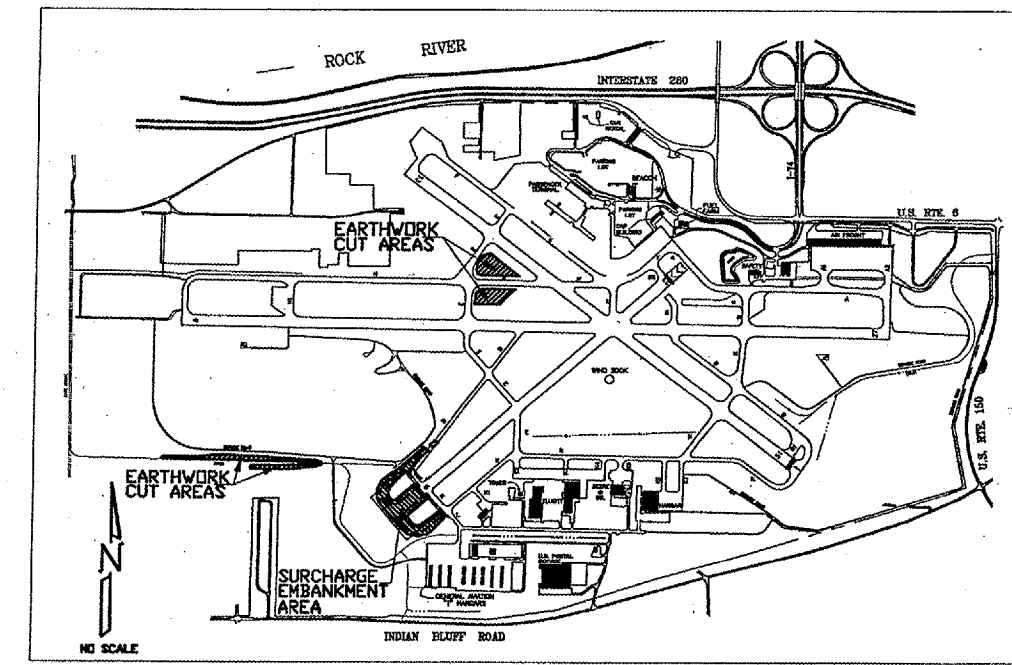
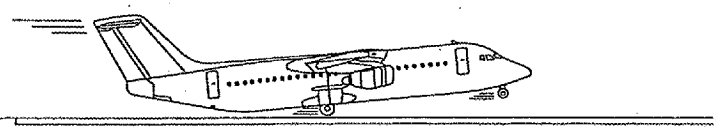




STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 DIVISION OF AERONAUTICS  
 CONSTRUCTION PLANS  
 FOR  
**QUAD-CITY  
 INTERNATIONAL  
 AIRPORT**

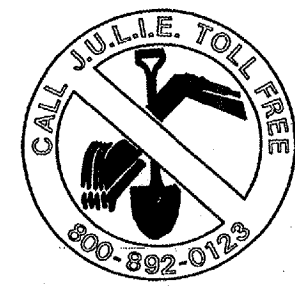


PROJECT LOCATION MAP

ROCK ISLAND COUNTY, ILLINOIS

EXCAVATION AND CONSTRUCTION OF 6' SURCHARGE EMBANKMENT FOR FUTURE EXTENSIONS TO RUNWAY 5 (500' X 150'), TAXIWAY D (745' X 50') AND TAXIWAY K (770' X 50') ALONG WITH ASSOCIATED UTILITY ADJUSTMENTS AND TURFING.

WARNING



CALL BEFORE YOU DIG

INDEX OF SHEETS	
SHEET NO.	TITLE
1	COVER SHEET AND INDEX OF SHEETS
2	SUMMARY OF QUANTITIES / GENERAL NOTES
3-4	PROPOSED SAFETY PLAN
5-6	TYPICAL SECTIONS
7	RUNWAY 5 AREA PLAN
8	RUNWAY 5 PLAN & PROFILE
9	TAXIWAY D PLAN & PROFILE
10	TAXIWAY K PLAN & PROFILE
11	TAXIWAY H BORROW AREA PLAN
12	DITCH BORROW AREA PLAN
13	RUNWAY 5 TEMPORARY RELOCATED THRESHOLD PLAN
14-15	LIGHTING & GUIDANCE SIGN DETAILS
16	MISCELLANEOUS DETAILS
17-18	PROPOSED STORM WATER POLLUTION PREVENTION PLAN
19-22	PROPOSED RUNWAY 5 CROSS SECTIONS
23-27	PROPOSED TAXIWAY D CROSS SECTIONS
28-31	PROPOSED TAXIWAY K CROSS SECTIONS
32-36	PROPOSED TAXIWAY H CROSS SECTIONS
37-49	PROPOSED DITCH CROSS SECTIONS

ILLINOIS PROJECT MLI-3496  
 A.I.P. PROJECT NO. 3-17-0068-XX  
 AIRPORT CLASSIFICATION - AIR CARRIER

AIRCRAFT APPROACH CATEGORY - B  
 AIRPLANE DESIGN GROUP - III  
 LATITUDE 41°26'52.4", LONGITUDE 90°30'33.9", ELEVATION 589'M.S.L.

<p>CONSULTING ENGINEERS                  MISSMAN, STANLEY &amp; ASSOC.</p> <p>ENGINEER'S SIGNATURE <i>Dennis R. Martin</i></p> <p>SEALED &amp; SIGNED 1/23/06</p> <p>DATE OF LICENSE EXPIRATION 11/30/07</p>	<p>METROPOLITAN AIRPORT AUTHORITY                  OF ROCK ISLAND COUNTY, ILLINOIS</p> <p><i>Bruce Carter</i></p> <p>1-23-06 DATE APPROVED</p>
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JANUARY 25, 2006  
 OFFICIAL DATE OF PLANS

PREPARED BY  
 MISSMAN, STANLEY & ASSOCIATES  
 Consulting Civil Engineers  
 ROCK ISLAND, ILLINOIS

## SUMMARY OF QUANTITIES

ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITIES	
			AS AWARDED	AS BUILT
AR108158	1/C #8 5KV UG CABLE IN UD	L.F.	670	
AR108258	2/C #8 5KV UG CABLE IN UD	L.F.	80	
AR125415	MIL - BASE MOUNTED	EACH	5	
AR125931	REPLACE LIGHT LENSE	EACH	4	
AR125944	ADJUST TAXI GUIDANCE SIGN	EACH	1	
AR125962	RELOCATE BASE MOUNTED LIGHT	EACH	6	
AR125964	RELOCATE TAXI GUIDANCE SIGN	EACH	1	
AR150512	ENGINEERS FIELD OFFICE (MONTHLY)	C.M.	8	
AR150530	TRAFFIC MAINTENANCE	L.S.	1	
AR151450	CLEARING AND GRUBBING	ACRE	2.1	
AR152410	UNCLASSIFIED EXCAVATION	C.Y.	48,330	
AR152440	BORROW EXCAVATION	C.Y.	14,165	
AR156500	TEMPORARY EROSION CONTROL	L.S.	1	
AR209511	CRUSHED AGGREGATE BASE, (CA1)	TON	100	
AR620510	PAVEMENT MARKING	S.F.	4,350	
AR620900	PAVEMENT MARKING REMOVAL	S.F.	12,100	
AR801604	SETTLEMENT PLATES	EACH	8	
AR801605	REPLACE TAXI GUIDANCE SIGN PANEL	EACH	7	
AR801614	SUPPLY TAXI GUIDANCE SIGN PANEL	EACH	20	
AR901510	SEEDING	ACRE	21	
AR908513	MULCHING - METHOD 3	ACRE	21	

**GENERAL NOTES:**

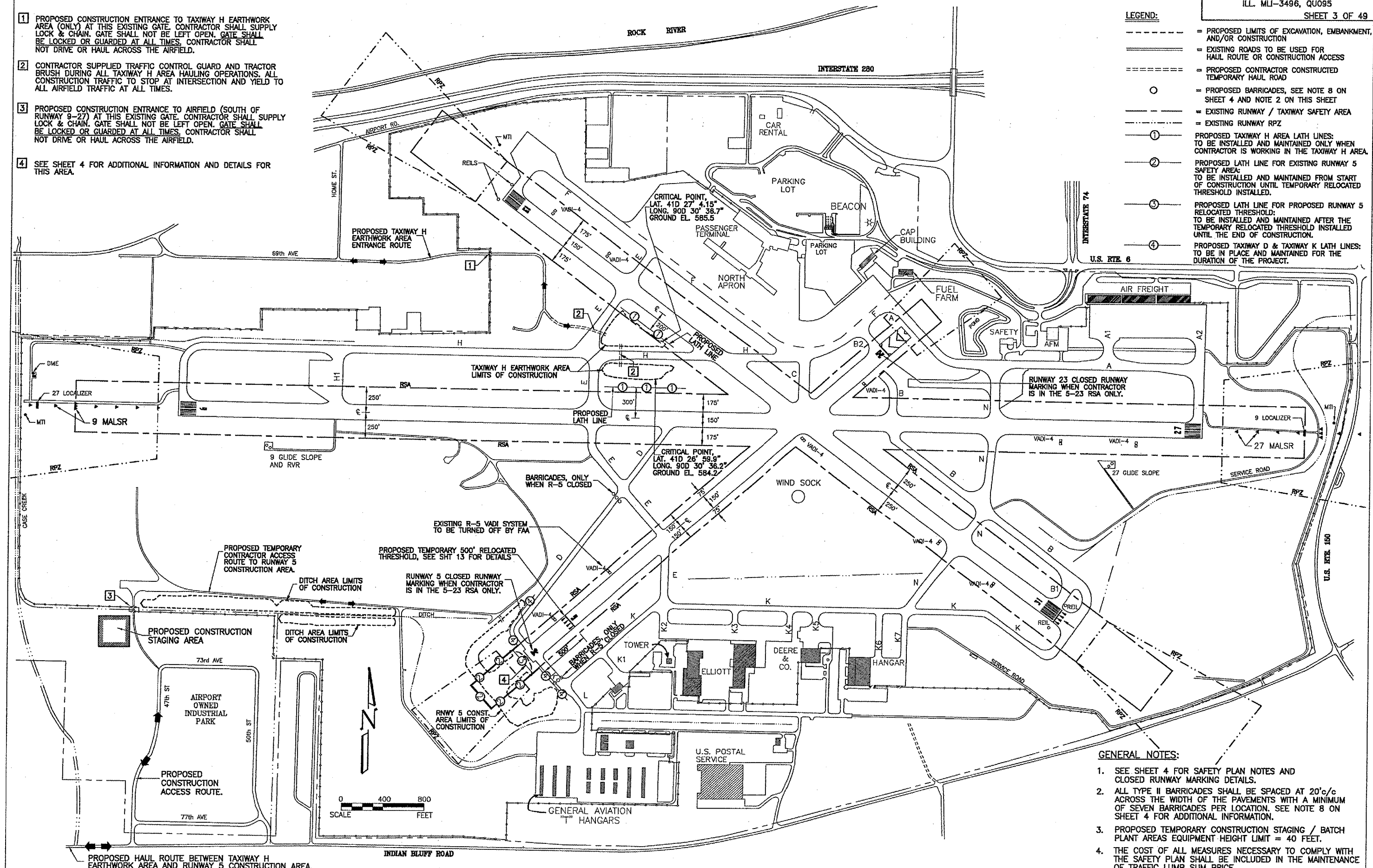
1. THE CONTRACTOR SHALL SALVAGE EXISTING AIRFIELD LIGHTING EQUIPMENT AS DETAILED IN THE CONSTRUCTION PLANS AND SPECIAL PROVISIONS PRIOR TO THE START OF EARTHWORK AND/OR PAVING ACTIVITIES. SALVAGED EQUIPMENT SHALL BE CLEANED AND REUSED OR DELIVERED TO THE METROPOLITAN AIRPORT AUTHORITY.
2. CROSS SECTION SLOPES, CENTERLINE PROFILE GRADES, AND ALL SPOT GRADES SHALL BE SUBJECT TO CHANGE, AS APPROVED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION.
3. THE CONTRACTOR SHALL EXCAVATE TEMPORARY EROSION CONTROL DRAINAGE SWALES, AS REQUIRED BY THE ENGINEER, TO CONTROL STORM WATER RUN-OFF.
4. THE CONTRACT 152 - EARTHWORK ITEMS SHALL INCLUDE ALL COSTS ASSOCIATED WITH EXCAVATION OF SOILS, HAULING OF SOILS, STOCKPILING SOILS, INSTALLATION OF SOILS, COMPACTION OF SOILS, GRADING OF SOILS, INSTALLATION AND REMOVAL OF HAUL ROADS OR ROUTES, RESTORATION OF HAUL ROADS OR ROUTES, DISPOSAL OF WASTE SOILS, CLEANING OF PAVEMENTS, AND ALL OTHER ITEMS THAT ARE REQUIRED TO COMPLETE THE EARTHWORK. THESE ITEMS SHALL BE PAID FOR BASED UPON THE CUBIC YARDS OF MATERIALS REMOVED AND/OR REQUIRED AS ACCEPTED BY THE ENGINEER. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
5. AFTER CONSTRUCTION HAS BEEN COMPLETED, THE CONTRACTOR SHALL SEED AND HYDRAULIC MULCH ALL DISTURBED AREAS PER SPECIAL PROVISIONS 901 AND 908. ONLY SEEDING AND MULCHING AREAS WITHIN THE LIMITS OF CONSTRUCTION/SEEDING WILL BE ELIGIBLE FOR PAYMENT UNDER THESE CONTRACT PAY ITEMS. AREAS OUTSIDE OF THE LIMITS OF CONSTRUCTION/SEEDING SHALL BE SEEDED AND MULCHED BY THE CONTRACTOR PER SPECIAL PROVISION 901/908, BUT SHALL NOT BE MEASURED FOR PAYMENT.
6. ITEM AR908513 MULCHING - METHOD 3 SHALL BE ACCOMPLISHED FOLLOWING THE METHODS AND PROCEDURES OUTLINED IN THE IDOT-DOA SUPPLEMENTAL SPECIFICATIONS AND IN THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION SECTION 251, METHOD 3 (HYDRAULIC MULCH).

**NUMBERED LEGEND:**

- 1 PROPOSED CONSTRUCTION ENTRANCE TO TAXIWAY H EARTHWORK AREA (ONLY) AT THIS EXISTING GATE. CONTRACTOR SHALL SUPPLY LOCK & CHAIN. GATE SHALL NOT BE LEFT OPEN. GATE SHALL BE LOCKED OR GUARDED AT ALL TIMES. CONTRACTOR SHALL NOT DRIVE OR HAUL ACROSS THE AIRFIELD.
- 2 CONTRACTOR SUPPLIED TRAFFIC CONTROL GUARD AND TRACTOR BRUSH DURING ALL TAXIWAY H AREA HAULING OPERATIONS. ALL CONSTRUCTION TRAFFIC TO STOP AT INTERSECTION AND YIELD TO ALL AIRFIELD TRAFFIC AT ALL TIMES.
- 3 PROPOSED CONSTRUCTION ENTRANCE TO AIRFIELD (SOUTH OF RUNWAY 9-27) AT THIS EXISTING GATE. CONTRACTOR SHALL SUPPLY LOCK & CHAIN. GATE SHALL NOT BE LEFT OPEN. GATE SHALL BE LOCKED OR GUARDED AT ALL TIMES. CONTRACTOR SHALL NOT DRIVE OR HAUL ACROSS THE AIRFIELD.
- 4 SEE SHEET 4 FOR ADDITIONAL INFORMATION AND DETAILS FOR THIS AREA.

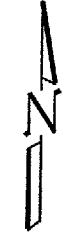
**LEGEND:**

- - - - - = PROPOSED LIMITS OF EXCAVATION, EMBANKMENT, AND/OR CONSTRUCTION
- ==== = EXISTING ROADS TO BE USED FOR HAUL ROUTE OR CONSTRUCTION ACCESS
- - - - - = PROPOSED CONTRACTOR CONSTRUCTED TEMPORARY HAUL ROAD
- = PROPOSED BARRICADES, SEE NOTE 8 ON SHEET 4 AND NOTE 2 ON THIS SHEET
- - - - - = EXISTING RUNWAY / TAXIWAY SAFETY AREA
- - - - - = EXISTING RUNWAY RPZ
- ① PROPOSED TAXIWAY H AREA LATH LINES: TO BE INSTALLED AND MAINTAINED ONLY WHEN CONTRACTOR IS WORKING IN THE TAXIWAY H AREA.
- ② PROPOSED LATH LINE FOR EXISTING RUNWAY 5 SAFETY AREA: TO BE INSTALLED AND MAINTAINED FROM START OF CONSTRUCTION UNTIL TEMPORARY RELOCATED THRESHOLD INSTALLED.
- ③ PROPOSED LATH LINE FOR PROPOSED RUNWAY 5 RELOCATED THRESHOLD: TO BE INSTALLED AND MAINTAINED AFTER THE TEMPORARY RELOCATED THRESHOLD INSTALLED UNTIL THE END OF CONSTRUCTION.
- ④ PROPOSED TAXIWAY D & TAXIWAY K LATH LINES: TO BE IN PLACE AND MAINTAINED FOR THE DURATION OF THE PROJECT.



**GENERAL NOTES:**

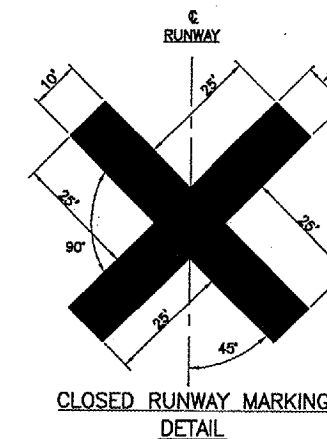
- 1. SEE SHEET 4 FOR SAFETY PLAN NOTES AND CLOSED RUNWAY MARKING DETAILS.
- 2. ALL TYPE II BARRICADES SHALL BE SPACED AT 20'c/c ACROSS THE WIDTH OF THE PAVEMENTS WITH A MINIMUM OF SEVEN BARRICADES PER LOCATION. SEE NOTE 8 ON SHEET 4 FOR ADDITIONAL INFORMATION.
- 3. PROPOSED TEMPORARY CONSTRUCTION STAGING / BATCH PLANT AREAS EQUIPMENT HEIGHT LIMIT = 40 FEET.
- 4. THE COST OF ALL MEASURES NECESSARY TO COMPLY WITH THE SAFETY PLAN SHALL BE INCLUDED IN THE MAINTENANCE OF TRAFFIC LUMP SUM PRICE.



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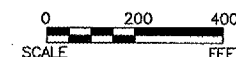
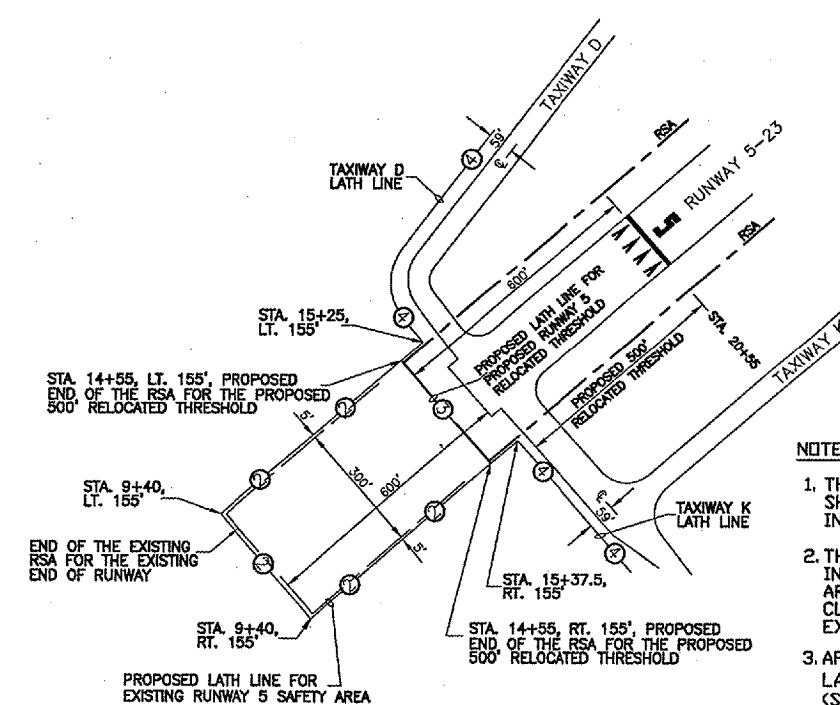
# SAFETY PLAN NOTES:

1. THE METROPOLITAN AIRPORT AUTHORITY (MAA) SHALL DETERMINE WHEN AND WHERE THE CONTRACTOR WILL BE ALLOWED TO WORK. THE CONTRACTOR SHALL BE PREPARED TO EXIT THE RESTRICTED AREAS, SAFETY AREAS AND/OR AIR FIELD AT ALL TIMES WHEN DIRECTED TO DO SO BY THE MAA. ALL PAVEMENTS SHALL BE OPEN TO AIRCRAFT TRAFFIC UNLESS "NOTAMED" OTHERWISE DURING THE CONSTRUCTION OF THIS PROJECT. SEE THE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION ON WORKING RESTRICTIONS AND CONDITIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLATION OF ALL BARRICADES AND LATH LINES AS SHOWN, PRIOR TO ANY EQUIPMENT ENTERING THE AIRFIELD OUTSIDE OF THE CONSTRUCTION STAGING AREA.
2. SOLID CLOSED RUNWAY MARKING CROSSES ARE REQUIRED AT EACH END OF THE RUNWAY DURING ALL RUNWAY CLOSURES. SEE SPECIAL PROVISIONS FOR DETAILS OF CROSSES.
3. THE CONTRACTOR SHALL NOT TRAVEL IN OR THROUGH THE RESTRICTED AREAS AND/OR SAFETY AREAS UNLESS PERMISSION IS RECEIVED AND CONTACT HAS BEEN MADE WITH THE FAA CONTROL TOWER.
4. THE CONTRACTOR'S EMPLOYEES SHALL PARK IN THE PROPOSED CONSTRUCTION STAGING AREA. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TRANSPORTING EMPLOYEES TO THE PROPOSED CONSTRUCTION SITE. ONLY CONTRACTOR'S MARKED VEHICLES AND EQUIPMENT SHALL BE ALLOWED ON THE AIRFIELD. ALL CONSTRUCTION VEHICLES AND EQUIPMENT, EXCEPT THE PAVING TRAIN, SHALL BE PARKED IN THE CONSTRUCTION STAGING AREA DURING ALL NON-WORKING HOUR. THE PAVING TRAIN MAY BE PARKED ON THE AIRFIELD OUTSIDE ALL RESTRICTED AREAS IN A LOCATION AUTHORIZED BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL LOCATE HIS TRAILER, THE ENGINEER'S FIELD OFFICE, AND ALL OTHER NECESSARY FACILITIES AND MATERIALS IN THE PROPOSED CONSTRUCTION STAGING AREA. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONSTRUCT WHATEVER ACCESS ROAD HE DEEMS NECESSARY BETWEEN THE EXISTING ROADS AND THE CONSTRUCTION AREAS. OVERNIGHT PARKING OF EMPLOYEE VEHICLES IN THE CONSTRUCTION STAGING AREA WILL NOT BE ALLOWED UNLESS AUTHORIZED BY THE MAA.
5. THE CONTRACTOR SHALL PROCURE ENOUGH QUAD CITY AIRPORT SECURITY/IDENTIFICATION BADGES FOR HIS EMPLOYEES AND SUBCONTRACTOR EMPLOYEES FROM THE AIRPORT AUTHORITY TO GUARANTEE AT LEAST ONE MEMBER OF EACH CONSTRUCTION CREW WILL HAVE A BADGE. ANY CONSTRUCTION CREW WITHOUT A BADGED MEMBER SHALL NOT BE ALLOWED ON THE AIRFIELD SITE. ALL INDIVIDUALS WHO ARE ISSUED SECURITY BADGES MUST CORRECTLY WEAR THEIR OWN BADGE WHILE ON THE AIRFIELD. BADGES MAY BE OBTAINED, AFTER MEETING SECURITY REQUIREMENTS, FROM THE SECURITY OFFICE AT THE QCI AIRPORT. A FIFTY DOLLAR (\$50.00) REFUNDABLE DEPOSIT IS REQUIRED FOR EACH BADGE.
6. THE CONTRACTOR SHALL USE THE DESIGNATED HAUL ROUTES, CONSTRUCTION STAGING AREA, AND ENTRANCE TO THE AIRFIELD AS SHOWN ON THE SAFETY PLAN SHEET. NO CONSTRUCTION TRAFFIC SHALL BE ALLOWED ON THE AIRFIELD OUTSIDE THE HAUL ROUTES AND WORK AREAS UNLESS OTHERWISE AUTHORIZED BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL SUPPLY AND INSTALL TEMPORARY LOCKS ON EXISTING GATES AT THE PROPOSED CONSTRUCTION ENTRANCE. THE CONTRACTOR SHALL BE RESPONSIBLE TO KEEP THE CONSTRUCTION ENTRANCE GATE CLOSED AT ALL TIMES. VIOLATIONS ARE SUBJECT TO FINES/PENALTIES AND THE CONTRACTOR SHALL PAY ANY FINES INCURRED, INCLUDING FINES INCURRED BY THE RESIDENT ENGINEER AND/OR MAA DUE TO THE CONTRACTOR'S NEGLIGENCE. ALL VEHICULAR TRAFFIC SHALL BE KEPT TO A MINIMUM. ALL VEHICLES ON THE APRONS, RAMPS, TAXIWAYS, OR RUNWAYS REQUIRE THE APPROVAL OF THE RESIDENT ENGINEER / MAA.
7. THE CONTRACTOR AND EACH SUBCONTRACTOR SHALL DESIGNATE, PRIOR TO BEGINNING CONSTRUCTION, A PERSON OR PERSONS WHO CAN BE CONTACTED IN AN EMERGENCY INVOLVING THEIR WORK OR EQUIPMENT. THESE DESIGNATED PEOPLE SHALL BE AVAILABLE ON A 24-HOUR / 7 DAYS PER WEEK BASIS.
8. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO INSTALL TYPE II BARRICADES EQUIPPED WITH FLASHING RED LIGHTS AND 20" X 20" ORANGE FLAGS AS SHOWN ON THE SAFETY PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR WILL FURNISH, MAINTAIN, AND MOVE THE BARRICADES AS REQUIRED BY THE RESIDENT ENGINEER. THE BARRICADES SHALL BE SUFFICIENTLY WEIGHTED WITH SANDBAGS OR OTHER APPROPRIATE METHOD TO WITHSTAND HIGH WINDS AND/OR JET BLAST WITHOUT DISLOCATION. BARRICADES SHALL BE CHECKED DAILY BY THE CONTRACTOR FOR PROPER PLACEMENT, ADEQUATE BALLAST, PROPER LIGHTING, PROPER FLAGGING AND WORKING BATTERIES. ALL DISCREPANCIES SHALL BE CORRECTED IMMEDIATELY. BARRICADES SHALL BE RELOCATED UPON THE COMPLETION OF EACH STAGE OF CONSTRUCTION. COST TO BE INCLUDED IN MAINTENANCE OF TRAFFIC LUMP SUM PRICE.
9. WHEN THE CONTRACTOR'S VEHICLES AND EQUIPMENT ARE ON THE AIRFIELD, THEY SHALL BE PROPERLY MARKED. THE MARKING SHALL CONSIST OF A THREE FOOT (3') SQUARE FLAG WITH A CHECKERED PATTERN OF INTERNATIONAL ORANGE AND WHITE SQUARES OF NOT LESS THAN ONE FOOT (1') ON EACH SIDE, DISPLAYED IN FULL VIEW ABOVE THE VEHICLE OR EQUIPMENT. EACH VEHICLE SHALL HAVE A FLASHING YELLOW LIGHT MOUNTED ON TOP OF THE ROOF.
10. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO RESTORE THE CONSTRUCTION STAGING AREA, SERVICE ROADS, ACCESS ROADS, AND HAUL ROADS TO THEIR ORIGINAL CONDITIONS FOLLOWING COMPLETION OF CONSTRUCTION. THIS MAY INCLUDE, BUT NOT BE LIMITED TO, REGRADING, FERTILIZING, SEEDING AND MULCHING OF EARTH SURFACES AND/OR REGRADING, GRAVELING & SEAL COATING OF TREATED SURFACES, AS REQUIRED, TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL GROUND SURFACES, GRAVEL ROADS, PAVEMENTS, AND OTHER FACILITIES DAMAGED BY THE CONTRACTOR WHILE COMPLETING THE PROPOSED WORK SHALL BE REPAIRED OR RETURNED TO ITS ORIGINAL STATE. COST TO BE INCLUDED IN THE MAINTENANCE OF TRAFFIC LUMP SUM PRICE.
11. THE CONTRACTOR SHALL IMMEDIATELY SWEEP OR PICK UP ANY SOIL, DEBRIS, AGGREGATE CHIPS OR ROCK, OR LOOSE MATERIALS WHICH HAS BEEN DROPPED ONTO AIRPORT ROADS, RUNWAYS, TAXIWAYS, OR SODDED AREAS.
12. THE DISPOSAL OF ALL MATERIALS NOT TO BE INCORPORATED IN EMBANKMENTS ON THE PROJECT SHALL BE ACCOMPLISHED BY THE CONTRACTOR AT A LOCATION OFF AIRPORT PROPERTY.
13. THE SEQUENCE OF CONSTRUCTION OPERATIONS AND DESCRIPTION OF CONDITIONS ARE OUTLINED IN THE PROJECT SPECIAL PROVISIONS. THE CONTRACTOR SHALL GIVE THE MAA A 96 HOUR NOTICE PRIOR TO THE START OF ANY WORK REQUIRING THE CLOSING OF ANY PAVEMENTS TO AIRCRAFT TRAFFIC SO THAT A NOTAM CAN BE ISSUED.
14. WORKING HOURS OF THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL CONFORM TO ALL APPLICABLE LOCAL LAWS, INCLUDING ANY NOISE CONTROL.
15. NO MOUNDS OF DIRT OR IRREGULARITIES GREATER THAN 3" WHICH, IN THE OPINION OF THE RESIDENT ENGINEER, COULD INTERFERE WITH ANY AIRFIELD OPERATIONS WILL BE PERMITTED ON THE AIRFIELD. NO EXPOSED FACES IN EXCESS OF ONE AND ONE-HALF (1-1/2) INCHES IN HEIGHT AND 2:1 SLOPES ON ANY EXCAVATION WILL BE PERMITTED WITHIN THE RESTRICTED AREAS.
16. DUST ABATEMENT MEASURES WILL BE REQUIRED, WHEN IN THE OPINION OF THE ENGINEER, A HAZARD TO AIR TRAFFIC, LOCAL RESIDENCES, OR CONSTRUCTION PROJECT PERSONNEL EXISTS. PREVENTIVE MEASURES TO BE ACCOMPLISHED BY THE CONTRACTOR SHALL INCLUDE, BUT NOT BE LIMITED TO, WATERING AND TREATMENT WITH CALCIUM CHLORIDE.
17. BY THE END OF EACH WORK DAY AND PRIOR TO LEAVING THE AIRFIELD, THE CONTRACTOR SHALL HAVE THOROUGHLY SWEEPED THE AIR TRAFFIC CORRIDORS ADJACENT TO THE WORK AREAS TO REMOVE DUST AND DEBRIS. IN ADDITION, ALL AIR TRAFFIC AREAS USED BY CONSTRUCTION PERSONNEL AND EQUIPMENT MUST BE CONTINUOUSLY SWEEPED AND MAINTAINED FREE OF DEBRIS. SWEEPERS SHALL BE PROVIDED BY THE CONTRACTOR FOR THE ENTIRE LENGTH OF THE CONTRACT AND SHALL BE OF A TYPE CAPABLE OF REMOVING ALL DUST AND DEBRIS TO THE SATISFACTION OF THE MAA. SWEEPERS MUST BE COMMERCIAL QUALITY AND APPROVED BY THE RESIDENT ENGINEER AND MAA PRIOR TO THE START OF CONSTRUCTION.
18. THE CONTRACTOR SHALL INSTALL AND MAINTAIN LATH LINES DURING THE LENGTH OF THE PROJECT AS SHOWN OR DIRECTED BY THE RESIDENT ENGINEER. SEE PROJECT SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
19. NO CHANGES SHALL BE MADE IN ANY PROVISIONS OF THIS SAFETY PLAN UNLESS APPROVED IN WRITING BY THE METROPOLITAN AIRPORT AUTHORITY, THE ILLINOIS DEPARTMENT OF TRANSPORTATION, DIVISION OF AERONAUTICS, AND THE FEDERAL AVIATION ADMINISTRATION. THE COST OF ALL MEASURES NECESSARY TO COMPLY WITH THE SAFETY PLAN SHALL BE INCLUDED IN THE MAINTENANCE OF TRAFFIC LUMP SUM PRICE.



