

03-10-2023 LETTING ITEM 088

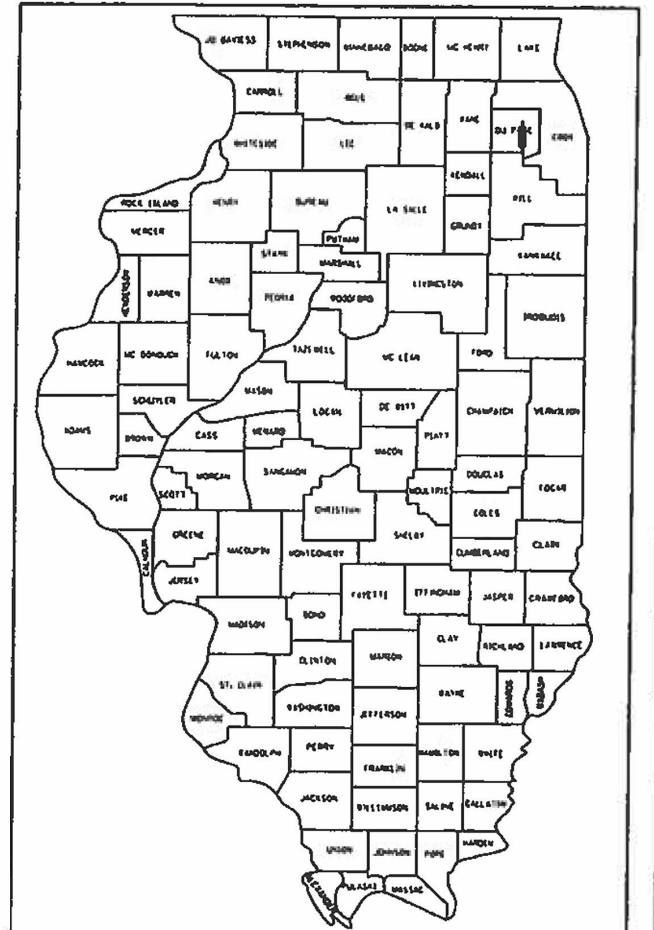
FOR INDEX OF SHEETS,
SEE SHEET NO. 2

FOR HIGHWAY STANDARDS,
SEE SHEET NO. 2

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	20-00085-00-RS	DUPAGE	37	1
		ILLINOIS	CONTRACT NO.	61J32

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION PLANS FOR PROPOSED FEDERAL AID HIGHWAY

MS ROUTE 7260 (TECHNOLOGY BOULEVARD)
FAU 0347 (IL ROUTE 38/ROOSEVELT ROAD) TO
FAU 0363 (FABYAN PARKWAY) RESURFACING,
CURB, AND SIDEWALK
SECTION NO.: 20-00085-00-RS
PROJECT NO.: KCBA(290)
CITY OF WEST CHICAGO
DUPAGE COUNTY
JOB NO.: C-91-158-22



LOCATION OF SECTION INDICATED THIS: - [shaded area]

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROVED 20th DEC 2022
[Signature]
CITY OF WEST CHICAGO, DIRECTOR OF PUBLIC WORKS

PASSED JANUARY 3 2023
[Signature]
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR
BID BASED ON
LIMITED REVIEW JANUARY 3 2023
[Signature]
REGIONAL ENGINEER

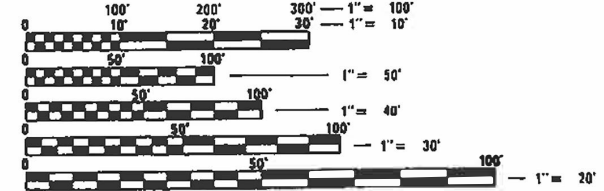
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OF THE STATE OF ILLINOIS

thomas
engineering group
service at the highest grade.

thomas engineering group, llc
2625 butterfield road
suite 209w
oak brook, il 60523
phone: 855-533-1700

FEDERAL AID PROGRAM ENGINEER: CARMEN E. RAMOS, P.E., SCHAMBERG, IL

TECHNOLOGY BOULEVARD
DESIGN DESIGNATION: MINOR COLLECTOR
SPEED LIMIT = 30 MPH (POSTED)
TRAFFIC ADT = 1,000 (2020)



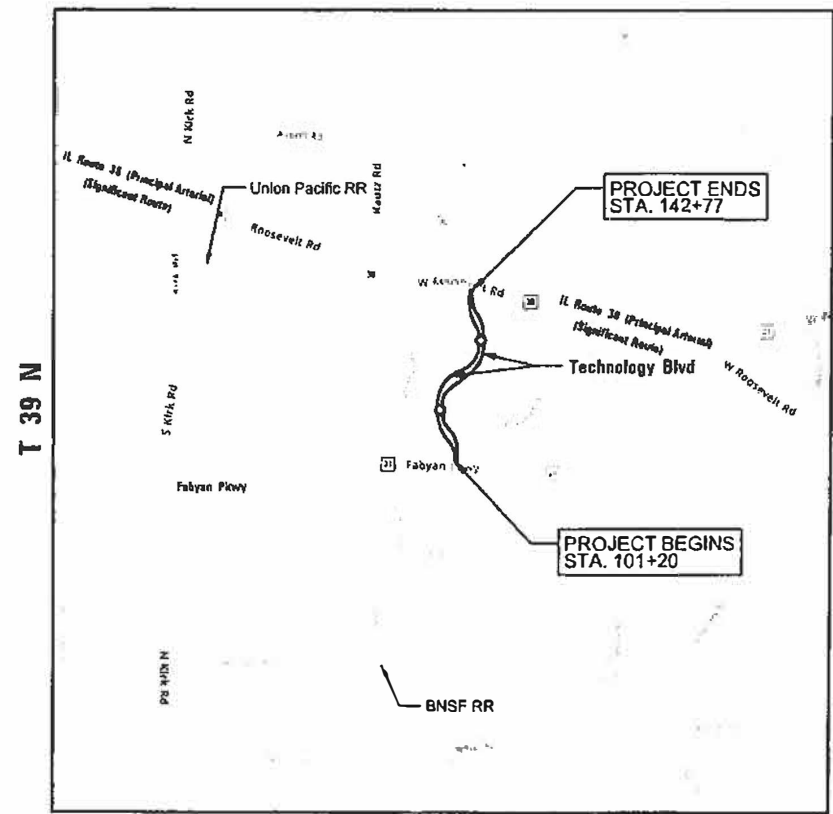
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811



PROJECT MANAGER: KEVIN VANDEWOESTYNE
(855) 533-1700
CONTRACT NO. 61J32

R 9 E



WINFIELD TOWNSHIP
LOCATION MAP
N.T.S.
TECHNOLOGY BOULEVARD GROSS AND NET LENGTH = 4,157 FT. (0.79 MILES)



[Signature]
KEVIN C. VANDEWOESTYNE, P.E.
NO. 062-060059
EXP. DATE 11-30-2023

INDEX OF SHEETS

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20-27	PAVEMENT MARKINGS AND LANDSCAPING PLANS
28	SIDEWALK PLANS
29	CONSTRUCTION DETAILS
30-37	IDOT DISTRICT ONE DETAILS

IDOT DISTRICT ONE DETAILS:

BD-08	DETAILS FOR FRAME AND LIDS ADJUSTMENTS WITH MILLING
BD-22	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
BD-24	CURB AND GUTTER REMOVAL AND REPLACEMENT
BD-32	BUTT JOINT AND HMA TAPER DETAILS
TC-10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
TC-13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
TC-16	SHORT TERM PAVEMENT MARKINGS- LETTERS AND SYMBOLS
TC-22	ARTERIAL ROAD INFORMATION SIGN

LIST OF IDOT HIGHWAY STANDARDS:

000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
442201-03	CLASS C AND D PATCHES
604091-05	FRAME AND GRATES TYPE 24
606001-08	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701006-05	OFF RD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W MOVING OPERATIONS- DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W UNDIVIDED
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS

GENERAL NOTES:

1. ALL CONSTRUCTION SHALL BE PERFORMED IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2022 (HEREIN AFTER REFERRED TO AS THE STANDARD SPECIFICATIONS); THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2023; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS, THE "DETAILS" ON THE PLANS AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS. ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST STANDARD OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION.
2. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE OWNER.
4. THE CONTRACTOR SHALL COOPERATE WITH THE CITY OF WEST CHICAGO IF ANY MUNICIPAL, UTILITY IMPROVEMENTS ARE REQUIRED WITHIN THE DURATION OF THE CONTRACT.
5. THE CONTRACTOR SHALL CONTACT KALPANA KANNAN-HOSADURGA, THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR, AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
6. QUANTITIES FOR MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS HAVE BEEN PROVIDED. THE ACTUAL NEED FOR THESE ITEMS SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER. IF CRACK FILLING IS NOT REQUIRED THE QUANTITIES WILL BE DEDUCTED PER THE TERMS OF ARTICLE 104.02.
7. QUANTITIES FOR CLASS D PATCHES HAVE BEEN PROVIDED IN THE CONTRACT BASED ON FIELD OBSERVATION OF EXISTING OBSERVATIONS OF EXISTING CONDITIONS. LOCATIONS SHOWN IN THE PLANS MAY DIFFER AND WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE ENGINEER. IF PAVEMENT PATCHING IS NOT REQUIRED THE QUANTITY WILL BE DEDUCTED PER THE TERMS OF ARTICLE 104.02.
8. THE CONTRACTOR SHALL MAINTAIN PROPER DRAINAGE AT ALL TIMES DURING THE COURSE OF CONSTRUCTION AND SHALL PREVENT STORM WATER FROM RUNNING INTO OR STANDING IN EXCAVATED AREAS.
9. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE BUTT JOINT AND BITUMINOUS TAPER DETAILS SHEET INCLUDED IN THE PLANS (BD-32).
10. ALL PAVEMENTS, CURB AND GUTTER, SIDEWALKS, DRIVEWAYS TO BE REMOVED SHALL BE SAWCUT PRIOR TO REMOVAL TO PREVENT DAMAGE TO ITEMS TO REMAIN.
11. THE CONTRACTOR SHALL SET AND CHECK ALL CURB FORMS AND STRING LINES PRIOR TO PLACING CONCRETE TO ENSURE POSITIVE DRAINAGE ALONG THE ROADWAY. IMPROPERLY DRAINING CURB SHALL BE REMOVED AND REPLACED BY THE CONTRACTOR.
12. ALL STREETS AND COMMERCIAL AND PARKING LOT ENTRANCES SHALL REMAIN OPEN DURING CONSTRUCTION.
13. PAVEMENT PATCHING SHALL BE SCHEDULED IMMEDIATELY FOLLOWING PAVEMENT REMOVAL TO REDUCE DEGRADATION OF THE EXISTING BASE.
14. THE MAXIMUM ALLOWABLE LANE DROP DIFFERENTIAL WILL BE 2"
15. THE CONTRACTOR SHALL PLACE FINAL THERMOPLASTIC PAVEMENT MARKINGS A MAXIMUM OF THREE (3) DAYS AFTER PLACEMENT OF THE FINAL HMA SURFACE COURSE.
16. PRIOR TO HMA RESURFACE REMOVAL, ALL OPEN DRAINAGE UTILITY STRUCTURES SHALL BE PROTECTED WITH FILTER FABRIC TO PREVENT ROADWAY DEBRIS FROM ENTERING UNDERGROUND UTILITIES. IF THE ENGINEER FINDS EVIDENCE OF CONSTRUCTION DEBRIS IN THE UTILITY STRUCTURES AFTER THE HMA SURFACE REMOVAL AND / OR AFTER THE BINDER COURSE AND SURFACE COURSE ARE COMPLETED, THE CONTRACTOR SHALL BE HELD RESPONSIBLE FOR REMOVING DEBRIS.
17. WORK SHALL BE CAREFULLY PLANNED BY THE CONTRACTOR TO REDUCE DISRUPTION TO RESIDENTS, THE BUSINESSES AND THE PUBLIC SEEKING TO ACCESS THE BUSINESSES. AT LEAST ONE LANE OF TRAFFIC MUST REMAIN OPEN AT ALL TIMES.
18. DURING AND AFTER CONSTRUCTION OPERATIONS, ANY LOOSE MATERIAL ON CITY ROADWAYS AS A RESULT OF CONTRACTOR OPERATIONS, INCLUDING BUT NOT LIMITED TO HMA SURFACE REMOVAL, BINDER COURSE AND SURFACE COURSE INSTALLATION, SHALL BE REMOVED AND DEPOSITED OFF SITE BY THE CLOSE OF EACH BUSINESS DAY. THIS APPLIES TO EXCESSIVE TACK COAT LEFT ON CITY ROADWAYS.
19. THE THICKNESS OF HMA MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASIS ON WHICH THEY ARE TO BE PLACED.
20. THE CONTRACTOR SHALL FURNISH, WHITE, PINK, OR PURPLE MARKING PAINT IN AEROSOL CANS, FOR USE BY THE ENGINEER. THE CONTRACTOR AND SUBCONTRACTORS SHALL ONLY USE THESE SAME COLORS FOR THEIR OWN MARKINGS, THEREFORE, NOT USING J.U.L.I.E. UTILITY COLORS.
21. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS, AT LEAST 24 HOURS IN ADVANCE IF ANY RESIDENTS OR BUSINESSES WILL HAVE NO OR LIMITED DRIVEWAY ACCESS DUE TO WORK PERFORMED BY THE CONTRACTOR.
22. CONTRACTOR IS TO PLAN HIS WORK SO THAT AT THE END OF EACH WORK DAY, THERE WILL BE NO OPEN HOLES IN THE PAVEMENT OR SIDEWALK AND THAT ALL BARRICADES WILL BE REMOVED FROM THE ROADWAY DURING NONWORKING HOURS, EXCEPT WHERE REQUIRED FOR PUBLIC SAFETY OR CURING OF CONCRETE.
23. ALL PROPOSED CONCRETE CURB AND GUTTER TYPES SHALL BE CONTINUOUSLY REINFORCED WITH TWO (2) NO. 4 REINFORCEMENT BARS.
24. AFTER SIDEWALK AND CURB AND GUTTER REPLACEMENT AND RESTORATION ACTIVITIES HAVE BEEN COMPLETED, THE CONTRACTOR SHALL REMOVE ALL LOOSE AND DEFECTIVE MATERIAL FROM THE SIDEWALK PAVEMENT, CURB AND GUTTER, AND PUBLIC RIGHT-OF-WAY TO THE SATISFACTION OF THE ENGINEER. THIS SHALL INCLUDE, BUT IS NOT LIMITED TO, BARRICADES, FORMS, GRAVEL, EXCESS TOP SOIL, AND EXCESS CONCRETE. AUXILIARY EQUIPMENT, SUCH AS BROOMS, SWEEPERS, AND SCRAPERS SHALL BE PROVIDED AS NECESSARY TO PERFORM WORK.
25. CONTRACTOR SHALL TAKE PRECAUTION BY PRESERVING EXISTING TREES WITHIN THE RIGHT-OF-WAY. IF ANY DAMAGE OCCURS, TREES SHALL BE REPLACED IN KIND PER ARTICLE 201.07 REPAIR OR REPLACEMENT OF EXISTING PLANT MATERIAL REQUIREMENTS STATED HEREIN.
26. A NOMINAL QUANTITY HAS BEEN INCLUDED FOR THE FOLLOWING PAY ITEMS:
 - SUPPLEMENTAL WATERING
 - FRAMES AND GRATES, TYPE 11
 - PARTIAL DEPTH PATCHING (SPECIAL)
 - PARTIAL DEPTH PATCHING



USER NAME = MitchellS	DESIGNED -	REVISED
	DRAWN -	REVISED -
PLOT SCALE = 2.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 1/12/2023	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

GENERAL NOTES

SCALE: SHEET OF SHEETS STA. TO STA.

FAU	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	21-00068-00-R5		37	2
CONTRACT NO. 61J32			ILLINOIS FED. AID PROJECT	

SUMMARY OF QUANTITIES					CONSTRUCTION CODE	
					STU 80/20	STU 80/20
SP. PROV.	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY MAINT.	SAFETY
					0005	0021
	21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	299	299	
CS	25200110	SODDING, SALT TOLERANT	SQ YD	299	299	
CS	25200200	SUPPLEMENTAL WATERING	UNIT	10	10	
	28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	6	6	
	28000510	INLET FILTERS	EACH	77	77	
	40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	17,918	17,918	
	40600370	LONGITUDINAL JOINT SEALANT	FOOT	8,844	8,844	
	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	20	20	
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	190	190	
	40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	1,095	1,095	
	40604172	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "E", N70	TON	2,601	2,601	
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	719		719
	44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	26,545	26,545	
	44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	877	877	
	44000600	SIDEWALK REMOVAL	SQ FT	739		739

△ SPECIALTY ITEMS

SUMMARY OF QUANTITIES					CONSTRUCTION CODE	
SP. PROV.	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	STU 80/20 ROADWAY MAINT.	STU 80/20 SAFETY
					0005	0021
	60260100	INLETS TO BE ADJUSTED	EACH	30	30	
	60404800	FRAMES AND GRATES, TYPE 11	EACH	15	15	
	60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	877	877	
	67100100	MOBILIZATION	L SUM	1	1	
	70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1	
	70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1	
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1	
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1	
	70107025	CHANGEABLE MESSAGE SIGN	CAL DA	60	60	
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	1,554		1,554
	70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	741		741
Δ	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTER AND SYMBOLS	SQ FT	506		506
Δ	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	6,254		6,254
Δ	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1,394		1,394
Δ	78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	144		144

Δ SPECIALTY ITEMS



USER NAME = DanW	DESIGNED -	REVISED
PLOT SCALE = 2.0000' / 1" =	DRAWN -	REVISED -
PLOT DATE = 12/28/2022	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

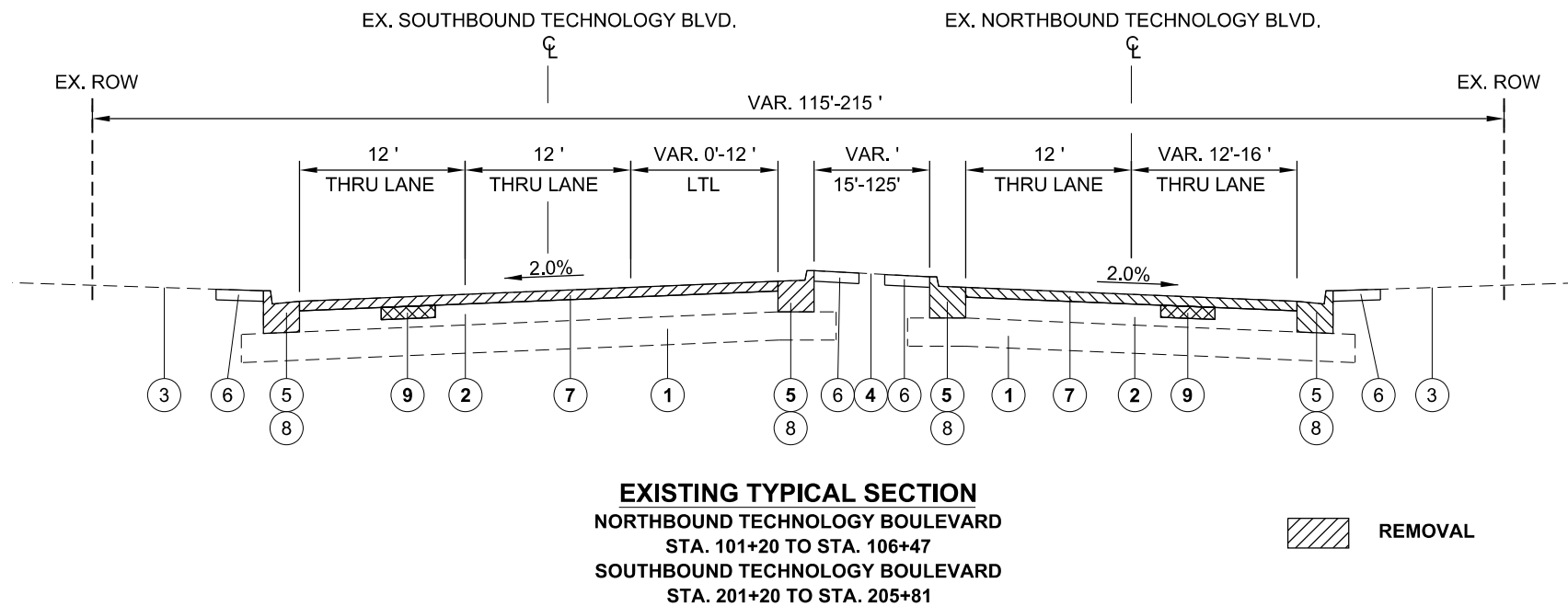
SUMMARY OF QUANTITIES

SCALE: SHEET OF SHEETS STA. TO STA.

