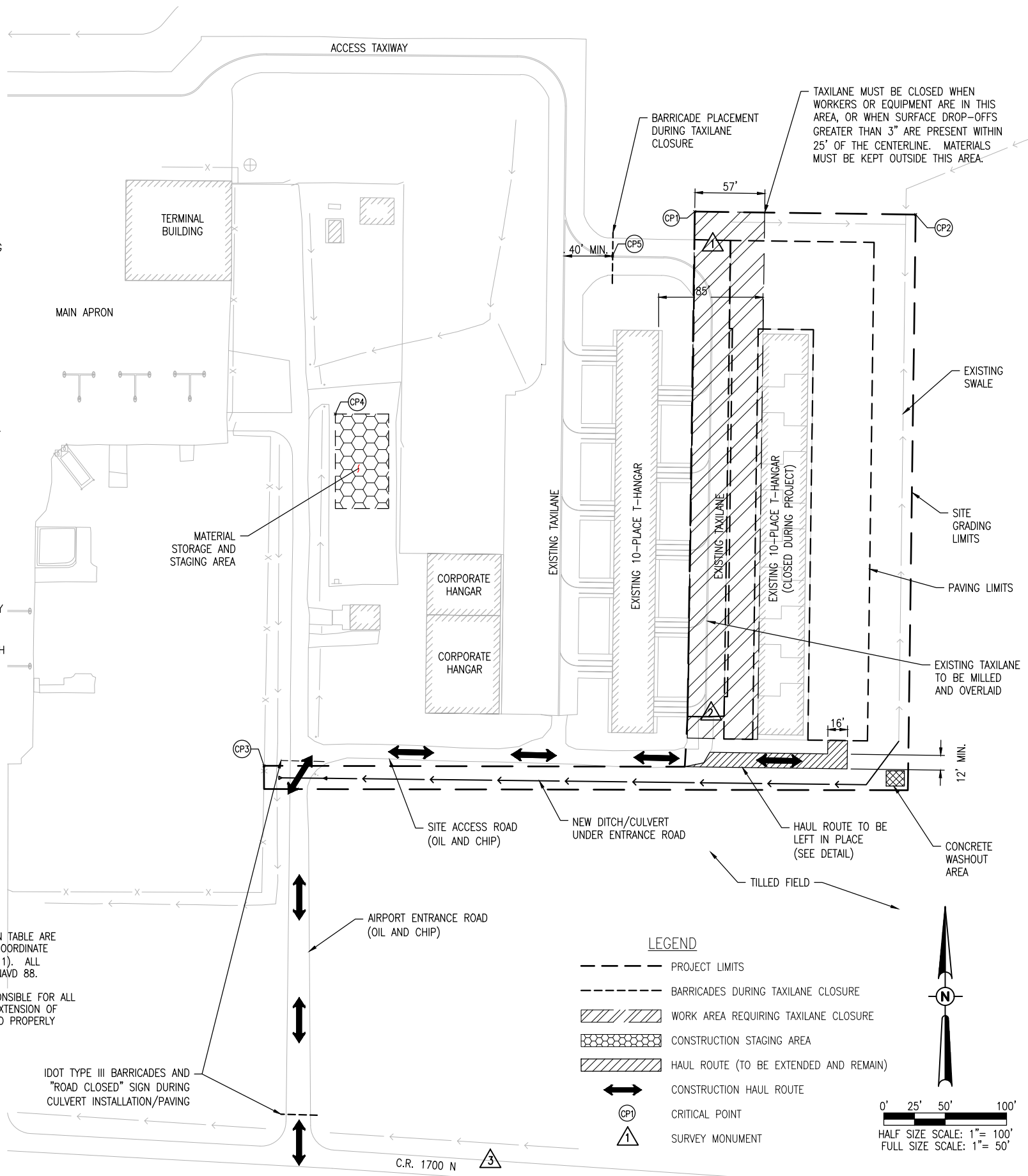


CONSTRUCTION SAFETY PLAN AND NOTES

- CONSTRUCTION SAFETY PLAN** - THE VANDALIA MUNICIPAL AIRPORT IS COMPRISED OF TWO PAVED RUNWAYS. THE PROPOSED T-HANGAR PAVING PROJECT WILL NOT REQUIRE RUNWAY CLOSURE, HOWEVER TEMPORARY CLOSURES OF THE TAXILANE ON THE EAST SIDE OF THE EXISTING T-HANGAR, AS WELL AS THE AIRPORT ENTRANCE ROAD WILL BE NECESSARY. THE NEW EAST T-HANGAR WILL ALSO BE CLOSED FOR THE DURATION OF THE PROJECT. SPECIFIC ITEMS REGARDING CLOSURES INCLUDE:
 - THE TAXILANE SHALL BE CLOSED ANYTIME THE CONTRACTOR IS WORKING WITHIN 40 FEET OF THE TAXILANE CENTERLINE, OR HAS SURFACE DROP-OFFS WITHIN 25 FEET OF THE CENTERLINE THAT ARE GREATER THAN 3 INCHES. THE TAXILANE IS ANTICIPATED TO BE CLOSED FROM THE TIME TRENCH DRAIN EXCAVATION BEGINS UNTIL THE FIRST LIFT OF ASPHALT OR CONCRETE IS PLACED.
 - THE AIRPORT MANAGER AND RESIDENT ENGINEER/TECHNICIAN SHALL BE NOTIFIED A MINIMUM OF 72 HOURS PRIOR TO THE DESIRED TAXILANE CLOSING TIME.
 - THE EXISTING TAXILANE SHALL BE PAVED WITHIN THREE DAYS OF MILLING.
 - BARRICADES SHALL BE PLACED AT THE LOCATION(S) SHOWN ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN.
 - THE CONTRACTOR SHALL MAKE EVERY EFFORT TO MINIMIZE CLOSURES AND ACCOMMODATE TENANTS OF THE EXISTING HANGAR. REGARDLESS WHETHER THE TAXILANE IS OPEN, THE CONTRACTOR SHALL SWEEP THE TAXILANE DAILY AND KEEP THE AREA WITHIN 40 FEET OF THE TAXILANE CENTERLINE FREE OF EQUIPMENT AND MATERIALS WHEN NOT WORKING.
 - THE CONTRACTOR WILL BE ALLOWED TO CLOSE THE AIRPORT ENTRANCE ROAD FOR 1 FULL DAY TO INSTALL AND BACKFILL THE CULVERT ACROSS THE ROAD, AND 1 DAY TO PAVE. COMPACTED AGGREGATE MAY BE USED AS A SHORT-TERM BACKFILL UNTIL PAVING CAN BE COMPLETED. AIRPORT SHALL BE NOTIFIED A MINIMUM OF 3 DAYS BEFORE DESIRED CLOSURE DATE(S).
- ALL PROVISIONS OF FAA ADVISORY CIRCULAR AC 150/5370-2 (CURRENT EDITION), "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION" APPLY TO THIS CONTRACT, EXCEPT AS MODIFIED BY THIS SAFETY PLAN, BY THE AIRPORT MANAGER, OR THROUGH THE RESIDENT ENGINEER/TECHNICIAN AT THE PRECONSTRUCTION CONFERENCE OR DURING THE COURSE OF THE CONTRACT.
- ALL COSTS ASSOCIATED WITH THE WORK AND MATERIALS INVOLVING CONSTRUCTION SAFETY & PHASING AS SHOWN OR DESCRIBED IN THE CONTRACT DOCUMENTS, OR AS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN OR AIRPORT MANAGER THROUGHOUT THE PROJECT, SHALL BE INCIDENTAL TO THE CONSTRUCTION CONTRACT.
- AIRFIELD SAFETY ASSURANCE** - AIRFIELD SAFETY SHALL BE HELD PARAMOUNT AT ALL TIMES. ANY INDIVIDUALS RESPONSIBLE FOR INCURSIONS OR POTENTIAL INCURSIONS WITH AIR TRAFFIC DUE TO NON-COMPLIANCE WITH REQUIREMENTS SET FOR IN THESE PLANS, SPECIFICATIONS, SPECIAL PROVISIONS, AND FAA ADVISORY CIRCULAR 150/5370-2F WILL BE SUBJECT TO AN IMMEDIATE SUSPENSION OF DRIVING PRIVILEGES ON THE AIRPORT OR A COMPLETE RESTRICTION FROM ENTERING THE AIR OPERATIONS AREA ALTOGETHER. THE AIRPORT MANAGER OR RESIDENT ENGINEER/TECHNICIAN MAY STOP THE WORK AT ANY TIME THEY BELIEVE AIRFIELD SAFETY IS BEING COMPROMISED.
- ALL CONSTRUCTION EQUIPMENT OPERATING IN THE PRESCRIBED CONSTRUCTION AREA IS REQUIRED TO DISPLAY A CHECKERBOARD FLAG PROPERLY LOCATED OR A ROTATING BEACON (STROBE) AS SPECIFIED IN AC 150/5210-5, "PAINTING, MARKING, AND LIGHTING OF VEHICLES USED ON AN AIRPORT" (CURRENT EDITION).
- THE MAXIMUM ANTICIPATED HEIGHT OF THE CONSTRUCTION EQUIPMENT IS 25 FEET OR AS SHOWN FOR A PARTICULAR LOCATION. EQUIPMENT WITH HEIGHTS GREATER THAN THAT SHOWN WILL NOT BE ALLOWED ONSITE UNLESS ADDITIONAL AIR SPACE APPROVAL IS ACQUIRED.
- CLOSED AIRFIELD PHASING AREAS, OPEN TRENCHES, AND OTHER HAZARDOUS AREAS AT THE CONSTRUCTION SITE SHALL BE PROMINENTLY MARKED WITH LIGHTED BARRICADES WITH STEADY BURNING OR FLASHING RED LIGHTS AS SPECIFIED IN 150/5370-2, "OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION", LATEST EDITION. LIGHTED BARRICADES MUST BE NO TALLER THAN 18" (EXCLUSIVE OF SUPPLEMENTARY LIGHTS AND FLAGS) ON THE TAXIWAYS. CONTRACTOR SHALL NIGHT CHECK BARRICADES DAILY FOR PROPER OPERATION.
- NO PAVEMENT DROP-OFFS GREATER THAN 3" ARE ALLOWED TO REMAIN ON ANY ACTIVE AIRFIELD PAVEMENT AREA. TEMPORARY EARTH OR SHOULDERS SHALL BE PLACED IF AN INCOMPLETE AREA IS TO BE REOPENED TO AIRCRAFT TRAFFIC.
- SOIL, DEBRIS, AND LOOSE MATERIAL DROPPED OR TRUCKED ONTO AIRPORT ROADS, TAXIWAYS, AND SOD SURFACES, OR WHICH CAN BE BLOWN ONTO SUCH SURFACES, SHALL BE IMMEDIATELY SWEEPED, PICKED UP AND REMOVED, OR PLACED INTO CLOSED CONTAINERS. ANY DAMAGE TO AIRPORT PROPERTY SHALL BE REPAIRED IMMEDIATELY AT NO COST TO THE OWNER.
- EACH CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND MAINTAINING AIRPORT LIGHTING AND NAVIGATIONAL ELECTRICAL SYSTEMS DURING CONSTRUCTION. A CONTACT PERSON AND TELEPHONE NUMBER FOR 24 HOUR EMERGENCY IMMEDIATE REPAIR SHALL BE SUBMITTED TO THE AIRPORT MANAGER AND ENGINEER. HAUL ROUTES CROSSING PAVEMENT, DRAINAGE, MISCELLANEOUS STRUCTURES AND/OR AIRFIELD CABLES SHALL BE PROTECTED FROM DAMAGE.
- ALL AIRCRAFT AND AIRPORT OPERATIONS HAVE THE RIGHT-OF-WAY. CONTRACTOR TO YIELD TO VEHICLES AND REMAIN CLEAR AT ALL TIMES.
- THE CONTRACTOR SHALL UTILIZE WATER AND/OR CHEMICALS APPROVED BY THE RESIDENT ENGINEER AS NECESSARY TO CONTROL DUST.
- RADIO CONTROL** - THE CONTRACTOR WILL BE REQUIRED TO PROVIDE AN AIRBAND RADIO AND MAINTAIN TWO-WAY RADIO CONTACT WITH THE VANDALIA MUNICIPAL AIRPORT UNICOM (122.8 Mhz) WHILE HE HAS PERSONNEL OR EQUIPMENT ON THE AIRFIELD.



