P RB	P RB
PEDESTRIAN SIGNAL HEAD AT RAILROAD INTERSECTIONS		
PEDESTRIAN SIGNAL HEAD WITH COUNTDOWN TIMER		
ILLUMINATED SIGN "NO LEFT TURN"/"NO RIGHT TURN"		
NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE. ALL DETECTOR LOOP CABLE TO BE SHIELDED	5	5
GROUND CABLE IN CONDUIT, NO. 6 SOLID COPPER (GREEN)	1#6	1#6
ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1/C	1	1
COAXIAL CABLE	C	C
VENDOR CABLE	V	V
COPPER INTERCONNECT CABLE, NO. 18, 3 PAIR TWISTED, SHIELDED	6#18	6#18
FIBER OPTIC CABLE -NO. 62.5/125, MM12F -NO. 62.5/125, MM12F SM12F -NO. 62.5/125, MM12F SM24F	12F 24F 36F	12F 24F 36F
GROUND ROD -(C) CONTROLLER -(M) MAST ARM -(P) POST -(S) SERVICE	C M P S	C M P S

TS SHT NO. 1

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET 1 OF 7 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	30
	TS-05	CONTRACT NO. 60V22		
		ILLINOIS FED. AID PROJECT		

LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.

2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.

3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.

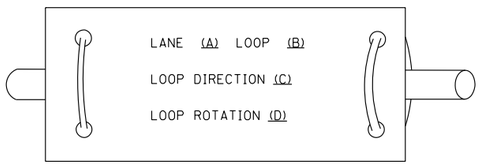
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.

5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.

6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.

7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

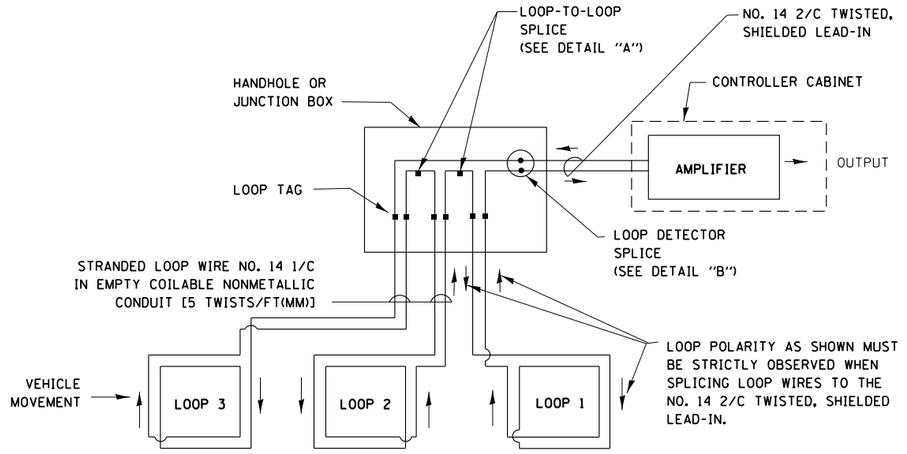


A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY

B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.

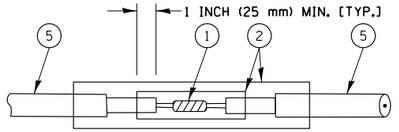
C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".

D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

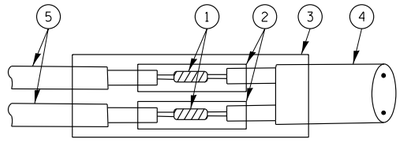


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

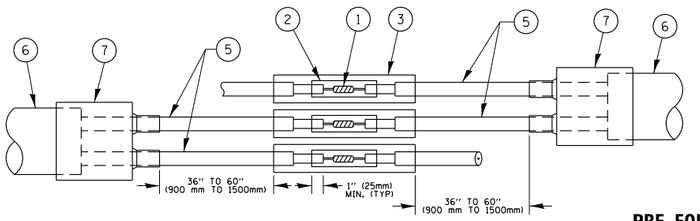


DETAIL "A"
LOOP-TO-LOOP SPLICE



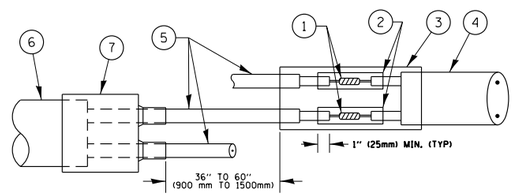
DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE

PRE-FORMED LOOP



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

(1) WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.

(2) WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.

(3) WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.

(4) NO. 14 2/C TWISTED, SHIELDED CABLE.

(5) LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

(6) PRE-FORMED LOOP

(7) XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

TS SHT NO. 2

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Default	PLOT DATE = 5/17/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

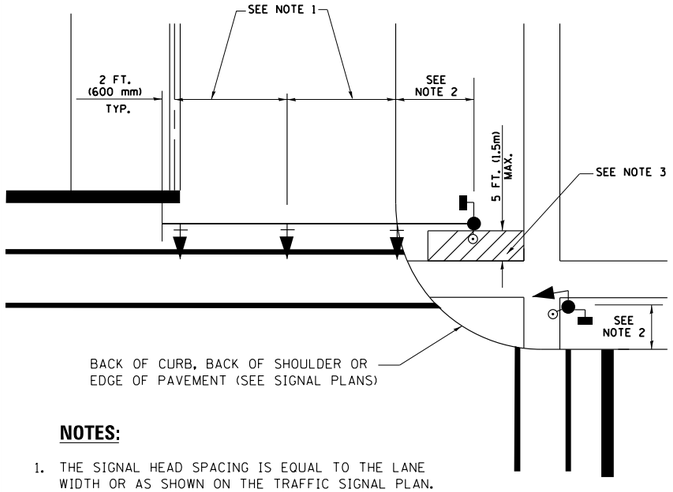
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET 2 OF 7 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	31
	TS-05	CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

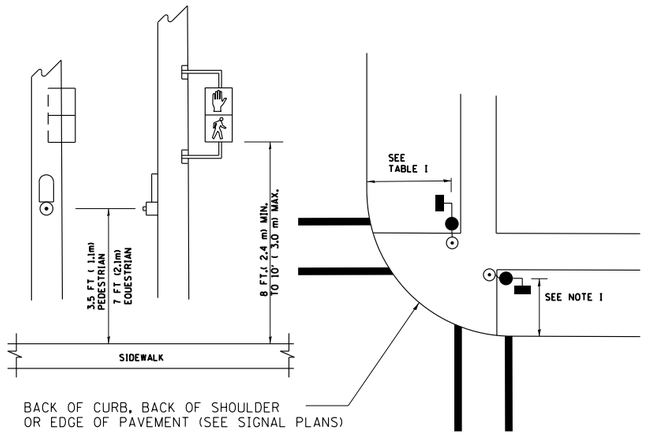
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

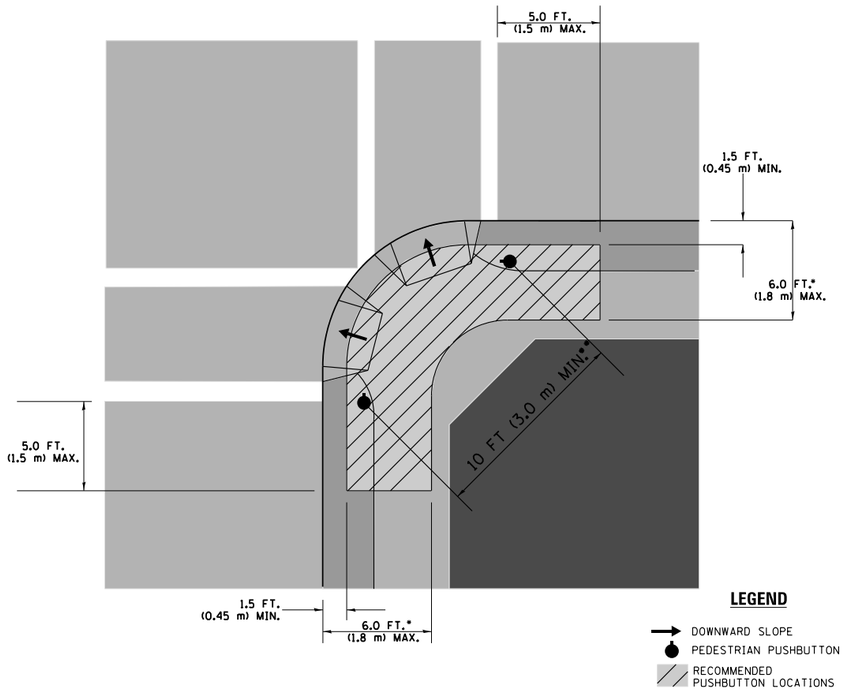
PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.

** WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TOTHE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

TS SHT NO. 3

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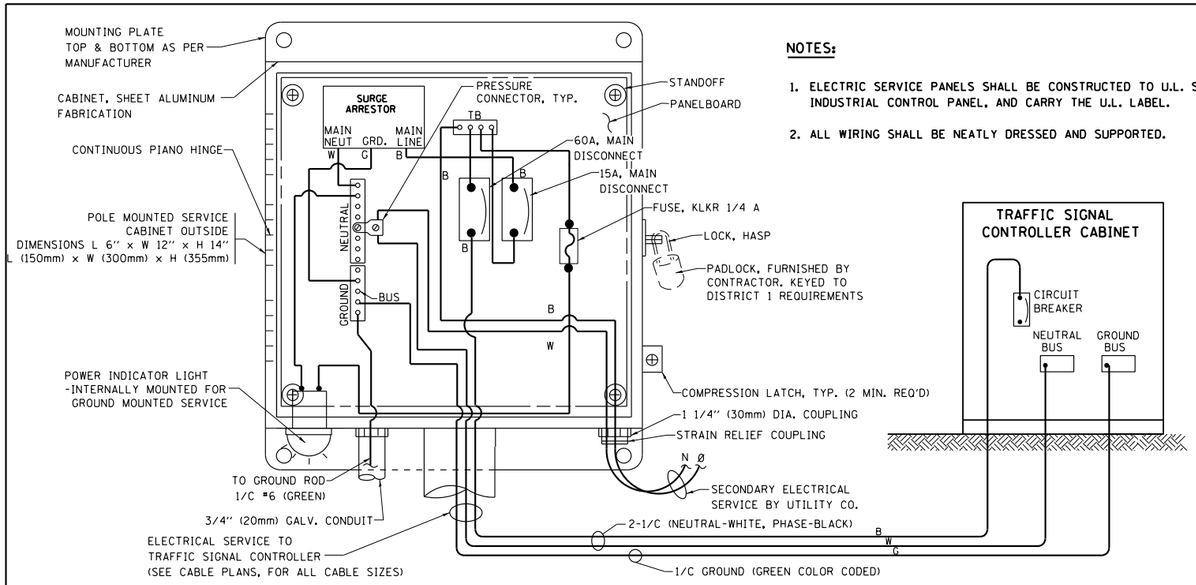
	PLOT DATE = 5/17/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET 3 OF 7 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	32
	TS-05	CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			



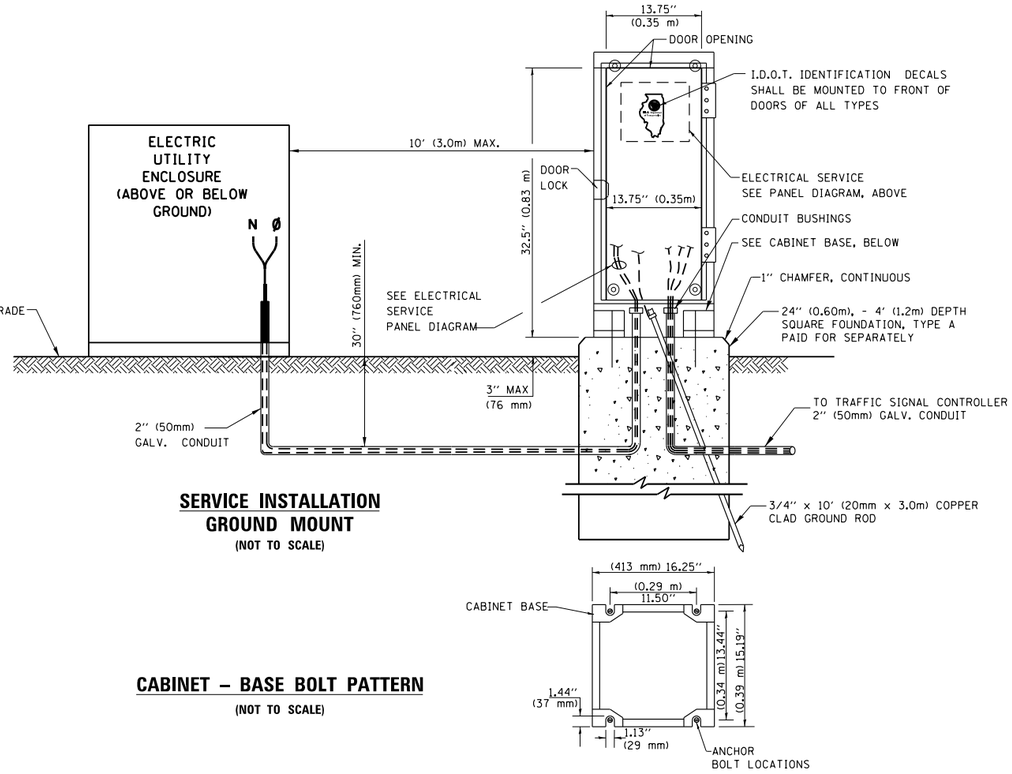
ELECTRICAL SERVICE – PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)

SERVICE INSTALLATION POLE MOUNT (SHOWN)

(NOT TO SCALE)

NOTES:

1. ELECTRIC SERVICE PANELS SHALL BE CONSTRUCTED TO U.L. STD 508, INDUSTRIAL CONTROL PANEL, AND CARRY THE U.L. LABEL.
2. ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.

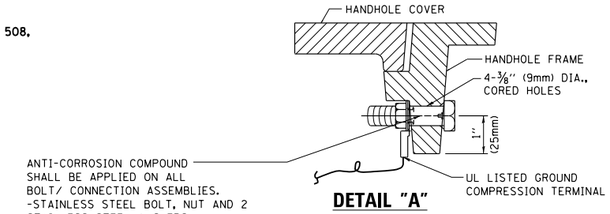


SERVICE INSTALLATION GROUND MOUNT

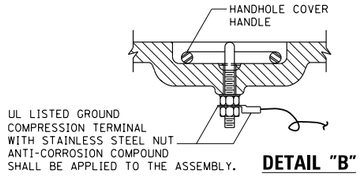
(NOT TO SCALE)

CABINET – BASE BOLT PATTERN

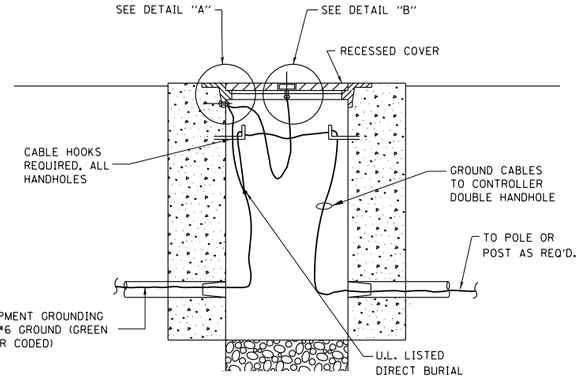
(NOT TO SCALE)



DETAIL "A"

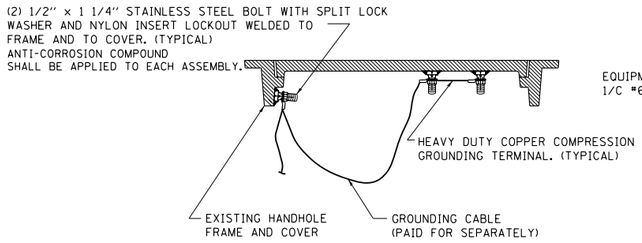


DETAIL "B"



HANDHOLE COVER & FRAME – GROUNDING DETAIL

(NOT TO SCALE)



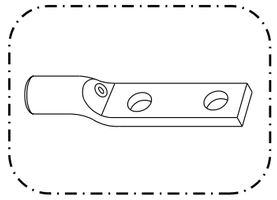
EXISTING HANDHOLE COVER & FRAME – GROUNDING DETAIL

(NOT TO SCALE)

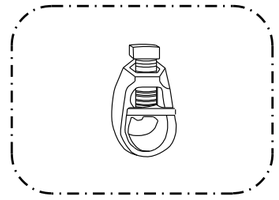
NOTES:

GROUNDING SYSTEM

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC,). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



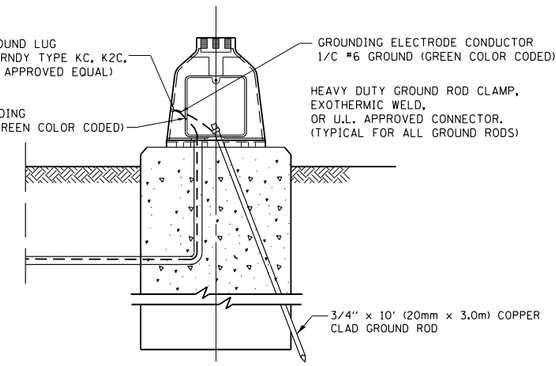
HEAVY-DUTY COMPRESSION TERMINAL (BURNDY TYPE YGHA OR APPROVED EQUAL)



3/4" (20mm) HEAVY-DUTY GROUND ROD CLAMP (BURNDY TYPE GRC OR APPROVED EQUAL)

NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



MAST ARM POLE / POST–GROUNDING DETAIL

(NOT TO SCALE)

TS SHT NO. 4

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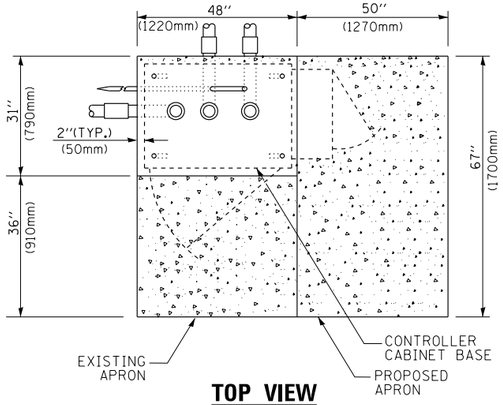
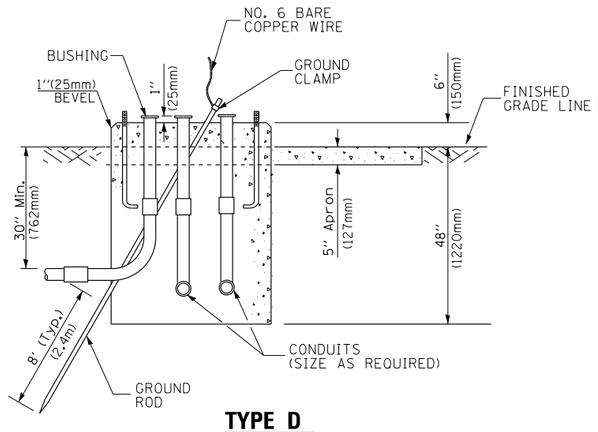
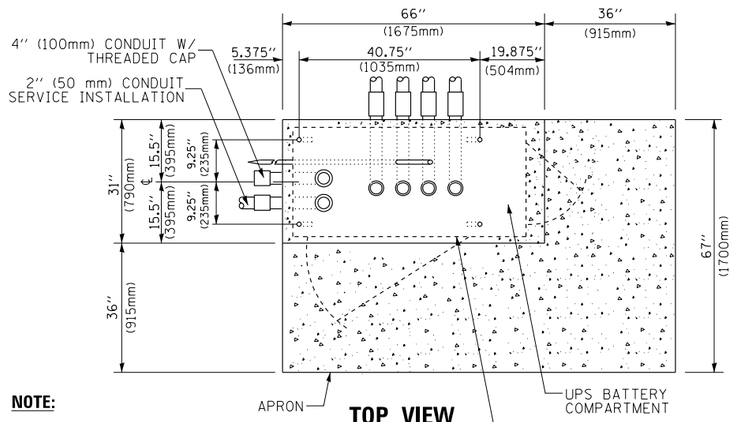
	PLOT SCALE = 100.0000 ' / in.	DRAWN -	REVISED -			330	464-B	COOK	97	33
Default	PLOT DATE = 5/17/2016	CHECKED -	REVISED -				TS–05	CONTRACT NO. 60V22		
		DATE -	REVISED -		SCALE: NONE SHEET 4 OF 7 SHEETS STA. TO STA.			ILLINOIS	FED. AID PROJECT	



TOP VIEW



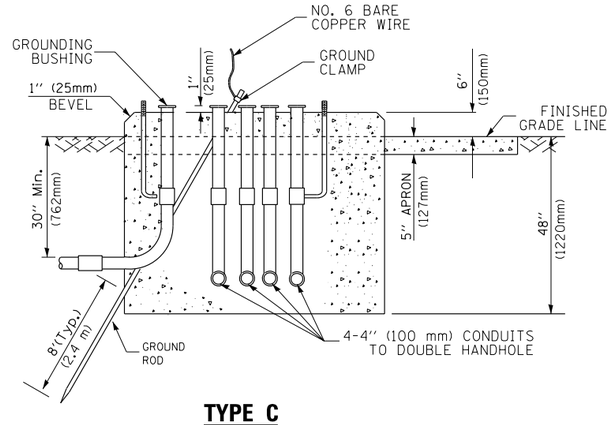
TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET



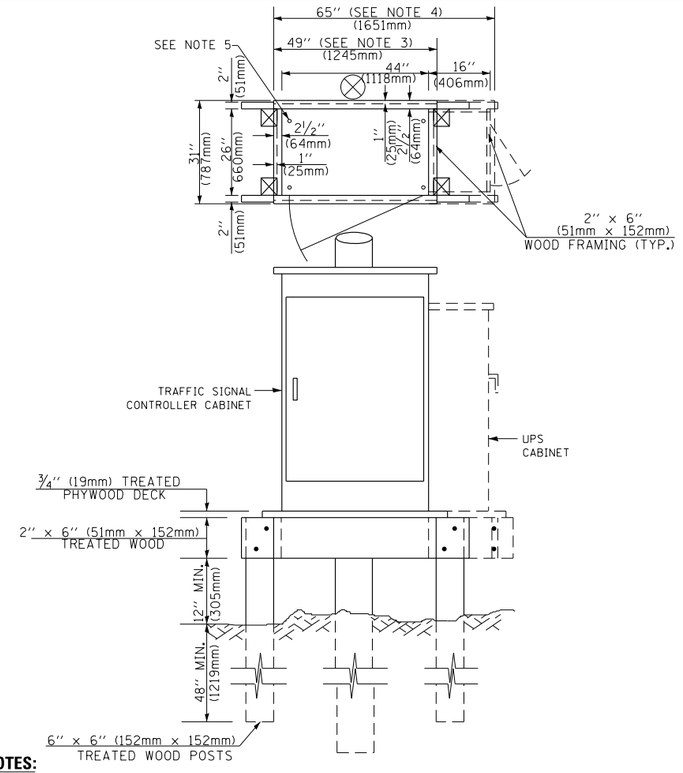
NOTE:

TOP OF FOUNDATION SHALL BE HIGHER THAN TOP OF DOUBLE HANDHOLE

TOP VIEW



TYPE C
FOR GROUND MOUNTED
SUPER P (TYPE IV) AND SUPER R (TYPE V)
CONTROLLER CABINETS



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26'' x 44'' (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16'' x 25'' (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION..

TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0'' (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0'' (1.2m)
TYPE D - CONTROLLER	4'-0'' (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0'' (1.2m)

DEPTH OF FOUNDATION

Mast Arm Length	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0'' (3.0 m)	30'' (750mm)	24'' (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6'' (4.1 m)	30'' (750mm)	24'' (600mm)	8	6(19)
	11'-0'' (3.4 m)	36'' (900mm)	30'' (750mm)	12	7(22)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	13'-0'' (4.0 m)	36'' (900mm)	30'' (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	15'-0'' (4.6 m)	36'' (900mm)	30'' (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0'' (6.4 m)	42'' (1060mm)	36'' (900mm)	16	8(25)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	25'-0'' (7.6 m)	42'' (1060mm)	36'' (900mm)	16	8(25)

NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36'' (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42'' (1060 mm) diameter foundations
4. For mast arm assemblies with dual arms refer to state standard 878001..

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

TS SHT NO. 5

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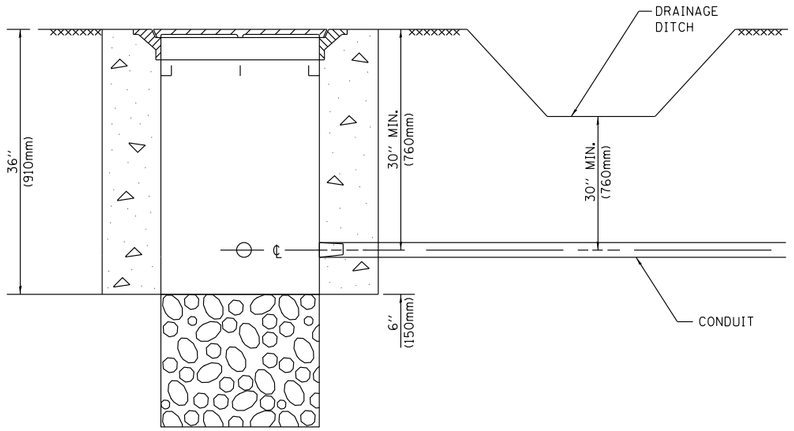
	PLOT DATE = 5/17/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET 5 OF 7 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	34
	TS-05	CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

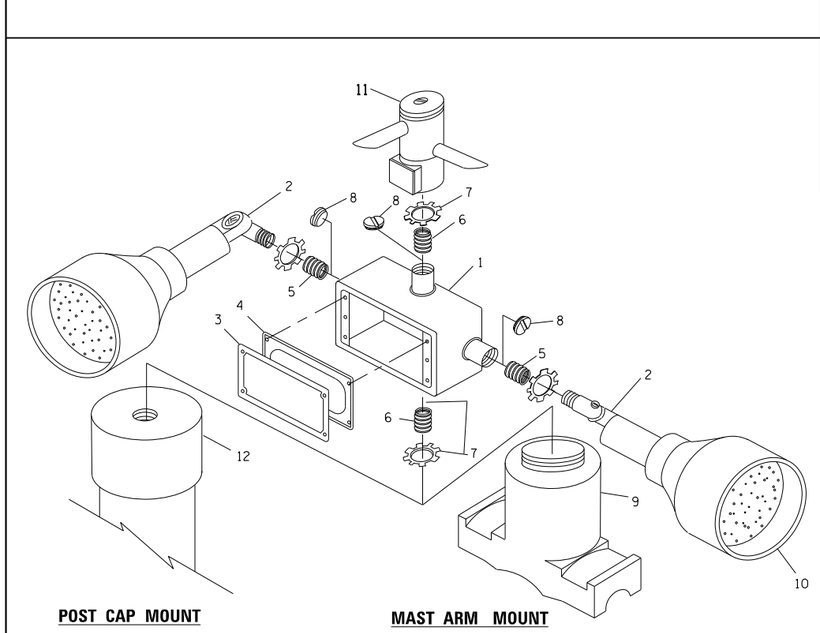


NOTES:

1. CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

HANDHOLE WITH MINIMUM CONDUIT DEPTH

(NOT TO SCALE)



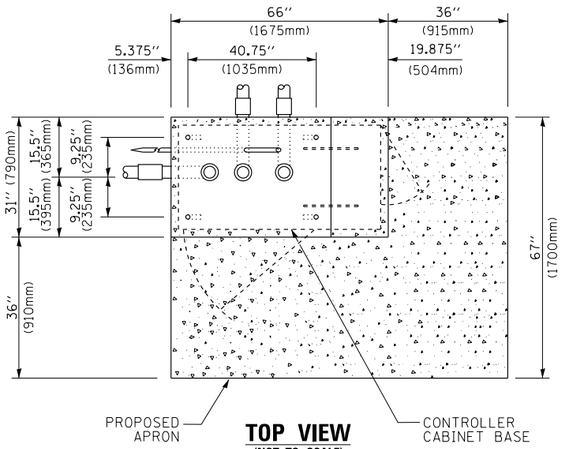
POST CAP MOUNT **MAST ARM MOUNT**

EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	¾"(19 mm) CLOSE NIPPLE
7	¾"(19 mm) LOCKNUT
8	¾"(19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

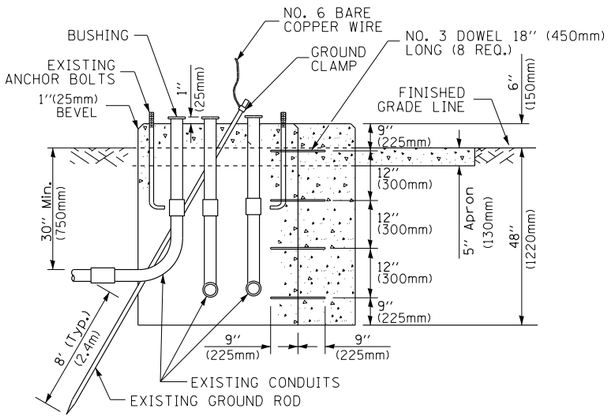
NOTES:

1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
   ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
   ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4 "(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



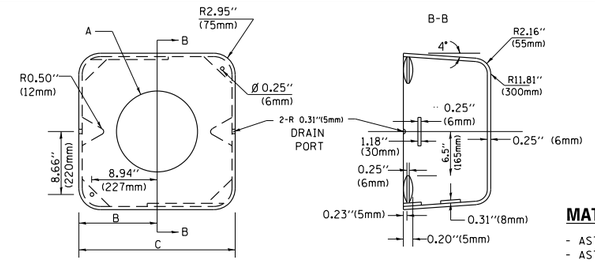
TOP VIEW

(NOT TO SCALE)



MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION

(NOT TO SCALE)



MATERIAL:

- ASTM A36 STEEL
- ASTM A-123 HOT DIPPED GALVANIZED

A	B	C	HEIGHT	WEIGHT
VARIES	9.5"(241mm)	19"(483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIES	10.75"(273mm)	21.5"(546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIES	13.0"(330mm)	26"(660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIES	18.5"(470mm)	37"(940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

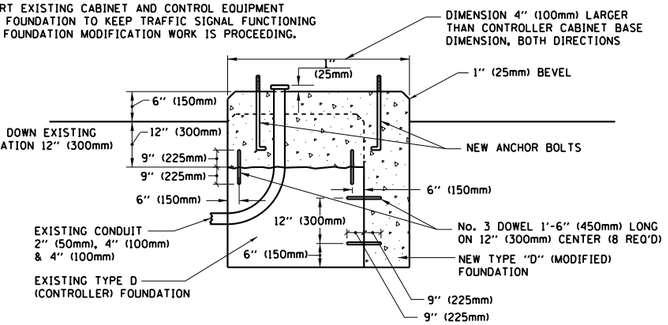
SHROUD

NOTES:

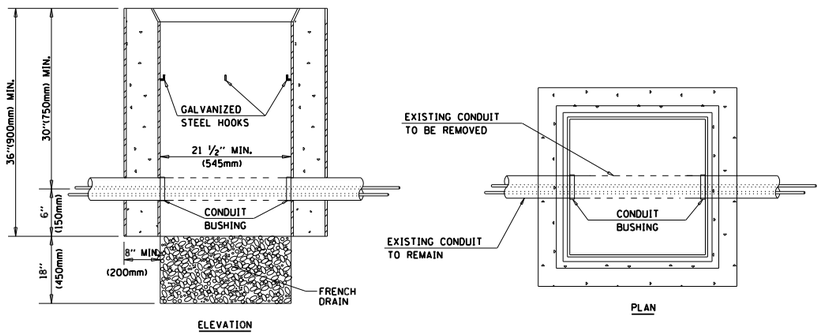
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION



NOTES:

1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

TS SHT NO. 6

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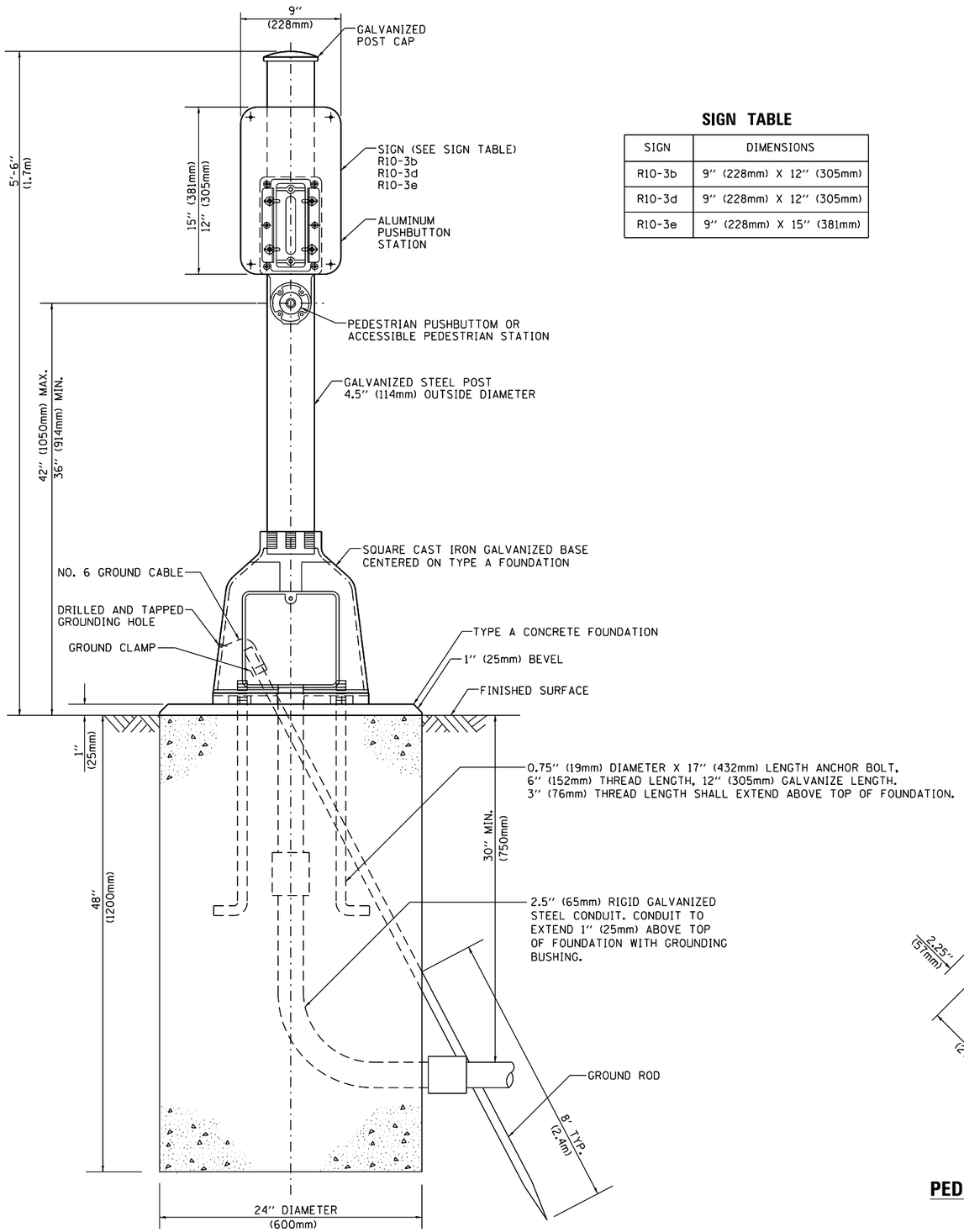
	PLOT DATE = 5/17/2016	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

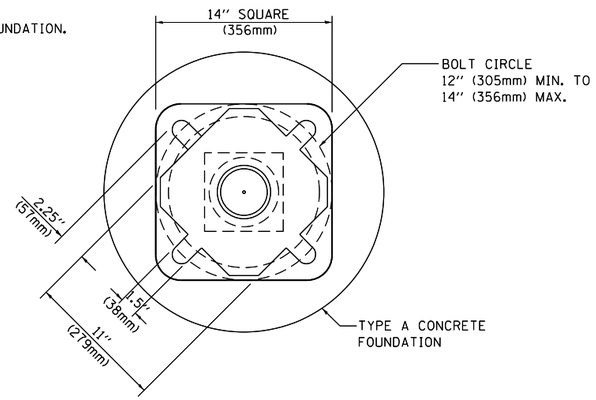
SCALE: NONE | SHEET 6 OF 7 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	35
	TS-05	CONTRACT NO. 60V22		
		ILLINOIS FED. AID PROJECT		



SIGN TABLE

SIGN	DIMENSIONS
R10-3b	9'' (228mm) X 12'' (305mm)
R10-3d	9'' (228mm) X 12'' (305mm)
R10-3e	9'' (228mm) X 15'' (381mm)



BOLT PATTERN

PEDESTRIAN PUSH BUTTON POST, TYPE A

TS SHT NO. 7

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Default	PLOT DATE = 5/17/2016	DATE -	REVISED -

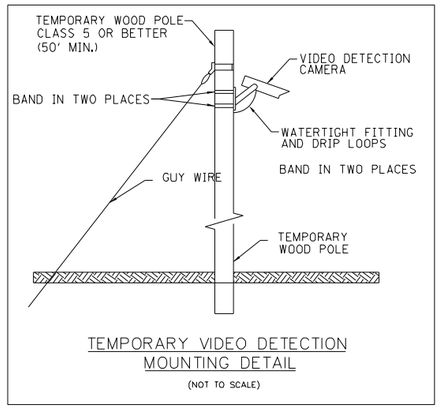
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET 7 OF 7 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	36
	TS-05	CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

TS SHT NO. 01



TEMPORARY VIDEO DETECTION
MOUNTING DETAIL

(NOT TO SCALE)

NOTE

SEE GENERAL NOTES NO. 1

TS 1010
ECON 38

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US12/45 (MANNHEIM RD) OVER ADDISON CREEK
TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN

SCALE: 1"=20' | SHEET 1 OF SHEETS | STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	37
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

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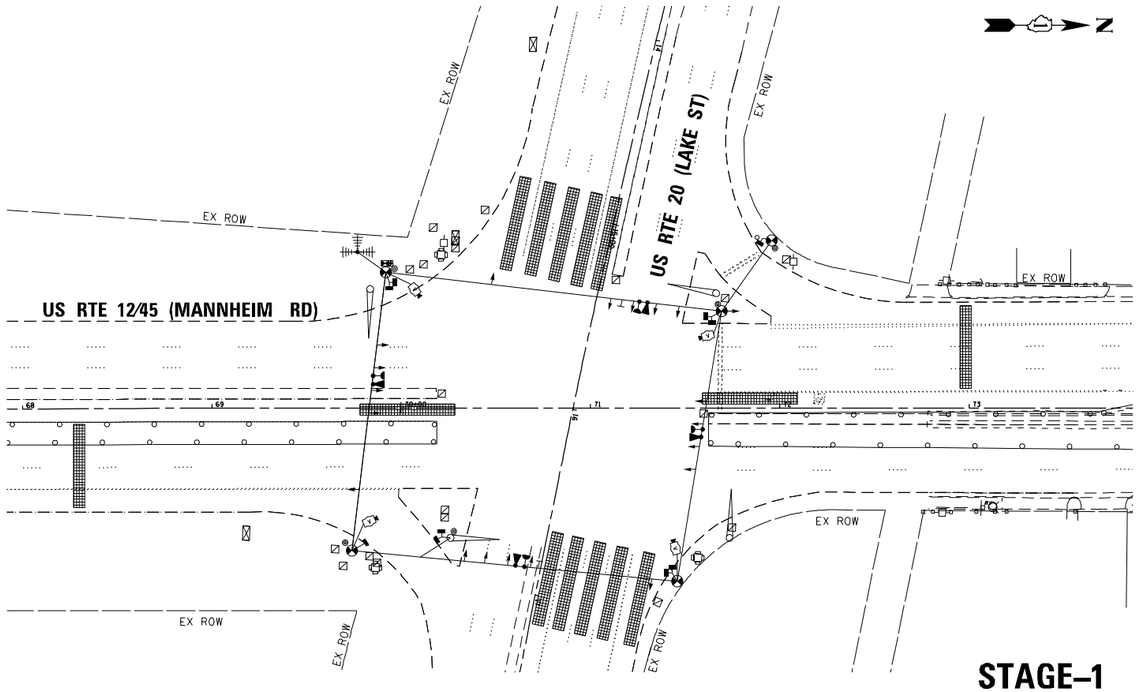
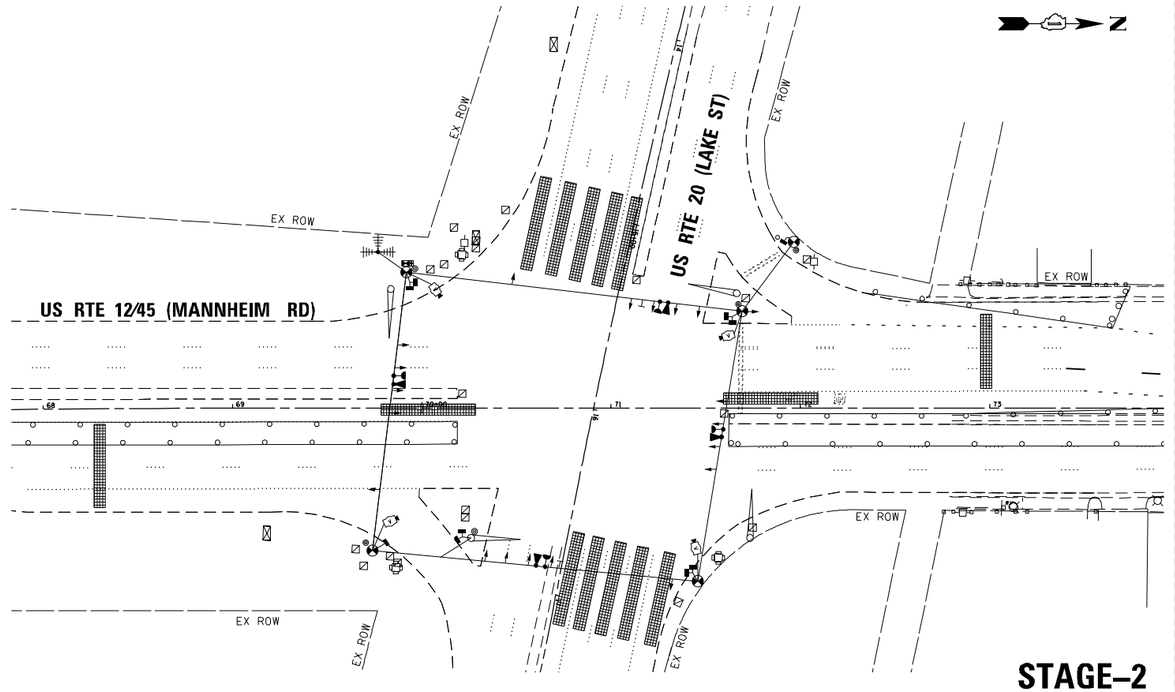
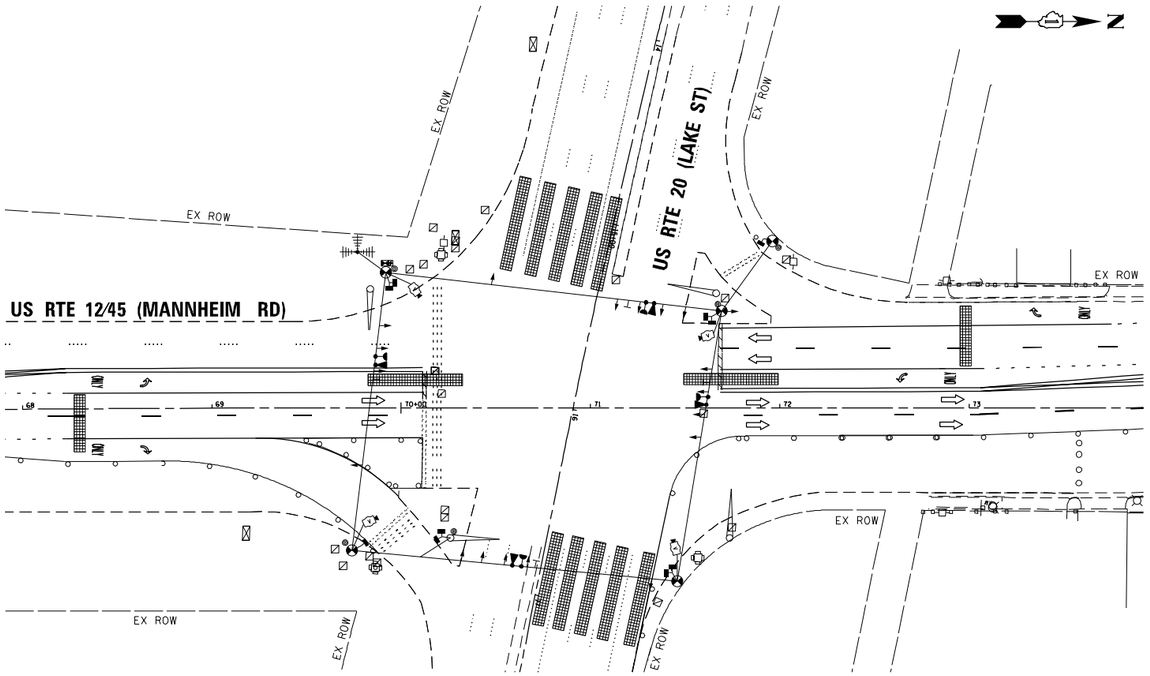
TS SHT NO. 02



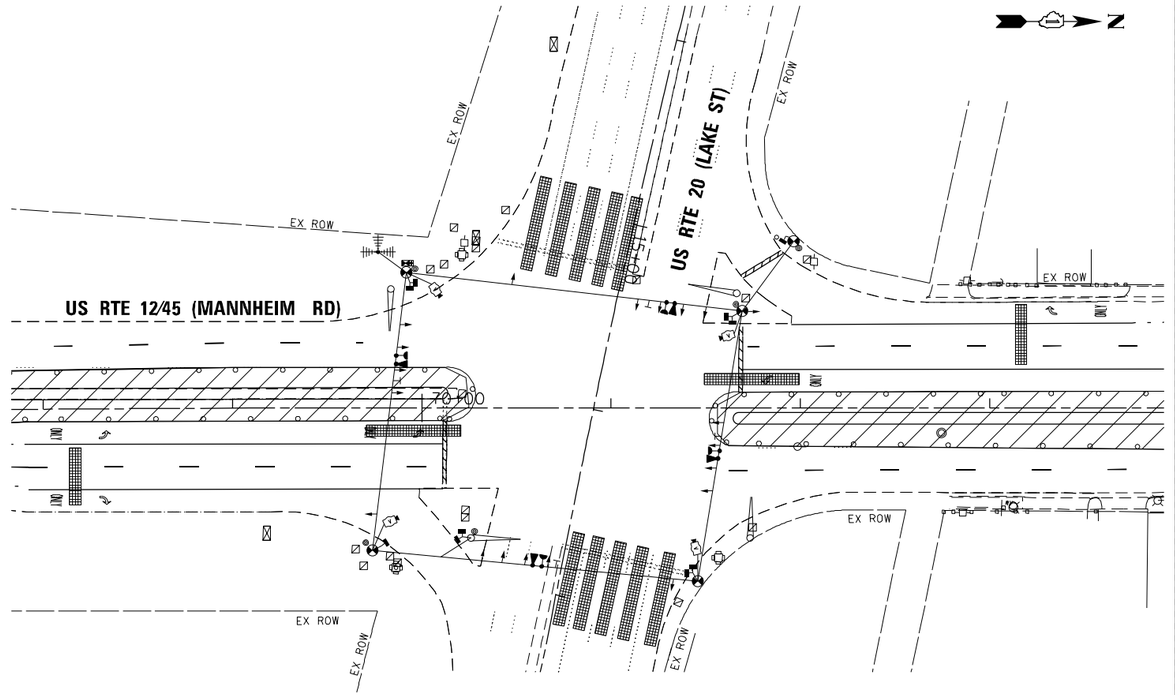
STAGE–1



STAGE–2



STAGE–3



STAGE–4

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USER NAME = Millennia Professional Services

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CHECKED - TVN	REVISED -	
DATE - 12/8/2017	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US12/45 (MANNHEIM RD) OVER ADDISON CREEK
TEMPORARY TRAFFIC SIGNAL STAGING PLAN

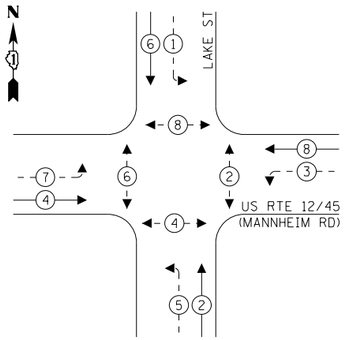
SCALE: 1"=40' | SHEET 2 OF 3 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	38
		CONTRACT NO. 60V22		
	ILLINOIS	FED. AID PROJECT		

P:\2011\ME11007_VarVar_PhII\CADD\W011_US12-45\Shts\D160V22-sht-St02_Temps_Staging.dgn

TS SHT NO. 03

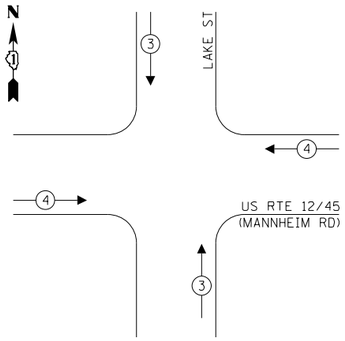
TEMPORARY CONTROLLER SEQUENCE



LEGEND:

- PROTECTED PHASE
- PROTECTED/PERMITTED PHASE
- PEDESTRIAN PHASE
- OL OVERLAP

TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE

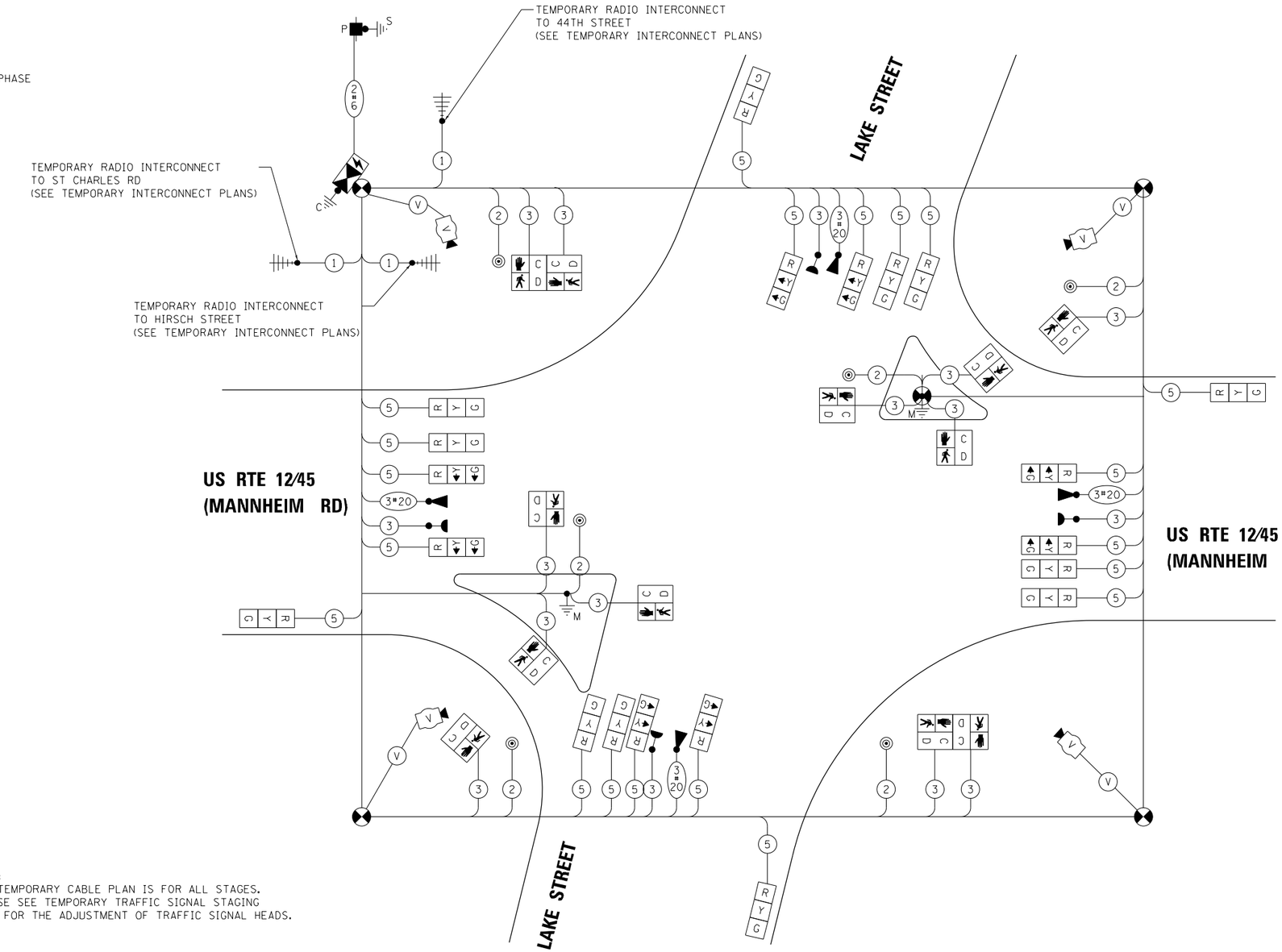


TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	20	11	50	110.0
(YELLOW)	12	20	5	12.0
(GREEN)	12	12	45	64.8
PERMISSIVE ARROW	16	10	10	16.0
PED. SIGNAL	12	20	100	240.0
CONTROLLER	1	100	100	100.0
UPS	1	25	100	25.0
VIDEO SYSTEM	1	150	100	150.0
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
			TOTAL =	717.8

ENERGY COSTS TO:

ENERGY SUPPLY: CONTACT: JOE STACHO
PHONE: (630) 424-5704
COMPANY: COMMONWEALTH EDISON
ACCOUNT NUMBER: ---



NOTE:
THE TEMPORARY CABLE PLAN IS FOR ALL STAGES.
PLEASE SEE TEMPORARY TRAFFIC SIGNAL STAGING PLAN FOR THE ADJUSTMENT OF TRAFFIC SIGNAL HEADS.