FAU	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 61J32			ILLINOIS FED. AID PROJECT	

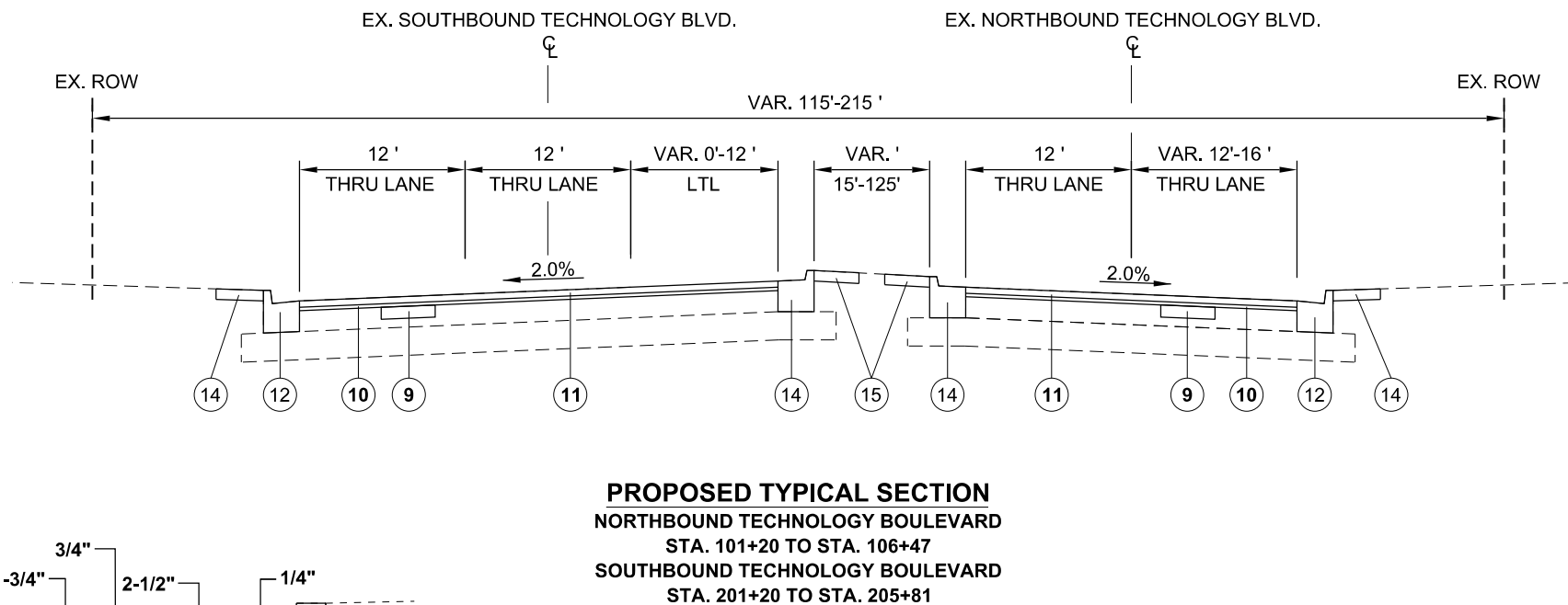
SUMMARY OF QUANTITIES					CONSTRUCTION CODE	
SP. PROV.	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	STU 80/20	STU 80/20
					ROADWAY MAINT.	SAFETY
					0005	0021
Δ	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	877		877
Δ	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	118		118
CS	X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1	
CS	X0326144	TACTILE/DETECTABLE WARNING SURFACE	SQ FT	88		88
CS	X0556100	PARTIAL DEPTH PATCHING (SPECIAL)	SQ YD	6,636	6,636	
CS	X4421000	PARTIAL DEPTH PATCHING	TON	836	836	
CS	Z0030850	TEMPORARY INFORMATION SIGNING	SQ YD	51.4	51.4	
CS = CONTRACT SPECIAL PROVISION (SEE SECTION III)						

Δ SPECIALTY ITEMS



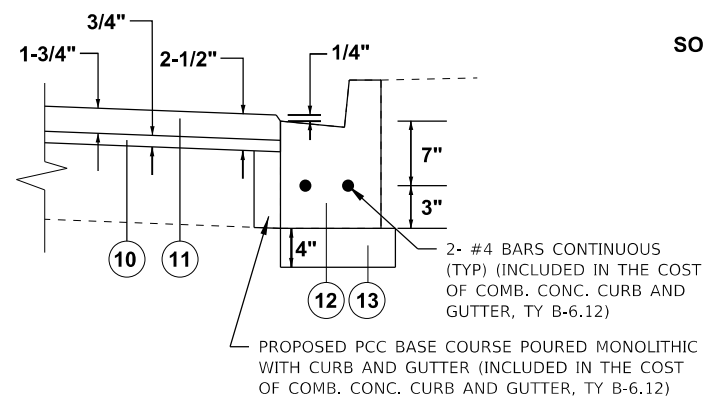
LEGEND

- ① EX SUB-GRADE
- ② EX HMA PAVEMENT, 16"
- ③ EX PARKWAY
- ④ EX LANDSCAPED MEDIAN
- ⑤ EX COMB. CONC. CURB AND GUTTER, TY B-6.12
- ⑥ SOD REMOVAL (SEE NOTES 1 AND 4)
- ⑦ HMA SURFACE REMOVAL, 2-1/4"
- ⑧ INTERMITTENT COMB. CURB AND GUTTER REMOVAL (SEE DETAIL A) (SEE NOTE 1 & 3)
- ⑨ PR. PARTIAL DEPTH PATCHING (SEE NOTE 1 & 3)
- ⑩ PR. POLYMERIZED HMA BINDER CRS, IL-4.75, N50, 3/4"
- ⑪ PR. POLYMERIZED HMA SURFACE CRS, MIX "E", IL-9.5, N70, 1-3/4" (SEE NOTE 2)
- ⑫ PR. COMB CONC CURB AND GUTTER, TY B-6.12 (SEE DETAIL A)
- ⑬ PR. AGGREGATE BASE COURSE, 4" (SEE NOTE 1)
- ⑭ PR RESTORATION (SEE NOTE 1)
-TOPSOIL FURNISH AND PLACE, 4"
-SODDING, SALT TOLERANT

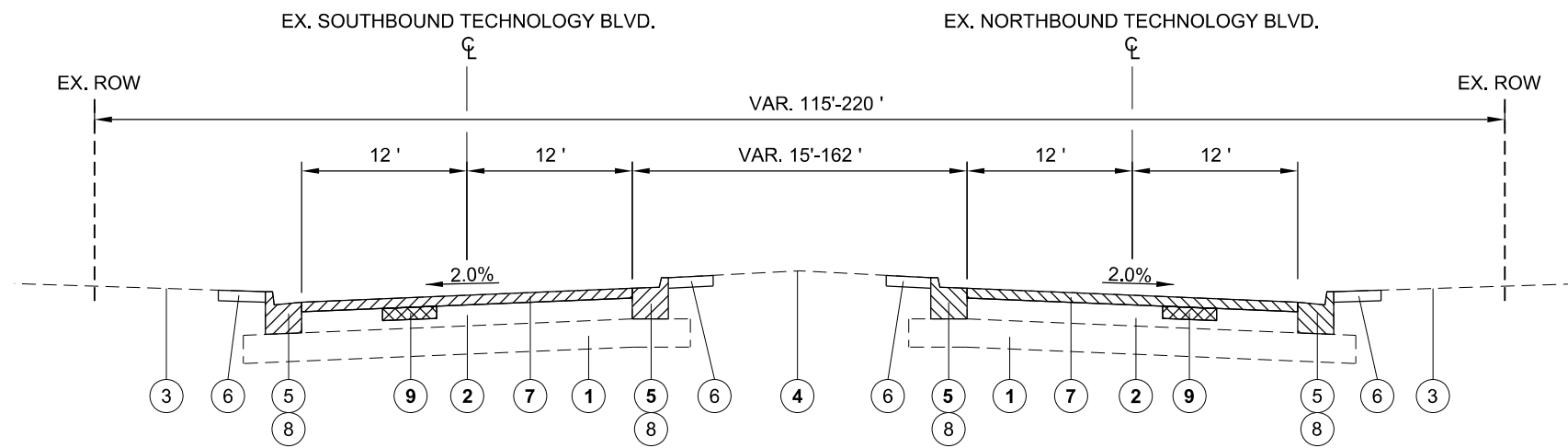


GENERAL NOTES:

1. LOCATIONS WILL BE SPECIFIED BY THE ENGINEER IN THE FIELD DURING CONSTRUCTION
2. ALL LONGITUDINAL JOINTS BETWEEN LANES MUST BE FINISHED BY THE END OF EACH WORKING DAY TO PREVENT COLD JOINT BONDING BETWEEN SUCCESSIVE DAYS OF PAVING
3. THE CONTRACTOR SHALL NOTIFY AFFECTED RESIDENTS AND BUSINESSES 24 HOURS IN ADVANCE OF OPERATIONS AFFECTING DRIVEWAY INGRESS AND EGRESS.
4. CONTRACTOR SHALL MINIMIZE DISTURBANCE OF EXISTING SOD LANDSCAPE. RESTORATION SHALL BE PAID FOR AS PR. RESTORATION



HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE TYPE	AIR VOIDS @ Ndes	QMP
ROADWAY RESURFACING		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", IL-9.5, N70	4% @ 70 Gyr.	LR-1030-2
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	3.5% @ 50 Gyr.	LR-1030-2
PARTIAL DEPTH PATCHING		
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	4% @ 70 Gyr.	LR-1030-2
QMP designations: Quality Control/Quality Assurance (QC/QA)		
<ol style="list-style-type: none"> 1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN. 2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL "PG 64 -22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS. 3. THE CONTRACTOR SHALL MILL BEFORE PATCHING. 4. THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE POLY. HMA BINDER CRS, IL-4.75, N50 		



EXISTING TYPICAL SECTION
 NORTHBOUND TECHNOLOGY BOULEVARD
 STA. 106+47 TO STA. 135+76
 SOUTHBOUND TECHNOLOGY BOULEVARD
 STA. 205+81 TO STA. 235+60

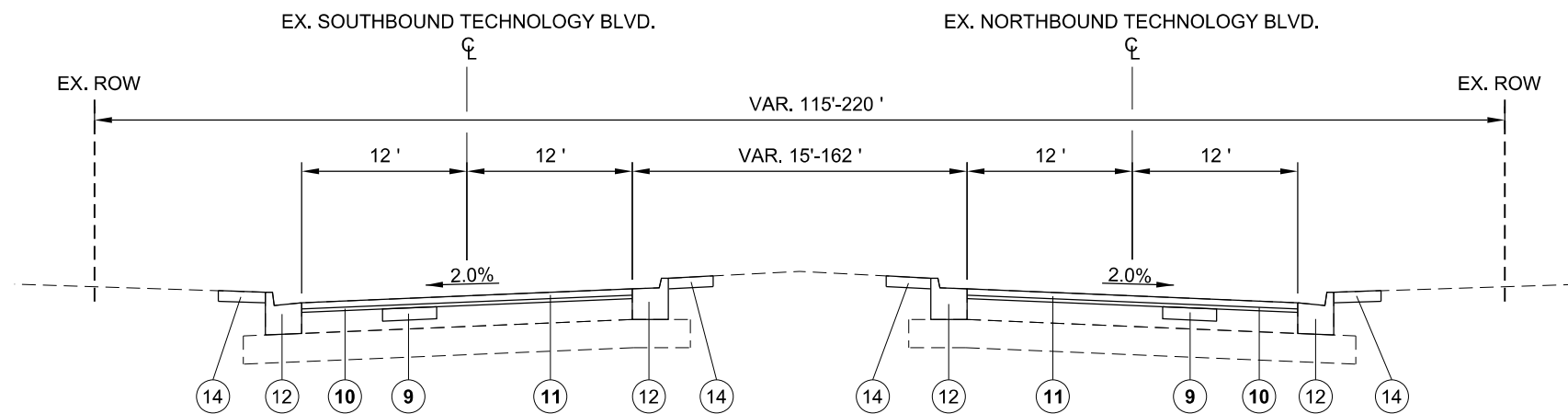
REMOVAL

LEGEND

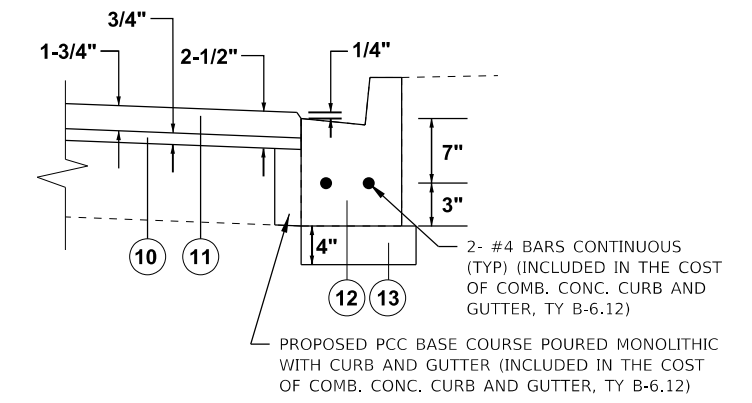
- ① EX SUB-GRADE
 - ② EX HMA PAVEMENT, 16"
 - ③ EX PARKWAY
 - ④ EX LANDSCAPED MEDIAN
 - ⑤ EX COMB. CONC. CURB AND GUTTER, TY B-6.12
 - ⑥ SOD REMOVAL (SEE NOTES 1 AND 4)
 - ⑦ HMA SURFACE REMOVAL, 2-1/4"
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 - ⑨ PR. PARTIAL DEPTH PATCHING (SEE NOTE 1 & 3)
 - ⑩ PR. POLYMERIZED HMA BINDER CRS, IL-4.75, N50, 3/4"
 - ⑪ PR. POLYMERIZED HMA SURFACE CRS, MIX "E", IL-9.5, N70, 1-3/4" (SEE NOTE 2)
 - ⑫ PR. COMB CONC CURB AND GUTTER, TY B-6.12 (SEE DETAIL A)
 - ⑬ PR. AGGREGATE BASE COURSE, 4" (SEE NOTE 1)
 - ⑭ PR RESTORATION (SEE NOTE 1)
- TOPSOIL FURNISH AND PLACE, 4"
 -SODDING, SALT TOLERANT

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PROPOSED TYPICAL SECTION
 NORTHBOUND TECHNOLOGY BOULEVARD
 STA. 106+47 TO STA. 135+76
 SOUTHBOUND TECHNOLOGY BOULEVARD
 STA. 205+81 TO STA. 235+60

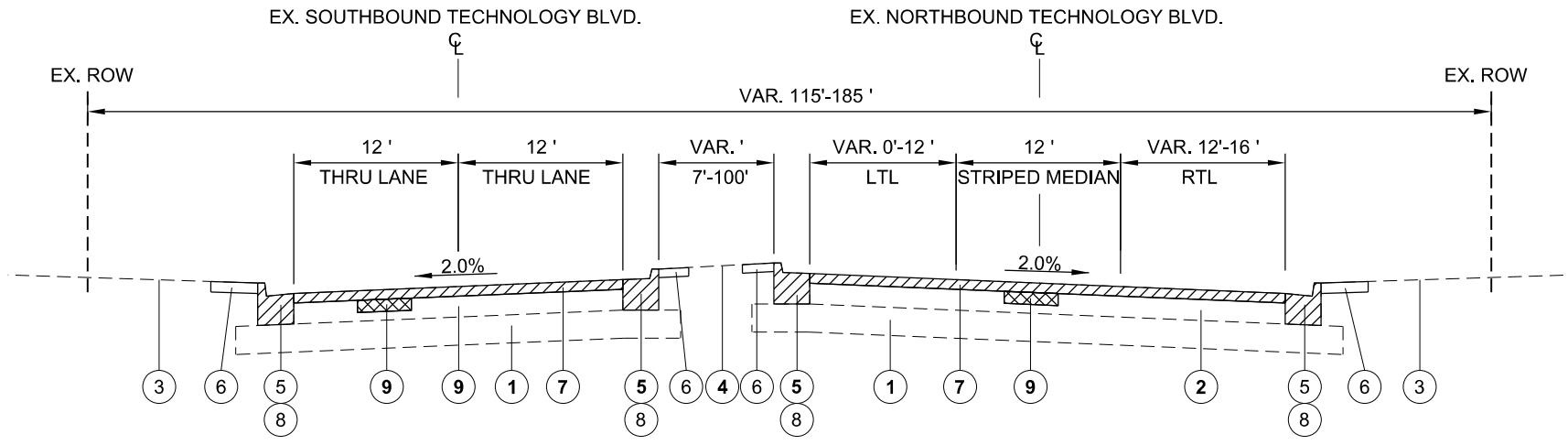


DETAIL A

COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

USER NAME = DonN	DESIGNED -	REVISED
DRAWN -	REVISOR -	REVISOR -
PLOT SCALE = 2.0000' / in.	CHECKED -	REVISOR -
PLOT DATE = 12/20/2022	DATE -	REVISOR -

FAU	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	21-00068-00-R5		37	7
CONTRACT NO. 61J32			ILLINOIS FED. AID PROJECT	



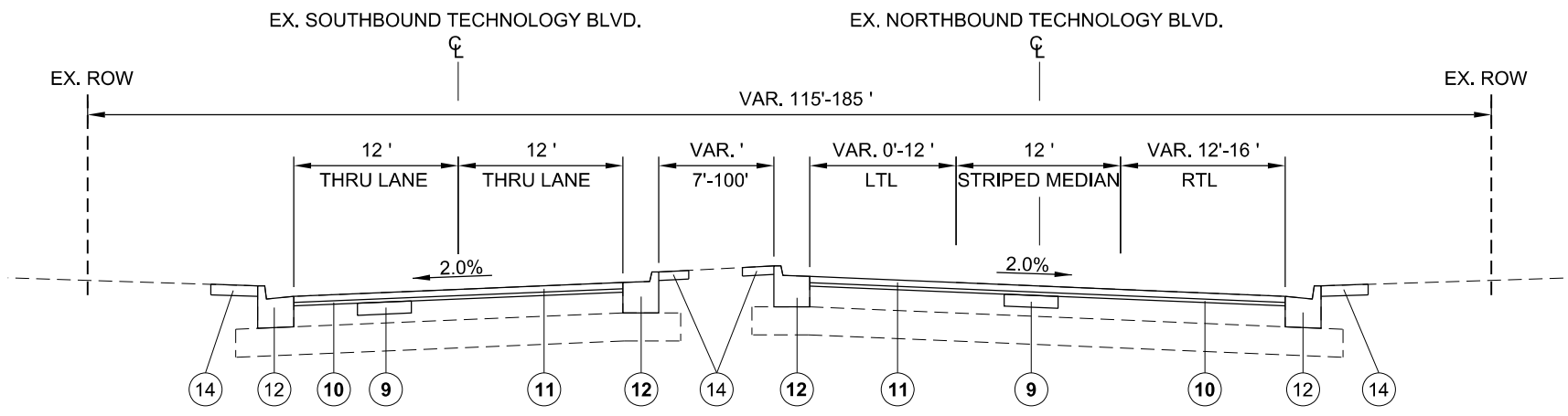
EXISTING TYPICAL SECTION
 NORTHBOUND TECHNOLOGY BOULEVARD
 STA. 135+76 TO STA. 142+77
 SOUTHBOUND TECHNOLOGY BOULEVARD
 STA. 235+60 TO STA. 243+17

REMOVAL

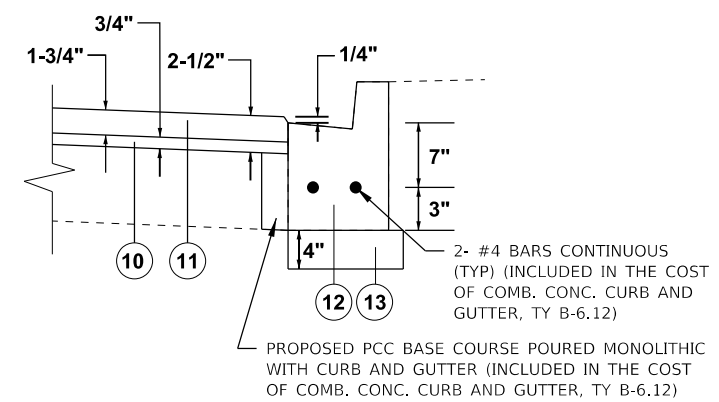
- LEGEND**
- ① EX SUB-GRADE
 - ② EX HMA PAVEMENT, 16"
 - ③ EX PARKWAY
 - ④ EX LANDSCAPED MEDIAN
 - ⑤ EX COMB. CONC. CURB AND GUTTER, TY B-6.12
 - ⑥ SOD REMOVAL (SEE NOTES 1 AND 4)
 - ⑦ HMA SURFACE REMOVAL, 2-1/4"
 - ⑧ INTERMITTENT COMB. CURB AND GUTTER REMOVAL (SEE DETAIL A) (SEE NOTE 1 & 3)
 - ⑨ PR. PARTIAL DEPTH PATCHING (SEE NOTE 1 & 3)
 - ⑩ PR. POLYMERIZED HMA BINDER CRS, IL-4.75, N50, 3/4"
 - ⑪ PR. POLYMERIZED HMA SURFACE CRS, MIX "E", IL-9.5, N70, 1-3/4" (SEE NOTE 2)
 - ⑫ PR. COMB CONC CURB AND GUTTER, TY B-6.12 (SEE DETAIL A)
 - ⑬ PR. AGGREGATE BASE COURSE, 4" (SEE NOTE 1)
 - ⑭ PR RESTORATION (SEE NOTE 1)
 -TOPSOIL FURNISH AND PLACE, 4"
 -SODDING, SALT TOLERANT

GENERAL NOTES:

1. LOCATIONS WILL BE SPECIFIED BY THE ENGINEER IN THE FIELD DURING CONSTRUCTION
2. ALL LONGITUDINAL JOINTS BETWEEN LANES MUST BE FINISHED BY THE END OF EACH WORKING DAY TO PREVENT COLD JOINT BONDING BETWEEN SUCCESSIVE DAYS OF PAVING
3. THE CONTRACTOR SHALL NOTIFY AFFECTED RESIDENTS AND BUSINESSES 24 HOURS IN ADVANCE OF OPERATIONS AFFECTING DRIVEWAY INGRESS AND EGRESS.
4. CONTRACTOR SHALL MINIMIZE DISTURBANCE OF EXISTING SOD LANDSCAPE. RESTORATION SHALL BE PAID FOR AS PR. RESTORATION



PROPOSED TYPICAL SECTION
 NORTHBOUND TECHNOLOGY BOULEVARD
 STA. 135+76 TO STA. 142+77
 SOUTHBOUND TECHNOLOGY BOULEVARD
 STA. 235+60 TO STA. 243+17



DETAIL A
 COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

USER NAME = DonN	DESIGNED -	REVISED -
DRAWN -	REVISOR -	
PLOT SCALE = 2.0000' / in.	CHECKED -	REVISOR -
PLOT DATE = 12/20/2022	DATE -	REVISOR -

FAU	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	21-00068-00-R5	DuPAGE	37	8
ILLINOIS FED. AID PROJECT			CONTRACT NO. 61J32	



CURVE DATA

PROP. CURVE WESTTECH1
 PI STA. = 201+57.05
 $\Delta = 9^\circ 33' 14''$ (LT)
 $D = 5^\circ 20' 34''$
 $R = 1,072.37'$
 $T = 89.62'$
 $L = 178.82'$
 $E = 3.74'$
 P.C. STA. = 200+67.44
 P.T. STA. = 202+46.25

PROP. CURVE WESTTECH2
 PI STA. = 204+19.03
 $\Delta = 35^\circ 24' 10''$ (LT)
 $D = 10^\circ 35' 03''$
 $R = 541.34'$
 $T = 172.78'$
 $L = 334.49'$
 $E = 26.90'$
 P.C. STA. = 202+46.25
 P.T. STA. = 205+80.75

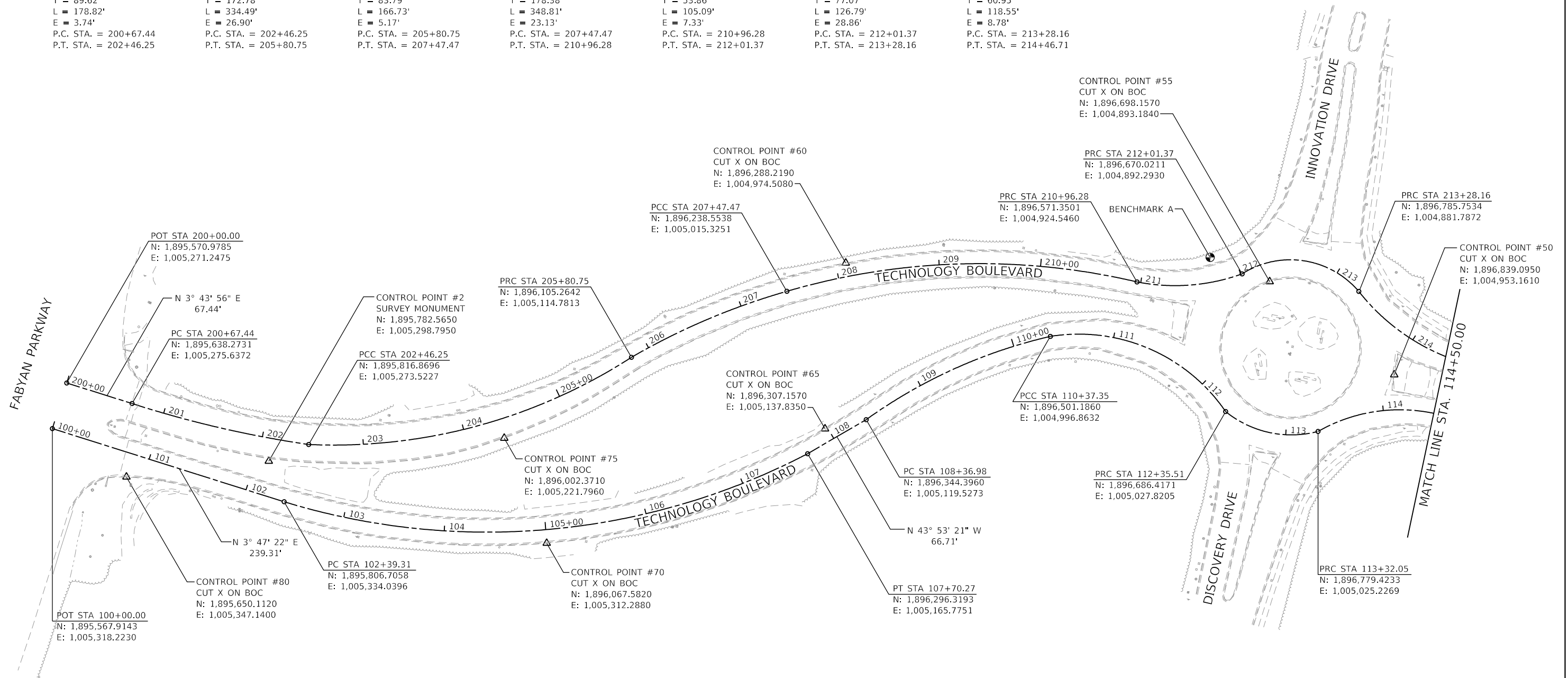
PROP. CURVE WESTTECH3
 PI STA. = 206+64.54
 $\Delta = 14^\circ 08' 05''$ (RT)
 $D = 8^\circ 28' 40''$
 $R = 675.84'$
 $T = 83.79'$
 $L = 166.73'$
 $E = 5.17'$
 P.C. STA. = 205+80.75
 P.T. STA. = 207+47.47