NO.	DESCRIPTION	NORTHING	EASTING	ELEV.
1	MAG NAIL IN PAVEMENT	846,318.11	748,210.15	
2	MAG NAIL IN PAVEMENT	845,935.61	748,208.94	
3	NGS "Y 256" DISK IN CULVERT HEADWALL	845,571	748,030	531.89

- SURVEY NOTES**
- ALL COORDINATE VALUES SHOWN IN TABLE ARE BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM, EAST ZONE, NAD-83 (2011). ALL ELEVATIONS ARE REFERENCED TO NAVD 88.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL CONSTRUCTION LAYOUT AND ANY EXTENSION OF THE CONTROL NETWORK NEEDED TO PROPERLY COMPLETE THE WORK.

POINT #	DESCRIPTION	LATITUDE	LONGITUDE	GRND. ELEV. (MSL)	HEIGHT (AGL)
1	EQUIPMENT	N038° 59' 16.549"	W089° 09' 49.610"	532.0	25'
2	EQUIPMENT	N038° 59' 16.541"	W089° 09' 47.331"	531.6	25'
3	EQUIPMENT	N038° 59' 12.053"	W089° 09' 54.002"	532.0	25'
4	EQUIPMENT	N038° 59' 14.892"	W089° 09' 53.299"	534.0	25'
5	BARRICADE	N038° 59' 16.185"	W089° 09' 50.454"	533.0	4'

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fax: 217-788-2503

Illinois Licensed
Professional Service Corporation
#184-001084

Vandalia Park District
900 North Locust Street
Vandalia, Illinois 62471

CONSTRUCT T-HANGAR ACCESS TAXIWAYS

IL. PROJ. No: VLA-4628

Contract No. VA020

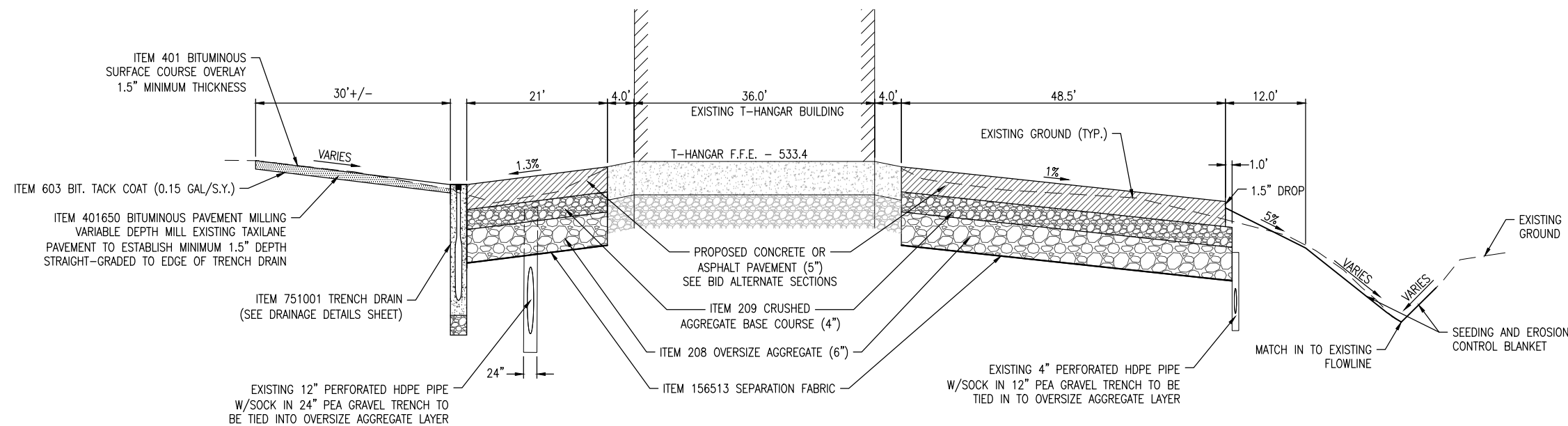
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		DES	DWN	REV

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CAD FILE: G-003-SFY.DWG
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DRAWN BY: KBS
REVIEWED BY: RAW

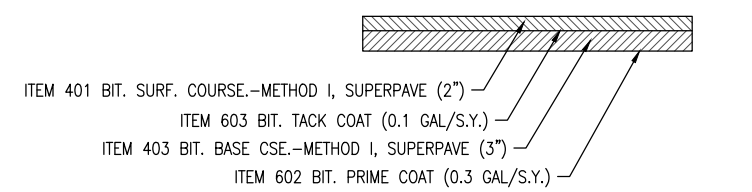
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EXISTING SITE AND SAFETY PLAN

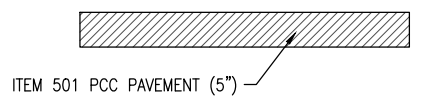
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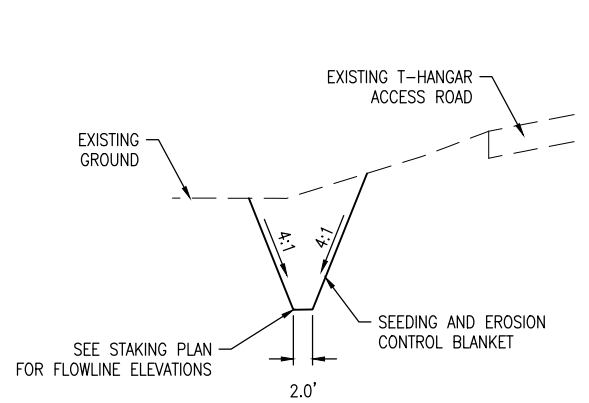
HANGAR SITE TYPICAL SECTION A-A
NOT TO SCALE



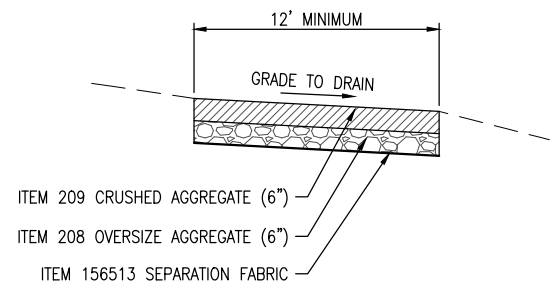
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(ASPHALT)
NOT TO SCALE



ALTERNATE B TYPICAL PAVEMENT SECTION
(CONCRETE)
NOT TO SCALE

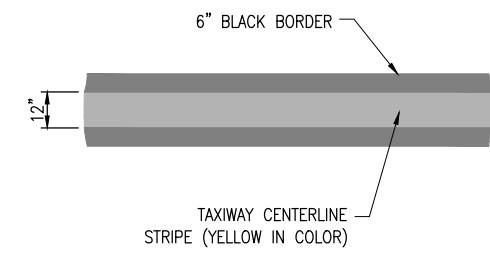


DITCH TYPICAL SECTION B-B
NOT TO SCALE



- NOTES:
1. HAUL ROUTE TO REMAIN IN PLACE AT CONCLUSION OF PROJECT.
 2. ALL WORK AND MATERIALS INVOLVING HAUL ROUTE SHALL BE PAID FOR UNDER ITEM AR150540 "HAUL ROUTE" - PER LUMP SUM.
 3. CONTRACTOR SHALL REGRADE AND CAP HAUL ROUTE WITH FRESH AGGREGATE AS NECESSARY AT CONCLUSION OF PROJECT.

HAUL ROUTE DETAIL
NOT TO SCALE



TAXIWAY CENTERLINE DETAIL
NOT TO SCALE

CONSTRUCT
T-HANGAR ACCESS
TAXIWAYS

IL. PROJ. No: VLA-4628

Contract No. VA020

NO.	DATE	DESCRIPTION		
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ISSUE: JAN. 12, 2018
PROJECT NO: 16A0042
CAD FILE: C-501-TYP.DWG
DESIGN BY: KBS
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SHEET TITLE
TYPICAL SECTION AND DETAILS

