03-09-2018 LETTING ITEM 124

F. A. U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
3752	10-00045-00-WR	WILL	109	1
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

CONTRACT #61D22

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PLANS FOR PROPOSED FEDERAL AID HIGHWAY

ST. FRANCIS ROAD (FAU 3752)
88TH AVENUE TO 80TH AVENUE
RECONSTRUCTION,RESURFACING, BIKE-PED PATH
SECTION NO.: 10-00045-00-WR
PROJECT NO.: B8RT(752)
VILLAGE *of* FRANKFORT
WILL COUNTY
C-91-137-15

INDEX OF SHEETS
SEE SHEET NO. 2

HIGHWAY STANDARDS
SEE SHEET NO. 2

DESIGN DESIGNATION – ST FRANCIS ROAD ADT 13,000 (2040) – LOCAL ROAD
PV=10561 SU=445 MU=111
% DESIGN TRAFFIC IN DESIGN LANE
P=95% S=4% M=1%
ACTUAL TF=.94
SSS=POOR

	ST FRANCIS ROAD
2011 ADT –	8,800
2040 ADT –	13,000
POSTED SPEED LIMIT –	40–45 mph
DESIGN PERIOD –	20 YEARS
DESIGN SPEED LIMIT –	45 mph
STREET CLASSIFICATION –	CLASS II

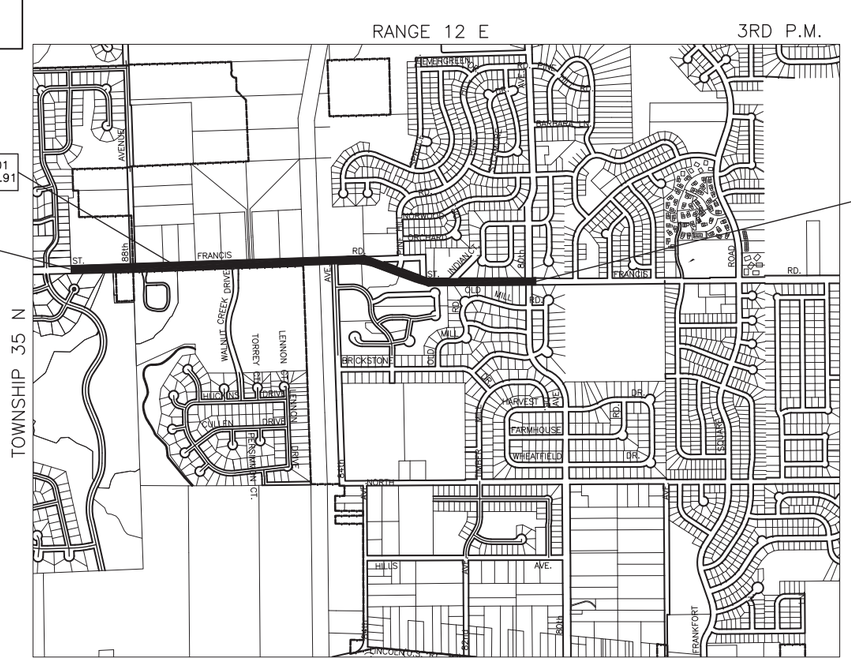
SCALES
PLAN - 1"=50'
PROFILE HORIZ. - 1"=50'
PROFILE VERT. - 1"=5'
CROSS SECTIONS - 1"=10'

100' 200' 300' - 1"= 100'
0 10' 20' 30' - 1"= 10'
0 50' 100' - 1"= 50'
0 50' 100' - 1"= 40'
0 50' 100' - 1"= 30'
0 50' 100' - 1"= 20'

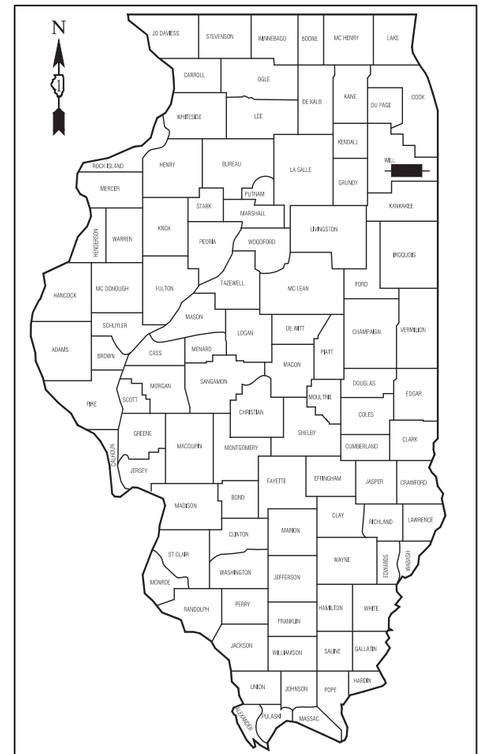
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J. U. L. I. E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1 - 800 - 892 - 0123 or 811

CONTRACT NO. 61D22



LOCATION MAP
GROSS LENGTH=6,188 FEET=1.17 MILES
NET LENGTH=6,188 FEET=1.17 MILES



LOCATION OF SECTION INDICATED THUS: -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
Approved 11-27-2017
Mayor, Village of Frankfort
Passed DECEMBER 12, 2017
District 1 Engineer of Local Roads & Streets
Released for Bid Based on Limited Review DECEMBER 12, 2017
REGIONAL ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

PREPARED BY OR UNDER THE DIRECT SUPERVISION OF:
11-20-2017

HARRY L. GILMORE, JR. 62-38376 REGISTERED PROFESSIONAL ENGINEER STATE OF ILLINOIS
LICENSE EXPIRES: 11/30/19

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE P.E. 847-705-4406 SCHAUMBURG, IL
CONSULTANTS: ROBINSON ENGINEERING, LTD. 708-331-6700

INDEX OF SHEETS

HIGHWAY STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
424001-10	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424016-04	MID-BLOCK CURB RAMPS
442201-03	CLASS C AND D PATCHES
515001-03	NAME PLATE FOR BRIDGES
542001-06	CONCRETE END SECTIONS FOR PIPE CULVERTS 15" (375mm) THRU 84" (2100mm) DIA.
542301-03	PRECAST REINFORCED CONCRETE FLARED END SECTION
542306-03	PRECAST REINFORCED CONCRETE ELLIPTICAL FLARED END SECTION
602001-02	CATCH BASIN TYPE A
602301-04	INLET - TYPE A
602401-04	MANHOLE TYPE A
602411-06	MANHOLE TYPE A, 7' DIAMETER
602601-05	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
602701-02	MANHOLE STEPS
604001-04	FRAME AND LIDS TYPE 1
604036-03	GRATE TYPE 8
604051-04	FRAME AND GRATE TYPE 11
604056-04	FRAME AND GRATE TYPE 11V
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
630001-12	STEEL PLATE BEAM GUARDRAIL
630301-08	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
701001-02	OFF-RD OPERATIONS, 2L, 2W, MORE THAN 15'(4.5m) AWAY
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15'(4.5m) TO 24" (600m) FROM PAVEMENT EDGE
701101-05	OFF-RD OPERATIONS, MULTILANE, 15'(4.5m) TO 24"(600 mm) FROM PAVEMENT EDGE
701201-04	LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS $\geq$ 45 MPH
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-04	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY FOR SPEEDS $\geq$ 45 MPH
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS DAY ONLY
701321-17	LANE CLOSURE, 2L, 2W, BRIDGE REPAIR WITH BARRIER
701326-04	LANE CLOSURE, 2L, 2W, PAVEMENT WIDENING, FOR SPEEDS $\geq$ 45 MPH
701426-09	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION FOR SPEEDS $\geq$ 45 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701611-01	URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-07	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
720011-01	METAL POSTS FOR SIGNS, MARKERS & DELINEATORS
725001-01	OBJECT AND TERMINAL MARKERS
729001-01	APPLICATIONS OF TYPES A&B METAL POSTS (FOR SIGNS AND MARKERS)
780001-05	TYPICAL PAVEMENT MARKINGS
782006	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS
BLR 24-2	MAILBOX TURNOUT FOR LOCAL ROADS

FILE NAME = 10423_12-INDX-01 - IDOT INDX-1

USER NAME =	DESIGNED — GA	REVISED —	
	CHECKED — HLG	REVISED —	
PLOT SCALE =	DRAWN — MED	REVISED —	
PLOT DATE = 06-09-16	CHECKED — AGP	REVISED —	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
INDEX OF SHEETS & STATE STANDARDS

SCALE: NONE | SHEET NO. 2 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	2
		CONTRACT NO. 61D22		

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT ----

GENERAL NOTES

1. ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED TO BE THE LATEST STANDARDS OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

2. ITEMS OF WORK LISTED IN THE SUMMARY OF QUANTITIES WHICH ARE NOT SPECIFICALLY INDICATED IN THE PLANS SHALL BE PERFORMED AT LOCATIONS AS DIRECTED BY THE ENGINEER.

3. DRAINAGE STRUCTURE ELEVATIONS: GRADES OF SEWER LINES WERE DETERMINED FROM AVAILABLE PLANS AND SURVEYS. ACCORDINGLY, AS DIRECTED BY THE ENGINEER, THE INVERTS OF THE PROPOSED DRAINAGE WILL BE REVISED TO MEET EXISTING FIELD CONDITIONS.

4. THE TOP OF ALL STRUCTURES SHALL BE FLUSH WITH THE ADJACENT SURFACE OR AT THE INDICATED ELEVATIONS SHOWN ON THE PLANS. ALL RIM ELEVATIONS OF STRUCTURES IN THE PROPOSED CURB LINE ARE GIVEN TO THE EDGE OF PAVEMENT. ALL OTHER RIM ELEVATIONS ARE GIVEN TO THE CENTER OF THE STRUCTURES.

5. FRAME ELEVATIONS ARE GIVEN ONLY TO ASSIST IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF THE STRUCTURE. FRAMES ON ALL NEW STRUCTURES WILL BE ADJUSTED TO THE FINAL ELEVATION OF THE AREA IN WHICH THEY ARE LOCATED AS PART OF THE STRUCTURE COST.

6. THE CAST IRON FRAMES AND COVERS OF FILLED, ABANDONED OR REMOVED MANHOLES, INLETS AND CATCH BASINS OR THOSE FRAMES AND COVERS UPON STRUCTURES RECEIVING NEW FRAMES AND COVERS SHALL BE STOCKPILED WITHIN THE RIGHT-OF-WAY, AS DIRECTED BY THE ENGINEER, AND PICKED UP BY FRANKFORT.

7. THE APPROXIMATE LOCATION OF KNOWN PUBLIC UTILITIES ARE SHOWN ON THE PLANS. HOWEVER, THE DEPARTMENT DOES NOT GUARANTEE THEIR ACCURACY. PRIOR TO COMMENCING OPERATIONS ON THE PROJECT WHICH MAY IN ANY WAY CREATE THE POSSIBILITY OF INVOLVEMENT WITH EXISTING UTILITIES, THE CONTRACTOR SHALL CONTACT THE FIRM (OR COMMUNITY) INVOLVED. ADJUSTMENT OF ALL PUBLIC UTILITIES WITHIN THE LIMITS OF THIS IMPROVEMENT WILL BE DONE BY THE RESPECTIVE OWNERS. NO ADDITIONAL COMPENSATION WILL BE ALLOWED DUE TO DELAYS OR INCONVENIENCE CAUSED BY THESE ADJUSTMENTS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY LOCATIONS OF UNDERGROUND INSTALLATION BEFORE STARTING CONSTRUCTION OPERATIONS.

8. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 SHALL BE INSTALLED WITH A MINIMUM THICKNESS EQUAL TO 9".

9. THE CONTRACTOR SHALL PROTECT ALL TREES WITHIN AND ADJACENT TO THE CONSTRUCTION SITE DURING THE CLEARING AND SUBSEQUENT CONSTRUCTION OPERATIONS IN ACCORDANCE WITH SECTION 201 OF THE STANDARD SPECIFICATIONS. THOSE TREES TO BE REMOVED AS SHOWN IN THE PLANS SHALL BE DONE IN ACCORDANCE WITH SECTION 201 AND 202 OF THE STANDARD SPECIFICATIONS.

10. BEFORE STARTING EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED).

11. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES.

12. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON VILLAGE PROPERTY WITHOUT WRITTEN PERMISSION FROM ENGINEER AND THE VILLAGE.

13. THE CONTRACTOR SHALL CONTACT ROBINSON ENGINEERING (708) 331-6700, THE VILLAGE OF FRANKFORT DEVELOPMENT COORDINATOR MR. ADAM NIELSEN (815) 469-2177 AND THE FRANKFORT TOWNSHIP HIGHWAY COMMISSIONER MR. BILL CARLSON (708) 479-9673 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

14. ALL HMA PAVING SHALL FOLLOW DESIGNATED DRIVING LANES AS SHOWN IN STRIPING DETAILS. NO LONGITUDINAL PAVING JOINT OR SEAMS ARE ALLOWED WITHIN THE DRIVING LANES. ALL LONGITUDINAL PAVING JOINTS OR SEAMS WILL BE BETWEEN THE DRIVING LANES.

15. AGGREGATE SUBGRADE IMPROVEMENT HAS BEEN PROVIDED FOR LOCATIONS WHERE SOILS TEND TO BE UNSTABLE WHEN WET. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH AGGREGATE SUBGRADE IMPROVEMENT WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE ENGINEER (BY USE OF A CONE PENETROMETER IN CONJUNCTION WITH THE IDOT SUBGRADE STABILITY MANUAL). IF UNSTABLE AND/OR UNSUITABLE MATERIALS ARE NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.

16. CONTRACTOR SHALL BE RESPONSIBLE AT ALL TIMES FOR TRAFFIC CONTROL AND PROTECTION IN ACCORDANCE WITH THE IDOT STANDARD SPECIFICATIONS ADOPTED APRIL 1, 2016, THE LATEST EDITION OF THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, AND THE STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS.

17. THE CONTRACTOR SHALL MAKE EVERY ATTEMPT NOT TO DAMAGE EXISTING TREES.

18. 10' TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER TO EXISTING CURB AND GUTTER OR TO TAPER FROM 6" TO 0", UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEM OF WORK SPECIFIED.

19. ALL STORM SEWERS, UNLESS OTHERWISE NOTED, SHALL CONFORM TO THE STATE SPECIFICATIONS FOR REINFORCED CONCRETE CULVERT, STORM DRAIN AND SEWER PIPE A.A.S.H.T.O. DESIGNATION M170 (A.S.T.M. DESIGNATION C76), (CLASS II AND IV).

20. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL DRIVEWAYS THROUGHOUT THE RECONSTRUCTION LIMITS AT ALL TIMES. IF DRIVEWAY ACCESS MUST BE RESTRICTED, THE CONTRACTOR SHALL NOTIFY THE RESIDENT IN WRITING 24 HOURS IN ADVANCE.

21. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS FOR THE PROTECTION OF EXISTING PLANT MATERIAL FOR WHICH THE CONTRACT DOES NOT PROVIDE REMOVAL. THE PROTECTION OF EXISTING PLANT MATERIAL AND THE REPAIR OR REPLACEMENT OF EXISTING PLANT MATERIAL DAMAGED BY THE CONTRACTOR SHALL BE DONE IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 201 OF THE STANDARD SPECIFICATIONS.

22. EROSION CONTROL WORK ITEMS ARE CONSIDERED TO BE HIGH PRIORITY ITEMS ON THIS CONTRACT. THE ENGINEER WILL IMPLEMENT ALL PROVISIONS OF THE SPECIFICATION NECESSARY TO ASSURE THAT EROSION CONTROL ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY WAY. ALL EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO BEGINNING ANY CONSTRUCTION ACTIVITIES.

23. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

24. SIGNAGE FOR ADVANCE NOTICE TO THE TRAVELING PUBLIC SHALL BE INSTALLED 10 DAYS PRIOR TO THE START OF WORK. CHANGEABLE MESSAGE SIGNS SHALL BE INSTALLED ON ST. FRANCIS ROAD IN EACH DIRECTION WITH WORDING AS DIRECTED BY THE ENGINEER.

25. NO TREE REMOVAL SHALL OCCUR UNTIL THE CONTRACTOR AND ENGINEER AGREE, ON SITE, ON THE REMOVAL QUANTITIES.

26. AREAS DISTURBED BY CONSTRUCTION SHALL BE KEPT TO A MINIMUM. ALL AREAS DISTURBED UNNECESSARILY SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE.

27. IN AREAS OF PAVEMENT WIDENING ALONG ST. FRANCIS ROAD, THE CONSTRUCTION OF THE BASE COURSE SHALL BE PAID FOR AS HMA BASE COURSE WIDENING, 8 1/4-INCH INCLUDING AREAS OF WIDENING GREATER THAN SIX (6) FEET.

SUMMARY OF QUANTITIES			UNIT	TOTAL QUANTITY	ROADWAY	BRIDGE	SAFETY	BIKE PATH	TRAINEES
					CONSTRUCTION TYPE CODE				
S.I.	CODE NO.	ITEM			0004	0013	0021	0028	0042
*	20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	100	50			50	
*	20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	15				15	
	20200100	EARTH EXCAVATION	CU YD	8125	6875			1250	
	20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	400	350			50	
	20400800	FURNISHED EXCAVATION	CU YD	725	725				
	20800150	TRENCH BACKFILL	CU YD	540	540				
	21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	14035	10895			3140	
*	25000310	SEEDING, CLASS 4	ACRE	0.25	0.25				
*	25000400	NITROGEN FERTILIZER NUTRIENT	POUND	180	140			40	
*	25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	180	140			40	
	25100900	TURF REINFORCEMENT MAT	SQ YD	185	185				
*	25200110	SODDING, SALT TOLERANT	SQ YD	14035	10895			3140	
*	25200200	SUPPLEMENTAL WATERING	UNIT	425	425				
	28000305	TEMPORARY DITCH CHECKS	FOOT	345	345				
	28000400	PERIMETER EROSION BARRIER	FOOT	2400	2400				
	28000500	INLET AND PIPE PROTECTION	EACH	10	10				
	28000510	INLET FILTERS	EACH	44	44				
	28100107	STONE RIPRAP, CLASS A4	SQ YD	925	60	865			
	28200200	FILTER FABRIC	SQ YD	925	60	865			
	30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	400	350			50	
	30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	14600	14600				
	35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	3320	2630			690	
	35101800	AGGREGATE BASE COURSE, TYPE B 6"	SQ YD	2800	2800				
	35501308	HOT-MIX ASPHALT BASE COURSE, 6"	SQ YD	3275	3275				

* – INDICATES SPECIALTY ITEMS

SUMMARY OF QUANTITIES			UNIT	TOTAL QUANTITY	ROADWAY	BRIDGE	SAFETY	BIKE PATH	TRAINEES
					CONSTRUCTION TYPE CODE				
S.I.	CODE NO.	ITEM			0004	0013	0021	0028	0042
	35501311	HOT-MIX ASPHALT BASE COURSE, 6 3/4"	SQ YD	12900	12900				
	40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	200	200				
	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	16	16				
	40600825	POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50	TON	475	475				
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	105	105				
	40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	685	75			610	
	40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	2085	2085				
	40700100	BITUMINOUS MATERIALS (TACK COAT)	POUND	14550	14122			428	
	40800025	BITUMINOUS MATERIALS (PRIME COAT)	POUND	12170	7955			4215	
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	7025				7025	
	42400800	DETECTABLE WARNINGS	SQ FT	375				375	
	44000100	PAVEMENT REMOVAL	SQ YD	12500	12500				
	44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	10800	10800				
	44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	210	210				
	44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1765	1415			350	
	44000600	SIDEWALK REMOVAL	SQ FT	2175				2175	
	44201717	CLASS D PATCHES, TYPE II, 6 INCH	SQ YD	30	30				
	44201721	CLASS D PATCHES, TYPE III, 6 INCH	SQ YD	50	50				
	50101500	REMOVAL OF EXISTING SUPERSTRUCTURES	EACH	1		1			
	50102400	CONCRETE REMOVAL	CU YD	11		11			
	50105220	PIPE CULVERT REMOVAL	FOOT	80	80				
	50200100	STRUCTURE EXCAVATION	CU YD	75		75			
	50300225	CONCRETE STRUCTURES	CU YD	119	50	69			
	50300255	CONCRETE SUPERSTRUCTURE	CU YD	17		17			

* – INDICATES SPECIALTY ITEMS

FILE NAME = 10423_12-QUAN-01 - IDOT-Q01	USER NAME =	DESIGNED – GA	REVISED –
		CHECKED – HLG	REVISED –
	PLOT SCALE =	DRAWN – ACAD	REVISED –
	PLOT DATE = 06-09-18	CHECKED – ACAD	REVISED –

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
SUMMARY OF QUANTITIES

SCALE: | SHEET NO. 4 OF 109 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	4
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

SUMMARY OF QUANTITIES

S.I.	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	BRIDGE	SAFETY	BIKE PATH	TRAINEES
					CONSTRUCTION TYPE CODE				
					0004	0013	0021	0028	0042
	50300260	BRIDGE DECK GROOVING	SQ YD	398		398			
	50300280	CONCRETE ENCASEMENT	CU YD	30		30			
	50300300	PROTECTIVE COAT	SQ YD	425		425			
	50400305	PRECAST PRESTRESSED CONCRETE DECK BEAMS (17" DEPTH)	SQ FT	3519		3519			
	50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	18685	1205	17480			
	50800515	BAR SPLICERS	EACH	131		131			
*	50901050	STEEL RAILING, TYPE SM	FOOT	184		184			
*	50901720	BICYCLE RAILING	FOOT	105				105	
	51200957	FURNISHING METAL SHELL PILES 12" X 0.250"	FOOT	283		283			
	51202305	DRIVING PILES	FOOT	283		283			
	51203200	TEST PILE METAL SHELLS	EACH	4		4			
	51204650	PILE SHOES	EACH	12		12			
	51500100	NAME PLATES	EACH	1		1			
	52200010	TEMPORARY SHEET PILING	SQ FT	315		315			
	54001001	BOX CULVERT END SECTIONS, CULVERT NO. 1	EACH	1	1				
	54011004	PRECAST CONCRETE BOX CULVERTS 10' X 4'	FOOT	478	478				
	54213657	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12"	EACH	1	1				
	54213660	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 15"	EACH	14	14				
	54213663	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 18"	EACH	3	3				
	54213669	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 24"	EACH	1	1				
	54213675	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 30"	EACH	1	1				
	54214719	PRECAST REINFORCED CONCRETE FLARED END SECTIONS - ELLIPTICAL, EQUIVALENT ROUND-SIZE 24"	EACH	1	1				
	550A2320	STORM SEWERS, RUBBER GASKET, CLASS A, TYPE 1 12"	FOOT	860	860				
	550A2330	STORM SEWERS, RUBBER GASKET, CLASS A, TYPE 1 15"	FOOT	273	273				

* – INDICATES SPECIALTY ITEMS

SUMMARY OF QUANTITIES

S.I.	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	BRIDGE	SAFETY	BIKE PATH	TRAINEES
					CONSTRUCTION TYPE CODE				
					0004	0013	0021	0028	0042
	550A2340	STORM SEWERS, RUBBER GASKET, CLASS A, TYPE 1 18"	FOOT	72	72				
	550A2360	STORM SEWERS, RUBBER GASKET, CLASS A, TYPE 1 24"	FOOT	43	43				
	550A2380	STORM SEWERS, RUBBER GASKET, CLASS A, TYPE 1 30"	FOOT	36	36				
	550A2520	STORM SEWERS, RUBBER GASKET, CLASS A, TYPE 2 12"	FOOT	1114	1114				
	550A4000	STORM SEWERS, CLASS A, TYPE 1 EQUIVALENT ROUND-SIZE 18"	FOOT	63	63				
	550A4100	STORM SEWERS, CLASS A, TYPE 1 EQUIVALENT ROUND-SIZE 24"	FOOT	18	18				
	55100500	STORM SEWER REMOVAL 12"	FOOT	10	10				
	55100700	STORM SEWER REMOVAL 15"	FOOT	269	269				
	55100900	STORM SEWER REMOVAL 18"	FOOT	82	82				
	55101900	STORM SEWER REMOVAL 48"	FOOT	54	54				
	55102000	STORM SEWER REMOVAL 54"	FOOT	130	130				
*	56100700	WATER MAIN 8"	FOOT	10	10				
*	56400100	FIRE HYDRANTS TO BE MOVED	EACH	2				2	
	58700300	CONCRETE SEALER	SQ FT	1680		1680			
	59000200	EPOXY CRACK INJECTION	FOOT	38		38			
	59100100	GEOCOMPOSITE WALL DRAIN	SQ YD	75		75			
	60108206	PIPE UNDERDRAINS, TYPE 2, 6"	FOOT	108	108				
	60200805	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 8 GRATE	EACH	1	1				
	60201105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 11 FRAME AND GRATE	EACH	5	5				
	60201110	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 11V FRAME AND GRATE	EACH	11	11				
	60218400	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	11	11				
	60219300	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 11 FRAME AND GRATE	EACH	2	2				
	60221100	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	2	2				
	60236800	INLETS, TYPE A, TYPE 11 FRAME AND GRATE	EACH	7	7				

* – INDICATES SPECIALTY ITEMS

FILE NAME = 10423_12-QUAN-01 - IDOT-Q01 (2)

USER NAME = | DESIGNED — GA | REVISED —
CHECKED — HLG | REVISED —
PLOT SCALE = | DRAWN — ACAD | REVISED —
PLOT DATE = 06-09-18 | CHECKED — ACAD | REVISED —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
SUMMARY OF QUANTITIES

SCALE: | SHEET NO. 5 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	5
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

S.I.	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	BRIDGE	SAFETY	BIKE PATH	TRAINEES
					CONSTRUCTION TYPE CODE				
					0004	0013	0021	0021	0042
	60236825	INLETS, TYPE A, TYPE 11V FRAME AND GRATE	EACH	14	14				
	60266600	VALVE BOXES TO BE ADJUSTED	EACH	1	1				
	60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	12	12				
	60500040	REMOVING MANHOLES	EACH	2	2				
	60500050	REMOVING CATCH BASINS	EACH	1	1				
	60500060	REMOVING INLETS	EACH	1	1				
	60600605	CONCRETE CURB, TYPE B	FOOT	42	42				
	60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	5650	5650				
	60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	340	340				
	60608300	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12	FOOT	660	660				
*	63000003	STEEL PLATE BEAM GUARDRAIL, TYPE A, 9 FOOT POSTS	FOOT	275			275		
*	63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	2			2		
*	63200310	GUARDRAIL REMOVAL	FOOT	590			590		
	67100100	MOBILIZATION	LSUM	1	1				
	70100405	TRAFFIC CONTROL AND PROTECTION, STANDARD 701321	EACH	1	1				
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	2275			2275		
	70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	750			750		
	70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	50			50		
	70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	47335			47335		
	70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	690			690		
	70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	95			95		
	70400100	TEMPORARY CONCRETE BARRIER	FOOT	350	350				
	70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	350	350				
	70600260	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	2	2				

* – INDICATES SPECIALTY ITEMS

S.I.	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	BRIDGE	SAFETY	BIKE PATH	TRAINEES
					CONSTRUCTION TYPE CODE				
					0004	0013	0021	0021	0042
	70600332	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	2	2				
*	72000100	SIGN PANEL - TYPE 1	SQ FT	75			75		
*	72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	15			15		
*	72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	4			4		
*	72400710	RELOCATE SIGN PANEL - TYPE 1	SQ FT	9			9		
*	72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	180			180		
*	73100100	BASE FOR TELESCOPING STEEL SIGN SUPPORT	EACH	12			12		
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	330			330		
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	20600			20600		
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	980			980		
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1460			1460		
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	170			170		
*	81400100	HANDHOLE	EACH	1			1		
*	81603050	UNIT DUCT, 600V, 3-1C NO.6, 1/C NO.8 GROUND, (XLP-TYPE USE), 1" DIA. POLYETHYLENE	FOOT	30			30		
*	83600200	LIGHT POLE FOUNDATION, 24" DIAMETER	FOOT	9			9		
*	84200804	REMOVAL OF POLE FOUNDATION	EACH	1			1		
*	84400105	RELOCATE EXISTING LIGHTING UNIT	EACH	1			1		
	Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	4			4		
	Z0012755	STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)	SQ FT	4			4		
	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	53	53				
	Z0046304	PIPE UNDERDRAINS FOR STRUCTURES 4"	FOOT	185			185		
	Z0076600	TRAINEES	HOUR	1000					1000
	Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	HOUR	1000					1000
	X0322508	PEDESTRIAN TRUSS SUPERSTRUCTURE	SQ FT	920			920		

* – INDICATES SPECIALTY ITEMS

FILE NAME = 10423_12-QUAN-01 - IDOT-Q01 (3)

USER NAME =	DESIGNED – GA	REVISED –	
	CHECKED – HLG	REVISED –	
PLOT SCALE =	DRAWN – ACAD	REVISED –	
PLOT DATE = 06-09-16	CHECKED – ACAD	REVISED –	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
SUMMARY OF QUANTITIES

SCALE: | SHEET NO. 6 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	6
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

SUMMARY OF QUANTITIES

S.I.	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	BRIDGE	SAFETY	BIKE PATH	TRAINEES
					CONSTRUCTION TYPE CODE				
					0004	0013	0021	0021	0042
	X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	16350			16350		
	X4811800	AGGREGATE SHOULDERS (SPECIAL)	SQ YD	750	750				
	X5030301	CONCRETE WEARING SURFACE (VARIABLE DEPTH)	SQ YD	391		391			
	X5401005	PRECAST CONCRETE BOX CULVERTS 10' X 4' (SPECIAL)	FOOT	310	310				
*	X5610708	WATER MAIN REMOVAL, 8"	FOOT	20	20				
	X5860110	GRANULAR BACKFILL FOR STRUCTURES	CU YD	54		54			
	X6026050	SANITARY MANHOLES TO BE ADJUSTED	EACH	1	1				
	X6026622	VALVE VAULTS TO BE REMOVED	EACH	1	1				
	X6061005	CONCRETE CURB, TYPE B (SPECIAL)	FOOT	171				171	
	X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	LSUM	1	1				
	X7015005	CHANGEABLE MESSAGE SIGN	CAL DAY	720	720				
*	X7016500	TEMPORARY BRIDGE TRAFFIC SIGNALS (SPECIAL)	EACH	1	1				
	X7040125	PINNING TEMPORARY CONCRETE BARRIER	EACH	72	72				
	XX006752	REMOVE, STORE AND RE-INSTALL EXISTING MONUMENT	LSUM	1	1				
	XX007170	GATE VALVE 8" IN 4 FT DIAMETER VALVE VAULT, TYPE 1 FRAME, CLOSED LID	EACH	1	1				

* – INDICATES SPECIALTY ITEMS

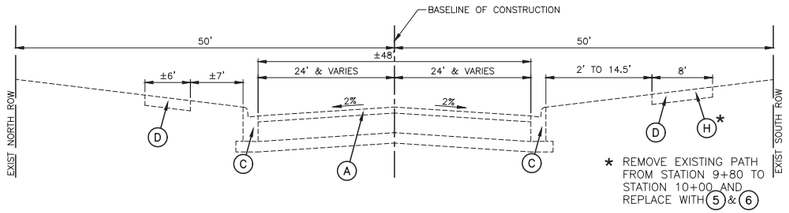
FILE NAME = 10423_12-QUAN-01 - IDOT-Q01 (4)	USER NAME =	DESIGNED – GA	REVISED –
		CHECKED – HLG	REVISED –
	PLOT SCALE =	DRAWN – ACAD	REVISED –
	PLOT DATE = 06-09-16	CHECKED – ACAD	REVISED –

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
SUMMARY OF QUANTITIES

SCALE: | SHEET NO. 7 OF 109 SHEETS | STA. | TO STA.

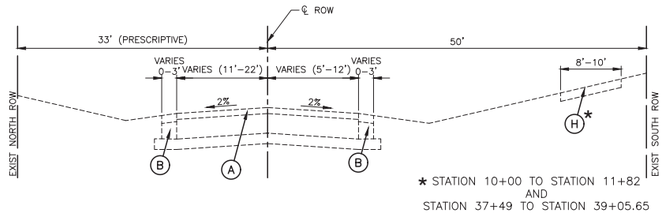
F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	7
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



EXISTING & PROPOSED TYPICAL CROSS SECTION

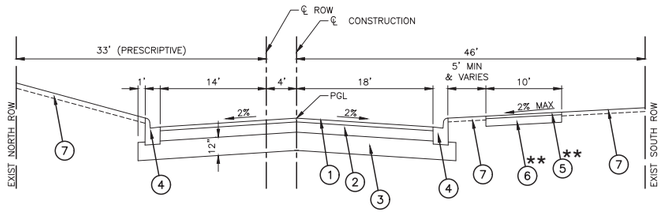
ST FRANCIS ROAD
STA 2+00 TO STA 10+00

NOTE: EXISTING STRIPING SHALL BE REMOVED AND REPLACED ACCORDING TO THE PAVEMENT MARKING AND SIGNAGE PLAN



EXISTING TYPICAL CROSS SECTION

ST FRANCIS ROAD
STA 10+00 TO STA 39+05.65



PROPOSED TYPICAL CROSS SECTION

ST FRANCIS ROAD
STA 10+00 TO STA 39+05.65

NOTES:

1.) SEE CROSS SECTIONS FOR PARKWAY SLOPES.

** THE TEMPORARY PAVEMENT SHALL BE UTILIZED AS THE MULTI-USE PATH BASE WHERE POSSIBLE. THESE AREAS SHALL BE OVERLAID WITH 1-1/2" HMA SURFACE COURSE, MIX "D", N50. (HMA PAVEMENT SECTION WILL BE REDUCED FROM 3" HMA SURFACE TO 1-1/2" HMA SURFACE. 6" AGGREGATE BASE COURSE, TYPE B, WILL NOT BE USED.) TEMPORARY PAVEMENT IS PROPOSED FROM STA. 10+00 TO STA. 19+35 AND FROM STA. 26+20 TO STA. 40+09.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS
FULL DEPTH PAVEMENT	
HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70(IL-9.5mm); 1 1/2"	4% @ 70 Gyr.
HOT MIX ASPHALT BASE COURSE, (HMA BINDER IL-19.0mm); 6 3/4" (IN 2 LIFTS)	4% @ 70 Gyr.
DRIVEWAYS	
HOT MIX ASPHALT SURFACE COURSE, MIX "D", N50(IL-9.5mm); 2"	4% @ 50 Gyr.
HOT MIX ASPHALT BASE COURSE, (HMA BINDER IL-19.0mm); 6" (IN 2 LIFTS)	4% @ 50 Gyr.
TEMPORARY PAVEMENT	
HOT MIX ASPHALT BASE COURSE, (HMA BINDER IL-19.0mm); 6" (IN 2 LIFTS)	4% @ 50 Gyr.
RESURFACING	
HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70(IL-9.5mm); 1 1/2"	4% @ 70 Gyr.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 3/4"	3.5% @ 50 Gyr.
MULTI-USE PATH	
HOT MIX ASPHALT SURFACE COURSE, MIX "D", N50(IL-9.5mm); 3" (2 LIFTS)	4% @ 50 Gyr.
PATCHING	
CLASS D PATCHES (HMA BINDER IL-19mm), 6" (IN 2 LIFTS)	4% @ 70 Gyr.

NOTE:
THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SY/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR AG 76-22" AND FOR NON-POLYMERIZED HMA MIXES "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

EARTHWORK QUANTITIES		
TOTAL CUT =	9811	CY
TOTAL EXISTING PAVEMENT REMOVAL =	1690	CY
TOTAL AVAILABLE CUT TO FILL (EARTH EXCAVATION) =	8121	CY
FURNISHED EXCAVATION (15% SHRINKAGE) =	835	CY
TOTAL FILL =	2302	CY
CUT TO FILL (15% SHRINKAGE) =	2650	CY
EXCESS MATERIAL TO BE HAULED AWAY =	5471	CY

EXISTING LEGEND

- (A) EXISTING BITUMINOUS PAVEMENT
- (B) INTERMITTENT GRAVEL SHOULDER
- (C) COMBINATION CURB & GUTTER
- (D) EXISTING PCC SIDEWALK / BIKE PATH
- (E) EXISTING BITUMINOUS PATH
- (F) HOT-MIX ASPHALT SURFACE REMOVAL - 2"
- (G) COMBINATION CURB & GUTTER - REMOVAL
- (H) EXISTING BITUMINOUS OR CONCRETE PATH - REMOVAL
- (I) FULL DEPTH PAVEMENT REMOVAL

PROPOSED LEGEND

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1½"
- (2) HOT-MIX ASPHALT BASE COURSE - (HMA BINDER IL-19.0mm) - 6 ¾"
- (3) AGGREGATE SUBGRADE IMPROVEMENTS - 12"
- (4) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (5) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 3"**
- (6) AGGREGATE BASE COURSE, TYPE B - 6"**
- (7) TOPSOIL FURNISH AND PLACE - 4" AND SODDING SALT TOLERANT
- (8) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - ¾"
- (9) HOT-MIX ASPHALT BASE COURSE - (HMA BINDER IL-19.0mm) - 6"
- (10) AGGREGATE BASE COURSE, TYPE B - 4"
- (11) TEMPORARY AGGREGATE SHOULDER - 3"
- (12) P.C.C. SIDEWALK - 5"

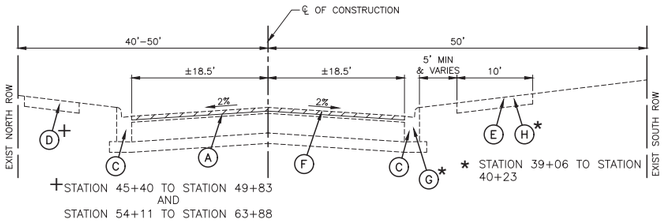
FILE NAME = 10423_12-TYPX-01 - IDOT P01	USER NAME =	DESIGNED - GA	REVISED -
		CHECKED - HLG	REVISED -
	PLOT SCALE =	DRAWN - MED	REVISED -
	PLOT DATE = 06-09-16	CHECKED - AG	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
TYPICAL CROSS SECTIONS

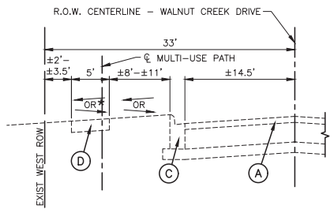
SCALE: NONE | SHEET NO. 8 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	8
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



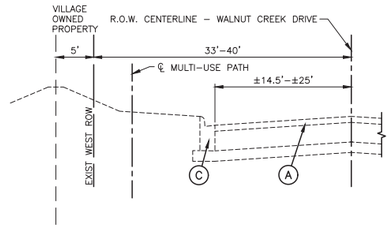
EXISTING TYPICAL CROSS SECTION

ST FRANCIS ROAD
STA 39+05.65 TO STA 63+88.49



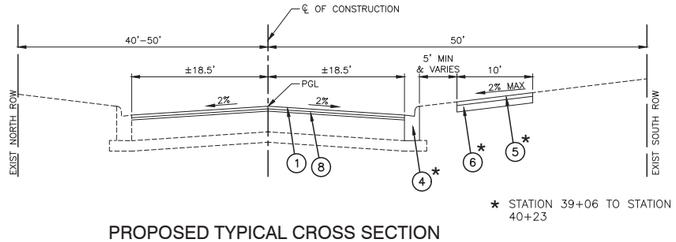
EXISTING TYPICAL CROSS SECTION

WALNUT CREEK DRIVE
STA 5+48.40 TO STA 9+17



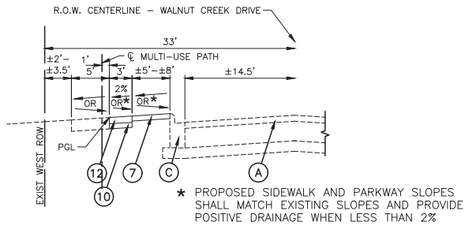
EXISTING TYPICAL CROSS SECTION

WALNUT CREEK DRIVE
STA 9+17 TO STA 14+65.39



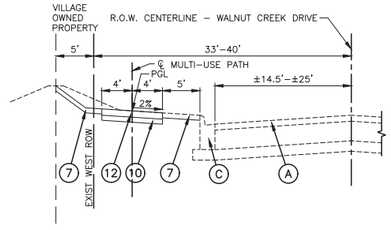
PROPOSED TYPICAL CROSS SECTION

ST FRANCIS ROAD
STA 39+05.65 TO STA 63+88.49



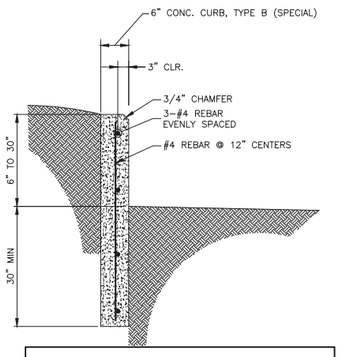
PROPOSED TYPICAL CROSS SECTION

WALNUT CREEK DRIVE
STA 5+48.40 TO STA 9+17



PROPOSED TYPICAL CROSS SECTION

WALNUT CREEK DRIVE
STA 9+17 TO STA 14+65.39

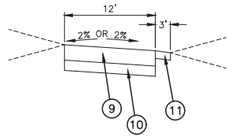


NOTE:

THE COST OF FURNISHING AND INSTALLING THE #4 REBARS SHALL BE INCLUDED IN THE COST OF THE CONCRETE CURB, TYPE B (SPECIAL), AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

CONCRETE CURB, TYPE B (SPECIAL) DETAIL

STA 9+80 TO 11+50 (SOUTH SIDE)



PROPOSED TEMPORARY PAVEMENT TYPICAL CROSS SECTION

ST FRANCIS ROAD
STA 10+00 TO STA 19+35
and
STA 26+20 TO STA 40+09

NOTES:

1. WIDTH VARIES IN AREAS WHERE TEMPORARY PAVEMENT TRANSITIONS TO EXISTING PAVEMENT.
2. THE TEMPORARY PAVEMENT SHALL BE REMOVED IN AREAS WHERE TEMPORARY PAVEMENT IS NOT BEING USED AS PART OF THE PERMANENT MULTI–USE PATH. IN AREAS WHERE THE TEMPORARY PAVEMENT IS BEING USED AS PART OF THE MULTI–USE PATH, THE NORTH 2' OF HOT–MIX ASPHALT MATERIAL SHALL BE REMOVED.
3. THE TEMPORARY AGGREGATE SHOULDER SHALL BE REMOVED UPON COMPLETION OF THE MAINLINE PAVEMENT AND WILL BE PAID FOR AS AGGREGATE FOR TEMPORARY ACCESS.

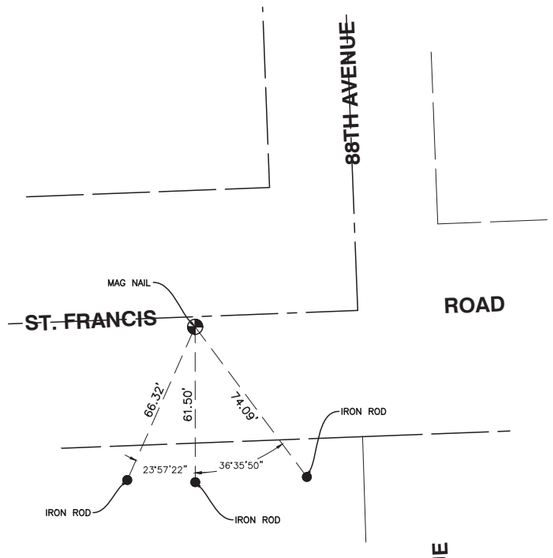
EXISTING LEGEND

- (A) EXISTING BITUMINOUS PAVEMENT
- (B) INTERMITTENT GRAVEL SHOULDER
- (C) COMBINATION CURB & GUTTER
- (D) EXISTING PCC SIDEWALK / BIKE PATH
- (E) EXISTING BITUMINOUS PATH
- (F) HOT–MIX ASPHALT SURFACE REMOVAL – 2"
- (G) COMBINATION CURB & GUTTER – REMOVAL
- (H) EXISTING BITUMINOUS OR CONCRETE PATH – REMOVAL
- (I) FULL DEPTH PAVEMENT REMOVAL

PROPOSED LEGEND

- (1) HOT–MIX ASPHALT SURFACE COURSE, MIX "D", N70 – 1½"
- (2) HOT–MIX ASPHALT BASE COURSE – (HMA BINDER IL–19.0mm) – 6 ¾"
- (3) AGGREGATE SUBGRADE IMPROVEMENTS – 12"
- (4) COMBINATION CONCRETE CURB AND GUTTER, TYPE B–6.12
- (5) HOT–MIX ASPHALT SURFACE COURSE, MIX "D", N50 – 3"
- (6) AGGREGATE BASE COURSE, TYPE B – 6"
- (7) TOPSOIL FURNISH AND PLACE – 4" AND SODDING SALT TOLERANT
- (8) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL–4.75, N50 – ¾"
- (9) HOT–MIX ASPHALT BASE COURSE – (HMA BINDER IL–19.0mm) – 6"
- (10) AGGREGATE BASE COURSE, TYPE B – 4"
- (11) TEMPORARY AGGREGATE SHOULDER – 3"
- (12) P.C.C. SIDEWALK – 5"

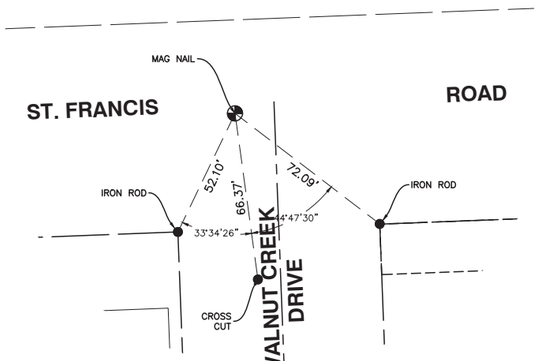
FILE NAME = 10423_12-TYPX-01 - IDOT P02	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION TYPICAL CROSS SECTIONS	F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		CHECKED — HLG	REVISED —			3752	10-00045-00-WR	WILL	109	9
	PLOT SCALE =	DRAWN — MED	REVISED —					CONTRACT NO. 61D22		
	PLOT DATE = 06-09-16	CHECKED — AG	REVISED —		SCALE: NONE / SHEET NO. 9 OF 109 SHEETS / STA. / TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



SCALE 1"=30'

STATION 10+00.00

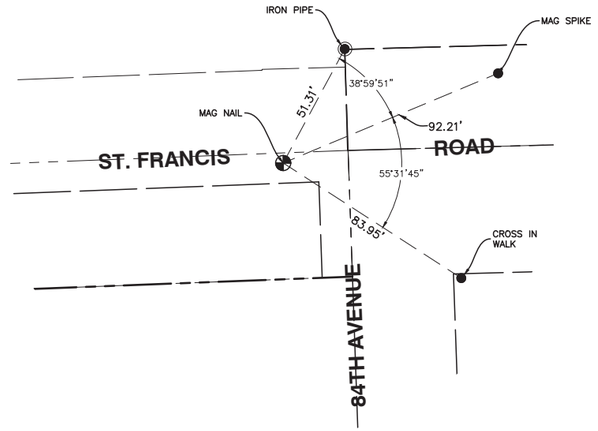
MAG NAIL



SCALE 1"=30'

STATION 24+00.00

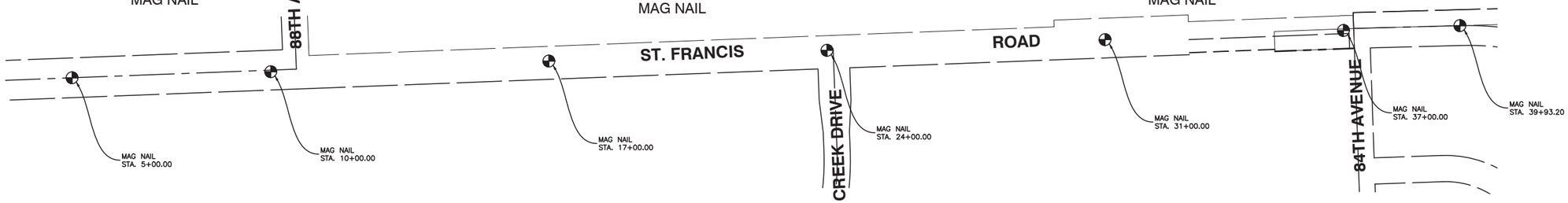
MAG NAIL

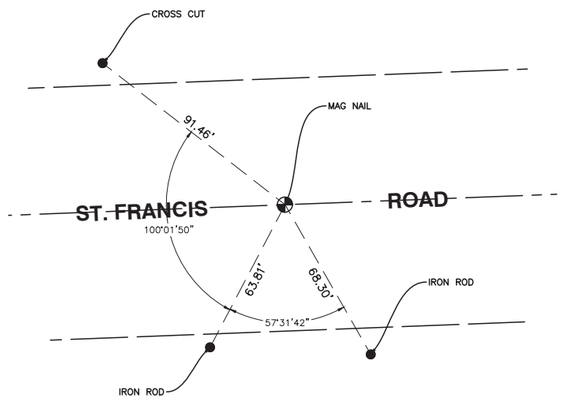


SCALE 1"=30'

STATION 37+00.00

MAG NAIL

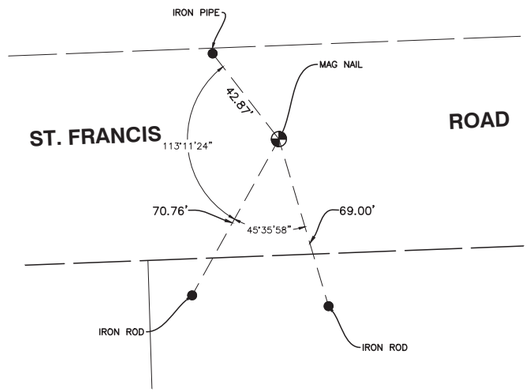




SCALE 1"=30'

STATION 5+00.00

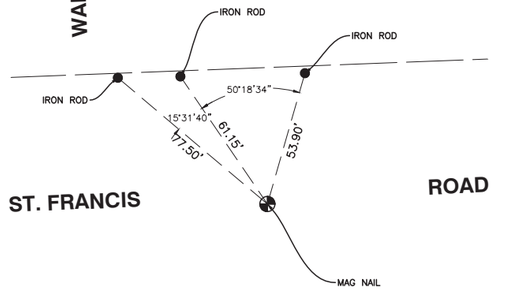
MAG NAIL



SCALE 1"=30'

STATION 17+00.00

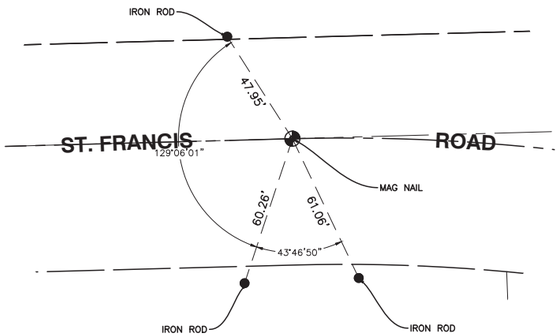
MAG NAIL



SCALE 1"=30'

STATION 31+00.00

MAG NAIL



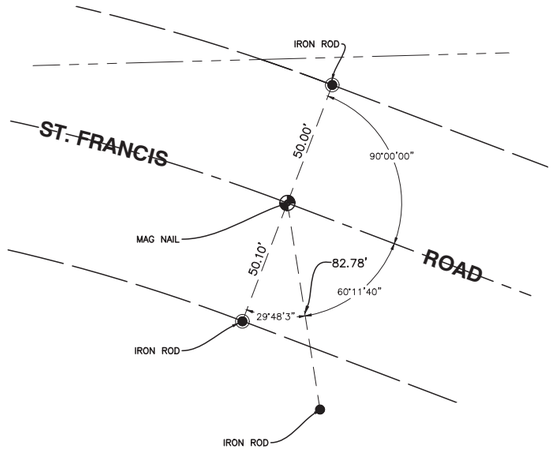
SCALE 1"=30'

STATION 39+93.20

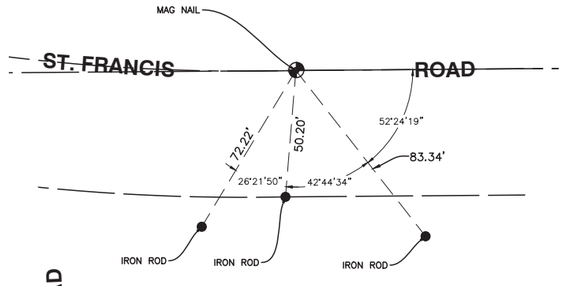
MAG NAIL

FILE NAME = 10423_12-ALIGN--TIES-01 - T01	USER NAME =	DESIGNED — GA	REVISED — 01-22-09 - IDOT REVIEW	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	RECONSTRUCTION ST. FRANCIS ROAD (FAU 3752)
LAST SAVED BY: MDOWNS ON 7/28/16		CHECKED — HLG	REVISED —		----
PLOTTED BY: MATTHEW DOWNS ON 12/22/17	PLOT SCALE =	DRAWN — RG	REVISED —		
	PLOT DATE = 06-09-16	CHECKED — AG	REVISED —		SCALE: 1"=50' | SHEET NO. 10 OF 109 SHEETS | STA. TO STA.

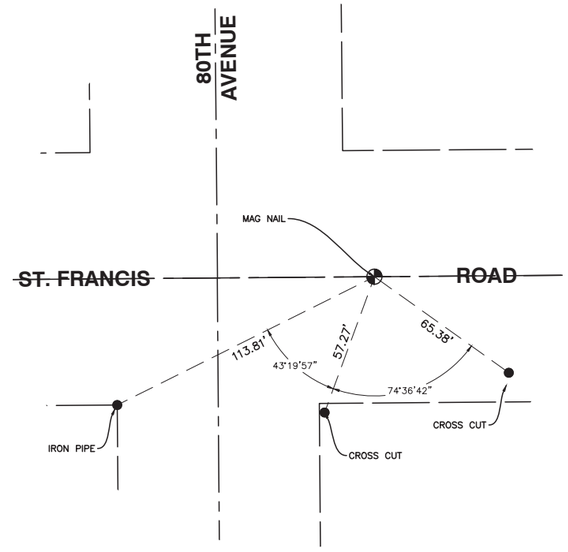
F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	10
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



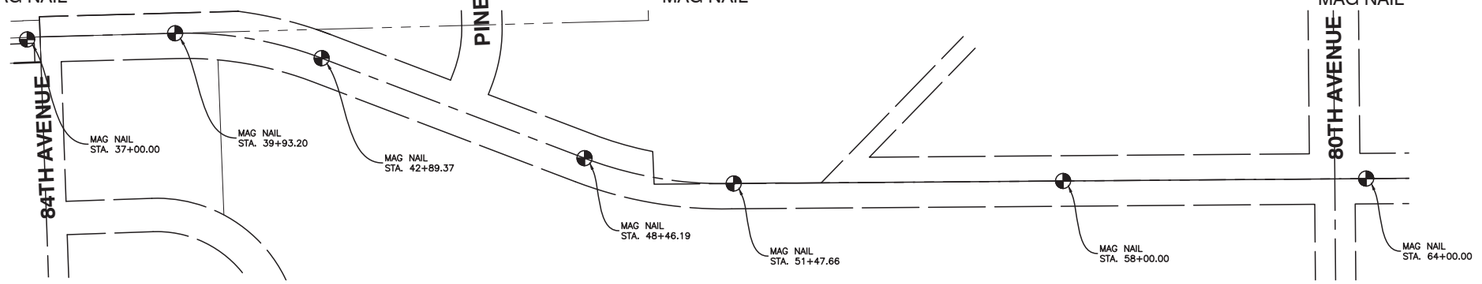
SCALE 1"=30'
STATION 42+89.37
MAG NAIL

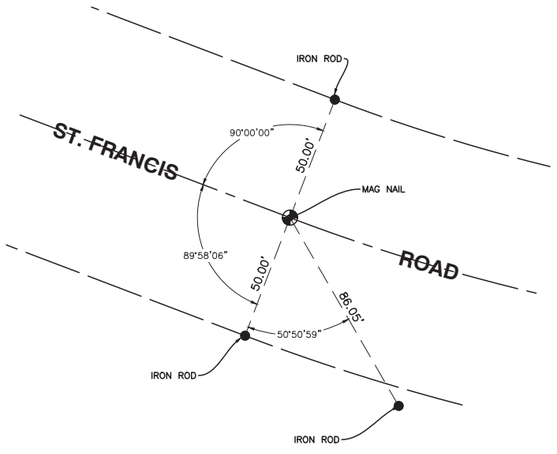


SCALE 1"=30'
STATION 51+47.66
MAG NAIL

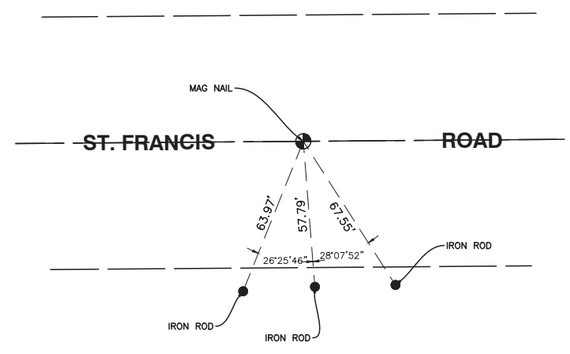


SCALE 1"=30'
STATION 64+00.00
MAG NAIL





SCALE 1"=30'
STATION 48+46.19
MAG NAIL



SCALE 1"=30'
STATION 58+00.00
MAG NAIL

BENCH MARK:

TP #19
SPIKE NAIL SET IN NORTH FACE OF POWER POLE, SOUTH SIDE OF ST. FRANCIS ROAD, 2ND POLE WEST OF THE CENTERLINE OF 84TH AVENUE, POLE #D6780
ELEVATION=697.50

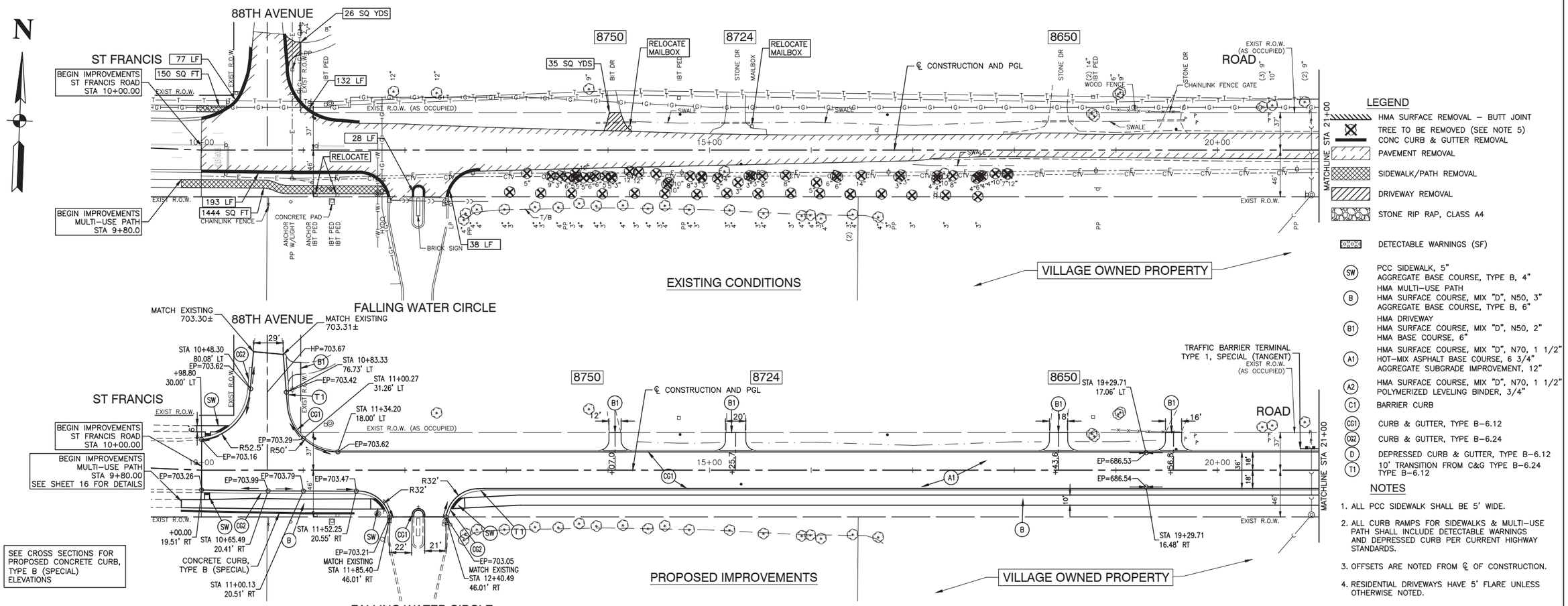
TP #9
NAIL SET IN SOUTH FACE OF POWER POLE WITH LIGHT, SOUTH SIDE OF ST. FRANCIS ROAD AT TEE INTERSECTION WITH PINE HILL DRIVE. POLE #27446
ELEVATION=696.06

TP #1
NAIL SET IN SOUTH FACE OF POWER POLE WITH LIGHT, AT SOUTHWEST CORNER OF ST. FRANCIS ROAD AND 80TH AVENUE
ELEVATION=713.65

DATUM: NGVD 29

FILE NAME = 10423_12-ALIGN--TIES-01 - T02	USER NAME =	DESIGNED — GA	REVISED — 01-22-09 - IDOT REVIEW	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	RECONSTRUCTION ST. FRANCIS ROAD (FAU 3752) ----			F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		CHECKED — HLG	REVISED —					3752	10-00045-00-WR	WILL	109	11
	PLOT SCALE =	DRAWN — RG	REVISED —							CONTRACT NO. 61D22		
LAST SAVED BY: MDOWNS ON 7/28/16 PLOTTED BY: MATTHEW DOWNS ON 12/22/17	PLOT DATE = 06-09-16	CHECKED — AG	REVISED —		SCALE: 1"=50'	SHEET NO. 11 OF 109 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

SECTION 14, TOWNSHIP 35, RANGE 12

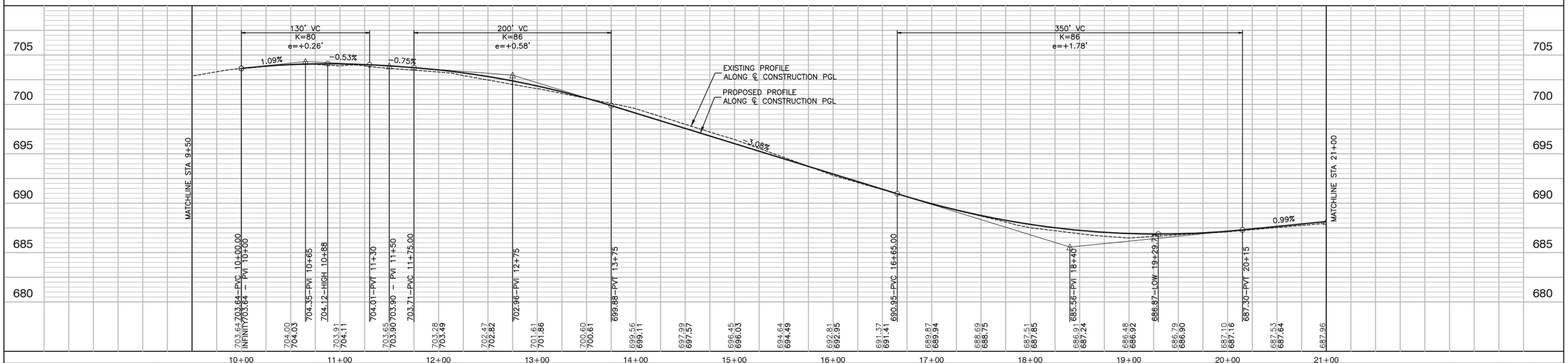


LEGEND

- HMA SURFACE REMOVAL – BUTT JOINT
- TREE TO BE REMOVED (SEE NOTE 5)
- CONC CURB & GUTTER REMOVAL
- PAVEMENT REMOVAL
- SIDEWALK/PATH REMOVAL
- DRIVEWAY REMOVAL
- STONE RIP RAP, CLASS A4
- DETECTABLE WARNINGS (SF)
- (SW) PCC SIDEWALK, 5" AGGREGATE BASE COURSE, TYPE B, 4"
- (B) HMA MULTI-USE PATH HMA SURFACE COURSE, MIX "D", N50, 3" AGGREGATE BASE COURSE, TYPE B, 6"
- (B1) HMA DRIVEWAY HMA SURFACE COURSE, MIX "D", N50, 2" HMA BASE COURSE, 6"
- (A1) HMA SURFACE COURSE, MIX "D", N70, 1 1/2" HOT-MIX ASPHALT BASE COURSE, 6 3/4" AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (A2) HMA SURFACE COURSE, MIX "D", N70, 1 1/2" POLYMERIZED LEVELING BINDER, 3/4"
- (C1) BARRIER CURB
- (CG1) CURB & GUTTER, TYPE B-6.12
- (CG2) CURB & GUTTER, TYPE B-6.24
- (D) DEPRESSED CURB & GUTTER, TYPE B-6.12
- (T1) 10' TRANSITION FROM C&G TYPE B-6.24 TYPE B-6.12

NOTES

1. ALL PCC SIDEWALK SHALL BE 5' WIDE.
2. ALL CURB RAMPS FOR SIDEWALKS & MULTI-USE PATH SHALL INCLUDE DETECTABLE WARNINGS AND DEPRESSED CURB PER CURRENT HIGHWAY STANDARDS.
3. OFFSETS ARE NOTED FROM ℄ OF CONSTRUCTION.
4. RESIDENTIAL DRIVEWAYS HAVE 5' FLARE UNLESS OTHERWISE NOTED.
5. MAJORITY OF TREE REMOVALS HAVE ALREADY BEEN COMPLETED BY OTHERS.



FILE NAME = 10423_12-PLPR-01 - IDOT PLPR 01			
	USER NAME =	DESIGNED — GA	REVISED —
		CHECKED — HLG	REVISED —
	PLOT SCALE =	DRAWN — MED	REVISED —
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
PLAN & PROFILE

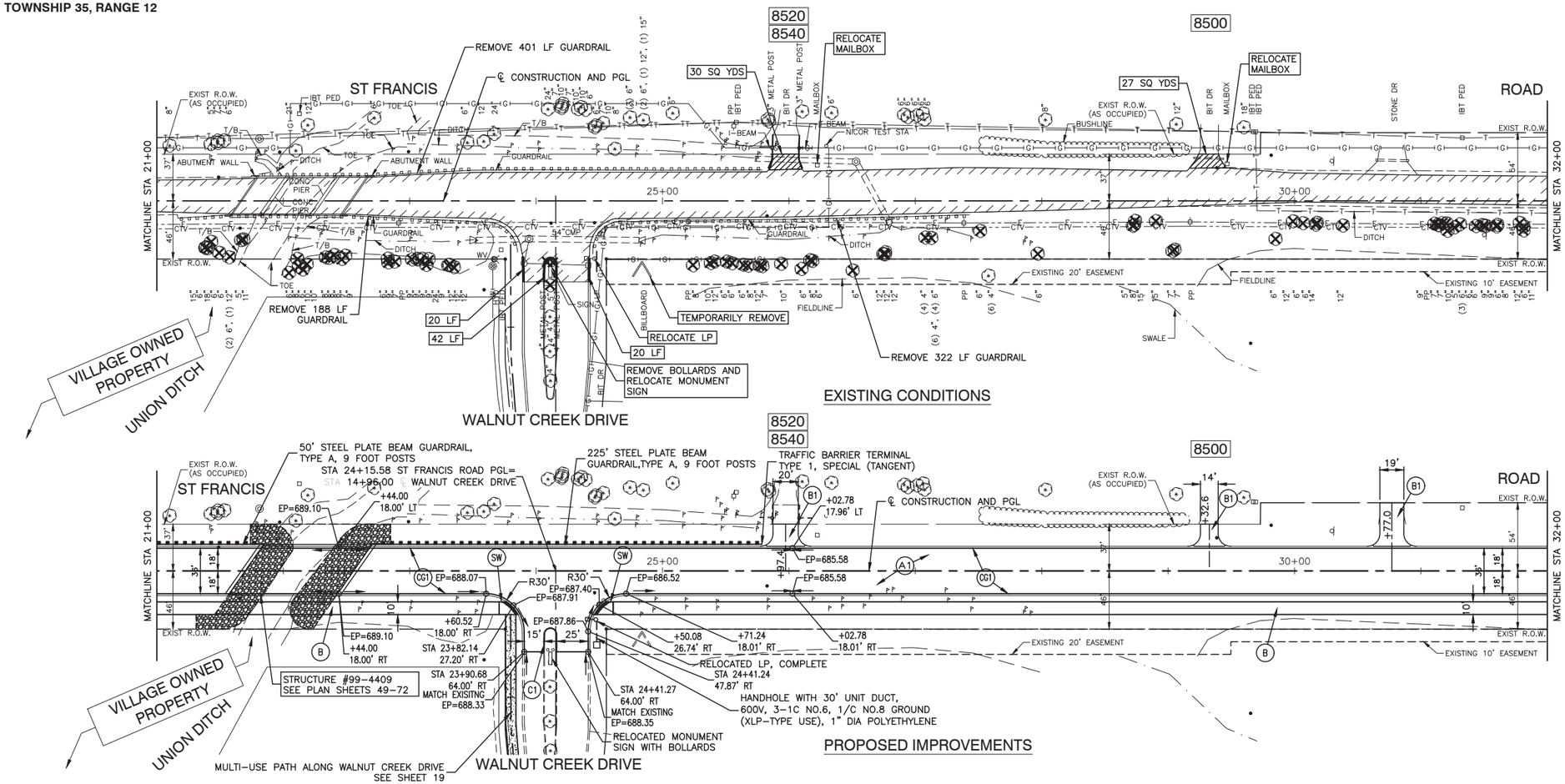
SCALE: H: 1"=50' V:1"=5 | SHEET NO. 12 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	12
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

PLAN — DATE | BY | SURVEYED | PLOTTED | ALIGNMENT CHECKED | RT. OF WAY CHECKED | CADD FILE NAME | NOTE BOOK | NO.

PROFILE — DATE | BY | SURVEYED | PLOTTED | BORES CHECKED | B.M. NOTED | NOTE BOOK

SECTION 14, TOWNSHIP 35, RANGE 12

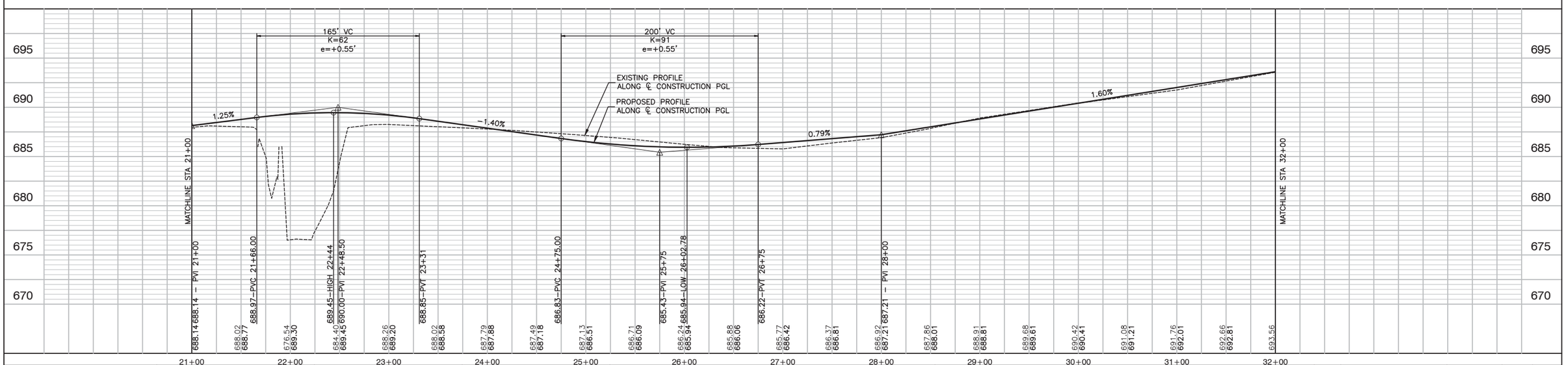


LEGEND

- HMA SURFACE REMOVAL – BUTT JOINT
- TREE TO BE REMOVED (SEE NOTE 6)
- CONC CURB & GUTTER REMOVAL
- PAVEMENT REMOVAL
- SIDEWALK/PATH REMOVAL
- DRIVEWAY REMOVAL
- STONE RIP RAP, CLASS A4
- DETECTABLE WARNINGS (SF)
- (SW) PCC SIDEWALK, 5" AGGREGATE BASE COURSE, TYPE B, 4"
- (B) HMA MULTI-USE PATH HMA SURFACE COURSE, MIX "D", N50, 3" AGGREGATE BASE COURSE, TYPE B, 6"
- (B1) HMA DRIVEWAY HMA SURFACE COURSE, MIX "D", N50, 2" HMA BASE COURSE, 6"
- (A1) HMA SURFACE COURSE, MIX "D", N70, 1 1/2" HOT-MIX ASPHALT BASE COURSE, 6 3/4" AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (A2) HMA SURFACE COURSE, MIX "D", N70, 1 1/2" POLYMERIZED LEVELING BINDER, 3/4"
- (C1) BARRIER CURB
- (CG1) CURB & GUTTER, TYPE B-6.12
- (CG2) CURB & GUTTER, TYPE B-6.24
- (D) DEPRESSED CURB & GUTTER, TYPE B-6.12
- (T1) 10' TRANSITION FROM C&G TYPE B-6.24 TYPE B-6.12

NOTES

1. ALL PCC SIDEWALK SHALL BE 5' WIDE.
2. ALL CURB RAMPS FOR SIDEWALKS & MULTI-USE PATH SHALL INCLUDE DETECTABLE WARNINGS AND DEPRESSED CURB PER CURRENT HIGHWAY STANDARDS.
3. OFFSETS ARE NOTED FROM ℄ OF CONSTRUCTION.
4. RESIDENTIAL DRIVEWAYS HAVE 5' FLARE UNLESS OTHERWISE NOTED.
5. THE BILLBOARD AT STATION 24+84 SHALL BE REMOVED AND REPLACED ONCE CONSTRUCTION IS COMPLETE. (THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.)
6. MAJORITY OF TREE REMOVALS HAVE ALREADY BEEN COMPLETED BY OTHERS.

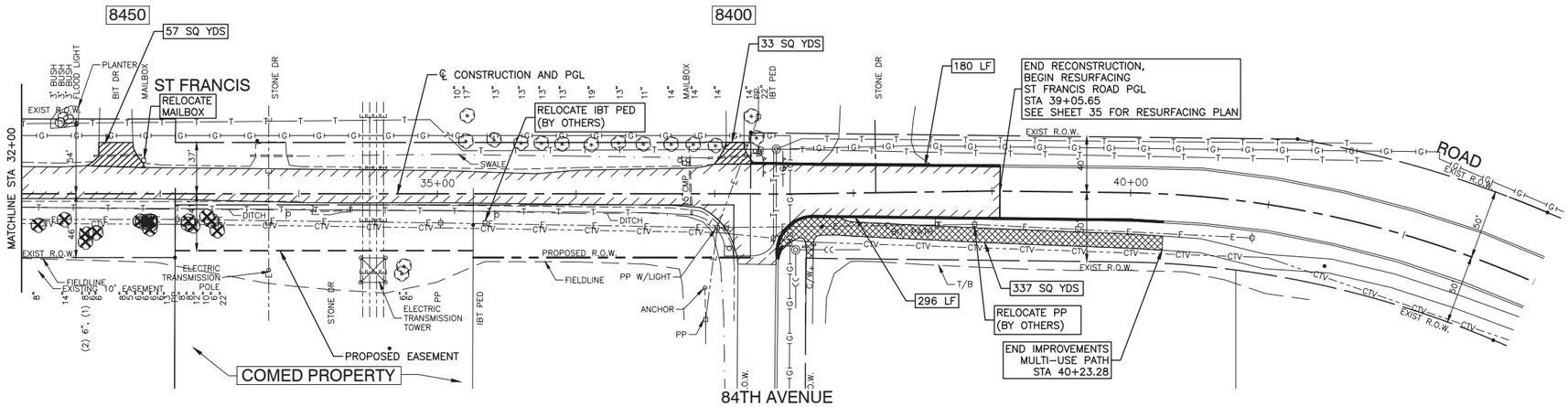


FILE NAME = 10423_12-PLPR-01 - IDOT PLPR 02	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION PLAN & PROFILE	F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		CHECKED — HLG	REVISED —			3752	10-00045-00-WR	WILL	109	13
	PLOT SCALE =	DRAWN — MED	REVISED —					CONTRACT NO. 61D22		
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —		SCALE: H: 1"=50' V:1"=5 SHEET NO. 13 OF 109 SHEETS STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

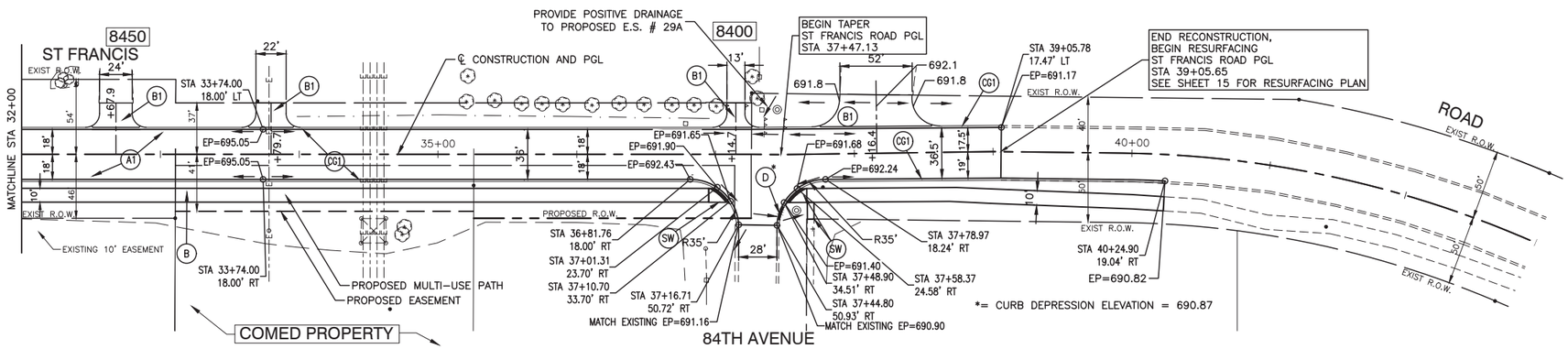
PLAN — SURVEYED, PLOTTED, ALIGNMENT CHECKED, RT. OF WAY CHECKED, CADD FILE NAME, NOTE BOOK, NO., BY, DATE

PROFILE — SURVEYED, PLOTTED, CROSS SECTIONS CHECKED, B.M. NOTED, NOTE BOOK, BY, DATE

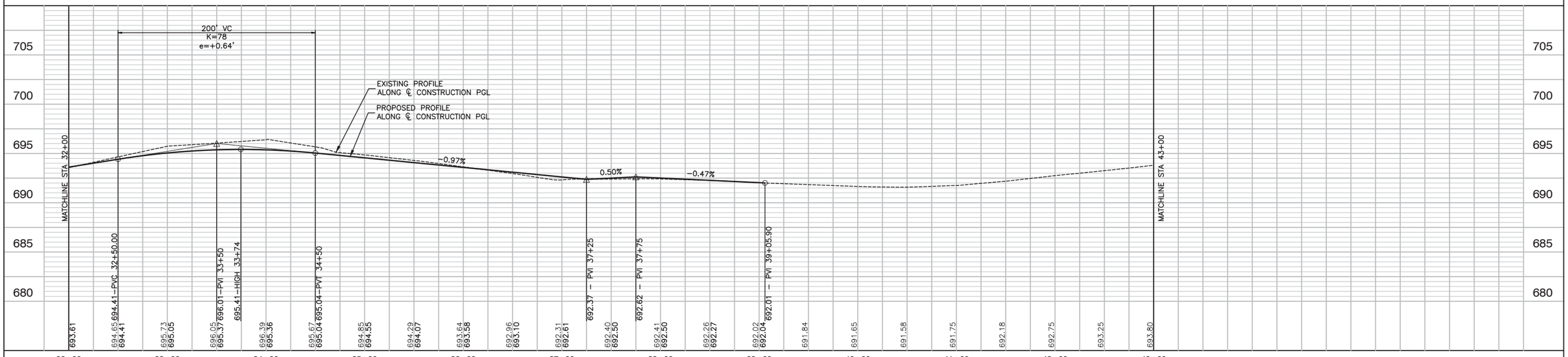
SECTION 14, TOWNSHIP 35, RANGE 12



EXISTING CONDITIONS



PROPOSED IMPROVEMENTS



LEGEND

- HMA SURFACE REMOVAL – BUTT JOINT
- TREE TO BE REMOVED (SEE NOTE 5)
- CONC CURB & GUTTER REMOVAL
- PAVEMENT REMOVAL
- SIDEWALK/PATH REMOVAL
- DRIVEWAY REMOVAL
- STONE RIP RAP, CLASS A4
- DETECTABLE WARNINGS (SF)
- (SW) PCC SIDEWALK, 5" / AGGREGATE BASE COURSE, TYPE B, 4"
- (B) HMA MULTI-USE PATH / HMA SURFACE COURSE, MIX "D", N50, 3" / AGGREGATE BASE COURSE, TYPE B, 6"
- (B1) HMA DRIVEWAY / HMA SURFACE COURSE, MIX "D", N50, 2" / HMA BASE COURSE, 6"
- (A1) HMA SURFACE COURSE, MIX "D", N70, 1 1/2" / HOT-MIX ASPHALT BASE COURSE, 6 3/4" / AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (A2) HMA SURFACE COURSE, MIX "D", N70, 1 1/2" / POLYMERIZED LEVELING BINDER, 3/4"
- (C1) BARRIER CURB
- (CG1) CURB & GUTTER, TYPE B-6.12
- (CG2) CURB & GUTTER, TYPE B-6.24
- (D) DEPRESSED CURB & GUTTER, TYPE B-6.12
- (T1) 10' TRANSITION FROM C&G TYPE B-6.24 TYPE B-6.12

N

NOTES

1. ALL PCC SIDEWALK SHALL BE 5' WIDE.
2. ALL CURB RAMPS FOR SIDEWALKS & MULTI-USE PATH SHALL INCLUDE DETECTABLE WARNINGS AND DEPRESSED CURB PER CURRENT HIGHWAY STANDARDS.
3. OFFSETS ARE NOTED FROM ℄ OF CONSTRUCTION.
4. RESIDENTIAL DRIVEWAYS HAVE 5' FLARE UNLESS OTHERWISE NOTED.
5. MAJORITY OF TREE REMOVALS HAVE ALREADY BEEN COMPLETED BY OTHERS.

PLAN: DATE | BY | SURVEYED | PLOTTED | DRAWN | RT. OF WAY CHECKED | ALIGNMENT CHECKED | CADD FILE NAME | NOTE BOOK | NO.

PROFILE: DATE | BY | SURVEYED | PLOTTED | DRAWN | CHECKED | B.M. NOTED | NOTE BOOK

FILE NAME = 10423_12-PLPR-01 - IDOT PLPR 03	USER NAME =	DESIGNED — GA	REVISED —
		CHECKED — HLG	REVISED —
	PLOT SCALE =	DRAWN — MED	REVISED —
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —

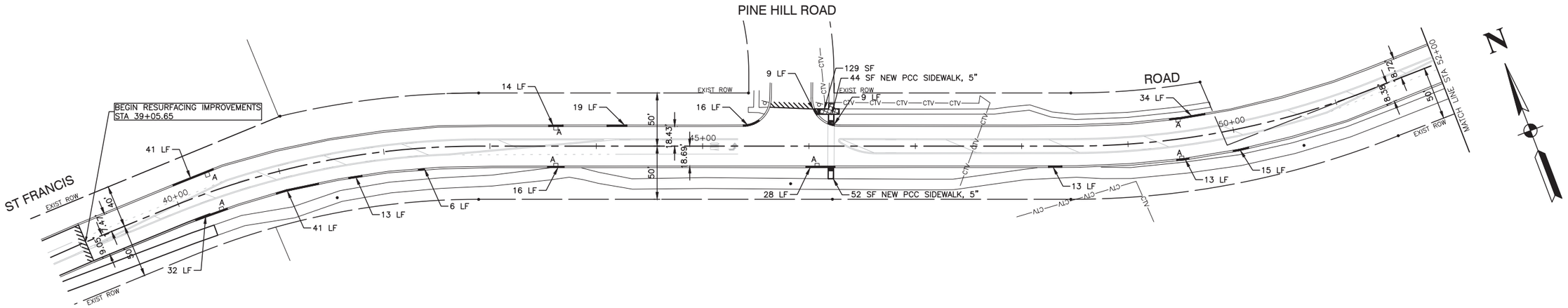
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

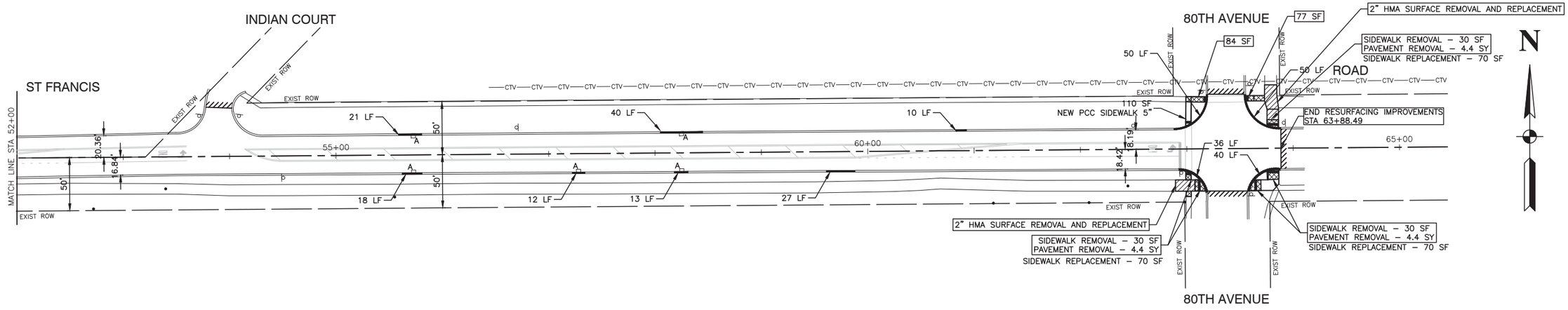
ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
PLAN & PROFILE

SCALE: H: 1"=50' V:1"=5 | SHEET NO. 14 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	14
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

SECTION 14, TOWNSHIP 35, RANGE 12





LEGEND

1. ALL PCC SIDEWALK SHALL BE 5' WIDE.
2. ALL CURB RAMPS FOR SIDEWALKS & BIKE PATH SHALL INCLUDE DETECTABLE WARNINGS AND DEPRESSED CURB PER CURRENT HIGHWAY STANDARDS.
3. REFER TO TYPICAL SECTIONS FOR RESURFACING INFORMATION.