TEMPORARY CABLE PLAN

(NOT TO SCALE)

TS 1010
ECON 38

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US12/45 (MANNHEIM RD) OVER ADDISON CREEK
TEMPORARY TRAFFIC SIGNAL CABLE PLAN AND EMERGENCY VEHICLE PREEMPTION SEQUENCE

SCALE: N/A | SHEET OF SHEETS | STA. - TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	39
		CONTRACT NO. 60V22		
		ILLINOIS FED. AID PROJECT		

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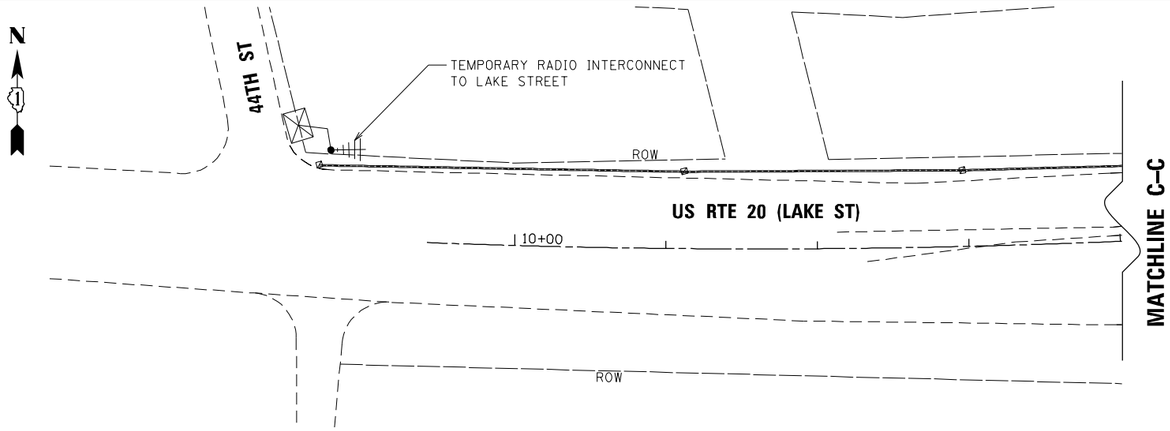

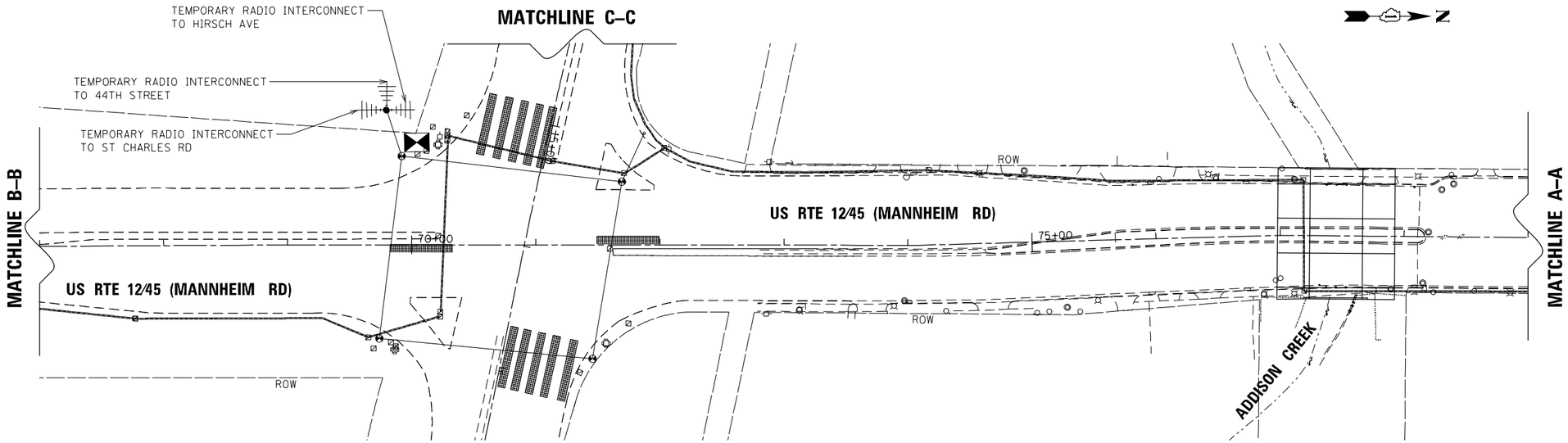

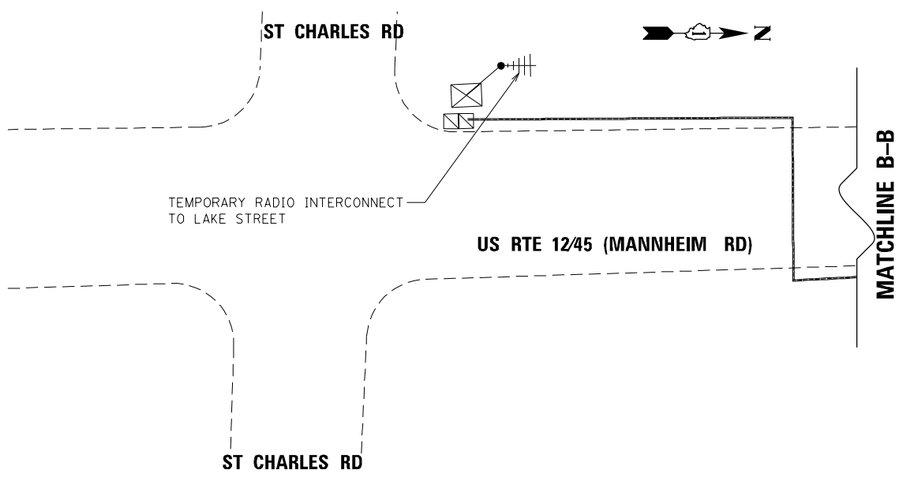

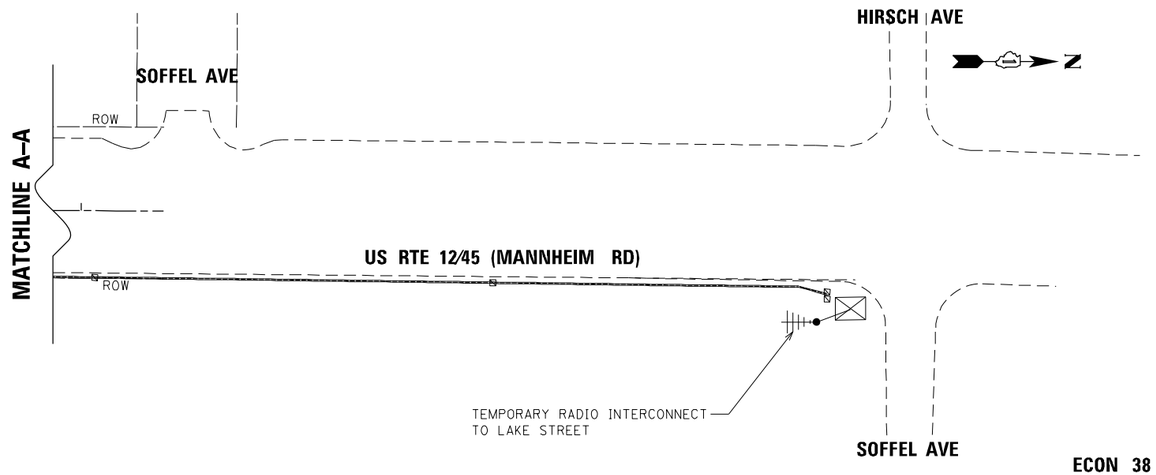

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CHECKED	TVN	REVISED	-
DATE	1/23/2018	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US12/45 (MANNHEIM RD) OVER ADDISON CREEK
TEMPORARY INTERCONNECTION PLAN (1 OF 1)

SCALE: · SHEET OF SHEETS STA. · TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	40
		CONTRACT NO. 60V22		
	ILLINOIS	FED. AID PROJECT		

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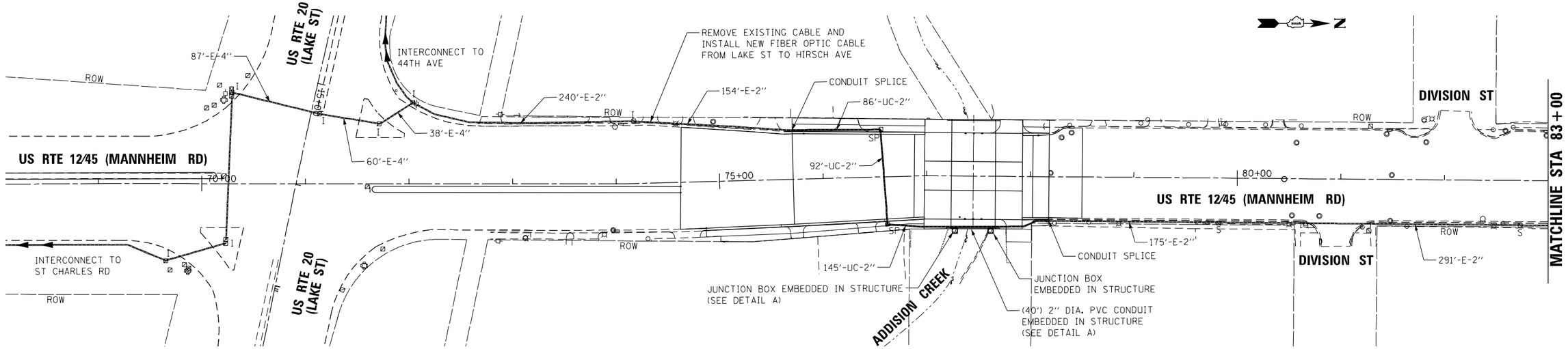

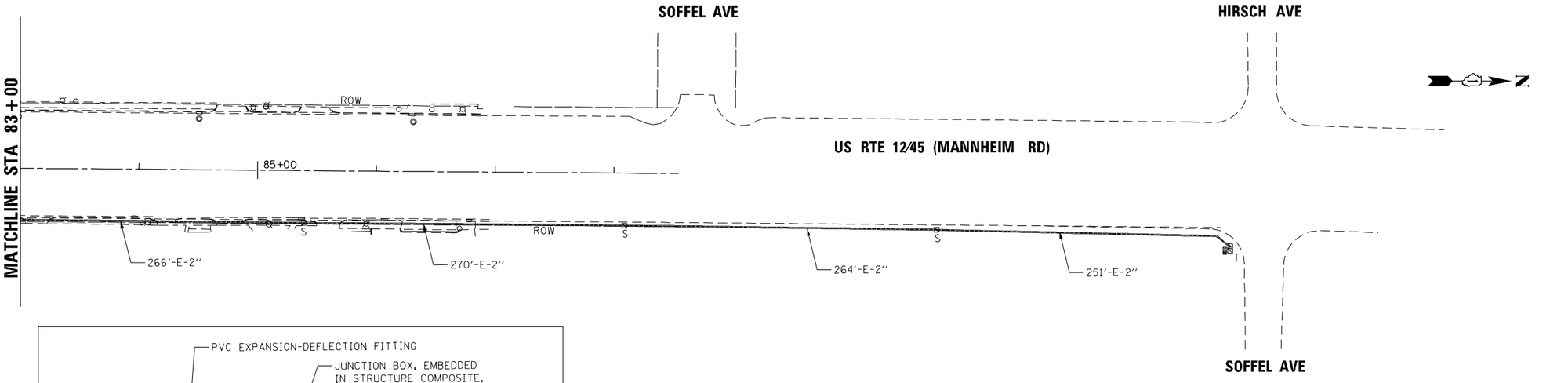

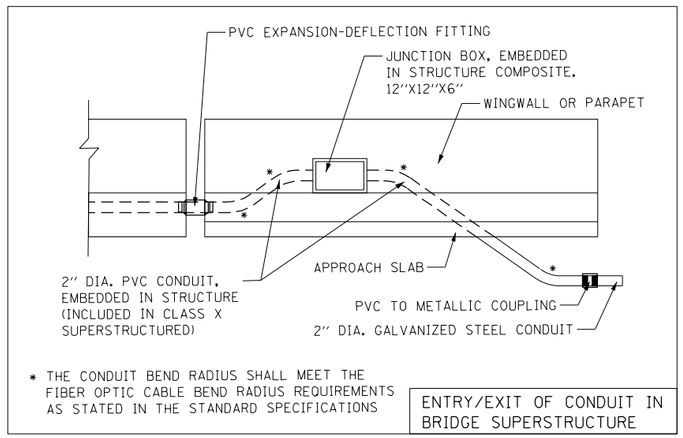

DETAIL A

ECON 38

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USER NAME = Millennia Professional Services

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DRAWN - JP	REVISED -	
CHECKED - TVN	REVISED -	
DATE - 12/8/2017	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US12/45 (MANNHEIM RD) OVER ADDISON CREEK
INTERCONNECTION PLAN

SCALE: · SHEET OF SHEETS STA. · TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	41
		CONTRACT NO. 60V22		
	ILLINOIS	FED. AID PROJECT		

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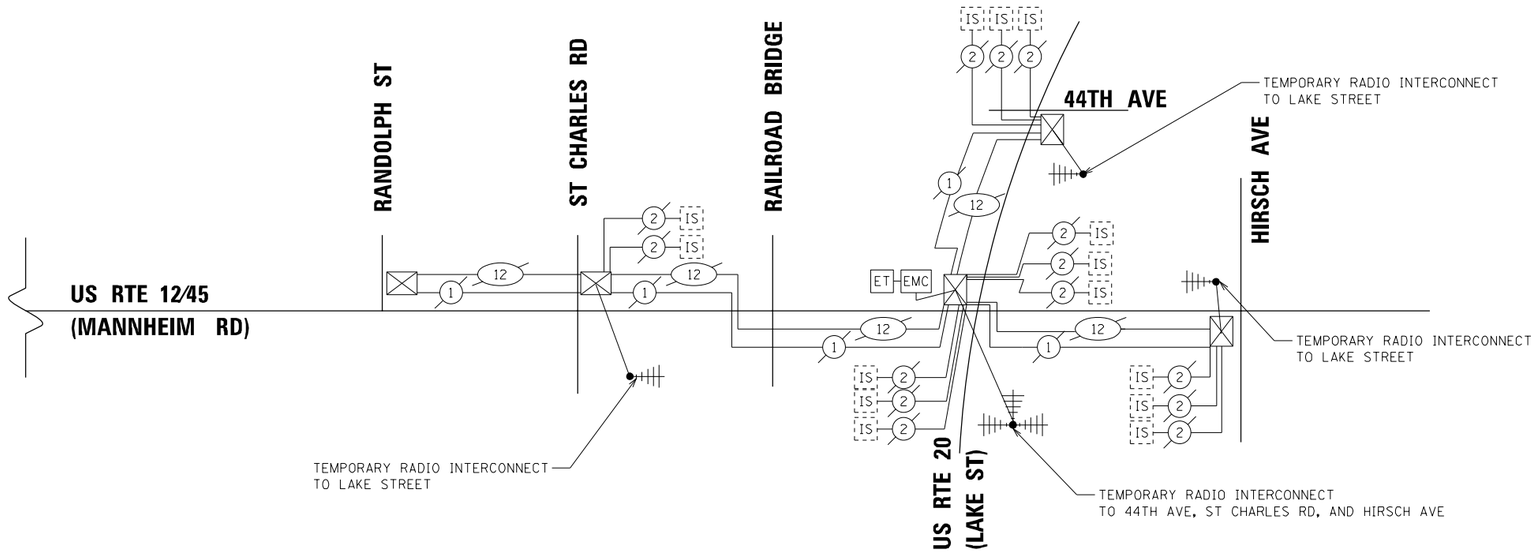
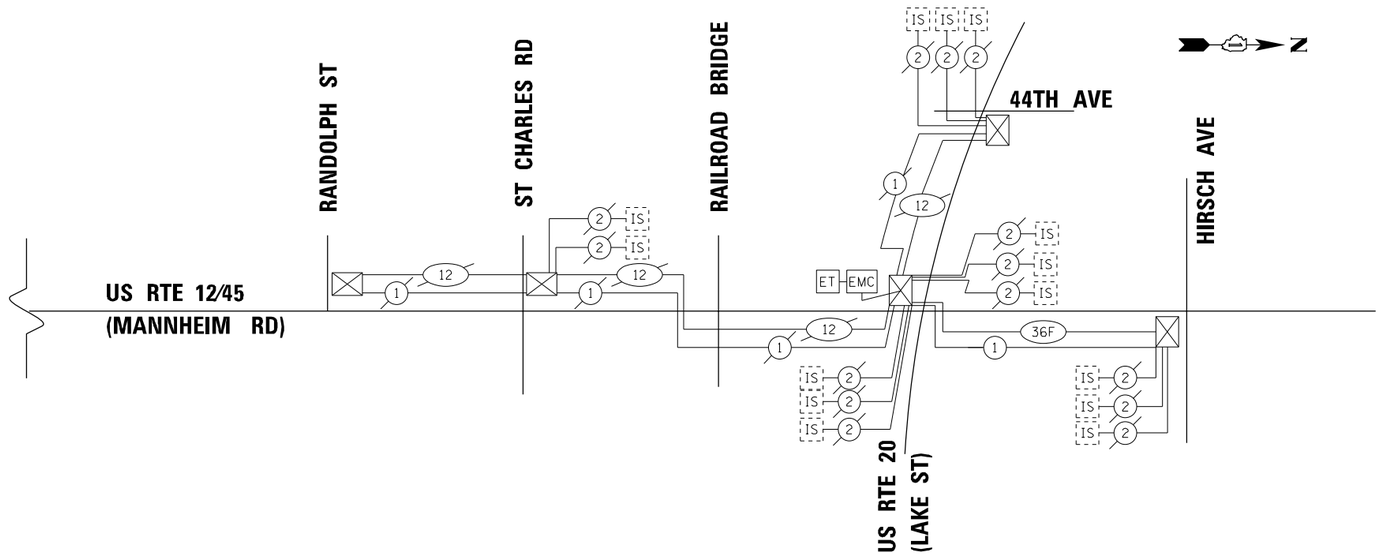


TEMPORARY SCHEMATIC

SCHEDULE OF QUANTITIES

	ITEM DESCRIPTION	UNITS	TOTAL QTY.
	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	323
	CONDUIT EMBEDDED IN STRUCTURE, 2" DIA., PVC	FOOT	40
	JUNCTION BOX, STAINLESS STEEL, EMBEDDED IN STRUCTURE, 12"X12"X6"	EACH	2
	HANDHOLE	EACH	2
	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	3
	TEMPORARY TRAFFIC SIGNAL INSTALLATION	L SUM	1
	ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	2419
	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	2096
	REMOVE EXISTING HANDHOLE	EACH	2
*	ROD AND CLEAN EXISTING CONDUIT	FOOT	500
	CONDUIT SPLICE	EACH	2
	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM24F	FOOT	2419
	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	3
	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1

* NOMINAL QUANTITY TO BE USED AS NEEDED AND AS APPROVED BY THE ENGINEER



PROPOSED SCHEMATIC

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CHECKED	TVN	REVISED	-
DATE	12/8/2017	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US12/45 (MANNHEIM RD) OVER ADDISON CREEK
TEMPORARY /PROPOSED SCHAMATIC AND SCHEDULE OF QUANTITIES

SCALE: · SHEET OF SHEETS STA. · TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	42
		CONTRACT NO. 60V22		
	ILLINOIS	FED. AID PROJECT		

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USER NAME = Millennia Professional Services

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GENERAL NOTES:

1. THE CONTRACTOR SHALL VERIFY ALL OF THE INFORMATION SHOWN ON THE CONTRACT DRAWINGS, WHICH WOULD AFFECT THE WORK UNDER THIS CONTRACT.

2. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ASCERTAIN EXISTING FIELD CONDITIONS BEFORE BIDDING ON THIS PROJECT, SPECIFICALLY AS THEY RELATE TO LUMP SUM ITEMS AND UNIT PRICE ITEMS.

3. ALL NEW CONDUITS, UNIT DUCTS, DIRECT BURIAL CABLES, AND APPURTENANCES ARE INDICATED DIAGRAMMATICALLY ON THE DRAWINGS. THE ACTUAL LOCATIONS IN THE FIELD SHALL MEET WITH APPROVAL OF THE ENGINEER.

4. THE ELECTRICAL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND ASSOCIATED SUPPLEMENTAL SPECIFICATIONS AND RECURRING PROVISIONS (LATEST EDITION).

5. THE SCALE SHOWN ON PLAN DRAWINGS APPLIES ONLY TO THE FULL SIZE PLANS AND NOT TO REDUCED SIZE PLANS.

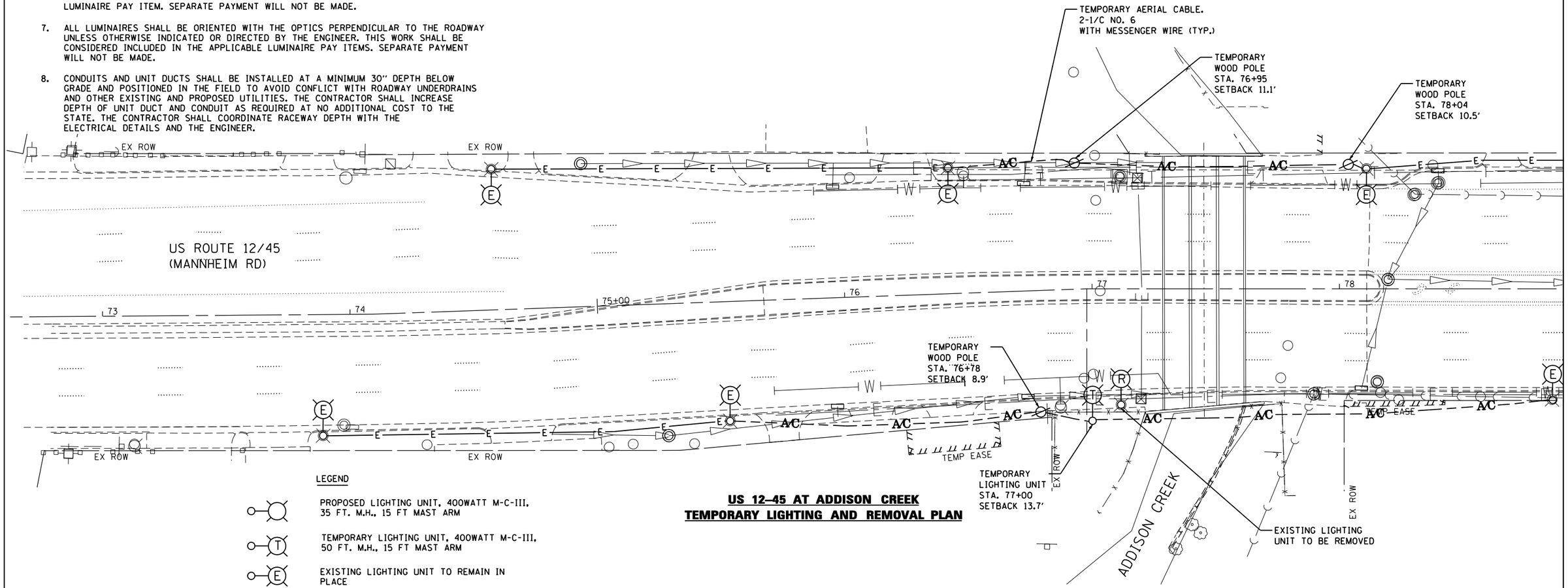
6. THE CONTRACTOR SHALL FURNISH AND INSTALL LUMINAIRE LAMPS IN ACCORDANCE WITH THE SUPPLIER'S RECOMMENDATIONS AND IN ACCORDANCE WITH THE SPECIFICATIONS. THE COST OF THIS WORK AND MATERIAL SHALL BE INCLUDED IN THE APPLICABLE LUMINAIRE PAY ITEM. SEPARATE PAYMENT WILL NOT BE MADE.

7. ALL LUMINAIRES SHALL BE ORIENTED WITH THE OPTICS PERPENDICULAR TO THE ROADWAY UNLESS OTHERWISE INDICATED OR DIRECTED BY THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE APPLICABLE LUMINAIRE PAY ITEMS. SEPARATE PAYMENT WILL NOT BE MADE.

8. CONDUITS AND UNIT DUCTS SHALL BE INSTALLED AT A MINIMUM 30" DEPTH BELOW GRADE AND POSITIONED IN THE FIELD TO AVOID CONFLICT WITH ROADWAY UNDERDRAINS AND OTHER EXISTING AND PROPOSED UTILITIES. THE CONTRACTOR SHALL INCREASE DEPTH OF UNIT DUCT AND CONDUIT AS REQUIRED AT NO ADDITIONAL COST TO THE STATE. THE CONTRACTOR SHALL COORDINATE RACEWAY DEPTH WITH THE ELECTRICAL DETAILS AND THE ENGINEER.

9. WHERE MULTIPLE CONDUITS ADJACENT TO EACH OTHER ARE INSTALLED IN A COMMON TRENCH, TRENCH AND BACKFILL WILL NOT BE PAID FOR EACH CONDUIT, BUT WILL BE PAID FOR THE LENGTH OF THE COMMON TRENCH ONLY.

10. WHERE THE CONTRACTOR'S EXCAVATION MEETS AN OBSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE ENGINEER FOR DIRECTION IN WRITING PRIOR TO EXCAVATION. THE CONTRACTOR SHALL RESTORE ANY DAMAGE TO EXISTING SYSTEMS OR UTILITIES AND REMOVE EXISTING OBSTRUCTIONS AND FOUNDATIONS TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE APPROPRIATE PAY ITEM.



US 12–45 AT ADDISON CREEK
TEMPORARY LIGHTING AND REMOVAL PLAN

LEGEND

- PROPOSED LIGHTING UNIT, 400WATT M-C-III, 35 FT. M.H., 15 FT MAST ARM
- TEMPORARY LIGHTING UNIT, 400WATT M-C-III, 50 FT. M.H., 15 FT MAST ARM
- EXISTING LIGHTING UNIT TO REMAIN IN PLACE
- EXISTING LIGHTING UNIT TO BE REMOVED
- TEMPORARY WOOD POLE, 60 FT. CLASS 4
- —A/C— TEMPORARY AERIAL CABLE. 2-1/C NO. 6 WITH MESSENGER WIRE
- ———E——— EXISTING UNDERGROUND WIRING TO REMAIN IN PLACE

FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - PKG	REVISED -
		DRAWN - EA	REVISED -
	PLOT SCALE = $SCALE$	CHECKED - PKG	REVISED -
	PLOT DATE = $DATE$	DATE - 3/15/2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US 12–45 AT ADDISON CREEK
TEMPORARY LIGHTING AND REMOVAL PLAN

SCALE: 1"=20' | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	43
		CONTRACT NO. 60V22		
FED. ROAD DIST. NO. -	ILLINOIS	FED. AID PROJECT		

GENERAL NOTES:

1. THE CONTRACTOR SHALL VERIFY ALL OF THE INFORMATION SHOWN ON THE CONTRACT DRAWINGS, WHICH WOULD AFFECT THE WORK UNDER THIS CONTRACT.

2. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ASCERTAIN EXISTING FIELD CONDITIONS BEFORE BIDDING ON THIS PROJECT, SPECIFICALLY AS THEY RELATE TO LUMP SUM ITEMS AND UNIT PRICE ITEMS.

3. ALL NEW CONDUITS, UNIT DUCTS, DIRECT BURIAL CABLES, AND APPURTENANCES ARE INDICATED DIAGRAMMATICALLY ON THE DRAWINGS. THE ACTUAL LOCATIONS IN THE FIELD SHALL MEET WITH APPROVAL OF THE ENGINEER.

4. THE ELECTRICAL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND ASSOCIATED SUPPLEMENTAL SPECIFICATIONS AND RECURRING PROVISIONS (LATEST EDITION).

5. THE SCALE SHOWN ON PLAN DRAWINGS APPLIES ONLY TO THE FULL SIZE PLANS AND NOT TO REDUCED SIZE PLANS.

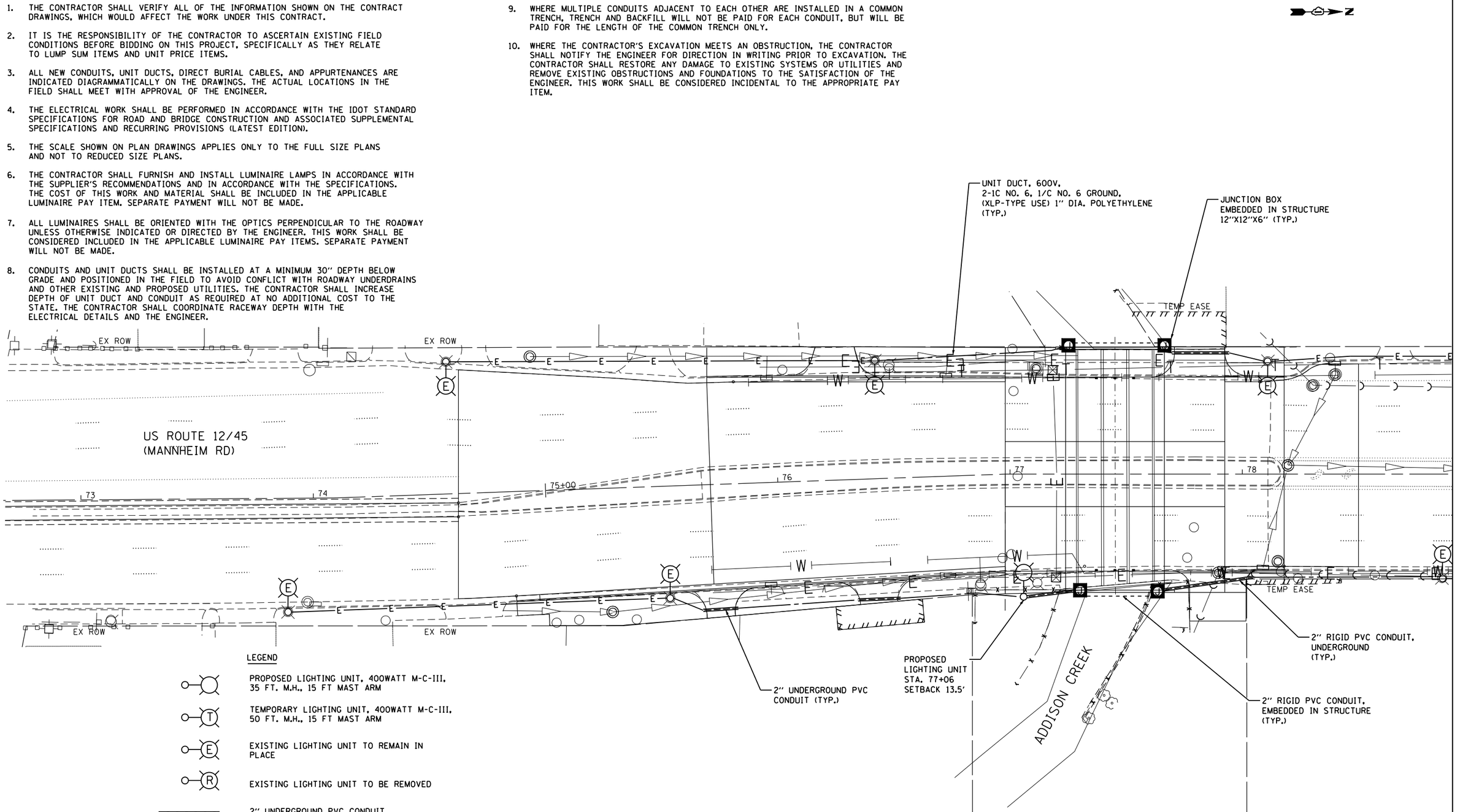
6. THE CONTRACTOR SHALL FURNISH AND INSTALL LUMINAIRE LAMPS IN ACCORDANCE WITH THE SUPPLIER'S RECOMMENDATIONS AND IN ACCORDANCE WITH THE SPECIFICATIONS. THE COST OF THIS WORK AND MATERIAL SHALL BE INCLUDED IN THE APPLICABLE LUMINAIRE PAY ITEM. SEPARATE PAYMENT WILL NOT BE MADE.

7. ALL LUMINAIRES SHALL BE ORIENTED WITH THE OPTICS PERPENDICULAR TO THE ROADWAY UNLESS OTHERWISE INDICATED OR DIRECTED BY THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE APPLICABLE LUMINAIRE PAY ITEMS. SEPARATE PAYMENT WILL NOT BE MADE.

8. CONDUITS AND UNIT DUCTS SHALL BE INSTALLED AT A MINIMUM 30" DEPTH BELOW GRADE AND POSITIONED IN THE FIELD TO AVOID CONFLICT WITH ROADWAY UNDERDRAINS AND OTHER EXISTING AND PROPOSED UTILITIES. THE CONTRACTOR SHALL INCREASE DEPTH OF UNIT DUCT AND CONDUIT AS REQUIRED AT NO ADDITIONAL COST TO THE STATE. THE CONTRACTOR SHALL COORDINATE RACEWAY DEPTH WITH THE ELECTRICAL DETAILS AND THE ENGINEER.

9. WHERE MULTIPLE CONDUITS ADJACENT TO EACH OTHER ARE INSTALLED IN A COMMON TRENCH, TRENCH AND BACKFILL WILL NOT BE PAID FOR EACH CONDUIT, BUT WILL BE PAID FOR THE LENGTH OF THE COMMON TRENCH ONLY.

10. WHERE THE CONTRACTOR'S EXCAVATION MEETS AN OBSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE ENGINEER FOR DIRECTION IN WRITING PRIOR TO EXCAVATION. THE CONTRACTOR SHALL RESTORE ANY DAMAGE TO EXISTING SYSTEMS OR UTILITIES AND REMOVE EXISTING OBSTRUCTIONS AND FOUNDATIONS TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE APPROPRIATE PAY ITEM.



LEGEND

- PROPOSED LIGHTING UNIT, 400WATT M-C-III, 35 FT. M.H., 15 FT MAST ARM
- TEMPORARY LIGHTING UNIT, 400WATT M-C-III, 50 FT. M.H., 15 FT MAST ARM
- EXISTING LIGHTING UNIT TO REMAIN IN PLACE
- EXISTING LIGHTING UNIT TO BE REMOVED
- 2" UNDERGROUND PVC CONDUIT
- 2" RIGID PVC CONDUIT, EMBEDDED IN STRUCTURE
- JUNCTION BOX EMBEDDED IN STRUCTURE, 12"X12"X6"
- UNIT DUCT, 600V, 2-1C NO. 6, 1/C NO. 6 GROUND, (XLP-TYPE USE) 1" DIA. POLYETHYLENE
- E — EXISTING UNDERGROUND WIRING TO REMAIN IN PLACE

US 12-45 AT ADDISON CREEK
PROPOSED LIGHTING PLAN

FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - PKG	REVISED -
		DRAWN - EA	REVISED -
	PLOT SCALE = $SCALE$	CHECKED - PKG	REVISED -
	PLOT DATE = $DATE$	DATE - 3/15/2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

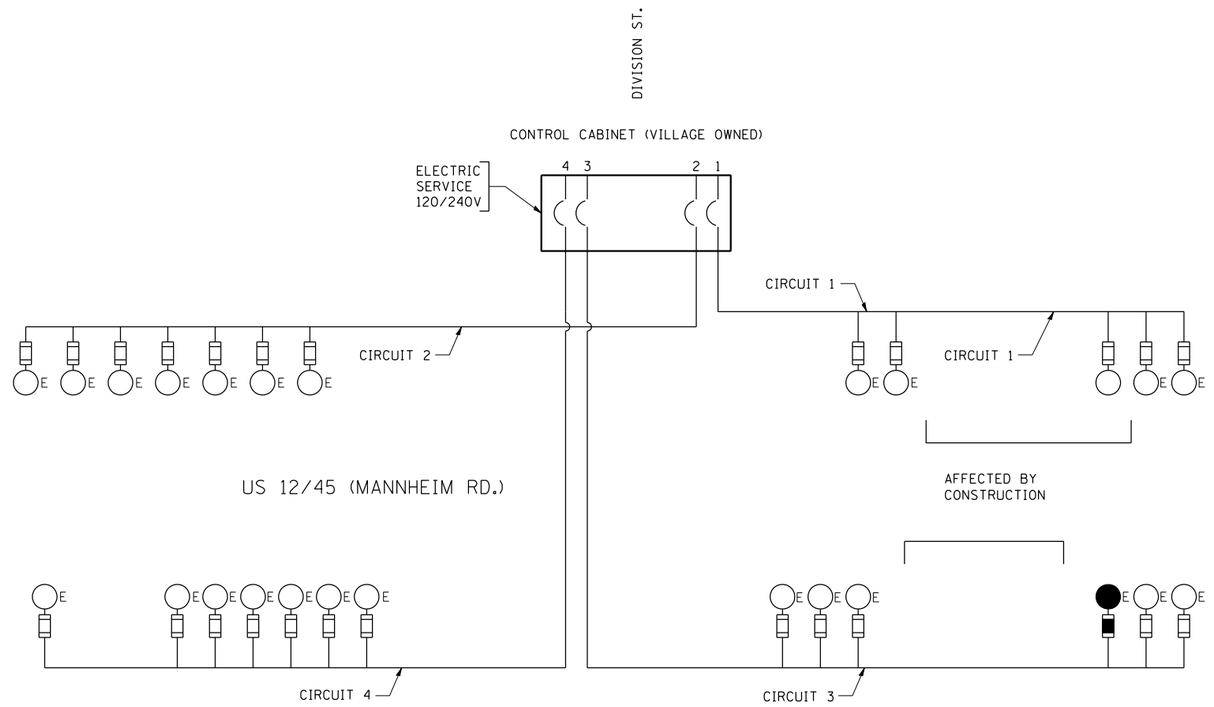
US 12-45 AT ADDISON CREEK
PROPOSED LIGHTING PLAN

SCALE: 1"=20' | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	44
		CONTRACT NO. 60V22		
FED. ROAD DIST. NO. -	ILLINOIS	FED. AID PROJECT		

LEGEND:

- 5A FUSE
- 3A FUSE
- CIRCUIT BREAKERS
- E EXISTING LUMINAIRE, 250W HPS
- E EXISTING LUMINAIRE, 400W HPS
- PROPOSED LUMINAIRE, 400W HPS



EXISTING SINGLE LINE DIAGRAM CONTROLLER

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		DRAWN - EA	REVISED -
	PLOT SCALE = $SCALE$	CHECKED - PKG	REVISED -
	PLOT DATE = $DATE$	DATE - 3/15/2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US 12–45 AT ADDISON CREEK
EXISTING SINGLE LINE DIAGRAM CONTROLLER

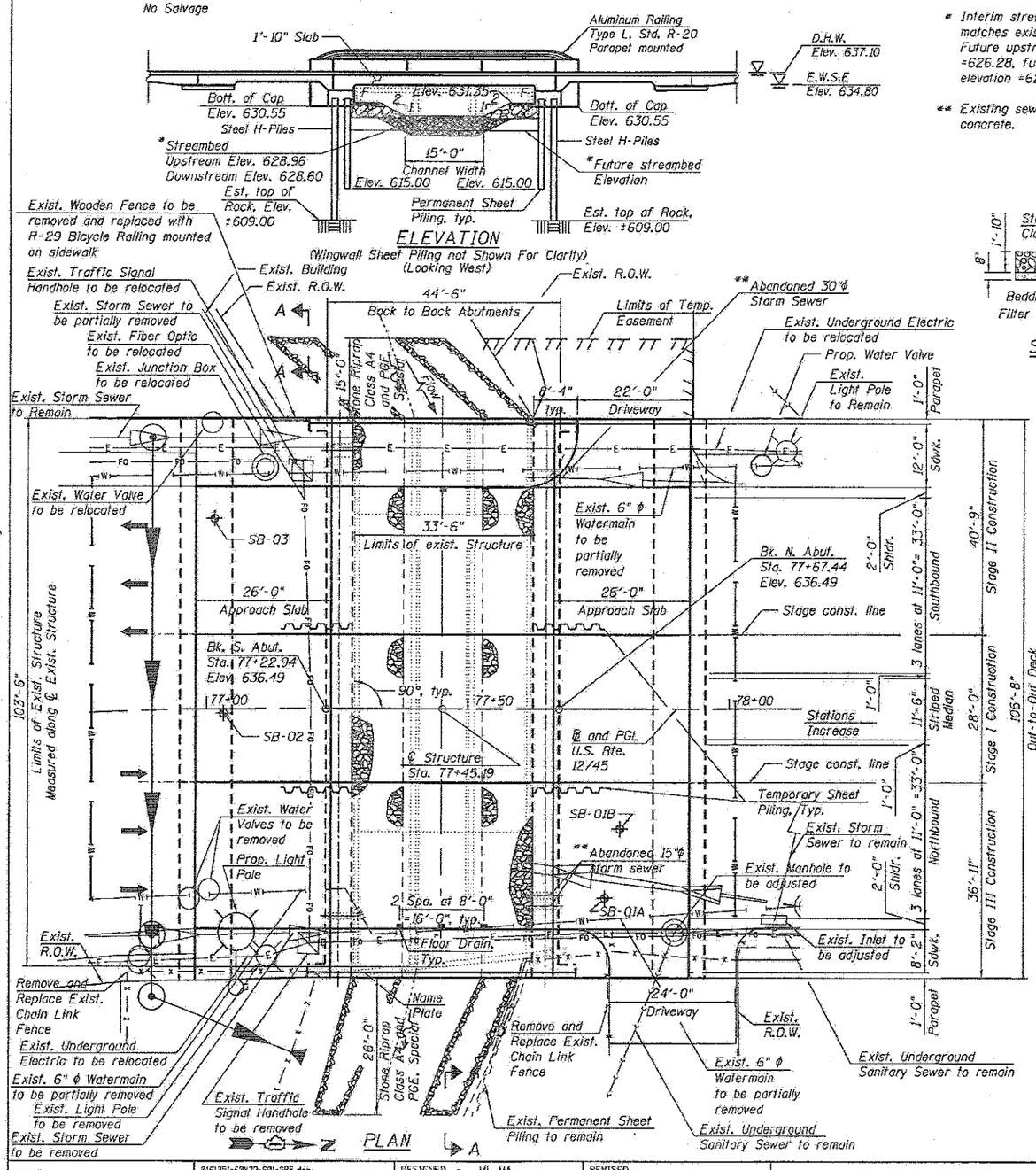
SCALE: NONE | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	45
		CONTRACT NO. 60V22		
FED. ROAD DIST. NO. -	ILLINOIS	FED. AID PROJECT		

Bench Mark: Found square cut in north part of east bridge parapet of Mannheim rd. bridge over Addison Creek. Elev. 637.02.

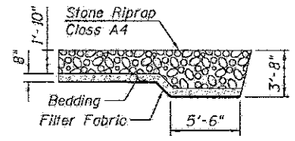
Existing Structure: S.N. 016-1036 originally constructed in 1924 as a cast-in-place box culvert three cells 10' wide and 4' high and 35'-4" length. The culvert was extended at an unknown date to be 60' long. A second culvert extension in 1964 resulted in a 94'-6" long culvert. In 1969, the center cell of the culvert was removed and dredged to achieve a final middle cell depth of 7'-3" vertical clearance. The culvert was extended to the east in 1974 to become 103'-6" long. Two lanes of traffic shall be maintained for each direction utilizing Stage Construction.

No Salvage

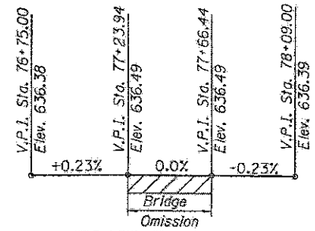


* Interim streambed elevation matches existing condition. Future upstream elevation =626.28, future downstream elevation =626.13 per MWRD.

** Existing sewer is plugged with concrete.



SECTION A-A



PROFILE GRADE
(Along ℄ US Rte. 12/45)

WATERWAY INFORMATION

Drainage Area = 8.31 sq. mi. Low Grade Elev. = 635.06 at Sta. 83+30 Max. Recorded H.W.E. = 637.33

Flood	Freq. Yr.	Discharge - C.F.S. Exist.	Discharge - C.F.S. Prop.	Opening Sq. Ft. Exist.	Opening Sq. Ft. Prop.	Nat. H.W.E.	Head - Ft. Exist.	Head - Ft. Prop.	Headwater El. Exist.	Headwater El. Prop.
	10	494	494	117	143	635.9	1.1	0.3	637.0	636.2
Design	50	774	774	117	143	637.1	1.1	0.4	638.2	637.5
Base	100	863	863	117	143	637.4	1.1	0.4	638.5	637.8
Overtopping										
Max. Calc.	500	1060	1060	117	143	638.2	0.9	0.3	639.1	638.5

10-year velocity through the existing structure = 5.2 fps. 10-year velocity through the proposed structure = 3.6 fps.
2 Year Peak Flow (Q) = 284 C.F.S. Estimated Water Surface Elevation = 634.8

DESIGN STRESSES

FIELD UNITS

f'_c = 3,500 psi
f'_c = 4,000 psi (Superstructure Concrete)
f_y = 60,000 psi (Reinforcement)
f_y = 50,000 psi (AASHTO M270 Grade 50)

SEISMIC DATA

Seismic Performance Zone (SPZ) = 1
Design Spectral Acceleration at 1.0 sec. (SD1) = 0.061g
Design Spectral Acceleration at 0.2 sec. (SDS) = 0.113g
Soil Site Class = C

DESIGN SPECIFICATIONS

2014 AASHTO LRFD Bridge Design Specifications,
7th Edition with 2015 and 2016 Interim Revisions.

LOADING HL-93

Allow 50 lb/sq. ft. for future wearing surface.

DESIGN SCOUR ELEVATION TABLE

Event / Limit State	Design Scour Elevations (ft.) S. Abut.	Design Scour Elevations (ft.) N. Abut.	Item 113
0.100	617.40	617.90	5
0.200	616.30	616.90	
Design	617.40	617.90	
Check	616.30	616.90	

LEGEND

- Soil Boring location
- Exist. Water Main
- Exist. Light Pole
- Exist. Underground Electric
- Exist. Water Valve
- Exist. Telephone Line
- Exist. Telephone Junction Box
- Exist. Traffic Signal Handhole
- Exist. Manhole
- Exist. Permanent Steel Sheet Piling
- Exist. Fiber Optic
- Exist. Storm Sewer
- Prop. Storm Sewer
- Prop. Water Main
- Prop. Water Valve
- Prop. Manhole
- Prop. End Section

NOTE:

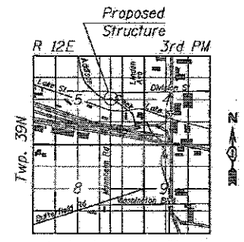
1. Layout of riprap may be varied to suit the ground in the field as directed by the Engineer.

APPROVED
For Structural Adequacy Only
Engineer of Bridges & Structures

STATE OF ILLINOIS — MOUSSA A. ISSA — 081-005738 — CHICAGO, ILLINOIS — LICENSED STRUCTURAL ENGINEER

Signed Moussa A. Issa
Moussa A. Issa, HBM Il. Lic. No. 081-005738
Expires 11-30-2018

Date 12/08/2017 For Sheets S-01 Thru S-30 (Total of 30 Sheets)



LOCATION SKETCH

GENERAL PLAN AND ELEVATION
U.S. RTE. 12/45
(MANNHEIM ROAD) OVER
ADDISON CREEK
F.A.P. RTE. 330 - SEC. 464-B
COOK COUNTY
STATION 77+45.19
STRUCTURE NO. 016-1351

FILE PATH = P:\111-532 IDOT PTB161 Item 8 Various-Various\Work Order #1 - US 12 over Addison Creek Culvert\Structural\Sheets\161351-60V22-S01-GPE.dgn	DESIGNED - MI, MA	REVISED	
	USER NAME = lisaburton	DRAWN - MA, KJD	REVISED
	PLOT SCALE = 24.00 ' / in.	CHECKED - LAB, MI	REVISED
	PLOT DATE = 12/8/2017	DATE - 12/08/2017	REVISED

HBM ENGINEERING GROUP, LLC — CONSULTING & DESIGN, INSPECTION & RATING, RESEARCH & TESTING — 4415 WEST HARRISON ST. SUITE 231 HILLSIDE, IL 60162 PHONE (708) 236-0900 FAX (708) 236-0901

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE: | SHEET S-01 OF S-30 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	46
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

GENERAL NOTES

1. Layout of the slope protection system may be varied to suit ground conditions in the field as directed by the Engineer.

2. Reinforcement bars designated (E) shall be epoxy coated.

3. The Contractor shall make allowance for the deflection of forms, shrinkage and settlement of falsework, in addition to allowance for dead load deflection. Forms for deck slab shall be removed prior to placement of bridge approach slab.

4. The Contractor shall exercise extreme caution during construction to make certain that construction activities, live load surcharge, structure excavation, driving piles and other loads applied will not have detrimental effects on adjacent structures, utilities, and the existing north east sheet piling. Any damage to adjacent structures, utilities, and the existing north east sheet piling during construction shall be repaired by the contractor at his expense at no charge to IDOT.

5. Hard driving of permanent sheet piling in hardpan clay may be encountered below elevation 619.00. The Contractor shall provide the appropriate driving equipment for such.

INDEX OF SHEETS

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER.	SUB.	TOTAL QUANTITY
Stone Riprap, Class A4	Sq Yd	-	329	329
Filter Fabric	Sq Yd	-	329	329
Removal of Existing Structures	Each	1	-	1
Structure Excavation	Cu Yd	-	478	478
Floor Drains	Each	6	-	6
Concrete Structures	Cu Yd	-	187.3	187.3
Concrete Superstructure	Cu Yd	382.5	-	382.5
Bridge Deck Grooving	Sq Yd	912	-	912
Protective Coat	Sq Yd	1,173	-	1,173
Concrete Superstructure (Approach Slab)	Cu Yd	259.0	-	259.0
Stud Shear Connectors	Each	608	-	608
Reinforcement Bars, Epoxy Coated	Pound	203,885	30,695	234,580
Bar Splicers	Each	728	224	952
Bicycle Railing	Foot	7	-	7
Aluminum Railing, Type L	Foot	103	-	103
Furnishing Steel Piles HP14x73	Foot	-	624	624
Driving Piles	Foot	-	624	624
Test Pile Steel HP14x73	Each	-	2	2
Pile Shoes	Each	-	28	28
Name Plates	Each	1	-	1
Temporary Sheet Piling	Sq Ft	-	650	650
Permanent Steel Sheet Piling	Sq Ft	-	4,020	4,020
Geocomposite Wall Drain	Sq Yd	-	76	76
Porous Granular Embankment, Special	Cu Yd	-	398	398
Granular Backfill For Structures	Cu Yd	-	164	164
Pipe Underdrains For Structures 4"	Foot	-	232	232

CONSTRUCTION SEQUENCE FOR EACH STAGE CONSTRUCTION

1. Install temporary Concrete barriers and Temporary sheet piling.

2. Perform pavement removal and Structure Excavation for the approach slab and the abutments construction.

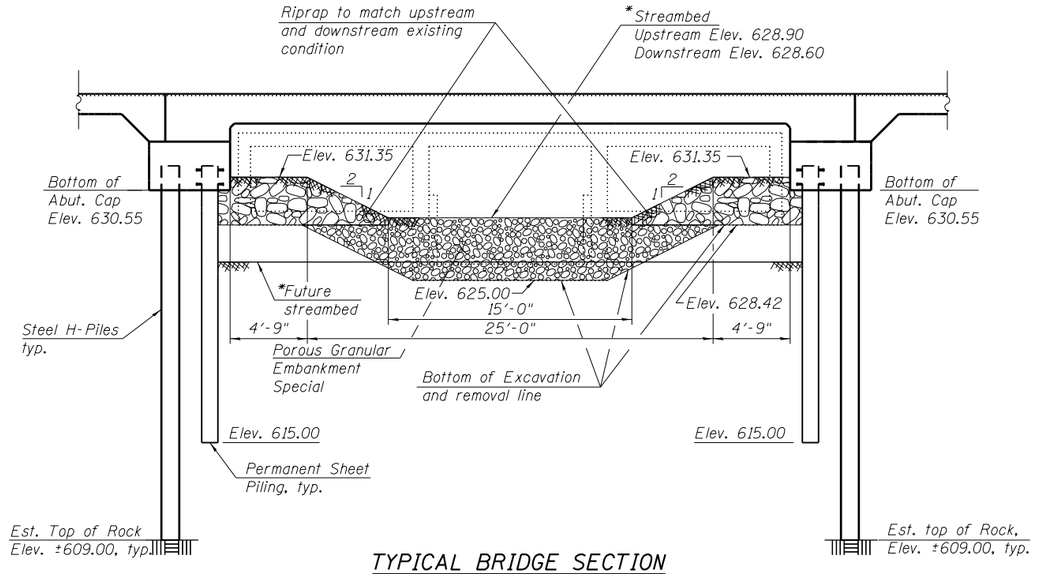
3. Install new permanent sheet piling and H-piles for the abutments.