**CONSTRUCT
T-HANGAR ACCESS
TAXIWAYS**

IL. PROJ. No: VLA-4628

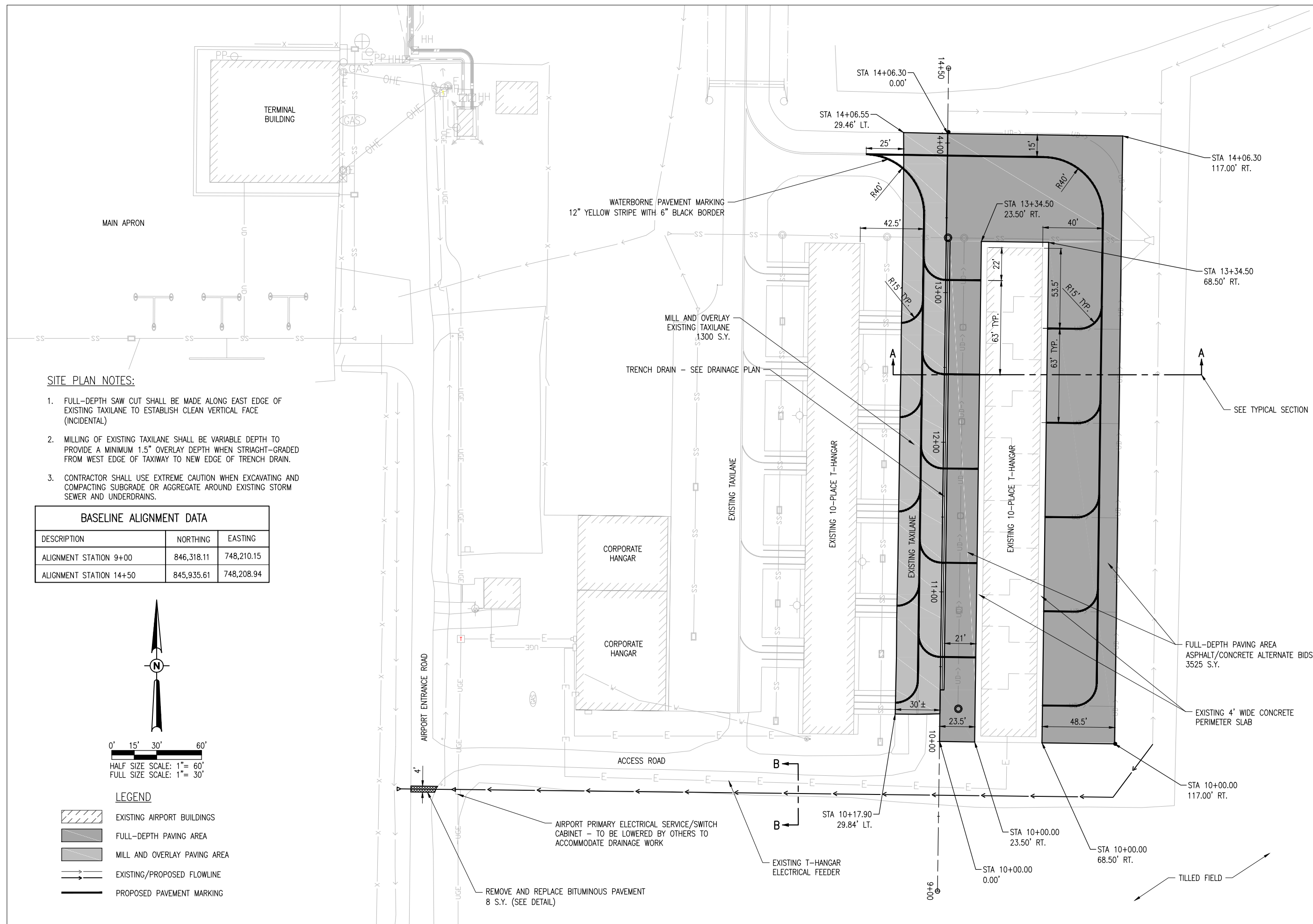
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ISSUE: JAN. 12, 2018
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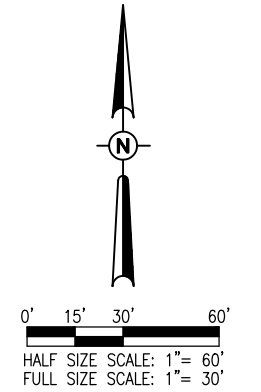
**PROPOSED SITE
PLAN**



- SITE PLAN NOTES:**
- FULL-DEPTH SAW CUT SHALL BE MADE ALONG EAST EDGE OF EXISTING TAXIWAY TO ESTABLISH CLEAN VERTICAL FACE (INCIDENTAL)
 - MILLING OF EXISTING TAXIWAY SHALL BE VARIABLE DEPTH TO PROVIDE A MINIMUM 1.5" OVERLAY DEPTH WHEN STRAIGHT-GRADED FROM WEST EDGE OF TAXIWAY TO NEW EDGE OF TRENCH DRAIN.
 - CONTRACTOR SHALL USE EXTREME CAUTION WHEN EXCAVATING AND COMPACTING SUBGRADE OR AGGREGATE AROUND EXISTING STORM SEWER AND UNDERDRAINS.

BASELINE ALIGNMENT DATA

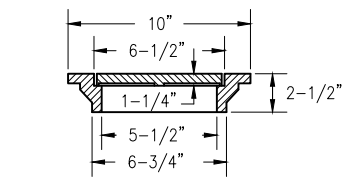
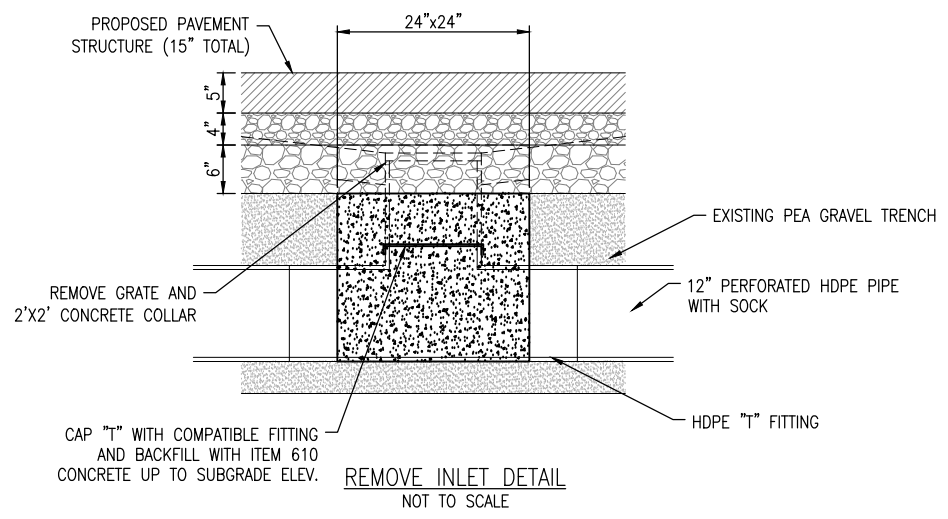
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ALIGNMENT STATION 9+00	846,318.11	748,210.15
ALIGNMENT STATION 14+50	845,935.61	748,208.94



LEGEND

	EXISTING AIRPORT BUILDINGS
	FULL-DEPTH PAVING AREA
	MILL AND OVERLAY PAVING AREA
	EXISTING/PROPOSED FLOWLINE
	PROPOSED PAVEMENT MARKING

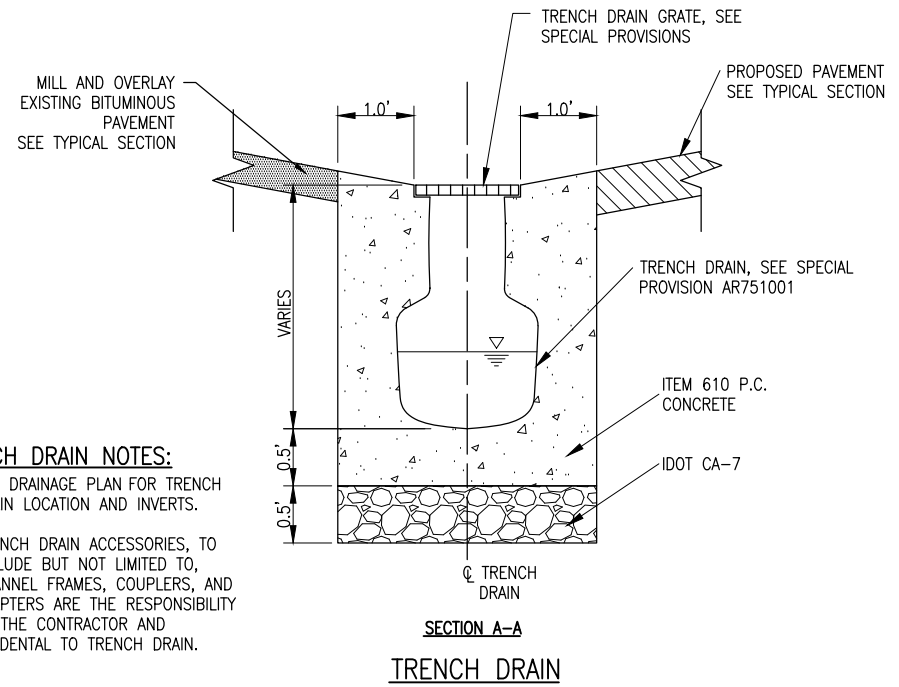
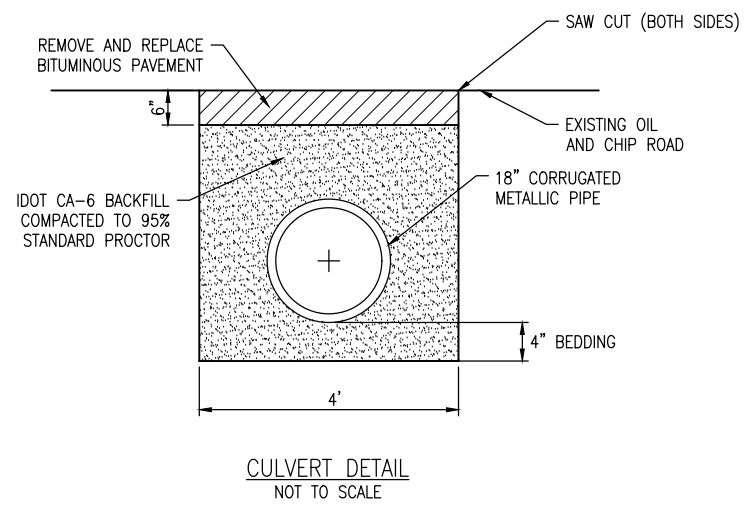
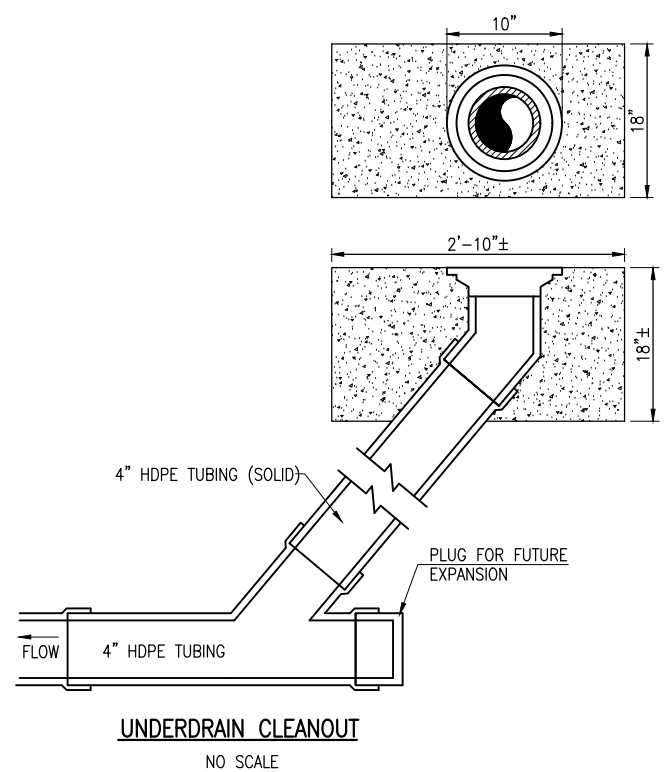
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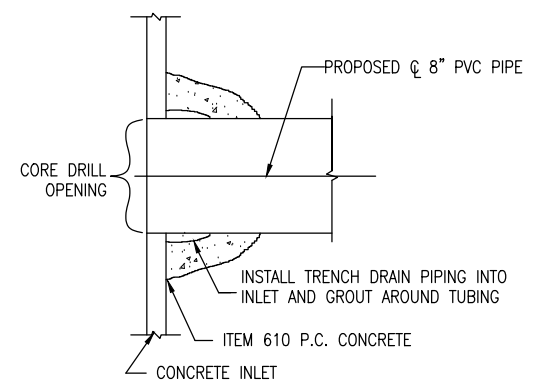
CAST IRON FRAME AND COVER
NEENAH R-6013, DEETER 1810,
EAST JORDAN 2790-6 OR
APPROVED EQUAL

CLEANOUT NOTES

1. DIAMETER OF PIPE AS SPECIFIED.
2. TOP OF CLEANOUTS SHALL BE 2" ABOVE FINISH GROUND LINE AT LOCATION SHOWN ON PLANS.
3. 1/2" CHAMFER TO BE USED ON ALL EXPOSED EDGES OF CLEANOUTS.
4. THE CONCRETE SHALL BE STRUCTURAL PORTLAND CEMENT CONCRETE (NON-REINFORCED) IN ACCORDANCE WITH ITEM 610.