PROP. CURVE WESTTECH4
 PI STA. = 209+25.85
 $\Delta = 29^\circ 33' 14''$ (RT)
 $D = 8^\circ 28' 22''$
 $R = 676.23'$
 $T = 178.38'$
 $L = 348.81'$
 $E = 23.13'$
 P.C. STA. = 207+47.47
 P.T. STA. = 210+96.28

PROP. CURVE WESTTECH5
 PI STA. = 211+50.15
 $\Delta = 30^\circ 59' 45''$ (LT)
 $D = 29^\circ 29' 45''$
 $R = 194.25'$
 $T = 53.86'$
 $L = 105.09'$
 $E = 7.33'$
 P.C. STA. = 210+96.28
 P.T. STA. = 212+01.37

PROP. CURVE WESTTECH6
 PI STA. = 212+78.43
 $\Delta = 82^\circ 07' 52''$ (RT)
 $D = 64^\circ 46' 42''$
 $R = 88.45'$
 $T = 77.07'$
 $L = 126.79'$
 $E = 28.86'$
 P.C. STA. = 212+01.37
 P.T. STA. = 213+28.16

PROP. CURVE WESTTECH7
 PI STA. = 213+89.11
 $\Delta = 32^\circ 48' 09''$ (LT)
 $D = 27^\circ 40' 08''$
 $R = 207.08'$
 $T = 60.95'$
 $L = 118.55'$
 $E = 8.78'$
 P.C. STA. = 213+28.16
 P.T. STA. = 214+46.71



CURVE DATA

PROP. CURVE EASTTECH1
 PI STA. = 105+18.99
 $\Delta = 44^\circ 29' 08''$ (LT)
 $D = 8^\circ 22' 42''$
 $R = 683.85'$
 $T = 279.67'$
 $L = 530.96'$
 $E = 54.98'$
 P.C. STA. = 102+39.31
 P.T. STA. = 107+70.27

PROP. CURVE EASTTECH2
 PI STA. = 109+38.49
 $\Delta = 22^\circ 38' 41''$ (RT)
 $D = 11^\circ 18' 05''$
 $R = 506.98'$
 $T = 101.51'$
 $L = 200.37'$
 $E = 10.06'$
 P.C. STA. = 108+36.98
 P.T. STA. = 110+37.35

PROP. CURVE EASTTECH3
 PI STA. = 111+48.50
 $\Delta = 64^\circ 40' 57''$ (RT)
 $D = 32^\circ 38' 32''$
 $R = 175.53'$
 $T = 111.14'$
 $L = 198.16'$
 $E = 32.23'$
 P.C. STA. = 110+37.35
 P.T. STA. = 112+35.51

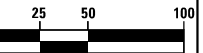
PROP. CURVE EASTTECH4
 PI STA. = 112+87.66
 $\Delta = 53^\circ 43' 43''$ (LT)
 $D = 55^\circ 39' 13''$
 $R = 102.95'$
 $T = 52.15'$
 $L = 96.54'$
 $E = 12.45'$
 P.C. STA. = 112+35.51
 P.T. STA. = 113+32.05

BENCHMARKS:

SOUTHWEST BOLT ON HYDRANT AT SOUTHWEST CORNER OF INNOVATION DRIVE AND TECHNOLOGY BOULEVARD (SOUTH ROUND-A-BOUT).
 ELEV. = 755.51

SOUTHWEST BOLT WITH TAG ON HYDRANT ON SOUTHBOUND TECHNOLOGY BOULEVARD BETWEEN ROUND-A-BOUITS.
 ELEV. = 759.85

NORTHEAST BOLT ON HYDRANT AT SOUTHEAST CORNER OF INNOVATION DRIVE AND TECHNOLOGY BOULEVARD (NORTH ROUND-A-BOUT).
 ELEV. = 765.13



HORIZONTAL SCALE IN FEET



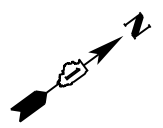
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PLOT SCALE = 1/8" = 100'-0"	DRAWN -	REVISED -
PLOT DATE = 12/20/2022	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ALIGNMENTS AND BENCHMARKS

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
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FAU	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	21-00068-00-R5	DuPAGE	37	9
ILLINOIS FED. AID PROJECT			CONTRACT NO. 61J32	



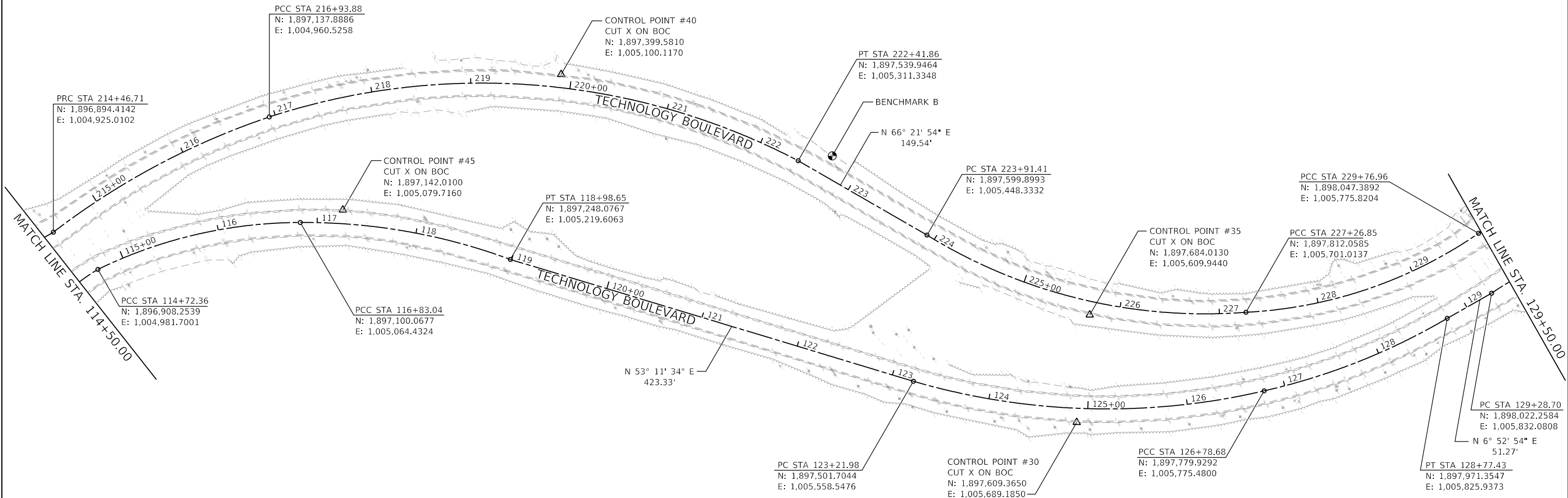
CURVE DATA

PROP. CURVE WESTTECH8
 PI STA. = 215+71.43
 $\Delta = 18^\circ 53' 13''$ (RT)
 $D = 7^\circ 38' 29''$
 $R = 749.82'$
 $T = 124.72'$
 $L = 247.17'$
 $E = 10.30'$
 P.C. STA. = 214+46.71
 P.T. STA. = 216+93.88

PROP. CURVE WESTTECH9
 PI STA. = 219+83.37
 $\Delta = 45^\circ 40' 28''$ (RT)
 $D = 8^\circ 20' 06''$
 $R = 687.41'$
 $T = 289.49'$
 $L = 547.98'$
 $E = 58.47'$
 P.C. STA. = 216+93.88
 P.T. STA. = 222+41.86

PROP. CURVE WESTTECH10
 PI STA. = 225+64.89
 $\Delta = 36^\circ 02' 58''$ (LT)
 $D = 10^\circ 44' 48''$
 $R = 533.14'$
 $T = 173.48'$
 $L = 335.44'$
 $E = 27.52'$
 P.C. STA. = 223+91.41
 P.T. STA. = 227+26.85

PROP. CURVE WESTTECH11
 PI STA. = 228+55.20
 $\Delta = 31^\circ 41' 49''$ (LT)
 $D = 12^\circ 40' 23''$
 $R = 452.10'$
 $T = 128.35'$
 $L = 250.11'$
 $E = 17.86'$
 P.C. STA. = 227+26.85
 P.T. STA. = 229+76.96



CURVE DATA

PROP. CURVE EASTTECH5
 PI STA. = 114+06.92
 $\Delta = 49^\circ 28' 47''$ (RT)
 $D = 35^\circ 15' 57''$
 $R = 162.47'$
 $T = 74.86'$
 $L = 140.30'$
 $E = 16.42'$
 P.C. STA. = 113+32.05
 P.T. STA. = 114+72.36

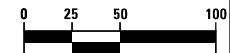
PROP. CURVE EASTTECH6
 PI STA. = 115+79.53
 $\Delta = 25^\circ 53' 21''$ (RT)
 $D = 12^\circ 17' 18''$
 $R = 466.27'$
 $T = 107.17'$
 $L = 210.68'$
 $E = 12.16'$
 P.C. STA. = 114+72.36
 P.T. STA. = 116+83.04

PROP. CURVE EASTTECH7
 PI STA. = 117+92.03
 $\Delta = 20^\circ 41' 38''$ (RT)
 $D = 9^\circ 35' 52''$
 $R = 596.97'$
 $T = 108.99'$
 $L = 215.61'$
 $E = 9.87'$
 P.C. STA. = 116+83.04
 P.T. STA. = 118+98.65

PROP. CURVE EASTTECH8
 PI STA. = 125+04.34
 $\Delta = 29^\circ 22' 33''$ (LT)
 $D = 8^\circ 14' 08''$
 $R = 695.71'$
 $T = 182.36'$
 $L = 356.70'$
 $E = 23.50'$
 P.C. STA. = 123+21.98
 P.T. STA. = 126+78.68

PROP. CURVE EASTTECH9
 PI STA. = 127+78.85
 $\Delta = 17^\circ 39' 40''$ (LT)
 $D = 8^\circ 53' 10''$
 $R = 644.78'$
 $T = 100.17'$
 $L = 198.75'$
 $E = 7.73'$
 P.C. STA. = 126+78.68
 P.T. STA. = 128+77.43

PROP. CURVE EASTTECH10
 PI STA. = 129+90.93
 $\Delta = 31^\circ 14' 25''$ (RT)
 $D = 25^\circ 44' 34''$
 $R = 222.57'$
 $T = 62.23'$
 $L = 121.36'$
 $E = 8.54'$
 P.C. STA. = 129+28.70
 P.T. STA. = 130+50.06



HORIZONTAL SCALE IN FEET

thomas engineering group, llc
 762 shoreline drive
 suite 200
 aurora, illinois 60504
 phone: 855-533-1700

USER NAME = DonN	DESIGNED -	REVISED -
PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/20/2022	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

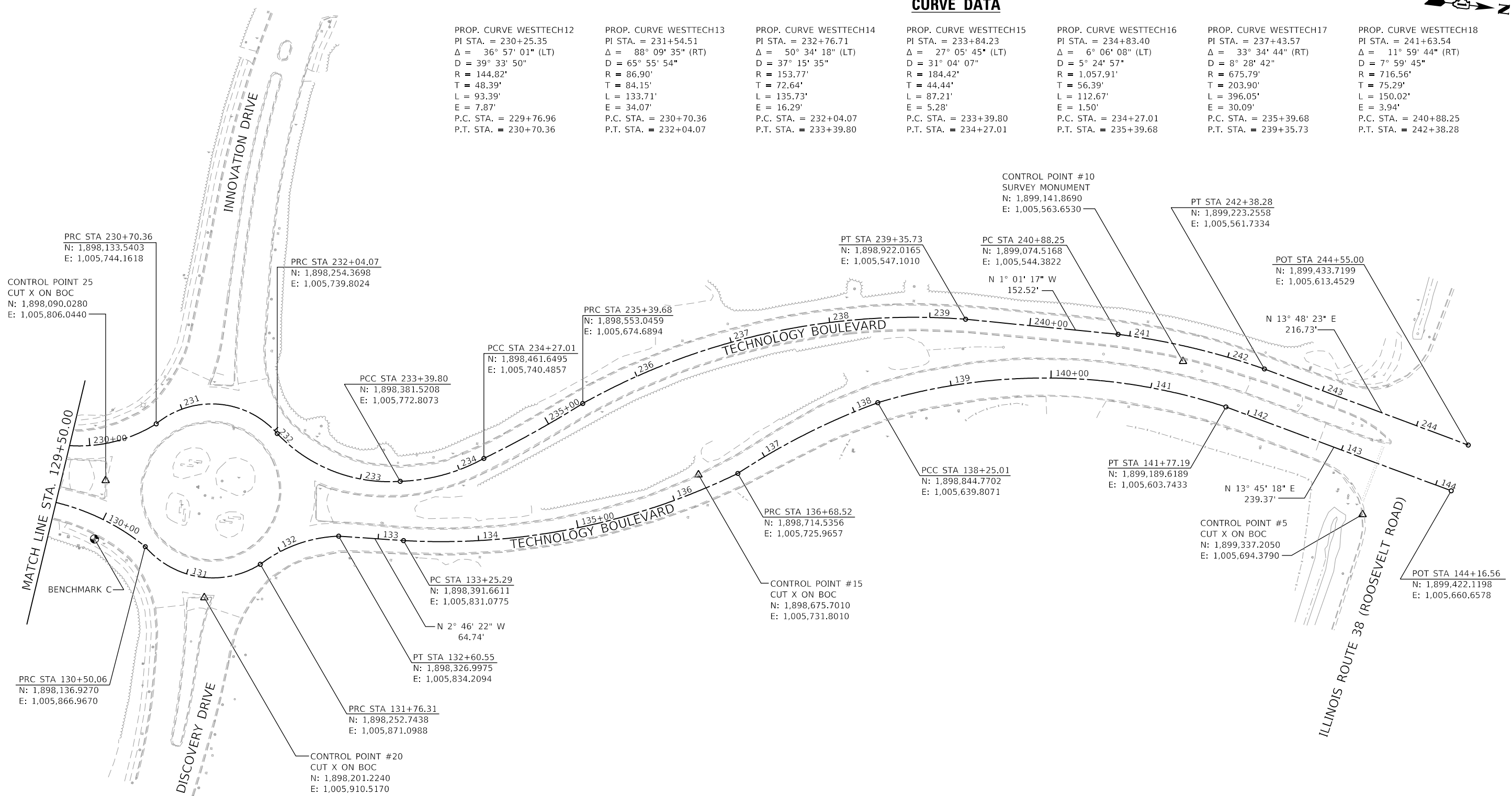
ALIGNMENTS AND BENCHMARKS			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

FAU	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	21-00068-00-R5	DuPAGE	37	10
CONTRACT NO. 61J32				
ILLINOIS FED. AID PROJECT				



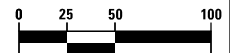
CURVE DATA

PROP. CURVE WESTTECH12 PI STA. = 230+25.35 $\Delta = 36^\circ 57' 01''$ (LT) D = 39° 33' 50" R = 144.82' T = 48.39' L = 93.39' E = 7.87' P.C. STA. = 229+76.96 P.T. STA. = 230+70.36	PROP. CURVE WESTTECH13 PI STA. = 231+54.51 $\Delta = 88^\circ 09' 35''$ (RT) D = 65° 55' 54" R = 86.90' T = 84.15' L = 133.71' E = 34.07' P.C. STA. = 230+70.36 P.T. STA. = 232+04.07	PROP. CURVE WESTTECH14 PI STA. = 232+76.71 $\Delta = 50^\circ 34' 18''$ (LT) D = 37° 15' 35" R = 153.77' T = 72.64' L = 135.73' E = 16.29' P.C. STA. = 232+04.07 P.T. STA. = 233+39.80	PROP. CURVE WESTTECH15 PI STA. = 233+84.23 $\Delta = 27^\circ 05' 45''$ (LT) D = 31° 04' 07" R = 184.42' T = 44.44' L = 87.21' E = 5.28' P.C. STA. = 233+39.80 P.T. STA. = 234+27.01	PROP. CURVE WESTTECH16 PI STA. = 234+83.40 $\Delta = 6^\circ 06' 08''$ (LT) D = 5° 24' 57" R = 1,057.91' T = 56.39' L = 112.67' E = 1.50' P.C. STA. = 234+27.01 P.T. STA. = 235+39.68	PROP. CURVE WESTTECH17 PI STA. = 237+43.57 $\Delta = 33^\circ 34' 44''$ (RT) D = 8° 28' 42" R = 675.79' T = 203.90' L = 396.05' E = 30.09' P.C. STA. = 235+39.68 P.T. STA. = 239+35.73	PROP. CURVE WESTTECH18 PI STA. = 241+63.54 $\Delta = 11^\circ 59' 44''$ (RT) D = 7° 59' 45" R = 716.56' T = 75.29' L = 150.02' E = 3.94' P.C. STA. = 240+88.25 P.T. STA. = 242+38.28
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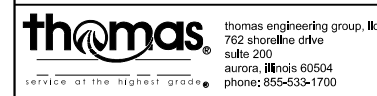


CURVE DATA

PROP. CURVE EASTTECH11 PI STA. = 131+26.52 $\Delta = 81^\circ 26' 58''$ (LT) D = 64° 30' 40" R = 88.82' T = 76.46' L = 126.26' E = 28.38' P.C. STA. = 130+50.06 P.T. STA. = 131+76.31	PROP. CURVE EASTTECH12 PI STA. = 132+19.81 $\Delta = 35^\circ 16' 55''$ (RT) D = 41° 53' 03" R = 136.80' T = 43.50' L = 84.24' E = 6.75' P.C. STA. = 131+76.31 P.T. STA. = 132+60.55	PROP. CURVE EASTTECH13 PI STA. = 135+00.69 $\Delta = 29^\circ 05' 38''$ (LT) D = 8° 28' 36" R = 675.93' T = 175.40' L = 343.23' E = 22.39' P.C. STA. = 133+25.29 P.T. STA. = 136+68.52	PROP. CURVE EASTTECH14 PI STA. = 137+47.10 $\Delta = 13^\circ 02' 31''$ (RT) D = 8° 20' 02" R = 687.51' T = 78.59' L = 156.49' E = 4.48' P.C. STA. = 136+68.52 P.T. STA. = 138+25.01	PROP. CURVE EASTTECH15 PI STA. = 140+06.79 $\Delta = 35^\circ 00' 25''$ (RT) D = 9° 56' 24" R = 576.42' T = 181.78' L = 352.18' E = 27.98' P.C. STA. = 138+25.01 P.T. STA. = 141+77.19
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HORIZONTAL SCALE IN FEET



USER NAME = DonN	DESIGNED -	REVISED -
PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/20/2022	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ALIGNMENTS AND BENCHMARKS

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
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FAU	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	21-00068-00-R5	DuPAGE	37	11
CONTRACT NO. 61J32				
ILLINOIS FED. AID PROJECT				

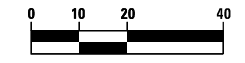
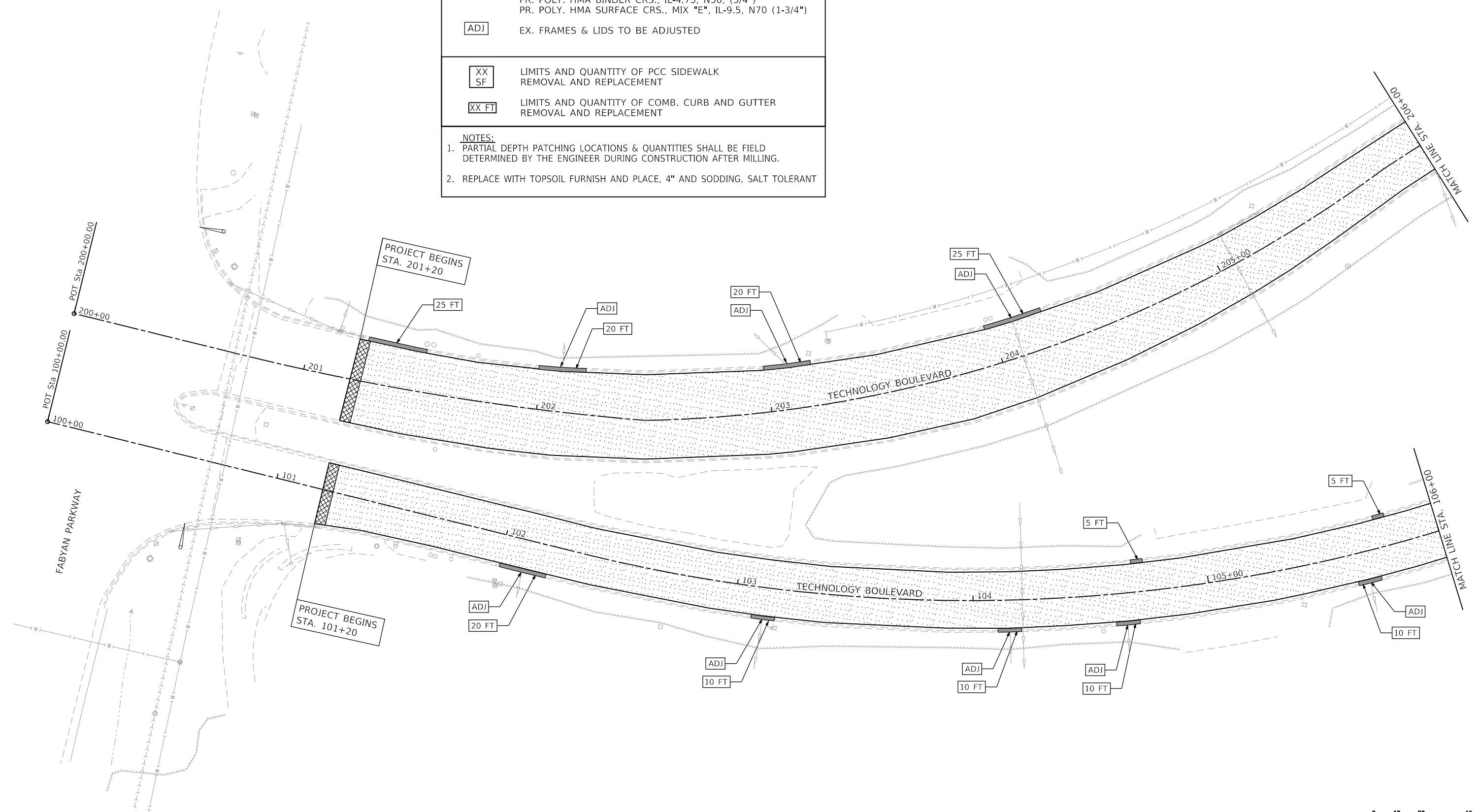


LEGEND:

- PR. HOT-MIX ASPHALT SURFACE REMOVAL-BUTT JOINTS (4.5' WIDE)
- PR. HOT-MIX ASPHALT SURFACE REMOVAL (2-1/2")
PR. POLY. HMA BINDER CRS., IL-4.75, N50, (3/4")
PR. POLY. HMA SURFACE CRS., MIX "E", IL-9.5, N70 (1-3/4")
- EX. FRAMES & LIDS TO BE ADJUSTED

- LIMITS AND QUANTITY OF PCC SIDEWALK REMOVAL AND REPLACEMENT
- LIMITS AND QUANTITY OF COMB. CURB AND GUTTER REMOVAL AND REPLACEMENT

- NOTES:**
1. PARTIAL DEPTH PATCHING LOCATIONS & QUANTITIES SHALL BE FIELD DETERMINED BY THE ENGINEER DURING CONSTRUCTION AFTER MILLING.
 2. REPLACE WITH TOPSOIL FURNISH AND PLACE, 4" AND SODDING, SALT TOLERANT



HORIZONTAL SCALE IN FEET

thomas thomas engineering group, llc
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suite 200
aurora, illinois 60504
phone: 855-533-1700
service at the highest grade

USER NAME = DonN	DESIGNED -	REVISED
PLOT SCALE = 40.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/20/2022	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROPOSED ROADWAY PLANS
TECHNOLOGY BOULEVARD**

SCALE: SHEET OF SHEETS STA. TO STA.