4. Install shear studs, forms and reinforcement and construct concrete Abutments.

5. Remove the existing portions of pavement and structure within limits of the current stage construction. See Civil plans.

6. Perform Channel Excavation, Install Porous Granular Embankment, Special and Stone Riprap.

7. Construct deck slab and approach slabs.



TYPICAL BRIDGE SECTION

* Interim streambed elevation matches existing conditions. Future upstream elevation = 626.28, future downstream elevation = 626.13 per MWRD.

STATION 77+45.19
BUILT 201- BY
STATE OF ILLINOIS
F.A. RT. 330 SEC. 464-B
LOADING HL-93
STR. NO. 016-1351

NAME PLATE

See Std. 515001

Locate Name Plate at the Front Face of East Parapet on the South Approach

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Porous Granular Embankment, Special	Cu Yd	398
Stone Riprap, Class A4	Sq Yd	329
Filter Fabric	Sq Yd	329

FILE PATH = P:\1111-532 IDOT FTB161 Item 8 (Various-Various)\Work Order #11 - US 12 over Addison Creek Culvert\Structural\Sheets\0161351-60V22-S02-Notes, Index & BOM.dgn

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RESEARCH & TESTING
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HILLSIDE, IL 60162
PHONE: (708) 236-0900
FAX: (708) 236-0901

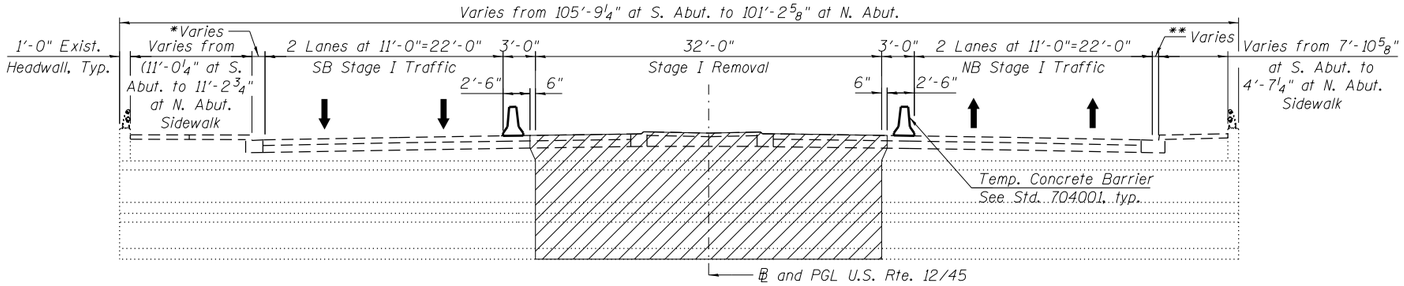
0161351-60V22-S02-Notes, Index & BOM.dgn	DESIGNED - MI, KJD, SK	REVISED
USER NAME = lisa.buntin	DRAWN - KJD, SK	REVISED
PLOT SCALE = 14.00 ' / in.	CHECKED - LAB, MI	REVISED
PLOT DATE = 1/25/2018	DATE - 12/08/2017	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES, INDEX OF SHEETS AND TOTAL BILL OF MATERIAL
STRUCTURE NO. 016-1351

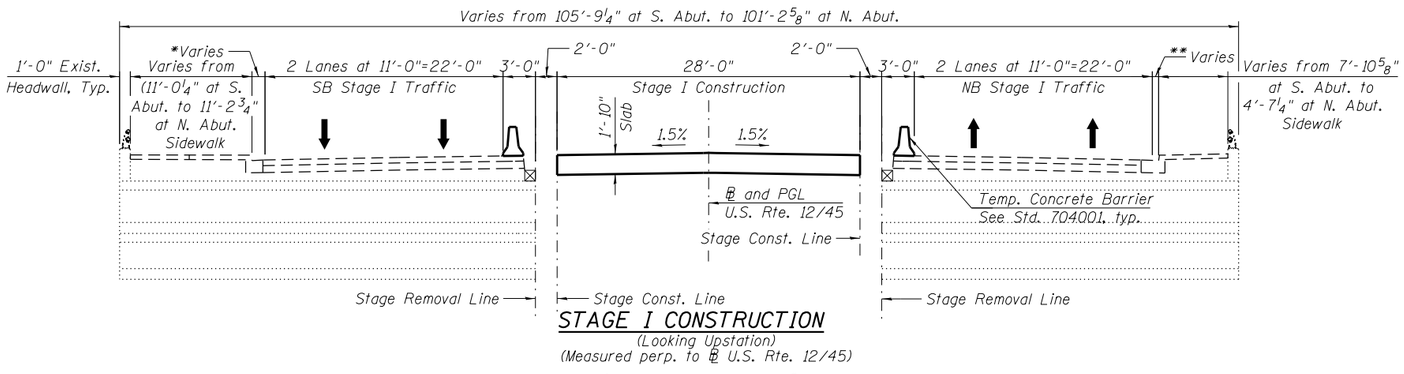
SCALE: | SHEET S-02 OF S-30 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	47
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			



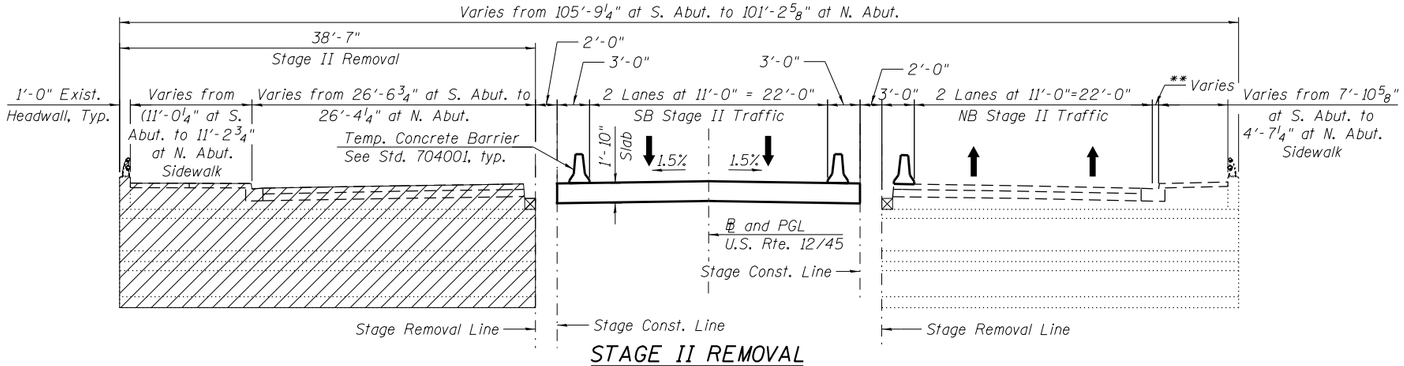
STAGE I REMOVAL

(Looking Upstation)
(Measured perp. to ℄ U.S. Rte 12/45)



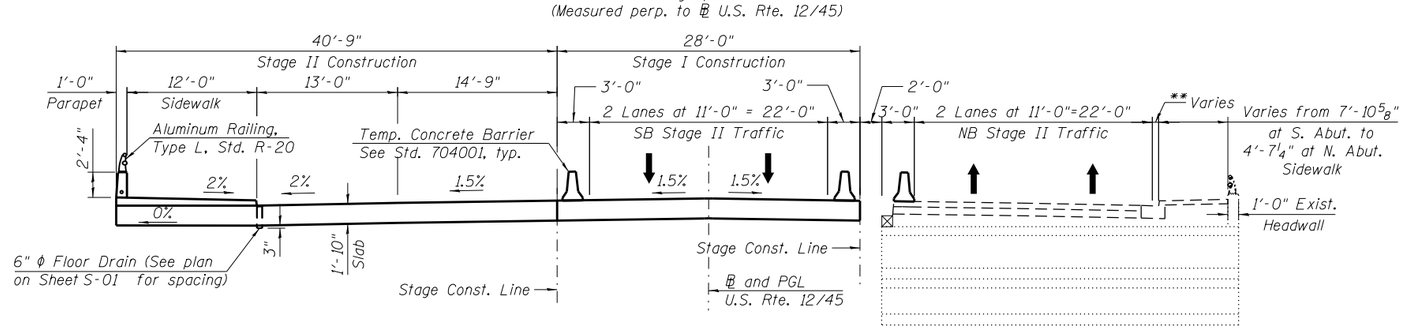
STAGE I CONSTRUCTION

(Looking Upstation)
(Measured perp. to ℄ U.S. Rte. 12/45)



STAGE II REMOVAL

(Looking Upstation)
(Measured perp. to ℄ U.S. Rte. 12/45)



STAGE II CONSTRUCTION

(Looking Upstation)
(Measured perp. to ℄ U.S. Rte. 12/45)

* Varies from 1'-6 3/4" at S. Abut. to 1'-4 1/4" at N. Abut.

** Varies from 1'-3 5/8" at S. Abut. to 0 1/2" at N. Abut.

LEGEND

- Removal of existing structure

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RESEARCH & TESTING
4415 WEST HARRISON ST. SUITE 231
HILLSIDE, IL 60162
PHONE: (708) 236-0900
FAX: (708) 236-0901

0161351-60V22-S03-Stage I & II Const.dgn	DESIGNED - MI, MA	REVISED
USER NAME = Stoyanka.Kotorakova	DRAWN - MA, KJD	REVISED
PLOT SCALE = 14.0 '/ / in.	CHECKED - LAB, MI	REVISED
PLOT DATE = 1/16/2018	DATE - 12/08/2017	REVISED

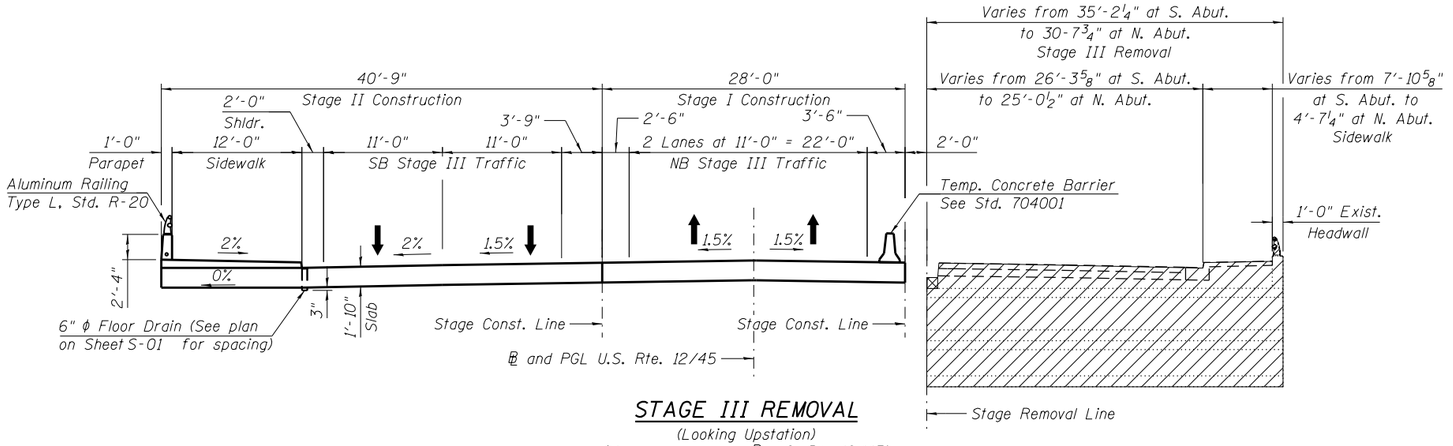
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGE I AND STAGE II REMOVAL AND CONSTRUCTION
STRUCTURE NO. 016-1351

SCALE: | SHEET S-03 OF S-30 SHEETS | STA. | TO STA.

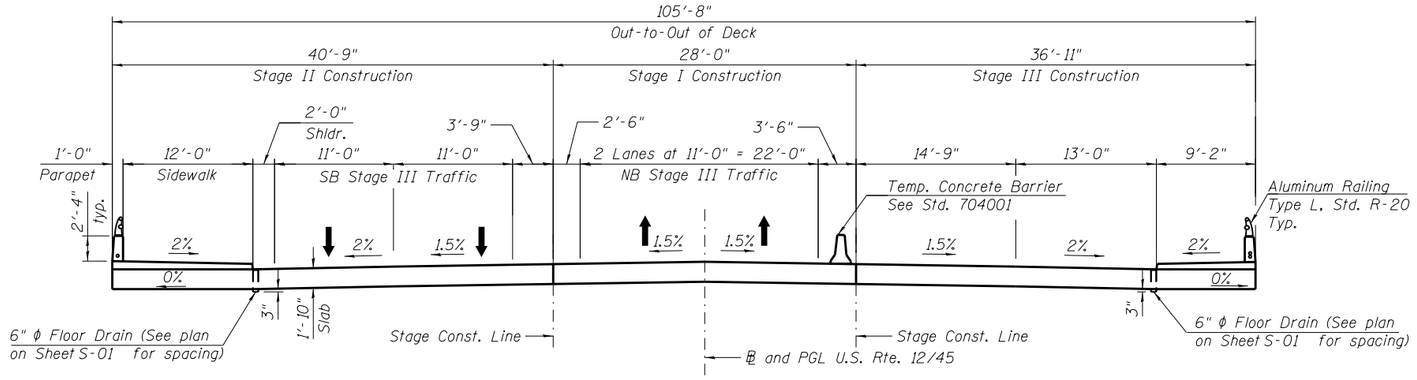
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	48
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

FILE PATH = P:\111-532 IDOT PTB161 Item 8 (Various-Various)\Work Order #11 - US 12 over Addison Creek Culvert\Structural\Sheets\0161351-60V22-S03-Stage I & II Const.dgn



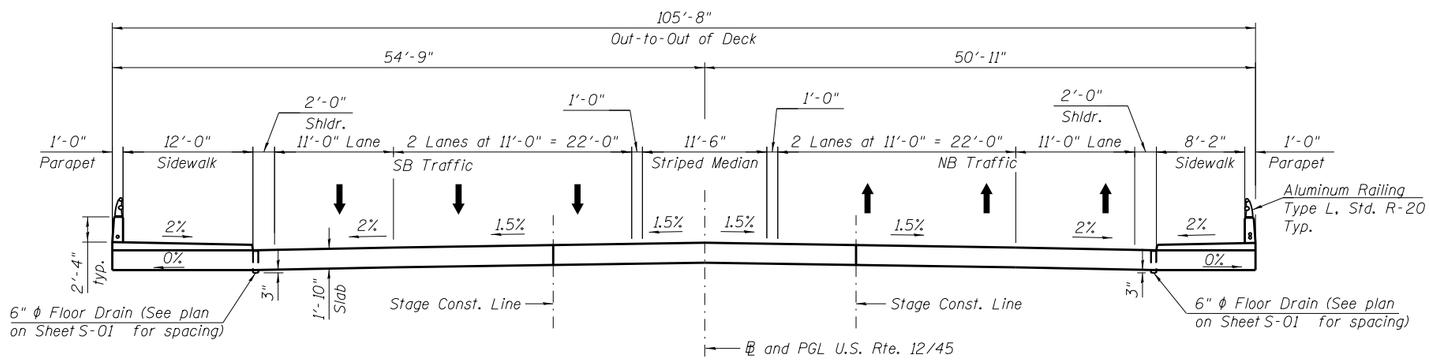
STAGE III REMOVAL

(Looking Upstation)
(Measured perp. to ℄ U.S. Rte 12/45)



STAGE III CONSTRUCTION

(Looking Upstation)
(Measured perp. to ℄ U.S. Rte 12/45)



FINAL CROSS SECTION

(Looking Upstation)
(Measured perp. to ℄ U.S. Rte 12/45)

LEGEND

▨ - Removal of existing structure

FILE PATH = P:\1111-532 IDOT PTB161 Item 8 Various-Various\Work Order #11 - US 12 over Addison Creek Culvert\Structural\Sheets\016l351-60V22-S04-Stage III Const.dgn

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RESEARCH & TESTING
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PHONE: (708) 236-0900
FAX: (708) 236-0901

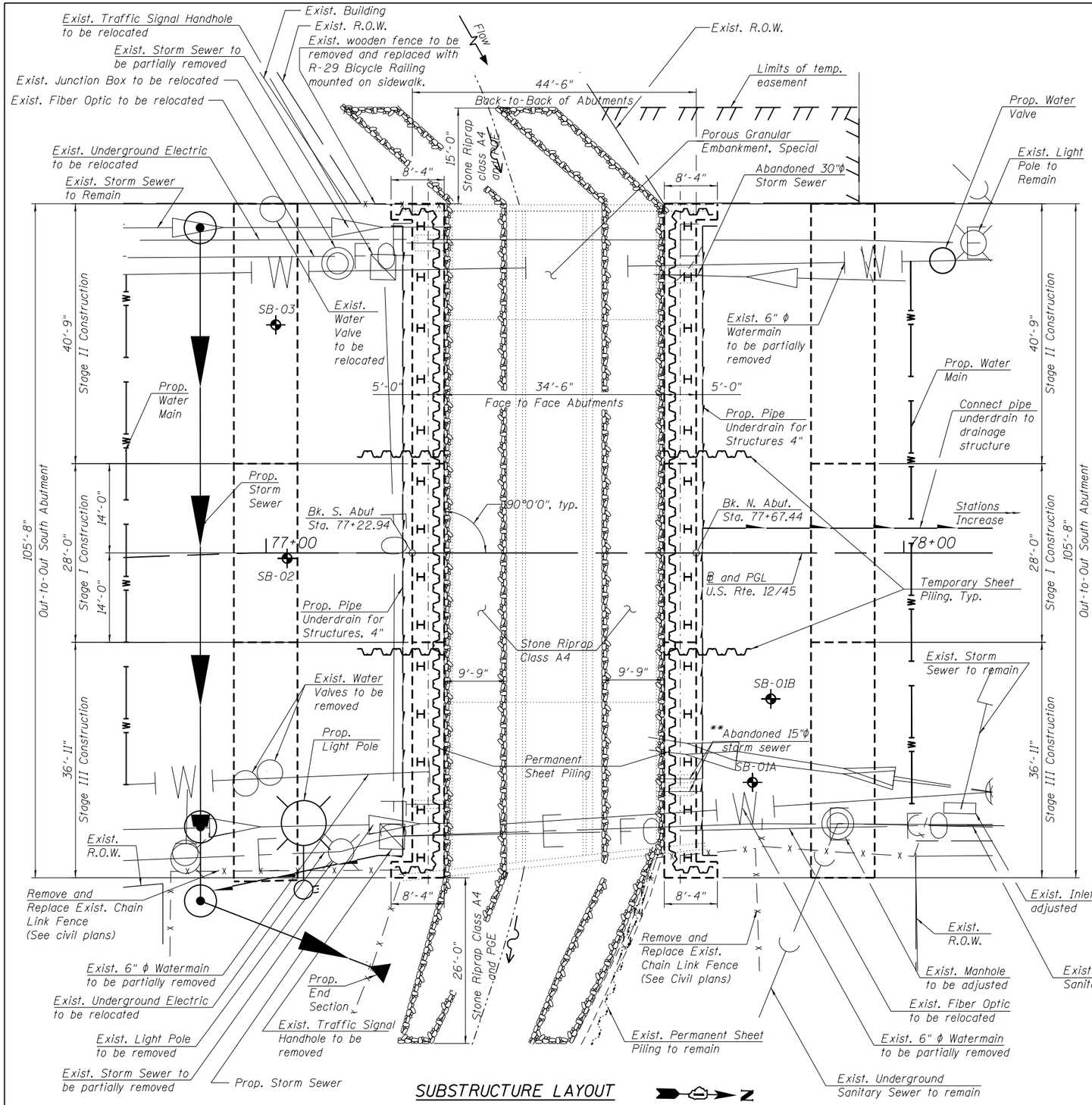
0161351-60V22-S04-Stage III Const.dgn	DESIGNED - MI, MA	REVISED	
USER NAME = Stoyanka.Kotorakova	DRAWN - MA, KJD	REVISED	
PLOT SCALE = 14:0 '1' / in.	CHECKED - LAB, MI	REVISED	
PLOT DATE = 1/16/2018	DATE - 12/08/2017	REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

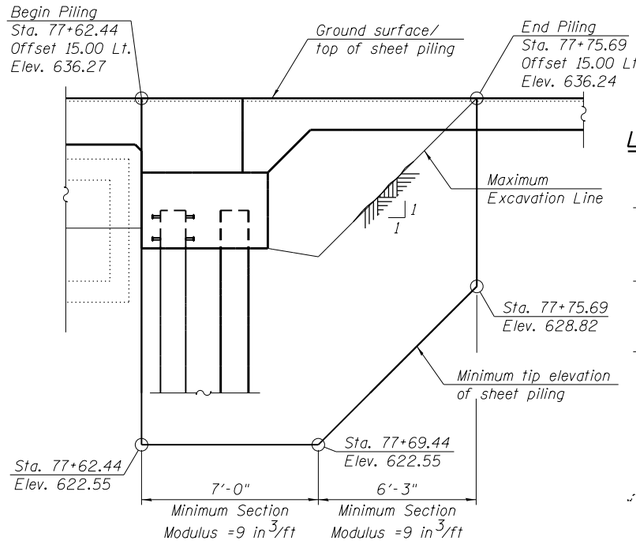
STAGE III REMOVAL AND CONSTRUCTION AND FINAL CROSS SECTION
STRUCTURE NO. 016-1351

SCALE: | SHEET S-04 OF S-30 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	49
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

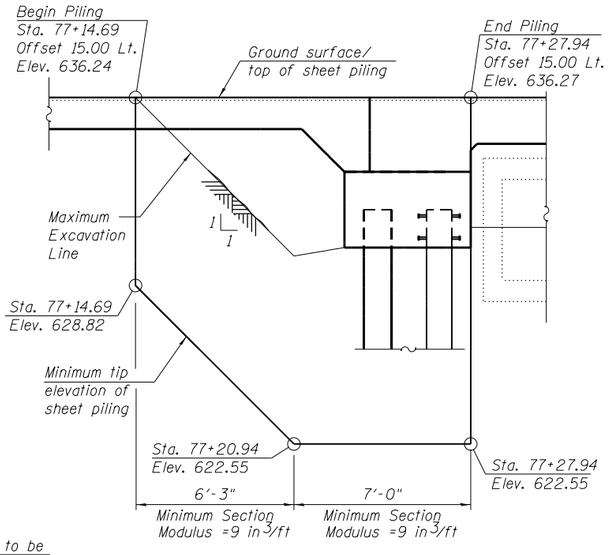


SUBSTRUCTURE LAYOUT



NORTHWEST TEMPORARY SHEET PILING

(Northeast Temporary Sheet Piling Similar)



SOUTHWEST TEMPORARY SHEET PILING

(Southeast Temporary Sheet Piling Similar)

LEGEND

- Soil Boring location
- Exist. Water Main
- Exist. Light Pole
- Exist. Underground Electric
- Exist. Water Valve
- Exist. Telephone Line
- Exist. Telephone Junction Box
- Exist. Traffic Signal Handhole
- Exist. Manhole
- Exist. Permanent Steel Sheet Piling
- Exist. Fiber Optic
- Exist. Storm Sewer
- Prop. Storm Sewer
- Prop. Water Main
- Prop. Water Valve
- Prop. Manhole
- Prop. End Section

NOTE:

1. If the Contractor chooses to alter the temporary cantilevered sheet piling design requirements shown on the plans, a design submitted including plan details and calculations will be required for review and acceptance by the Engineer.

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Temporary Sheet Piling	Sq Ft	650

HBM ENGINEERING GROUP, LLC.
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FAX: (708) 236-0901

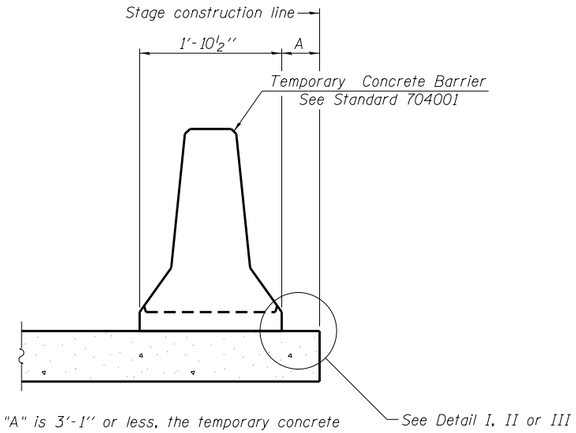
0161351-60V22-S05-Substructure Layout.dgn	DESIGNED - KJD	REVISED
USER NAME = lisa.buntin	DRAWN - KJD	REVISED
PLOT SCALE = 18.00 ' / in.	CHECKED - LAB, MI	REVISED
PLOT DATE = 12/8/2017	DATE - 12/08/2017	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBSTRUCTURE LAYOUT AND TEMPORARY SHEET PILING
STRUCTURE NO. 016-1351

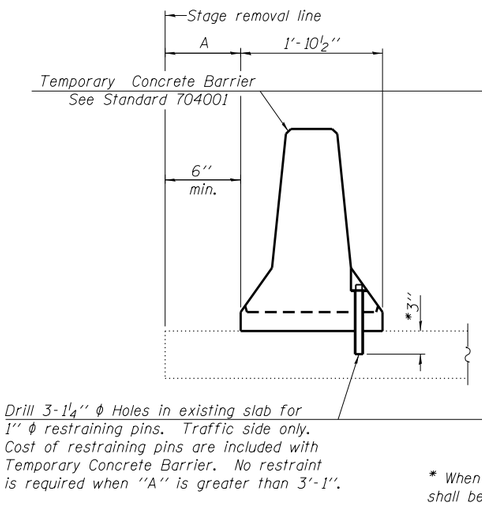
SCALE: | SHEET S-05 OF S-30 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	50
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			



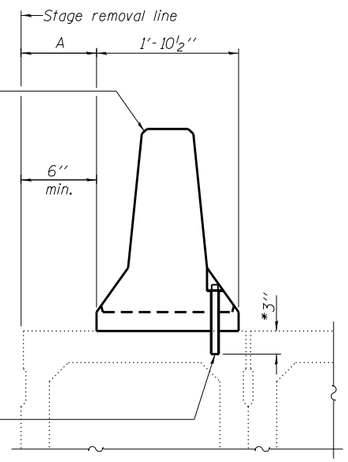
When "A" is 3'-1'' or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1''.

NEW SLAB OR NEW DECK BEAM



Drill 3-1¼'' ϕ Holes in existing slab for 1'' ϕ restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when ''A'' is greater than 3'-1''.

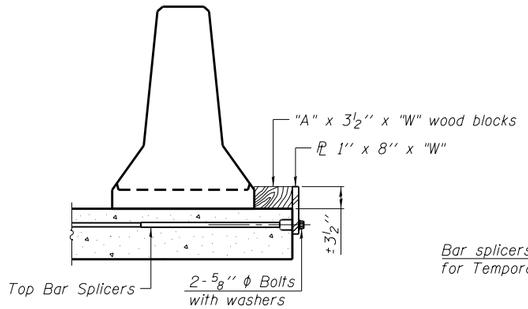
EXISTING SLAB



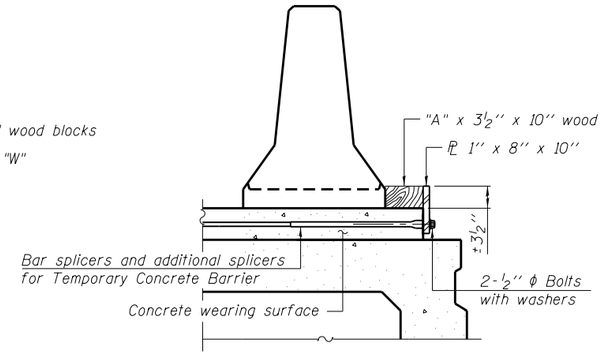
* When hot-mix asphalt wearing surface is present, embedment shall be 3'' plus the wearing surface depth.

EXISTING DECK BEAM

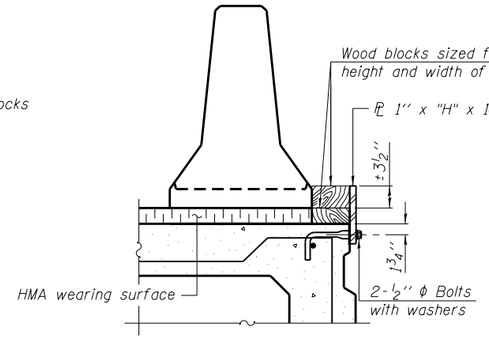
SECTIONS THRU SLAB OR DECK BEAM



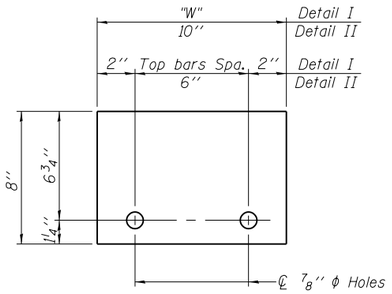
DETAIL I



DETAIL II

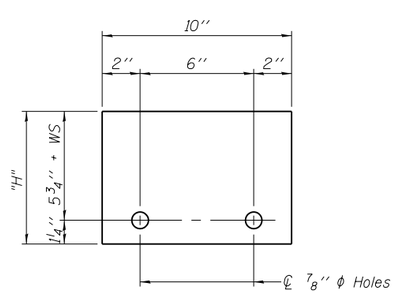


DETAIL III



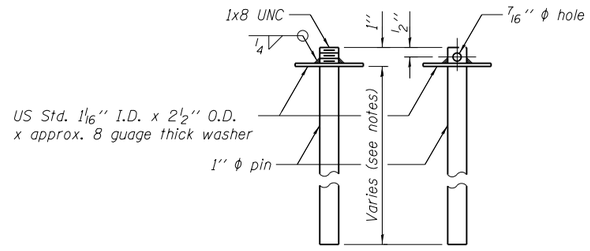
STEEL RETAINER ℄ 1'' x 8'' x "W"

(Detail I and II)

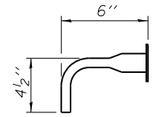


STEEL RETAINER ℄ 1'' x "H" x 10''

(Detail III)



RESTRAINING PIN



BAR SPLICER FOR #4 BAR - DETAIL III

Notes:

Cost of retainer assembly is included with Temporary Concrete Barrier.

A retainer assembly shall be located at the approximate ℄ of each temporary concrete barrier.

The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.

When the 'A' dimension is less than 1½'', the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6'' to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.

Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0'' centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.

Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6'' apart, shall be placed at 6'-0'' centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

R-27

2-17-2017

HBM ENGINEERING GROUP, LLC.
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0161351-60V22-S06-Temp Conc Barrier.dgn	DESIGNED - KJD	REVISED
USER NAME = lisa.buntin	DRAWN - KJD	REVISED
PLOT SCALE = 0.17 '/ in.	CHECKED - LAB, MI	REVISED
PLOT DATE = 12/8/2017	DATE - 12/08/2017	REVISED

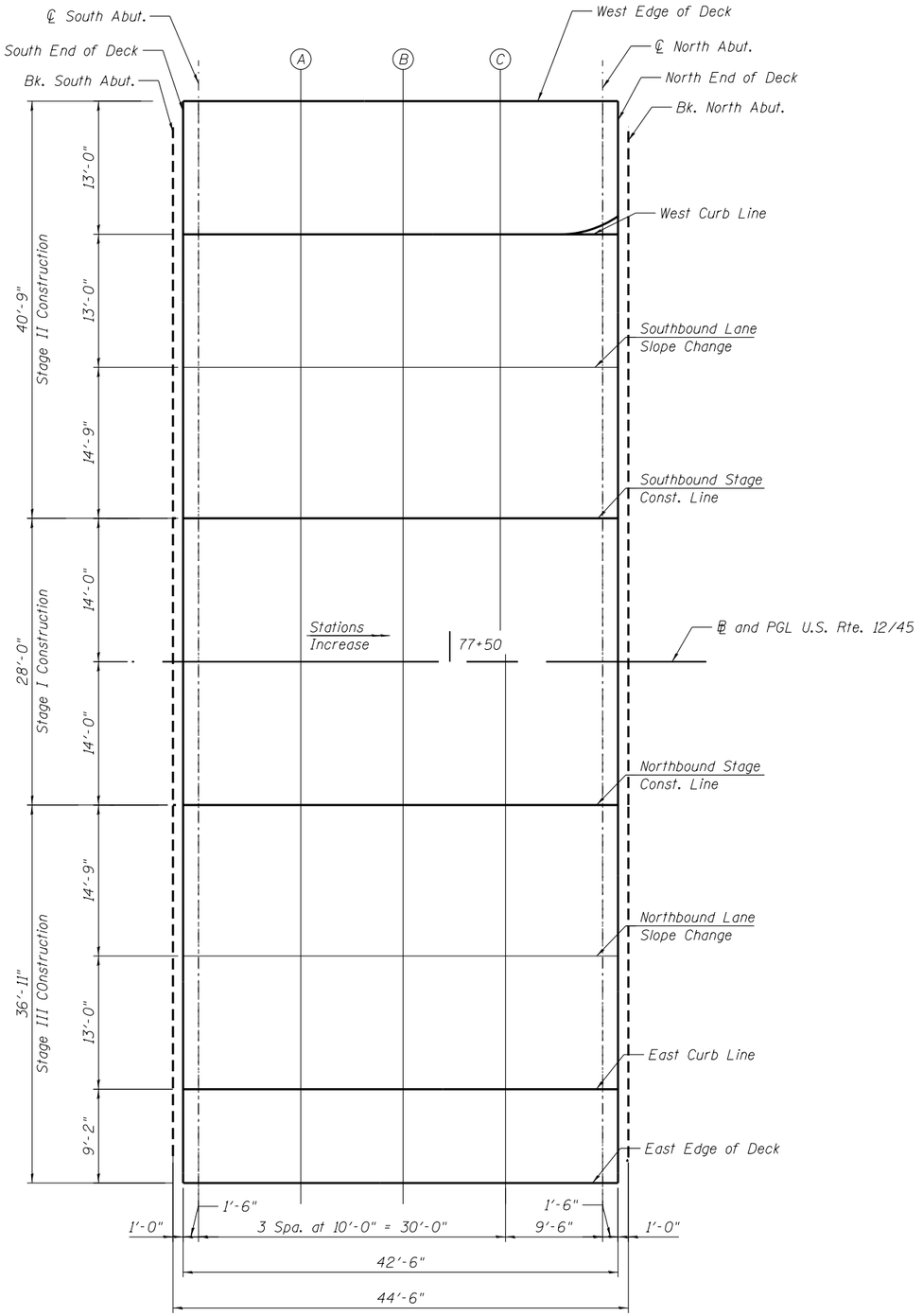
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION
STRUCTURE NO. 016-1351

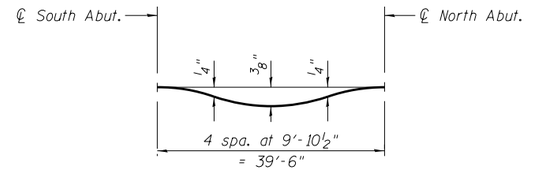
SCALE: | SHEET S-06 OF S-30 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	51
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

FILE PATH = P:\1111-532 IDOT PTB161 Item 8 (Various-Various)\Work Order #11 - US 12 over Addison Creek Culvert\Structural\Sheets\0161351-60V22-S06-Temp Conc Barrier.dgn



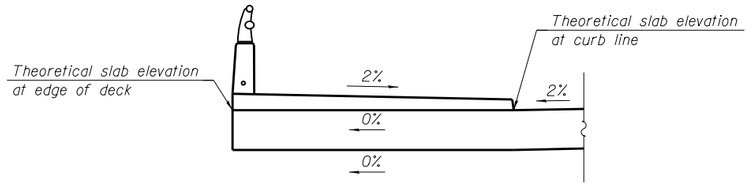
PLAN



DEAD LOAD DEFLECTION DIAGRAM

(Includes weight of concrete only.)

Note:
The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown below and on Sheet S-08.



PROJECTION OF DECK UNDER SIDEWALK

WEST EDGE OF DECK

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Bk. S. Abut.	77+22.94	-54.75'	635.80	635.80
South End of Deck	77+23.94	-54.75'	635.80	635.80
℄ S. Abut.	77+25.44	-54.75'	635.80	635.80
A	77+35.44	-54.75'	635.80	635.82
B	77+45.44	-54.75'	635.80	635.83
C	77+55.44	-54.75'	635.80	635.82
℄ N. Abut.	77+64.94	-54.75'	635.80	635.80
North End of Deck	77+66.44	-54.75'	635.80	635.80
Bk. N. Abut.	77+67.44	-54.75'	635.80	635.80

WEST CURB LINE

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Bk. S. Abut.	77+22.94	-41.75'	635.80	635.80
South End of Deck	77+23.94	-41.75'	635.80	635.80
℄ S. Abut.	77+25.44	-41.75'	635.80	635.80
A	77+35.44	-41.75'	635.80	635.82
B	77+45.44	-41.75'	635.80	635.83
C	77+55.44	-41.75'	635.80	635.82
℄ N. Abut.	77+64.94	-41.75'	635.80	635.80
North End of Deck	77+66.44	-41.75'	635.80	635.80
Bk. N. Abut.	77+67.44	-41.75'	635.80	635.80

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0161351-60V22-S07-Slab Elev Location.dgn	DESIGNED - LAB	REVISED
USER NAME = lisa.buntin	DRAWN - LAB, KJD	REVISED
PLOT SCALE = 14.00 ' / in.	CHECKED - MI	REVISED
PLOT DATE = 12/8/2017	DATE - 12/08/2017	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATION LOCATIONS
STRUCTURE NO. 016-1351

SCALE: | SHEET S-07 OF S-30 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	52
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

FILE PATH = P:\1111-532 IDOT PTB161 Item 8 (Various-Various)\Work Order #11 - US 12 over Addison Creek Culvert\Structural\Sheets\0161351-60V22-S07-Slab Elev Location.dgn

SOUTHBOUND LANE SLOPE CHANGE

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Bk. S. Abut.	77+22.94	-28.75'	636.06	636.06
South End of Deck	77+23.94	-28.75'	636.06	636.06
℄ S. Abut.	77+25.44	-28.75'	636.06	636.06
A	77+35.44	-28.75'	636.06	636.07
B	77+45.44	-28.75'	636.06	636.08
C	77+55.44	-28.75'	636.06	636.07
℄ N. Abut.	77+64.94	-28.75'	636.06	636.06
North End of Deck	77+66.44	-28.75'	636.06	636.06
Bk. N. Abut.	77+67.44	-28.75'	636.06	636.06

NORTHBOUND LANE SLOPE CHANGE

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Bk. S. Abut.	77+22.94	28.75'	636.06	636.06
South End of Deck	77+23.94	28.75'	636.06	636.06
℄ S. Abut.	77+25.44	28.75'	636.06	636.06
A	77+35.44	28.75'	636.06	636.07
B	77+45.44	28.75'	636.06	636.08
C	77+55.44	28.75'	636.06	636.07
℄ N. Abut.	77+64.94	28.75'	636.06	636.06
North End of Deck	77+66.44	28.75'	636.06	636.06
Bk. N. Abut.	77+67.44	28.75'	636.06	636.06

SOUTHBOUND STAGE CONST. LINE

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Bk. S. Abut.	77+22.94	-14.00'	636.28	636.28
South End of Deck	77+23.94	-14.00'	636.28	636.28
℄ S. Abut.	77+25.44	-14.00'	636.28	636.28
A	77+35.44	-14.00'	636.28	636.30
B	77+45.44	-14.00'	636.28	636.30
C	77+55.44	-14.00'	636.28	636.30
℄ N. Abut.	77+64.94	-14.00'	636.28	636.28
North End of Deck	77+66.44	-14.00'	636.28	636.28
Bk. N. Abut.	77+67.44	-14.00'	636.28	636.28

EAST CURB LINE

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Bk. S. Abut.	77+22.94	41.75'	635.80	635.80
South End of Deck	77+23.94	41.75'	635.80	635.80
℄ S. Abut.	77+25.44	41.75'	635.80	635.80
A	77+35.44	41.75'	635.80	635.82
B	77+45.44	41.75'	635.80	635.83
C	77+55.44	41.75'	635.80	635.82
℄ N. Abut.	77+64.94	41.75'	635.80	635.80
North End of Deck	77+66.44	41.75'	635.80	635.80
Bk. N. Abut.	77+67.44	41.75'	635.80	635.80

℄ AND PGL U.S. RTE. 12/45

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Bk. S. Abut.	77+22.94	0.00'	636.49	636.49
South End of Deck	77+23.94	0.00'	636.49	636.49
℄ S. Abut.	77+25.44	0.00'	636.49	636.49
A	77+35.44	0.00'	636.49	636.51
B	77+45.44	0.00'	636.49	636.51
C	77+55.44	0.00'	636.49	636.51
℄ N. Abut.	77+64.94	0.00'	636.49	636.49
North End of Deck	77+66.44	0.00'	636.49	636.49
Bk. N. Abut.	77+67.44	0.00'	636.49	636.49

EAST EDGE OF DECK

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Bk. S. Abut.	77+22.94	50.92'	635.80	635.80
South End of Deck	77+23.94	50.92'	635.80	635.80
℄ S. Abut.	77+25.44	50.92'	635.80	635.80
A	77+35.44	50.92'	635.80	635.82
B	77+45.44	50.92'	635.80	635.83
C	77+55.44	50.92'	635.80	635.82
℄ N. Abut.	77+64.94	50.92'	635.80	635.80
North End of Deck	77+66.44	50.92'	635.80	635.80
Bk. N. Abut.	77+67.44	50.92'	635.80	635.80

NORTHBOUND STAGE CONST. LINE

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Bk. S. Abut.	77+22.94	14.00'	636.28	636.28
South End of Deck	77+23.94	14.00'	636.28	636.28
℄ S. Abut.	77+25.44	14.00'	636.28	636.28
A	77+35.44	14.00'	636.28	636.30
B	77+45.44	14.00'	636.28	636.30
C	77+55.44	14.00'	636.28	636.30
℄ N. Abut.	77+64.94	14.00'	636.28	636.28
North End of Deck	77+66.44	14.00'	636.28	636.28
Bk. N. Abut.	77+67.44	14.00'	636.28	636.28

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0161351-60V22-S08-Top of Slab Elev.dgn	DESIGNED - LAB	REVISED
USER NAME = lisa.buntin	DRAWN - LAB, KJD	REVISED
PLOT SCALE = 14.00 ' / in.	CHECKED - MI	REVISED
PLOT DATE = 12/8/2017	DATE - 12/08/2017	REVISED

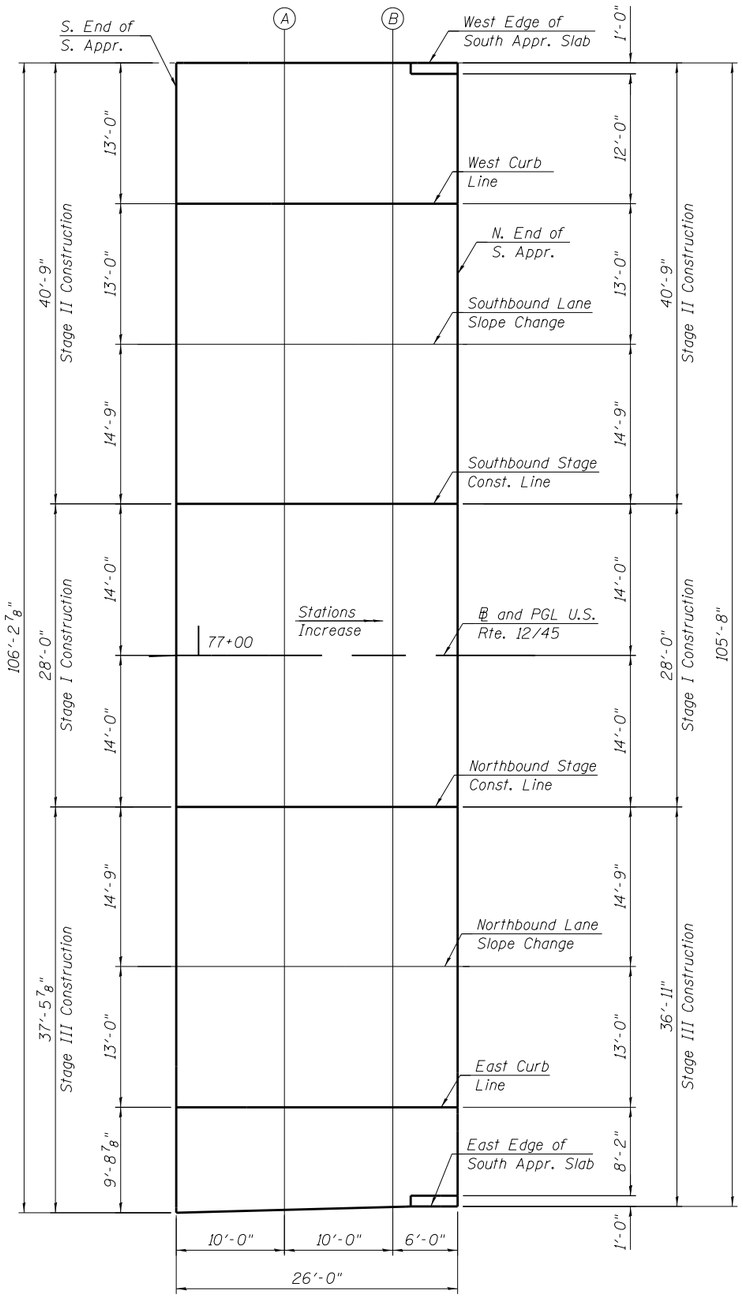
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS
STRUCTURE NO. 016-1351

SCALE: | SHEET S-08 OF S-30 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	53
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

FILE PATH = P:\1111-532 IDOT PTB161 Item 8 (Various-Various)\Work Order #11 - US 12 over Addison Creek Culvert\Structural\Sheets\0161351-60V22-S08-Top of Slab Elev.dgn



PLAN

WEST EDGE OF SOUTH APPR. SLAB

Location	Station	Offset	Theoretical Grade Elevations
S. End of S. Appr.	76+97.94	-54.75′	635.74
A	77+07.94	-54.75′	635.76
B	77+17.94	-54.75′	635.78
N. End of S. Appr.	77+23.94	-54.75′	635.80

WEST CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
S. End of S. Appr.	76+97.94	-41.75′	635.74
A	77+07.94	-41.75′	635.76
B	77+17.94	-41.75′	635.78
N. End of S. Appr.	77+23.94	-41.75′	635.80

SOUTHBOUND LANE SLOPE CHANGE

Location	Station	Offset	Theoretical Grade Elevations
S. End of S. Appr.	76+97.94	-28.75′	636.00
A	77+07.94	-28.75′	636.02
B	77+17.94	-28.75′	636.04
N. End of S. Appr.	77+23.94	-28.75′	636.06

SOUTHBOUND STAGE CONST. LINE

Location	Station	Offset	Theoretical Grade Elevations
S. End of S. Appr.	76+97.94	-14.00′	636.22
A	77+07.94	-14.00′	636.24
B	77+17.94	-14.00′	636.27
N. End of S. Appr.	77+23.94	-14.00′	636.28

℄ AND PGL U.S. RTE. 12/45

Location	Station	Offset	Theoretical Grade Elevations
S. End of S. Appr.	76+97.94	0.00′	636.43
A	77+07.94	0.00′	636.45
B	77+17.94	0.00′	636.48
N. End of S. Appr.	77+23.94	0.00′	636.49

NORTHBOUND STAGE CONST. LINE

Location	Station	Offset	Theoretical Grade Elevations
S. End of S. Appr.	76+97.94	14.00′	636.22
A	77+07.94	14.00′	636.24
B	77+17.94	14.00′	636.27
N. End of S. Appr.	77+23.94	14.00′	636.28

NORTHBOUND LANE SLOPE CHANGE

Location	Station	Offset	Theoretical Grade Elevations
S. End of S. Appr.	76+97.94	28.75′	636.00
A	77+07.94	28.75′	636.02
B	77+17.94	28.75′	636.04
N. End of S. Appr.	77+23.94	28.75′	636.06

EAST CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
S. End of S. Appr.	76+97.94	41.75′	635.74
A	77+07.94	41.75′	635.76
B	77+17.94	41.75′	635.78
N. End of S. Appr.	77+23.94	41.75′	635.80

EAST EDGE OF SOUTH APPR. SLAB

Location	Station	Offset	Theoretical Grade Elevations
S. End of S. Appr.	76+97.94	51.49′	635.74
A	77+07.94	51.23′	635.76
B	77+17.94	50.96′	635.78
N. End of S. Appr.	77+23.94	50.92′	635.80

HBM ENGINEERING GROUP, LLC.
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FAX: (708) 236-0901

0161351-60V22-S09-Top of S Appr Elev.dgn	DESIGNED - LAB	REVISED
USER NAME = lisa.buntin	DRAWN - LAB, KJD	REVISED
PLOT SCALE = 14.00 ′ / in.	CHECKED - MI	REVISED
PLOT DATE = 12/8/2017	DATE - 12/08/2017	REVISED

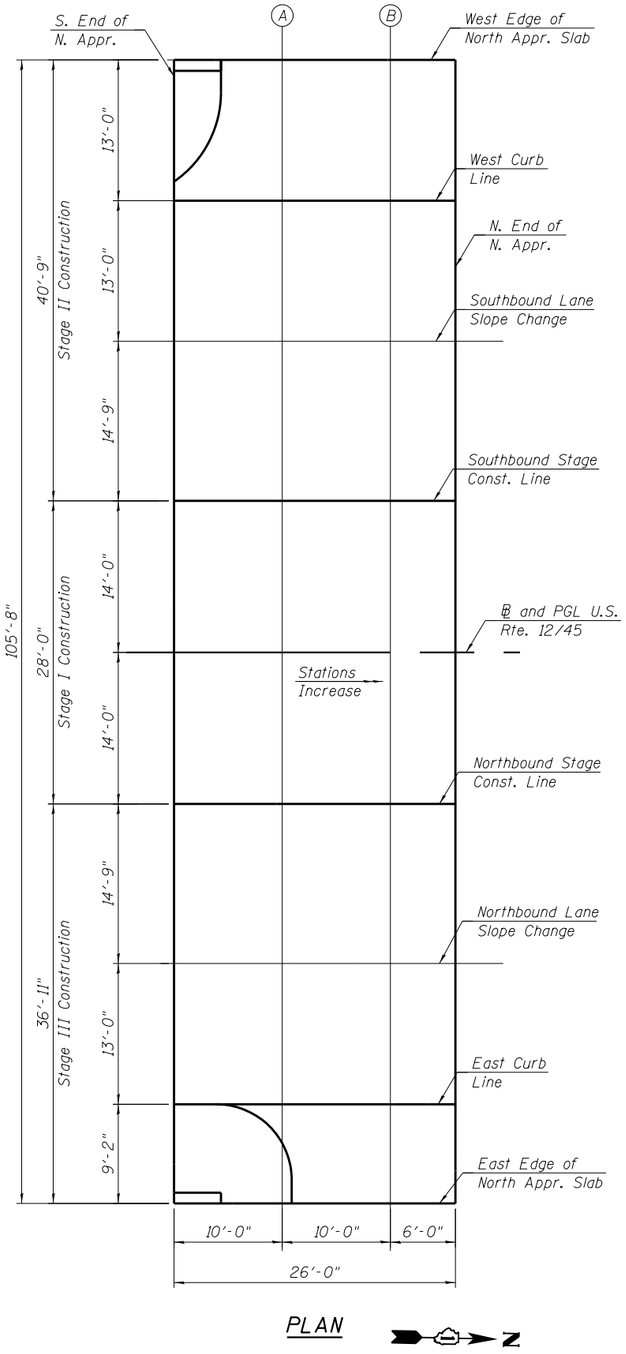
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TOP OF SOUTH APPROACH SLAB ELEVATIONS
STRUCTURE NO. 016-1351

SCALE: | SHEET S-09 OF S-30 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	54
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

FILE PATH = P:\1111-532 IDOT FTB161 Item 8 (Various-Various)\Work Order #11 - US 12 over Addison Creek Culvert\Structural\Sheets\0161351-60V22-S09-Top of S Appr Elev.dgn



PLAN

WEST EDGE OF NORTH APPR. SLAB

Location	Station	Offset	Theoretical Grade Elevations
S. End of N. Appr.	77+66.44	-54.75′	635.80
A	77+76.44	-54.75′	635.78
B	77+86.44	-54.75′	635.75
N. End of N. Appr.	77+92.44	-54.75′	635.74

WEST CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
S. End of N. Appr.	77+66.44	-41.75′	635.80
A	77+76.44	-41.75′	635.78
B	77+86.44	-41.75′	635.75
N. End of N. Appr.	77+92.44	-41.75′	635.74

SOUTHBOUND LANE SLOPE CHANGE

Location	Station	Offset	Theoretical Grade Elevations
S. End of N. Appr.	77+66.44	-28.75′	636.06
A	77+76.44	-28.75′	636.04
B	77+86.44	-28.75′	636.01
N. End of N. Appr.	77+92.44	-28.75′	636.00

SOUTHBOUND STAGE CONST. LINE

Location	Station	Offset	Theoretical Grade Elevations
S. End of N. Appr.	77+66.44	-14.00′	636.28
A	77+76.44	-14.00′	636.26
B	77+86.44	-14.00′	636.23
N. End of N. Appr.	77+92.44	-14.00′	636.22