- TRENCH DRAIN NOTES:**
1. SEE DRAINAGE PLAN FOR TRENCH DRAIN LOCATION AND INVERTS.
 2. TRENCH DRAIN ACCESSORIES, TO INCLUDE BUT NOT LIMITED TO, CHANNEL FRAMES, COUPLERS, AND ADAPTERS ARE THE RESPONSIBILITY OF THE CONTRACTOR AND INCIDENTAL TO TRENCH DRAIN.



- NOTES**
1. HOLE FOR TRENCH DRAIN PIPING TO BE CORE DRILLED INTO INLET AT ELEVATION SPECIFIED.
 2. CONNECTION IS INCIDENTAL TO PIPE.
- TRENCH DRAIN CONCRETE COLLAR AND GROUT CONNECTION**

CONSTRUCT
T-HANGAR ACCESS
TAXIWAYS

IL. PROJ. No: VLA-4628

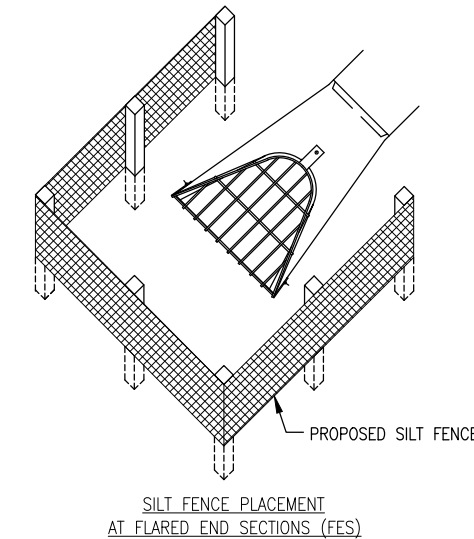
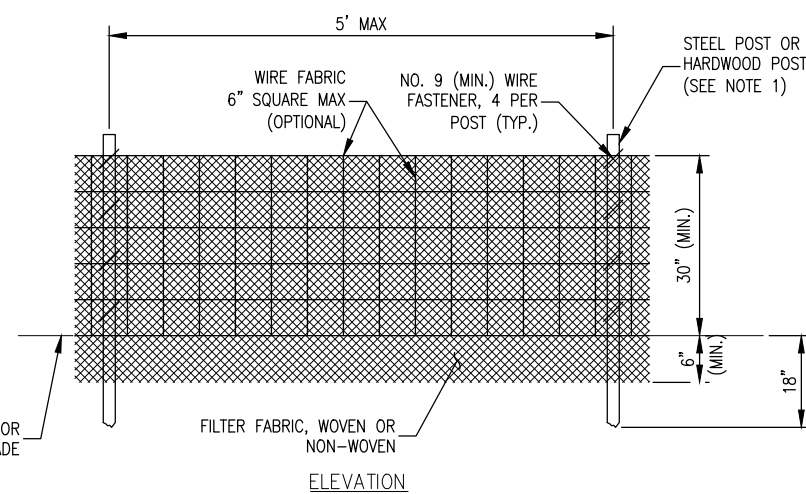
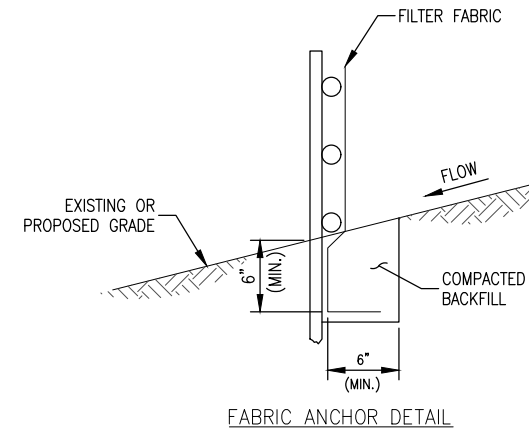
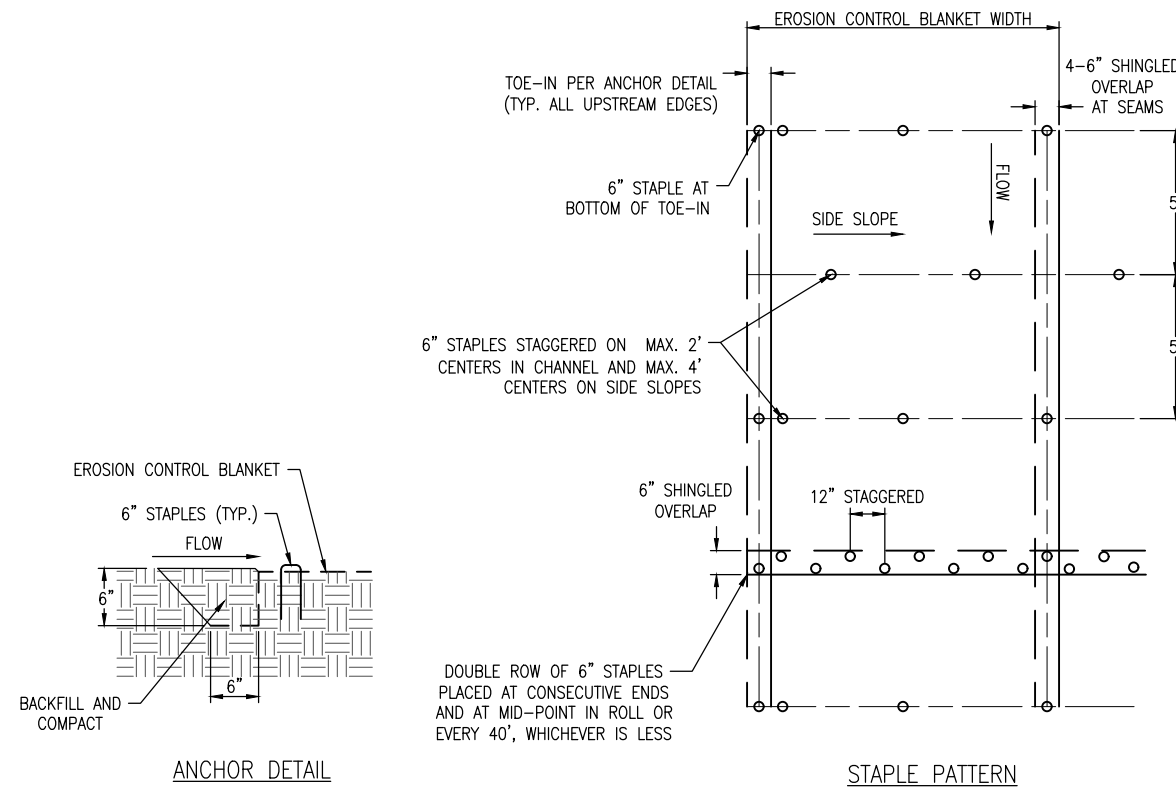
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CAD FILE: C-511-DRN.DWG				
DESIGN BY: KBS				
DRAWN BY: KBS				
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SHEET TITLE				

DRAINAGE DETAILS

STORM WATER POLLUTION PREVENTION NOTES

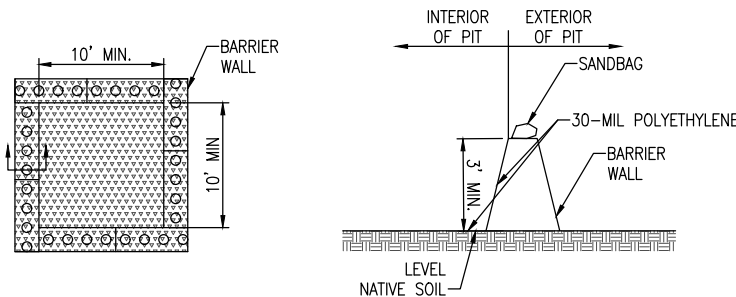
1. THE CONTRACTOR SHALL IMPLEMENT ALL PROVISIONS OF THE CONTRACT DOCUMENTS TO ASSURE THAT STORM WATER POLLUTION PREVENTION ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY MANNER. SEDIMENTATION MUST NOT BE TRANSPORTED OFF THE CONSTRUCTION SITE. PERMANENT DRAINAGE FEATURES AND VEGETATIVE MEASURES SHALL BE PROVIDED AS SOON AS POSSIBLE.
2. THE MAINTENANCE OF ALL STORM WATER POLLUTION PREVENTION MEASURES IS INCIDENTAL TO THE ASSOCIATED ITEM.
3. THE CONTRACTOR SHALL BE REQUIRED TO IMPLEMENT AND MAINTAIN STORM WATER POLLUTION PREVENTION PRACTICES AND MEASURES PRIOR TO THE STRIPPING OF EXISTING VEGETATION WHEREVER POSSIBLE AND AS SOON AS CONSTRUCTION PERMITS IN OTHER AREAS. POLLUTION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE CONTRACT DOCUMENTS, INCLUDING THESE CONSTRUCTION PLANS, AND WITH STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL, ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, CURRENT ISSUE. THE CONTRACTOR SHALL ADJUST HIS OPERATIONS AND IMPLEMENT POLLUTION CONTROL MEASURES SO THAT NO RUNOFF FROM STRIPPED AREAS WILL LEAVE THE CONSTRUCTION SITE OTHER THAN THROUGH SEDIMENT TRAPS OR OTHER SUITABLE CONTROL MEASURES.
4. POLLUTION CONTROL ITEMS SHALL BE PROVIDED AS NOTED ON THE STORM WATER POLLUTION PREVENTION PLAN AND IN THE STORM WATER POLLUTION PREVENTION DETAILS AND AS DIRECTED BY THE ENGINEER. THE LIMITS OF SUCH MEASURES SHALL BE STAKED BY THE CONTRACTOR PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. SUCH LIMITS MAY BE ADJUSTED BY THE ENGINEER TO ACCOUNT FOR ACTUAL SITE CONDITIONS EXPERIENCED DURING CONSTRUCTION. ADDITIONAL COMPENSATION FOR MEASURES EXCEEDING THE PLAN QUANTITIES WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR EACH ITEM.
5. THE CONTRACTOR IS TO MAINTAIN AND ADJUST, REPAIR OR REPLACE ALL POLLUTION PREVENTION MEASURES AS REQUIRED OR AS DIRECTED BY THE ENGINEER UNTIL PERMANENT VEGETATION HAS BEEN ESTABLISHED. MAINTENANCE OF POLLUTION CONTROL MEASURES IS TO BE PROVIDED AT NO ADDITIONAL COST TO THE CONTRACT.
6. ADDITIONAL STORMWATER POLLUTION PREVENTION MEASURES ARE EXISTING ON SITE LOCATED AT DRAINAGE FACILITIES AND ALONG THE PROPERTY LINE.