FAU	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	21-00068-00-R5	DuPAGE	37	12
CONTRACT NO. 61J32			ILLINOIS FED. AID PROJECT	

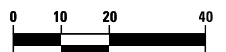
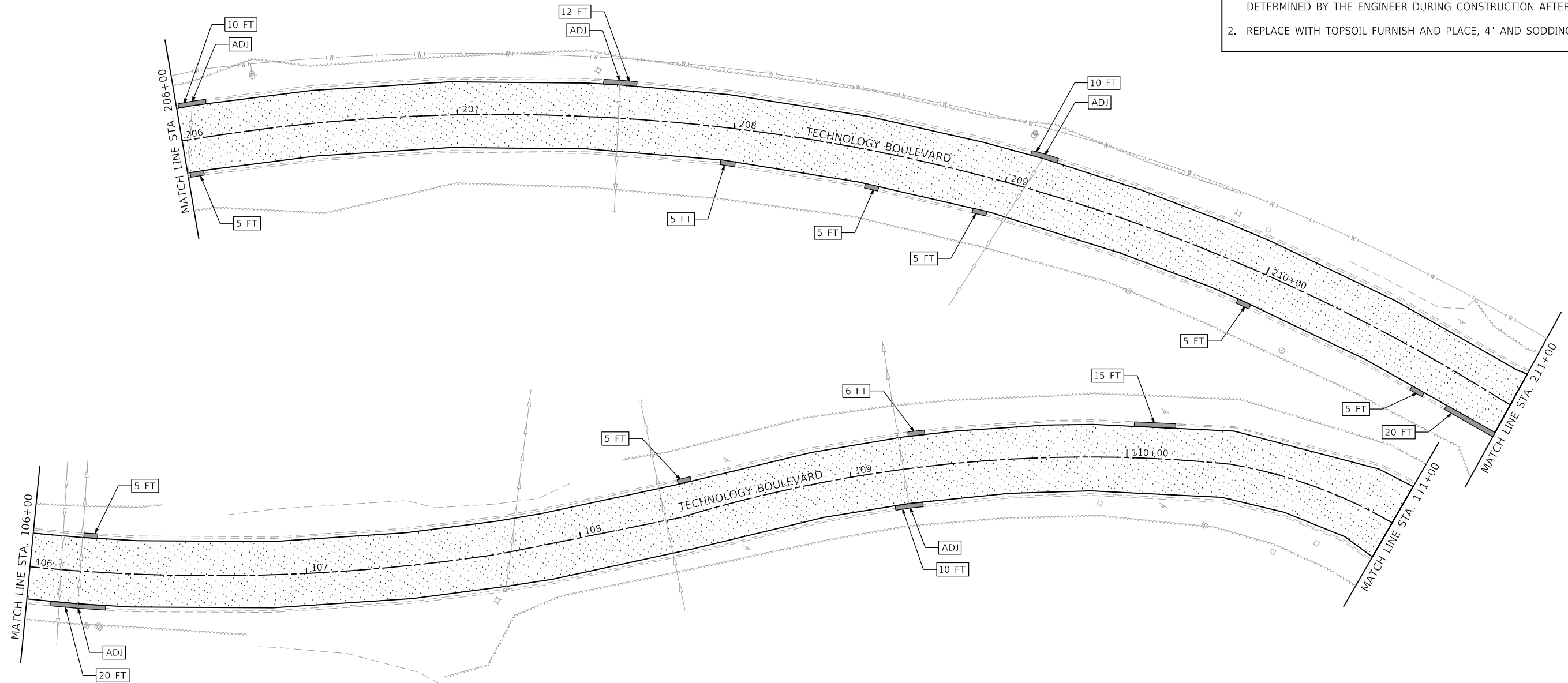


LEGEND:

- PR. HOT-MIX ASPHALT SURFACE REMOVAL- BUTT JOINTS (4.5' WIDE)
- PR. HOT-MIX ASPHALT SURFACE REMOVAL (2-1/2")
PR. POLY. HMA BINDER CRS., IL-4.75, N50, (3/4")
PR. POLY. HMA SURFACE CRS., MIX "E", IL-9.5, N70 (1-3/4")
- EX. FRAMES & LIDS TO BE ADJUSTED
- LIMITS AND QUANTITY OF PCC SIDEWALK REMOVAL AND REPLACEMENT
- LIMITS AND QUANTITY OF COMB. CURB AND GUTTER REMOVAL AND REPLACEMENT

NOTES:

1. PARTIAL DEPTH PATCHING LOCATIONS & QUANTITIES SHALL BE FIELD DETERMINED BY THE ENGINEER DURING CONSTRUCTION AFTER MILLING.
2. REPLACE WITH TOPSOIL FURNISH AND PLACE, 4" AND SODDING, SALT TOLERANT



HORIZONTAL SCALE IN FEET

thomas thomas engineering group, llc
762 shoreline drive
suite 200
aurora, illinois 60504
phone: 855-533-1700
service at the highest grade

USER NAME = DonN	DESIGNED -	REVISED -
DRAWN -	REVISOR -	
PLOT SCALE = 40.0000' / in.	CHECKED -	REVISOR -
PLOT DATE = 12/20/2022	DATE -	REVISOR -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROPOSED ROADWAY PLANS
TECHNOLOGY BOULEVARD**

SCALE: SHEET OF SHEETS STA. TO STA.

FAU	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	21-00068-00-R5		37	13
CONTRACT NO. 61J32			ILLINOIS FED. AID PROJECT	



LEGEND:

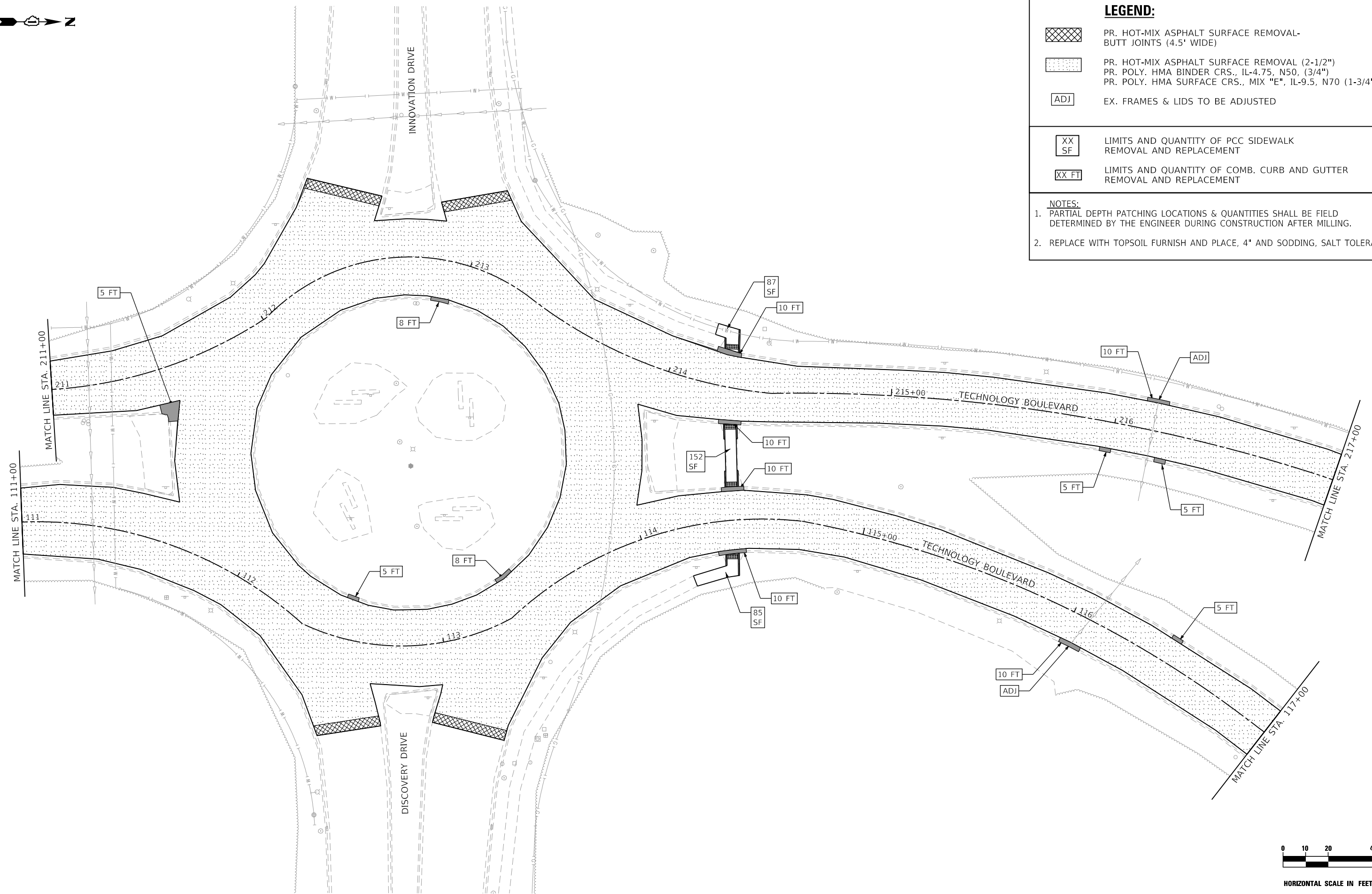
- PR. HOT-MIX ASPHALT SURFACE REMOVAL-BUTT JOINTS (4.5' WIDE)
- PR. HOT-MIX ASPHALT SURFACE REMOVAL (2-1/2")
PR. POLY. HMA BINDER CRS., IL-4.75, N50, (3/4")
PR. POLY. HMA SURFACE CRS., MIX "E", IL-9.5, N70 (1-3/4")
- ADJ EX. FRAMES & LIDS TO BE ADJUSTED

XX SF LIMITS AND QUANTITY OF PCC SIDEWALK REMOVAL AND REPLACEMENT

XX FT LIMITS AND QUANTITY OF COMB. CURB AND GUTTER REMOVAL AND REPLACEMENT

NOTES:

1. PARTIAL DEPTH PATCHING LOCATIONS & QUANTITIES SHALL BE FIELD DETERMINED BY THE ENGINEER DURING CONSTRUCTION AFTER MILLING.
2. REPLACE WITH TOPSOIL FURNISH AND PLACE, 4" AND SODDING, SALT TOLERANT



HORIZONTAL SCALE IN FEET

thomas
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 service at the highest grade

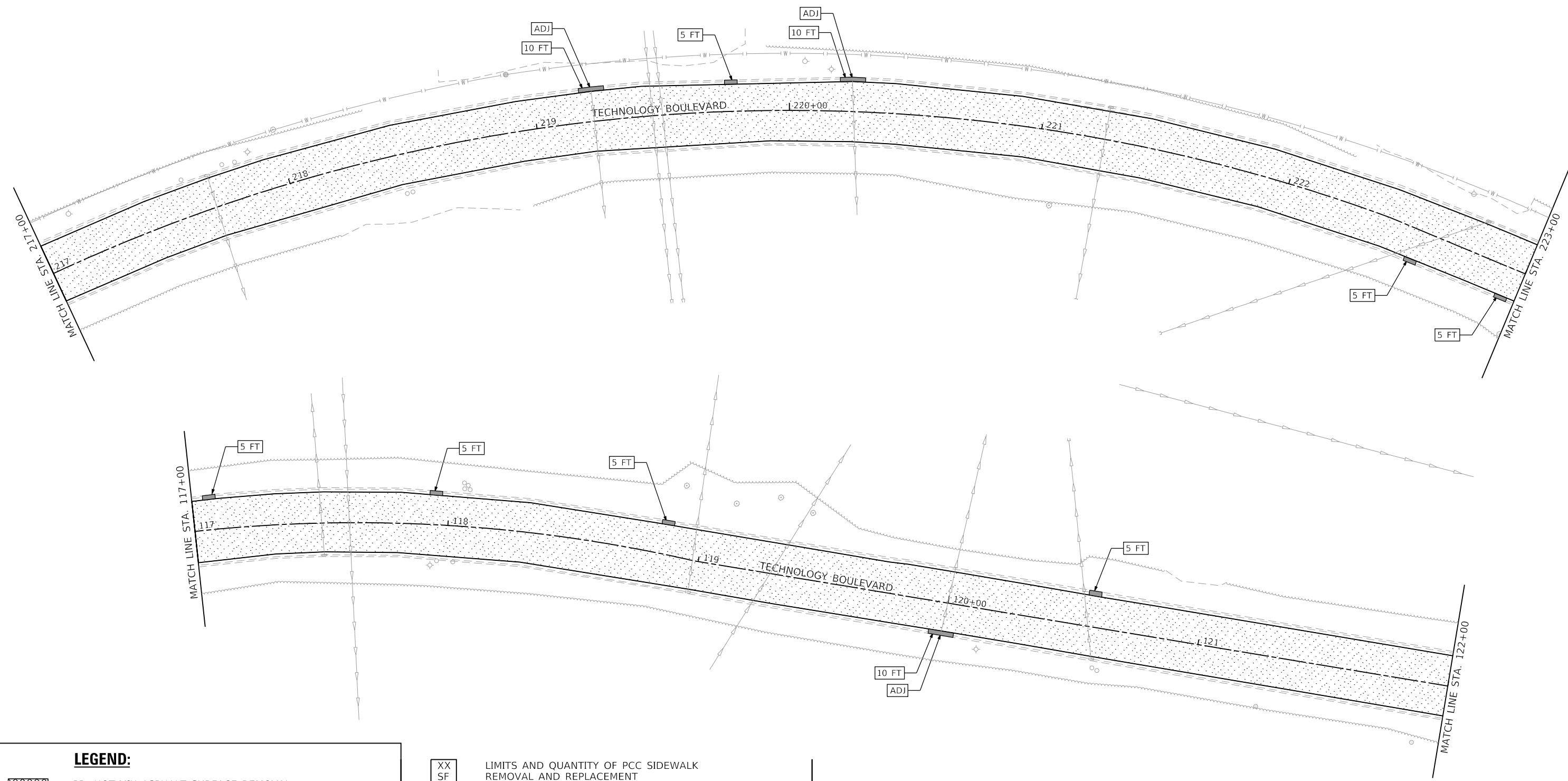
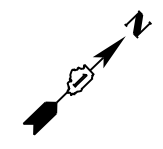
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DRAWN -	REVISÉD -	
PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/20/2022	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**




**PROPOSED ROADWAY PLANS
 TECHNOLOGY BOULEVARD**



SCALE: SHEET OF SHEETS STA. TO STA.

FAU	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	21-00068-00-R5	DUPAGE	37	14
CONTRACT NO. 61J32			ILLINOIS FED. AID PROJECT	



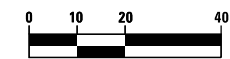
LEGEND:

-  PR. HOT-MIX ASPHALT SURFACE REMOVAL- BUTT JOINTS (4.5' WIDE)
-  PR. HOT-MIX ASPHALT SURFACE REMOVAL (2-1/2")
PR. POLY. HMA BINDER CRS., IL-4.75, N50, (3/4")
PR. POLY. HMA SURFACE CRS., MIX "E", IL-9.5, N70 (1-3/4")
-  EX. FRAMES & LIDS TO BE ADJUSTED

-  LIMITS AND QUANTITY OF PCC SIDEWALK REMOVAL AND REPLACEMENT
-  LIMITS AND QUANTITY OF COMB. CURB AND GUTTER REMOVAL AND REPLACEMENT

NOTES:

1. PARTIAL DEPTH PATCHING LOCATIONS & QUANTITIES SHALL BE FIELD DETERMINED BY THE ENGINEER DURING CONSTRUCTION AFTER MILLING.
2. REPLACE WITH TOPSOIL FURNISH AND PLACE, 4" AND SODDING, SALT TOLERANT



HORIZONTAL SCALE IN FEET

thomas thomas engineering group, llc
762 shoreline drive
suite 200
aurora, illinois 60504
phone: 855-533-1700

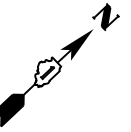
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PLOT SCALE = 40.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/20/2022	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**






**PROPOSED ROADWAY PLANS
TECHNOLOGY BOULEVARD**

SCALE: SHEET OF SHEETS STA. TO STA.

FAU	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	21-00068-00-R5		37	15
CONTRACT NO. 61J32			ILLINOIS FED. AID PROJECT	

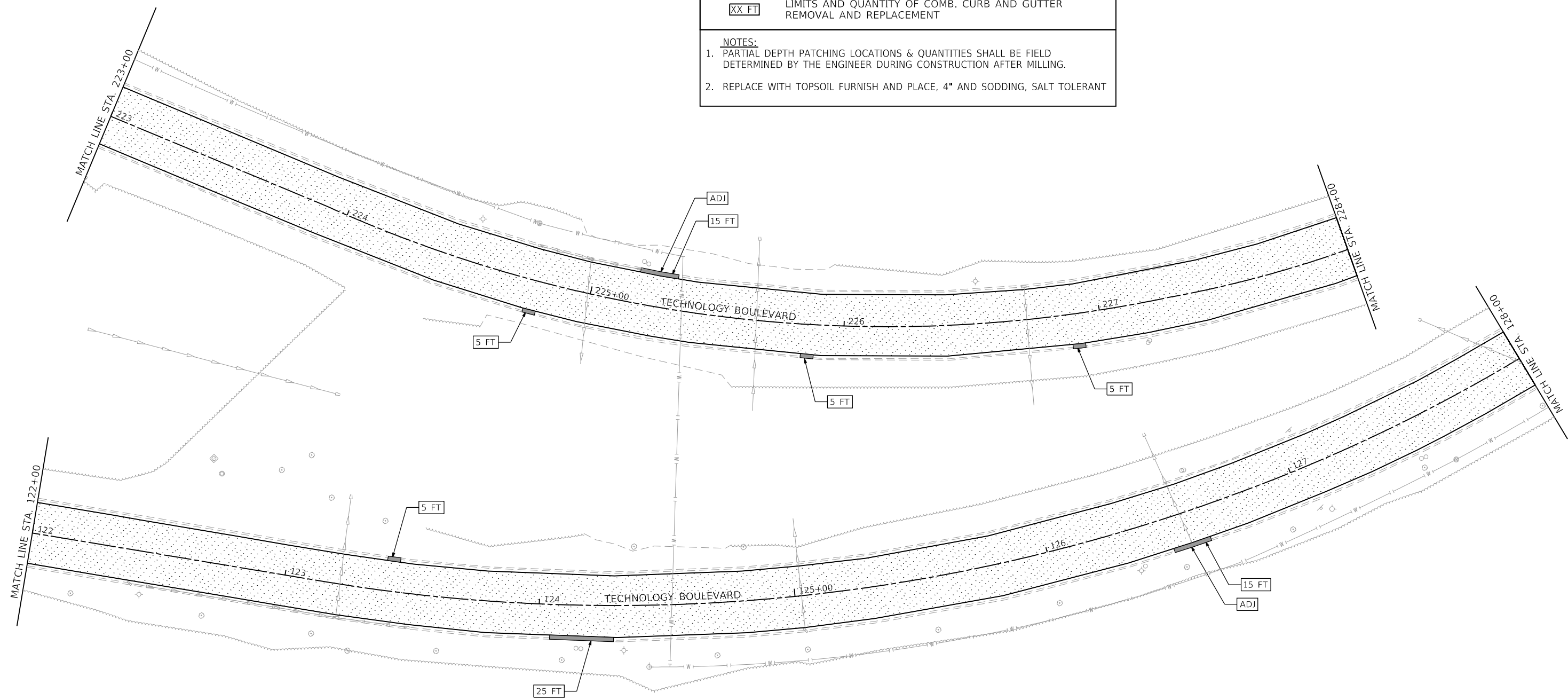


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HORIZONTAL SCALE IN FEET

thomas
 thomas engineering group, llc
 762 shoreline drive
 suite 200
 aurora, illinois 60504
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 service at the highest grade

USER NAME = DonN	DESIGNED -	REVISED
	DRAWN -	REVISED -
PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/20/2022	DATE -	REVISED -




**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PROPOSED ROADWAY PLANS
 TECHNOLOGY BOULEVARD**

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
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FAU	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	21-00068-00-R5	DuPAGE	37	16
CONTRACT NO. 61J32				
ILLINOIS FED. AID PROJECT				

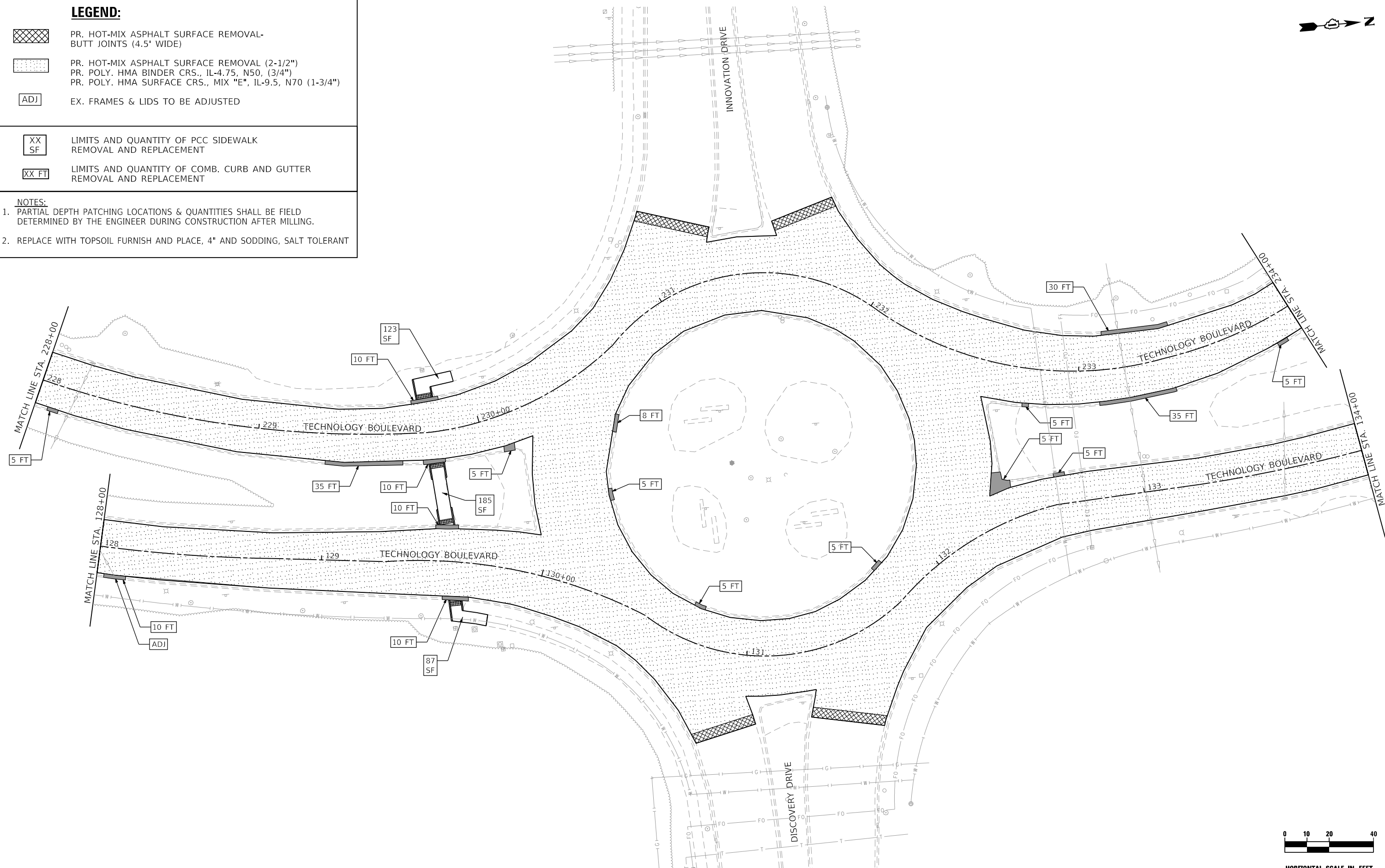
LEGEND:

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-  EX. FRAMES & LIDS TO BE ADJUSTED

 LIMITS AND QUANTITY OF PCC SIDEWALK REMOVAL AND REPLACEMENT

 LIMITS AND QUANTITY OF COMB. CURB AND GUTTER REMOVAL AND REPLACEMENT

- NOTES:**
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	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROPOSED ROADWAY PLANS
TECHNOLOGY BOULEVARD**

SCALE: SHEET OF SHEETS STA. TO STA.