℄ AND PGL U.S. RTE. 12/45

Location	Station	Offset	Theoretical Grade Elevations
S. End of N. Appr.	77+66.44	0.00′	636.49
A	77+76.44	0.00′	636.47
B	77+86.44	0.00′	636.44
N. End of N. Appr.	77+92.44	0.00′	636.43

NORTHBOUND STAGE CONST. LINE

Location	Station	Offset	Theoretical Grade Elevations
S. End of N. Appr.	77+66.44	14.00′	636.28
A	77+76.44	14.00′	636.26
B	77+86.44	14.00′	636.23
N. End of N. Appr.	77+92.44	14.00′	636.22

NORTHBOUND LANE SLOPE CHANGE

Location	Station	Offset	Theoretical Grade Elevations
S. End of N. Appr.	77+66.44	28.75′	636.06
A	77+76.44	28.75′	636.04
B	77+86.44	28.75′	636.01
N. End of N. Appr.	77+92.44	28.75′	636.00

EAST CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
S. End of N. Appr.	77+66.44	41.75′	635.80
A	77+76.44	41.75′	635.78
B	77+86.44	41.75′	635.75
N. End of N. Appr.	77+92.44	41.75′	635.74

EAST EDGE OF NORTH APPR. SLAB

Location	Station	Offset	Theoretical Grade Elevations
S. End of N. Appr.	77+66.44	50.92′	635.98
A	77+76.44	50.92′	635.96
B	77+86.44	50.92′	635.94
N. End of N. Appr.	77+92.44	50.92′	635.92

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0161351-60V22-S10-Top of N Appr Elev.dgn	DESIGNED - LAB	REVISED
USER NAME = lisa.buntin	DRAWN - LAB, KJD	REVISED
PLOT SCALE = 14.00 ′ / in.	CHECKED - MI	REVISED
PLOT DATE = 12/8/2017	DATE - 12/08/2017	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TOP OF NORTH APPROACH SLAB ELEVATIONS
STRUCTURE NO. 016-1351

SCALE: | SHEET S-10 OF S-30 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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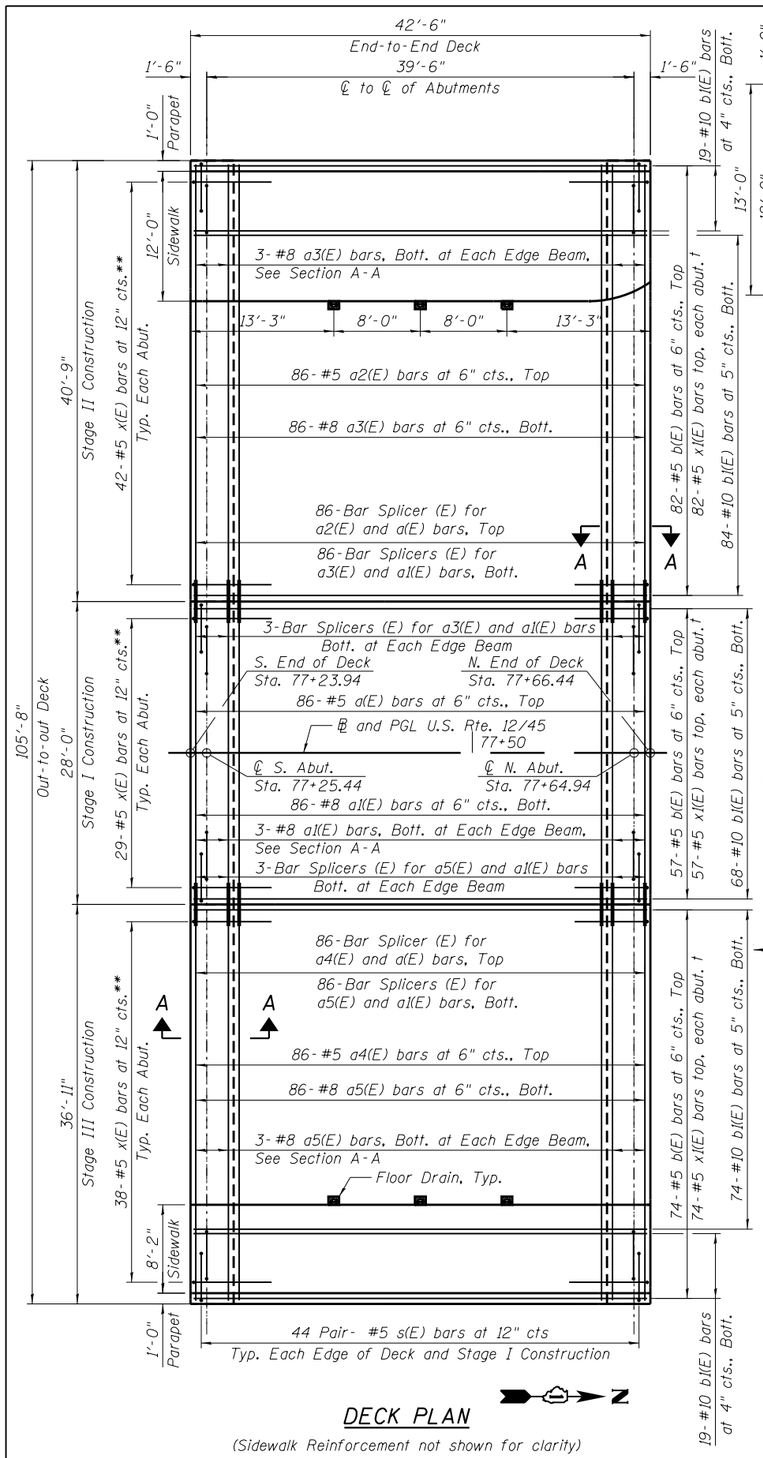
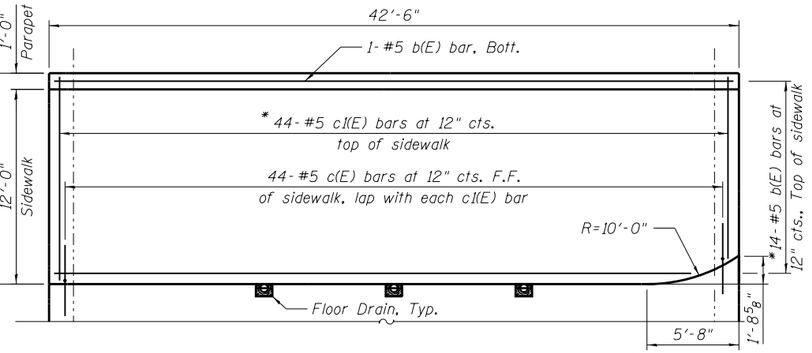
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			



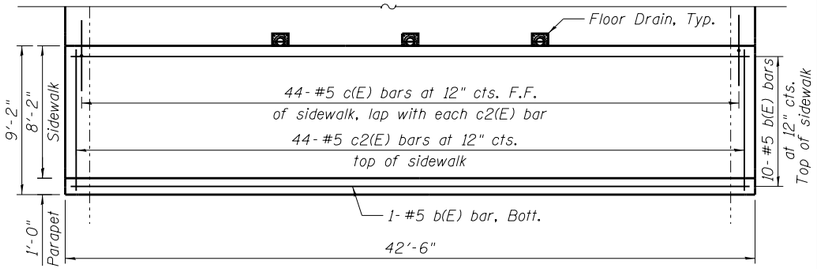
DECK PLAN

(Sidewalk Reinforcement not shown for clarity)



WEST SIDEWALK PLAN

(Showing Sidewalk Reinforcement)

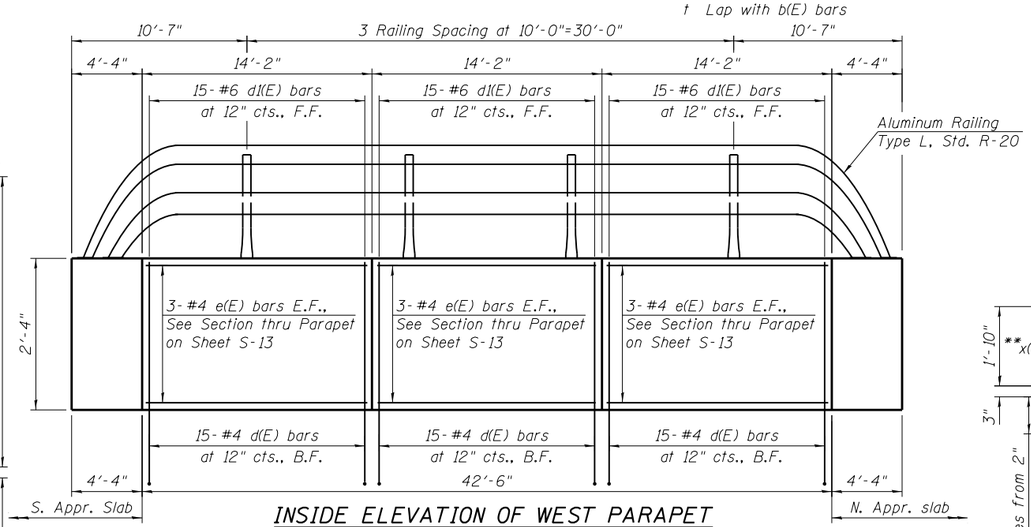


EAST SIDEWALK PLAN

(Showing Sidewalk Reinforcement)

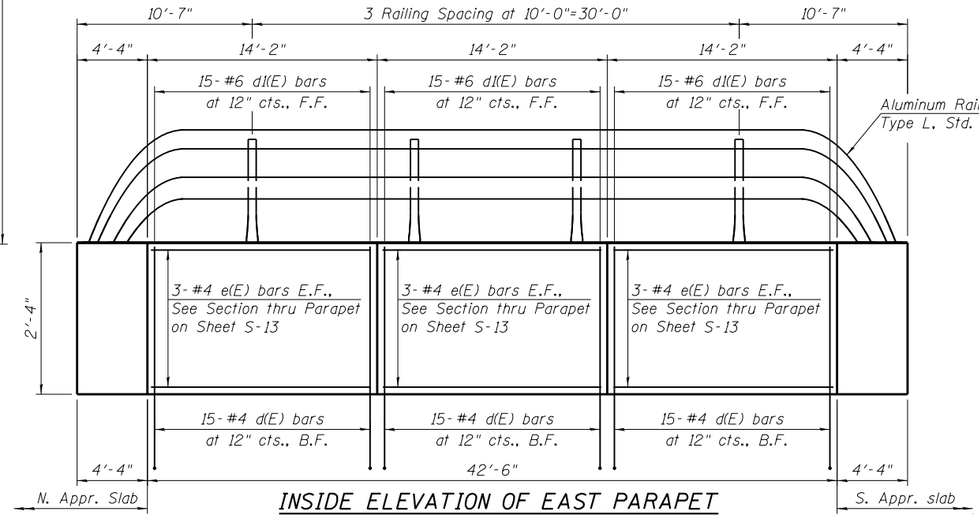
Z

* Order bars of full length and cut in field to fit
** Bend in field to fit
† Lap with b(E) bars



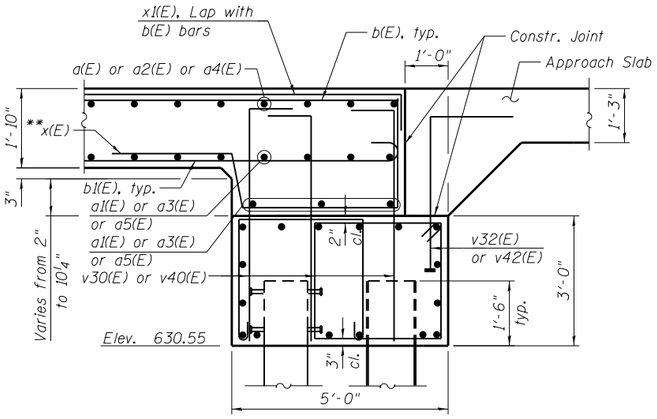
INSIDE ELEVATION OF WEST PARAPET

(Looking West)



INSIDE ELEVATION OF EAST PARAPET

(Looking East)



SECTION A-A

NOTES

1. For Deck Cross Section, See Sheet S-12 .
2. For Section thru Parapet, Parapet Joint detail, Bar Diagrams, and Bill of Material, see Sheet S-13 .
3. For Bar Splicers, see Sheet S-28.
4. For Approach Slab parapet details, see Sheets S-15 and S-17.
5. For bars v30(E) and v32(E), see Sheets S-21 and S-23.
6. For bars v40(E) and v42(E), see Sheets S-24 and S-26 .

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0161351-60V22-S11 Deck Plan.dgn	DESIGNED - SK, LAB	REVISED
USER NAME = lisa.buntin	DRAWN - SK, MAA	REVISED
PLOT SCALE = 14:0 '1' / in.	CHECKED - LAB, MI	REVISED
PLOT DATE = 1/25/2018	DATE - 12/08/2017	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DECK PLAN
STRUCTURE NO. 016-1351

SCALE: | SHEET S-11 OF S-30 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	56
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

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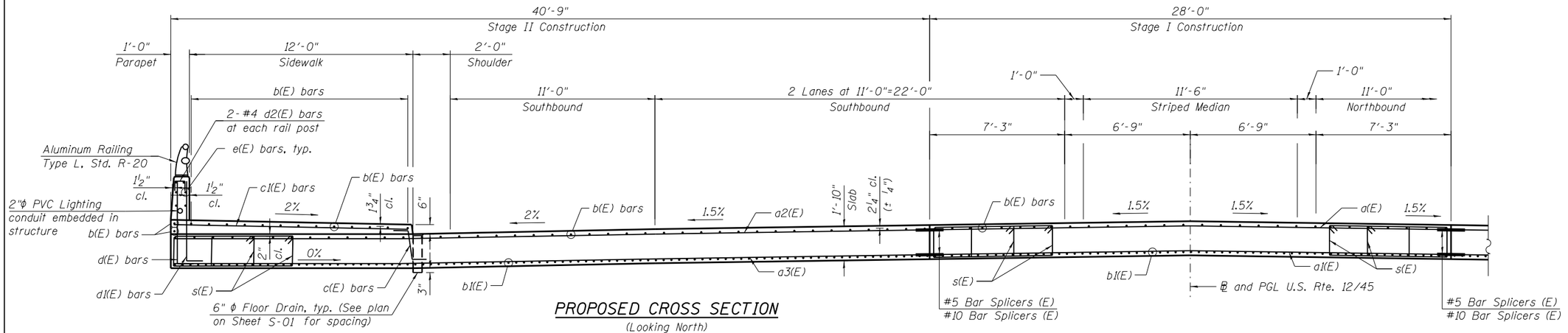
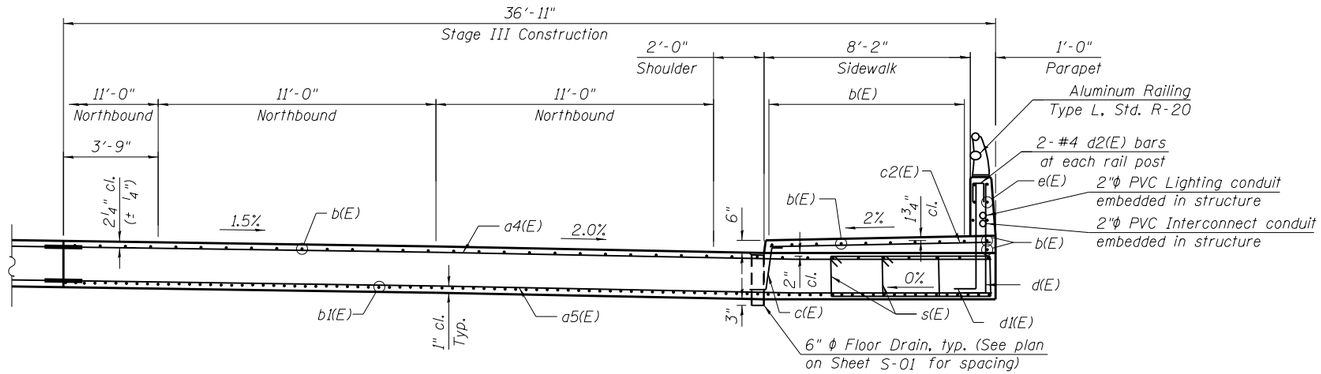


PROPOSED CROSS SECTION

(Looking North)



PROPOSED CROSS SECTION

(Looking North)

NOTES:

1. For additional notes, see Sheet S-11.

FILE PATH = P:\1111-532 IDOT PTB161 Item 8 (Various-Various)\Work Order #11 - US 12 over Addison Creek Culvert\Structural\Sheets\01613511-60V22-S12 Deck Cross Sec.dgn

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FAX: (708) 236-0901

01613511-60V22-S12 Deck Cross Sec.dgn	DESIGNED - SK, LAB	REVISED
USER NAME = Stoyanka.Kotorakova	DRAWN - SK, MAA	REVISED
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PLOT DATE = 1/16/2018	DATE - 12/08/2017	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DECK CROSS SECTION
STRUCTURE NO. 016-1351

SCALE: | SHEET S-12 OF S-30 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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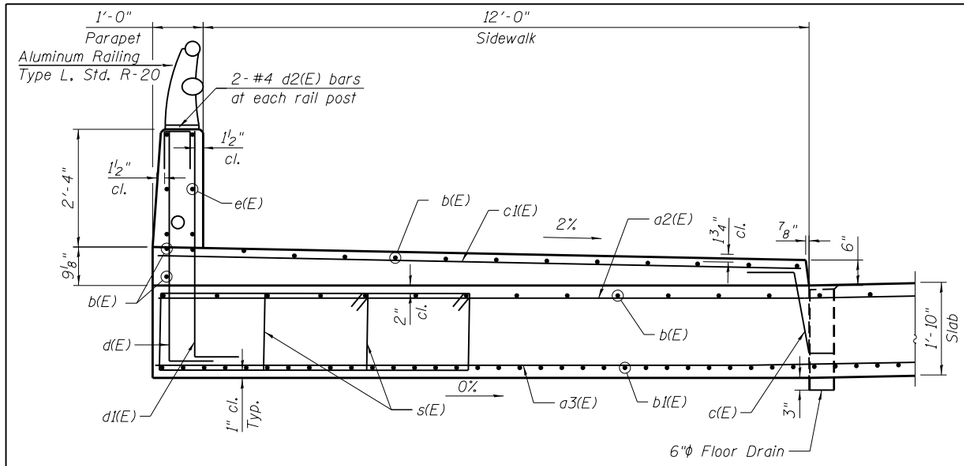
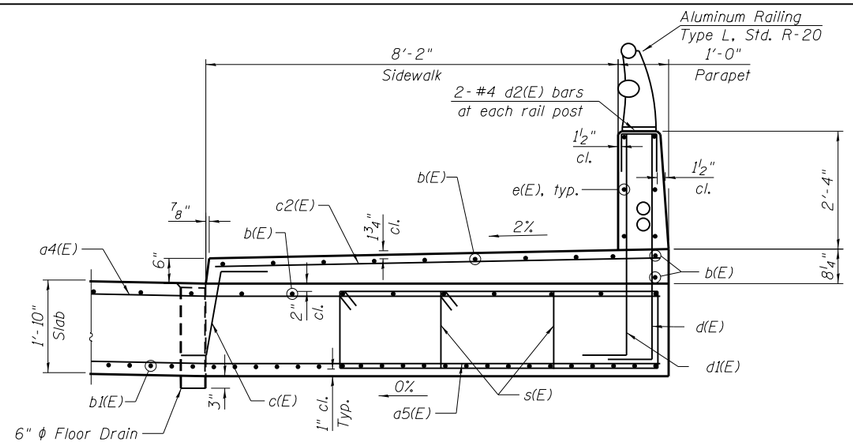
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			



SECTION THRU PARAPET

(West Sidewalk)



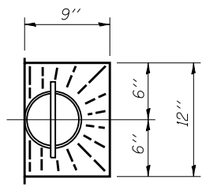
SECTION THRU PARAPET

(East Sidewalk)

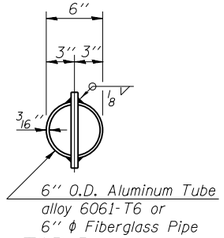
SUPERSTRUCTURE BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	86	#5	27'-9"	
a1(E)	92	#8	27'-9"	
a2(E)	86	#5	40'-6"	
a3(E)	92	#8	40'-6"	
a4(E)	86	#5	36'-8"	
a5(E)	92	#8	36'-8"	
b(E)	213	#5	42'-2"	
b1(E)	264	#10	45'-0"	
c(E)	88	#5	3'-6"	
c1(E)	44	#5	12'-8"	
c2(E)	44	#5	8'-10"	
d(E)	90	#4	5'-3"	
d1(E)	90	#6	5'-6"	
d2(E)	20	#4	2'-0"	
e(E)	36	#4	13'-10"	
s(E)	352	#5	11'-11"	
x(E)	218	#5	9'-8"	
x1(E)	426	#5	8'-10"	
Floor Drains			Each	6
Concrete Superstructure			Cu Yd	360.5
Bridge Deck Grooving			Sq Yd	385
Protective Coat			Sq Yd	549
Reinforcement Bars, Epoxy Coated			Pound	108,900

Minimum Bar Laps	
Bar	Lap
#4	2'-5"
#5	3'-0"
#6	3'-7"
#8	4'-9"

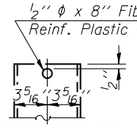


TOP PLAN

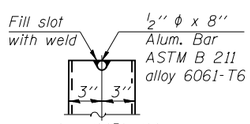


TOP PLAN

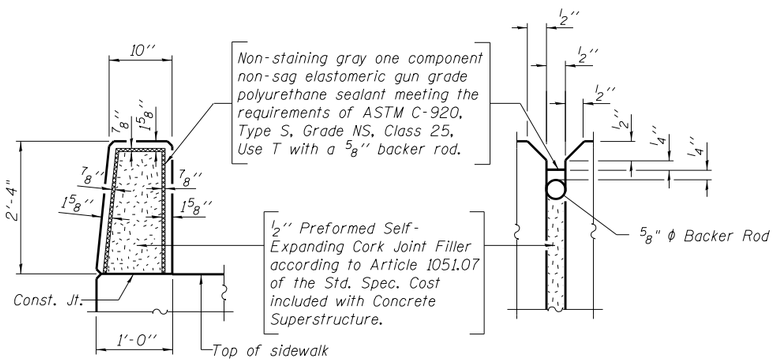
(Showing Aluminum Tube)



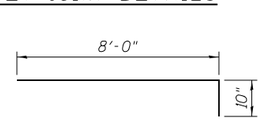
FIBERGLASS PIPE



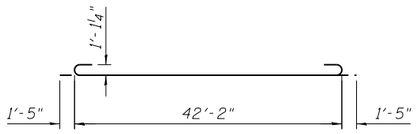
ALUMINUM TUBE



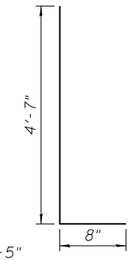
PARAPET JOINT DETAILS



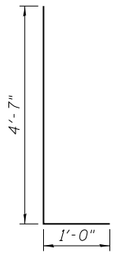
BARS x1(E)



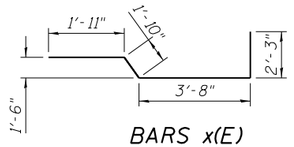
BAR b1(E)



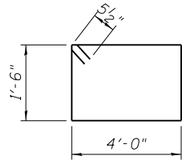
BAR d(E)



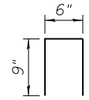
BAR d1(E)



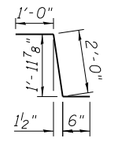
BARS x(E)



BARS s(E)



BAR d2(E)



BAR c(E)

NOTES:

1. The exterior surfaces of the floor drains shall be painted according to Article 506 with the finish coat as specified. The exterior surfaces of the drains shall be cleaned according to the Society of Protective Coating's Spec. SSPC-SP1 prior to painting.

2. Fiberglass pipe shall conform to ASTM D 2996, with short-time rupture strength hoop tensile stress of 30,000 p.s.i. minimum.

FILE PATH = P:\1111-532 IDOT PTB161 Item 8 (Various-Various)\Work Order #11 - US 12 over Addison Creek Culvert\Structural\Sheets\0161351-60V22-S13 Deck Sec & Det.dgn

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FAX: (708) 236-0901

0161351-60V22-S13 Deck Sec & Det.dgn	DESIGNED - SK, LAB	REVISED
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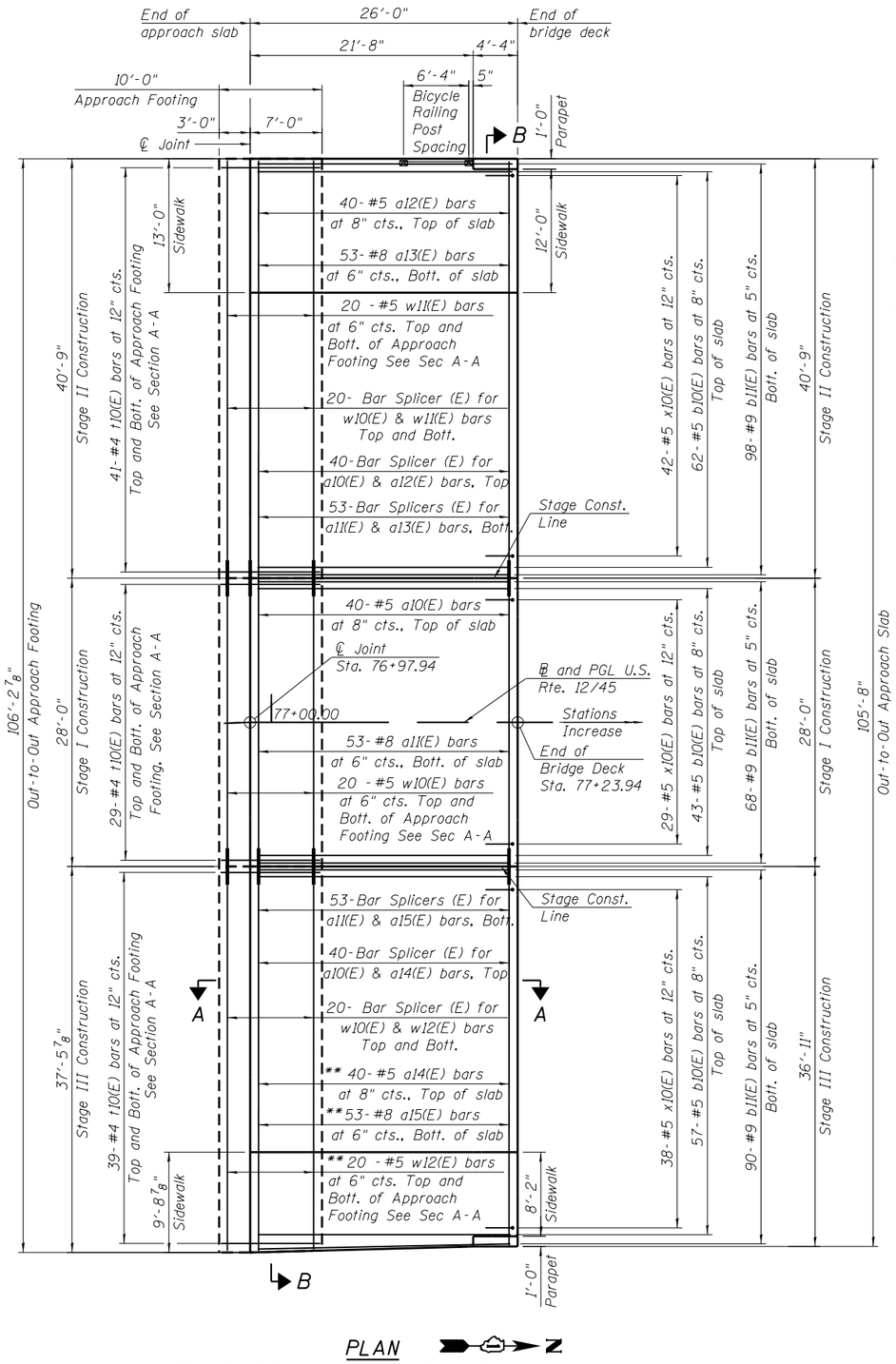
PLOT SCALE = 6.0 '' / in.	CHECKED - LAB, MI	REVISED
PLOT DATE = 1/25/2018	DATE - 12/08/2017	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DECK SECTIONS AND DETAILS
STRUCTURE NO. 016-1351

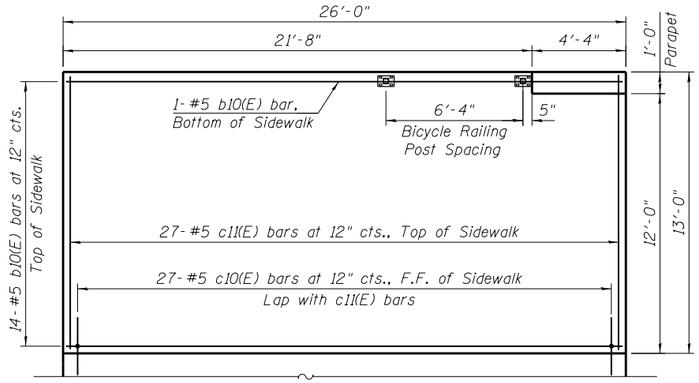
SCALE: | SHEET S-13 OF S-30 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	58
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

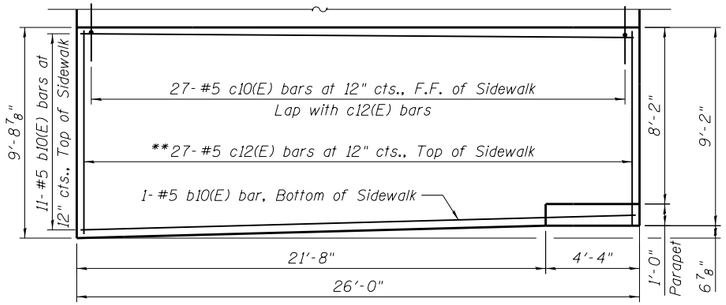


PLAN

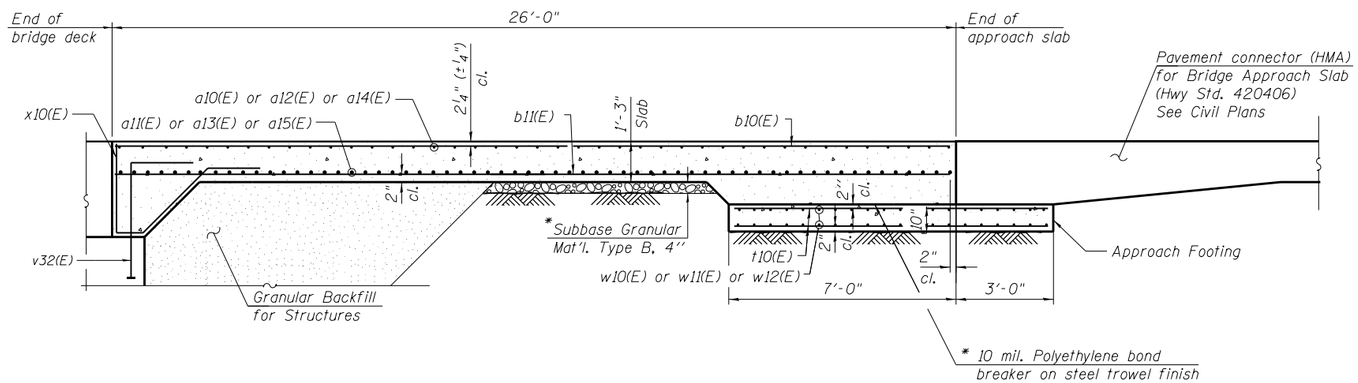
(Sidewalk reinforcement not shown for clarity)



WEST SIDEWALK PLAN



EAST SIDEWALK PLAN



SECTION A-A

*Cost included with Concrete Superstructure (Approach Slab).

**Order bars at full length and cut in field to fit.

NOTES:

1. For v32(E) bar details, granular backfill for structures and drainage treatment details, see Sheets S-21 and S-23.
2. For Section B-B, Bar diagrams and Bill of Material, see Sheet S-15.

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PLOT DATE = 12/8/2017	DATE - 12/08/2017	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOUTH APPROACH SLAB PLAN
STRUCTURE NO. 016-1351

SCALE: | SHEET S-14 OF S-30 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	59
		CONTRACT NO. 60V22		
	ILLINOIS	FED. AID PROJECT		

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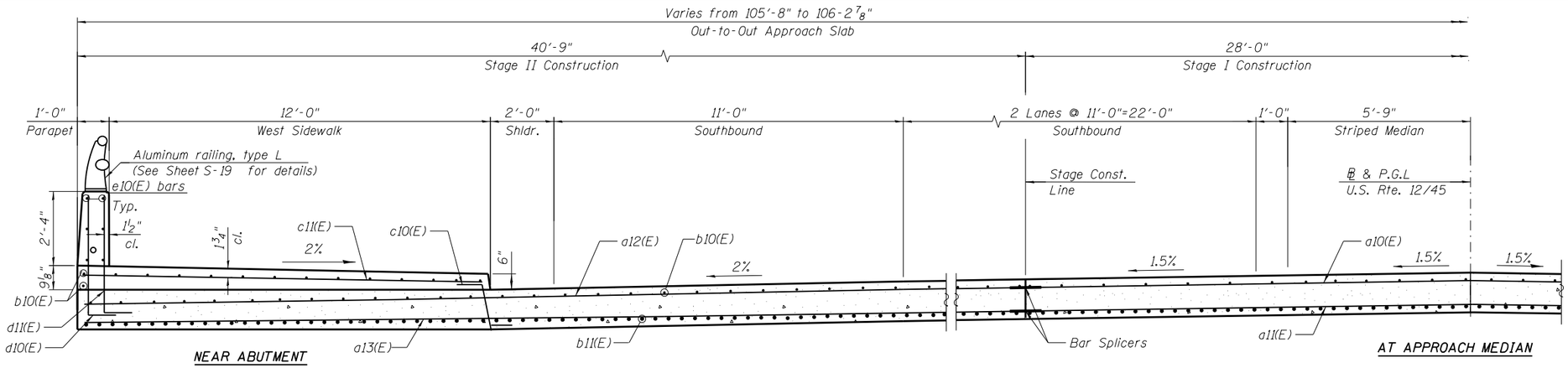
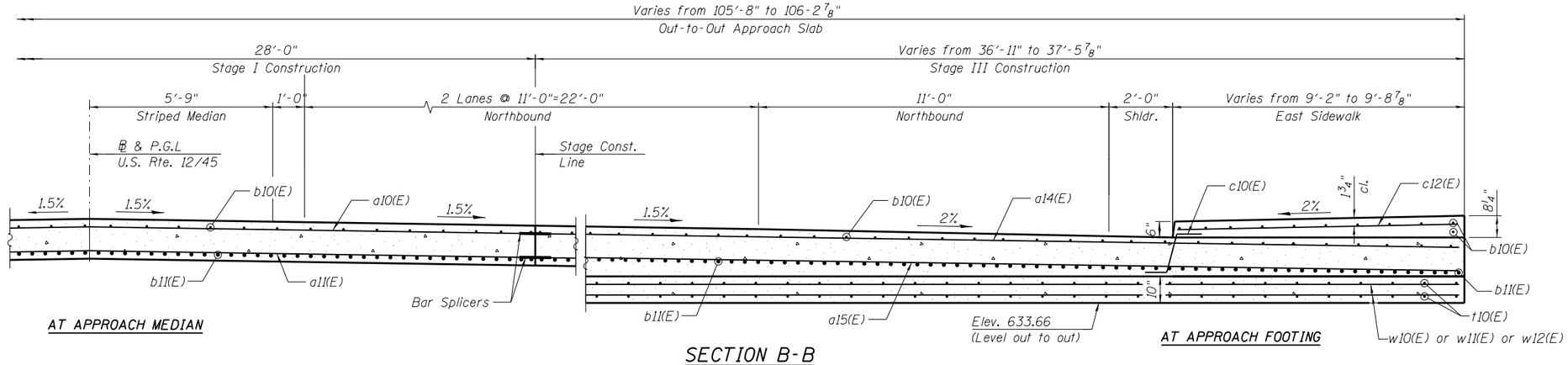
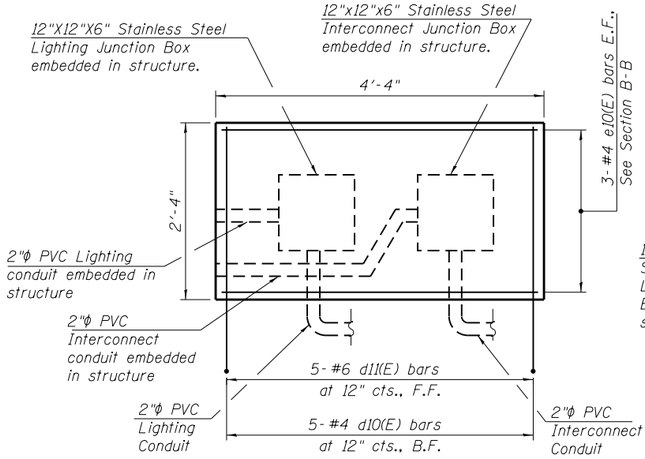




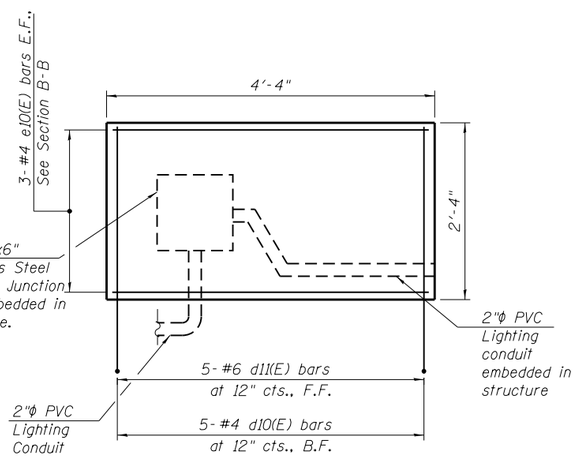
SECTION B-B

(See Plan for dimensions not shown)



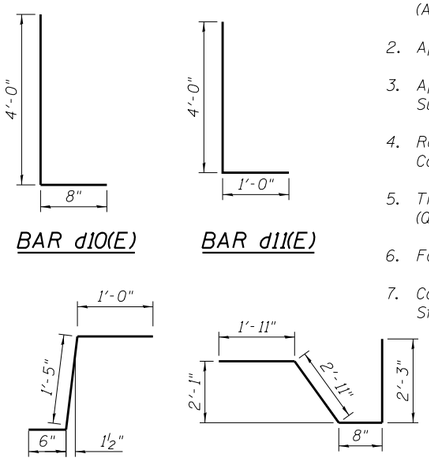
INSIDE ELEVATION OF EAST PARAPET

(Railing not shown for clarity)



INSIDE ELEVATION OF WEST PARAPET

(Railing not shown for clarity)



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a10(E)	40	#5	27'-10"	———
a11(E)	53	#8	27'-10"	———
a12(E)	40	#5	40'-2"	———
a13(E)	53	#8	40'-2"	———
a14(E)	40	#5	37'-3"	———
a15(E)	53	#8	37'-3"	———
b10(E)	189	#5	25'-8"	———
b11(E)	256	#9	25'-8"	———
c10(E)	54	#5	2'-11"	⌐
c11(E)	27	#5	12'-8"	———
c12(E)	27	#5	9'-5"	———
d10(E)	10	#4	4'-8"	L
d11(E)	10	#6	5'-0"	L
e10(E)	12	#4	4'-0"	———
t10(E)	218	#4	9'-8"	———
w10(E)	40	#5	27'-10	———
w11(E)	40	#5	40'-2"	———
w12(E)	40	#5	37'-3"	———
x10(E)	109	#5	7'-9"	⌐¬
Concrete Structures		Cu. Yd.	32.6	
Concrete Superstructure		Cu. Yd.	17.6	
Bridge Deck Grooving		Sq. Yd.	236	
Protective Coat		Sq. Yd.	313	
Concrete Superstructure (Approach Slab)		Cu. Yd.	127.4	
Reinforcement Bars, Epoxy Coated		Pound	52,890	

Minimum Bar Laps	
Bar	Lap
#4	2'-5"
#5	3'-0"
#6	3'-7"
#8	4'-9"
#9	5'-10"

NOTES:

1. Approach slab shall be paid for as Concrete Superstructure (Approach Slab).
2. Approach footing concrete shall be paid for as Concrete Structures.
3. Approach sidewalk and parapets shall be paid for as Concrete Superstructure.
4. Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated.
5. The approach footing maximum applied service bearing pressure (Qmax) = 2.0 ksf.
6. For bar splicer details, see Sheet S-28.
7. Cost of excavation for approach footing included with Concrete Structures.

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0161351-60V22-S15-S Appr Slab Det.dgn	DESIGNED - SK, MAA	REVISED
USER NAME = lisa.buntin	DRAWN - SK, MAA	REVISED
PLOT SCALE = 4.00 ' / in.	CHECKED - LAB, MI	REVISED
PLOT DATE = 12/8/2017	DATE - 12/08/2017	REVISED

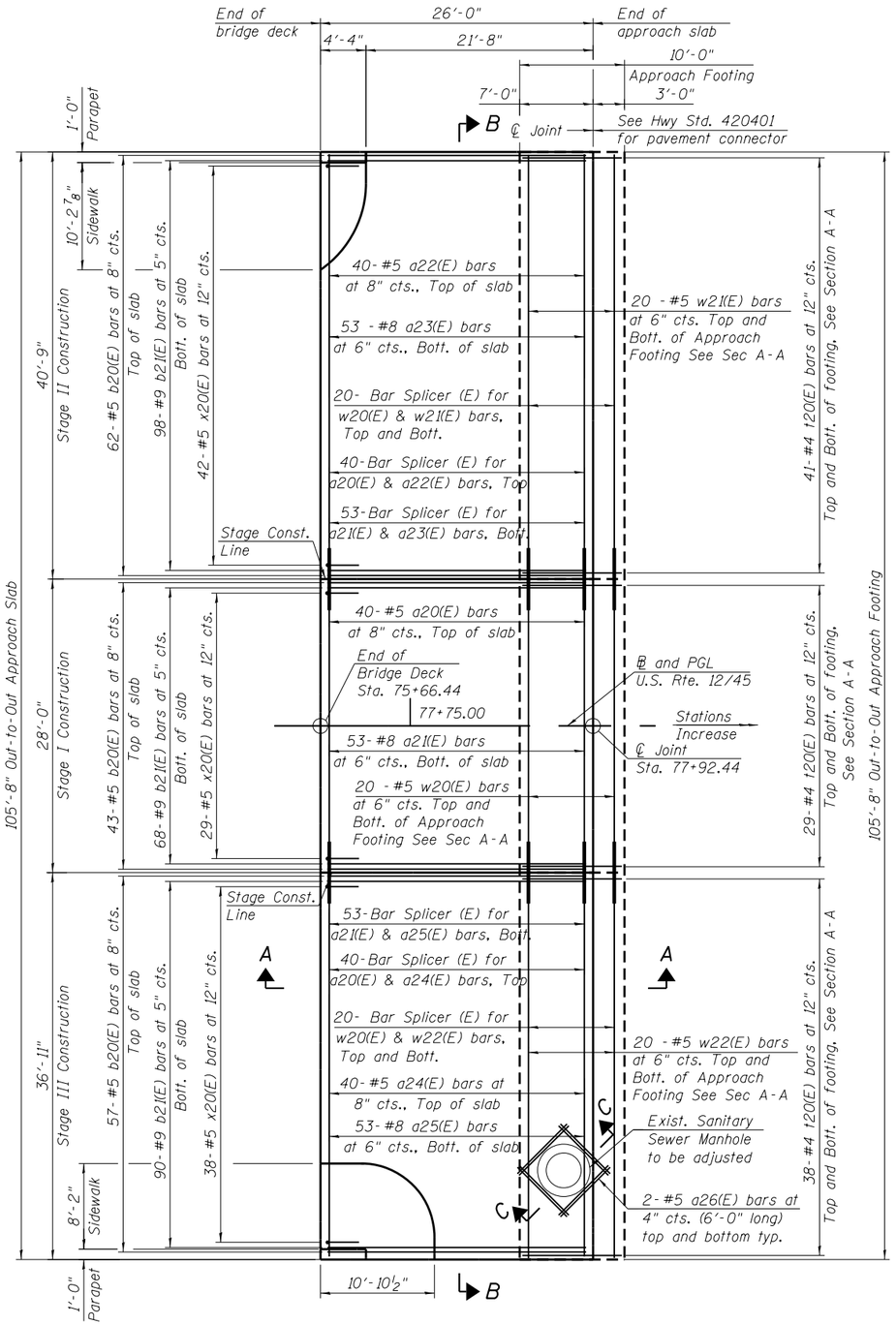
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOUTH APPROACH SLAB DETAILS
STRUCTURE NO. 016-1351

SCALE: | SHEET S-15 OF S-30 SHEETS | STA. | TO STA.

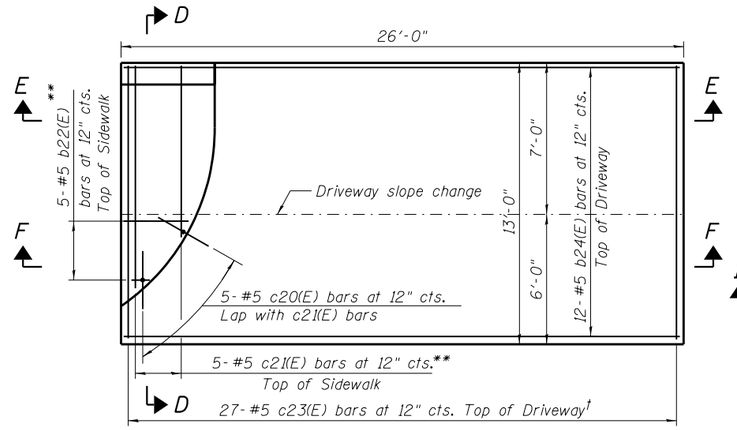
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	60
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

FILE PATH = P:\1111-532 IDOT PTB161 Item 8 Various\Various\Work Order #11 - US 12 over Addison Creek Culver\Structural\Sheets\0161351-60V22-S15-S Appr Slab Det.dgn

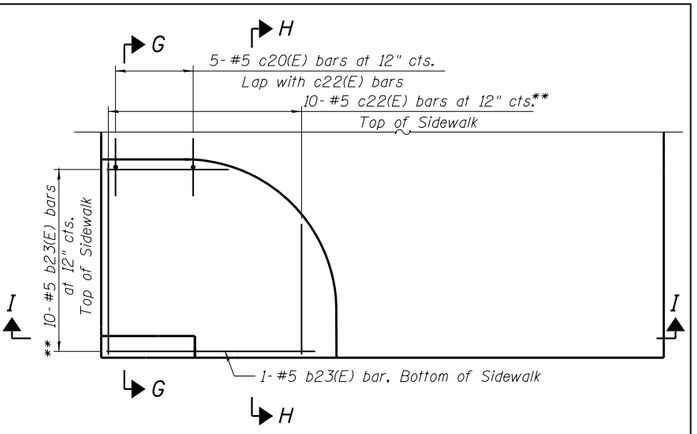


PLAN ➡ Z

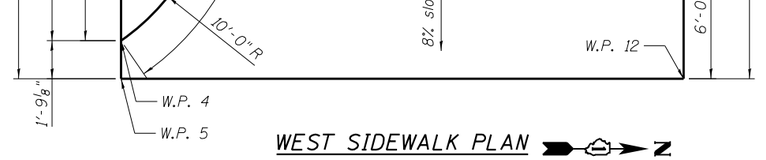
(Sidewalk reinforcement not shown for clarity)



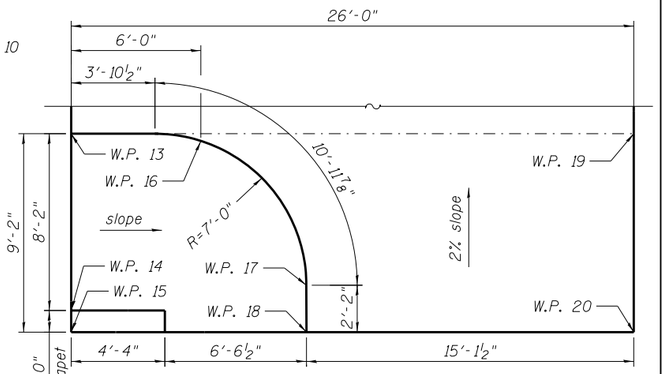
WEST SIDEWALK PLAN ➡ Z



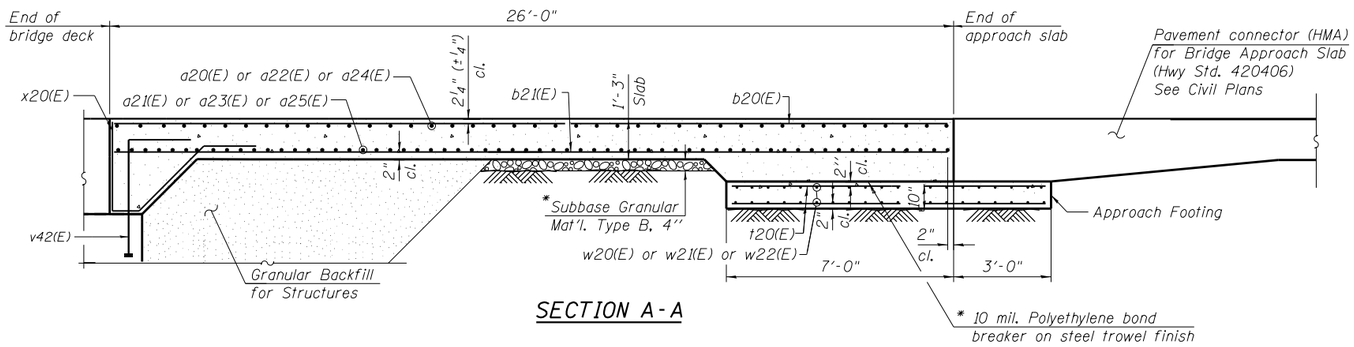
EAST SIDEWALK PLAN ➡ Z



WEST SIDEWALK PLAN ➡ Z



EAST SIDEWALK PLAN ➡ Z



SECTION A-A

* Cost included with Concrete Superstructure (Approach Slab).

** Order bars full length and cut in field to fit.

† Bend bars to fit.

NOTES:

1. For v42(E) bar details, Granular backfill for Structures and drainage treatment details, see Sheets S-24 and S-26.
2. For Sections A-A thru C-C, Bar diagrams and Bill of Material, see Sheet S-17.