LEGEND

- CONCRETE REMOVAL OR HMA PAVEMENT REMOVAL
- BUTT JOINTS
- DETECTABLE WARNINGS
- CURB REMOVAL & REPLACEMENT
- A — ADJUST STRUCTURE

FILE NAME = 10423_12-PLAN-01 - IDOT P02	USER NAME =	DESIGNED — GA	REVISED —
		CHECKED — HLG	REVISED —
	PLOT SCALE =	DRAWN — ACAD	REVISED —
	PLOT DATE = 06-09-16	CHECKED — ACAD	REVISED —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

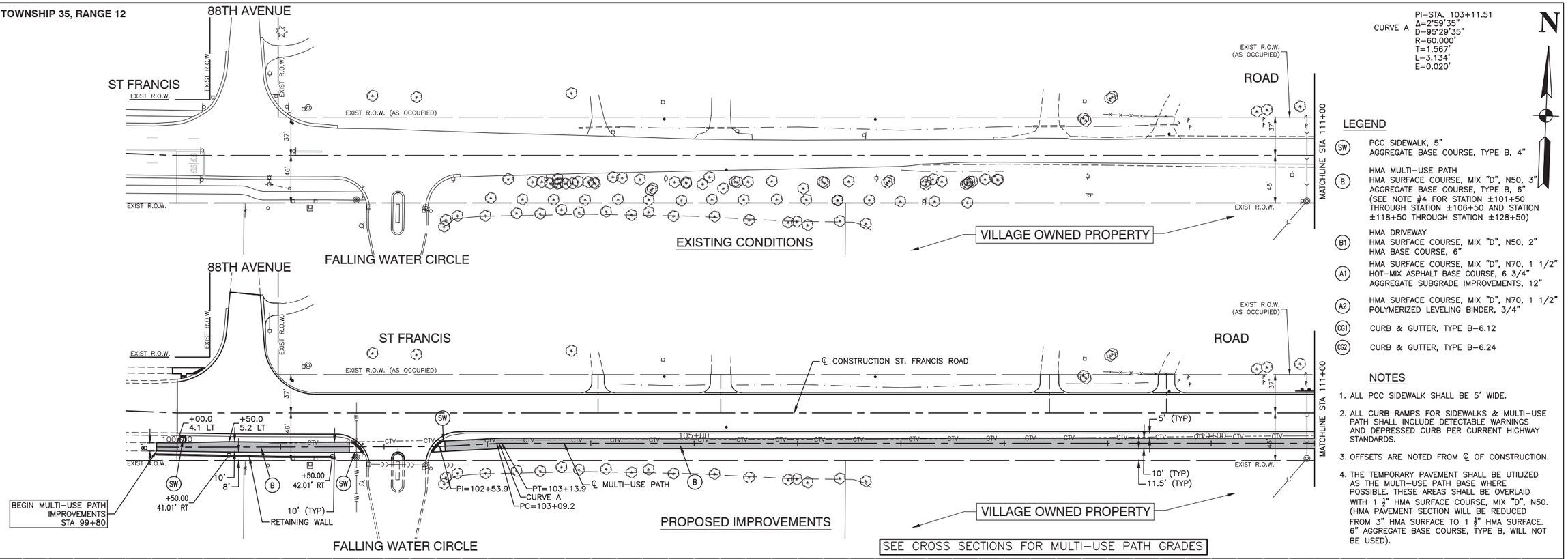
ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
RESURFACING PLAN

SCALE: | SHEET NO. 15 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	15
		CONTRACT NO. 61D22		

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT ----

SECTION 14, TOWNSHIP 35, RANGE 12



CURVE A
PI=STA. 103+11.51
Δ=2°59'35"
D=95°29'35"
R=60.000'
T=1.567'
L=3.134'
E=0.020'

N

LEGEND

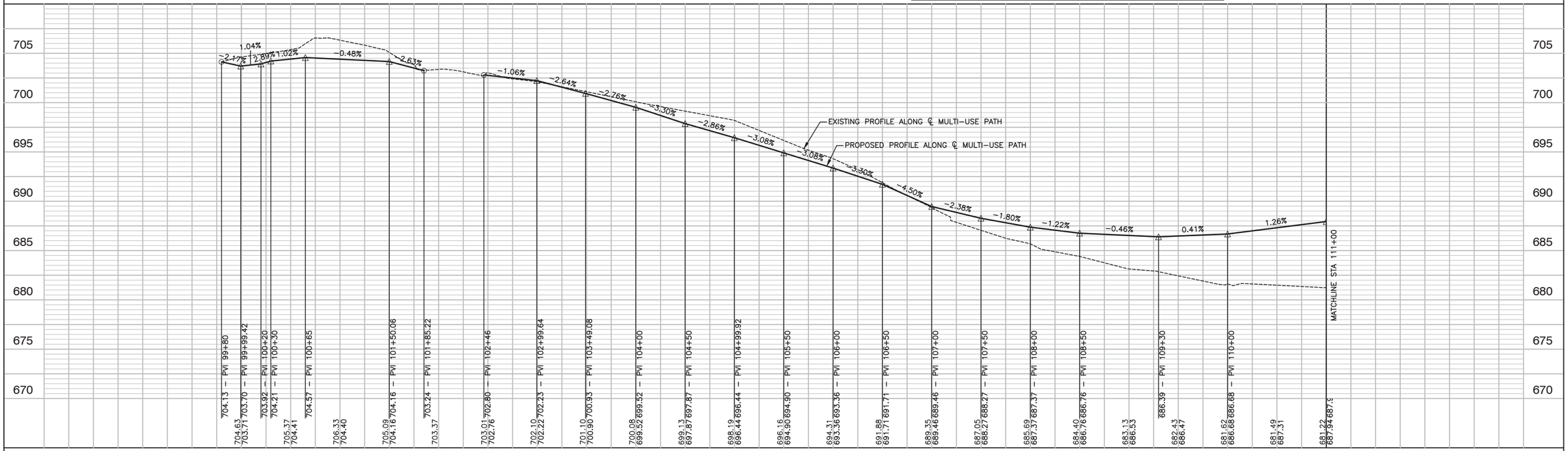
SW	PCC SIDEWALK, 5" AGGREGATE BASE COURSE, TYPE B, 4"
B	HMA MULTI-USE PATH HMA SURFACE COURSE, MIX "D", N50, 3" AGGREGATE BASE COURSE, TYPE B, 6" (SEE NOTE #4 FOR STATION ±101+50 THROUGH STATION ±106+50 AND STATION ±118+50 THROUGH STATION ±128+50)
B1	HMA DRIVEWAY HMA SURFACE COURSE, MIX "D", N50, 2" HMA BASE COURSE, 6"
A1	HMA SURFACE COURSE, MIX "D", N70, 1 1/2" HOT-MIX ASPHALT BASE COURSE, 6 3/4" AGGREGATE SUBGRADE IMPROVEMENTS, 12"
A2	HMA SURFACE COURSE, MIX "D", N70, 1 1/2" POLYMERIZED LEVELING BINDER, 3/4"
CC1	CURB & GUTTER, TYPE B-6.12
CC2	CURB & GUTTER, TYPE B-6.24

NOTES

1. ALL PCC SIDEWALK SHALL BE 5' WIDE.
2. ALL CURB RAMPS FOR SIDEWALKS & MULTI-USE PATH SHALL INCLUDE DETECTABLE WARNINGS AND DEPRESSED CURB PER CURRENT HIGHWAY STANDARDS.
3. OFFSETS ARE NOTED FROM ℄ OF CONSTRUCTION.
4. THE TEMPORARY PAVEMENT SHALL BE UTILIZED AS THE MULTI-USE PATH BASE WHERE POSSIBLE. THESE AREAS SHALL BE OVERLAID WITH 1 ½" HMA SURFACE COURSE, MIX "D", N50. (HMA PAVEMENT SECTION WILL BE REDUCED FROM 3" HMA SURFACE TO 1 ½" HMA SURFACE. 6" AGGREGATE BASE COURSE, TYPE B, WILL NOT BE USED).

PLAN — BY | DATE | SURVEYED | PLOTTED | ALIGNMENT CHECKED | RT. OF WAY CHECKED | CADD FILE NAME | NOTE BOOK | NO.

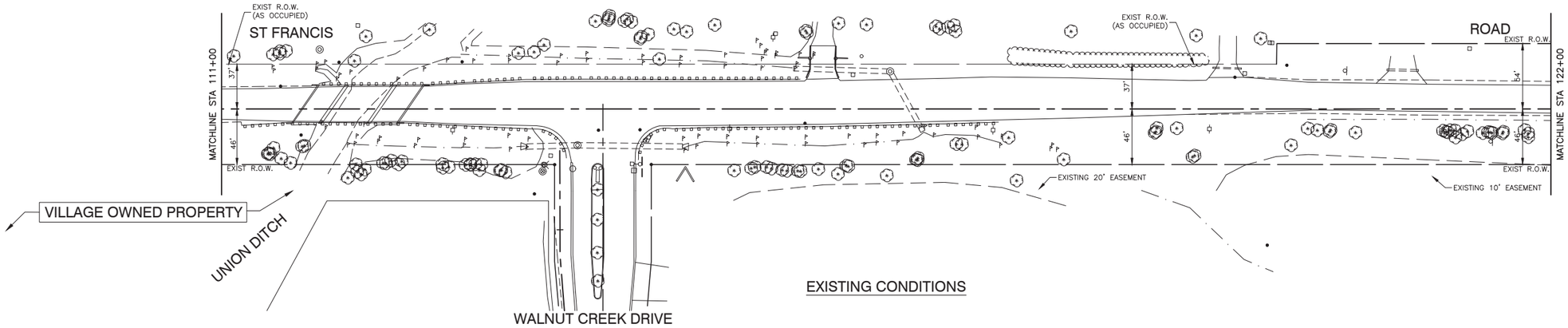
PROFILE — BY | DATE | SURVEYED | PLOTTED | PROFILE CHECKED | B.M. NOTED | NOTE BOOK



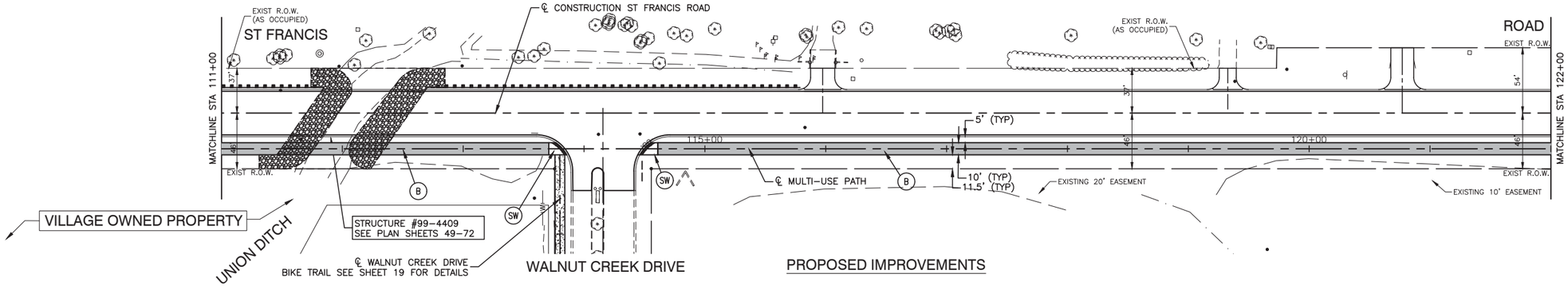
FILE NAME = 10423_12-PLPR-01 - BIKE PLPR(8)	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION MULTI-USE PATH PLAN AND PROFILES
		CHECKED — HLG	REVISED —		
	PLOT SCALE =	DRAWN — MED	REVISED —		
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —		SCALE: H: 1"=50' V:1"=5 | SHEET NO. 16 OF 109 SHEETS | STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	16
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

SECTION 14, TOWNSHIP 35, RANGE 12

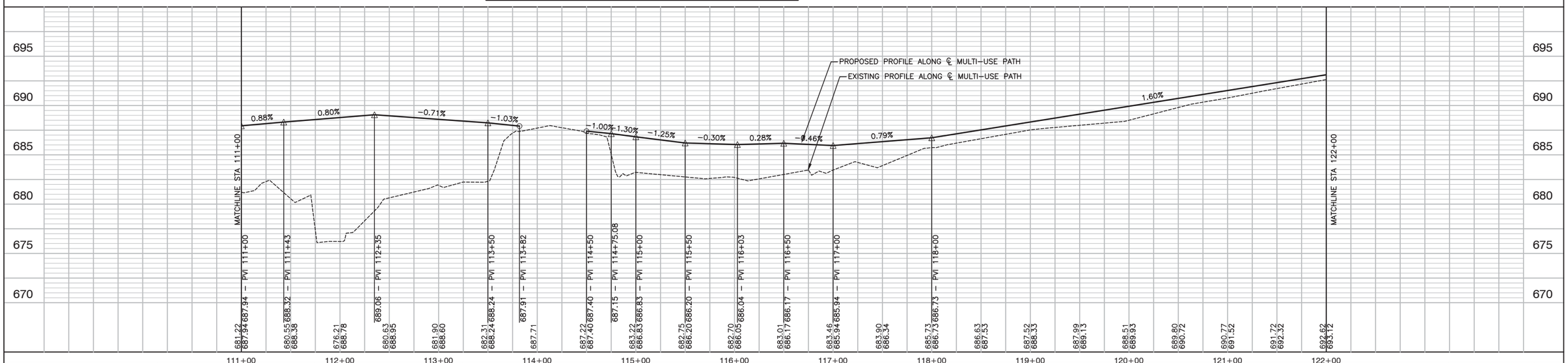


EXISTING CONDITIONS



PROPOSED IMPROVEMENTS

SEE CROSS SECTIONS FOR MULTI-USE PATH GRADES



LEGEND

- (SW) PCC SIDEWALK, 5"
  AGGREGATE BASE COURSE, TYPE B, 4"
- (B) HMA MULTI-USE PATH
  HMA SURFACE COURSE, MIX "D", N50, 3"
  AGGREGATE BASE COURSE, TYPE B, 6"
  (SEE NOTE #4 FOR STATION ±101+50 THROUGH STATION ±106+50 AND STATION ±118+50 THROUGH STATION ±128+50)
- (B1) HMA DRIVEWAY
  HMA SURFACE COURSE, MIX "D", N50, 2"
  HMA BASE COURSE, 6"
- (A1) HMA SURFACE COURSE, MIX "D", N70, 1 1/2"
  HOT-MIX ASPHALT BASE COURSE, 6 3/4"
  AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (A2) HMA SURFACE COURSE, MIX "D", N70, 1 1/2"
  POLYMERIZED LEVELING BINDER, 3/4"
- (CG1) CURB & GUTTER, TYPE B-6.12
- (CG2) CURB & GUTTER, TYPE B-6.24

NOTES

1. ALL PCC SIDEWALK SHALL BE 5' WIDE.
2. ALL CURB RAMPS FOR SIDEWALKS & MULTI-USE PATH SHALL INCLUDE DETECTABLE WARNINGS AND DEPRESSED CURB PER CURRENT HIGHWAY STANDARDS.
3. OFFSETS ARE NOTED FROM ℄ OF CONSTRUCTION.
4. THE TEMPORARY PAVEMENT SHALL BE UTILIZED AS THE MULTI-USE PATH BASE WHERE POSSIBLE. THESE AREAS SHALL BE OVERLAID WITH 1 ½" HMA SURFACE COURSE, MIX "D", N50. (HMA PAVEMENT SECTION WILL BE REDUCED FROM 3" HMA SURFACE TO 1 ½" HMA SURFACE. 6" AGGREGATE BASE COURSE, TYPE B, WILL NOT BE USED).

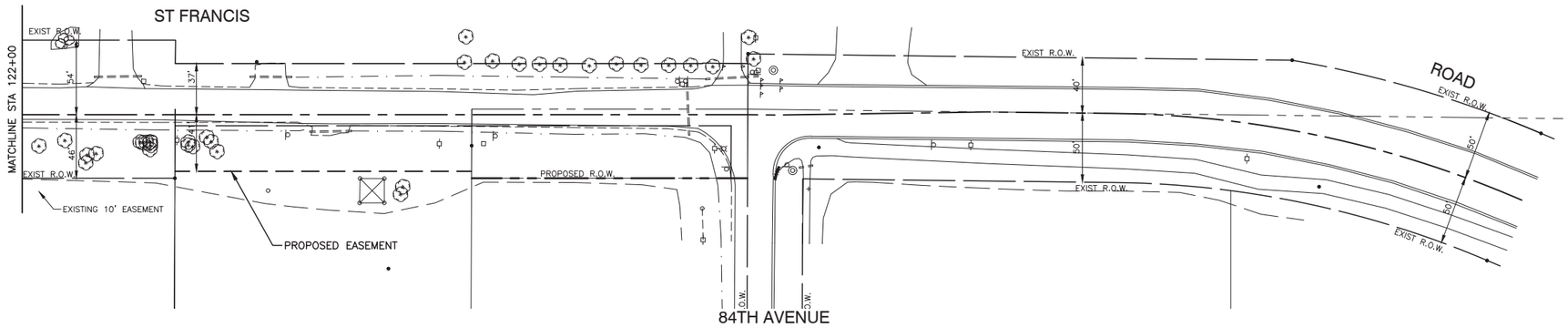
PLAN: SURVEYED, PLOTTED, ALIGNMENT CHECKED, RT. OF WAY CHECKED, CADD FILE NAME, NOTE BOOK, NO., BY, DATE

PROFILE: SURVEYED, PLOTTED, GRADES CHECKED, B.M. NOTED, NOTE BOOK, BY, DATE

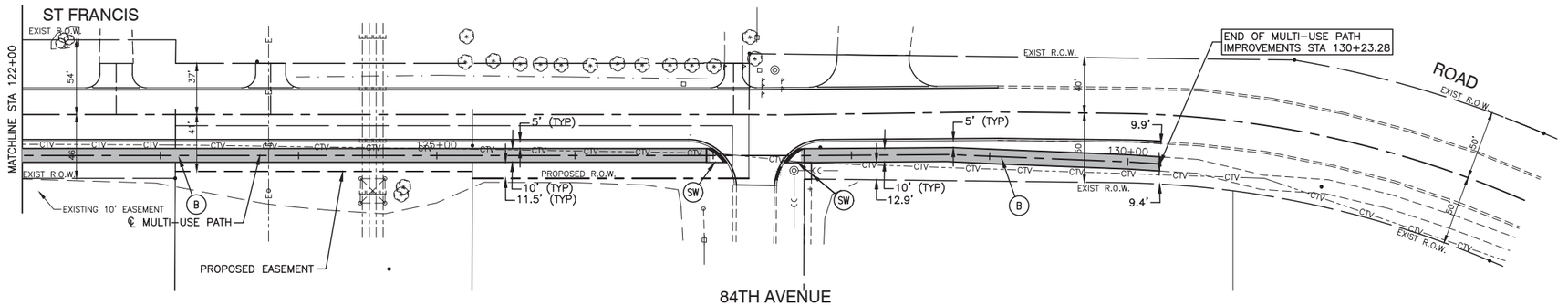
FILE NAME = 10423_12-PLPR-01 - BIKE PLPR(9)	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION MULTI-USE PATH PLAN AND PROFILES
		CHECKED — HLG	REVISED —		
	PLOT SCALE =	DRAWN — MED	REVISED —		
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —		SCALE: H: 1"=50' V:1"=5 | SHEET NO. 17 OF 109 SHEETS | STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	17
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

SECTION 14, TOWNSHIP 35, RANGE 12

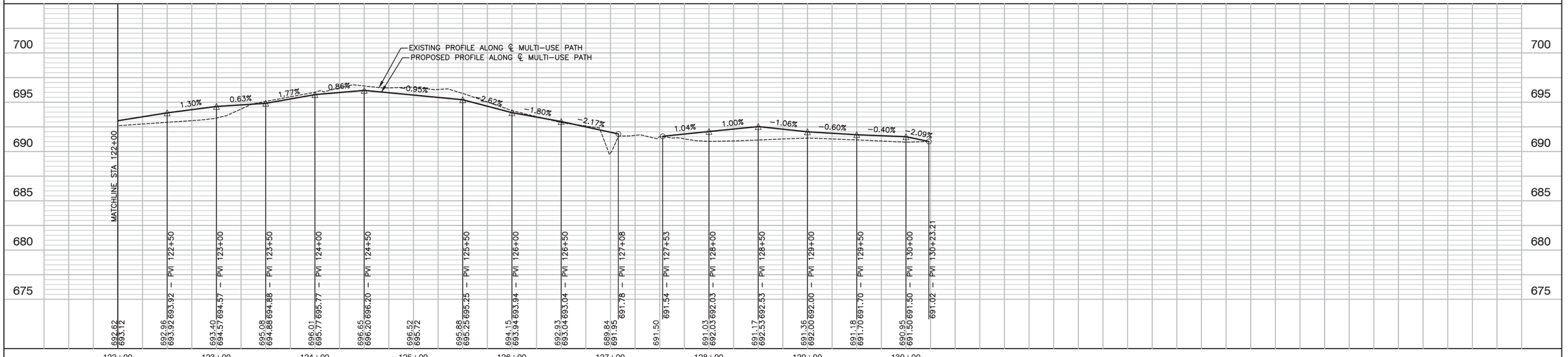


EXISTING CONDITIONS



PROPOSED IMPROVEMENTS

SEE CROSS SECTIONS FOR MULTI-USE PATH GRADES



N

LEGEND

SW	PCC SIDEWALK, 5" AGGREGATE BASE COURSE, TYPE B, 4"
B	HMA MULTI-USE PATH HMA SURFACE COURSE, MIX "D", N50, 3" AGGREGATE BASE COURSE, TYPE B, 6" (SEE NOTE #4 FOR STATION ±101+50 THROUGH STATION ±106+50 AND STATION ±118+50 THROUGH STATION ±128+50)
B1	HMA DRIVEWAY HMA SURFACE COURSE, MIX "D", N50, 2" HMA BASE COURSE, 6"
A1	HMA SURFACE COURSE, MIX "D", N70, 1 1/2" HOT-MIX ASPHALT BASE COURSE, 6 3/4" AGGREGATE SUBGRADE IMPROVEMENT, 12"
A2	HMA SURFACE COURSE, MIX "D", N70, 1 1/2" POLYMERIZED LEVELING BINDER, 3/4"
CG1	CURB & GUTTER, TYPE B-6.12
CG2	CURB & GUTTER, TYPE B-6.24

NOTES

1. ALL PCC SIDEWALK SHALL BE 5' WIDE.
2. ALL CURB RAMPS FOR SIDEWALKS & MULTI-USE PATH SHALL INCLUDE DETECTABLE WARNINGS AND DEPRESSED CURB PER CURRENT HIGHWAY STANDARDS.
3. OFFSETS ARE NOTED FROM ℄ OF CONSTRUCTION.
4. THE TEMPORARY PAVEMENT SHALL BE UTILIZED AS THE MULTI-USE PATH BASE WHERE POSSIBLE. THESE AREAS SHALL BE OVERLAID WITH 1 ½" HMA SURFACE COURSE, MIX "D", N50. (HMA PAVEMENT SECTION WILL BE REDUCED FROM 3" HMA SURFACE TO 1 ½" HMA SURFACE. 6" AGGREGATE BASE COURSE, TYPE B, WILL NOT BE USED).

PLAN — DATE / BY / SURVEYED / PLOTTED / ALIGNMENT CHECKED / RT. OF WAY CHECKED / CADD FILE NAME / NOTE BOOK / NO.

PROFILE — DATE / BY / SURVEYED / PLOTTED / PROFILE CHECKED / E.M. NOTED / NOTE BOOK

FILE NAME = 10423_12-PLPR-01 - BIKE PLPR(10)	USER NAME =	DESIGNED — GA	REVISED —
		CHECKED — HLG	REVISED —
	PLOT SCALE =	DRAWN — MED	REVISED —
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

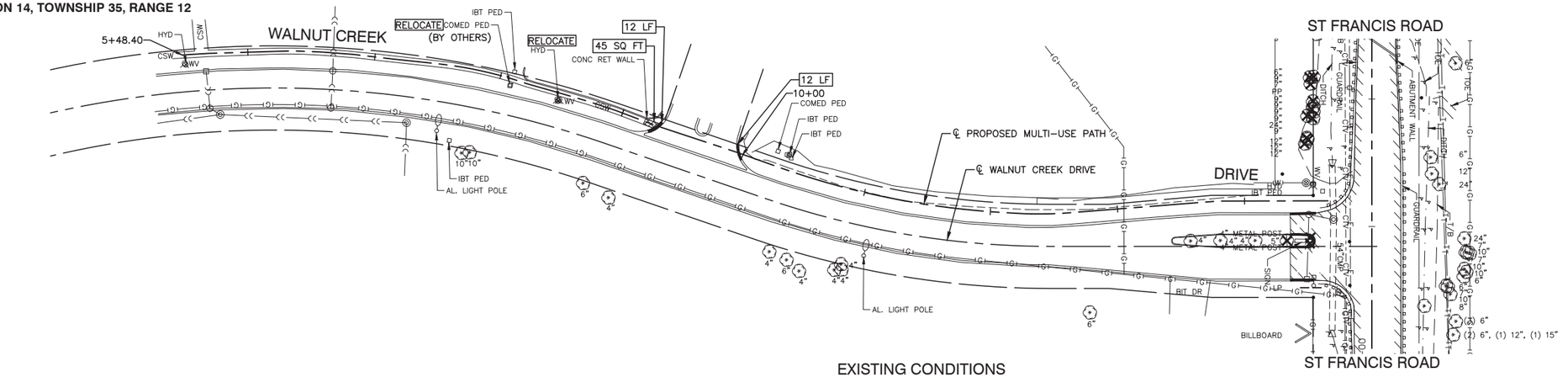
ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
MULTI-USE PATH PLAN AND PROFILES

SCALE: H: 1"=50' V:1"=5 | SHEET NO. 18 OF 109 SHEETS | STA. | TO STA.

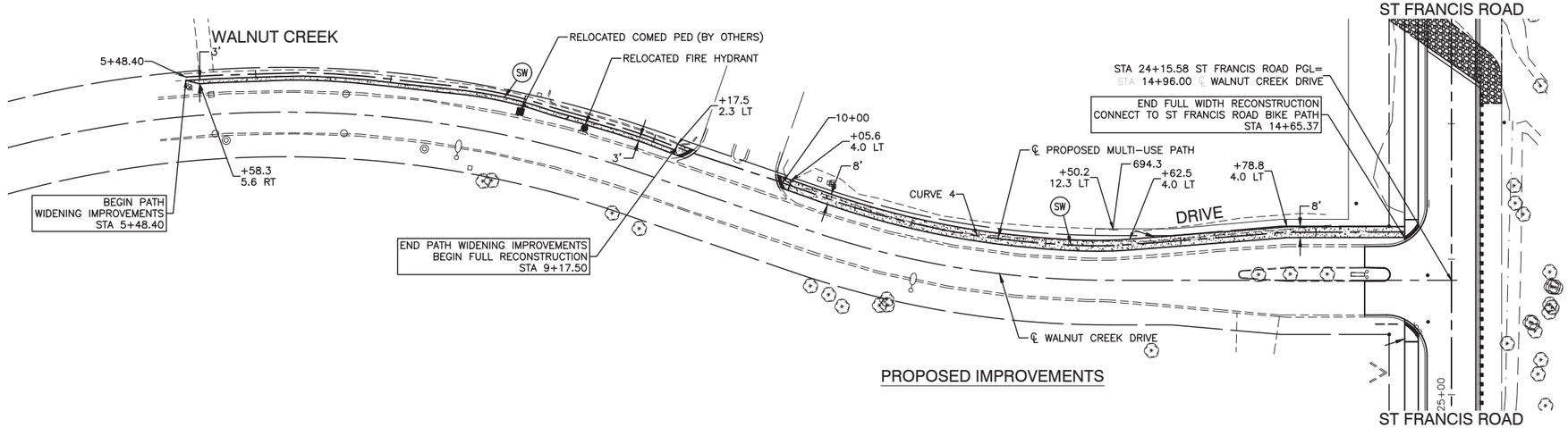
F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	18
		CONTRACT NO. 61D22		

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT ----

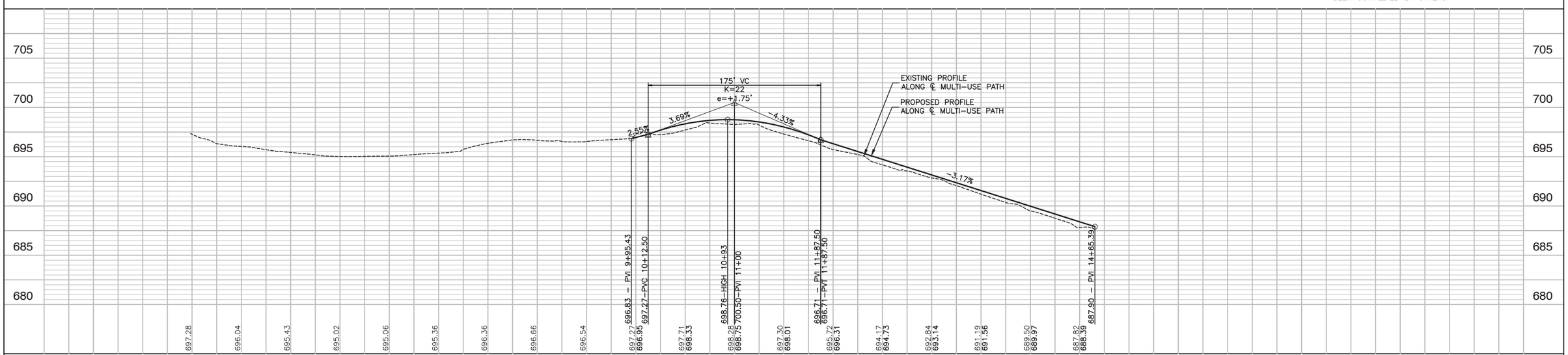
SECTION 14, TOWNSHIP 35, RANGE 12



EXISTING CONDITIONS



PROPOSED IMPROVEMENTS



PI=STA. 11+19.41
Δ=16°09'38"
D=9°02'14"
CURVE 4 R=634.000'
T=90.009'
L=178.823'
E=6.357'

LEGEND

- HMA SURFACE REMOVAL – BUTT JOINT
- TREE TO BE REMOVED (SEE NOTE 5)
- CONC CURB & GUTTER REMOVAL
- PAVEMENT REMOVAL
- SIDEWALK/PATH REMOVAL
- DRIVEWAY REMOVAL
- STONE RIP RAP, CLASS A4
- DETECTABLE WARNINGS (SF)
- (SW) PCC SIDEWALK, 5"
  AGGREGATE BASE COURSE, TYPE B, 4"
- (B) HMA MULTI-USE PATH
  HMA SURFACE COURSE, MIX "D", N50, 3"
  AGGREGATE BASE COURSE, TYPE B, 6"
- (B1) HMA DRIVEWAY
  HMA SURFACE COURSE, MIX "D", N50, 2"
  HMA BASE COURSE, 6"
- (A1) HMA SURFACE COURSE, MIX "D", N70, 1 1/2"
  HOT-MIX ASPHALT BASE COURSE, 6 3/4"
  AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (A2) HMA SURFACE COURSE, MIX "D", N70, 1 1/2"
  POLYMERIZED LEVELING BINDER, 3/4"
- (C1) BARRIER CURB
- (CG1) CURB & GUTTER, TYPE B-6.12
- (CG2) CURB & GUTTER, TYPE B-6.24
- (D) DEPRESSED CURB & GUTTER, TYPE B-6.12
- (T1) 10' TRANSITION FROM C&G TYPE B-6.24 TYPE B-6.12

NOTES

1. ALL PCC SIDEWALK SHALL BE 5' WIDE.
2. ALL CURB RAMPS FOR SIDEWALKS & MULTI-USE PATH SHALL INCLUDE DETECTABLE WARNINGS AND DEPRESSED CURB PER CURRENT HIGHWAY STANDARDS.
3. OFFSETS ARE NOTED FROM ℄ OF CONSTRUCTION.
4. RESIDENTIAL DRIVEWAYS HAVE 5' FLARE UNLESS OTHERWISE NOTED.
5. MAJORITY OF TREE REMOVALS HAVE ALREADY BEEN COMPLETED BY OTHERS.

PLAN: SURVEYED, PLOTTED, ALIGNMENT CHECKED, RT. OF WAY CHECKED, CADD FILE NAME, NOTE BOOK, NO., BY, DATE

PROFILE: SURVEYED, PLOTTED, GRADES CHECKED, B.M. NOTED, NOTE BOOK, BY, DATE

FILE NAME = 10423_12-PLPR-01 - PLPR WAL 01	USER NAME =	DESIGNED — GA	REVISED —
		CHECKED — HLG	REVISED —
	PLOT SCALE =	DRAWN — MED	REVISED —
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

WALNUT CREEK DRIVE
RECONSTRUCTION
MULTI-USE PATH PLAN AND PROFILES

SCALE: H: 1"=50' V:1"=5 | SHEET NO. 19 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	19
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

SECTION 14, TOWNSHIP 35, RANGE 12

LEGEND

- COMPLETELY CLOSED PORTION
- TYPE III BARRICADE WITH FLASHING LIGHT
- SIGN
- CHANGEABLE MESSAGE SIGN

GENERAL NOTES

1. TRAFFIC CONTROL AND PROTECTION, INCLUDING DETOUR PLAN/ROUTING, SHALL BE PERFORMED IN ACCORDANCE WITH THE SPECIAL PROVISIONS FOR THIS PROJECT, SECTION 701 OF THE IDOT "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", APPLICABLE GUIDELINES IN THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", AND APPLICABLE HIGHWAY STANDARDS FOR TRAFFIC CONTROL.
2. ANY DROP-OFF GREATER THAN THREE INCHES WITHIN EIGHT FEET OF THE PAVEMENT EDGE, SHALL BE PROTECTED BY TYPE I OR II BARRICADES (OR DRUMS) EQUIPPED WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS SPACES AT 50-FOOT CENTERS. BARRICADES THAT MUST BE PLACED IN EXCAVATED AREAS SHALL HAVE LEG EXTENSIONS INSTALLED SUCH THAT THE TOP OF THE BARRICADE IS IN COMPLIANCE WITH THE HEIGHT REQUIREMENTS OF THE IDOT HIGHWAY STANDARDS.
3. ARROW BOARDS WILL BE REQUIRED WHEN IMPLEMENTING ALL LANE CLOSURES.
4. PRIOR TO THE START OF CONSTRUCTION, REQUIRED TRAFFIC CONTROL DEVICES SHALL BE IN PLACE.
5. IDOT TRAFFIC CONTROL STANDARDS IN THE SPECIFICATION BOOK SHALL BE USED AS GUIDES IN IMPLEMENTING THE TRAFFIC CONTROL, INCLUDING SIGN LOCATIONS, FOR THIS PROJECT.
6. MONO-DIRECTIONAL FLASHING LIGHTS AND ORANGE WARNING FLAGS SHALL BE MOUNTED ON ALL "ROAD CONSTRUCTION AHEAD" SIGNS.
7. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL PROPERTIES DURING CONSTRUCTION OPERATIONS.
8. DEPICTED TRAFFIC CONTROL IS A MINIMUM REQUIREMENT, AND OTHER WORK OR SIGNING MAY BE REQUIRED AS PER THE INSTRUCTION OF THE ENGINEER IN CHARGE. THE COST INCURRED TO COMPLY WILL BE INCLUDED IN THE PAY ITEMS "TRAFFIC CONTROL AND PROTECTION, (SPECIAL).
9. THE ILLINOIS DEPARTMENT OF TRANSPORTATION, AND THE ENGINEER SHALL BE NOTIFIED IN WRITING AT LEAST THREE WEEKS PRIOR TO THE DAY THE DETOUR IS TO BE IN EFFECT. THE CONTRACTOR WILL CONTACT THE APPROPRIATE LOCAL AGENCIES AND INTERESTED PARTIES.
10. THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH THE NAMES AND PHONE NUMBERS OF HIS REPRESENTATIVES ON THE CONSTRUCTION SITE AND HIS REPRESENTATIVE RESPONSIBLE FOR THE DETOUR SIGNING, PRIOR TO THE START OF WORK.
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE FIELD LOCATION OF ALL DETOUR AND CONSTRUCTION SIGNING. THE CONTRACTOR MAY REQUEST THE ENGINEER TO FIELD VERIFY THE POSITIONS OF ANY SIGNS.
12. ALL EXISTING SIGNING THAT IS NOT APPLICABLE WHILE THE DETOUR IS IN EFFECT SHALL BE COMPLETELY COVERED, OR REMOVED, BY THE CONTRACTOR IN A MANNER MEETING THE APPROVAL OF THE ENGINEER.
13. ADVANCED CLOSURE SIGNS (CHANGEABLE MESSAGE SIGNS) SHALL BE ERECTED AT LEAST 10 DAYS PRIOR TO ANY ROAD CLOSURES.
14. THE CONTRACTOR SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470, 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.

1 — ROAD CONSTRUCTION AHEAD — W20-1 48"X48"
2 — DETOUR AHEAD — W20-2 48"X48"
3 — ROAD CLOSED 500 FT — W20-3 48"X48"
4 — DETOUR — M4-9 30"X24"
6 — DETOUR — M4-9R 30"X24"
7 — DETOUR — M4-9L 30"X24"
8 — END DETOUR — M4-8a 24"X18"
9 — ROAD CLOSED AHEAD — W20-3 48"X48"
6A — DETOUR — M4-9R (MODIFIED) 30"X24"
7A — DETOUR — M4-9L (MODIFIED) 30"X24"
10 — NORTH — M3-1(0) 24"X12"
11 — WEST — M3-4(0) 24"X12"
12 — SOUTH — M3-3(0) 24"X12"
13 — EAST — M3-2(0) 24"X12"
14 — ROAD CLOSED — R11-2 48"X30"
16 — DETOUR — M4-10R 48"X18"
17 — DETOUR — M4-10L 48"X18"
18 — Walnut Creek Dr — D3-1(0) 30"X9"
19 — ROAD CLOSED TO THRU TRAFFIC — R11-4 60"X30"

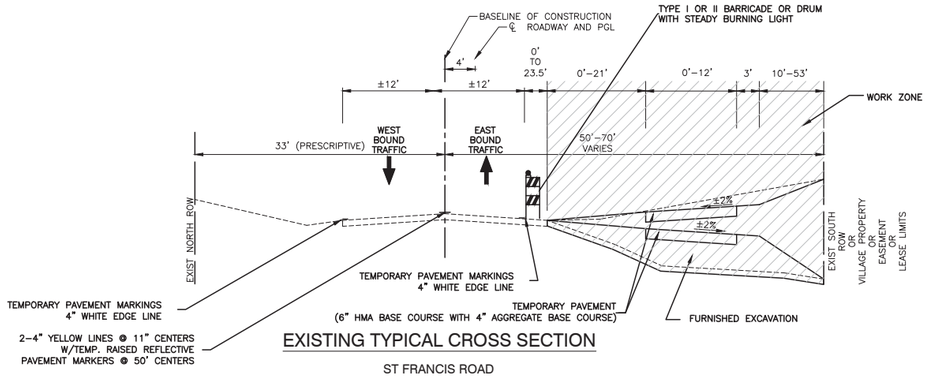
FILE NAME = 10423_12-DETR-01 - IDOT DETR	USER NAME =	DESIGNED — GA	REVISED —
		CHECKED — HLG	REVISED —
	PLOT SCALE =	DRAWN — MED	REVISED —
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
DETOUR PLAN

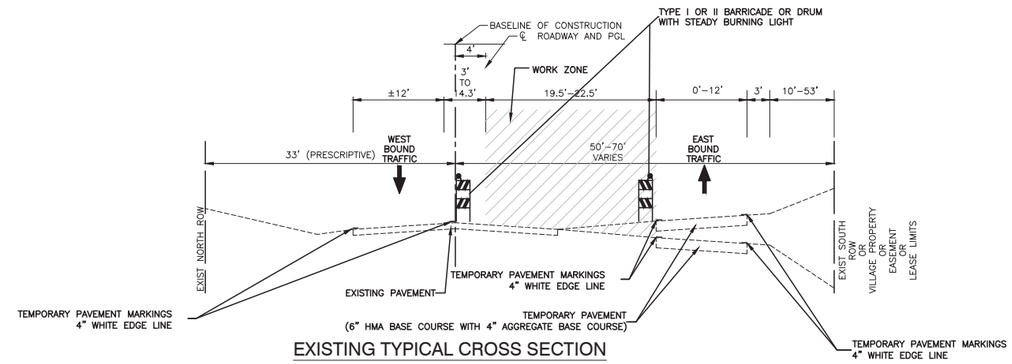
SCALE: 1" = 250' | SHEET NO. 20 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	20
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



EXISTING TYPICAL CROSS SECTION

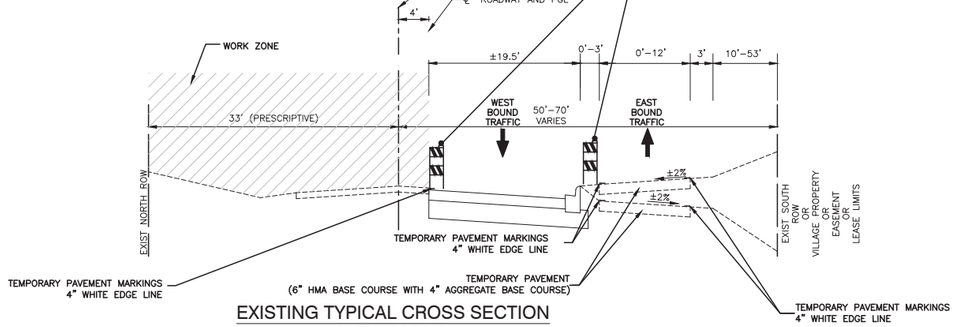
ST FRANCIS ROAD
STAGE 1
STA 10+00 TO STA 40+09



EXISTING TYPICAL CROSS SECTION

ST FRANCIS ROAD
STAGE 2 *
STA 10+00 TO STA 19+35
STA 26+20 TO STA 40+09

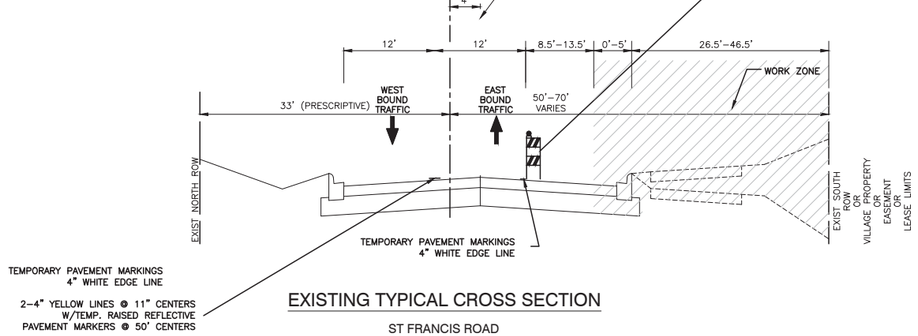
*SEE PLANS AND IDOT STANDARD 701321-14 FOR STATION 19+35 TO 26+20
PLAN TAPERS AND GEOMETRY SUPERSEDE STANDARD.



EXISTING TYPICAL CROSS SECTION

ST FRANCIS ROAD
STAGE 3 *
STA 10+00 TO STA 19+35
STA 26+20 TO STA 40+09

*SEE PLANS AND IDOT STANDARD 701321-14 FOR STATION 19+35 TO 26+20
PLAN TAPERS AND GEOMETRY SUPERSEDE STANDARD.



EXISTING TYPICAL CROSS SECTION

ST FRANCIS ROAD
STAGE 4
STA 10+00 TO STA 40+09

SUGGESTED CONSTRUCTION STAGING NOTES

STAGE 1

- CLEARING / TREE REMOVAL
- TEMPORARY PAVEMENT
- STORM SEWER WORK – SOUTH SIDE OF ST. FRANCIS ROAD
- CURB ON PORTIONS OF SOUTH SIDE OF ST. FRANCES ROAD

STAGE 2

- STORM SEWER WORK – BALANCE OF SOUTH SIDE OF ST. FRANCIS ROAD
- CLEARING AND REMOVAL
- PORTIONS OF CURB; AGGREGATE SUBBASE, & HMA BASE AND BINDER ON SOUTH SIDE OF ST. FRANCIS ROAD
- STAGE 1 BRIDGE WORK – BRIDGE REMOVAL AND REPLACEMENT – SOUTH SIDE OF ST FRANCIS ROAD

STAGE 3

- STORM SEWER WORK – NORTH SIDE OF ST. FRANCIS ROAD
- CLEARING AND REMOVAL
- CURB, AGGREGATE SUBBASE, & HMA BASE AND BINDER ON NORTH SIDE OF ST. FRANCIS ROAD
- STAGE 2 BRIDGE WORK – BRIDGE REMOVAL AND REPLACEMENT – NORTH SIDE OF ST FRANCIS ROAD

STAGE 4

- BALANCE OF CURB ON SOUTH SIDE OF ST. FRANCIS ROAD
- REMOVE APPLICABLE PORTIONS OF TEMPORARY PAVEMENT
- CONSTRUCT MULTI-USE PATH
- HMA SURFACE ENTIRE PROJECT
- LANDSCAPING, SIGNING, AND STRIPING

FILE NAME = 10423_12-TYPX-01 - IDOT P03	USER NAME =	DESIGNED — GA	REVISED —
		CHECKED — HLG	REVISED —
	PLOT SCALE =	DRAWN — MED	REVISED —
	PLOT DATE = 06-09-16	CHECKED — AG	REVISED —

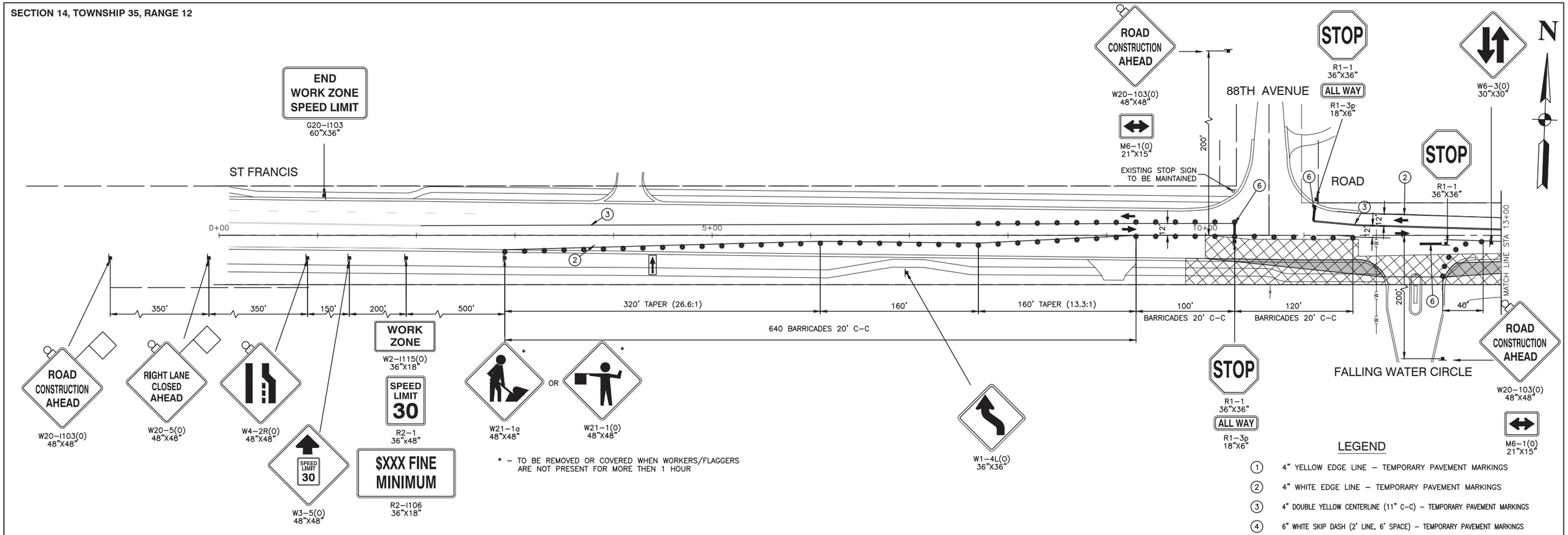
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
SUGGESTED CONSTRUCTION STAGING TYPICAL CROSS SECTIONS

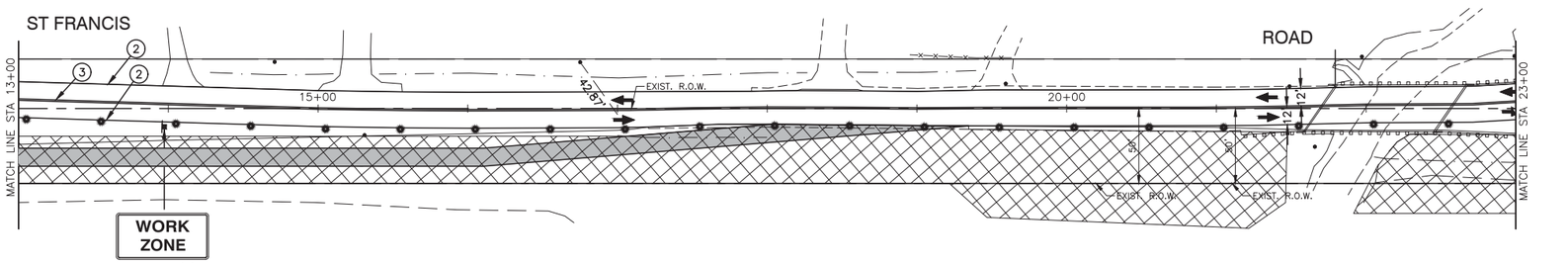
SCALE: NONE | SHEET NO. 21 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	21
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

SECTION 14, TOWNSHIP 35, RANGE 12



* – TO BE REMOVED OR COVERED WHEN WORKERS/FLAGGERS ARE NOT PRESENT FOR MORE THEN 1 HOUR



MAINTENANCE OF TRAFFIC GENERAL NOTES

1. TRAFFIC CONTROL AND PROTECTION, INCLUDING DETOUR PLAN/ROUTING, SHALL BE PERFORMED IN ACCORDANCE WITH THE SPECIAL PROVISIONS FOR THIS PROJECT, SECTION 701 OF THE IDOT "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", APPLICABLE GUIDELINES IN THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", AND APPLICABLE HIGHWAY STANDARDS FOR TRAFFIC CONTROL.
2. ANY DROP-OFF GREATER THAN THREE INCHES WITHIN EIGHT FEET OF THE PAVEMENT EDGE, SHALL BE PROTECTED BY TYPE I OR II BARRICADES (OR DRUMS) EQUIPPED WITH MONO-DIRECTIONAL STEADY-BURN LIGHTS SPACES AT 50-FOOT CENTERS. BARRICADES THAT MUST BE PLACED IN EXCAVATED AREAS SHALL HAVE LEG EXTENSIONS INSTALLED SUCH THAT THE TOP OF THE BARRICADE IS IN COMPLIANCE WITH THE HEIGHT REQUIREMENTS OF THE IDOT HIGHWAY STANDARDS.
3. ARROW BOARDS WILL BE REQUIRED WHEN IMPLEMENTING ALL LANE CLOSURES PRIOR TO THE START OF CONSTRUCTION, REQUIRED TRAFFIC CONTROL DEVICES SHALL BE IN PLACE.
4. IDOT TRAFFIC CONTROL STANDARDS LISTED BY REFERENCE ON THE TITLE SHEET SHALL BE USED AS GUIDES IN IMPLEMENTING THE TRAFFIC CONTROL, INCLUDING SIGN LOCATIONS, FOR THIS PROJECT.
5. MONO-DIRECTIONAL FLASHING LIGHTS AND ORANGE WARNING FLAGS SHALL BE MOUNTED ON ALL "ROAD CONSTRUCTION AHEAD" SIGNS.
6. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL PROPERTIES DURING CONSTRUCTION OPERATIONS.
7. DEPICTED TRAFFIC CONTROL IS A MINIMUM REQUIREMENT, AND OTHER WORK OR SIGNING MAY BE REQUIRED AS PER THE INSTRUCTION OF THE ENGINEER IN CHARGE. THE COST INCURRED TO COMPLY WILL BE INCLUDED IN THE PAY ITEMS "TRAFFIC CONTROL AND PROTECTION, SPECIAL" AND "TRAFFIC CONTROL AND PROTECTION FOR TEMPORARY DETOUR".
8. THE VILLAGE OF FRANKFORT, THE FRANKFORT TOWNSHIP HIGHWAY DEPARTMENT, AND THE ENGINEER SHALL BE NOTIFIED IN WRITING AT LEAST THREE WEEKS PRIOR TO THE DAY THE DETOUR IS TO BE IN EFFECT. THE CONTRACTOR WILL CONTACT THE APPROPRIATE LOCAL AGENCIES AND INTERESTED PARTIES.
9. THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH THE NAMES AND PHONE NUMBERS OF HIS REPRESENTATIVES ON THE CONSTRUCTION SITE AND HIS REPRESENTATIVE RESPONSIBLE FOR THE DETOUR SIGNING, PRIOR TO THE START OF WORK
10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE FIELD LOCATION OF ALL DETOUR AND CONSTRUCTION SIGNING. THE CONTRACTOR MAY REQUEST THE ENGINEER TO FIELD VERIFY THE POSITIONS OF ANY SINGS.
11. ALL EXISTING SIGNING THAT IS NOT APPLICABLE WHILE THE DETOUR IS IN EFFECT SHALL BE COMPLETELY COVERED, OR REMOVED, BY THE CONTRACTOR IN A MANNER MEETING THE APPROVAL OF THE ENGINEER
12. WALNUT CREEK DRIVE WILL BE CLOSED TO TRAFFIC DURING CONSTRUCTION AND WILL BE DETOURED AS DEPICTED IN THE DETOUR PLAN.
13. TWO-WAY TRAFFIC WILL BE MAINTAINED ON ST. FRANCIS ROAD AT ALL TIMES.
14. ADVANCED CLOSURE SIGNS (CHANGEABLE MESSAGE SIGNS) SHALL BE ERECTED AT LEAST 10 DAYS PRIOR TO ANY ROAD CLOSURES.
15. THE CONTRACTOR SHALL NOTIFY THE IDOT ARTERIAL TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470, A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
16. 88TH AVENUE, FALLING WATER CIRCLE, AND 84TH AVENUE SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES.

LEGEND

(1)	4" YELLOW EDGE LINE – TEMPORARY PAVEMENT MARKINGS
(2)	4" WHITE EDGE LINE – TEMPORARY PAVEMENT MARKINGS
(3)	4" DOUBLE YELLOW CENTERLINE (11" C-C) – TEMPORARY PAVEMENT MARKINGS
(4)	6" WHITE SKIP DASH (2' LINE, 6' SPACE) – TEMPORARY PAVEMENT MARKINGS
(5)	6" YELLOW SKIP DASH (2' LINE, 6' SPACE) – TEMPORARY PAVEMENT MARKINGS
(6)	24" WHITE STOP BAR – TEMPORARY PAVEMENT MARKINGS
(7)	TEMPORARY SYMBOLS AND LETTERS, LARGE
(8)	4" WHITE SKIP DASH (10' LINE, 30' SPACE) – TEMPORARY PAVEMENT MARKINGS
(9)	6" WHITE LANE LINE – TEMPORARY PAVEMENT MARKINGS
	WORK AREA
	TEMPORARY PAVEMENT TO BE CONSTRUCTED
	VERTICAL PANEL WITH STEADY BURNING LIGHT (50' C-C)
	TYPE I OR II BARRICADE OR DRUM WITH STEADY BURNING LIGHT (50' C-C UNLESS OTHERWISE NOTED)
	TYPE III BARRICADE WITH STEADY BURNING LIGHT
	SIGN
	TYPE F BARRICADE
	CRASH ATTENUATER
	TRAFFIC FLOW
	FLASHING ARROW BOARD

NOTES

1. BARRICADES ALONG EXISTING EDGE OF TRAFFIC MAXIMUM SPACING 50' C-C.
2. WORK TO BE DONE UNDER IDOT TEMPORARY TRAFFIC CONTROL STANDARDS WITH FLAGGERS.
3. EXISTING SIGNS TO BE MAINTAINED, E.G. STOP SIGNS.
4. MAINTAIN DRUMS ALONG CONSTRUCTION AREA TURNS.
5. SEE DETOUR PLAN FOR ADDITIONAL SIGNAGE.
6. ALL EXISTING PAVEMENT MARKINGS THAT ARE IN CONFLICT WITH THE PROPOSED STAGING PLAN SHALL BE REMOVED PRIOR TO THE IMPLEMENTATION OF THE STAGING PLAN.

FILE NAME = 10423_12-TCON-01 - TCON-P1- P01	USER NAME =	DESIGNED – GA	REVISED –
		CHECKED – HLG	REVISED –
	PLOT SCALE =	DRAWN – MED	REVISED –
	PLOT DATE = 06-09-16	CHECKED – AG	REVISED –

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

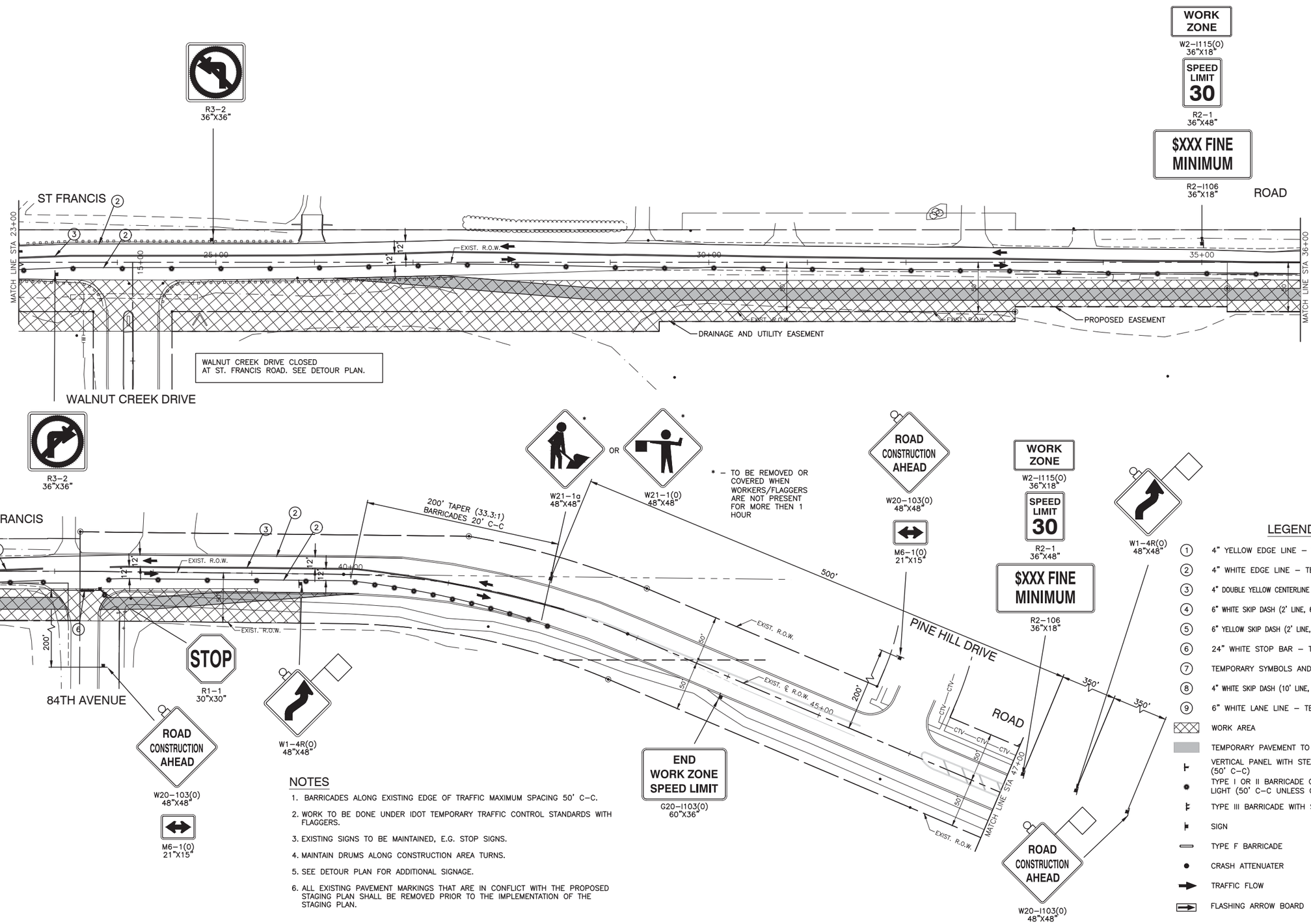
ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
SUGGESTED CONSTRUCTION STAGING - STAGE I

SCALE: 1"=50' | SHEET NO. 22 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	22
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

SECTION 14, TOWNSHIP 35, RANGE 12

N



NOTES

1. BARRICADES ALONG EXISTING EDGE OF TRAFFIC MAXIMUM SPACING 50' C-C.
2. WORK TO BE DONE UNDER IDOT TEMPORARY TRAFFIC CONTROL STANDARDS WITH FLAGGERS.
3. EXISTING SIGNS TO BE MAINTAINED, E.G. STOP SIGNS.
4. MAINTAIN DRUMS ALONG CONSTRUCTION AREA TURNS.
5. SEE DETOUR PLAN FOR ADDITIONAL SIGNAGE.
6. ALL EXISTING PAVEMENT MARKINGS THAT ARE IN CONFLICT WITH THE PROPOSED STAGING PLAN SHALL BE REMOVED PRIOR TO THE IMPLEMENTATION OF THE STAGING PLAN.

LEGEND

1. 4" YELLOW EDGE LINE – TEMPORARY PAVEMENT MARKINGS
2. 4" WHITE EDGE LINE – TEMPORARY PAVEMENT MARKINGS
3. 4" DOUBLE YELLOW CENTERLINE (11" C-C) – TEMPORARY PAVEMENT MARKINGS
4. 6" WHITE SKIP DASH (2' LINE, 6' SPACE) – TEMPORARY PAVEMENT MARKINGS
5. 6" YELLOW SKIP DASH (2' LINE, 6' SPACE) – TEMPORARY PAVEMENT MARKINGS
6. 24" WHITE STOP BAR – TEMPORARY PAVEMENT MARKINGS
7. TEMPORARY SYMBOLS AND LETTERS, LARGE
8. 4" WHITE SKIP DASH (10' LINE, 30' SPACE) – TEMPORARY PAVEMENT MARKINGS
9. 6" WHITE LANE LINE – TEMPORARY PAVEMENT MARKINGS

WORK AREA

TEMPORARY PAVEMENT TO BE CONSTRUCTED

VERTICAL PANEL WITH STEADY BURNING LIGHT (50' C-C)

TYPE I OR II BARRICADE OR DRUM WITH STEADY BURNING LIGHT (50' C-C UNLESS OTHERWISE NOTED)

TYPE III BARRICADE WITH STEADY BURNING LIGHT

SIGN

TYPE F BARRICADE

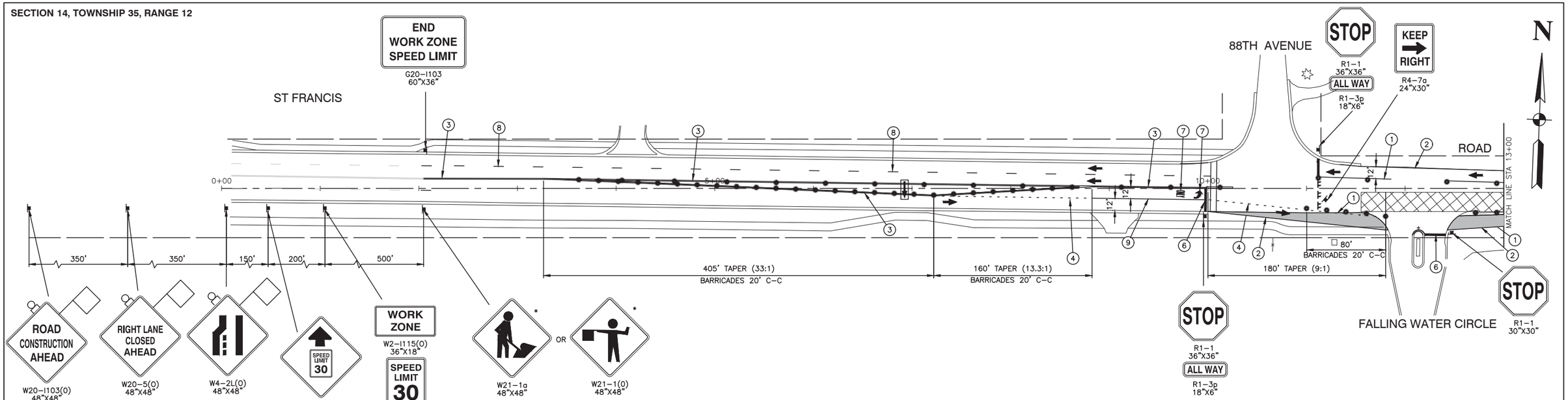
CRASH ATTENUATER

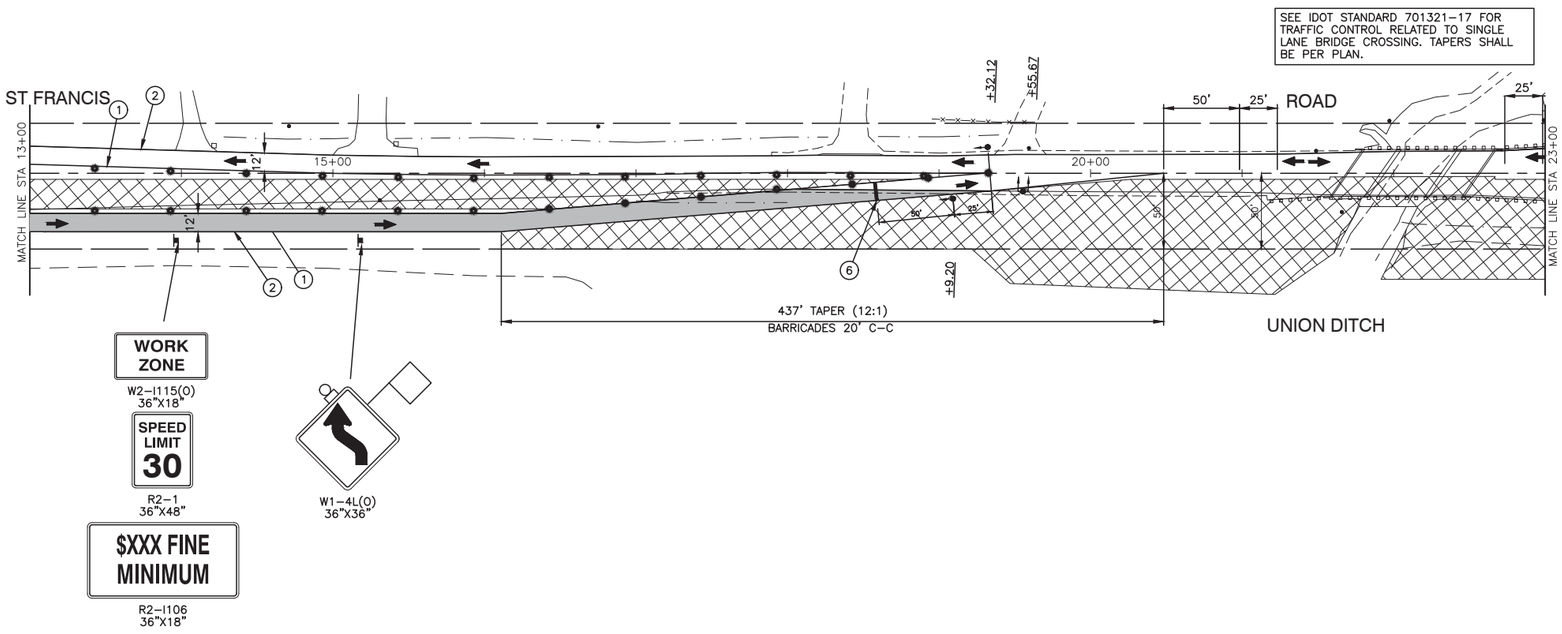
TRAFFIC FLOW

FLASHING ARROW BOARD

FILE NAME = 10423_12-TCON-01 - TCON-P1- P02	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION SUGGESTED CONSTRUCTION STAGING - STAGE I			F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		CHECKED — HLG	REVISED —					3752	10-00045-00-WR	WILL	109	23
	PLOT SCALE =	DRAWN — MED	REVISED —							CONTRACT NO. 61D22		
	PLOT DATE = 06-09-16	CHECKED — AG	REVISED —		SCALE: 1"=50'	SHEET NO. 23 OF 109 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

SECTION 14, TOWNSHIP 35, RANGE 12





LEGEND

1. 4" YELLOW EDGE LINE – TEMPORARY PAVEMENT MARKINGS
2. 4" WHITE EDGE LINE – TEMPORARY PAVEMENT MARKINGS
3. 4" DOUBLE YELLOW CENTERLINE (11" C-C) – TEMPORARY PAVEMENT MARKINGS
4. 6" WHITE SKIP DASH (2' LINE, 6' SPACE) – TEMPORARY PAVEMENT MARKINGS
5. 6" YELLOW SKIP DASH (2' LINE, 6' SPACE) – TEMPORARY PAVEMENT MARKINGS
6. 24" WHITE STOP BAR – TEMPORARY PAVEMENT MARKINGS
7. TEMPORARY SYMBOLS AND LETTERS, LARGE
8. 4" WHITE SKIP DASH (10' LINE, 30' SPACE) – TEMPORARY PAVEMENT MARKINGS
9. 6" WHITE LANE LINE – TEMPORARY PAVEMENT MARKINGS

WORK AREA

TEMPORARY PAVEMENT

VERTICAL PANEL WITH STEADY BURNING LIGHT (50' C-C)

TYPE I OR II BARRICADE OR DRUM WITH STEADY BURNING LIGHT (50' C-C UNLESS OTHERWISE NOTED)

TYPE III BARRICADE WITH STEADY BURNING LIGHT

SIGN

TYPE F BARRICADE

CRASH ATTENUATER

TRAFFIC FLOW

FLASHING ARROW BOARD

TRAFFIC SIGNAL (1 HEAD)

TRAFFIC SIGNAL (2 HEAD)

NOTES

1. BARRICADES ALONG EXISTING EDGE OF TRAFFIC MAXIMUM SPACING 50' C-C.
2. WORK TO BE DONE UNDER IDOT TEMPORARY TRAFFIC CONTROL STANDARDS WITH FLAGGERS.
3. EXISTING SIGNS TO BE MAINTAINED, E.G. STOP SIGNS.
4. MAINTAIN DRUMS ALONG CONSTRUCTION AREA TURNS.
5. SEE DETOUR PLAN FOR ADDITIONAL SIGNAGE.
6. ALL EXISTING PAVEMENT MARKINGS THAT ARE IN CONFLICT WITH THE PROPOSED STAGING PLAN SHALL BE REMOVED PRIOR TO THE IMPLEMENTATION OF THE STAGING PLAN.

FILE NAME = 10423_12-TCON-01 - TCON-P2- P01	USER NAME =	DESIGNED – GA	REVISED –
		CHECKED – HLG	REVISED –
	PLOT SCALE =	DRAWN – MED	REVISED –
	PLOT DATE = 06-09-16	CHECKED – AG	REVISED –

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

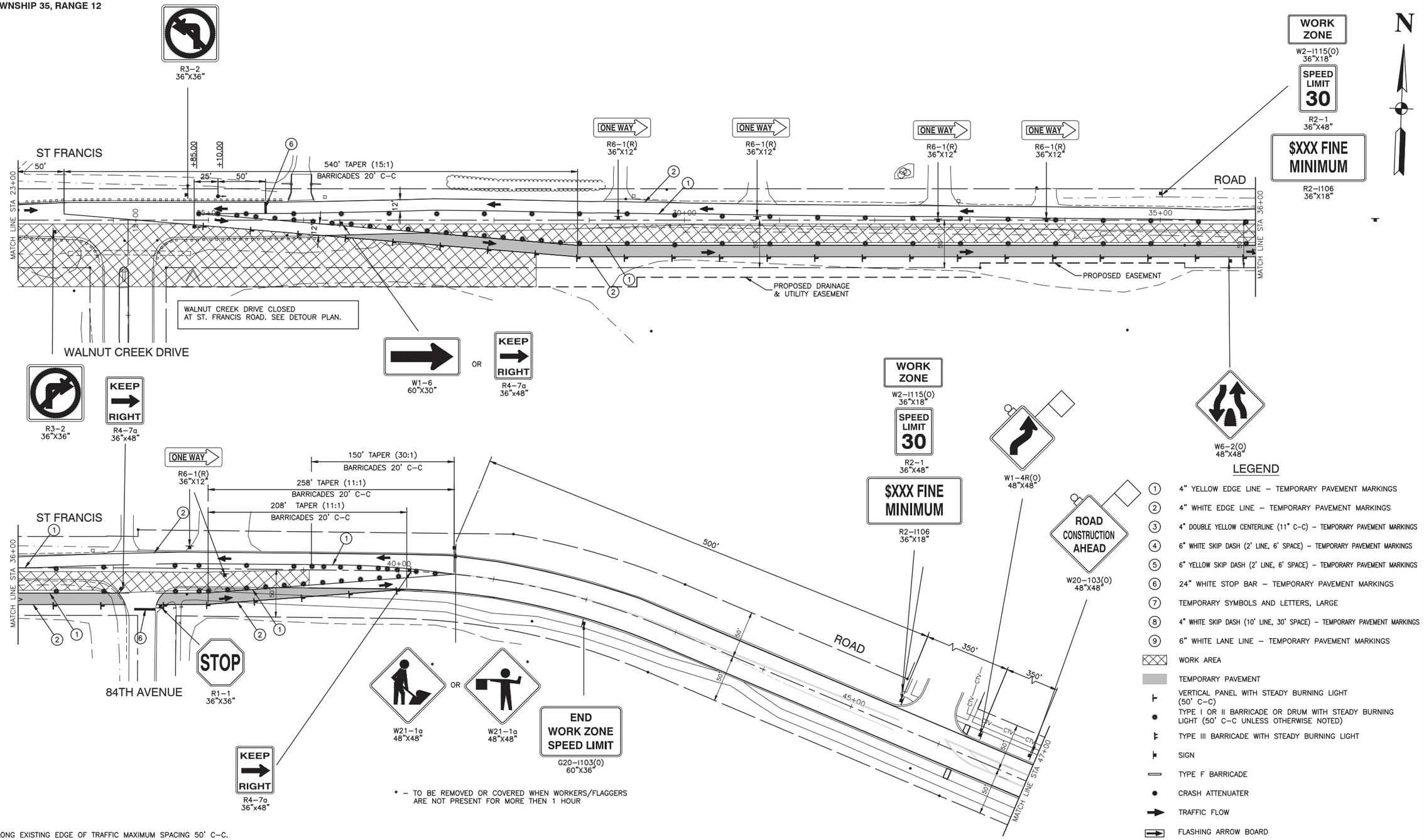
ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
SUGGESTED CONSTRUCTION STAGING - STAGE II

SCALE: 1"=50' | SHEET NO. 24 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	24
		CONTRACT NO. 61D22		

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT ----

SECTION 14, TOWNSHIP 35, RANGE 12



* – TO BE REMOVED OR COVERED WHEN WORKERS/FLAGGERS ARE NOT PRESENT FOR MORE THEN 1 HOUR

LEGEND

1. 4" YELLOW EDGE LINE – TEMPORARY PAVEMENT MARKINGS
2. 4" WHITE EDGE LINE – TEMPORARY PAVEMENT MARKINGS
3. 4" DOUBLE YELLOW CENTERLINE (11" C-C) – TEMPORARY PAVEMENT MARKINGS
4. 6" WHITE SKIP DASH (2' LINE, 6' SPACE) – TEMPORARY PAVEMENT MARKINGS
5. 6" YELLOW SKIP DASH (2' LINE, 6' SPACE) – TEMPORARY PAVEMENT MARKINGS
6. 24" WHITE STOP BAR – TEMPORARY PAVEMENT MARKINGS
7. TEMPORARY SYMBOLS AND LETTERS, LARGE
8. 4" WHITE SKIP DASH (10' LINE, 30' SPACE) – TEMPORARY PAVEMENT MARKINGS
9. 6" WHITE LANE LINE – TEMPORARY PAVEMENT MARKINGS

WORK AREA

TEMPORARY PAVEMENT

VERTICAL PANEL WITH STEADY BURNING LIGHT (50' C-C)

TYPE I OR II BARRICADE OR DRUM WITH STEADY BURNING LIGHT (50' C-C UNLESS OTHERWISE NOTED)

TYPE III BARRICADE WITH STEADY BURNING LIGHT

SIGN

TYPE F BARRICADE

CRASH ATTENUATER

TRAFFIC FLOW

FLASHING ARROW BOARD

TRAFFIC SIGNAL (1 HEAD)

TRAFFIC SIGNAL (2 HEAD)

NOTES

1. BARRICADES ALONG EXISTING EDGE OF TRAFFIC MAXIMUM SPACING 50' C-C.
2. WORK TO BE DONE UNDER IDOT TEMPORARY TRAFFIC CONTROL STANDARDS WITH FLAGGERS.
3. EXISTING SIGNS TO BE MAINTAINED, E.G. STOP SIGNS.
4. MAINTAIN DRUMS ALONG CONSTRUCTION AREA TURNS.
5. SEE DETOUR PLAN FOR ADDITIONAL SIGNAGE.
6. ALL EXISTING PAVEMENT MARKINGS THAT ARE IN CONFLICT WITH THE PROPOSED STAGING PLAN SHALL BE REMOVED PRIOR TO THE IMPLEMENTATION OF THE STAGING PLAN.

FILE NAME = 10423_12-TCON-01 - TCON-P2- P02	USER NAME =	DESIGNED — GA	REVISED —
		CHECKED — HLG	REVISED —
	PLOT SCALE =	DRAWN — MED	REVISED —
	PLOT DATE = 06-09-16	CHECKED — AG	REVISED —

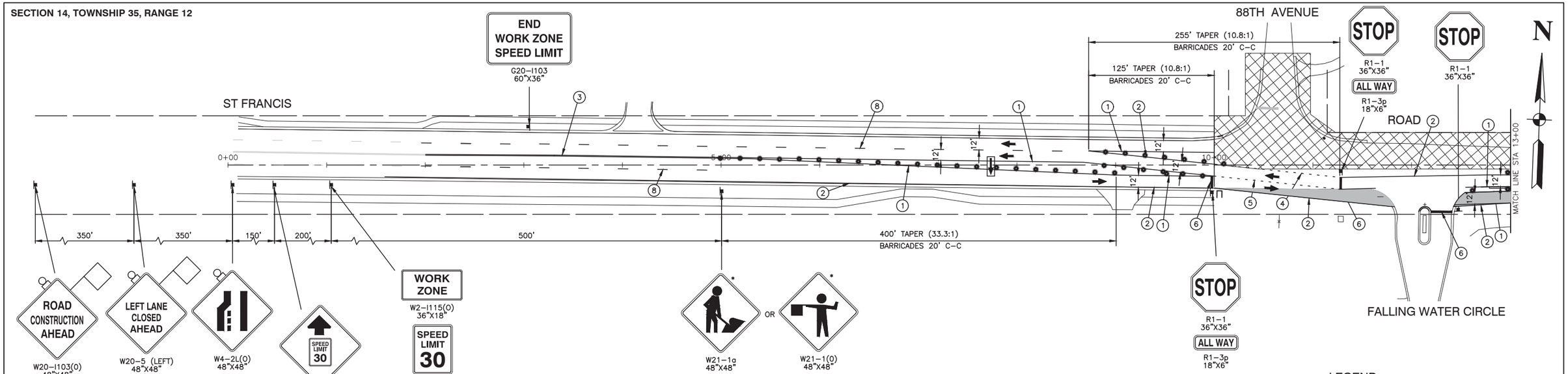
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

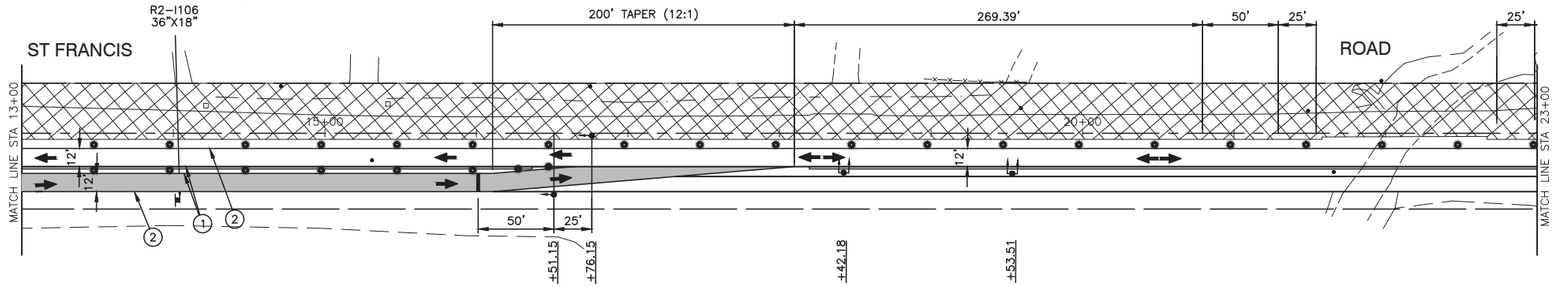
ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
SUGGESTED CONSTRUCTION STAGING - STAGE II

SCALE: 1"=50' | SHEET NO. 25 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	25
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

SECTION 14, TOWNSHIP 35, RANGE 12





LEGEND

1. 4" YELLOW EDGE LINE – TEMPORARY PAVEMENT MARKINGS
2. 4" WHITE EDGE LINE – TEMPORARY PAVEMENT MARKINGS
3. 4" DOUBLE YELLOW CENTERLINE (11" C-C) – TEMPORARY PAVEMENT MARKINGS
4. 6" WHITE SKIP DASH (2' LINE, 6' SPACE) – TEMPORARY PAVEMENT MARKINGS
5. 6" YELLOW SKIP DASH (2' LINE, 6' SPACE) – TEMPORARY PAVEMENT MARKINGS
6. 24" WHITE STOP BAR – TEMPORARY PAVEMENT MARKINGS
7. TEMPORARY SYMBOLS AND LETTERS, LARGE
8. 4" WHITE SKIP DASH (10' LINE, 30' SPACE) – TEMPORARY PAVEMENT MARKINGS
9. 6" WHITE LANE LINE – TEMPORARY PAVEMENT MARKINGS

WORK AREA

TEMPORARY PAVEMENT

VERTICAL PANEL WITH STEADY BURNING LIGHT (50' C-C)

TYPE I OR II BARRICADE OR DRUM WITH STEADY BURNING LIGHT (50' C-C UNLESS OTHERWISE NOTED)

TYPE III BARRICADE WITH STEADY BURNING LIGHT

SIGN

TYPE F BARRICADE

CRASH ATTENUATER

TRAFFIC FLOW

FLASHING ARROW BOARD

TRAFFIC SIGNAL (1 HEAD)

TRAFFIC SIGNAL (2 HEAD)

NOTES

1. BARRICADES ALONG EXISTING EDGE OF TRAFFIC MAXIMUM SPACING 50' C-C.
2. WORK TO BE DONE UNDER IDOT TEMPORARY TRAFFIC CONTROL STANDARDS WITH FLAGGERS.
3. EXISTING SIGNS TO BE MAINTAINED, E.G. STOP SIGNS.
4. MAINTAIN DRUMS ALONG CONSTRUCTION AREA TURNS.
5. SEE DETOUR PLAN FOR ADDITIONAL SIGNAGE.
6. ALL EXISTING PAVEMENT MARKINGS THAT ARE IN CONFLICT WITH THE PROPOSED STAGING PLAN SHALL BE REMOVED PRIOR TO THE IMPLEMENTATION OF THE STAGING PLAN.