**CLOSED RUNWAY MARKINGS NOTES:**

1. SOLID CROSS FOR CLOSED RUNWAY IS REQUIRED AT EACH END OF THE RUNWAY DURING ALL RUNWAY CLOSURES AND SHALL BE PLACED DIRECTLY OVER THE RUNWAY IDENTIFICATION NUMERALS.
2. THE CONTRACTOR SHALL INSTALL, REMOVE AND REINSTALL THE CROSSES AS REQUIRED BY WORKING CONDITIONS AND AS APPROVED BY THE RESIDENT ENGINEER.
3. COLOR OF ALL CROSSES SHALL BE AVIATION YELLOW.
4. SOLID CROSSES ARE TO BE CONSTRUCTED OF PLYWOOD, CANVAS, OR ANY OTHER APPROVED SOLID MATERIALS AND SHALL BE ADHERED TO THE RUNWAY IN A MANNER ACCEPTABLE TO THE RESIDENT ENGINEER. THE CONTRACTOR SHALL MAINTAIN THE CROSSES IN EXCELLENT CONDITION.



**NOTES:**

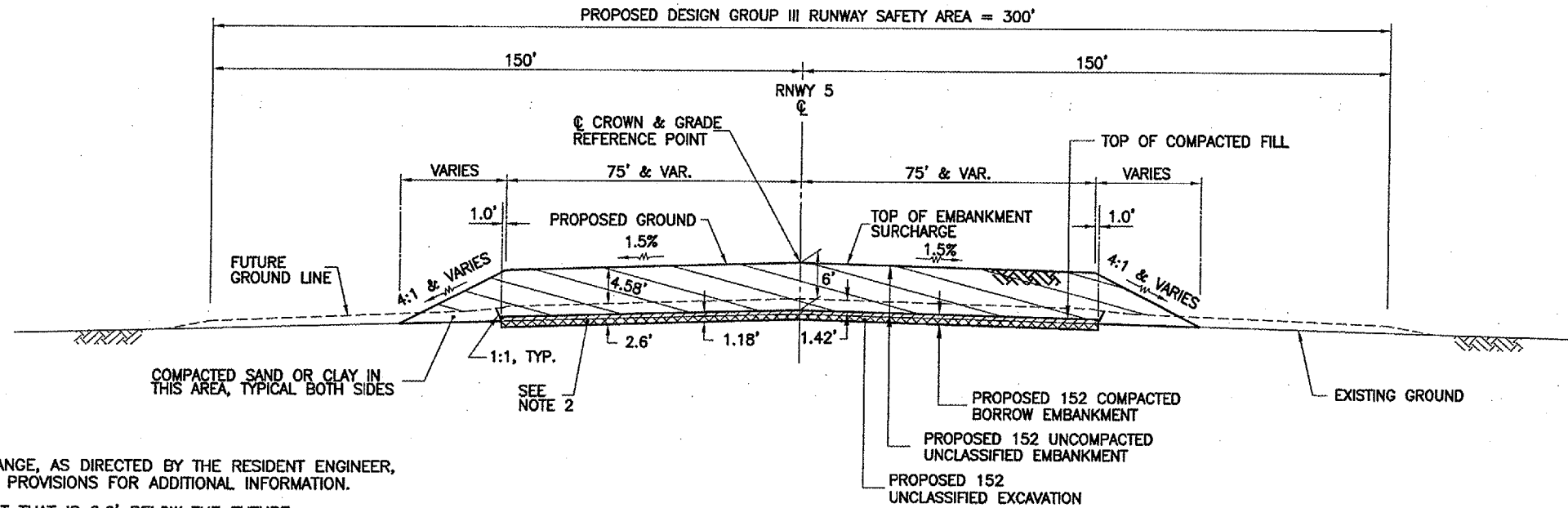
1. THE EXISTING RUNWAY 5 THRESHOLD SHALL BE RELOCATED PRIOR TO INSTALLING LATH LINE ③.
2. THE CONTRACTOR SHALL NOT WORK IN THE EXISTING RUNWAY 5 SAFETY AREA (UNLESS THE RUNWAY IS CLOSED) PRIOR TO RELOCATING THE EXISTING RUNWAY THRESHOLD.
3. AFTER LATH LINE ③ IS INSTALLED, LATH LINE ② MAY BE REMOVED (SOUTHWEST OF STA. 14+55).

**RUNWAY 5 AREA LATH LINE LAYOUT PLAN**

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AIRCRAFT APPROACH CATEGORY B  
 AIRPLANE DESIGN GROUP III

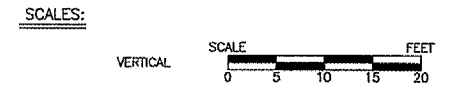
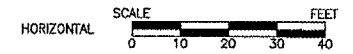
PROPOSED TYPICAL SECTION  
 RUNWAY 5-23, STA. 10+10 TO 15+25



- SOIL MATERIAL LEGEND:
- UNCOMPACTED SANDY OR CLAY MATERIALS
  - COMPACTED SANDY BORROW FILL
  - REMOVE EXTG. CLAY SOILS AND INSTALL COMPACTED SANDY BORROW MATERIALS
  - COMPACTED SANDY OR CLAY MATERIALS

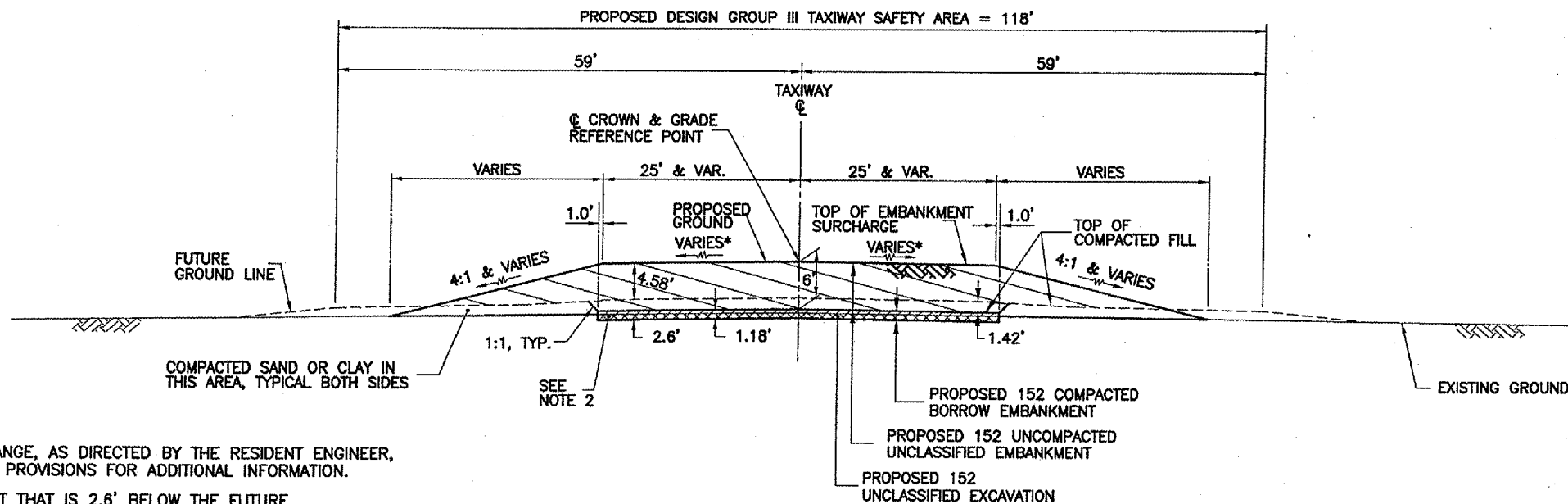
NOTES:

1. ALL CROSS SECTION SLOPES SUBJECT TO CHANGE, AS DIRECTED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
2. REMOVE THE EXISTING CLAY SOILS TO A POINT THAT IS 2.6' BELOW THE FUTURE PAVEMENT SURFACE. EXTEND THE REMOVAL AREA TO A POINT WHICH IS 1.0' OUTSIDE OF THE EDGE OF THE FUTURE PAVEMENT. AFTER THE COMPLETION OF THE REMOVAL AND PRIOR TO THE PLACEMENT OF UNCOMPACTED EMBANKMENT SOILS, INSTALL COMPACTED BORROW SAND MATERIALS TO A POINT THAT IS 1.42' BELOW THE FUTURE PAVEMENT SURFACE. THE CLAY SOIL MATERIALS THAT ARE REMOVED SHALL BE INCORPORATED INTO THE SHOULDER FILL AREAS. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.



PROPOSED TYPICAL SECTION  
 TAXIWAY D, STA. 107+41.88 TO 15+25 &  
 TAXIWAY K, STA. 207+16.88 TO 215+37

AIRCRAFT APPROACH CATEGORY B  
 AIRPLANE DESIGN GROUP III

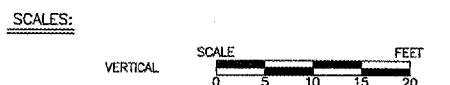
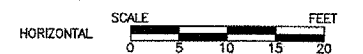


- SOIL MATERIAL LEGEND:
- UNCOMPACTED SANDY OR CLAY MATERIALS
  - COMPACTED SANDY BORROW FILL
  - REMOVE EXTG. CLAY SOILS AND INSTALL COMPACTED SANDY BORROW MATERIALS
  - COMPACTED SANDY OR CLAY MATERIALS

NOTES:

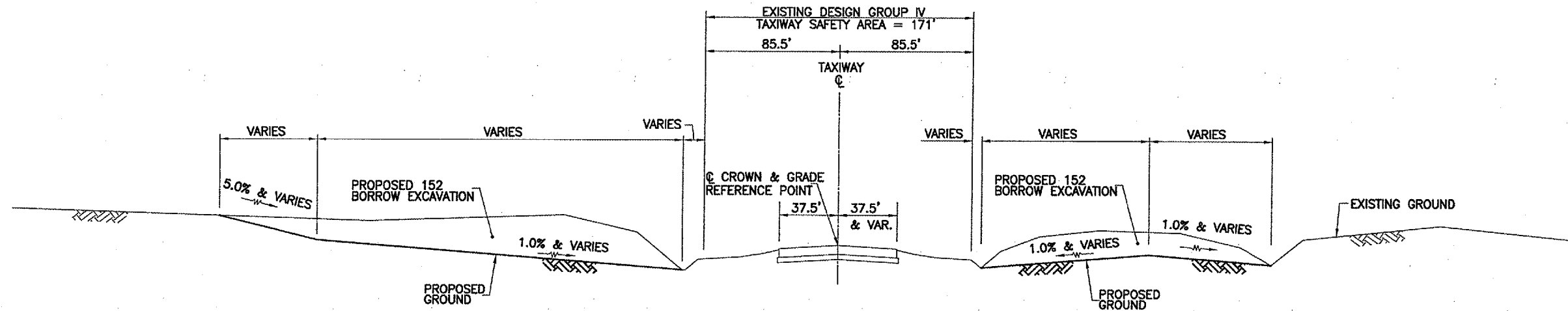
1. ALL CROSS SECTION SLOPES SUBJECT TO CHANGE, AS DIRECTED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
2. REMOVE THE EXISTING CLAY SOILS TO A POINT THAT IS 2.6' BELOW THE FUTURE PAVEMENT SURFACE. EXTEND THE REMOVAL AREA TO A POINT WHICH IS 1.0' OUTSIDE OF THE EDGE OF THE FUTURE PAVEMENT. AFTER THE COMPLETION OF THE REMOVAL AND PRIOR TO THE PLACEMENT OF UNCOMPACTED EMBANKMENT SOILS, INSTALL COMPACTED BORROW SAND MATERIALS TO A POINT THAT IS 1.42' BELOW THE FUTURE PAVEMENT SURFACE. THE CLAY SOIL MATERIALS THAT ARE REMOVED SHALL BE INCORPORATED INTO THE SHOULDER FILL AREAS. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

\* = 1.5% & VARIES FOR TAXIWAY D  
 1.0% & VARIES FOR TAXIWAY K



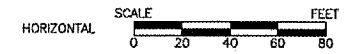
AIRCRAFT APPROACH CATEGORY D  
 AIRPLANE DESIGN GROUP IV

PROPOSED TYPICAL SECTION  
 TAXIWAY H, STA. 430+00 TO 437+00

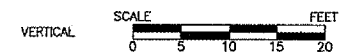


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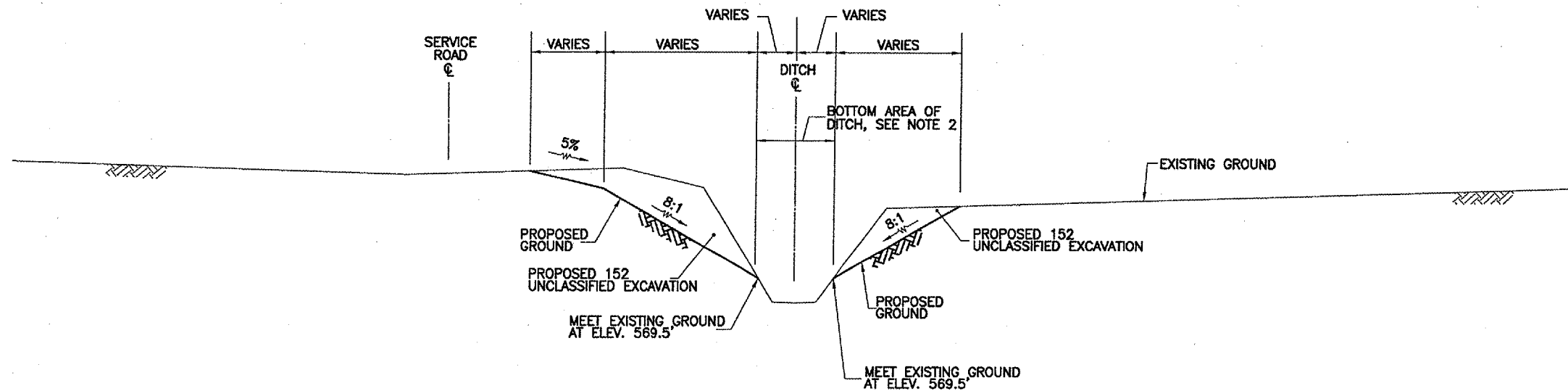
1. ALL CROSS SECTION SLOPES SUBJECT TO CHANGE, AS DIRECTED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.



SCALES:

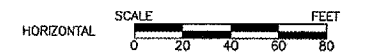


PROPOSED TYPICAL SECTION  
 EXISTING DITCH, STA. 180+00 TO 206+00



NOTES:

1. ALL CROSS SECTION SLOPES SUBJECT TO CHANGE, AS DIRECTED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
2. DO NOT DISTURB THE BOTTOM AREA OF THE EXISTING DITCH.



SCALES:





QUAD CITY INTERNATIONAL AIRPORT  
 RUNWAY 5 EXTENSION - EARTHWORK  
 ILL. MLI-3496, QU095  
 SHEET 7 OF 49

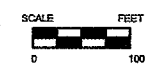
LEGEND:

- EDGE LIGHT
- GUIDANCE SIGN
- ELECTRICAL CABLE
- STORM SEWER OR UNDERDRAIN
- INLET
- OUTLET
- STORM MANHOLE
- SANITARY MANHOLE
- DRAINAGE DITCH
- CONDUIT OR DUCT BANK
- FUTURE PAVEMENT
- PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS

NOTE:

SEE PLAN & PROFILE SHEETS FOR LOCATION OF EXISTING UNDERGROUND ELECTRICAL AND FIELD TILES.

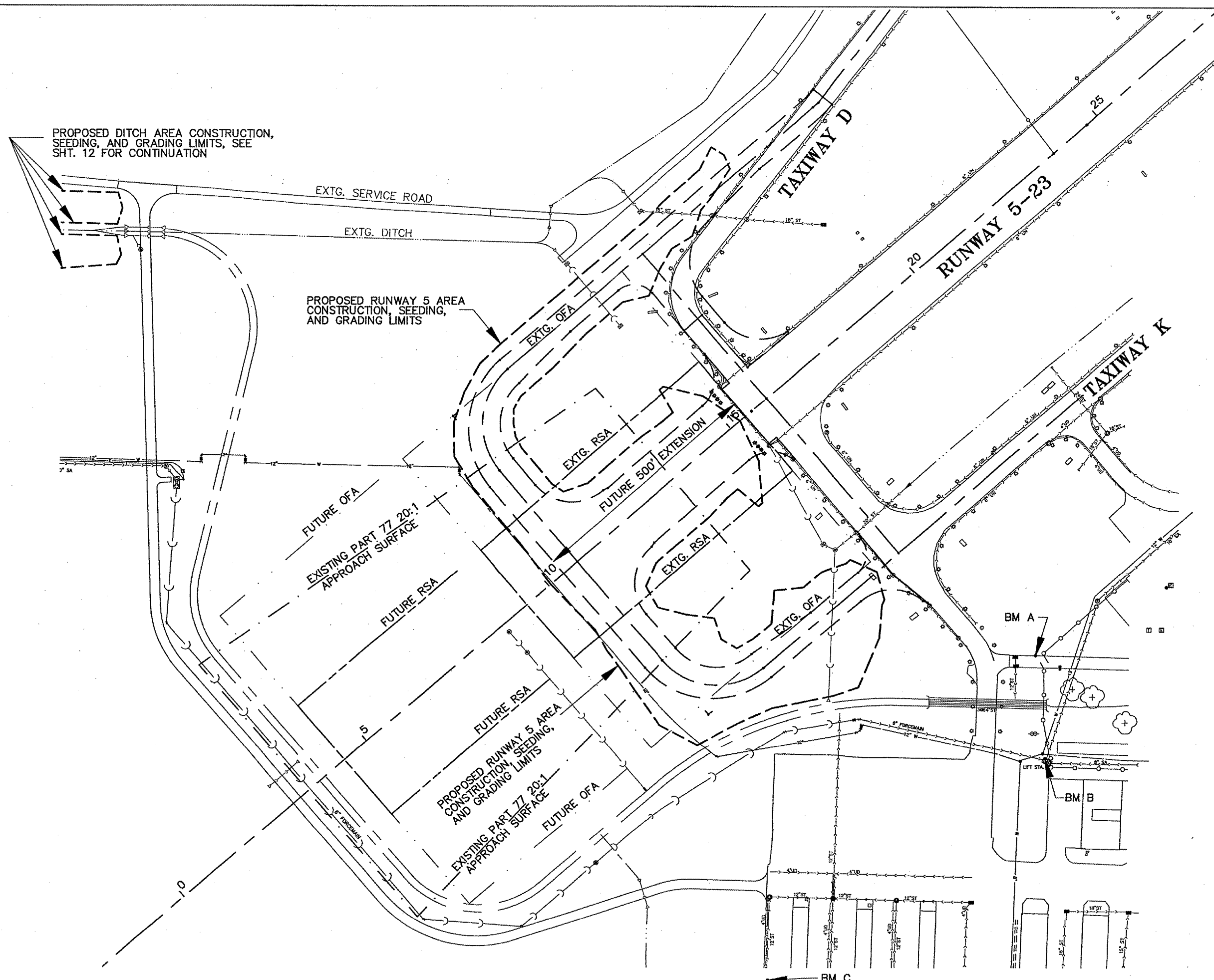
TEMPORARY BENCH MARKS		
B.M.	ELEV.	DESCRIPTION
BM 20	580.91	BRASS CAP @ N.W. COR. OF FAA CONTROL TOWER
BM A	577.63	CHIS. □ ON TOP OF N. CURB, STA. 16+75', RT. 775'
BM B	576.73	CHIS. X ON N RIM OF LIFT STA M.H., STA. 15+45', RT. 958'
BM C	579.90	CHIS. □ ON S COR. OF CATCH BASIN, STA. 131+30', RT. 195'



PROPOSED DITCH AREA CONSTRUCTION, SEEDING, AND GRADING LIMITS, SEE SHT. 12 FOR CONTINUATION

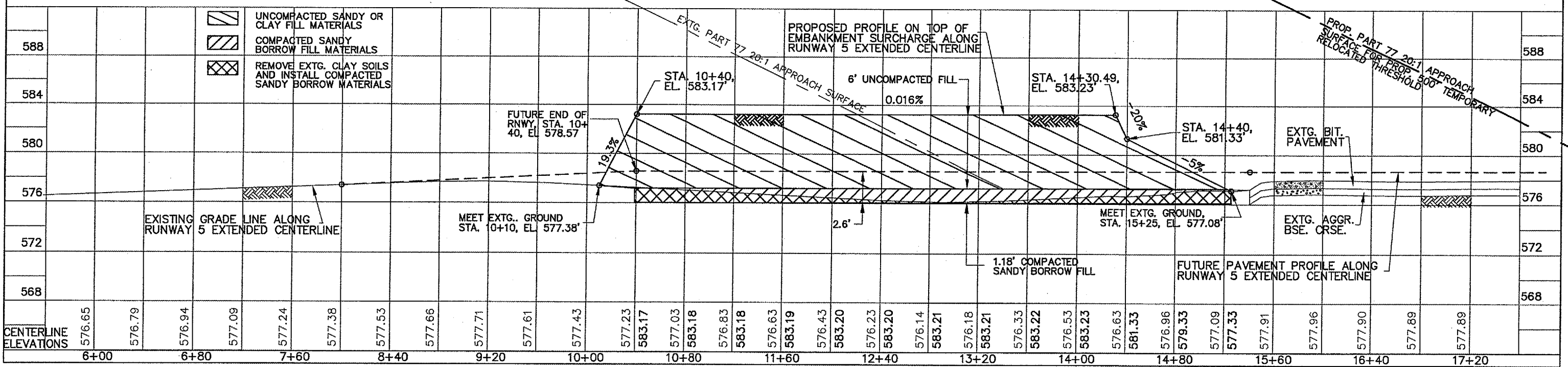
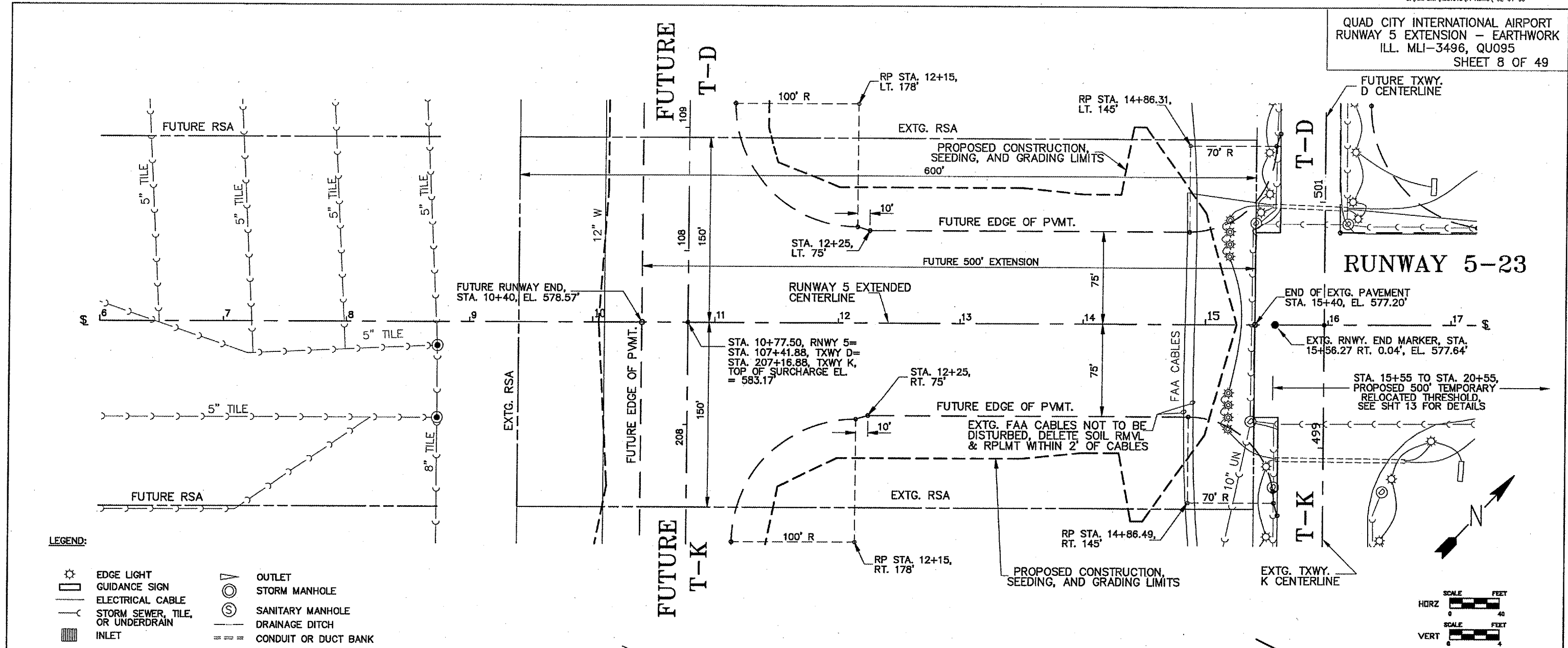
PROPOSED RUNWAY 5 AREA CONSTRUCTION, SEEDING, AND GRADING LIMITS

PROPOSED RUNWAY 5 AREA CONSTRUCTION, SEEDING, AND GRADING LIMITS



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QUAD CITY INTERNATIONAL AIRPORT  
 RUNWAY 5 EXTENSION - EARTHWORK  
 ILL. MLI-3496, QU095  
 SHEET 8 OF 49



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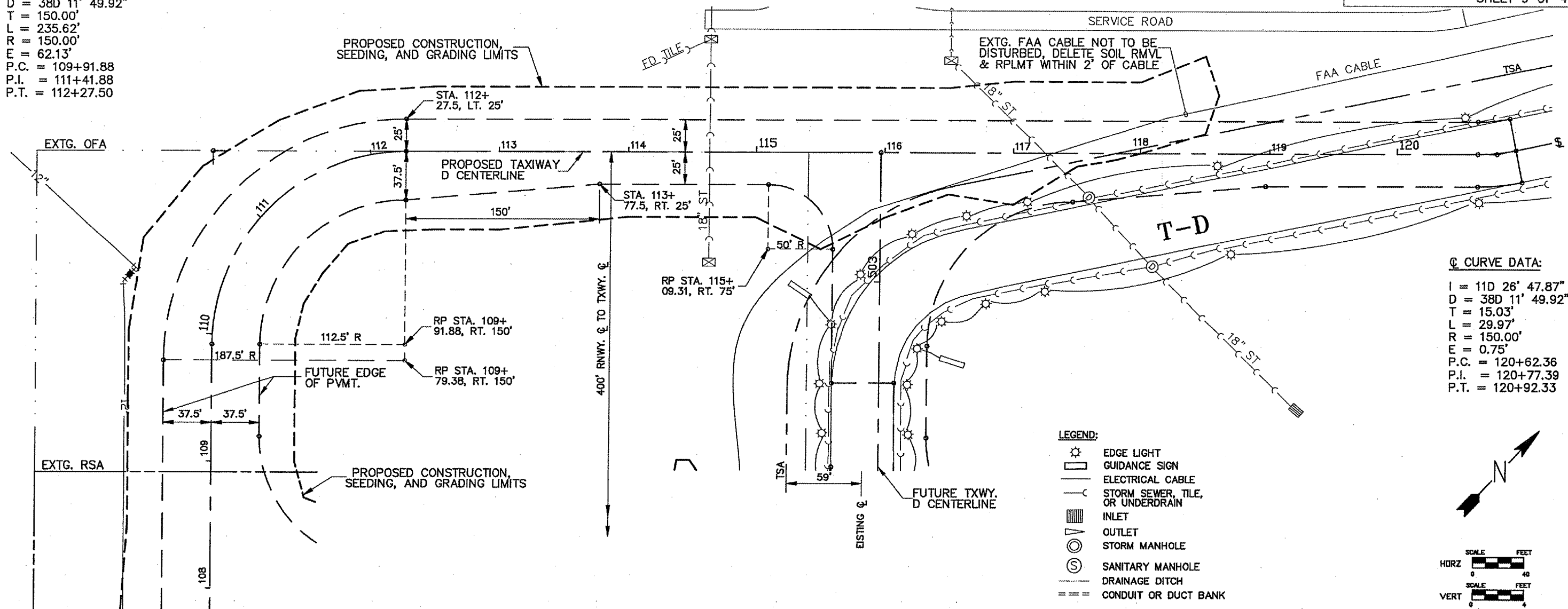


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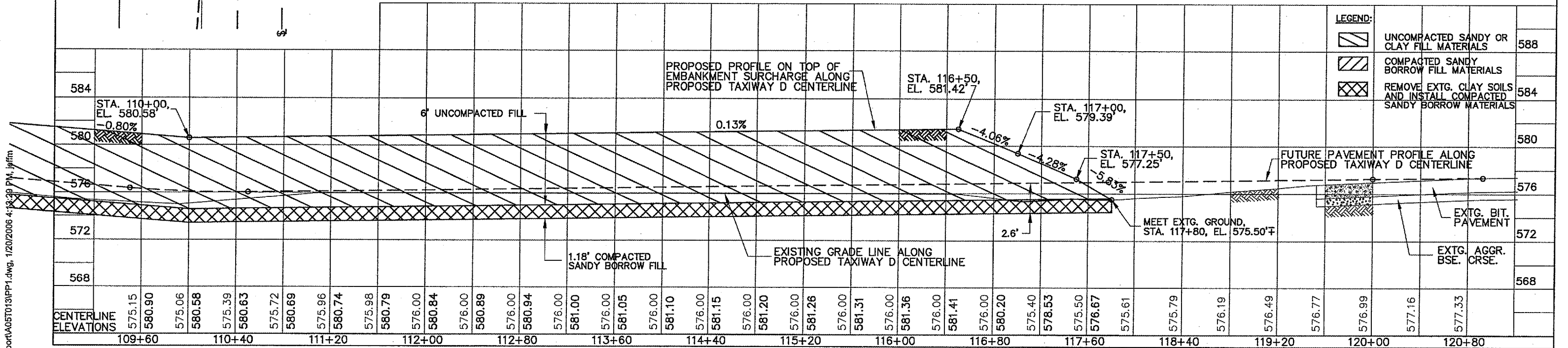
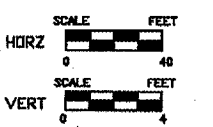
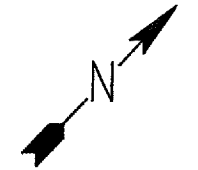
I = 90D 00' 00"  
 D = 38D 11' 49.92"  
 T = 150.00'  
 L = 235.62'  
 R = 150.00'  
 E = 62.13'  
 P.C. = 109+91.88  
 P.I. = 111+41.88  
 P.T. = 112+27.50

**☉ CURVE DATA:**

I = 11D 26' 47.87"  
 D = 38D 11' 49.92"  
 T = 15.03'  
 L = 29.97'  
 R = 150.00'  
 E = 0.75'  
 P.C. = 120+62.36  
 P.I. = 120+77.39  
 P.T. = 120+92.33



- LEGEND:**
- ☉ EDGE LIGHT
  - GUIDANCE SIGN
  - ELECTRICAL CABLE
  - STORM SEWER, TILE, OR UNDERDRAIN
  - ▭ INLET
  - ▽ OUTLET
  - ⊙ STORM MANHOLE
  - ⊙ SANITARY MANHOLE
  - DRAINAGE DITCH
  - === CONDUIT OR DUCT BANK



- LEGEND:**
- ▨ UNCOMPACTED SANDY OR CLAY FILL MATERIALS
  - ▩ COMPACTED SANDY BORROW FILL MATERIALS
  - ▣ REMOVE EXTG. CLAY SOILS AND INSTALL COMPACTED SANDY BORROW MATERIALS

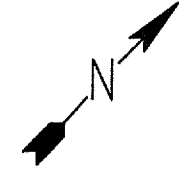
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QUAD CITY INTERNATIONAL AIRPORT  
 RUNWAY 5 EXTENSION - EARTHWORK  
 ILL. MLI-3496, QU095  
 SHEET 10 OF 49