FILE PATH = P:\1111-532 IDOT FTB161 Item 8 (Various-Various)\Work Order #11 - US 12 over Addison Creek Culvert\Structural\Sheets\0161351-60V22-S16-N Appr Slab Plan.dgn

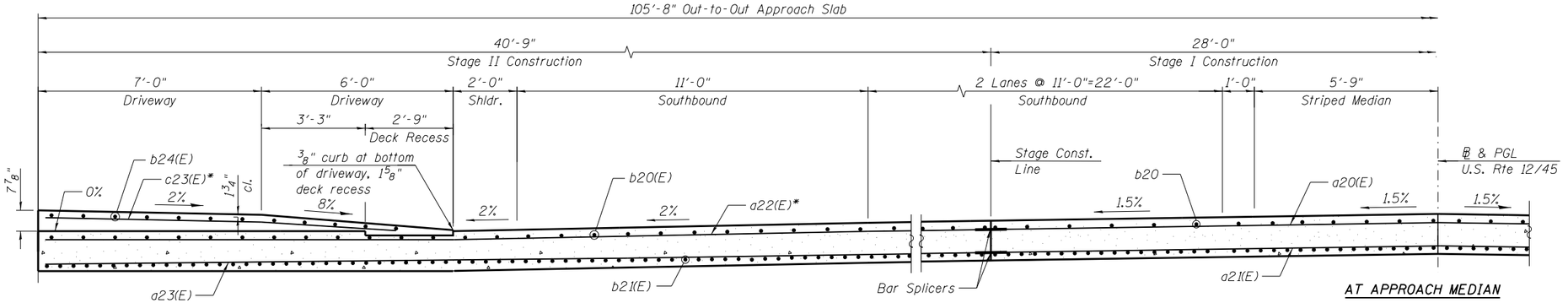
HBM ENGINEERING GROUP, LLC. CONSULTING & DESIGN INSPECTION & RATING RESEARCH & TESTING — 4415 WEST HARRISON ST. SUITE 231 HILLSIDE, IL 60162 PHONE: (708) 236-0900 FAX: (708) 236-0901	0161351-60V22-S16-N Appr Slab Plan.dgn	DESIGNED - SK, MAA	REVISED
	USER NAME = lisa.buntin	DRAWN - SK, MAA	REVISED
	PLOT SCALE = 14.00 ' / in.	CHECKED - LAB, MI	REVISED
	PLOT DATE = 12/8/2017	DATE - 12/08/2017	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

NORTH APPROACH SLAB PLAN
STRUCTURE NO. 016-1351

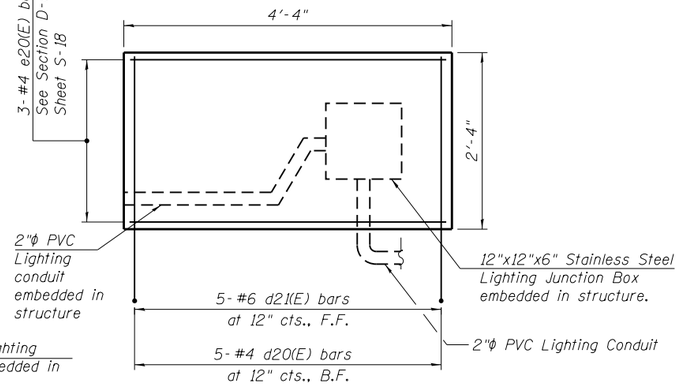
SCALE: | SHEET S-16 OF S-30 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	61
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			



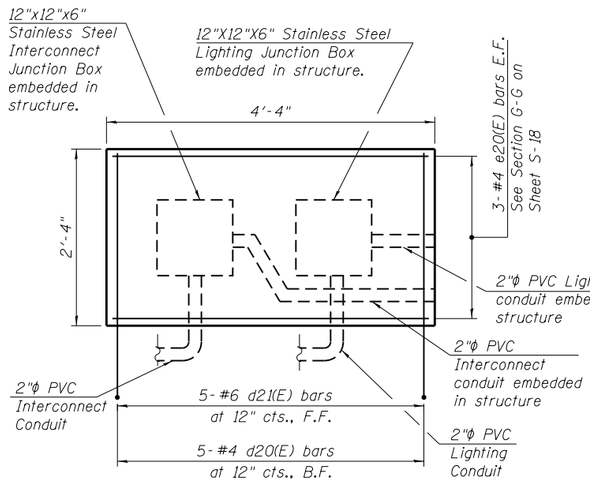
SECTION B-B

(See Plan for dimensions not shown)
(Sidewalks not shown for clarity)



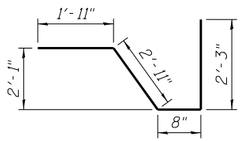
INSIDE ELEVATION OF WEST PARAPET

(Railing not shown for clarity)



INSIDE ELEVATION OF EAST PARAPET

(Railing not shown for clarity)



BARS x20(E)

BILL OF MATERIAL

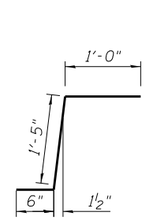
Bar	No.	Size	Length	Shape
a20(E)	40	#5	27'-9"	
a21(E)	53	#8	27'-9"	
a22(E)	40	#5	40'-6"	
a23(E)	53	#8	40'-6"	
a24(E)	40	#5	36'-8"	
a25(E)	53	#8	36'-8"	
a26(E)	16	#5	6'-0"	
b20(E)	162	#5	25'-8"	
b21(E)	256	#9	25'-8"	
b22(E)	5	#5	4'-0"	
b23(E)	11	#5	10'-6"	
b24(E)	12	#5	25'-8"	
c20(E)	10	#5	2'-11"	
c21(E)	5	#5	9'-11"	
c22(E)	10	#5	8'-10"	
c23(E)	27	#5	10'-0"	
d20(E)	10	#4	4'-8"	
d21(E)	10	#4	5'-0"	
e20(E)	12	#4	4'-0"	
t20(E)	216	#4	9'-8"	
w20(E)	40	#5	27'-9"	
w21(E)	40	#5	40'-6"	
w22(E)	40	#5	36'-8"	
x20(E)	109	#5	7'-9"	
Concrete Structures		Cu. Yd.	32.5	
Concrete Superstructure		Cu. Yd.	4.4	
Bridge Deck Grooving		Sq. Yd.	291	
Protective Coat		Sq. Yd.	311	
Concrete Superstructure (Approach Slab)		Cu. Yd.	131.6	
Reinforcement Bars, Epoxy Coated		Pound	53,670	

*Bend to fit in field

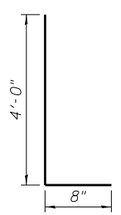
NOTES:

1. Approach slab shall be paid for as Concrete Superstructure (Approach Slab).
2. Approach footing concrete shall be paid for as Concrete Structures.
3. Approach sidewalk and parapets shall be paid for as Concrete Superstructure.
4. Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated.
5. The approach footing maximum applied service bearing pressure (Qmax) = 2.0 ksf.
6. For bar splicer details, see Sheet S-28.
7. Cost of excavation for approach footing included with Concrete Structures.

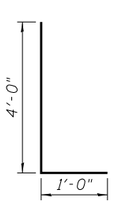
Minimum Bar Laps	
Bar	Lap
#4	2'-5"
#5	3'-0"
#6	3'-7"
#8	4'-9"
#9	5'-10"



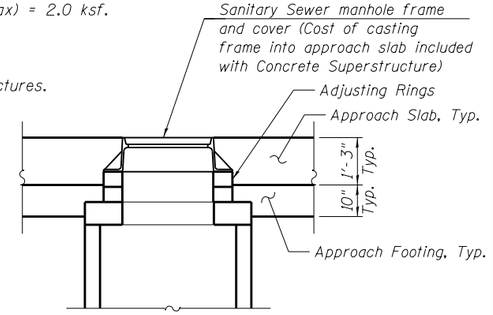
BAR c20(E)



BAR d20(E)



BAR d21(E)



SECTION C-C

(For sanitary sewer manhole, adjusting rings, frame and cover dimensions, details and quantities, see civil plans)

FILE PATH = P:\1111-532 IDOT PTB161 Item 8 (Various-Various)\Work Order #11 - US 12 over Addison Creek Culver\Structural\Sheets\0161351-60V22-S17-N Appr Slab Det.dgn

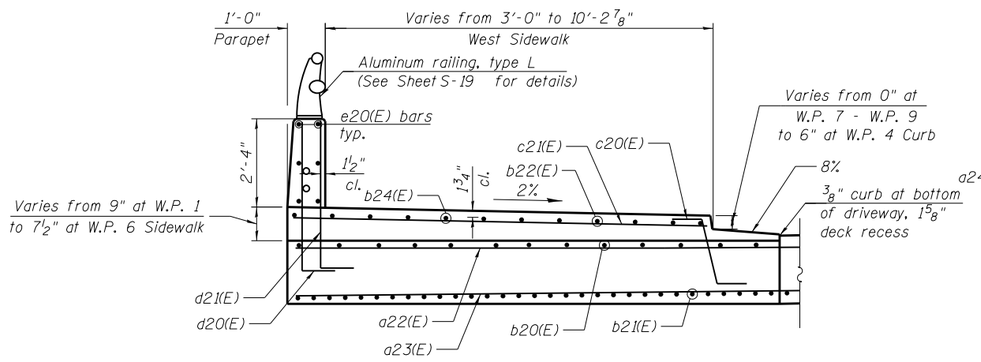
HBM ENGINEERING GROUP, LLC. CONSULTING & DESIGN INSPECTION & RATING RESEARCH & TESTING — 4415 WEST HARRISON ST. SUITE 231 HILLSIDE, IL 60162 PHONE: (708) 236-0900 FAX: (708) 236-0901	0161351-60V22-S17-N Appr Slab Det.dgn	DESIGNED - SK, MAA	REVISED
	USER NAME = lisa.buntin	DRAWN - SK, MAA	REVISED
	PLOT SCALE = 4.00 ' / in.	CHECKED - LAB, MI	REVISED
	PLOT DATE = 12/8/2017	DATE - 12/08/2017	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

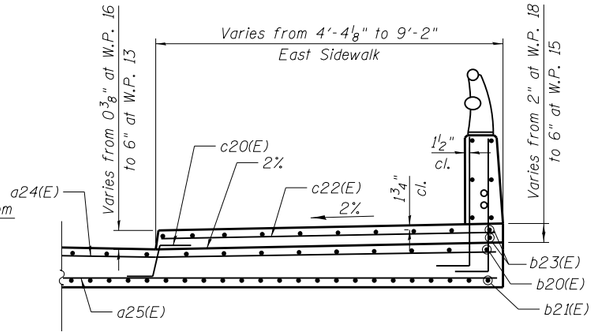
NORTH APPROACH SLAB DETAILS
STRUCTURE NO. 016-1351

SCALE: | SHEET S-17 OF S-30 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	62
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			



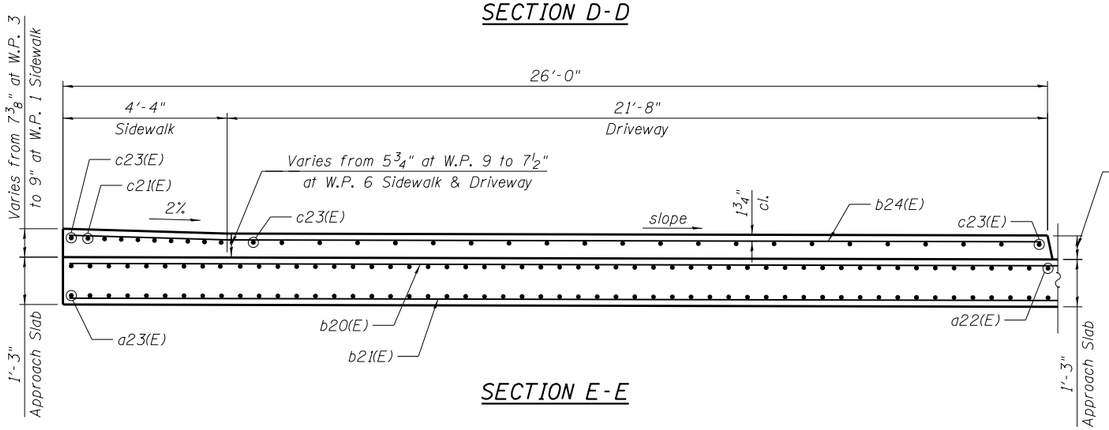
SECTION D-D



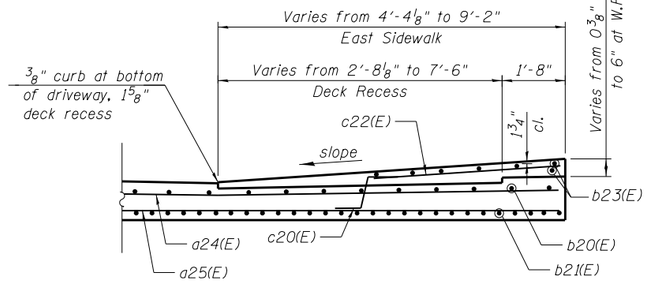
SECTION G-G

W.P. ELEVATIONS

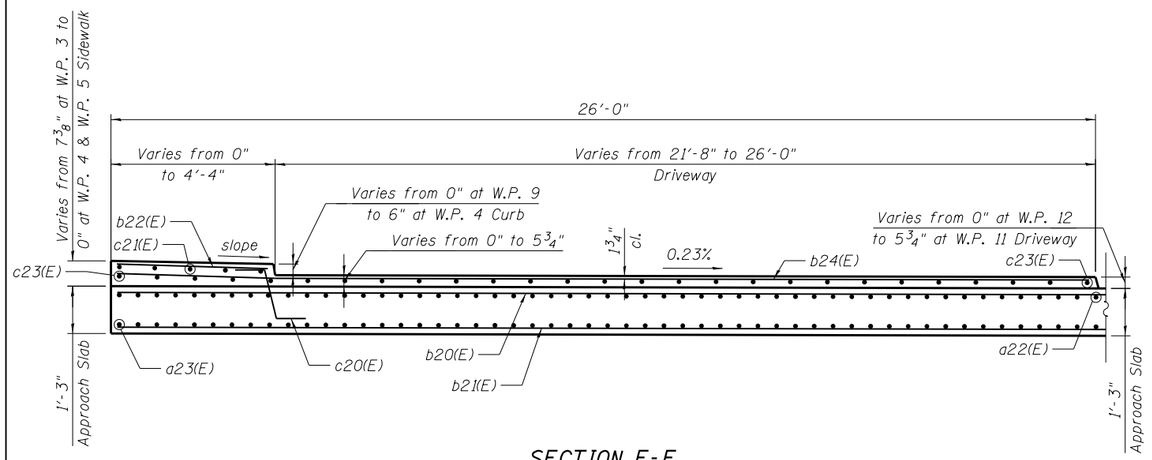
Location	Top of Driveway Elevation	Top of Sidewalk Elevation
W.P. 1	-	636.49
W.P. 2	-	636.49
W.P. 3	-	636.37
W.P. 4	635.82	636.29
W.P. 5	635.83	-
W.P. 6	636.40	636.40
W.P. 7	636.37	636.40
W.P. 8	636.31	636.34
W.P. 9	636.3	636.33
W.P. 10	636.39	-
W.P. 11	636.25	-
W.P. 12	635.77	-
W.P. 13	-	636.30
W.P. 14	-	636.46
W.P. 15	-	636.46
W.P. 16	635.8	635.83
W.P. 17	635.94	635.97
W.P. 18	635.98	636.01
W.P. 19	635.77	-
W.P. 20	635.95	-



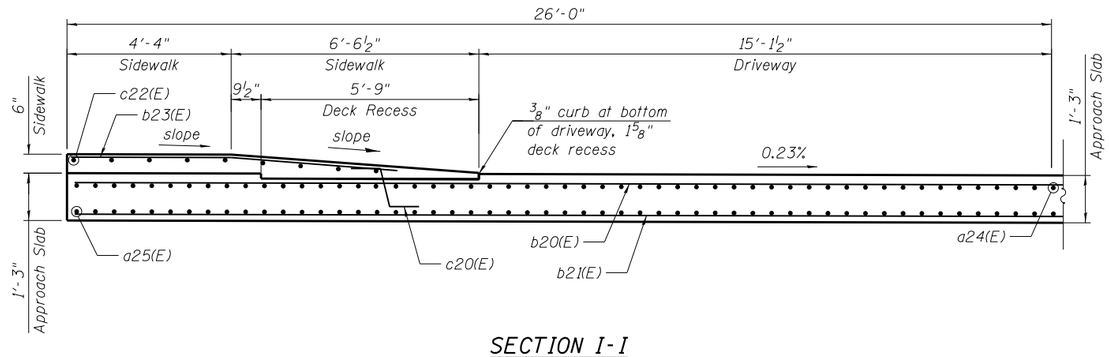
SECTION E-E



SECTION H-H



SECTION F-F



SECTION I-I

HBM ENGINEERING GROUP, LLC
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HILLSIDE, IL 60162
PHONE: (708) 236-0900
FAX: (708) 236-0901

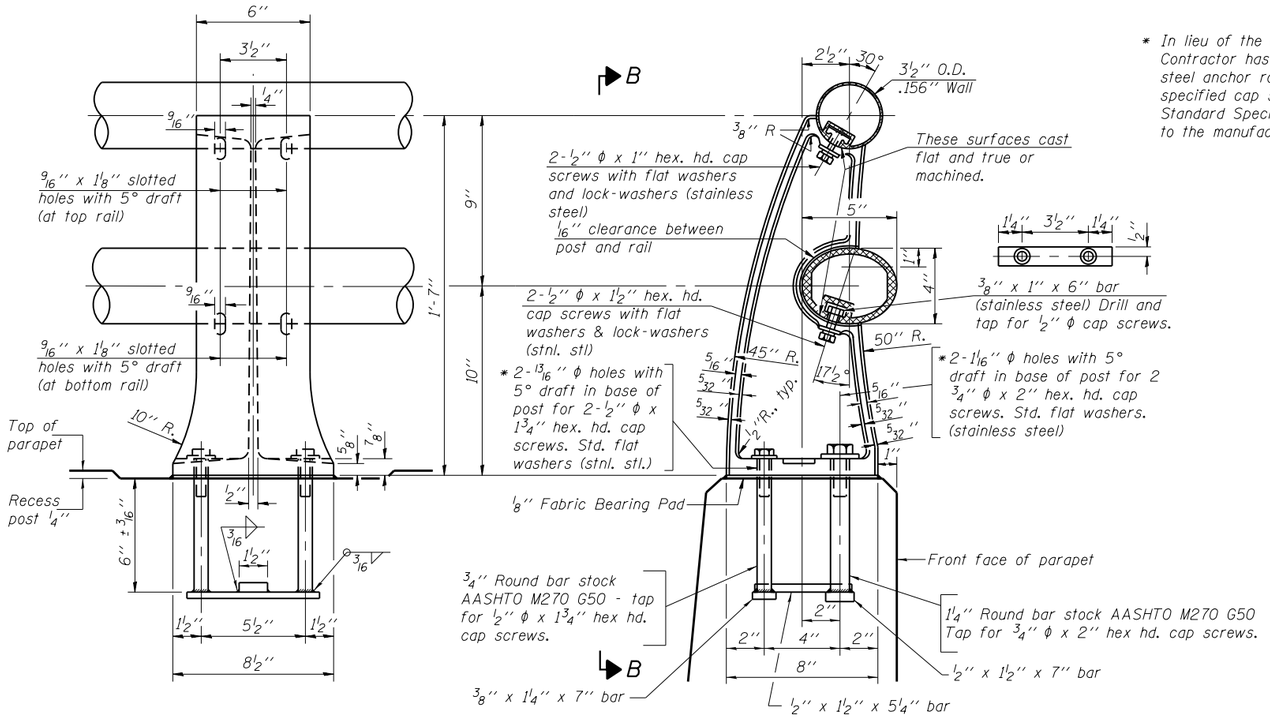
0161351-60V22-S18-N Appr Slab Det2.dgn	DESIGNED - LAB	REVISED
USER NAME = lisa.buntin	DRAWN - LAB	REVISED
PLOT SCALE = 4.0 '' / in.	CHECKED - MI	REVISED
PLOT DATE = 12/8/2017	DATE - 12/08/2017	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

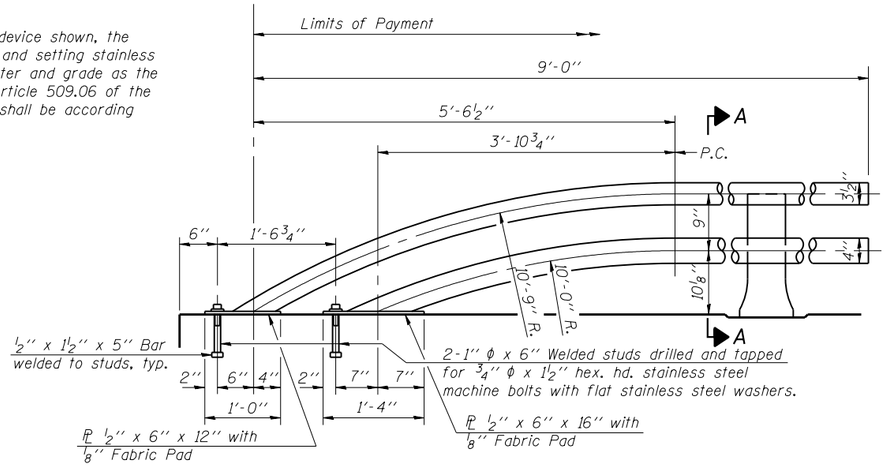
NORTH APPROACH SIDEWALK DETAILS
STRUCTURE NO. 016-1351

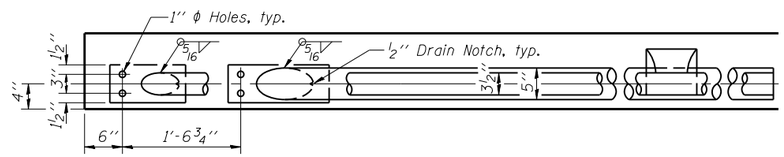
SCALE: | SHEET S-18 OF S-30 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	63
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			



* *In lieu of the cast-in-place anchor device shown, the Contractor has the option of drilling and setting stainless steel anchor rods of the same diameter and grade as the specified cap screws according to Article 509.06 of the Standard Specifications. Embedment shall be according to the manufacturer's specifications.*





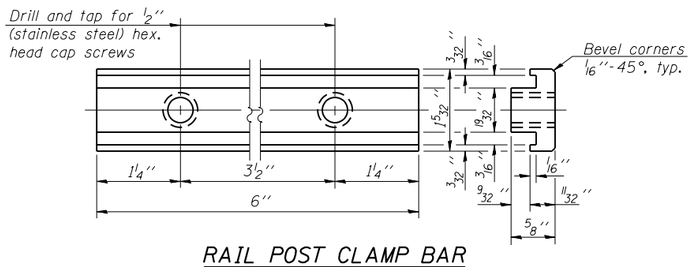
RAIL TERMINAL SECTION

Note: The end rail post shall be set back as required for the terminal rail section.

VIEW B-B

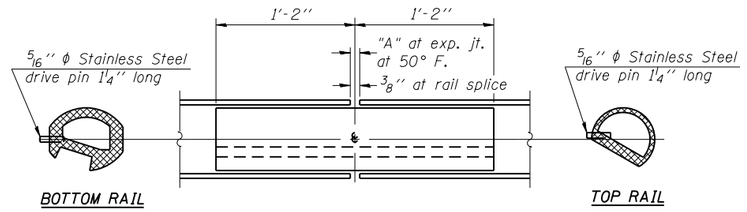
SECTION A-A

RAIL POST DETAILS



RAIL POST CLAMP BAR

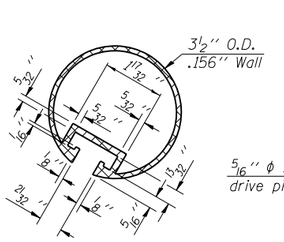
For Top Rail



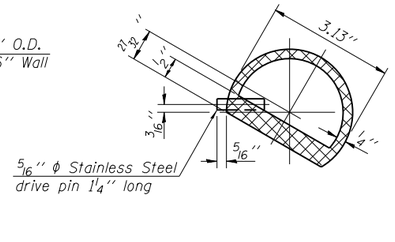
RAIL SPLICE

T	"A"
≤ 4''	2½''
> 4'' ≤ 6½''	3¾''
> 6½'' ≤ 9''	5''
> 9'' ≤ 13''	7''

T = Total movement at expansion joint as shown on the design plans.

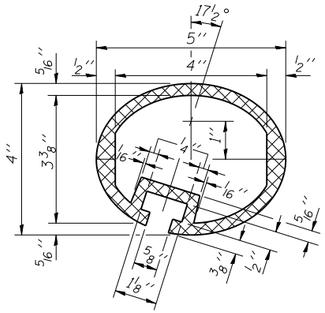


SECTION THRU TOP RAIL

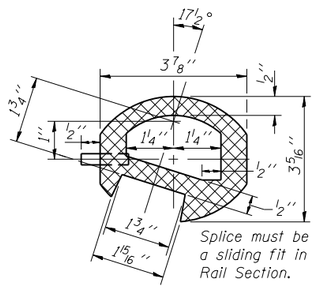


SECTION THRU SPLICE

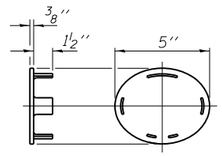
For Top Rail



SEC. THRU ELLIPTICAL RAIL SECTION

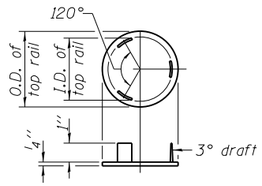


SEC. THRU SPLICE



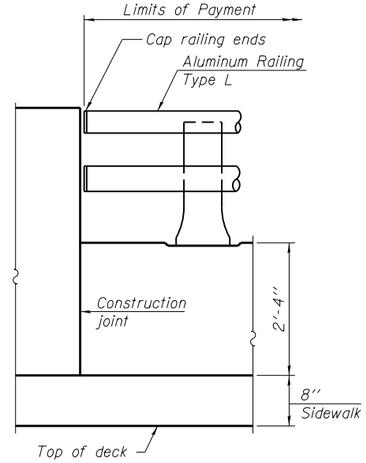
CAST END CAP

For bottom rail
DRIVE FIT TYPE



CAST END CAP

For top rail



RAIL END TREATMENT FOR TYPE 5 AND 6 TERMINAL

Notes:

All Posts shall be normal to parapet.

All joints in rail shall be spliced per detail.

All exposed rail ends shall be capped per detail.

Provide 1-⅛'' and 2-1/16'' Aluminum Shims for 25% of the Posts. Rail elements shall be parallel to Grade-high spots will be ground and low spots shimmed.

See Sheet S-11 of S-30 for rail post spacing.

BILL OF MATERIAL

Item	Unit	Quantity
Aluminum Railing, Type L	Foot	103

R-20 2-17-2017 (7'-0'' to 10'-0'' Post spacing)

HBM ENGINEERING GROUP, LLC.
CONSULTING & DESIGN
INSPECTION & RATING
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4415 WEST HARRISON ST.
SUITE 231
HILLSIDE, IL 60162
PHONE: (708) 236-0900
FAX: (708) 236-0901

0161351-60V22-S19-Railing.dgn	DESIGNED - KJD	REVISED
USER NAME = lisa.buntin	DRAWN - KJD	REVISED
PLOT SCALE = 0.17 '/ in.	CHECKED - LAB, MI	REVISED
PLOT DATE = 12/8/2017	DATE - 12/08/2017	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ALUMINUM RAILING, TYPE L
STRUCTURE NO. 016-1351

SCALE: | SHEET S-19 OF S-30 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	64
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

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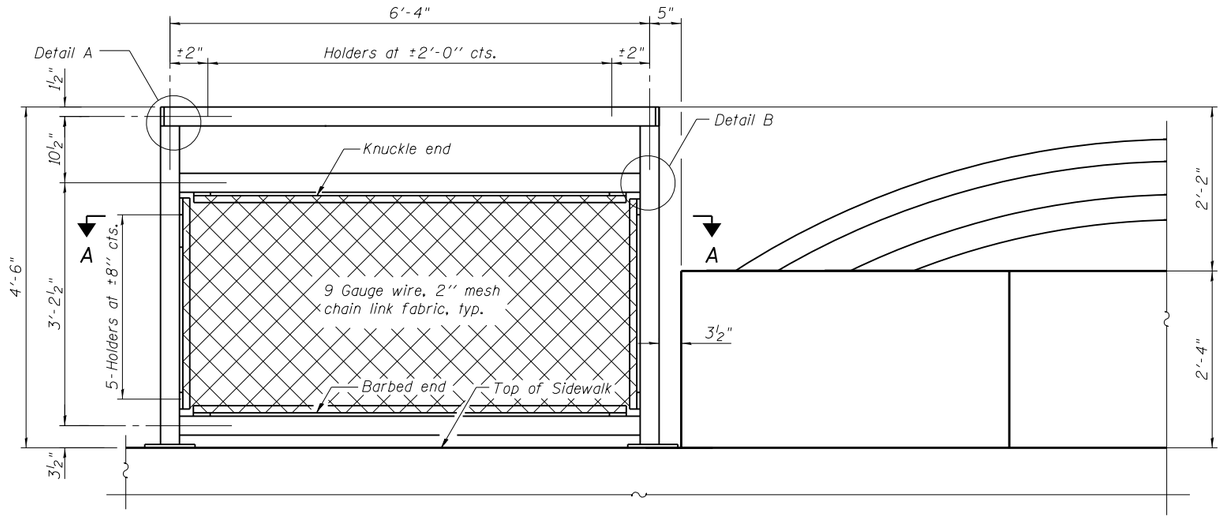
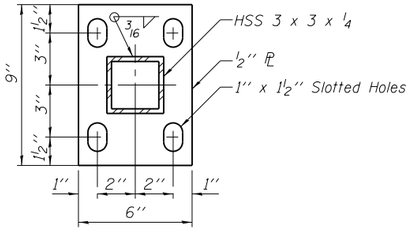
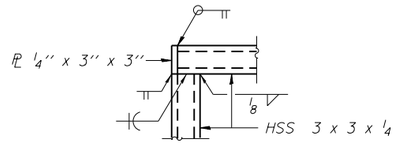


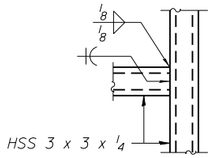
BICYCLE RAILING



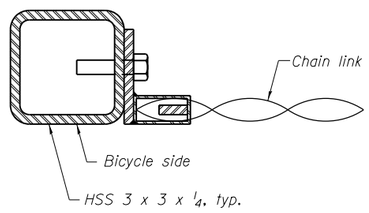
BASE PL



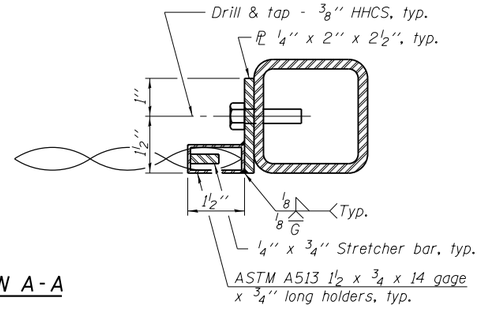
DETAIL A

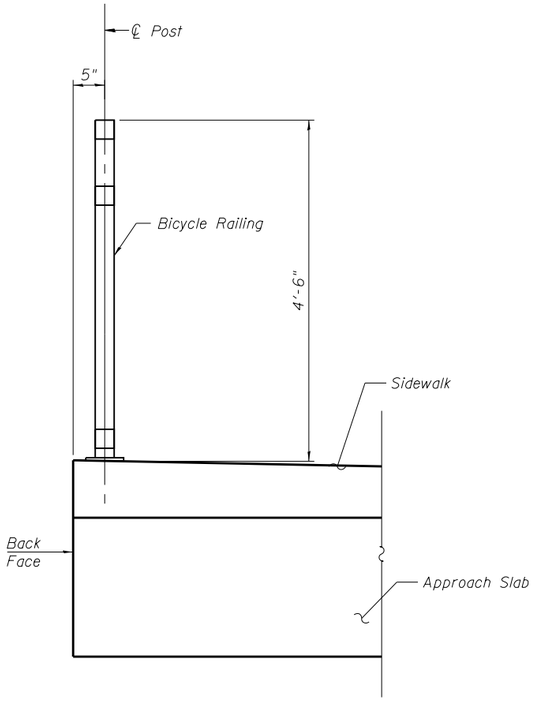


DETAIL B

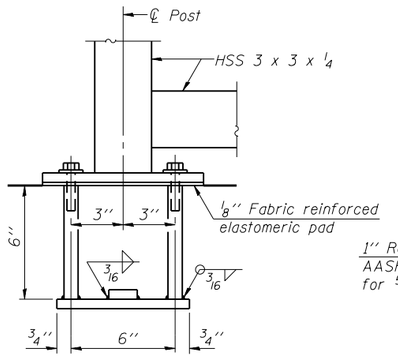


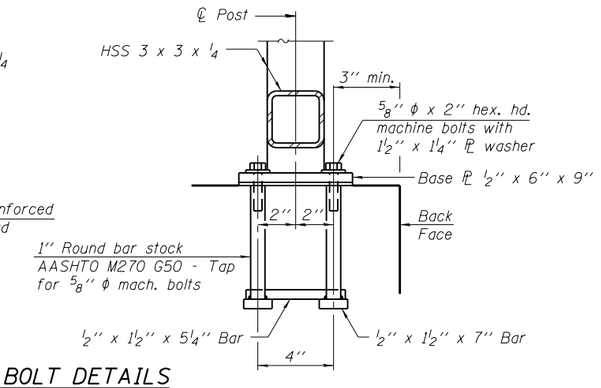
SECTION A-A





SECTION THRU SOUTH APPROACH SLAB WEST SIDEWALK





ANCHOR BOLT DETAILS

In lieu of the cast-in-place anchor device shown, the Contractor has the option of drilling and setting ⅝" ϕ anchor rods according to Article 509.06 of the Standard Specifications. Embedment shall be according to the manufacturer's specifications.

BILL OF MATERIAL

Item	Unit	Quantity
Bicycle Railing	Foot	7

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PHONE: (708) 236-0900
FAX: (708) 236-0901

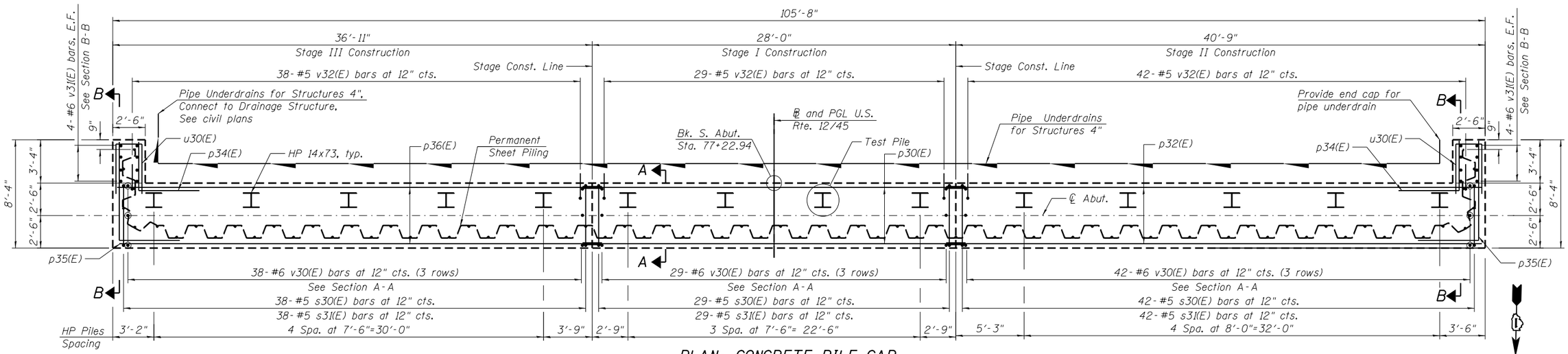
0161351-60V22-S20-Bicycle Railing.dgn	DESIGNED - KJD	REVISED
USER NAME = Stoyanka.Kotorakova	DRAWN - KJD	REVISED
PLOT SCALE = 2.00 ' / in.	CHECKED - LAB, MI	REVISED
PLOT DATE = 1/16/2018	DATE - 12/08/2017	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

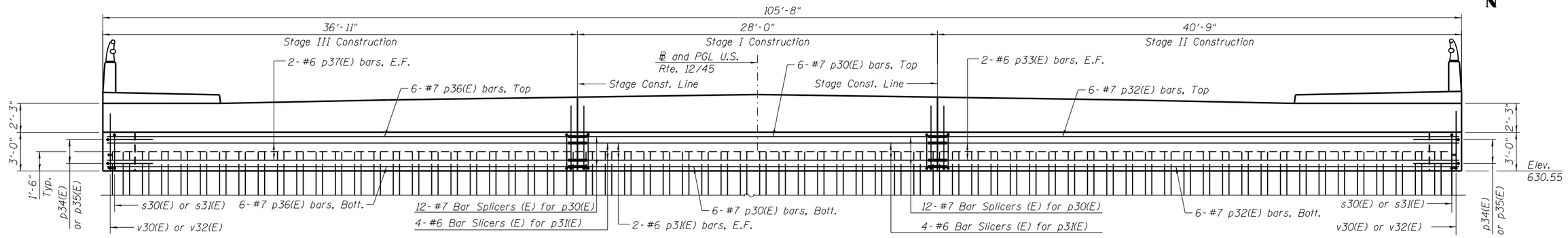
BICYCLE RAILING
STRUCTURE NO. 016-1351

SCALE: | SHEET S-20 OF S-30 SHEETS | STA. | TO STA.

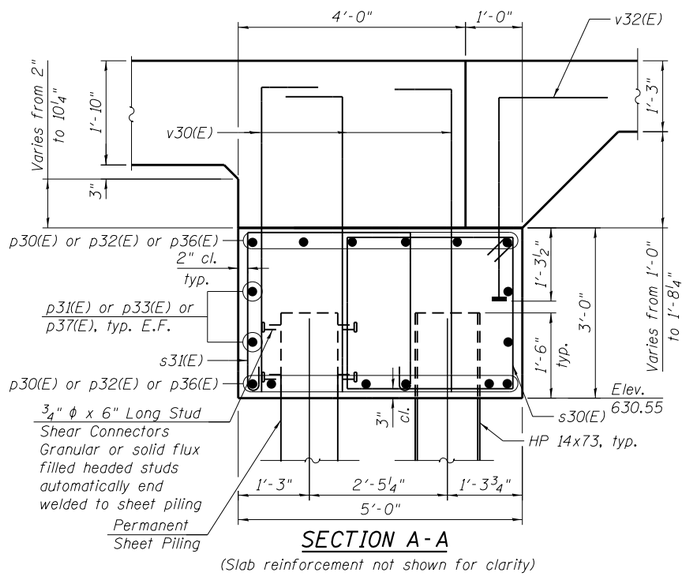
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	65
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			



PLAN- CONCRETE PILE CAP

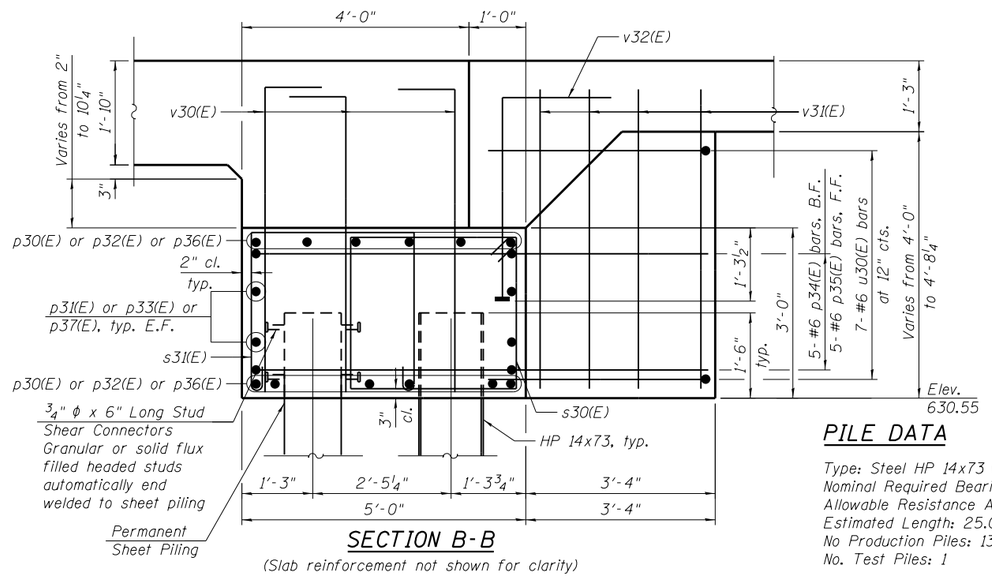


ELEVATION - CONCRETE PILE CAP



SECTION A-A

(Slab reinforcement not shown for clarity)



SECTION B-B

(Slab reinforcement not shown for clarity)

PILE DATA

Type: Steel HP 14x73 with pile shoes
Nominal Required Bearing: 578 kips
Allowable Resistance Available: 318 kips
Estimated Length: 25.0 ft
No Production Piles: 13
No. Test Piles: 1

NOTES:

1. For Sections and details, Bar Diagrams, and Bill of Material, see Sheet S-23.
2. For HP Pile details, see Sheet S-27.
3. For permanent sheet piling details, see Sheet S-22.
4. For Bar Splicers, see Sheet S-28.
5. All drainage system components shall extend 2'-0" from the end of the east wingwall and connected to a drainage structure. See civil plans for details.

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0161351-60V22-S21-S Abut Plan & Elev.dgn	DESIGNED - MA, SK	REVISED
USER NAME = lisa.buntin	DRAWN - SK, MAA	REVISED
PLOT SCALE = 8ø '1' / in.	CHECKED - LAB, MI	REVISED
PLOT DATE = 12/8/2017	DATE - 12/08/2017	REVISED

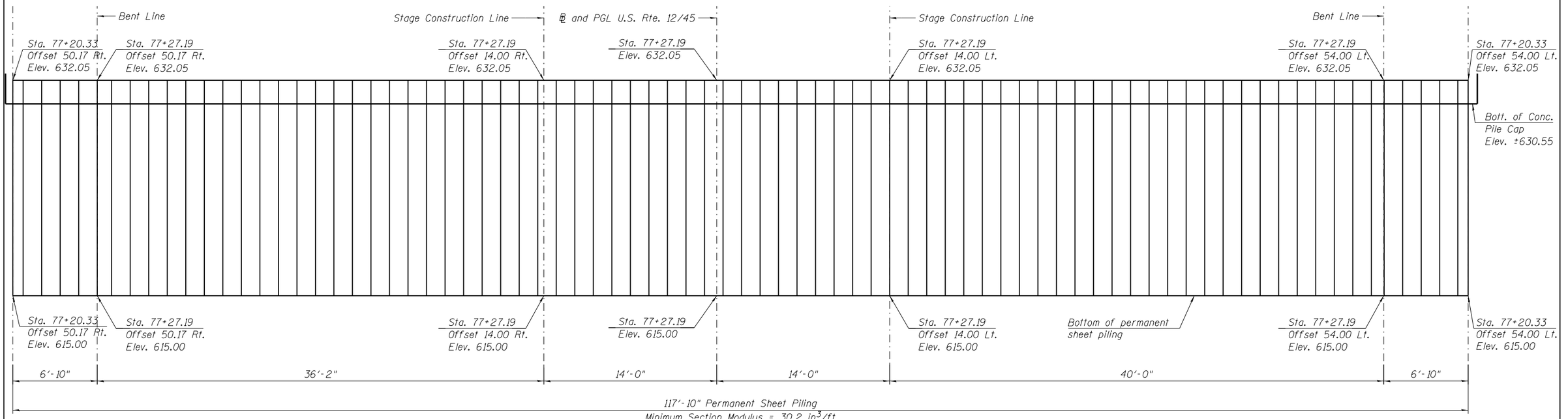
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOUTH ABUTMENT PLAN AND ELEVATION
STRUCTURE NO. 016-1351

SCALE: | SHEET S-21 OF S-30 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	66
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

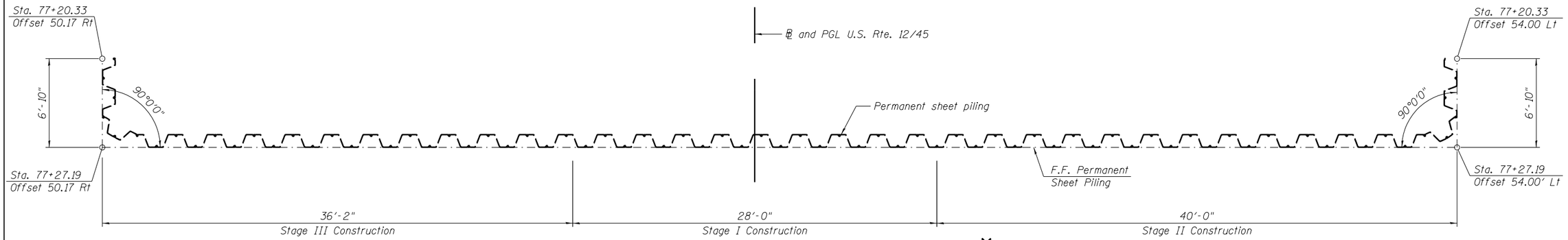
FILE PATH = P:\1111-532 IDOT PTB161 Item 8 (Various-Various)\Work Order #11 - US 12 over Addison Creek Culvert\Structural\Sheets\0161351-60V22-S21-S Abut Plan & Elev.dgn



ELEVATION - PERMANENT SHEET PILING

(Looking at Front Face)

(All dimensions and offsets are along Front Face of sheet piling)



PLAN - PERMANENT SHEET PILING

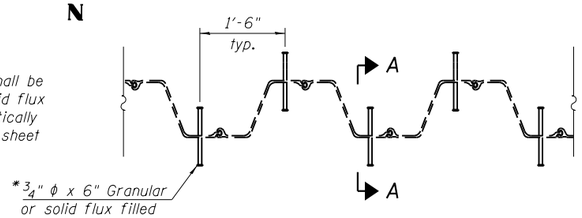
(Stud Shear Connectors are not shown for clarity)

N

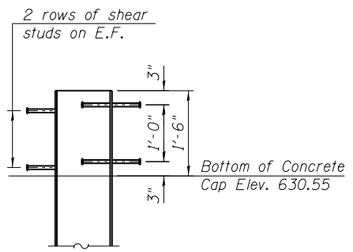
NOTES:

1. Station and offset points are provided along the front face of sheet piling.
2. Contractor shall submit for review and approval shop drawings that detail fabrication, erection, installation, etc. of permanent steel sheet piling.
3. Permanent sheet pile shall meet requirements of ASTM A328. Sheet pile shall have an effective section modulus meeting or exceeding section modulus shown. Use of an alternative section is subject to approval of the engineer. See special provisions for additional information.
4. Hard driving of permanent sheet piling in hardpan clay may be encountered below elevation 619.00. The Contractor shall provide the appropriate driving equipment for such.
5. For additional notes, see Sheet S-21.

*Stud Shear Connectors shall be ¾" ϕ x 6" granular or solid flux filled headed studs automatically end welded in the field to sheet piling.



SECTION THRU CAP



SECTION A-A

HBM ENGINEERING GROUP, LLC.
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RESEARCH & TESTING
4415 WEST HARRISON ST. SUITE 231
HILLSIDE, IL 60162
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FAX: (708) 236-0901

0161351-60V22-S22-S Abut Sec & Det 1.dgn	DESIGNED - MA, SK	REVISED
USER NAME = lisa.buntin	DRAWN - SK, MAA	REVISED
PLOT SCALE = 8ø '-' / in.	CHECKED - LAB, MI	REVISED
PLOT DATE = 12/8/2017	DATE - 12/08/2017	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOUTH ABUTMENT PERMANENT SHEET PILING
STRUCTURE NO. 016-1351

SCALE: | SHEET S-22 OF S-30 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	67
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

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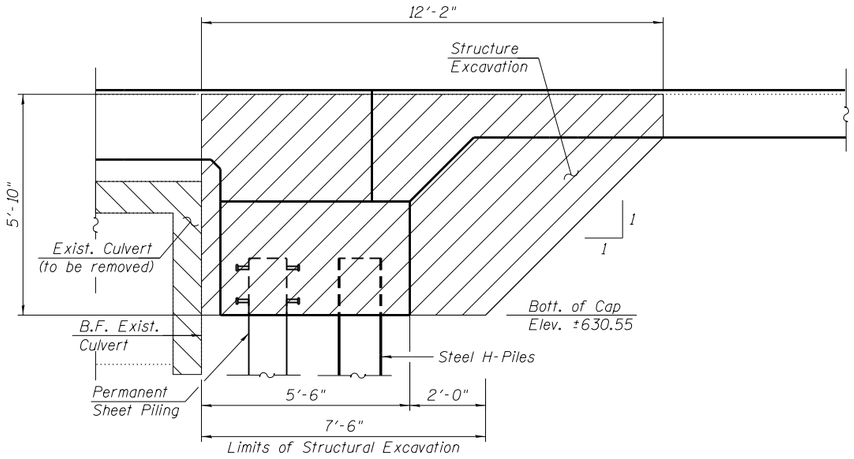
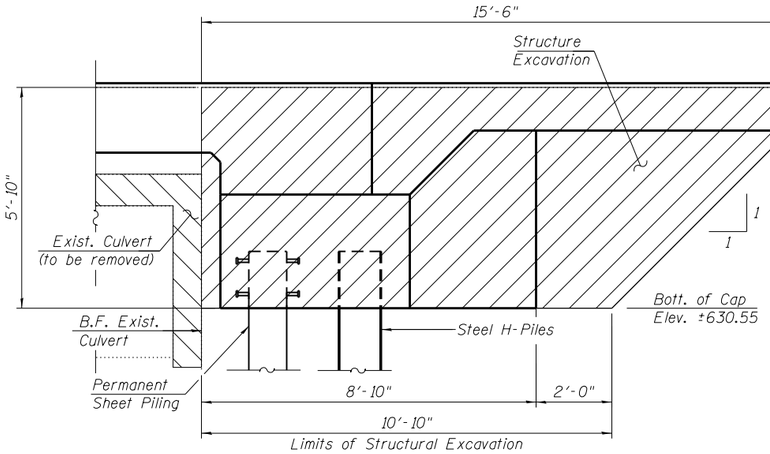
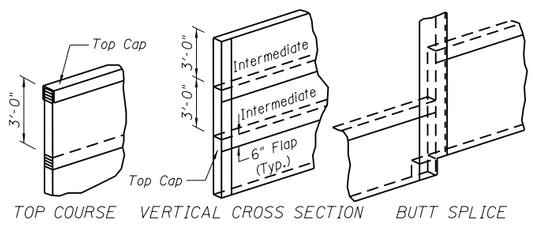


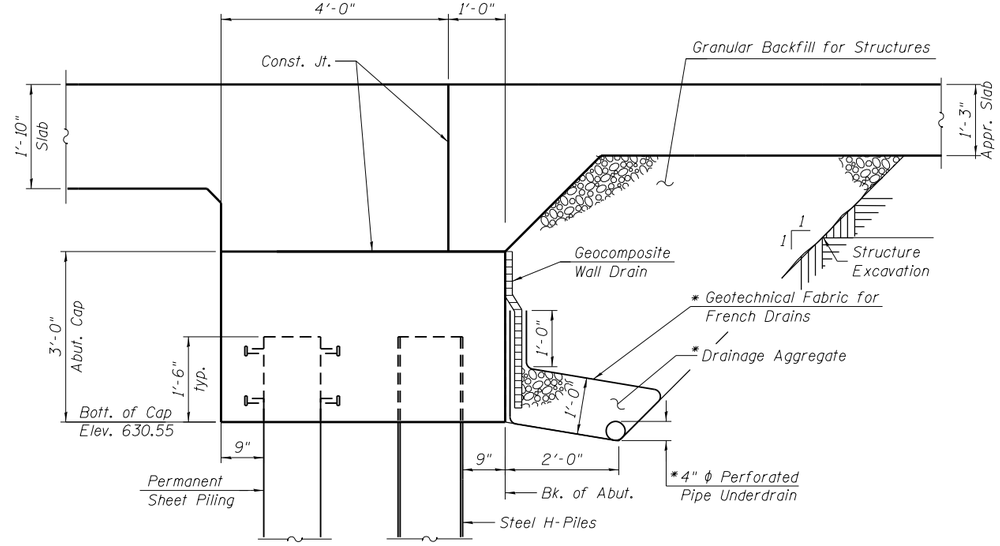
STRUCTURE EXCAVATION- ABUTMENT



STRUCTURE EXCAVATION- WINGWALL



GEOCOMPOSITE WALL DRAIN DETAILS



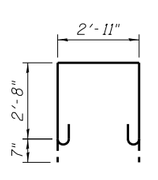
TYPICAL SECTION THRU ABUTMENT

*Cost included with "Pipe Underdrains for Structures, 4"

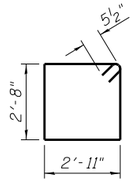
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
p30(E)	12	#7	27'-10"	
p31(E)	4	#6	27'-10"	
p32(E)	12	#7	40'-6"	
p33(E)	4	#6	40'-6"	
p34(E)	10	#6	7'-2"	
p35(E)	10	#6	11'-10"	
p36(E)	12	#7	36'-8"	
p37(E)	4	#6	36'-8"	
s30(E)	109	#5	12'-1"	
s31(E)	109	#5	9'-5"	
u30(E)	14	#6	9'-10"	
v30(E)	327	#6	5'-10"	
v31(E)	16	#6	4'-10"	
v32(E)	109	#5	3'-10"	

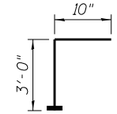
Structure Excavation	Cu. Yd.	239
Concrete Structures	Cu. Yd.	61.1
Stud Shear Connectors	Each	304
Reinforcement Bars, Epoxy Coated	Pound	9,560
Furnishing Steel Piles HP14x73	Foot	325
Driving Piles	Foot	325
Test Pile Steel HP14x73	Each	1
Pile Shoes	Each	14
Permanent Sheet Piling	Sq. Ft.	2,010
Geocomposite Wall Drain	Sq. Yd.	38
Granular Backfill for Structures	Cu. Yd.	82
Pipe Underdrains for Structures, 4"	Foot	116



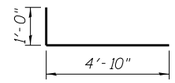
BAR s31(E)



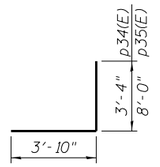
BAR s30(E)



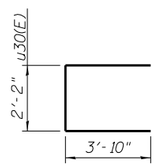
BAR v32(E)
(Headed)



BAR v30(E)



BARS p34(E) or p35(E)



BAR u30(E)

Minimum Bar Laps	
Bar	Lap
#5	3'-2"
#6	3'-10"
#7	4'-5"
#8	5'-1"

NOTE:

1. For notes, see Sheets S-21 and S-22.

LEGEND:

- Granular Backfill for Structures
- Structure Excavation
- Removal of Existing Structures

FILE PATH = P:\111-532 IDOT PTB161 Item 8 (Various-Various)\Work Order #11 - US 12 over Addison Creek Culvert\Structural\Sheets\01613S1-60V22-S23-S Abut Sec & Det II.dgn

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PHONE: (708) 236-0900
FAX: (708) 236-0901

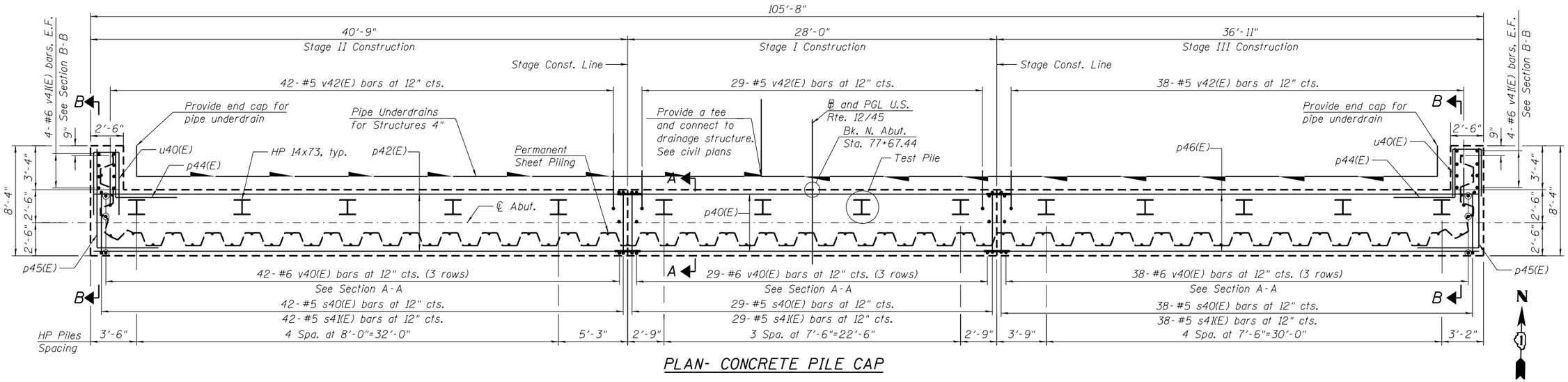
01613S1-60V22-S23-S Abut Sec & Det II.dgn	DESIGNED - MA, SK	REVISED
USER NAME = lisa.buntin	DRAWN - SK, MAA	REVISED
PLOT SCALE = 4ø '-' / in.	CHECKED - LAB, MI	REVISED
PLOT DATE = 12/8/2017	DATE - 12/08/2017	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

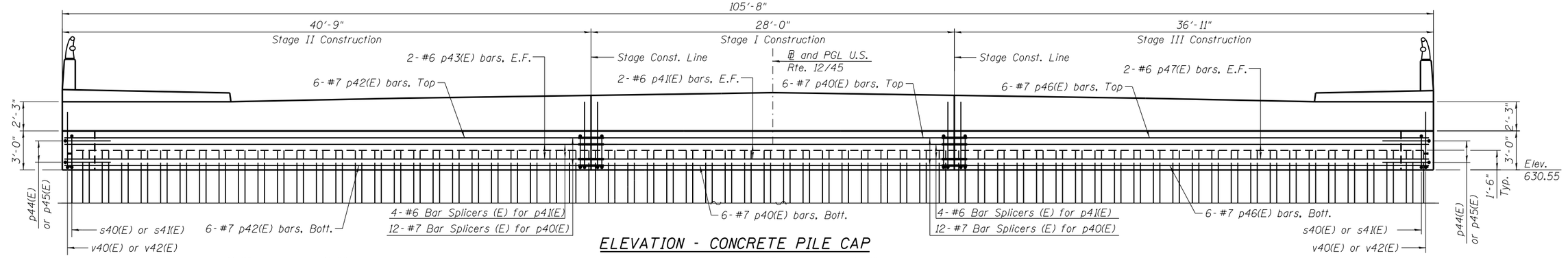
SOUTH ABUTMENT SECTIONS AND DETAILS
STRUCTURE NO. 016-1351

SCALE: | SHEET S-23 OF S-30 SHEETS | STA. | TO STA.