FILE NAME = 10423_12-TCON-01 - TCON-P3- P01	USER NAME =	DESIGNED — GA	REVISED —
		CHECKED — HLG	REVISED —
	PLOT SCALE =	DRAWN — MED	REVISED —
	PLOT DATE = 06-09-16	CHECKED — AG	REVISED —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

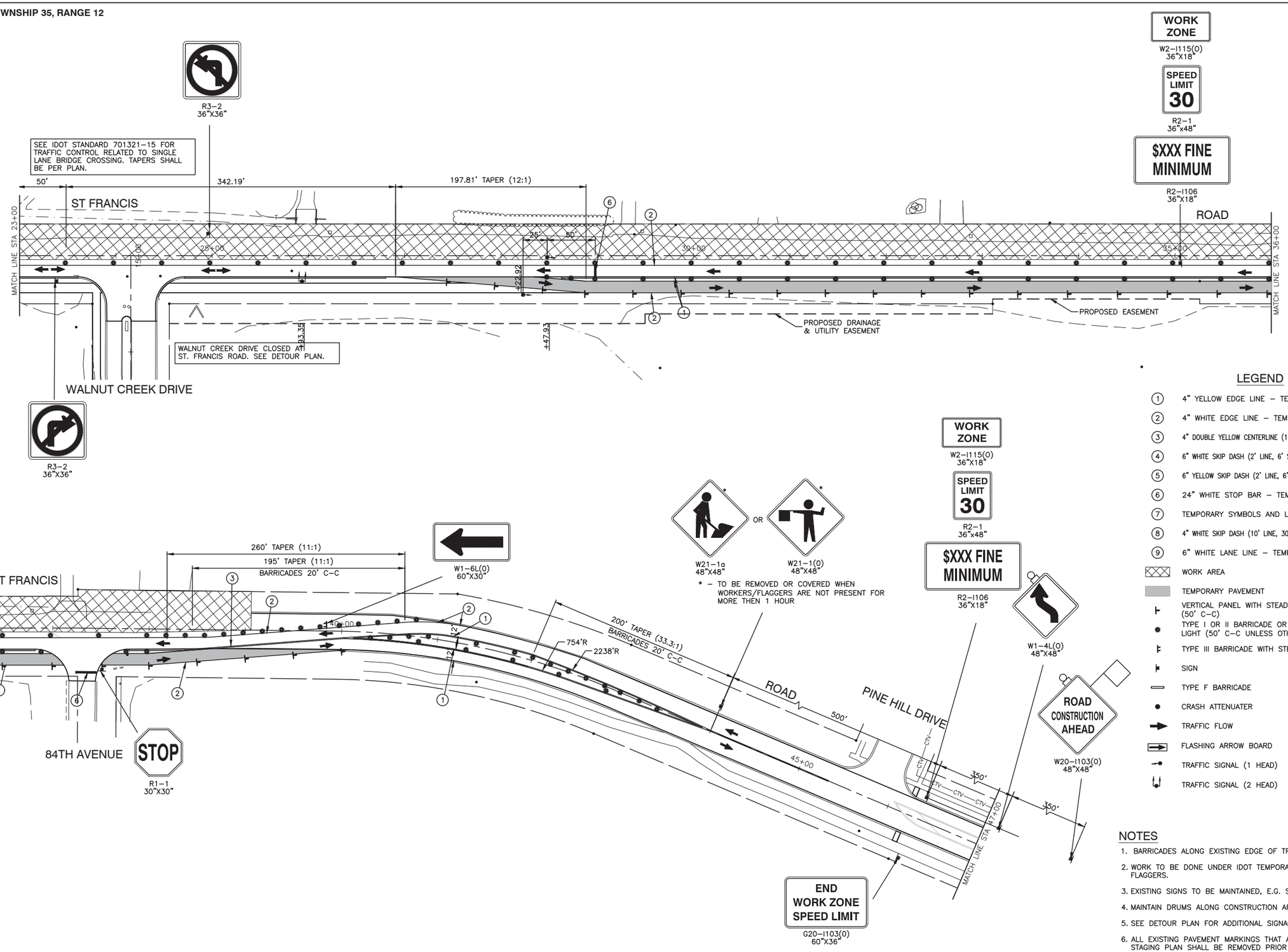
ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
SUGGESTED CONSTRUCTION STAGING - STAGE III

SCALE: 1"=50' | SHEET NO. 26 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	26
		CONTRACT NO. 61D22		

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT ----

SECTION 14, TOWNSHIP 35, RANGE 12



LEGEND

1. 4" YELLOW EDGE LINE – TEMPORARY PAVEMENT MARKINGS
2. 4" WHITE EDGE LINE – TEMPORARY PAVEMENT MARKINGS
3. 4" DOUBLE YELLOW CENTERLINE (11" C-C) – TEMPORARY PAVEMENT MARKINGS
4. 6" WHITE SKIP DASH (2' LINE, 6' SPACE) – TEMPORARY PAVEMENT MARKINGS
5. 6" YELLOW SKIP DASH (2' LINE, 6' SPACE) – TEMPORARY PAVEMENT MARKINGS
6. 24" WHITE STOP BAR – TEMPORARY PAVEMENT MARKINGS
7. TEMPORARY SYMBOLS AND LETTERS, LARGE
8. 4" WHITE SKIP DASH (10' LINE, 30' SPACE) – TEMPORARY PAVEMENT MARKINGS
9. 6" WHITE LANE LINE – TEMPORARY PAVEMENT MARKINGS

WORK AREA

TEMPORARY PAVEMENT

VERTICAL PANEL WITH STEADY BURNING LIGHT (50' C-C)

TYPE I OR II BARRICADE OR DRUM WITH STEADY BURNING LIGHT (50' C-C UNLESS NOTHERWISE NOTED)

TYPE III BARRICADE WITH STEADY BURNING LIGHT

SIGN

TYPE F BARRICADE

CRASH ATTENUATER

TRAFFIC FLOW

FLASHING ARROW BOARD

TRAFFIC SIGNAL (1 HEAD)

TRAFFIC SIGNAL (2 HEAD)

NOTES

1. BARRICADES ALONG EXISTING EDGE OF TRAFFIC MAXIMUM SPACING 50' C-C.
2. WORK TO BE DONE UNDER IDOT TEMPORARY TRAFFIC CONTROL STANDARDS WITH FLAGGERS.
3. EXISTING SIGNS TO BE MAINTAINED, E.G. STOP SIGNS.
4. MAINTAIN DRUMS ALONG CONSTRUCTION AREA TURNS.
5. SEE DETOUR PLAN FOR ADDITIONAL SIGNAGE.
6. ALL EXISTING PAVEMENT MARKINGS THAT ARE IN CONFLICT WITH THE PROPOSED STAGING PLAN SHALL BE REMOVED PRIOR TO THE IMPLEMENTATION OF THE STAGING PLAN.

FILE NAME = 10423_12-TCON-01 - TCON-P3- P02	USER NAME =	DESIGNED — GA	REVISED —
		CHECKED — HLG	REVISED —
	PLOT SCALE =	DRAWN — MED	REVISED —
	PLOT DATE = 06-09-16	CHECKED — AG	REVISED —

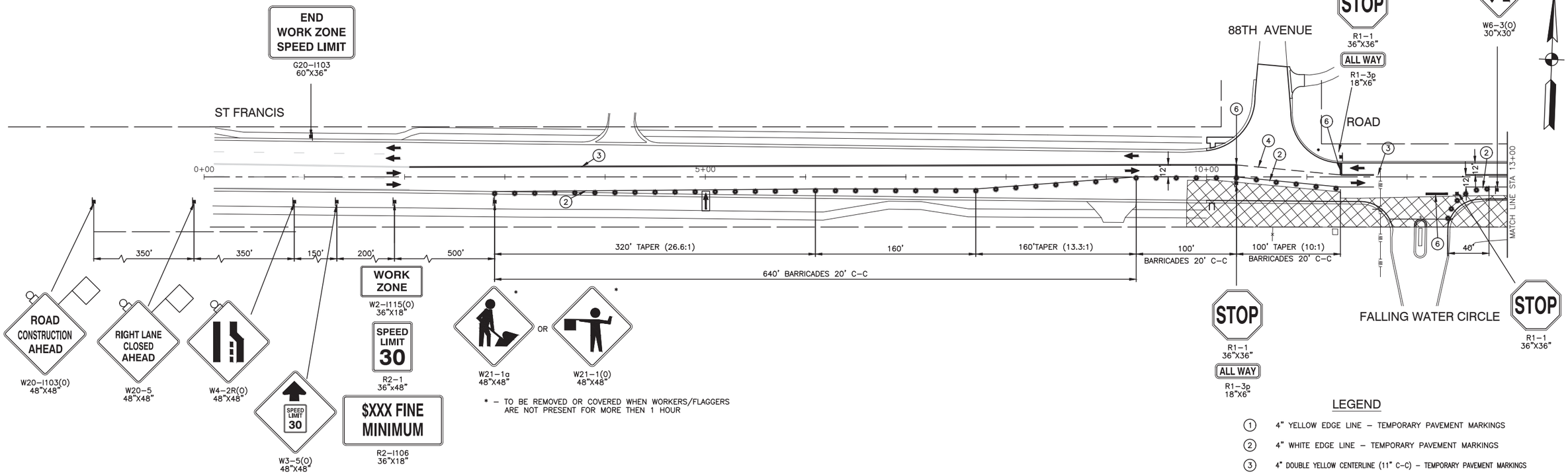
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

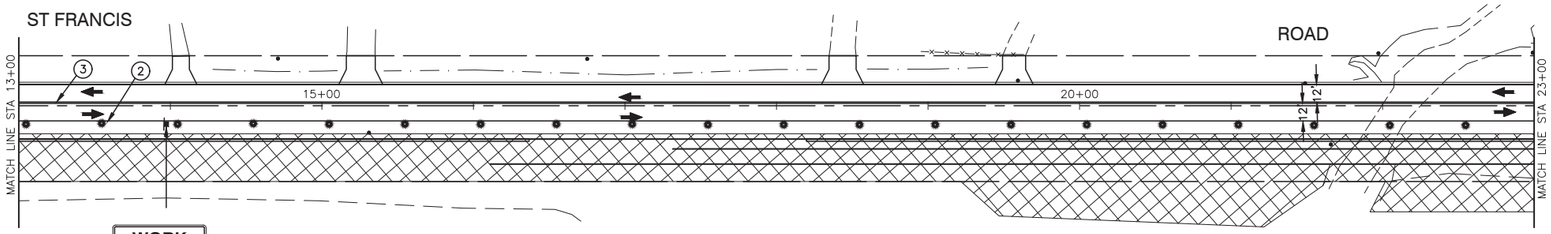
ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
SUGGESTED CONSTRUCTION STAGING - STAGE III

SCALE: 1"=50' | SHEET NO. 27 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	27
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

SECTION 14, TOWNSHIP 35, RANGE 12





LEGEND

- (1) 4" YELLOW EDGE LINE – TEMPORARY PAVEMENT MARKINGS
- (2) 4" WHITE EDGE LINE – TEMPORARY PAVEMENT MARKINGS
- (3) 4" DOUBLE YELLOW CENTERLINE (11" C-C) – TEMPORARY PAVEMENT MARKINGS
- (4) 6" WHITE SKIP DASH (2' LINE, 6' SPACE) – TEMPORARY PAVEMENT MARKINGS
- (5) 6" YELLOW SKIP DASH (2' LINE, 6' SPACE) – TEMPORARY PAVEMENT MARKINGS
- (6) 24" WHITE STOP BAR – TEMPORARY PAVEMENT MARKINGS
- (7) TEMPORARY SYMBOLS AND LETTERS, LARGE
- (8) 4" WHITE SKIP DASH (10' LINE, 30' SPACE) – TEMPORARY PAVEMENT MARKINGS
- (9) 6" WHITE LANE LINE – TEMPORARY PAVEMENT MARKINGS
- WORK AREA
- TEMPORARY PAVEMENT
- VERTICAL PANEL WITH STEADY BURNING LIGHT (50' C-C)
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURNING LIGHT (50' C-C UNLESS OTHERWISE NOTED)
- TYPE III BARRICADE WITH STEADY BURNING LIGHT
- SIGN
- TYPE F BARRICADE
- CRASH ATTENUATER
- TRAFFIC FLOW
- FLASHING ARROW BOARD

NOTES

1. BARRICADES ALONG EXISTING EDGE OF TRAFFIC MAXIMUM SPACING 50' C-C.
2. WORK TO BE DONE UNDER IDOT TEMPORARY TRAFFIC CONTROL STANDARDS WITH FLAGGERS.
3. EXISTING SIGNS TO BE MAINTAINED, E.G. STOP SIGNS.
4. MAINTAIN DRUMS ALONG CONSTRUCTION AREA TURNS.
5. SEE DETOUR PLAN FOR ADDITIONAL SIGNAGE.
6. ALL EXISTING PAVEMENT MARKINGS THAT ARE IN CONFLICT WITH THE PROPOSED STAGING PLAN SHALL BE REMOVED PRIOR TO THE IMPLEMENTATION OF THE STAGING PLAN.

FILE NAME = 10423_12-TCON-01 - TCON-P4- P01	USER NAME =	DESIGNED — GA	REVISED —
		CHECKED — HLG	REVISED —
	PLOT SCALE =	DRAWN — MED	REVISED —
	PLOT DATE = 06-09-16	CHECKED — AG	REVISED —

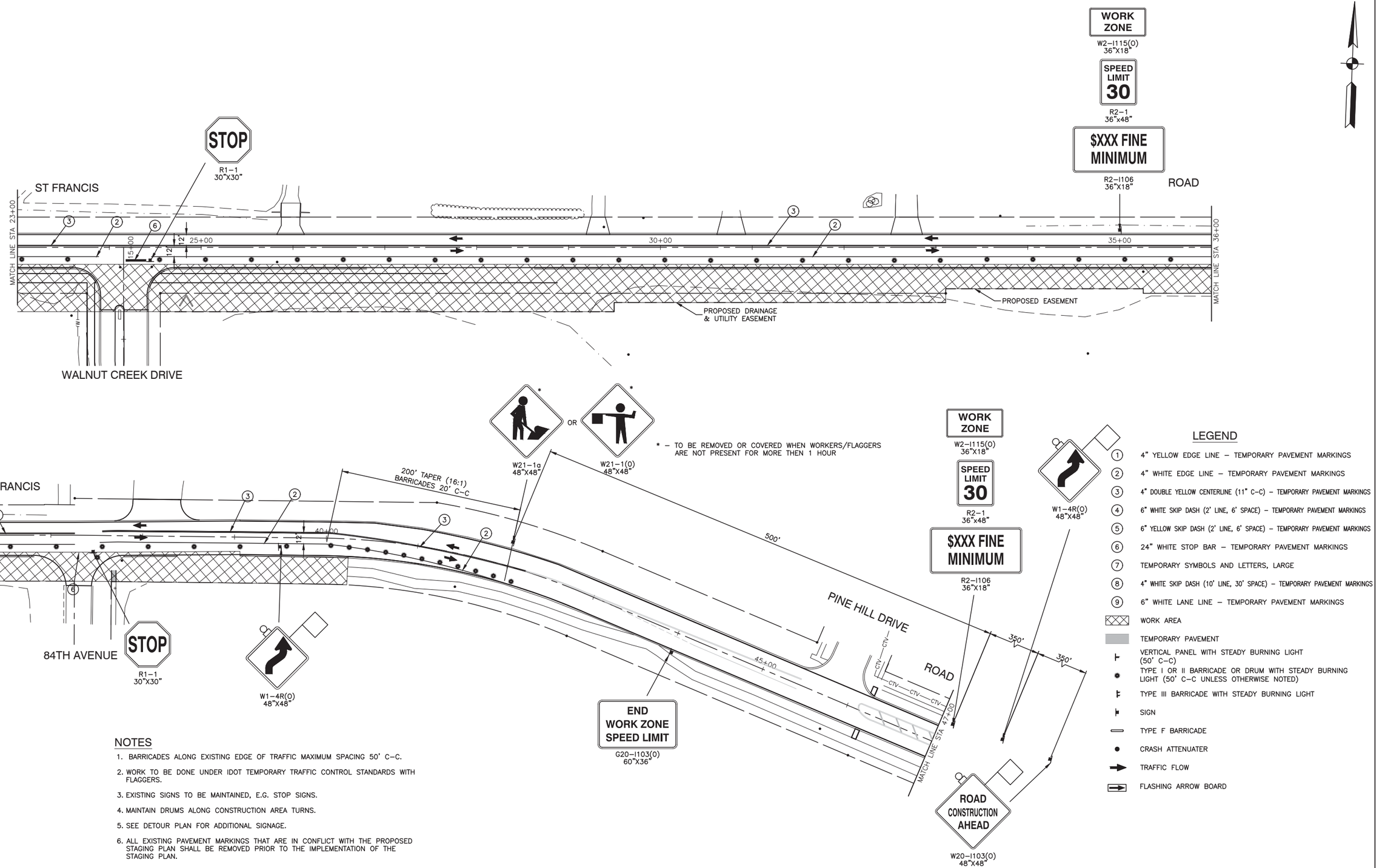
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
SUGGESTED CONSTRUCTION STAGING - STAGE IV

SCALE: 1"=50' | SHEET NO. 28 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	28
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

SECTION 14, TOWNSHIP 35, RANGE 12



NOTES

1. BARRICADES ALONG EXISTING EDGE OF TRAFFIC MAXIMUM SPACING 50' C-C.
2. WORK TO BE DONE UNDER IDOT TEMPORARY TRAFFIC CONTROL STANDARDS WITH FLAGGERS.
3. EXISTING SIGNS TO BE MAINTAINED, E.G. STOP SIGNS.
4. MAINTAIN DRUMS ALONG CONSTRUCTION AREA TURNS.
5. SEE DETOUR PLAN FOR ADDITIONAL SIGNAGE.
6. ALL EXISTING PAVEMENT MARKINGS THAT ARE IN CONFLICT WITH THE PROPOSED STAGING PLAN SHALL BE REMOVED PRIOR TO THE IMPLEMENTATION OF THE STAGING PLAN.

LEGEND

1. 4" YELLOW EDGE LINE - TEMPORARY PAVEMENT MARKINGS
2. 4" WHITE EDGE LINE - TEMPORARY PAVEMENT MARKINGS
3. 4" DOUBLE YELLOW CENTERLINE (11" C-C) - TEMPORARY PAVEMENT MARKINGS
4. 6" WHITE SKIP DASH (2' LINE, 6' SPACE) - TEMPORARY PAVEMENT MARKINGS
5. 6" YELLOW SKIP DASH (2' LINE, 6' SPACE) - TEMPORARY PAVEMENT MARKINGS
6. 24" WHITE STOP BAR - TEMPORARY PAVEMENT MARKINGS
7. TEMPORARY SYMBOLS AND LETTERS, LARGE
8. 4" WHITE SKIP DASH (10' LINE, 30' SPACE) - TEMPORARY PAVEMENT MARKINGS
9. 6" WHITE LANE LINE - TEMPORARY PAVEMENT MARKINGS

- WORK AREA
- TEMPORARY PAVEMENT
- VERTICAL PANEL WITH STEADY BURNING LIGHT (50' C-C)
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURNING LIGHT (50' C-C UNLESS OTHERWISE NOTED)
- TYPE III BARRICADE WITH STEADY BURNING LIGHT
- SIGN
- TYPE F BARRICADE
- CRASH ATTENUATER
- TRAFFIC FLOW
- FLASHING ARROW BOARD

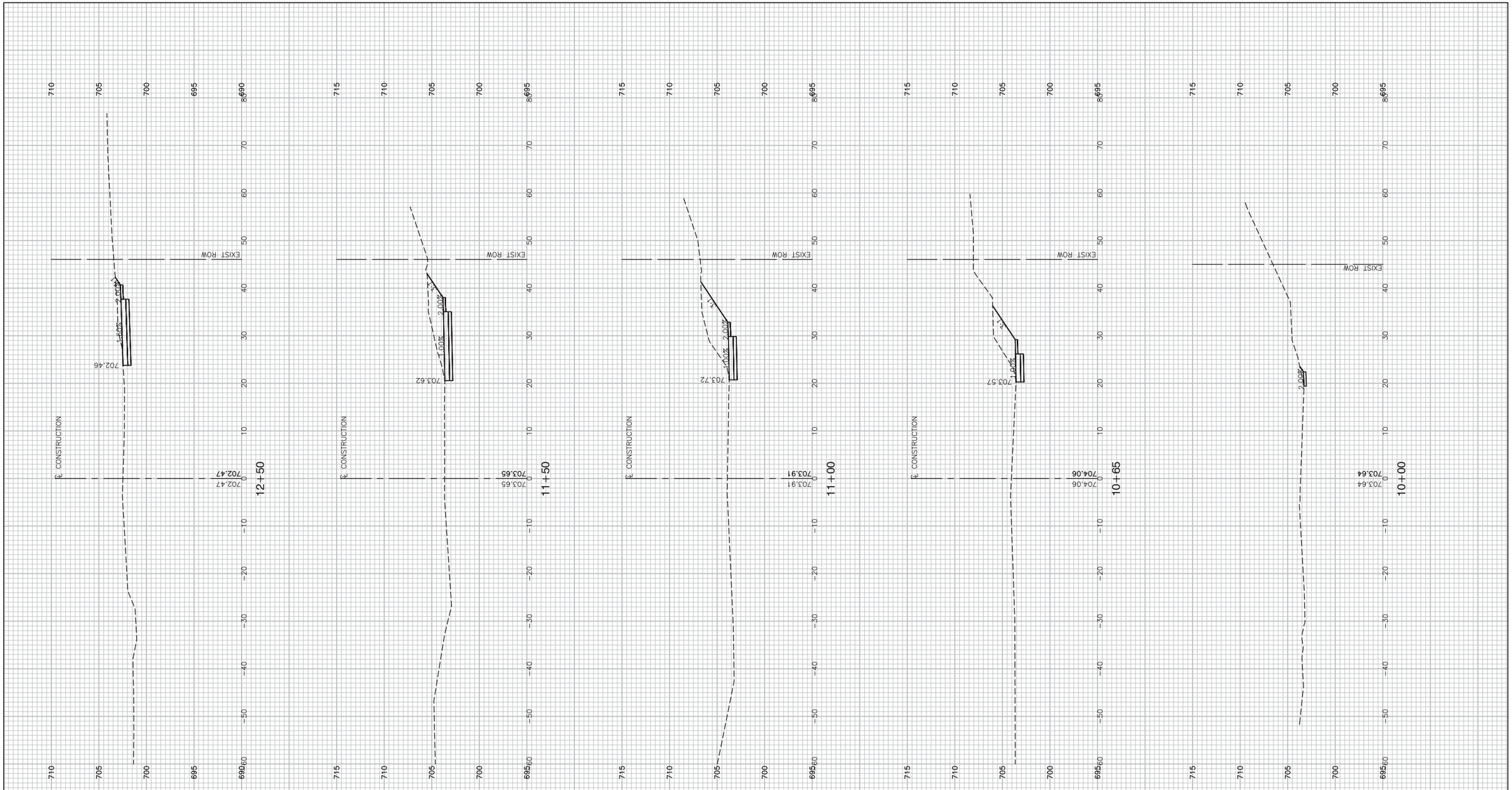
FILE NAME = 10423_12-TCON-01 - TCON-P4- P02	USER NAME =	DESIGNED - GA	REVISED -
		CHECKED - HLG	REVISED -
	PLOT SCALE =	DRAWN - MED	REVISED -
	PLOT DATE = 06-09-16	CHECKED - AG	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
SUGGESTED CONSTRUCTION STAGING - STAGE IV

SCALE: 1"=50' | SHEET NO. 29 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	29
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



FILE NAME = 10423_12-XSCT-02 - IDOT_X(1)	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION SUGGESTED CONSTRUCTION STAGING CROSS SECTIONS
		CHECKED — HLG	REVISED —		
	PLOT SCALE =	DRAWN — MED	REVISED —		
	PLOT DATE = 06-09-16	CHECKED — AG	REVISED —		SCALE: H 1"=10' V 1"=5' | SHEET NO. 30 OF 109 SHEETS | STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	30
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

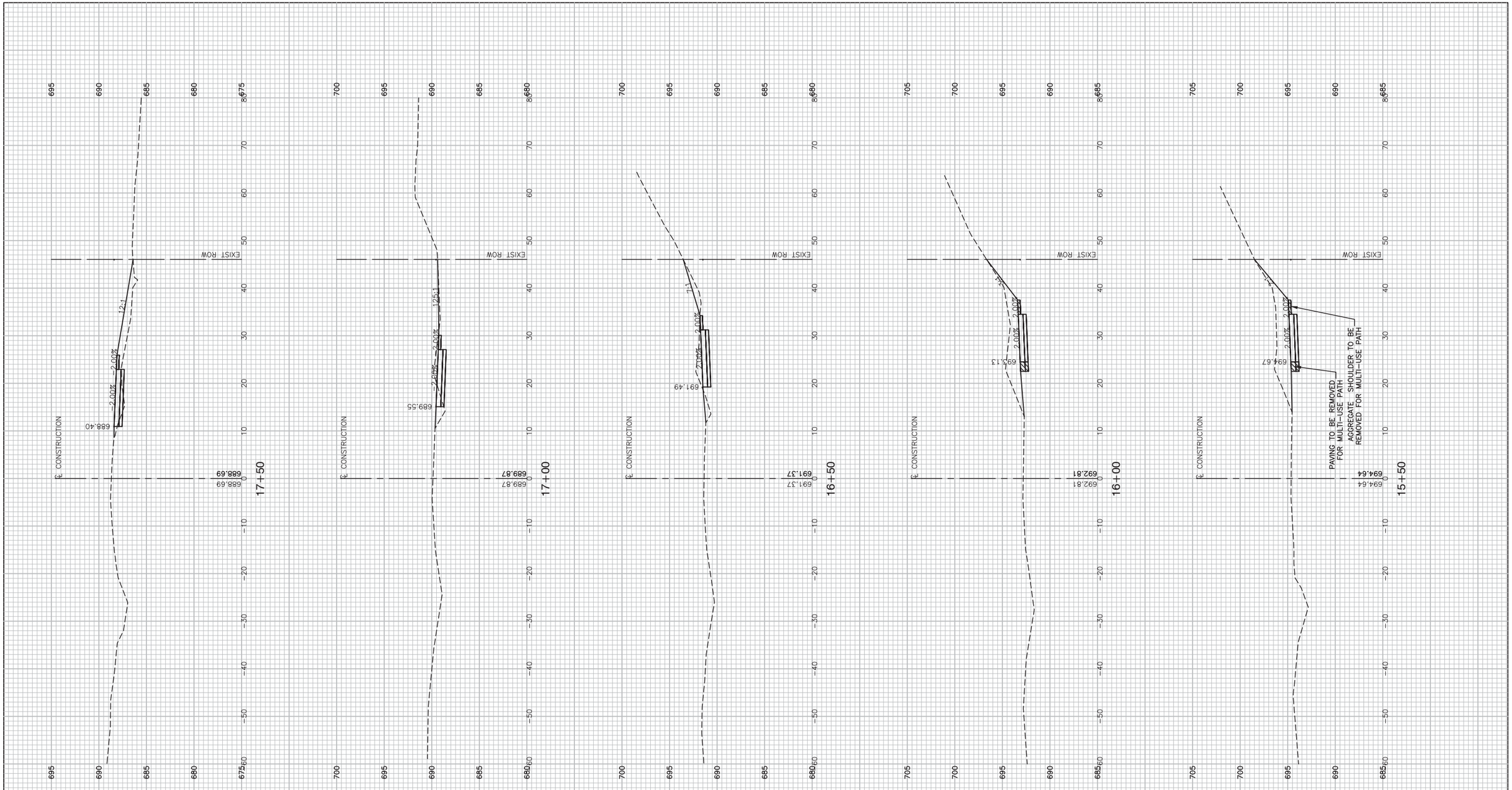
FILE NAME = 10423_12-XSCT-02 - IDOT_X(2)	USER NAME =	DESIGNED — GA	REVISED —
		CHECKED — HLG	REVISED —
	PLOT SCALE =	DRAWN — MED	REVISED —
	PLOT DATE = 06-09-16	CHECKED — AG	REVISED —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
SUGGESTED CONSTRUCTION STAGING CROSS SECTIONS

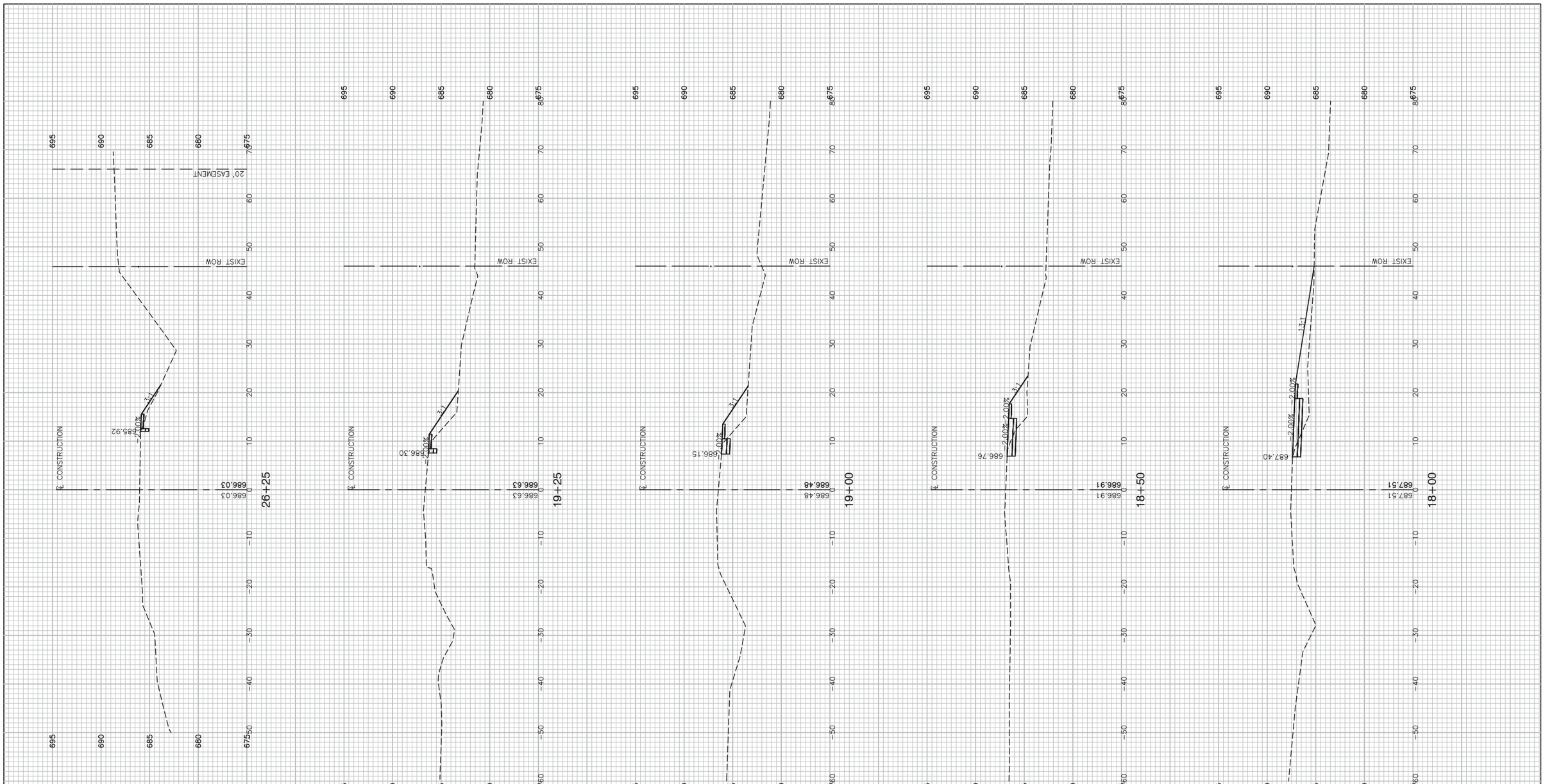
SCALE: H 1"=10' V 1"=5' | SHEET NO. 31 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	31
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



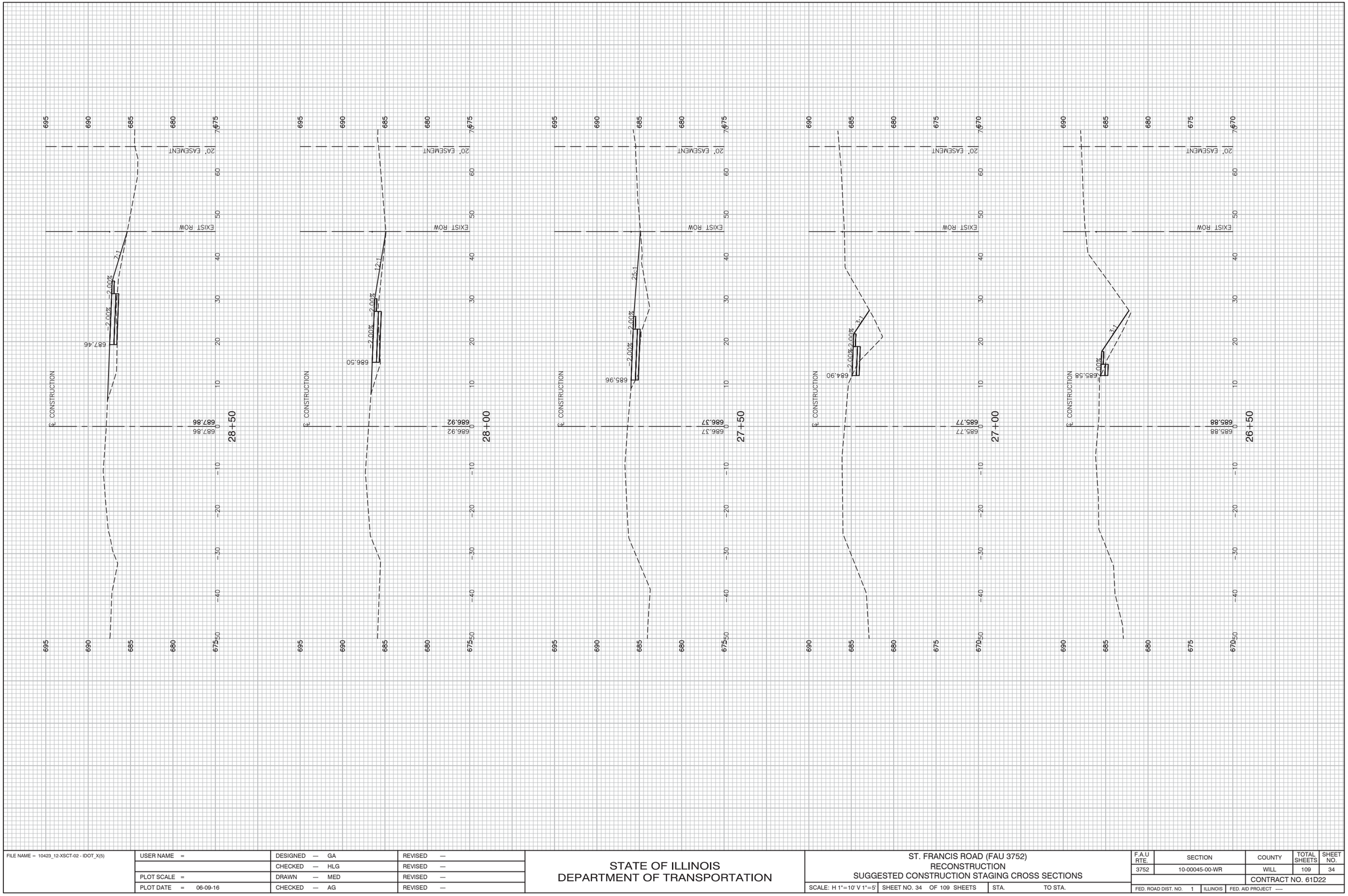
FILE NAME = 10423_12-XSCT-02 - IDOT_X(3)	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION SUGGESTED CONSTRUCTION STAGING CROSS SECTIONS
		CHECKED — HLG	REVISED —		
	PLOT SCALE =	DRAWN — MED	REVISED —		
	PLOT DATE = 06-09-16	CHECKED — AG	REVISED —		SCALE: H 1"=10' V 1"=5' | SHEET NO. 32 OF 109 SHEETS | STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	32
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



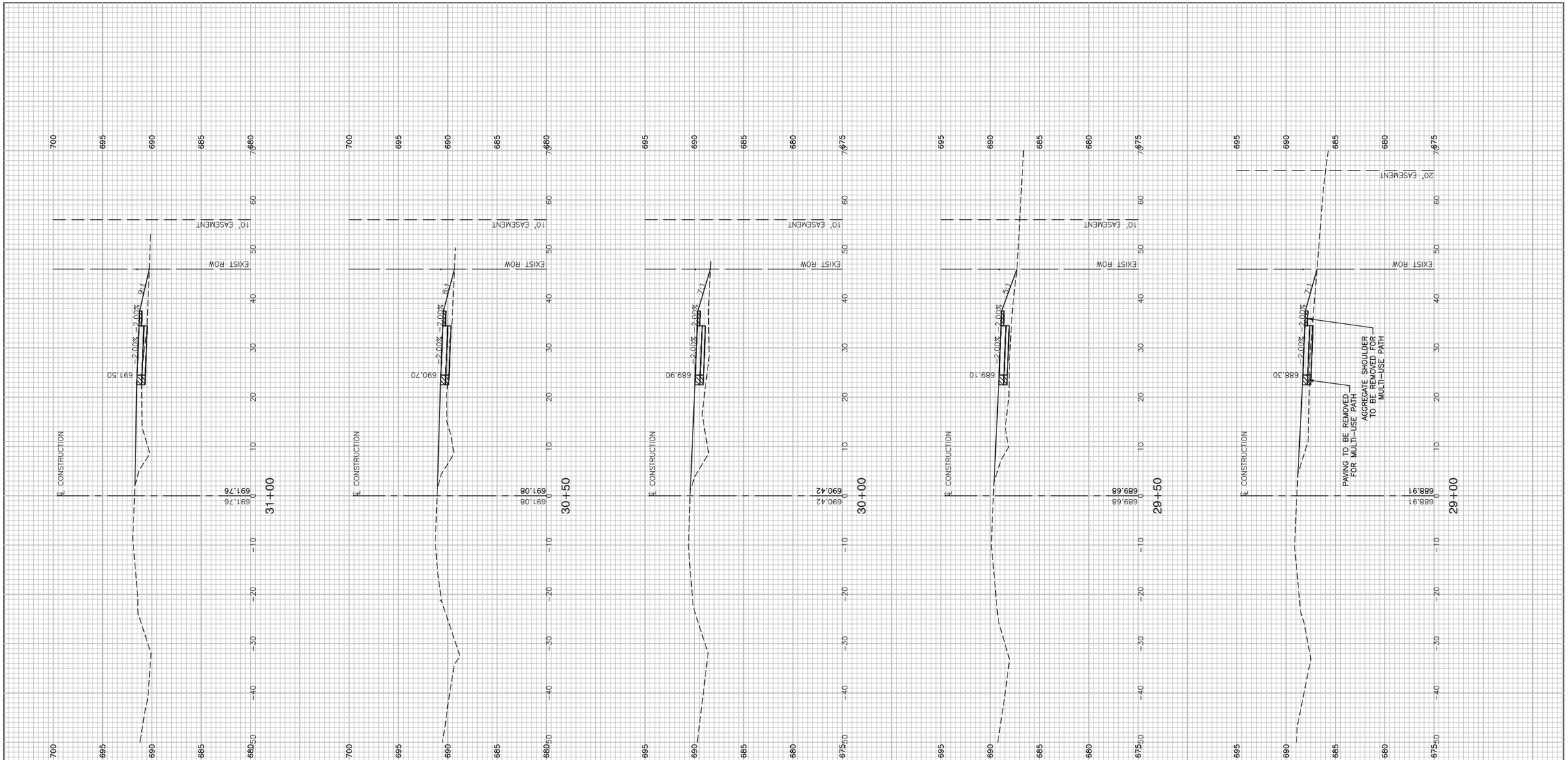
FILE NAME = 10423_12-XSCT-02 - IDOT_X(4)	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION SUGGESTED CONSTRUCTION STAGING CROSS SECTIONS
		CHECKED — HLG	REVISED —		
	PLOT SCALE =	DRAWN — MED	REVISED —		
	PLOT DATE = 06-09-16	CHECKED — AG	REVISED —		SCALE: H 1"=10' V 1"=5' | SHEET NO. 33 OF 109 SHEETS | STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	33
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



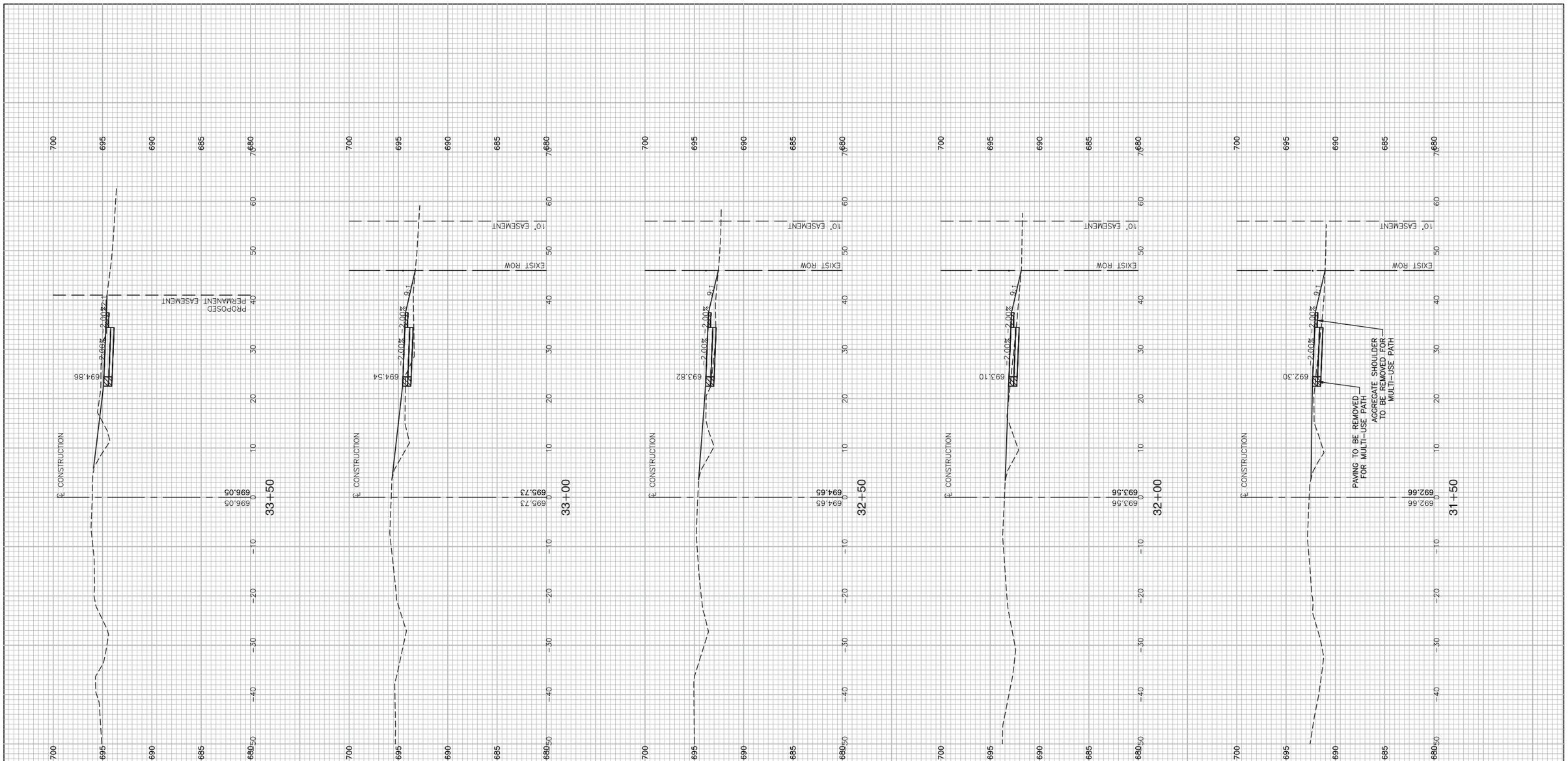
FILE NAME = 10423_12-XSCT-02 - IDOT_X(5)	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION SUGGESTED CONSTRUCTION STAGING CROSS SECTIONS
		CHECKED — HLG	REVISED —		
	PLOT SCALE =	DRAWN — MED	REVISED —		
	PLOT DATE = 06-09-16	CHECKED — AG	REVISED —		SCALE: H 1"=10' V 1"=5' | SHEET NO. 34 OF 109 SHEETS | STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	34
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



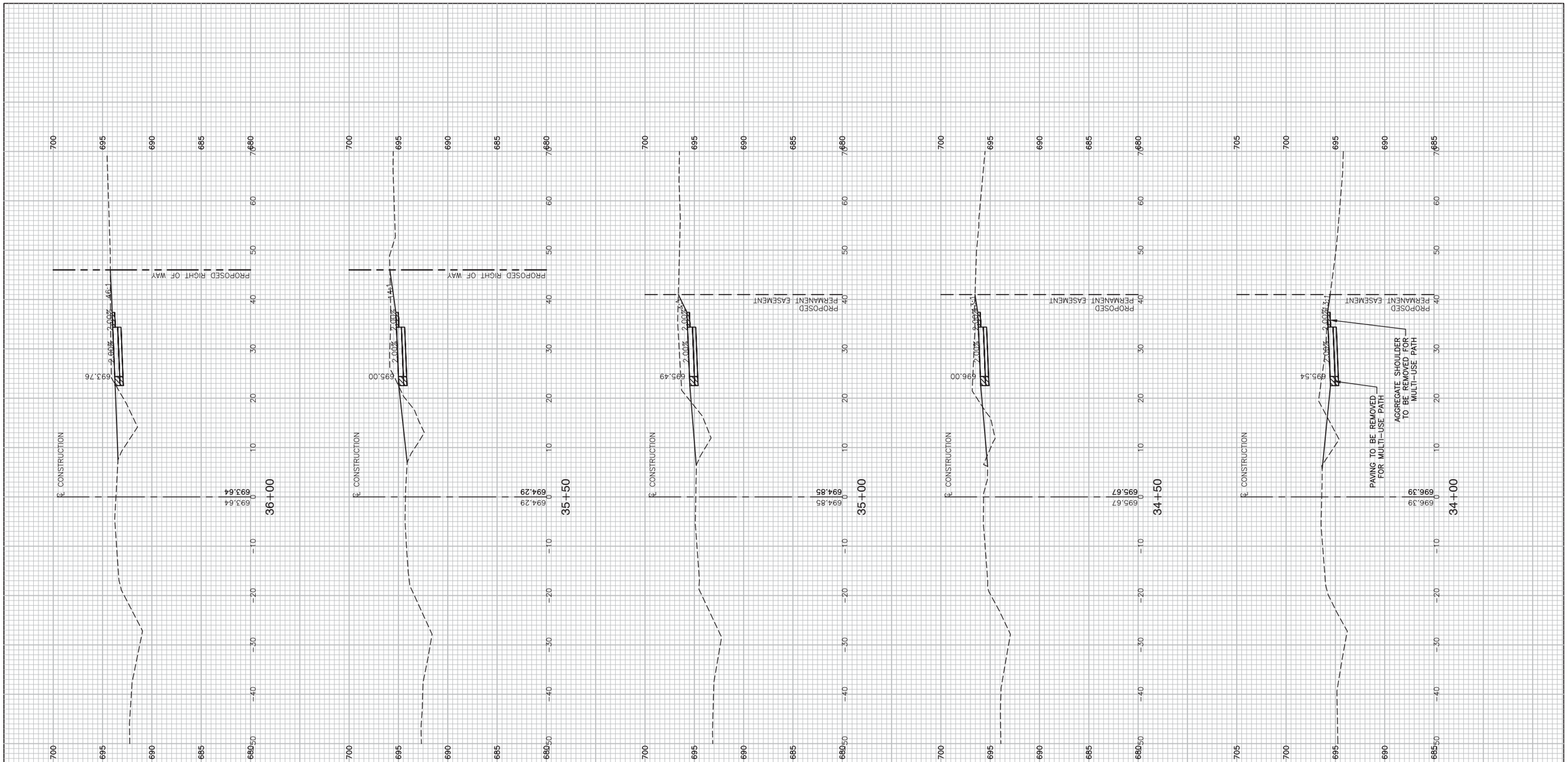
FILE NAME = 10423_12-XSCT-02 - IDOT_X(6)	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION SUGGESTED CONSTRUCTION STAGING CROSS SECTIONS
		CHECKED — HLG	REVISED —		
	PLOT SCALE =	DRAWN — MED	REVISED —		
	PLOT DATE = 06-09-16	CHECKED — AG	REVISED —		SCALE: H 1"=10' V 1"=5' | SHEET NO. 35 OF 109 SHEETS | STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	35
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



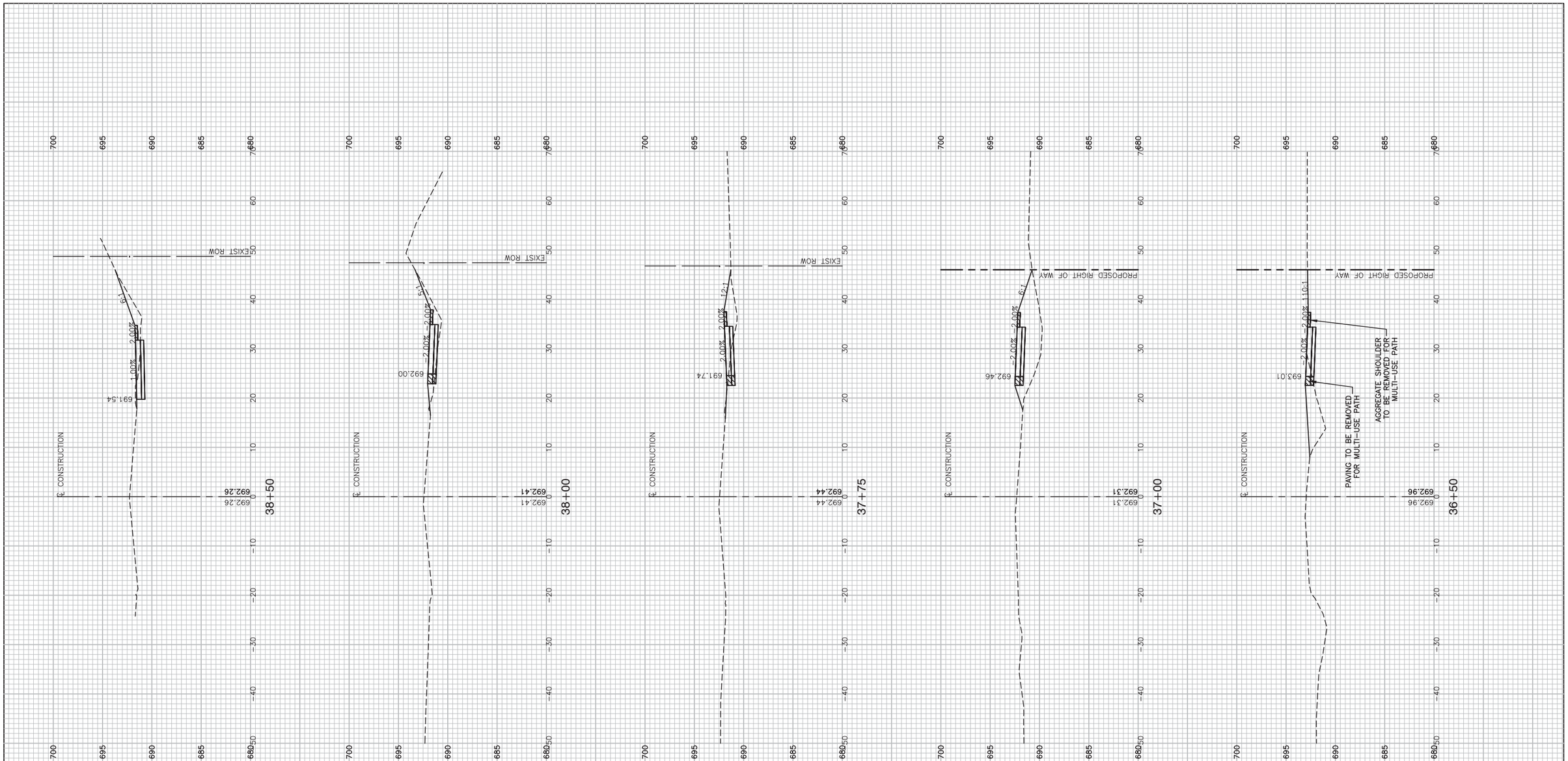
FILE NAME = 10423_12-XSCT-02 - IDOT_X(7)	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION SUGGESTED CONSTRUCTION STAGING CROSS SECTIONS
		CHECKED — HLG	REVISED —		
	PLOT SCALE =	DRAWN — MED	REVISED —		
	PLOT DATE = 06-09-16	CHECKED — AG	REVISED —		SCALE: H 1"=10' V 1"=5' | SHEET NO. 36 OF 109 SHEETS | STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	36
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



FILE NAME = 10423_12-XSCT-02 - IDOT_X(8)	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION SUGGESTED CONSTRUCTION STAGING CROSS SECTIONS
		CHECKED — HLG	REVISED —		
	PLOT SCALE =	DRAWN — MED	REVISED —		
	PLOT DATE = 06-09-16	CHECKED — AG	REVISED —		SCALE: H 1"=10' V 1"=5' | SHEET NO. 37 OF 109 SHEETS | STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	37
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



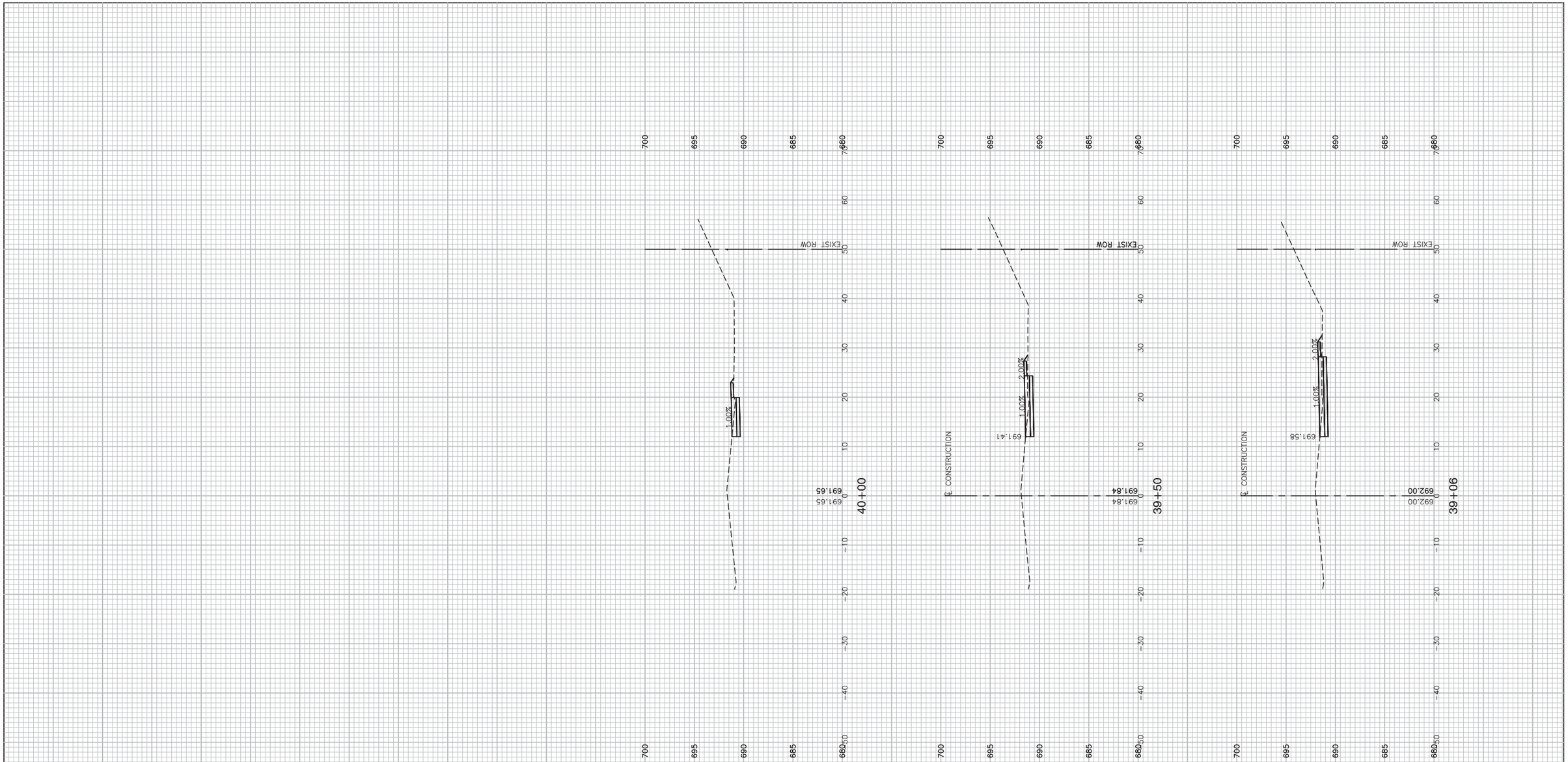
FILE NAME = 10423_12-XSCT-02 - IDOT_X(9)	USER NAME =	DESIGNED — GA	REVISED —
		CHECKED — HLG	REVISED —
	PLOT SCALE =	DRAWN — MED	REVISED —
	PLOT DATE = 06-09-16	CHECKED — AG	REVISED —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
SUGGESTED CONSTRUCTION STAGING CROSS SECTIONS

SCALE: H 1"=10' V 1"=5' | SHEET NO. 38 OF 109 SHEETS | STA. | TO STA.

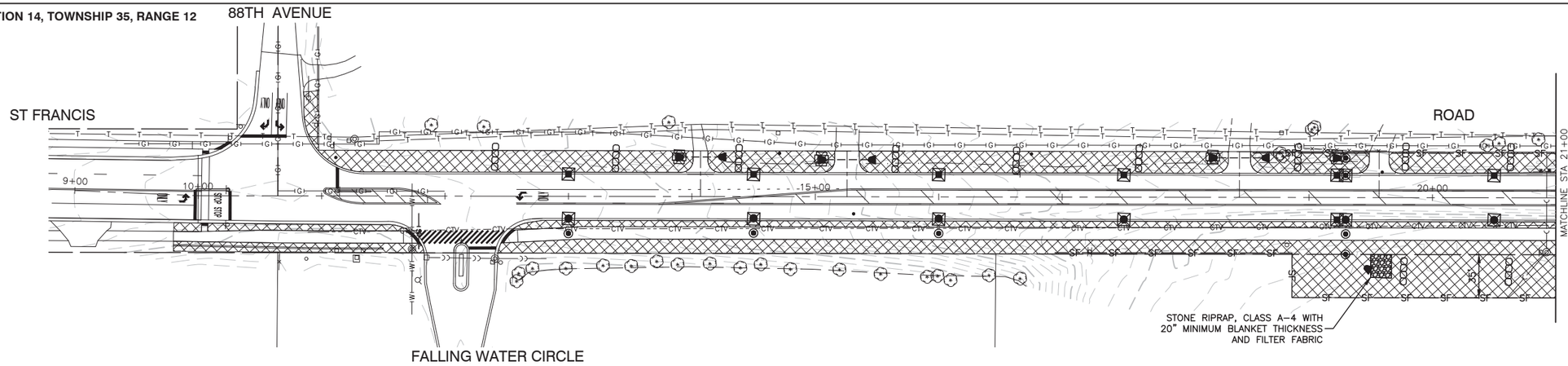
F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	38
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

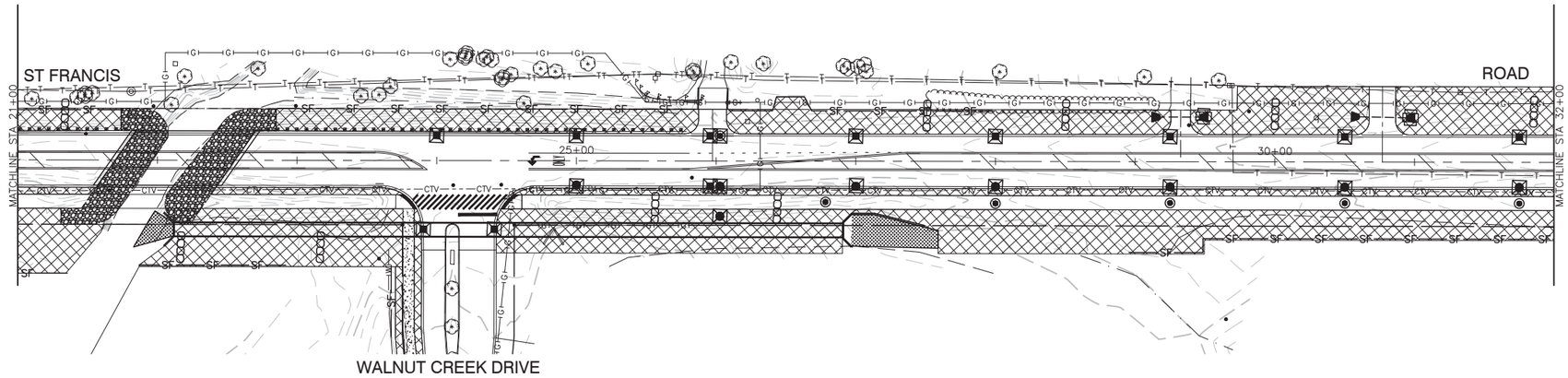


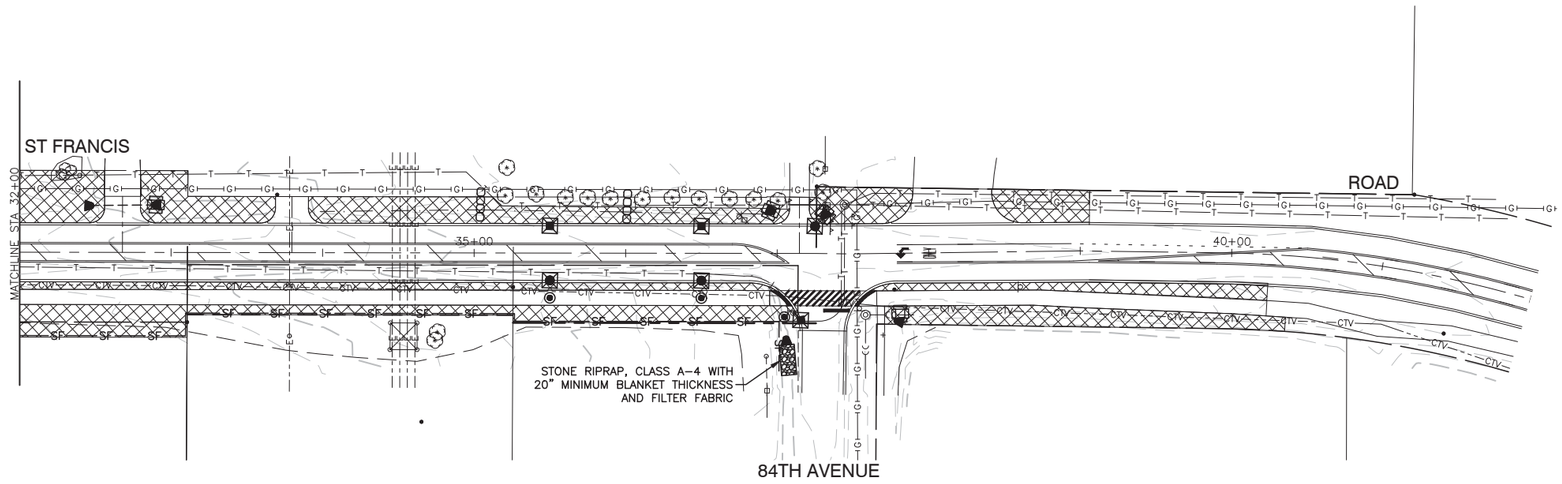
FILE NAME = 10423_12-XSCT-02 - IDOT_X(10)	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION SUGGESTED CONSTRUCTION STAGING CROSS SECTIONS
		CHECKED — HLG	REVISED —		
	PLOT SCALE =	DRAWN — MED	REVISED —		
	PLOT DATE = 06-09-16	CHECKED — AG	REVISED —		SCALE: H 1"=10' V 1"=5' | SHEET NO. 39 OF 109 SHEETS | STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	39
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

SECTION 14, TOWNSHIP 35, RANGE 12







N

LEGEND

- INLET FILTER
- INLET AND PIPE PROTECTION
- RIP RAP
- SF — PERIMETER EROSION BARRIER
- TEMPORARY DITCH CHECKS (EXCELSIOR LOG)
- SODDING, SALT TOLERANT TOPSOIL, FURNISH AND PLACE 4"
- TURF REINFORCEMENT MAT WITH SEEDING, CLASS 4

NOTES:

1. EXISTING AND FUTURE DRAINAGE PATTERNS ARE THE SAME.
2. TOTAL AREA OF SOIL DISTURBANCE IS APPROXIMATELY 7.1 ACRES.
3. ALL SILT FENCE SHALL BE PLACED 1' INSIDE ROW UNLESS OTHERWISE SPECIFIED.

FILE NAME = 10423_12-LNSC-01 - IDOT P01	USER NAME =	DESIGNED — GA	REVISED —
		CHECKED — HLG	REVISED —
	PLOT SCALE =	DRAWN — MED	REVISED —
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

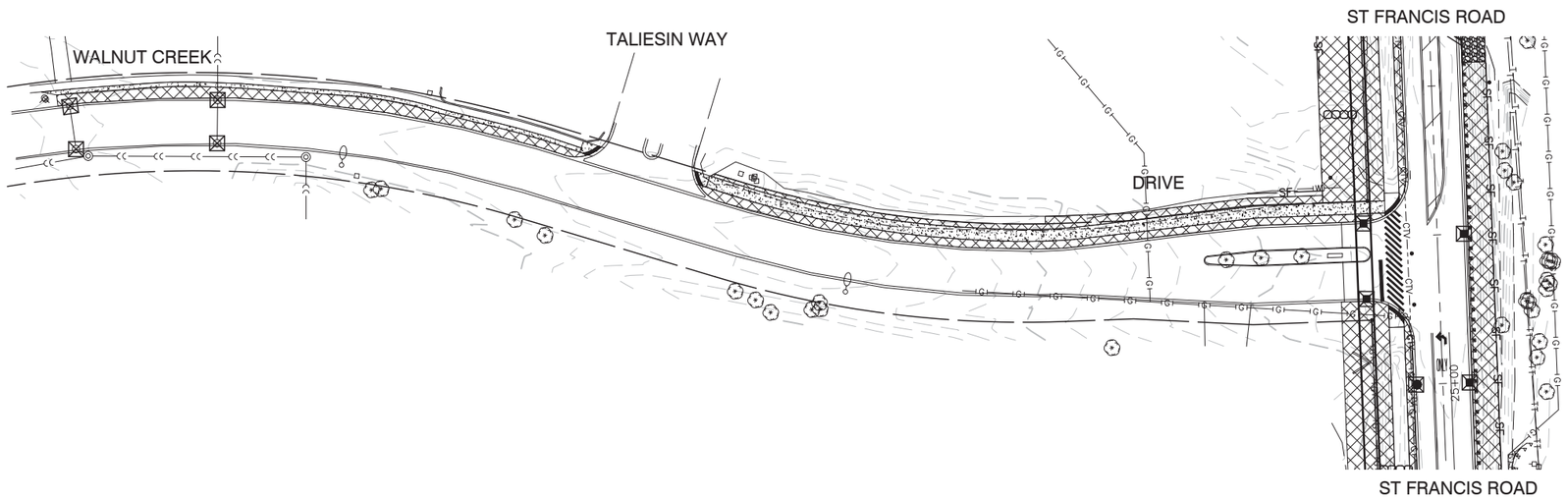
ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
EROSION CONTROL (SWPPP)

SCALE: 1"=50' | SHEET NO. 40 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	40
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT CMM-M-4003(424)		

SECTION 14, TOWNSHIP 35, RANGE 12

N



EROSION CONTROL NOTES:

WITHIN 24 HOURS FROM THE TIME SEEDING HAS BEEN PERFORMED, THE SEED AREA SHALL BE GIVEN A COVERING OF MULCH. THE MULCH SHALL CONSIST OF HAND OR MACHINE APPLICATION OF STRAW, MULCH AT RATE OF 2 TON PER ACRE. MULCH SHALL EITHER BE ANCHORED WITH A MECHANICAL STABILIZER OR PARTIALLY COATED WITH EMULSIFIED ASPHALT. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT.

ALL EROSION CONTROL PRACTICES SHALL BE INSTALLED PRIOR TO STARTING EACH PHASE OF CONSTRUCTION.

ANY OBSERVED DISRUPTION TO THE EROSION CONTROL PRACTICES SHALL BE IMMEDIATELY REPAIRED BY THE CONTRACTOR.

ANY EXISTING SUBSURFACE DRAINAGE SYSTEM OR FIELD TILES THAT ARE DISTURBED DURING CONSTRUCTION SHALL BE RESTORED.

ANY DUST OR MUD TRACKED ONTO STREETS SHALL BE CLEANED AT THE END OF EACH WORKING DAY.

ALL SOIL EROSION, AND SEDIMENTATION CONTROL PRACTICES SHALL BE IN ACCORDANCE WITH THE ILLINOIS URBAN MANUAL.

ALL SOIL SHALL BE STABILIZED WITHIN 7 DAYS OF SOIL DISTURBANCE.

STOCK PILES SHALL BE STABILIZED WITHIN 7 DAYS OF SOIL DISTURBANCE BY MEANS OF TEMPORARY SEEDING.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSPECTION AND MAINTENANCE OF EROSION CONTROL PRACTICES. REQUIRED INSPECTION REPORTS SHALL BE SUBMITTED TO THE WILL/SOUTH COOK SOIL AND WATER CONSERVATION DISTRICT AT 1201 GOUGAR ROAD, NEW LENOX 60451.

ALL EROSION CONTROL PRACTICES SHALL BE INSPECTED WEEKLY AND AFTER ANY RAINFALL GREATER THAN 0.5".

THE SEEDING DATES FOR TEMPORARY SEEDING SHALL BE EARLY SPRING TO SEPTEMBER 30. THE SEEDING DATES FOR BARE EARTH SEEDING AND INTER SEEDING CLASS 4 AND CLASS 4B SHALL BE FROM MAY 15 TO JUNE 30 AND FROM OCTOBER 15 TO DECEMBER 1.

TEMPORARY SEEDING SHALL BE FERTILIZED WITH 500 LBS/ACRE OF 10-10-10 FERTILIZER OR EQUIVALENT. (TO BE USED AT THE DISCRETION OF VILLAGE OF PEOTONE AND/OR VILLAGE ENGINEER).

STORM WATER POLLUTION PREVENTION NOTES:

1. THE CONTRACTOR IS RESPONSIBLE FOR HAVING THE SWPPP ON SITE AT ALL TIMES.
2. THE CONTRACTOR SHALL TAKE THE NECESSARY STEPS TO CONTROL WASTE SUCH AS DISCARDED BUILDING MATERIALS, CONCRETE TRUCK WASHOUT, CHEMICALS, LITTER AND SANITARY WASTE AT THE CONSTRUCTION SITE THAT MAY CAUSE ADVERSE IMPACTS TO WATER QUALITY.

TEMPORARY SEEDING

Seeds	lbs/acre-Pure Live Seed
Oats	90
or Cereal Rye	90
or Wheat	90
or Perennial Ryegrass	25

SOIL PROTECTION CHART

STABILIZATION TYPE	JAN	FEB	MAR	APR	MAY	JUN	JUL	AUG	SEP	OCT	NOV	DEC
PERMANENT SEEDING			A →			*	*		→			
DORMANT SEEDING	B →										B →	
TEMPORARY SEEDING			C →			*	D*		→			
SODDING			E ** →						→			
MULCHING	F →											→

A. KENTUCKY BLUEGRASS 90 LBS/AC MIXED WITH PERENNIAL RYEGRASS 30 LBS/AC

B. KENTUCKY BLUEGRASS 135 LBS/AC MIXED WITH PERENNIAL RYEGRASS 45 LBS/AC + 2 TONS STRAW MULCH/AC

C. SPRING OATS 100 LBS/AC

D. WHEAT OR CEREAL RYE 150 LBS/AC

E. SOD (NURSERY GROWN KENTUCKY BLUEGRASS)

F. STRAW MULCH 2 TONS/AC

* IRRIGATION NEEDED DURING JUNE AND JULY
** IRRIGATION NEEDED FOR 2 TO 3 WEEKS AFTER APPLYING SOD

LEGEND

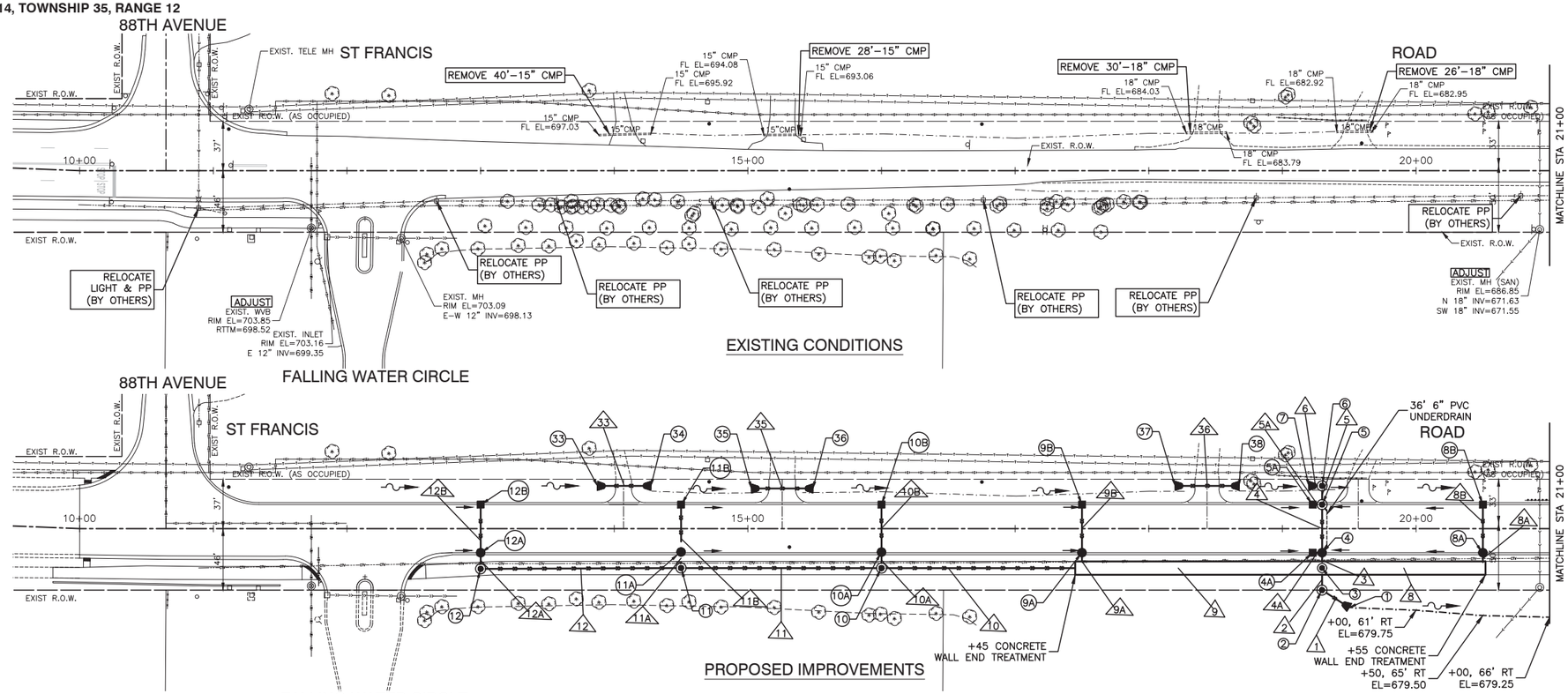
- INLET FILTER
- INLET AND PIPE PROTECTION
- RIP RAP
- SF — PERIMETER EROSION BARRIER
- TEMPORARY DITCH CHECKS (EXCELSIOR LOG)
- SODDING, SALT TOLERANT TOPSOIL, FURNISH AND PLACE 4"
- TURF REINFORCEMENT MAT WITH SEEDING, CLASS 4

NOTES:

1. EXISTING AND FUTURE DRAINAGE PATTERNS ARE THE SAME.
2. TOTAL AREA OF SOIL DISTURBANCE IS APPROXIMATELY 7.1 ACRES.
3. ALL SILT FENCE SHALL BE PLACED 1' INSIDE ROW UNLESS OTHERWISE SPECIFIED.

FILE NAME = 10423_12-LNSC-01 - IDOT P02	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	WALNUT CREEK DRIVE MULTI-USE PATH EROSION CONTROL (SWPPP)			F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		CHECKED — HLG	REVISED —					3752	10-00045-00-WR	WILL	109	41
	PLOT SCALE =	DRAWN — MED	REVISED —							CONTRACT NO. 61D22		
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —		SCALE: 1"=50'	SHEET NO. 41 OF 109 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT CMM-M-4003(424)		

SECTION 14, TOWNSHIP 35, RANGE 12



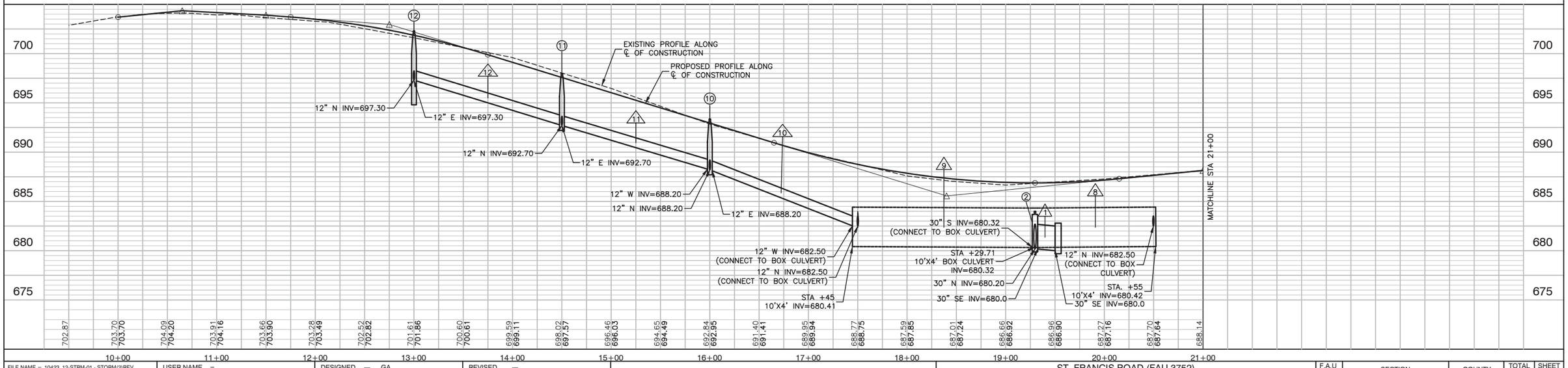
N

NOTES:

1. A PRECAST BULKHEAD/WALL SHALL BE PROVIDED AT THE WEST END (STA. 17+45) AND EAST END (STA. 20+55) OF THE 10'X4' BOX CULVERT. A TYPE 1 FRAME WITH CLOSED LID SHALL ALSO BE PROVIDED AT STATION 19+29.71 FOR ACCESS. THE COST OF THE PRECAST BULKHEAD/WALL AND TYPE 1 FRAME WITH CLOSED LID SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE PRECAST CONCRETE BOX CULVERT 10'X4' (SPECIAL).
2. ALL OFFSETS AND ELEVATIONS TO CURB LINE STRUCTURES ARE GIVEN TO THE EDGE OF PAVEMENT. OFFSETS AND ELEVATIONS FOR FLARED END SECTIONS ARE GIVEN AT THE FLARED END. ALL OTHER OFFSETS AND ELEVATIONS ARE GIVEN TO THE CENTER OF THE STRUCTURE.

PLAN — SURVEYED, PLOTTED, ALIGNMENT CHECKED, RT. OF WAY CHECKED, CADD FILE NAME, NOTE BOOK, NO., BY, DATE

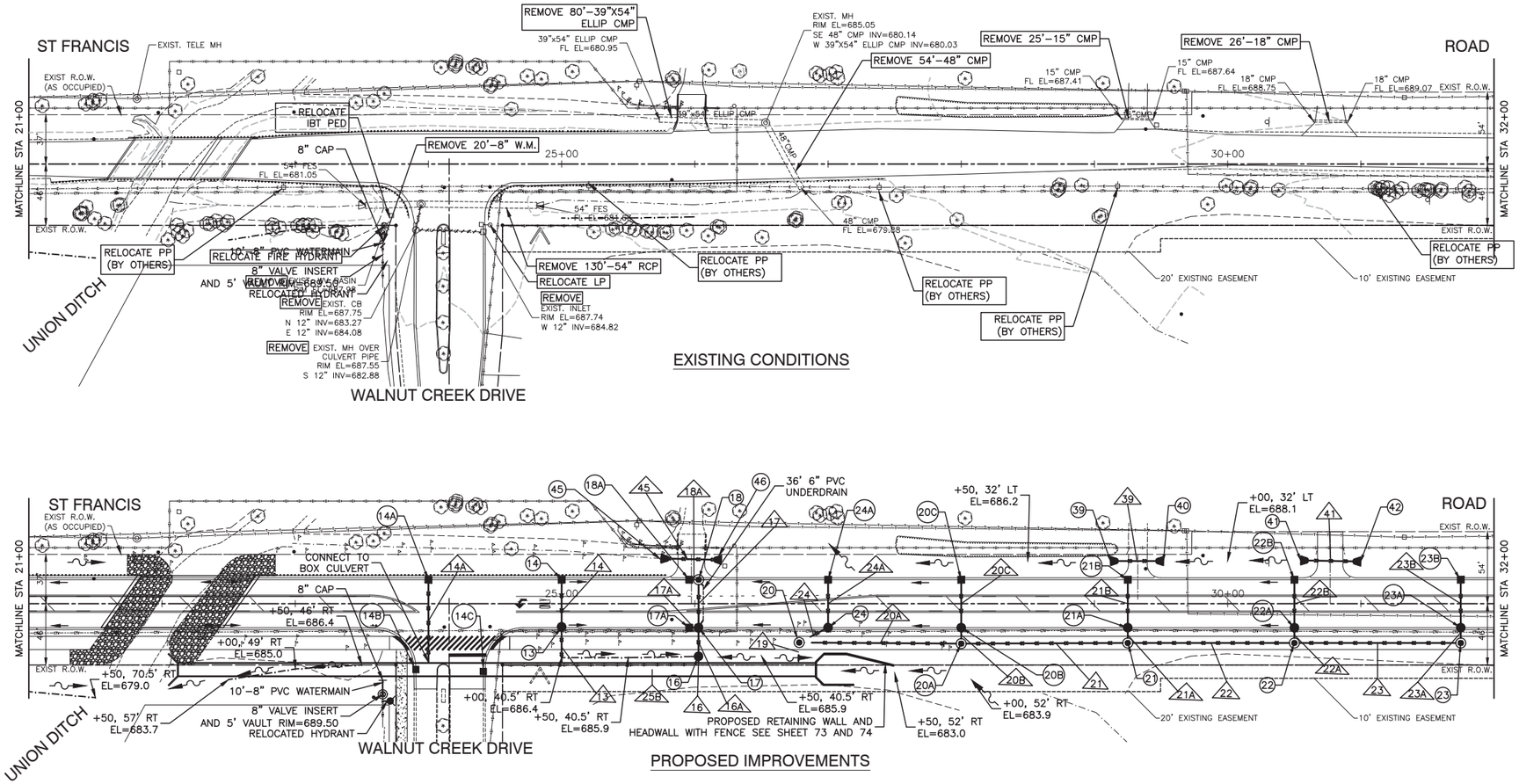
PROFILE — SURVEYED, PLOTTED, CROSS CHECKED, B.M. NOTED, NOTE BOOK, BY, DATE



FILE NAME = 10423_12-STRM-01 - STORM(2)REV	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION DRAINAGE & UTILITIES
		CHECKED — HLG	REVISED —		
	PLOT SCALE =	DRAWN — MED	REVISED —		
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —		SCALE: H 1"=50' V 1"=5' | SHEET NO. 42 OF 109 SHEETS | STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	42
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

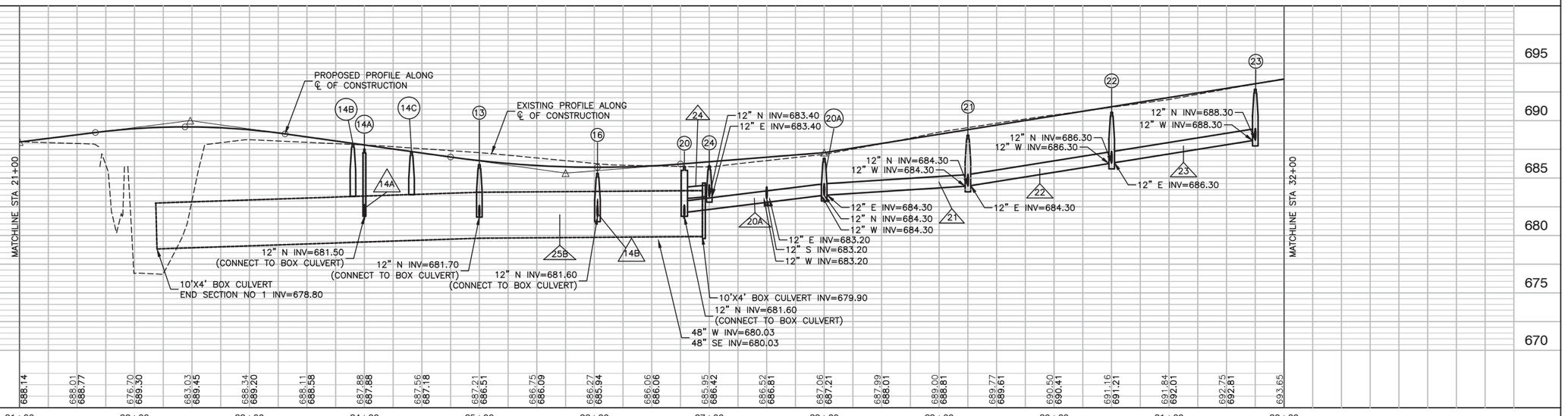
SECTION 14, TOWNSHIP 35, RANGE 12



N

NOTES:

1. ALL OFFSETS AND ELEVATIONS TO CURB LINE STRUCTURES ARE GIVEN TO THE EDGE OF PAVEMENT. OFFSETS AND ELEVATIONS FOR FLARED END SECTIONS ARE GIVEN AT THE FLARED END. ALL OTHER OFFSETS AND ELEVATIONS ARE GIVEN TO THE CENTER OF THE STRUCTURE.
2. 8" CAP ON PROPOSED WATERMAIN AT STA. 23+66 SHALL BE INCIDENTAL TO THE COST OF THE 8" WATERMAIN.