NOTES:

1. FENCE POST SHALL BE EITHER STEEL "T" LINE POST OR HARDWOOD POST WITH A MINIMUM SECTIONAL AREA OF 2.0 SQUARE INCHES. A CARPENTER'S (NOMINAL) 2"x2" POST WILL MEET SPECIFICATIONS.
2. TOP AND BOTTOM WIRE OF WIRE FABRIC SHALL BE MINIMUM GAGE NO. 9. INTERMEDIATE WIRES OF THE WIRE FABRIC SHALL BE MINIMUM GAGE NO. 11.
3. WIRE FABRIC SHALL BE SECURELY FASTENED TO FENCE POSTS WITH NO. 9 GAGE WIRE MINIMUM. FOUR (4) FASTENERS PER POST REQUIRED.
4. FILTER FABRIC SHALL BE SECURELY FASTENED TO WIRE FABRIC AND POSTS WITH TIES OR STAPLES SPACED AT 12" APART AT THE TOP, MIDDLE AND BOTTOM.
5. WHEN TWO SECTIONS OF FILTER FABRIC MEET, THEY SHALL BE OVERLAPPED BY 6" AND FOLDED AND ATTACHED TO THE WIRE FABRIC AT A POST.
6. FILTER FABRIC SHALL BE IN ACCORDANCE WITH SPECIAL PROVISIONS WITH APPARENT OPENING SIZE (AOS) OF AT LEAST 40 FOR NONWOVEN AND WOVEN (OR MAXIMUM OF 0.60mm).
7. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS AND THE USE OF TEMPORARY OR PERMANENT MEASURES.
8. ALL STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
9. SILT FENCE SHALL BE INSTALLED PRIOR TO ANY GRADING WORK IN THE AREA TO BE PROTECTED. PERIODIC INSPECTION SHALL BE PERFORMED AND REQUIRED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN EVENT.
10. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED AND REPLACED WHEN BULGES DEVELOP IN THE SILT FENCE.
11. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION. DISCHARGES SHALL BE ROUTED THROUGH AN EFFECTIVE SEDIMENT CONTROL MEASURE (E.G. SEDIMENT TRAP, SEDIMENT BASIN, OR OTHER APPROPRIATE MEASURE).
12. FENCE POSTS SHALL BE REMOVED WHEN DIRECTED AT PROJECT END.
13. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER OR GOVERNING AGENCY.

CONCRETE WASHOUT DETAIL



NOTES:

1. IMPERMEABLE SHEETING MUST EXTEND OVER ENTIRE BASIN AND BERM TO PREVENT ESCAPE OF DISCHARGE.
2. PROTECT AREA AROUND UNIT FOR 10 FEET WITH PLASTIC UNDER AND AROUND UNIT TO CONTAIN SPILLS OR OVERFLOW.
3. FACILITY LINED WITH 30-MIL POLYETHYLENE LINER AND SECURED USING SAND BAGS, OR OTHER ANCHORS, AND SHALL BE FREE OF HOLES OR TEARS.
4. FACILITY IS TO BE LOCATED ON LEVEL GROUND.
5. WASHOUT NEEDS TO BE COVERED OR LIQUIDS TO BE REMOVED PRIOR TO IMPENDING STORMS TO PREVENT OVERFLOW.
6. IF EFFLUENT CANNOT BE REMOVED PRIOR TO ANTICIPATED RAINFALL EVENT, PLACE AND SECURE A NON-COLLAPSING, NON WATER COLLECTING COVER OVER THE WASHOUT FACILITY TO PREVENT ACCUMULATION AND PRECIPITATION OVERFLOW.
7. REMOVE WASHOUT WATER FROM HIGH VOLUME FACILITIES WITH A VACUUM TRUCK AND DISPOSE OF PROPERLY. DO NOT DISCHARGE WASTEWATER INTO THE ENVIRONMENT. (NOTE: ACIDITY, NOT PARTICULATES, IS ENVIRONMENTALLY HAZARDOUS)
8. DO NOT DISCHARGE WASHOUT WATER INTO THE ENVIRONMENT; FACILITATE EVAPORATION OF LOW VOLUME WASHOUT WATER.
9. INSPECT LINE FOR TEARS. AN INTACT LINER WILL ENSURE THAT CONCRETE WASTEWATER WILL NOT ESCAPE THE WASHOUT FACILITY.
10. REPLACE DAMAGED LINER IMMEDIATELY.
11. CHECK AREA SURROUNDING FACILITY FOR SIGNS OF EFFLUENT ESCAPING CONTAINMENT.
12. INSPECT WASHOUT AREA FOLLOWING POUR TO EVALUATE EFFECTIVENESS.
13. CHECK DEPTH OF SOLIDS TO ENSURE VOLUME IS SUFFICIENT FOR NEXT POUR.
14. INSPECT WASHOUTS PRIOR TO POUR TO ENSURE SUFFICIENT VOLUME IS AVAILABLE TO CONTAIN WASHOUT.
15. REMOVE TEMPORARY CONCRETE WASHOUT FACILITIES WHEN NO LONGER NEEDED AND RESTORE DISTURBED AREAS TO ORIGINAL CONDITION.
16. DISPOSE OF SOLIDIFIED CONCRETE WASTE, CONSIDERED CLEAN CONSTRUCTION OR DEMOLITION DEBRIS (CCDD) AS PER THE IEPA ACT (415 ILCS5).
17. COST OF TEMPORARY WASHOUT AREA IS INCIDENTAL TO PCC PAVING.

CONSTRUCT
T-HANGAR ACCESS
TAXIWAYS

IL. PROJ. No: VLA-4628

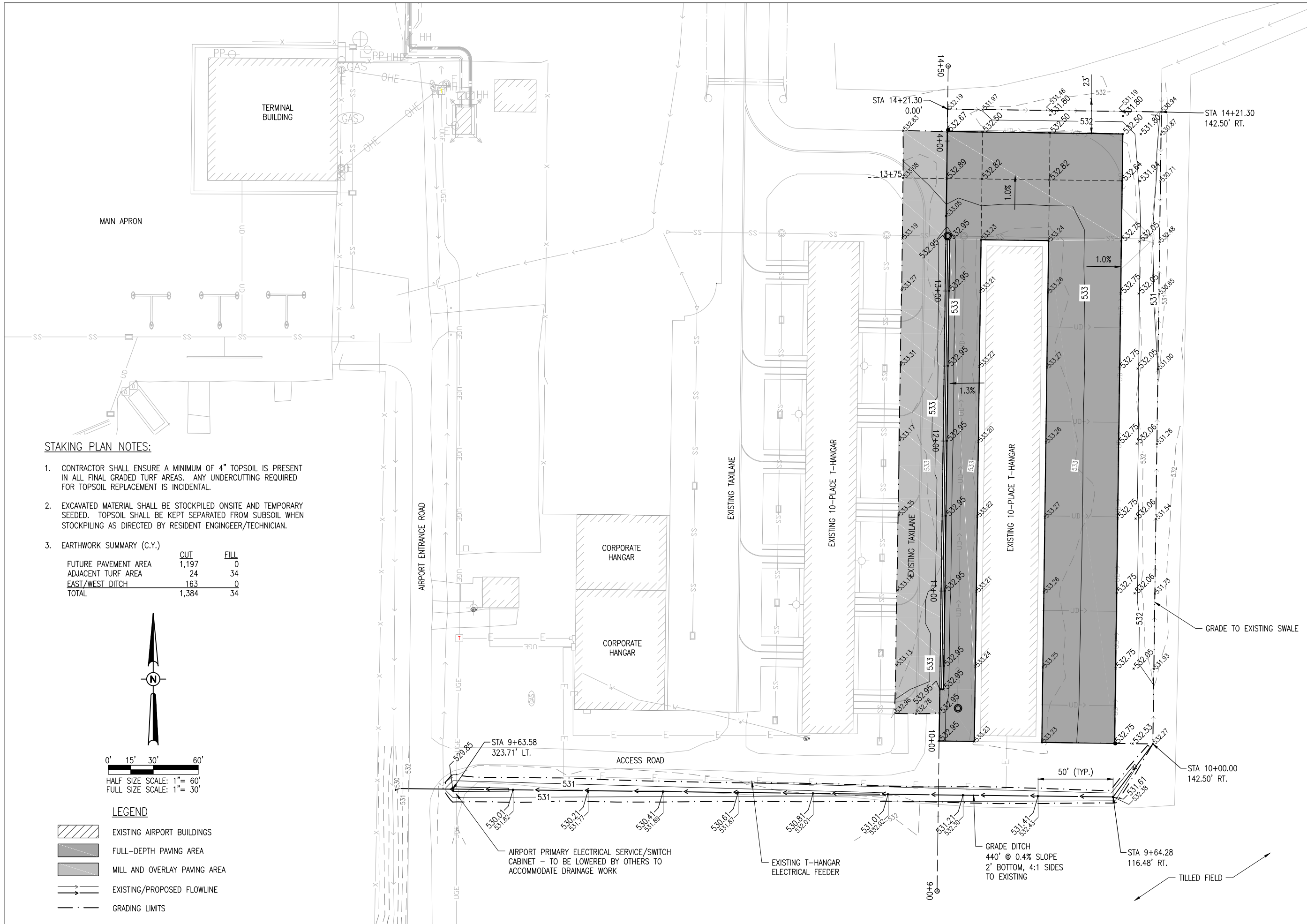
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NO.	DATE	DESCRIPTION		
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ISSUE: JAN. 12, 2018
PROJECT NO: 16A0042
CAD FILE: C-581-SWP.DWG
DESIGN BY: KBS
DRAWN BY: JAP
REVIEWED BY: RAW

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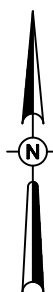
STORM WATER
POLLUTION
PREVENTION PLAN
DETAILS



STAKING PLAN NOTES:

1. CONTRACTOR SHALL ENSURE A MINIMUM OF 4" TOPSOIL IS PRESENT IN ALL FINAL GRADED TURF AREAS. ANY UNDERCUTTING REQUIRED FOR TOPSOIL REPLACEMENT IS INCIDENTAL.
2. EXCAVATED MATERIAL SHALL BE STOCKPILED ONSITE AND TEMPORARY SEEDED. TOPSOIL SHALL BE KEPT SEPARATED FROM SUBSOIL WHEN STOCKPILED AS DIRECTED BY RESIDENT ENGINEER/TECHNICIAN.
3. EARTHWORK SUMMARY (C.Y.)