FAU	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	21-00068-00-R5	DuPAGE	37	17
CONTRACT NO. 61J32			ILLINOIS FED. AID PROJECT	



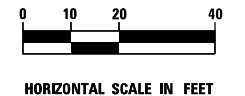
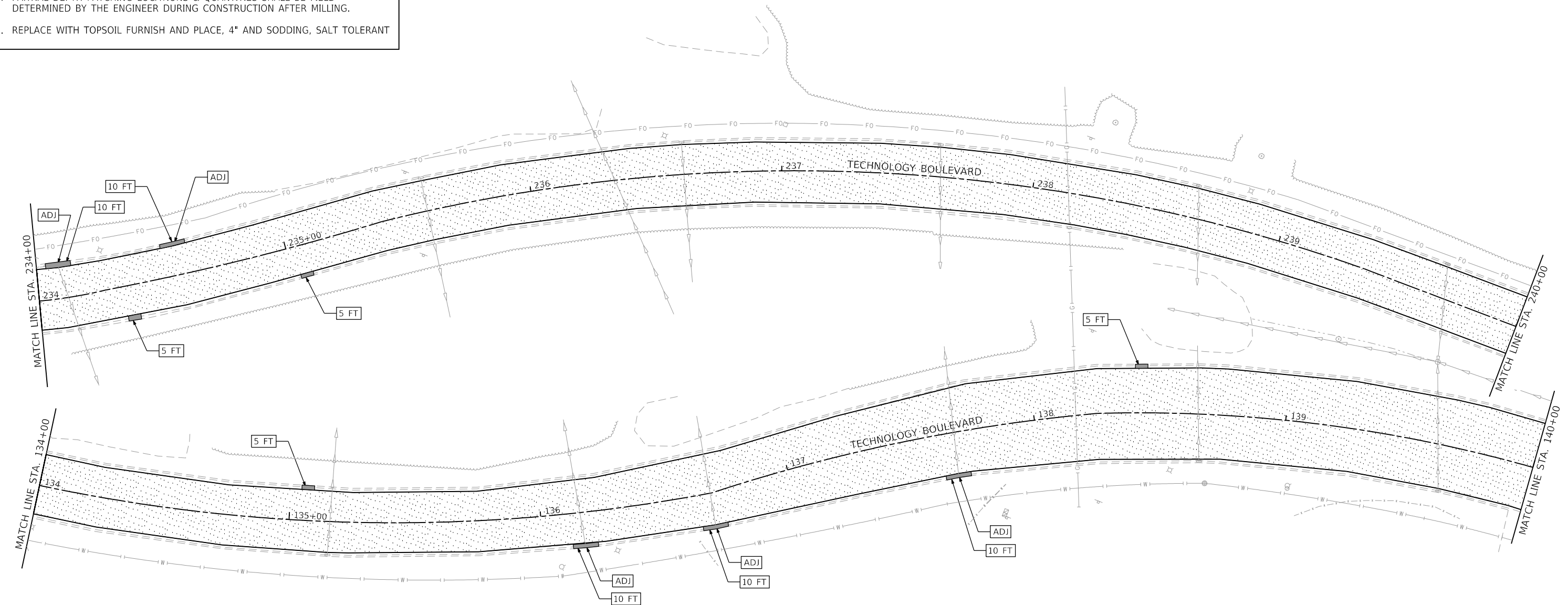
LEGEND:

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- ADJ EX. FRAMES & LIDS TO BE ADJUSTED

XX
SF LIMITS AND QUANTITY OF PCC SIDEWALK
REMOVAL AND REPLACEMENT

XX FT LIMITS AND QUANTITY OF COMB. CURB AND GUTTER
REMOVAL AND REPLACEMENT

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	DATE -	REVISED -	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROPOSED ROADWAY PLANS
TECHNOLOGY BOULEVARD**

SCALE: SHEET OF SHEETS STA. TO STA.

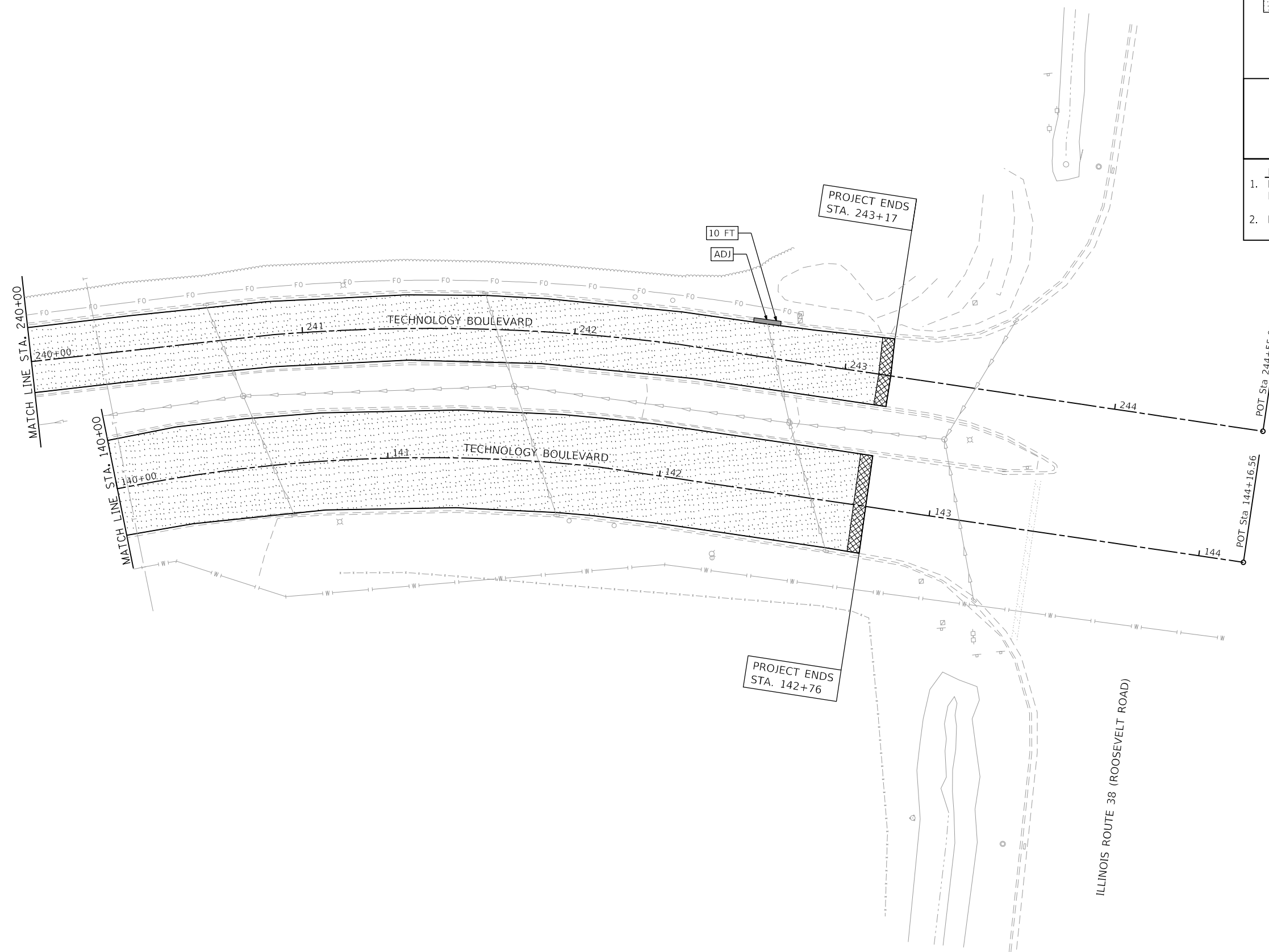
FAU	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	21-00068-00-R5	DuPAGE	37	18
CONTRACT NO. 61J32				
ILLINOIS FED. AID PROJECT				



LEGEND:

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HORIZONTAL SCALE IN FEET

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 service at the highest grade

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PLOT SCALE = 40.0000' / in.	DRAWN -	REVISED -
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	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PROPOSED ROADWAY PLANS
 TECHNOLOGY BOULEVARD**

SCALE: SHEET OF SHEETS STA. TO STA.

FAU	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	21-00068-00-R5	DuPAGE	37	19
CONTRACT NO. 61J32			ILLINOIS FED. AID PROJECT	

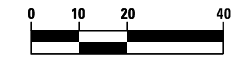
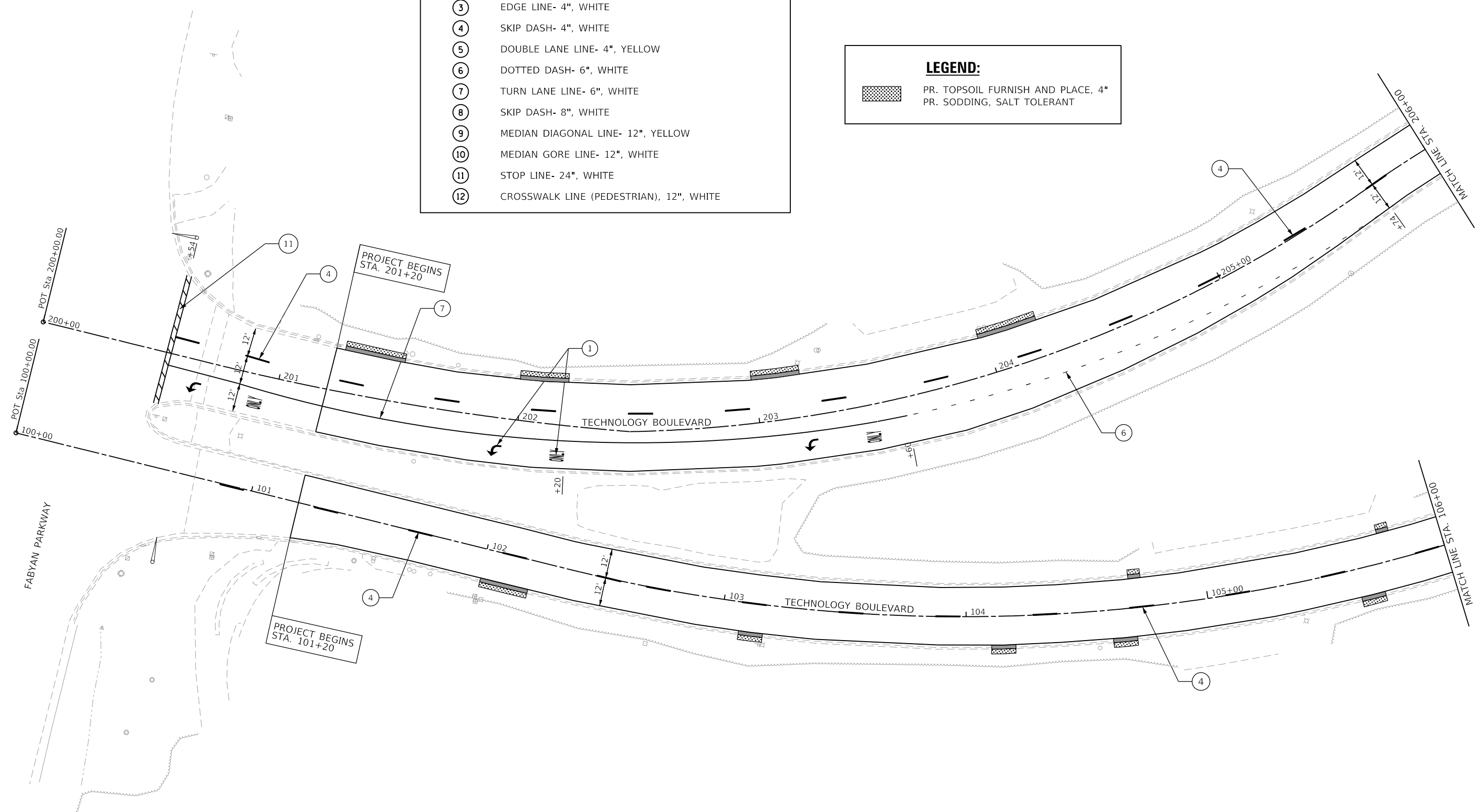


THERMOPLASTIC PAVEMENT MARKING (TYPICAL WHERE SHOWN)

①	LETTERS AND SYMBOLS, WHITE
②	EDGE LINE- 4", YELLOW
③	EDGE LINE- 4", WHITE
④	SKIP DASH- 4", WHITE
⑤	DOUBLE LANE LINE- 4", YELLOW
⑥	DOTTED DASH- 6", WHITE
⑦	TURN LANE LINE- 6", WHITE
⑧	SKIP DASH- 8", WHITE
⑨	MEDIAN DIAGONAL LINE- 12", YELLOW
⑩	MEDIAN GORE LINE- 12", WHITE
⑪	STOP LINE- 24", WHITE
⑫	CROSSWALK LINE (PEDESTRIAN), 12", WHITE

LEGEND:

	PR. TOPSOIL FURNISH AND PLACE, 4"
	PR. SODDING, SALT TOLERANT



HORIZONTAL SCALE IN FEET

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suite 200
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USER NAME = DonN	DESIGNED -	REVISED
PLOT SCALE = 40.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/20/2022	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING AND LANDSCAPING PLANS
TECHNOLOGY BOULEVARD**

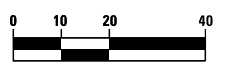
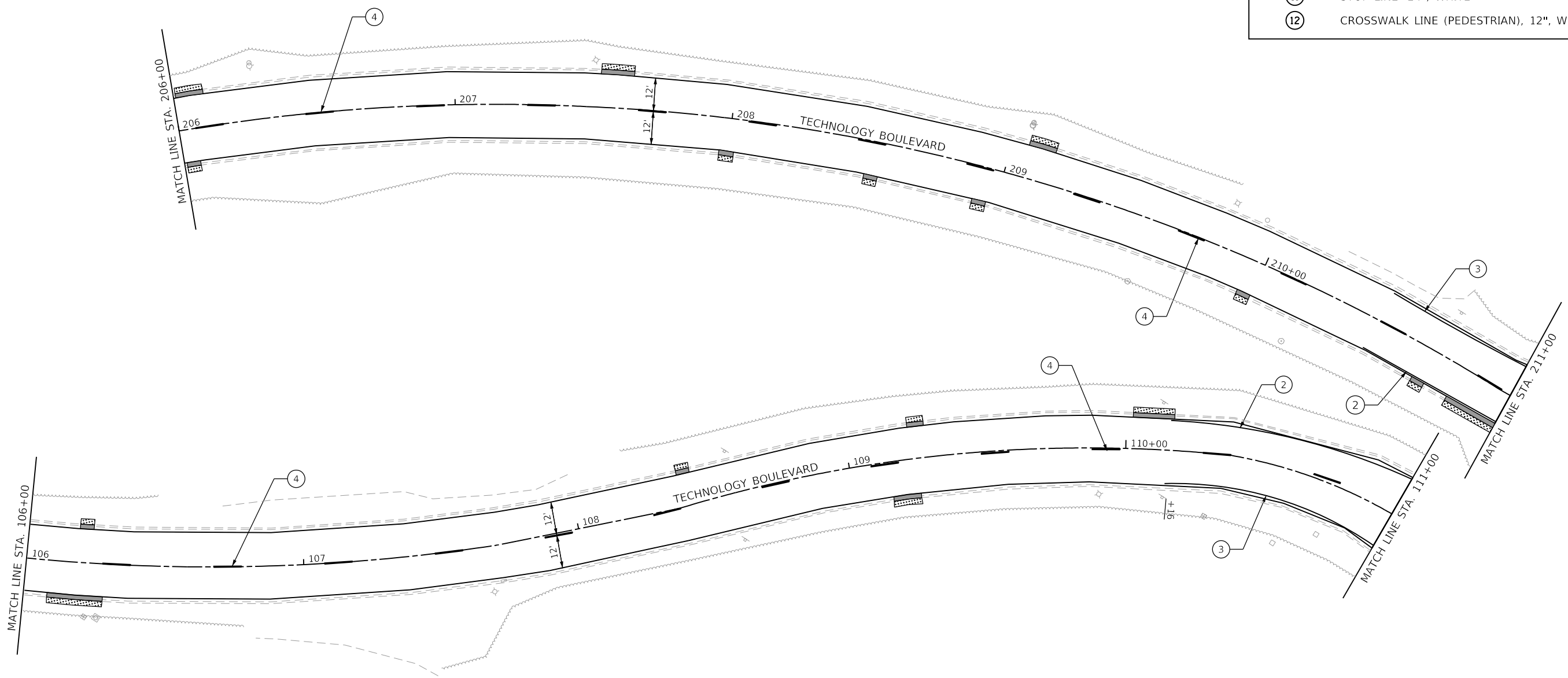
SCALE: SHEET OF SHEETS STA. TO STA.

FAU	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	21-00068-00-R5	DuPAGE	37	20
CONTRACT NO. 61J32			ILLINOIS FED. AID PROJECT	



LEGEND:
 PR. TOPSOIL FURNISH AND PLACE, 4"
 PR. SODDING, SALT TOLERANT

- THERMOPLASTIC PAVEMENT MARKING (TYPICAL WHERE SHOWN)**
- ① LETTERS AND SYMBOLS, WHITE
 - ② EDGE LINE- 4", YELLOW
 - ③ EDGE LINE- 4", WHITE
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 - ⑫ CROSSWALK LINE (PEDESTRIAN), 12", WHITE



HORIZONTAL SCALE IN FEET

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USER NAME = DonN	DESIGNED -	REVISED
DRAWN -	REVISOR -	
PLOT SCALE = 40.0000' / in.	CHECKED -	REVISOR -
PLOT DATE = 12/20/2022	DATE -	REVISOR -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING AND LANDSCAPING PLANS
 TECHNOLOGY BOULEVARD**

SCALE: SHEET OF SHEETS STA. TO STA.