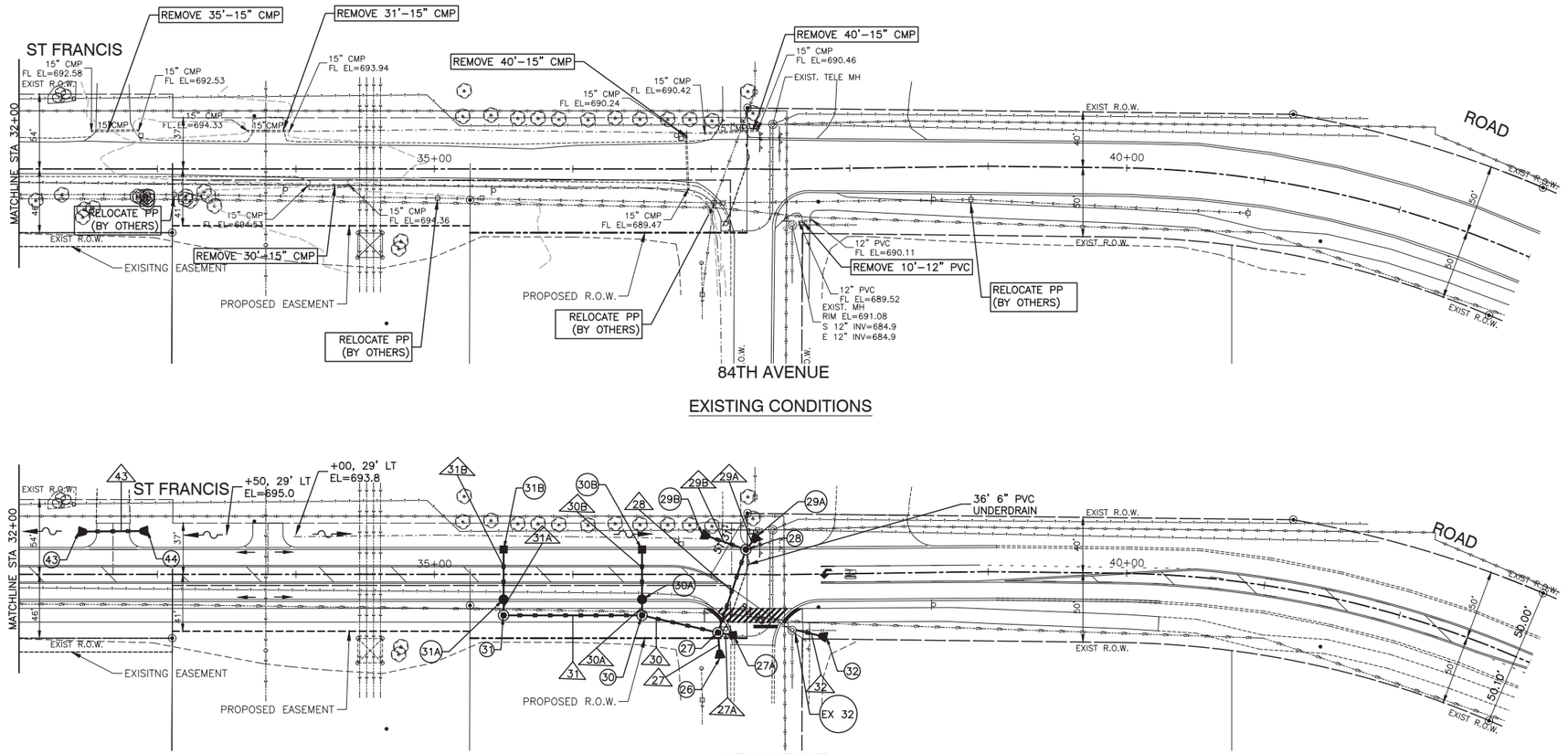
FILE NAME = 10423_12-STRM-01 - IDOT STRM(3)	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION DRAINAGE & UTILITIES
		CHECKED — HLG	REVISED —		
	PLOT SCALE =	DRAWN — MED	REVISED —		SCALE: H 1"=50' V 1"=5' | SHEET NO. 43 OF 109 SHEETS | STA. TO STA.
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —		

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	43
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

PLAN — SURVEYED, PLOTTED, ALIGNMENT CHECKED, RT. OF WAY CHECKED, CADD FILE NAME, NOTE BOOK, NO.____, BY, DATE

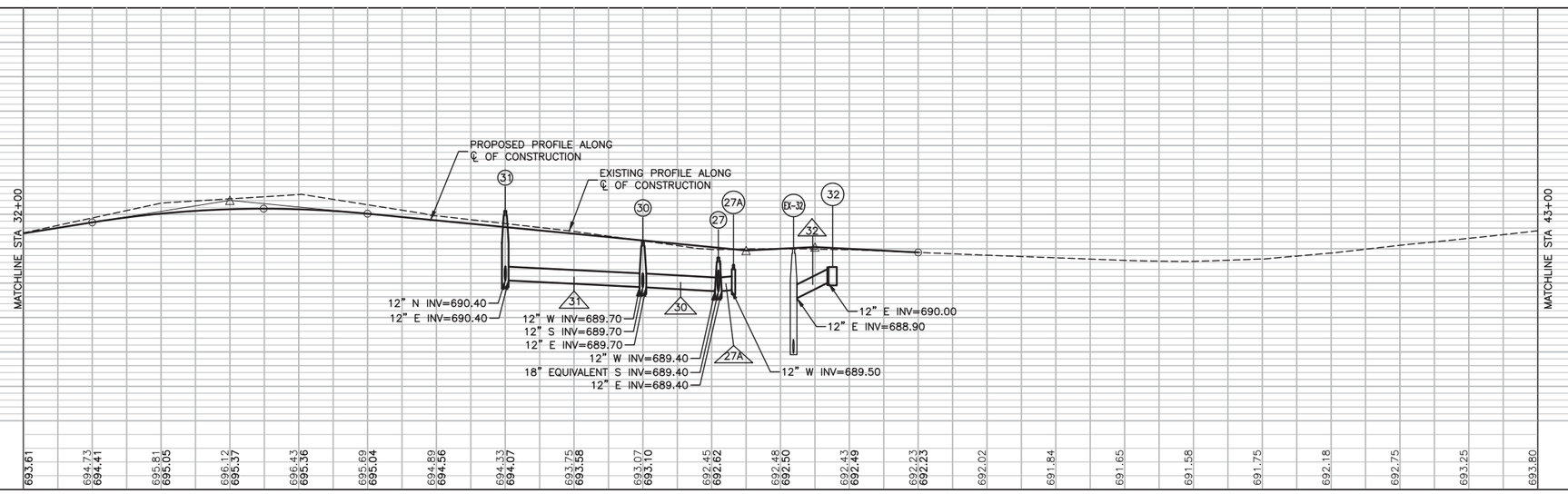
PROFILE — SURVEYED, PLOTTED, CROSS SECTIONS CHECKED, B.M. NOTED, NOTE BOOK, BY, DATE

SECTION 14, TOWNSHIP 35, RANGE 12



EXISTING CONDITIONS

PROPOSED IMPROVEMENTS



NOTES:

1. ALL OFFSETS AND ELEVATIONS TO CURB LINE STRUCTURES ARE GIVEN TO THE EDGE OF PAVEMENT. OFFSETS AND ELEVATIONS FOR FLARED END SECTIONS ARE GIVEN AT THE FLARED END. ALL OTHER OFFSETS AND ELEVATIONS ARE GIVEN TO THE CENTER OF THE STRUCTURE.

PLAN: DATE | BY | SURVEYED, PLOTTED, ALIGNMENT CHECKED, RT. OF WAY CHECKED, CADD FILE NAME | NOTE BOOK NO.

PROFILE: DATE | BY | SURVEYED, PLOTTED, CROSS SECTIONS CHECKED, B.M. NOTED | NOTE BOOK

FILE NAME = 10423_12-STRM-01 - IDOT STRM(4)	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION DRAINAGE & UTILITIES
		CHECKED — HLG	REVISED —		
	PLOT SCALE =	DRAWN — MED	REVISED —		SCALE: H 1"=50' V 1"=5' | SHEET NO. 44 OF 109 SHEETS | STA. TO STA.
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —		

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	44
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

(1)
STA=19+50, 59.0' RT
CONCRETE F.E.S.
30" NW INV=680.00

(2)
STA.=19+29.66, 46.0' RT
MH TA 5ø, T1F CL
RIM=684.00
30" N INV=680.20
30" SE INV=680.20

(3)
STA.=19+29.71, 29.5' RT
T1F CL
RIM=687.65
(CONSTRUCT ON
BOX CULVERT, SPECIAL)

(4)
STA.=19+29.71, 18.0' RT
CB TA 4ø, T11 F&G
RIM=686.51
24" N INV=681.70
24" S INV=681.70
12" W INV=682.80

(4A)
STA.=19+22.71, 18.0' RT
INL TA, T11 F&G
RIM=686.52
12" E INV=683.00

(5)
STA.=19+29.71, 18.0' LT
MH TA 4ø, T11 F&G
RIM=686.51
18" N INV=682.40
24" S INV=682.40
12" W INV=682.40

(5A)
STA.=19+22.71, 18.0' LT
INL TA, T11 F&G
RIM=686.52
12" E INV=682.60

(6)
STA.=19+29.71, 32.0' LT
MH TA 4ø, T1F CL
RIM=686.00
18" W INV=683.30
18" S INV=682.80

(7)
STA.=19+19.71, 32.0' LT
CONCRETE F.E.S. W/GRATE
RIM=685.34
18" E INV=683.55

(8A)
STA.=20+50.00, 18.0' RT
CB TA 4ø, T11 F&G
RIM=687.28
12" N INV=683.30
12" S INV=682.80

(8B)
STA.=20+50.00, 18.0' LT
INL TA, T11V F&G
RIM=687.28
12" S INV=683.80

(9A)
STA.=17+50.00, 18.0' RT
CB TA 4ø, T11 F&G
RIM=688.39
12" S INV=682.80
12" N INV=684.40

(9B)
STA.=17+50.00, 18.0' LT
INL TA, T11V F&G
RIM=688.39
12" S INV=684.90

(10)
STA.=16+00.00, 29.6' RT
MH TA 4ø, T1F CL
RIM=693.36
12" E INV=688.20
12" N INV=688.20
12" W INV=688.20

(10A)
STA.=16+00.00, 17.6' RT
CB TA 4ø, T11V F&G
RIM=692.60
12" S INV=688.60
12" N INV=688.60

(10B)
STA.=16+00.00, 18.0' LT
INL TA, T11V F&G
RIM=692.59
12" S INV=689.10

(11)
STA.=14+50.00, 29.5' RT
MH TA 4ø, T1F CL
RIM=697.98
12" E INV=692.70
12" N INV=692.70
12" W INV=692.70

(11A)
STA.=14+50.00, 17.5' RT
CB TA 4ø, T11V F&G
RIM=697.22
12" S INV=693.20
12" N INV=693.20

(11B)
STA.=14+50.00, 18.0' LT
INL TA, T11V F&G
RIM=697.21
12" S INV=693.70

(12)
STA.=13+00.00, 30.1' RT
MH TA 4ø, T1F CL
RIM=702.30
12" E INV=697.30
12" N INV=697.30

(12A)
STA.=13+00.06, 18.0' RT
CB TA 4ø, T11V F&G
RIM=701.50
12" S INV=697.50
12" N INV=697.50

(12B)
STA.=13+00.00, 18.0' LT
INL TA, T11V F&G
RIM=701.50
12" S INV=698.00

(13)
STA.=25+00.00, 17.4' RT
CB TA 4ø, T11V F&G
RIM=686.17
12" S INV=682.10
12" N INV=682.10

(14)
STA.=25+00.00, 18.0' LT
INL TA, T11V F&G
RIM=686.15
12" S INV=682.60

(14A)
STA.=24+00.00, 18.0' LT
INL TA, T11V F&G
RIM=687.52
12" S INV=684.00

(14B)
STA=23+90, 49.5' RT
INL TA, T11 F&G
RIM =687.75
CONSTRUCT ON
BOX CULVERT

(14C)
STA=24+41, 51.0' RT
INL TA, T11 F&G
RIM =687.74
CONSTRUCT ON
BOX CULVERT

(16)
STA.=26+02.78, 39.5' RT
CB TA 4ø, T8 GRATE
RIM=685.40
12" S INV=681.70
12" N INV=681.70

(17)
STA.=26+02.78, 18.0' RT
CB TA 4ø, T11 F&G
RIM=685.58
12" N INV=681.80
12" W INV=681.80
12" S INV=681.80

(17A)
STA.=25+95.78, 18.0' RT
INL TA, T11 F&G
RIM=685.58
12" E INV=681.90

(18)
STA.=26+02.84, 18.0' LT
MH TA 4ø, T11 F&G
RIM=685.57
12" W INV=682.00
12" S INV=682.00

(18A)
STA.=25+95.78, 18.0' LT
INL TA, T11 F&G
RIM=685.58
12" E INV=682.10

(20)
STA.=26+78.35, 29.2' RT
MH TA 4ø.
RIM= 686.00
12" E INV=682.00
12" NE INV=683.20
12" S INV=682.00

(20A)
STA.=28+00.00, 30.0' RT
MH TA 4ø, T1F CL
RIM=686.71
12" E INV=683.50
12" N INV=683.50
12" W INV=683.50

(20B)
STA.=28+00.00, 18.0' RT
CB TA 4ø, T11V F&G
RIM=686.85
12" N INV=683.60
12" S INV=683.60

(20C)
STA.=28+00.00, 18.0' LT
INL TA, T11V F&G
RIM=686.85
12" S INV=683.80

(21)
STA.=29+25.00, 29.5' RT
MH TA 4ø, T1F CL
RIM=688.73
12" N INV=684.30
12" E INV=684.30
12" W INV=684.30

(21A)
STA.=29+25.00, 18.0' RT
CB TA 4ø, T11V F&G
RIM=688.85
12" N INV=684.80
12" S INV=684.78

(21B)
STA.=29+25.00, 18.0' LT
INL TA, T11V F&G
RIM=688.85
12" S INV=685.30

(22)
STA.=30+50.10, 29.5' RT
MH TA 4ø, T1F CL
RIM=690.73
12" E INV=686.30
12" W INV=686.30
12" N INV=686.30

(22A)
STA.=30+50.10, 18.0' RT
CB TA 4ø, T11V F&G
RIM=690.85
12" S INV=686.78
12" N INV=686.80

(22B)
STA.=30+50.10, 18.0' LT
INL TA, T11V F&G
RIM=690.85
12" S INV=687.30

(23)
STA.=31+75.00, 29.5' RT
MH TA 4ø, T1F CL
RIM=692.73
12" N INV=688.30
12" W INV=688.30

(23A)
STA.=31+75.00, 18.0' RT
CB TA 4ø, T11V F&G
RIM=692.85
12" N INV=688.80
12" S INV=688.78

(23B)
STA.=31+75.00, 18.0' LT
INL TA, T11V F&G
RIM=692.85
12" S INV=689.30

(24)
STA.=27+00.00, 18.0' RT
CB TA 4ø, T11V F&G
RIM=686.06
12" N INV=683.40
12" SW INV=683.40

(24A)
STA.=27+00.01, 18.0' LT
INL TA, T11V F&G
RIM=686.06
12" S INV=683.50

(26)
STA=37+06.32, 60.3' RT
CONCRETE F.E.S.
24" EQUIVALENT INV=689.36

(27)
STA.=37+04.81, 42.0' RT
MH TA 5ø, TIF CL
RIM=691.86
18" N INV=689.40
12" W INV=689.40
12" E INV=689.40
24" EQUIVALENT S INV=689.40

(27A)
STA.=37+15.81, 43.9' RT
INL TA, T11 F&G
RIM=691.33
12" W INV=689.50

(28)
STA.=37+25.00, 17.9' LT
CB TA 4ø, T11 F&G
RIM=692.01
15" W INV=689.80
15" NE INV=689.80
18" EQUIVALENT S INV=689.80

(29A)
STA=37+33.06, 28.0' LT
CONCRETE F.E.S.
15" SW INV=690.20

(29B)
STA=36+93.26, 29.0' LT
CONCRETE F.E.S.
15" SW INV=690.10

(30)
STA.=36+50.00, 29.6' RT
MH TA 4ø, T1F CL
RIM=693.04
12" W INV=689.70
12" N INV=689.70
12" E INV=689.70

(30A)
STA.=36+50.00, 18.0' RT
CB TA 4ø, T11V F&G
RIM=692.74
12" N INV=689.80
12" S INV=689.80

(30B)
STA.=36+50.00, 17.9' LT
INL TA, T11V F&G
RIM=692.74
12" S INV=689.90

(31)
STA.=35+50.00, 29.6' RT
MH TA 4ø, T1F CL
RIM=695.25
12" N INV=690.20
12" E INV=690.20

(31A)
STA.=35+50.00, 18.0' RT
CB TA 4ø, T11V F&G
RIM=693.71
12" N INV=690.30
12" S INV=690.30

(31B)
STA.=35+50.00, 18.0' LT
INL TA, T11V F&G
RIM=693.71
12" S INV=690.50

(32)
STA.=37+82.60, 45.7' RT
Concrete Flared End Section
RIM=691.25
12" W INV=690.00

(33)
STA.=13+87.45, 32.0' LT
Concrete Flared End Section
RIM=699.02
15" E INV=697.50

(32)
STA=37+83.80, 42.5' RT
CONCRETE F.E.S.
12" INV=690.00

(35)
STA.=15+02.69, 30.0' LT
Concrete Flared End Section
RIM=695.12
15" E INV=693.60

(36)
STA.=15+48.69, 30.1' LT
Concrete Flared End Section
RIM=694.52
15" W INV=693.00

(37)
STA.=18+19.74, 32.0' LT
Concrete Flared End Section
RIM=686.49
18" E INV=684.70

(38)
STA.=18+67.62, 32.0' LT
Concrete Flared End Section
RIM=685.89
18" W INV=684.10

(39)
STA.=29+12.07, 32.0' LT
Concrete Flared End Section
RIM=688.32
15" E INV=686.80

(40)
STA.=29+51.81, 32.0' LT
Concrete Flared End Section
RIM=688.52
15" W INV=687.00

(41)
STA.=30+54.83, 32.5' LT
Concrete Flared End Section
RIM=690.22
15" E INV=688.70

(42)
STA.=30+99.82, 31.7' LT
Concrete Flared End Section
RIM=690.62
15" W INV=689.10

(43)
STA.=32+43.17, 30.9' LT
Concrete Flared End Section
RIM=694.02
15" E INV=692.50

(44)
STA.=32+92.94, 31.4' LT
Concrete Flared End Section
RIM=694.22
15" W INV=692.70

(43)
STA=32+43.17, 30.9' LT
CONCRETE F.E.S.
15" INV=692.50

(44)
STA=32+92.94, 31.4' LT
CONCRETE F.E.S.
15" INV=682.70

(45)
STA=25+74.82, 31' LT
CONCRETE F.E.S.
15" INV=682.1

(46)
STA=26+19.89 31' LT
CONCRETE F.E.S.
15" INV=682.30

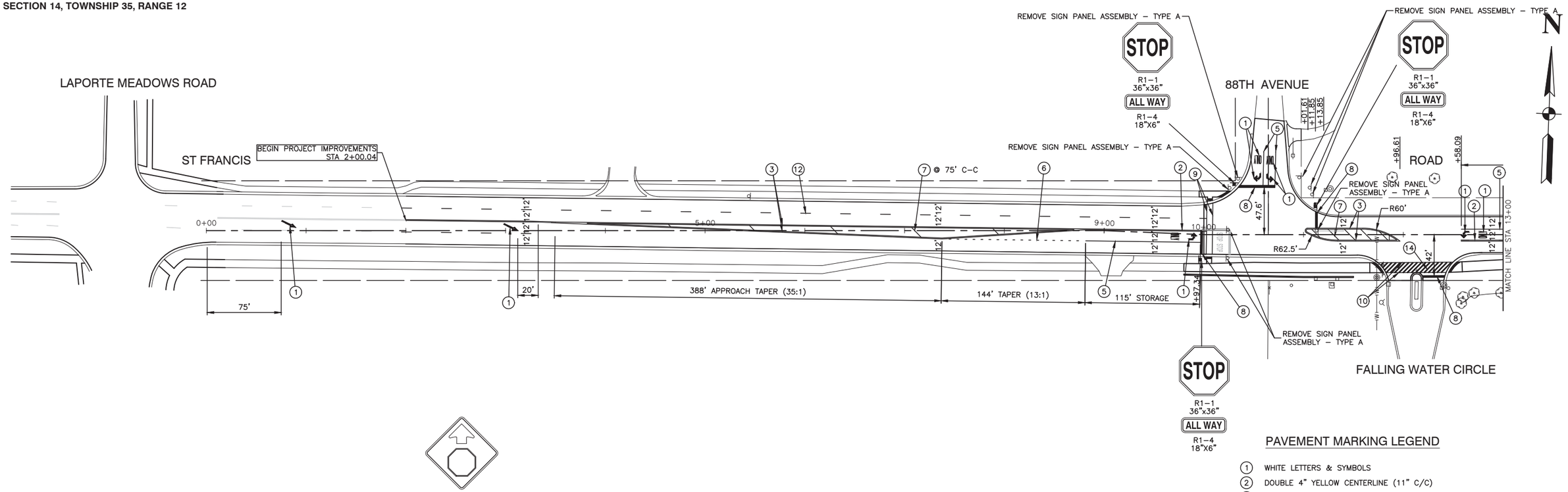
1 — 24'–30" RCCP, SST1 @ 0.83% (1.0)
2 — 12'–30" RCCP, SST1 @ 1.00% (2.3)
3 — 7'–24" RCCP, SST1 @ 1.43% (2.8)
4 — 36'–24" RCCP, SST1 @ 1.95% (6.4)
4A — 7'–12" RCCP, SST1 @ 2.86% (0.8)
5 — 14'–18" RCCP, SST1 @ 2.85% (1.2)
5A — 7'–12" RCCP, SST1 @ 2.86% (1.1)
6 — 10'–18" RCCP, SST1 @ 2.50% (1.0)
8 — 125'–10'X4' BOX CULVERT, SPECIAL @ 0.08% (21.0)
8A — 7'–12" RCCP, SST2 @ 4.23% (1.8)
8B — 36'–12" RCCP, SST1 @ 1.39% (4.4)
9 — 185'–10'X4' BOX CULVERT, SPECIAL @ 0.08% (65.3)
9A — 7'–12" RCCP, SST2 @ 4.29% (1.8)
9B — 36'–12" RCCP, SST1 @ 1.39% (4.4)
10 — 145'–12" RCCP, SST2 @ 3.93% (41.5)
10A — 12'–12" RCCP, SST2 @ 3.33% (2.5)
10B — 36'–12" RCCP, SST1 @ 1.41% (4.4)
11 — 150'–12" RCCP, SST2 @ 3.00% (41.6)
11A — 12'–12" RCCP, SST2 @ 4.17% (2.6)
11B — 35'–12" RCCP, SST1 @ 1.41% (4.4)
12 — 150'–12" RCCP, SST2 @ 3.07% (40.4)
12A — 12'–12" RCCP, SST2 @ 1.65% (2.4)
12B — 36'–12" RCCP, SST1 @ 1.39% (4.4)
13 — 27'–12" RCCP, SST2 @ 1.48% (6.7)
14 — 35'–12" RCCP, SST1 @ 1.41% (4.5)
14A — 62'–12" RCCP, SST2 @ 3.97% (14.3)
16 — 5'–12" RCCP, SST2 @ 2.17% (0.6)
16A — 22'–12" RCCP, SST1 @ 0.47% (2.7)
17 — 36'–12" RCCP, SST1 @ 0.56% (4.2)
17A — 7'–12" RCCP, SST1 @ 1.43% (0.8)
18A — 7'–12" RCCP, SST1 @ 1.42% (1.7)
19 — 16' –12" RCCP, SST2 @ 2.50% (1.0)
20A — 122'–12" RCCP, SST2 @ 1.23% (30.8)
20B — 12'–12" RCCP, SST1 @ 0.83% (2.6)
20C — 36'–12" RCCP, SST1 @ 0.56% (7.5)
21 — 125'–12" RCCP, SST2 @ 0.64% (16.3)
21A — 12'–12" RCCP, SST2 @ 4.17% (2.1)
21B — 36'–12" RCCP, SST1 @ 1.39% (4.6)
22 — 125'–12" RCCP, SST2 @ 1.60% (24.3)
22A — 12'–12" RCCP, SST2 @ 4.17% (2.1)
22B — 36'–12" RCCP, SST1 @ 1.39% (4.6)
23 — 125'–12" RCCP, SST2 @ 1.60% (24.3)
23A — 12'–12" RCCP, SST2 @ 4.17% (2.1)
23B — 36'–12" RCCP, SST1 @ 1.39% (4.6)
24 — 24'–12" RCCP, SST1 @ 0.82% (0.4)
24A — 36'–12" RCCP, SST1 @ 0.28% (1.0)
25B — 478'–10'X4' BOX CULVERT @ 0.23% (1.0)
27 — 18'–24" EQUIVALENT RCCP, SST1 @ 0.20% (1.0)
27A — 11'–12" RCCP, SST1 @ 0.89% (1.0)
28 — 63'–18" RCCP, SST1 @ 0.63% (1.6)
29A — 63'–18" EQUIVALENT RCCP, SST1 @ 6.3% (9.2)
29B — 34'–15" RCCP, SST1 @ 0.89% (0.9)
30 — 56'–12" RCCP, SST1 @ 0.53% (2.3)
30A — 12'–12" RCCP, SST1 @ 0.87% (12.6)
30B — 36'–12" RCCP, SST1 @ 0.28% (2.6)
31 — 100'–12" RCCP, SST2 @ 0.50% (6.8)
31A — 12'–12" RCCP, SST2 @ 0.87% (32.7)
31B — 36'–12" RCCP, SST1 @ 0.56% (4.0)
32 — 22'–12" RCCP, SST1 @ 4.90% (8.4)
33 — 40'–15" RCCP, SST1 @ 1.50% (2.1)
35 — 46'–15" RCCP, SST1 @ 1.30% (1.0)
36 — 48'–18" RCCP, SST1 @ 1.25% (1.0)
39 — 40'–15" RCCP, SST1 @ 0.50% (1.0)
41 — 45'–15" RCCP, SST1 @ 0.89% (1.0)
43 — 50'–15" RCCP, SST1 @ 0.40% (1.0)
45 — 45'–15" RCCP, SST1 @ 0.44% (21.0)

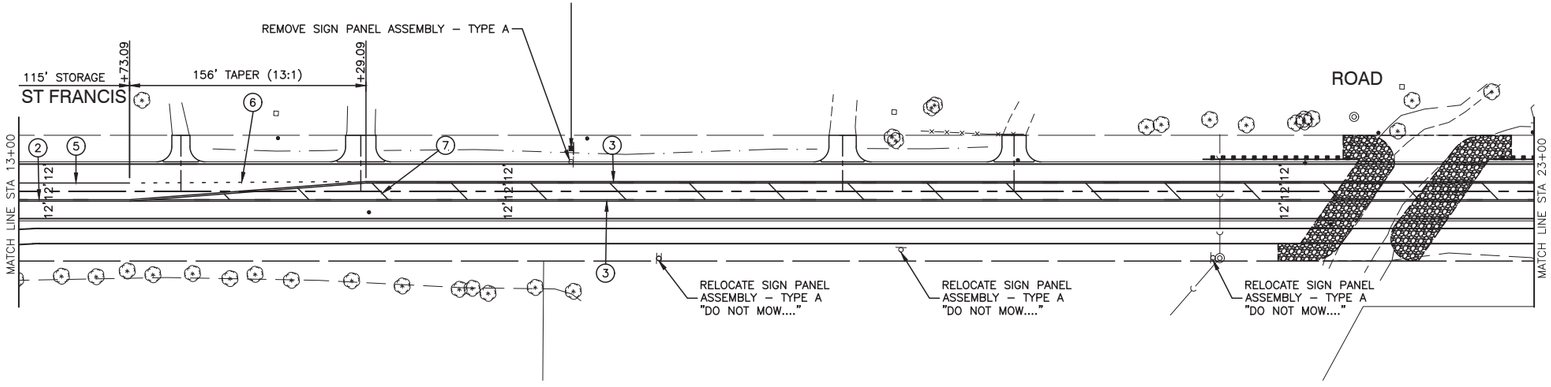
NOTES:

1. (XX.X) DENOTES CUBIC YARDS OF TRENCH BACK FILL.

FILE NAME = 10423_12-STRM-01 - _PIPE_SCHED	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION PIPE AND STRUCTURE SCHEDULE			F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		CHECKED — HLG	REVISED —					3752	10-00045-00-WR	WILL	109	45
	PLOT SCALE =	DRAWN — MED	REVISED —							CONTRACT NO. 61D22		
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —		SCALE:	SHEET NO. 45 OF 109 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

SECTION 14, TOWNSHIP 35, RANGE 12





PAVEMENT MARKING LEGEND

1. WHITE LETTERS & SYMBOLS
2. DOUBLE 4" YELLOW CENTERLINE (11" C/C)
3. DOUBLE YELLOW MEDIAN OUTLINE (11" C/C)
3A. 4" YELLOW LINE
4. 4" WHITE EDGE LINE
5. 6" WHITE LANE LINE
6. 6" WHITE SKIP–DASH LINE (2' LINE – 6' SPACE)
7. 12" YELLOW DIAGONAL LINE
8. 24" WHITE STOP BAR
9. 6" WHITE CROSSWALK (6' C/C)
10. 6" WHITE CROSSWALK (10' C/C)
11. 12" WHITE LINE (45° ANGLE, 20' C/C)
12. 4" WHITE SKIP – DASH LINE (10' LINE – 30' SPACE)
13. 4" YELLOW SKIP – DASH LINE (10' LINE – 30' SPACE)
14. 12" WHITE DIAGONAL LINE (3' C–C)

TRAFFIC SIGN

NOTES

1.) SEE IDOT STANDARD TC–13 FOR PAVEMENT MARKING DETAILS.

2.) ALL LEGEND ITEMS ARE THERMOPLASTIC PAVEMENT MARKING OF THE WIDTH AND TYPE SPECIFIED.

3.) ALL RELOCATED SIGN PANELS WILL BE INSTALLED WITH NEW TELESCOPING STEEL SIGN SUPPORTS.

4.) ALL EXISTING SIGNS THAT CONFLICT WITH CONSTRUCTION SHALL BE REMOVED AND STORED OFFSITE. ALL EXISTING SIGNS SHALL BE RELOCATED AS DIRECTED BY THE ENGINEER. ANY SIGNS DAMAGED DURING CONSTRUCTION SHALL BE REPLACED AT THE COST OF THE CONTRACTOR.

FILE NAME = 10423_12-PMKG-01 - IDOT P01	USER NAME =	DESIGNED — GA	REVISED —
		CHECKED — HLG	REVISED —
	PLOT SCALE =	DRAWN — MED	REVISED —
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —

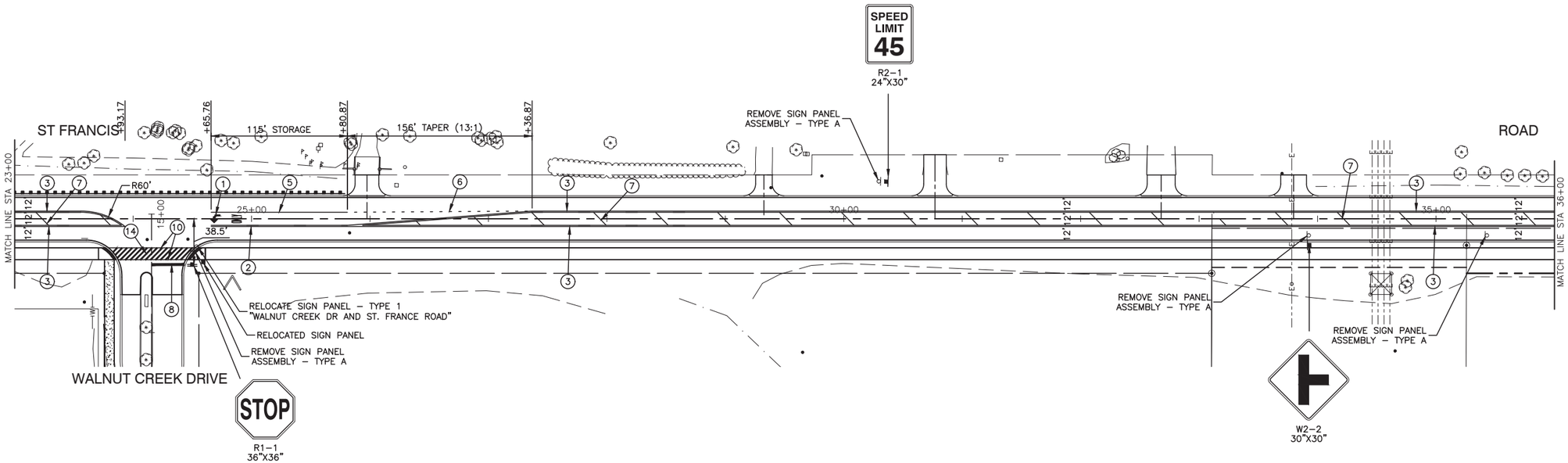
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
PAVEMENT MARKING & SIGNING

SCALE: 1"=50' | SHEET NO. 46 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	46
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

SECTION 14, TOWNSHIP 35, RANGE 12



PAVEMENT MARKING LEGEND

(1)	WHITE LETTERS & SYMBOLS
(2)	DOUBLE 4" YELLOW CENTERLINE (11" C/C)
(3)	DOUBLE YELLOW MEDIAN OUTLINE (11" C/C)
(3A)	4" YELLOW LINE
(4)	4" WHITE EDGE LINE
(5)	6" WHITE LANE LINE
(6)	6" WHITE SKIP–DASH LINE (2' LINE – 6' SPACE)
(7)	12" YELLOW DIAGONAL LINE
(8)	24" WHITE STOP BAR
(9)	6" WHITE CROSSWALK (6' C/C)
(10)	6" WHITE CROSSWALK (10' C/C)
(11)	12" WHITE LINE (45° ANGLE, 20' C/C)
(12)	4" WHITE SKIP – DASH LINE (10' LINE – 30' SPACE)
(13)	4" YELLOW SKIP – DASH LINE (10' LINE – 30' SPACE)
(14)	12" WHITE DIAGONAL LINE (3' C–C)
	TRAFFIC SIGN

NOTES

1.) SEE IDOT STANDARD TC–13 FOR PAVEMENT MARKING DETAILS.

2.) ALL LEGEND ITEMS ARE THERMOPLASTIC PAVEMENT MARKING OF THE WIDTH AND TYPE SPECIFIED.

3.) ALL RELOCATED SIGN PANELS WILL BE INSTALLED WITH NEW TELESCOPING STEEL SIGN SUPPORTS.

4.) ALL EXISTING SIGNS THAT CONFLICT WITH CONSTRUCTION SHALL BE REMOVED AND STORED OFFSITE. ALL EXISTING SIGNS SHALL BE RELOCATED AS DIRECTED BY THE ENGINEER. ANY SIGNS DAMAGED DURING CONSTRUCTION SHALL BE REPLACED AT THE COST OF THE CONTRACTOR.

FILE NAME = 10423_12-PMKG-01 - IDOT P02	USER NAME =	DESIGNED — GA	REVISED —
		CHECKED — HLG	REVISED —
	PLOT SCALE =	DRAWN — MED	REVISED —
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

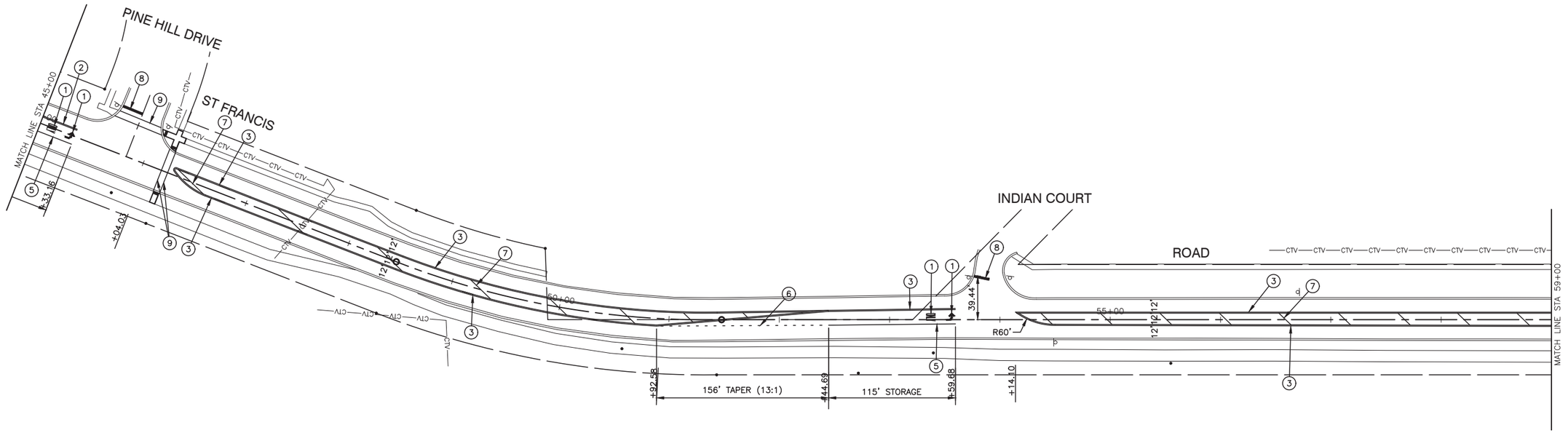
ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
PAVEMENT MARKING & SIGNING

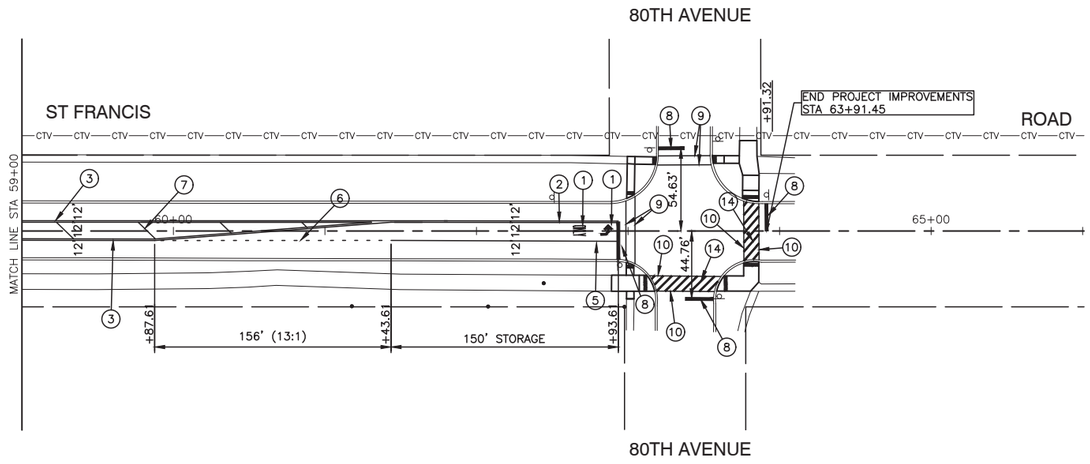
SCALE: 1"=50' | SHEET NO. 47 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	47
		CONTRACT NO. 61D22		

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT ----

SECTION 14, TOWNSHIP 35, RANGE 12





NOTES

1.) SEE IDOT STANDARD TC-13 FOR PAVEMENT MARKING DETAILS.

2.) ALL LEGEND ITEMS ARE THERMOPLASTIC PAVEMENT MARKING OF THE WIDTH AND TYPE SPECIFIED.

3.) ALL RELOCATED SIGN PANELS WILL BE INSTALLED WITH NEW TELESCOPING STEEL SIGN SUPPORTS.

4.) ALL EXISTING SIGNS THAT CONFLICT WITH CONSTRUCTION SHALL BE REMOVED AND STORED OFFSITE. ALL EXISTING SIGNS SHALL BE RELOCATED AS DIRECTED BY THE ENGINEER. ANY SIGNS DAMAGED DURING CONSTRUCTION SHALL BE REPLACED AT THE COST OF THE CONTRACTOR.

PAVEMENT MARKING LEGEND

(1)	WHITE LETTERS & SYMBOLS
(2)	DOUBLE 4" YELLOW CENTERLINE (11" C/C)
(3)	DOUBLE YELLOW MEDIAN OUTLINE (11" C/C)
(3A)	4" YELLOW LINE
(4)	4" WHITE EDGE LINE
(5)	6" WHITE LANE LINE
(6)	6" WHITE SKIP-DASH LINE (2' LINE - 6' SPACE)
(7)	12" YELLOW DIAGONAL LINE
(8)	24" WHITE STOP BAR
(9)	6" WHITE CROSSWALK (6' C/C)
(10)	6" WHITE CROSSWALK (10' C/C)
(11)	12" WHITE LINE (45° ANGLE, 20' C/C)
(12)	4" WHITE SKIP - DASH LINE (10' LINE - 30' SPACE)
(13)	4" YELLOW SKIP - DASH LINE (10' LINE - 30' SPACE)
(14)	12" WHITE DIAGONAL LINE (3' C-C)
	TRAFFIC SIGN

FILE NAME = 10423_12-PMKG-01 - IDOT P03	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION PAVEMENT MARKING & SIGNING
		CHECKED — HLG	REVISED —		
	PLOT SCALE =	DRAWN — MED	REVISED —		
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —		SCALE: 1"=50' | SHEET NO. 48 OF 109 SHEETS | STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	48
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

Benchmark: Frankfort FFT 178. Two nails in north face of 2nd power pole east of Indian Court, south side of St. Francis Road. Elevation 713.22 (NGVD 29).

Existing Structure: S.N. 099-4401 built in 1984 as TR Route 105, Section 82-06112-00-BR at Station 9+85. Structure consists of three span 17" PPC deck beams on cast-in-place stub abutments with bituminous wearing surface founded on metal shell piles and two reinforced concrete pile bent piers. 91'-10" Bk to Bk of abutments 30'-0" Out-to-Out of deck. Deck beams to be removed and replaced maintaining traffic by using Stage Construction. Superstructure and substructure to be widened.

No salvage.

SUGGESTED CONSTRUCTION SEQUENCE

1. Stage 1 Removal.
2. Stage 1 Construction.
3. Multi-Use Path Bridge Installation.
4. Stage 2 Removal.
5. Stage 2 Construction

SCOPE OF WORK

1. Staged Construction.
2. Remove Existing Structure to limits shown.
3. Install proposed Abutment and Pier Substructure.
4. Install proposed Rip-Rap.
5. Install proposed Deck Beams and Concrete Wearing Surface.
6. Install Type SM Rail.
7. Install Multi-Use Path Bridge.

INDEX OF SHEETS

DESIGN SPECIFICATIONS

NEW CONSTRUCTION

2010 AASHTO LRFD Bridge Design Specifications with 2010 Interims.
2009 AASHTO LRFD Guide Specification for Design of Pedestrian Bridge

LOADING HL-93

Allow 50#/sq. ft. for future wearing surface.

SEISMIC DATA

Seismic Performance Zone (SPZ) = 1
Design Spectral Acceleration at 1.0 sec. (S_{D1}) = 0.094
Design Spectral Acceleration at 0.2 sec. (S_{DS}) = 0.161
Soil Site Class = D

DESIGN STRESSES

NEW CONSTRUCTION

f'c = 3,500 psi (Substructure)
f'c = 4,000 psi (Superstructure)
fy = 60,000 psi (Reinforcement)
fy = 50,000 psi (M270 Grade 50)

PRECAST PRESTRESSED UNITS

f'c = 6,000 psi
f'ci = 5,000 psi
fpu = 270,000 psi (1/2"ϕ low lax strands)
fpbt = 201,960 psi (1/2"ϕ low lax strands)

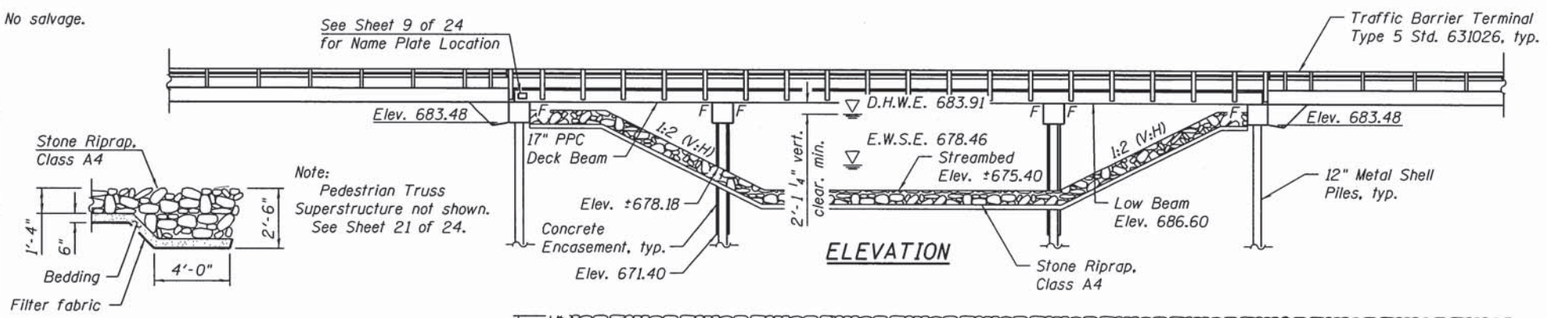
EXISTING CONSTRUCTION

fy = 40,000 psi (Reinforcement)
f'c = 1,400 psi

MAIN UNION DITCH
RE-BUILT 2017 BY
VILLAGE OF FRANKFORT
SEC 10-000-16-00-BR
F.A.U. 3752 STATION 22+09.91
STRUCTURE NO. 099-4401
LOADING HL-93

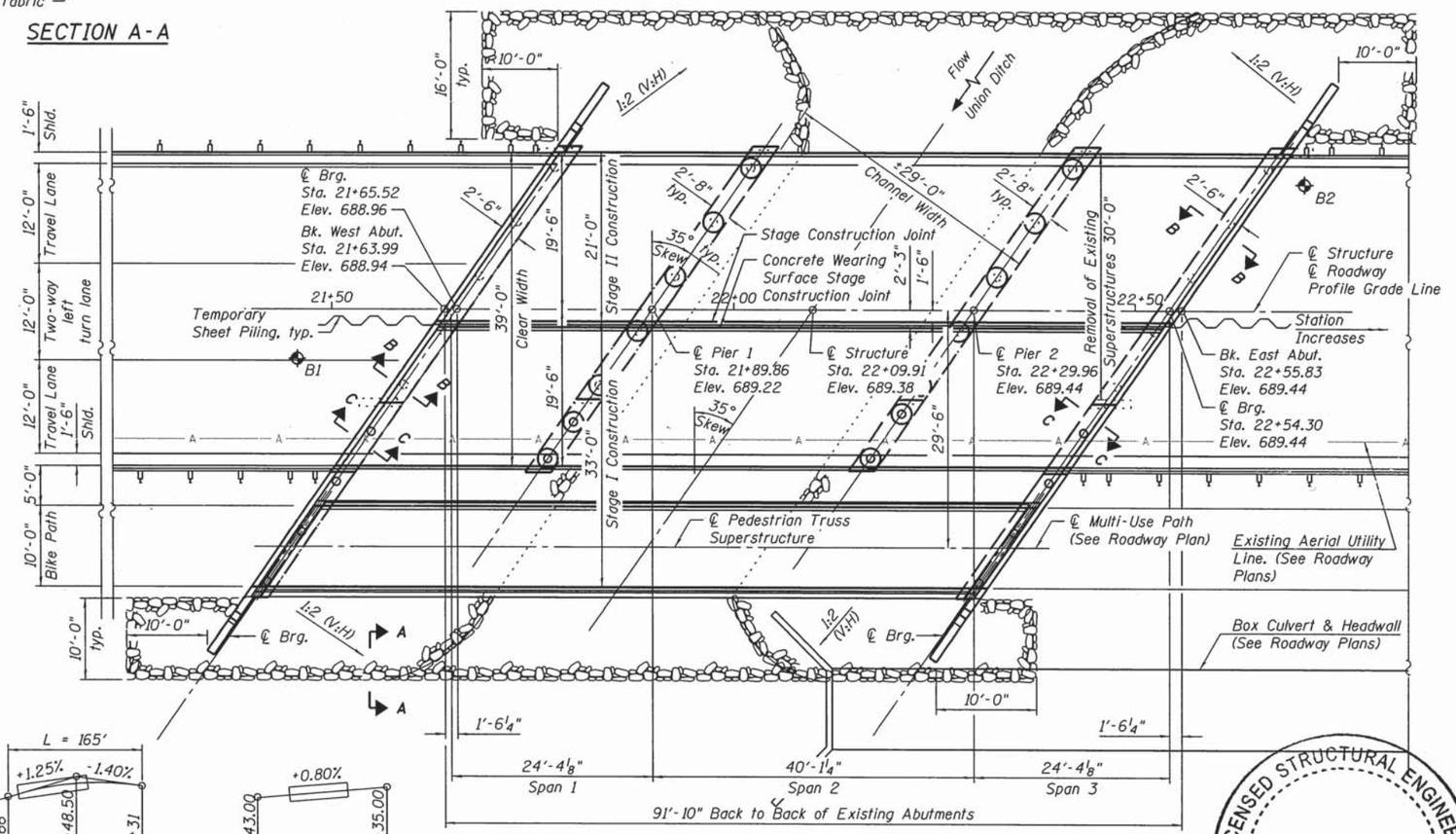
NAME PLATE

See Std. 515001
Existing Name Plate shall be cleaned and relocated next to new Name Plate.
Cost included with Name Plates.
See Sheet 9 of 24 for Name Plate Location.

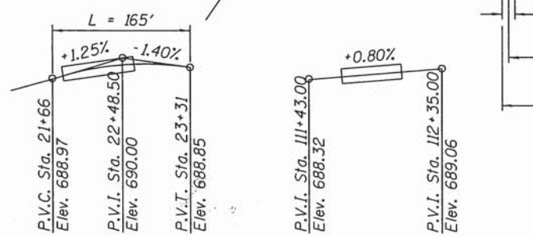


ELEVATION

SECTION A-A



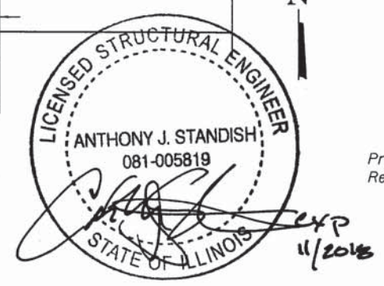
PLAN



PROFILE GRADE
(F.A.U. 3752 along ℄ of Roadway)

PROFILE GRADE
(Along ℄ of Bikepath)

I certify that to the best of knowledge, information and belief, this bridge design is structurally adequate for the design loading shown on the plans. The design is an economical one for the style of structure and complies with requirements of the current AASHTO LRFD Bridge Design Specifications.



N

DESIGN SCOUR ELEVATION TABLE

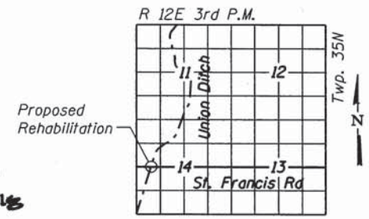
Design Scour Elevation (ft.)	West Abut.	West Pier	East Pier	East Abut.
	674.15	669.73	670.56	668.56

WATERWAY INFORMATION

Drainage Area = 17.2 square miles Prop. Overtopping Elev. 686.09 @ Sta. 17+00

Flood	Freq. Yr.	Q C.F.S.	Opening Sq. Ft. Exist.	Opening Sq. Ft. Prop.	Nat. H.W.E.	Head - Ft. Exist.	Head - Ft. Prop.	Headwater El. Exist.	Headwater El. Prop.
	10	464	286.00	340.79	683.33	0.04	0.00	683.37	683.33
Design	30	570	318.88	380.12	683.91	0.13	0.11	684.04	684.02
Base	100	707	356.65	418.61	684.42	0.26	0.25	684.68	684.67
Max. Calc.	500	858	387.73	449.68	684.77	0.41	0.42	685.18	685.19

10-year velocity through existing bridge = 1.84 fps 10 year velocity through proposed bridge = 1.39 fps.



LOCATION SKETCH

GENERAL PLAN & ELEVATION
ST. FRANCIS ROAD
OVER UNION DITCH
F.A.U. 3752
SECTION (10-00046-00-BR)
WILL COUNTY
STA. 22+09.91
S.N. 099-4401

FILE NAME = S:\JOL\6688-6694\6828\813\Micros\CADD_Sheets\099-4401-XXX-001-GP&E.dgn

STRAND ASSOCIATES®
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = BenL	DESIGNED BRL	REVISED -
	CHECKED AJS	REVISED -
PLOT SCALE =	DRAWN BJF	REVISED -
PLOT DATE = 11/16/2017	CHECKED BRL	REVISED -

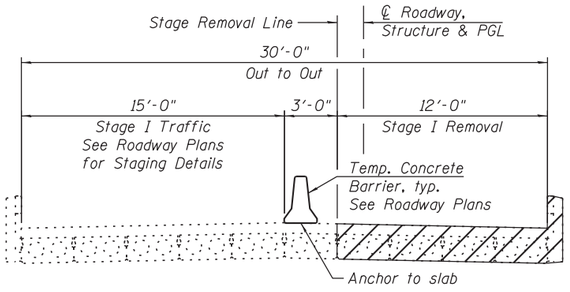
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN AND ELEVATION
STRUCTURE NO. 099-4401
SHEET NO. 1 OF 24 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00046-00-BR	WILL	109	49
	-	CONTRACT NO. 61D22		
		ILLINOIS FED. AID PROJECT		

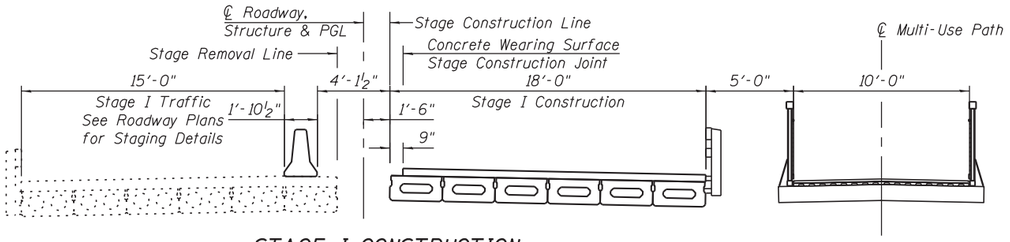
Legend

Removal



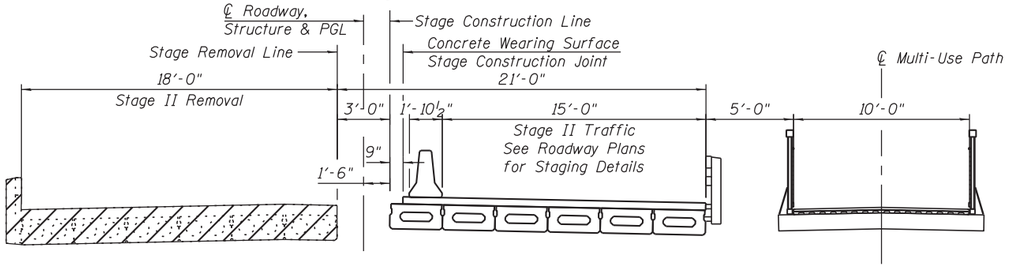
STAGE I REMOVAL

(Looking East)



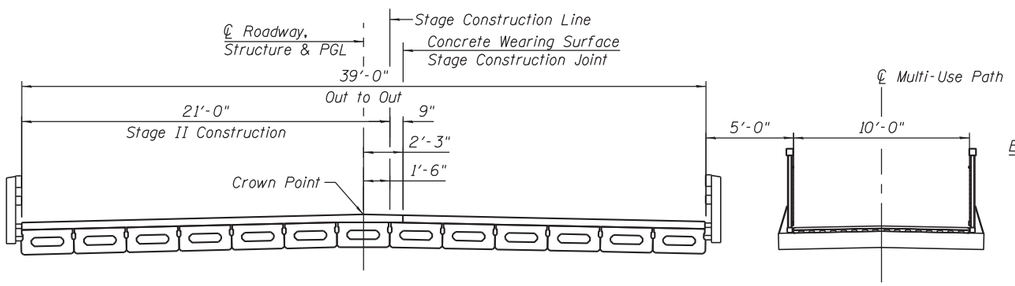
STAGE I CONSTRUCTION

(Looking East)



STAGE II REMOVAL

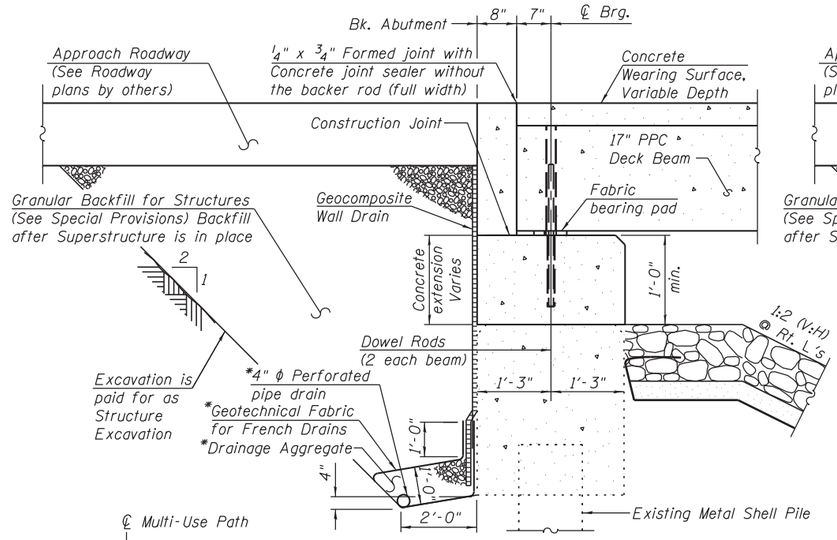
(Looking East)



COMPLETED CROSS SECTION

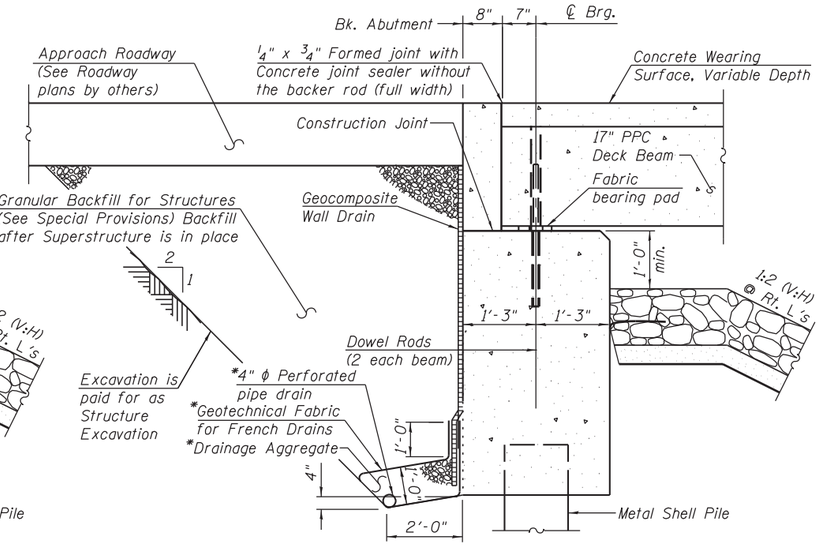
(Looking East)

Note: See Sheets 14 and 17 for Abutment and Pier Removal Limits respectively.



SECTION B-B THRU ABUTMENT

(Dimensions are at Rt. L's)



SECTION C-C THRU ABUTMENT

(Dimensions are at Rt. L's)

*Included in the cost of Pipe Underdrains for Structures. (See Special Provisions)

Note:
All drainage system components shall extend to 2'-0" from the end of each wingwall except an outlet pipe shall extend until intersecting with the side slopes. The pipes shall drain into concrete headwalls. (See Article 601.05 of the Standard Specifications and Highway Standard 601101).

GENERAL NOTES

Reinforcement bars designated (E) shall be epoxy coated.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Concrete Sealer shall be applied to the designated areas of the abutments and piers.

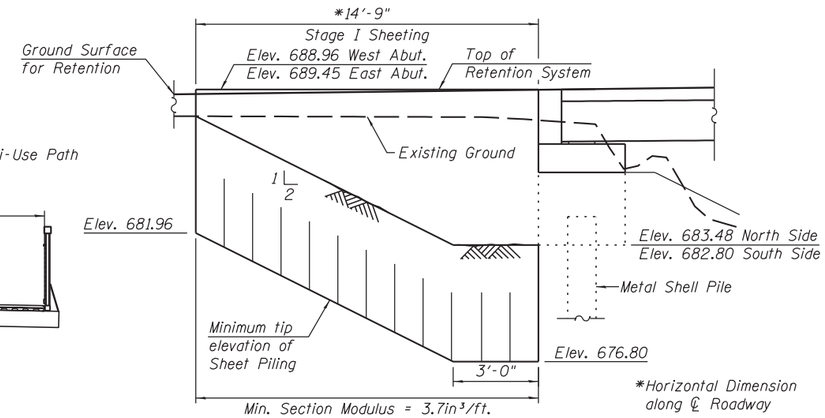
Layout of the slope protection system may be varied to suit ground conditions in the field as directed by the Engineer.

The embankment configuration shown shall be the minimum that must be placed and compacted prior to construction of the abutments.

Existing reinforcement shall be cleaned and incorporated into the new construction. Cost included with Concrete Removal.

A cantilevered sheet piling design does not appear feasible and additional members or other retention systems may be necessary. The Contractor shall submit a temporary soil retention system design including plan details and calculations for review and acceptance by the Engineer.

If a portion of the drilled shaft web walls or concrete encasement is under water, reinforcement may be placed underwater into forms. Concrete shall be tremied according to Article 503.08 of the Standard Specifications to an elevation of 1'-0" above the water line at the time of construction.



TEMPORARY SOIL RETENTION SYSTEM

(Horizontal Dimensions at Rt. L's)

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Stone Riprap, Class A4	Sq. Yd.		865	865
Filter Fabric	Sq. Yd.		865	865
Removal of Existing Superstructures	Each	1		1
Concrete Removal	Cu. Yd.		11	11
Structure Excavation	Cu. Yd.		75	75
Concrete Structures	Cu. Yd.		69	69
Concrete Superstructure	Cu. Yd.	17		17
Bridge Deck Grooving	Sq. Yd.	398		398
Concrete Encasement	Cu. Yd.		30	30
Protective Coat	Sq. Yd.	425		425
Precast Prestressed Concrete Deck Beams (17" Depth)	Sq. Ft.	3,519		3,519
Reinforcement Bars, Epoxy Coated	Pound	5,080	12,400	17,480
Bar Splicers	Each	91	40	131
Steel Railing, Type SM	Foot	184		184
Furnishing Metal Shell Piles 12"x0.25"	Foot		283	283
Driving Piles	Foot		283	283
Test Pile Metal Shells	Each		4	4
Pile Shoes	Each		12	12
Name Plates	Each	1		1
Temporary Sheet Piling	Sq. Ft.		315	315
Concrete Sealer	Sq. Ft.		1,680	1,680
Epoxy Crack Injection	Foot		38	38
Geocomposite Wall Drain	Sq. Yd.		75	75
Structural Repair of Concrete (Depth Equal to or Less than 5")	Sq. Ft.		4.0	4.0
Structural Repair of Concrete (Depth Greater than 5")	Sq. Ft.		4.0	4.0
Pipe Underdrains for Structures, 4"	Foot		185	185
Pedestrian Truss Superstructure	Sq. Ft.	920		920
Concrete Wearing Surface, (Variable Depth)	Sq. Yd.	391		391
Granular Backfill for Structures	Cu. Yd.		54	54

FILE NAME = S:\JOL\6608-6699\6628\0813\Micros\CADD\Sheets\099-4401-XXX-002-GD.dgn

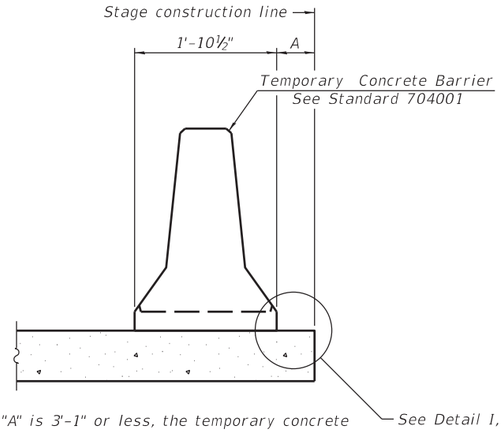
STRAND ASSOCIATES®
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = BenL	DESIGNED BRL	REVISED -
	CHECKED AJS	REVISED -
PLOT SCALE =	DRAWN BJF	REVISED -
PLOT DATE = 11/16/2017	CHECKED BRL	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

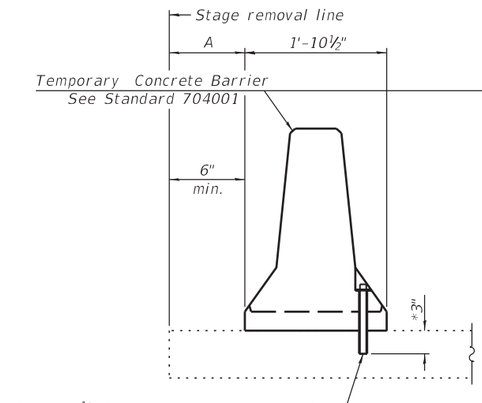
GENERAL DATA AND STAGE CONSTRUCTION DETAILS
STRUCTURE NO. 099-4401
SHEET NO. 2 OF 24 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00046-00-BR	WILL	109	50
		CONTRACT NO. 61D22		
	ILLINOIS FED. AID PROJECT			



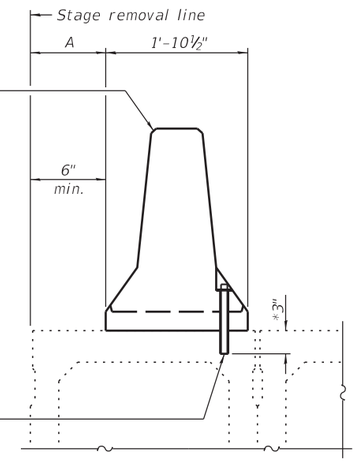
When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM



Drill 3-1¼" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

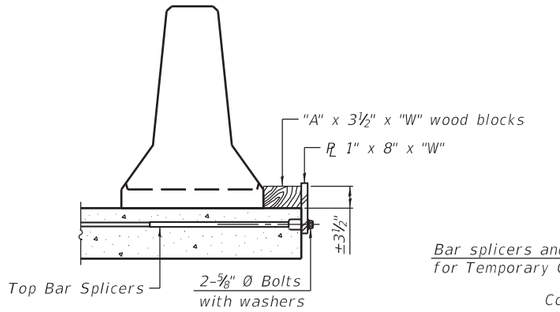
EXISTING SLAB



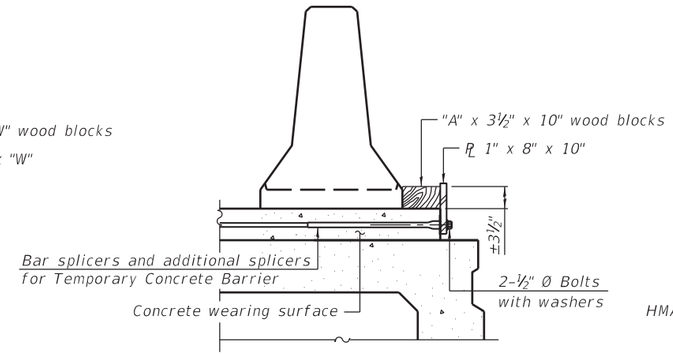
* When hot-mix asphalt wearng surface is present, embedment shall be 3" plus the wearing surface depth.

EXISTING DECK BEAM

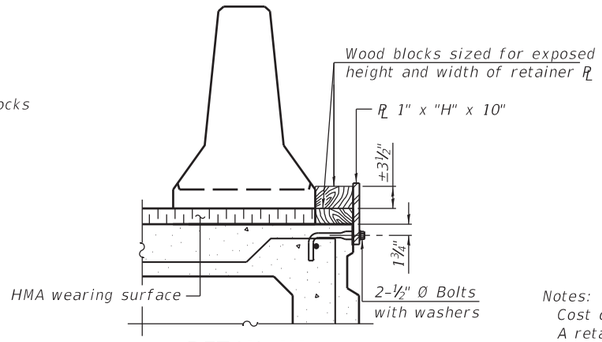
SECTIONS THRU SLAB OR DECK BEAM



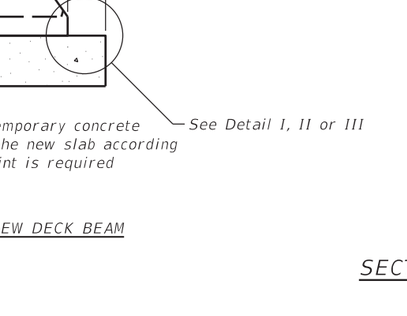
DETAIL I



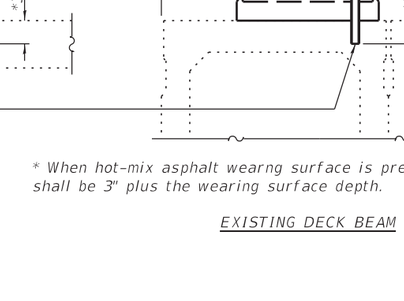
DETAIL II



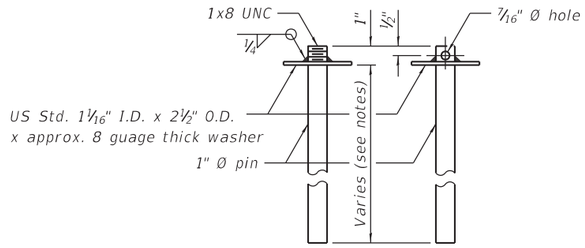
DETAIL III



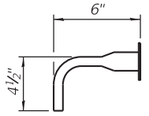
STEEL RETAINER ℄ 1" x 8" x "W"
(Detail I and II)



STEEL RETAINER ℄ 1" x "H" x 10"
(Detail III)



RESTRAINING PIN



BAR SPLICER FOR #4 BAR - DETAIL III

Notes:

Cost of retainer assembly is included with Temporary Concrete Barrier.

A retainer assembly shall be located at the approximate ℄ of each temporary concrete barrier.

The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.

When the 'A' dimension is less than 1½", the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.

Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.

Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

FILE NAME = S:\DOL\6680-6699\6628\813\Micro\CADD_Sheets\0099-4401-XXX-003-TCB.dgn

R-27

8-11-2017

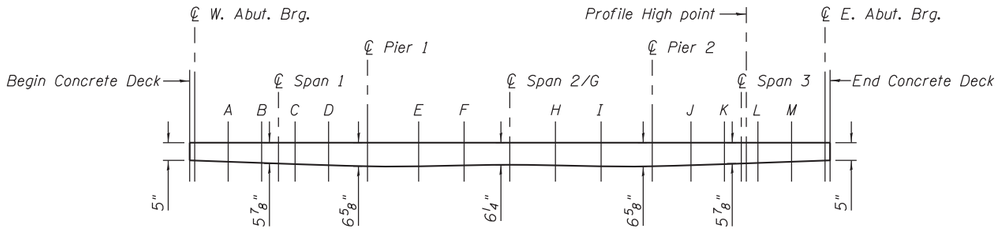
STRAND ASSOCIATES®
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = BenL	DESIGNED BRL	REVISED -
	CHECKED AJS	REVISED -
PLOT SCALE =	DRAWN BJF	REVISED -
PLOT DATE = 11/16/2017	CHECKED BRL	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION
STRUCTURE NO. 099-4401
SHEET NO. 3 OF 24 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00046-00-BR	WILL	109	51
		CONTRACT NO. 61D22		
	ILLINOIS FED. AID PROJECT			

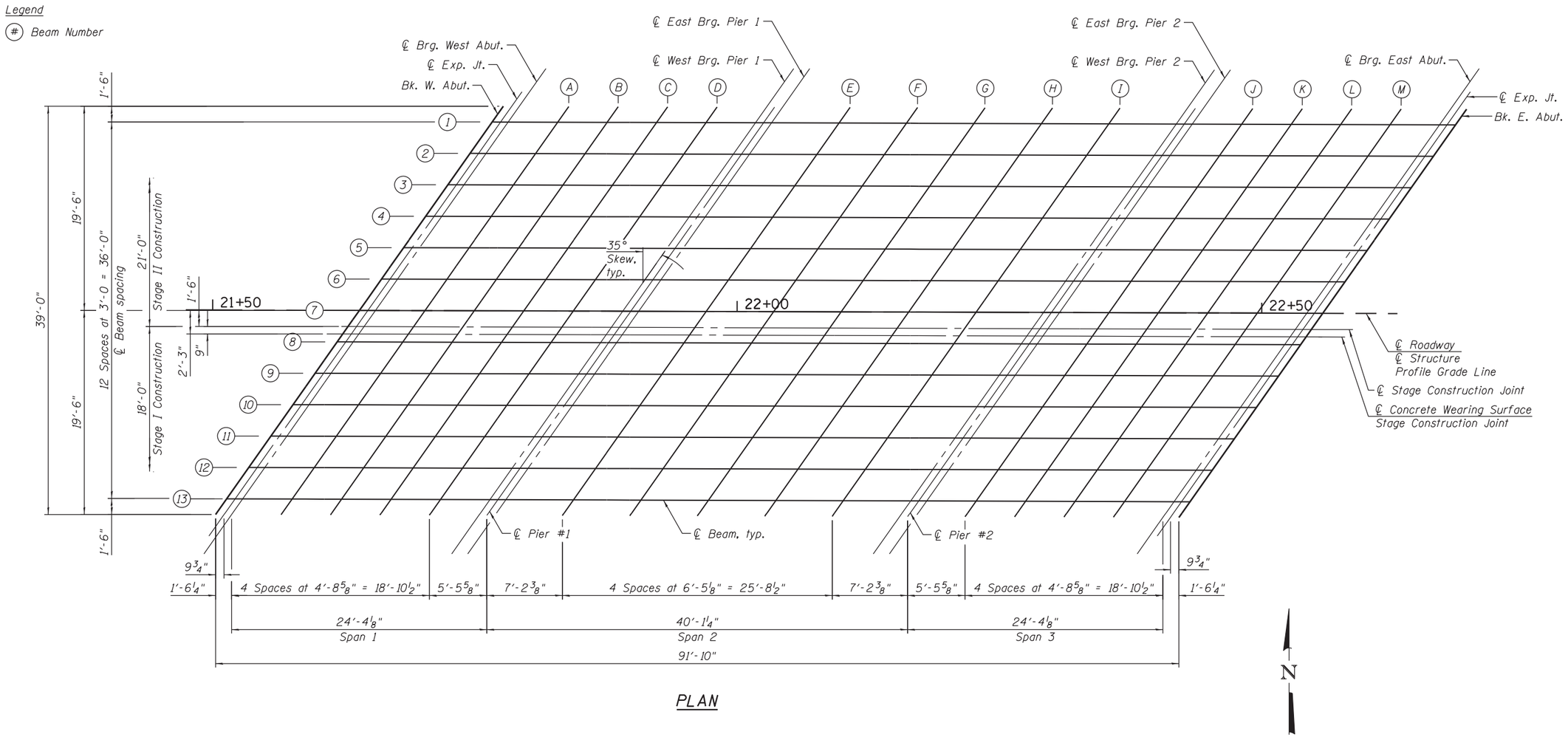


Note: Dimensions are shown at PGL line and are shown to provide profile depth at nearest ℄ Brg. ℄ Span or ℄ Pier

ANTICIPATED CONCRETE WEARING SURFACE PROFILE

(For information only)

Estimated Beam 1 & 3 Camber = 1/4"
Estimated Beam 2 Camber = 3/4"



PLAN

FILE NAME = S:\JOL\16680-6691\6828\0113\Micros\CADD_Sheets\099-4401-XXX-004-TSE.dgn

STRAND ASSOCIATES®
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = BenL	DESIGNED BRL	REVISED -
	CHECKED AJS	REVISED -
PLOT SCALE =	DRAWN BJF	REVISED -
PLOT DATE = 11/16/2017	CHECKED BRL	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS (1 OF 4)
STRUCTURE NO. 099-4401
SHEET NO. 4 OF 24 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00046-00-BR	WILL	109	52
		CONTRACT NO. 61D22		
	ILLINOIS FED. AID PROJECT			

NORTH FASCIA LINE

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Back of W. Abut.	21+77.65	-19.50	688.69	688.69
CL Brg. W. Abut.	21+79.17	-19.50	688.71	688.71
A	21+83.89	-19.50	688.76	688.76
B	21+88.60	-19.50	688.80	688.81
CL Span 1	21+90.96	-19.50	688.82	688.83
C	21+93.32	-19.50	688.84	688.85
D	21+98.03	-19.50	688.88	688.88
CL W. Brg. Pier 1	22+02.75	-19.50	688.91	688.91
CL Pier 1	22+03.51	-19.50	688.91	688.91
CL E. Brg. Pier 1	22+04.27	-19.50	688.92	688.92
E	22+10.71	-19.50	688.96	688.97
F	22+17.14	-19.50	688.99	689.01
CL Span 2/G	22+23.56	-19.50	689.01	689.03
H	22+30.00	-19.50	689.03	689.05
I	22+36.43	-19.50	689.04	689.05
CL W. Brg. Pier 2	22+42.85	-19.50	689.04	689.04
CL Pier 2	22+43.62	-19.50	689.04	689.04
CL E. Brg. Pier 2	22+44.38	-19.50	689.04	689.04
J	22+49.09	-19.50	689.04	689.05
K	22+53.81	-19.50	689.04	689.05
CL Span 3	22+56.17	-19.50	689.03	689.04
L	22+58.52	-19.50	689.03	689.04
M	22+63.24	-19.50	689.02	689.02
CL Brg. E. Abut.	22+67.95	-19.50	689.00	689.01
Back of E. Abut.	22+69.48	-19.50	688.99	688.99

BEAM 1

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Back of W. Abut.	21+76.60	-18.00	688.72	688.72
CL Brg. W. Abut.	21+78.12	-18.00	688.73	688.73
A	21+82.84	-18.00	688.78	688.78
B	21+87.55	-18.00	688.83	688.83
CL Span 1	21+89.98	-18.00	688.85	688.85
C	21+92.27	-18.00	688.87	688.87
D	21+96.98	-18.00	688.91	688.91
CL W. Brg. Pier 1	22+01.70	-18.00	688.94	688.94
CL Pier 1	22+02.46	-18.00	688.95	688.95
CL E. Brg. Pier 1	22+03.22	-18.00	688.95	688.95
E	22+09.65	-18.00	688.99	688.99
F	22+16.09	-18.00	689.02	689.03
CL Span 2/G	22+22.52	-18.00	689.05	689.06
H	22+28.95	-18.00	689.07	689.08
I	22+35.38	-18.00	689.08	689.08
CL W. Brg. Pier 2	22+41.80	-18.00	689.08	689.08
CL Pier 2	22+42.57	-18.00	689.08	689.08
CL E. Brg. Pier 2	22+43.33	-18.00	689.08	689.08
J	22+48.04	-18.00	689.08	689.08
K	22+52.76	-18.00	689.08	689.08
CL Span 3	22+55.12	-18.00	689.07	689.07
L	22+57.47	-18.00	689.07	689.07
M	22+62.19	-18.00	689.06	689.06
CL Brg. E. Abut.	22+66.90	-18.00	689.04	689.04
Back of E. Abut.	22+68.43	-18.00	689.04	689.04

BEAM 2

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Back of W. Abut.	21+74.50	-15.00	688.76	688.76
CL Brg. W. Abut.	21+76.02	-15.00	688.77	688.77
A	21+80.74	-15.00	688.82	688.82
B	21+85.45	-15.00	688.87	688.87
CL Span 1	21+87.81	-15.00	688.89	688.89
C	21+90.17	-15.00	688.91	688.91
D	21+94.88	-15.00	688.95	688.95
CL W. Brg. Pier 1	21+99.60	-15.00	688.99	688.99
CL Pier 1	22+00.36	-15.00	688.99	688.99
CL E. Brg. Pier 1	22+01.12	-15.00	689.00	689.00
E	22+07.55	-15.00	689.04	689.05
F	22+13.98	-15.00	689.07	689.08
CL Span 2/G	22+20.42	-15.00	689.10	689.11
H	22+26.85	-15.00	689.12	689.13
I	22+33.28	-15.00	689.13	689.14
CL W. Brg. Pier 2	22+39.70	-15.00	689.14	689.14
CL Pier 2	22+40.46	-15.00	689.14	689.14
CL E. Brg. Pier 2	22+41.23	-15.00	689.14	689.14
J	22+45.94	-15.00	689.14	689.14
K	22+50.66	-15.00	689.14	689.14
CL Span 3	22+53.02	-15.00	689.14	689.14
L	22+55.37	-15.00	689.13	689.13
M	22+60.09	-15.00	689.12	689.12
CL Brg. E. Abut.	22+64.80	-15.00	689.11	689.11
Back of E. Abut.	22+66.33	-15.00	689.10	689.10

BEAM 3

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Back of W. Abut.	21+72.40	-12.00	688.79	688.79
CL Brg. W. Abut.	21+73.92	-12.00	688.81	688.81
A	21+78.64	-12.00	688.86	688.86
B	21+83.35	-12.00	688.91	688.91
CL Span 1	21+85.71	-12.00	688.93	688.93
C	21+88.07	-12.00	688.95	688.95
D	21+92.78	-12.00	688.99	688.99
CL W. Brg. Pier 1	21+97.50	-12.00	689.03	689.03
CL Pier 1	21+98.26	-12.00	689.04	689.04
CL E. Brg. Pier 1	21+99.02	-12.00	689.04	689.04
E	22+05.45	-12.00	689.09	689.10
F	22+11.88	-12.00	689.12	689.13
CL Span 2/G	22+18.31	-12.00	689.15	689.17
H	22+24.75	-12.00	689.17	689.18
I	22+31.18	-12.00	689.19	689.20
CL W. Brg. Pier 2	22+37.60	-12.00	689.20	689.20
CL Pier 2	22+38.36	-12.00	689.20	689.20
CL E. Brg. Pier 2	22+39.13	-12.00	689.20	689.20
J	22+43.84	-12.00	689.20	689.20
K	22+48.56	-12.00	689.20	689.20
CL Span 3	22+50.92	-12.00	689.20	689.20
L	22+53.27	-12.00	689.20	689.20
M	22+57.99	-12.00	689.19	689.19
CL Brg. E. Abut.	22+62.70	-12.00	689.18	689.18
Back of E. Abut.	22+64.23	-12.00	689.17	689.17

BEAM 4

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Back of W. Abut.	21+70.29	-9.00	688.83	688.83
CL Brg. W. Abut.	21+71.82	-9.00	688.85	688.85
A	21+76.54	-9.00	688.90	688.90
B	21+81.25	-9.00	688.95	688.96
CL Span 1	21+83.61	-9.00	688.97	688.97
C	21+85.96	-9.00	688.99	689.00
D	21+90.68	-9.00	689.04	689.04
CL W. Brg. Pier 1	21+95.40	-9.00	689.08	689.08
CL Pier 1	21+96.16	-9.00	689.08	689.08
CL E. Brg. Pier 1	21+96.92	-9.00	689.09	689.09
E	22+03.35	-9.00	689.13	689.14
F	22+09.78	-9.00	689.17	689.19
CL Span 2/G	22+16.21	-9.00	689.20	689.22
H	22+22.64	-9.00	689.23	689.25
I	22+29.08	-9.00	689.25	689.26
CL W. Brg. Pier 2	22+35.50	-9.00	689.26	689.26
CL Pier 2	22+36.26	-9.00	689.26	689.26
CL E. Brg. Pier 2	22+37.03	-9.00	689.26	689.26
J	22+41.74	-9.00	689.26	689.26
K	22+46.46	-9.00	689.26	689.26
CL Span 3	22+48.81	-9.00	689.26	689.26
L	22+51.17	-9.00	689.26	689.26
M	22+55.88	-9.00	689.25	689.25
CL Brg. E. Abut.	22+60.60	-9.00	689.24	689.24
Back of E. Abut.	22+62.13	-9.00	689.24	689.24