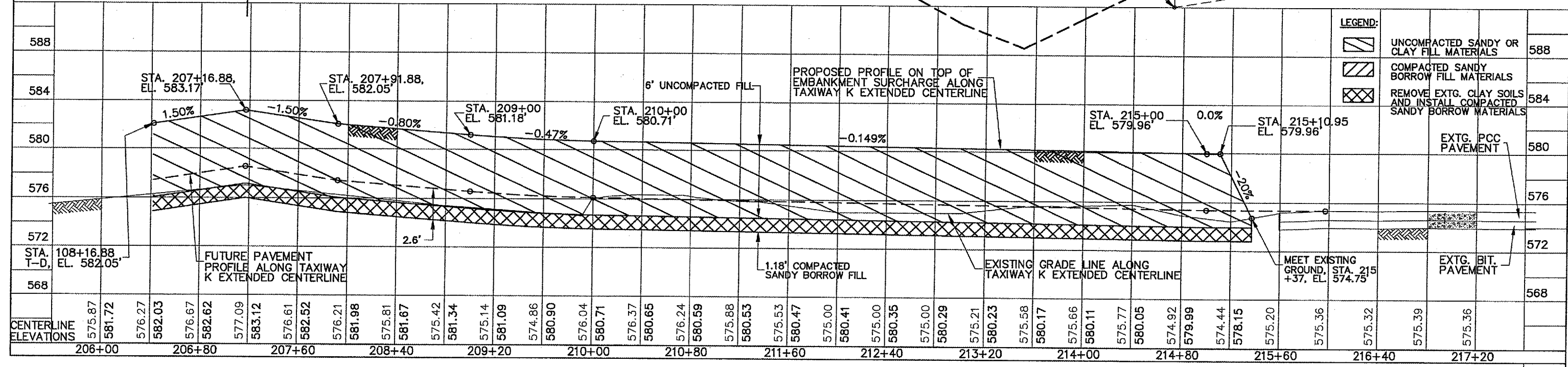
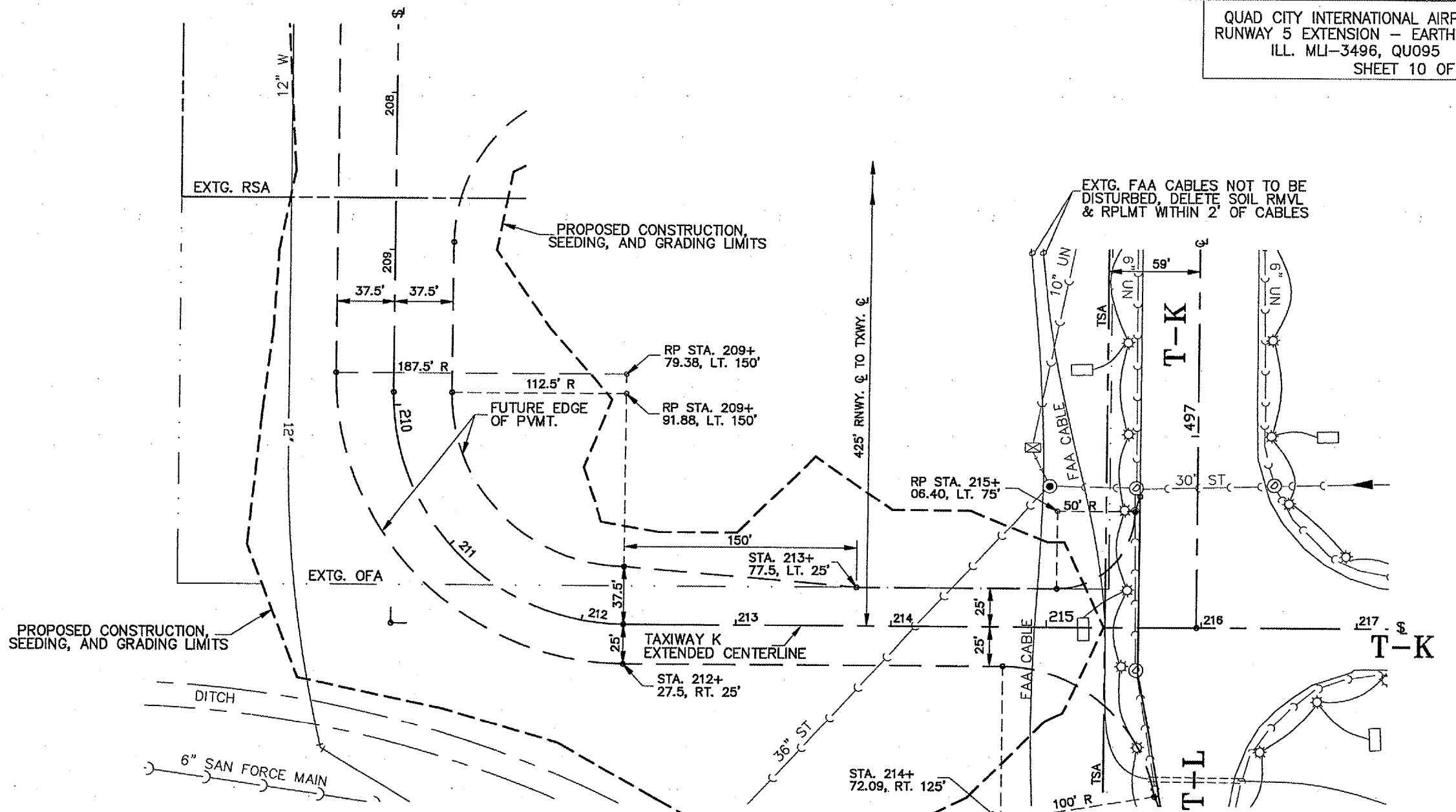
- LEGEND:**
- ⊙ EDGE LIGHT
  - ▭ GUIDANCE SIGN
  - ELECTRICAL CABLE
  - STORM SEWER, TILE, OR UNDERDRAIN
  - ▭ INLET
  - ▽ OUTLET
  - ⊙ STORM MANHOLE
  - ⊙ SANITARY MANHOLE
  - DRAINAGE DITCH
  - === CONDUIT OR DUCT BANK

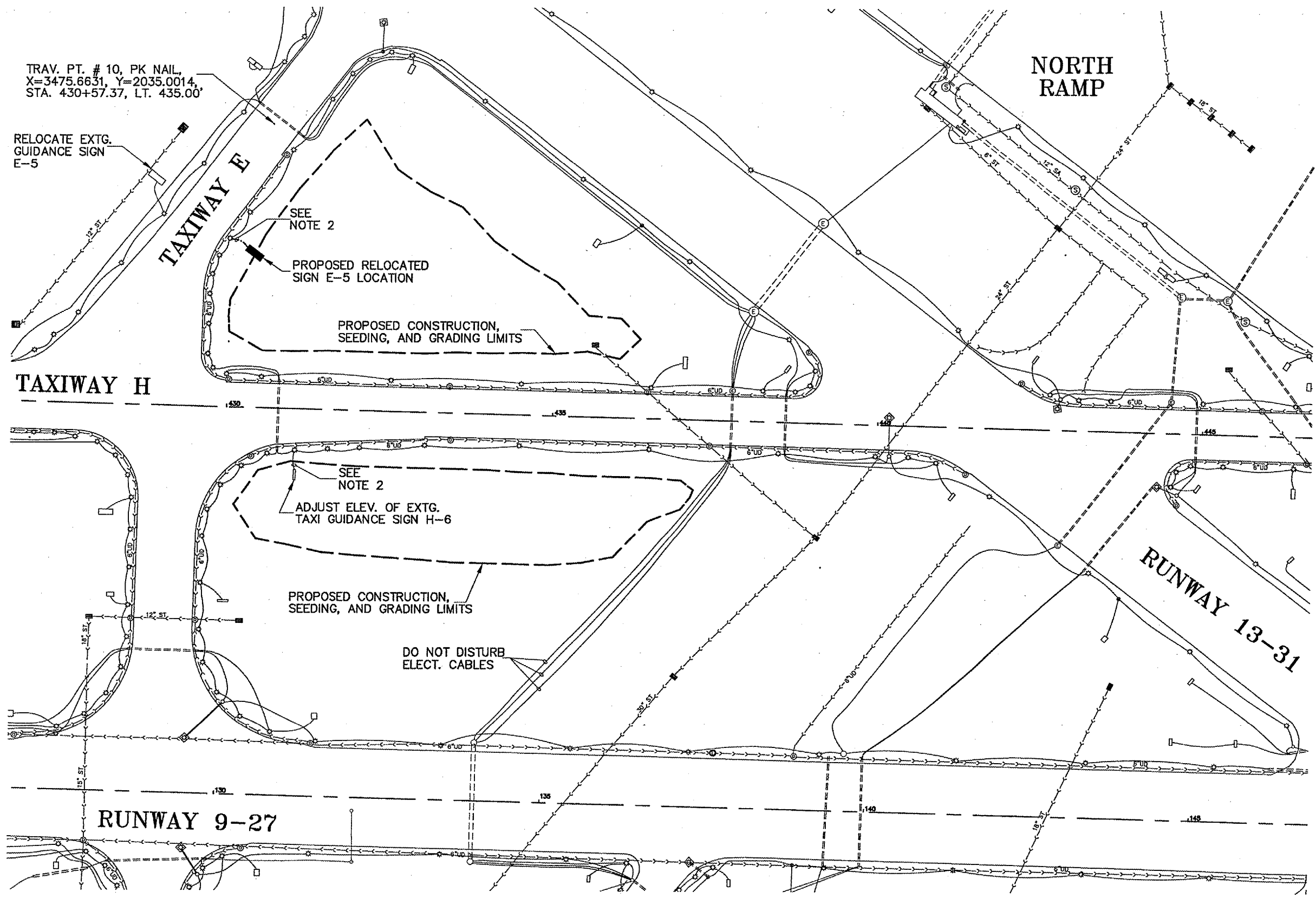
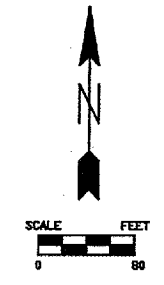
**⊙ CURVE DATA:**

I = 90D 00' 00"  
 D = 38D 11' 49.92"  
 T = 150.00'  
 L = 235.62'  
 R = 150.00'  
 E = 62.13'  
 P.C. = 209+91.88  
 P.I. = 211+41.88  
 P.T. = 212+27.50



RUNWAY 5  
 CENTERLINE





TRAV. PT. # 10, PK NAIL,  
X=3475.6631, Y=2035.0014,  
STA. 430+57.37, LT. 435.00'

RELOCATE EXTG.  
GUIDANCE SIGN  
E-5

TAXIWAY E

SEE  
NOTE 2

PROPOSED RELOCATED  
SIGN E-5 LOCATION

PROPOSED CONSTRUCTION,  
SEEDING, AND GRADING LIMITS

TAXIWAY H

SEE  
NOTE 2

ADJUST ELEV. OF EXTG.  
TAXI GUIDANCE SIGN H-6

PROPOSED CONSTRUCTION,  
SEEDING, AND GRADING LIMITS

DO NOT DISTURB  
ELECT. CABLES

RUNWAY 13-31

RUNWAY 9-27

NORTH  
RAMP

**NOTES:**

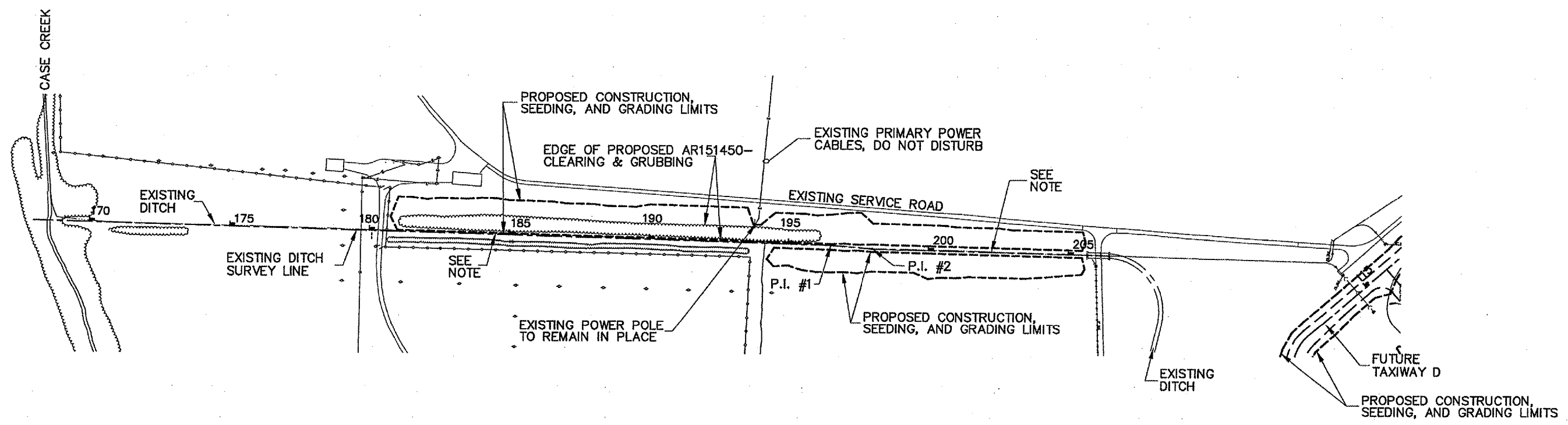
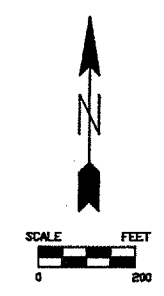
1. SEE SHEETS 14 - 16 FOR ELECTRICAL & GUIDANCE SIGN DETAILS.
2. PROPOSED ELECTRICAL CABLES, 2/C, #8, EPR CABLES IN 1-1/2" HDPE UNIT DUCT.
3. REPLACE 7 EACH EXISTING SIGN PANELS, SEE SHEET 16 FOR DETAILS.

**LEGEND:**

- EDGE LIGHT
- GUIDANCE SIGN
- ELECTRICAL CABLE
- STORM SEWER OR UNDERDRAIN
- INLET
- OUTLET
- STORM MANHOLE
- SANITARY MANHOLE
- DRAINAGE DITCH
- CONDUIT OR DUCT BANK
- PROPOSED CONSTRUCTION,  
SEEDING, AND GRADING LIMITS

QUAD CITY INTERNATIONAL AIRPORT  
 RUNWAY 5 EXTENSION - EARTHWORK  
 ILL. MLI-3496, QU095  
 SHEET 12 OF 49

**SURVEY LINE DATA**  
 P.I.#1 = STA. 196+50  
 I = 2D 52' 6.88"  
 P.I.#2 = STA. 198+00  
 I = 3D 7' 47.04"

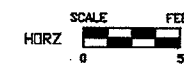


**NOTE:**  
 DO NOT DISTURB THE BOTTOM AREA  
 OF THE EXISTING DRAINAGE DITCH.

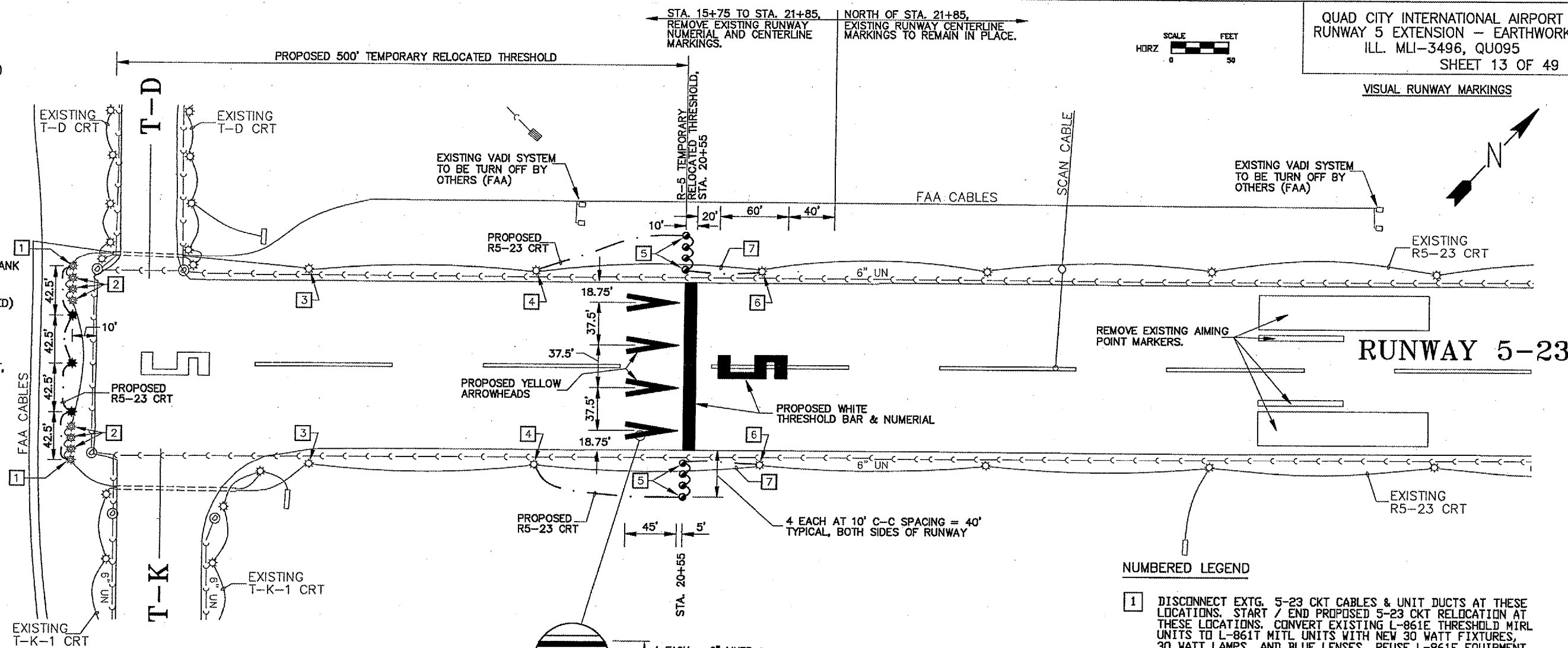
- LEGEND:**
- EDGE LIGHT
  - GUIDANCE SIGN
  - ELECTRICAL CABLE
  - STORM SEWER OR UNDERDRAIN
  - INLET
  - OUTLET
  - STORM MANHOLE
  - SANITARY MANHOLE
  - DRAINAGE DITCH
  - CONDUIT OR DUCT BANK
  - FENCE LINE
  - POWER POLE
  - PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS

**LEGEND:**

- ☉ EXISTING EDGE LIGHT (L-861T MITL OR L-861 MIRL)
- ☉ EXISTING EDGE LIGHT (L-861E THRESHOLD MIRL)
- ▭ EXISTING GUIDANCE SIGN
- EXISTING ELECTRICAL CABLE
- EXISTING STORM SEWER, TILE, OR UNDERDRAIN
- ▨ EXISTING INLET
- ▽ EXISTING OUTLET
- ⊙ EXISTING STORM MANHOLE
- ⊙ EXISTING SANITARY MANHOLE
- EXISTING DRAINAGE DITCH
- EXISTING CONDUIT OR DUCT BANK
- PROPOSED LOCATION FOR RELOCATED OR NEW L-861E THRESHOLD MIRL (GREEN / RED)
- PROPOSED LOCATION FOR RELOCATED OR NEW L-861T MITL (BLUE)
- PROPOSED ELECTRICAL CIRCUIT, 1/C, #8, EPR CABLE IN 1" HDPE UNIT DUCT

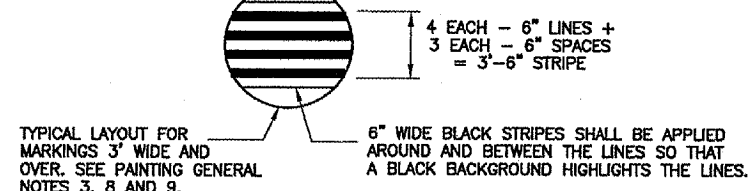


VISUAL RUNWAY MARKINGS



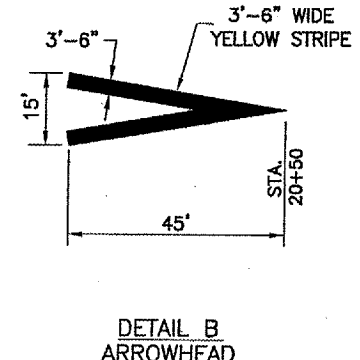
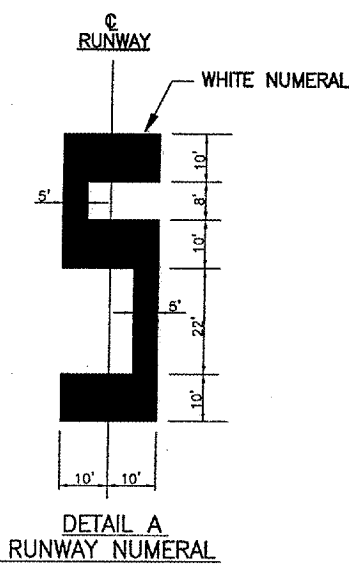
**NUMBERED LEGEND**

- 1 DISCONNECT EXTG. 5-23 CKT CABLES & UNIT DUCTS AT THESE LOCATIONS. START / END PROPOSED 5-23 CKT RELOCATION AT THESE LOCATIONS. CONVERT EXISTING L-861E THRESHOLD MIRL UNITS TO L-861T MITL UNITS WITH NEW 30 WATT FIXTURES, 30 WATT LAMPS, AND BLUE LENSES, REUSE L-861E EQUIPMENT AT LOCATIONS 5.
- 2 RELOCATE EXISTING L-861E THRESHOLD MIRL UNITS (6 EACH) TO LOCATIONS 5.
- 3 CONVERT EXISTING L-861 MIRL UNITS TO L-861T MITL UNITS WITH NEW BLUE LENSES.
- 4 DISCONNECT EXTG. 5-23 CKT CABLES & UNIT DUCTS AT THESE LOCATIONS. START / END PROPOSED 5-23 CKT RELOCATION AT THESE LOCATIONS. CONVERT EXISTING L-861 MIRL UNITS TO L-861T MITL UNITS WITH NEW BLUE LENSES.
- 5 PROPOSED LOCATIONS FOR RELOCATED THRESHOLD LIGHTS. PROVIDE TWO L-867 BASE CANS, SIZE B, 12" DIA. X 24" HEIGHT TO REPLACE BASE CANS LEFT AT LOCATIONS 1. SOUTHWEST FACE = GREEN FILTER, NORTHEAST FACE = RED FILTER.
- 6 DISCONNECT EXTG. 5-23 CKT CABLES & UNIT DUCTS AT THESE LOCATIONS. START / END PROPOSED 5-23 CKT RELOCATION AT THESE LOCATIONS.
- 7 EXISTING CABLES TO BE TEMPORARY DISCONNECTED FROM RUNWAY CIRCUIT. CABLES TO BE RECONNECTED ONCE TEMPORARY RELOCATED THRESHOLD REMOVED.



**PAINING GENERAL NOTES:**

1. ALL RUNWAY MARKINGS SHALL BE WHITE UNLESS NOTED OTHERWISE.
2. ALL TAXIWAY MARKINGS SHALL BE YELLOW UNLESS NOTED OTHERWISE.
3. RUNWAY MARKINGS 3' FEET WIDE AND OVER SHALL CONSIST OF A SERIES OF LONGITUDINAL STRIPES 6" WIDE WITH EQUAL WIDTH SPACING BETWEEN THE STRIPES (STRIATED MARKINGS.)
4. ALL MARKINGS SHALL HAVE A REFLECTIVE MEDIA APPLIED IN ACCORDANCE WITH THE SPECIAL PROVISIONS.
5. TAXIWAY EDGE AND CENTERLINE STRIPES SHALL BE SOLID TO MATCH THE EXISTING TAXIWAY MARKINGS. RUNWAY EDGE, CENTERLINE, NUMERIAL, TOUCHDOWN ZONE, AIMING POINT, THRESHOLD STRIPES AND THRESHOLD BAR SHALL BE STRIATED TO MATCH THE EXISTING RUNWAY MARKINGS.
6. EXISTING TAXIWAY AND RUNWAY MARKINGS DAMAGED BY CONSTRUCTION OF THE PROJECT SHALL BE REPLACED BY THE CONTRACTOR AT HIS EXPENSE.
7. MARKING LAYOUT BY CONTRACTOR, SEE SPECIAL PROVISIONS.
8. ALL NEW PAVEMENT MARKINGS SHALL BE OUTLINED WITH A SIX INCH (6") WIDE BLACK BORDER. FOR HOLD LINES, THE BLACK BORDER WIDTH SHALL BE INCREASED TO TWELVE INCHES (12").
9. BLACK BACKGROUND STRIPES SHALL BE APPLIED BETWEEN YELLOW / WHITE PAVEMENT STRIPES ON STRIATED MARKINGS.
10. THE MAA RESERVES AND SHALL HAVE THE RIGHT TO DELETE THE PAVEMENT MARKING PAY ITEMS FROM THE CONTRACT WORK. THE SPONSOR MAY OPT TO COMPLETE THIS WORK WITH THEIR OWN WORK FORCE.

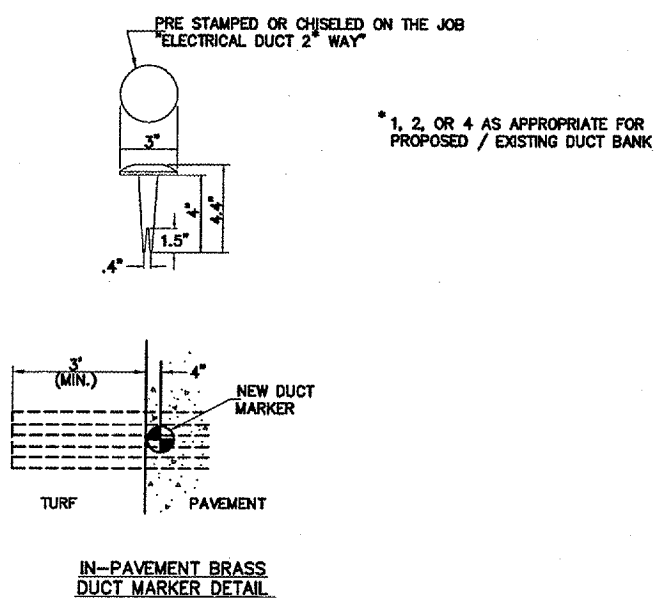
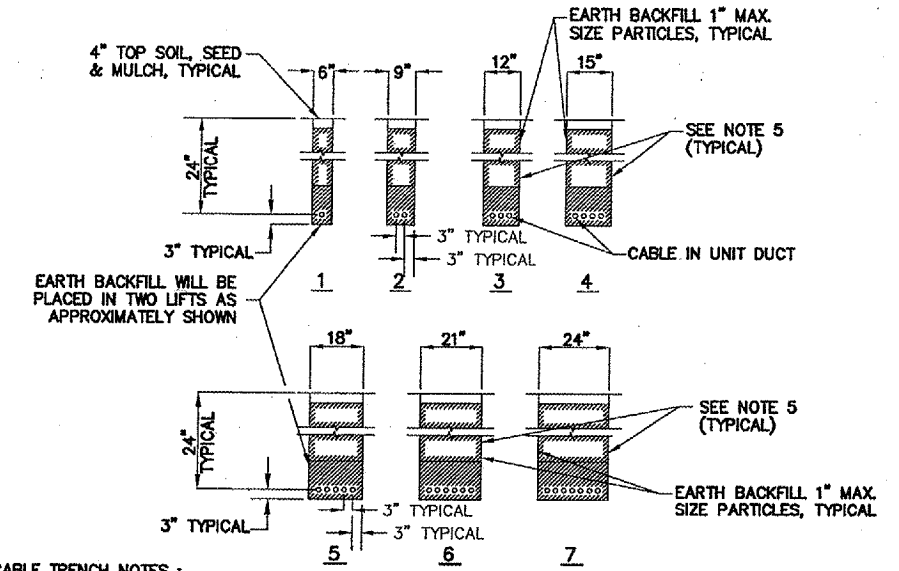
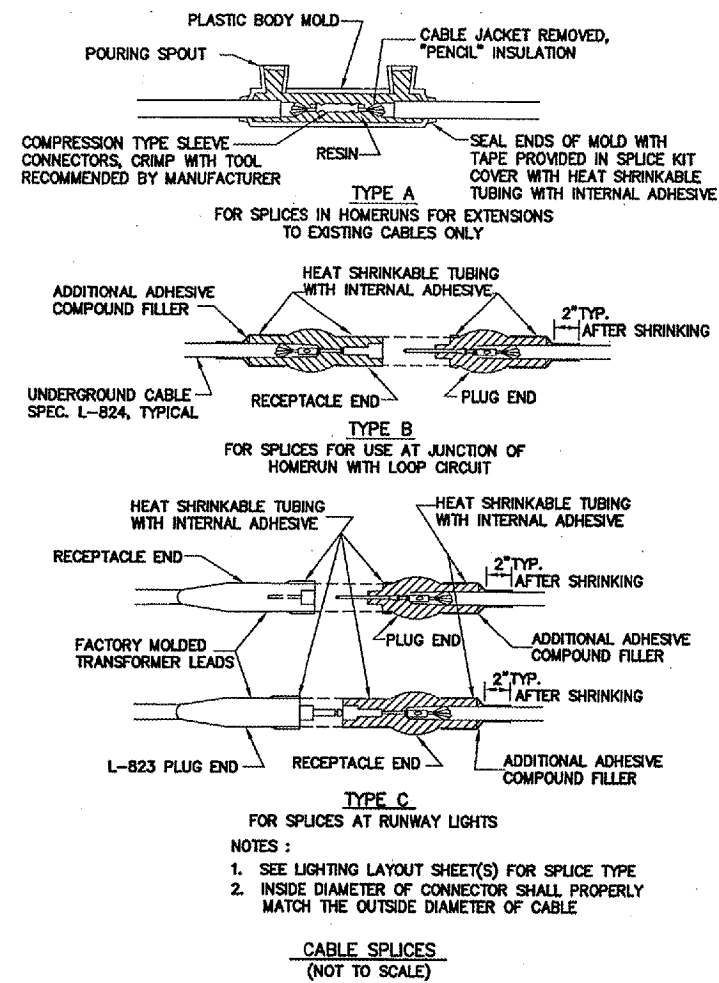


**GENERAL NOTES:**

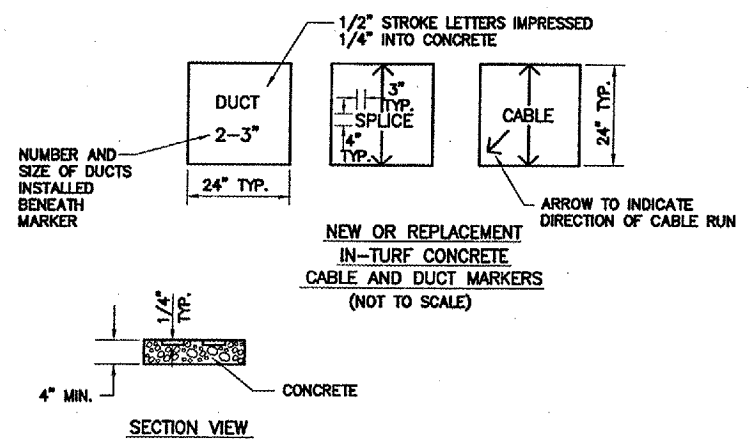
1. SEE SHEETS 14-15 FOR ELECTRICAL GENERAL NOTES AND DETAILS.
2. IF REQUIRED BY THE FAA, THE CONTRACTOR SHALL COMPLETE THE FOLLOWING ITEMS AT THE PROPOSED / EXISTING RUNWAY EDGE LIGHTS (9 LOCATIONS) BETWEEN STA. 15+25 & STA. 20+00. DISCONNECT L-830 TRANSFORMERS FROM R5-23 CIRCUIT. CONNECT L-823 CONNECTORS ON R5-23 CIRCUIT CABLES TO BY-PASS TRANSFORMERS AND PROVIDE CONTINUITY IN THE RUNWAY CIRCUIT. COVER RUNWAY EDGE LIGHTS WITH BURLAP SACKS AS DIRECTED BY THE RESIDENT ENGINEER. COSTS INCIDENTAL TO LIGHTING UNIT PRICES.
3. ALL EXISTING ELECTRICAL CIRCUITS TO REMAIN ACTIVE AT ALL TIMES. CONTRACTOR TO PROVIDE, INSTALL, AND MAINTAIN TEMPORARY ABOVE GROUND JUMPER CABLES AS REQUIRED TO PROVIDE CIRCUIT CONTINUITY IN ALL CIRCUITS AT ALL TIMES DURING CONSTRUCTION. COSTS TO BE INCLUDED IN THE LIGHTING CONTRACT UNIT PRICES. LENGTH OF JUMPER CABLES SHALL NOT BE MEASURED FOR PAYMENT AND/OR PAYED FOR.