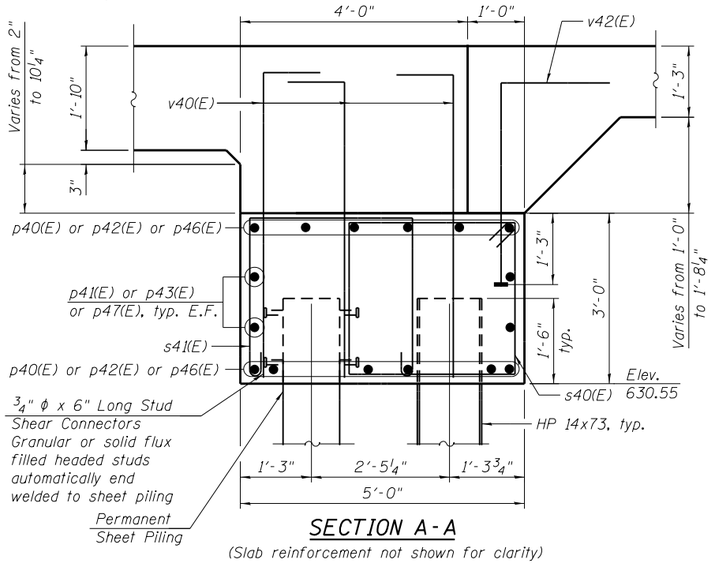
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	68
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			



PLAN- CONCRETE PILE CAP

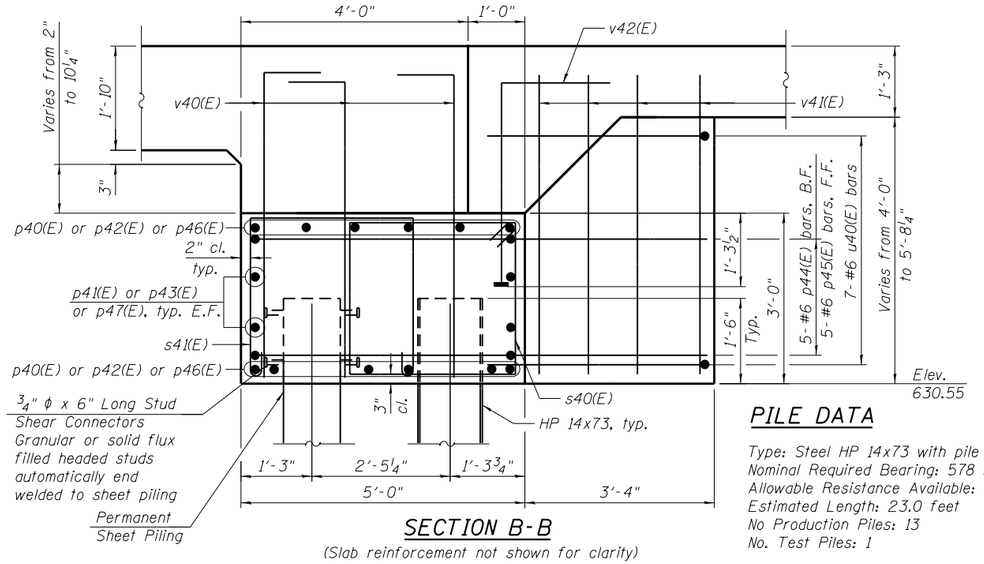


ELEVATION - CONCRETE PILE CAP



SECTION A-A

(Slab reinforcement not shown for clarity)



SECTION B-B

(Slab reinforcement not shown for clarity)

NOTES:

1. For Sections and details, Bar Diagrams, and Bill of Material, see Sheet S-26.
2. For HP Pile details, see Sheet S-27.
3. For permanent sheet piling details, see Sheet S-25.
4. For Bar Splicers, see Sheet S-28.
5. All drainage system components shall extend 2'-0" from the end of the east wingwall and connected to a drainage structure. See civil plans for details.

PILE DATA

Type: Steel HP 14x73 with pile shoes
Nominal Required Bearing: 578 kips
Allowable Resistance Available: 318 kips
Estimated Length: 23.0 feet
No Production Piles: 13
No. Test Piles: 1

FILE PATH = P:\1111-532 IDOT PTB161 Item 8 (Various-Various)\Work Order #11 - US 12 over Addison Creek Culvert\Structural\Sheets\01613S1-60V22-S24-N Abut Plan & Elev.dgn

HBM ENGINEERING GROUP, LLC.
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PHONE: (708) 236-0900
FAX: (708) 236-0901

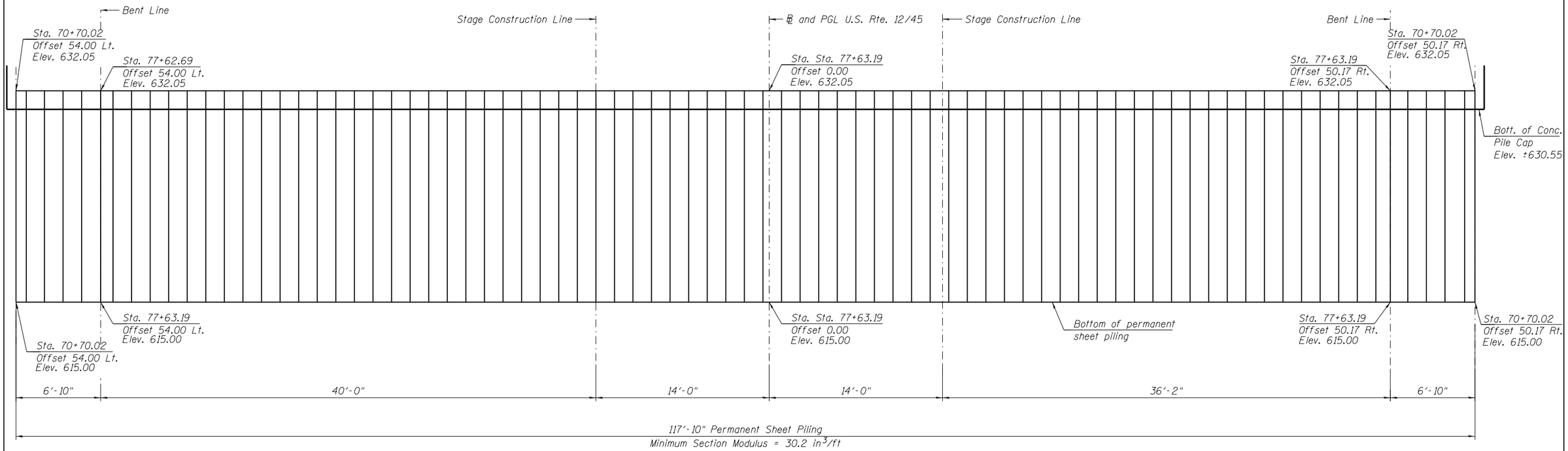
01613S1-60V22-S24-N Abut Plan & Elev.dgn	DESIGNED - MA, SK	REVISED
USER NAME = lisa.buntin	DRAWN - SK, MAA	REVISED
PLOT SCALE = 8ø '' / in.	CHECKED - LAB, MI	REVISED
PLOT DATE = 12/8/2017	DATE - 12/08/2017	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

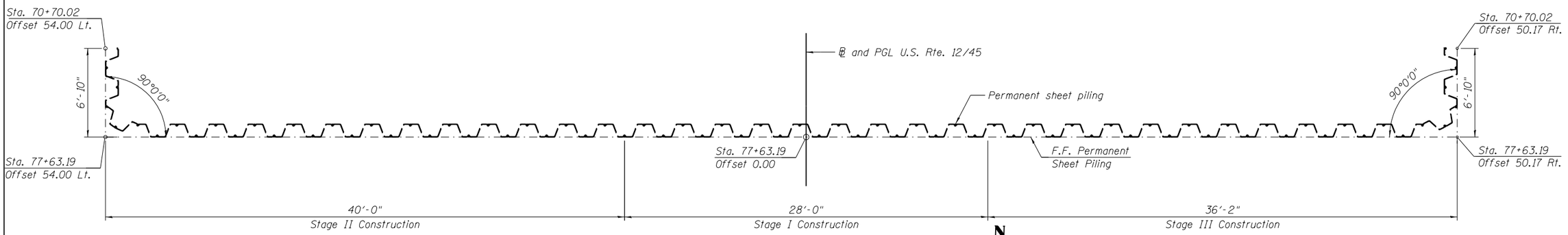
NORTH ABUTMENT PLAN AND ELEVATION
STRUCTURE NO. 016-1351

SCALE: | SHEET S-24 OF S-30 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	69
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			



ELEVATION - PERMANENT SHEET PILING



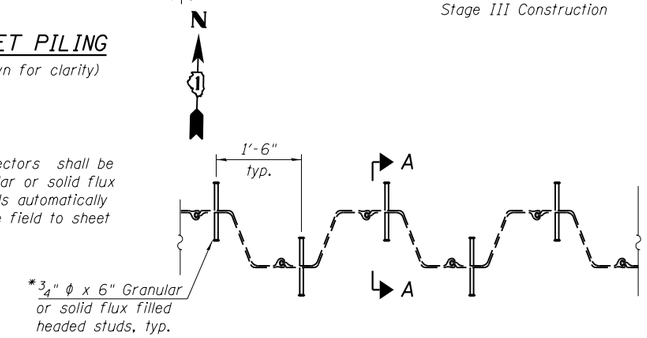
PLAN - PERMANENT SHEET PILING

(Stud Shear Connectors are not shown for clarity)

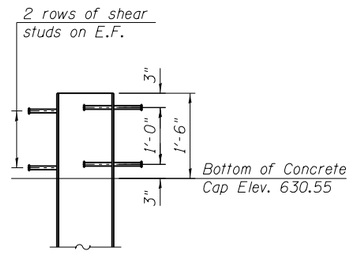
NOTES:

1. Station and offset points are provided along the front face of sheet piling.
2. Contractor shall submit for review and approval shop drawings that detail fabrication, erection, installation, etc. of permanent steel sheet piling.
3. Permanent sheet pile shall meet requirements of ASTM A328. Sheet pile shall have an effective section modulus meeting or exceeding section modulus shown. Use of an alternative section is subject to approval of the engineer. See special provisions for additional information.
4. Hard driving of permanent sheet piling in hardpan clay maybe encountered below elevation 619.00. The Contractor shall provide the appropriate driving equipment for such.
5. For additional notes, see Sheet S-24.

*Stud Shear Connectors shall be $\frac{3}{4}$" ϕ x 6" granular or solid flux filled headed studs automatically end welded in the field to sheet piling.



SECTION THRU CAP



SECTION A-A

FILE PATH = P:\1111-532 IDOT PTB161 Item 8 (Various-Various)\Work Order #11 - US 12 over Addison Creek Culvert\Structural\Sheets\01613S1-60V22-S25-N Abut Sec & Det 1.dgn

HBM ENGINEERING GROUP, LLC. CONSULTING & DESIGN INSPECTION & RATING RESEARCH & TESTING — 4415 WEST HARRISON ST. SUITE 231 HILLSIDE, IL 60162 PHONE: (708) 236-0900 FAX: (708) 236-0901			
01613S1-60V22-S25-N Abut Sec & Det 1.dgn	DESIGNED - MA, SK	REVISED	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
USER NAME = lisa.buntin	DRAWN - SK, MAA	REVISED	
PLOT SCALE = 8.00 ' / in.	CHECKED - LAB, MI	REVISED	
PLOT DATE = 12/8/2017	DATE - 12/08/2017	REVISED	

NORTH ABUTMENT PERMANENT SHEET PILING
STRUCTURE NO. 016-1351

SCALE: | SHEET S-25 OF S-30 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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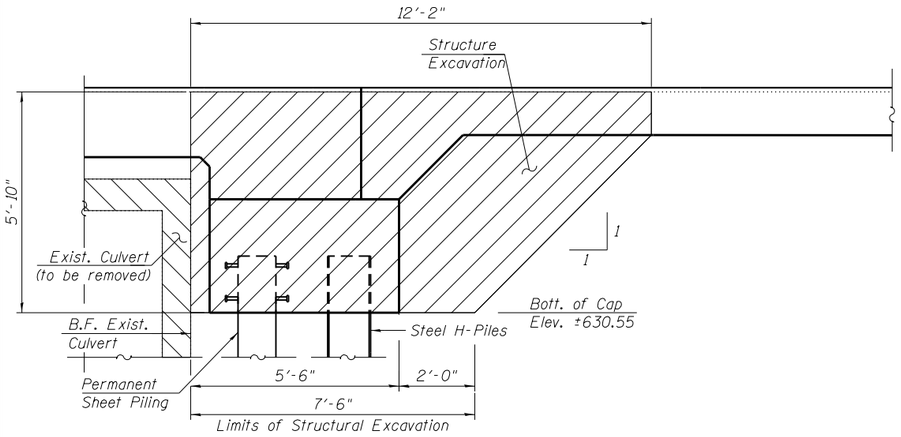
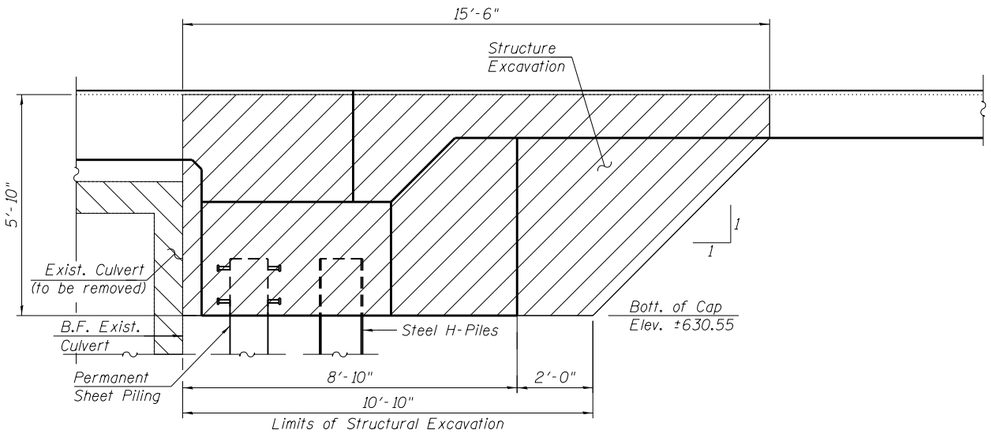
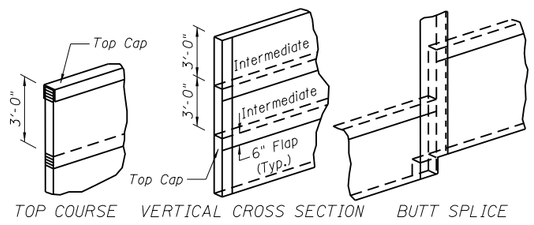
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			



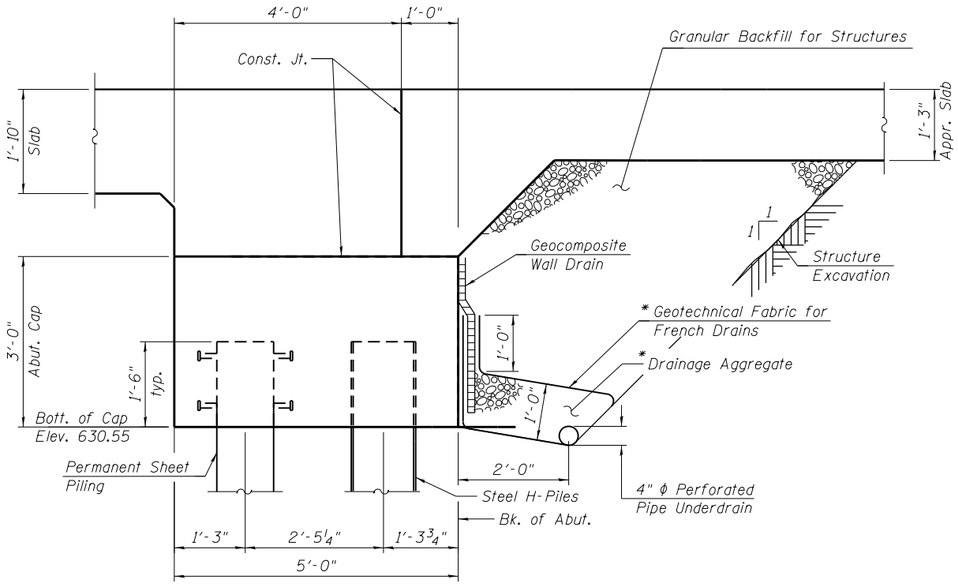
STRUCTURE EXCAVATION- ABUTMENT



STRUCTURE EXCAVATION- WINGWALL



GEOCOMPOSITE WALL DRAIN DETAILS

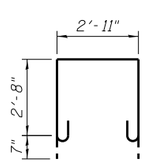


TYPICAL SECTION THRU ABUTMENT

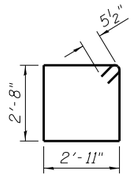
*Cost included with "Pipe Underdrains for Structures, 4"

BILL OF MATERIAL

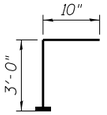
Bar	No.	Size	Length	Shape
p40(E)	12	#7	27'-10"	
p41(E)	4	#6	27'-10"	
p42(E)	12	#7	40'-6"	
p43(E)	4	#6	40'-6"	
p44(E)	10	#6	7'-2"	
p45(E)	10	#6	11'-10"	
p46(E)	12	#7	36'-8"	
p47(E)	4	#6	36'-8"	
s40(E)	109	#5	12'-1"	
s41(E)	109	#5	9'-5"	
u40(E)	14	#6	9'-10"	
v40(E)	327	#6	5'-10"	
v41(E)	16	#6	4'-10"	
v42(E)	109	#5	3'-10"	
Structure Excavation		Cu. Yd.	239	
Concrete Structures		Cu. Yd.	61.1	
Stud Shear Connectors		Each	304	
Reinforcement Bars, Epoxy Coated		Pound	9,560	
Furnishing Steel Piles HP14x73		Foot	299	
Driving Piles		Foot	299	
Test Pile Steel HP14x73		Each	1	
Pile Shoes		Each	14	
Permanent Sheet Piling		Sq. Ft.	2,010	
Geocomposite Wall Drain		Sq. Yd.	38	
Granular Backfill for Structures		Cu. Yd.	82	
Pipe Underdrains for Structures, 4"		Foot	116	



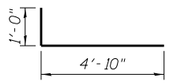
BAR s41(E)



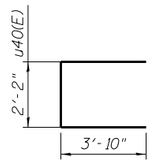
BAR s40(E)



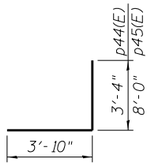
BAR v42(E)
(Headed)



BAR v40(E)



BAR u40(E)



BARS p44(E) or p45(E)

Minimum Bar Laps	
Bar	Lap
#5	3'-2"
#6	3'-10"
#7	4'-5"
#8	5'-1"

NOTE:

1. For additional notes, see Sheets S-24 and S-25.

LEGEND:

- Granular Backfill for Structures
- Structure Excavation
- Concrete Removal

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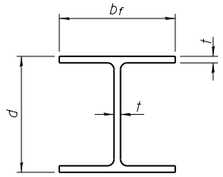
0161351-60V22-S26-N Abut Sec & Det II.dgn	DESIGNED - MA, SK	REVISED
USER NAME = lisa.buntin	DRAWN - SK, MAA	REVISED
PLOT SCALE = 4.00 ' / in.	CHECKED - LAB, MI	REVISED
PLOT DATE = 12/8/2017	DATE - 12/08/2017	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

NORTH ABUTMENT SECTIONS AND DETAILS
STRUCTURE NO. 016-1351

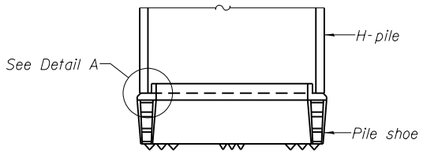
SCALE: | SHEET S-26 OF S-30 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	71
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

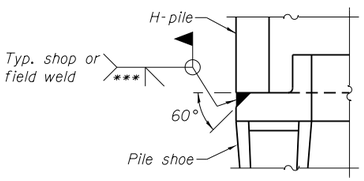


STEEL PILE TABLE

Designation	Depth d	Flange width b_f	Web and Flange thickness t	Encasement diameter A
HP 14x117	$14\frac{1}{4}''$	$14\frac{7}{8}''$	$\frac{13}{16}''$	30''
x102	14''	$14\frac{3}{4}''$	$\frac{11}{16}''$	30''
x89	$13\frac{7}{8}''$	$14\frac{3}{4}''$	$\frac{5}{8}''$	30''
x73	$13\frac{5}{8}''$	$14\frac{5}{8}''$	$\frac{1}{2}''$	30''
HP 12x84	$12\frac{1}{4}''$	$12\frac{1}{4}''$	$\frac{11}{16}''$	24''
x74	$12\frac{1}{8}''$	$12\frac{1}{4}''$	$\frac{5}{8}''$	24''
x63	12''	$12\frac{1}{8}''$	$\frac{1}{2}''$	24''
x53	$11\frac{3}{4}''$	12''	$\frac{7}{16}''$	24''
HP 10x57	10''	$10\frac{1}{4}''$	$\frac{9}{16}''$	24''
x42	$9\frac{3}{4}''$	$10\frac{1}{8}''$	$\frac{7}{16}''$	24''
HP 8x36	8''	$8\frac{1}{8}''$	$\frac{7}{16}''$	18''

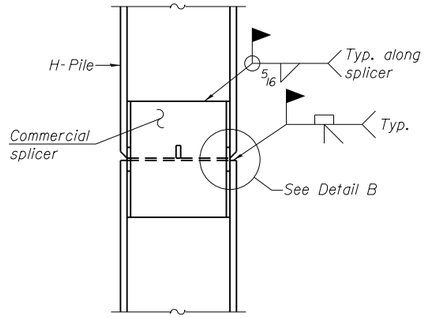


ELEVATION

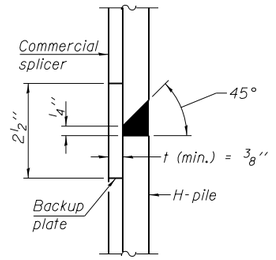


DETAIL A

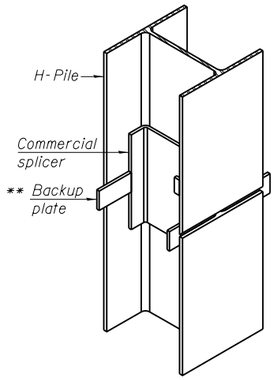
SHOE ATTACHMENT



ELEVATION

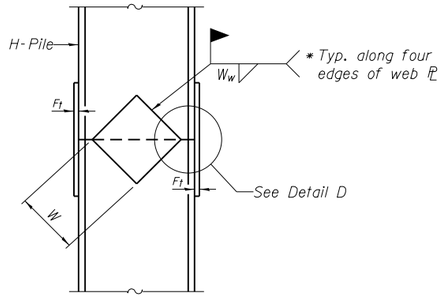


DETAIL "B"

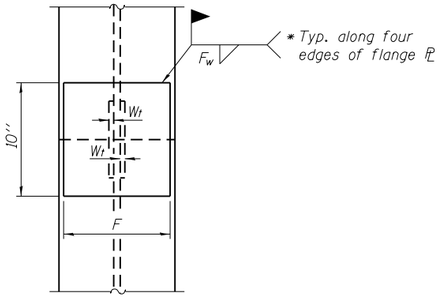


ISOMETRIC VIEW

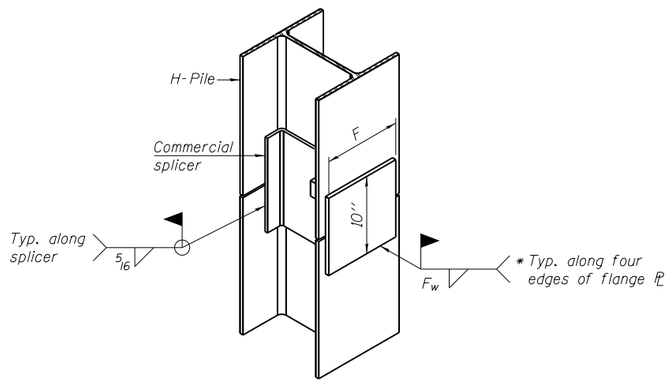
WELDED COMMERCIAL SPLICE



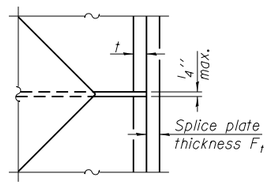
ELEVATION



END VIEW



ISOMETRIC VIEW



DETAIL D

Designation	F	F_f	F_w	W	W_f	W_w
HP 14x117	$12\frac{1}{2}''$	1''	$\frac{7}{8}''$	$7\frac{3}{4}''$	$\frac{5}{8}''$	$\frac{1}{2}''$
x102	$12\frac{1}{2}''$	$\frac{7}{8}''$	$\frac{3}{4}''$	$7\frac{3}{4}''$	$\frac{5}{8}''$	$\frac{1}{2}''$
x89	$12\frac{1}{2}''$	$\frac{3}{4}''$	$\frac{11}{16}''$	$7\frac{3}{4}''$	$\frac{5}{8}''$	$\frac{1}{2}''$
x73	$12\frac{1}{2}''$	$\frac{5}{8}''$	$\frac{9}{16}''$	$7\frac{3}{4}''$	$\frac{5}{8}''$	$\frac{1}{2}''$
HP 12x84	10''	$\frac{7}{8}''$	$\frac{11}{16}''$	$6\frac{1}{2}''$	$\frac{5}{8}''$	$\frac{1}{2}''$
x74	10''	$\frac{7}{8}''$	$\frac{11}{16}''$	$6\frac{1}{2}''$	$\frac{5}{8}''$	$\frac{1}{2}''$
x63	10''	$\frac{5}{8}''$	$\frac{1}{2}''$	$6\frac{1}{2}''$	$\frac{1}{2}''$	$\frac{3}{8}''$
x53	10''	$\frac{5}{8}''$	$\frac{1}{2}''$	$6\frac{1}{2}''$	$\frac{1}{2}''$	$\frac{3}{8}''$
HP 10x57	8''	$\frac{3}{4}''$	$\frac{9}{16}''$	$5\frac{1}{4}''$	$\frac{1}{2}''$	$\frac{3}{8}''$
x42	8''	$\frac{5}{8}''$	$\frac{9}{16}''$	$5\frac{1}{4}''$	$\frac{1}{2}''$	$\frac{3}{8}''$
HP 8x36	7''	$\frac{5}{8}''$	$\frac{7}{16}''$	$4\frac{1}{4}''$	$\frac{1}{2}''$	$\frac{3}{8}''$

WELDED PLATE FIELD SPLICE

WELDED COMMERCIAL SPLICE ALTERNATE

* Interrupt welds $\frac{1}{4}''$ from end of web and/or each flange.

** Remove portions of backup plates that extend outside the flanges.

*** Weld size per pile shoe manufacturer ($\frac{5}{16}''$ min.).

Note:
The steel H-piles shall be according to AASHTO M270 Grade 50.

F-HP

2-17-2017

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01613S1-60V22-S27-HP Pile Details.dgn	DESIGNED - KJD	REVISED
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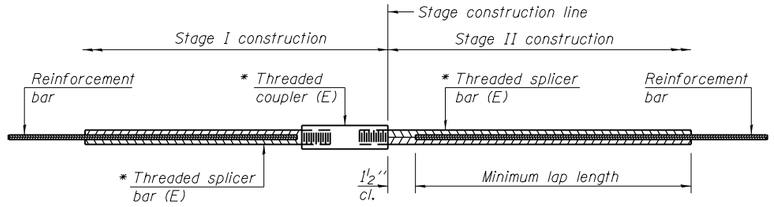
PLOT DATE = 12/8/2017	DATE - 12/08/2017	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HP PILE DETAILS
STRUCTURE NO. 016-1351

SCALE: | SHEET S-27 OF S-30 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	72
		CONTRACT NO. 60V22		
		ILLINOIS FED. AID PROJECT		

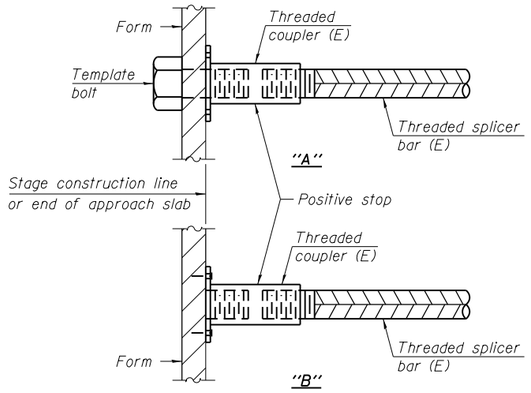


STANDARD BAR SPLICER ASSEMBLY

Threaded splicer bar length = min. lap length + 1½" + thread length

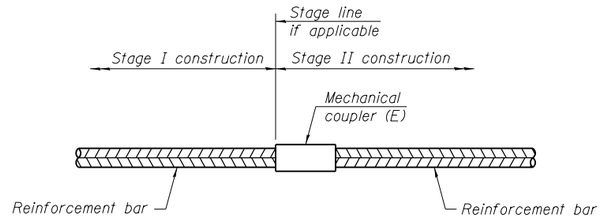
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
Deck	#5	172	3'-0"
Deck	#8	184	4'-9"
South Approach Slab	#5	160	3'-0"
South Approach Slab	#8	106	4'-9"
North Approach Slab	#5	160	3'-0"
North Approach Slab	#8	106	4'-9"
South Abut	#6	8	3'-10"
South Abut	#7	24	4'-5"
North Abut	#6	8	3'-10"
North Abut	#7	24	4'-5"



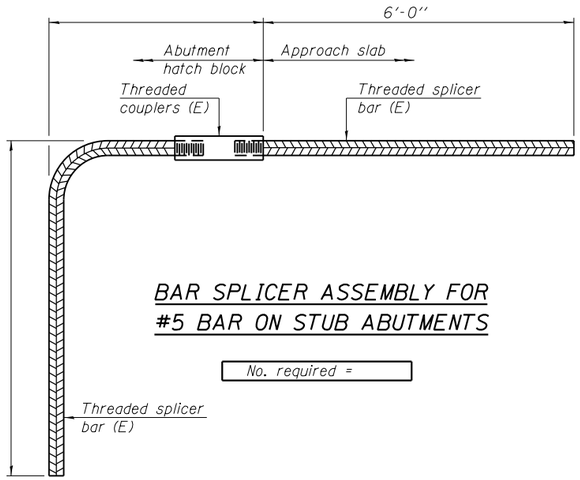
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
(E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars.

Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

2-17-2017

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PLOT DATE = 12/8/2017	DATE - 12/08/2017	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
STRUCTURE NO. 016-1351

SCALE: | SHEET S-28 OF S-30 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	73
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

FILE PATH = P:\1111-532 IDOT PTB161 Item 8 (Various-Various)\Work Order "11 - US 12 over Addison Creek Culvert\Structural\Sheets\0161351-60V22-S28-Bar Splicers.dgn

Wang Engineering
wangeng@wangeng.com
1145 N Main Street
Lombard, IL 60148
Telephone: 630 953-9928
Fax: 630 953-9938

BORING LOG SB-01A

WEI Job No.: 616-02-04

Client: **Millennia Professional Services of Illinois**
Project: **Mannheim Road over Addison Creek**
Location: **Stone Park, Illinois**

Datum: NAVD 88
Elevation: 635.74 ft
North: 1906121.24 ft
East: 1106698.68 ft
Station:
Offset:

Depth (ft)	Elevation (ft)	SOIL AND ROCK DESCRIPTION	Sample Type / recovery	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)
	634.6	14-inch thick ASPHALT					
		Medium dense, gray SILT, with very fine sand --FILL--		1	8 10 11	NP	15
5				2	5 9 10	NP	15
				3	11 8 7	NR	
	627.7	Very stiff, brown, SILTY CLAY LOAM, trace gravel		4	5 6 7	2.25 P	21
10	625.7	Boring terminated at 10.00 ft					
15							
20							
25							

GENERAL NOTES

Begin Drilling: **11-11-2013** Complete Drilling: **11-11-2013**
Drilling Contractor: **Wang Testing Services** Drill Rig: **CME-55**
Driller: **P&N** Logger: **F. Bozga** Checked by:
Drilling Method: **3.25" HSA, boring backfilled upon completion**

WATER LEVEL DATA

While Drilling: **8.00 ft**
At Completion of Drilling:
Time After Drilling: **NA**
Depth to Water: **NA**

The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.

Wang Engineering
wangeng@wangeng.com
1145 N Main Street
Lombard, IL 60148
Telephone: 630 953-9928
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BORING LOG SB-01B

WEI Job No.: 616-02-04

Client: **Millennia Professional Services of Illinois**
Project: **Mannheim Road over Addison Creek**
Location: **Stone Park, Illinois**

Datum: NAVD 88
Elevation: 636.01 ft
North: 1906123.83 ft
East: 1106685.56 ft
Station:
Offset:

Depth (ft)	Elevation (ft)	SOIL AND ROCK DESCRIPTION	Sample Type / recovery	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)
	635.7	3.5-inch thick ASPHALT					
	635.0	--PAVEMENT-- 8-inch thick CONCRETE --PAVEMENT--		1	3 5 7	2.50 P	28
		Stiff to verry stiff, brown to black, SILTY CLAY LOAM --FILL--		2	2 3 6	1.50 P	28
5	631.0	Wet SAND --FILL--					
	630.3	Stiff, brown CLAY		3	2 2 3	1.48 B	26
	628.0	Verry stiff, brown to gray SILTY CLAY, trace gravel		4	3 4 6	2.46 B	21
10				5	6 9 14	3.12 B	20
				6	4 7 9	2.13 B	19
15	620.5	Dense, gray, SILTY LOAM					
				7	21 22 26	NP	13
	618.0	Hard, gray SILTY CLAY LOAM, trace gravel					
		4-inch gray rock fragments		8	9 18 50/5	6.07 B	14
20	615.5	--HARD DRILLING--					
		Verry dense, gray, SILTY LOAM, trace gravel		9	50/5	NP	11
	613.5	Hard, gray, SILTY CLAY LOAM, trace gravel					
25				10	9 16 26	8.36 B	15

Depth (ft)	Elevation (ft)	SOIL AND ROCK DESCRIPTION	Sample Type / recovery	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)
	610.5	Very dense, brown weathered dolostone fragments		11	50/4	NP	9
	609.0	--WEATHERED BEDROCK-- Strong, brown, very poor rock mass quality, thin bedded, moderatelly weathered DOLOSTONE, up to 4-inch beds, <4-inch spaced joints, horizontal with less than 0.2-inch infilling, hard joint wall, moderatelly vuggy, highly fractured		1	C O R E		
30	604.0	--RUN 1 - RECOVERY= 53%-- --RQD = 0% -- Boring terminated at 32.00 ft					
35							
40							
45							
50							

GENERAL NOTES

Begin Drilling: **11-14-2013** Complete Drilling: **11-14-2013**
Drilling Contractor: **Wang Testing Services** Drill Rig: **CME-55**
Driller: **P&F** Logger: **F. Bozga** Checked by: **DRAFT**
Drilling Method: **2.25" SSA to 10', mud rotary thereafter, boring backfilled upon completion**

WATER LEVEL DATA

While Drilling: **5.50 ft**
At Completion of Drilling: **4 (MUD)**
Time After Drilling: **NA**
Depth to Water: **NA**

The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.

WANGENGINC 6160204.GPJ WANGENG.GDT 11/18/13

FILE PATH = P:\111-532 IDOT PTB161 Item 8 Various\Various\Work Order #1 - US 12 over Addison Creek Culvert\Structural\Sheets\0161351-60V22-S29-Boring Logs 1.dgn

HBM ENGINEERING GROUP, LLC.
CONSULTING & DESIGN
INSPECTION & RATING
RESEARCH & TESTING
4415 WEST HARRISON ST.
SUITE 231
HILLSIDE, IL 60162
PHONE: (708) 236-0900
FAX: (708) 236-0901

0161351-60V22-S29-Boring Logs 1.dgn	DESIGNED - KJD	REVISED
USER NAME = lisa.buntin	DRAWN - KJD	REVISED
PLOT SCALE = 0.17 ' / in.	CHECKED - LAB, MI	REVISED
PLOT DATE = 12/8/2017	DATE - 12/08/2017	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BORING LOGS I
STRUCTURE NO. 016-1351

SCALE: | SHEET S-29 OF S-30 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	74
		CONTRACT NO. 60V22		
	ILLINOIS	FED. AID PROJECT		

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BORING LOG SB-02

WEI Job No.: 616-02-04

Client: **Millennia Professional Services of Illinois**
Project: **Mannheim Road over Addison Creek**
Location: **Stone Park, Illinois**

Datum: NAVD 88
Elevation: 636.46 ft
North: 1906047.49 ft
East: 1106665.27 ft
Station:
Offset:

Profile	Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type / recovery	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)
	636.0	6-inch thick, ASPHALT						
	635.5	6-inch thick CRUSHED STONE --BASE COURSE--						
		Stiff to very stiff, brown and black SILTY CLAY LOAM --FILL--			1	3 5 6	2.25 P	24
			5		2	2 3 3	1.75 P	27
	631.0	Medium stiff, brown and gray SANDY CLAY LOAM --FILL--			3	1 3 3	0.82 B	25
	628.5	Stiff to very stiff, brown to gray CLAY to SILTY CLAY, trace gravel	10		4	2 3 6	2.13 B	21
					5	5 8 9	2.79 B	18
			15		6	3 5 6	1.89 B	17
	621.0	Very dense, gray SILTY LOAM			7	20 25 33	NP	11
	618.5	Hard, gray SILTY CLAY LOAM, trace gravel	20		8	13 9 13	6.56 B	14
					9	25 26 26	> 4.50 P	9
			25		10	14 16 25	8.20 B	11
	611.0	Very dense, brown GRAVELLY SAND --WEATHERED BEDROCK-- --HARD DRILLING-- --Possible Cobbles--			11	48 50/5	NP	9
			30		12	60/5	NP	10
	605.5	Strong, brown, poor rock mass quality, thin bedded, slightly weathered to fresh DOLOSTONE, up to 12-inch beds, 2- to 12-inch spaced joints, horizontal and oblique with less than 0.2-inch infilling, hard joint wall, slightly weathered joints, moderately vuggy, moderately fractured --RUN 1 - RECOVERY= 100%-- --RQD = 47% --	35		1	CORE		
			40					
	595.5	Boring terminated at 41.00 ft						
			45					
			50					

GENERAL NOTES

Begin Drilling: **11-12-2013** Complete Drilling: **11-12-2013**
Drilling Contractor: **Wang Testing Services** Drill Rig: **CME-55**
Driller: **R&J** Logger: **F. Bozga** Checked by: **DRAFT**
Drilling Method: **2.25" SSA to 10', mud rotary thereafter, boring backfilled upon completion**

WATER LEVEL DATA

While Drilling: **NA**
At Completion of Drilling: **4.00 ft**
Time After Drilling: **NA**
Depth to Water: **NA**

The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.

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BORING LOG SB-03

WEI Job No.: 616-02-04

Client: **Millennia Professional Services of Illinois**
Project: **Mannheim Road over Addison Creek**
Location: **Stone Park, Illinois**

Datum: NAVD 88
Elevation: 635.76 ft
North: 1906044.86 ft
East: 1106628.71 ft
Station:
Offset:

Profile	Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type / recovery	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)
		12-inch thick ASPHALT						
	634.8	Loose to medium dense, brown SANDY GRAVEL --FILL--			1	8 10 8	NP	7
			5		2	2 3 3	NP	12
					3	3 3 2	NP	23
	627.8	Very stiff, brown to gray SILTY CLAY, trace gravel	10		4	5 7 11	3.77 B	21
					5	3 6 9	2.95 B	16
			15		6	3 6 10	2.54 B	19
	620.3	Medium dense, gray SILTY LOAM, some sand seams			7	10 12 14	NP	15
	618.0	Hard. gray SILTY CLAY LOAM, trace gravel	20		8	5 11 18	6.72 B	15
					9	13 19 33	9.68 B	11
			25		10	12 14 18	6.15 B	11
	608.8	Very dense, brown SANDY GRAVEL, some weathered dolostone fragments --WEATHERED BEDROCK--			11	11 28 50/4	6.81 B	11
	607.8	Strong, brown, very poor rock mass quality, thin bedded, moderately weathered DOLOSTONE, up to 4-inch beds, 1- to 4-inch spaced joints, horizontal with less than 0.2-inch infilling, hard joint wall, slightly weathered joints, moderately vuggy, moderately fractured --RUN 1 - RECOVERY= 85%-- --RQD = 7% --	30		1	CORE		
	602.8	Boring terminated at 12.50 ft	35					
			40					
			45					
			50					

GENERAL NOTES

Begin Drilling: **11-12-2013** Complete Drilling: **11-14-2013**
Drilling Contractor: **Wang Testing Services** Drill Rig: **CME-55**
Driller: **P/F** Logger: **F. Bozga** Checked by: **DRAFT**
Drilling Method: **2.25" SSA to 10', mud rotary thereafter, boring backfilled upon completion**

WATER LEVEL DATA

While Drilling: **3.00 ft**
At Completion of Drilling: **3 (MUD)**
Time After Drilling: **NA**
Depth to Water: **NA**

The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.

WANGENGINC 6160204.GPJ WANGENG.GDT 11/18/13

FILE PATH = P:\1111-532 IDOT PTB161 Item 8 (Various-Various)\Work Order #11 - US 12 over Addison Creek Culvert\Structural\Sheets\0161351-60V22-S30-Boring Logs II.dgn

HBM ENGINEERING GROUP, LLC.
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4415 WEST HARRISON ST.
SUITE 231
HILLSIDE, IL 60162
PHONE: (708) 236-0900
FAX: (708) 236-0901

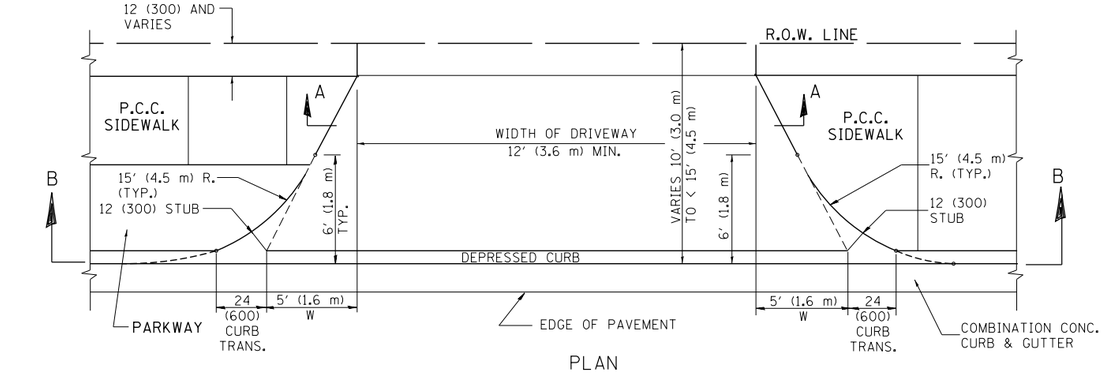
0161351-60V22-S30-Boring Logs II.dgn	DESIGNED - KJD	REVISED
USER NAME = lisa.buntin	DRAWN - KJD	REVISED
PLOT SCALE = 0.17 ' / in.	CHECKED - LAB, MI	REVISED
PLOT DATE = 12/8/2017	DATE - 12/08/2017	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BORING LOGS II
STRUCTURE NO. 016-1351

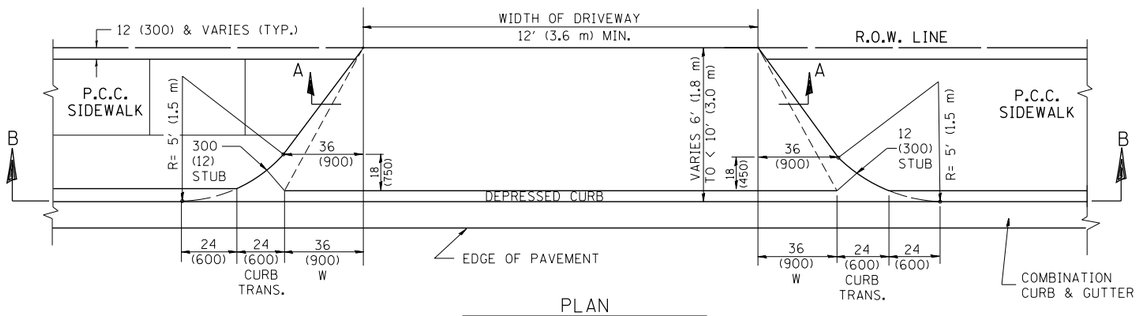
SCALE: | SHEET S-30 OF S-30 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	75
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			



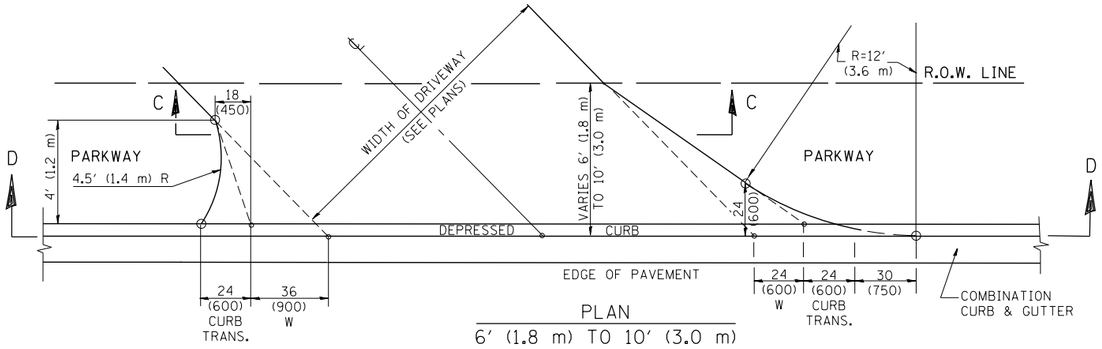
PLAN

10' (3.0 m) TO < 15' (4.5 m)



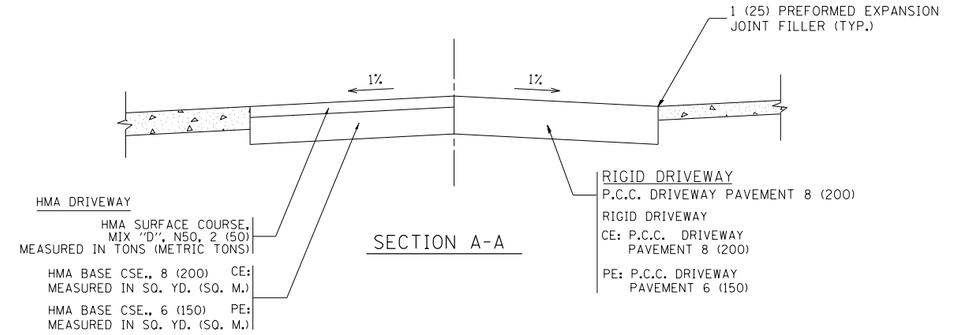
PLAN

6' (1.8 m) TO < 10' (3.0 m)

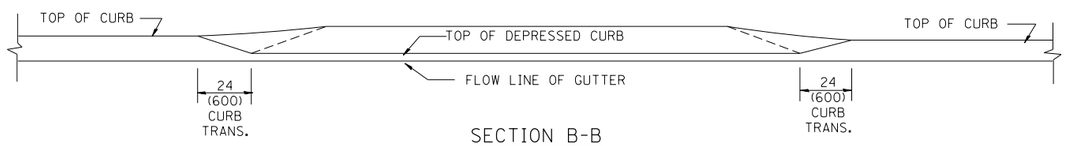


PLAN

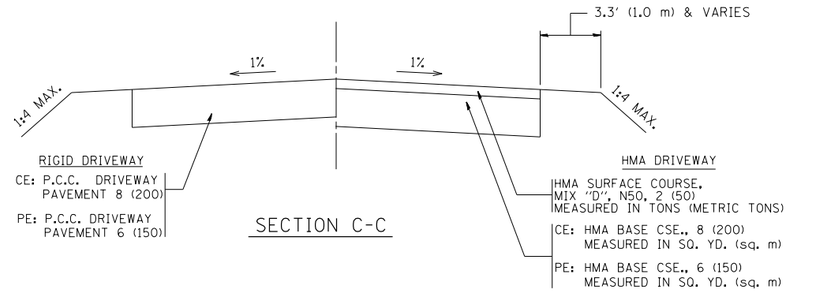
6' (1.8 m) TO 10' (3.0 m)



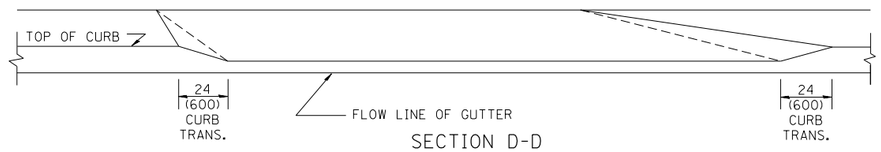
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

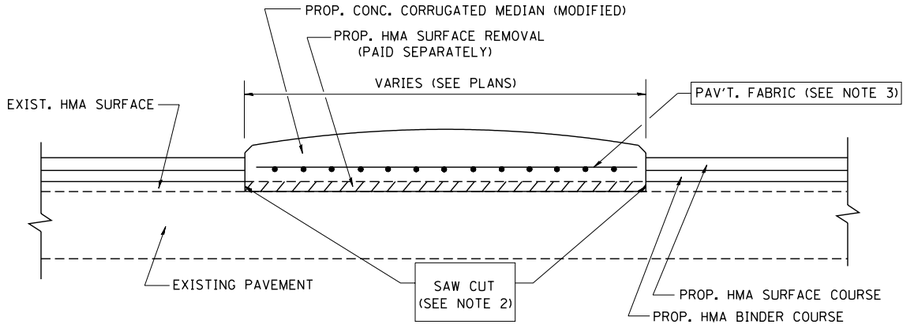
FILE NAME = c:\pw_work\pwidot\leysa\d0108315\bd02.dgn	USER NAME = leysa	DESIGNED - R. SHAH	REVISED - M. GOMEZ 04-06-01
		DRAWN -	REVISED - P. LaFLEUR 04-15-03
	PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - R. BORO 01-01-07
	PLOT DATE = 10/28/2011	DATE - 11-06-95	REVISED - R. BORO 09-06-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