BEAM 5

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Back of W. Abut.	21+68.19	-6.00	688.87	688.87
CL Brg. W. Abut.	21+69.72	-6.00	688.89	688.89
A	21+74.43	-6.00	688.94	688.94
B	21+79.15	-6.00	688.99	689.00
CL Span 1	21+81.51	-6.00	689.02	689.02
C	21+83.86	-6.00	689.04	689.04
D	21+88.58	-6.00	689.09	689.09
CL W. Brg. Pier 1	21+93.30	-6.00	689.13	689.13
CL Pier 1	21+94.06	-6.00	689.13	689.13
CL E. Brg. Pier 1	21+94.82	-6.00	689.14	689.14
E	22+01.25	-6.00	689.19	689.19
F	22+07.68	-6.00	689.23	689.24
CL Span 2/G	22+14.11	-6.00	689.26	689.27
H	22+20.54	-6.00	689.29	689.30
I	22+26.97	-6.00	689.31	689.31
CL W. Brg. Pier 2	22+33.40	-6.00	689.32	689.32
CL Pier 2	22+34.16	-6.00	689.33	689.33
CL E. Brg. Pier 2	22+34.93	-6.00	689.33	689.33
J	22+39.64	-6.00	689.33	689.33
K	22+44.36	-6.00	689.33	689.33
CL Span 3	22+46.71	-6.00	689.33	689.33
L	22+49.07	-6.00	689.33	689.33
M	22+53.78	-6.00	689.33	689.33
CL Brg. E. Abut.	22+58.50	-6.00	689.32	689.32
Back of E. Abut.	22+60.03	-6.00	689.31	689.31

FILE NAME = S:\IDL\6600-6699\6622B(B)\Micros\CADD_Sheets\0099-4401-XXX-004-TSE.dgn

STRAND ASSOCIATES®
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = BenL	DESIGNED BRL	REVISED -	
	CHECKED AJS	REVISED -	
PLOT SCALE =	DRAWN BJF	REVISED -	
PLOT DATE = 11/16/2017	CHECKED BRL	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS (2 OF 4)
STRUCTURE NO. 099-4401
SHEET NO. 5 OF 24 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00046-00-BR	WILL	109	53
		CONTRACT NO. 61D22		
	ILLINOIS FED. AID PROJECT			

BEAM 6

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Back of W. Abut.	21+66.09	-3.00	688.91	688.91
CL Brg. W. Abut.	21+67.62	-3.00	688.93	688.93
A	21+72.33	-3.00	688.98	688.98
B	21+77.05	-3.00	689.04	689.04
CL Span 1	21+79.41	-3.00	689.06	689.06
C	21+81.76	-3.00	689.08	689.08
D	21+86.48	-3.00	689.13	689.13
CL W. Brg. Pier 1	21+91.20	-3.00	689.17	689.17
CL Pier 1	21+91.96	-3.00	689.18	689.18
CL E. Brg. Pier 1	21+92.72	-3.00	689.18	689.18
E	21+99.15	-3.00	689.23	689.24
F	22+05.58	-3.00	689.28	689.29
CL Span 2/G	22+12.01	-3.00	689.31	689.32
H	22+18.44	-3.00	689.34	689.35
I	22+24.87	-3.00	689.37	689.37
CL W. Brg. Pier 2	22+31.30	-3.00	689.38	689.38
CL Pier 2	22+32.06	-3.00	689.38	689.38
CL E. Brg. Pier 2	22+32.83	-3.00	689.38	689.38
J	22+37.54	-3.00	689.39	689.39
K	22+42.25	-3.00	689.39	689.39
CL Span 3	22+44.61	-3.00	689.39	689.39
L	22+46.97	-3.00	689.39	689.39
M	22+51.68	-3.00	689.39	689.39
CL Brg. E. Abut.	22+56.40	-3.00	689.38	689.38
Back of E. Abut.	22+57.93	-3.00	689.38	689.38

℄ STRUCTURE, ℄ ROADWAY & PGL/BEAM 7

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Back of W. Abut.	21+63.99	0.00	688.94	688.94
CL Brg. W. Abut.	21+65.52	0.00	688.96	688.96
A	21+70.23	0.00	689.02	689.02
B	21+74.95	0.00	689.07	689.07
CL Span 1	21+77.31	0.00	689.10	689.10
C	21+79.66	0.00	689.12	689.12
D	21+84.38	0.00	689.17	689.17
CL W. Brg. Pier 1	21+89.09	0.00	689.21	689.21
CL Pier 1	21+89.86	0.00	689.22	689.22
CL E. Brg. Pier 1	21+90.62	0.00	689.23	689.23
E	21+97.05	0.00	689.28	689.29
F	22+03.48	0.00	689.32	689.33
CL Span 2/G	22+09.91	0.00	689.36	689.38
H	22+16.34	0.00	689.39	689.40
I	22+22.77	0.00	689.42	689.43
CL W. Brg. Pier 2	22+29.20	0.00	689.44	689.44
CL Pier 2	22+29.96	0.00	689.44	689.44
CL E. Brg. Pier 2	22+30.72	0.00	689.44	689.44
J	22+35.44	0.00	689.45	689.45
K	22+40.15	0.00	689.45	689.45
CL Span 3	22+42.51	0.00	689.45	689.45
High Point	22+44.00	0.00	689.45	689.45
L	22+44.87	0.00	689.45	689.45
M	22+49.58	0.00	689.45	689.45
CL Brg. E. Abut.	22+54.30	0.00	689.45	689.45
Back of E. Abut.	22+55.83	0.00	689.44	689.44

CONCRETE WEARING SURFACE STAGE CONSTRUCTION JOINT

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Back of W. Abut.	21+62.42	2.25	688.87	688.87
CL Brg. W. Abut.	21+62.42	2.25	688.87	688.87
A	21+63.94	2.25	688.89	688.89
B	21+68.66	2.25	688.95	688.95
CL Span 1	21+73.37	2.25	689.01	689.01
C	21+75.73	2.25	689.03	689.04
D	21+78.09	2.25	689.06	689.06
CL W. Brg. Pier 1	21+82.80	2.25	689.11	689.11
CL Pier 1	21+87.52	2.25	689.15	689.15
CL E. Brg. Pier 1	21+88.82	2.25	689.16	689.16
E	21+89.05	2.25	689.16	689.16
F	21+95.48	2.25	689.22	689.23
CL Span 2/G	22+01.91	2.25	689.26	689.28
H	22+08.34	2.25	689.30	689.32
I	22+14.77	2.25	689.34	689.36
CL W. Brg. Pier 2	22+21.20	2.25	689.36	689.37
CL Pier 2	22+27.62	2.25	689.38	689.38
CL E. Brg. Pier 2	22+28.39	2.25	689.39	689.39
J	22+29.15	2.25	689.39	689.39
K	22+33.86	2.25	689.40	689.40
CL Span 3	22+38.58	2.25	689.40	689.41
L	22+40.94	2.25	689.40	689.41
M	22+43.29	2.25	689.40	689.41
CL Brg. E. Abut.	22+48.01	2.25	689.40	689.40
Back of E. Abut.	22+54.25	2.25	689.40	689.40

BEAM 8

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Back of W. Abut.	21+61.89	3.00	688.86	688.86
CL Brg. W. Abut.	21+63.42	3.00	688.87	688.87
A	21+68.13	3.00	688.93	688.93
B	21+72.85	3.00	688.99	688.99
CL Span 1	21+75.21	3.00	689.02	689.02
C	21+77.56	3.00	689.04	689.04
D	21+82.28	3.00	689.09	689.09
CL W. Brg. Pier 1	21+86.99	3.00	689.13	689.13
CL Pier 1	21+87.76	3.00	689.14	689.14
CL E. Brg. Pier 1	21+88.52	3.00	689.15	689.15
E	21+94.95	3.00	689.20	689.20
F	22+01.38	3.00	689.25	689.26
CL Span 2/G	22+07.81	3.00	689.29	689.30
H	22+14.24	3.00	689.32	689.33
I	22+20.67	3.00	689.35	689.36
CL W. Brg. Pier 2	22+27.10	3.00	689.37	689.37
CL Pier 2	22+27.86	3.00	689.37	689.37
CL E. Brg. Pier 2	22+28.62	3.00	689.38	689.38
J	22+33.34	3.00	689.39	689.39
K	22+28.05	3.00	689.39	689.39
CL Span 3	22+40.41	3.00	689.39	689.39
L	22+42.77	3.00	689.39	689.39
M	22+47.48	3.00	689.39	689.39
CL Brg. E. Abut.	22+52.20	3.00	689.39	689.39
Back of E. Abut.	22+53.73	3.00	689.39	689.39

BEAM 9

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Back of W. Abut.	21+59.79	6.00	688.77	688.77
CL Brg. W. Abut.	21+61.32	6.00	688.79	688.79
A	21+66.03	6.00	688.84	688.84
B	21+70.75	6.00	688.90	688.90
CL Span 1	21+73.11	6.00	688.93	688.93
C	21+75.46	6.00	688.96	688.96
D	21+80.18	6.00	689.01	689.01
CL W. Brg. Pier 1	21+84.89	6.00	689.05	689.05
CL Pier 1	21+85.66	6.00	689.06	689.06
CL E. Brg. Pier 1	21+86.42	6.00	689.07	689.07
E	21+92.85	6.00	689.12	689.13
F	21+99.28	6.00	689.17	689.18
CL Span 2/G	22+05.71	6.00	689.22	689.23
H	22+12.14	6.00	689.24	689.26
I	22+18.57	6.00	689.28	689.28
CL W. Brg. Pier 2	22+25.00	6.00	689.30	689.30
CL Pier 2	22+25.76	6.00	689.31	689.31
CL E. Brg. Pier 2	22+26.52	6.00	689.31	689.31
J	22+31.21	6.00	689.32	689.32
K	22+35.95	6.00	689.33	689.33
CL Span 3	22+38.31	6.00	689.33	689.33
L	22+40.67	6.00	689.33	689.33
M	22+45.38	6.00	689.33	689.33
CL Brg. E. Abut.	22+50.40	6.00	689.33	689.33
Back of E. Abut.	22+51.63	6.00	689.33	689.33

BEAM 10

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Back of W. Abut.	21+57.69	9.00	688.67	688.67
CL Brg. W. Abut.	21+59.22	9.00	688.69	688.69
A	21+63.93	9.00	688.75	688.75
B	21+68.65	9.00	688.81	688.81
CL Span 1	21+71.00	9.00	688.84	688.84
C	21+73.36	9.00	688.87	688.87
D	21+78.08	9.00	688.92	688.92
CL W. Brg. Pier 1	21+82.79	9.00	688.96	688.96
CL Pier 1	21+83.56	9.00	688.93	688.93
CL E. Brg. Pier 1	21+84.32	9.00	688.98	688.98
E	21+90.75	9.00	689.04	689.05
F	21+97.18	9.00	689.09	689.11
CL Span 2/G	22+03.61	9.00	689.14	689.15
H	22+10.04	9.00	689.17	689.19
I	22+16.47	9.00	689.20	689.21
CL W. Brg. Pier 2	22+22.90	9.00	689.23	689.23
CL Pier 2	22+23.66	9.00	689.23	689.23
CL E. Brg. Pier 2	22+24.42	9.00	689.23	689.23
J	22+29.14	9.00	689.25	689.25
K	22+33.85	9.00	689.26	689.26
CL Span 3	22+36.21	9.00	689.26	689.26
L	22+38.57	9.00	689.26	689.26
M	22+43.38	9.00	689.26	689.26
CL Brg. E. Abut.	22+48.00	9.00	689.26	689.26
Back of E. Abut.	22+49.52	9.00	689.26	689.26

FILE NAME = S:\DOL\6600-6699\6622B(B)\Micros\CADD_Sheets\0099-4401-XXX-004-TSE.dgn

STRAND ASSOCIATES®
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = BenL	DESIGNED BRL	REVISED -	
	CHECKED AJS	REVISED -	
PLOT SCALE =	DRAWN BJF	REVISED -	
PLOT DATE = 11/16/2017	CHECKED BRL	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS (3 OF 4)
STRUCTURE NO. 099-4401
SHEET NO. 6 OF 24 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00046-00-BR	WILL	109	54
		CONTRACT NO. 61D22		
	ILLINOIS	FED. AID PROJECT		

BEAM 11

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Back of W. Abut.	21+55.59	12.00	688.59	688.59
CL Brg. W. Abut.	21+57.12	12.00	688.61	688.61
A	21+61.83	12.00	688.67	688.67
B	21+66.55	12.00	688.72	688.72
CL Span 1	21+68.90	12.00	688.75	688.75
C	21+71.26	12.00	688.78	688.78
D	21+75.97	12.00	688.84	688.84
CL W. Brg. Pier 1	21+80.69	12.00	688.89	688.89
CL Pier 1	21+81.46	12.00	688.89	688.89
CL E. Brg. Pier 1	21+82.22	12.00	688.90	688.90
E	21+88.65	12.00	688.96	688.97
F	21+95.08	12.00	689.01	689.02
CL Span 2/G	22+01.51	12.00	689.06	689.08
H	22+07.94	12.00	689.10	689.11
I	22+14.37	12.00	689.13	689.14
CL W. Brg. Pier 2	22+20.80	12.00	689.16	689.16
CL Pier 2	22+21.56	12.00	689.16	689.16
CL E. Brg. Pier 2	22+22.32	12.00	689.17	689.17
J	22+27.04	12.00	689.18	689.18
K	22+31.75	12.00	689.19	689.19
CL Span 3	22+34.11	12.00	689.20	689.20
L	22+36.47	12.00	689.20	689.20
M	22+41.18	12.00	689.20	689.20
CL Brg. E. Abut.	22+45.90	12.00	689.20	689.20
Back of E. Abut.	22+47.42	12.00	689.20	689.20

BEAM 12

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Back of W. Abut.	21+53.49	15.00	688.50	688.50
CL Brg. W. Abut.	21+55.02	15.00	688.52	688.52
A	21+59.73	15.00	688.58	688.58
B	21+64.45	15.00	688.64	688.64
CL Span 1	21+66.80	15.00	688.66	688.66
C	21+69.16	15.00	688.70	688.70
D	21+73.87	15.00	688.75	688.75
CL W. Brg. Pier 1	21+78.59	15.00	688.80	688.80
CL Pier 1	21+79.35	15.00	688.81	688.81
CL E. Brg. Pier 1	21+80.12	15.00	688.82	688.82
E	21+86.55	15.00	688.88	688.89
F	21+92.98	15.00	688.94	688.95
CL Span 2/G	21+99.41	15.00	688.99	689.00
H	22+05.84	15.00	689.03	689.04
I	22+12.27	15.00	689.06	689.07
CL W. Brg. Pier 2	22+18.70	15.00	689.09	689.09
CL Pier 2	22+19.46	15.00	689.10	689.10
CL E. Brg. Pier 2	22+20.22	15.00	689.10	689.10
J	22+24.94	15.00	689.11	689.12
K	22+29.65	15.00	689.13	689.13
CL Span 3	22+32.01	15.00	689.13	689.13
L	22+34.37	15.00	689.14	689.14
M	22+39.80	15.00	689.14	689.14
CL Brg. E. Abut.	22+43.80	15.00	689.14	689.14
Back of E. Abut.	22+45.32	15.00	689.14	689.14

BEAM 13

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Back of W. Abut.	21+51.39	18.00	688.41	688.41
CL Brg. W. Abut.	21+52.92	18.00	688.43	688.43
A	21+57.63	18.00	688.49	688.49
B	21+62.34	18.00	688.55	688.55
CL Span 1	21+64.70	18.00	688.58	688.58
C	21+67.06	18.00	688.61	688.61
D	21+71.77	18.00	688.66	688.66
CL W. Brg. Pier 1	21+76.49	18.00	688.72	688.72
CL Pier 1	21+77.25	18.00	688.72	688.72
CL E. Brg. Pier 1	21+78.02	18.00	688.73	688.73
E	21+84.44	18.00	688.80	688.80
F	21+90.87	18.00	688.86	688.87
CL Span 2/G	21+97.31	18.00	688.91	688.92
H	22+03.74	18.00	688.95	688.97
I	22+10.17	18.00	688.99	688.99
CL W. Brg. Pier 2	22+16.59	18.00	689.02	689.02
CL Pier 2	22+17.36	18.00	689.03	689.03
CL E. Brg. Pier 2	22+18.12	18.00	689.03	689.03
J	22+22.84	18.00	689.05	689.05
K	22+27.55	18.00	689.06	689.06
CL Span 3	22+29.91	18.00	689.06	689.07
L	22+32.26	18.00	689.07	689.07
M	22+36.98	18.00	689.08	689.08
CL Brg. E. Abut.	22+41.70	18.00	689.08	689.08
Back of E. Abut.	22+43.22	18.00	689.08	689.08

SOUTH FASCIA LINE

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Back of W. Abut.	21+50.34	19.50	688.36	688.36
CL Brg. W. Abut.	21+51.86	19.50	688.38	688.38
A	21+56.58	19.50	688.44	688.44
B	21+61.29	19.50	688.50	688.51
CL Span 1	21+63.65	19.50	688.53	688.54
C	21+66.01	19.50	688.56	688.56
D	21+70.72	19.50	688.62	688.62
CL W. Brg. Pier 1	21+75.44	19.50	688.67	688.67
CL Pier 1	21+76.20	19.50	688.68	688.68
CL E. Brg. Pier 1	21+76.97	19.50	688.68	688.68
E	21+83.40	19.50	688.75	688.76
F	21+89.83	19.50	688.81	688.83
CL Span 2/G	21+96.26	19.50	688.86	688.88
H	22+02.39	19.50	688.91	688.93
I	22+09.12	19.50	688.95	688.96
CL W. Brg. Pier 2	22+15.54	19.50	688.98	688.98
CL Pier 2	22+16.31	19.50	688.98	688.98
CL E. Brg. Pier 2	22+17.07	19.50	688.99	688.99
J	22+21.79	19.50	689.00	689.01
K	22+26.50	19.50	689.02	689.03
CL Span 3	22+28.86	19.50	689.04	689.04
L	22+31.21	19.50	689.03	689.04
M	22+35.93	19.50	689.04	689.05
CL Brg. E. Abut.	22+40.56	19.50	689.04	689.05
Back of E. Abut.	22+42.17	19.50	689.04	689.04

FILE NAME = S:\JOL\6688-6699\6628\013\Micros\CADD_Sheets\099-4401-XXX-004-TSE.dgn

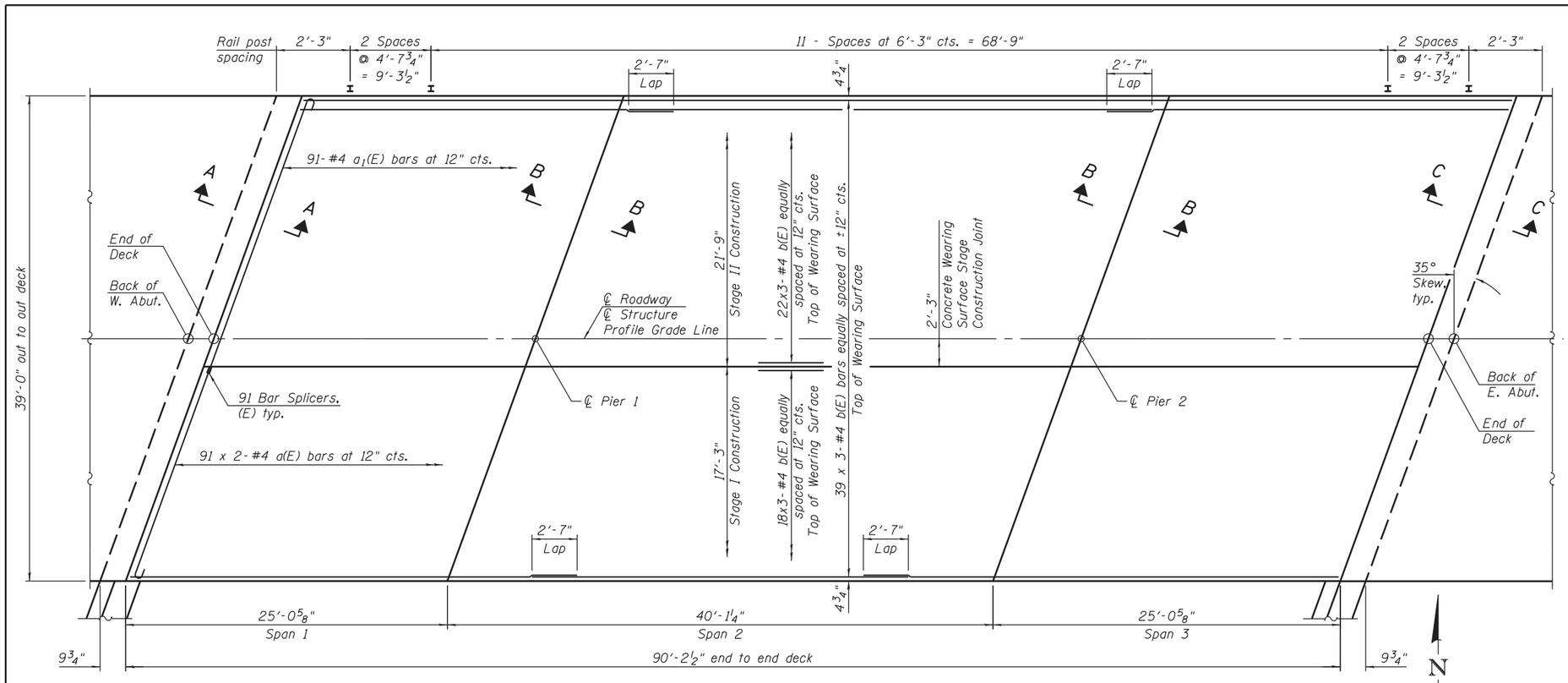
STRAND ASSOCIATES®
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = BenL	DESIGNED BRL	REVISED -	
	CHECKED AJS	REVISED -	
PLOT SCALE =	DRAWN BJF	REVISED -	
PLOT DATE = 11/16/2017	CHECKED BRL	REVISED -	

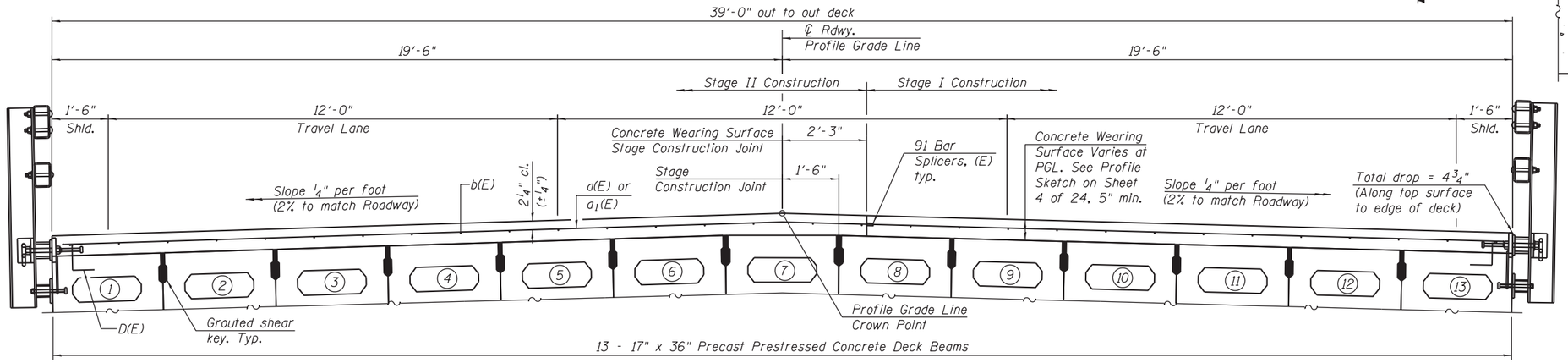
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS (4 OF 4)
STRUCTURE NO. 099-4401
SHEET NO. 7 OF 24 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00046-00-BR	WILL	109	55
		CONTRACT NO. 61D22		
	ILLINOIS FED. AID PROJECT			



PLAN
(Pedestrian Truss Not Shown)

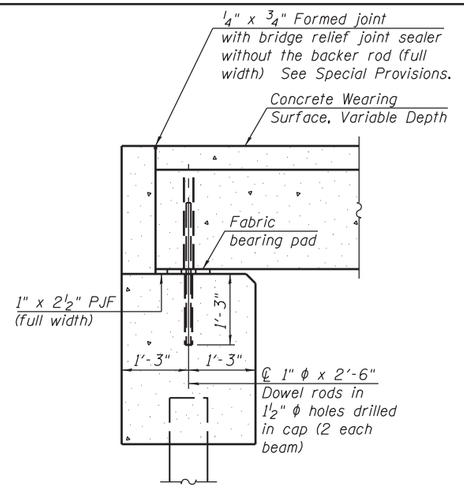


CROSS SECTION
(Looking East)

Legend

(#) Beam Number

Notes:
See Sheet 9 of 24 for Superstructure Details and Bill of Material.
Bars indicated thus 18 x 3-#4 etc. indicates 18 lines of bars with 3 lengths per line.
Spacing of d(E) bars shall be measured along the ℄ of structure.



SECTION A-A & C-C
(Dimensions are at Rt. L's)
(Section C-C is opposite hand)

SECTION B-B
(Dimensions are at Rt. L's)

*1" Jt. shall be filled with non-shrink grout. 1" dimension may vary to accomodate tolerance in beam lengths.

Notes:
All concrete wearing surfaces shall be placed prior to casting a backwall.
See Sheet 13 of 24 for fabric bearing pad details.

MINIMUM BAR LAP
#4 bar = 2'-7"

FILE NAME = SA:\IDL\6688-6699\6628\013\Micros\CADD_Sheets\099-4401-XXX-008-SUPER.dgn

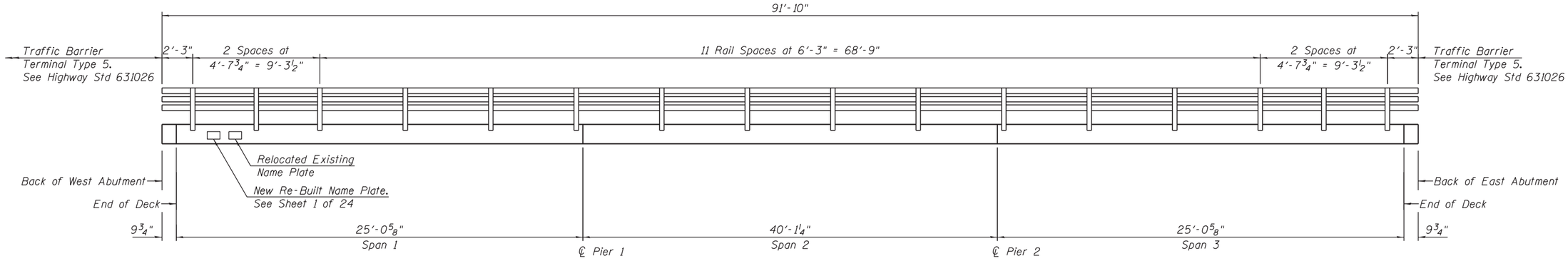
STRAND ASSOCIATES®
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = BenL	DESIGNED BRL	REVISED -
	CHECKED AJS	REVISED -
PLOT SCALE =	DRAWN BJF	REVISED -
PLOT DATE = 11/16/2017	CHECKED BRL	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

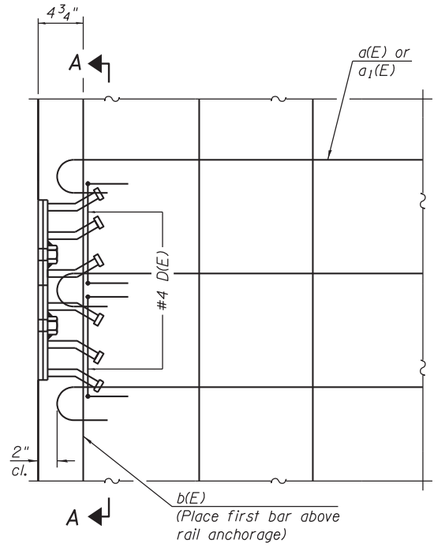
SUPERSTRUCTURE
STRUCTURE NO. 099-4401
SHEET NO. 8 OF 24 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00046-00-BR	WILL	109	56
		CONTRACT NO. 61D22		
	ILLINOIS FED. AID PROJECT			



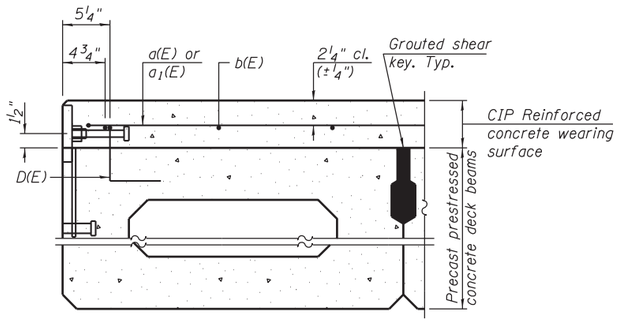
ELEVATION

Notes:
See Sheet 10 of 24 for railing details.

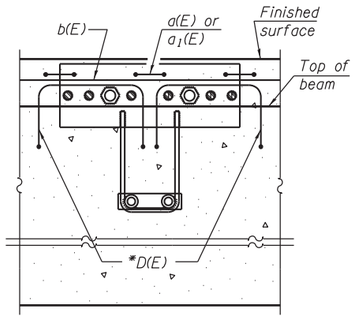


PLAN

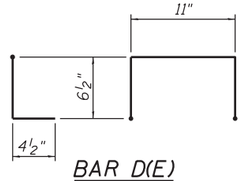
Notes:
Formwork necessary for the wearing surface may be secured utilizing the bottom rail anchorage inserts and/or additional inserts cast into the beam.



SECTION THRU FASCIA BEAM

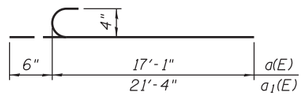


SECTION A-A



BAR D(E)

* Place 2-#4 D(E) bars in beam at each post location as shown. D(E) bar included in cost of beam.



BAR a(E) & a1(E)

MINIMUM BAR LAP

#4 bar = 2'-7"

SUPERSTRUCTURE BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	91	#4	17'-1"	C
a1(E)	91	#4	21'-4"	C
b(E)	120	#4	31'-9"	—
Bridge Deck Grooving			Sq. Yd.	398
Protective Coat			Sq. Yd.	398
Reinforcement Bars, Epoxy Coated			Pound	5,080
Bar Splicers			Each	91
Concrete Wearing Surface, (Variable Depth)			Sq. Yd.	391

See Sheet 22 of 24 for Mechanical Splicer Details.

FILE NAME = S:\JOL\6680--6699\6628\813\Micros\CADD\Sheets\6699-4401-XXX-089-SUPDET.dgn

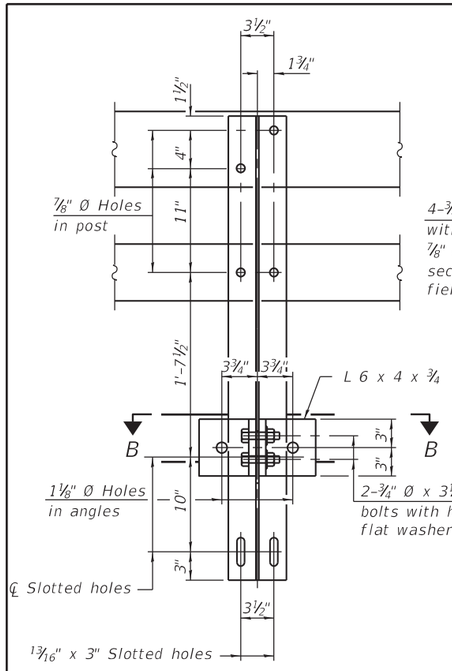
STRAND ASSOCIATES®
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = BenL	DESIGNED BRL	REVISED -
	CHECKED AJS	REVISED -
PLOT SCALE =	DRAWN BJF	REVISED -
PLOT DATE = 11/16/2017	CHECKED BRL	REVISED -

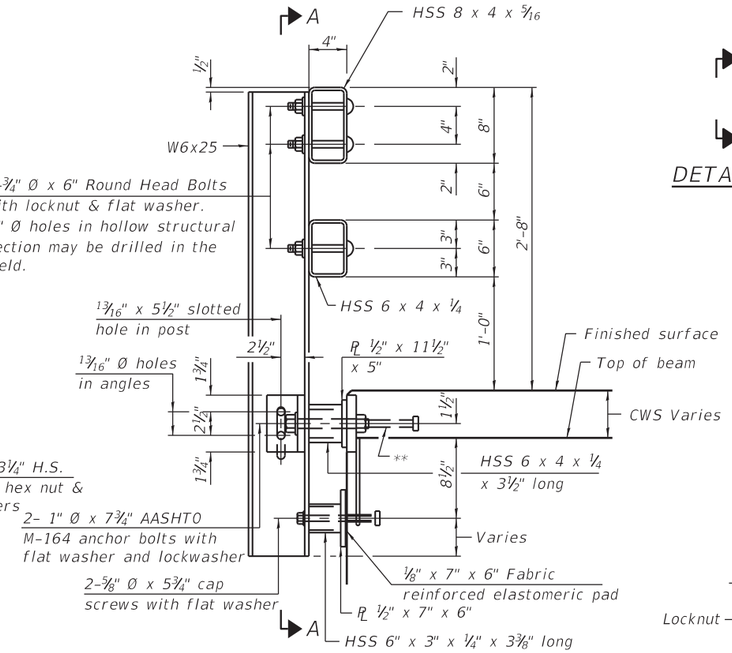
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE DETAILS
STRUCTURE NO. 099-4401
SHEET NO. 9 OF 24 SHEETS

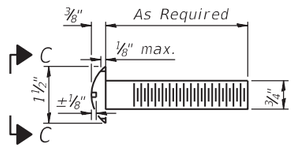
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00046-00-BR	WILL	109	57
		CONTRACT NO. 61D22		
	ILLINOIS FED. AID PROJECT			



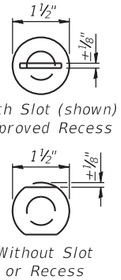
SECTION A-A



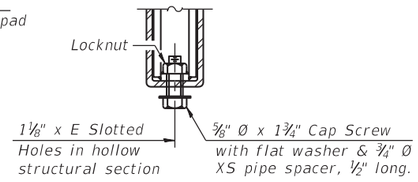
SECTION AT RAIL POST



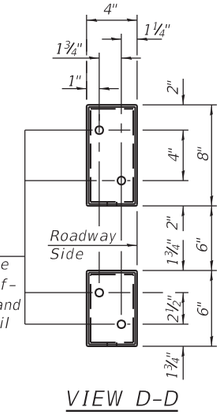
DETAIL OF ¾" Ø ROUND HEAD BOLT



VIEW C-C

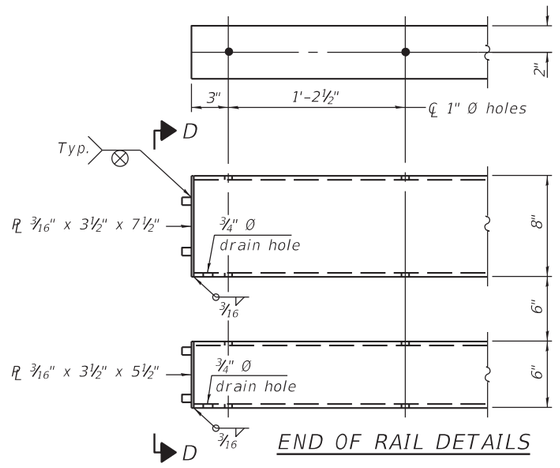


RAIL SPLICE CONNECTION AT EXPANSION JT.

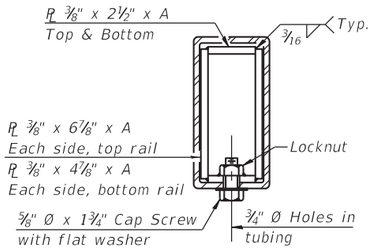


℄ -⅝" reduced base welded studs. Provide 4-⅝" washers and self-locking nuts or nuts and jam nuts for guardrail connection shown on Std. 631032 .

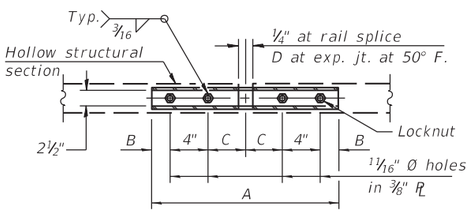
VIEW D-D



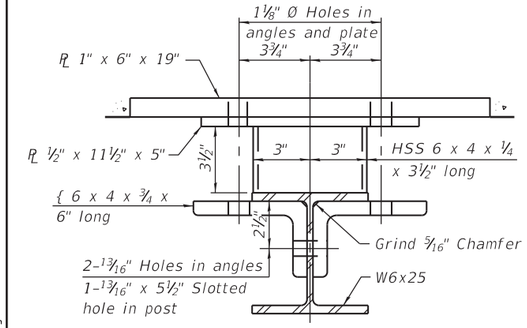
END OF RAIL DETAILS



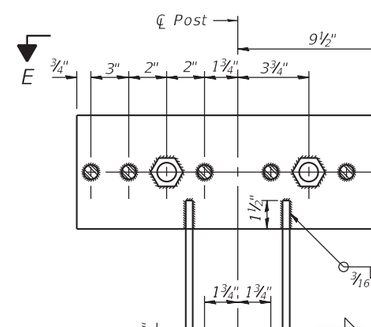
SECTION AT RAIL SPLICE



PLAN-BOTT. SPLICE PL TYPICAL

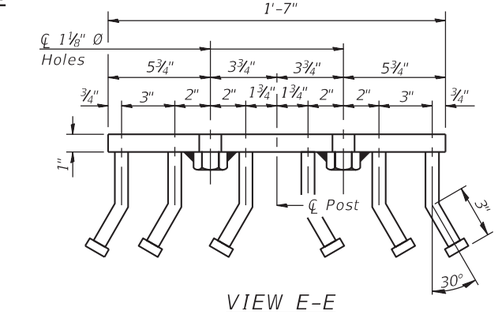


SECTION B-B

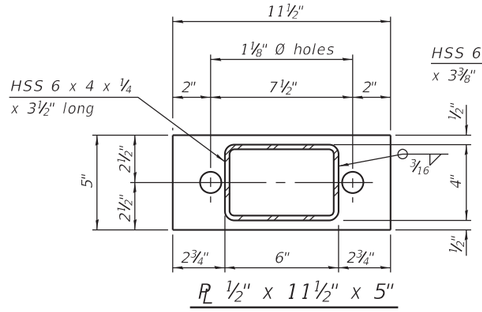


ANCHOR DEVICE

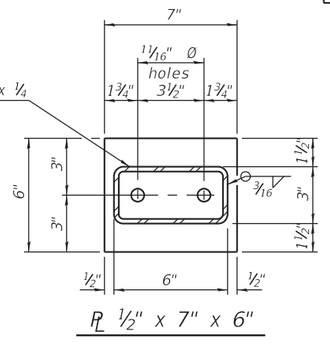
*Threaded areas shall be plugged or blocked off during casting of beam. Galvanized after fabrication.



VIEW E-E



PL ½" x 11½" x 5"



PL ½" x 7" x 6"

Notes:

For multi-span bridges, sufficient ¼" x 6" x 1'-2" galvanized steel shims shall be provided to align rail between adjacent spans.

Cost included with Steel Railing, Type SM.

Steel rail elements shall be galvanized according to Article 509.05 of the Standard Specifications.

** The studs of the anchor devices shall be placed below the top reinforcement bars and the outermost longitudinal reinforcement bar shall be placed directly above the studs of the rail post anchor device.

SPLICE DIMENSIONS

T	D	A	B	C	E
≤ 4"	2½"	1'-8"	2"	4"	2½"
> 4" ≤ 6½"	3¾"	2'-0"	2½"	5½"	3½"
> 6½" ≤ 9"	5"	2'-4"	3½"	6½"	9"
> 9" ≤ 13"	7"	2'-10"	4½"	8½"	11"
Rail Splice	¼"	1'-8"	2"	4"	———

T = Total movement at expansion joint as shown on the design plans.

BILL OF MATERIAL

Item	Unit	Quantity
Steel Railing, Type SM	Foot	184

R-34CWS 8-11-2017 (6'-3" Maximum Post Spacing) (5" minimum to 7⅛" maximum CWS thickness)

FILE NAME = S:\JOL\6600-6699\6628\013\Micros\CADD\Sheets\099-4401-XXX-010-RAIL.dgn

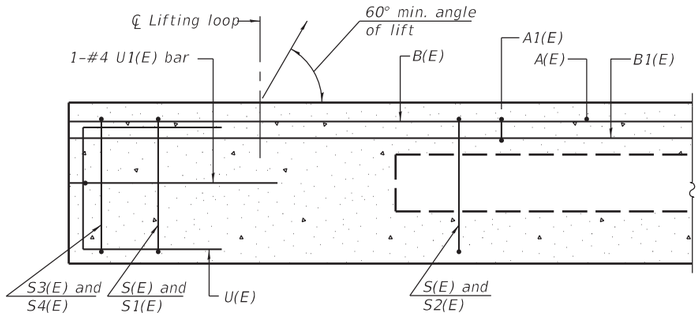
STRAND ASSOCIATES®
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = BenL	DESIGNED BRL	REVISED -	
	CHECKED AJS	REVISED -	
PLOT SCALE =	DRAWN BJF	REVISED -	
PLOT DATE = 11/16/2017	CHECKED BRL	REVISED -	

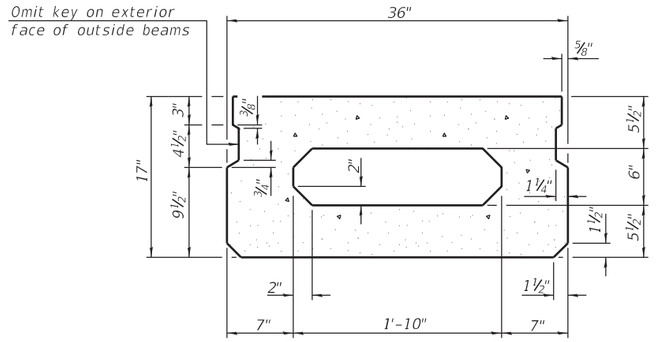
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STEEL RAILING, TYPE SM WITH CONCRETE WEARING SURFACE
STRUCTURE NO. 099-4401
SHEET NO. 10 OF 24 SHEETS

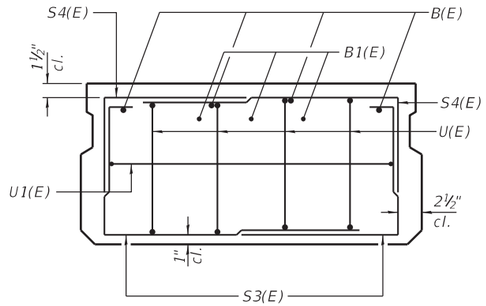
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00046-00-BR	WILL	109	58
		CONTRACT NO. 61D22		
	ILLINOIS FED. AID PROJECT			



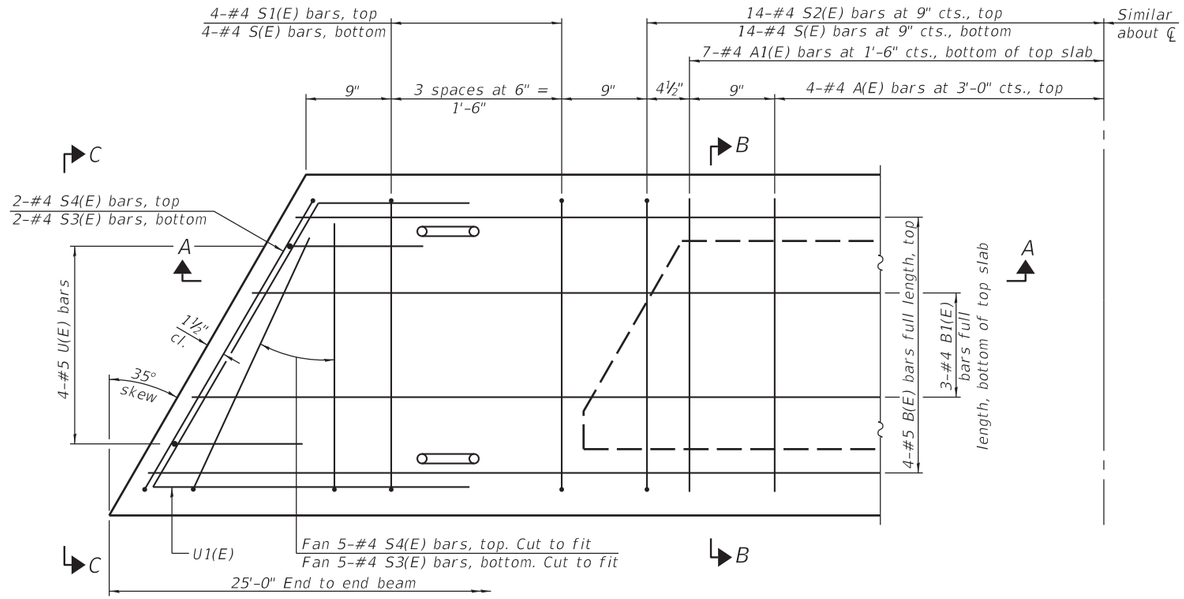
SECTION A-A



SECTION B-B
(Showing dimensions)



VIEW C-C



PLAN VIEW

Note: Spacing of S(E) and S2(E) bars may be adjusted up to 4" in the immediate area of the transverse tie diaphragms to miss the block outs for the transverse ties.

SECTION B-B
(Showing reinforcement and permissible strand locations)

Note: Place the number of strands specified in each row symmetrically about the centerline of beam in the permissible strand locations shown.

MINIMUM BAR LAP
#4 bar = 1'-11"
#5 bar = 2'-6"

BAR LIST
ONE BEAM ONLY
(For information only)

Bar	No.	Size	Length	Shape
A(E)	8	#4	2'-7"	—
A1(E)	14	#4	2'-10"	⌒
B(E)	4	#5	24'-8"	—
B1(E)	3	#4	24'-8"	—
S(E)	36	#4	5'-9"	⊔
S1(E)	8	#4	4'-3"	⊓
S2(E)	28	#4	4'-6"	⊓
S3(E)	12	#4	4'-10"	⊔
S4(E)	12	#4	4'-1"	⊔
U(E)	8	#5	3'-8"	⊏
U1(E)	2	#4	7'-8"	⊏

Note: See sheet 13 of 24 for additional details and Bill of Material.

FILE NAME = S:\JOL\6688-6699\6628\813\Micros\CADD\Sheets\6999-4401-XXX-811-BEAM1.dgn

PD-1736-L 2-17-2017

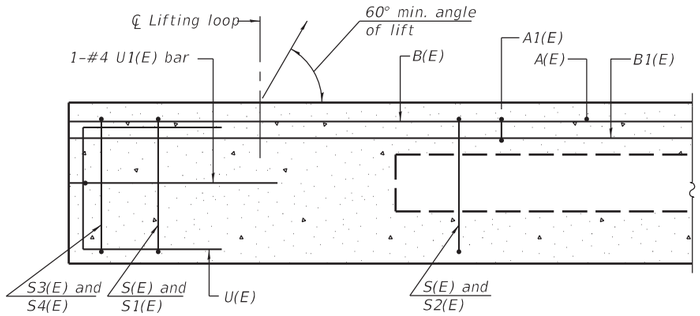
STRAND ASSOCIATES®
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = BenL	DESIGNED BRL	REVISED -
	CHECKED AJS	REVISED -
PLOT SCALE =	DRAWN BJF	REVISED -
PLOT DATE = 11/16/2017	CHECKED BRL	REVISED -

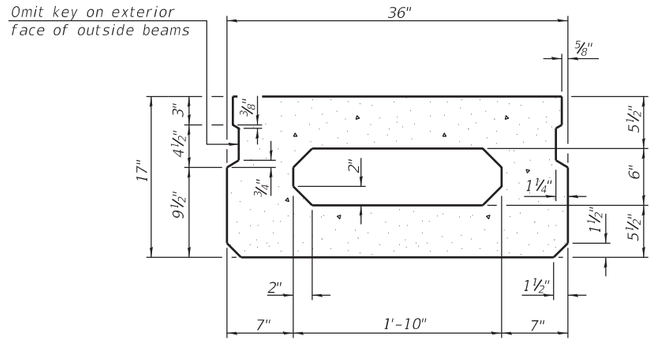
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

17" x 36" PPC DECK BEAM (SPAN 1 & 3)
STRUCTURE NO. 099-4401
SHEET NO. 11 OF 24 SHEETS

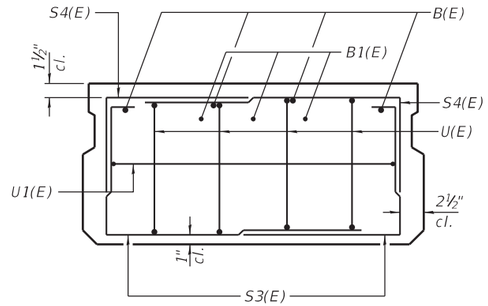
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00046-00-BR	WILL	109	59
		CONTRACT NO. 61D22		
	ILLINOIS FED. AID PROJECT			



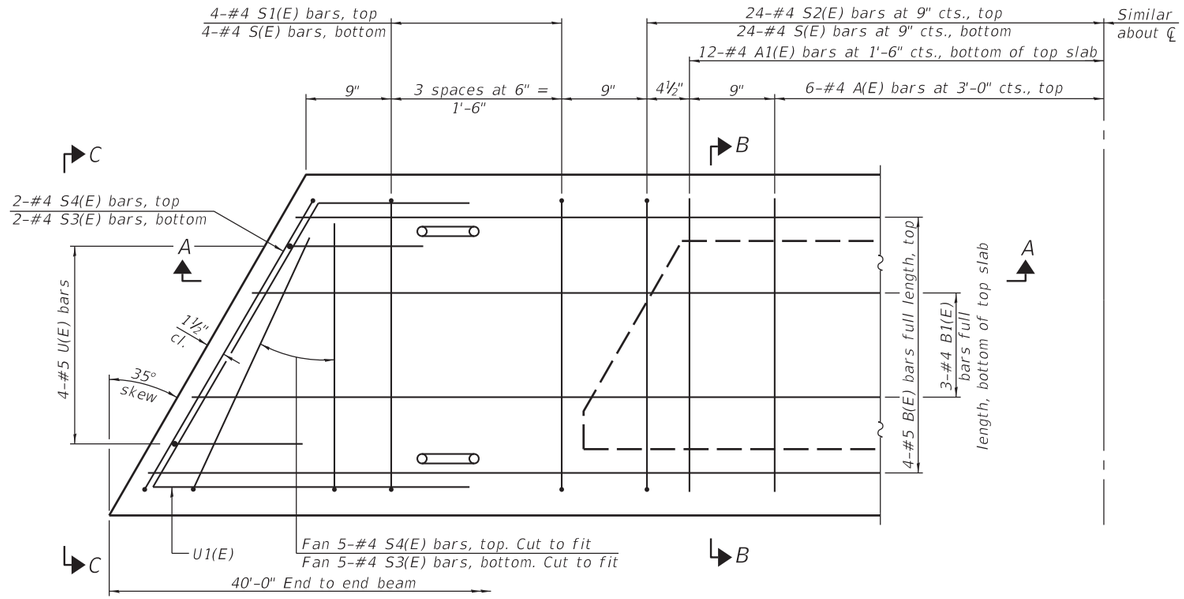
SECTION A-A



SECTION B-B
(Showing dimensions)



VIEW C-C



PLAN VIEW

Note: Spacing of S(E) and S2(E) bars may be adjusted up to 4" in the immediate area of the transverse tie diaphragms to miss the block outs for the transverse ties.

SECTION B-B
(Showing reinforcement and permissible strand locations)

Note: Place the number of strands specified in each row symmetrically about the centerline of beam in the permissible strand locations shown.

MINIMUM BAR LAP
#4 bar = 1'-11"
#5 bar = 2'-6"

BAR LIST
ONE BEAM ONLY
(For information only)

Bar	No.	Size	Length	Shape
A(E)	12	#4	2'-7"	—
A1(E)	24	#4	2'-10"	⌒
B(E)	4	#5	38'-9"	—
B1(E)	3	#4	39'-8"	—
S(E)	56	#4	5'-9"	⊔
S1(E)	8	#4	4'-3"	⊓
S2(E)	48	#4	4'-6"	⊓
S3(E)	12	#4	4'-10"	⊔
S4(E)	12	#4	4'-1"	⊔
U(E)	8	#5	3'-8"	⊏
U1(E)	2	#4	7'-8"	⊏

Note: See sheet 13 of 24 for additional details and Bill of Material.

FILE NAME = S:\JOL\6688-6699\6628\813\Micros\CADD\Sheets\6999-4401-XXX-812-BEAM2.dgn

PD-1736-L 2-17-2017

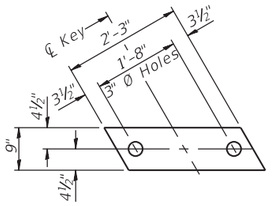
STRAND ASSOCIATES®
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = BenL	DESIGNED BRL	REVISED -
	CHECKED AJS	REVISED -
PLOT SCALE =	DRAWN BJF	REVISED -
PLOT DATE = 11/16/2017	CHECKED BRL	REVISED -

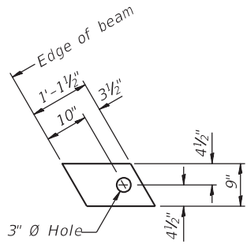
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

17" x 36" PPC DECK BEAM (SPAN 2)
STRUCTURE NO. 099-4401
SHEET NO. 12 OF 24 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00046-00-BR	WILL	109	60
		CONTRACT NO. 61D22		
	ILLINOIS FED. AID PROJECT			

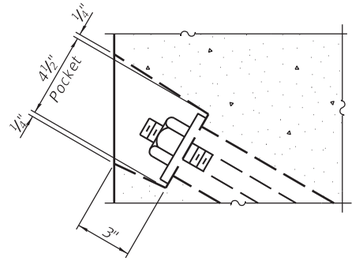


FABRIC BEARING PAD
(Interior)



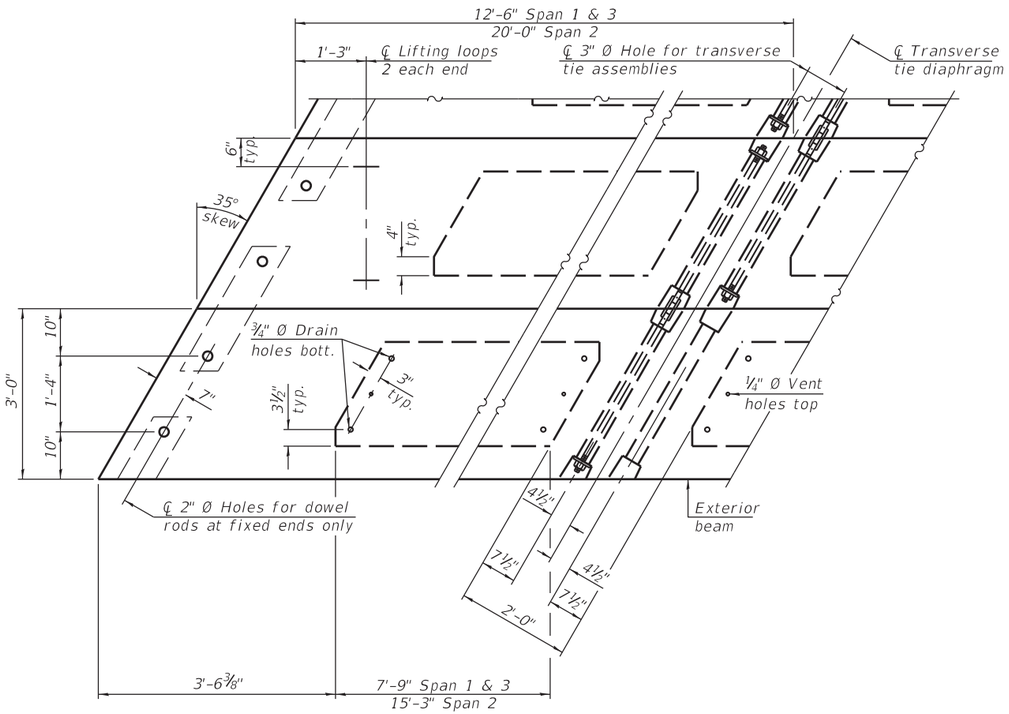
FABRIC BEARING PAD
(Exterior)

FIXED



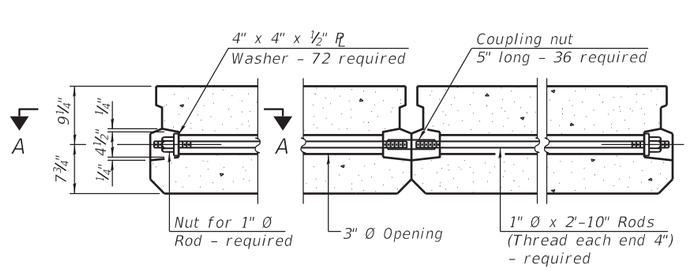
SECTION A-A

Notes:
All bearing pads shall be 1" thick.
Omit holes when using expansion bearings.
Expansion bearing pad shall be bonded to the substructure.

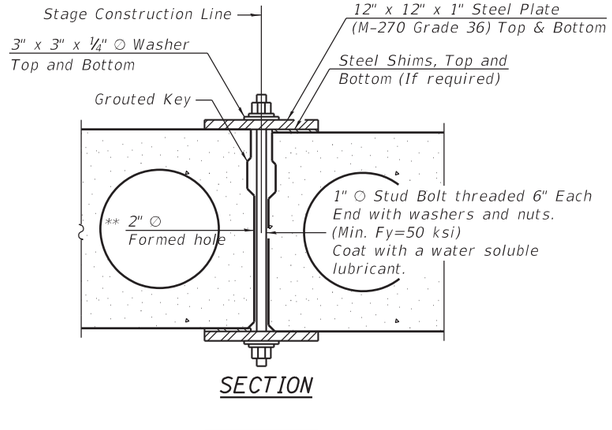


PLAN VIEW

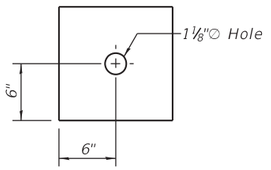
Note: Connect beams in pairs with the transverse tie configuration shown.



TYPICAL TRANSVERSE TIE ASSEMBLY



SECTION

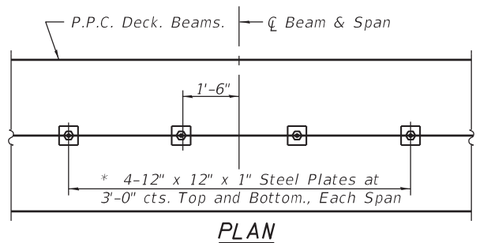


CLAMPING PLATE

SHEAR KEY CLAMPING DETAILS AT STAGE CONST. JT.

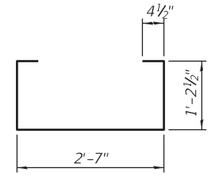
Cost included with Precast Prestressed Concrete Deck Beams.
See Stage Construction Details for traffic lanes.

** Cast semicircular recesses in the sides of each beam adjacent to the stage construction line. These recesses should align to form a hole at the appropriate locations for the clamping device bolts.

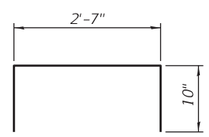


PLAN

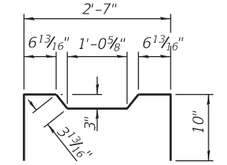
*Space plates to miss Temporary Bridge Rail Posts.



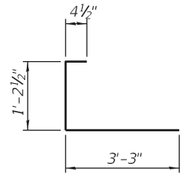
BAR S(E)



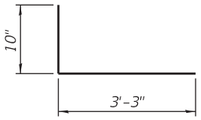
BAR S1(E)



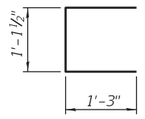
BAR S2(E)



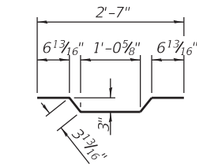
BAR S3(E)



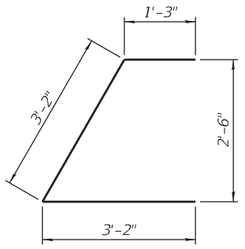
BAR S4(E)



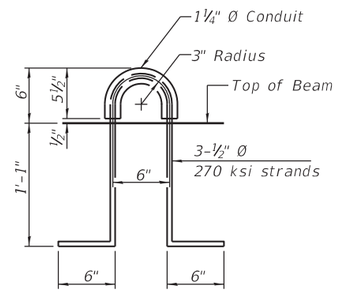
BAR U(E)



BAR A1(E)



BAR U1(E)



LIFTING LOOP DETAIL

BILL OF MATERIAL

Precast Prestressed Conc. Deck Bms. (17" depth)	Sq. Ft.	3,519

NOTES

Prestressing steel shall be uncoated high strength, low relaxation 7-wire strand, Grade 270. The nominal diameter shall be ½" and the nominal cross-sectional area shall be 0.153 sq. in.

The 1" Ø rods in the transverse tie assembly shall be tightened to a snug fit and the threads set. Pockets on exterior faces of bridge shall be filled with grout after transverse tie assembly is in place.

Two ⅛" fabric adjusting shims of the dimensions of the exterior bearing pad shall be provided for each bearing pad location.

A minimum 2½" Ø lifting pin shall be used to engage the lifting loops during handling.

Corrosion Inhibitor, per Article 1020.05(b)(10) and 1021.07 of the Standard Specifications, shall be used in the concrete for precast prestressed concrete deck beams.

Compressive strength of prestressed concrete, f'c, shall be 6000 psi.

Compressive strength of prestressed concrete at release, f'ci, shall be 5000 psi.

PD-1736-LD 2-17-2017

FILE NAME = S:\JOL\6608-6699\6628\013\Micros\CADD.Sheets\099-4401-XXX-013-BEAM3.dgn

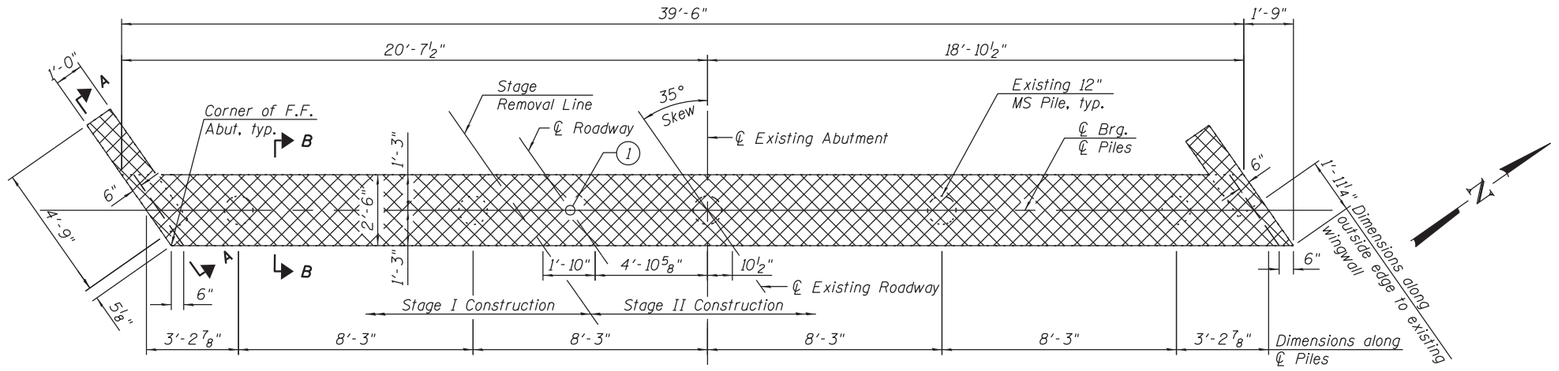
STRAND ASSOCIATES®
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = BenL	DESIGNED BRL	REVISED -	
	CHECKED AJS	REVISED -	
PLOT SCALE =	DRAWN BJF	REVISED -	
PLOT DATE = 11/16/2017	CHECKED BRL	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

17" x 36" PPC DECK BEAM DETAILS
STRUCTURE NO. 099-4401
SHEET NO. 13 OF 24 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00046-00-BR	WILL	109	61
		CONTRACT NO. 61D22		
	ILLINOIS FED. AID PROJECT			



PLAN AT ABUTMENT

(West Abut. shown, East Abut Similar)

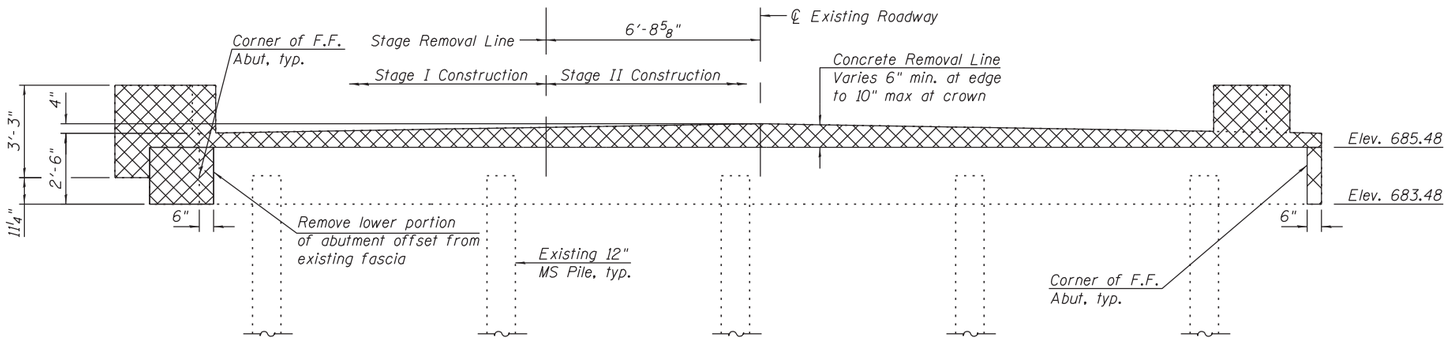
(1) Sta 21+65.52 West (Sta. 22+54.30 East)

TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Concrete Removal	Cu Yd	6.0
Epoxy Crack Injection	Foot	4.0
Structural Repair of Concrete (Depth Equal to or Less than 5 inches)	Sq Ft	3.0

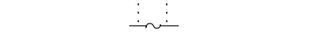
Existing reinforcement shall be cleaned and incorporated into the new construction. Cost included with Concrete Removal.

If existing reinforcement is damaged or cut during concrete removal, provide new dowels to match.

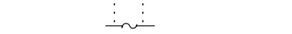


WEST ABUTMENT ELEVATION

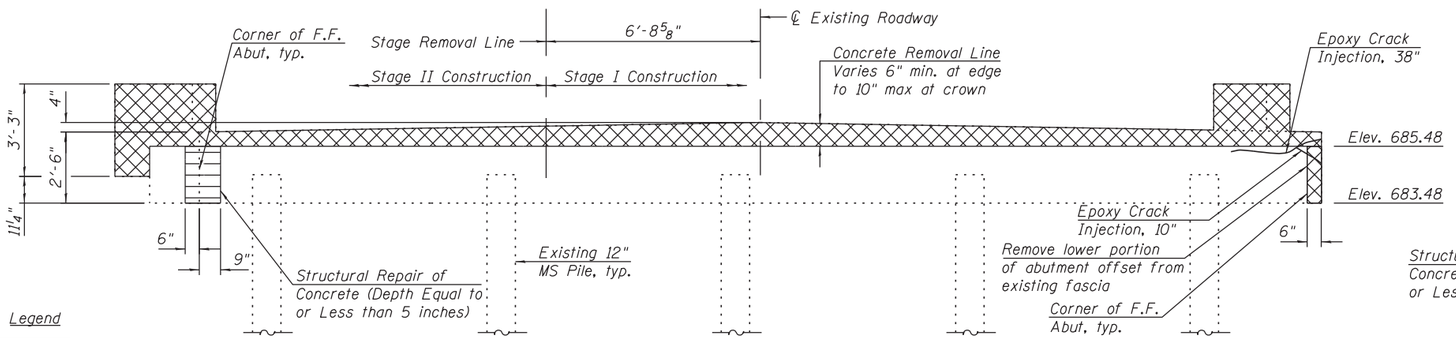
(Looking West)



SECTION A-A



SECTION B-B



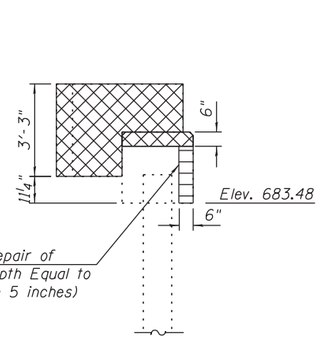
Legend

Concrete Removal

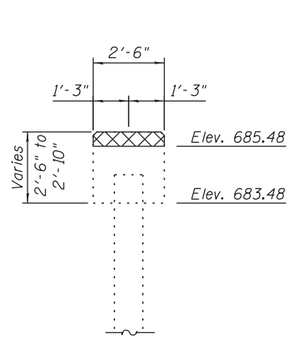
Structural Repair of Concrete (Depth Equal to or Less than 5 inches)

EAST ABUTMENT ELEVATION

(Looking East)



SECTION A-A



SECTION B-B

SECTION A-A

SECTION B-B

FILE NAME = S:\IDL\6600-6699\6622B(13)\Micros\CADD\Sheets\099-4401-XXX-014-ABUTREM.dgn

STRAND ASSOCIATES®
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

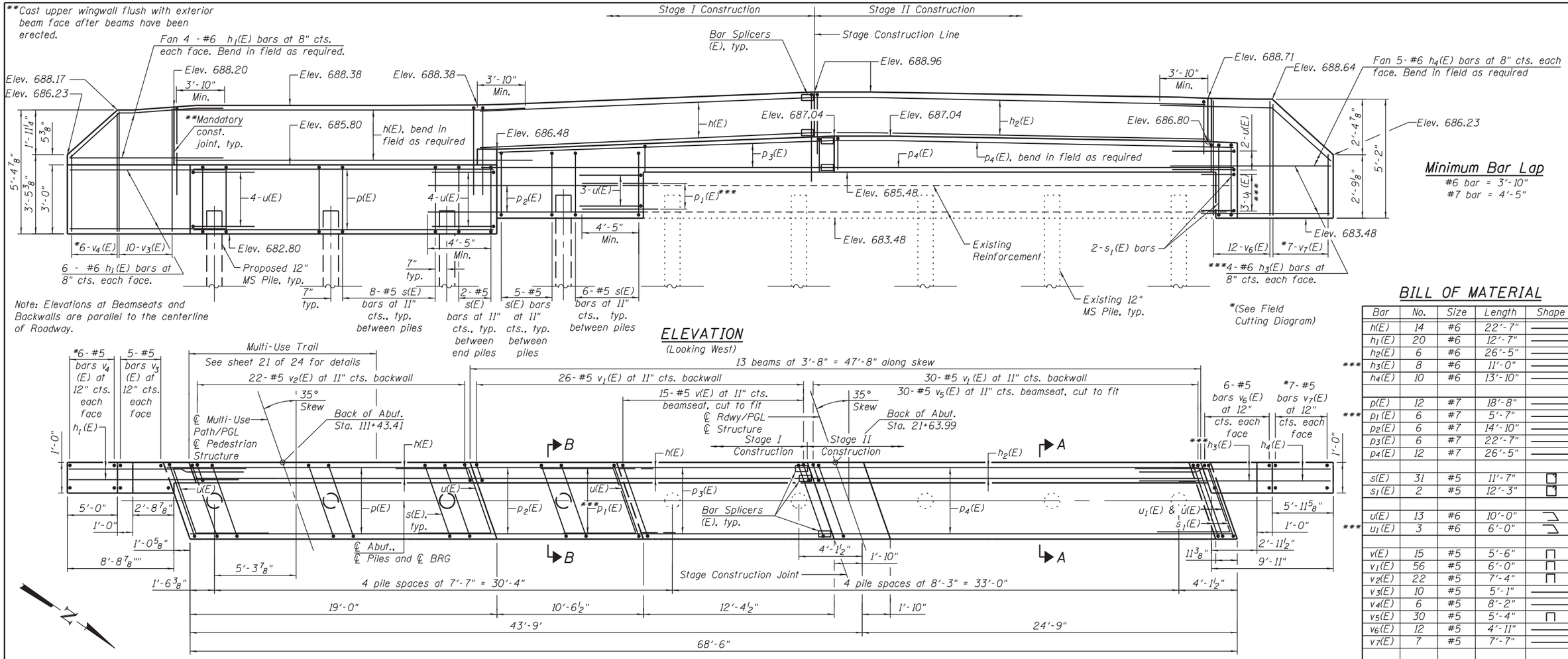
USER NAME = BenL	DESIGNED BRL	REVISED -
	CHECKED AJS	REVISED -
PLOT SCALE =	DRAWN BJF	REVISED -
PLOT DATE = 11/16/2017	CHECKED BRL	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ABUTMENT REMOVAL DETAILS
STRUCTURE NO. 099-4401

SHEET NO. 14 OF 24 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00046-00-BR	WILL	109	62
		CONTRACT NO. 61D22		
		ILLINOIS FED. AID PROJECT		



ELEVATION
(Looking West)

Minimum Bar Lap
#6 bar = 3'-10"
#7 bar = 4'-5"

BILL OF MATERIAL

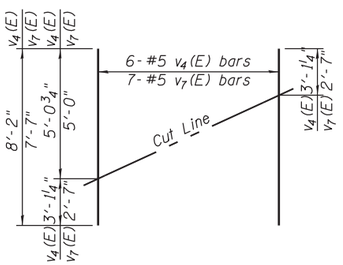
	Bar	No.	Size	Length	Shape
	h(E)	14	#6	22'-7"	
	h1(E)	20	#6	12'-7"	
	h2(E)	6	#6	26'-5"	
***	h3(E)	8	#6	11'-0"	
	h4(E)	10	#6	13'-10"	
	p(E)	12	#7	18'-8"	
***	p1(E)	6	#7	5'-7"	
	p2(E)	6	#7	14'-10"	
	p3(E)	6	#7	22'-7"	
	p4(E)	12	#7	26'-5"	
	s(E)	31	#5	11'-7"	
	s1(E)	2	#5	12'-3"	
	u(E)	13	#6	10'-0"	
***	u1(E)	3	#6	6'-0"	
	v(E)	15	#5	5'-6"	
	v1(E)	56	#5	6'-0"	
	v2(E)	22	#5	7'-4"	
	v3(E)	10	#5	5'-1"	
	v4(E)	6	#5	8'-2"	
	v5(E)	30	#5	5'-4"	
	v6(E)	12	#5	4'-11"	
	v7(E)	7	#5	7'-7"	

Structure Excavation	Cu. Yd.	36
Concrete Structures	Cu. Yd.	23
Protective Coat	Sq. Yd.	13
Reinforcement Bars, Epoxy Coated	Pound	4,740
Bar Splicers	Each	12
Furnishing Metal Shell Piles (12"x0.250")	Foot	108
Driving Piles	Foot	108
Test Pile Metal Shell	Each	1
Pile Shoes	Each	4
Concrete Sealer	Sq. Ft.	460

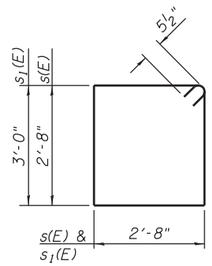
PROPOSED PILE DATA
Type: Metal Shell-12"ϕx0.25" walls with Pile Shoes
Nominal Required Bearing: 330 kips
Factored Resistance Available: 180 kips
Est. Length: 36'-0"
No. Production Piles: 3
No. Test Piles: 1

EXISTING PILE DATA
Type: Metal Shell-12"ϕx0.1793" wall
Nominal Required Bearing: 285 kips
Factored Resistance Available: 142.5 kips
Est. Length: 35'-0"
No. Piles at Pier: 5

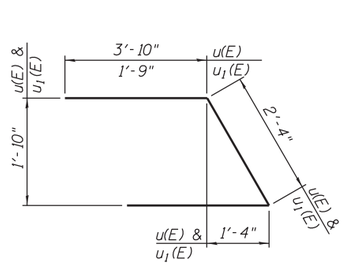
PLAN



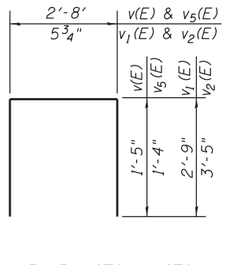
FIELD CUTTING DIAGRAM
Order v(E) full length. Cut as shown and use remainder of bars in opposite face.



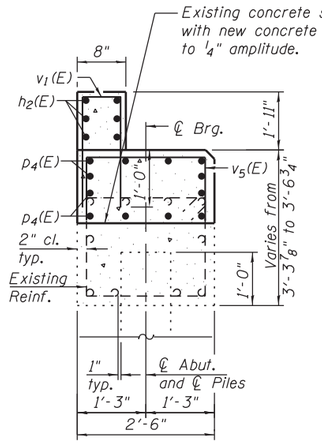
BARS s(E) & s1(E)



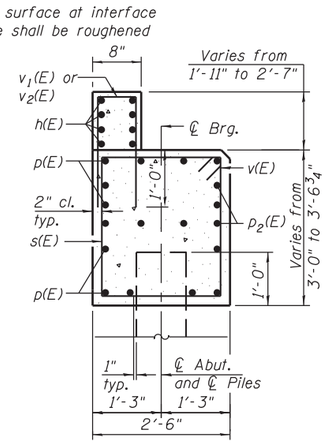
BARS u(E) & u1(E)



BAR v(E), v1(E), v2(E), & v5(E)



SECTION A-A
(Dimensions are at Rt. L's)



SECTION B-B
(Dimensions are at Rt. L's)

For details of piles and Concrete Encasement, see Sheet 20 of 24.
**Cast backwall after beams and concrete wearing surface have been erected.
Apply Concrete Sealer to new concrete area of abutment seats and backwall. Apply Protective Coat to exposed wingwalls.
Existing reinforcement shall be cleaned and incorporated into the new construction.
***Epoxy grout p1(E), h3(E) and u1(E) bars in drilled holes of the depth specified by the manufacturer to achieve full tension capacity and according to Article 584 of the Standard Specifications. Bar lengths shown are based on 12 inch min. drilled holes. Actual bar lengths are required to be adjusted by the contractor before ordering of material.

FILE NAME = S:\JOL\6680-6699\6628\813\Micros\CADD_Sheets\099-4401-XXX-0115-WESTABUT.dgn

STRAND ASSOCIATES®
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = BenL	DESIGNED BRL	REVISED -
	CHECKED AJS	REVISED -
PLOT SCALE =	DRAWN BJF	REVISED -
PLOT DATE = 11/16/2017	CHECKED BRL	REVISED -

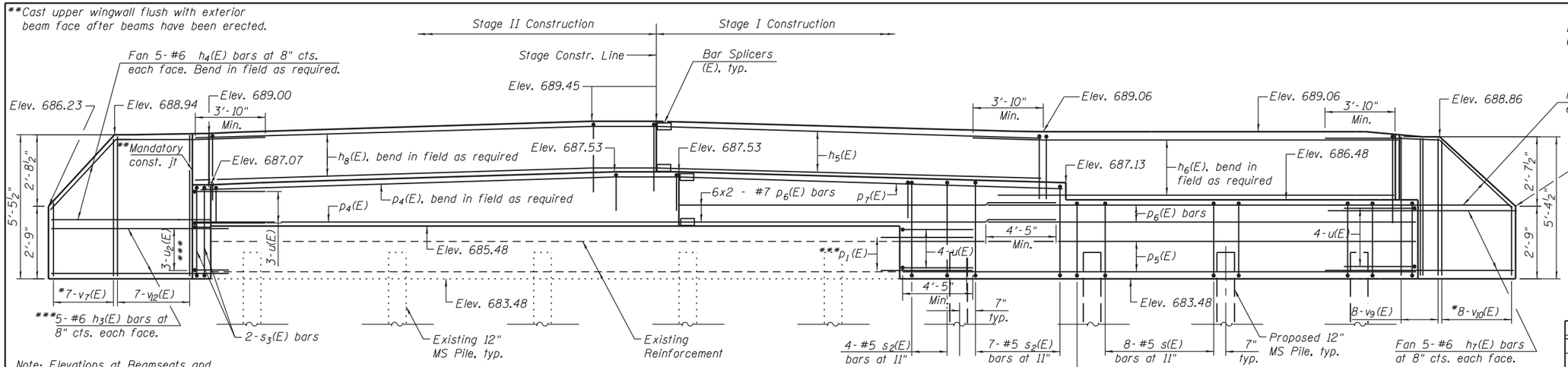
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

WEST ABUTMENT
STRUCTURE NO. 099-4401
SHEET NO. 15 OF 24 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00046-00-BR	WILL	109	63
		CONTRACT NO. 61D22		
	ILLINOIS FED. AID PROJECT			

**Cast upper wingwall flush with exterior beam face after beams have been erected.

Bars indicated thus 6x2-#7 etc. indicates 6 lines of bars with 2 lengths per line.



Note: Elevations at Beamseats and Backwalls are parallel to the centerline of Roadway.

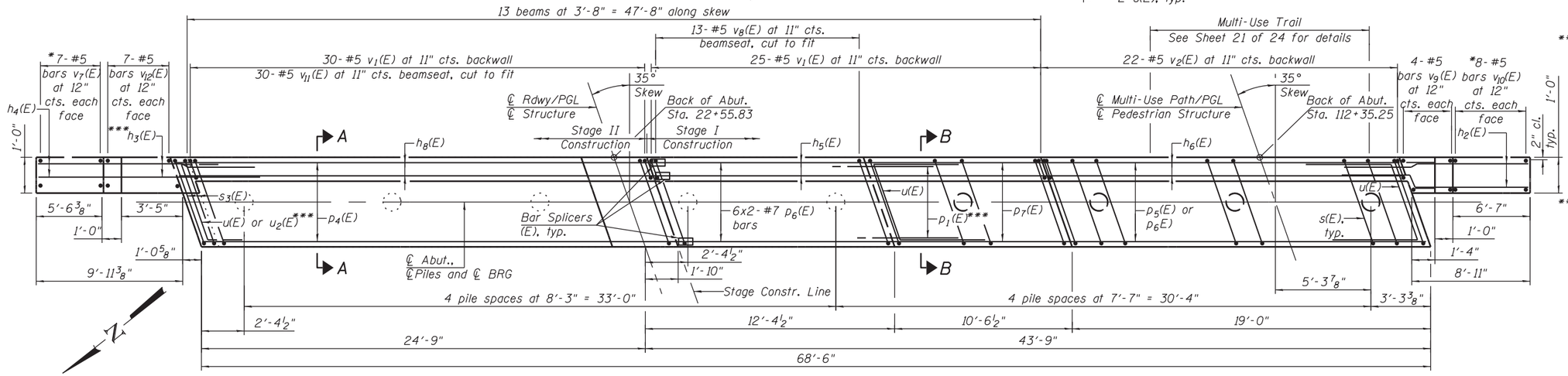
ELEVATION
(Looking East)

Minimum Bar Lap

#6 bar = 3'-10"
#7 bar = 4'-5"

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
$h_3(E)$	10	#6	11'-0"	
$h_4(E)$	10	#6	13'-10"	
$h_5(E)$	6	#6	20'-9"	
$h_6(E)$	6	#6	23'-8"	
$h_7(E)$	18	#6	12'-9"	
$h_8(E)$	6	#6	26'-3"	
*** $p_1(E)$	6	#7	5'-7"	
$p_4(E)$	12	#7	26'-5"	
$p_5(E)$	6	#7	31'-0"	
$p_6(E)$	12	#7	23'-0"	
$p_7(E)$	6	#7	21'-11"	
$s(E)$	20	#5	11'-7"	
$s_2(E)$	11	#5	12'-11"	
$s_3(E)$	2	#5	12'-9"	
$u(E)$	8	#6	10'-0"	
*** $u_2(E)$	11	#6	5'-10"	
$v_1(E)$	55	#5	6'-0"	
$v_2(E)$	22	#5	7'-4"	
$v_7(E)$	7	#5	7'-7"	
$v_8(E)$	13	#5	6'-8"	
$v_9(E)$	8	#5	5'-0"	
$v_{10}(E)$	8	#5	7'-5"	
$v_{11}(E)$	30	#5	6'-6"	
$v_{12}(E)$	14	#5	5'-2"	
Structure Excavation		Cu. Yd.	39	
Concrete Structures		Cu. Yd.	24	
Protective Coat		Sq. Yd.	14	
Reinforcement Bars, Epoxy Coated		Pound	4,950	
Bar Splicers		Each	18	
Furnishing Metal Shell Piles (12"x0.250")		Foot	105	
Driving Piles		Foot	105	
Test Pile Metal Shell		Each	1	
Pile Shoes		Each	4	
Concrete Sealer		Sq. Ft.	490	



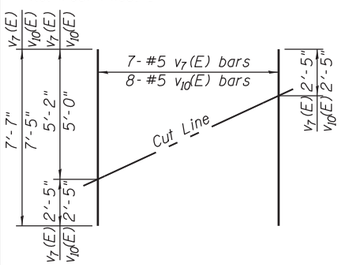
PLAN

PROPOSED PILE DATA

Type: Metal Shell-12"ϕx0.25" walls with Pile Shoes
Nominal Required Bearing: 330 kips
Factored Resistance Available: 180 kips
Est. Length: 35'-0"
No. Production Piles: 3
No. Test Piles: 1

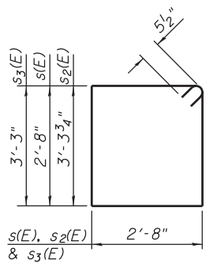
EXISTING PILE DATA

Type: Metal Shell-12"ϕx0.1793" walls
Nominal Required Bearing: 285 kips
Factored Resistance Available: 142.5 kips
Est. Length: 35'-0"
No. Piles at Pier: 5

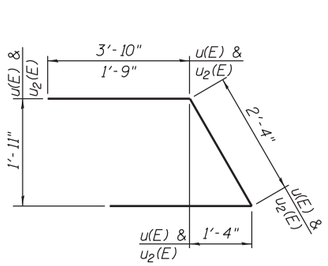


FIELD CUTTING DIAGRAM

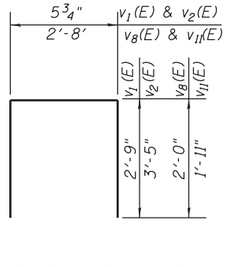
Order v(E) full length. Cut as shown and use remainder of bars in opposite face.