**GENERAL ELECTRICAL NOTES:**

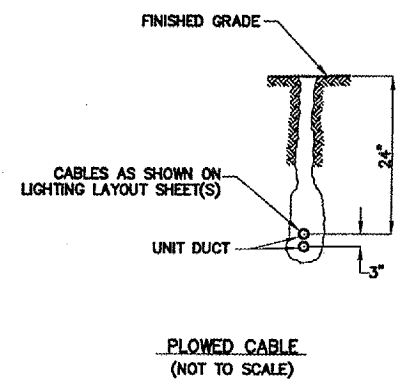
1. ALL EXCAVATION SHALL BE DONE VERY CAREFULLY. EXCAVATION BY HAND DIGGING SHALL BE REQUIRED AROUND ALL EXISTING DUCT BANKS, SPLICE CANS, MANHOLES, AND EXISTING CABLES. MANY EXISTING ACTIVE UNDERGROUND CABLES, WHOSE EXACT LOCATIONS CANNOT BE DETERMINED, ARE FOUND IN THE PROJECT AREA. IN ORDER TO AVOID EXISTING UNDERGROUND CABLES, THE CONTRACTOR SHALL CONNECT A THUMPER TO ALL EXISTING CIRCUITS AFTER WHICH THEY SHALL BE STAKED IN ALL AREAS REQUIRING TRENCHING OR EXCAVATION. CONTRACTOR SHALL ALSO NOTE THAT LOW VOLTAGE, FAA CABLES ALSO RUN UNDERGROUND THROUGHOUT THESE AREAS. ANY CABLE DAMAGED SHALL BE REPAIRED OR REPLACED TO ORIGINAL CONDITION AT THE CONTRACTOR'S EXPENSE.
2. A MINIMUM OF 3 FEET OF SLACK SHALL BE PROVIDED IN THE CABLES AT EACH TRANSFORMER, CONNECTOR, OR SPLICE POINT. ALL CABLE SPLICES SHALL OCCUR IN MANHOLES, LIGHT WELLS OR SPLICE CANS, UNLESS NOTED OTHERWISE.
3. THE ELECTRICAL INSTALLATION, AS A MINIMUM, SHALL MEET THE NATIONAL ELECTRICAL CODE AND LOCAL REGULATIONS.
4. ALL MANUFACTURERS FOR SUPPLYING AIRPORT LIGHTING EQUIPMENT SHALL APPEAR ON THE CURRENT FAA "APPROVED AIRPORT EQUIPMENT" LIST FOUND IN AC 150/5345-53B. THE EQUIPMENT SHALL COMPLY WITH THE APPLICABLE CURRENT FAA ADVISORY CIRCULAR LISTED IN THE FAA "APPROVED AIRPORT EQUIPMENT" LIST FOUND IN AC 150/5345-2 (AIRPORTS ELECTRONIC BULLETIN BOARD NUMBER 14).
5. THE CONTRACTOR SHALL ASCERTAIN THAT ALL LIGHTING SYSTEM COMPONENTS FURNISHED BY HIM (INCLUDING FAA APPROVED EQUIPMENT) ARE COMPATIBLE IN ALL RESPECTS WITH EACH OTHER AND THE REMAINDER OF THE NEW/EXISTING SYSTEM. ANY NONCOMPATIBLE COMPONENTS FURNISHED BY THIS CONTRACTOR SHALL BE REPLACED BY HIM AT NO ADDITIONAL COST TO THE AIRPORT SPONSOR WITH A SIMILAR UNIT, APPROVED BY THE ENGINEER (DIFFERENT MODEL OR DIFFERENT MANUFACTURER) THAT IS COMPATIBLE WITH THE REMAINDER OF THE AIRPORT LIGHTING SYSTEM.
6. IF THE CONTRACTOR SELECTS TO FURNISH AND INSTALL AIRPORT LIGHTING EQUIPMENT WHICH REQUIRES ADDITIONAL WIRING, TRANSFORMERS, ADAPTERS, MOUNTINGS, ETC., BEYOND THAT SHOWN ON THE DRAWINGS AND/OR LISTED IN THE SPECIFICATIONS, THEN THE COST FOR THE ADDITIONAL ITEMS SHALL BE INCLUDED TO THE CONTRACT UNIT PRICES.
7. THE CONTRACTOR INSTALLED EQUIPMENT (INCLUDING FAA APPROVED) SHALL NOT GENERATE ANY ELECTROMAGNETIC INTERFERENCE IN THE EXISTING AND/OR NEW COMMUNICATIONS, WEATHER, AIR NAVIGATION, AND AIR TRAFFIC CONTROL EQUIPMENT. ANY EQUIPMENT GENERATING SUCH INTERFERENCE SHALL BE REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST WITH THE EQUIPMENT MEETING THE APPLICABLE SPECIFICATIONS AND NOT GENERATING ANY INTERFERENCE.
8. WHEN A SPECIFIC TYPE, STYLE, CLASS, ETC. OF FAA APPROVED EQUIPMENT IS SPECIFIED ONLY THAT TYPE, STYLE, CLASS, WILL BE ACCEPTABLE, EVEN THOUGH EQUIPMENT OF OTHER TYPES, STYLES, CLASSES, ETC. MAY BE FAA APPROVED.
9. ALL CONCRETE FOR ELECTRICAL EQUIPMENT SHALL COMPLY WITH SPECIFICATION 610-STRUCTURAL PC CONCRETE 3500 PSI AT 28 DAYS, AIR ENTRAINED CONCRETE MIX SHALL BE USED.
10. BASE MOUNTED BREAKABLE COUPLINGS SHALL NOT HAVE WEEP HOLES TO THE OUTSIDE. PLUGGED UP HOLES SHALL NOT BE ACCEPTABLE. IT SHALL HAVE A 1/4" DIAMETER OR EQUIVALENT OPENING FOR DRAINAGE FROM THE SPACE AROUND THE CONNECTOR INTO THE BASE.
11. THE ELEVATION OF THE BREAKABLE COUPLING GROOVE SHALL NOT EXCEED 1 1/2" ABOVE THE EDGE OF THE COVER IN THE CASE OF A BASE MOUNTED COUPLING.
12. ALL PERMANENT CABLE SPLICES SHALL OCCUR IN MANHOLES, LIGHT WELLS, OR SPLICE CANS, UNLESS NOTED OTHERWISE.
13. MIMIC PANEL COLORS : TAXIWAY D CIRCUIT = ORANGE, TAXIWAY K-1 CIRCUIT = RED AND RUNWAY 5-23 CIRCUIT = WHITE.



NOTE: PAVING CONTRACTOR SHALL INSTALL NEW BRASS DUCT MARKERS IN THE PROPOSED PAVEMENT AT ALL LOCATIONS WHERE THE PROPOSED PAVEMENT CROSSES EXISTING OR PROPOSED ELECTRICAL DUCTS. COST OF DUCT MARKERS SHALL BE INCLUDED IN THE 401 AND / OR 501 CONTRACT UNIT PRICES.



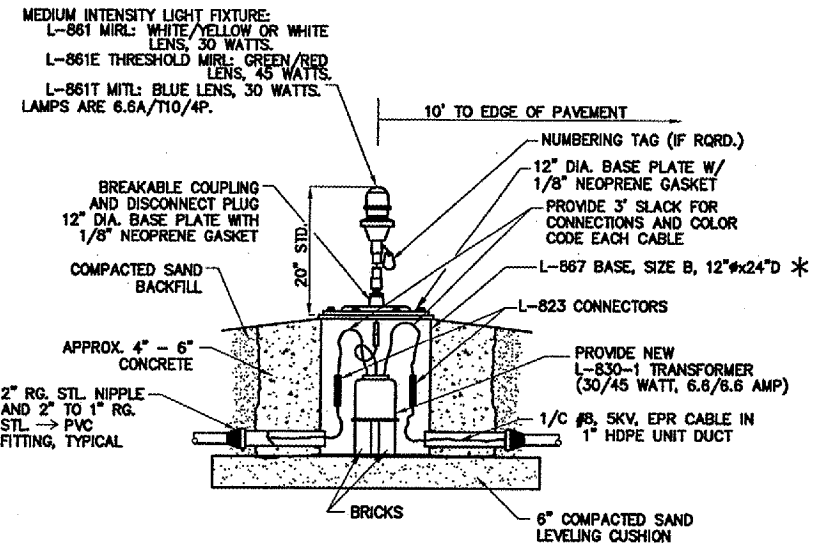
- NOTES:**
1. NEW MARKERS ARE NOT REQUIRED FOR THIS PROJECT. THE CONTRACTOR SHALL BE REQUIRED TO ADJUST THE ELEVATION OF EXISTING MARKERS AND / OR REPLACE EXISTING MARKERS DAMAGED DURING CONSTRUCTION.
  2. COST OF CONCRETE MARKERS IS INCIDENTAL TO THE ASSOCIATED ITEMS OF DUCT OR CABLE.
  3. EDGE EXPOSED CONCRETE WITH A 1/4" RADIUS TOOL.
  4. WHERE ADDITIONAL SPACE TO FIT THE LEGEND IS REQUIRED, SOME OF THE FOLLOWING METHODS SHALL BE EMPLOYED.
    - A. REDUCE LETTER SIZE TO 3" HIGH, 2" WIDE
    - B. INCREASE THE MARKER SIZE TO 30" X 30" MAX.
    - C. PROVIDE ADDITIONAL MARKERS PLACED SIDE BY SIDE



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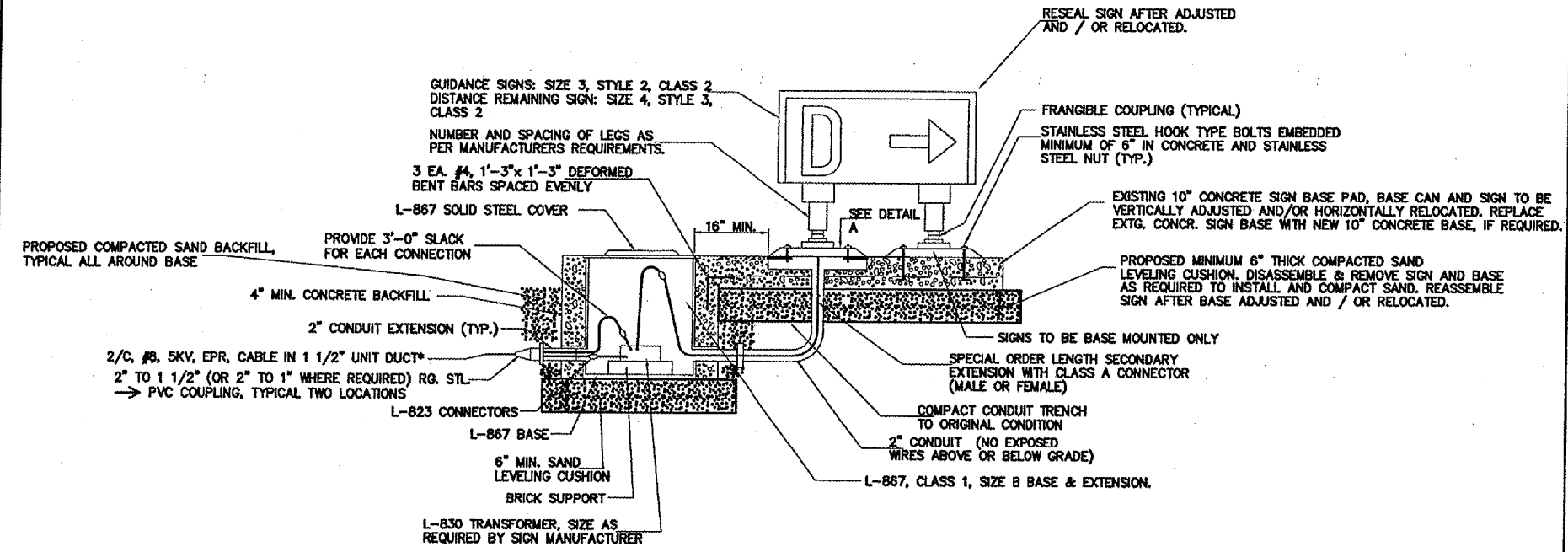


ADJUSTMENT / RELOCATION OF EXISTING GUIDANCE SIGNS



**L-861 EDGE LIGHT INSTALLATION / RELOCATION DETAILS**  
 MEDIUM INTENSITY RUNWAY LIGHT (MIRL)  
 MEDIUM INTENSITY TAXIWAY LIGHT (MITL)  
 BASE MOUNTED, 6.6 AMP SERIES CIRCUIT  
 NOT TO SCALE

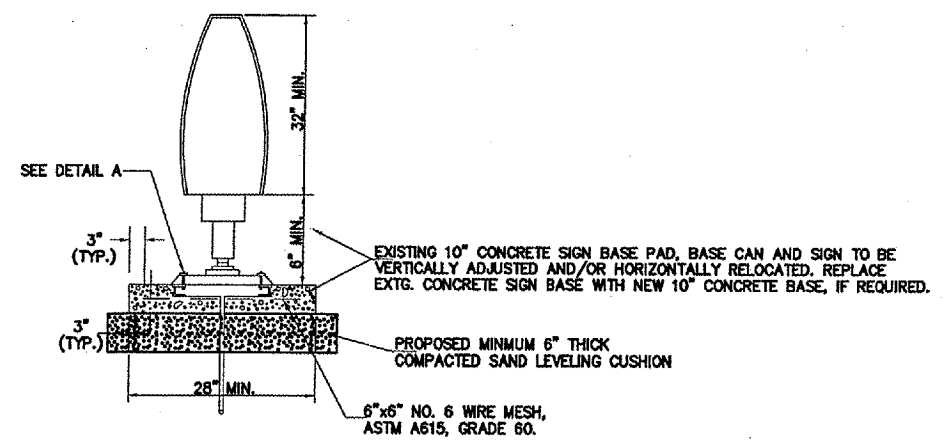
- NOTES :**
- BREAKING GROOVE OF BREAKABLE COUPLING SHALL BE 3" TO 3 1/2" ABOVE FINISHED GRADE.
  - \* ORDER L-867 BASE WITH ADDITIONAL 2" CONDUIT HUB APPROXIMATELY 90° FROM MAIN ENTRANCE HUB WHERE INDICATED ON PLAN SHEETS.
  - IF THE CONDITION OF THE EXISTING LIGHTING EQUIPMENT TO BE REMOVED IS ACCEPTABLE TO THE RESIDENT ENGINEER, THE CONTRACTOR MAY REUSE THE REMOVED LIGHT FIXTURES, BASE PLATES, CANS, AND TRANSFORMERS AT THE PROPOSED LIGHT LOCATIONS. IF THE EXISTING EDGE LIGHT EQUIPMENT IS REUSED, THEN THE CONTRACTOR SHALL SUPPLY AND INSTALL 6" COMPACTED SAND LEVELING CUSHION, EXTEND THE EXISTING STEEL CONDUIT TO A POINT OUTSIDE OF THE PROPOSED CONCRETE BACKFILL, ADDITIONAL P.C. CONCRETE BACKFILL ALL AROUND (MIN. 4" THICKNESS), COMPACTED SAND BACKFILL, NEW 1/8" NEOPRENE GASKETS, RETAP BASE HOLES AS REQ., NEW STAINLESS STEEL BOLTS, NUTS, & WASHERS, AND NEW LENS & LAMPS AS REQUIRED. CONTRACTOR SHALL REPLACE ANY DAMAGED EQUIPMENT AS DIRECTED BY THE RESIDENT ENGINEER. CONTRACTOR SHALL SUPPLY AND INSTALL NEW EDGE LIGHT EQUIPMENT AS REQUIRED.



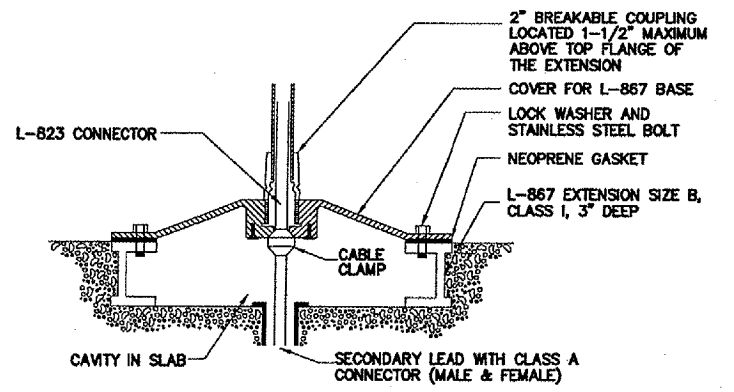
FRONT VIEW

\* - FOR SIGN ELEVATION ADJUSTMENTS, RAISE EXISTING UNIT DUCT AS REQUIRED TO ADJUST ELEVATION OF TRANSFORMER CAN.

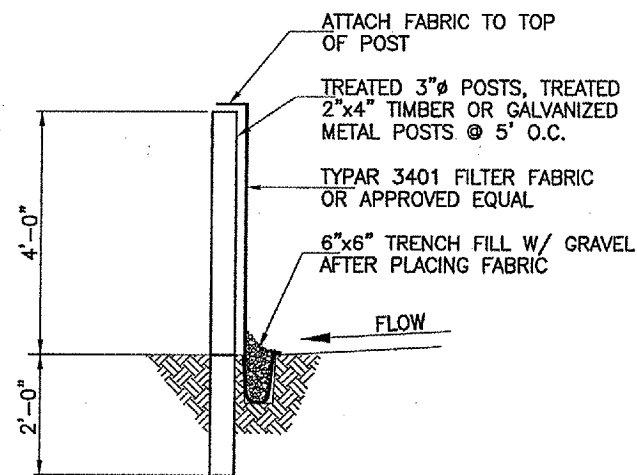
- NOTES:**
- COLOR CODE TAPE FOR WIRE IDENTIFICATION 6" BEFORE L-823 CONNECTORS.
  - CONTRACTOR MAY REUSE EXISTING MATERIALS AND EQUIPMENT. CONTRACTOR SHALL SUPPLY AND INSTALL NEW EQUIPMENT AND MATERIALS AS REQUIRED TO COMPLETE SIGN ADJUSTMENT / RELOCATION.
  - DRAINAGE TO FLOW AWAY FROM SIGN. NO WATER PONDING AROUND SIGN BASE PERMITTED.



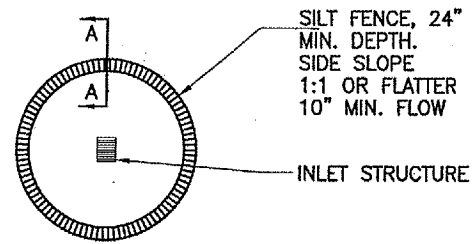
SIDE VIEW



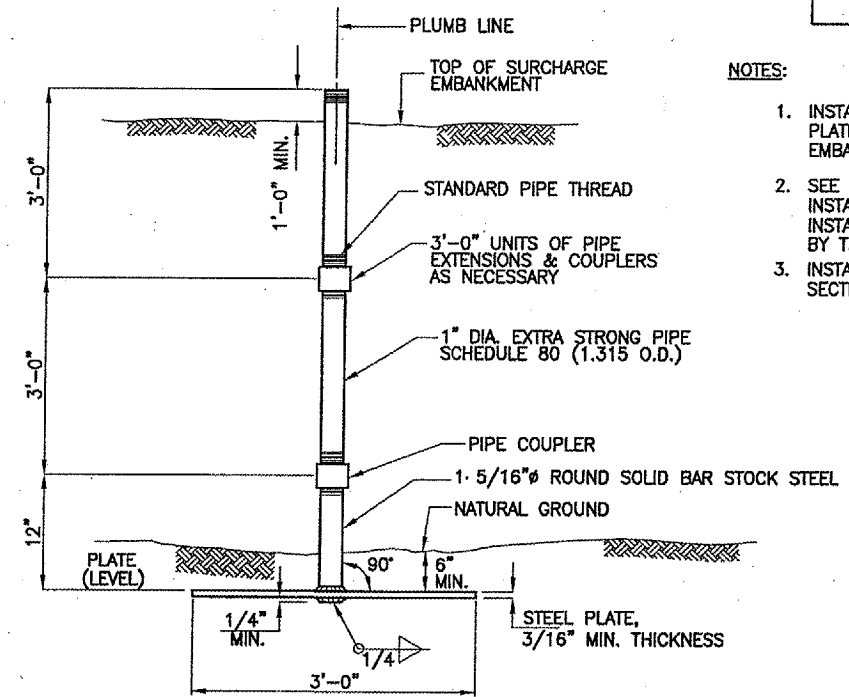
DETAIL A



SECTION A-A  
 SILT FENCE DETAIL  
 NOT TO SCALE



TEMPORARY SEDIMENT TRAP  
 AT ALL INLET STRUCTURES



PROPOSED SETTLEMENT PLATES  
 (8 EACH REQ'D)

NOTES:

1. INSTALL PROPOSED SETTLEMENT PLATES PRIOR TO INSTALLING THE EMBANKMENT SURCHARGE SOILS.
2. SEE SPECIFICATIONS FOR ADDITIONAL INSTALLATION DETAILS. PLATES TO BE INSTALLED IN THE LOCATIONS SELECTED BY THE ENGINEER.
3. INSTALL ADDITIONAL 3' LONG PIPE SECTIONS AS REQUIRED.

REMOVE EXISTING AND INSTALL PROPOSED GUIDANCE SIGN PANELS  
 SIZE 3, STYLE 2, CLASS 2

SIGN NUMBER	FACE	EXISTING LEGEND	LETTERS		PROPOSED LEGEND	LETTERS		CIRCUIT	PAY ITEMS	REMARK
			BACKGROUND	BACKGROUND		BACKGROUND	BACKGROUND			
E-5	NE	↙ H   R   E   E   H ↗	B/Y	B/Y	↙ H   R   E   E   H ↗	B/Y	B/Y	T-E	AR801605	REPLACE 4 PANELS
	SW		B/B	B/B	NO CHANGE	B/B	B/B			
H-6	W		B/B	B/B	NO CHANGE	B/B	B/B	T-H	AR801605	REPLACE 3 PANELS
	E	← E   H   E →	B/Y	B/Y	← E   H   E →	B/Y	B/Y			

TABLE LEGEND:

- B/Y = BLACK LETTERS ON YELLOW BACKGROUND (TYPE L-858Y)  
 Y/B = YELLOW LETTERS ON BLACK BACKGROUND (TYPE L-858L)  
 B/B = BLACK BLANK PANEL  
 Y/Y = YELLOW BLANK PANEL  
 W/R = WHITE LETTERS ON RED BACKGROUND (TYPE L-858R)

- N = NORTH      NW = NORTHWEST  
 S = SOUTH      SE = SOUTHEAST  
 E = EAST      NE = NORTHEAST  
 W = WEST      SW = SOUTHWEST

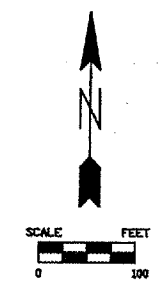
SIGN GENERAL NOTES:

- 1) "LEGEND" COLUMN IN TABLE INDICATES NUMBER OF PANELS (MODULES), SPECIFIC CHARACTERS PER PANEL AND BLANK PANELS. SIGNS ARE TO BE MANUFACTURED AND SUPPLIED AS SHOWN IN THE LEGEND COLUMN UNLESS OTHERWISE APPROVED BY THE MAA.
- 2) ALL SIGNS SHALL COMPLY WITH FAA ADVISORY CIRCULAR 150/5345-44E.
- 3) CONTRACTOR SHALL VERIFY ALL BASE DIMENSIONS WITH MANUFACTURER PRIOR TO INSTALLATION OF ANCHOR BOLTS.
- 4) THE CONTRACTOR SHALL DISASSEMBLE EXISTING GUIDANCE SIGNS AS REQUIRED TO INSTALL PROPOSED NEW SIGN PANELS AND REUSE EXISTING PANELS AS INDICATED IN TABLE. EXISTING PANELS REMOVED MAY ONLY BE REUSED, IF THE CONDITION OF THE PANEL IS ACCEPTABLE TO THE MAA AND THE ENGINEER. THE CONTRACTOR SHALL REPLACE ALL DAMAGED EXISTING PANELS AND EQUIPMENT AS REQUIRED BY THE ENGINEER. ONCE THE SIGNS HAVE BEEN REBUILT, THE CONTRACTOR SHALL SEAL SIGNS AND RETURN THE EXISTING SIGNS TO THEIR ORIGINAL CONDITION. EXISTING PANELS REMOVED AND NOT REUSED SHALL BE DELIVERED BY THE CONTRACTOR TO THE OWNER (MAA).
- 5) SIGNS SHALL BE DOUBLE FACED AS INDICATED IN TABLE (TYPE L-858Y, L-858R, OR L-969L). SIGNS AND REPLACEMENT PANELS SHALL BE COMPATIBLE IN ALL RESPECTS WITH EACH OTHER AND WITH THE EXISTING SIGNS CURRENTLY IN PLACE AT THE QUAD CITY INTERNATIONAL AIRPORT. SIGNS AND REPLACEMENT PANELS SHALL BE LUMACURVE, OR APPROVED EQUAL.
- 6) "FACE" COLUMN INDICATES DIRECTION OF SIGN FACE.
- 7) SEE SHEET 14 - 15 FOR ELECTRICAL GENERAL NOTES AND ELECTRICAL DETAILS.
- 8) SEE SHEET 15 FOR SIGN DETAILS.
- 9) WHERE PROPOSED SIGNS ARE INSTALLED BESIDE EXISTING SIGNS PROVIDE 1' SPACE BETWEEN SIGNS.

NUMBER OF MODULES	TRANSFORMER WATTAGE
1	100
2	300
3	500
4	500

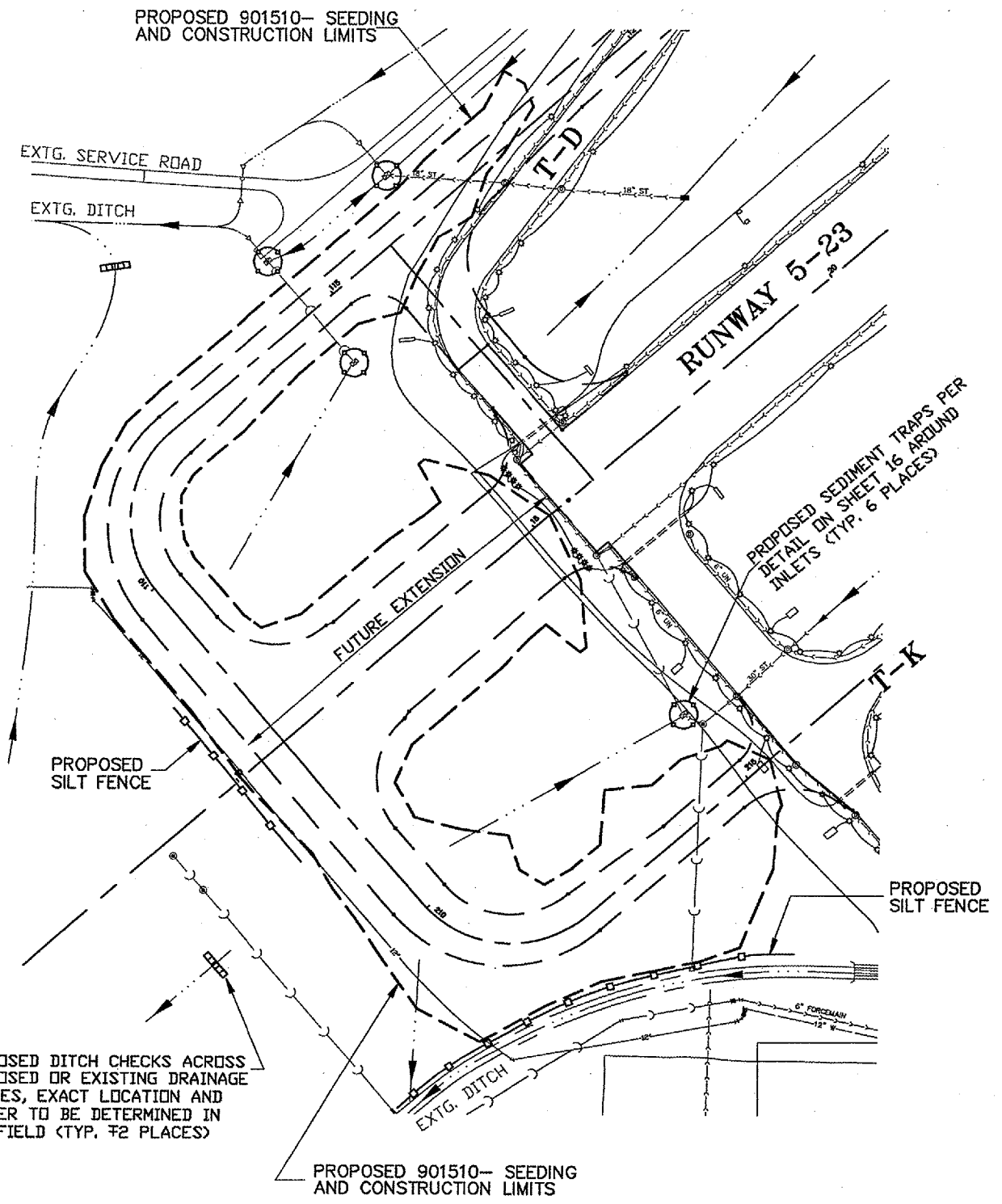
TRANSFORMERS SHALL BE 6.6/6.6 AMP.  
 \* = OR AS REQUIRED BY SIGN MANUFACTURER.

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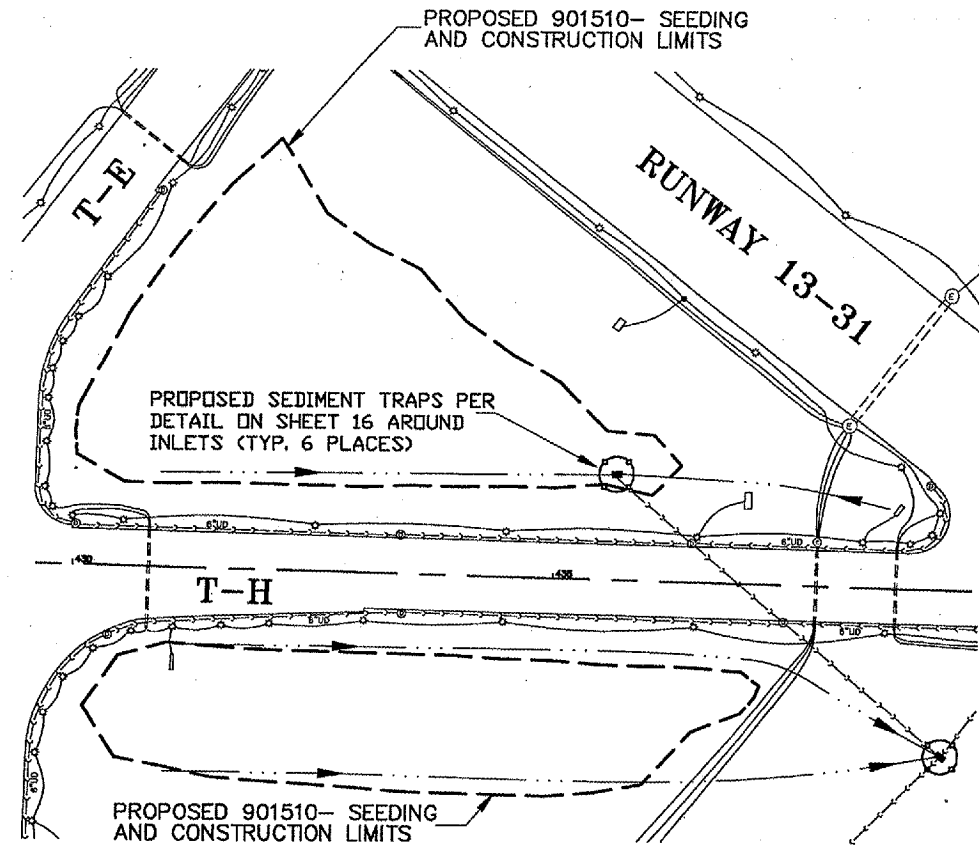


LEGEND:

- EXTG. EDGE LIGHT
- EXTG. GUIDANCE SIGN
- EXTG. ELECTRICAL CABLE
- EXTG. STORM SEWER OR UNDERDRAIN
- EXTG. INLET
- EXTG. OUTLET
- EXTG. MANHOLE
- EXTG. DRAINAGE DITCH
- PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS
- EXISTING FENCE
- EXISTING OR PROPOSED DRAINAGE SWALE
- PROPOSED SILT FENCE
- PROPOSED TEMPORARY STRAW BALE DITCH CHECK
- PROPOSED SEDIMENT TRAP



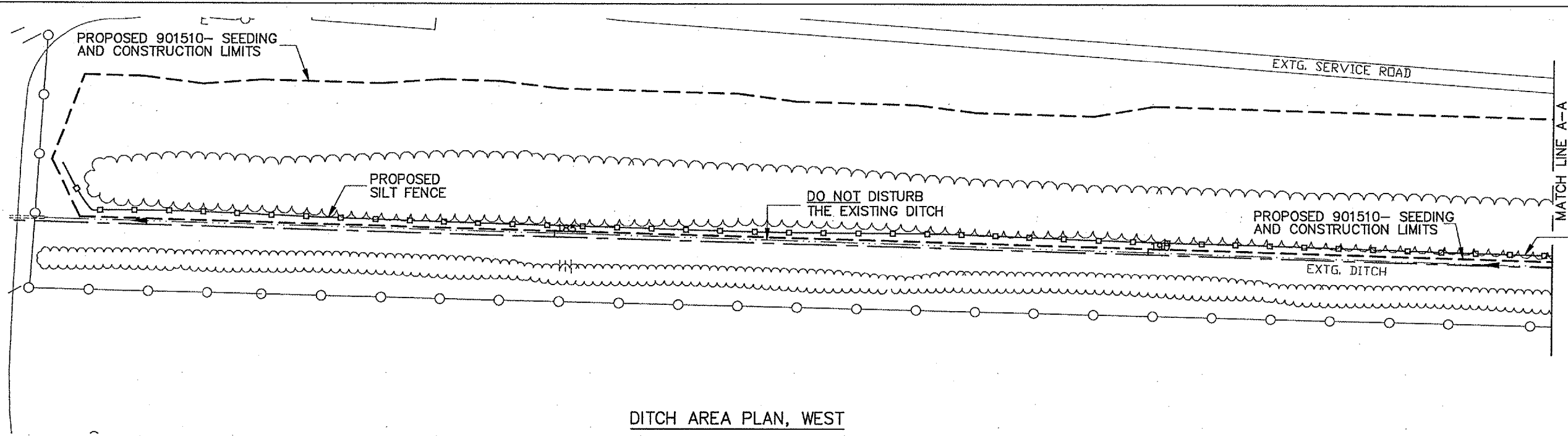
RUNWAY 5 AREA PLAN



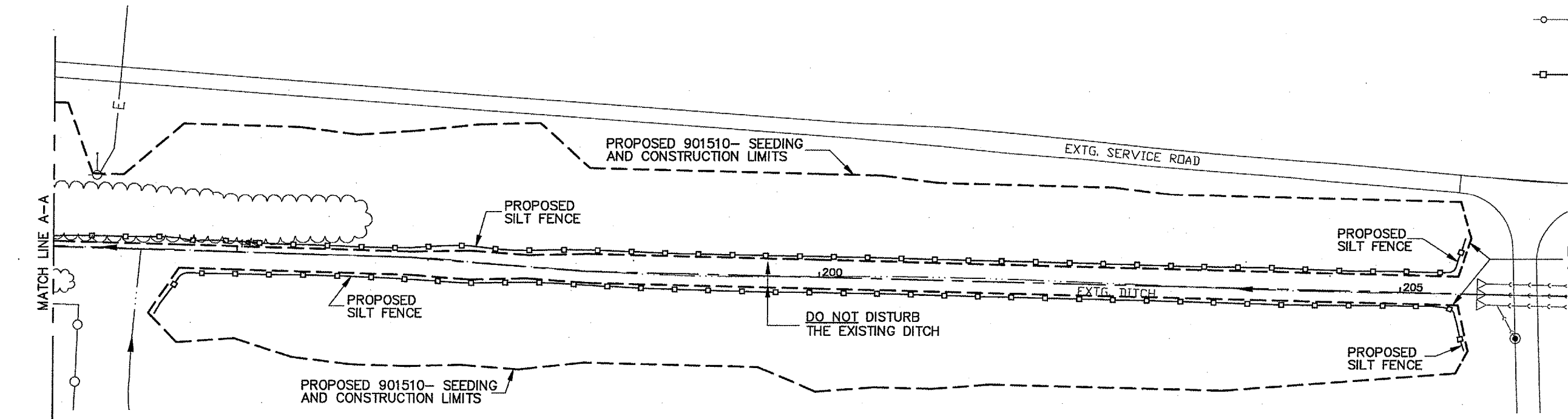
TAXIWAY H AREA PLAN

GENERAL NOTES:

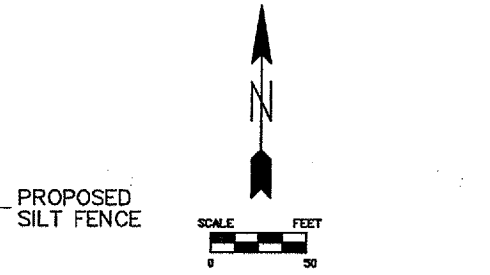
1. TEMPORARY DITCH CHECKS - TWO BALES HIGH WITH SILT FENCING SHALL BE REQUIRED PER IDOT STANDARD 280001 - TEMPORARY EROSION SITE CONTROL SYSTEM DRAWING. THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING, MAINTAINING, AND REMOVING DITCH CHECKS, SILT FENCE, AND SEDIMENT TRAPS TO THE SATISFACTION OF THE RESIDENT ENGINEER. THIS INCLUDES, BUT IS NOT LIMITED TO, CLEANING EROSION SOILS AS REQUIRED.
2. LOCATION OF THE DITCH CHECKS, SILT FENCES, AND SEDIMENT TRAPS SHOWN ARE APPROXIMATE. ACTUAL LOCATIONS TO BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION.
3. ONCE CONSTRUCTION HAS BEEN COMPLETED, OR TEMPORARILY SUSPENDED FOR LONGER THAN 21 DAYS (SUCH AS A WINTER SHUTDOWN), THE CONTRACTOR SHALL SEED ALL AREAS DISTURBED IN ACCORDANCE WITH ITEM 901510 WITHIN 14 DAYS OF THE LAST DISTURBANCE. DITCH CHECKS, SILT FENCES, AND SEDIMENT TRAPS SHALL REMAIN IN PLACE AND BE MAINTAINED UNTIL THE CONTRACTOR ESTABLISHES A GOOD STAND OF GRASS OF UNIFORM COLOR AND DENSITY TO THE SATISFACTION OF THE ENGINEER.
4. THE CONTRACTOR AND EACH SUBCONTRACTOR RESPONSIBLE FOR WATER POLLUTION CONTROL SHALL DESIGNATE, PRIOR TO BEGINNING CONSTRUCTION, A PERSON OR PERSONS WHO CAN BE CONTACTED IN AN EMERGENCY INVOLVING THEIR WATER POLLUTION CONTROL ITEMS. THESE DESIGNATED PEOPLE SHALL BE AVAILABLE TO REPAIR AND MAINTAIN WATER POLLUTION CONTROL DEVICES ON A 24-HOUR / 7 DAYS PER WEEK BASIS.
5. CONTRACTOR TO EXCAVATE TEMPORARY EROSION CONTROL DRAINAGE SWALE AS REQUIRED TO PREVENT RAIN WATER PONDING AND TO CONTROL STORM WATER RUN-OFF.
6. CONTRACTOR SHALL ADHERE TO THE CITY OF MOLINE'S EROSION AND SEDIMENT CONTROL REGULATIONS AND THE ILLINOIS MANUAL ON EROSION AND SEDIMENT CONTROL.
7. SEDIMENT AND EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO ANY CONSTRUCTION THAT DISTURBS EXISTING STORM WATER RUN-OFF CONDITIONS AND/OR GROUND VEGETATION.
8. EROSION CONTROL MEASURES SHALL BE INSPECTED WEEKLY AND AFTER EACH PRECIPITATION EVENT AND REPLACED OR REPAIRED AS NECESSARY.
9. RESIDENT ENGINEER SHALL CHECK THAT ALL FILL AREAS ARE TO A MINIMUM COMPACTION OF 95% OF THE MATERIALS STANDARD PROCTOR MAXIMUM DRY DENSITY.
10. SILT FENCE, SEDIMENT TRAPS, AND HAY BALES SHALL BE CLEANED OR REPLACED WHEN SILT BUILDS UP TO WITHIN ONE FOOT OF THE TOP OF THE SILT FENCE OR HAY BALES.
11. ADDITIONAL EROSION CONTROL DEVICES SHALL BE USED AS REQUIRED. THE COSTS OF ALL MEASURES NECESSARY TO COMPLY WITH THIS STORM WATER POLLUTION PREVENTION PLAN SHALL BE INCLUDED IN THE ITEM 156500 - TEMPORARY EROSION CONTROL LUMP SUM PRICES.
12. SEE SHEET 18 FOR STORM WATER MANAGERS SIGNATURE CHART, ANTICIPATED PROBABLE CONSTRUCTION ACTIVITIES SCHEDULE, AND EROSION / SEDIMENT CONTROL MEASURES LIST.



DITCH AREA PLAN, WEST



DITCH AREA PLAN, EAST



- LEGEND:**
- EXTG. EDGE LIGHT
  - EXTG. GUIDANCE SIGN
  - EXTG. ELECTRICAL CABLE
  - EXTG. STORM SEWER OR UNDERDRAIN
  - EXTG. INLET
  - EXTG. OUTLET
  - EXTG. MANHOLE
  - EXTG. DRAINAGE DITCH
  - PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS
  - EXISTING FENCE
  - EXISTING OR PROPOSED DRAINAGE SWALE
  - PROPOSED SILT FENCE
  - PROPOSED TEMPORARY STRAW BALE DITCH CHECK
  - PROPOSED SEDIMENT TRAP

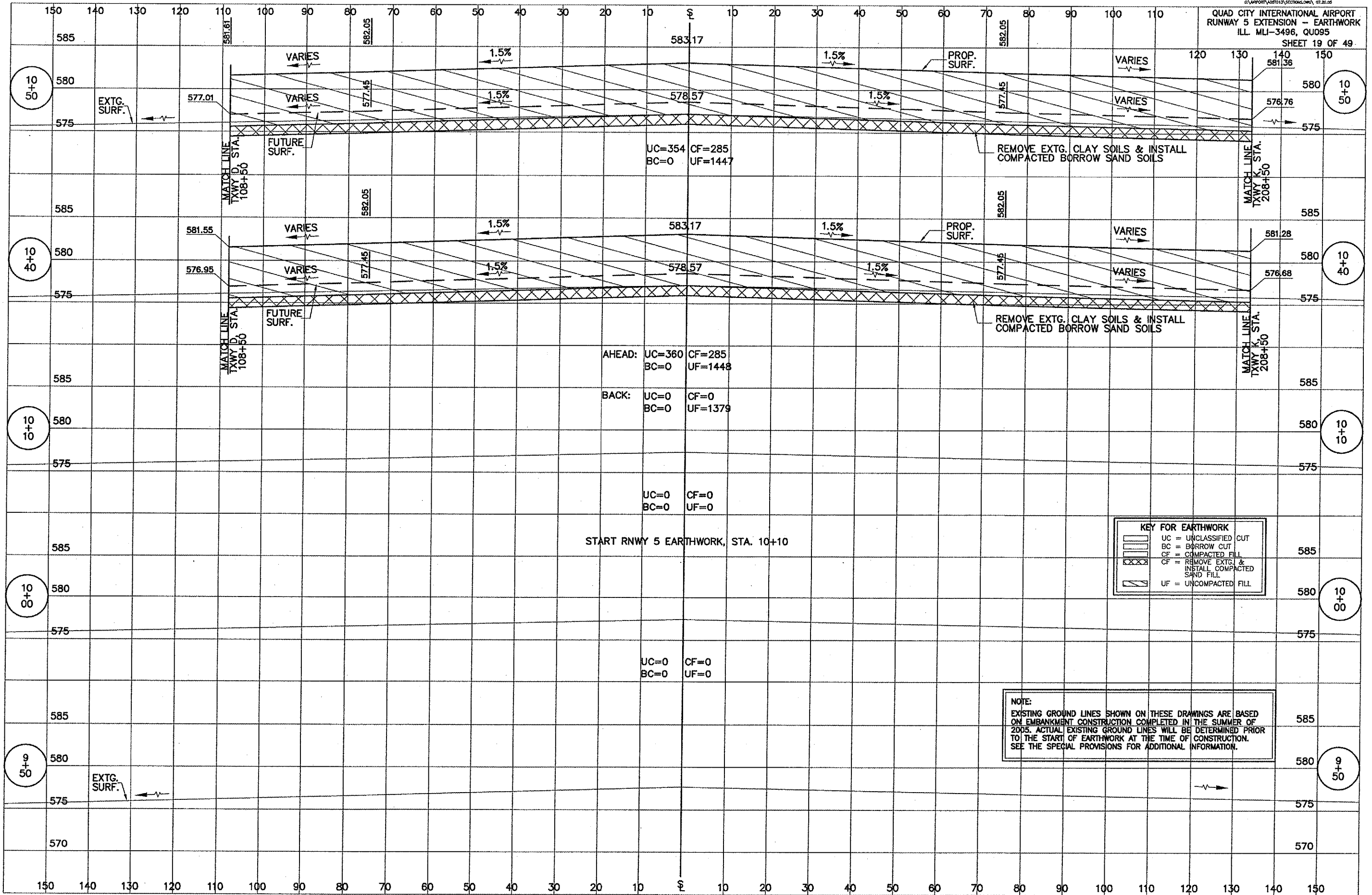
**NOTE:**  
 SEE SHEET 17 FOR STORM WATER POLLUTION PREVENTION GENERAL NOTES.

STORM WATER MANAGERS					
	NAME	ADDRESS	TELEPHONE NUMBER		SIGNATURE
			WORK	HOME	
CONTRACTOR					
SUBCONTRACTOR					
SUBCONTRACTOR					

ANTICIPATED PROBABLE CONSTRUCTION ACTIVITIES SCHEDULE													
NO.	ITEM DESCRIPTION	WEEK											
		1	2	3	4	5	6	7	8	9	10	11	12
1	INSTALL SEDIMENT/EROSION CONTROLS	█											
2	CLEARING AND GRADING		█	█	█	█	█	█	█	█	█	█	█
3	EXCAVATION AND EMBANKMENT			█	█	█	█	█	█	█	█	█	█
4	TURFING									█	█	█	█
5	MAINTAIN SEDIMENT/EROSION CONTROL	█	█	█	█	█	█	█	█	█	█	█	█
6	CLEAN-UP											█	█

EROSION AND SEDIMENT CONTROL MEASURES		
ITEM	LOCATION	SPECIAL PROVISION/PAY ITEM
TEMPORARY DITCH CHECKS	RUNWAY 5, STA. 8+24, RT. AND TAXIWAY D, 113+09, LT.	156500
TEMPORARY SILT FENCE	AT ALL SEDIMENT TRAP & DITCH CHECK LOCATIONS AND NORTH OF EXTG. DITCH, R-5 STA. 8+80 RT. TO T-K STA. 214+00 RT.; R-5 STA. 10+00 LT. & RT.; DITCH STA. 180+75 TO STA. 205+63 LT.; AND DITCH STA. 194+25 TO STA. 205+63 RT.	156500
TEMPORARY SEDIMENT TRAPS	TXWY. D, STA. 114+64, LT. & RT.; TXWY. D, STA. 116+50, LT.; TXWY. K, STA. 214+90, LT.; TXWY. H, STA. 435+62, LT.; AND TXWY. H, STA. 439+11, RT.	156500

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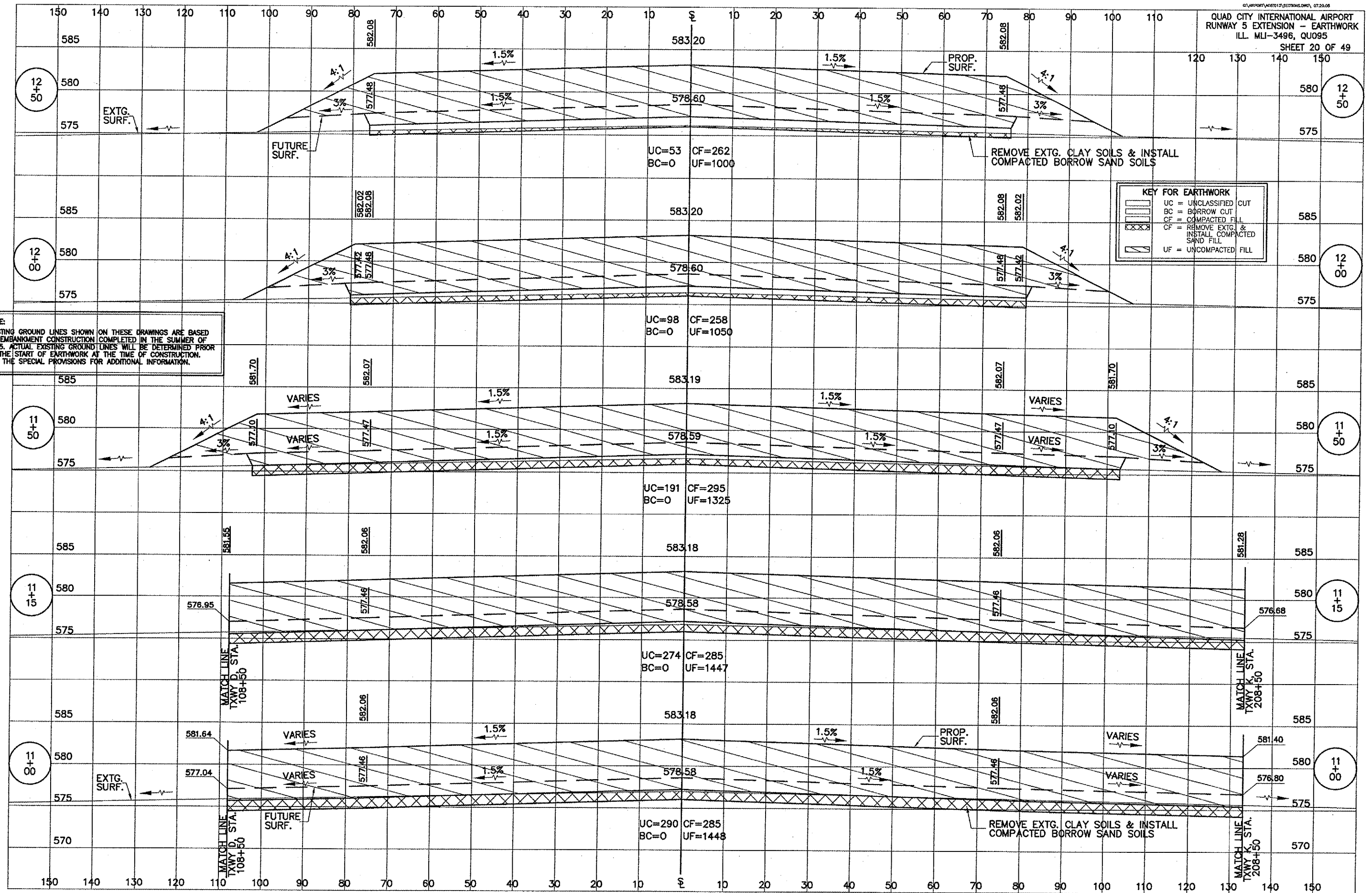


**KEY FOR EARTHWORK**

[Symbol]	UC = UNCLASSIFIED CUT
[Symbol]	BC = BORROW CUT
[Symbol]	CF = COMPACTED FILL
[Symbol]	CF = REMOVE EXTG. & INSTALL COMPACTED SAND FILL
[Symbol]	UF = UNCOMPACTED FILL

**NOTE:**  
EXISTING GROUND LINES SHOWN ON THESE DRAWINGS ARE BASED ON EMBANKMENT CONSTRUCTION COMPLETED IN THE SUMMER OF 2005. ACTUAL EXISTING GROUND LINES WILL BE DETERMINED PRIOR TO THE START OF EARTHWORK AT THE TIME OF CONSTRUCTION. SEE THE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.



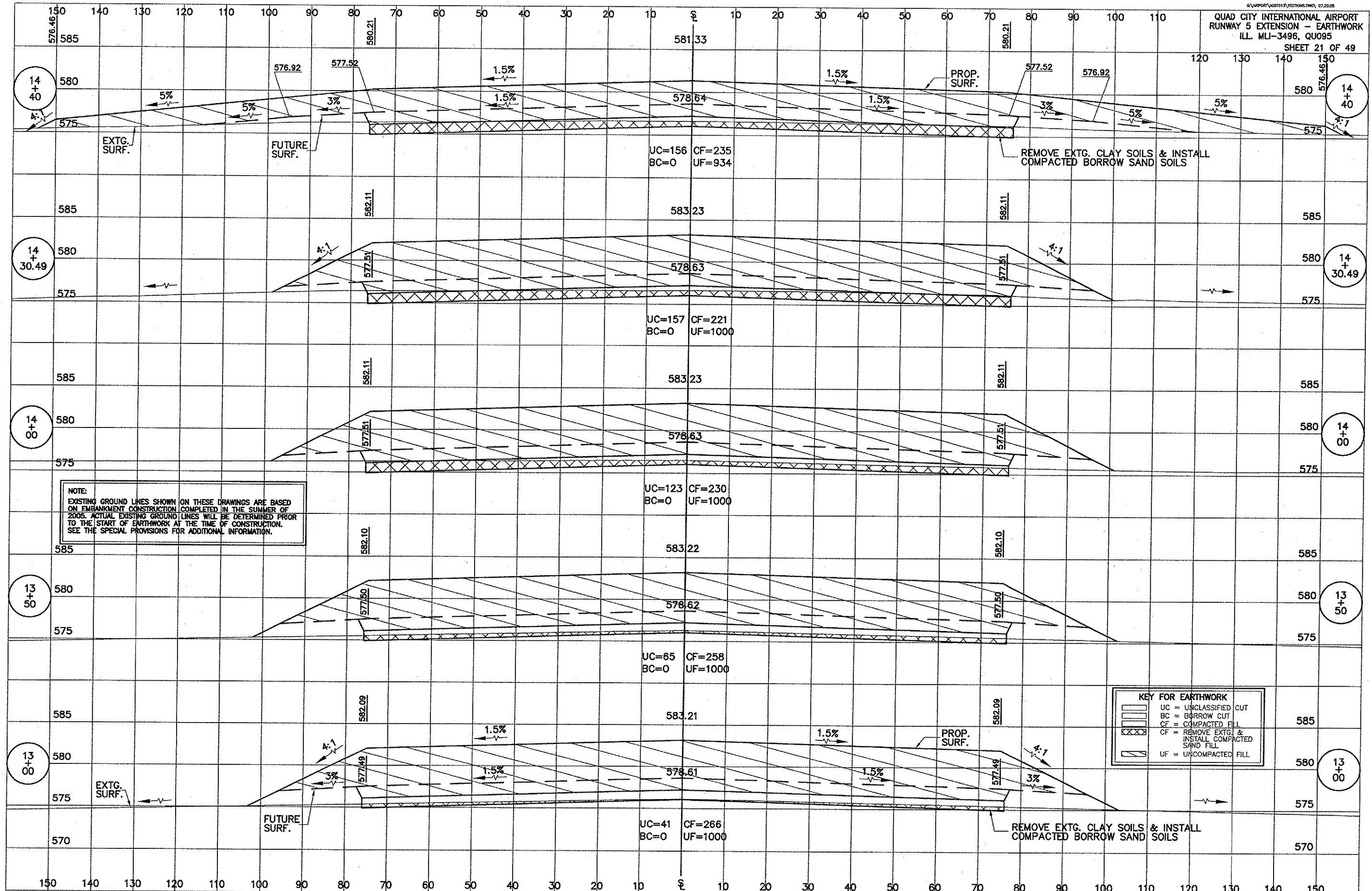


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	UF = UNCOMPACTED FILL
	REMOVE EXTG. & INSTALL COMPACTED SAND FILL

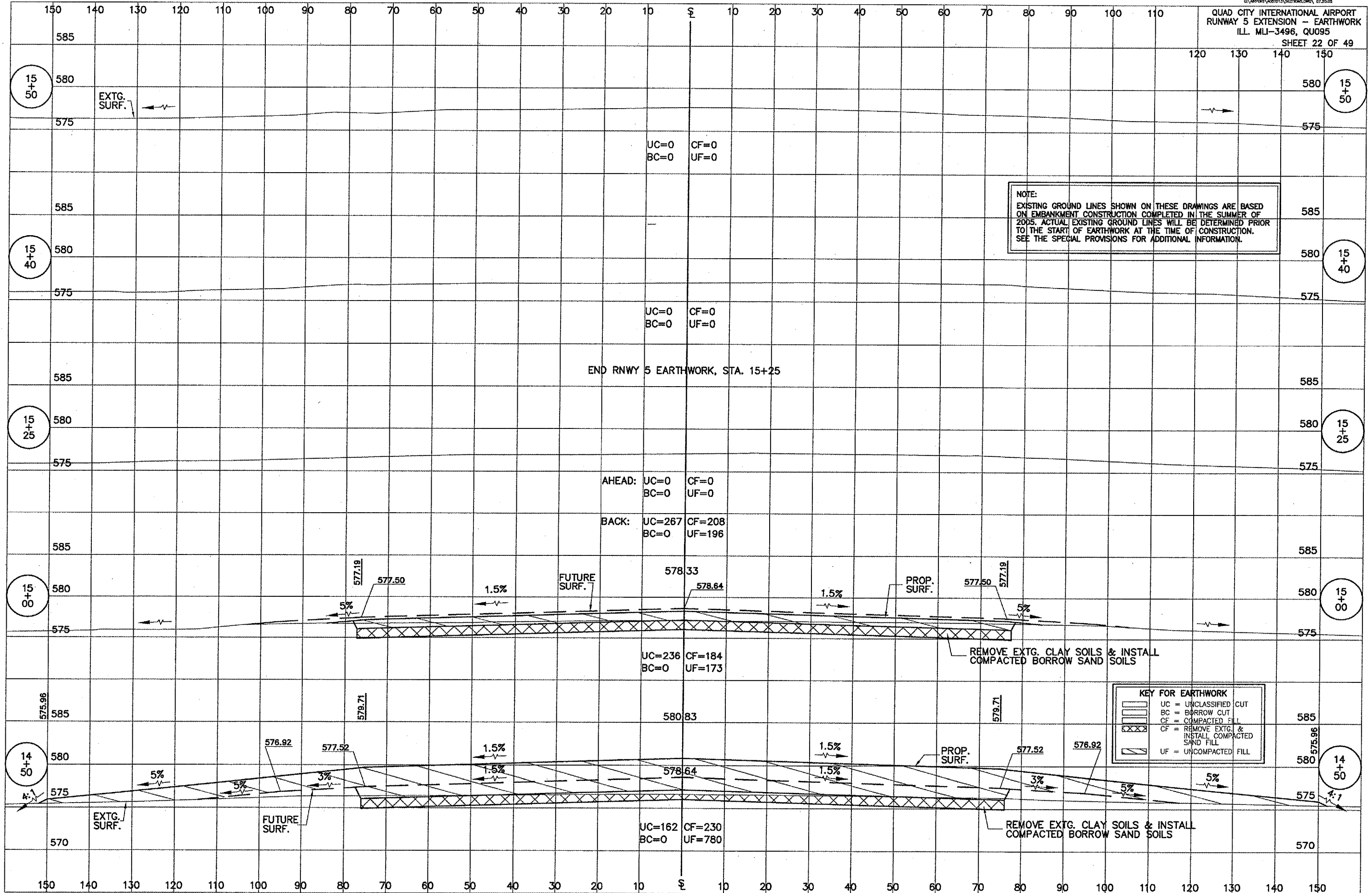
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NOTE:  
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KEY FOR EARTHWORK	
[Symbol: Dashed line]	UC = UNCLASSIFIED CUT
[Symbol: Horizontal lines]	BC = BORROW CUT
[Symbol: Diagonal lines]	CF = COMPACTED FILL
[Symbol: Cross-hatch]	CF = REMOVE EXTG. & INSTALL COMPACTED SAND FILL
[Symbol: Vertical lines]	UF = UNCOMPACTED FILL

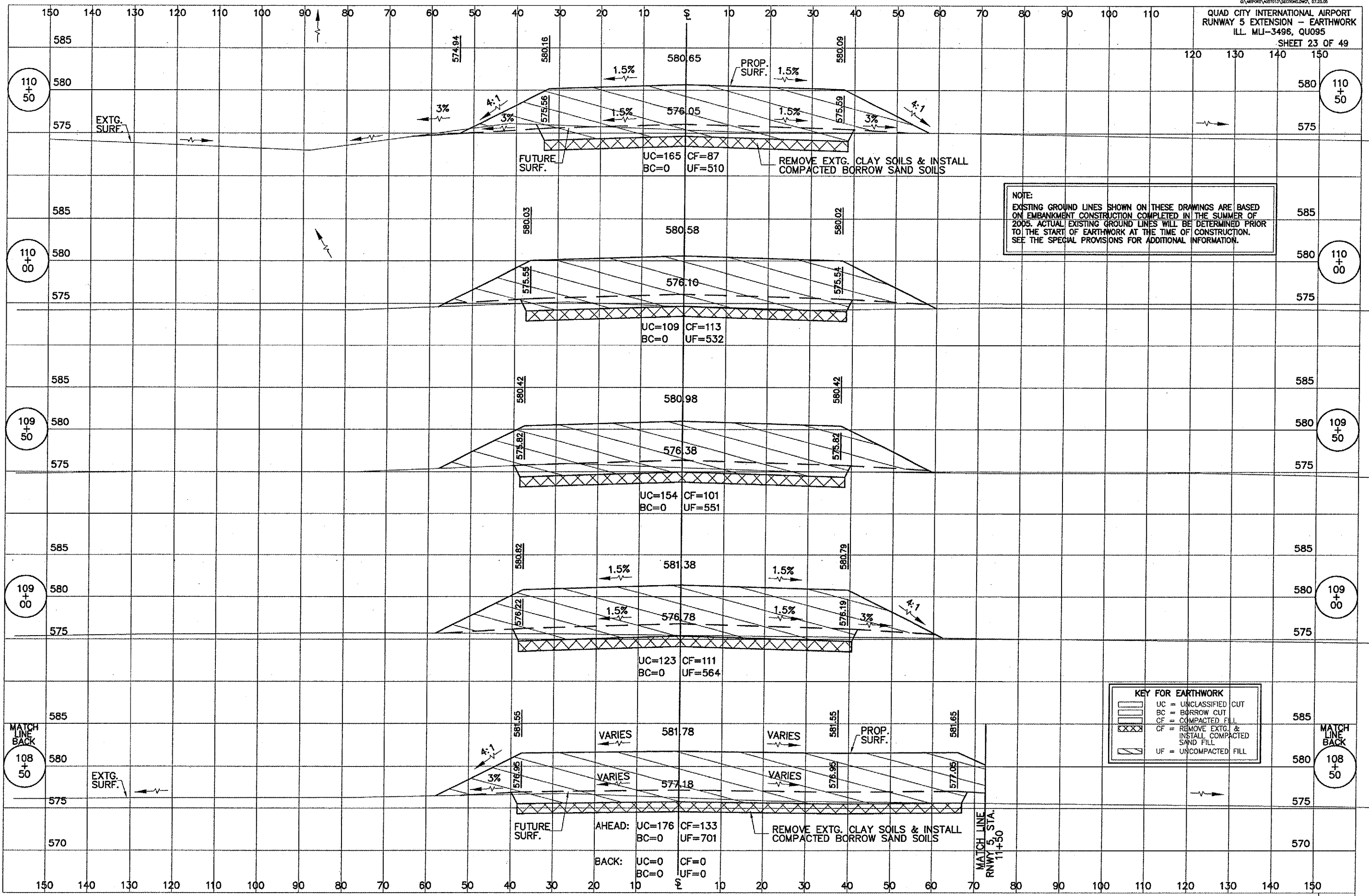


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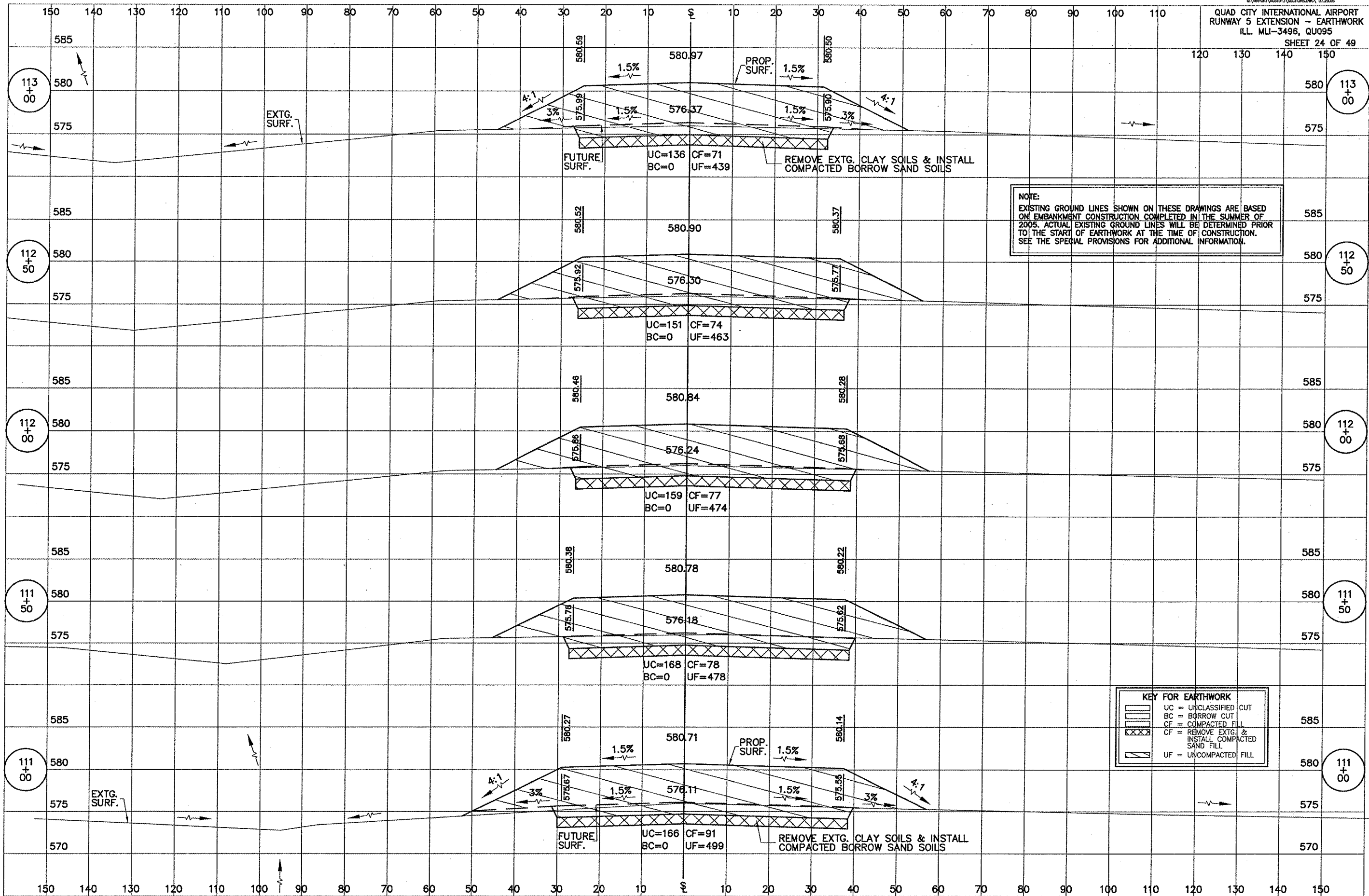
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NOTE:  
EXISTING GROUND LINES SHOWN ON THESE DRAWINGS ARE BASED ON EMBANKMENT CONSTRUCTION COMPLETED IN THE SUMMER OF 2005. ACTUAL EXISTING GROUND LINES WILL BE DETERMINED PRIOR TO THE START OF EARTHWORK AT THE TIME OF CONSTRUCTION. SEE THE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

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[Symbol]	BC = BORROW CUT
[Symbol]	CF = COMPACTED FILL
[Symbol]	CF = REMOVE EXTG. & INSTALL COMPACTED SAND FILL
[Symbol]	UF = UNCOMPACTED FILL