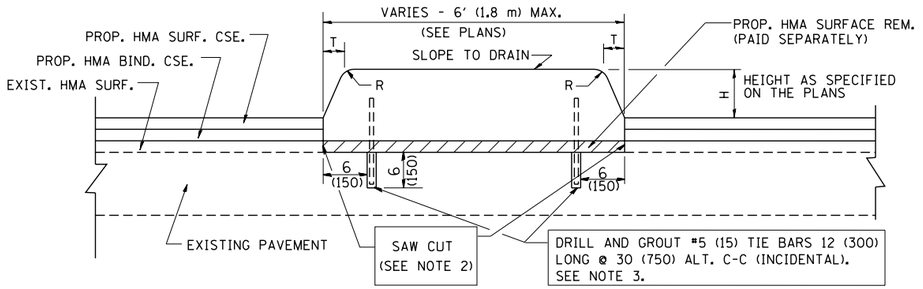
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	76
	BD400-02 (BD-02)	CONTRACT NO. **60V22**		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



NOTES:
1. CORRUGATED MEDIAN (MODIFIED) SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 606 OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE PORTIONS OF STATE STANDARD 606306.
2. WITH THE APPROVAL OF THE ENGINEER, THE CONTRACTOR MAY DELETE THE SAW CUT IF A NEAT JOINT CAN BE OBTAINED BY MILLING THE HMA SURFACE TO BE REMOVED. SAW CUT WILL BE INCLUDED IN THE COST OF CORRUGATED MEDIAN (MODIFIED)
3. PAVEMENT FABRIC WILL BE INCLUDED IN THE COST OF CORRUGATED MEDIAN (MODIFIED)

DETAILS FOR CORRUGATED MEDIAN (MODIFIED)

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT (SQUARE METER) FOR "CORRUGATED MEDIAN (MODIFIED)"



H	R	T
6(150)	1(25)	1(25)
9(225)	1(25)	2(50)

NOTES:
1. CONCRETE MEDIAN TYPE SB (DOWELLED) SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF STATE STANDARD 606301 AND SECTION 606 OF THE STANDARD SPECIFICATIONS.
2. WITH THE APPROVAL OF THE ENGINEER, THE CONTRACTOR MAY DELETE THE SAW CUT IF A NEAT JOINT CAN BE OBTAINED BY MILLING THE HMA SURFACE TO BE REMOVED. SAW CUT WILL BE INCLUDED IN THE COST OF "CONCRETE MEDIAN TYPE SB (DOWELLED)"
3. FOR MEDIAN WIDTH LESS THAN 4' (1.2 m) USE ONE ROW OF #5 (15) BARS @ 30 (750) C-C ALONG THE MEDIAN CENTERLINE. TIE BARS WILL BE INCLUDED IN THE COST OF "CONCRETE MEDIAN TYPE SB (DOWELLED)"

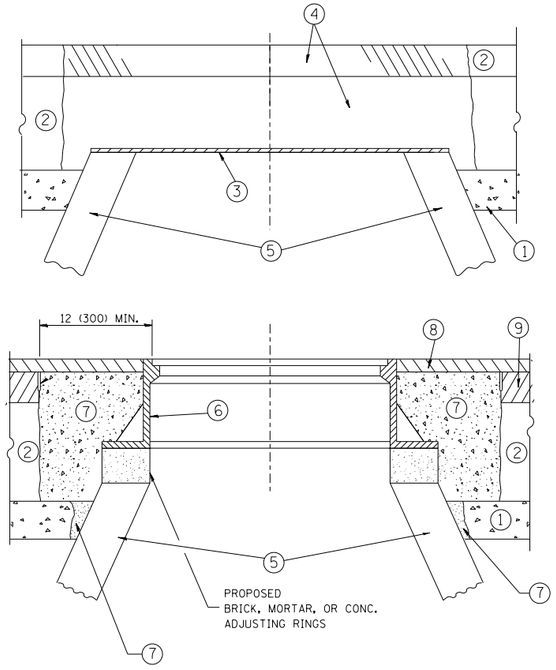
DETAILS FOR CONCRETE MEDIAN TYPE SB (DOWELLED)

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT (SQUARE METER) FOR "CONCRETE MEDIAN TYPE SB (DOWELLED)"

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\bd05.dgn	USER NAME = gaglianobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 09-09-94	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**	**DETAILS FOR CONCRETE MEDIAN TYPE SB (DOWELLED) CORRUGATED MEDIAN (MODIFIED)**
		DRAWN -	REVISED - R. SHAH 10-25-94		
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - E. GOMEZ 08-28-00		
	PLOT DATE = 1/4/2008	DATE - 05-14-90	REVISED - R. BORO 01-01-07		SCALE: NONE / SHEET NO. 1 OF 1 SHEETS / STA. / TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	77
	BD600-02 (BD-5)	CONTRACT NO. 60V22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

1. SUB-BASE GRANULAR MATERIAL
2. EXISTING PAVEMENT
3. 36 (900) DIAMETER METAL PLATE
4. PROPOSED CRUSHED STONE AND HMA SURFACE MIX
5. EXISTING STRUCTURE
6. FRAME AND LID (SEE NOTES)
7. CLASS PP-1* CONCRETE
8. PROPOSED HMA SURFACE COURSE
9. PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME = c:\pw_work\pwidot\bauerdl\d0108315\bd08.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 1968.5000 ' / m	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 12-06-11

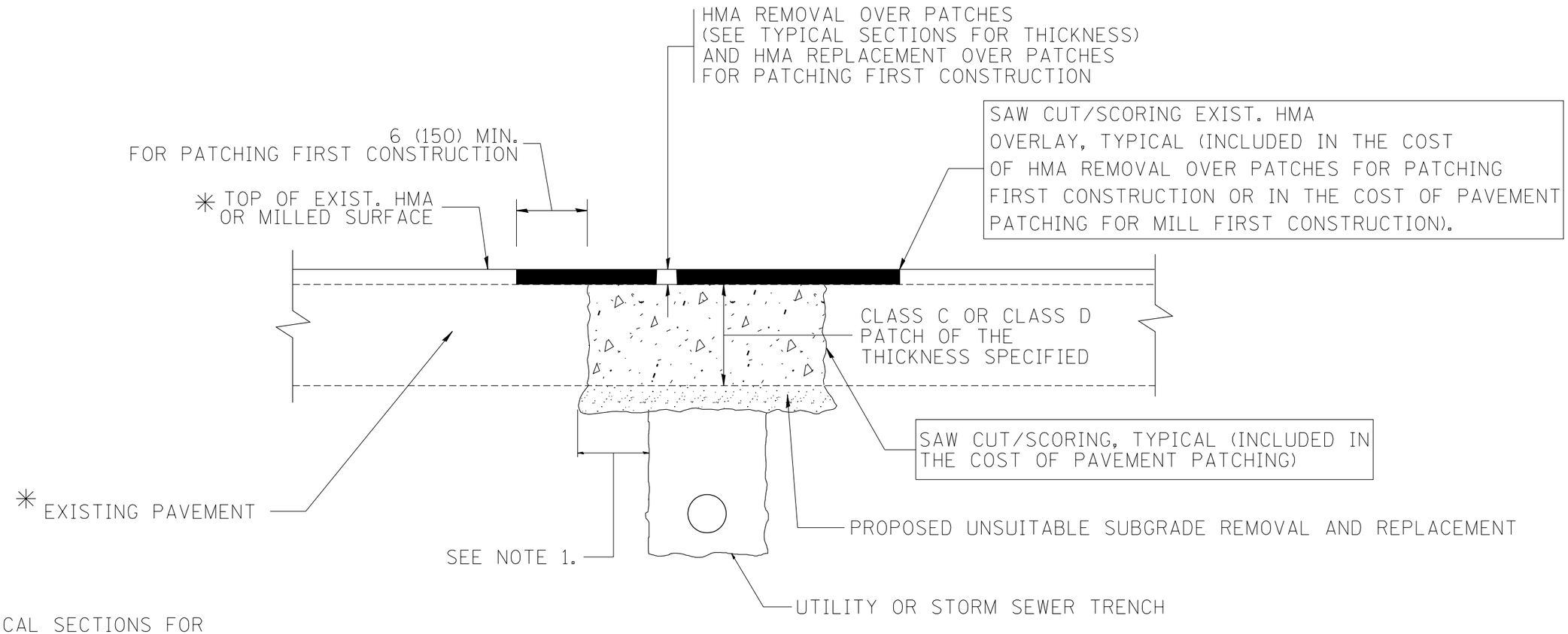
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	78
	BD600-03 (BD-8)	CONTRACT NO. 60V22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

bd08.dgn 12/6/2011 10:53:13 AM User:bauerdl



✳ SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST $4\frac{1}{2}$ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

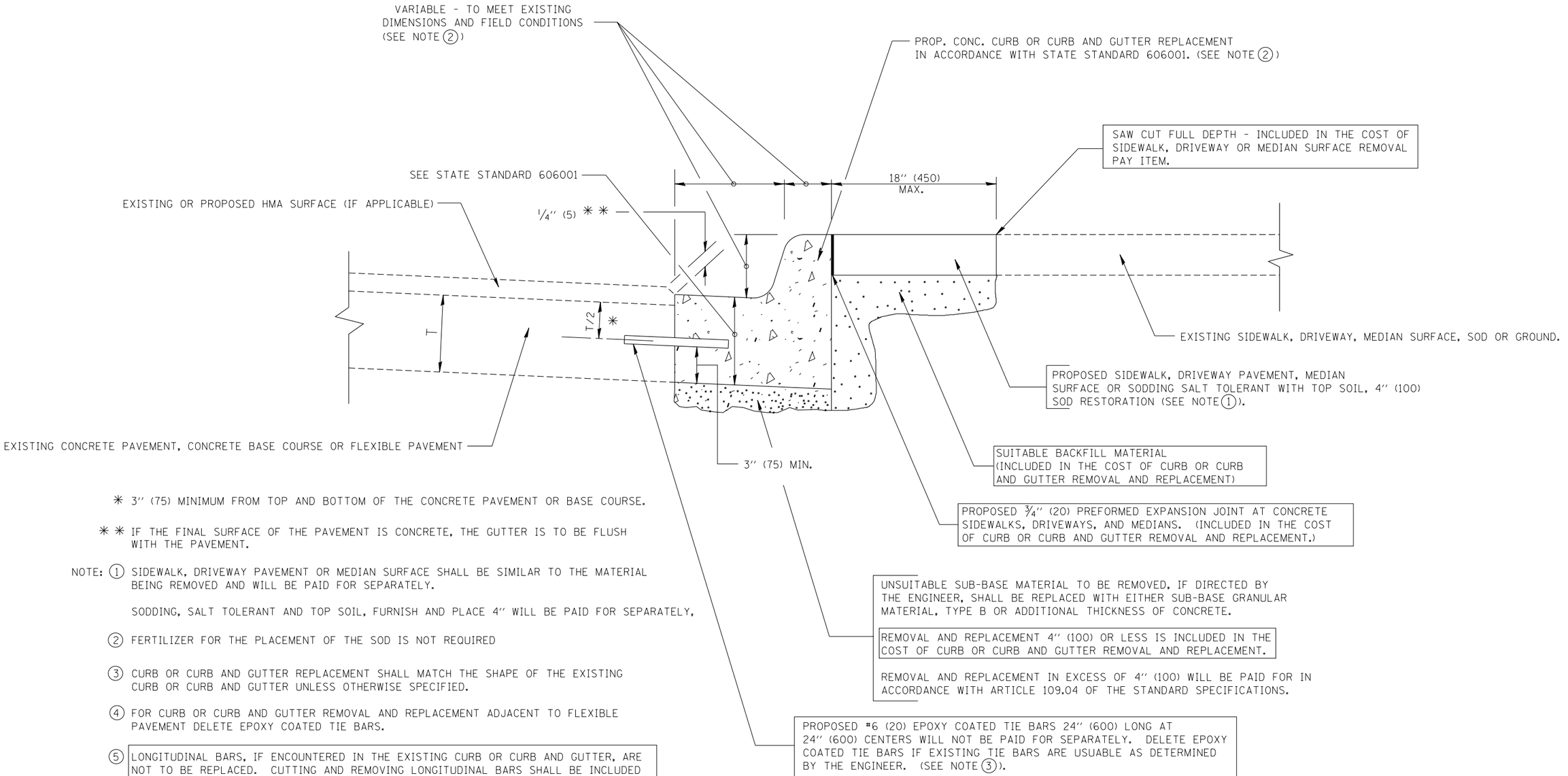
FILE NAME = c:\projects\diststd22x34\bd22.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98
		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - R. BORO 09-04-07
	PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - K. ENG 10-27-08

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	79
	BD400-04 (BD-22)	CONTRACT NO. **60V22**		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

* * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: (1) SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

(2) FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

(3) CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

(4) FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

(5) LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

(6) THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

(7) THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

(8) THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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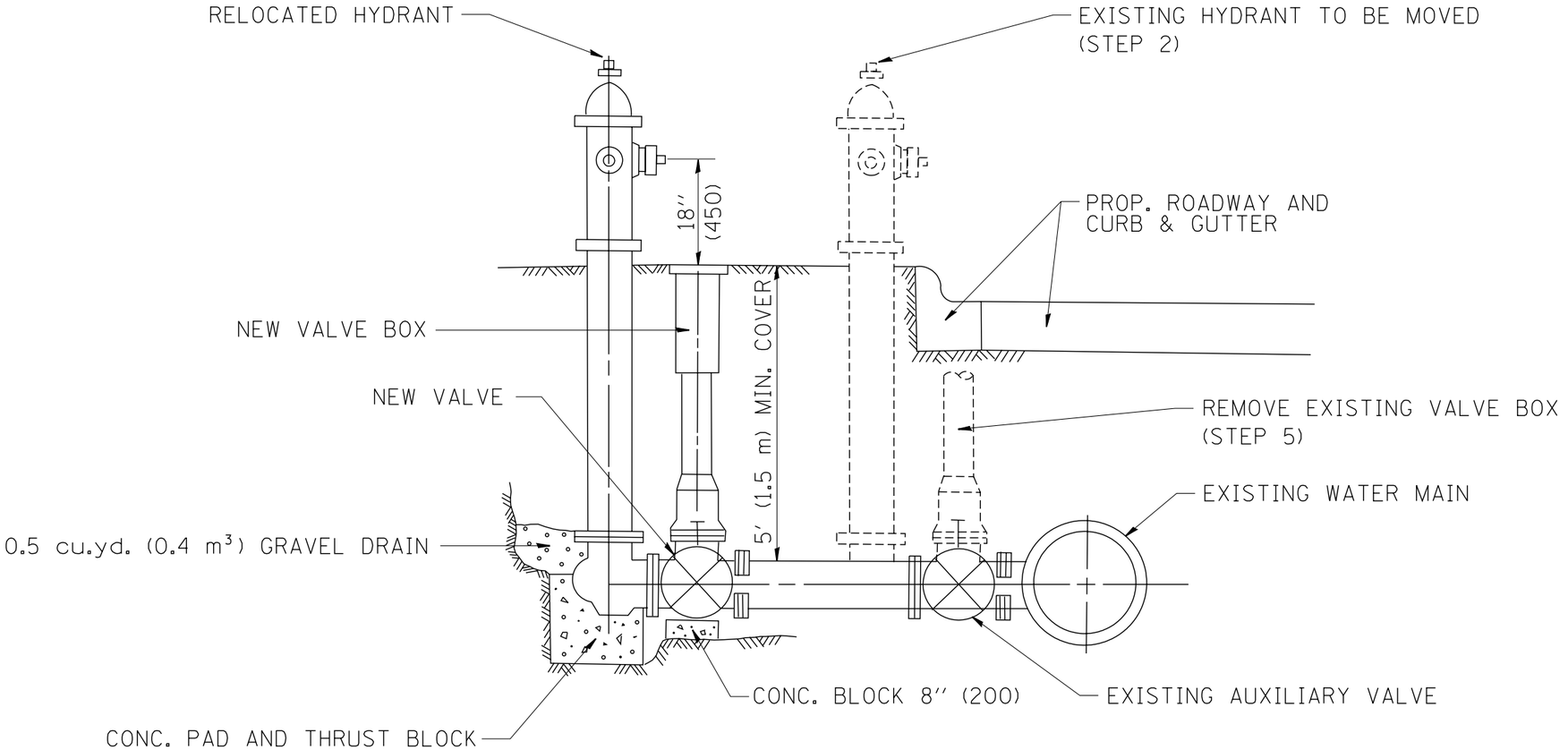
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01
	PLOT DATE = 12/15/2009	DATE - 03-11-94	REVISED - R. BORO 12-15-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	80
	BD600-06 (BD-24)	CONTRACT NO. **60V22**		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



SEQUENCE OF CONSTRUCTION:

1. CLOSE EXISTING VALVE.
2. REMOVE EXISTING HYDRANT.
3. INSTALL HYDRANT EXTENSION AND NEW VALVE.
4. RELOCATE EXISTING HYDRANT.
5. OPEN EXISTING VALVE, REMOVE BOX.
6. BACKFILL.
7. FLUSH AND TEST FOR CHLORIDE RESIDUAL AND PROVIDE TEST.

ALL WORK TO BE DONE IN ACCORDANCE WITH ARTICLE 564 OF THE STANDARD SPECIFICATIONS. NEW VALVE AND BOX SHALL BE SAME MAKE AND MODEL AS EXISTING.

FIRE HYDRANT TO BE MOVED

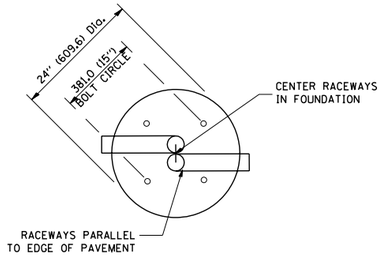
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\bd36.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED - R. SHAH 09-09-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FIRE HYDRANT TO BE MOVED
		DRAWN -	REVISED - R. SHAH 10-25-94		
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED -		SCALE: NONE / SHEET NO. 1 OF 1 SHEETS / STA. / TO STA.
	PLOT DATE = 1/4/2008	DATE -	REVISED -		

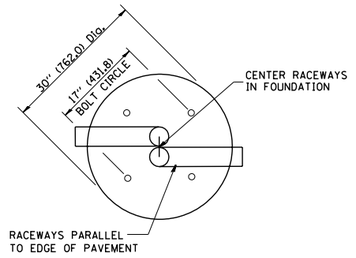
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	81
	BD-36	CONTRACT NO. 60V22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

LIGHT POLE FOUNDATION DEPTH TABLE
40 FT. (12.192 m) TO 47.5 FT. (14.478 m) MOUNTING HEIGHT

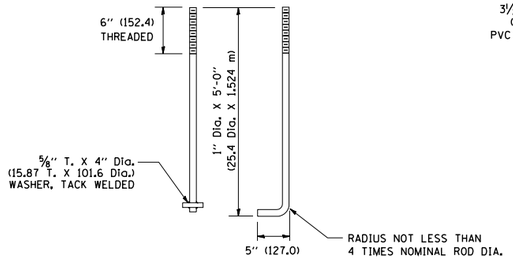
SOIL CONDITIONS	DESIGN DEPTH "D" OF FOUNDATION	
	SINGLE ARM POLE	TWIN ARM POLE
SOFT CLAY Qu = 0.375 TON/SQ. FT.	13'-0" (3.96 m)	15'-0" (4.57 m)
MEDIUM CLAY Qu = 0.75 TON/SQ.FT	9'-6" (2.09 m)	10'-9" (3.23 m)
STIFF CLAY Qu = 1.50 TON/SQ. FT.	7'-0" (2.13 m)	8'-0" (2.44 m)
LOOSE SAND Ø = 34°	9'-0" (2.74 m)	10'-0" (3.05 m)
MEDIUM SAND Ø = 37.5°	8'-3" (2.52 m)	9'-0" (2.74 m)
DENSE SAND Ø = 40°	7'-9" (2.36 m)	9'-0" (2.74 m)



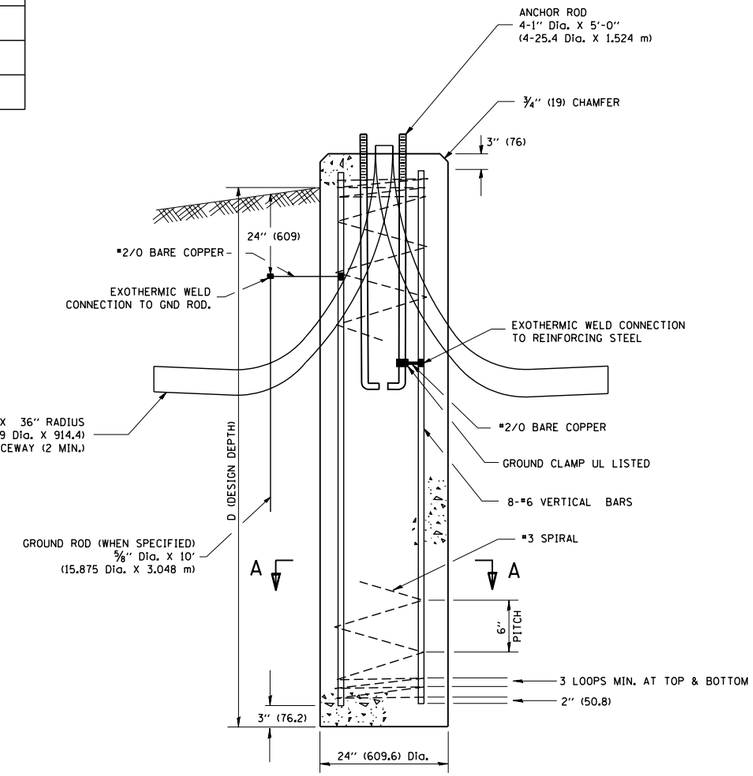
TOP VIEW



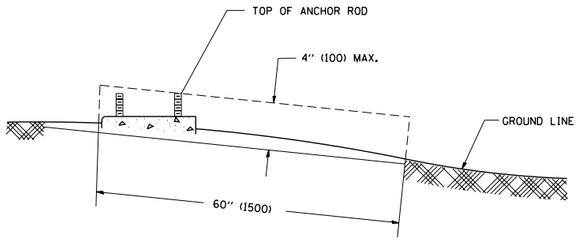
TOP VIEW



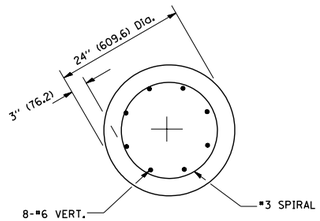
ANCHOR ROD DETAIL



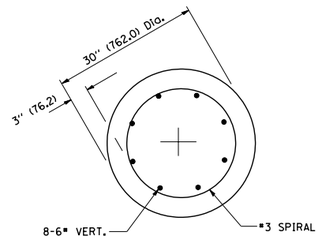
FOUNDATION DETAIL



FOUNDATION EXTENSION DETAIL



SECTION A-A



SECTION A-A

NOTES

1. ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
2. THE ANCHOR RODS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IN PLACED.
3. THE FOUNDATION SHALL NOT PROTRUDE MORE THAN 100MM (4 IN.) ABOVE THE FINISHED GRADE WITHIN A 60 IN. (1.5 m) CHORD ACROSS THE FOUNDATION, WITH ANCHOR RODS INCLUDED, IN ACCORDANCE WITH AASHTO GUIDELINES. IF THE FOUNDATION HEIGHT, INCLUDING ANCHOR RODS, EXTENDS BEYOND THESE SPECIFIED LIMITS, THE FOUNDATION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE. SEE FOUNDATION EXTENSION DETAIL.
4. THE HOLE FOR THE FOUNDATION SHALL BE MADE BY DRILLING WITH AN AUGER, OF THE SAME DIAMETER AS THE FOUNDATION. IF SOIL CONDITIONS REQUIRE THE USE OF A LINER TO FORM THE HOLE, THE LINER SHALL BE WITHDRAWN AS THE CONCRETE IS DEPOSITED.
5. THE TOP OF THE FOUNDATION SHALL BE CONSTRUCTED LEVEL. A LINER OR FORM SHALL BE USED TO PRODUCE A UNIFORM SMOOTH SIDE TO THE TOP OF THE FOUNDATION. FOUNDATION TOP SHALL BE CHAMFERED 3/4-IN. (20 mm).
6. THE CONCRETE SHALL BE CLASS SI. CONCRETE SHALL CURE ACCORDING TO ARTICLE 1020.13 BEFORE LIGHT POLES ARE INSTALLED.
7. THE ANCHOR ROD SHALL BE A HOOK ROD TYPE. COLD BENDING OF THE ANCHOR ROD WILL NOT BE ALLOWED. THE RADIUS OF THE HOOK BEND SHALL NOT BE LESS THAN 4 TIMES THE NOMINAL DIAMETER OF THE ANCHOR ROD. A TACK WELDED ANCHOR ROD MAY BE SUBSTITUTED WITH THE APPROVAL OF THE ENGINEER.
8. THE ANCHOR RODS SHALL BE ACCORDING TO ASTM F1554 GRADE 725 (GRADE 105). NUTS SHALL BE HEXAGON NUTS ACCORDING TO ASTM A 194 2H OR ASTM A 563 DH, AND WASHERS SHALL BE ACCORDING TO ASTM F 436.
9. ANCHOR RODS, NUTS AND WASHERS SHALL BE COMPLETELY GALVANIZED BY EITHER THE HOT-DIPPED PROCESS CONFORMING WITH AASHTO M 232, THE MECHANICAL PLATING METHOD CONFORMING TO AASHTO M 298, CLASS 50 WITH A MAXIMUM COATING THICKNESS OF 150 UM(6 MILS) OR THE ELECTROLYTIC PROCESS ACCORDING TO ASTM F 1136.
10. THE ANCHOR RODS SHALL BE THREADED A MINIMUM OF 6 INCHES (150 mm) WITH A MINIMUM OF 3 INCHES (75 mm) OF THREADED ANCHOR ROD EMBEDDED IN THE FOUNDATION.
11. ANCHOR RODS SHALL PROJECT 2 3/4" (69.9 mm) ABOVE THE TOP OF THE FOUNDATION. IF BREAKAWAY COUPLINGS ARE SPECIFIED, THE CONTRACTOR SHALL CAREFULLY COORDINATE THE ANCHOR ROD PROJECTION WITH THE INSTALLATION REQUIREMENTS OF THE BREAKAWAY COUPLINGS.
12. THE CONTRACTOR SHALL USE A #3 SPIRAL AT 6" (152.4 mm) PITCH OR MAY SUBSTITUTE #3 TIES AT 12" (304.8 mm) O.C. WITH THE APPROVAL OF THE ENGINEER.
13. THE CABLE TRENCHES AND FOUNDATION SHALL BE BACK FILLED AND COMPACTED AS SPECIFIED BEFORE THE LIGHT POLE IS ERECTED.
14. THE RACEWAYS SHALL PROJECT 1" (25.4 mm) ABOVE THE TOP OF THE FOUNDATION.

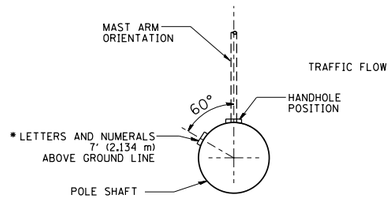
FILE NAME = W:\diststd\22x34\be301.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED - 04-22-02
		DRAWN -	REVISED -
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 1/4/2008	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

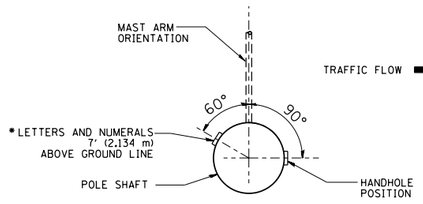
LIGHT POLE FOUNDATION
40' (12.192 m) TO 47 1/2' (14.478 m) M.H. 15" (381 mm) BOLT CIRCLE

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

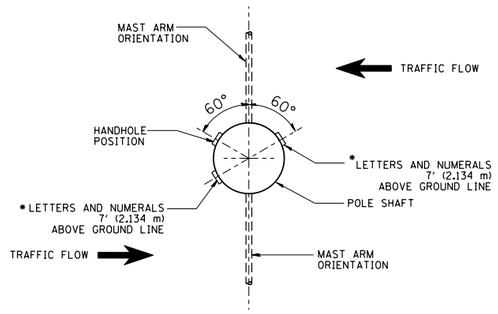
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	82
	BE-301	CONTRACT NO. 60V22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



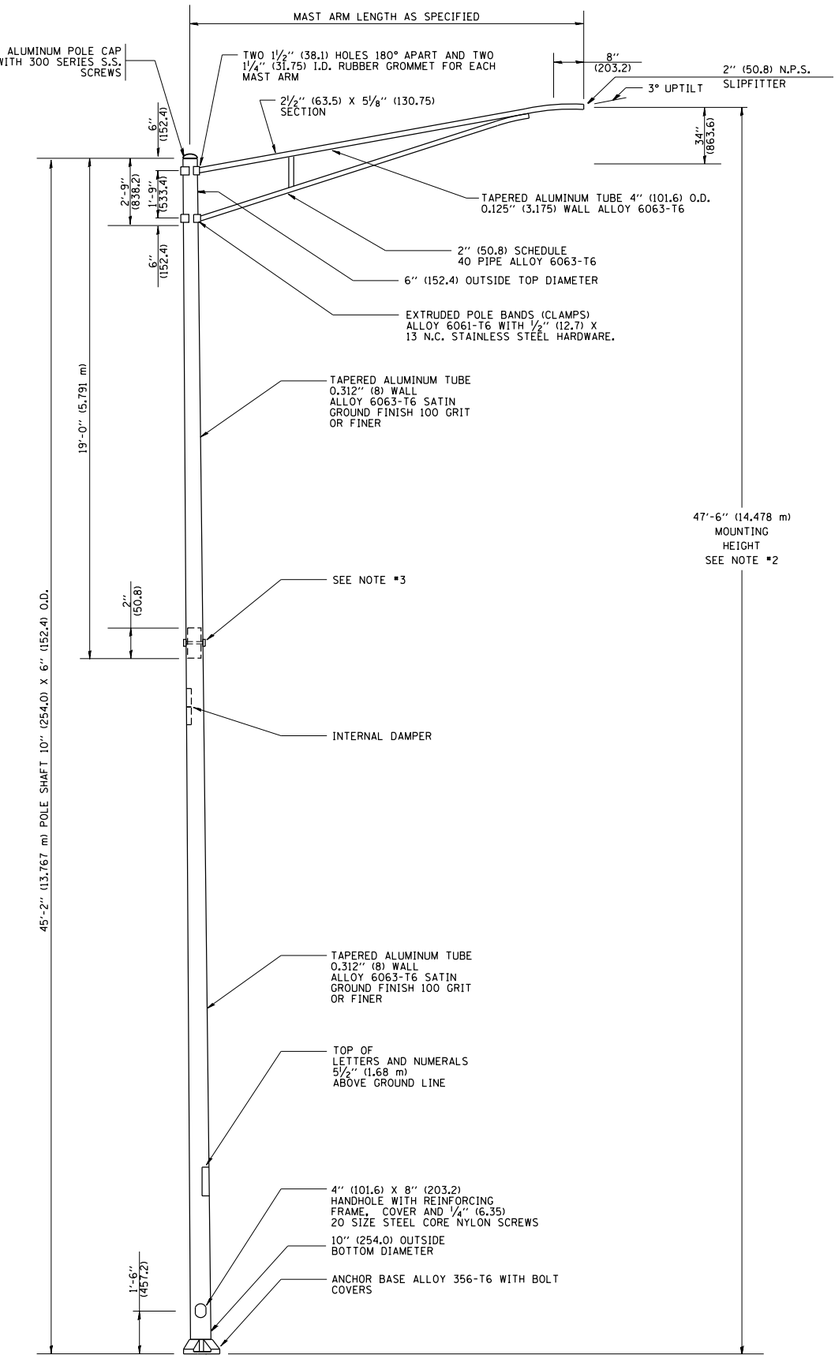
POSITION OF HANDHOLE AND POLE NUMBER FOR SINGLE MAST ARM POLES MOUNTED ON BRIDGE PARAPET OR BARRIER WALL

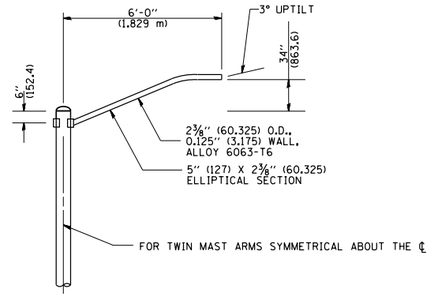


POSITION OF HANDHOLE AND POLE NUMBER FOR SINGLE MAST ARM POLES



POSITION OF HANDHOLE AND POLE NUMBER FOR TWIN MAST ARM POLES

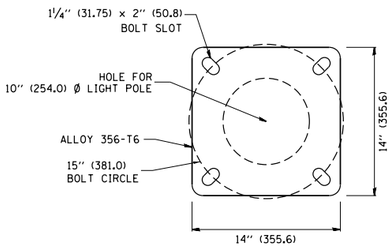




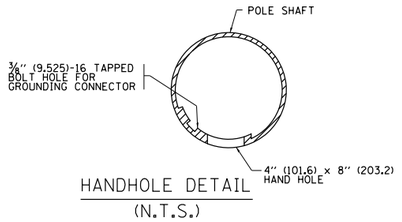
6' (1.8 m) SINGLE MEMBER MAST ARM
(N.T.S.)

NOTES:

1. ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
2. MOUNTING HEIGHT IS DEFINED AS THE DISTANCE FROM THE CENTERLINE OF THE TENON TO THE BOTTOM OF THE ANCHOR BASE.
3. TWO PIECE SHAFT WILL BE MATCHED MARKED AND INTERCHANGEABLE BETWEEN DIFFERENT UNITS. FIELD DRILLING OF THE HOLES WILL NOT BE ALLOWED.
4. THE LIGHT POLE WILL MEET AASHTO DESIGN CRITERIA AS SPECIFIED.
5. THE INSTALLING CONTRACTOR WILL PROVIDE A UL LISTED GROUNDING CONNECTOR. BURNDY K2C23, T&B SP4DL OR APPROVED EQUAL.
6. LIGHT POLES WILL NOT BE INSTALLED WITHOUT MAST ARMS AND LUMINAIRES.
7. LIGHT POLES WILL BE SET PLUMB ON THE FOUNDATION WITHOUT THE USE OF LEVELING NUTS, WASHERS OR SHIMS.
8. LIGHTING UNIT IDENTIFICATION NUMBERS SHALL BE INSTALLED BEFORE THE LIGHTING UNIT IS ENERGIZED.



LIGHT POLE BASE PLATE DETAIL
15 INCH (381.0) BOLT CIRCLE



HANDHOLE DETAIL
(N.T.S.)

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\Dist\Standards\CADData\CADsheets\be400.dgn Default	USER NAME = drivakosgn	DESIGNED -	REVISED - R. TOMSONS 09-06-00	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ALUMINUM LIGHT POLE 47'-6" (14.478 m) MOUNTING HEIGHT	F.A. P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50.000 ' / in.	DRAWN -	REVISED - R. TOMSONS 09-03-03			330	464-B	COOK	97	83
	PLOT DATE = 12/21/2015	CHECKED -	REVISED - R. TOMSONS 01-18-13				BE-400		CONTRACT NO. 60V22	
		DATE -	REVISED - R. TOMSONS 03-18-15		SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.		ILLINOIS	FED. AID PROJECT		

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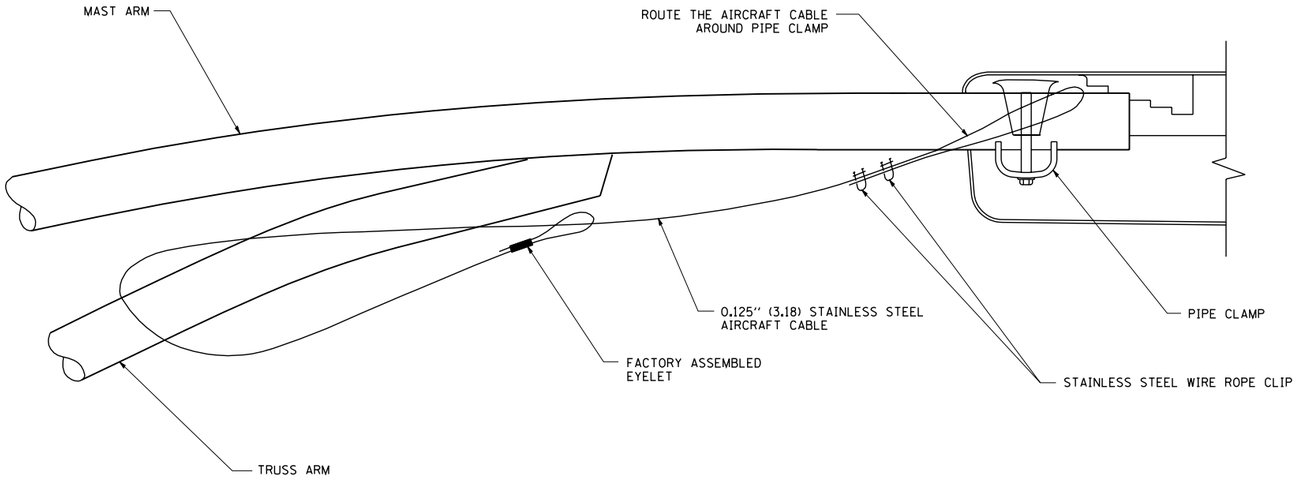
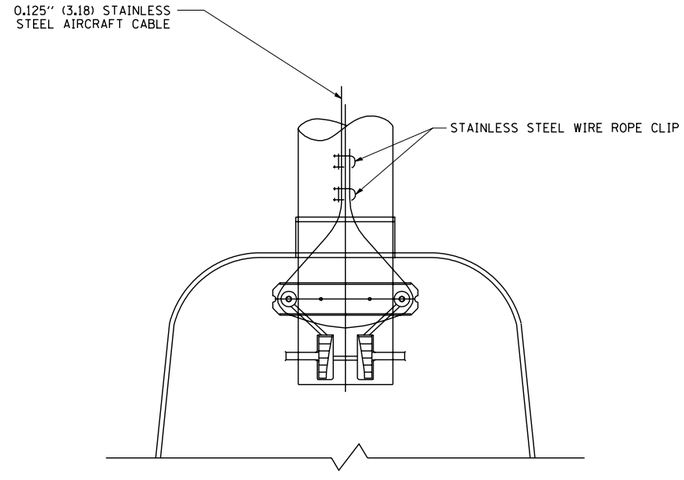


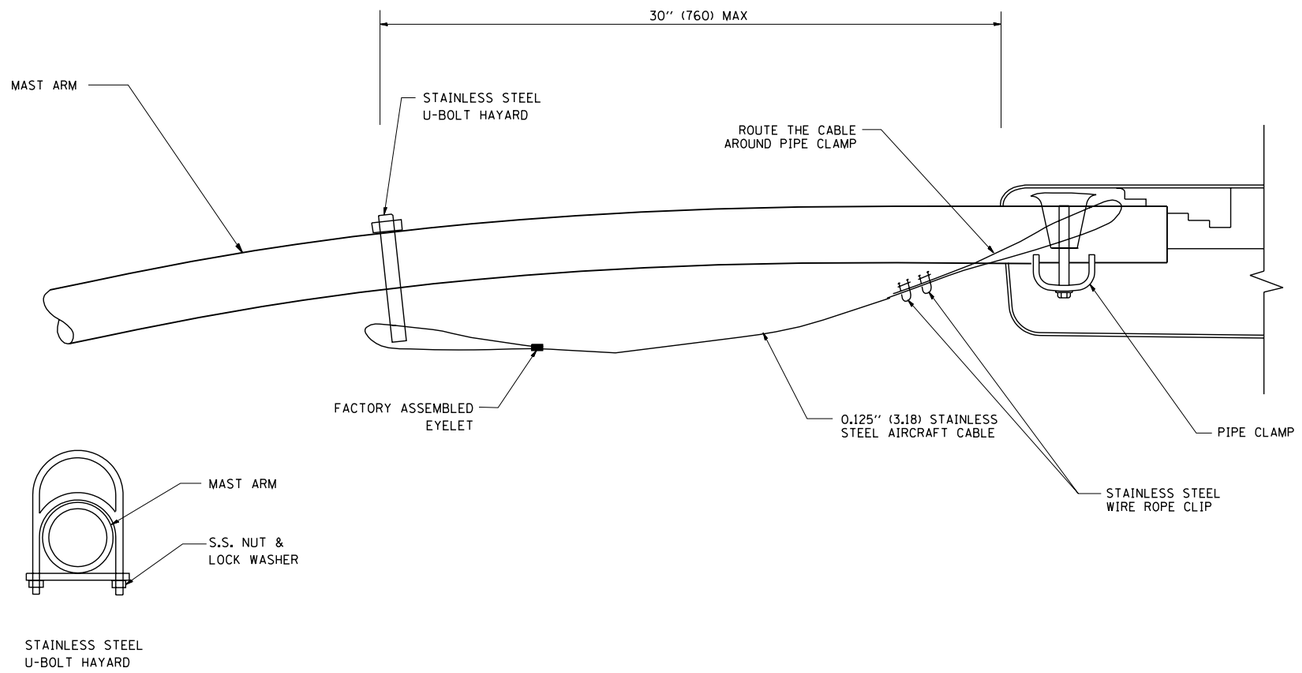
SIDE VIEW (TRUSS ARM)

N.T.S.



BOTTOM VIEW

N.T.S.



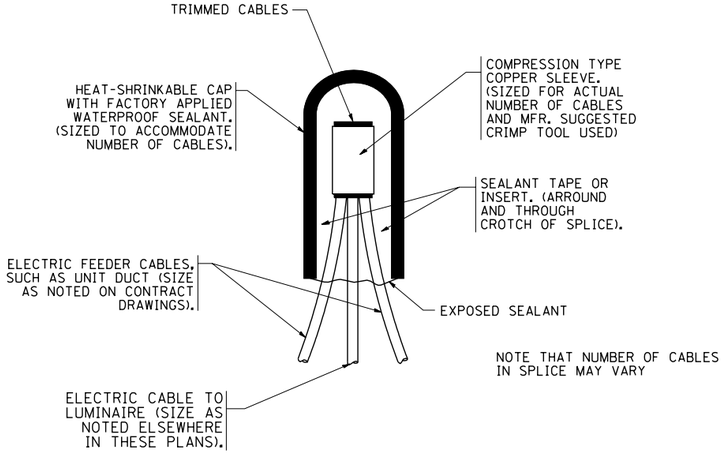
SIDE VIEW (SINGLE MEMBER OR DAVIT ARM)

N.T.S.

NOTES:

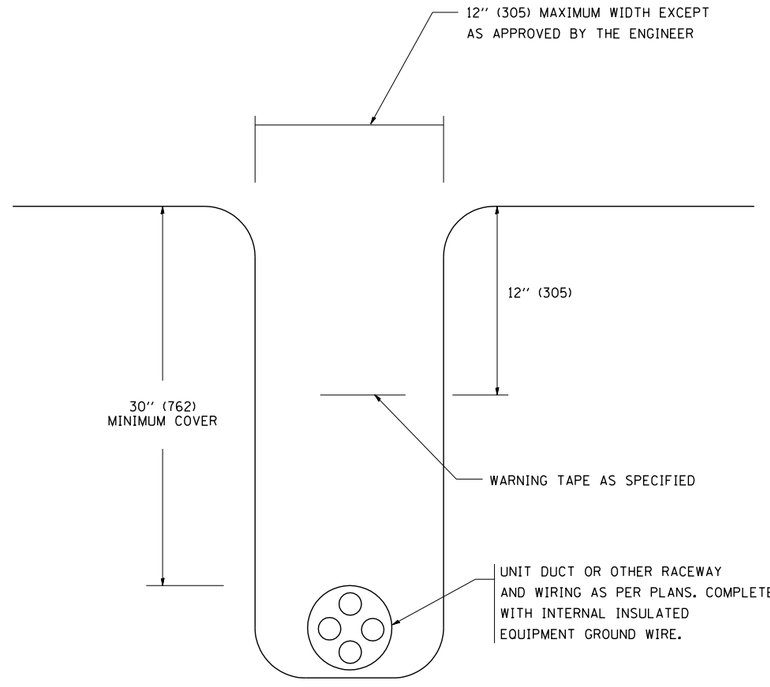
1. ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.
2. CONTRACTOR SHALL ADJUST THE WIRE CLIP TO ELIMINATE ANY SLACK FROM THE WIRE ROPE.
3. THE 0.125" (3.18) STAINLESS STEEL AIRCRAFT CABLE SHALL REMAIN VISIBLE FROM THE GROUND LEVEL.
4. THE BREAKING STRENGTH OF THE CABLE SHALL BE 1700 LBS. MIN.

FILE NAME = W:\diststd\22x34\be701.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED - 08-08-03	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	LUMINAIRE SAFETY CABLE ASSEMBLY			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -					330	464-B	COOK	97	84
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED -						BE-701		CONTRACT NO. 60V22	
	PLOT DATE = 1/4/2008	DATE -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



TYPICAL SPLICE DETAIL

N.T.S.



TYPICAL WIRING IN TRENCH DETAIL

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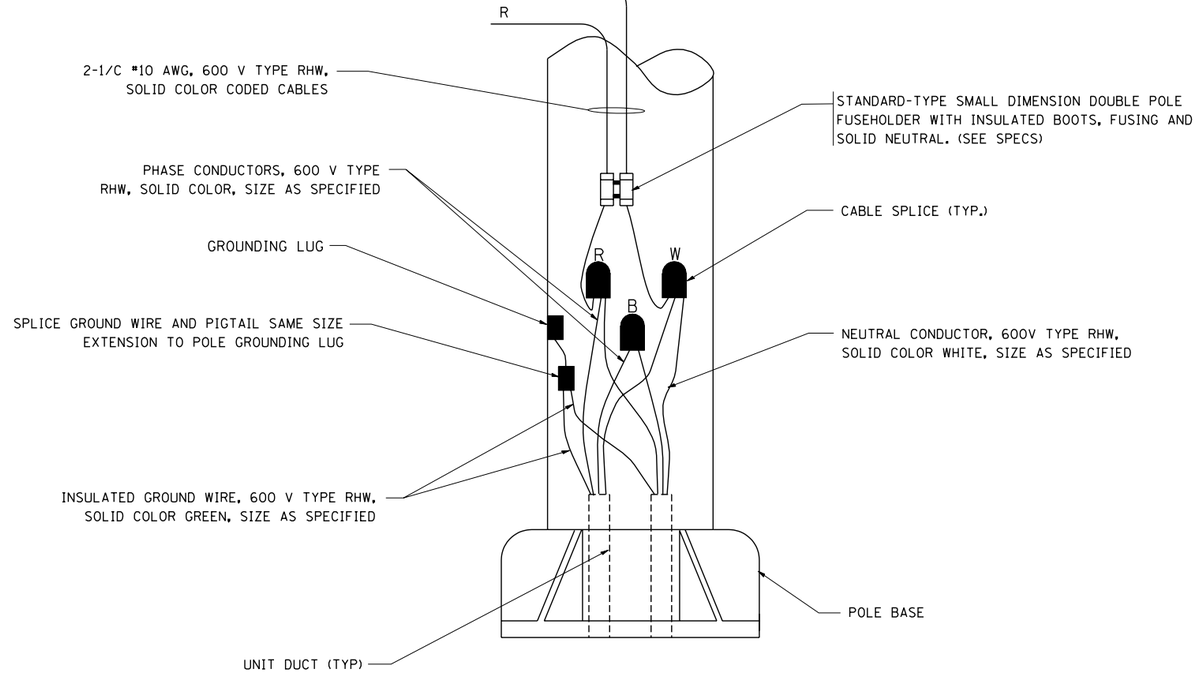

POLE WIRING DETAIL

N.T.S.

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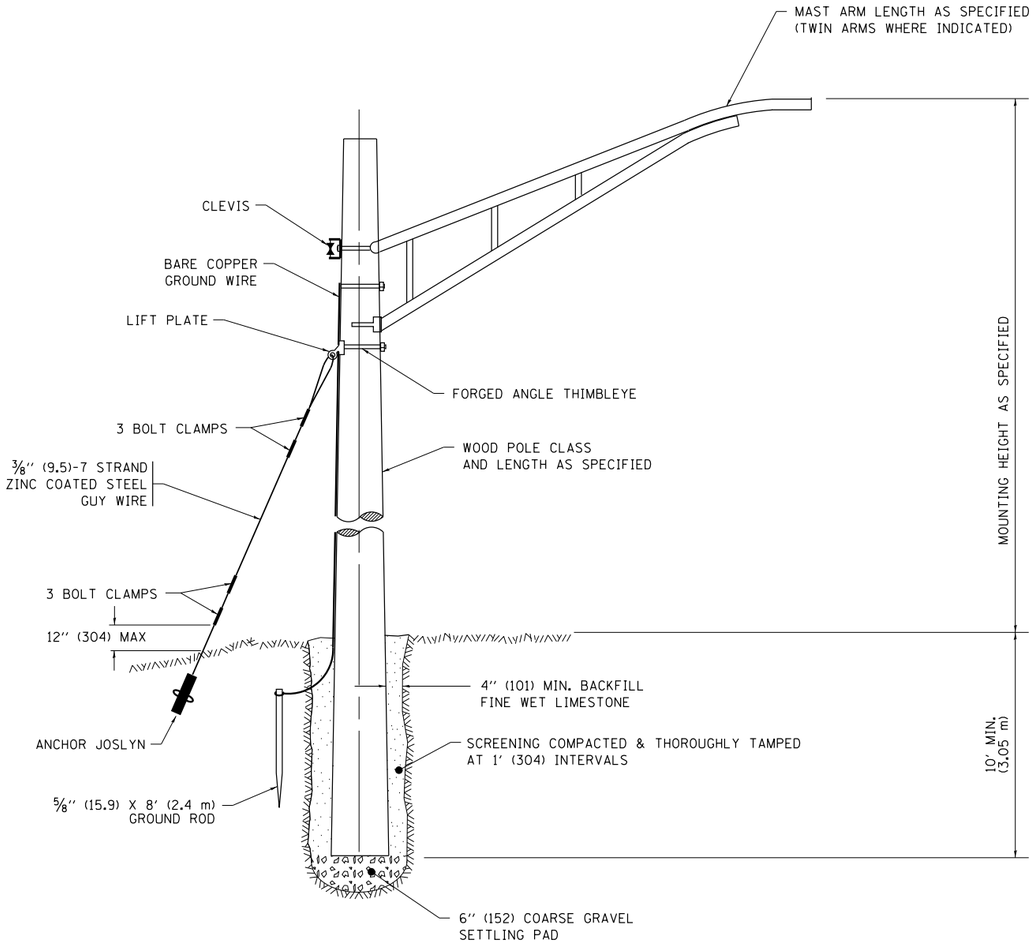
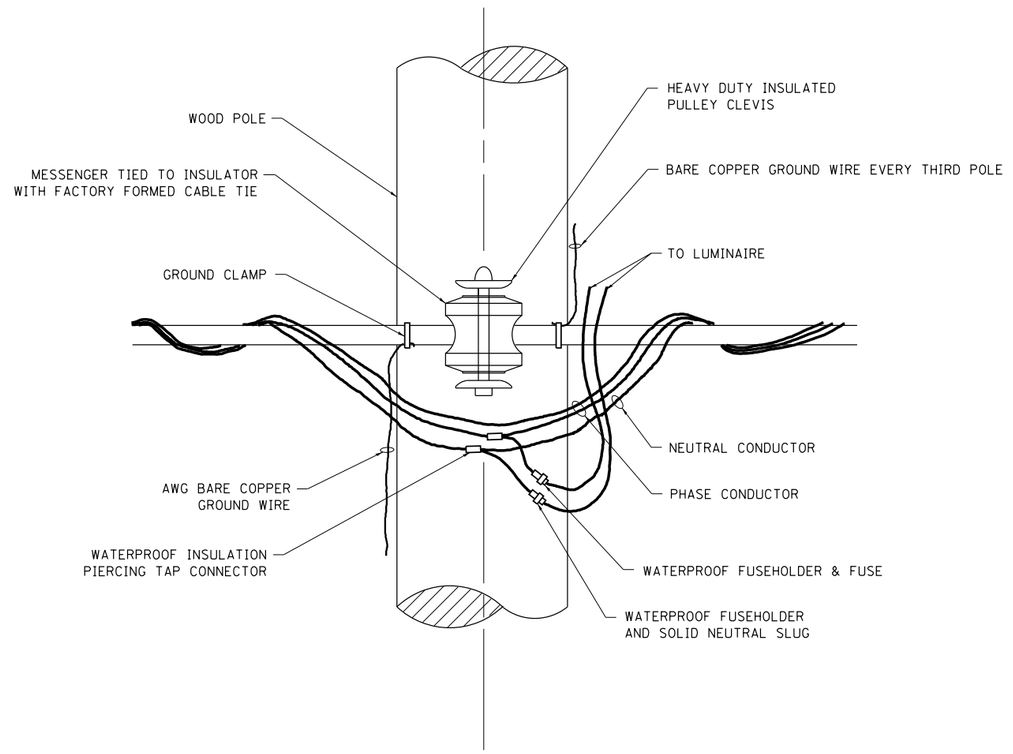
		DRAWN -	REVISED -					330	464-B	COOK	97	85
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED -						**BE-702**		CONTRACT NO. 60V22	
	PLOT DATE = 1/4/2008	DATE -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



TEMPORARY LIGHT POLE DETAIL



TEMPORARY LIGHT POLE ATTACHMENT DETAIL

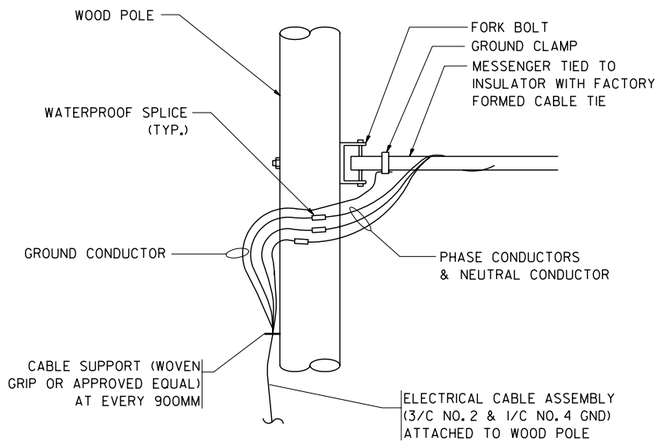
NOTE:

1. ALL DIMENSIONS IN INCHES (MILLIMETERS) UNLESS OTHERWISE INDICATED.
2. MAST ARM SHALL BE RATED FOR THE SPECIFIED MOUNTING HEIGHT.