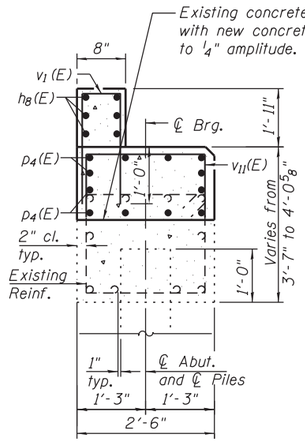
BARS s(E), s_2(E) & s_3(E)



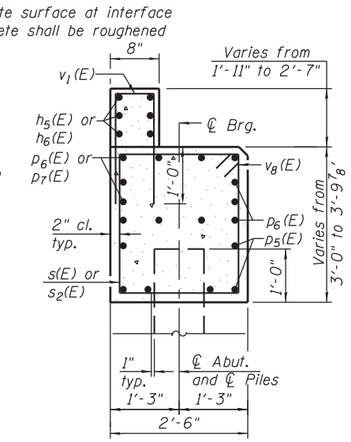
BARS u(E) &u_2(E)



BAR v_1(E), v_2(E), v_8(E), & v_{11}(E)



SECTION A-A
(Dimensions are at Rt. L's)



SECTION B-B
(Dimensions are at Rt. L's)

For details of piles and Concrete Encasement, see Sheet 20 of 24.

**Cast backwall after beams and concrete wearing surface have been erected.

Apply Concrete Sealer to new concrete area of abutment seats and backwall. Apply Protective Coat to exposed wingwalls.

Existing reinforcement shall be cleaned and incorporated into the new construction.

***Epoxy grout p_1(E), h_5(E) and u_2(E) bars in drilled holes of the depth specified by the manufacturer to achieve full tension capacity and according to Article 584 of the Standard Specifications. Bar lengths shown are based on 12 inch min. drilled holes. Actual bar lengths are required to be adjusted by the contractor before ordering of material.

FILE NAME = S:\IDL\66B8-6699\6628\0131\Micros\CADD_Sheets\099-4401-XXX-B16-EASTABUT.dgn

STRAND ASSOCIATES®
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = BenL	DESIGNED BRL	REVISED -
	CHECKED AJS	REVISED -
PLOT SCALE =	DRAWN BJF	REVISED -
PLOT DATE = 11/16/2017	CHECKED BRL	REVISED -

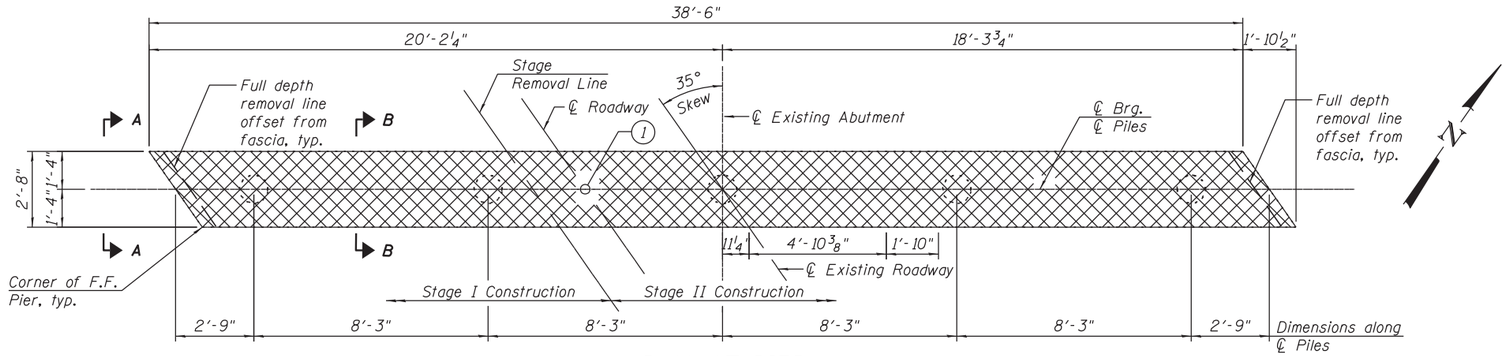
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EAST ABUTMENT
STRUCTURE NO. 099-4401
SHEET NO. 16 OF 24 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00046-00-BR	WILL	109	64
		CONTRACT NO. 61D22		
	ILLINOIS FED. AID PROJECT			

TOTAL BILL OF MATERIAL

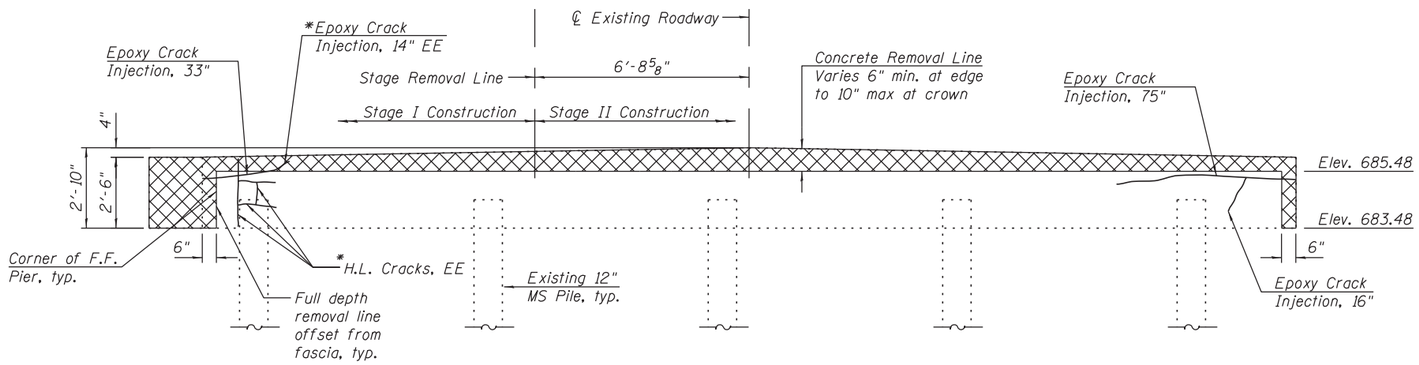
ITEM	UNIT	TOTAL
Concrete Removal	Cu Yd	5.0
Epoxy Crack Injection	Foot	34.0
Structural Repair of Concrete (Depth Equal to or Less than 5 inches)	Cu Yd	1.0
Structural Repair of Concrete (Depth Greater than 5 inches)	Cu Yd	4.0



PLAN AT PIER

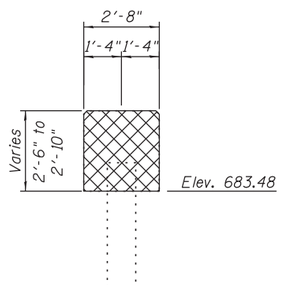
(Pier 1 Shown; Pier 2 Similar)

(1) Sta 21+89.86 Pier 1
(Sta. 22+29.96 Pier 2)

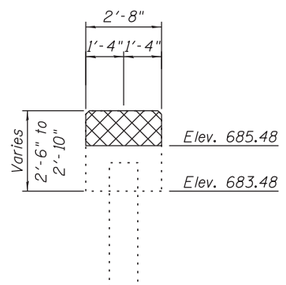


PIER 1 ELEVATION

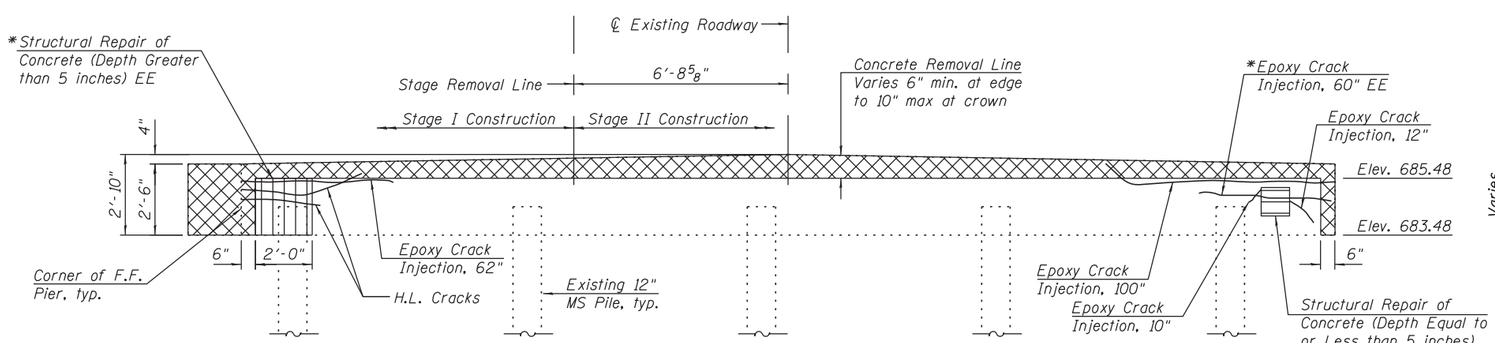
(West Elev. Shown; East Elev. Similar)



SECTION A-A

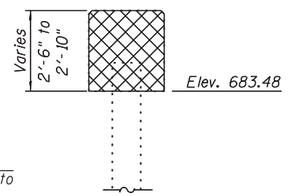


SECTION B-B

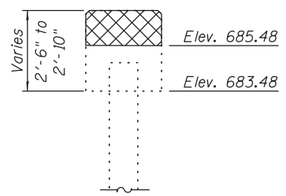


PIER 2 ELEVATION

(West Elev. Shown; East Elev. Similar)



SECTION A-A



SECTION B-B

Legend

- Concrete Removal
- Structural Repair of Concrete (Depth Equal to or Less than 5 inches)
- Structural Repair of Concrete (Depth Greater than 5 inches)

Existing reinforcement shall be cleaned and incorporated into the new construction. Cost included with Concrete Removal.

If existing reinforcement is damaged or cut during concrete removal, provide new dowels to match.

**EE designates East Elevation side of substructure element.*

FILE NAME = S:\JOL\6680-6699\6628\813\Micros\CADD\Sheets\099-4401-XXX-817-PIERREM.dgn

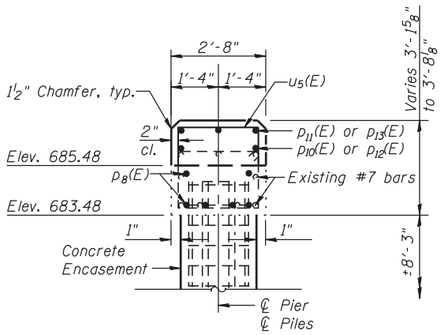
STRAND ASSOCIATES®
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = BenL	DESIGNED BRL	REVISED -	
	CHECKED AJS	REVISED -	
PLOT SCALE =	DRAWN BJF	REVISED -	
PLOT DATE = 11/16/2017	CHECKED BRL	REVISED -	

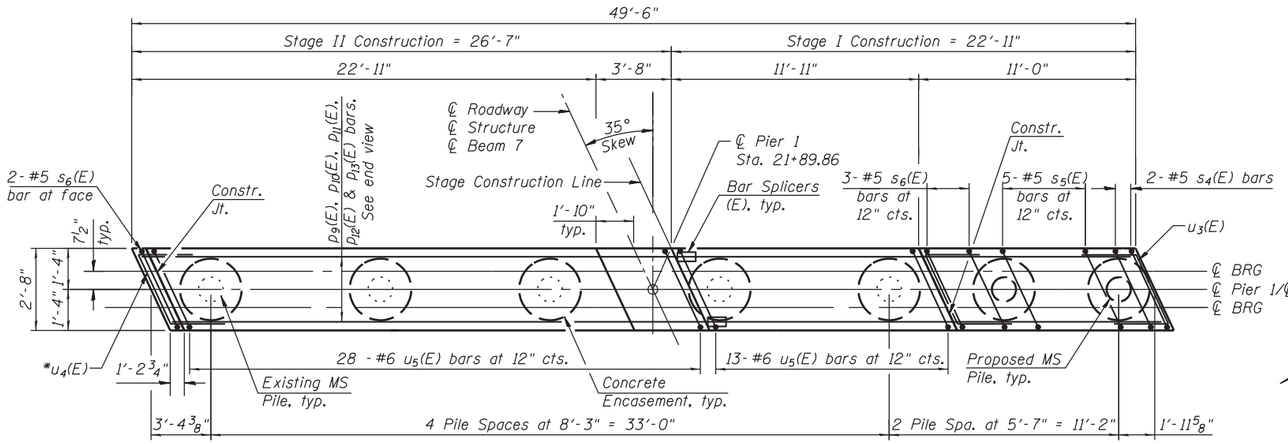
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER REMOVAL DETAILS
STRUCTURE NO. 099-4401
SHEET NO. 17 OF 24 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00046-00-BR	WILL	109	65
		CONTRACT NO. 61D22		
	ILLINOIS FED. AID PROJECT			

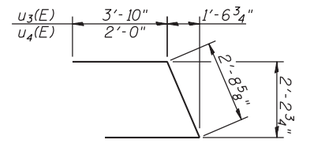


SECTION VIEW
(Existing)

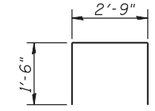


Note: Bearing pads not shown for clarity.

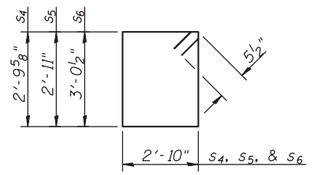
TOP PLAN



BAR u3(E) & u4(E)

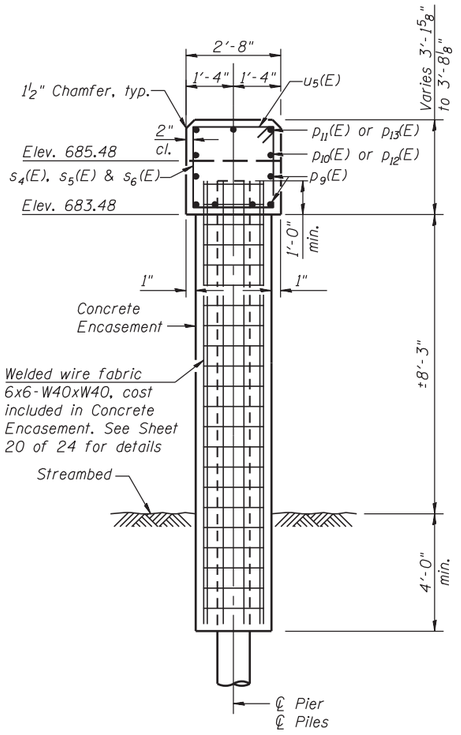


BAR u5(E)

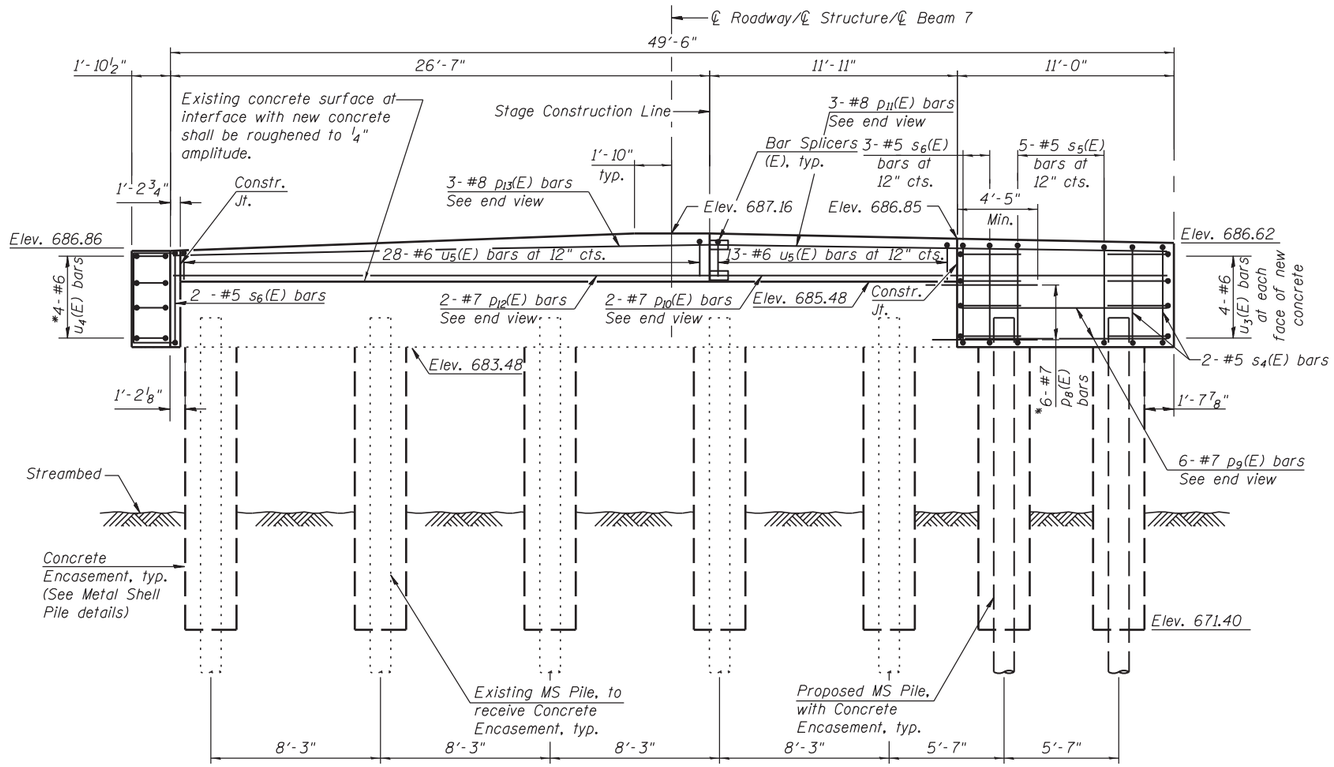


BAR s4(E), s5(E) & s6(E)

MINIMUM BAR LAP
#6 bar = 3'-10"
#7 bar = 4'-5"



END VIEW
(Proposed)



ELEVATION
(Looking East)

BILL OF MATERIAL

	Bar	No.	Size	Length	Shape
*	$p_8(E)$	6	#7	5'-5"	——
	$p_9(E)$	6	#7	10'-8"	——
	$p_{10}(E)$	2	#7	22'-7"	——
	$p_{11}(E)$	3	#8	22'-7"	——
	$p_{12}(E)$	2	#7	26'-3"	——
	$p_{13}(E)$	3	#8	26'-3"	——
	$s_4(E)$	2	#5	12'-1"	□
	$s_5(E)$	5	#5	12'-4"	□
	$s_6(E)$	5	#5	12'-7"	□
	$u_3(E)$	8	#6	10'-5"	⊐
*	$u_4(E)$	4	#6	6'-7"	⊐
	$u_5(E)$	39	#6	5'-9"	⊐
	Concrete Structures		Cu. Yd.	10	
	Reinforcement Bars, Epoxy Coated		Pound	1,340	
	Bar Splicers		Each	5	
	Furnishing Metal Shell Piles, 12"x0.25"		Foot	36	
	Driving Piles		Foot	36	
	Test Pile Metal Shells		Each	1	
	Concrete Encasement		Cu. Yd.	15	
	Pile Shoes		Each	2	
	Concrete Sealer		Sq. Ft.	350	

PROPOSED PILE DATA
Type: Metal Shell-12"ϕx0.25" walls with Pile Shoes
Nominal Required Bearing: 330 kips
Factored Resistance Available: 180 kips
Est. Length: 36'-0"
No. Production Piles: 1
No. Test Piles: 1

EXISTING PILE DATA
Type: Metal Shell-12"ϕx0.1793" wall
Nominal Required Bearing: 285 kips
Factored Resistance Available: 142.5 kips
Est. Length: 35'-0"
No. Piles at Pier: 5

*Epoxy grout $u_4(E)$ and $p_8(E)$ bars in drilled holes of the depth specified by the manufacturer to achieve full tension capacity and according to Article 584 of the Standard Specifications. Bar lengths shown are based on 12 inch min. drilled holes. Actual bar lengths are required to be adjusted by the contractor before ordering of material.

Notes:
Space reinforcement in cap to miss anchor dowel rods.
Existing reinforcement shall be cleaned and incorporated into the new construction. Cost included with Concrete Removal.
For details of piles and concrete encasement, see Sheet 20 of 24.
Apply Concrete Sealer to top and sides of new concrete area of the pier cap.

FILE NAME = S:\DOL\6680-6699\6628\813\Micro\CADD Sheets\0099-4401-XXX-818-PIER1.dgn

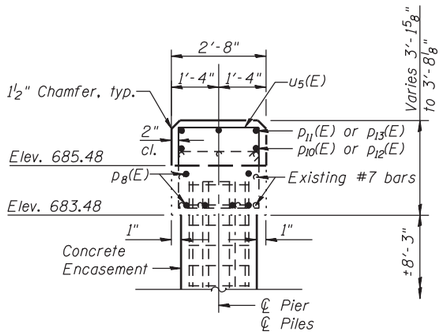
STRAND ASSOCIATES®
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = BenL	DESIGNED BRL	REVISED -
	CHECKED AJS	REVISED -
PLOT SCALE =	DRAWN BJF	REVISED -
PLOT DATE = 11/16/2017	CHECKED BRL	REVISED -

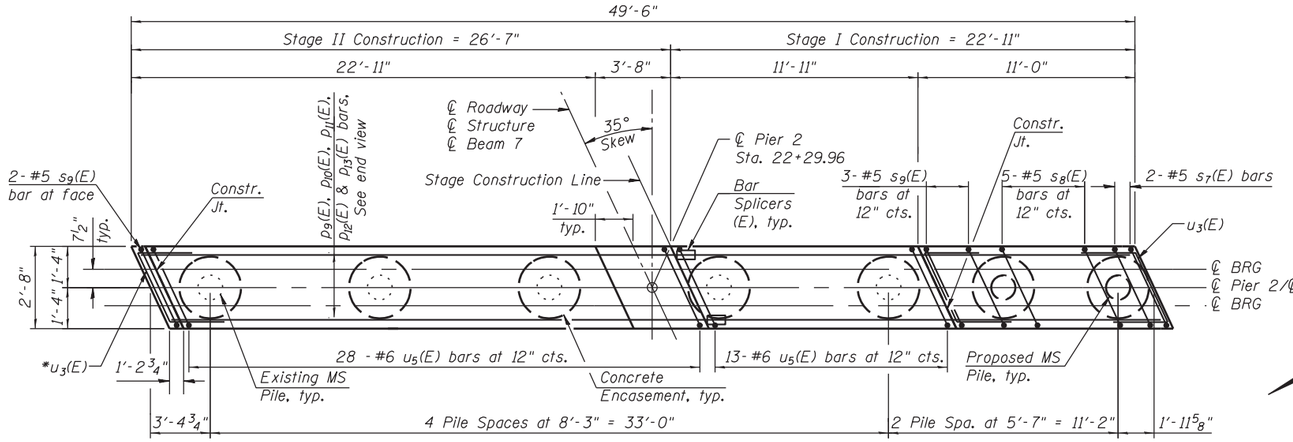
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER 1
STRUCTURE NO. 099–4401
SHEET NO. 18 OF 24 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00046-00-BR	WILL	109	66
		CONTRACT NO. 61D22		
	ILLINOIS FED. AID PROJECT			

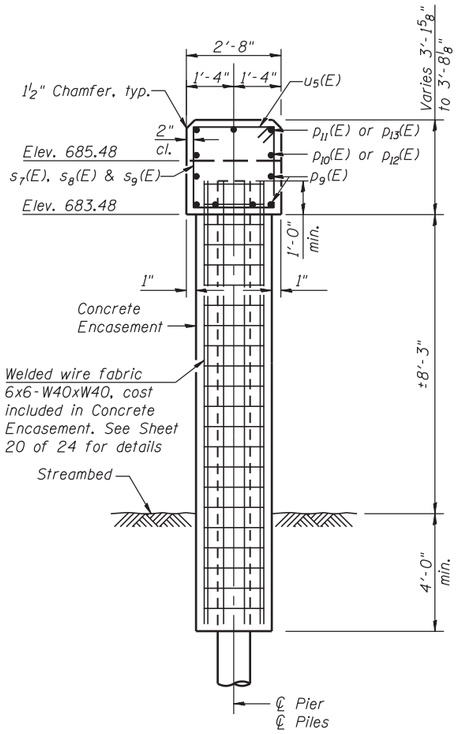


SECTION VIEW
(Existing)

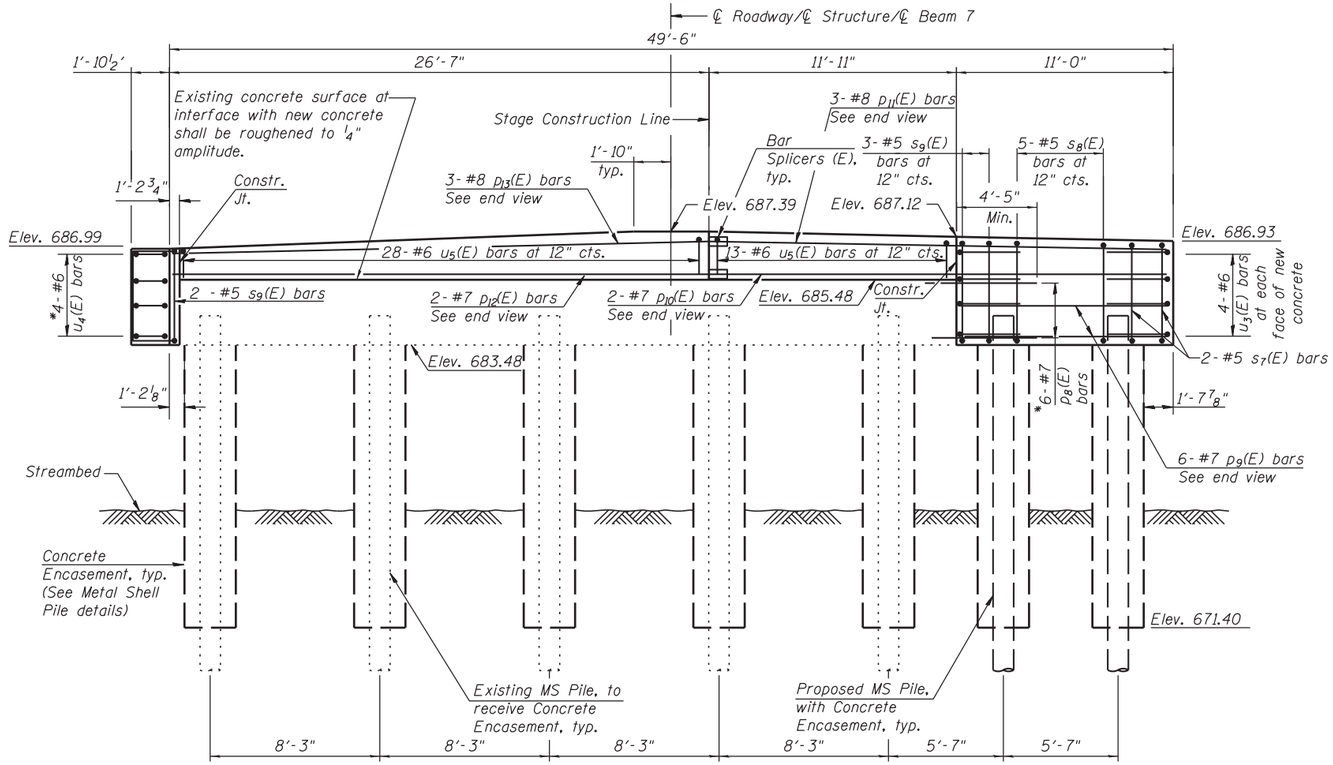


Note: Bearing pads not shown for clarity.

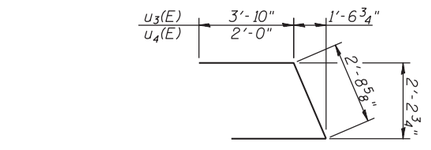
TOP PLAN



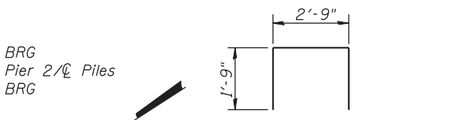
END VIEW
(Proposed)



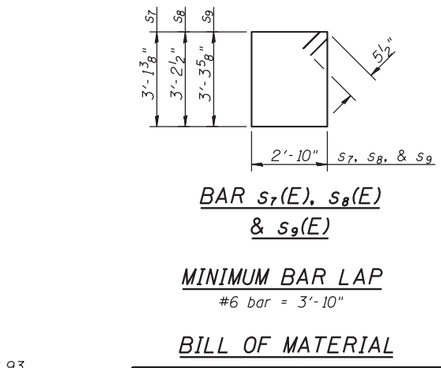
ELEVATION
(Looking East)



BAR $u_3(E)$ & $u_4(E)$



BAR $u_5(E)$



BAR $s_7(E)$, $s_8(E)$ & $s_9(E)$

MINIMUM BAR LAP
#6 bar = 3'-10"

BILL OF MATERIAL

	Bar	No.	Size	Length	Shape
*	$p_8(E)$	6	#7	5'-5"	——
	$p_9(E)$	6	#7	10'-8"	——
	$p_{10}(E)$	2	#7	22'-7"	——
	$p_{11}(E)$	3	#8	22'-7"	——
	$p_{12}(E)$	2	#7	26'-3"	——
	$p_{13}(E)$	3	#8	26'-3"	——
	$s_7(E)$	2	#5	12'-9"	□
	$s_8(E)$	5	#5	12'-11"	□
	$s_9(E)$	5	#5	13'-1"	□
	$u_3(E)$	8	#6	10'-5"	⊐
*	$u_4(E)$	4	#6	6'-7"	⊐
	$u_5(E)$	39	#6	5'-9"	⊐
	Concrete Structures		Cu. Yd.	12	
	Reinforcement Bars, Epoxy Coated		Pound	1,370	
	Bar Splicers		Each	5	
	Furnishing Metal Shell Piles, 12"x0.25"		Foot	34	
	Driving Piles		Foot	34	
	Test Pile Metal Shells		Each	1	
	Concrete Encasement		Cu. Yd.	15	
	Pile Shoes		Each	2	
	Concrete Sealer		Sq. Ft.	380	

PROPOSED PILE DATA
Type: Metal Shell-12"ϕx0.25" walls with Pile Shoes
Nominal Required Bearing: 330 kips
Factored Resistance Available: 180 kips
Est. Length: 34'-0"
No. Production Piles: 1
No. Test Piles: 1

EXISTING PILE DATA
Type: Metal Shell-12"ϕx0.1793" wall
Nominal Required Bearing: 285 kips
Factored Resistance Available: 142.5 kips
Est. Length: 35'-0"
No. Piles at Pier: 5

*Epoxy grout $u_4(E)$ and $p_8(E)$ bars in drilled holes of the depth specified by the manufacturer to achieve full tension capacity and according to Article 584 of the Standard Specifications. Bar lengths shown are based on 12 inch min. drilled holes. Actual bar lengths are required to be adjusted by the contractor before ordering of material.

Notes:
Space reinforcement in cap to miss anchor dowel rods.
Existing reinforcement shall be cleaned and incorporated into the new construction. Cost included with Concrete Removal.
For details of piles and concrete encasement, see Sheet 20 of 24.
Apply Concrete Sealer to top and sides of new concrete area of the pier cap.

FILE NAME = S:\JOL\6688-6899\6628\813\Micros\CADD\Sheets\899-4401-XXX-815-PIER2.dgn

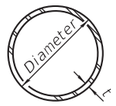
STRAND ASSOCIATES®
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = BenL	DESIGNED BRL	REVISED -	
	CHECKED AJS	REVISED -	
PLOT SCALE =	DRAWN BJF	REVISED -	
PLOT DATE = 11/16/2017	CHECKED BRL	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

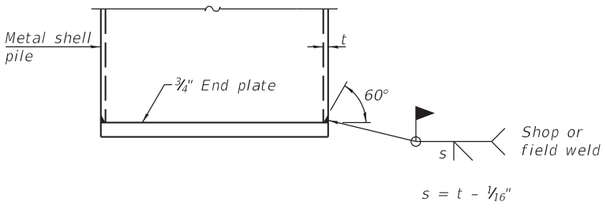
PIER 2
STRUCTURE NO. 099-4401
SHEET NO. 19 OF 24 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00046-00-BR	WILL	109	67
		CONTRACT NO. 61D22		
	ILLINOIS FED. AID PROJECT			



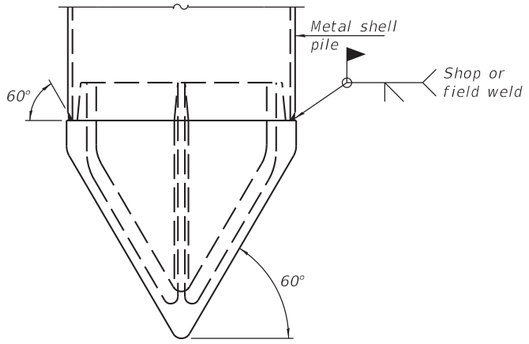
METAL SHELL PILE TABLE

Designation and outside diameter	Wall thickness t	Weight per foot (Lbs./ft.)	Inside volume (yd.³/ft.)
PP12	0.250"	31.37	0.0267
PP14	0.250"	36.71	0.0368
PP14	0.312"	45.61	0.0361
PP16	0.312"	52.32	0.0478
PP16	0.375"	62.64	0.0470



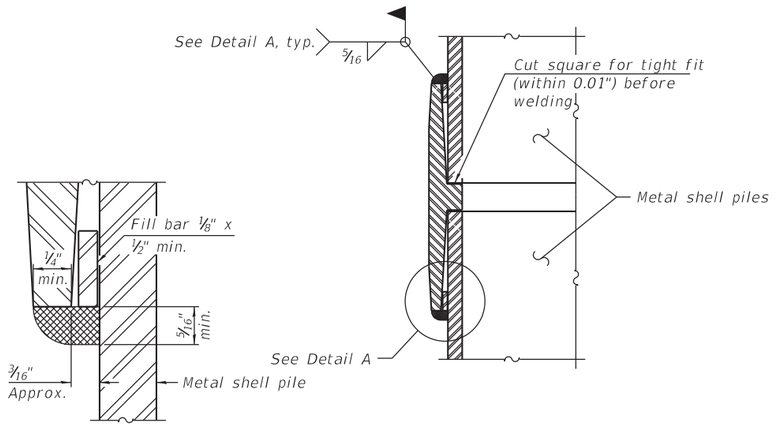
$s = t - 1/16"$

END PLATE ATTACHMENT



PILE SHOE ATTACHMENT

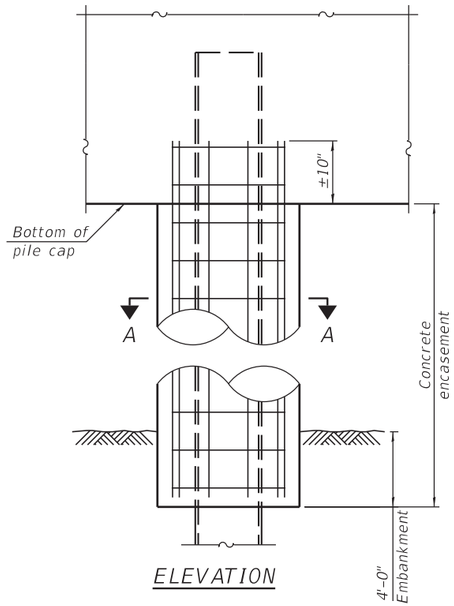
(When called for on the plans, the Contractor shall furnish metal shell pile shoes consisting of a single piece conical pile point as shown. The pile shoes shall be cast in one piece steel according to either ASTM A 148 Grade 90-60 or AASHTO M 103 Grade 65-35 and shall provide full bearing over the full circumference of the metal shell pile. The pile shoe shall have tapered leads to assure proper alignment and fitting and shall be secured to the pile with a circumferential weld).



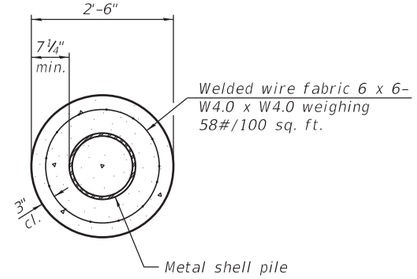
DETAIL A

WELDED COMMERCIAL SPLICE

Notes:
The 1/8" x 1/2" min. fill bar may be constructed of 2 bars with a 1/8" max. gap between them.
Pile segments shall be driven to solid contact with splicer before welding.



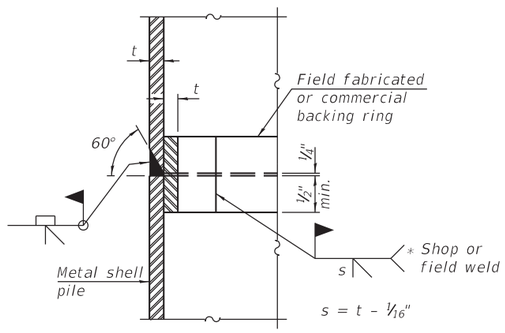
ELEVATION



SECTION A-A

INDIVIDUAL PILE CONCRETE ENCASEMENT AT PIERS

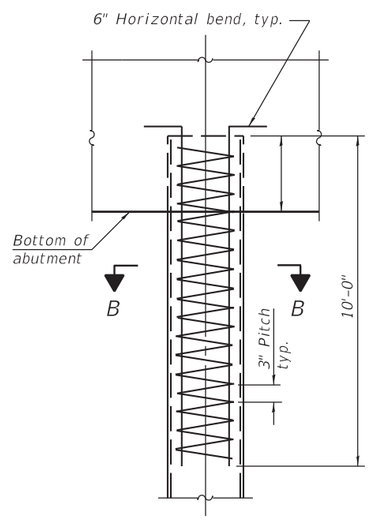
Welded Wire Fabric shall be included with the cost of Concrete Encasement.



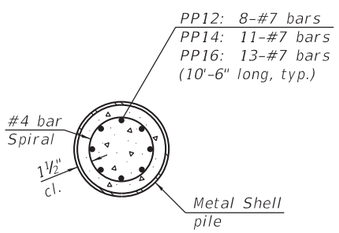
$s = t - 1/16"$

COMPLETE PENETRATION WELD SPLICE

* *Field fabricated backing ring may be made from pile shell by removing segment to allow reducing circumference and vertically rejoin with partial joint penetration weld.*



ELEVATION



SECTION B-B

REINFORCEMENT AT ABUTMENTS

Metal Shell reinforcement at abutments shall be included with the cost of Furnishing Metal Shell Piles.

Note:
The metal shell piles shall be according to Article 1006.05 of the Standard Specifications.

F-MS 8-11-2017

FILE NAME = S:\JOL\6600-6699\6628\813\Micros\C4DD_Sheets\8699-4401-XXX-828-MPILE.dgn

STRAND ASSOCIATES®
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = BenL	DESIGNED BRL	REVISED -
	CHECKED AJS	REVISED -
PLOT SCALE =	DRAWN BJF	REVISED -
PLOT DATE = 11/16/2017	CHECKED BRL	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

METAL SHELL PILE DETAILS
STRUCTURE NO. 099-4401
SHEET NO. 20 OF 24 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00046-00-BR	WILL	109	68
		CONTRACT NO. 61D22		
	ILLINOIS FED. AID PROJECT			

Scope of Work:
2 new concrete abutment extensions from existing bridge abutments supported on Metal Shell Piles will be placed to support a prefabricated pedestrian truss bridge.

Existing Structure: None.

Salvage: None.

GENERAL NOTES:

Details on this drawing are not to scale.

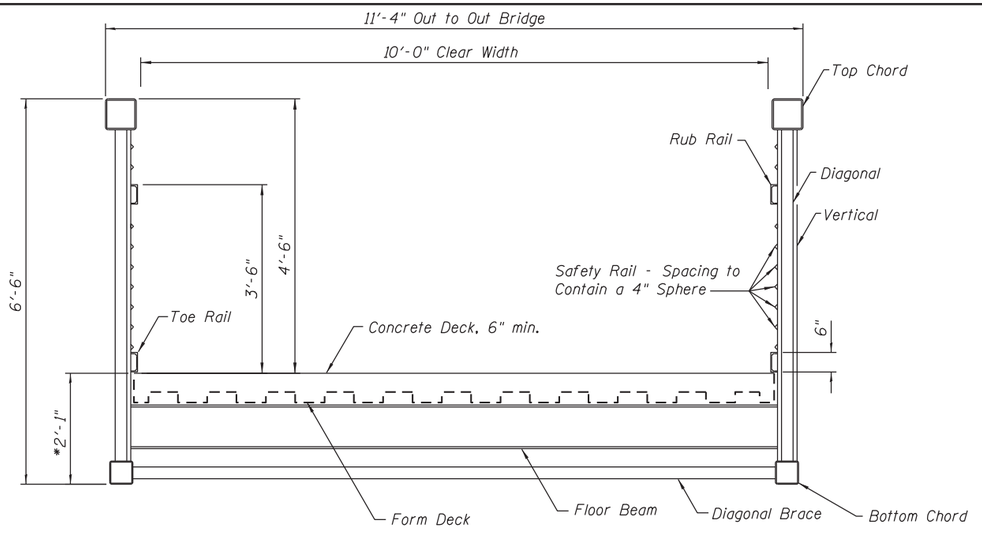
All details and dimensions shown on this sheet are to be verified by the bridge manufacturer.

Compression seal shall comply with Art. 503.10B.

See Specifications for prefabricated bridge.

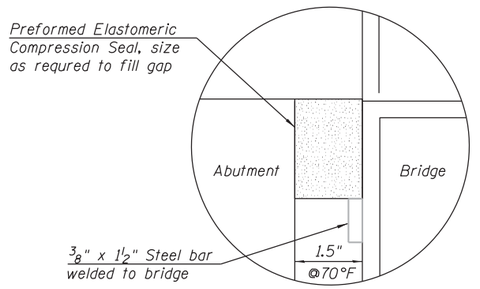
Bearing seat surfaces shall be constructed or adjusted to the designated elevations within a tolerance of $\frac{1}{8}$" (0.01 ft.). Adjustment shall be made either by grinding the surface or by shimming the bearings.

Reinforcement bars designated (E) shall be expoxy coated.



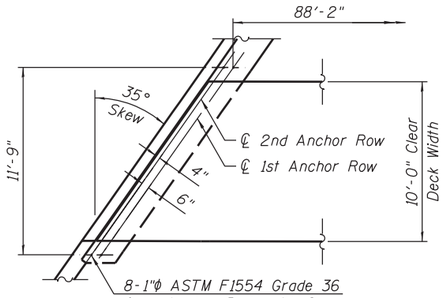
BRIDGE SECTION DETAIL

(Not to scale)

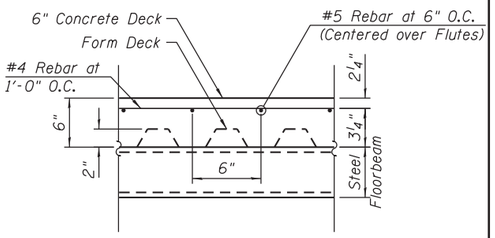


COMPRESSION SEAL DETAIL

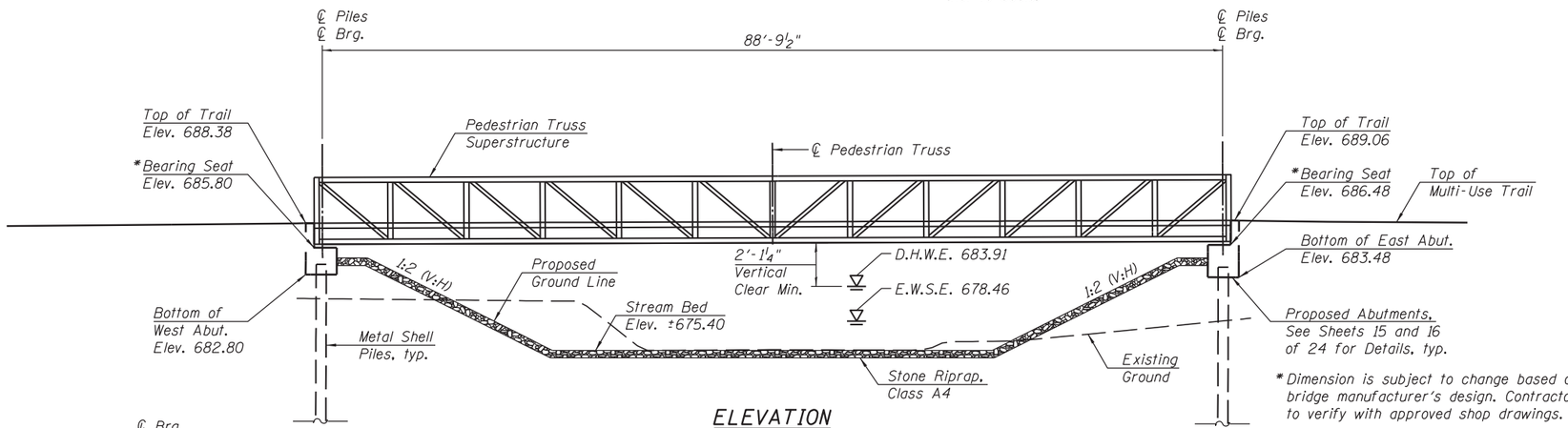
(If Gap Exceeds $\frac{3}{4}$" at Abutment)



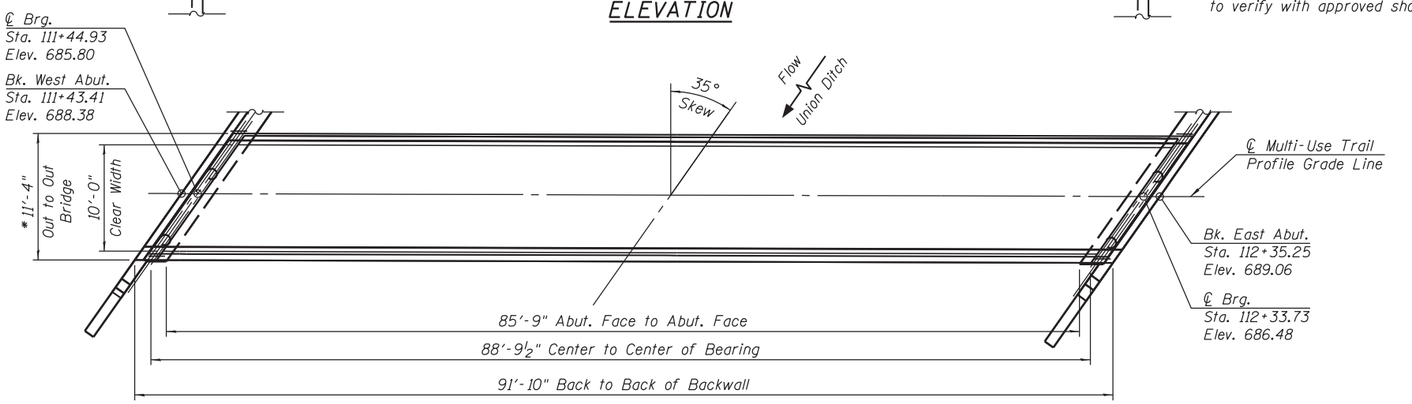
ABUT. PLAN DETAIL



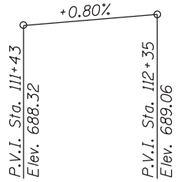
TYP. SLAB REINFORCEMENT DETAIL



ELEVATION



PLAN



PROFILE GRADE

(Along ℄ of Bikepath)

DESIGN STRESSES

FIELD UNITS

f'c = 3,500 psi
fy = 60,000 psi (Reinforcement)
fy = 50,000 psi (Steel) ATSM A847
fy = 36,000 psi (Steel) ASTM A36

DESIGN SPECIFICATIONS

2009 AASHTO LRFD Guide Specifications for the Design of Pedestrian Bridges

LOADING

90 psf Live Load
10,000 lb. Vehicle Load
35 psf Wind Load

TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Concrete Superstructure	Cu. Yd.	17
Pedestrian Truss Superstructure	Sq. Ft.	920

FILE NAME = S:\JOL\6688-6699\6828\813\Micros\CADD_Sheets\099-4401-XXX-821-PEDTRUSS.dgn

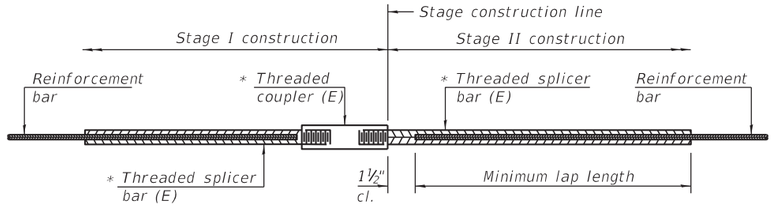
STRAND ASSOCIATES®
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = BenL	DESIGNED BRL	REVISED -	
	CHECKED AJS	REVISED -	
PLOT SCALE =	DRAWN BJF	REVISED -	
PLOT DATE = 11/16/2017	CHECKED BRL	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PEDESTRIAN TRUSS SUPERSTRUCTURE DETAILS
STRUCTURE NO. 099-4401
SHEET NO. 21 OF 24 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00046-00-BR	WILL	109	69
		CONTRACT NO. 61D22		
	ILLINOIS FED. AID PROJECT			

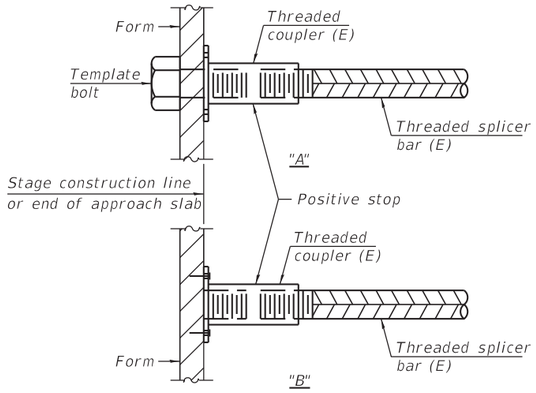


STANDARD BAR SPLICER ASSEMBLY

Threaded splicer bar length = min. lap length + 1½" + thread length

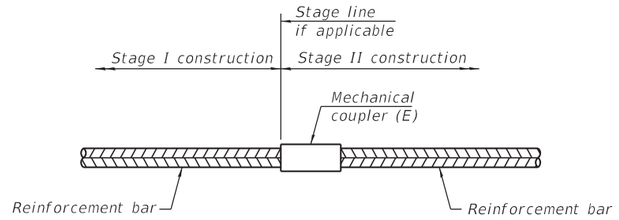
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
Superstructure-CWS	#4	91	2'-7"
West Abutment	#7	6	4'-5"
West Abutment	#6	6	3'-10"
East Abutment	#7	12	4'-5"
East Abutment	#6	6	3'-10"
West Pier P1	#7	2	4'-5"
West Pier P1	#8	3	5'-1"
East Pier P2	#7	2	4'-5"
East Pier P2	#8	3	5'-1"



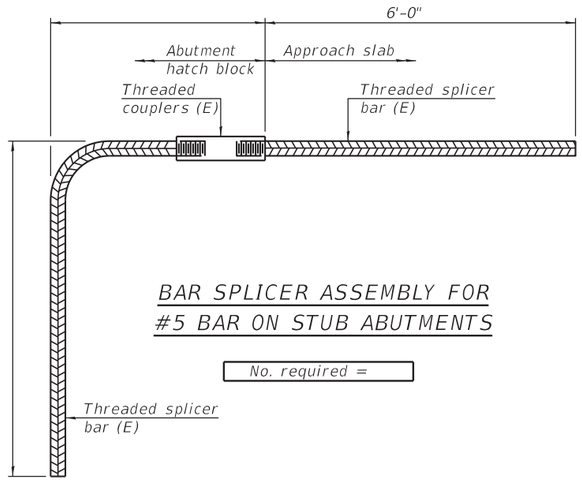
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
(E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars.

Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

FILE NAME = S:\IDL\6688-6899\6628\013\Micros\CADD_Sheets\099-4401-XXX-022-SPLICER.dgn

BSD-1 2-17-2017

STRAND ASSOCIATES®
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = BenL	DESIGNED BRL	REVISED -
	CHECKED AJS	REVISED -
PLOT SCALE =	DRAWN BJF	REVISED -
PLOT DATE = 11/16/2017	CHECKED BRL	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
STRUCTURE NO. 099-4401
SHEET NO. 22 OF 24 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00046-00-BR	WILL	109	70
		CONTRACT NO. 61D22		
	ILLINOIS FED. AID PROJECT			

Testing Service Corporation

STRUCTURE BORING LOG

ROUTE FAU 3752 DESCRIPTION Bridge Rehabilitation Date Started 6/7/11 Date Completed 6/7/11

SECT. 10-00046-00-BR STRUCT. NO. 099-4401 DRILLED BY TSC L-76,375

COUNTY Will LOCATION St. Francis Road S. 14 , TWP. 35 , RNG. 12

Boring No. 1
Station 11+50
Offset 7.00ft RT
Surface Elev. 688.30 ft

Surface Water Elev. —
Groundwater Elev.:
when drilling 675.3
at Completion —
after — Hrs. —

DEPTH	BLOWS	Qu tsf	W %	DEPTH	BLOWS	Qu tsf	W %
Bituminous Concrete 687.50							
FILL - Light brown Crushed Stone, moist 686.30	15 5 2	P 1.0	3.8 17.6		2 3 4		16.1
FILL - Brown CLAY LOAM, trace gravel, very moist A-6 685.30				Loose brownish-gray SILTY LOAM, trace gravel, very moist A-4			
FILL - Light brown silty Sand and Crushed Stone, damp -5	18 21 27		3.1	-30	3 4 5		18.4
682.80							
Black SILTY CLAY (Topsoil), moist A-7-6	7 5 6	P 1.75	23.3				
680.30				655.30			
Soft brown and dark brown CLAY LOAM, trace sand, very moist A-6 -10	1 1 2	P 0.5	26.3	Soft brownish-gray SILTY CLAY LOAM, trace gravel, very moist A-4	3 5 6	B 0.6	15.9
677.80				-35			
Soft brown and gray CLAY, trace sand, very moist A-7-6	1 1 3	P 0.5 2.0	28.9 21.6				
676.30				650.30			
Stiff to very stiff gray CLAY, trace gravel, moist A-6	3 6 7	B 1.6	18.8	Medium dense brownish-gray SILTY LOAM, trace gravel, moist A-4	8 20 50/3"		11.0
-15				648.80 -40			
	3 5 7	P 2.0	15.6	DOLOMITE, Light gray, mottled red-green, argillaceous, thin bedded, dense, slight fracture @ 42½'			
670.30							
Medium stiff gray CLAY LOAM, trace gravel, occasional Cobbles, very moist A-6 -20	5 8 7	P 0.75	17.0	Recovery = 100% RQD = 18%			
667.80				-45			
Very stiff brown CLAY LOAM, trace gravel, occasional Cobbles, moist A-4/A-6	5 15 11	B 3.2	10.0	642.30			
665.30				End of Boring at 46.0'			
Loose brownish-gray SILTY LOAM, trace gravel, very moist A-4	3 5 5		14.3	SPT Hammer = CME Automatic			
-25				Rotary Wash Drill Below 20.0' -50			

SPT. (N) = Sum of last two blow values in sample. (Qu) B=Bulge S=Shear P=Penetration Test
Stations, Depths, Offset, and Elevations are in Feet

Soil Boring Station 11+50 7.00 ft RT. = 21+45.67 6.10 ft RT on Plan Station

Testing Service Corporation

STRUCTURE BORING LOG

ROUTE FAU 3752 DESCRIPTION Bridge Rehabilitation Date Started 6/6/11 Date Completed 6/6/11

SECT. 10-00046-00-BR STRUCT. NO. 099-4401 DRILLED BY TSC L-76,375

COUNTY Will LOCATION St. Francis Road S. 14 , TWP. 35 , RNG. 12

Boring No. 2
Station 12+78
Offset 9.00ft LT
Surface Elev. 687.60 ft

Surface Water Elev. —
Groundwater Elev.:
when drilling 674.6
at Completion —
after — Hrs. —

DEPTH	BLOWS	Qu tsf	W %	DEPTH	BLOWS	Qu tsf	W %
Bituminous Concrete 686.80				662.10			
FILL - Light brown Crushed Stone, damp 685.60	16 7 3	P 1.5	2.8 16.5	Loose to medium dense brownish-gray SILTY LOAM, occasional Cobbles, very moist A-4	4 4 6		14.8
FILL - Brown to dark brown CLAY LOAM, trace to little gravel, trace organic, very moist A-6	2 2 3	P 1.5	15.2				
-5					3 4 5		15.1
682.10				-30			
FILL - Brown and black CLAY, trace gravel, trace roots, wood and topsoil, moist A-7-6	2 2 4	P 2.5	20.5				
679.60							
Black SILTY CLAY (Topsoil), very moist A-7-6 -10	2 3 4	P 0.75	33.0		3 5 7		16.7
676.60				-35			
Medium stiff to stiff brown CLAY LOAM, trace gravel, very moist A-6	2 2 2	B 1.0	24.1				
674.60				649.60			
Very stiff brownish-gray CLAY, trace gravel, moist A-6	4 8 11	B 2.7	18.3	Hard drilling on possible Fractured Bedrock		100/0.5"	
-15				-40			
672.10						100/0.5"	
Medium stiff to stiff brownish-gray CLAY LOAM, trace to little gravel, very moist A-6	4 5 6	P 0.75	16.5				
				644.10			
-20	3 3 5	P 1.25	12.3	Auger Refusal at 43.5'		100/0"	
667.10				SPT Hammer = CME Automatic			
Stiff to very stiff brownish-gray CLAY LOAM, trace gravel, moist A-6	3 3 4	P 2.0	13.5	Rotary Wash Drill Below 20.0'			
664.60				-45			
Medium stiff brownish-gray SILTY CLAY LOAM, trace gravel, very moist A-4/A--6	2 5 4	P 0.75	14.5				
-25				-50			

SPT. (N) = Sum of last two blow values in sample. (Qu) B=Bulge S=Shear P=Penetration Test
Stations, Depths, Offset, and Elevations are in Feet

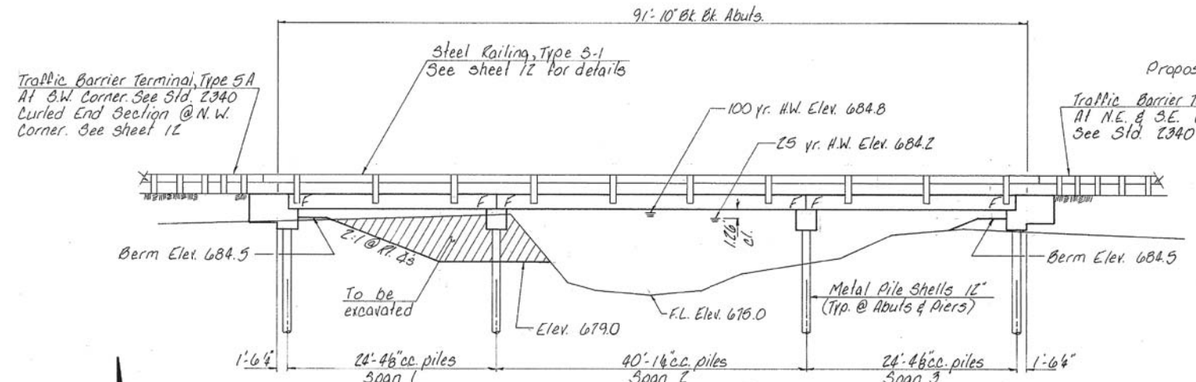
Soil Boring Station 12+78 9.00 ft LT. = 22+71.10 15.68 ft LT on Plan Station

FILE NAME = S:\JOL\6680-6699\6628\813\Micros\CADD\Sheets\0099-4401-XXX-823-SBL.dgn

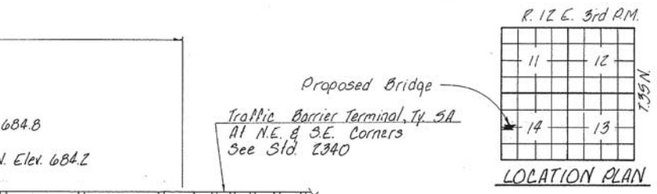
ILDOT_BORING 78375.GPJ IDOT.GDT 9/14/11

STRAND ASSOCIATES® 1170 SOUTH HOUBOLT ROAD JOLIET, ILLINOIS 60431 (815) 744-4200	USER NAME = BenL	DESIGNED BRL	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SOIL BORING LOGS STRUCTURE NO. 099-4401	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		CHECKED AJS	REVISED -			3752	10-00046-00-BR	WILL	109	71
	PLOT SCALE =	DRAWN BJF	REVISED -						CONTRACT NO. 61D22	
	PLOT DATE = 11/16/2017	CHECKED BRL	REVISED -		SHEET NO. 23 OF 24 SHEETS			ILLINOIS	FED. AID PROJECT	

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAP 105	82-0611Z-00-BR	WILL	15	9
FED. ROAD DIST. NO. 7	ILLINOIS	PROJECT		



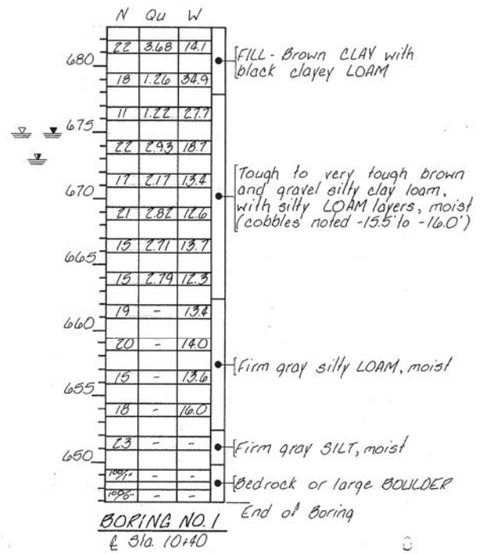
ELEVATION



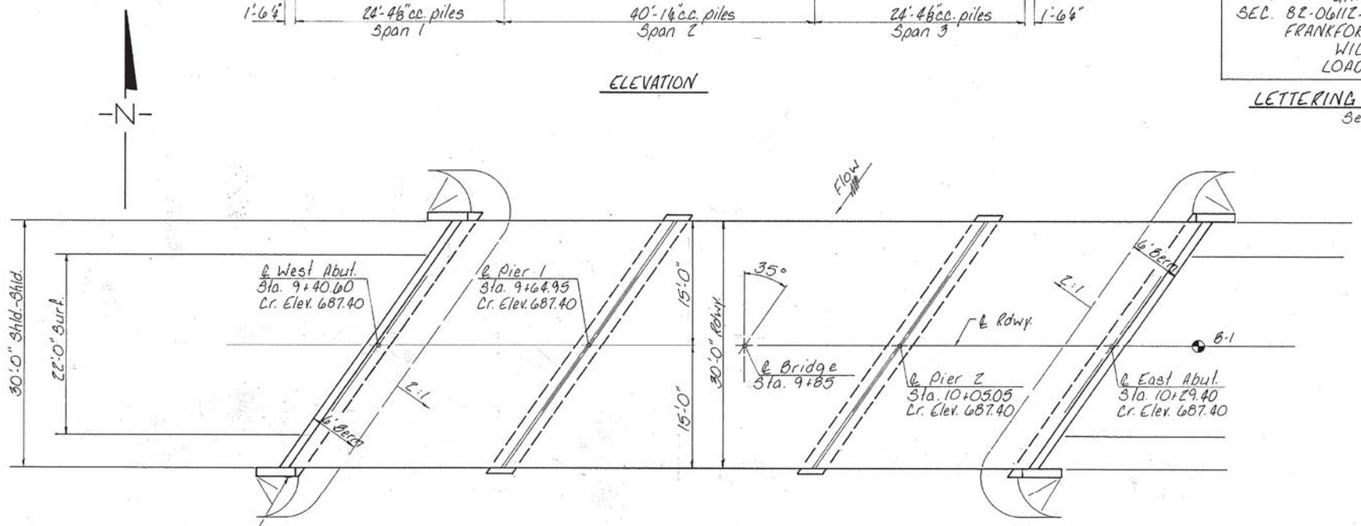
LOCATION PLAN

STRUCTURE NO. 099-4401
UNION DITCH
SEC. 82-0611Z-00-BR BUILT 198_
FRANKFORT ROAD DISTRICT
WILL COUNTY
LOADING HS20

LETTERING FOR NAME PLATE
See Std. 2113



BORING NO. 1
℄ Sta. 10+40



See sheet 13 for Name Plate Location

PLAN

BORING DATA

N = Standard Penetration Test - Blows per foot to drive 2" O.D. Split Spoon Sampler 12" with 140# hammer falling 30"
Qu = Unconfined Compressive Strength - T/sf
W = Water Content - Percentage oven dry weight - %
▽ = Water Level While drilling
▽ = Water Level at completion
▽ = Water Level after 24 hours

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Leveling Binder (Machine Method)	Ton	11		11
Class 'A' Concrete	Cu. Yd.		41.1	41.1
Reinforcement Bars	Pound		5,060	5,060
Steel Railing, Type S-1	Lin. Ft.	186		186
Name Plates	Each		1	1
Metal Pile Shells 12"	Lin. Ft.		540	540
Test Pile Metal Shells	Each		2	2
Bit. Conc. Surf. Crse., Cl. I	Ton	25		25
Bituminous Materials (Prime Coat)	Gallon	24		24
Precast Prestressed Concrete Deck Beams (17" Depth)	Sq. Ft.	2,700		2,700

GENERAL NOTES

The contractor shall drive 2 concrete test piles in permanent locations, one at the West Abutment and one at Pier 2, as directed by the Engineer, before ordering the remainder of the piles.

WATERWAY DATA

Drainage Area ____ 18.1 Sq. Mi.
*Existing Opening (25 yr.) ____ 160 Sq. Ft.
Required Opening (25 yr.) ____ 300 Sq. Ft.
Proposed Opening (25 yr.) ____ 300 Sq. Ft.
Design Discharge (25 yr.) ____ 710 C.F.S.
Created Head (25 yr.) ____ 0.2 Ft.
100 yr. Discharge ____ 940 C.F.S.
100 yr. Created Head ____ 0.2 Ft.
*Existing Approach Opening (25 yr.) 140 Sq. Ft.
The requirements of Division of Waterway Resources have been fulfilled in accordance with Statewide Permit No. 2

DESIGN STRESSES

f'c = 5,000 psi (Prestressed Beams)
f'ci = 4,000 psi (Prestressed Beams)
f'c = 1,400 psi (Class 'A' Concrete)
f's = 270,000 psi (Prestressed Strands)
fsi = 189,000 psi (Prestressed Strands)
fs = 20,000 psi (Reinf. Bars - Field Units)
fy = 60,000 psi (Reinf. Bars - Precast Units)
n = 9 (Class 'A' Concrete)
Loading HS 20-44
Design Specifications: AASHTO 1977 & 1978-1982 Interims.
25 #/Sq. Ft. included in dead load for future wearing surface.

I certify that to the best of my knowledge, information, and belief, that this bridge design is structurally adequate for the design loading shown on the plans. The design is an economical one for the style of structure and complies with requirements of the current "AASHTO Standard Specifications for Highway Bridges".

Fred J. Stone Jr.
Illinois Structural No. 2934

FRED J. STONE JR. REGISTERED 2934 ILLINOIS STRUCTURAL ENGINEER

GENERAL PLAN & ELEVATION
SECTION 82-0611Z-00-BR
FRANKFORT ROAD DISTRICT
WILL COUNTY
STATION 9+85

COLLINS AND RICE
CONSULTING ENGINEERS

DESIGNED F.S. CHECKED J.K.K.
DRAWN R.N. DATE 9-30-83 NO. 1780

FILE NAME = S:\JOL\6680-6699\6628\813\Micros\CADD\Sheets\099-4401-XXX-824-EOPE.dgn

STRAND ASSOCIATES®
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

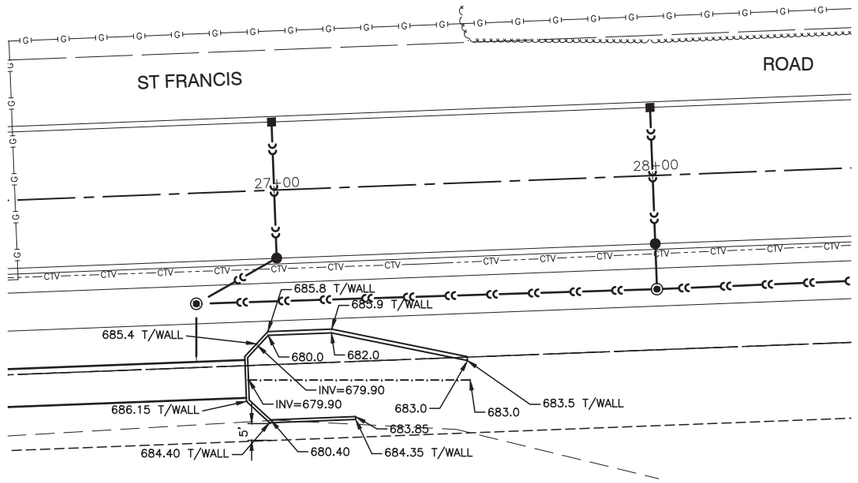
USER NAME = BenL	DESIGNED BRL	REVISED -	
	CHECKED AJS	REVISED -	
PLOT SCALE =	DRAWN BJF	REVISED -	
PLOT DATE = 11/16/2017	CHECKED BRL	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING GENERAL PLAN AND ELEVATION
STRUCTURE NO. 099-4401
SHEET NO. 24 OF 24 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00046-00-BR	WILL	109	72
		CONTRACT NO. 61D22		
	ILLINOIS	FED. AID PROJECT		

SECTION 14, TOWNSHIP 35, RANGE 12

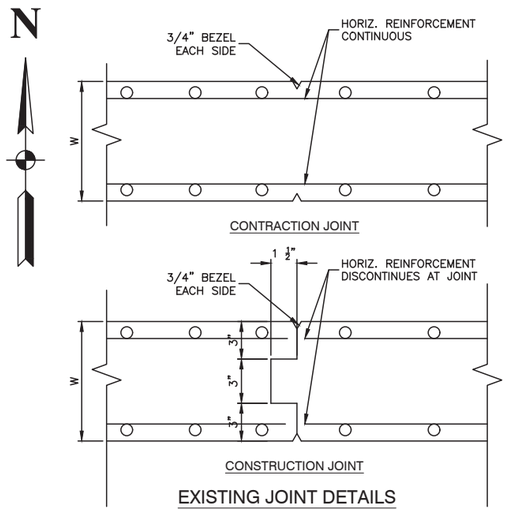


PROPOSED RETAINING WALL ENLARGED PLAN

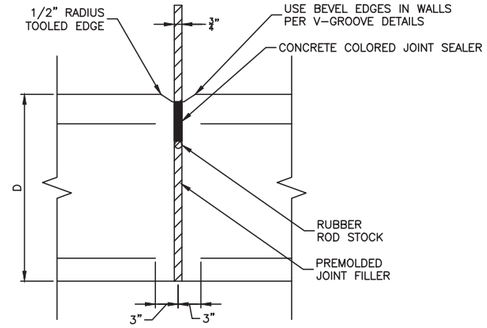
SCALE 1"=20'

BILL OF MATERIALS

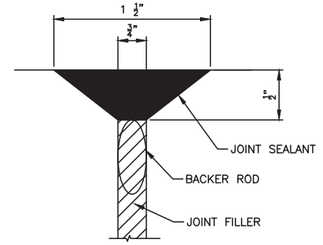
CONCRETE STRUCTURES – 50 CY
REINFORCEMENT BARS, EPOXY COATED – 1,205 LBS



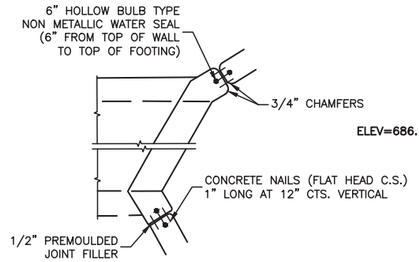
EXISTING JOINT DETAILS



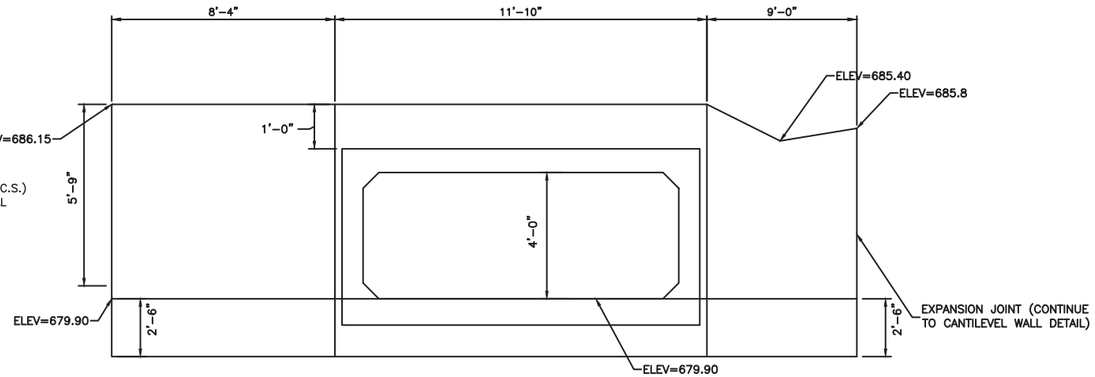
TYPICAL DETAIL AT JOINT SEALANT



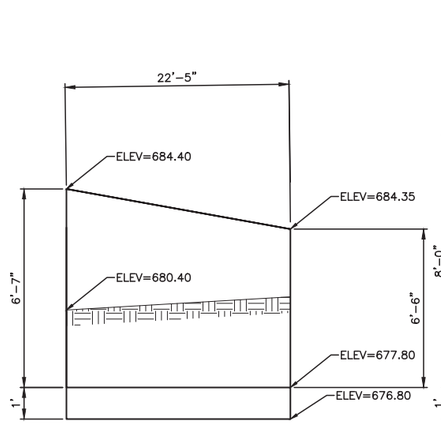
TYPICAL DETAIL AT JOINT SEALANT



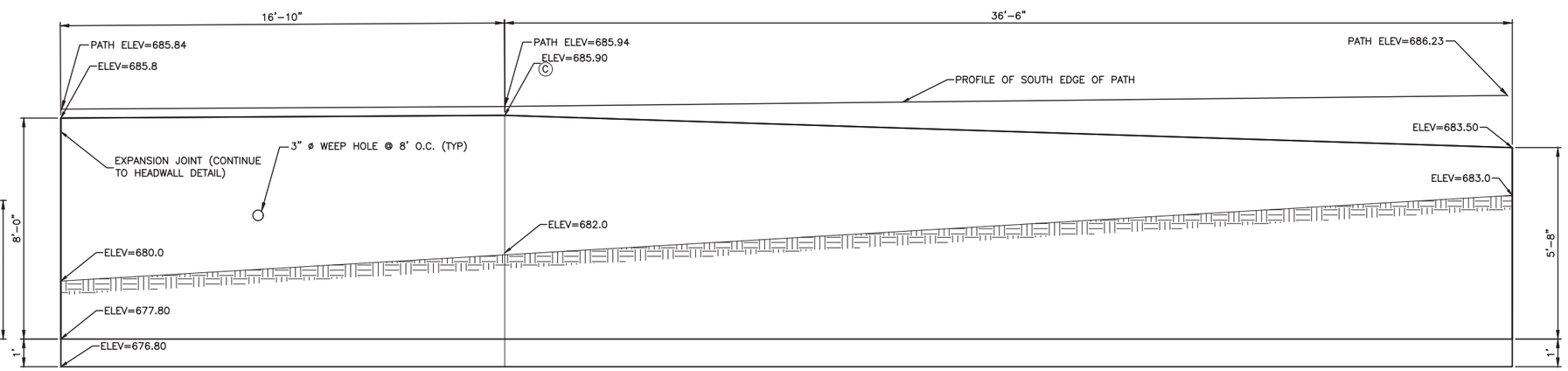
CORNER DETAIL



HEADWALL ELEVATION VIEW
NOT TO SCALE



CANTILEVER WALL ELEVATION-EAST SIDE
NOT TO SCALE

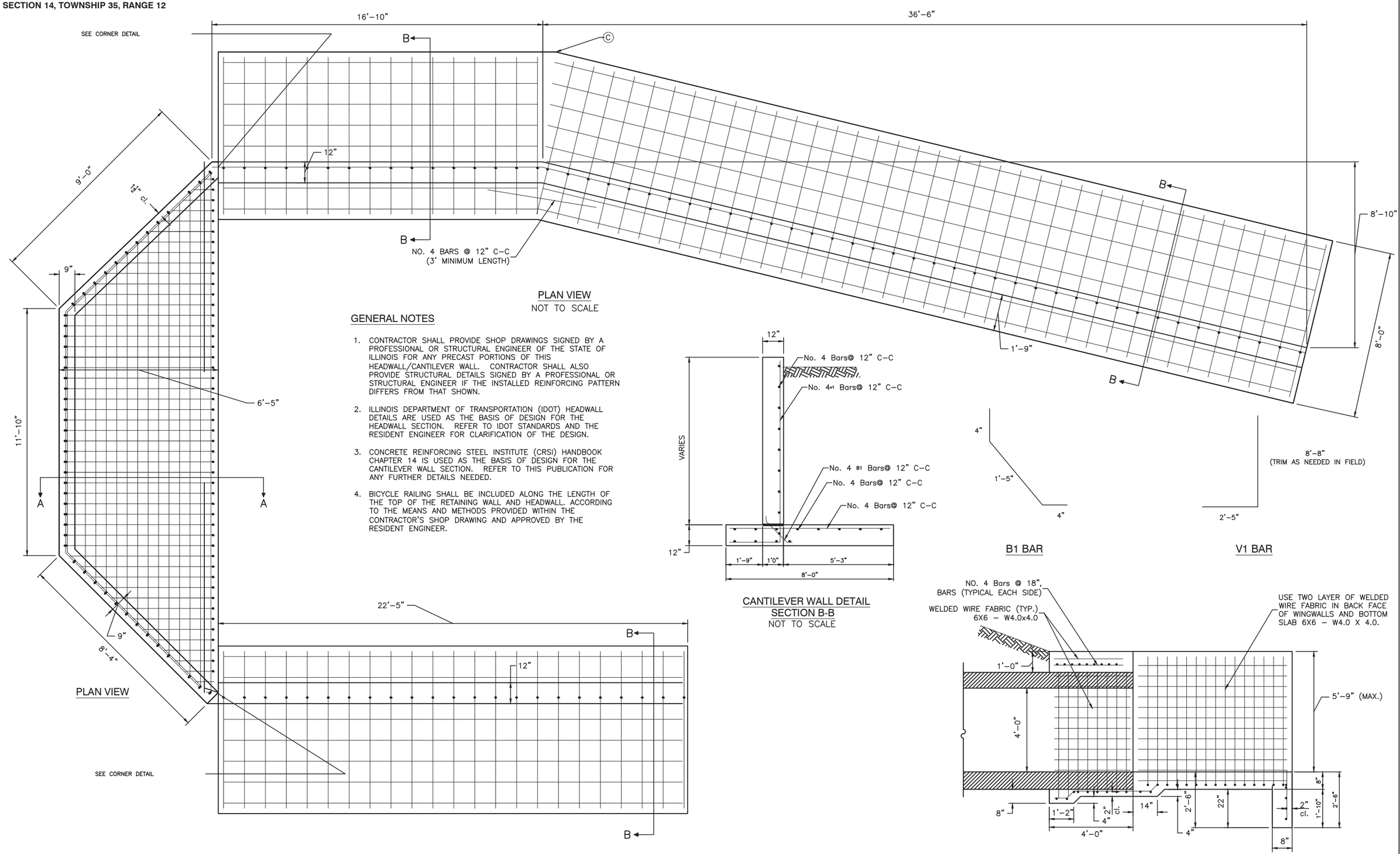


CANTILEVER WALL ELEVATION-WEST SIDE
NOT TO SCALE

FILE NAME = 10423_12-STRM-01 - IDOT P01	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION HEADWALL AND CANTILEVER WALL DETAILS - STA 26+90		
		CHECKED — HLG	REVISED —				
	PLOT SCALE =	DRAWN — MED	REVISED —				
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —		SCALE:	SHEET NO. 73 OF 109 SHEETS	STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	73
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

SECTION 14, TOWNSHIP 35, RANGE 12



PLAN VIEW
NOT TO SCALE

GENERAL NOTES

1. CONTRACTOR SHALL PROVIDE SHOP DRAWINGS SIGNED BY A PROFESSIONAL OR STRUCTURAL ENGINEER OF THE STATE OF ILLINOIS FOR ANY PRECAST PORTIONS OF THIS HEADWALL/CANTILEVER WALL. CONTRACTOR SHALL ALSO PROVIDE STRUCTURAL DETAILS SIGNED BY A PROFESSIONAL OR STRUCTURAL ENGINEER IF THE INSTALLED REINFORCING PATTERN DIFFERS FROM THAT SHOWN.
2. ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT) HEADWALL DETAILS ARE USED AS THE BASIS OF DESIGN FOR THE HEADWALL SECTION. REFER TO IDOT STANDARDS AND THE RESIDENT ENGINEER FOR CLARIFICATION OF THE DESIGN.
3. CONCRETE REINFORCING STEEL INSTITUTE (CRSI) HANDBOOK CHAPTER 14 IS USED AS THE BASIS OF DESIGN FOR THE CANTILEVER WALL SECTION. REFER TO THIS PUBLICATION FOR ANY FURTHER DETAILS NEEDED.
4. BICYCLE RAILING SHALL BE INCLUDED ALONG THE LENGTH OF THE TOP OF THE RETAINING WALL AND HEADWALL. ACCORDING TO THE MEANS AND METHODS PROVIDED WITHIN THE CONTRACTOR'S SHOP DRAWING AND APPROVED BY THE RESIDENT ENGINEER.

CANTILEVER WALL DETAIL
SECTION B-B
NOT TO SCALE

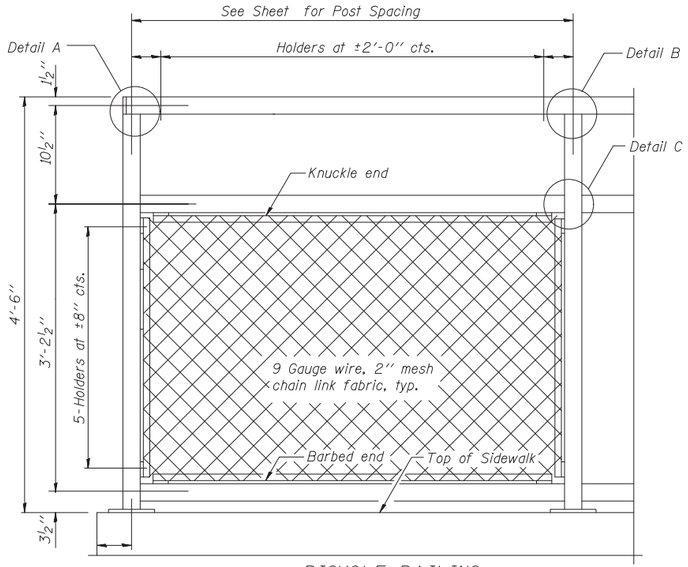
B1 BAR

V1 BAR

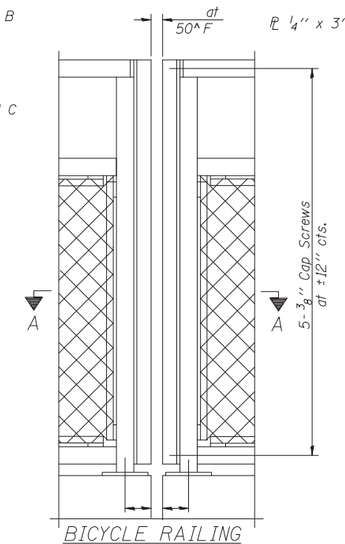
SECTION A-A
NOT TO SCALE

FILE NAME = 10423_12-STRM-01 - IDOT P02	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION HEADWALL AND CANTILEVER WALL DETAILS - STA 26+90
		CHECKED — HLG	REVISED —		
	PLOT SCALE =	DRAWN — MED	REVISED —		
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —		SCALE: SHEET NO. 74 OF 109 SHEETS STA. TO STA.