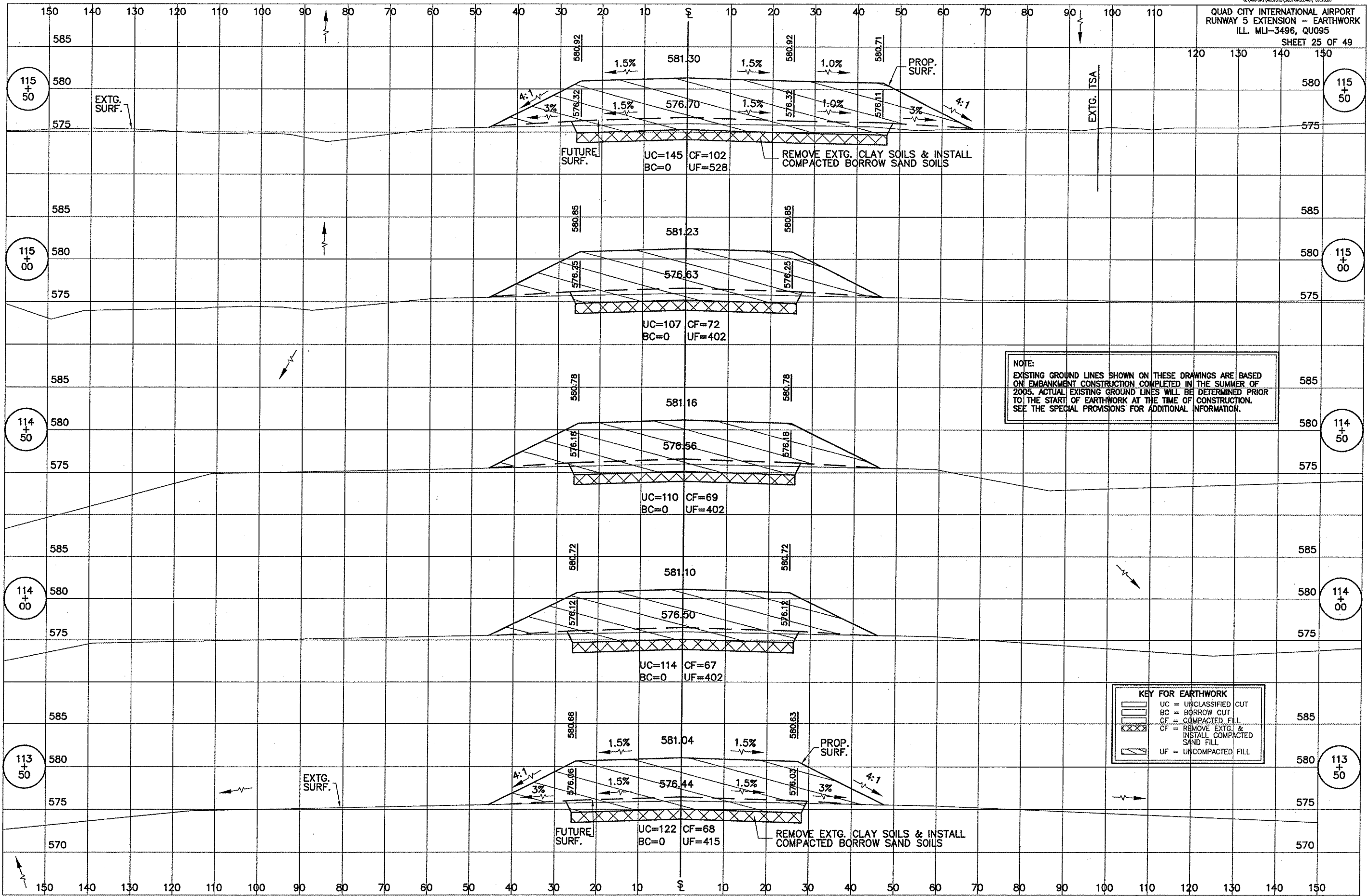
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**KEY FOR EARTHWORK**

	UC = UNCLASSIFIED CUT
	BC = BORROW CUT
	CF = COMPACTED FILL
	CF = REMOVE EXTG. & INSTALL COMPACTED SAND FILL
	UF = UNCOMPACTED FILL

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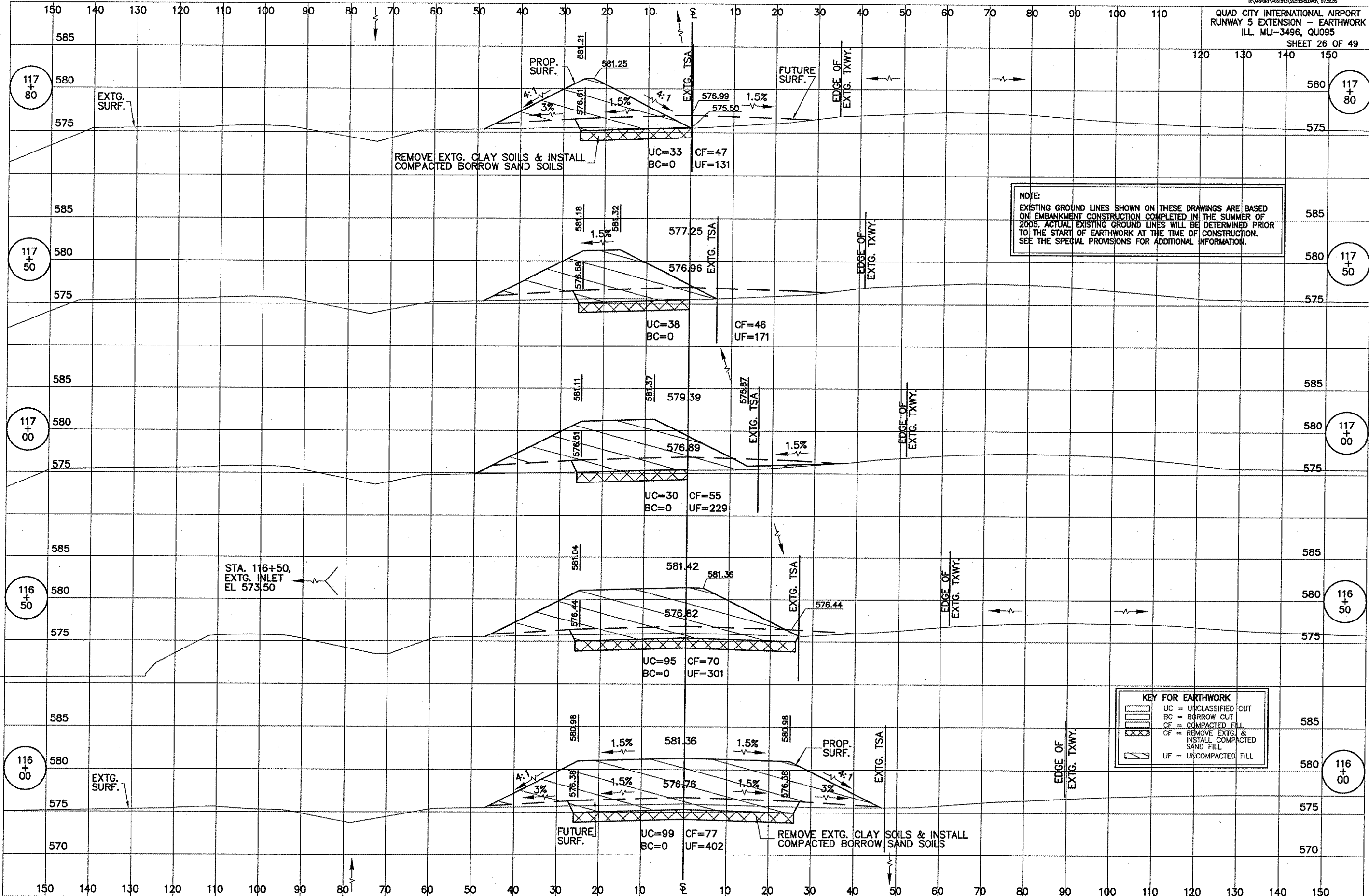
NOTE:  
EXISTING GROUND LINES SHOWN ON THESE DRAWINGS ARE BASED ON EMBANKMENT CONSTRUCTION COMPLETED IN THE SUMMER OF 2005. ACTUAL EXISTING GROUND LINES WILL BE DETERMINED PRIOR TO THE START OF EARTHWORK AT THE TIME OF CONSTRUCTION. SEE THE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

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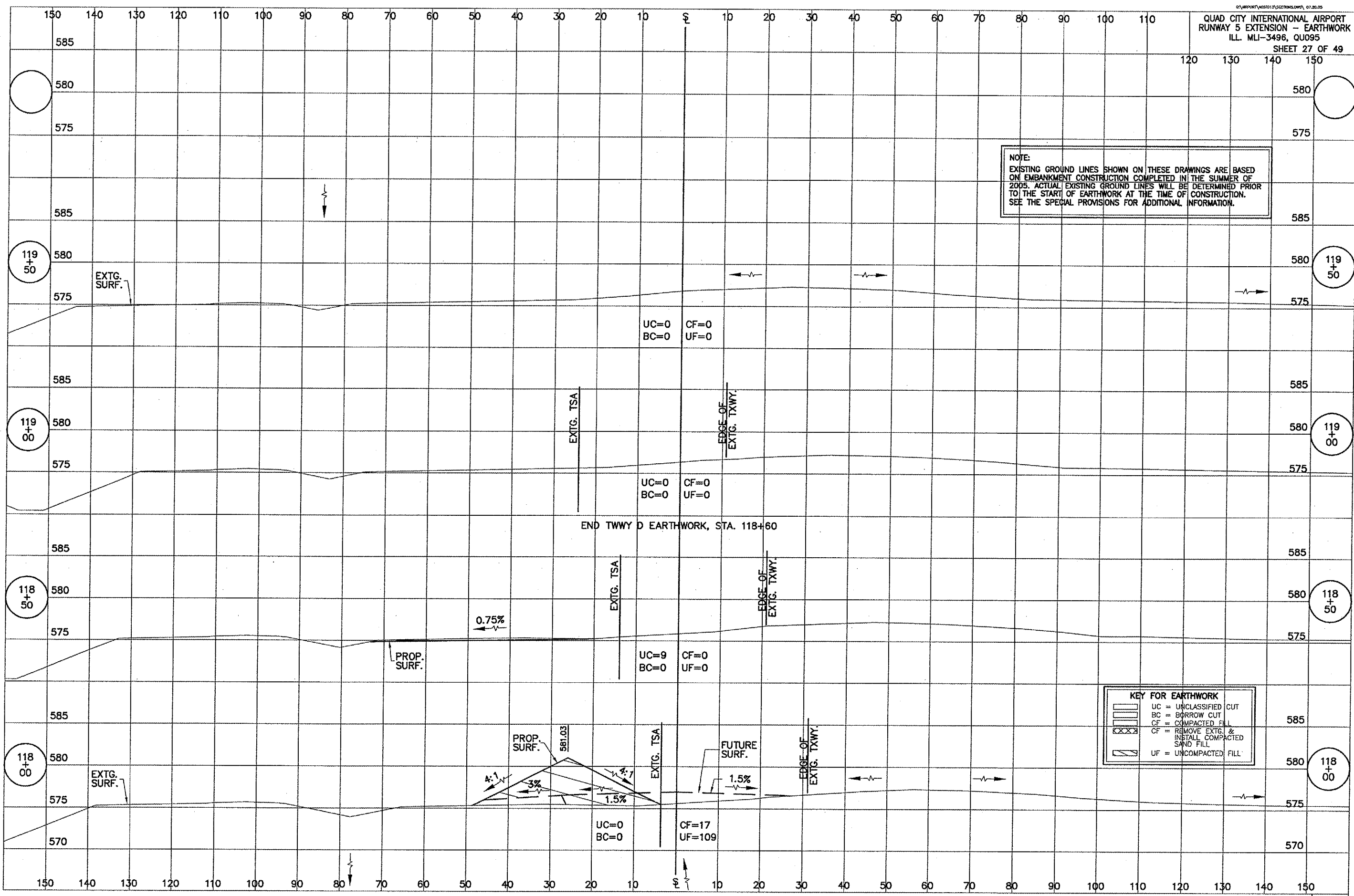
NOTE:  
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[Symbol]	BC = BORROW CUT
[Symbol]	CF = COMPACTED FILL
[Symbol]	CF = REMOVE EXTG. & INSTALL COMPACTED SAND FILL
[Symbol]	UF = UNCOMPACTED FILL

STA. 116+50,  
EXTG. INLET  
EL 573.50

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NOTE:  
EXISTING GROUND LINES SHOWN ON THESE DRAWINGS ARE BASED ON EMBANKMENT CONSTRUCTION COMPLETED IN THE SUMMER OF 2005. ACTUAL EXISTING GROUND LINES WILL BE DETERMINED PRIOR TO THE START OF EARTHWORK AT THE TIME OF CONSTRUCTION. SEE THE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

KEY FOR EARTHWORK	
[Symbol: Horizontal lines]	UC = UNCLASSIFIED CUT
[Symbol: Vertical lines]	BC = BORROW CUT
[Symbol: Diagonal lines /]	CF = COMPACTED FILL
[Symbol: Diagonal lines \]	CF = REMOVE EXTG. & INSTALL COMPACTED SAND FILL
[Symbol: Dotted pattern]	UF = UNCOMPACTED FILL

END TWY D EARTHWORK, STA. 118+60

0.75%

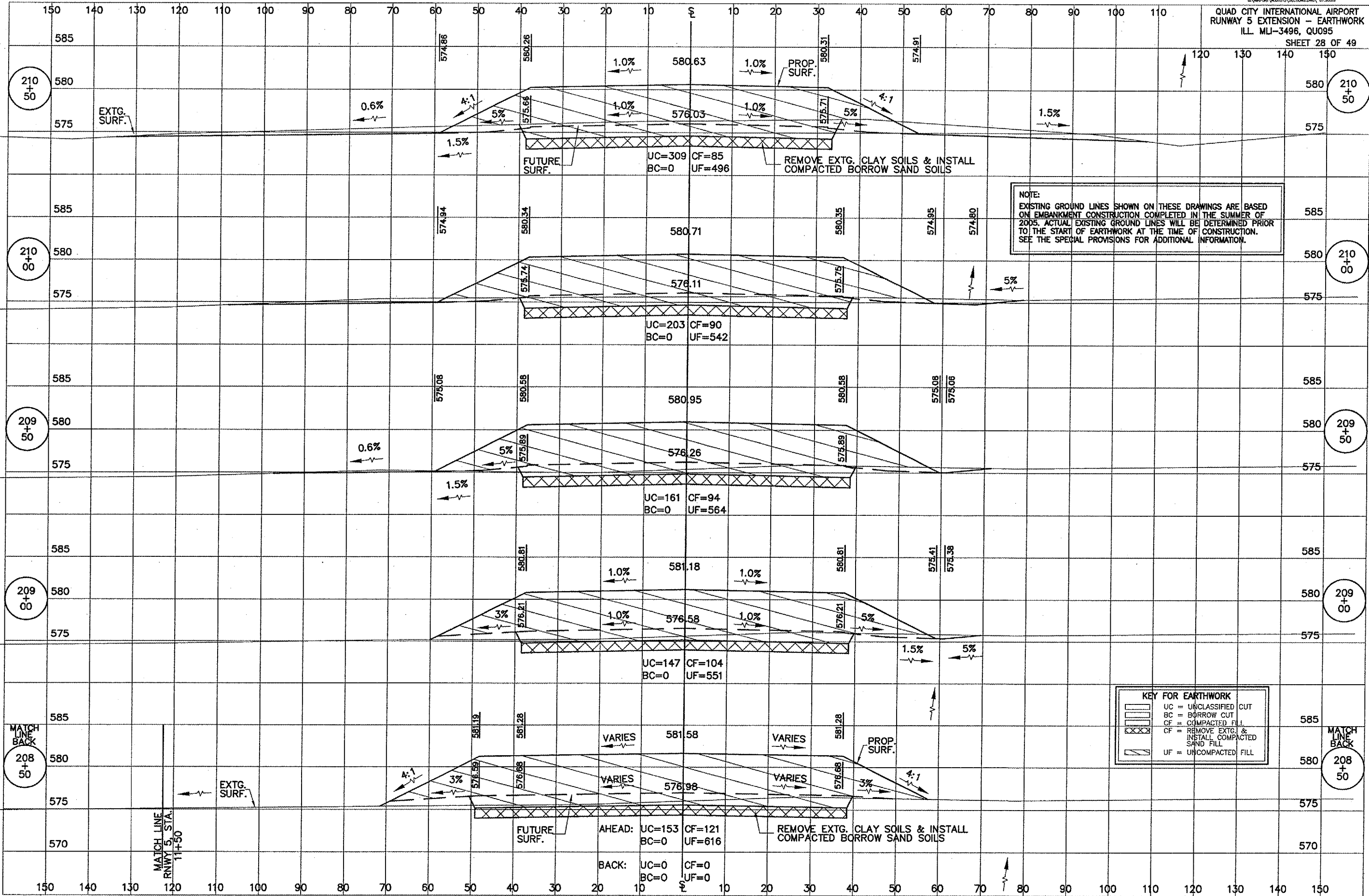
PROP. SURF.

PROP. SURF.

FUTURE SURF.

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BC=0

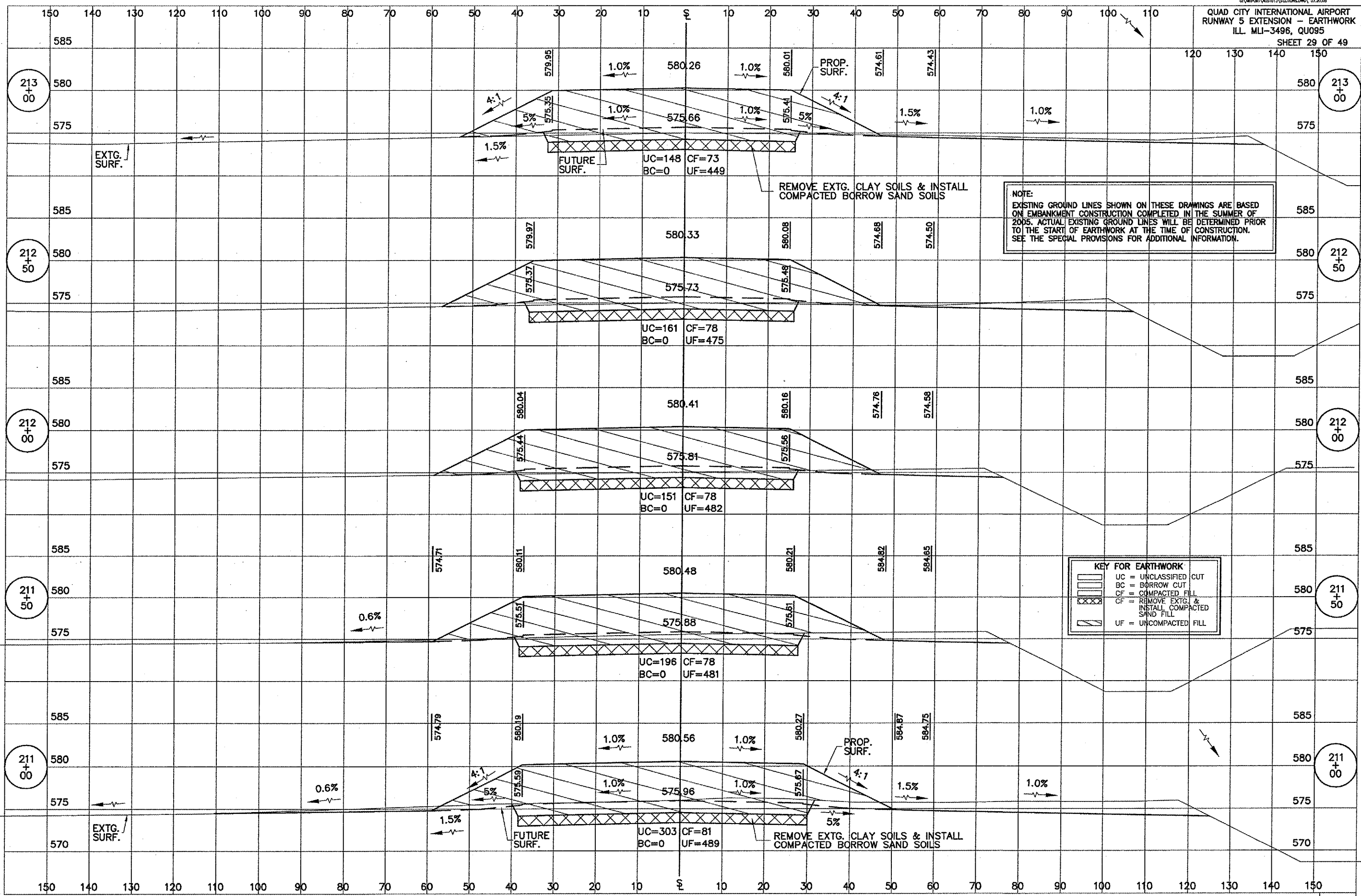
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UF=109



**KEY FOR EARTHWORK**

[Symbol]	UC = UNCLASSIFIED CUT
[Symbol]	BC = BORROW CUT
[Symbol]	CF = COMPACTED FILL
[Symbol]	REMOVE EXTG. & INSTALL COMPACTED SAND FILL
[Symbol]	UF = UNCOMPACTED FILL

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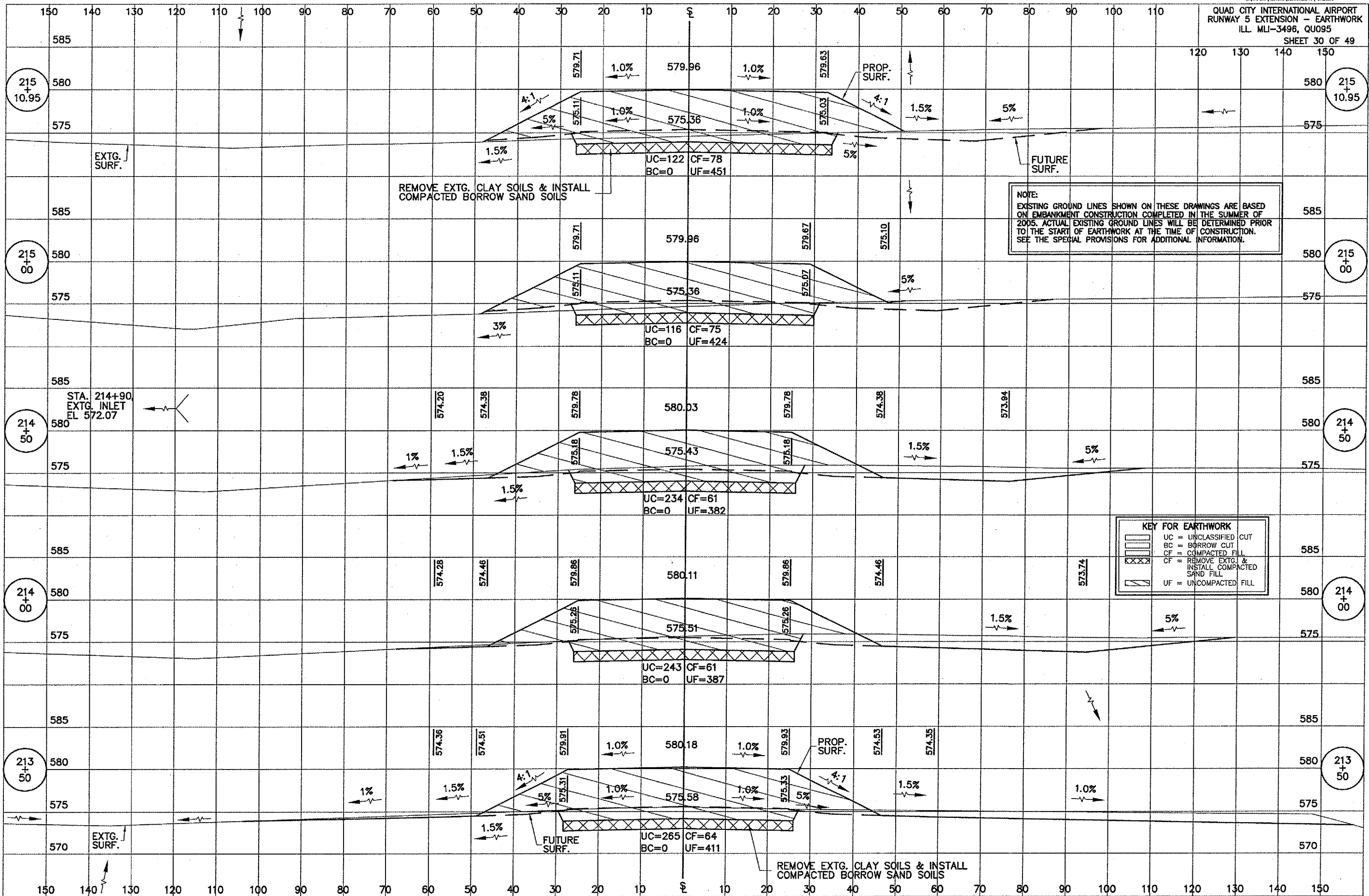


NOTE:  
EXISTING GROUND LINES SHOWN ON THESE DRAWINGS ARE BASED ON EMBANKMENT CONSTRUCTION COMPLETED IN THE SUMMER OF 2005. ACTUAL EXISTING GROUND LINES WILL BE DETERMINED PRIOR TO THE START OF EARTHWORK AT THE TIME OF CONSTRUCTION. SEE THE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

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[Symbol]	BC = BORROW CUT
[Symbol]	CF = COMPACTED FILL
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[Symbol]	UF = UNCOMPACTED FILL

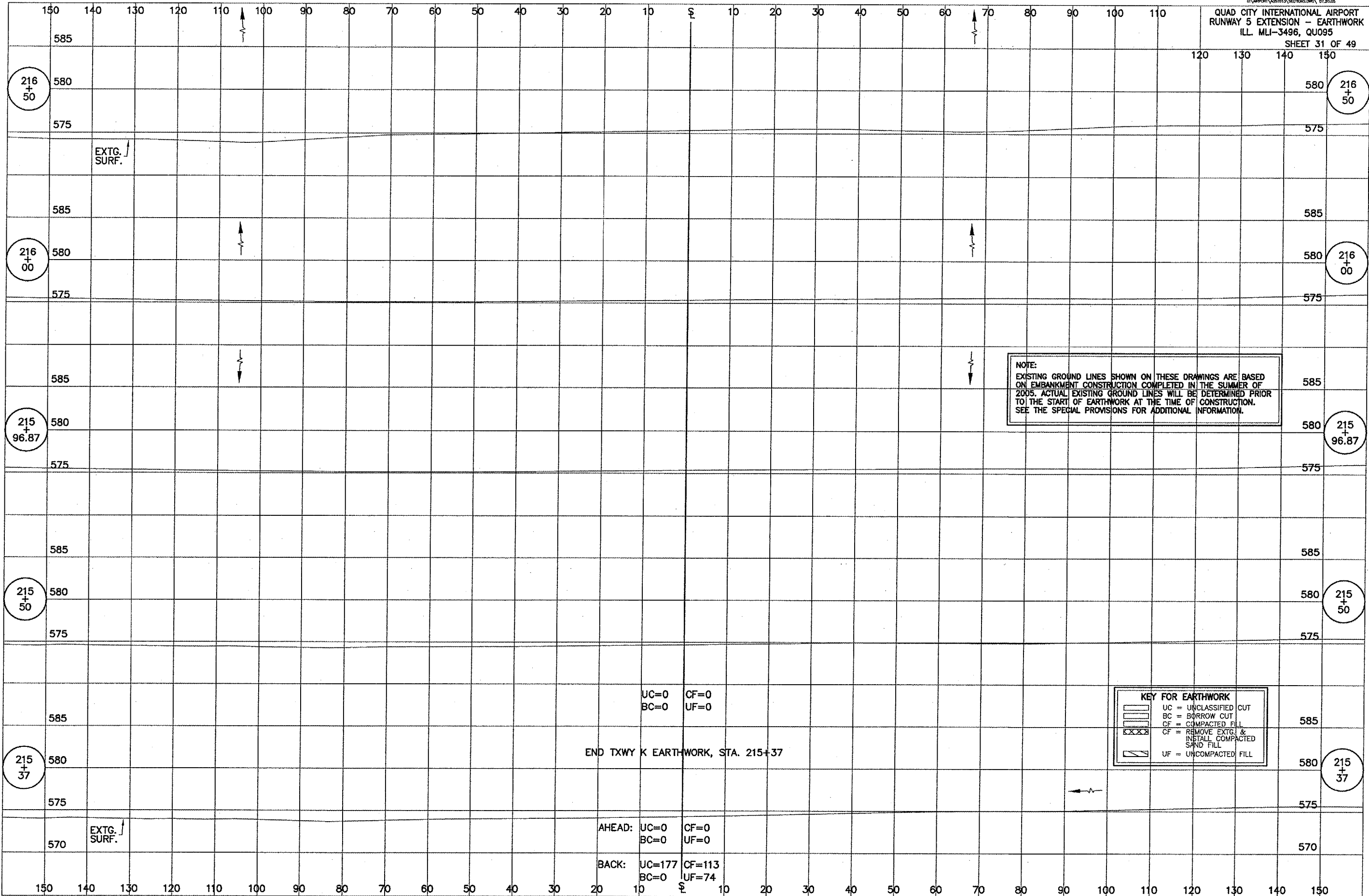
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[Symbol]	UC = UNCLASSIFIED CUT
[Symbol]	BC = BORROW CUT
[Symbol]	CF = COMPACTED FILL
[Symbol]	UF = UNCOMPACTED FILL
[Symbol]	REMOVE EXTG. & INSTALL COMPACTED SAND FILL

TAXIWAY K CROSS SECTIONS, STA 213+50 TO STA 215+10.95

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NOTE:  
EXISTING GROUND LINES SHOWN ON THESE DRAWINGS ARE BASED ON EMBANKMENT CONSTRUCTION COMPLETED IN THE SUMMER OF 2005. ACTUAL EXISTING GROUND LINES WILL BE DETERMINED PRIOR TO THE START OF EARTHWORK AT THE TIME OF CONSTRUCTION. SEE THE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

**KEY FOR EARTHWORK**

- UC = UNCLASSIFIED CUT
- BC = BORROW CUT
- CF = COMPACTED FILL
- XXXX = REMOVE EXTG. & INSTALL COMPACTED SAND FILL
- UF = UNCOMPACTED FILL