	CUT	FILL
FUTURE PAVEMENT AREA	1,197	0
ADJACENT TURF AREA	24	34
EAST/WEST DITCH	163	0
TOTAL	1,384	34



0' 15' 30' 60'
HALF SIZE SCALE: 1" = 60'
FULL SIZE SCALE: 1" = 30'

LEGEND

- EXISTING AIRPORT BUILDINGS
- FULL-DEPTH PAVING AREA
- MILL AND OVERLAY PAVING AREA
- EXISTING/PROPOSED FLOWLINE
- GRADING LIMITS

**CONSTRUCT
T-HANGAR ACCESS
TAXIWAYS**

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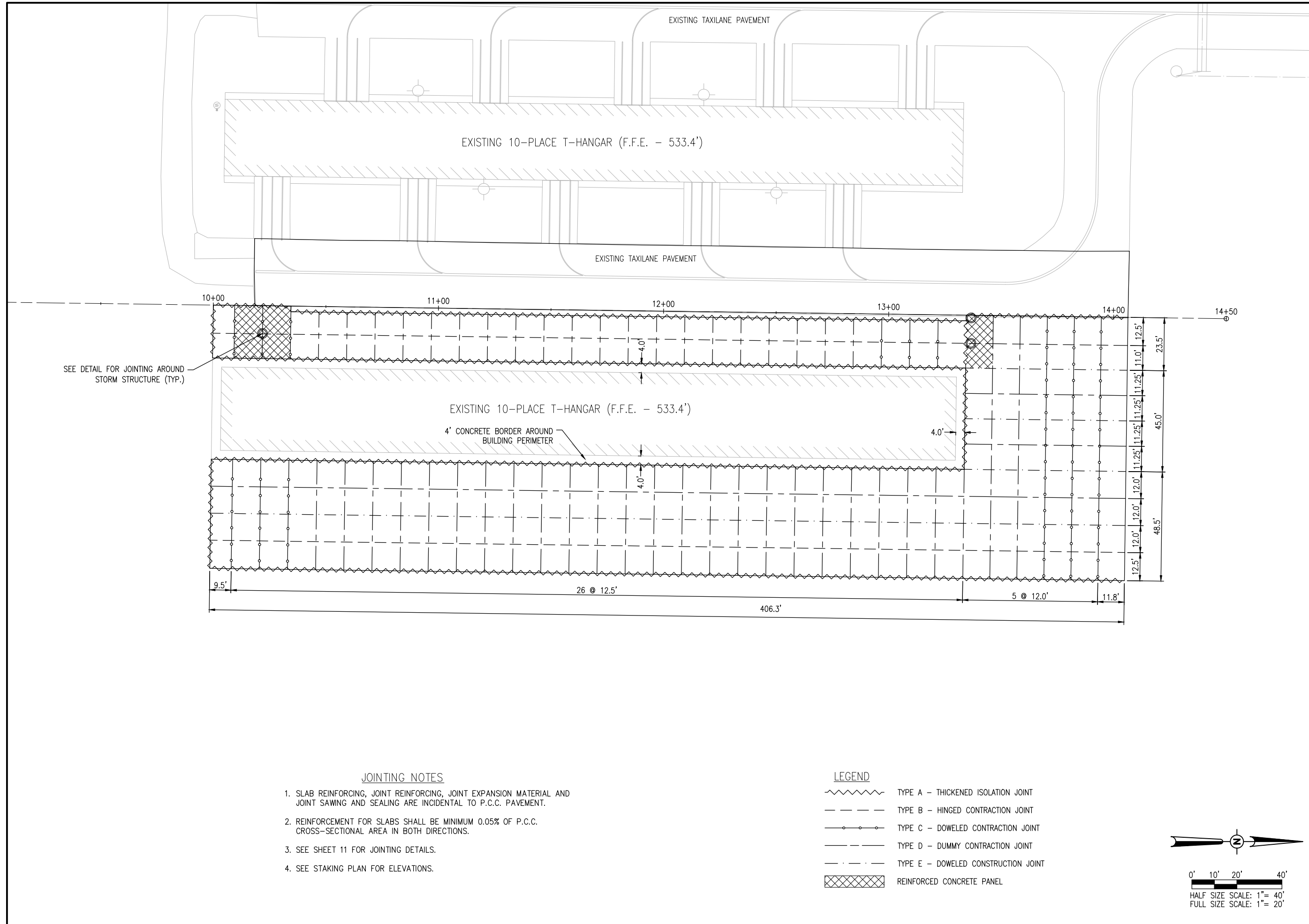
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STAKING PLAN



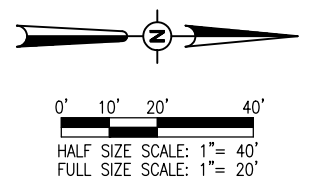
SEE DETAIL FOR JOINTING AROUND
STORM STRUCTURE (TYP.)

JOINTING NOTES

1. SLAB REINFORCING, JOINT REINFORCING, JOINT EXPANSION MATERIAL AND JOINT SAWING AND SEALING ARE INCIDENTAL TO P.C.C. PAVEMENT.
2. REINFORCEMENT FOR SLABS SHALL BE MINIMUM 0.05% OF P.C.C. CROSS-SECTIONAL AREA IN BOTH DIRECTIONS.
3. SEE SHEET 11 FOR JOINTING DETAILS.
4. SEE STAKING PLAN FOR ELEVATIONS.

LEGEND

- TYPE A - THICKENED ISOLATION JOINT
- TYPE B - HINGED CONTRACTION JOINT
- TYPE C - DOWELED CONTRACTION JOINT
- TYPE D - DUMMY CONTRACTION JOINT
- TYPE E - DOWELED CONSTRUCTION JOINT
- REINFORCED CONCRETE PANEL



**CONSTRUCT
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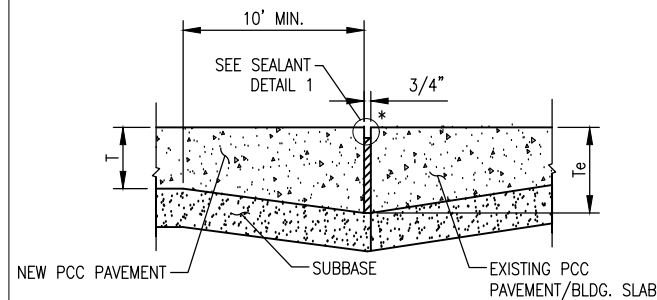
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DESIGN BY: KBS
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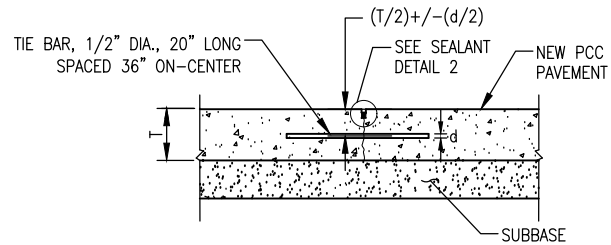
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**JOINTING PLAN
(ALT. B ONLY)**

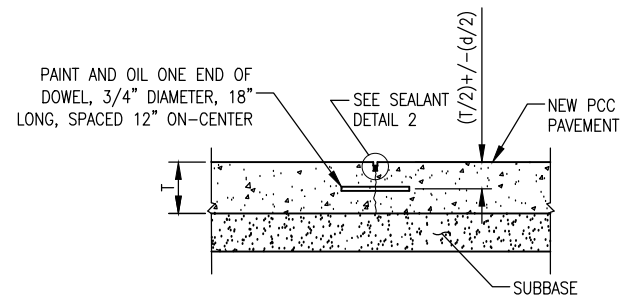


*EXPANSION MATERIAL NOT REQUIRED ADJACENT TO ASPHALT BUT AT LEAST T + 2".
Te = 1.25T TO THE NEAREST 1"