FAU	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 61J32				
ILLINOIS FED. AID PROJECT				

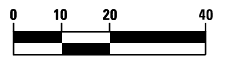
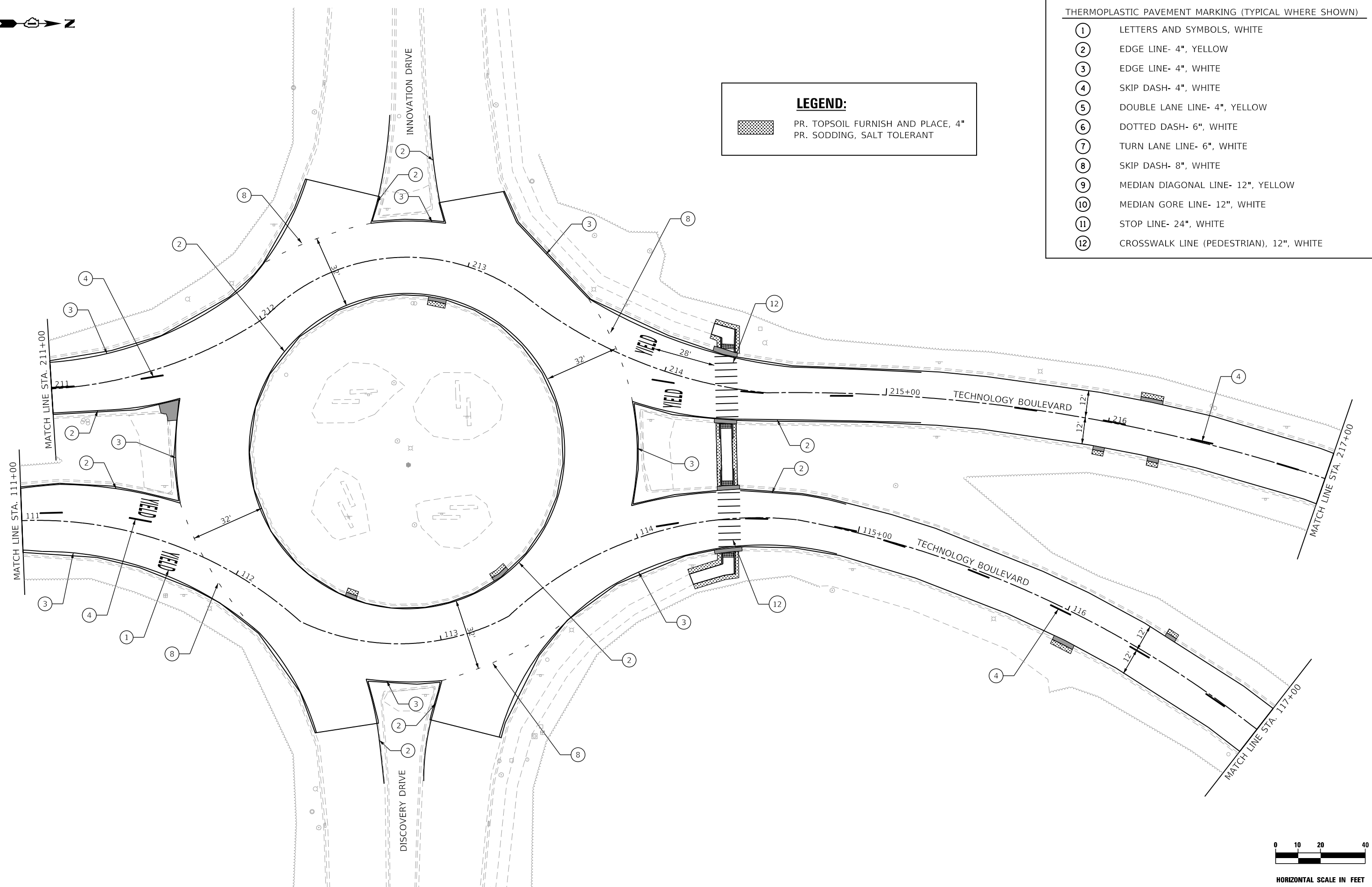


THERMOPLASTIC PAVEMENT MARKING (TYPICAL WHERE SHOWN)

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- ② EDGE LINE- 4", YELLOW
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- ⑤ DOUBLE LANE LINE- 4", YELLOW
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LEGEND:

- PR. TOPSOIL FURNISH AND PLACE, 4"
- PR. SODDING, SALT TOLERANT



HORIZONTAL SCALE IN FEET

thomas thomas engineering group, llc
762 shoreline drive
suite 200
aurora, illinois 60504
phone: 855-533-1700
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USER NAME = DonN	DESIGNED -	REVISD -
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PLOT SCALE = 40.0000' / in.	CHECKED -	REVISD -
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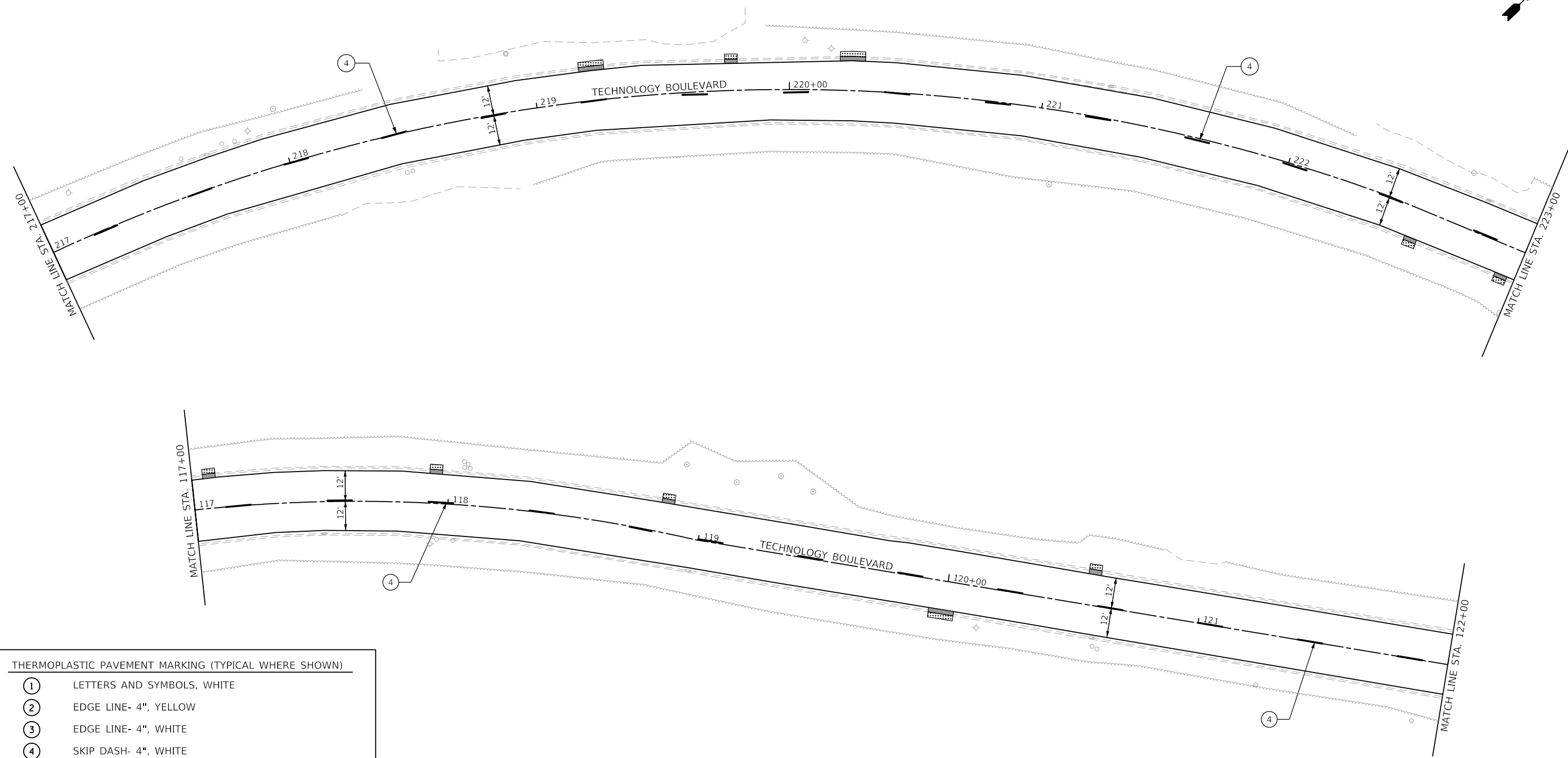
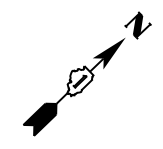
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DRAWN -	REVISD -
CHECKED -	REVISD -
DATE -	REVISD -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING AND LANDSCAPING PLANS
TECHNOLOGY BOULEVARD**

SCALE: SHEET OF SHEETS STA. TO STA.

FAU	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	21-00068-00-R5	DuPAGE	37	22
CONTRACT NO. 61J32			ILLINOIS FED. AID PROJECT	

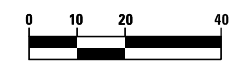


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LEGEND:

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	PR. SODDING, SALT TOLERANT



HORIZONTAL SCALE IN FEET

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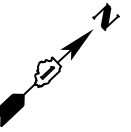
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING AND LANDSCAPING PLANS
TECHNOLOGY BOULEVARD**

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
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FAU	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	21-00068-00-R5		37	23
CONTRACT NO. 61J32			ILLINOIS FED. AID PROJECT	

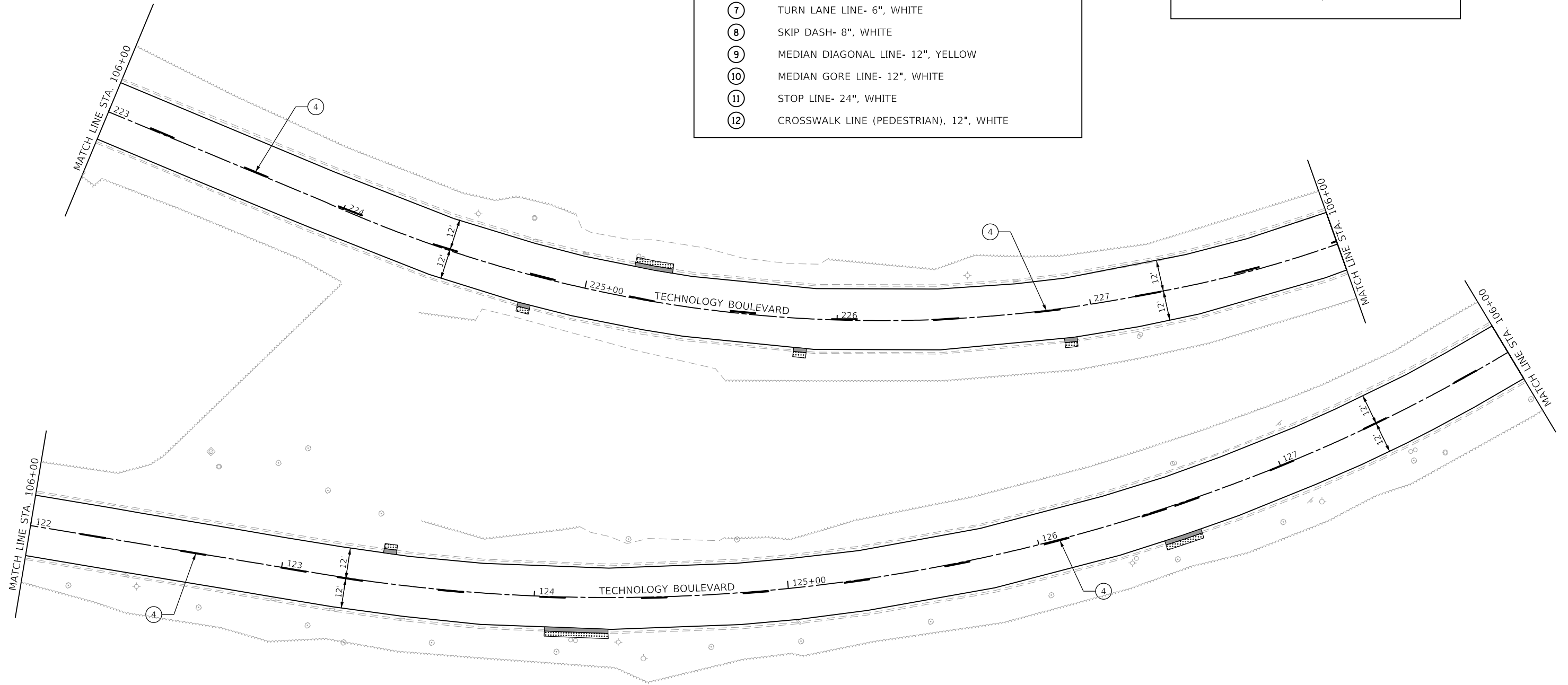


THERMOPLASTIC PAVEMENT MARKING (TYPICAL WHERE SHOWN)

- ① LETTERS AND SYMBOLS, WHITE
- ② EDGE LINE- 4", YELLOW
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LEGEND:

- PR. TOPSOIL FURNISH AND PLACE, 4"
- PR. SODDING, SALT TOLERANT



HORIZONTAL SCALE IN FEET

thomas
 thomas engineering group, llc
 762 shoreline drive
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 aurora, illinois 60504
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 service at the highest grade

USER NAME = DonN	DESIGNED -	REVISED -
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**STATE OF ILLINOIS
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**PAVEMENT MARKING AND LANDSCAPING PLANS
 TECHNOLOGY BOULEVARD**

SCALE: SHEET OF SHEETS STA. TO STA.

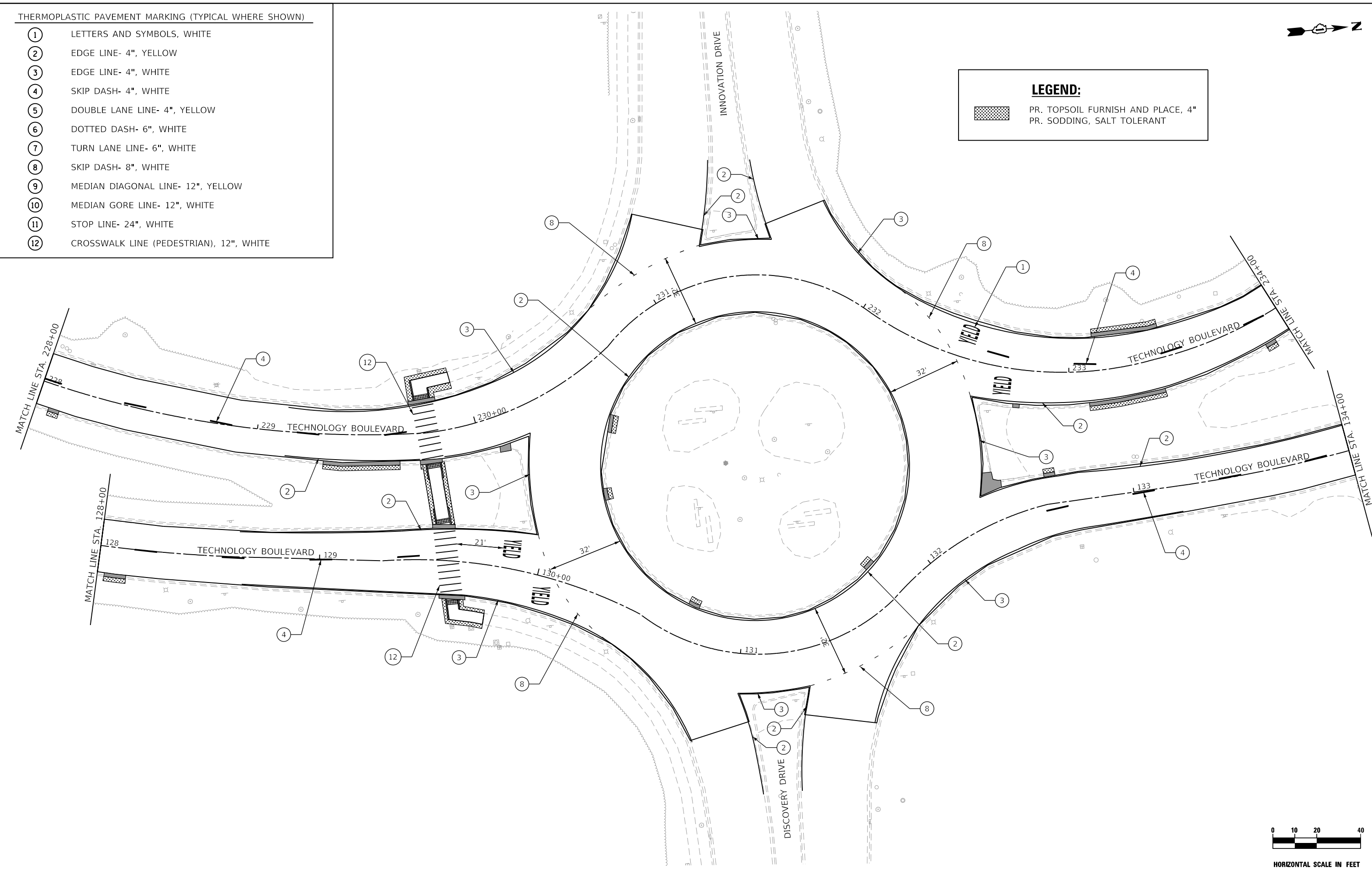
FAU	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	21-00068-00-R5	DuPAGE	37	24
CONTRACT NO. 61J32				
ILLINOIS FED. AID PROJECT				

THERMOPLASTIC PAVEMENT MARKING (TYPICAL WHERE SHOWN)

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LEGEND:

- PR. TOPSOIL FURNISH AND PLACE, 4"
- PR. SODDING, SALT TOLERANT



HORIZONTAL SCALE IN FEET

thomas service at the highest grade
 thomas engineering group, llc
 762 shoreline drive
 suite 200
 aurora, illinois 60504
 phone: 855-533-1700

USER NAME = DonN	DESIGNED -	REVISED -
PLOT SCALE = 40.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/20/2022	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING AND LANDSCAPING PLANS
 TECHNOLOGY BOULEVARD**

SCALE: SHEET OF SHEETS STA. TO STA.

FAU	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	21-00068-00-R5	DuPAGE	37	25
CONTRACT NO. 61J32			ILLINOIS FED. AID PROJECT	

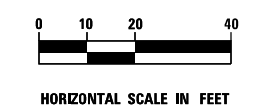
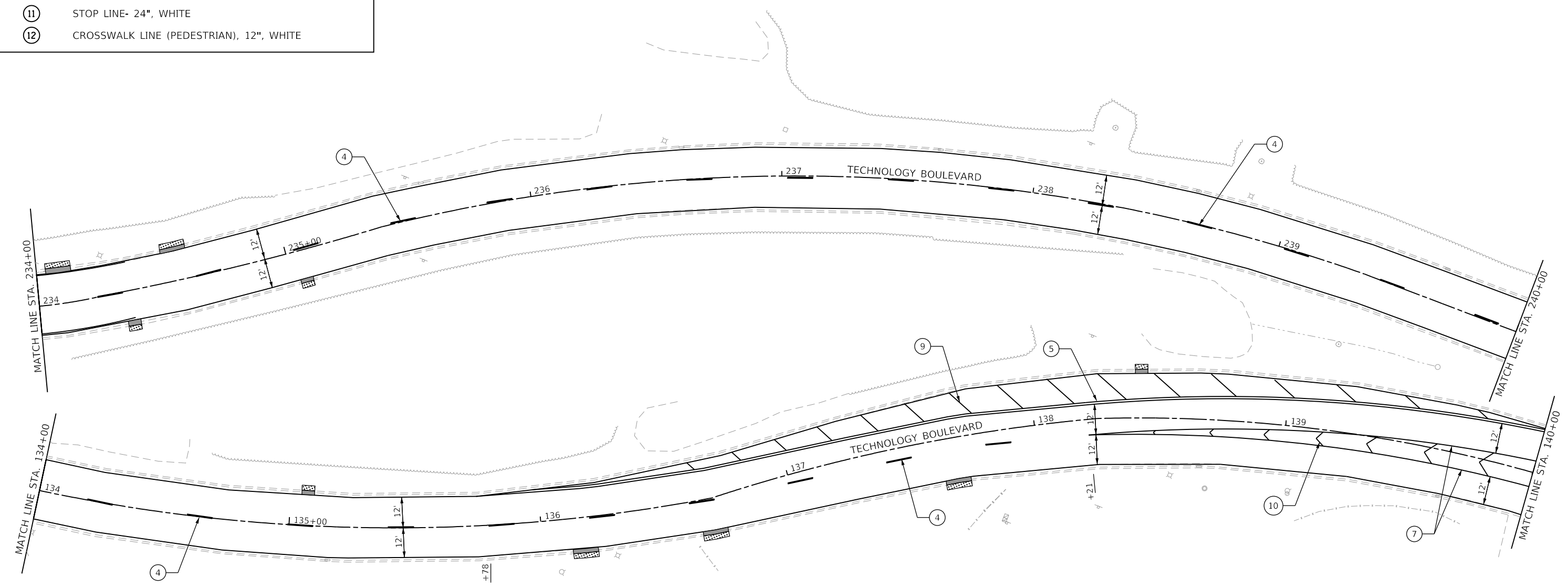


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- ⑩ MEDIAN GORE LINE- 12", WHITE
- ⑪ STOP LINE- 24", WHITE
- ⑫ CROSSWALK LINE (PEDESTRIAN), 12", WHITE

LEGEND:

PR. TOPSOIL FURNISH AND PLACE, 4"
PR. SODDING, SALT TOLERANT



thomas engineering group, llc
762 shoreline drive
suite 200
aurora, illinois 60504
phone: 855-533-1700

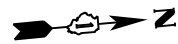
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING AND LANDSCAPING PLANS
TECHNOLOGY BOULEVARD**

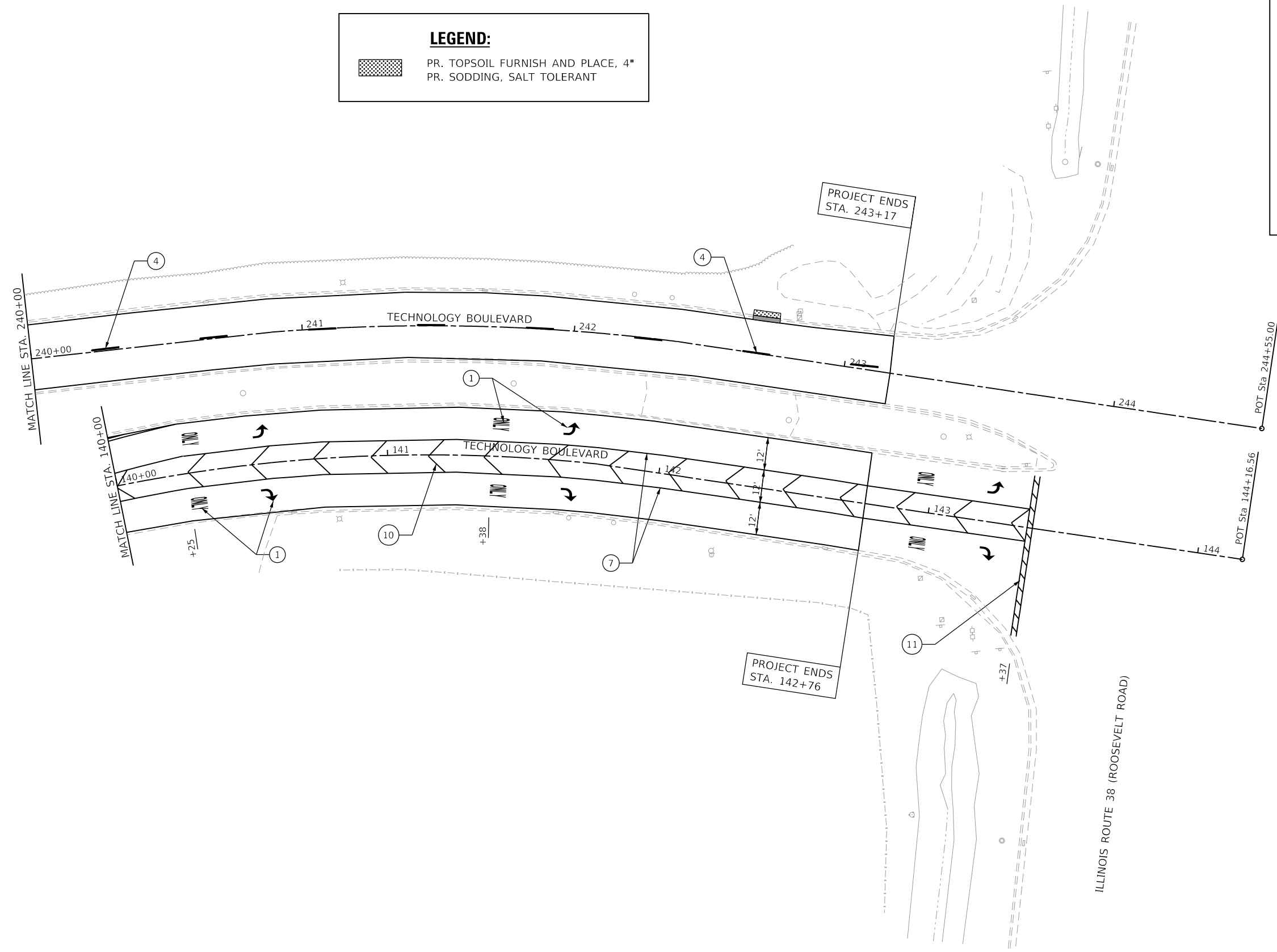
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FAU	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	21-00068-00-R5	DuPAGE	37	26
CONTRACT NO. 61J32				
ILLINOIS FED. AID PROJECT				



LEGEND:

 PR. TOPSOIL FURNISH AND PLACE, 4"
PR. SODDING, SALT TOLERANT



THERMOPLASTIC PAVEMENT MARKING (TYPICAL WHERE SHOWN)

①	LETTERS AND SYMBOLS, WHITE
②	EDGE LINE- 4", YELLOW
③	EDGE LINE- 4", WHITE
④	SKIP DASH- 4", WHITE
⑤	DOUBLE LANE LINE- 4", YELLOW
⑥	DOTTED DASH- 6", WHITE
⑦	TURN LANE LINE- 6", WHITE
⑧	SKIP DASH- 8", WHITE
⑨	MEDIAN DIAGONAL LINE- 12", YELLOW
⑩	MEDIAN GORE LINE- 12", WHITE
⑪	STOP LINE- 24", WHITE
⑫	CROSSWALK LINE (PEDESTRIAN), 12", WHITE



HORIZONTAL SCALE IN FEET

thomas
thomas engineering group, llc
762 shoreline drive
suite 200
aurora, illinois 60504
phone: 855-533-1700
service at the highest grade