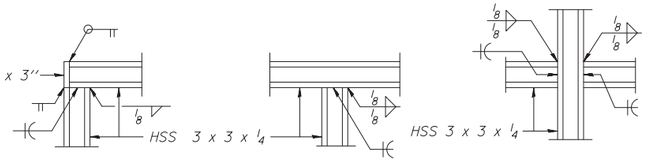
F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	74
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



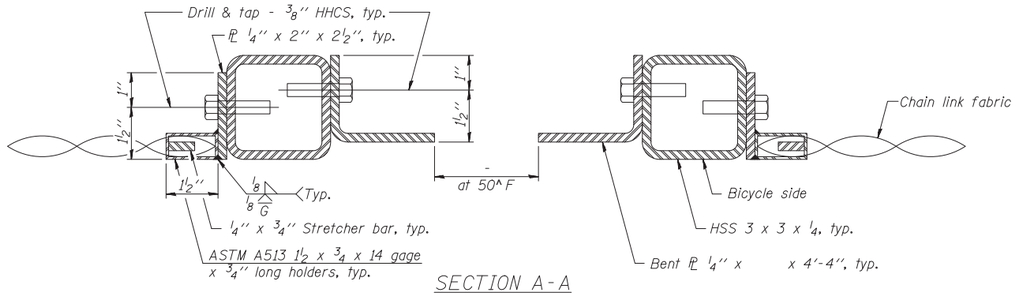
BICYCLE RAILING



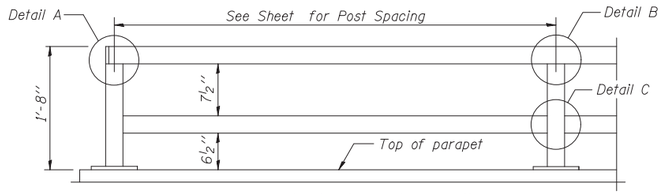
BICYCLE RAILING



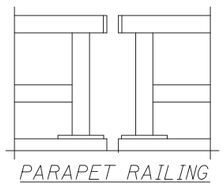
DETAIL A DETAIL B DETAIL C



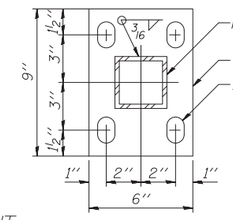
SECTION A-A



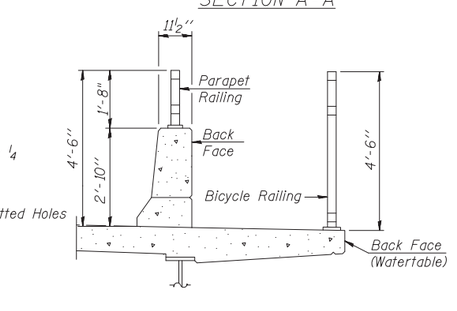
PARAPET RAILING
ELEVATION
(Inside Face of Two Element Rail)



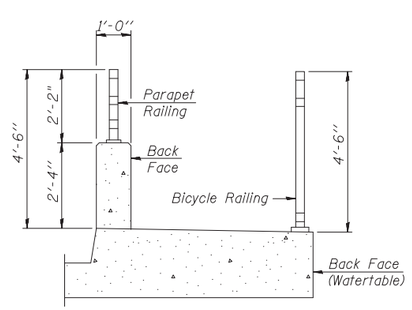
PARAPET RAILING
ELEVATION AT EXPANSION JOINT
(Two Element Rail Shown - Three Element Rail Similar)



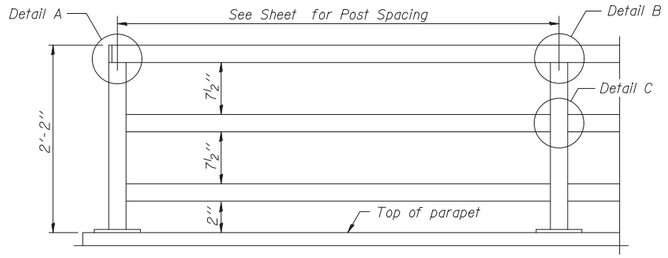
BASE PL



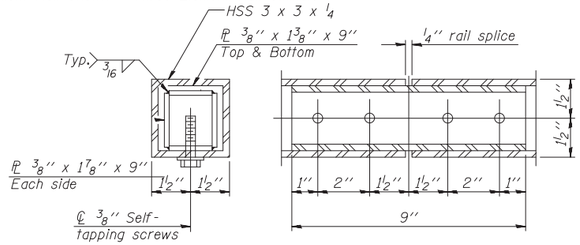
SECTION THRU DECK



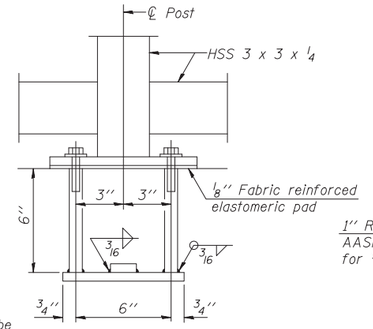
SECTION THRU SIDEWALK



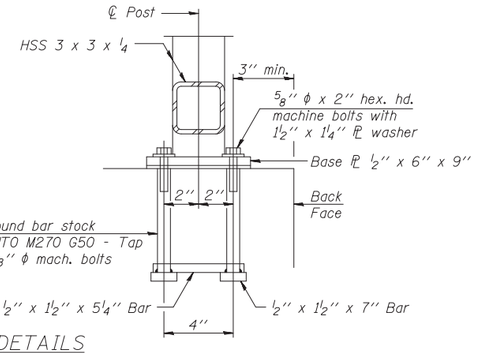
PARAPET RAILING
ELEVATION
(Inside Face of Three Element Rail)



RAIL SPLICE



ANCHOR BOLT DETAILS



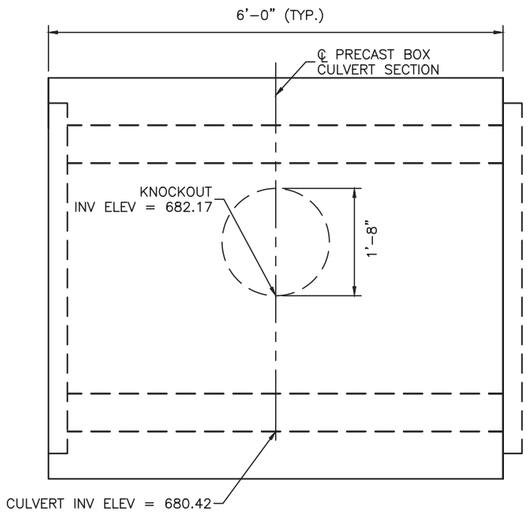
Notes:

1. All structural steel tubing, post and railing, for parapet railing shall be CVN tested according to 1006.34(b) of the Standard Specifications.
2. The designer should add the appropriate note as applicable.
   A. When railing is galvanized:
      All steel rail elements shall be galvanized according to Article 509.05 of the Standard Specifications.
   B. When railing is painted:
      All post, railing, splices, anchor devices, and bent plates shall be painted using the (List the appropriate paint system for Structural Steel).

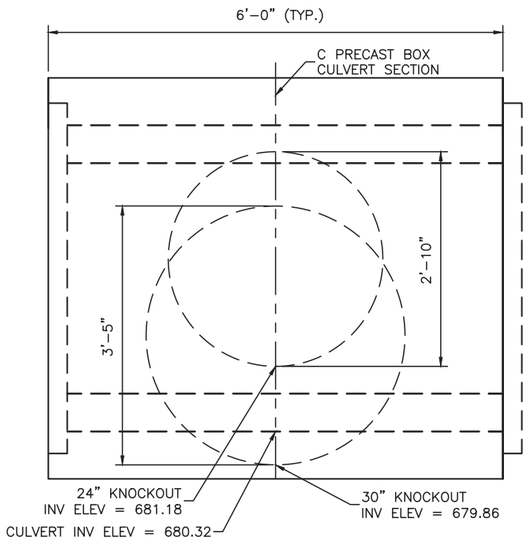
In lieu of the cast-in-place anchor device shown, the Contractor has the option of drilling and setting ⅝" ϕ anchor rods according to Article 509.06 of the Standard Specifications. Embedment shall be according to the manufacturer's specifications.

R-29 07-05-16 (10'-0" Maximum Post Spacing)

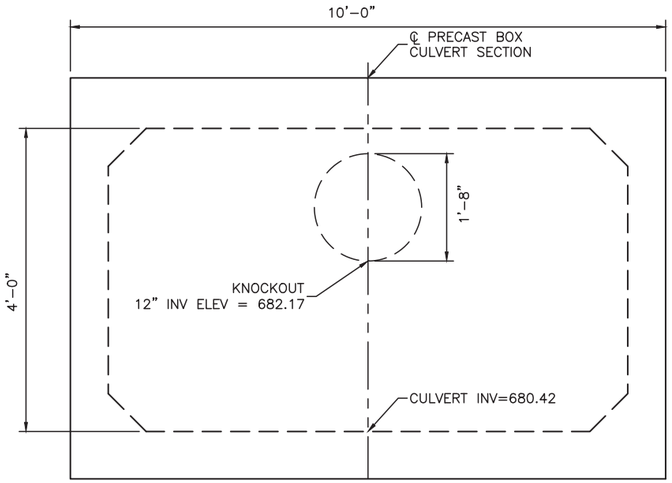
FILE NAME = 10423_12-DTLS-01 - IDOT P01 (3)	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BICYCLE RAILING DETAIL CONSTRUCTION DETAILS	F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		CHECKED — HLG	REVISED —			3752	10-00045-00-WR	WILL	109	75
	PLOT SCALE =	DRAWN — ACAD	REVISED —					CONTRACT NO. 61D22		
	PLOT DATE = 06-09-16	CHECKED — ACAD	REVISED —		SCALE: SHEET NO. 75 OF 109 SHEETS STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



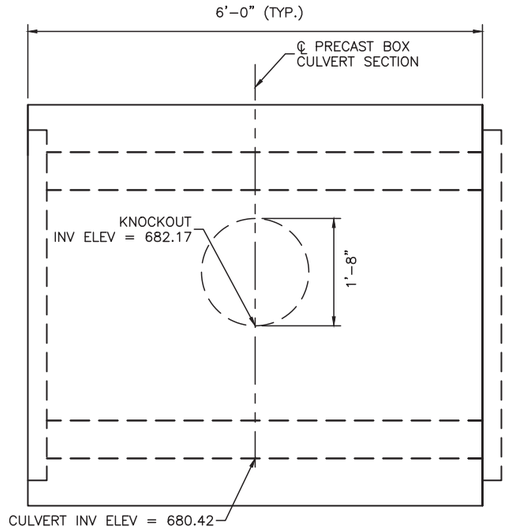
12" PIPE CONNECTION DETAIL
STA 17+50



24" & 30" PIPE CONNECTION DETAIL
STA 19+29.66



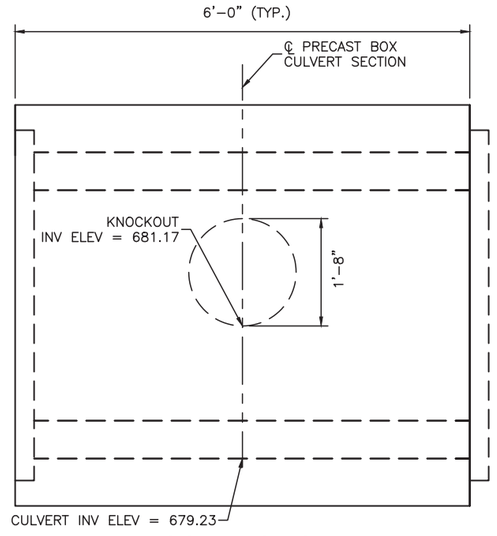
12" PIPE CONNECTION DETAIL
STA 17+45



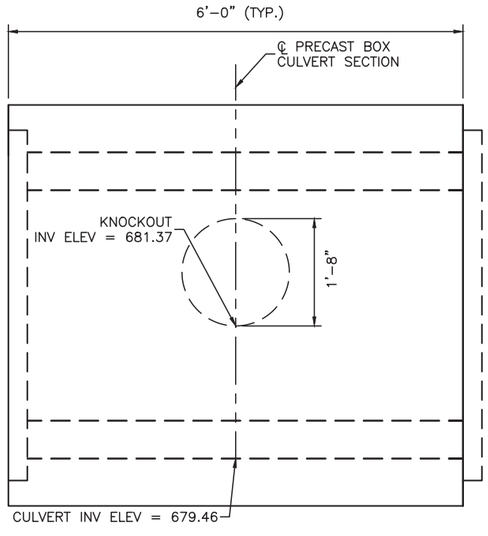
12" PIPE CONNECTION DETAIL
STA 20+50

NOTE:

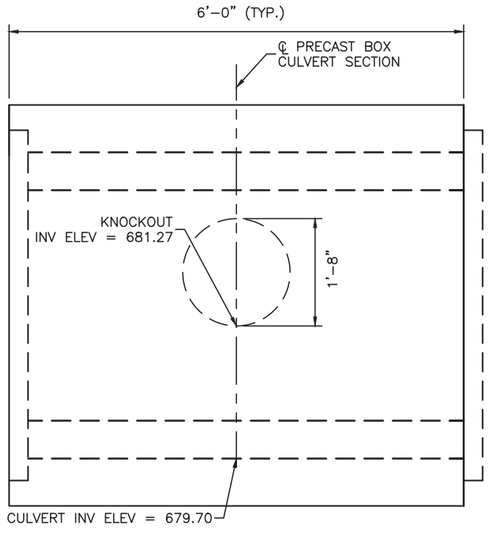
KNOCKOUTS FOR PROPOSED STORM SEWER CONNECTIONS HAVE BEEN OVERSIZED 4 INCHES IN DIAMETER TO ACCOMMODATE A CORE BOOT CONNECTION.



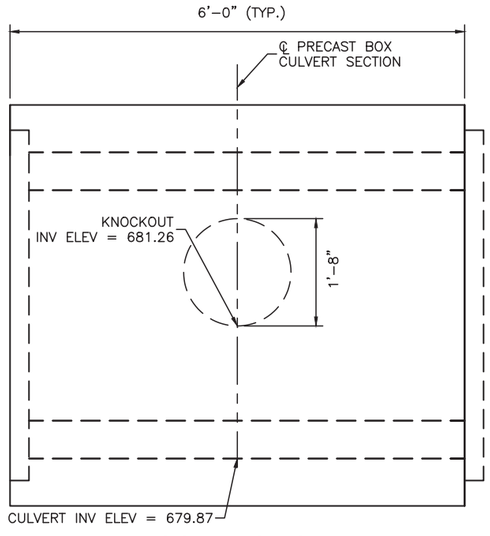
12" PIPE CONNECTION DETAIL
STA 24+00



12" PIPE CONNECTION DETAIL
STA 25+00



12" PIPE CONNECTION DETAIL
STA 26+02.84



12" PIPE CONNECTION DETAIL
STA 26+80

GENERAL NOTES

1. DESIGNS SHOWN CONFORM TO ASTM C1577. FILL HEIGHT SHALL BE ASSUMED AS <2.0 FT.
2. REFER TO ASTM C1577 AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION CULVERT MANUAL FOR INFORMATION OR DETAILS NOT SHOWN.
3. ALL CONCRETE SHALL BE CLASS PC ACCORDING TO SECTION 1020, AND SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH PER ASTM C 1577.
4. THE CONTRACTOR SHALL FURNISH SHOP DRAWINGS FOR THIS CULVERT ACCORDING TO THE DESIGN SHOWN HERE OR ANY ALTERNATE DESIGN THAT EQUAL TO OR EXCEEDS THIS BOX DESIGN FOR A FILL HEIGHT LESS THAN 2 FT. SHOP DRAWINGS SHALL BE SUBMITTED TO AND APPROVED BY THE ENGINEER PRIOR TO FABRICATION.
5. JOINTS FOR PRECAST SECTIONS SHALL BE BELL AND SPIGOT TYPE AND SHALL BE SEALED WITH MASTIC OR SAND/CEMENT GROUT.
6. LIFTING HOLES SHALL BE FILLED WITH CONCRETE PLUGS AND MASTIC AFTER THE BOX SECTIONS ARE IN PLACE.
7. DESIGN AND CONSTRUCTION METHODS SHALL CONFORM TO SECTION 540 OF THE ILLINOIS STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.

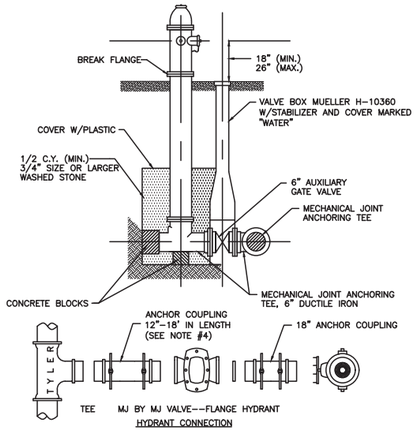
FILE NAME = 10423_12-DTLS-01 - IDOT P01	USER NAME =	DESIGNED — GA	REVISED —
		CHECKED — HLG	REVISED —
	PLOT SCALE =	DRAWN — ACAD	REVISED —
	PLOT DATE = 06-09-16	CHECKED — ACAD	REVISED —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
10'x4' BOX CULVERT DETAIL INFORMATION

SCALE: | SHEET NO. 76 OF 109 SHEETS | STA. | TO STA.

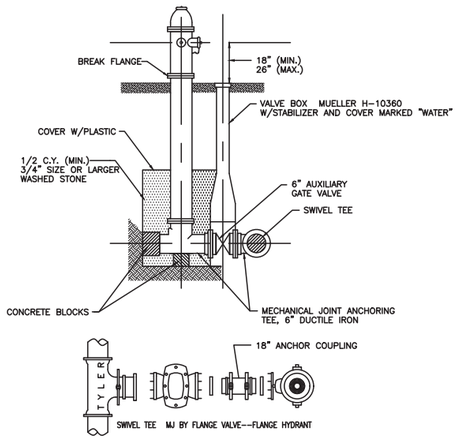
F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	76
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



HYDRANT CONNECTION

NOTES:
1. ALL CASTINGS SHALL BE MADE IN THE U.S.A. WITH U.S.A. MATERIALS. FIRE HYDRANTS SHALL MEET AWWA C-502 AND SHALL BE MUELLER "CENTURION" A-423, WITH AN OIL- ENCASED BONNET, STAINLESS STEEL BOLTS, 5-1/4" VALVE OPENING, TWO 2-1/2" HOSE NOZZLES AND ONE 4-1/2" PUMPER NOZZLE. THREADS SHALL CONFORM TO NATIONAL STANDARD SPECIFICATIONS.
2. HYDRANTS SHALL BE INSTALLED NO CLOSER THAN THREE FEET NOR FARTHER THAN 8 FEET FROM THE BACK OF CURB. NO HYDRANT SHALL BE INSTALLED WITHIN 48" OF ANY OBSTRUCTION NOR SHALL ANY OBSTRUCTION BE PLACED WITHIN 48" OF A HYDRANT.
3. THE HYDRANTS SHALL BE PAINTED RED BY THE MANUFACTURER.
4. ANCHOR COUPLING TO BE INSTALLED AT A LENGTH TO PROVIDE A MINIMUM 1' BETWEEN BACK OF CURB/EDGE OF PAVEMENT AND AUXILLARY VALVE.
5. AUXILLARY VALVES SHALL ALWAYS BE INSTALLED IN THE PARKWAY. INSTALLATION IN PAVED SURFACE IS PROHIBITED.

FIRE HYDRANT INSTALLATION (WHERE MAIN IS IN THE PAVEMENT)

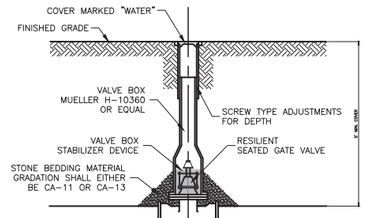


SWIVEL TEE - HYDRANT CONNECTION

NOTES:
1. ALL CASTINGS SHALL BE MADE IN THE U.S.A. WITH U.S.A. MATERIALS. FIRE HYDRANTS SHALL MEET AWWA C-502 AND SHALL BE MUELLER "CENTURION" A-423 WITH A 5-1/4" VALVE OPENING, TWO 2-1/2" HOSE NOZZLES AND ONE 4-1/2" PUMPER NOZZLE. THREADS SHALL CONFORM TO NATIONAL STANDARD SPECIFICATIONS.
2. HYDRANTS SHALL BE INSTALLED NO CLOSER THAN THREE FEET NOR FARTHER THAN 8 FEET FROM THE BACK OF CURB. NO HYDRANT SHALL BE INSTALLED WITHIN 48" OF ANY OBSTRUCTION NOR SHALL ANY OBSTRUCTION BE PLACED WITHIN 48" OF A HYDRANT.
3. THE HYDRANTS SHALL BE PAINTED RED BY THE MANUFACTURER.

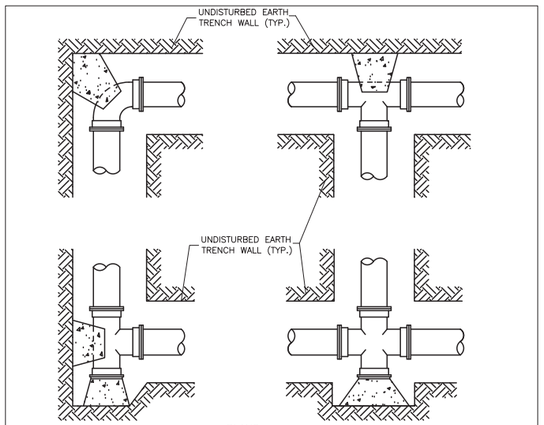
FIRE HYDRANT INSTALLATION W/SWIVEL TEE (WHERE MAIN IS NOT IN PAVEMENT)

EXHIBIT 5E
JUNE 2007
REVISED JANUARY 2009



NOTES:
1. ALL VALVES SHALL OPEN COUNTER CLOCKWISE AND CLOSE CLOCKWISE WITH NON-RISING STEM.
2. STABILIZER DEVICE MUST BE APPROVED BY THE ENGINEER AND VILLAGE PRIOR TO MATERIAL ORDERING.

TYPICAL VALVE IN BOX INSTALLATION



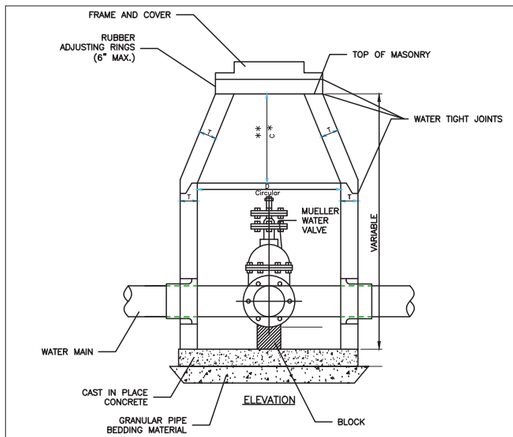
PLANS

SECTION

NOTES:
1. PROVIDE PRECAST OR CAST-IN-PLACE CONCRETE THRUST BLOCKS OF ADEQUATE SIZE (12" MINIMUM) AND THRUST BEARING SURFACE TO PREVENT MOVEMENT OF PIPELINE UNDER PRESSURE.
2. PLACE THE BASE AND THE THRUST BEARING SIDES OF THRUST BLOCK DIRECTLY AGAINST UNDISTURBED EARTH.
3. PLACE THRUST BLOCKING SO THE FITTING JOINTS WILL BE ACCESSIBLE FOR REPAIR.

TYPICAL THRUST BLOCK INSTALLATIONS

EXHIBIT 5C
JUNE 2007



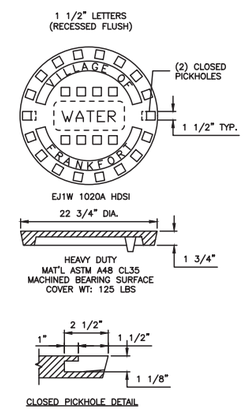
ELEVATION

DIAMETER OF WATER MAIN	D
10" AND UNDER	4'-0"
12" AND OVER	5'-0"

NOTES:
1. CONCENTRIC CONE REQUIRED.
2. USE 4'-0" DIAMETER FOR WATER MAIN SIZES 4" THRU 10". 5'-0"ø FOR SIZES 12" THRU 20". 6'-0"ø FOR 20" OR GREATER.
3. VALVE VAULT TO CONFORM TO ASTM C478.
4. ALL VALVES SMALLER THAN 12" SHALL BE AWWA C509, CAST IRON BODY, BRONZE FITTED, MODIFIED WEDGE DISC, RESILIENT SEAT TYPE WITH NON-RISING STEM AND O-RING PACKING DESIGNED FOR 200 POUND WORKING PRESSURE, MUELLER A2360-20.
5. ALL VALVES 12" AND LARGER SHALL BE BUTTERFLY VALVES MUELLER 3211-20. ALL VALVES SHALL OPEN COUNTER CLOCKWISE WITH NON-RISING STEM (EXCEPT HAND VALVES).
6. ALL COVERS SHALL BE STAMPED "VILLAGE OF FRANKFORT" PER EXHIBIT 2B.

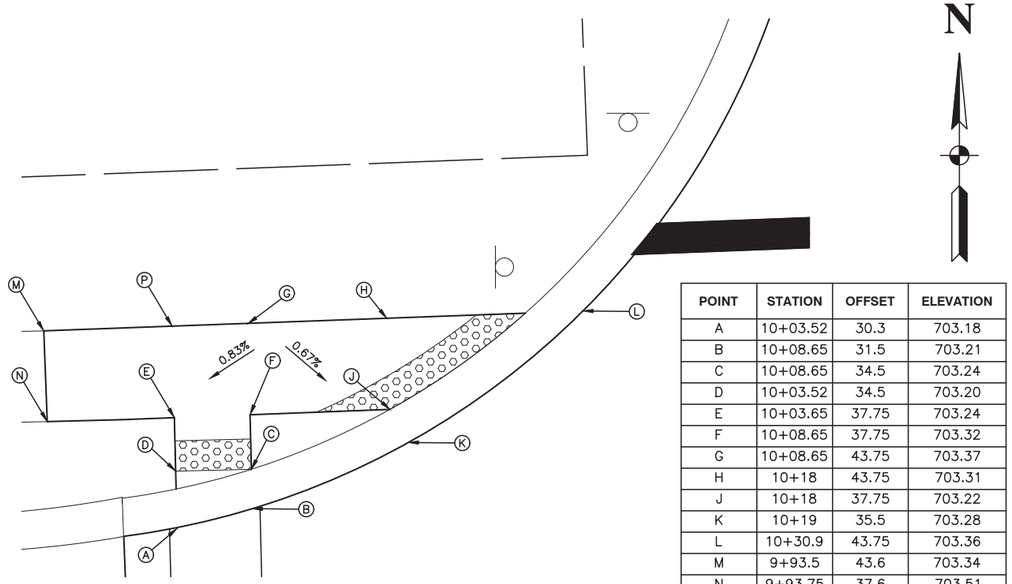
VALVE VAULT DETAIL

EXHIBIT 5D
JUNE 2007



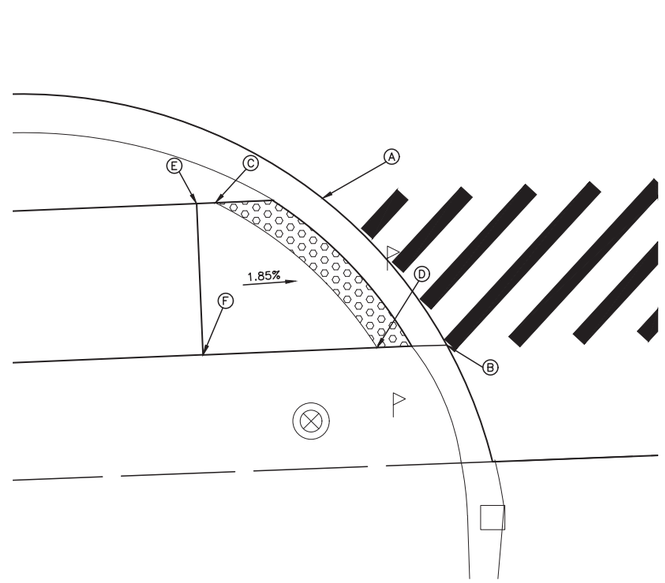
SPECIAL LETTERED MANHOLE COVER

FILE NAME = 10423_12-DTLS-01 - IDOT P01 (2)	USER NAME =	DESIGNED - GA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION CONSTRUCTION DETAILS			F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		CHECKED - HLG	REVISED -					3752	10-00045-00-WR	WILL	109	77
	PLOT SCALE =	DRAWN - ACAD	REVISED -							CONTRACT NO. 61D22		
	PLOT DATE = 06-09-16	CHECKED - ACAD	REVISED -		SCALE:	SHEET NO. 77 OF 109 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



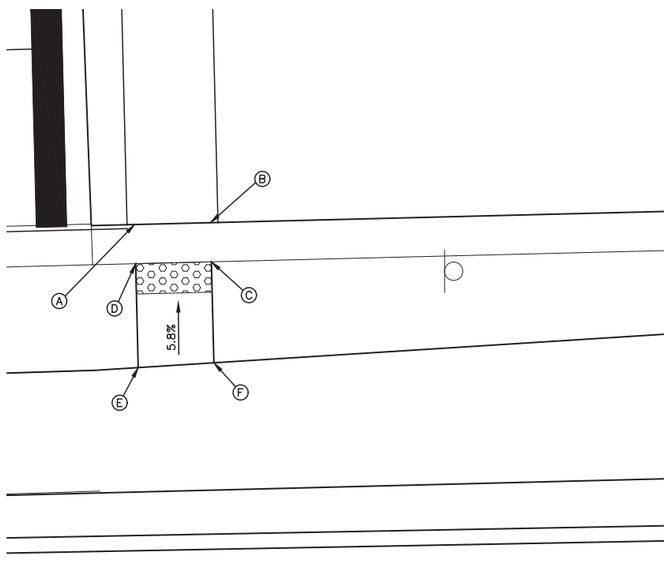
POINT	STATION	OFFSET	ELEVATION
A	10+03.52	30.3	703.18
B	10+08.65	31.5	703.21
C	10+08.65	34.5	703.24
D	10+03.52	34.5	703.20
E	10+03.65	37.75	703.24
F	10+08.65	37.75	703.32
G	10+08.65	43.75	703.37
H	10+18	43.75	703.31
J	10+18	37.75	703.22
K	10+19	35.5	703.28
L	10+30.9	43.75	703.36
M	9+93.5	43.6	703.34
N	9+93.75	37.6	703.51
P	10+03.65	43.75	703.29

ST FRANCIS ROAD AT 88TH AVENUE
NW CORNER



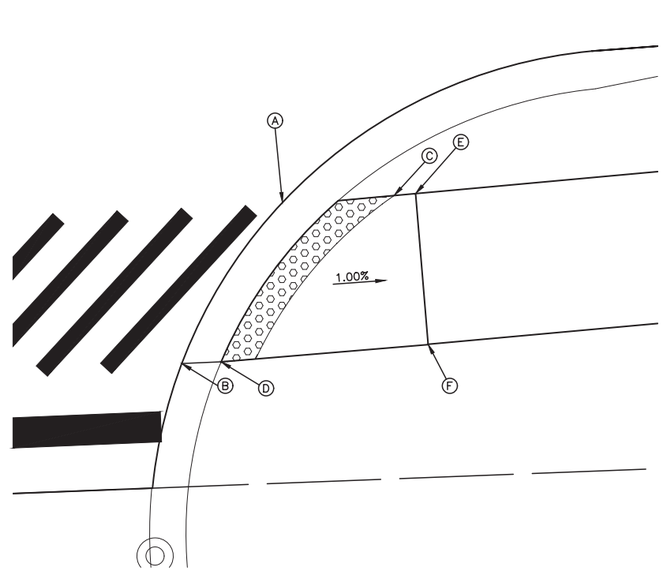
POINT	STATION	OFFSET	ELEVATION
A	11+74.65	28.2	703.34
B	11+82.48	38.2	703.26
C	11+67.6	28.2	703.40
D	11+77.9	38.2	703.23
E	11+66.40	28.2	703.43
F	11+66.40	38.2	703.49

ST FRANCIS ROAD AT FALLING WATER CIRCLE
SW CORNER



POINT	STATION	OFFSET	ELEVATION
A	10+02.86	19.55	703.26
B	10+07.9	19.61	703.32
C	10+07.6	23.19	703.26
D	10+03.1	23.13	703.20
E	10+02.73	29.00	703.62
F	10+07.74	28.86	703.69

ST FRANCIS ROAD AT 88TH AVENUE
SW CORNER



POINT	STATION	OFFSET	ELEVATION
A	12+49.92	27.5	702.86
B	12+42.89	37.9	702.97
C	12+57.3	27.3	702.71
D	12+47.7	37.8	702.91
E	12+58.72	27.3	702.69
F	12+59.17	37.3	702.79

ST FRANCIS ROAD AT FALLING WATER CIRCLE
SE CORNER

FILE NAME = 10423_12-DTLS-03 - IDOT P01

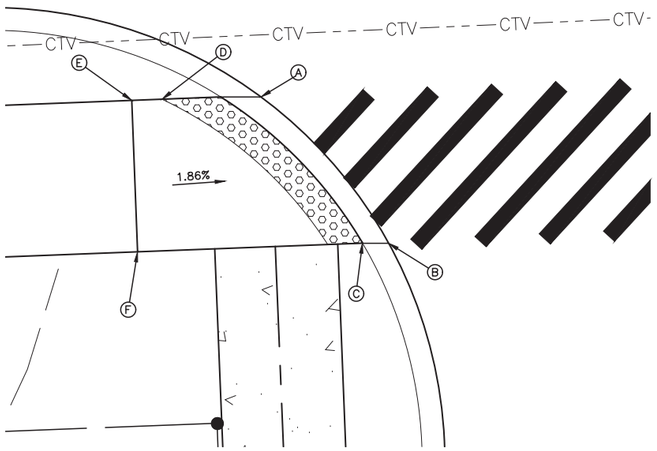
USER NAME =	DESIGNED — GA	REVISED —
	CHECKED — HLG	REVISED —
PLOT SCALE =	DRAWN — ACAD	REVISED —
PLOT DATE = 06-09-16	CHECKED — ACAD	REVISED —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
ADA RAMP DETAILS

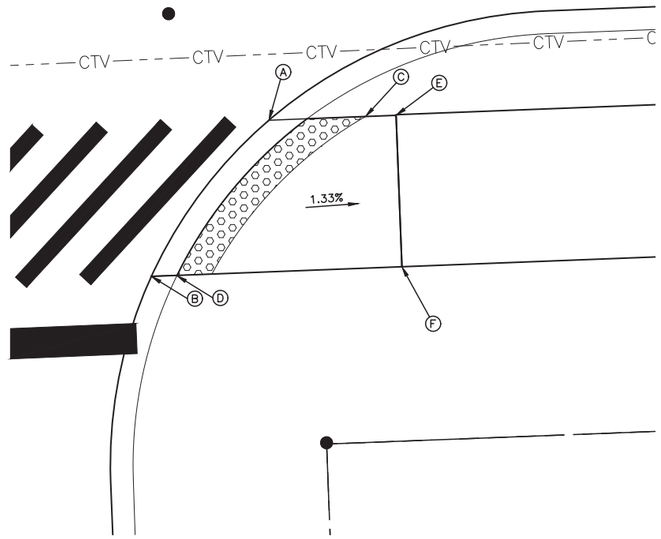
SCALE: 1"=5' | SHEET NO. 78 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	78
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



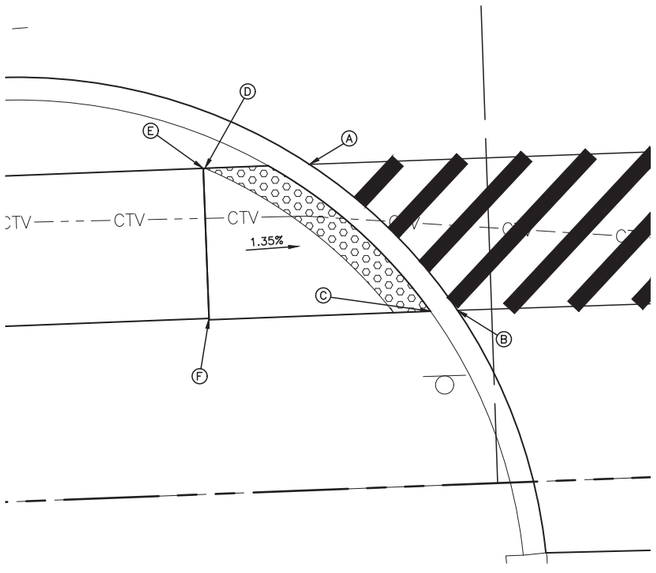
POINT	STATION	OFFSET	ELEVATION
A	23+79.2	24.5	687.91
B	23+87.3	34.5	687.82
C	23+83.3	34.50	687.82
D	23+72	24.50	688.00
E	23+70.7	24.5	688.02
F	23+70.7	34.5	688.08

ST FRANCIS ROAD AT WALNUT CREEK DR
SW CORNER



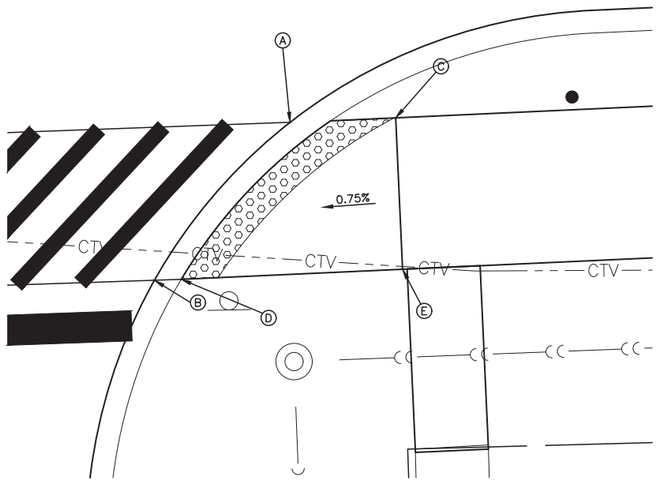
POINT	STATION	OFFSET	ELEVATION
A	24+52.6	24.5	687.34
B	24+44.5	34.5	687.52
C	24+59	24.5	687.24
D	24+48.5	34.5	687.50
E	24+61.0	24.5	687.22
F	24+61.0	34.5	687.32

ST FRANCIS ROAD AT WALNUT CREEK DR
SE CORNER



POINT	STATION	OFFSET	ELEVATION
A	37+02.1	24.5	691.9
B	37+11.5	34.5	691.65
C	37+07.25	34.50	691.66
D	36+95.0	24.5	691.99
E	36+95.0	24.5	691.99
F	36+95.1	34.5	691.90

ST FRANCIS ROAD AT 84TH AVENUE
SW CORNER



POINT	STATION	OFFSET	ELEVATION
A	37+58.4	24.6	691.68
B	37+48.9	34.5	691.40
C	37+65.4	24.6	691.75
D	37+53	34.54	691.43
E	37+65.3	34.5	691.58

ST FRANCIS ROAD AT 84TH AVENUE
SE CORNER

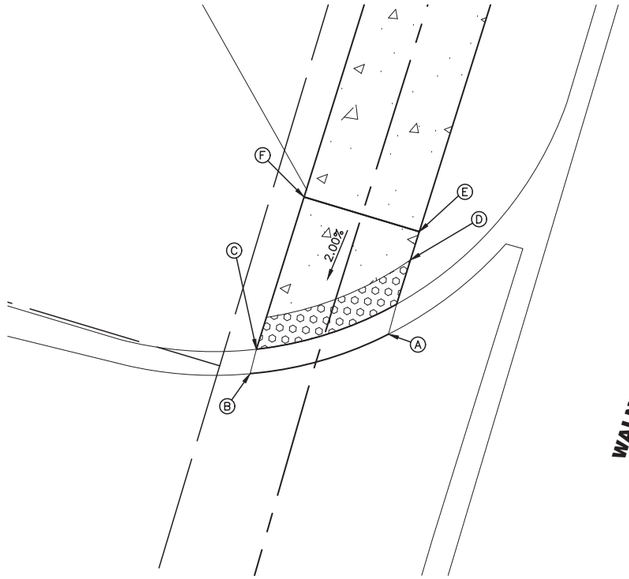
FILE NAME = 10423_12-DTLS-03 - IDOT P02	USER NAME =	DESIGNED — GA	REVISED —
		CHECKED — HLG	REVISED —
	PLOT SCALE =	DRAWN — ACAD	REVISED —
	PLOT DATE = 06-09-16	CHECKED — ACAD	REVISED —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
ADA RAMP DETAILS

SCALE: 1"=5' | SHEET NO. 79 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	79
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

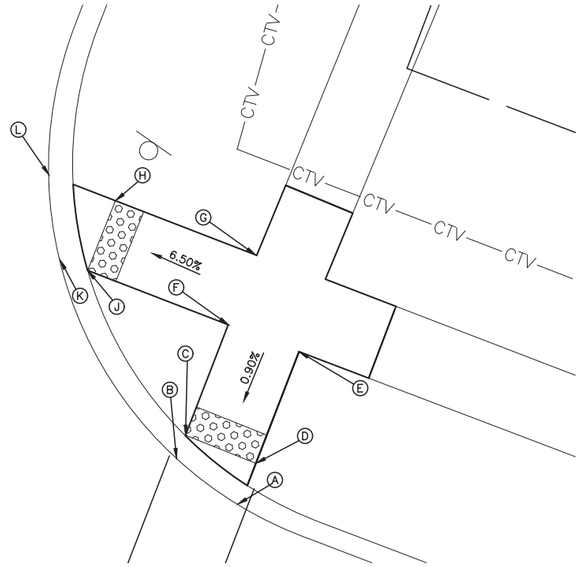


WALNUT CREEK DRIVE

N

POINT	STATION	OFFSET	ELEVATION
A	9+98.6	4	697.06
B	9+93.48	4	696.61
C	9+97.25	4	696.64
D	10+03	4	697.10
E	10+06	4	697.10
F	10+06	4	696.90

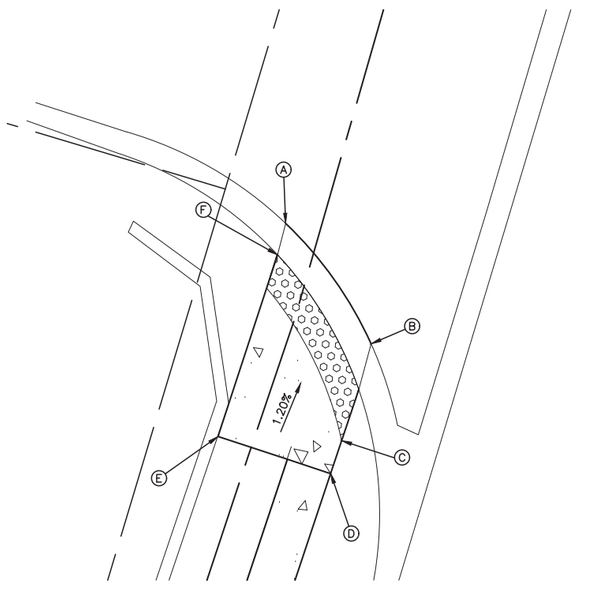
WALNUT CREEK DRIVE AT TALIESIN WAY
NW CORNER



N

POINT	STATION	OFFSET	ELEVATION
A	46+23	20.5	695.96
B	46+18	20	695.86
C	46+18.5	22.5	695.85
D	46+23	22	695.95
E	46+18	30	696.02
F	46+23	30	695.92
G	46+18.5	35	696.02
H	46+09	34.75	695.28
J	46+09	30.25	695.35
K	46+06	30	695.36
L	46+04	35	695.25

ST FRANCIS ROAD AT PINE HILL DRIVE
NE CORNER

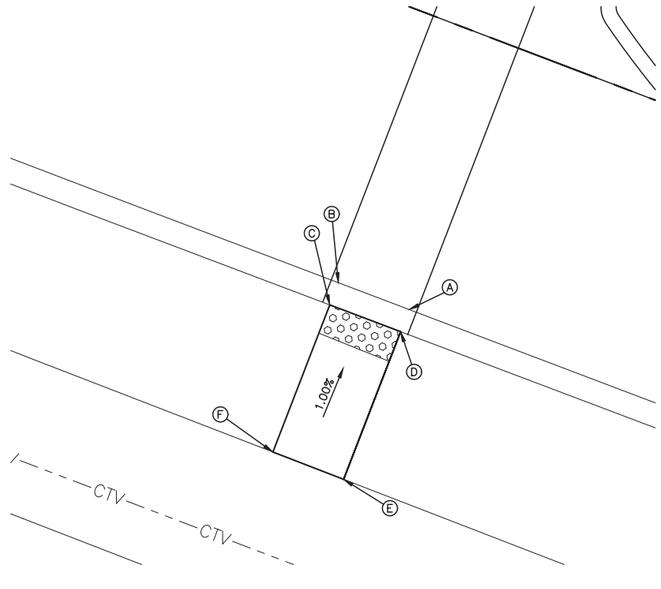


WALNUT CREEK DRIVE

N

POINT	STATION	OFFSET	ELEVATION
A	9+32.3	2.3	696.42
B	9+26.5	5.3	696.59
C	9+19.8	5.5	696.61
D	9+17.5	5.52	696.60
E	9+17.5	2.3	696.60
F	9+27.9	2.3	696.43

WALNUT CREEK DRIVE AT TALIESIN WAY
SW CORNER



N

POINT	STATION	OFFSET	ELEVATION
A	46+24	18.7	695.81
B	46+20	18.7	695.72
C	46+18	20.25	695.71
D	46+23	20.25	695.80
E	46+20	30.7	695.97
F	46+24	30.7	695.89

ST FRANCIS ROAD AT PINE HILL DRIVE
SE CORNER

FILE NAME = 10423_12-DTLS-03 - IDOT P03	USER NAME =	DESIGNED — GA	REVISED —
		CHECKED — HLG	REVISED —
	PLOT SCALE =	DRAWN — ACAD	REVISED —
	PLOT DATE = 06-09-16	CHECKED — ACAD	REVISED —

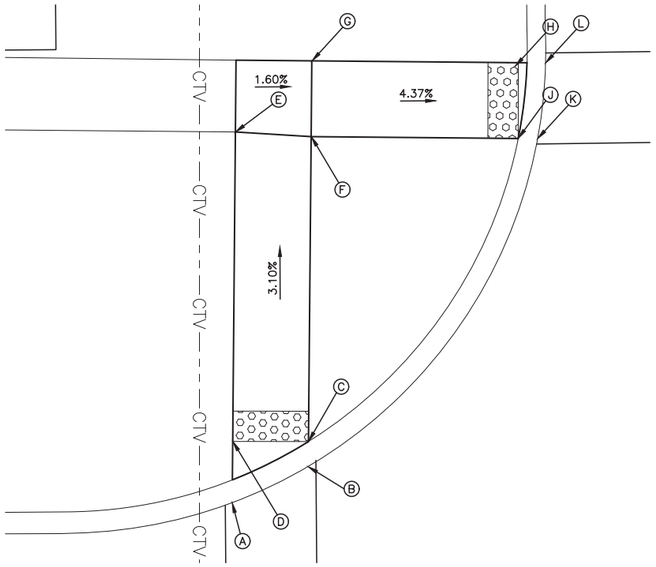
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
ADA RAMP DETAILS

SCALE: 1"=5' | SHEET NO. 80 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	80
		CONTRACT NO. 61D22		

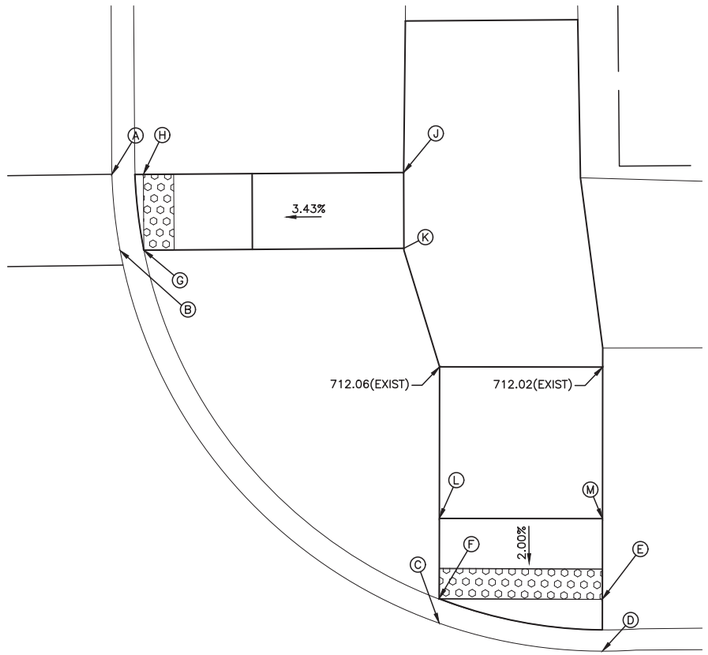
FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT ----



N

POINT	STATION	OFFSET	ELEVATION
A	63+00	20	712.71
B	63+05	22	712.60
C	63+05	24	712.59
D	63+00	24	712.68
E	63+00	44.3	712.06
F	63+05	44.3	711.98
G	63+05	49	711.98
H	63+18	49	711.32
J	63+18	44	711.39
K	63+19.5	44	711.40
L	63+20	49	711.32

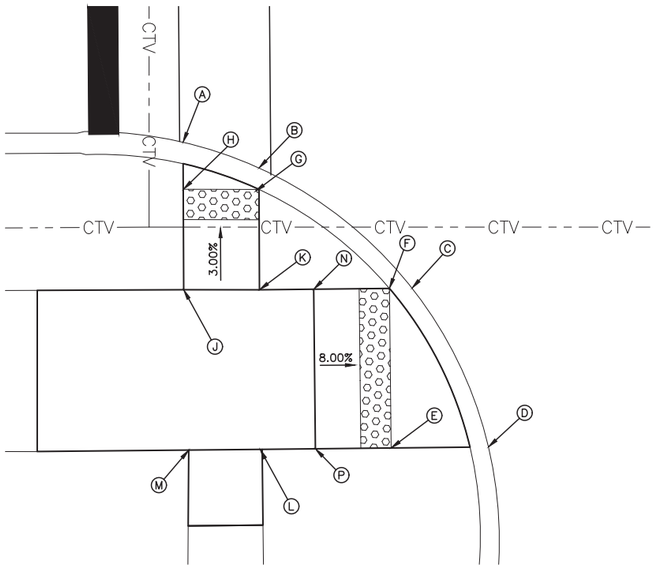
ST FRANCIS ROAD AT 80TH AVENUE
NW CORNER



N

POINT	STATION	OFFSET	ELEVATION
A	63+54	50	711.15
B	63+55	45	711.25
C	63+76	20	712.04
D	63+86	18	712.07
E	63+86	21.5	712.08
F	63+76	21.5	712.03
G	63+57	45	711.24
H	63+57	50	711.15
J	63+73	50	711.70
K	63+73	45	711.79
L	63+75	27	712.13
M	63+86	27	712.18

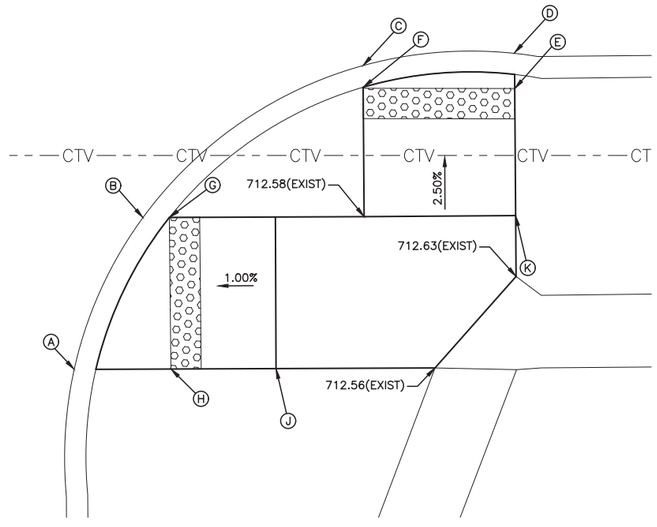
ST FRANCIS ROAD AT 80TH AVENUE
NE CORNER



N

POINT	STATION	OFFSET	ELEVATION
A	62+99	20	713.01
B	63+04	21	712.91
C	63+15	29	712.47
D	63+18	40	712.31
E	63+12	40	712.40
F	63+12	29	712.46
G	63+04	23	712.90
H	62+99	23	713.00
J	62+99	29	713.20
K	63+04	29	713.10
L	63+04	40	713.04
M	63+00	40	713.14
N	63+07	30	712.82
P	63+07	40	712.76

ST FRANCIS ROAD AT 80TH AVENUE
SW CORNER



N

POINT	STATION	OFFSET	ELEVATION
A	63+57	40	712.33
B	63+62	30	712.47
C	63+76	20	712.42
D	63+86	19	712.27
E	63+86	21	712.28
F	63+76	21	712.41
G	63+63	30	712.46
H	63+63	40	712.37
J	63+70	40	712.43
K	63+86	29	712.50

ST FRANCIS ROAD AT 80TH AVENUE
SE CORNER

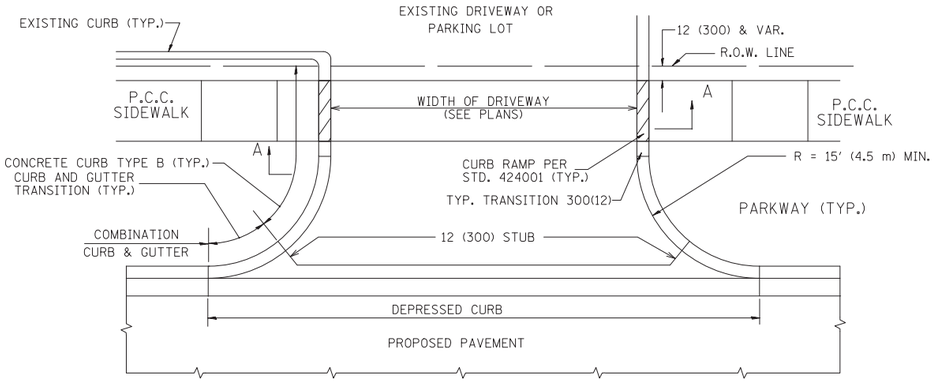
FILE NAME = 10423_12-DTLS-03 - IDOT P04	USER NAME =	DESIGNED — GA	REVISED —
		CHECKED — HLG	REVISED —
	PLOT SCALE =	DRAWN — ACAD	REVISED —
	PLOT DATE = 06-09-16	CHECKED — ACAD	REVISED —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

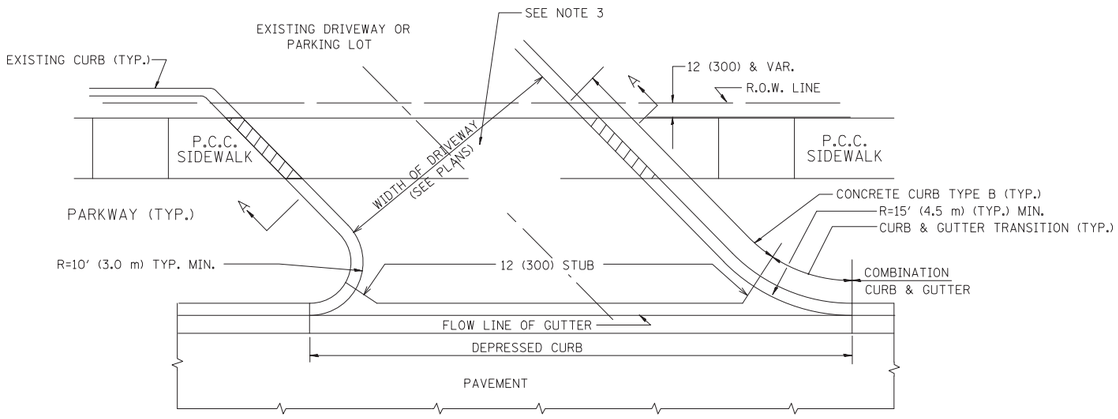
ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
ADA RAMP DETAILS

SCALE: 1"=5' | SHEET NO. 81 OF 109 SHEETS | STA. | TO STA.

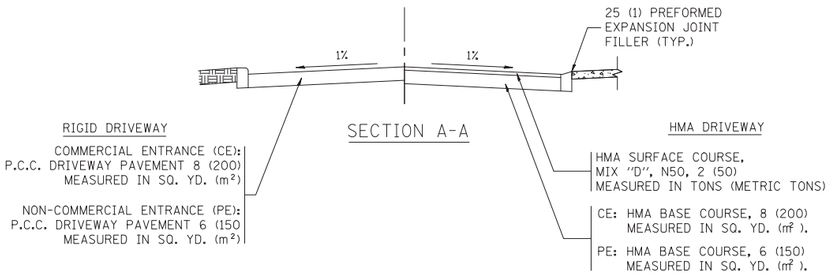
F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	81
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

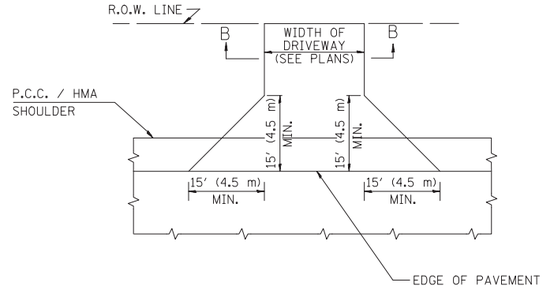


WITH CONCRETE CURB, TYPE B

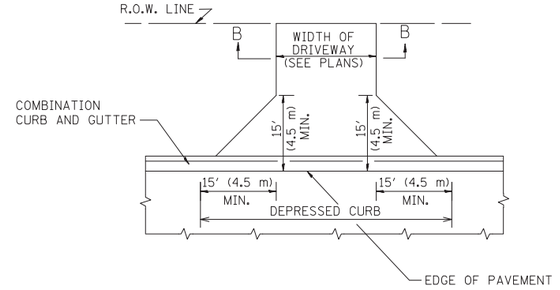


WITH CONCRETE CURB, TYPE B

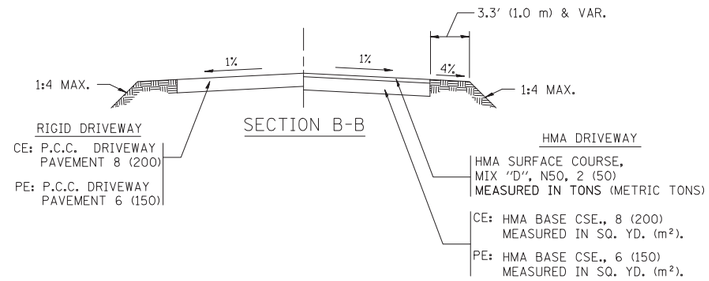




ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



RURAL FIELD ENTRANCE (FE)

HMA SURFACE COURSE,
MIX "D", N50, 2 (50)
MEASURED IN TONS (METRIC TONS)

AGGREGATE BASE CSE., TYPE B, 8 (200)
MEASURED IN SQ. YD. (m^2).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

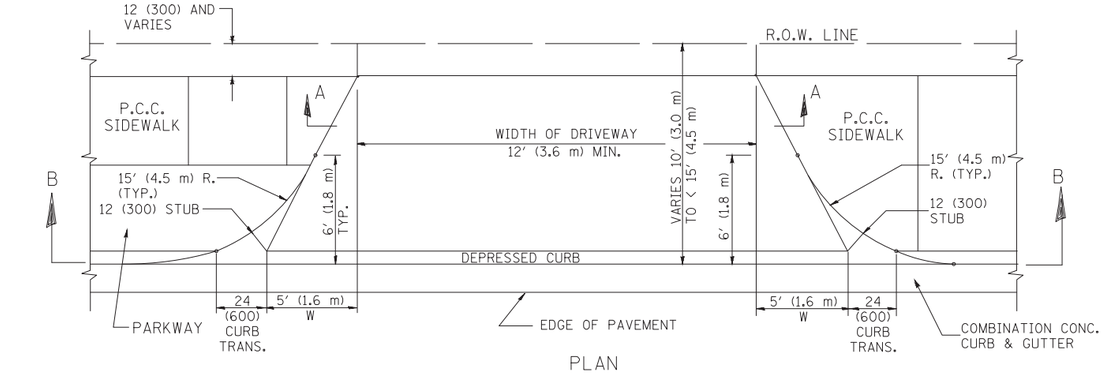
FILE NAME = 10423_12-DTLS-02 - IDOT P(1)	USER NAME =	DESIGNED — GA	REVISED — P. LaFLUER 04-15-03
		CHECKED — HLG	REVISED — R. BORO 01-01-07
	PLOT SCALE =	DRAWN — MED	REVISED — R. BORO 06-11-08
	PLOT DATE = 06-09-16	CHECKED — AGP	REVISED — R. BORO 09-06-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS – DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)

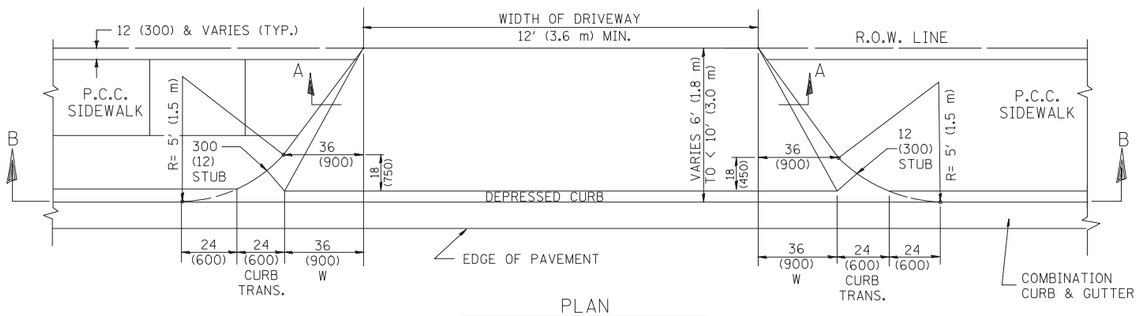
SCALE: | SHEET NO. 82 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	82
	BD0156-07 (BD-01)	CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



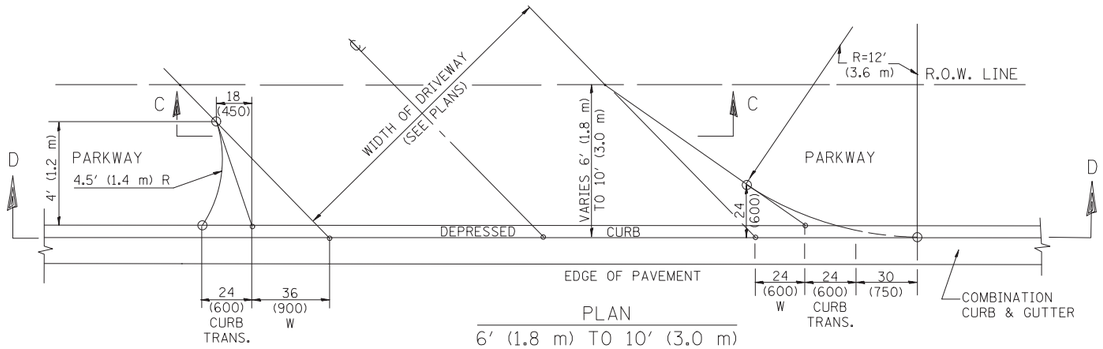
PLAN

10' (3.0 m) TO < 15' (4.5 m)



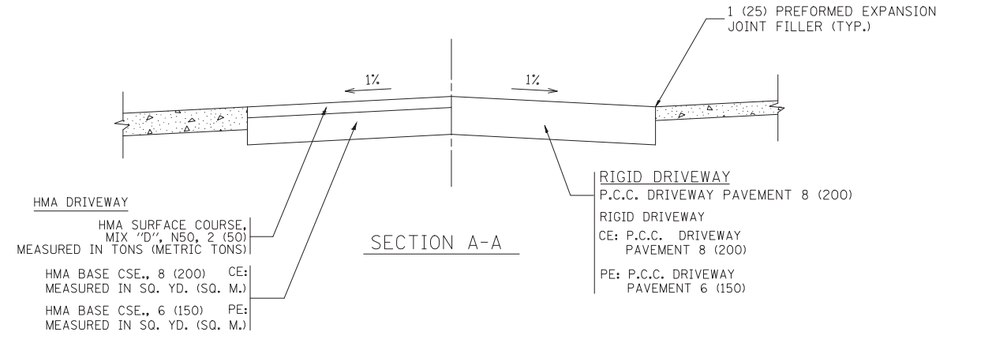
PLAN

6' (1.8 m) TO < 10' (3.0 m)

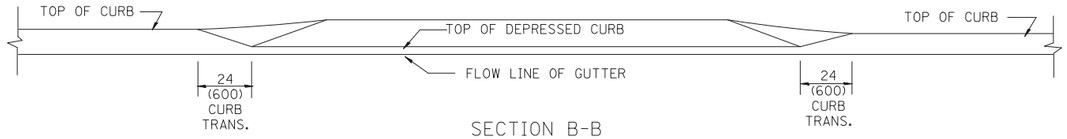


PLAN

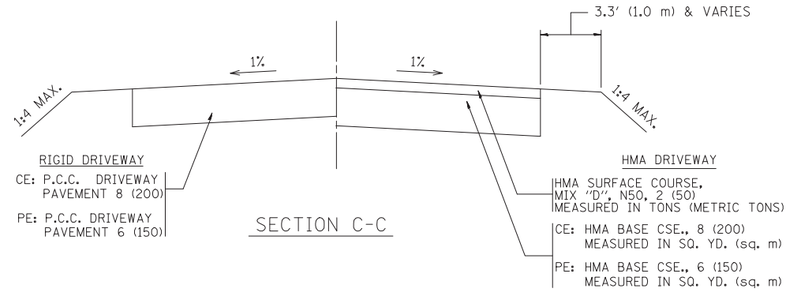
6' (1.8 m) TO 10' (3.0 m)



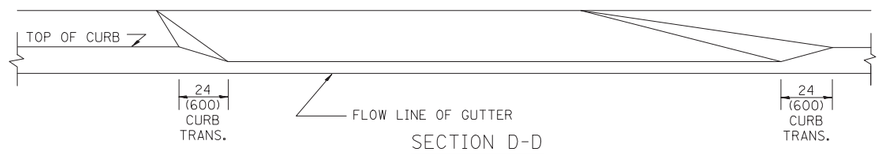
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

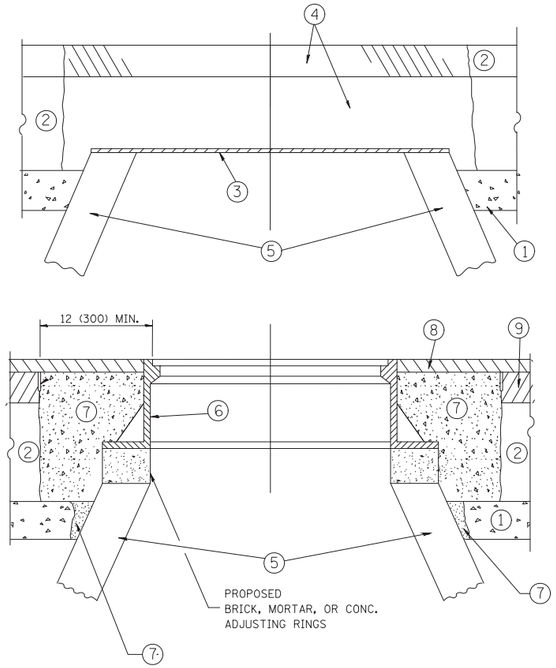
COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

FILE NAME = 10423_12-DTLS-02 - IDOT P(2)	USER NAME =	DESIGNED — GA	REVISED — M. GOMEZ 04-06-01	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	**DRIVEWAY DETAILS**	F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		CHECKED — HLG	REVISED — P. LaFLEUR 04-15-03		**DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)**	3752	10-00045-00-WR	WILL	109	83
	PLOT SCALE =	DRAWN — ACAD	REVISED — R. BORO 01-01-07				**BD400-02 (BD-02)**	CONTRACT NO. 61D22		
	PLOT DATE = 06-09-16	CHECKED — ACAD	REVISED — R. BORO 09-06-11		SCALE: SHEET NO. 83 OF 109 SHEETS STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

1. SUB-BASE GRANULAR MATERIAL
2. EXISTING PAVEMENT
3. 36 (900) DIAMETER METAL PLATE
4. PROPOSED CRUSHED STONE AND HMA SURFACE MIX
5. EXISTING STRUCTURE
6. FRAME AND LID (SEE NOTES)
7. CLASS PP-1* CONCRETE
8. PROPOSED HMA SURFACE COURSE
9. PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

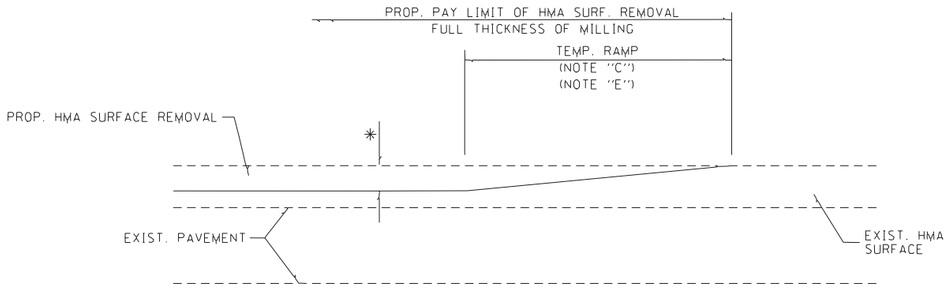
FILE NAME = 10423_12-DTLS-02 - IDOT P(3)	USER NAME =	DESIGNED — GA	REVISED — R. WIEDEMAN 05-14-04
		CHECKED — HLG	REVISED — R. BORO 01-01-07
	PLOT SCALE =	DRAWN — MED	REVISED — R. BORO 03-09-11
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED — R. BORO 12-06-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: | SHEET NO. 84 OF 109 SHEETS | STA. | TO STA.

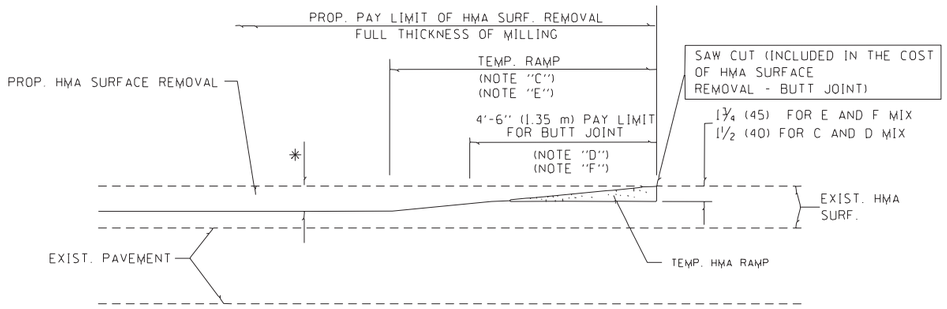
F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	84
	~~BD600-03 (BD-9)~~	CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

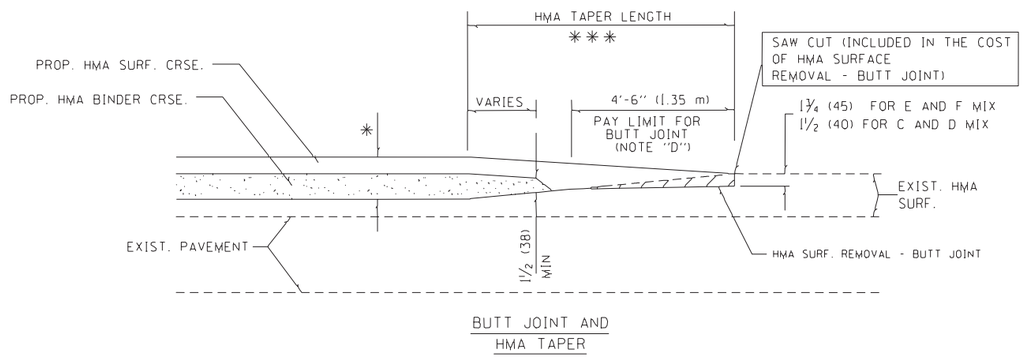


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

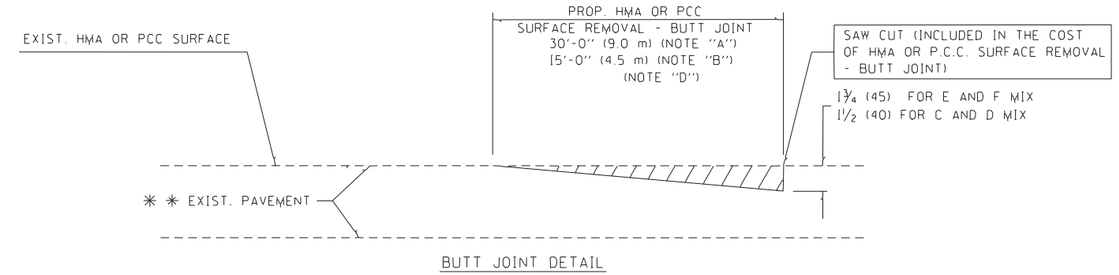
OPTION 2

TYPICAL TEMPORARY RAMP

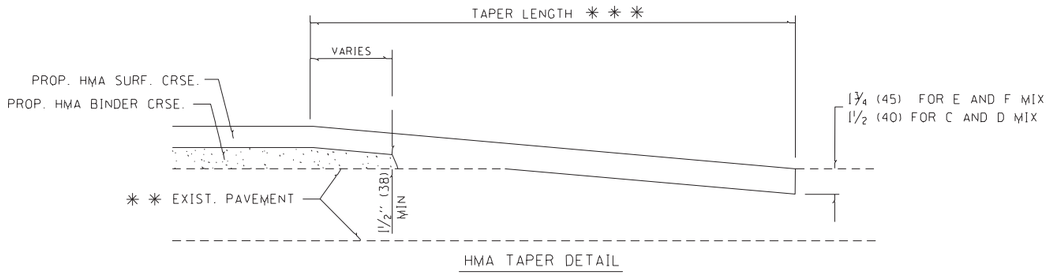


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.

B: MINOR SIDE ROADS.

C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.

F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

* * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

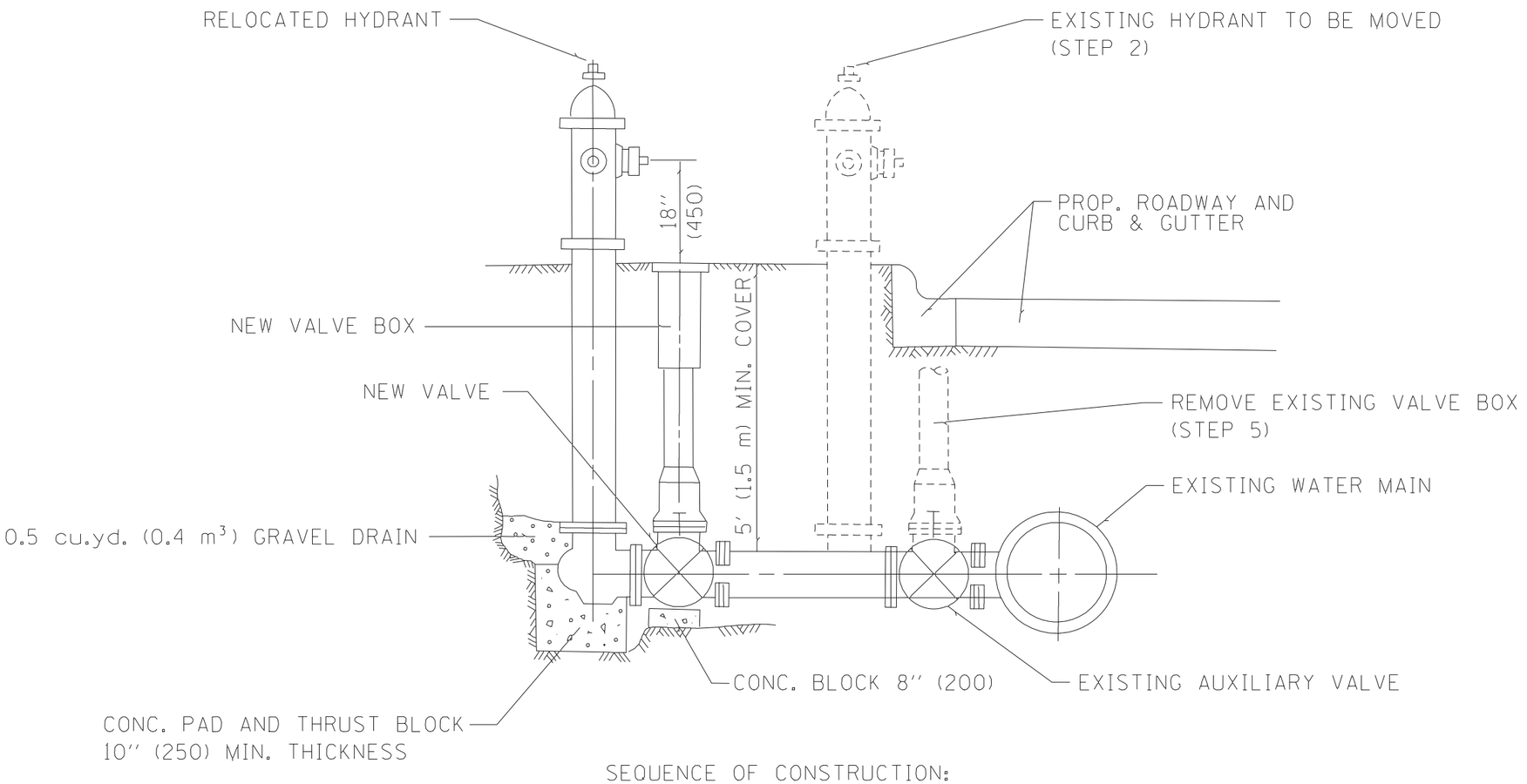
BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = 10423_12-DTLS-02 - IDOT P(4)	USER NAME =	DESIGNED — GA	REVISED — R. SHAH 10-25-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINT AND HMA TAPER DETAILS
		CHECKED — HLG	REVISED — A. ABBAS 03-21-97		
	PLOT SCALE =	DRAWN — MED	REVISED — M. GOMEZ 04-06-01		
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED — R. BORO 01-01-07		SCALE: / SHEET NO. 85 OF 109 SHEETS / STA. / TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	85
	BD400-05 BD32	CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



SEQUENCE OF CONSTRUCTION:

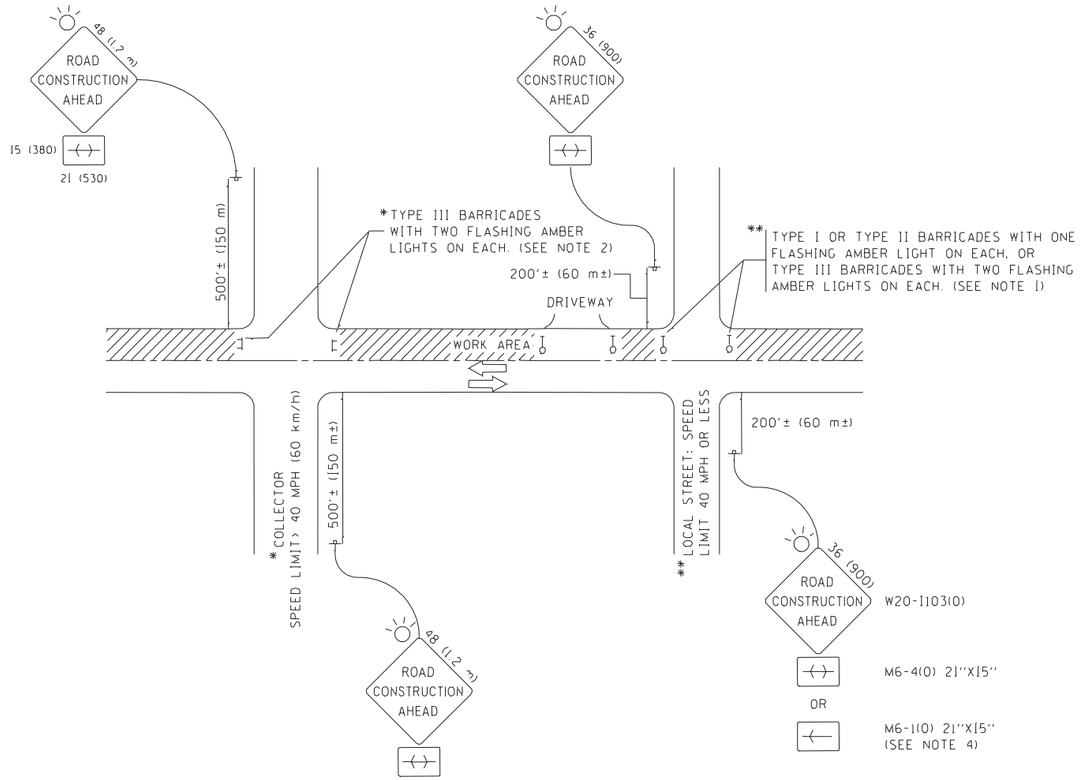
1. CLOSE EXISTING VALVE.
2. REMOVE EXISTING HYDRANT.
3. INSTALL HYDRANT EXTENSION AND NEW VALVE.
4. RELOCATE EXISTING HYDRANT.
5. OPEN EXISTING VALVE, REMOVE BOX.
6. BACKFILL.
7. FLUSH AND TEST FOR CHLORIDE RESIDUAL AND PROVIDE TEST.

ALL WORK TO BE DONE IN ACCORDANCE WITH ARTICLE 564 OF THE STANDARD SPECIFICATIONS. NEW VALVE AND BOX SHALL BE SAME MAKE AND MODEL AS EXISTING.

FIRE HYDRANT TO BE MOVED

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = 10423_12-DTLS-02 - IDOT P(5)	USER NAME =	DESIGNED — GA	REVISED — R. SHAH 09-09-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FIRE HYDRANT TO BE MOVED			F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		CHECKED — HLG	REVISED — R. SHAH 10-25-94					3752	10-00045-00-WR	WILL	109	86
	PLOT SCALE =	DRAWN — MED	REVISED —					BD-36		CONTRACT NO. 61D22		
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —		SCALE:	SHEET NO. 86 OF 109 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



NOTES

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES. 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES. 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

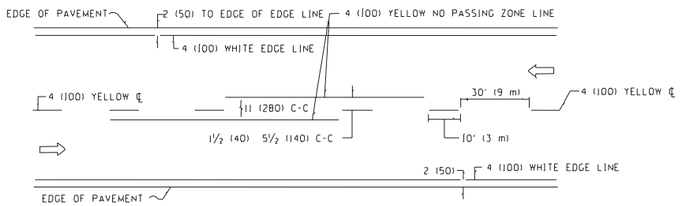
FILE NAME = 10423_12-DTLS-02 - IDOT P(6)	USER NAME =	DESIGNED — GA	REVISED — A. HOUSEH 10-15-96
		CHECKED — HLG	REVISED — T. RAMMACHER 01-06-00
	PLOT SCALE =	DRAWN — MED	REVISED — A. SCHUETZE 07-01-13
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED — A. SCHUETZE 09-15-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

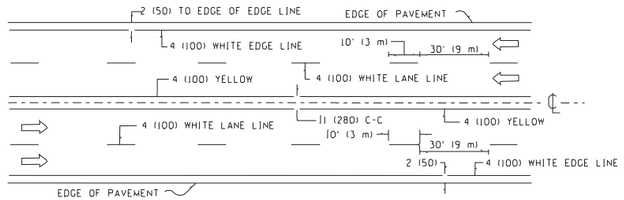
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: | SHEET NO. 87 OF 109 SHEETS | STA. | TO STA.