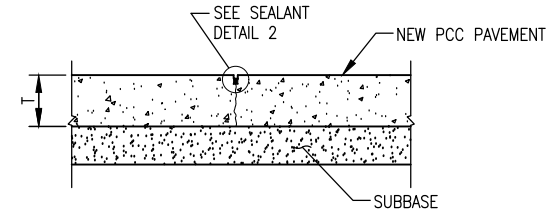
TYPE A - THICKENED EDGE ISOLATION JOINT



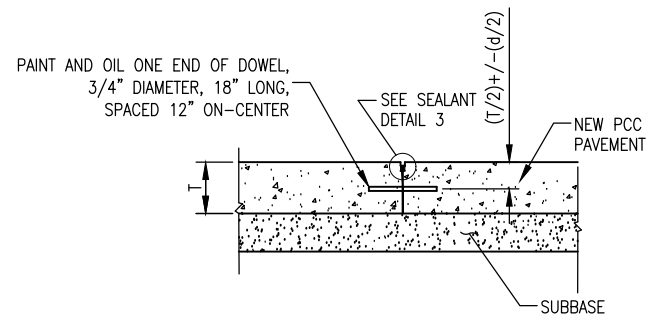
TYPE B - HINGED CONTRACTION JOINT



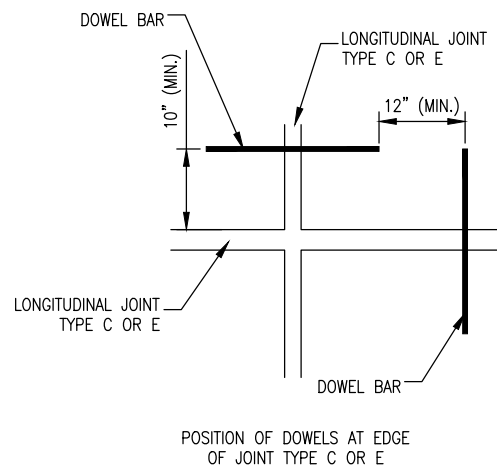
TYPE C - DOWELED CONTRACTION JOINT



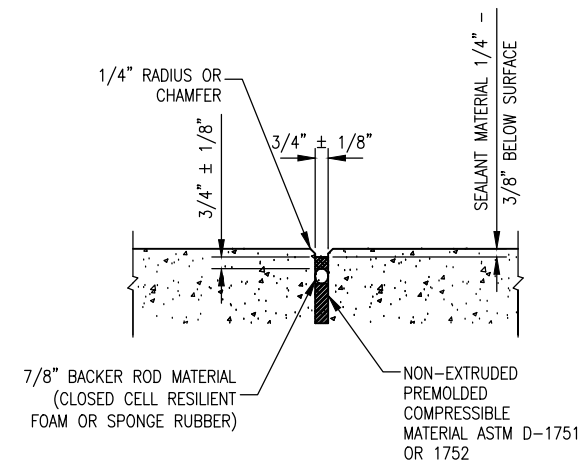
TYPE D - DUMMY CONTRACTION JOINT



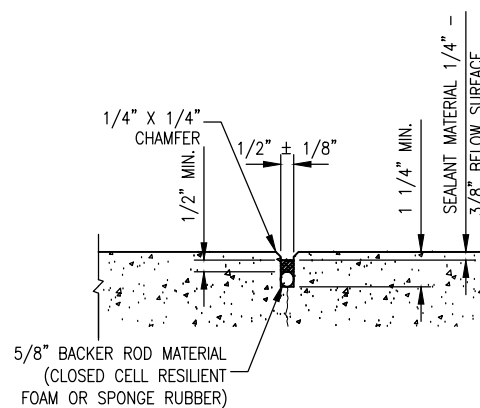
TYPE E - DOWELED CONSTRUCTION JOINT



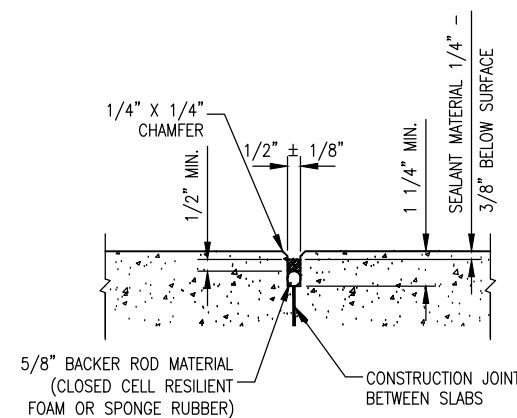
DOWEL PLAN VIEW



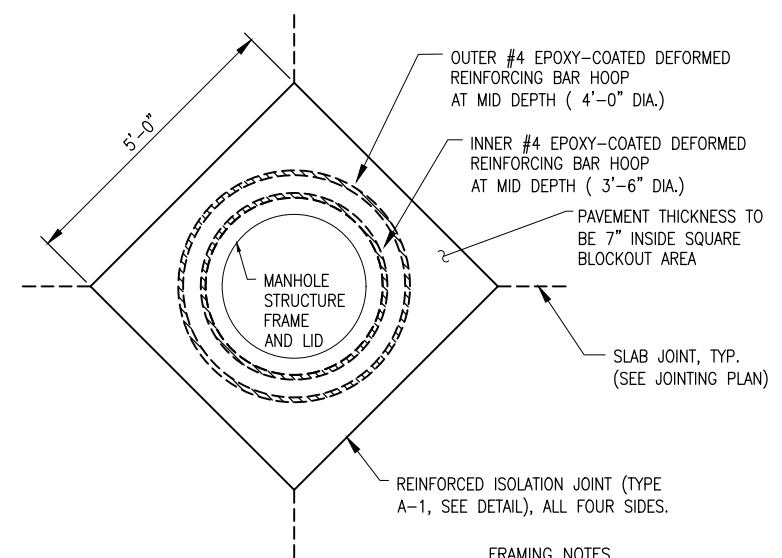
DETAIL 1 - SEALANT



DETAIL 2 - SEALANT



DETAIL 3 - SEALANT



JOINTING AND REINFORCING AT IN PAVEMENT MANHOLES

FRAMING NOTES

- HOOP REINFORCEMENT REQUIRED AND SHALL BE ONE PIECE CONSTRUCTION HAVING A MINIMUM LAP LENGTH OF 2'-0".

NOTES:

- ALL JOINT SEALING TO BE INCIDENTAL TO ITEM AR501505 - 5" PCC PAVEMENT.
- ALL JOINT SEALANT TO BE SILICONE.