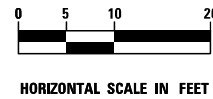
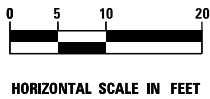
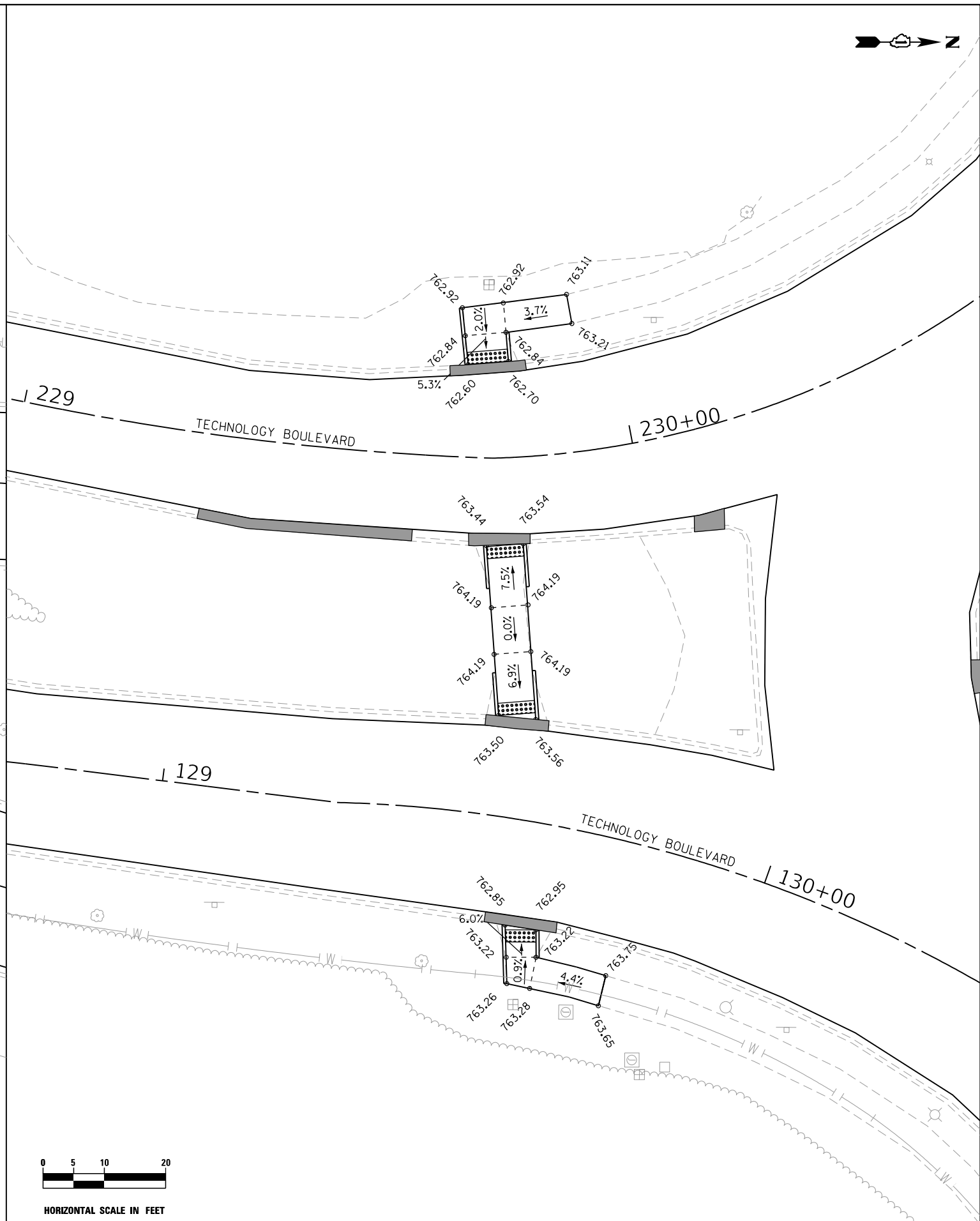
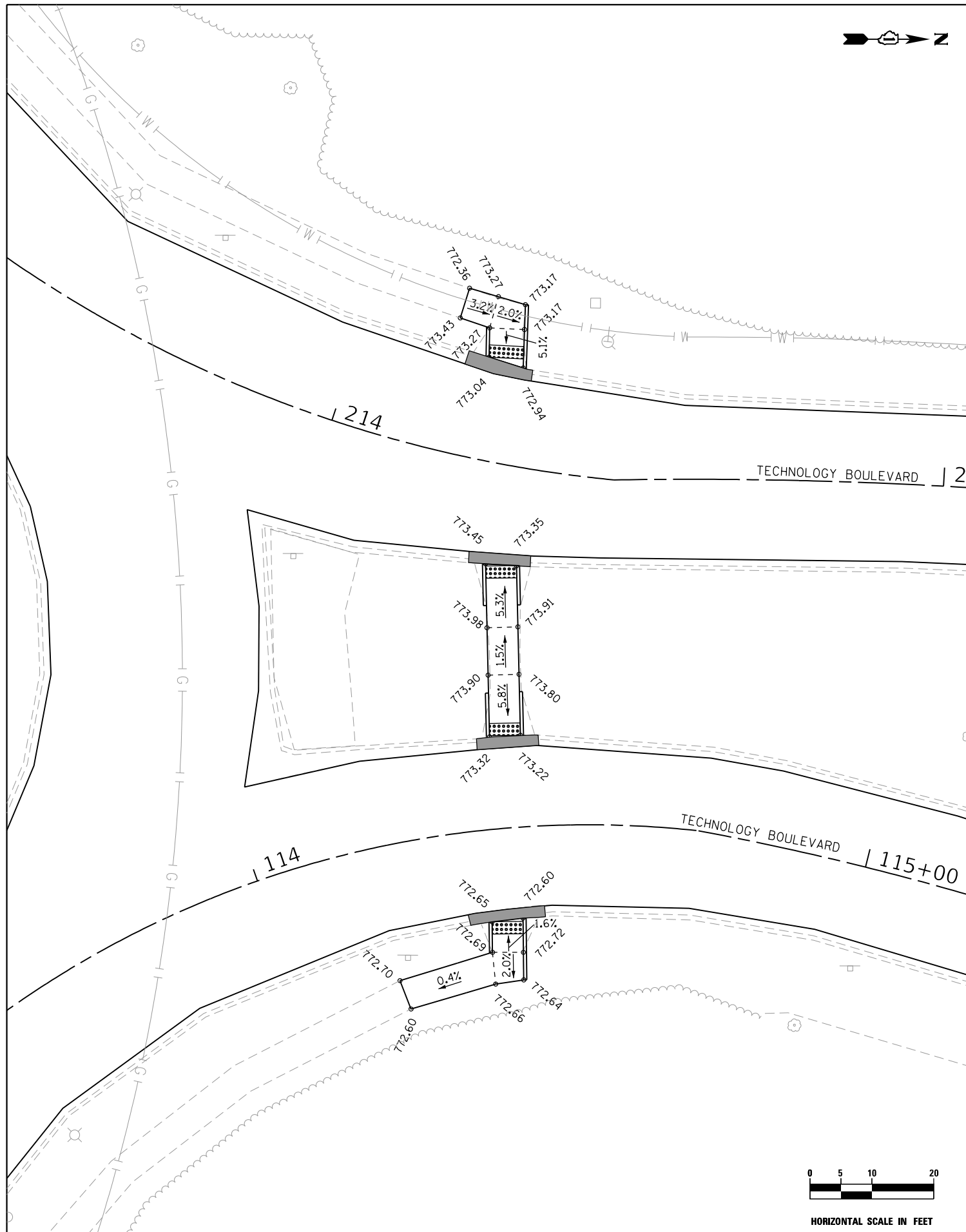
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING AND LANDSCAPING PLANS
TECHNOLOGY BOULEVARD

SCALE: SHEET OF SHEETS STA. TO STA.

FAU	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	21-00068-00-R5	DuPAGE	37	27
				CONTRACT NO. 61J32
ILLINOIS FED. AID PROJECT				



thomas service at the highest grade.
 thomas engineering group, llc
 762 shoreline drive
 suite 200
 aurora, illinois 60504
 phone: 855-533-1700

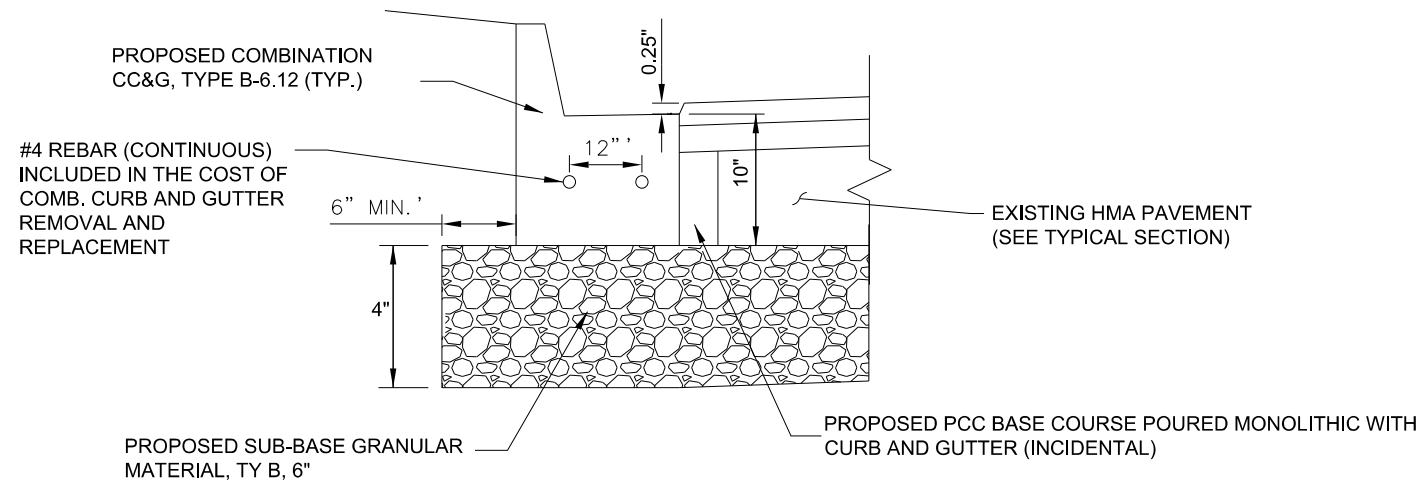
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAILS

SCALE: SHEET OF SHEETS STA. TO STA.

FAU	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	21-00068-00-R5	DuPAGE	37	28
CONTRACT NO. 61J32			ILLINOIS FED. AID PROJECT	



DETAIL A: COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12

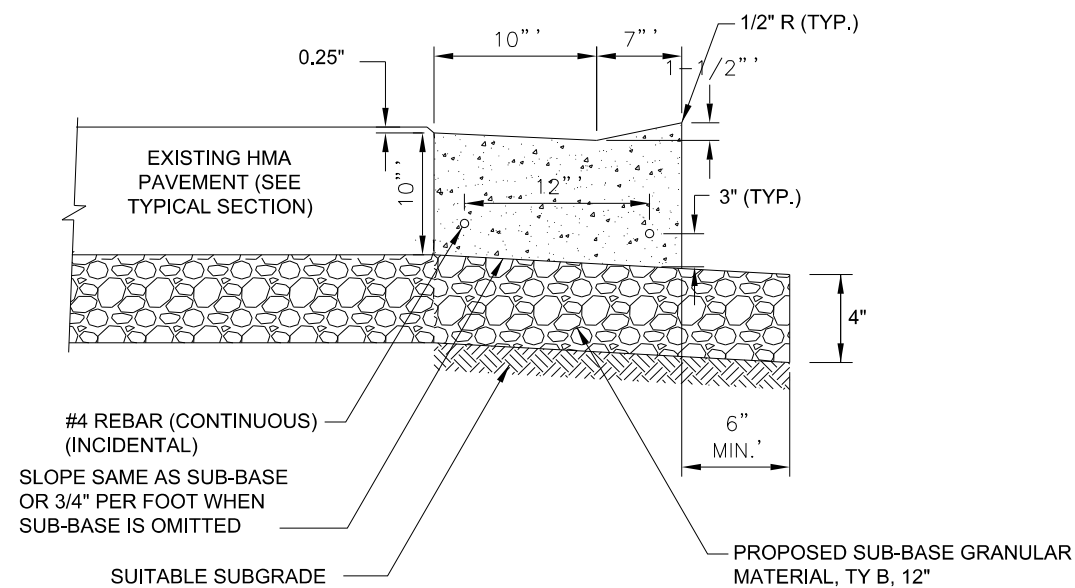
NOT TO SCALE

COMBINATION CONCRETE CURB AND GUTTER REQUIREMENTS

1. TWO NO. 4 REINFORCEMENT BARS SHALL BE INSTALLED CONTINUOUSLY IN ALL CURB AND GUTTER.
2. WHEN CURB AND GUTTER IS CONSTRUCTED ADJACENT TO EXISTING PAVEMENT, THE VERTICAL THICKNESS OF THE GUTTER FLAG SHALL BE 9" OR EQUAL TO THE THICKNESS OF THE ADJACENT PAVEMENT, WHICHEVER IS GREATER. ALSO, TIE BARS SHALL BE OMITTED.
3. DRAINAGE OPENINGS - AT ALL LOCATIONS WHERE METAL CASTINGS ARE TO BE INCORPORATED IN THE CURB AND GUTTER, A 1" THICK PREFORMED JOINT FILLER, CONFORMING TO THE CROSS SECTIONS OF THE CURB AND GUTTER, SHALL BE INSTALLED IN THE CURB AND GUTTER A DISTANCE OF 5FT FROM EACH SIDE OF THE METAL CASTING.
4. TRANSITIONS - THE TRANSITION FROM FULL HEIGHT CURB TO DEPRESSED CURB SHALL BE MADE AT THE RATE OF 2" PER FOOT OF LENGTH OR FLATTER.
5. JOINTS - IN ADDITION TO THE REQUIREMENT OF HIGHWAY STANDARD 606001 AND ARTICLE 606.06 OF THE STANDARD SPECIFICATIONS, JOINTS SHALL BE CONSTRUCTED AS FOLLOWS:

CONSTRUCTION JOINTS AND EXPANSION JOINTS SHALL BE INSTALLED IN THE CURB AND GUTTER IN PROLONGATION WITH JOINTS IN ADJACENT P.C.C. PAVEMENT OR BASE COURSE.

ALL EXPANSION JOINTS SHALL BE PROVIDED WITH A 1 1/4" DIA. X 18" COATED SMOOTH DOWEL BAR CONFORMING TO ARTICLE 1006.11(b) OF THE STANDARD SPECIFICATIONS. THE DOWEL BAR SHALL BE FITTED WITH A CAP HAVING A PINCHED STOP THAT WILL PROVIDE 1" OF EXPANSION.



DETAIL B: DEPRESSED CURB FOR INTERSECTIONS

NOT TO SCALE

NOTES:

1. THE TOP OF CURB SHALL BE DEPRESSED WHERE THE CURB AND GUTTER IS CONSTRUCTED ACROSS ALLEYS, DRIVEWAY, AND SIDEWALKS AS DETAILED IN THE PLANS OR WHERE DIRECTED BY THE ENGINEER OR PUBLIC WORKS DEPARTMENT.

NOTES:

DETECTABLE WARNING TILES SHALL ALIGN WITH THE CROSSWALK STRIPES OR STREET CROSSING. IF FIELD CONDITIONS PROHIBIT THIS, TILE ALIGNMENT SHALL BE AT THE DISCRETION OF THE ENGINEER.

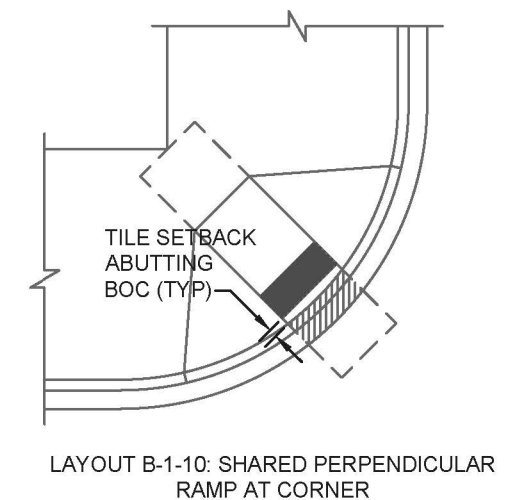
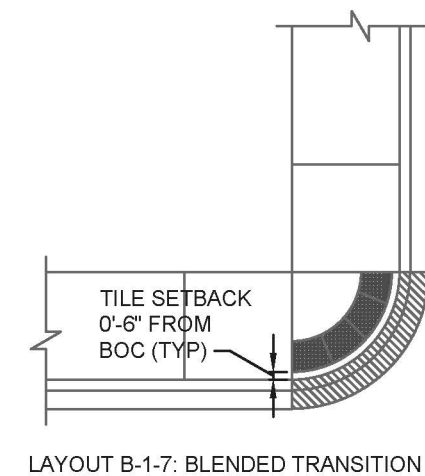
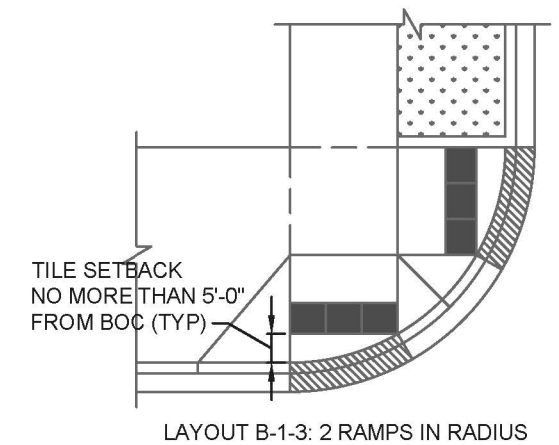
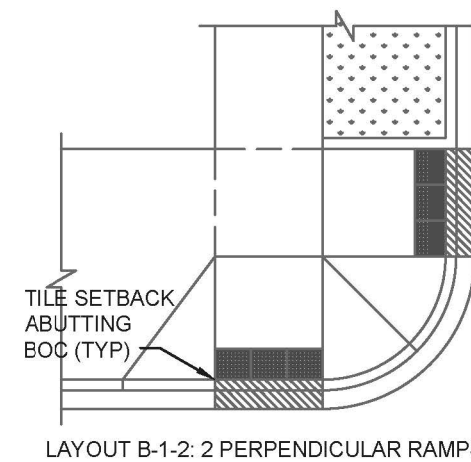
CURB RAMP LAYOUT B-1-2 IS PREFERRED WHENEVER POSSIBLE. WHERE RAMPS ARE LOCATED IN THE CORNER RADIUS, LAYOUT B-1-3 SHALL BE USED.

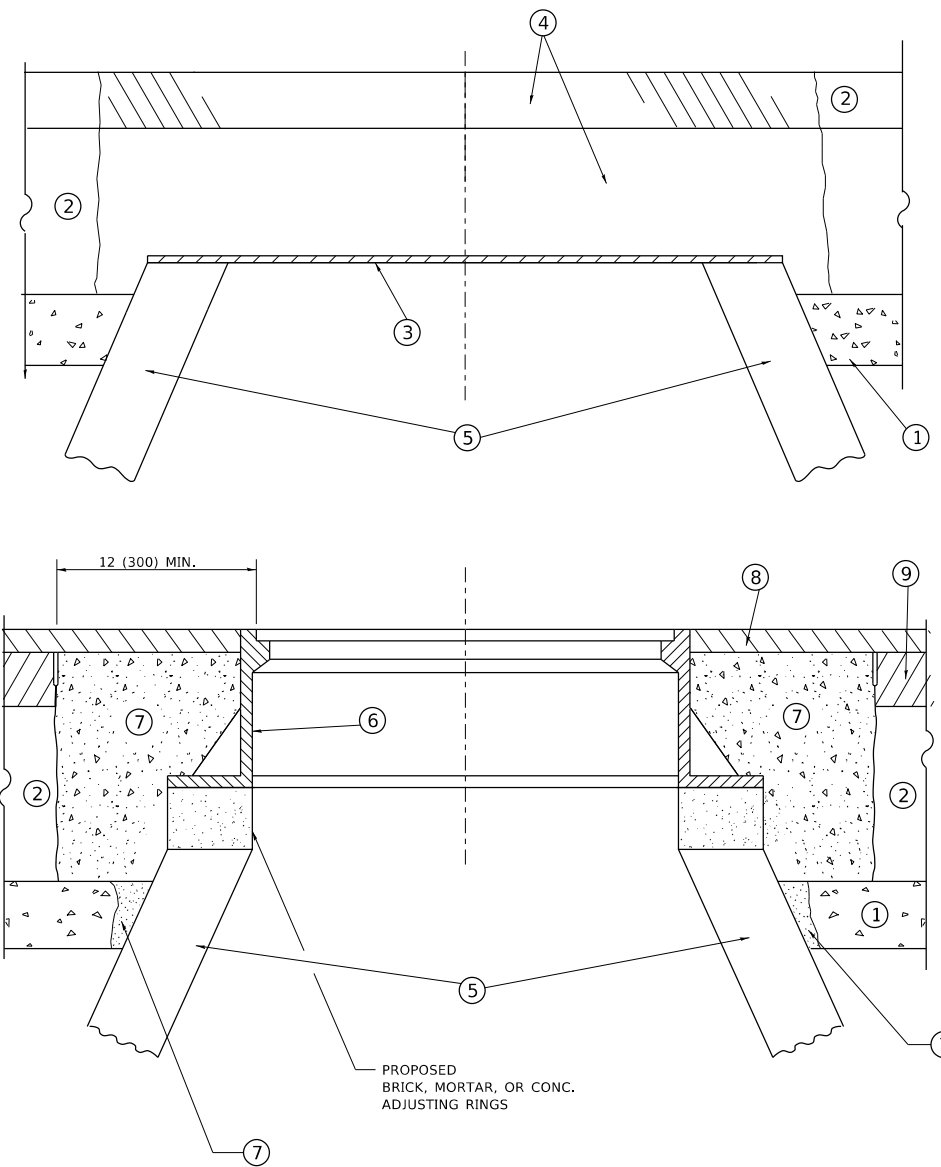
CURB RAMP PLACEMENT SHALL BE COORDINATED AS REQUIRED TO ALLOW FOR A 4' MINIMUM WIDTH SIDEWALK AROUND EACH CORNER OF INTERSECTION. SIDEWALK NOT TO BE OBSTRUCTED BY CURB RAMPS OR OTHER BARRIERS AND SHALL HAVE A CROSS SLOPE OF 1:50 MAXIMUM (2%).

THE BLENDED TRANSITION LAYOUT B-1-7 (AND SIMILAR) MAY BE USED WHERE TWO RAMPS ARE NOT POSSIBLE DUE TO GEOMETRIC CONSTRAINTS SUCH AS LIMITED SIDEWALK WIDTH OR GRADE ELEVATIONS. THE BLENDED TRANSITION SHALL NOT BE USED IF ACCESS TO AN EXISTING FACILITY WOULD BE REDUCED.

THE SHARED PERPENDICULAR RAMP AT CORNER LAYOUT B-1-10 IS NOT PREFERRED AND MAY ONLY BE USED WITH PERMISSION FROM THE ENGINEER.

WHERE APPLICABLE, RADIAL TILES MAY BE REQUIRED. IF USING RADIAL TILES, THE CONTRACTOR SHALL VERIFY THAT THE CURB RADIUS MATCHES THE AVAILABLE TILE RADII WITH THE TILE MANUFACTURER. CONTRACTOR MUST MAKE THIS DETERMINATION AND VERIFY IN THE FIELD.





**DETAILS FOR FRAMES AND LIDS ADJUSTMENT
WITH MILLING**

NOTES

- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
- CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
- THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 1 1/2 (40) HMA TO REMAIN AFTER MILLING).