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\Dist\$DRAWN\CADData\CADsheets\be800.dgn	USER NAME = footemj	DESIGNED -	REVISED - 08-08-03	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TEMPORARY LIGHT POLE DETAILS
	PLOT SCALE = 50.000 ' / in.	CHECKED -	REVISED - R.T. 07-26-16		
Default	PLOT DATE = 9/1/2016	DATE -	REVISED -		SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

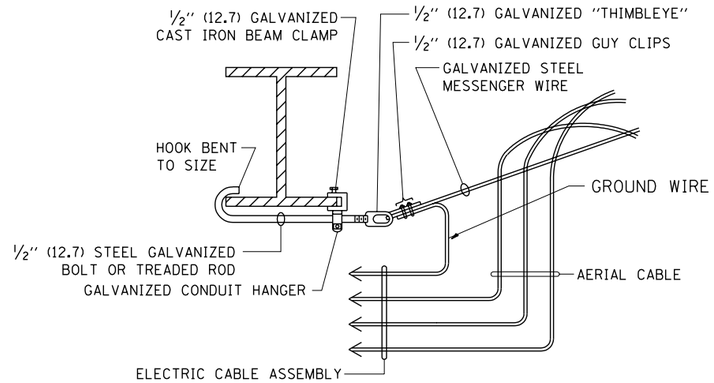
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	86
	BE-800	CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

be800.dgn 9/1/2016 3:02:44 PM User:footemj



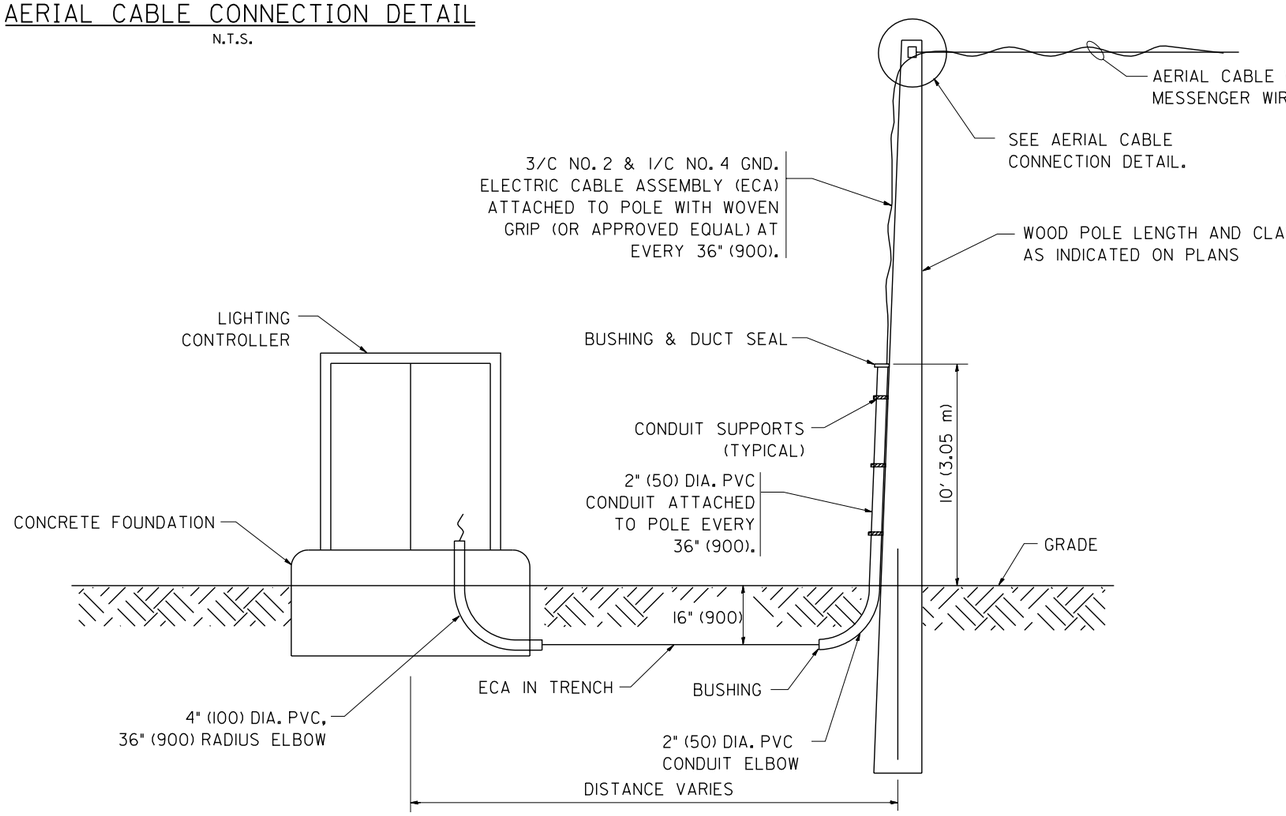
AERIAL CABLE CONNECTION DETAIL

N.T.S.



AERIAL CABLE ATTACHED TO STRUCTURE

NOT TO SCALE



WOOD POLE TO LIGHTING CONTROLLER WIRING CONNECTION DETAIL

N.T.S.

NOTES:

1. ALL DIMENSIONS IN INCHES (MILLIMETERS) UNLESS OTHERWISE INDICATED.
2. SEE PROPOSED LIGHTING PLAN FOR CONDUIT, CABLE AND ROUTING.
3. THE CONTRACTOR SHALL PROVIDE INTERMEDIATE SUPPORTS TO MAINTAIN MINIMUM CLEARANCES. REFER TO AERIAL AERIAL CABLE ATTACHED TO STRUCTURE DETAIL.
4. COST OF SPLICES AND MOUNTING HARDWARE SHALL BE INCLUDED IN THE UNIT PRICE FOR AERIAL CABLE.

FILE NAME = W:\diststd\22x34\be801.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED - 08-08-03
		DRAWN -	REVISED -
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 1/4/2008	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY AERIAL CABLE INSTALLATION

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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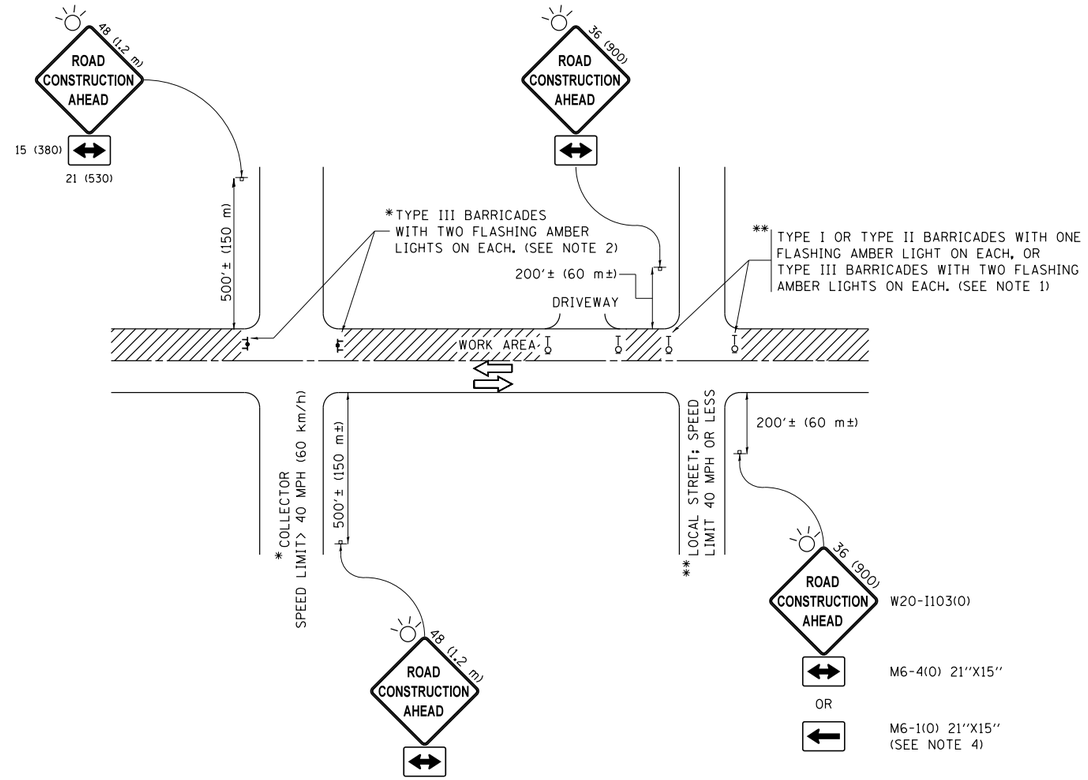
	BE-801	CONTRACT NO. 60V22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE ''ROAD CONSTRUCTION AHEAD'' SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE ''ROAD CONSTRUCTION AHEAD'' SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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			REVISED -T. RAMMACHER 01-06-00
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Default	PLOT DATE = 9/15/2016	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A. P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	88
	TC-10	CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

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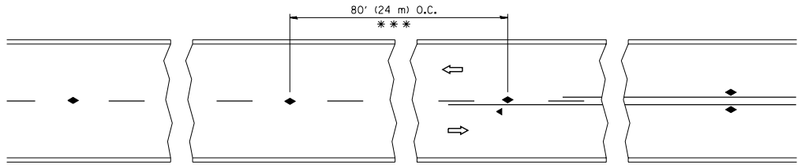
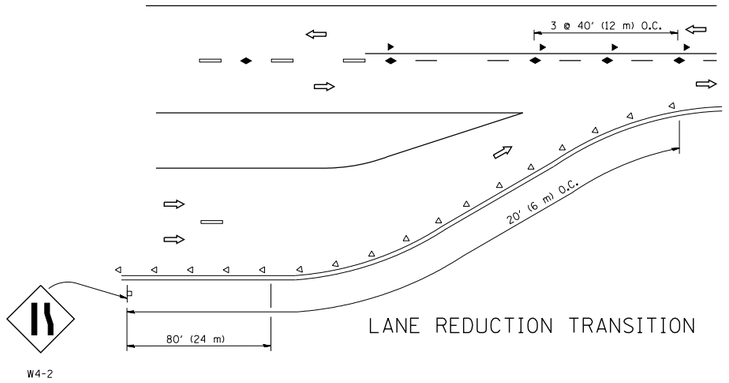
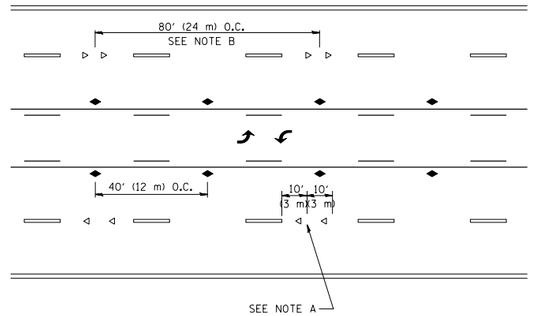


* * * REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

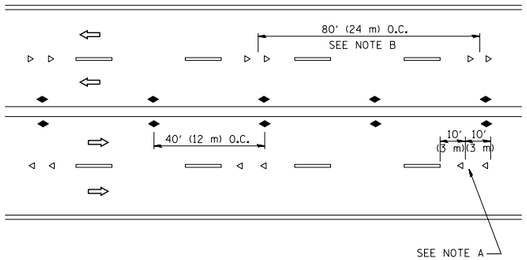
TWO-LANE/TWO-WAY



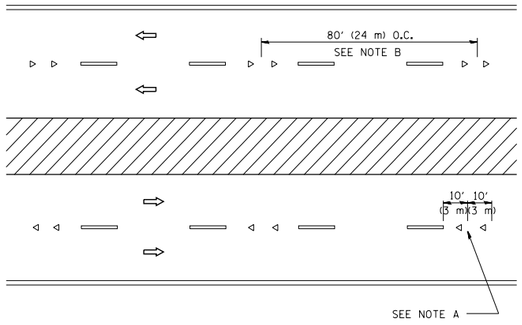
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

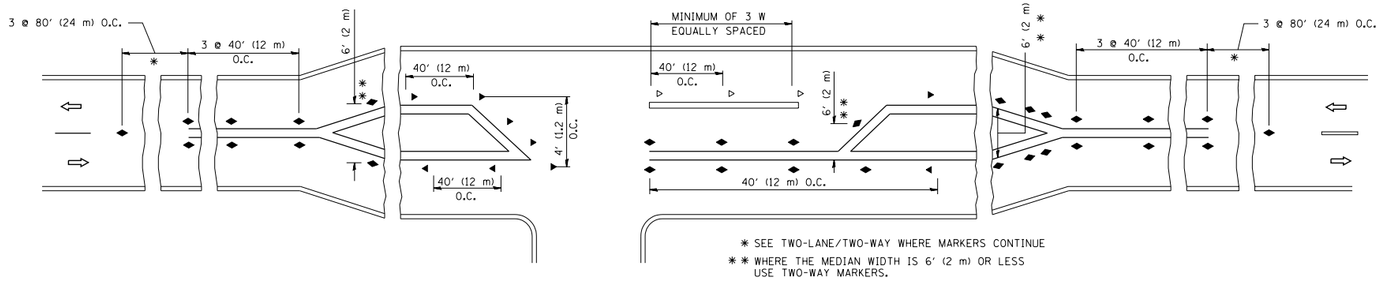
SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

LANE MARKER NOTES

A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE

** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = c:\pw_work\pwidot\leysa\d0108315\tc11.dgn	USER NAME = leysa	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			F.A.R RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - T. RAMMACHER 03-12-99					330	464-B	COOK	97	89
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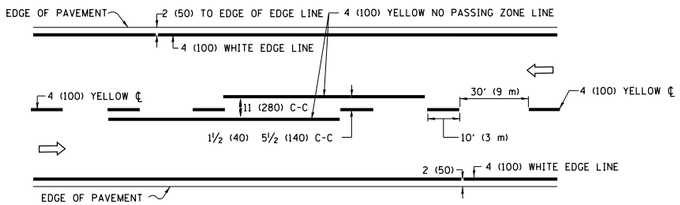
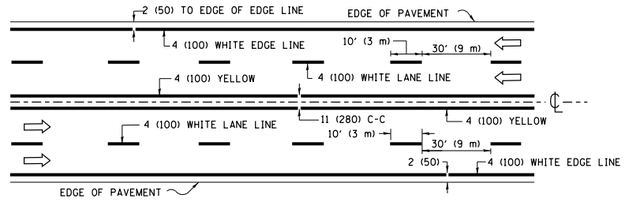
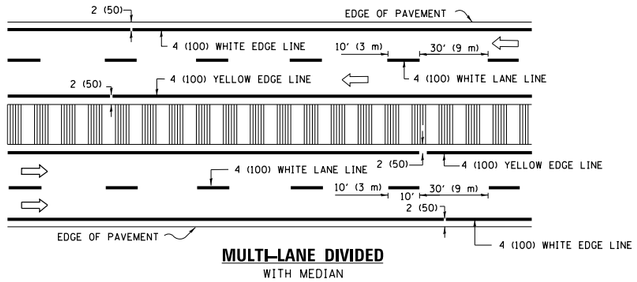
	PLOT DATE = 3/2/2011	DATE -	REVISED - C. JUCIUS 09-09-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.		FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	



2–LANE ROADWAY

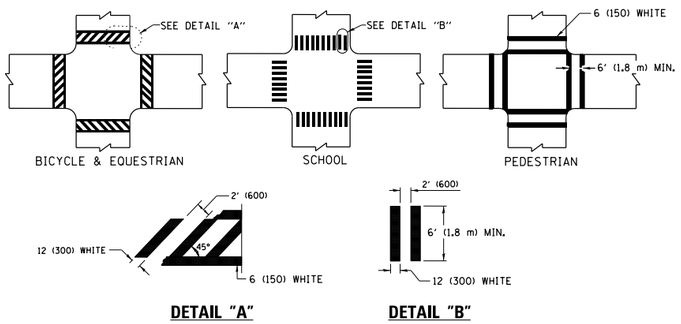


MULTI–LANE UNDIVIDED



MULTI–LANE DIVIDED
WITH MEDIAN

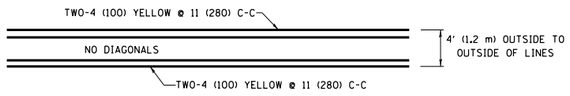
TYPICAL LANE AND EDGE LINE MARKING



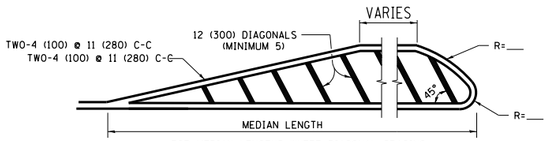
DETAIL "A" **DETAIL "B"**

TYPICAL CROSSWALK MARKING

✳ MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



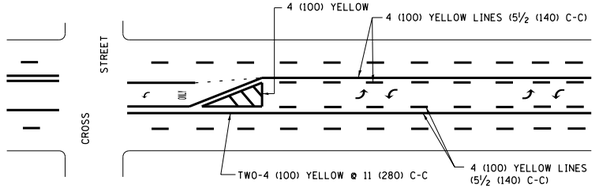
4' (1.2 m) WIDE MEDIANS ONLY



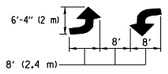
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

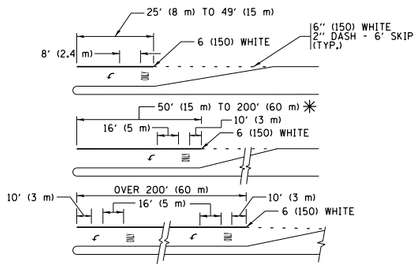


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO–WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

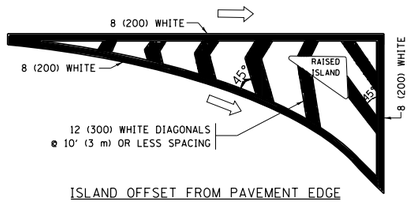


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
↰ AREA = 15.6 SQ. FT. (1.5 m^2) **ONLY** AREA = 20.8 SQ. FT. (1.9 m^2)

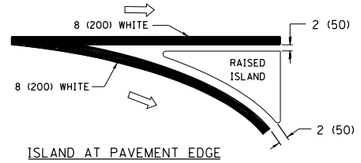
✳ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

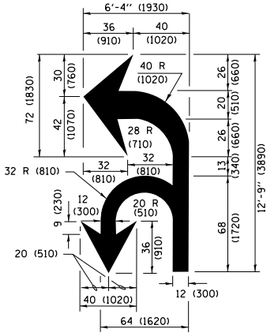


ISLAND OFFSET FROM PAVEMENT EDGE

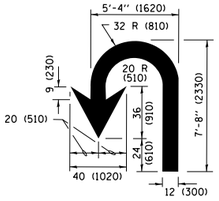


ISLAND AT PAVEMENT EDGE

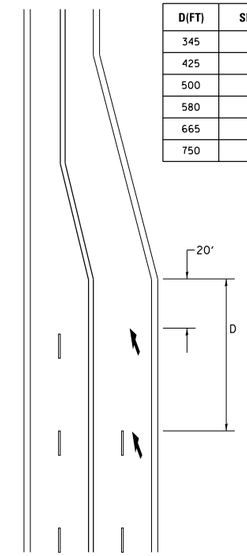
TYPICAL ISLAND MARKING



COMBINATION LEFT AND U–TURN



U–TURN



LANE REDUCTION TRANSITION

✳ LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 ⊠ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 ⊠ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 ⊠ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 ⊠ 6 (150) 12 (300) ⊠ 45° 12 (300) ⊠ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 ⊠ 4 (100) WITH 12 (300) DIAGONALS ⊠ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS ⊠ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m^2) EACH "X"=54.0 SQ. FT. (5.0 m^2)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) ⊠ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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	PLOT DATE = 4/13/2016	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15 REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	90
	TC–13	CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

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TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

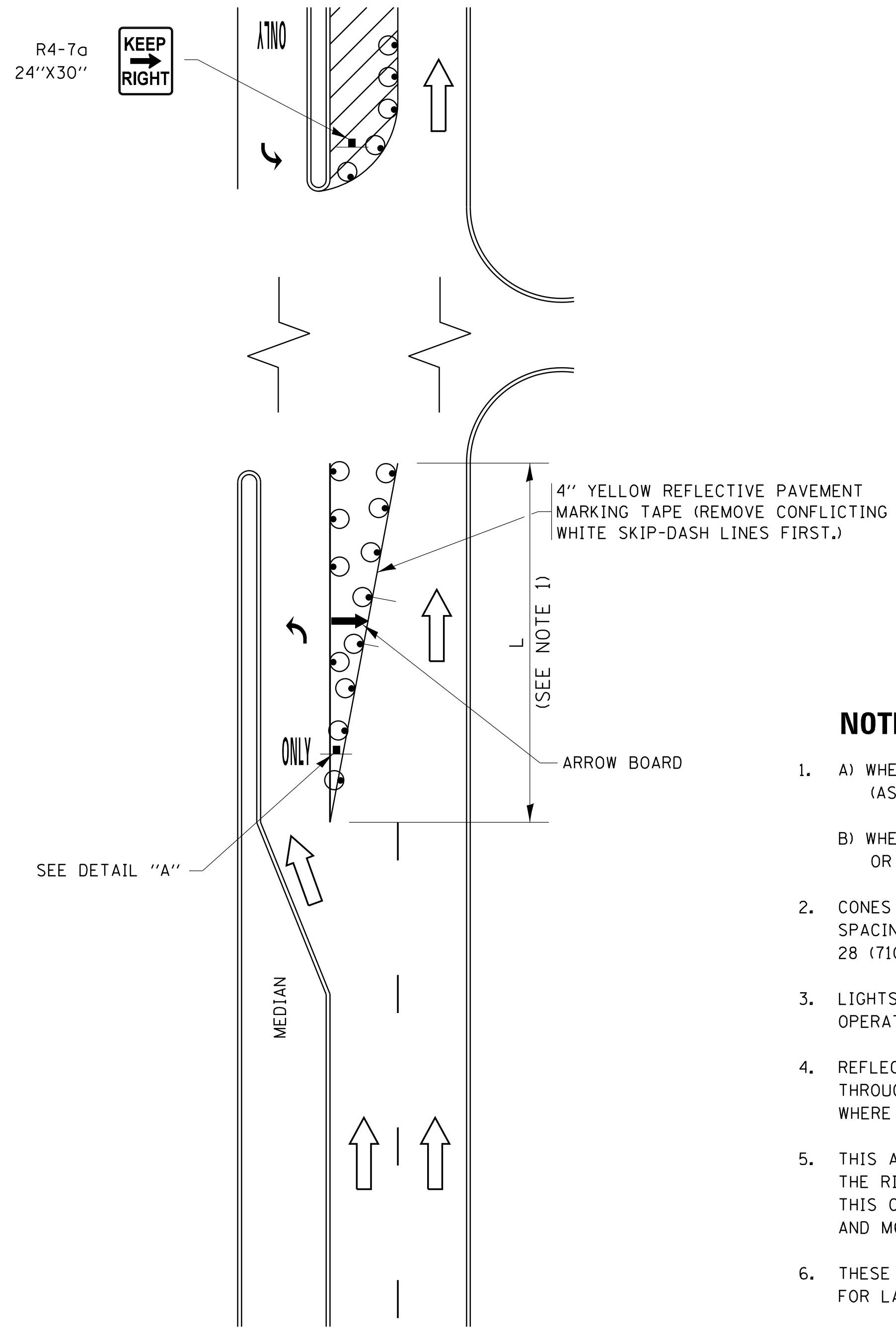


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

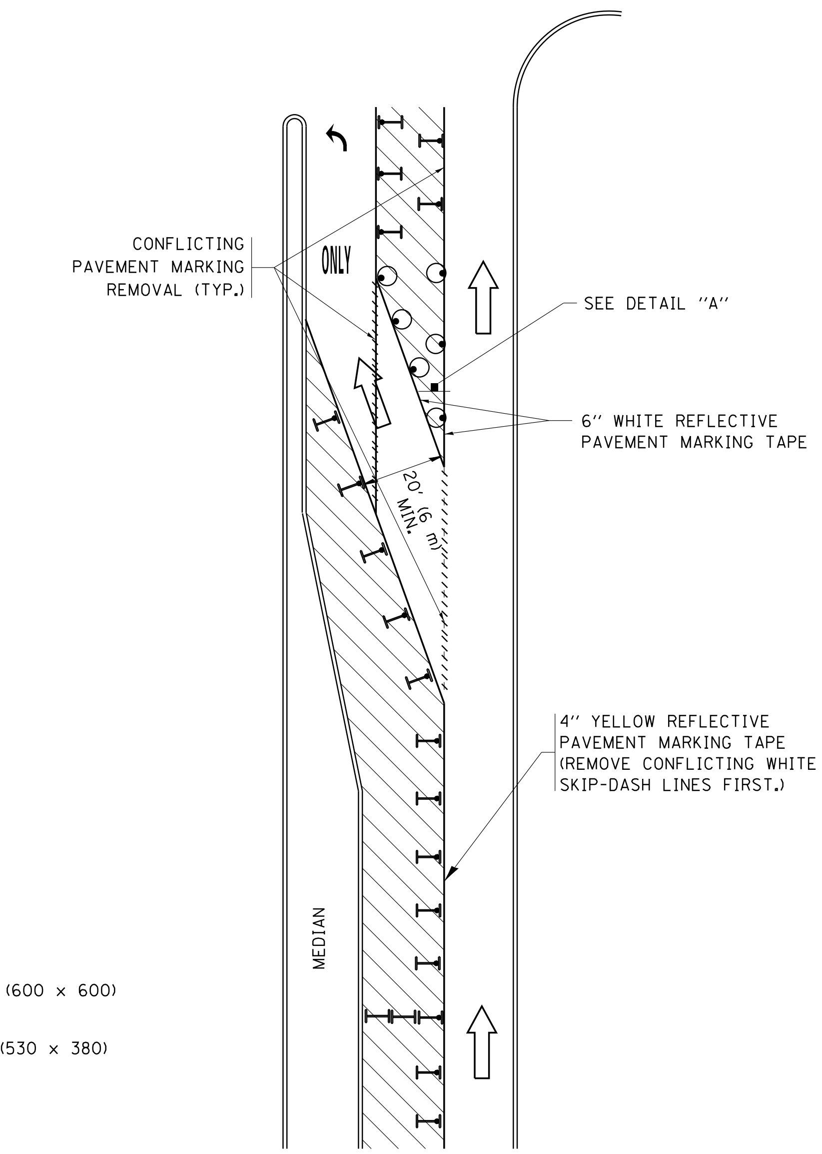


FIGURE 2

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

1. A) WHEN "L" IS ≤ THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.

   B) WHEN "L" IS > THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.

2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.

3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.

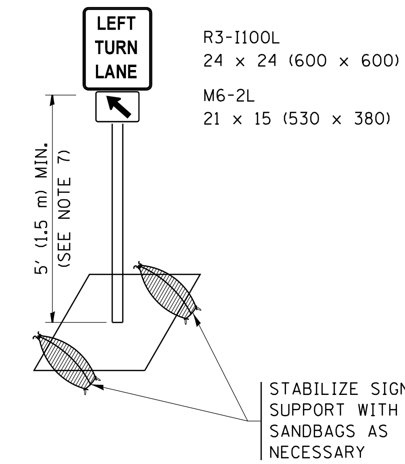
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.

5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.

6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.

7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.

8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

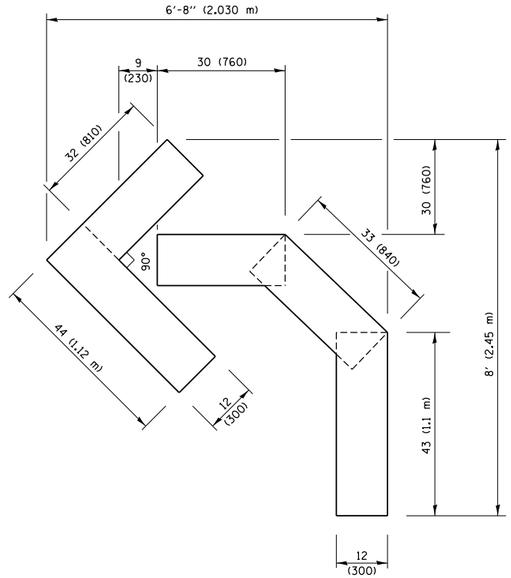


DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

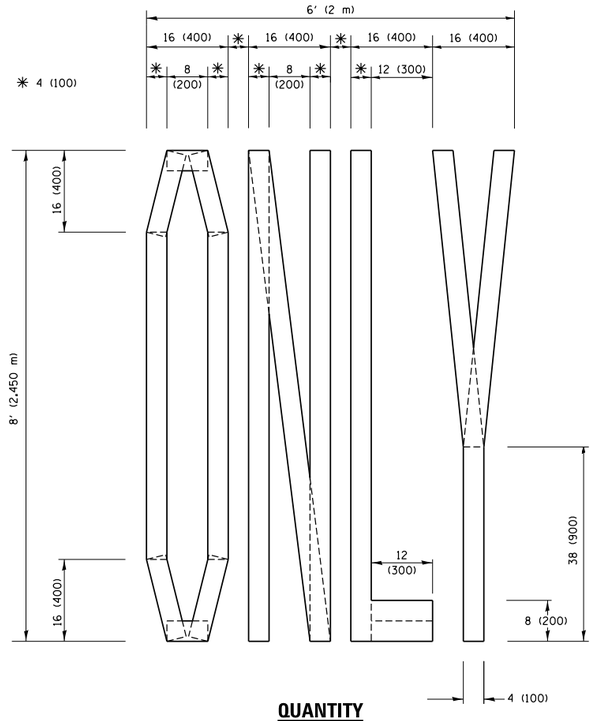
FILE NAME =	USER NAME = footemj	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\Dist...		REVISED - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13		
	PLOT SCALE = 50.0000 ' / in.	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16		
Default	PLOT DATE = 9/15/2016	REVISED - T. RAMMACHER 01-06-00	REVISED -		SCALE: NONE · SHEET 1 OF 1 SHEETS · STA. · TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	91
	TC-14	CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			



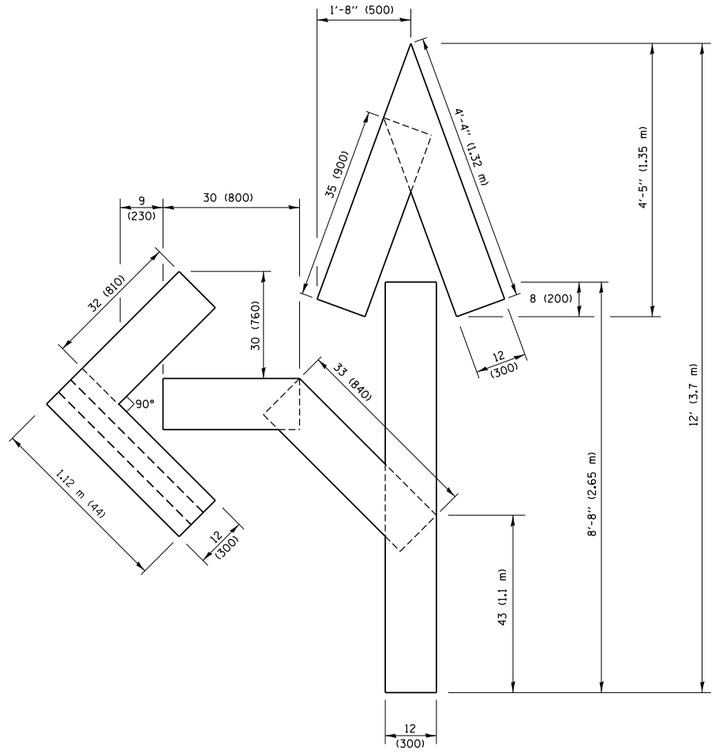
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

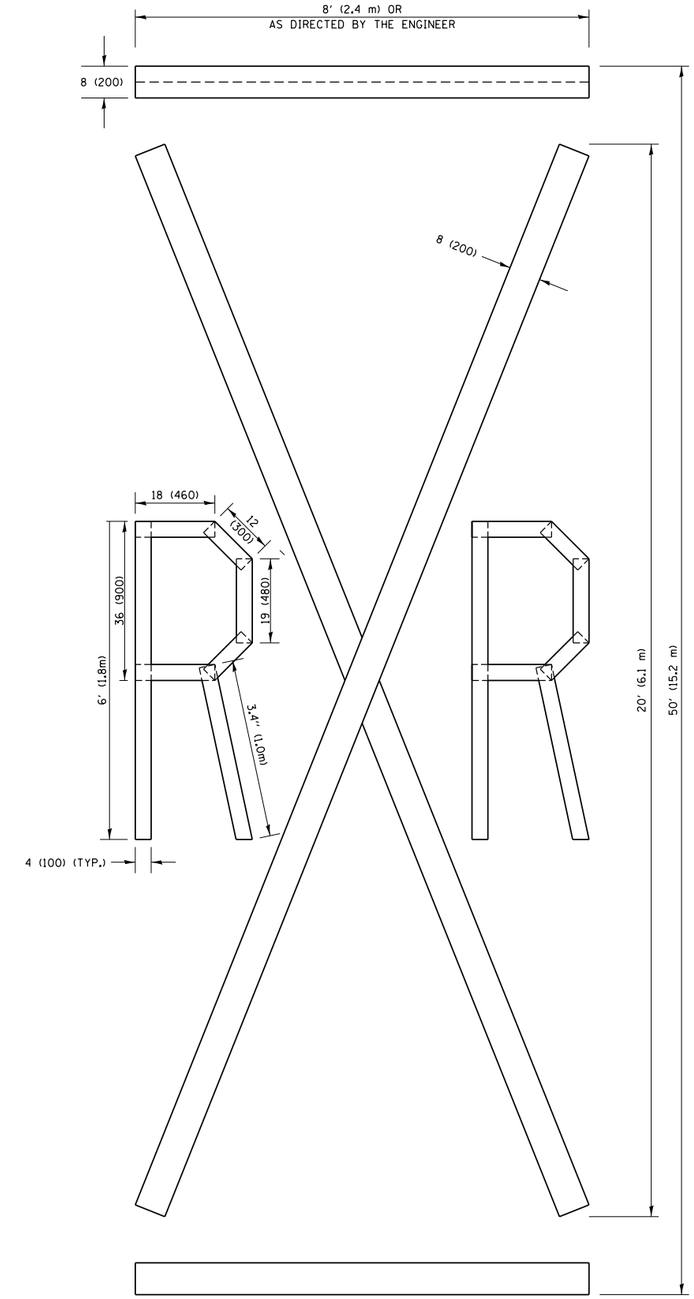


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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			REVISED -E. GOMEZ 08-28-00
	PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED -E. GOMEZ 08-28-00
	PLOT DATE = 9/15/2016	DATE - 09-18-94	REVISED -A. SCHUETZE 09-15-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	92
	TC-16	CONTRACT NO. 60V22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

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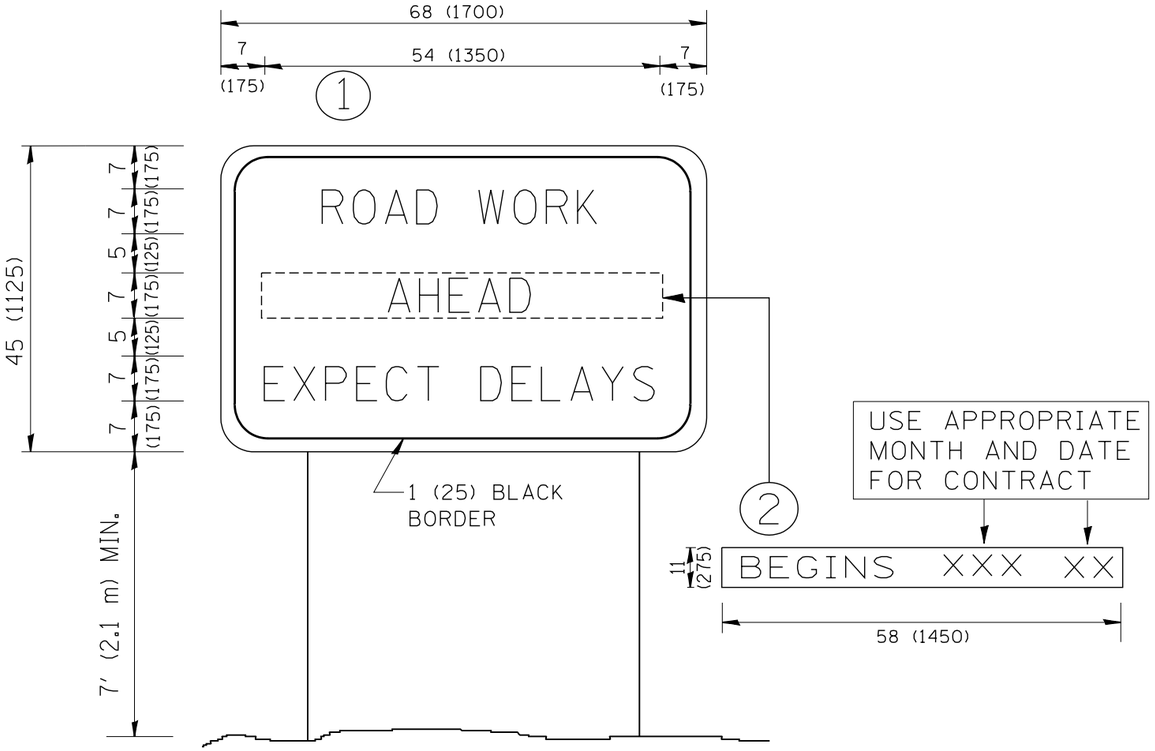


NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE ''ROAD CONSTRUCTION AHEAD'' SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR ''TEMPORARY INFORMATION SIGNING'' FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

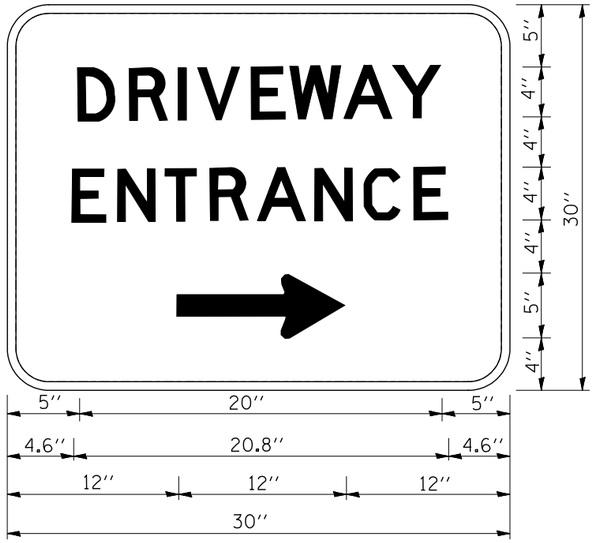
FILE NAME = W:\diststd\22x34\tc22.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED - R. MIRS 09-15-97
		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD INFORMATION SIGN

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	93
	TC-22	CONTRACT NO. 60V22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



3.0'' RADIUS, 0.5'' BORDER, WHITE ON GREEN; REFLECTORIZED
''DRIVEWAY'' D; ''ENTRANCE'' D; STANDARD ARROW CUSTOM 12.0'' x 5.0''

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM ''TEMPORARY INFORMATION SIGNING''.

FILE NAME = W:\diststd\22x34\tc26.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED - C. JUCIUS 02-15-07
		DRAWN -	REVISED -
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 1/4/2008	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

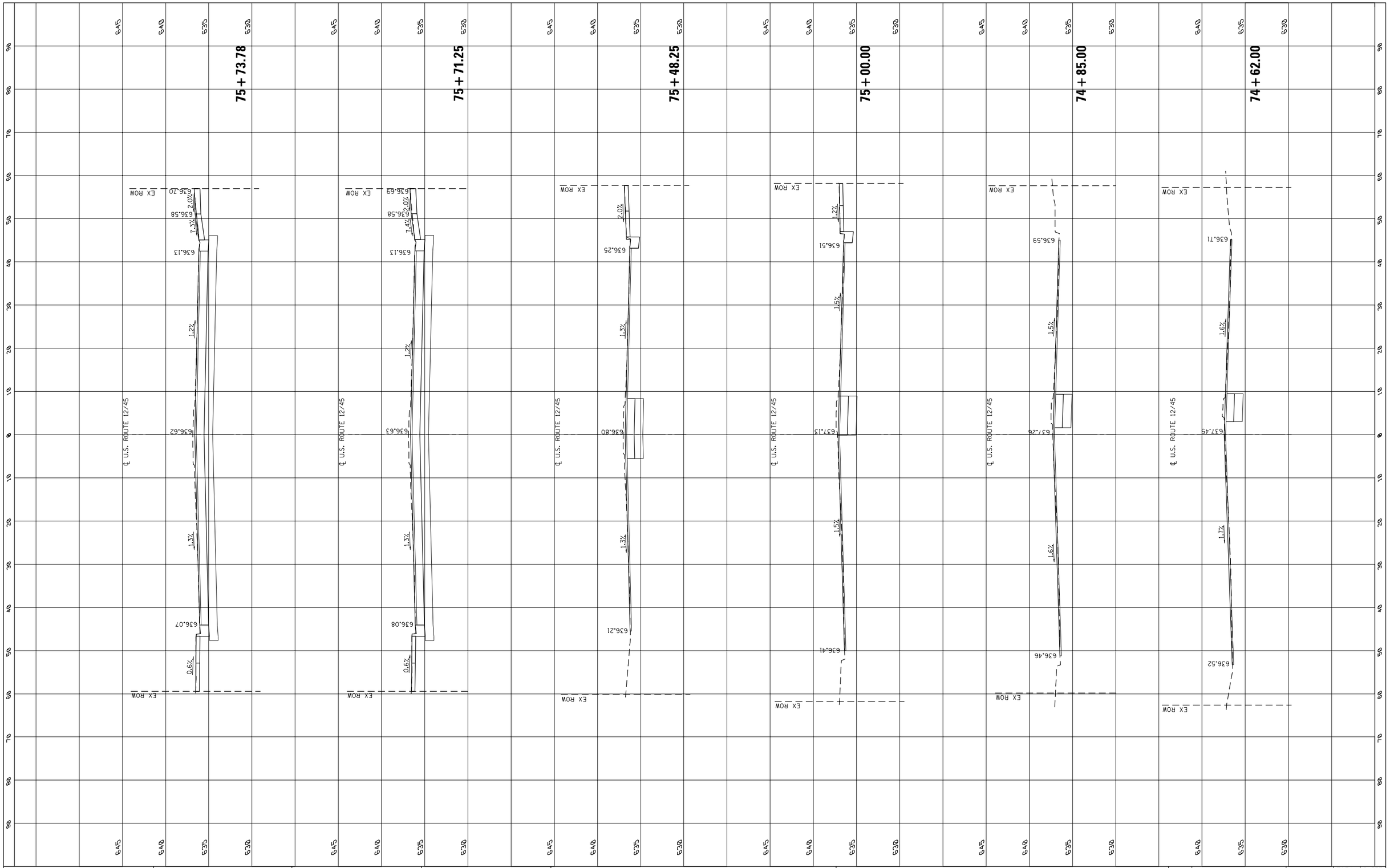
DRIVEWAY ENTRANCE SIGNING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	94
	TC-26	CONTRACT NO. 60V22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

FINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
TEMPLATE		
NOTE BOOK	AREAS	
NO.	AREAS CHECKED	

ORIGINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
TEMPLATE		
NOTE BOOK	AREAS	
NO.	AREAS CHECKED	



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

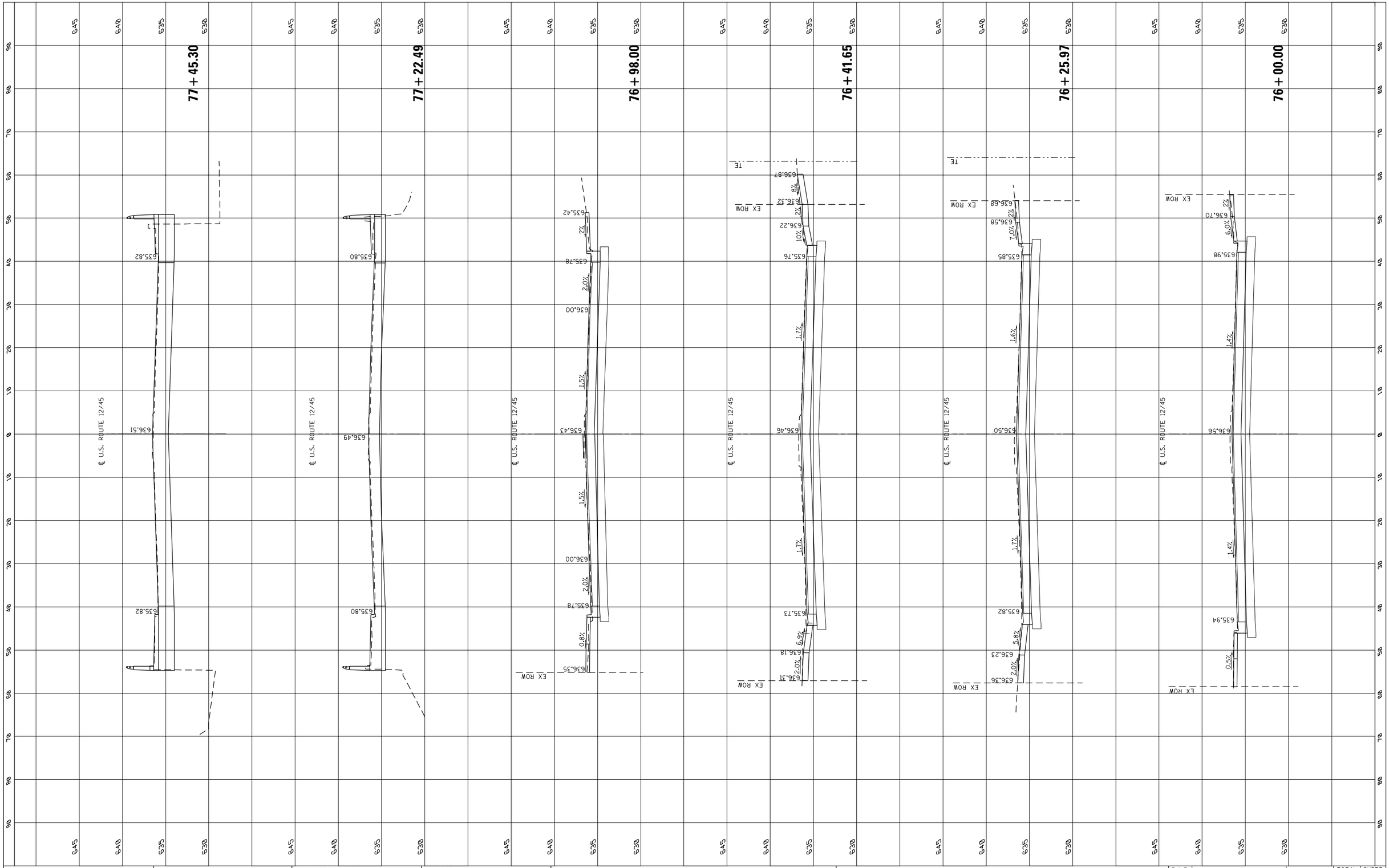
US 12/45 (MANNHEIM ROAD) OVER ADDISON CREEK
CROSS SECTIONS

SCALE: | SHEET 1 OF 3 SHEETS | STA. 74+71.19 | TO STA. 75+73.78

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	95
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

FINAL SURVEY	BY	DATE
NOTE BOOK	SURVEYED	
NO.	PLOTTED	
	TEMPLATE	
	AREAS	
	AREAS CHECKED	

ORIGINAL SURVEY	BY	DATE
NOTE BOOK	SURVEYED	
NO.	PLOTTED	
	TEMPLATE	
	AREAS	
	AREAS CHECKED	



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	PLOT SCALE = 20.0000 ' / in.	CHECKED - TVN	REVISED -
$MODELNAME$	PLOT DATE = 12/12/2017	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

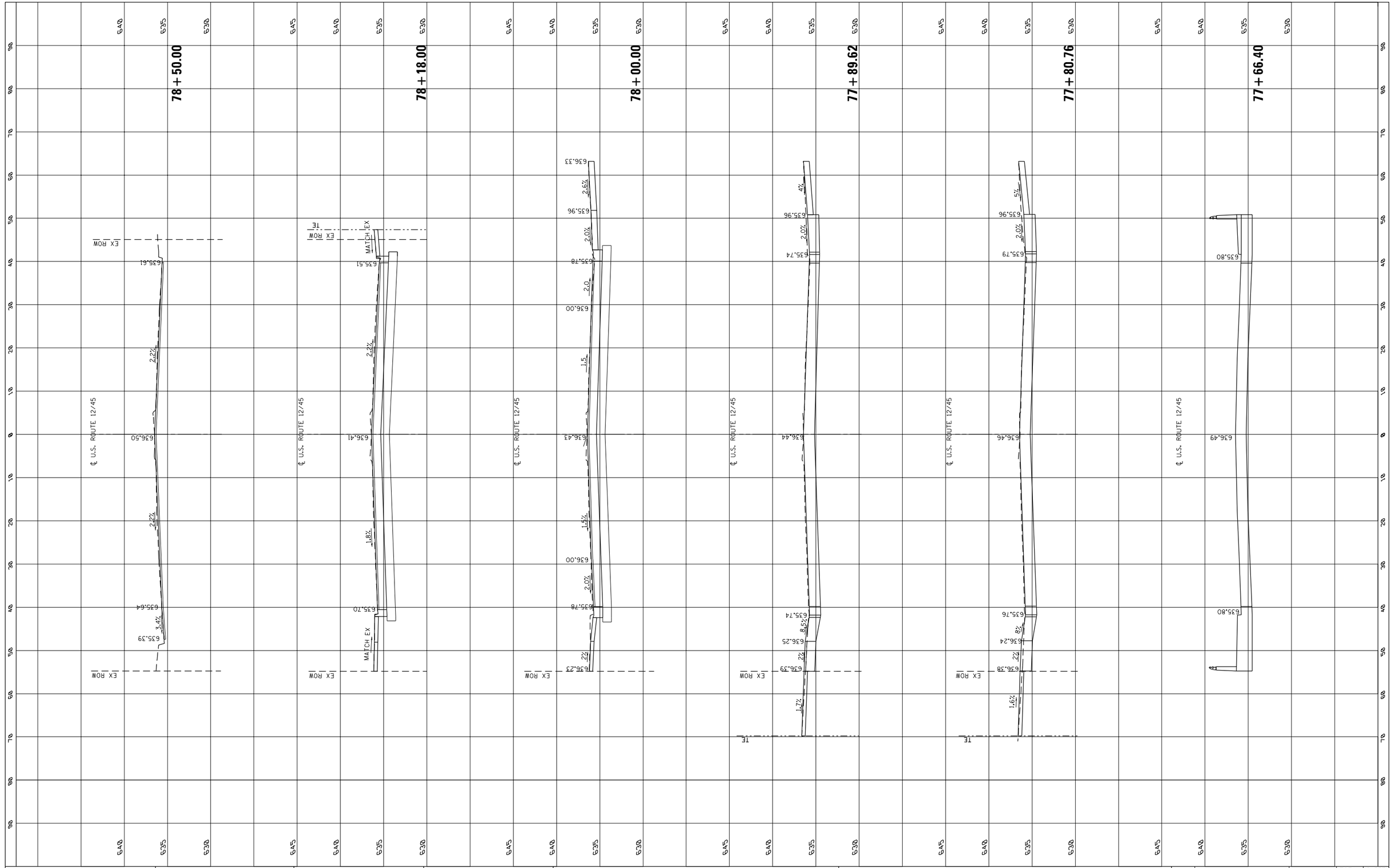
US 12/45 (MANNHEIM ROAD) OVER ADDISON CREEK
CROSS SECTIONS

SCALE: | SHEET 2 OF 3 SHEETS | STA. 76+00.00 TO STA. 77+45.30

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	96
		CONTRACT NO. 60V22		
	ILLINOIS FED. AID PROJECT			

FINAL SURVEY	BY	DATE
NOTE BOOK	SURVEYED	
NO.	PLOTTED	
	TEMPLATE	
	AREAS	
	AREAS CHECKED	

ORIGINAL SURVEY	BY	DATE
NOTE BOOK	SURVEYED	
	PLOTTED	
	TEMPLATE	
	AREAS	
NO.	AREAS CHECKED	



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$MODELNAME$	PLOT DATE = 12/15/2017	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

US 12/45 (MANNHEIM ROAD) OVER ADDISON CREEK
CROSS SECTIONS

SCALE: | SHEET 3 OF 3 SHEETS | STA. 77+61.00 | TO STA. 78+37.60

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464-B	COOK	97	97
		CONTRACT NO. 60V22		
	ILLINOIS	FED. AID PROJECT		