CONSTRUCT
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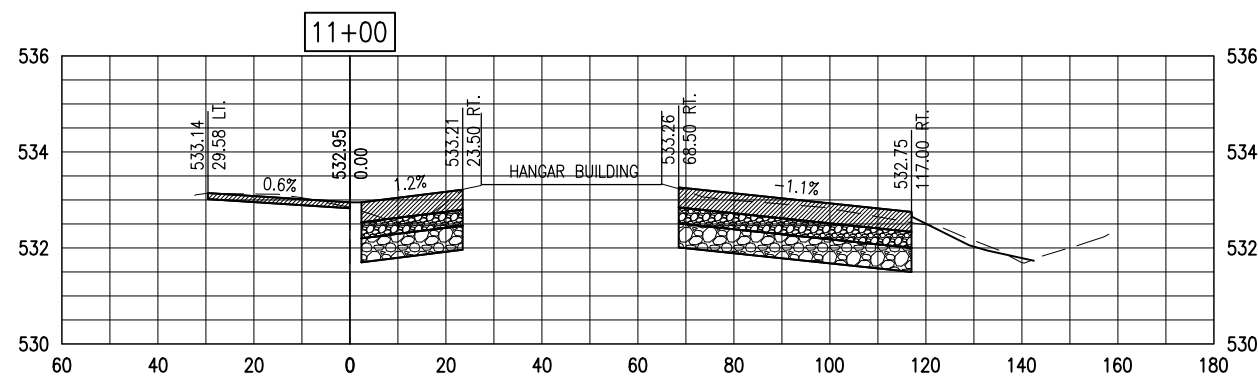
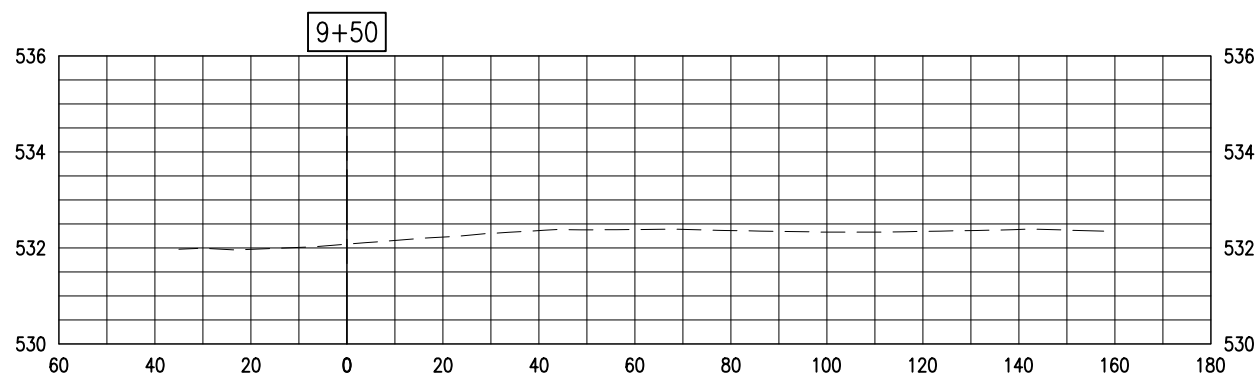
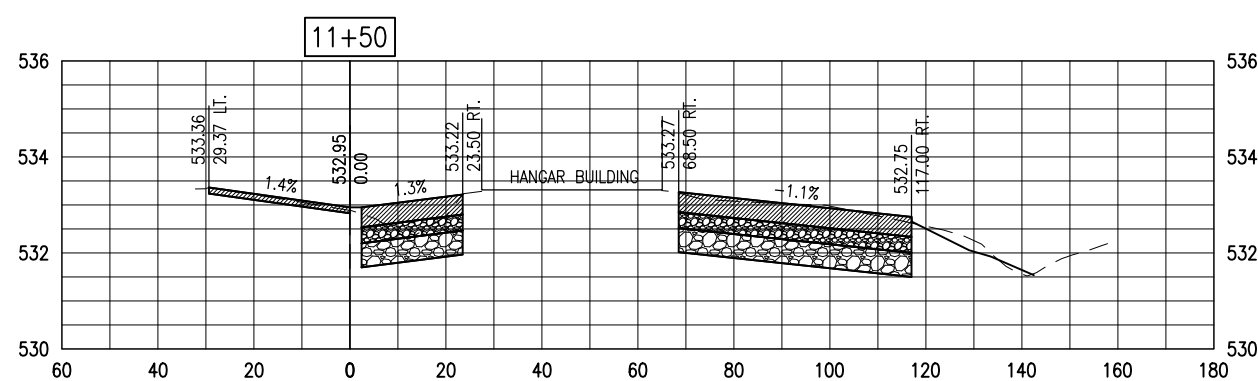
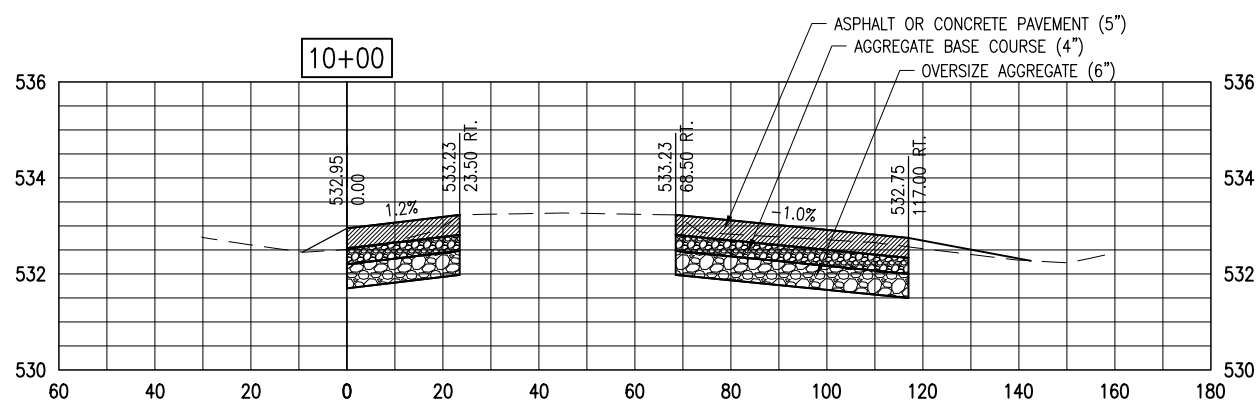
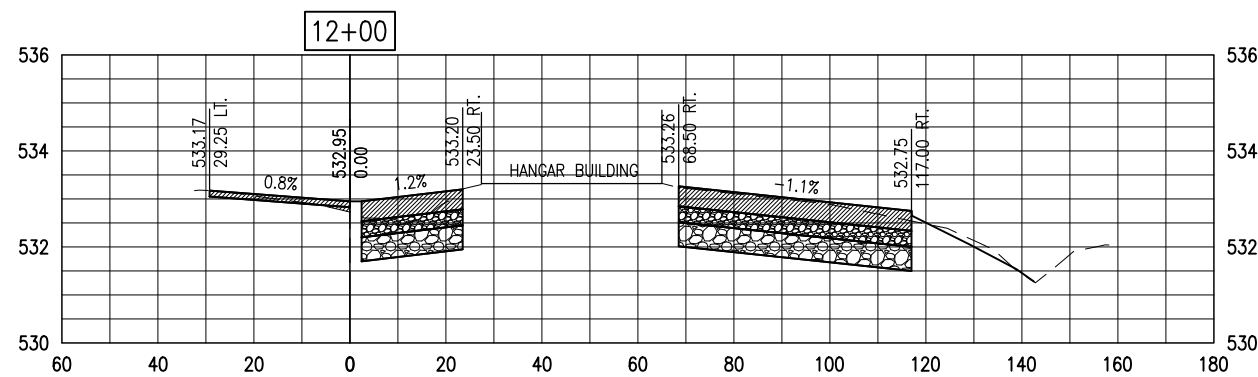
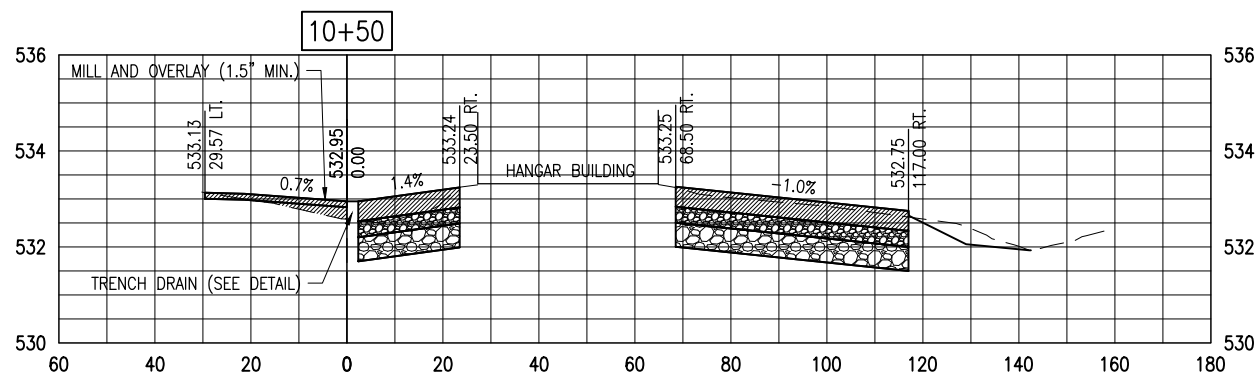
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JOINTING DETAILS
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CONSTRUCT
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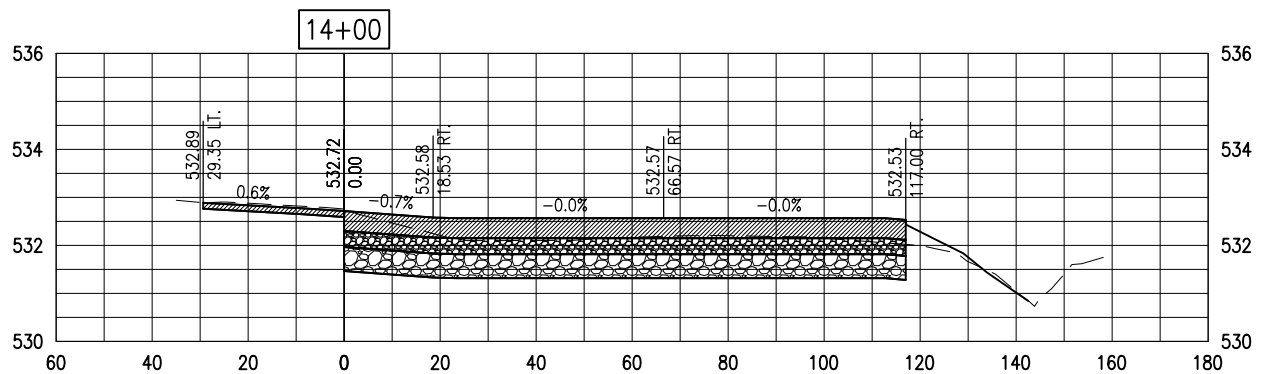
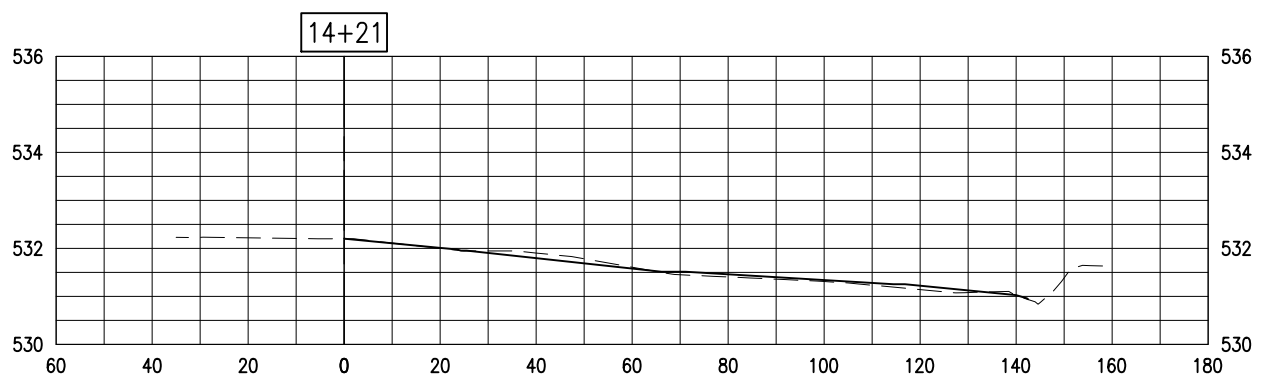
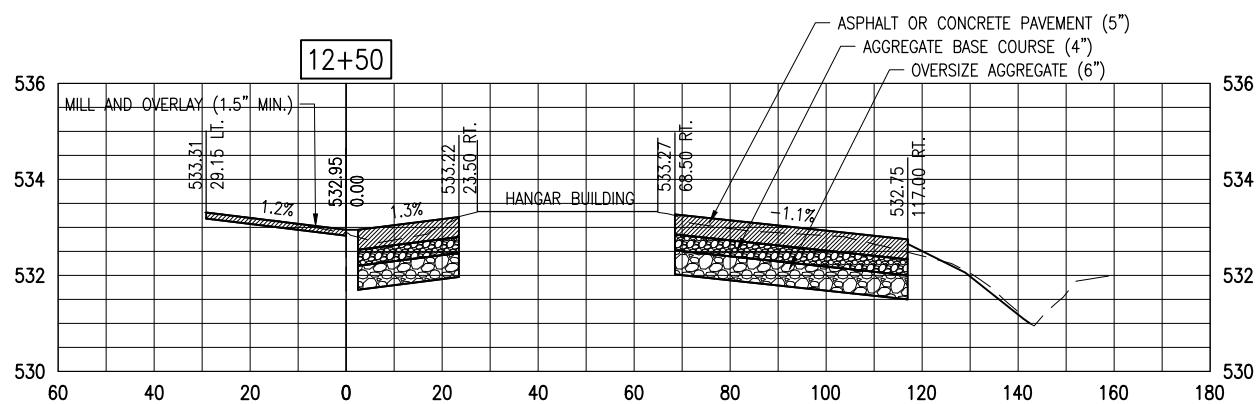
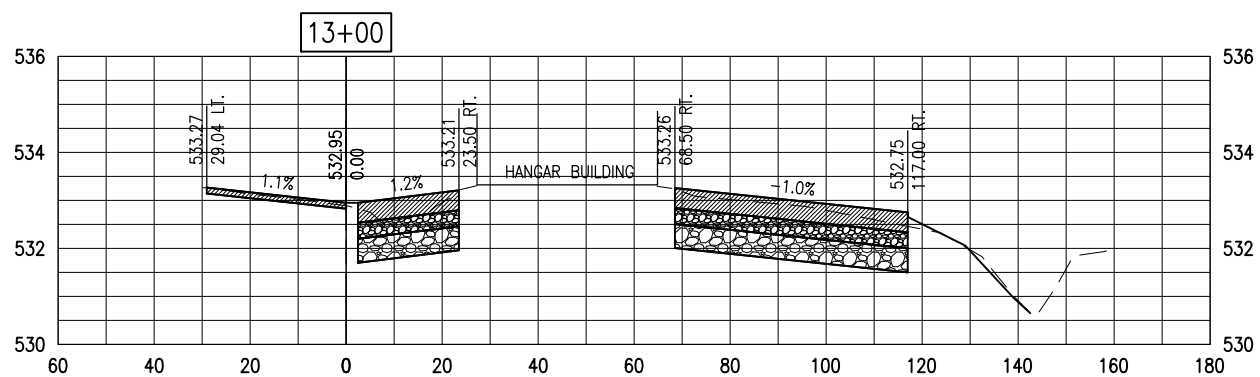
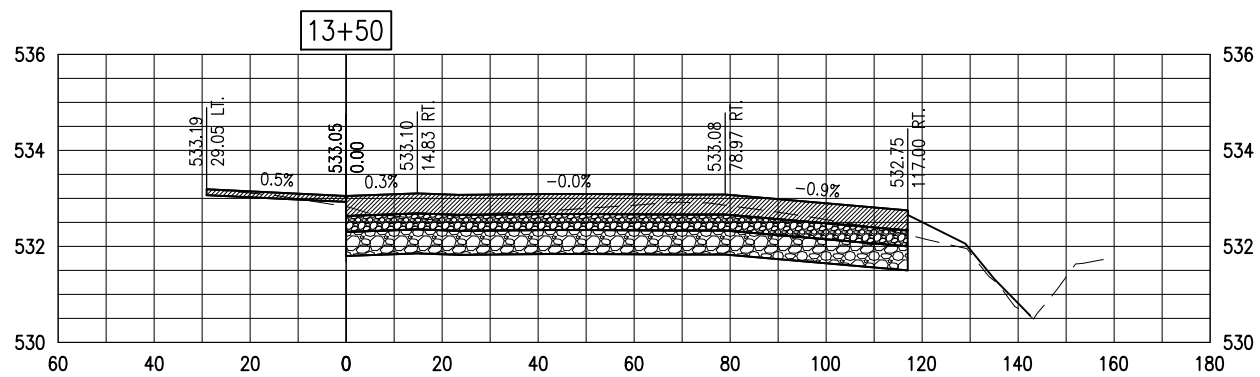
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CROSS SECTIONS
SHEET 1



CONSTRUCT
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TAXIWAYS

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CROSS SECTIONS
SHEET 2