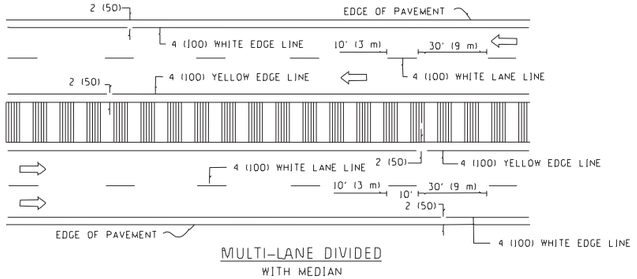
F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	87
	TC-10	CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



2-LANE ROADWAY

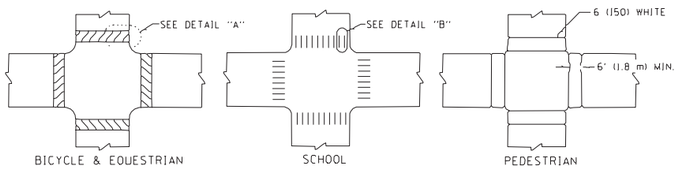


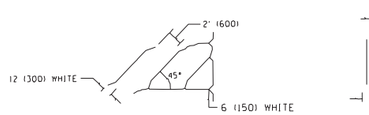
MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

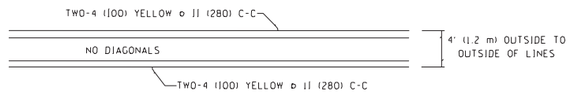
TYPICAL LANE AND EDGE LINE MARKING



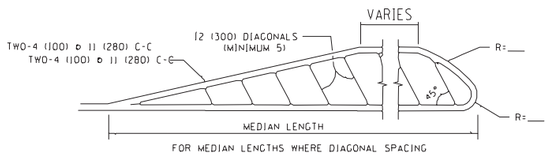


TYPICAL CROSSWALK MARKING

✳ MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



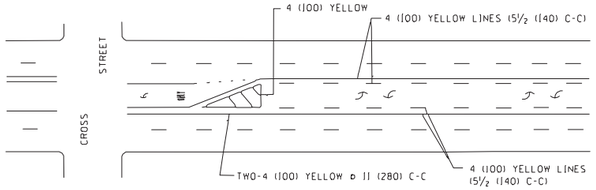
4' (1.2 m) WIDE MEDIANS ONLY



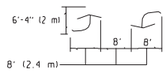
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

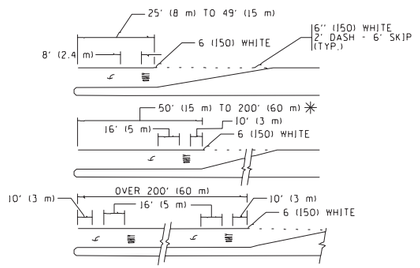


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED. WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

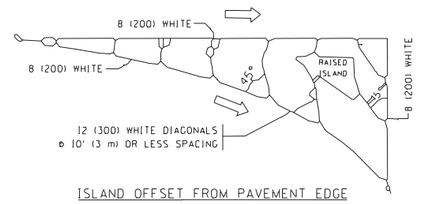


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
↰ AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

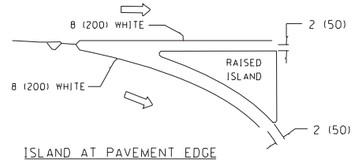
✳ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

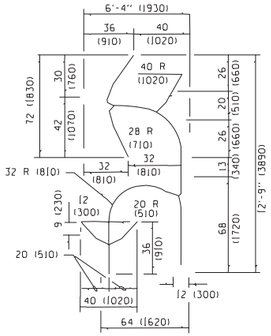


ISLAND OFFSET FROM PAVEMENT EDGE

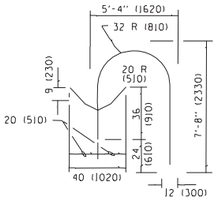


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

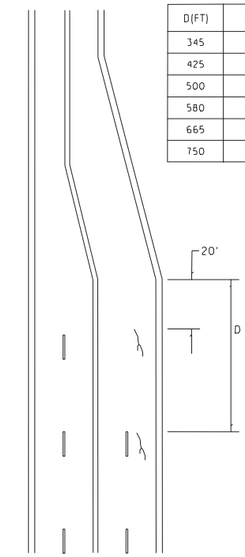


COMBINATION LEFT AND U-TURN



U-TURN

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55



LANE REDUCTION TRANSITION

✳ LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 ▫ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 ▫ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE. FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 ▫ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 ▫ 6 (150) 12 (300) ▫ 45° 12 (300) ▫ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK IF PRESENT. OTHERWISE PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE. WHERE POSSIBLE
PAINTED MEDIANS	2 ▫ 4 (100) WITH 12 (300) DIAGONALS ▫ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS ▫ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES. "RR" IS 6' (1.8 m) LETTERS. 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) ▫ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = 10423_12-DTLS-02 - IDOT P(7)

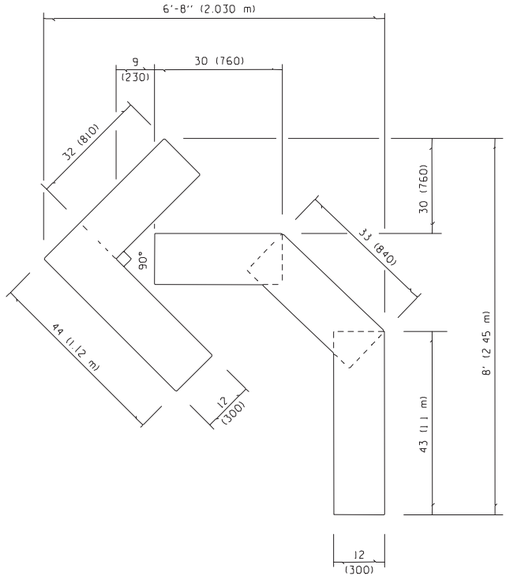
USER NAME =		DESIGNED — GA	REVISED — C. JUCIUS 09-09-09		
		CHECKED — HLG	REVISED — C. JUCIUS 07-01-13		
PLOT SCALE =		DRAWN — MED	REVISED — C. JUCIUS 12-21-15		
PLOT DATE =	06-09-16	CHECKED — APG	REVISED — C. JUCIUS 04-12-16		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

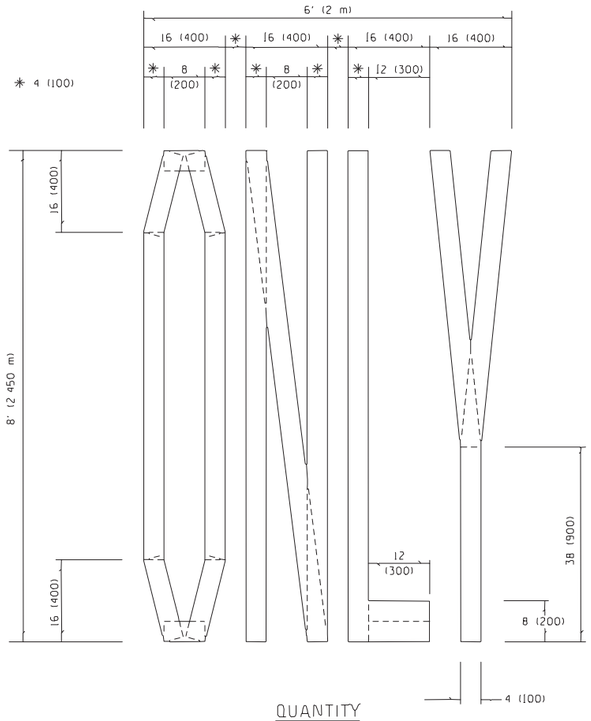
SCALE: | SHEET NO. 88 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	88
	TC-13	CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



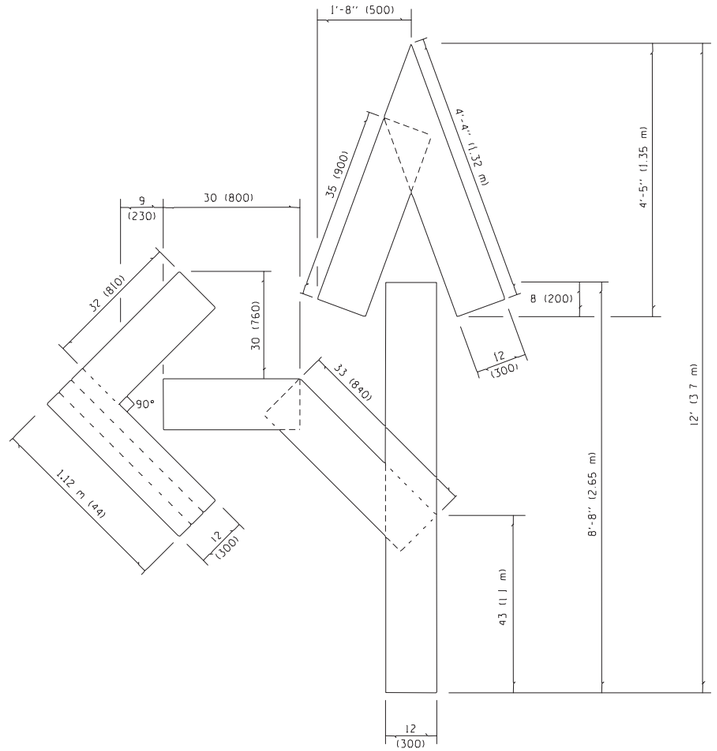
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

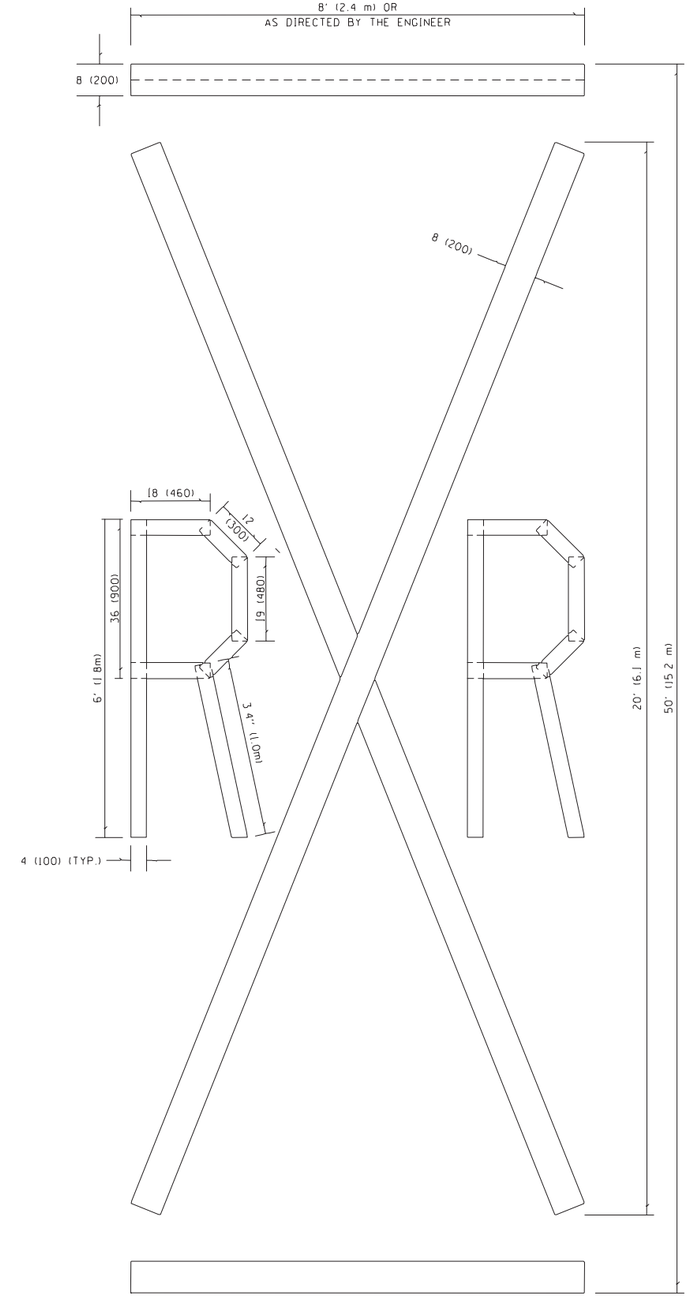


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE.

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.

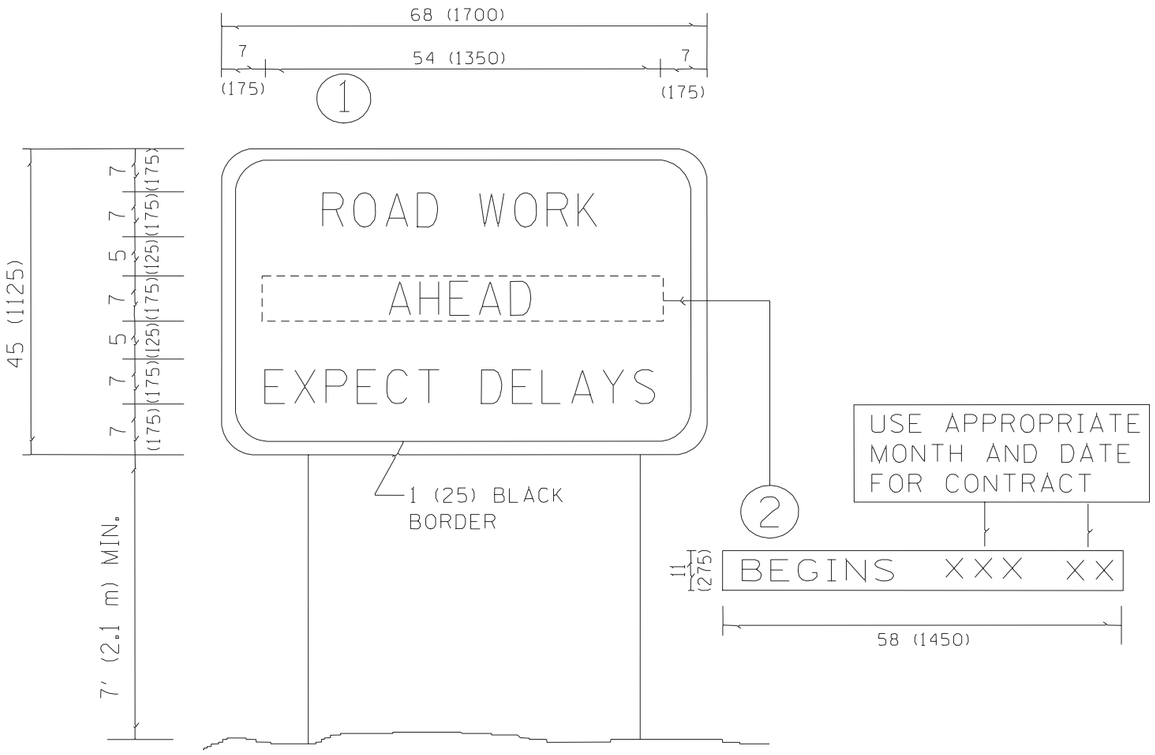


QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = 10423_12-DTLS-02 - IDOT P(8)	USER NAME =	DESIGNED — GA	REVISED —T. RAMMACHER 03-02-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS	F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		CHECKED — HLG	REVISED —E. GOMEZ 08-28-00			3752	10-00045-00-WR	WILL	109	89
	PLOT SCALE =	DRAWN — MED	REVISED —E. GOMEZ 08-28-00				TC-16	CONTRACT NO. 61D22		
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —A. SCHUETZE 09-15-16		SCALE:	SHEET NO. 89 OF 109 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----	



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE ''ROAD CONSTRUCTION AHEAD'' SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR ''TEMPORARY INFORMATION SIGNING'' FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

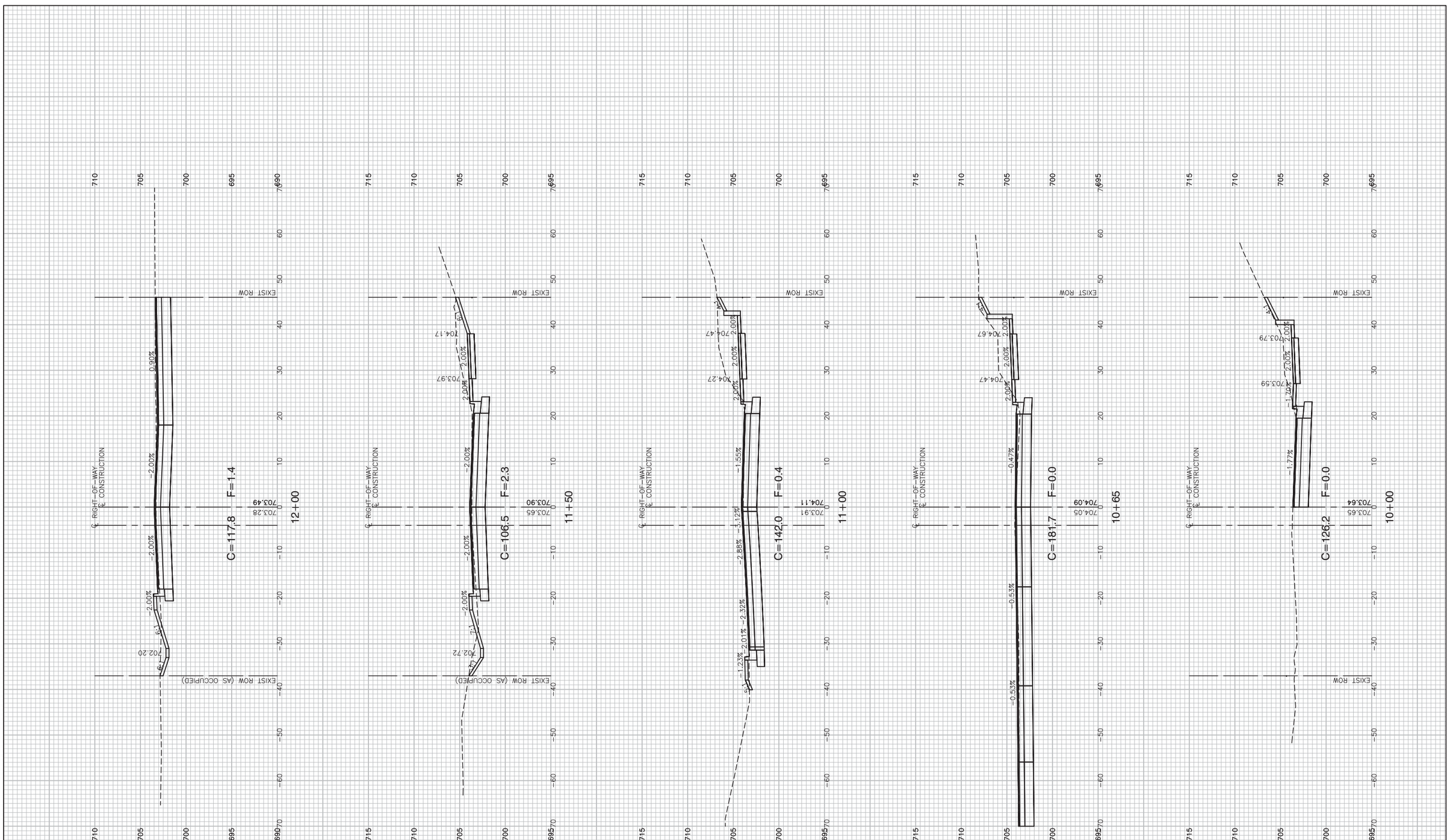
FILE NAME = 10423_12-DTLS-02 - IDOT P(9)	USER NAME =	DESIGNED — GA	REVISED — R. MIRS 09-15-97
		CHECKED — HLG	REVISED — R. MIRS 12-11-97
	PLOT SCALE =	DRAWN — MED	REVISED — T. RAMMACHER 02-02-99
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED — C. JUCIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD
INFORMATION SIGN

SCALE: | SHEET NO. 90 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	90
	TC-22	CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



FILE NAME = 10423_12-XSCT-01 - IDOT_X(1)	USER NAME =	DESIGNED — GA	REVISED —
		CHECKED — HLG	REVISED —
	PLOT SCALE =	DRAWN — MED	REVISED —
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —

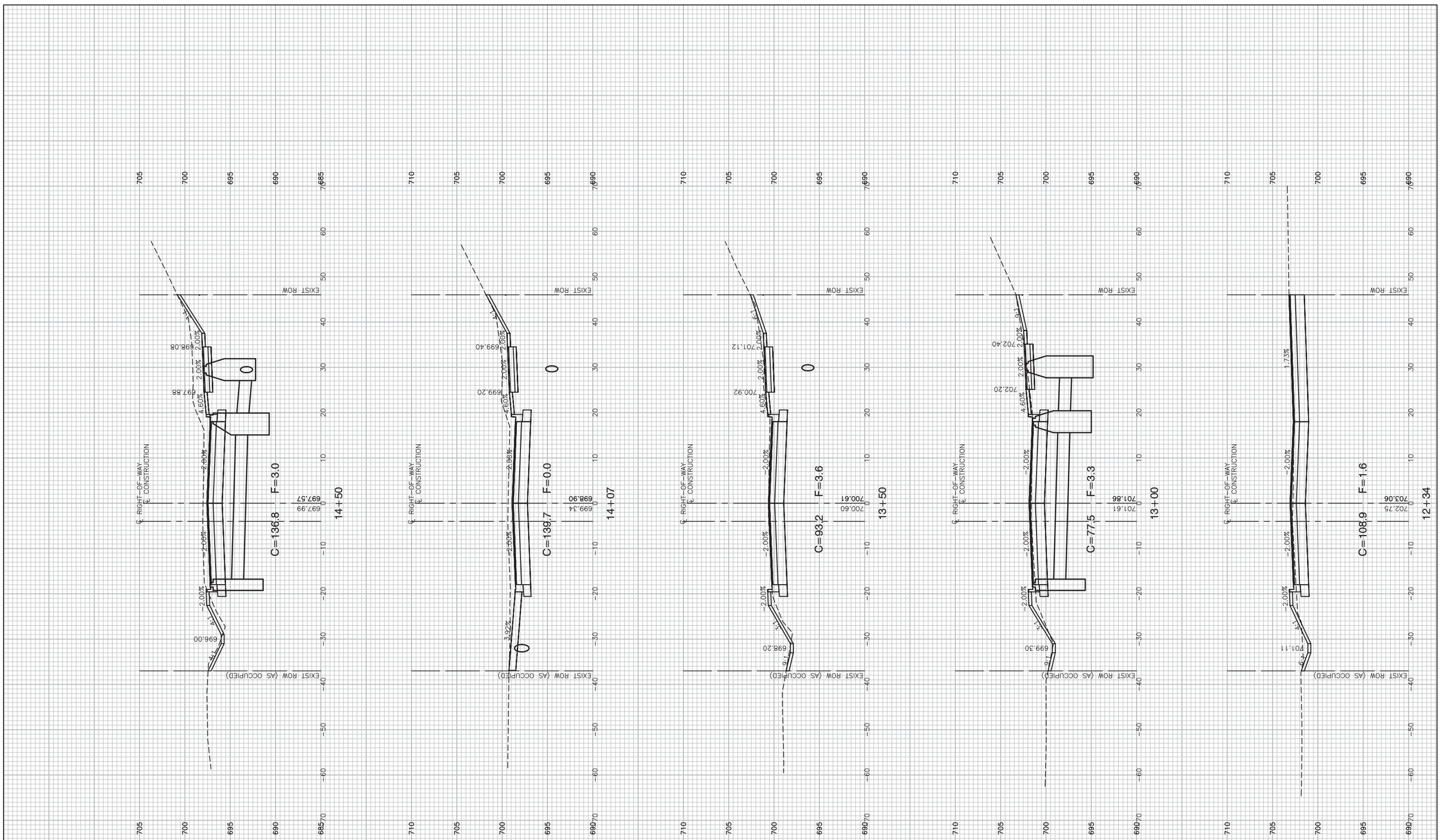
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
CROSS SECTIONS - ST. FRANCIS ROAD

SCALE: H 1"=10' V 1"=5' | SHEET NO. 91 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	91
		CONTRACT NO. 61D22		

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT ----



FILE NAME = 10423_12-XSCT-01 - IDOT_X(2)	USER NAME =	DESIGNED — GA	REVISED —
		CHECKED — HLG	REVISED —
	PLOT SCALE =	DRAWN — MED	REVISED —
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —

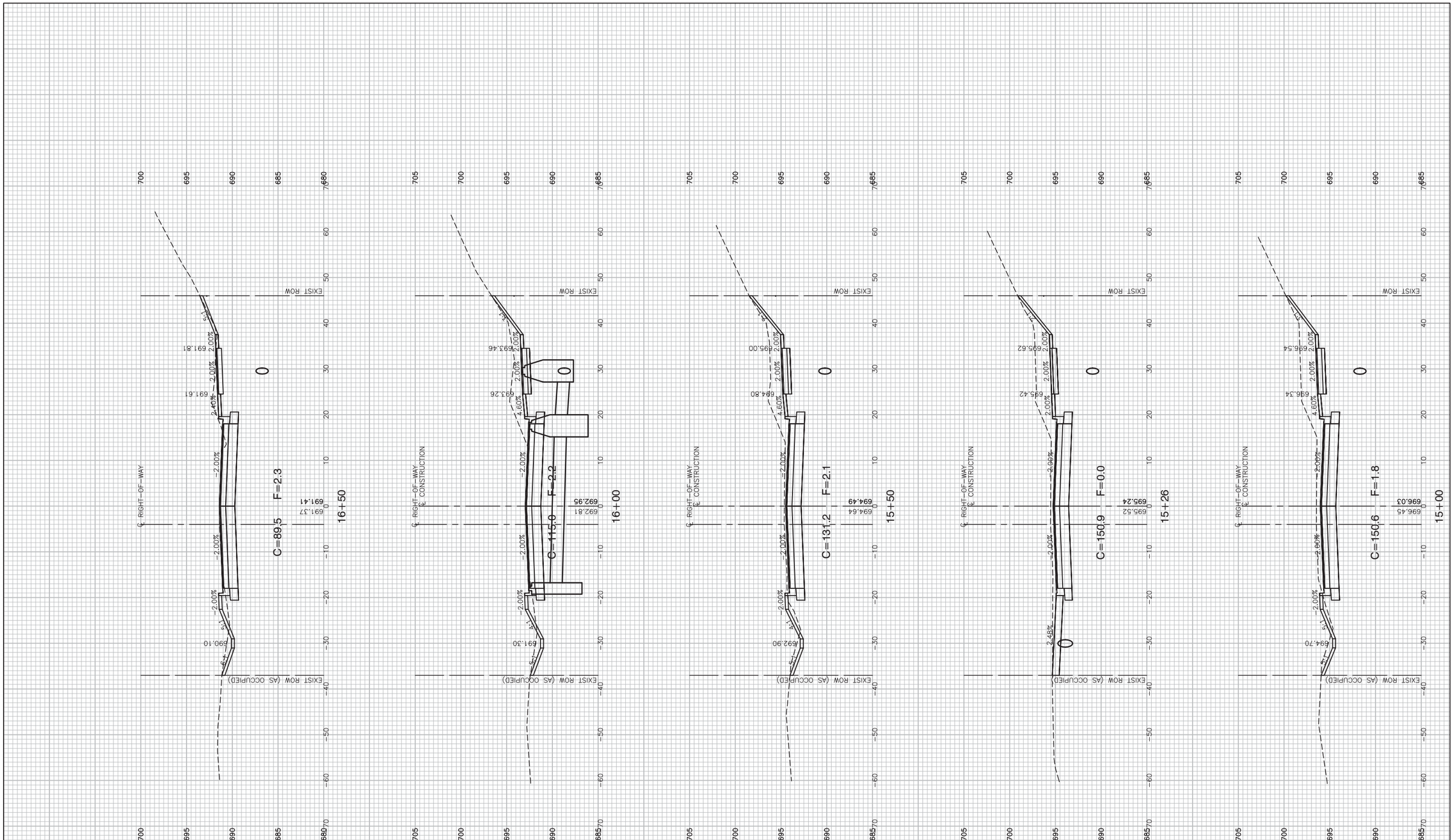
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
CROSS SECTIONS - ST. FRANCIS ROAD

SCALE: H 1"=10' V 1"=5' | SHEET NO. 92 OF 109 SHEETS | STA. | TO STA.

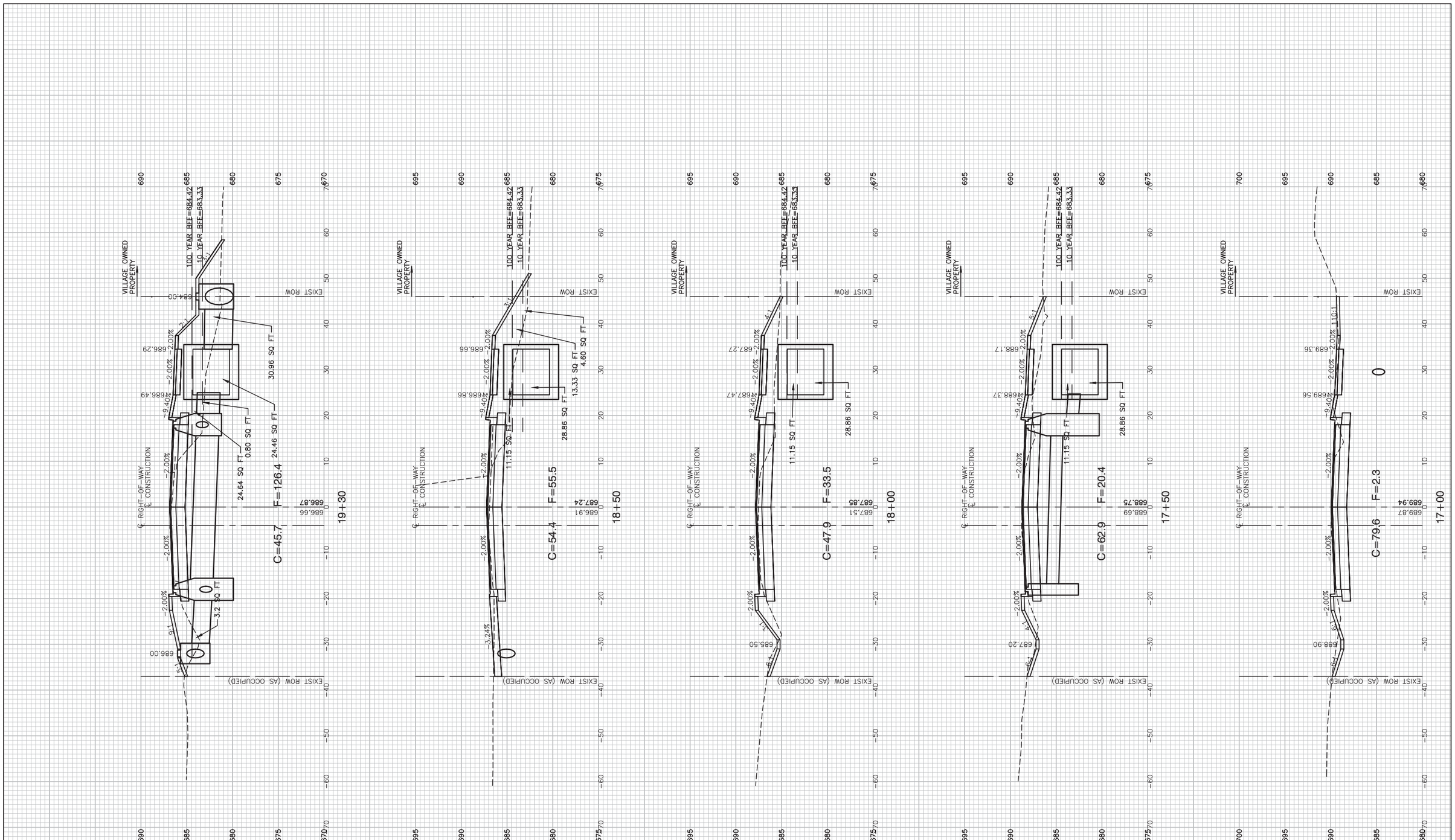
F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	92
		CONTRACT NO. 61D22		

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT ----



FILE NAME = 10423_12-XSCT-01 - IDOT_X(3)	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION CROSS SECTIONS - ST. FRANCIS ROAD
		CHECKED — HLG	REVISED —		
	PLOT SCALE =	DRAWN — MED	REVISED —		
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —		SCALE: H 1"=10' V 1"=5' | SHEET NO. 93 OF 109 SHEETS | STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	93
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



FILE NAME = 10423_12-XSCT-01 - IDOT_X(4)	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION CROSS SECTIONS - ST. FRANCIS ROAD
		CHECKED — HLG	REVISED —		
	PLOT SCALE =	DRAWN — MED	REVISED —		SCALE: H 1"=10' V 1"=5' SHEET NO. 94 OF 109 SHEETS STA. TO STA.
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —		

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	94
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

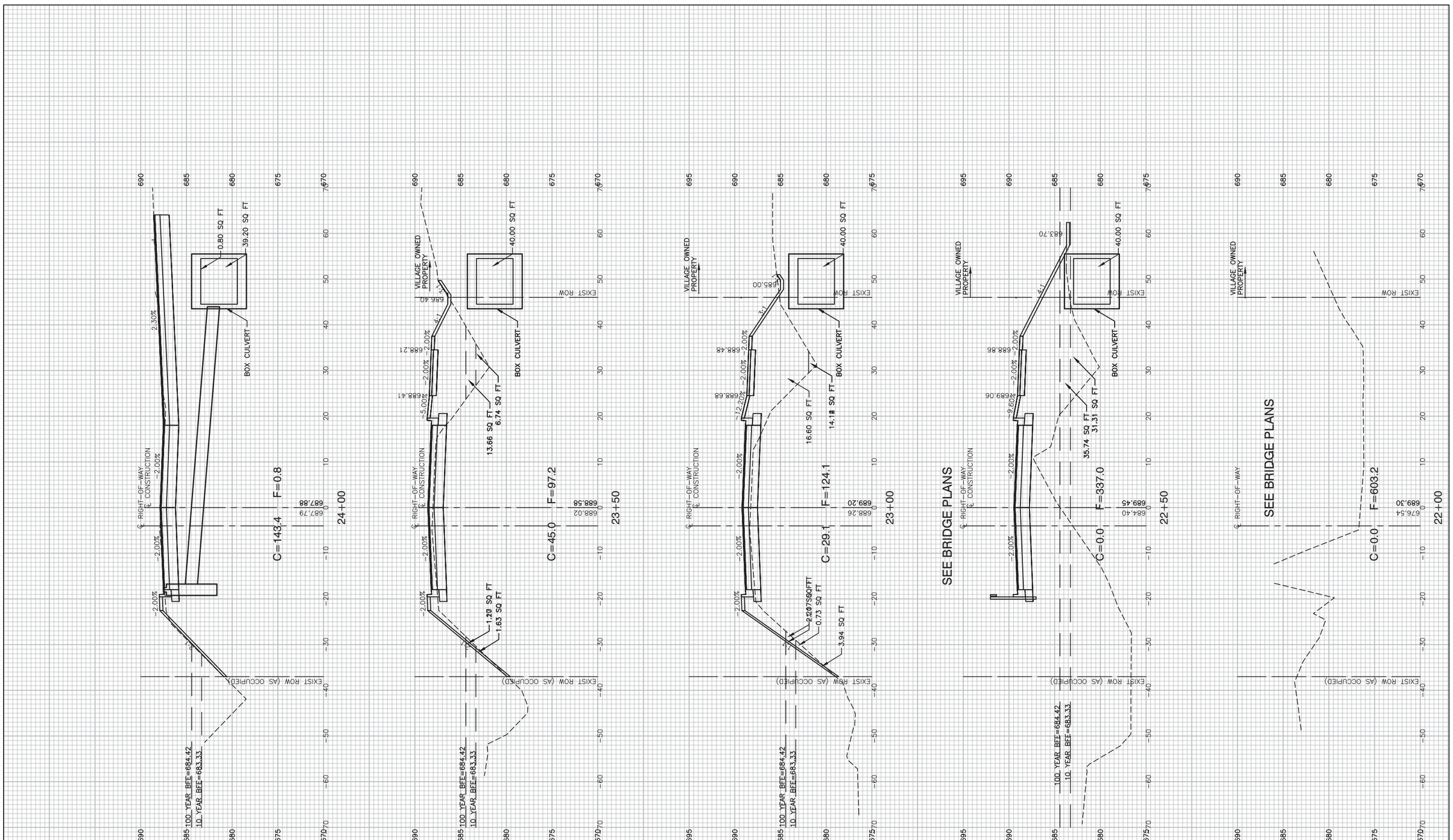
FILE NAME = 10423_12-XSCT-01 - IDOT_X(5)	USER NAME =	DESIGNED — GA	REVISED —
		CHECKED — HLG	REVISED —
	PLOT SCALE =	DRAWN — MED	REVISED —
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
CROSS SECTIONS - ST. FRANCIS ROAD

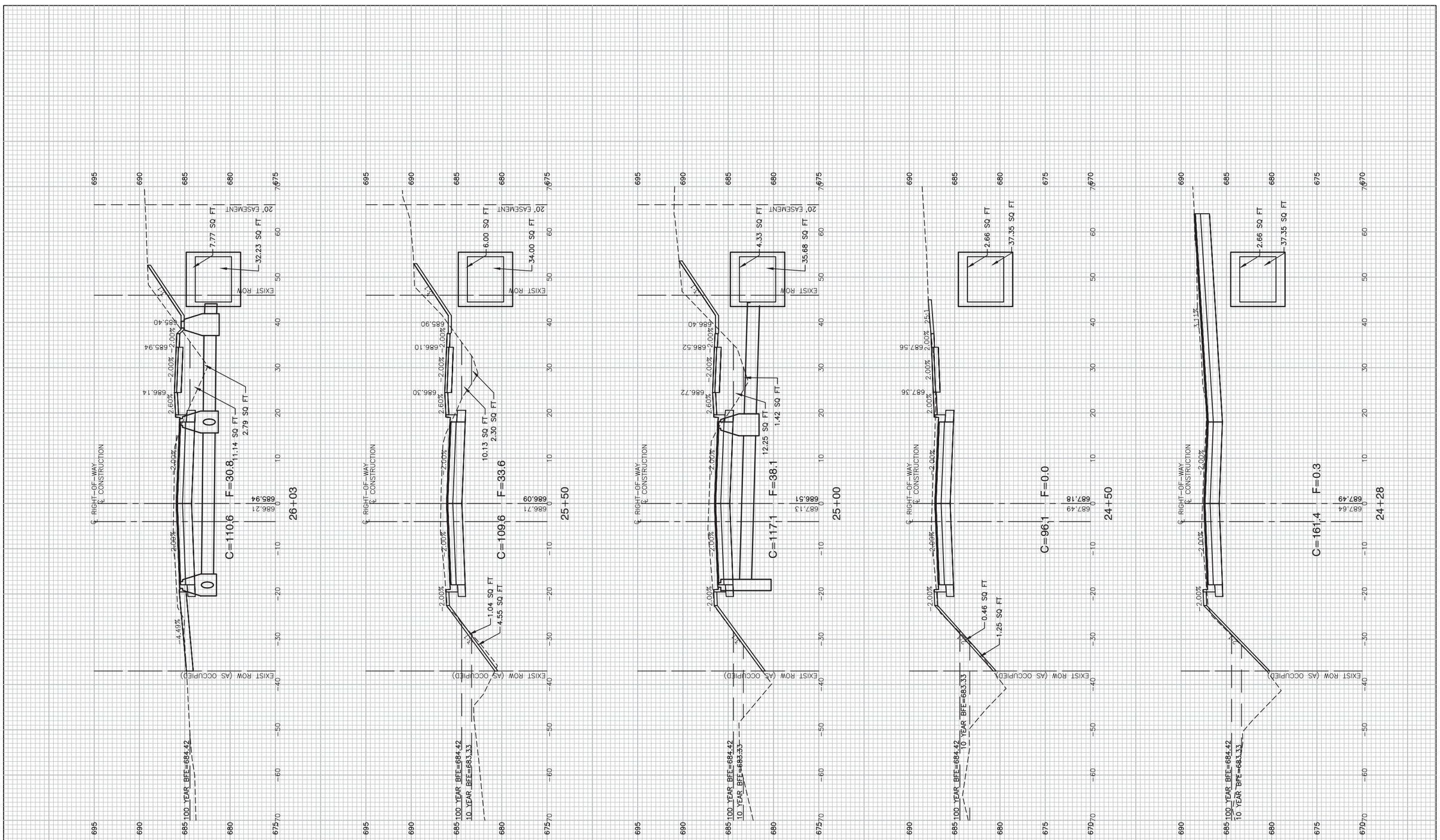
SCALE: H 1"=10' V 1"=5' | SHEET NO. 95 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	95
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



FILE NAME = 10423_12-XSCT-01 - IDOT_X(6)	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION CROSS SECTIONS - ST. FRANCIS ROAD
		CHECKED — HLG	REVISED —		
	PLOT SCALE =	DRAWN — MED	REVISED —		
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —		SCALE: H 1"=10' V 1"=5' | SHEET NO. 96 OF 109 SHEETS | STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	96
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



FILE NAME = 10423_12-XSCT-01 - IDOT_X(7)	USER NAME =	DESIGNED — GA	REVISED —
		CHECKED — HLG	REVISED —
	PLOT SCALE =	DRAWN — MED	REVISED —
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —

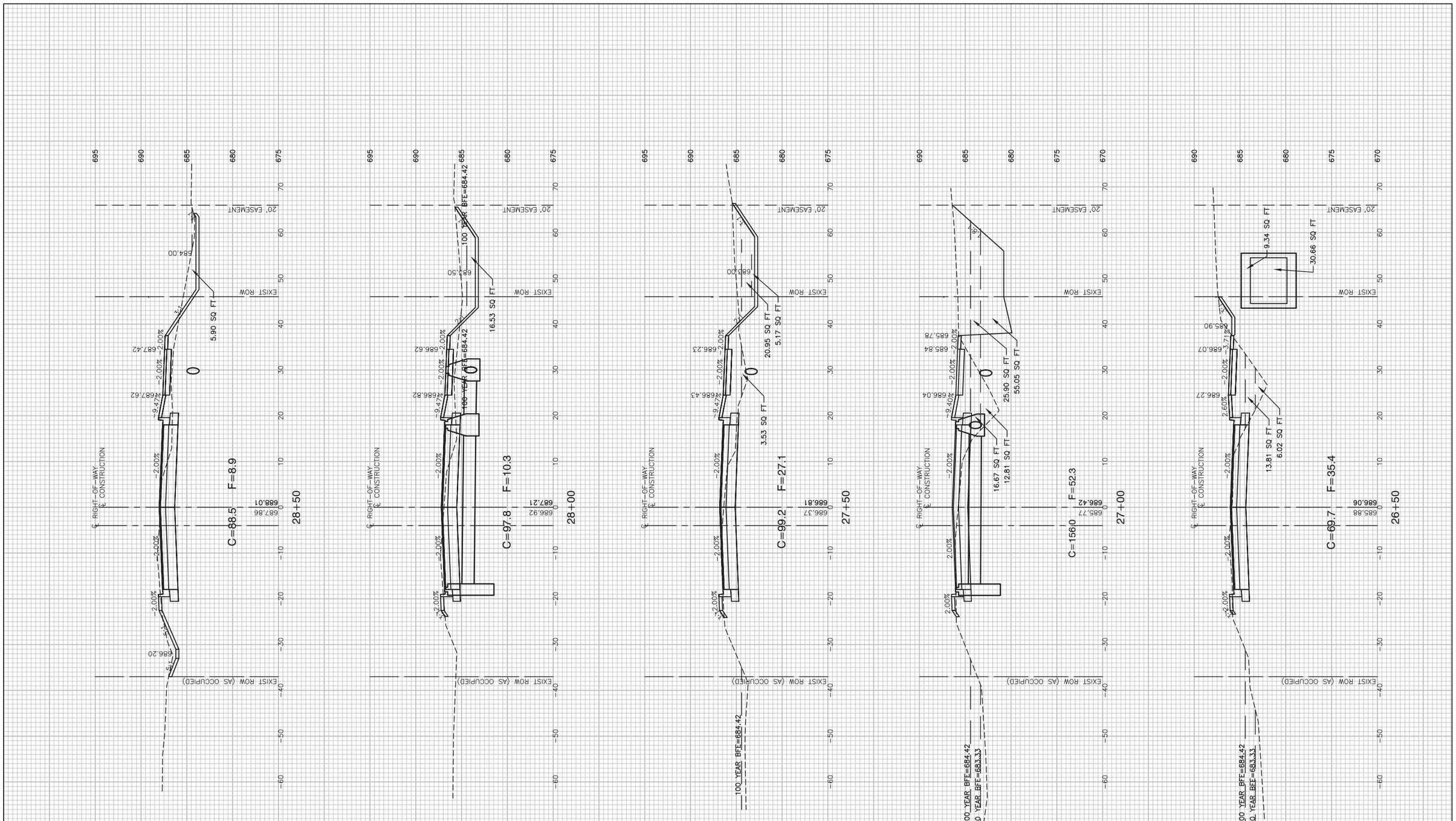
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
CROSS SECTIONS - ST. FRANCIS ROAD

SCALE: H 1"=10' V 1"=5' | SHEET NO. 97 OF 109 SHEETS | STA. | TO STA.

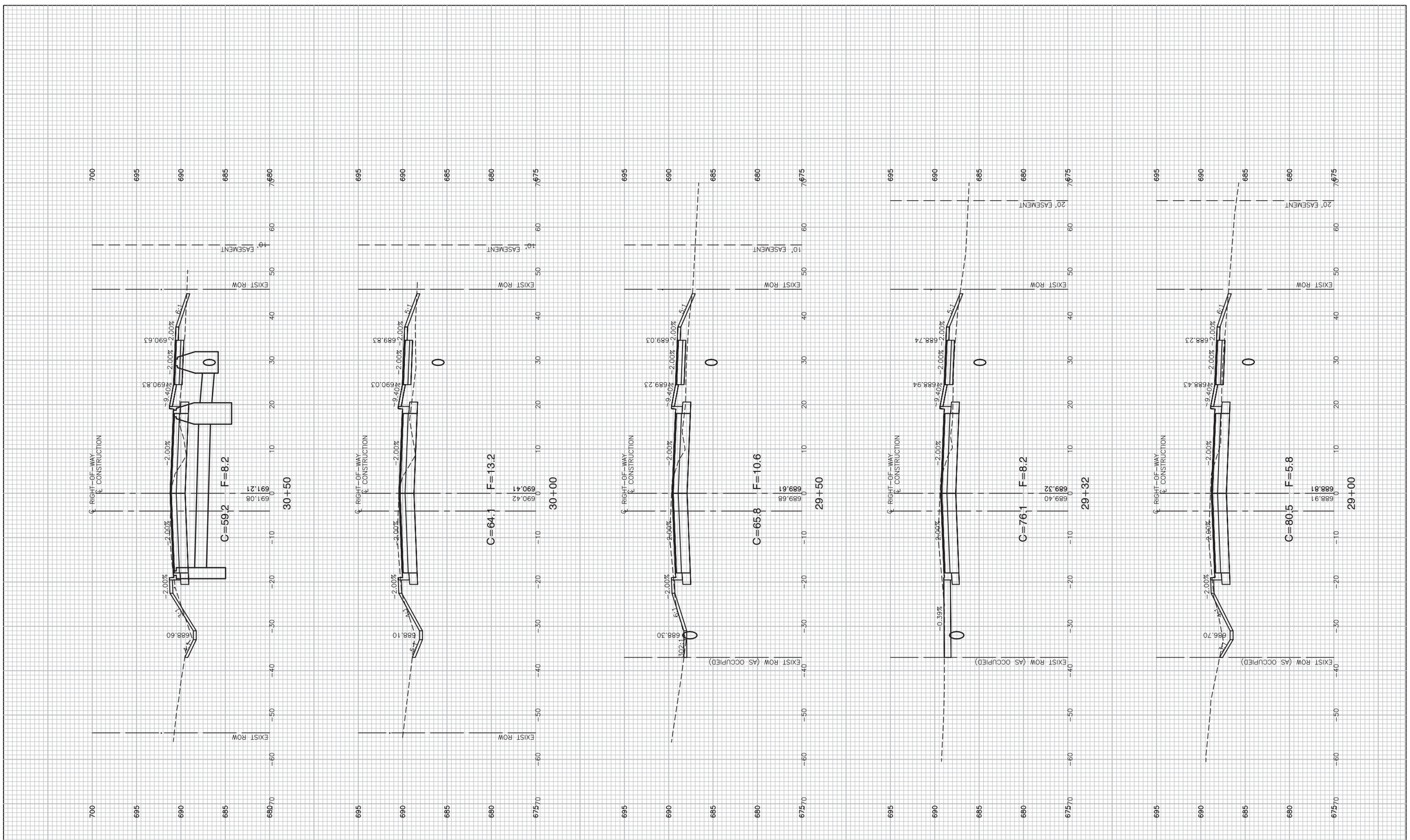
F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	97
		CONTRACT NO. 61D22		

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT ----



FILE NAME = 10423_12-XSCT-01 - IDOT_X(8)	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION CROSS SECTIONS - ST. FRANCIS ROAD
		CHECKED — HLG	REVISED —		
	PLOT SCALE =	DRAWN — MED	REVISED —		
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —		SCALE: H 1"=10' V 1"=5' | SHEET NO. 98 OF 109 SHEETS | STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	98
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



FILE NAME = 10423_12-XSCT-01 - IDOT_X(9)

USER NAME =	DESIGNED — GA	REVISED —
	CHECKED — HLG	REVISED —
PLOT SCALE =	DRAWN — MED	REVISED —
PLOT DATE = 06-09-16	CHECKED — APG	REVISED —

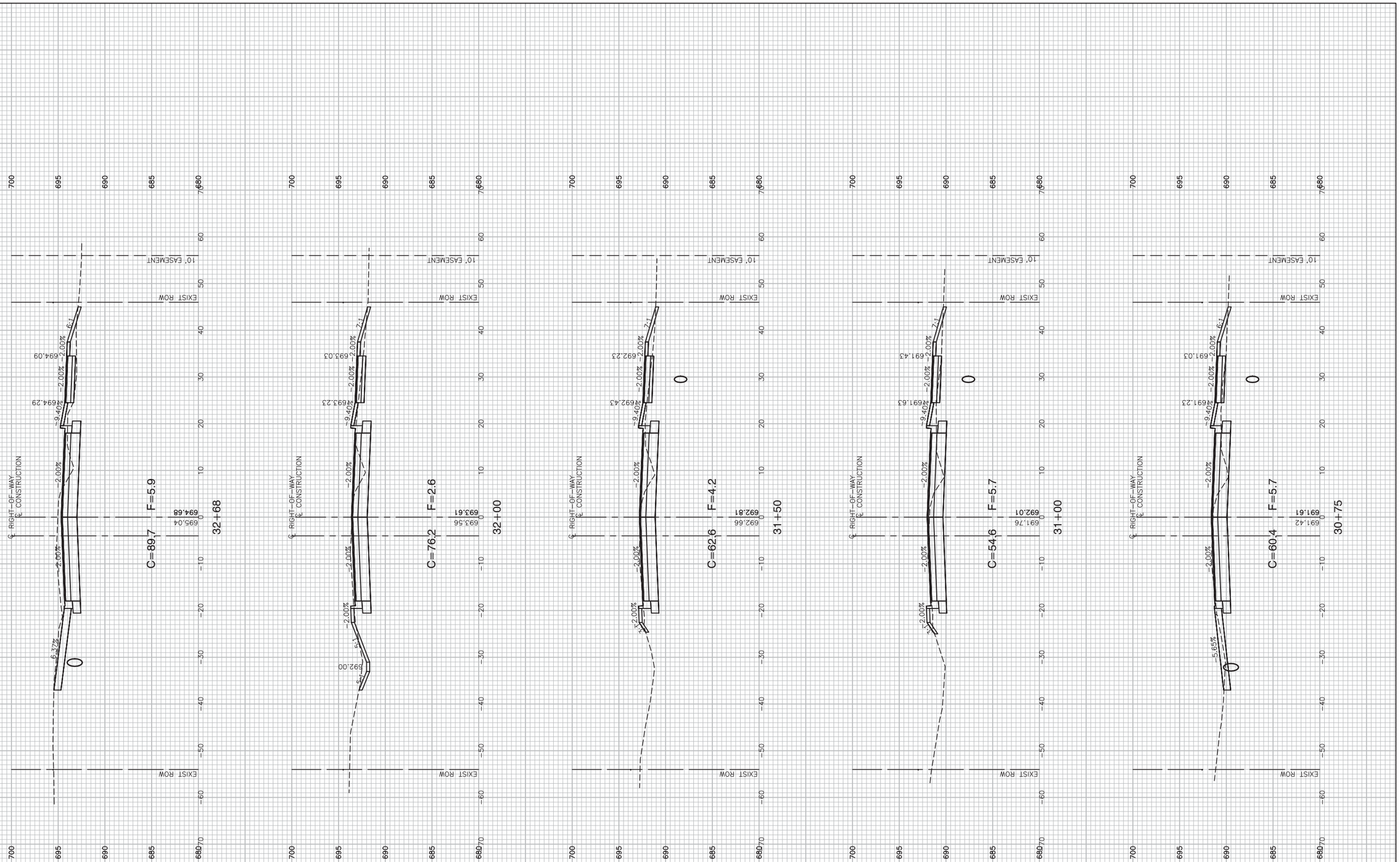
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
CROSS SECTIONS - ST. FRANCIS ROAD

SCALE: H 1"=10' V 1"=5' | SHEET NO. 99 OF 109 SHEETS | STA. | TO STA.

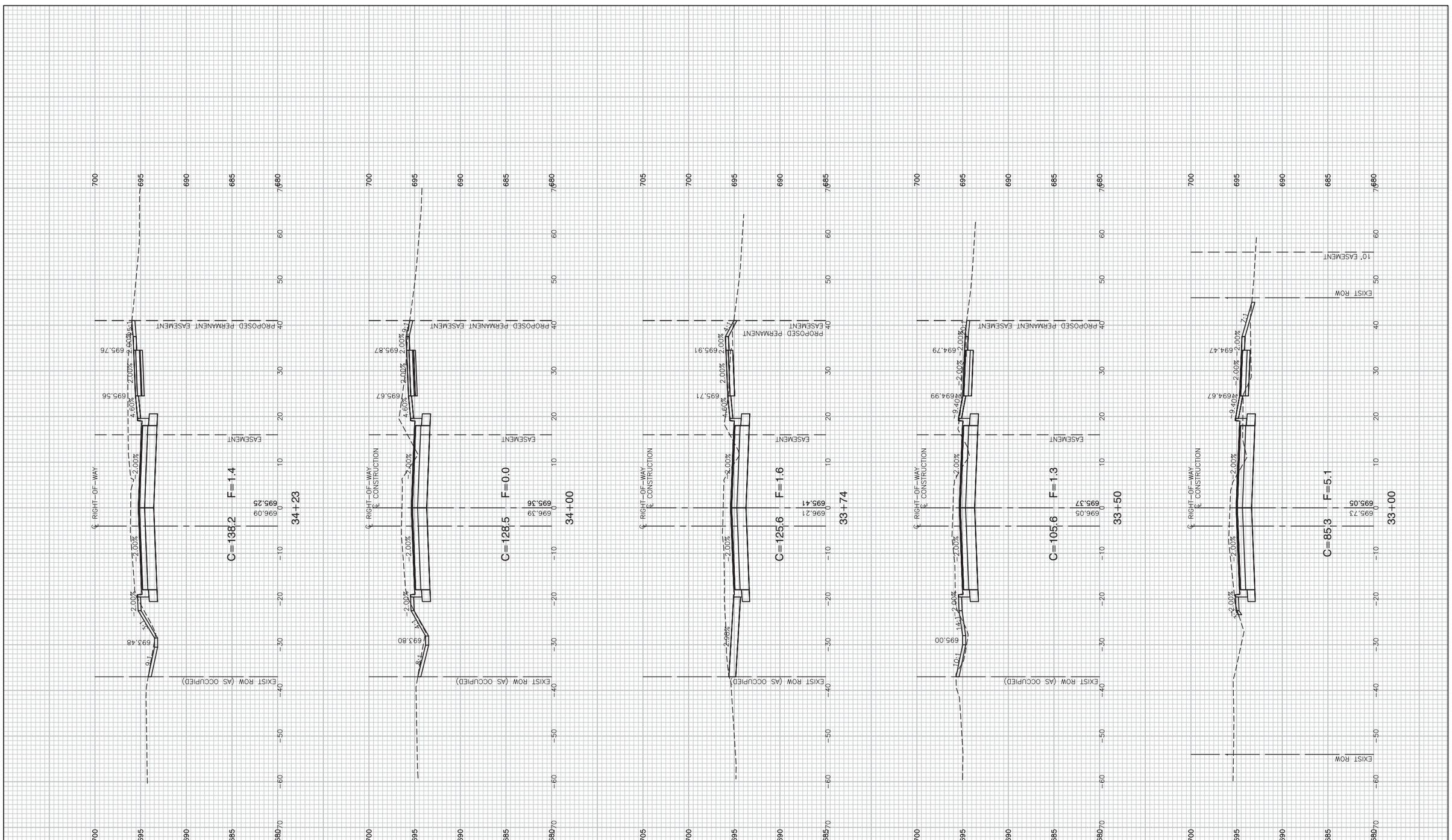
F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	99
		CONTRACT NO. 61D22		

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT ----



FILE NAME = 10423_12-XSCT-01 - IDOT_X(10)	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION CROSS SECTIONS - ST. FRANCIS ROAD
		CHECKED — HLG	REVISED —		
	PLOT SCALE =	DRAWN — MED	REVISED —		
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —		SCALE: H 1"=10' V 1"=5' | SHEET NO. 100 OF 109 SHEETS | STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	100
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



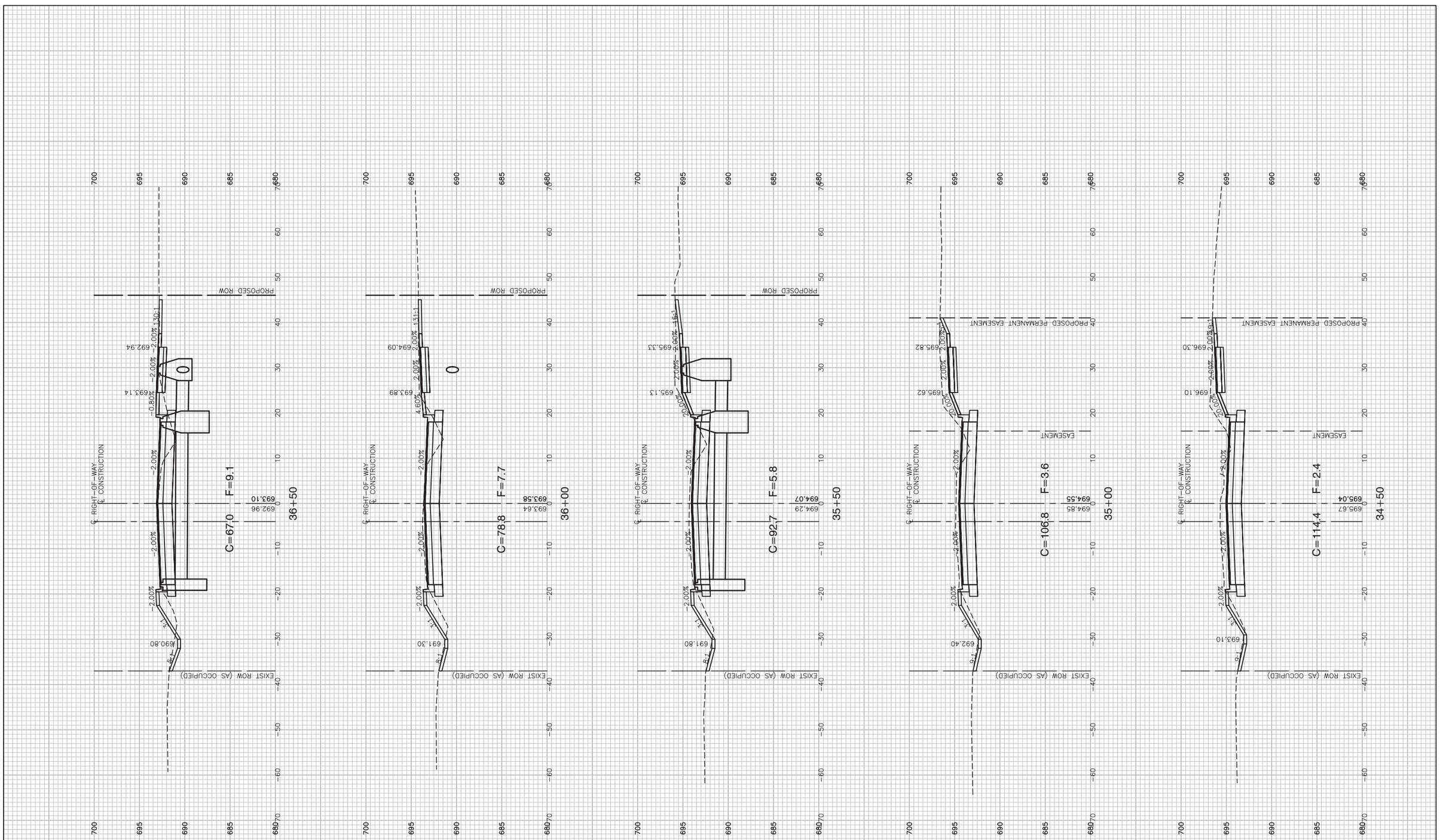
FILE NAME = 10423_12-XSCT-01 - IDOT_X(11)	USER NAME =	DESIGNED — GA	REVISED —	
		CHECKED — HLG	REVISED —	
	PLOT SCALE =	DRAWN — MED	REVISED —	
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
CROSS SECTIONS - ST. FRANCIS ROAD

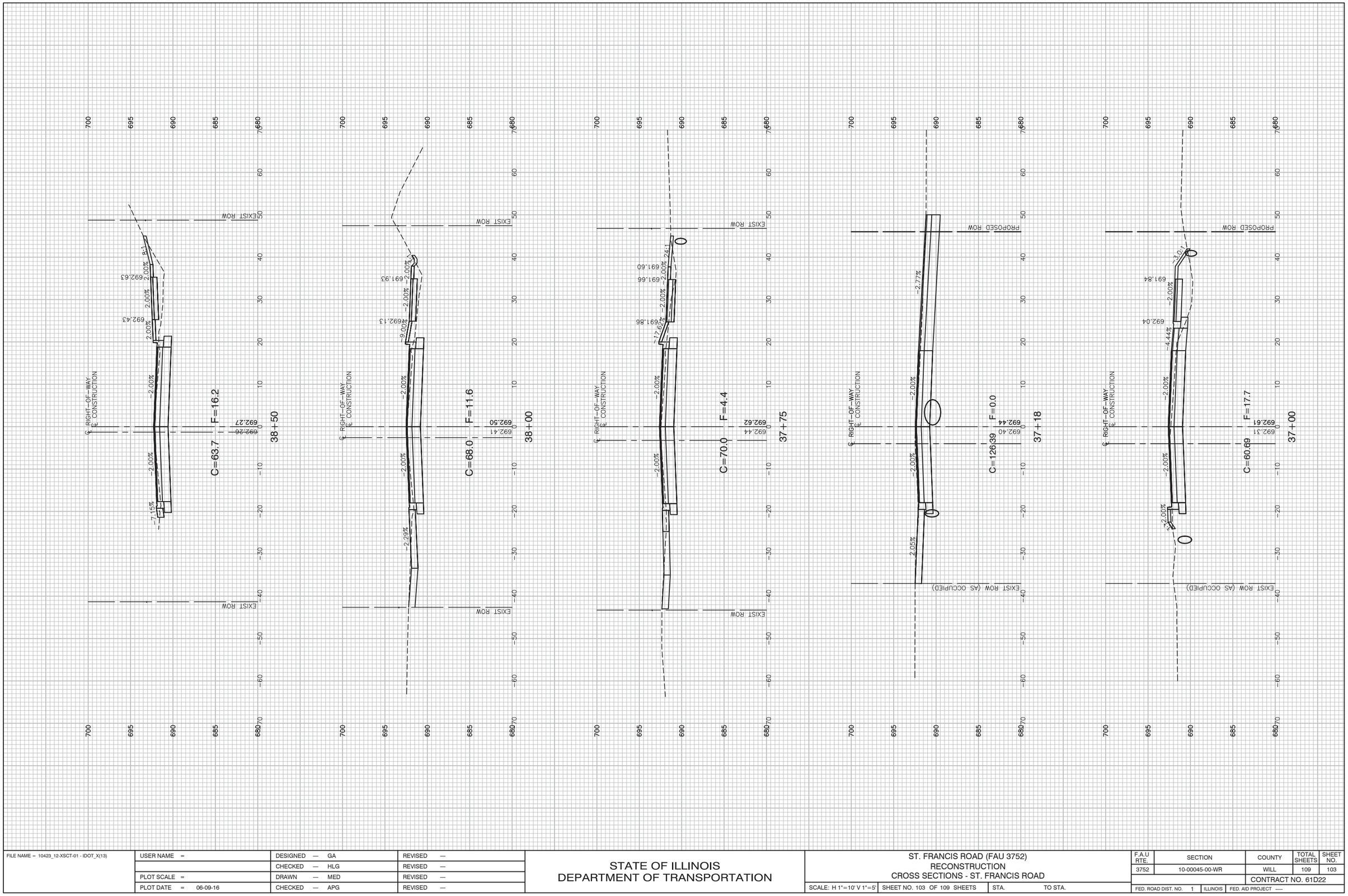
SCALE: H 1"=10' V 1"=5' | SHEET NO. 101 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	101
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



FILE NAME = 10423_12-XSCT-01 - IDOT_X(12)	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION CROSS SECTIONS - ST. FRANCIS ROAD
		CHECKED — HLG	REVISED —		
	PLOT SCALE =	DRAWN — MED	REVISED —		
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —		SCALE: H 1"=10' V 1"=5' | SHEET NO. 102 OF 109 SHEETS | STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	102
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

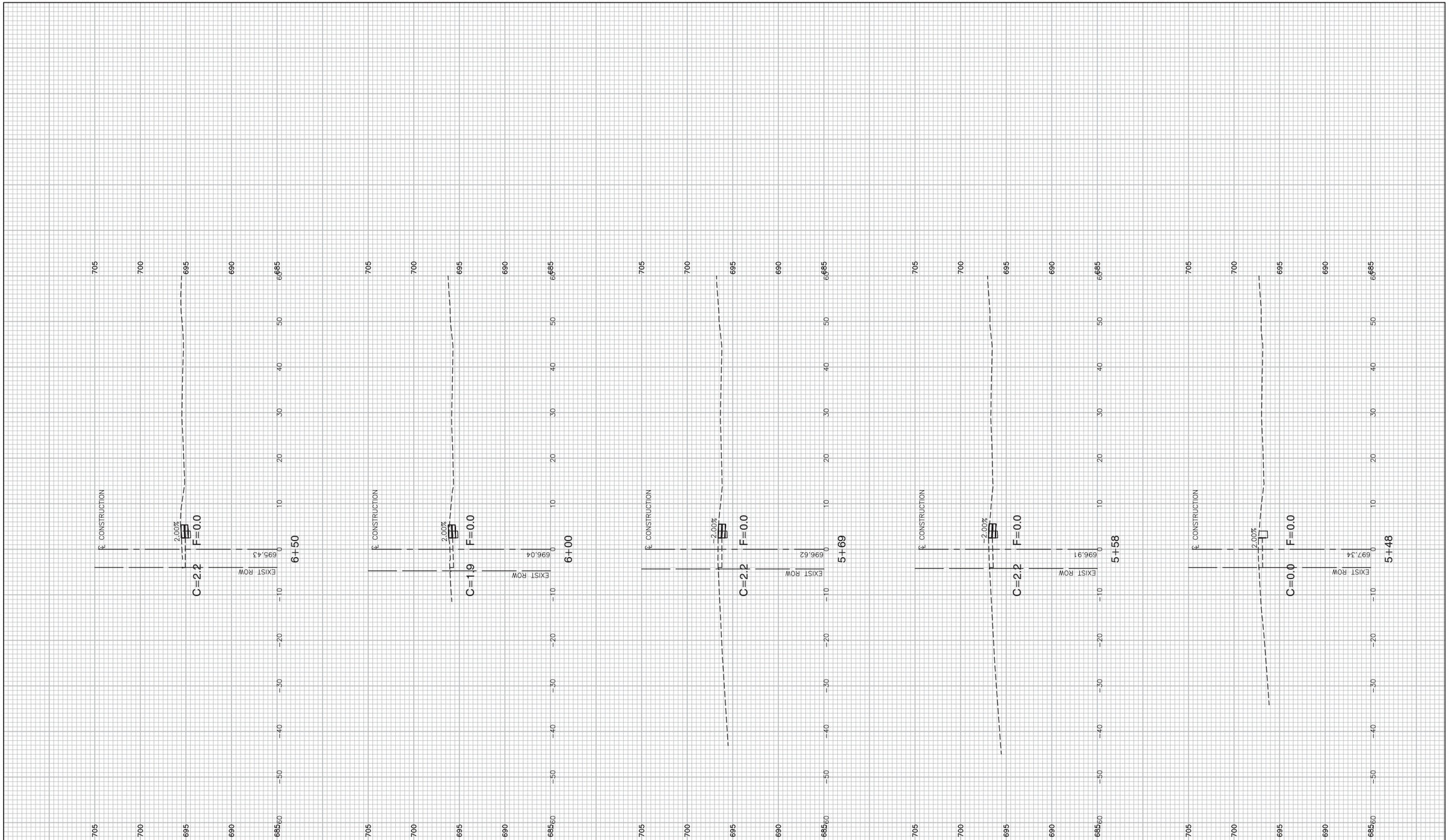


FILE NAME = 10423_12-XSCT-01 - IDOT_X(13)	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION CROSS SECTIONS - ST. FRANCIS ROAD
		CHECKED — HLG	REVISED —		
	PLOT SCALE =	DRAWN — MED	REVISED —		
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —		SCALE: H 1"=10' V 1"=5' | SHEET NO. 103 OF 109 SHEETS | STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	103
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		

FILE NAME = 10423_12-XSCT-01 - IDOT_X(14)	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION CROSS SECTIONS - ST. FRANCIS ROAD
		CHECKED — HLG	REVISED —		
	PLOT SCALE =	DRAWN — MED	REVISED —		
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —		SCALE: H 1"=10' V 1"=5' | SHEET NO. 104 OF 109 SHEETS | STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	104
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



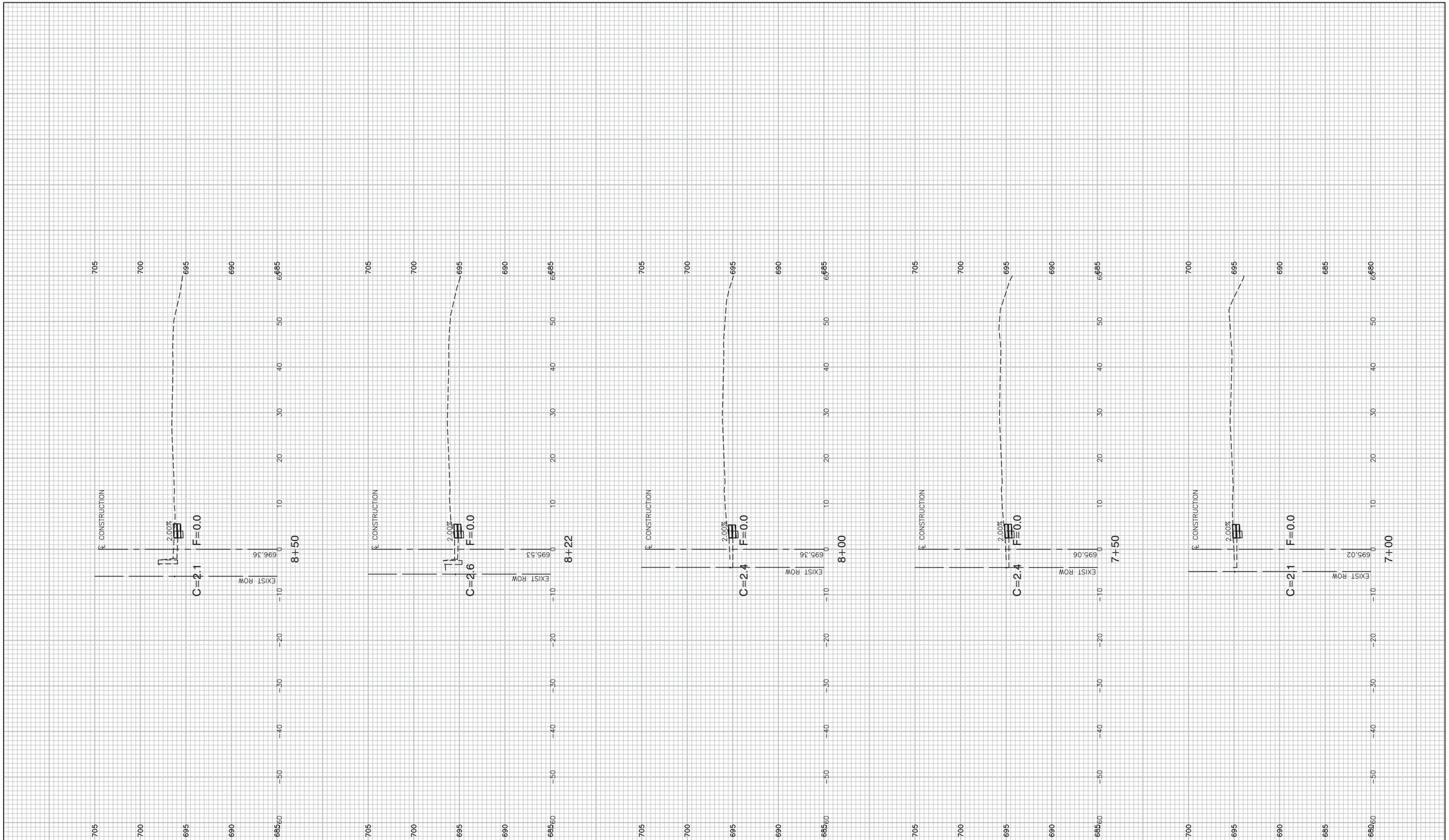
FILE NAME = 10423_12-XSCT-01 - IDOT_X(26)	USER NAME =	DESIGNED — GA	REVISED —
		CHECKED — HLG	REVISED —
	PLOT SCALE =	DRAWN — MED	REVISED —
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. FRANCIS ROAD (FAU 3752)
RECONSTRUCTION
CROSS SECTIONS - WALNUT CREEK DRIVE MULTI-USE PATH

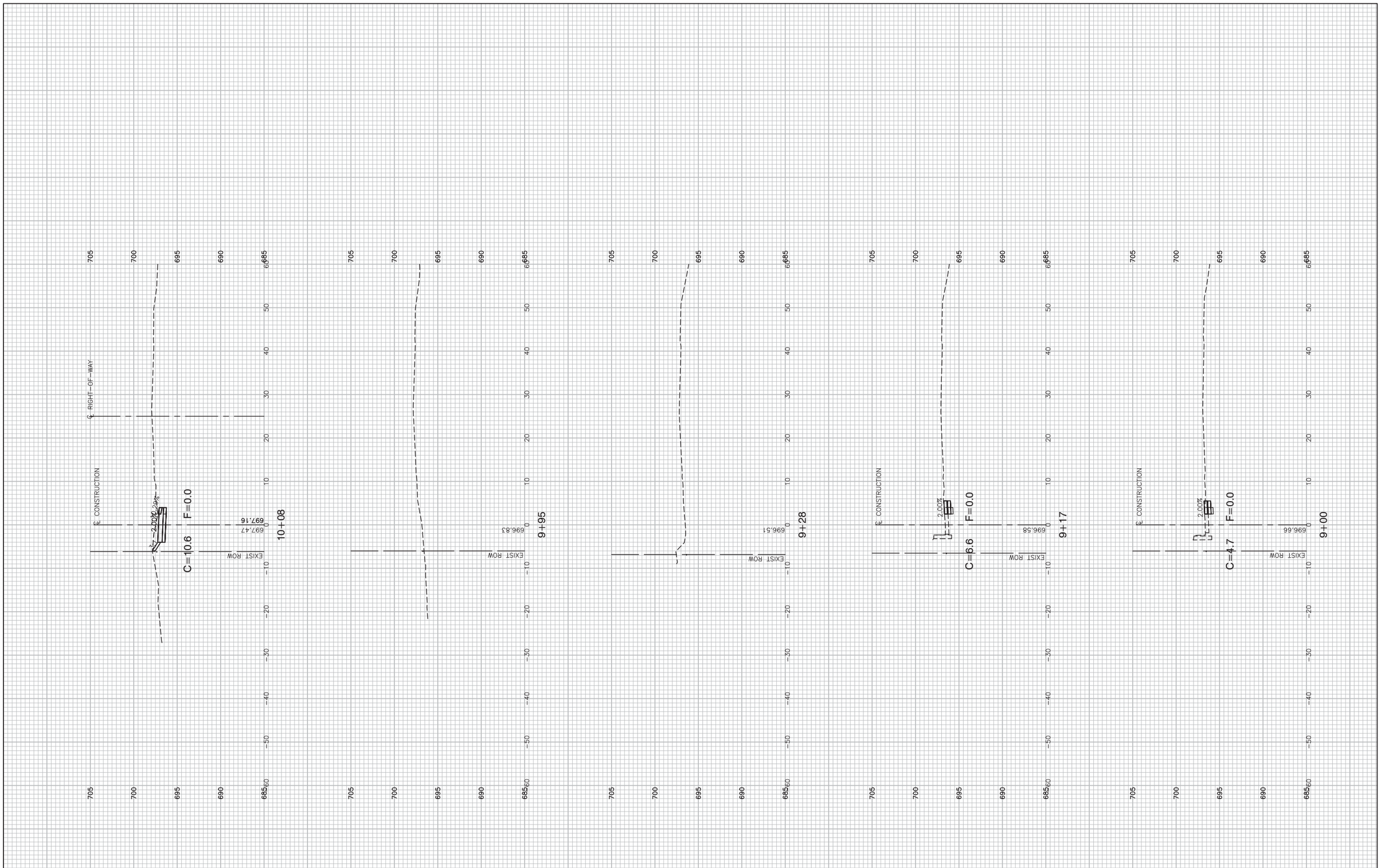
SCALE: H 1"=10' V 1"=5' | SHEET NO. 105 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	105
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



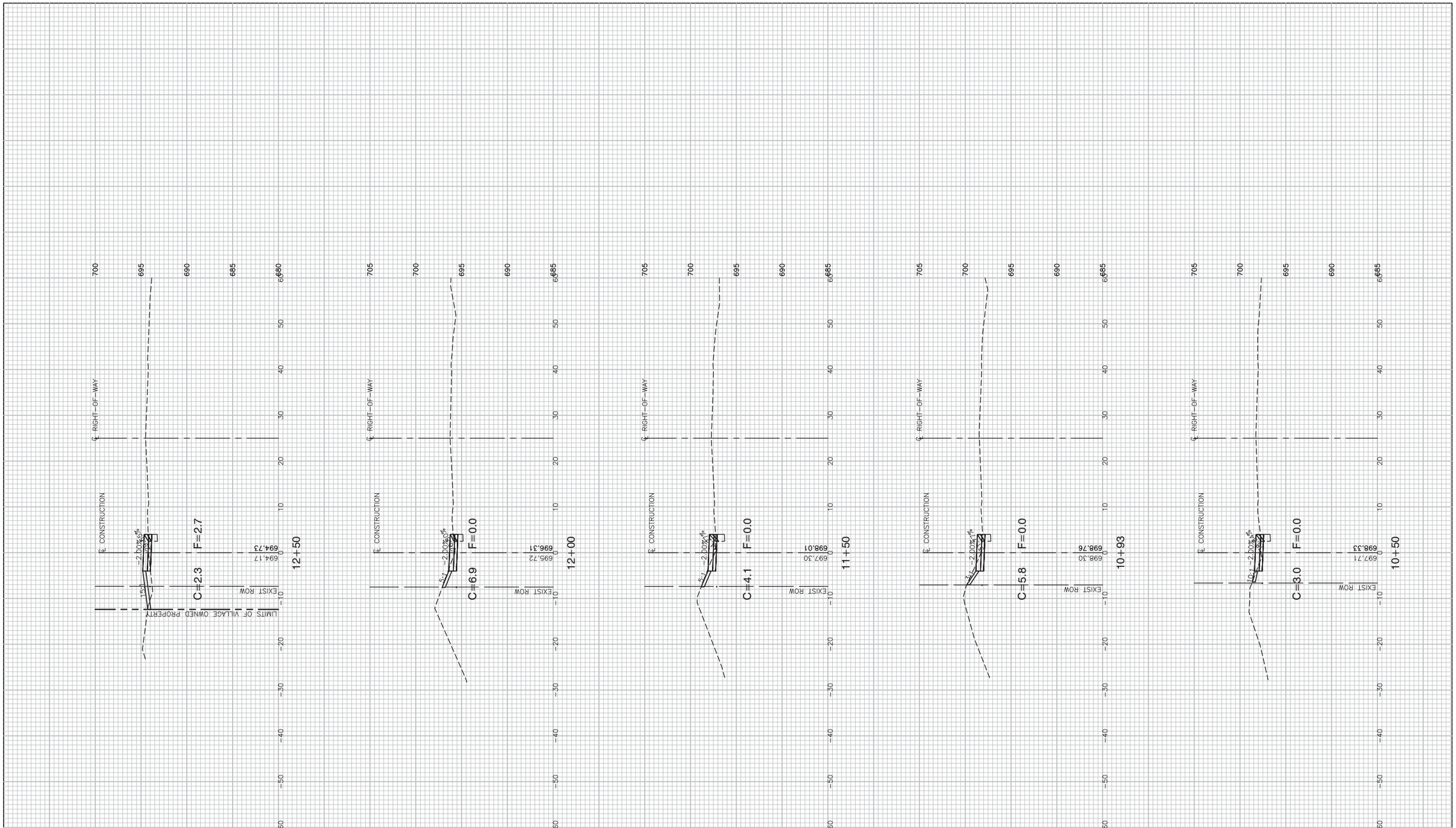
FILE NAME = 10423_12-XSCT-01 - IDOT_X(27)	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION CROSS SECTIONS - WALNUT CREEK DRIVE MULTI-USE PATH
		CHECKED — HLG	REVISED —		
	PLOT SCALE =	DRAWN — MED	REVISED —		
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —		SCALE: H 1"=10' V 1"=5' | SHEET NO. 106 OF 109 SHEETS | STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	106
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



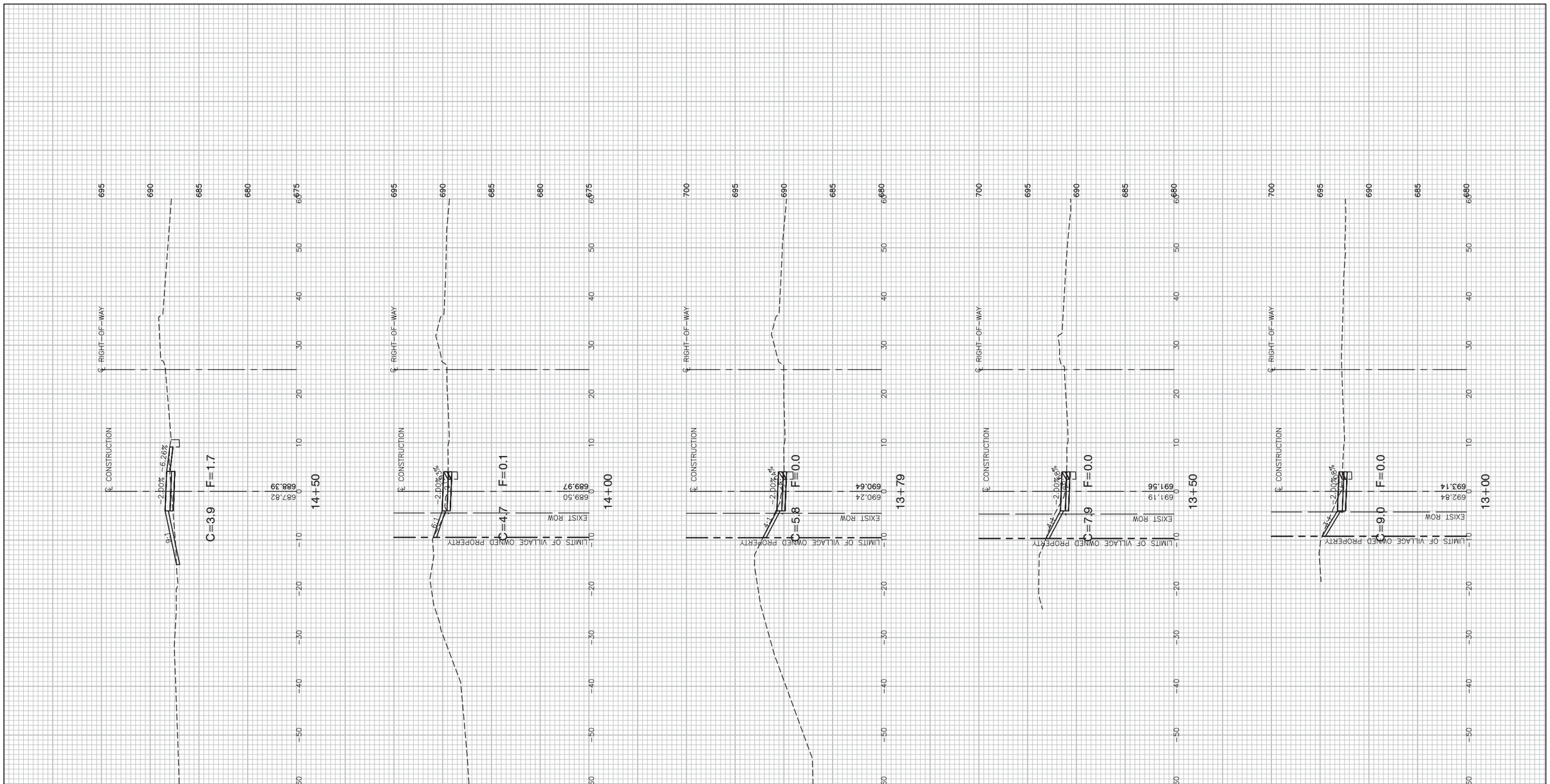
FILE NAME = 10423_12-XSCT-01 - IDOT_X(28)	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION CROSS SECTIONS - WALNUT CREEK DRIVE MULTI-USE PATH
		CHECKED — HLG	REVISED —		
	PLOT SCALE =	DRAWN — MED	REVISED —		
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —		SCALE: H 1"=10' V 1"=5' | SHEET NO. 107 OF 109 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	107
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



FILE NAME = 10423_12-XSCT-01 - IDOT_X(29)	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION CROSS SECTIONS - WALNUT CREEK DRIVE MULTI-USE PATH
		CHECKED — HLG	REVISED —		
	PLOT SCALE =	DRAWN — MED	REVISED —		
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —		SCALE: H 1"=10' V 1"=5' | SHEET NO. 108 OF 109 SHEETS | STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	108
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		



FILE NAME = 10423_12-XSCT-01 - IDOT_X(30)	USER NAME =	DESIGNED — GA	REVISED —	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ST. FRANCIS ROAD (FAU 3752) RECONSTRUCTION CROSS SECTIONS - WALNUT CREEK DRIVE MULTI-USE PATH
		CHECKED — HLG	REVISED —		
	PLOT SCALE =	DRAWN — MED	REVISED —		
	PLOT DATE = 06-09-16	CHECKED — APG	REVISED —		SCALE: H 1"=10' V 1"=5' | SHEET NO. 109 OF 109 SHEETS | STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3752	10-00045-00-WR	WILL	109	109
		CONTRACT NO. 61D22		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT ----		