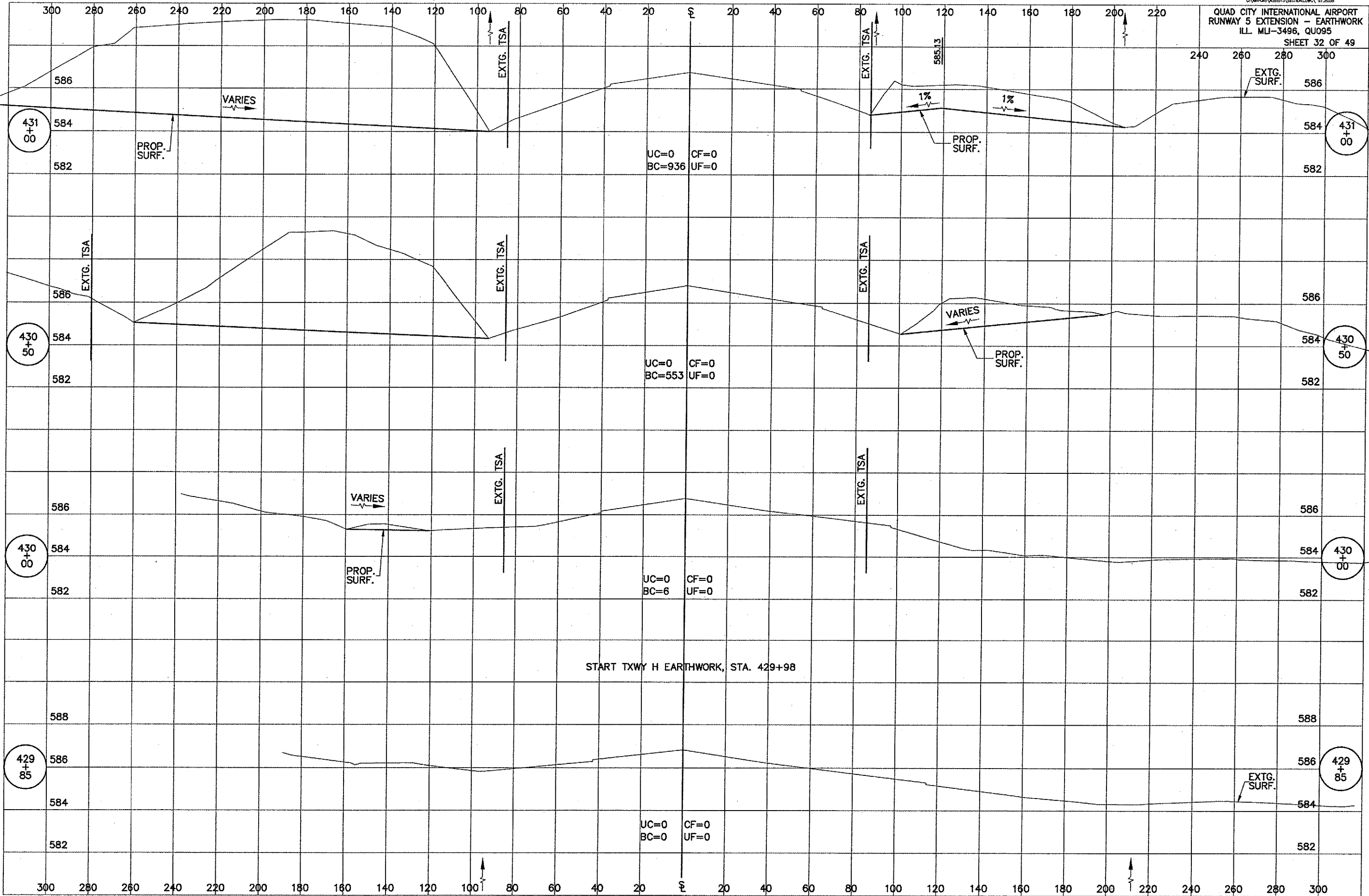
UC=0 CF=0  
BC=0 UF=0

END TXWY K EARTHWORK, STA. 215+37

AHEAD: UC=0 CF=0  
BC=0 UF=0

BACK: UC=177 CF=113  
BC=0 UF=74

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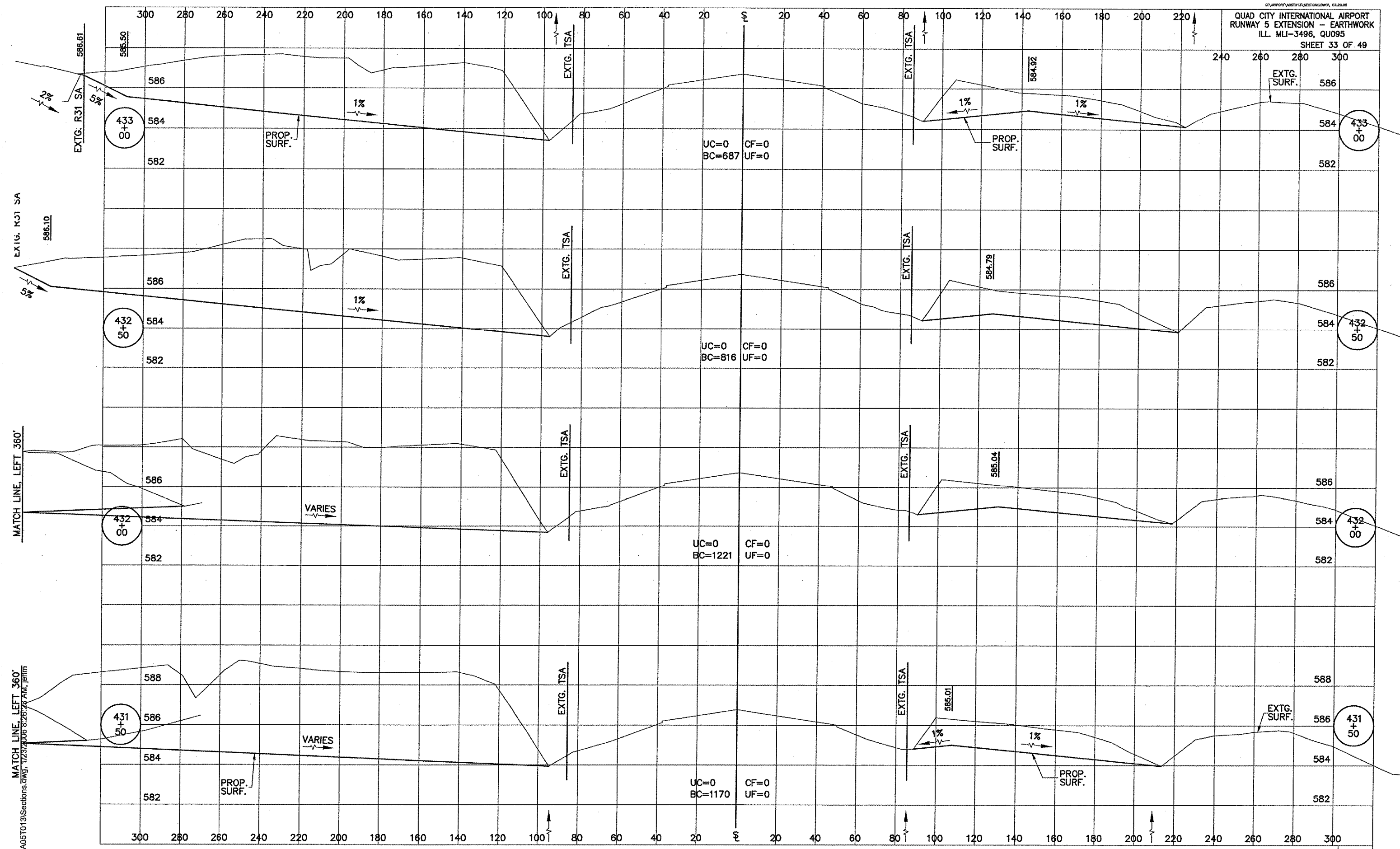


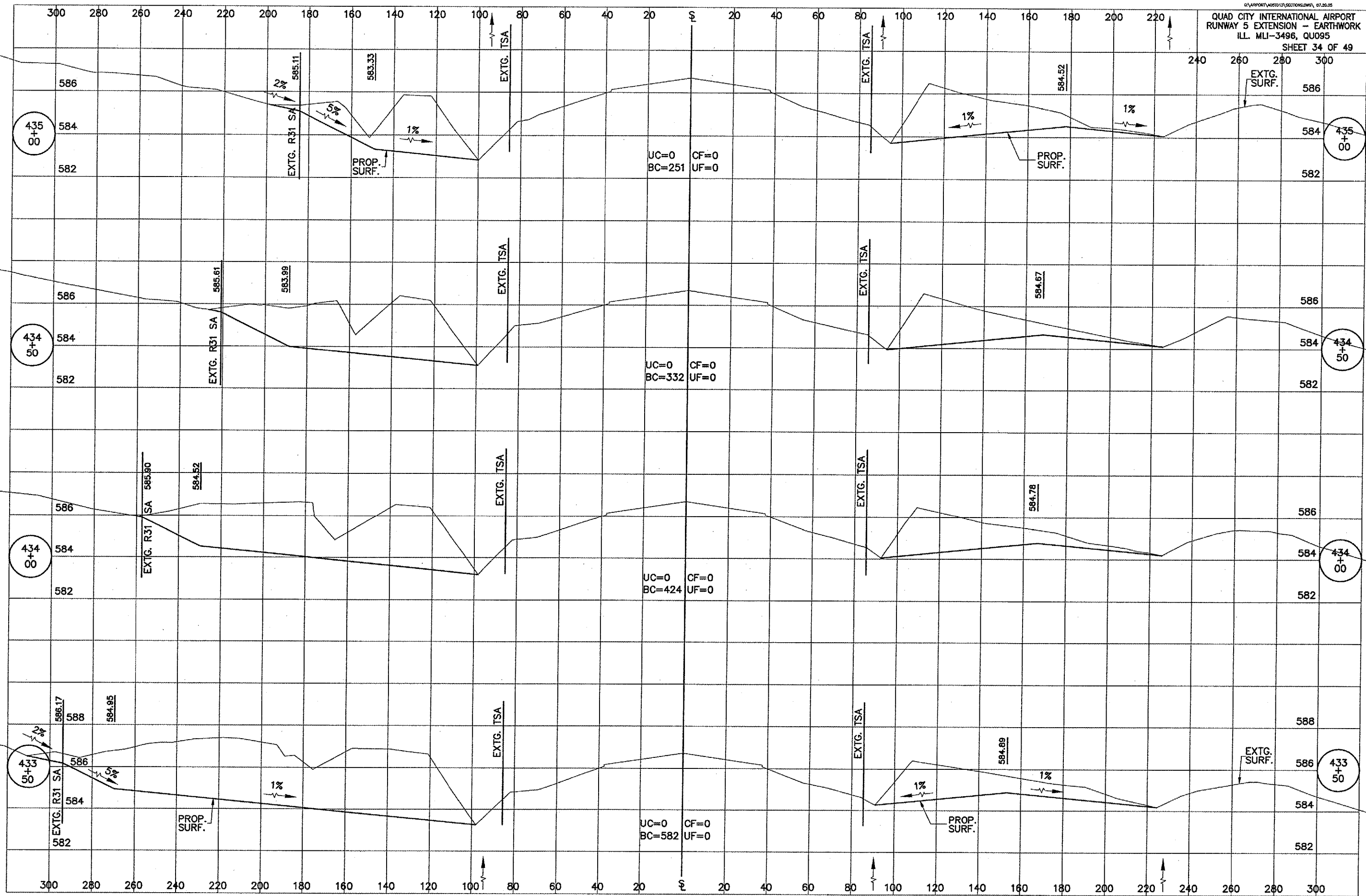
TAXIWAY H CROSS SECTIONS, STA 429+85 TO STA 431+00

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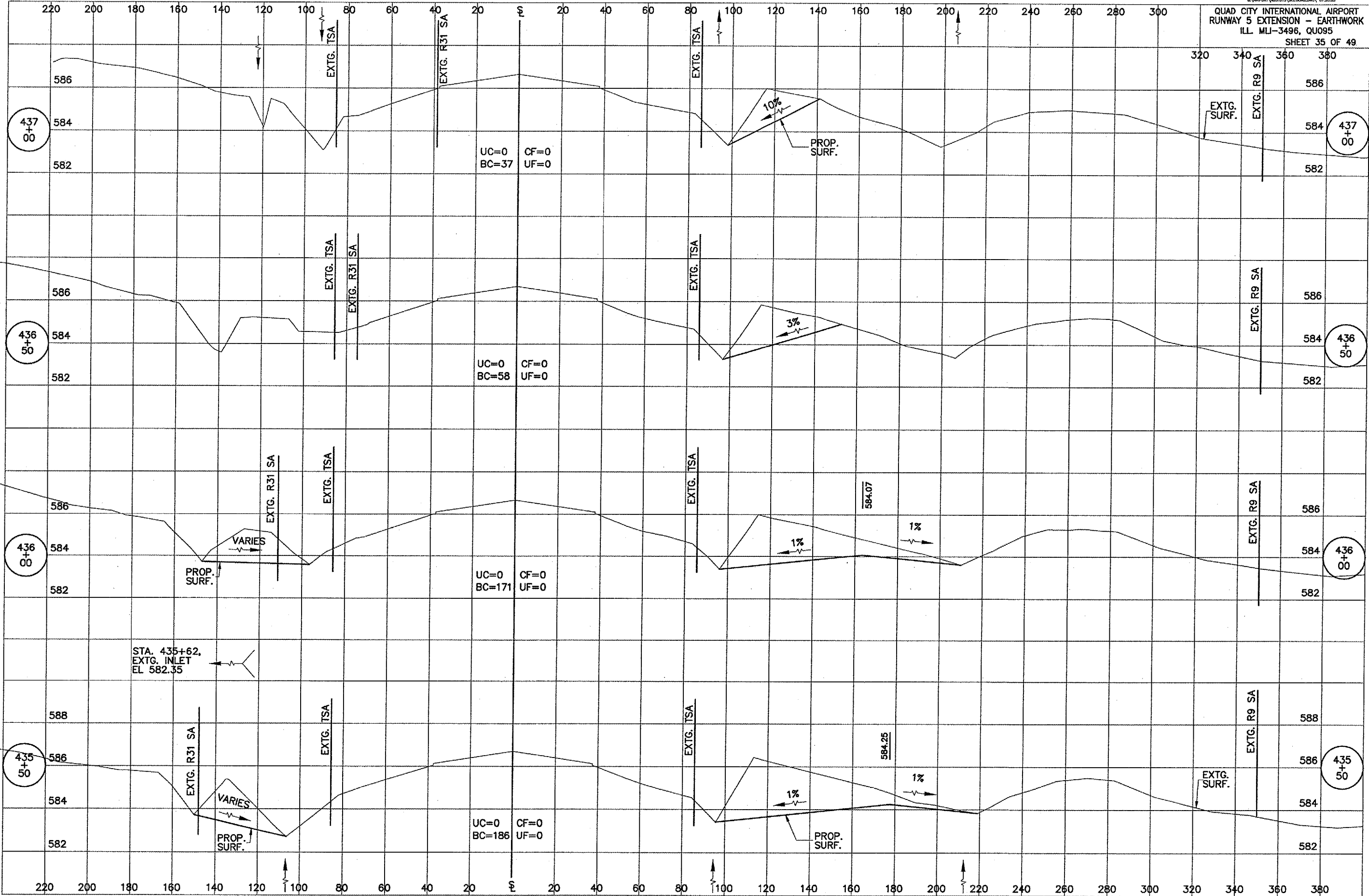


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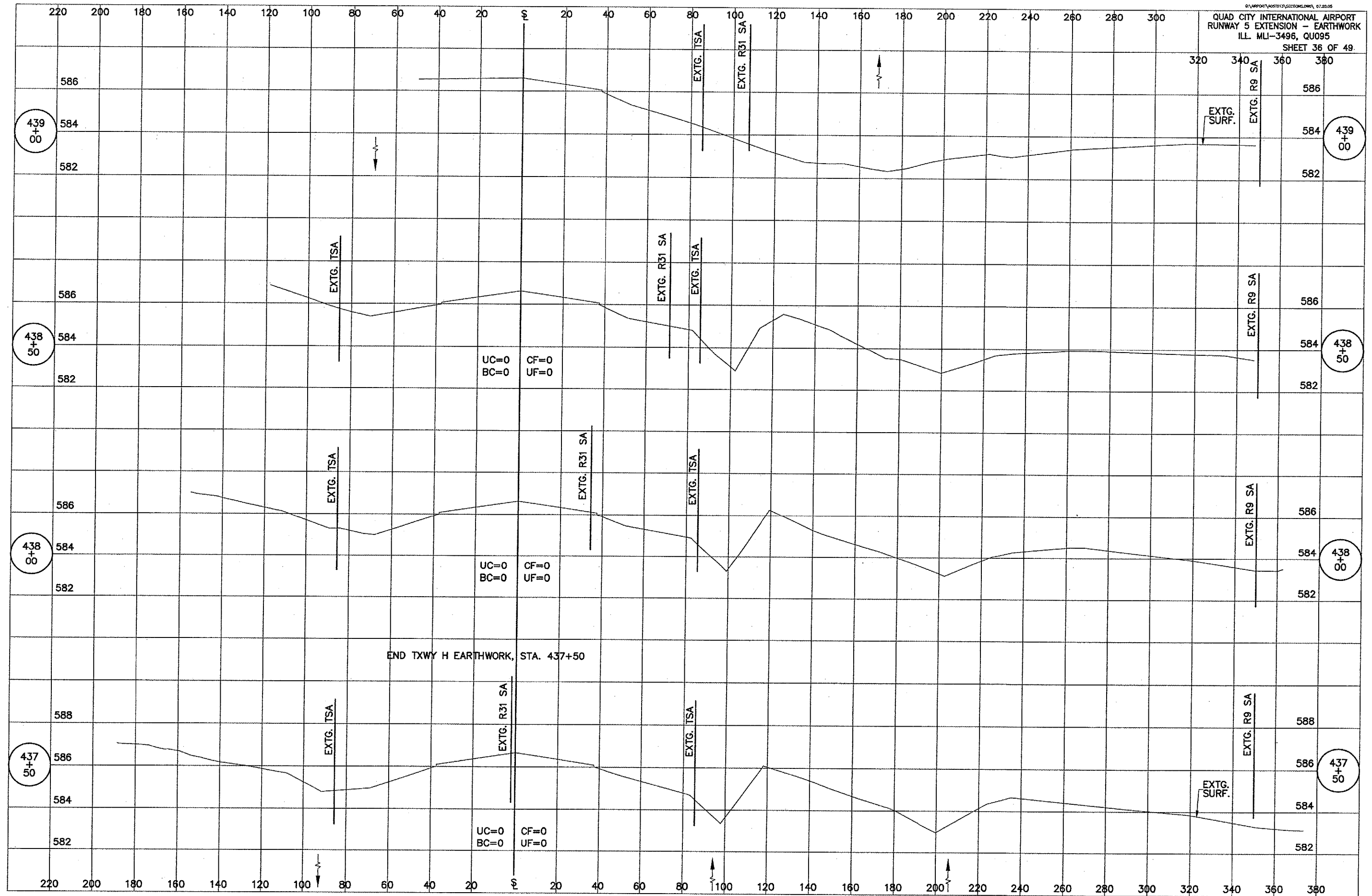




TAXIWAY H CROSS SECTIONS, STA 433+50 TO STA 435+00

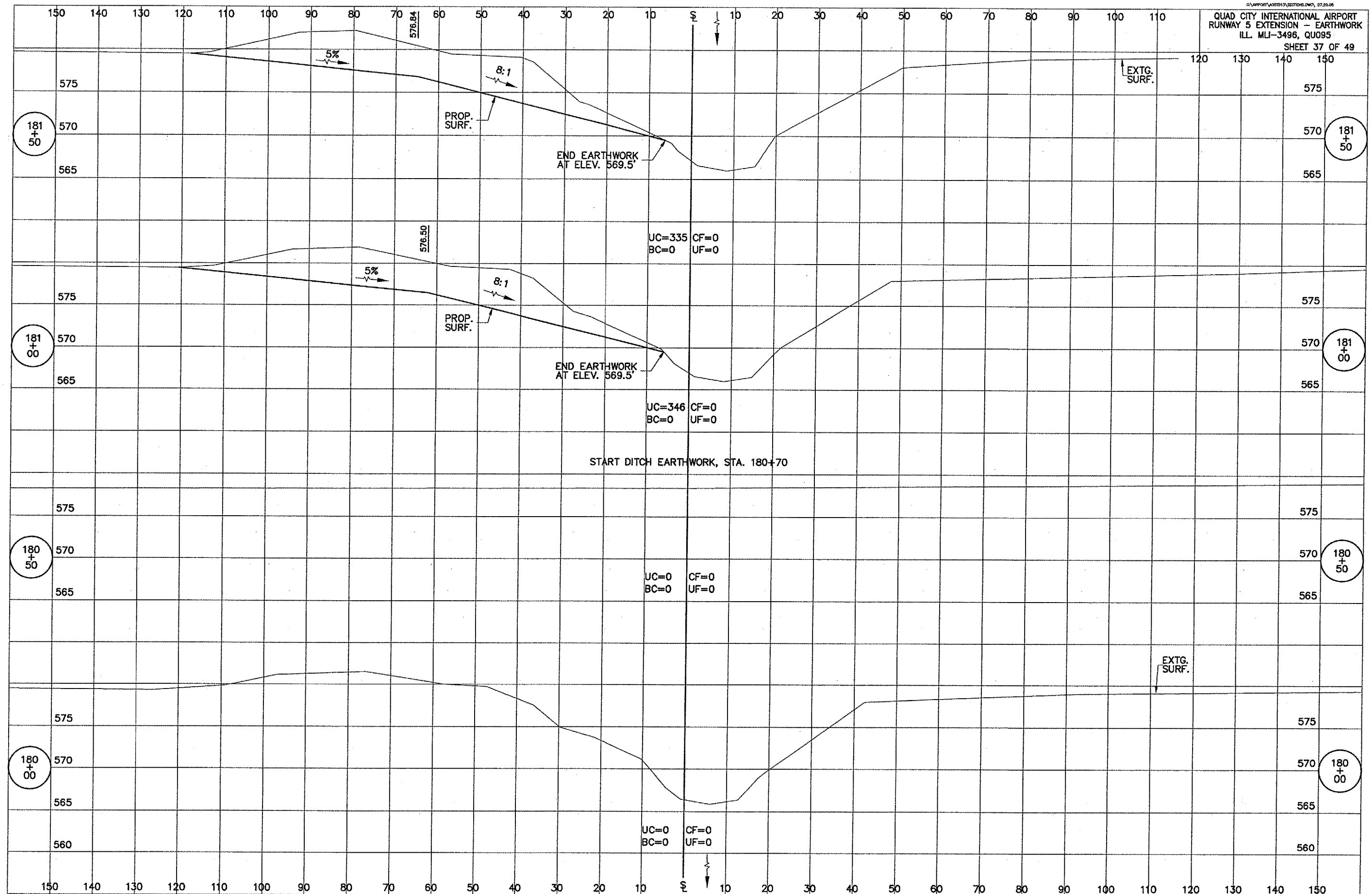


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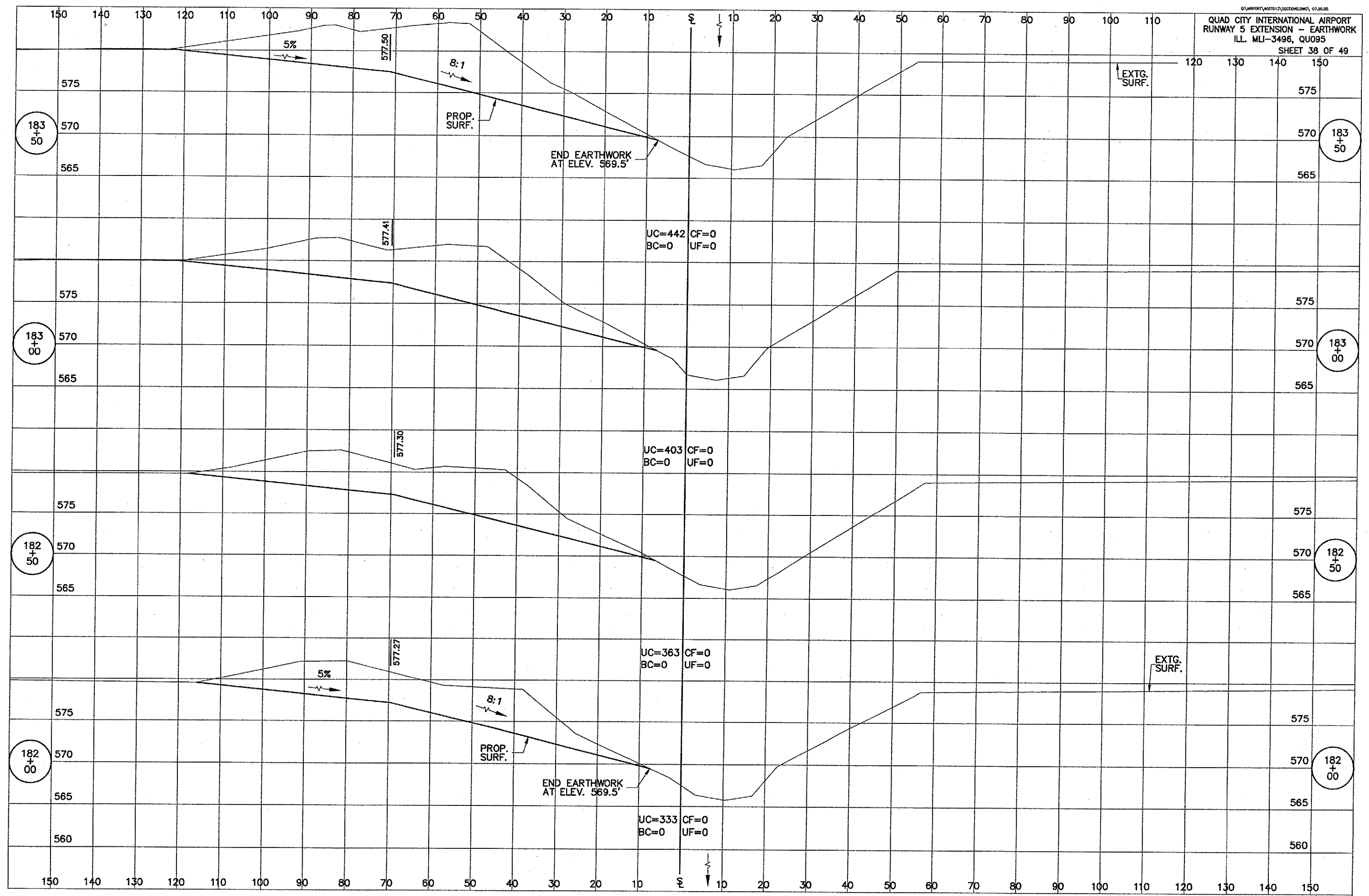


TAXIWAY H CROSS SECTIONS, STA 437+50 TO STA 439+00

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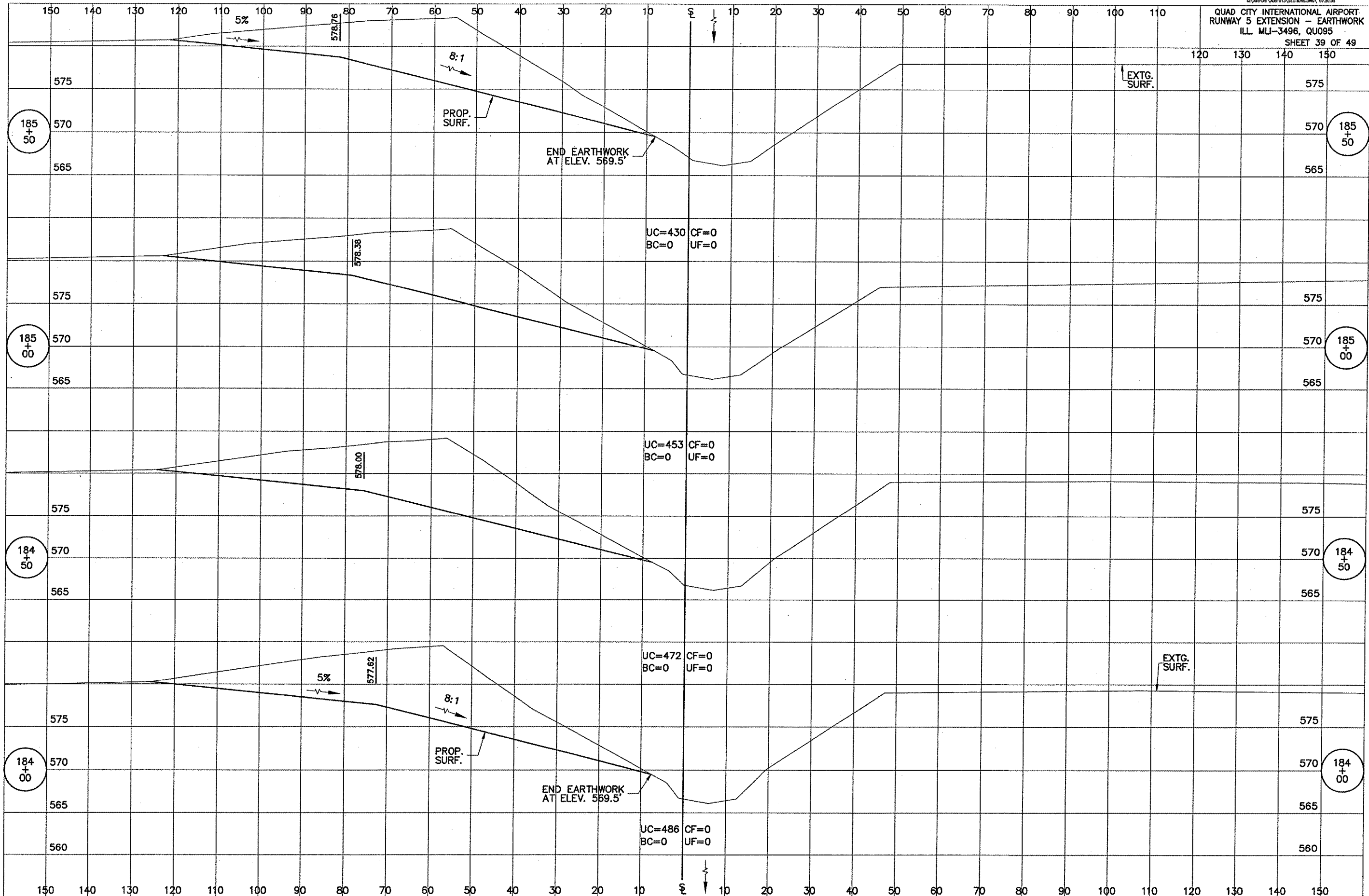


DITCH CROSS SECTIONS, STA 180+00 TO STA 181+50



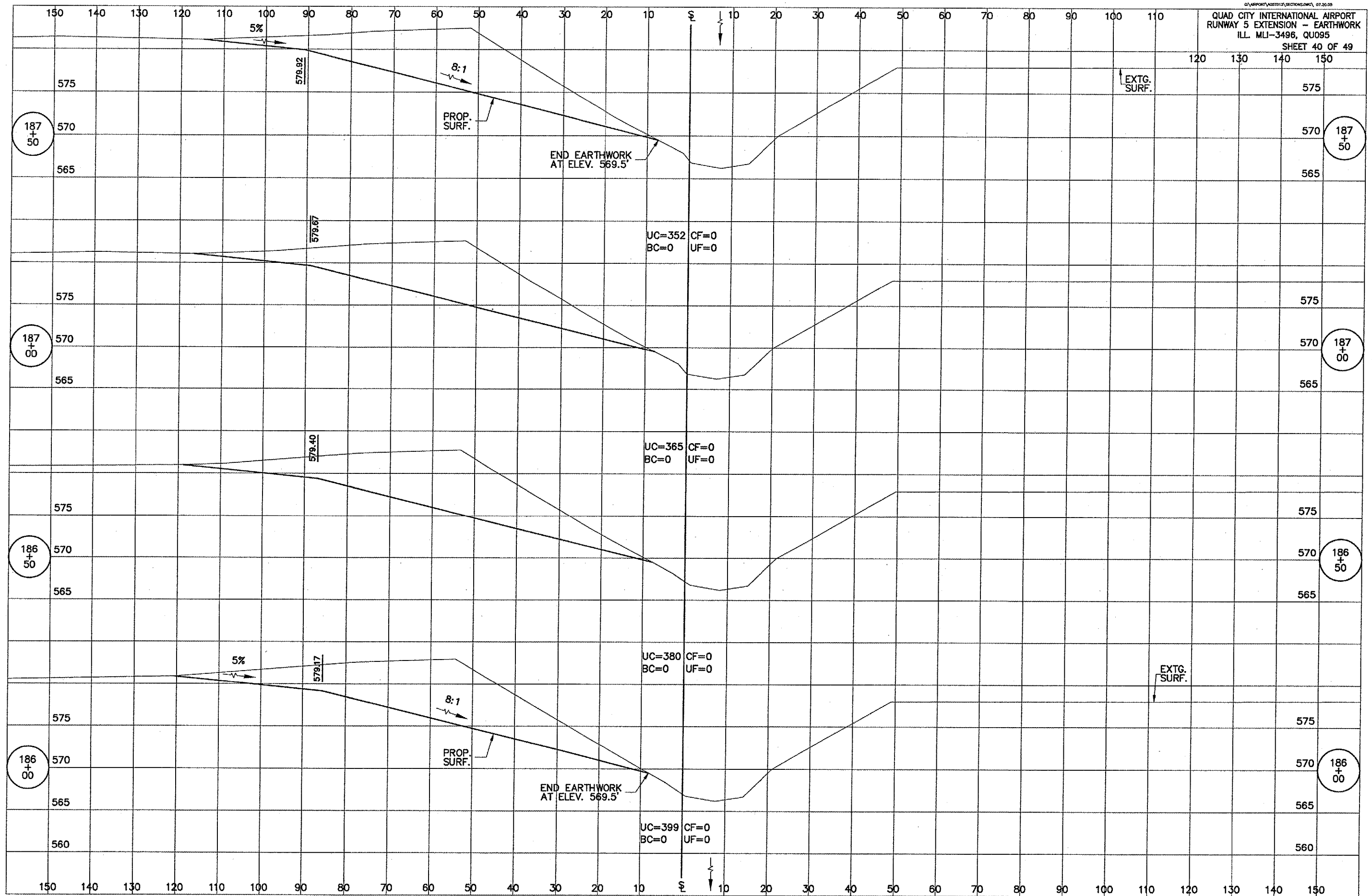
DITCH CROSS SECTIONS, STA 182+00 TO STA 183+50





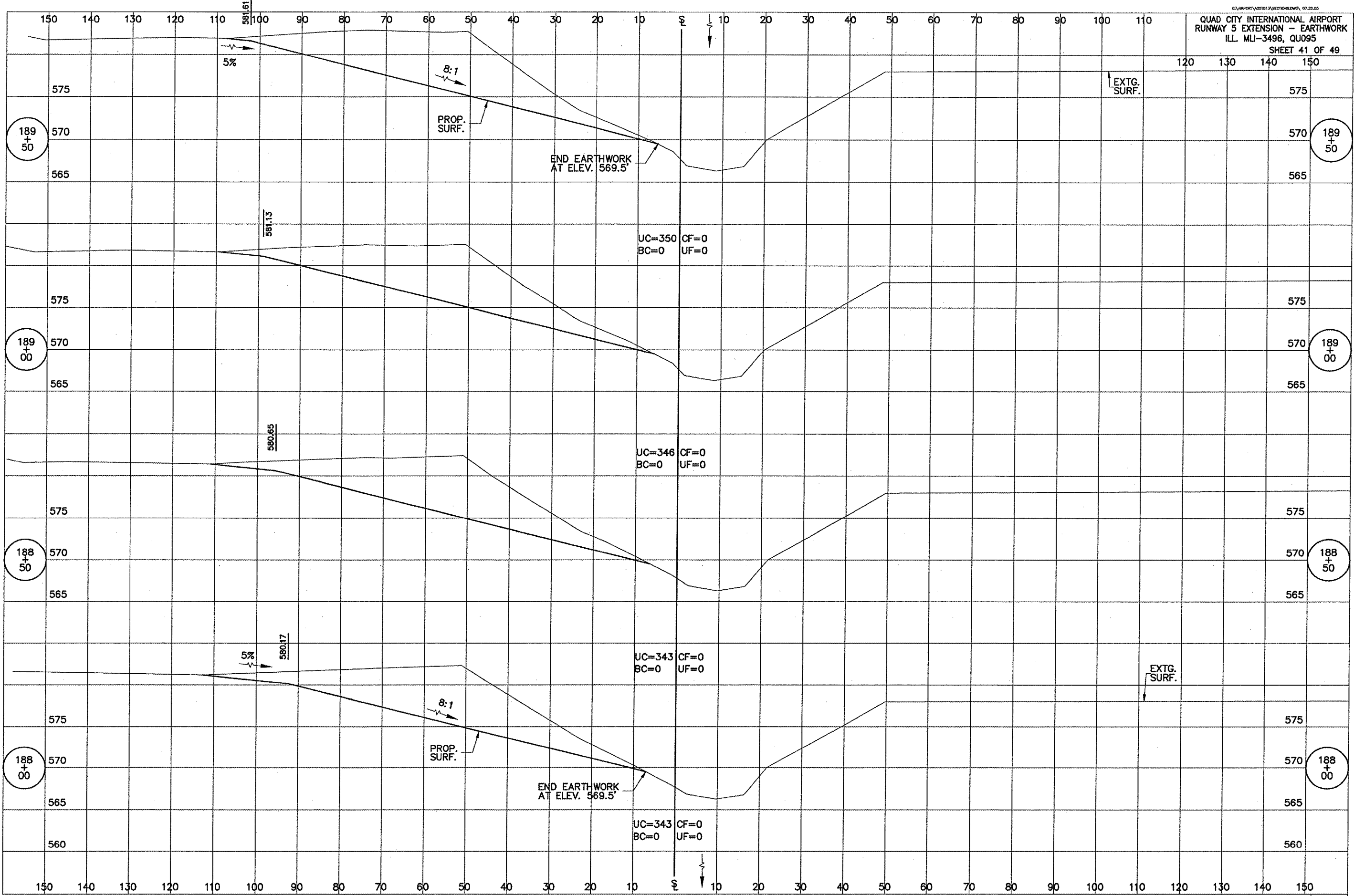
DITCH CROSS SECTIONS, STA 184+00 TO STA 185+50

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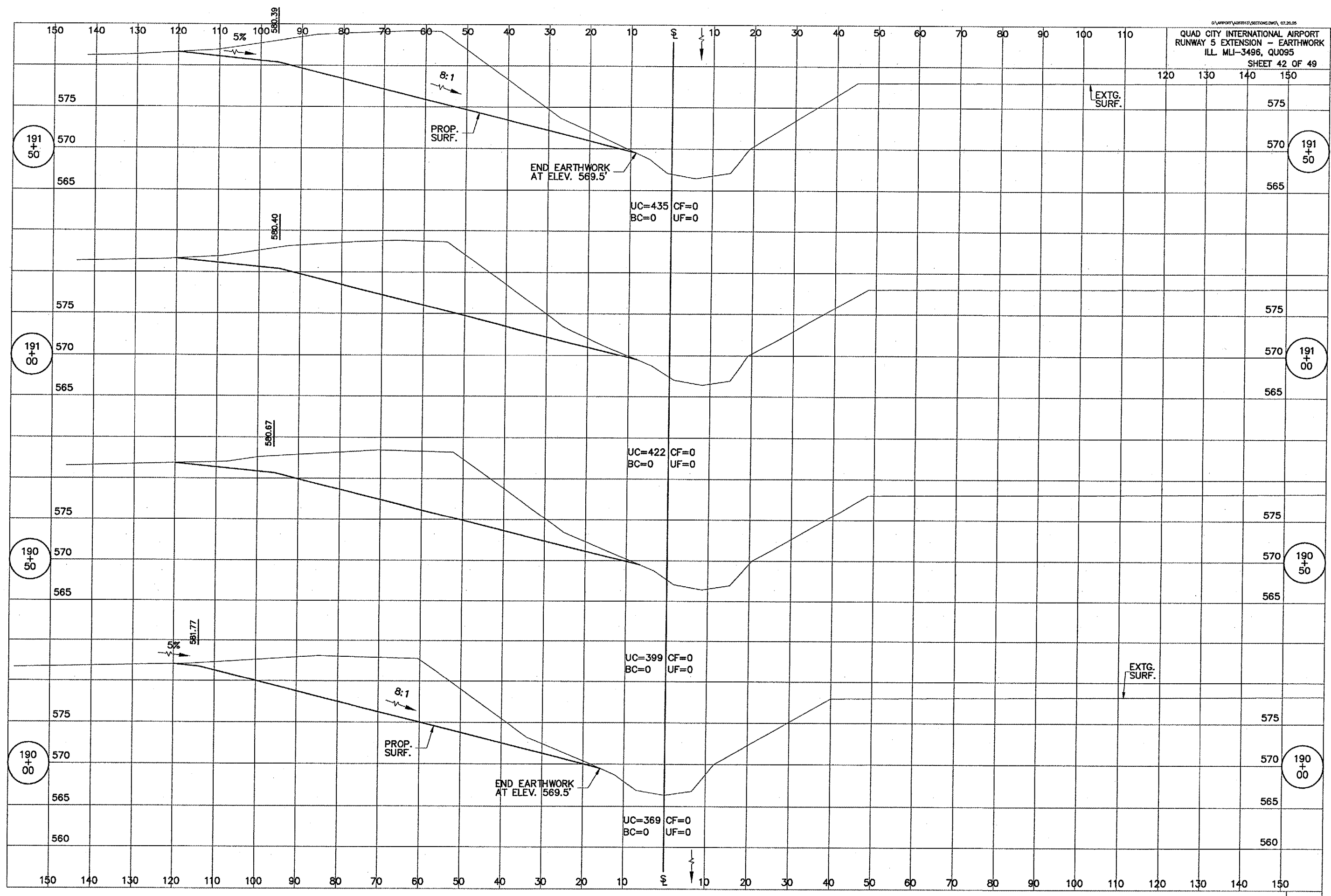
DITCH CROSS SECTIONS, STA 186+00 TO STA 187+50

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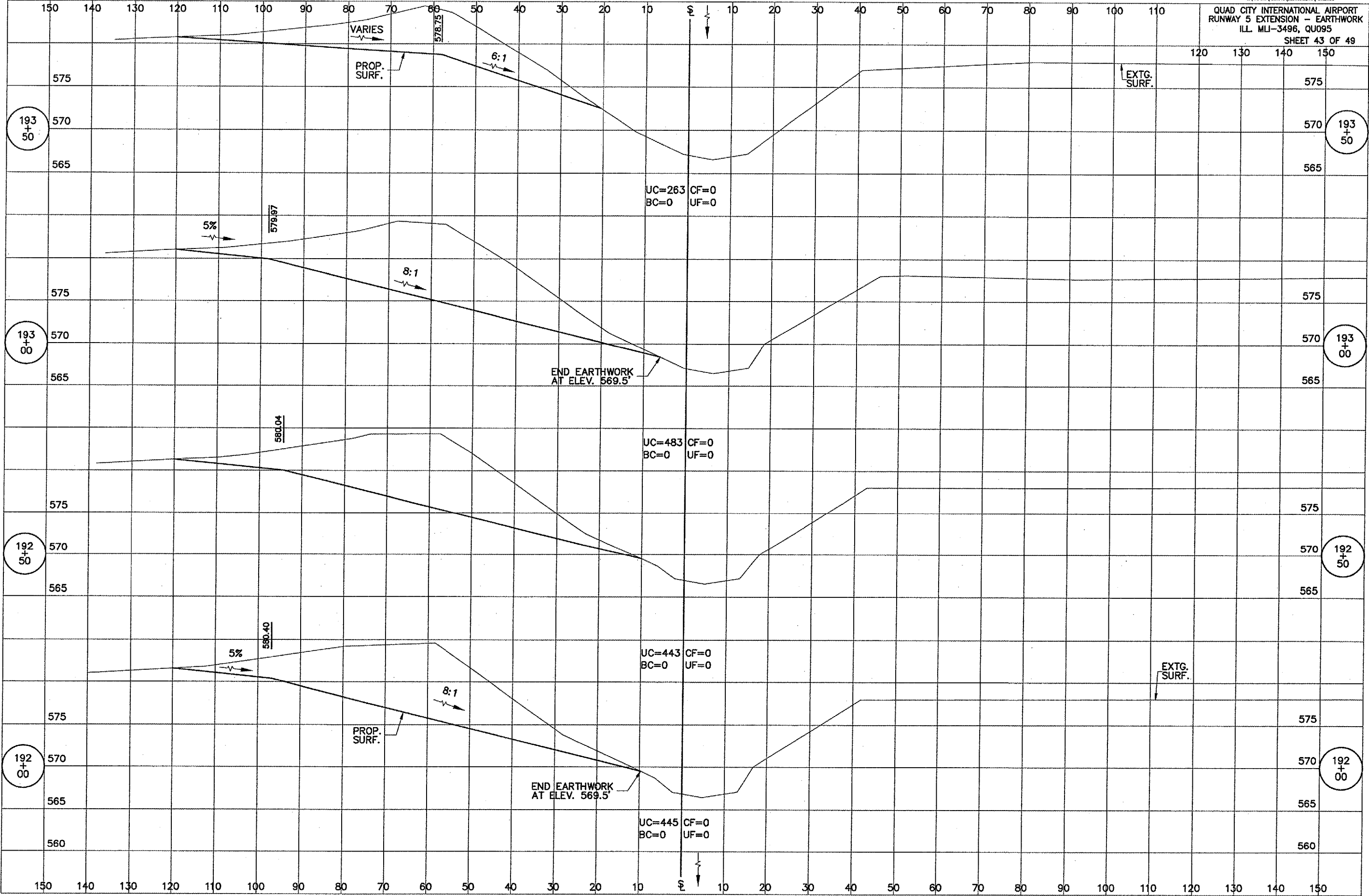
DITCH CROSS SECTIONS, STA 188+00 TO STA 189+50

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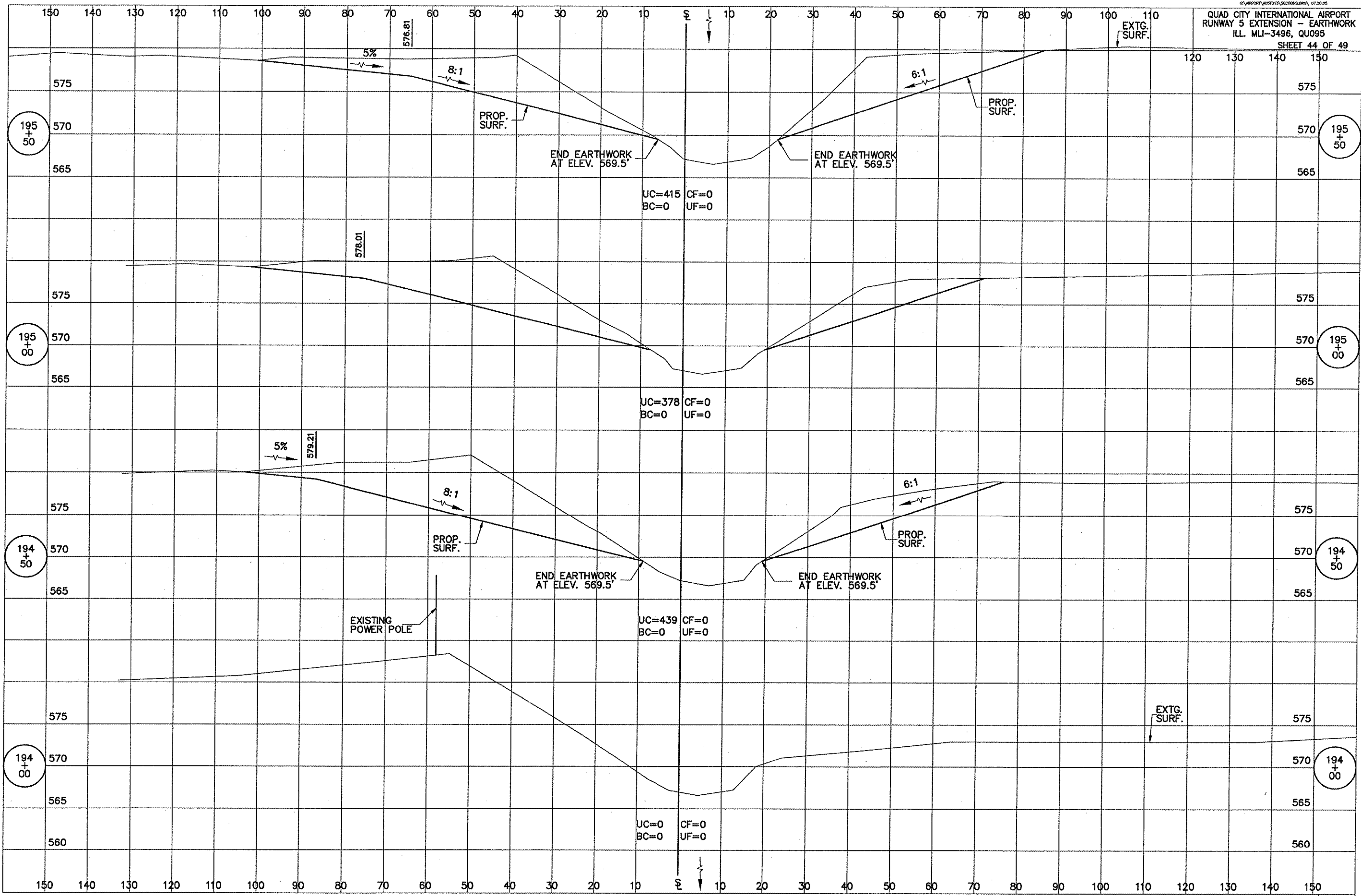


DITCH CROSS SECTIONS, STA 190+00 TO STA 191+50

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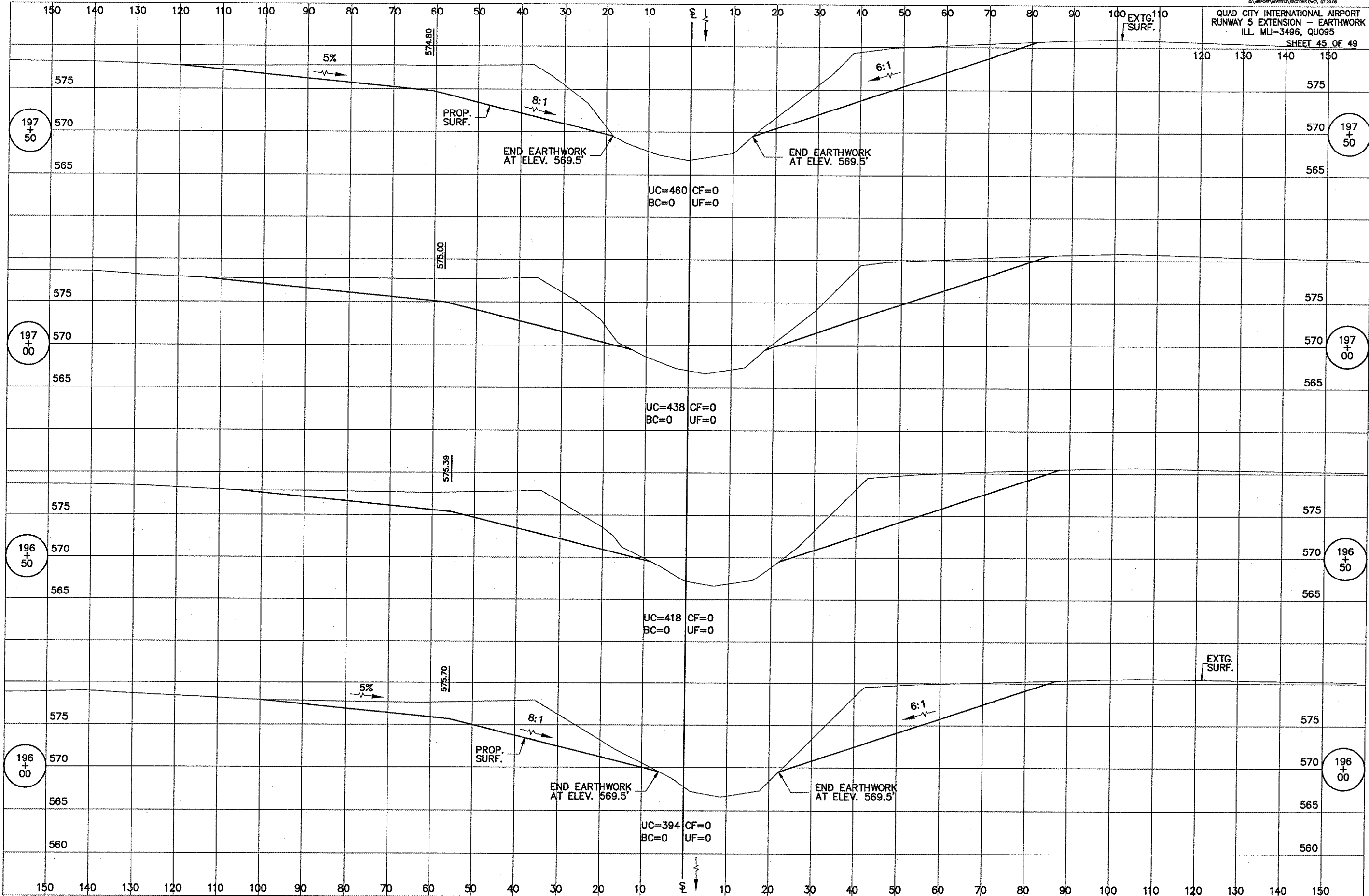
DITCH CROSS SECTIONS, STA 192+00 TO STA 193+50



DITCH CROSS SECTIONS, STA 194+00 TO STA 195+50

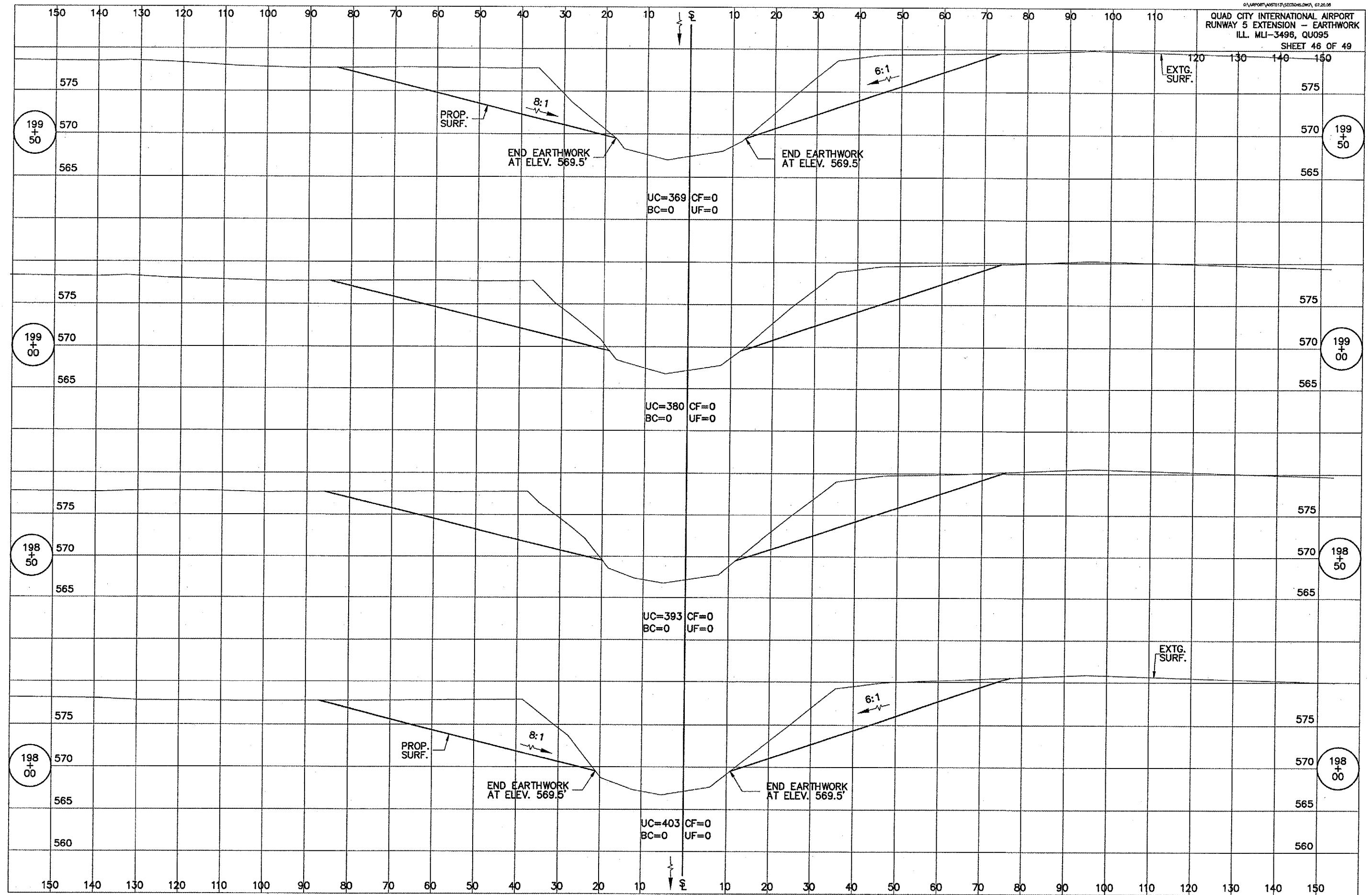
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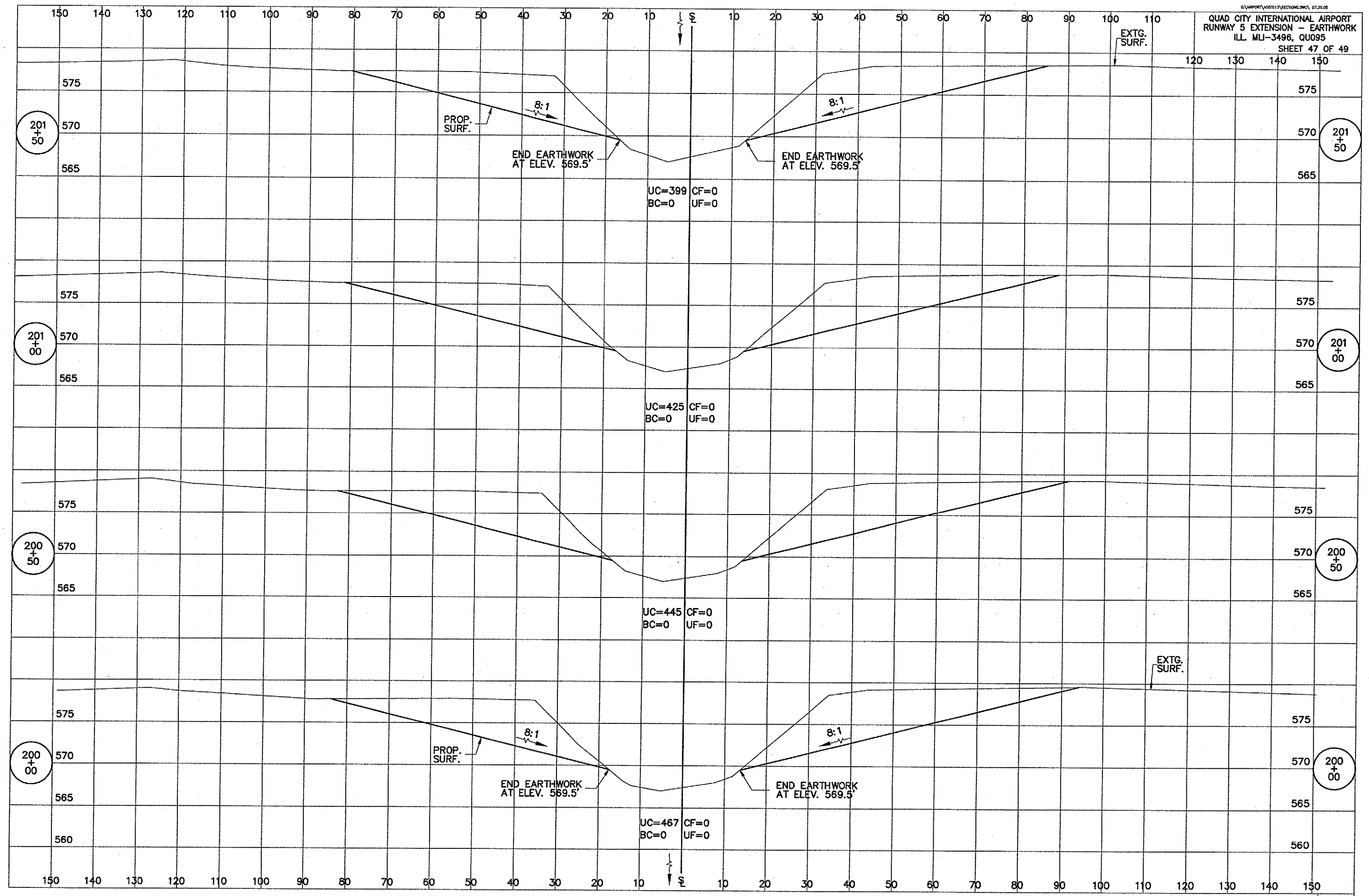
DITCH CROSS SECTIONS, STA 196+00 TO STA 197+50

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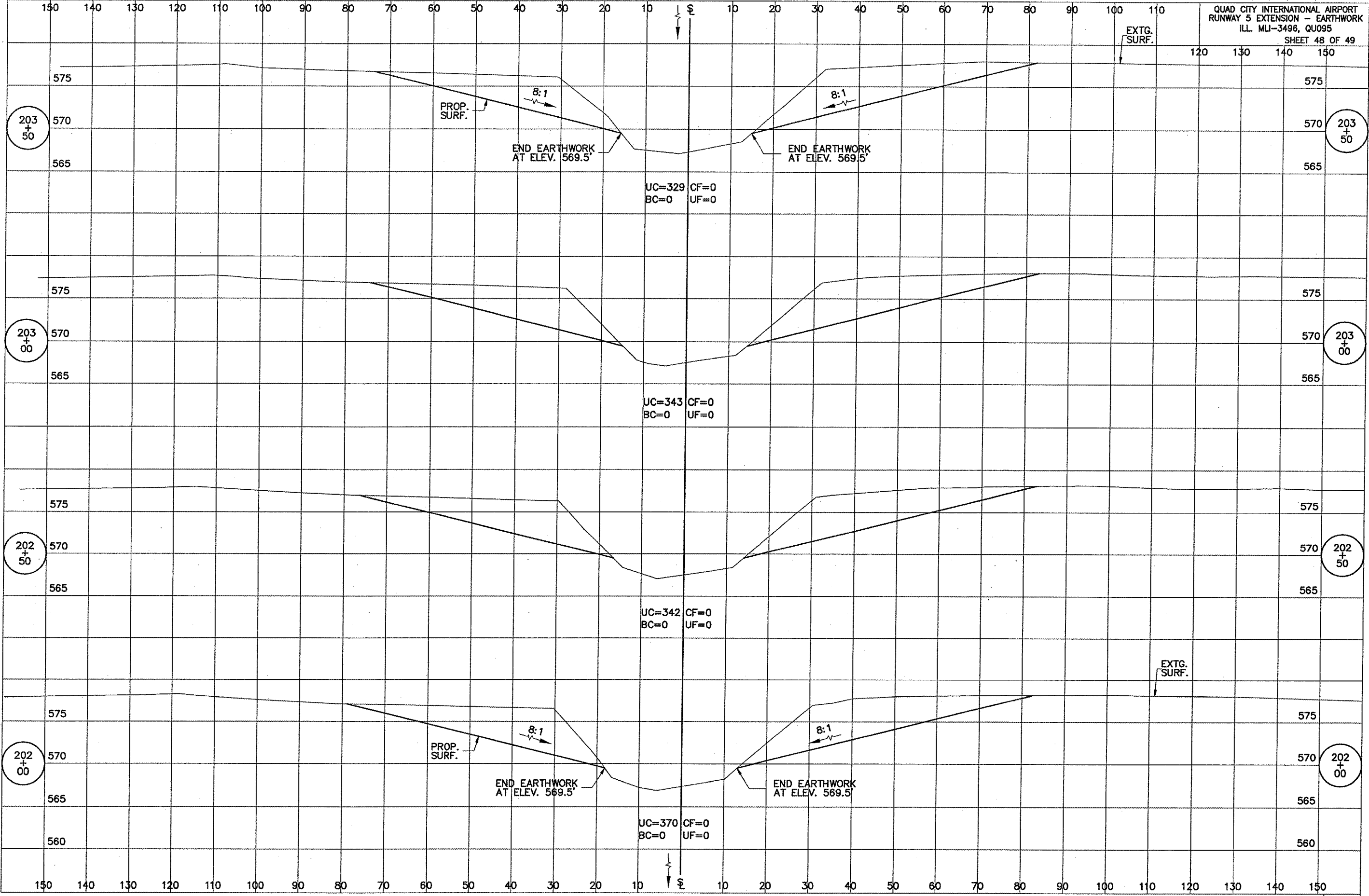


DITCH CROSS SECTIONS, STA 198+00 TO STA 199+50

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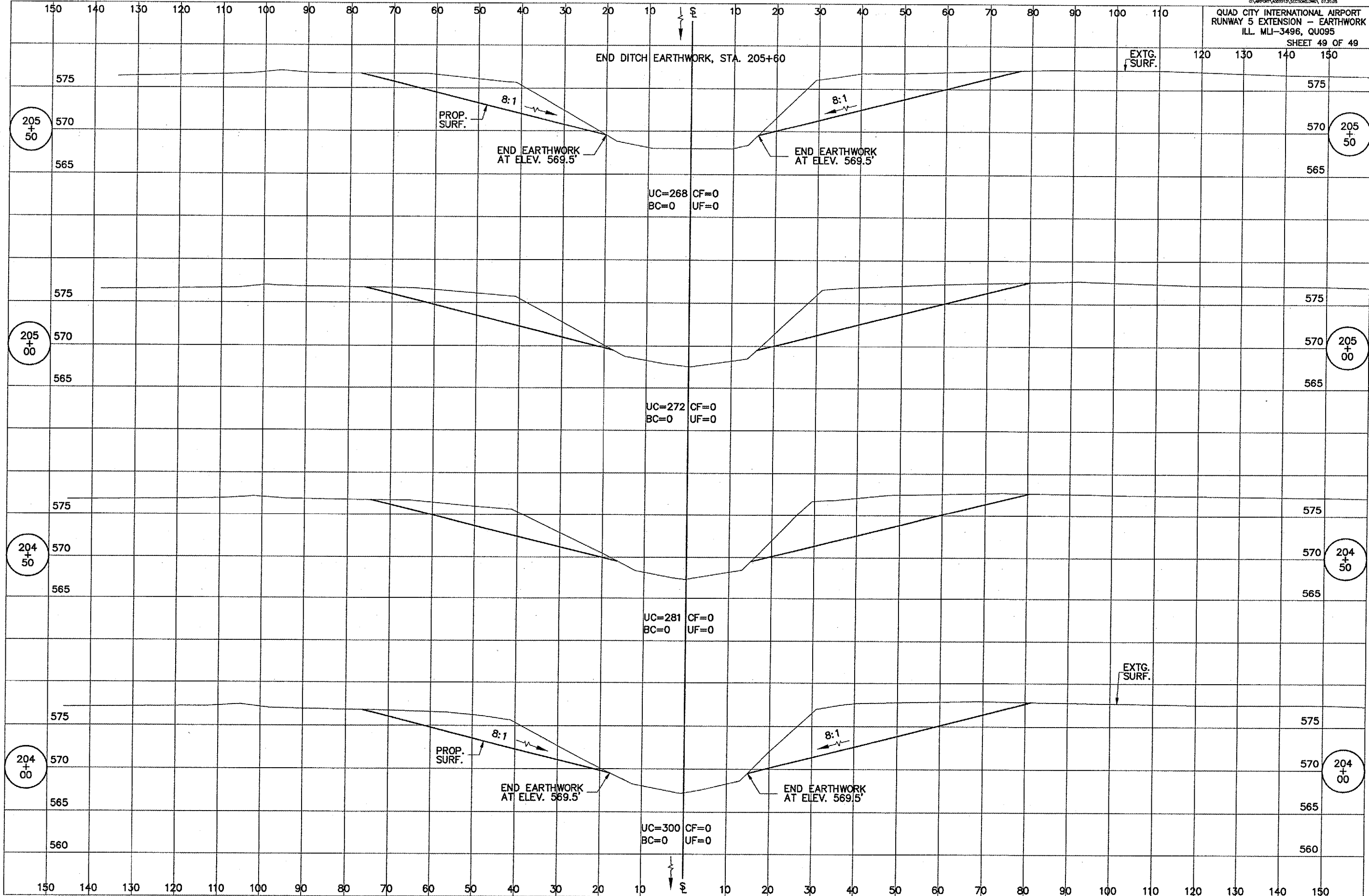


DITCH CROSS SECTIONS, STA 200+00 TO STA 201+50



DITCH CROSS SECTIONS, STA 202+00 TO STA 203+50

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DITCH CROSS SECTIONS, STA 204+00 TO STA 205+50

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