STAGE 2 (AFTER PAVEMENT MILLING)

- REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS*PP-1 CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- | | |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL | ⑥ FRAME AND LID (SEE NOTES) |
| ② EXISTING PAVEMENT | ⑦ CLASS*PP-1 CONCRETE |
| ③ 36 (900) DIAMETER METAL PLATE | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE |
| ⑤ EXISTING STRUCTURE | |

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

- REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
- THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
- NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
- WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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PLOT DATE = 2/2/2022	DATE - 10-25-94	REVISED - K. SMITH 02-01-22

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

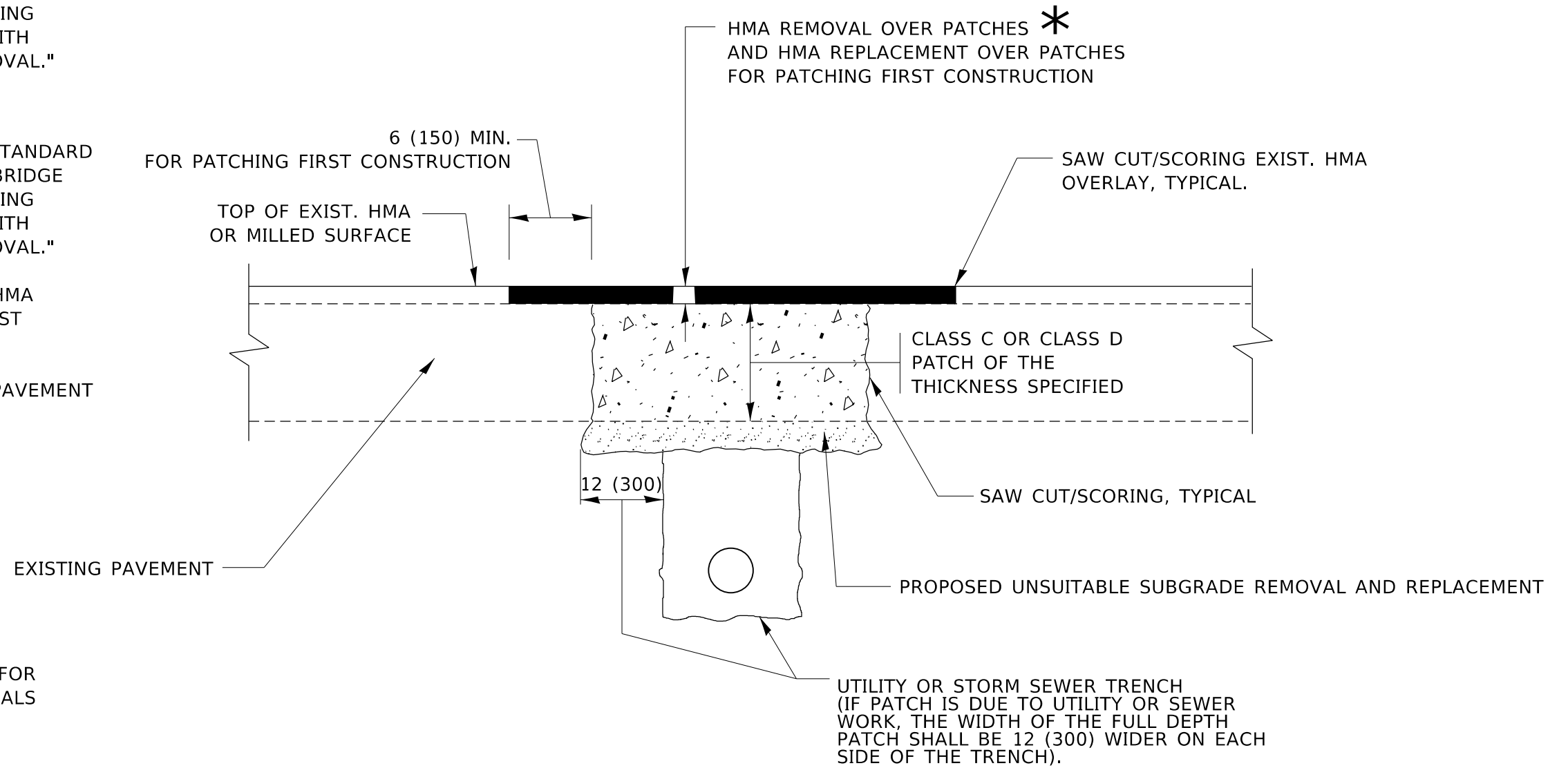
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7260	20-00085-00-RS	DuPage	37	30
BD600-03 (BD-08)			CONTRACT NO. 61J32	
ILLINOIS FED. AID PROJECT				

METHOD OF MEASUREMENT

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

BASIS OF PAYMENT

1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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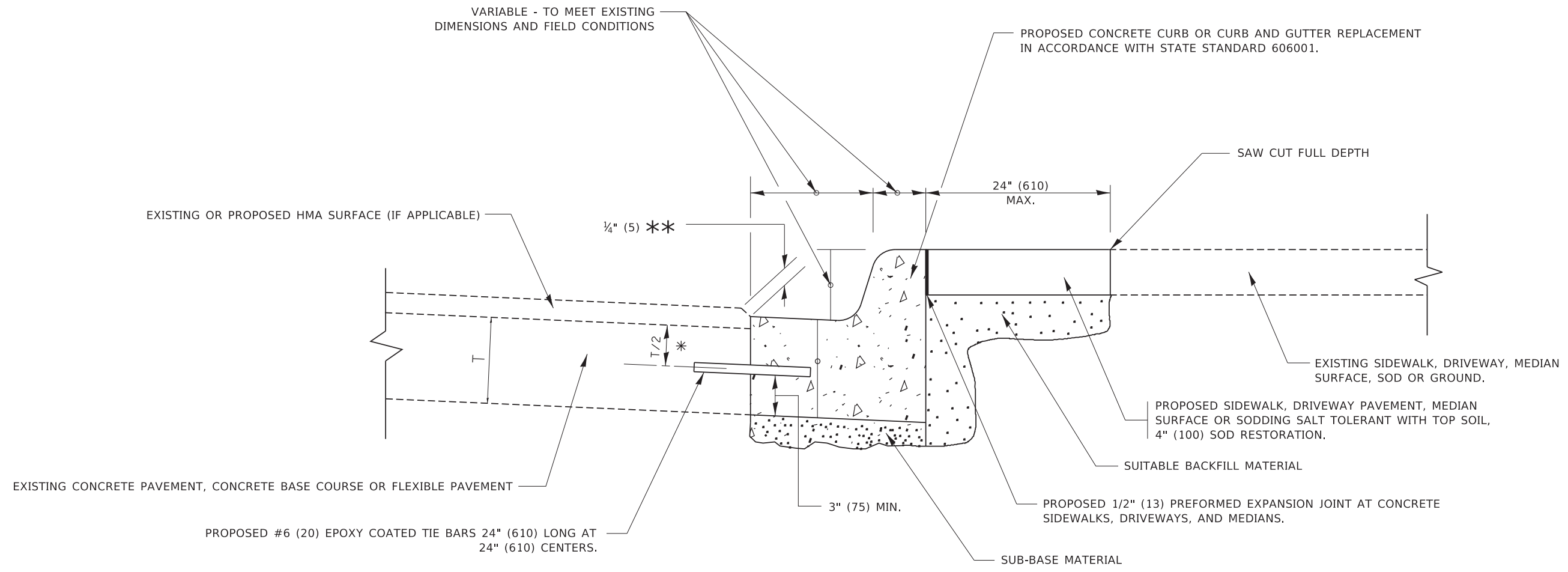
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PLOT DATE = 2/2/2022	DATE - 10-25-94	REVISED - K. SMITH 02-01-22

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	20-00085-00-RS	DuPage	37	31
BD400-04 (BD-22)		CONTRACT NO. 61J32		
ILLINOIS FED. AID PROJECT				



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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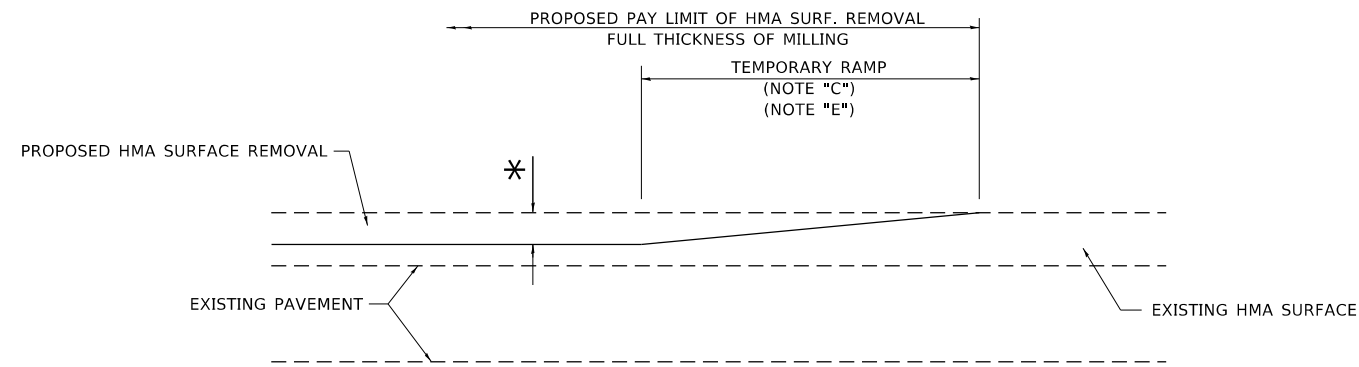
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PLOT DATE = 7/11/2019	DATE - 03-11-94	REVISED - K. SMITH 07-11-19

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CURB OR CURB AND GUTTER
REMOVAL AND REPLACEMENT**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

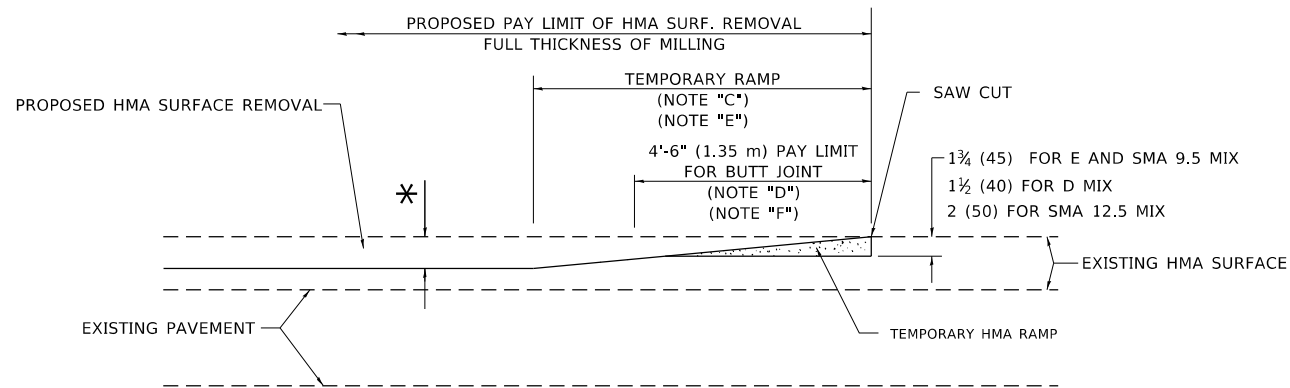
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ILLINOIS FED. AID PROJECT				



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

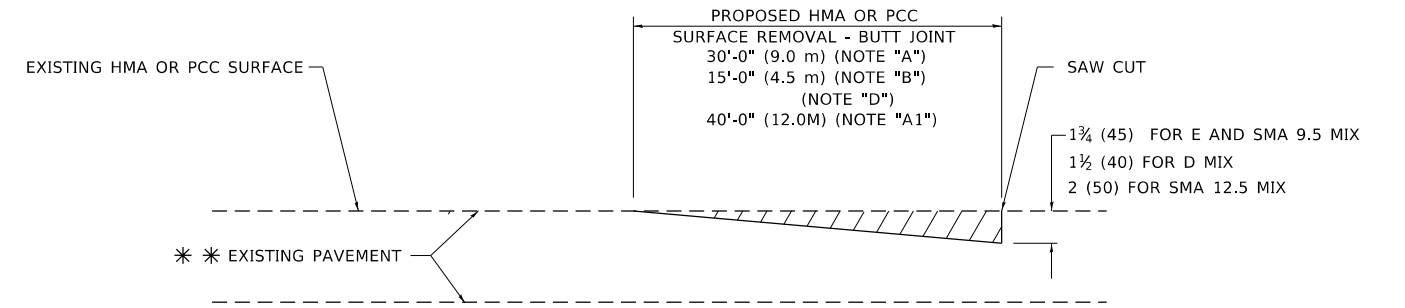


HMA CONSTRUCTED TEMPORARY RAMP

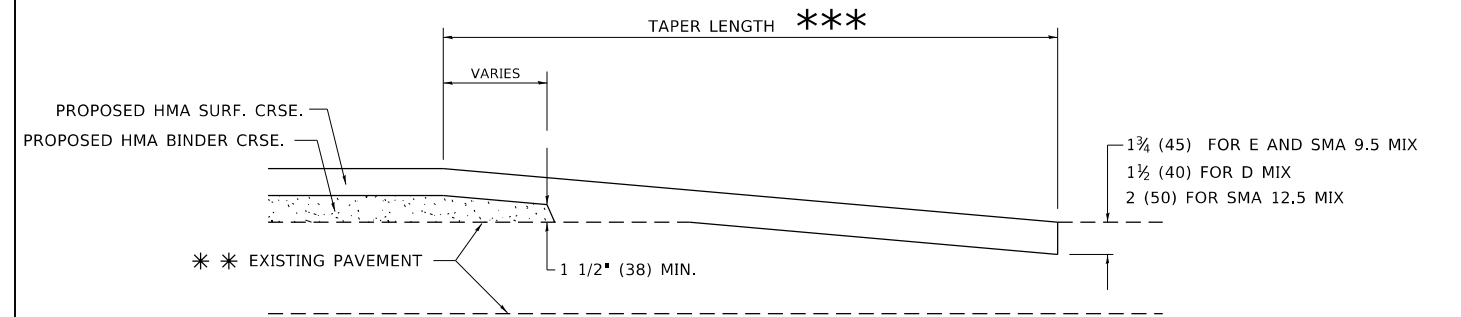
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

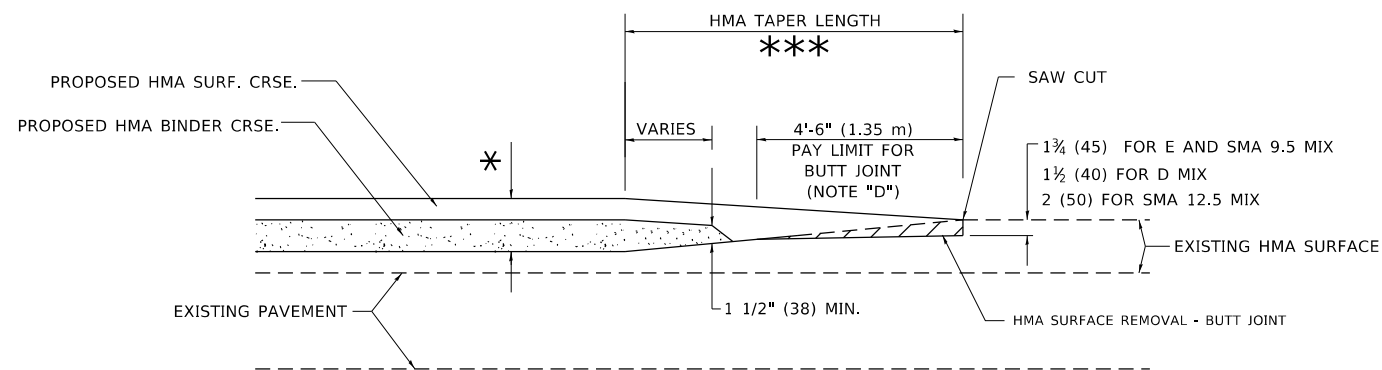
GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

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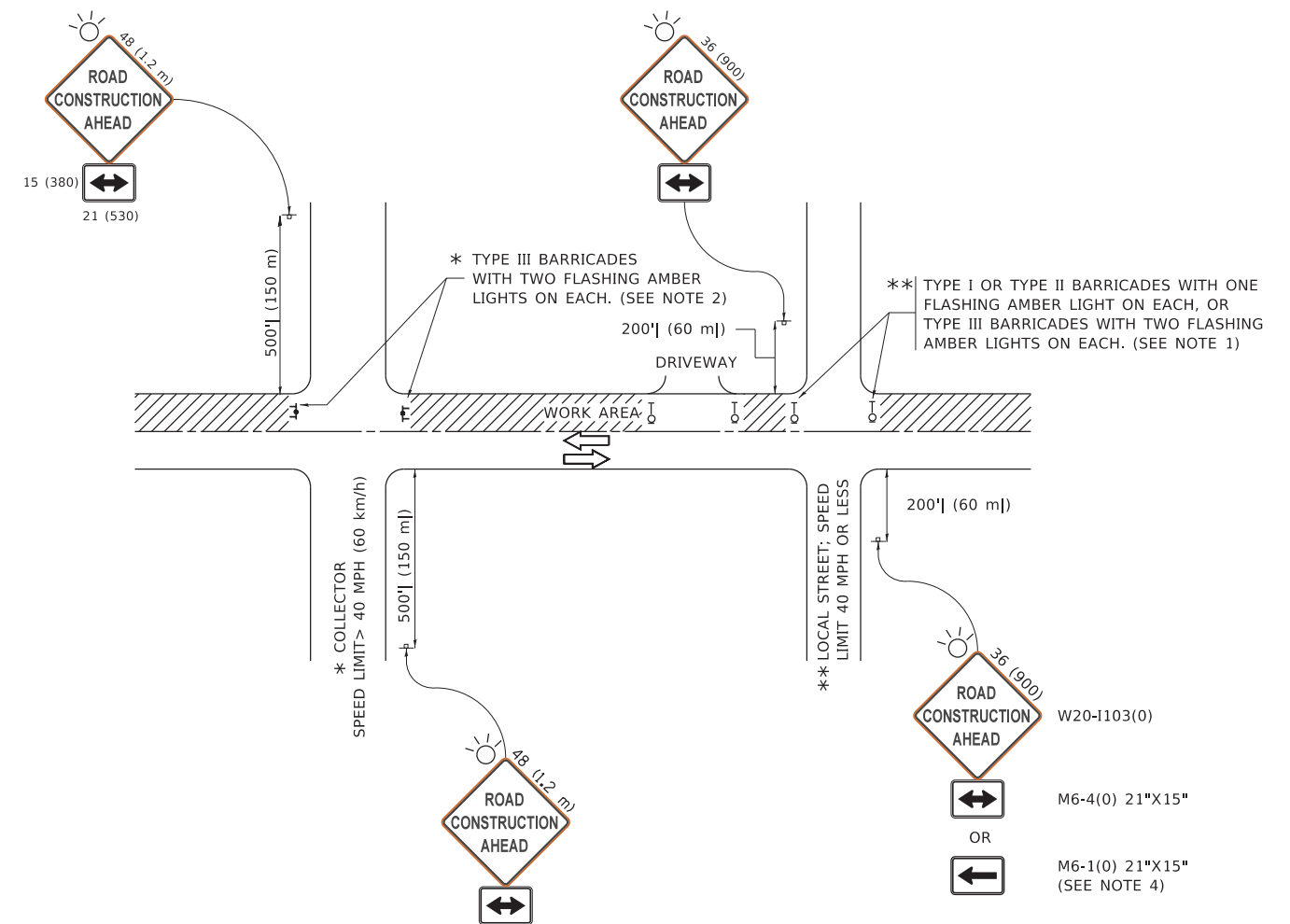
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PLOT DATE = 2/2/2022	DATE - 06-13-90	REVISED - R. BORO 01-01-07
		REVISED - K. SMITH 02-01-22

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	20-00085-00-RS	DuPage	37	33
BD400-05 BD-32		CONTRACT NO. 61J32		
ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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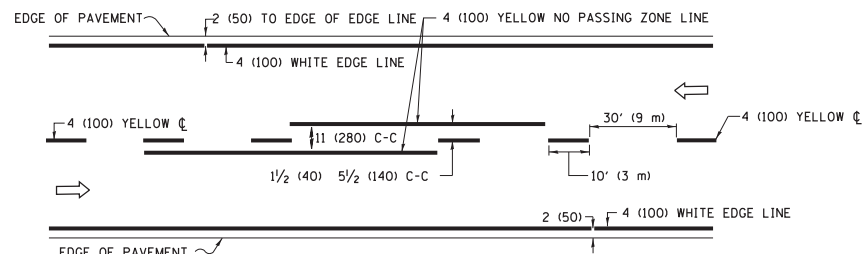
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

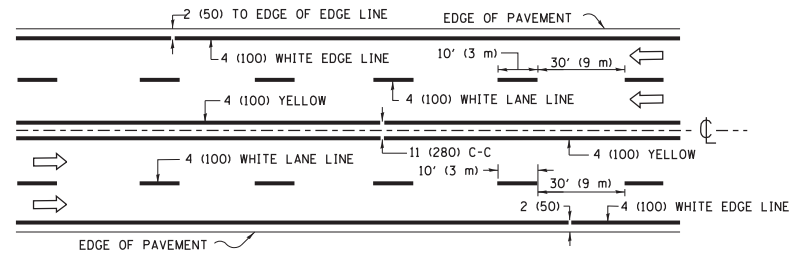
**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

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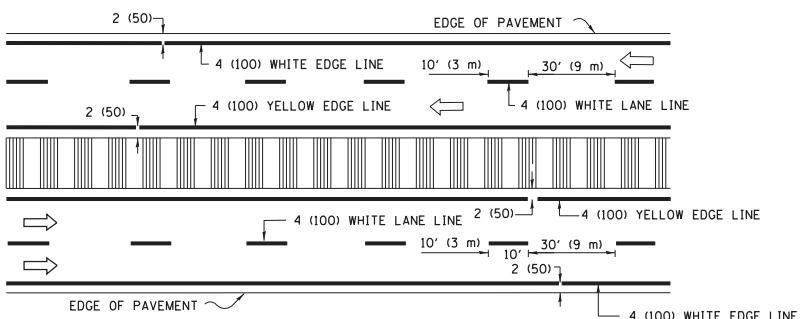
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	20-00085-00-RS	DuPage	37	34
TC-10			CONTRACT NO. 61J32	
ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

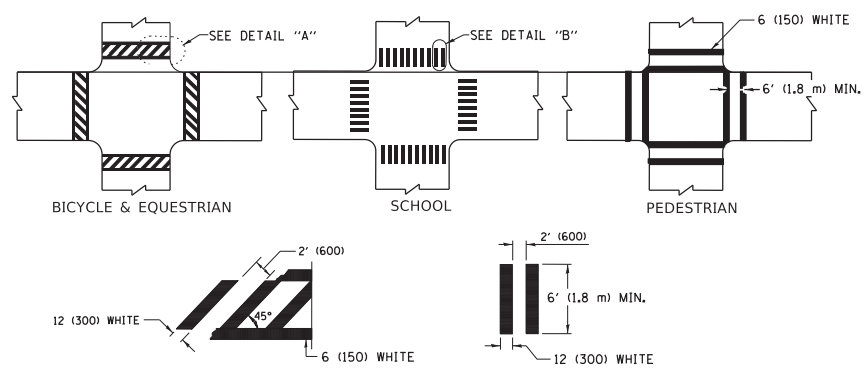


MULTI-LANE UNDIVIDED



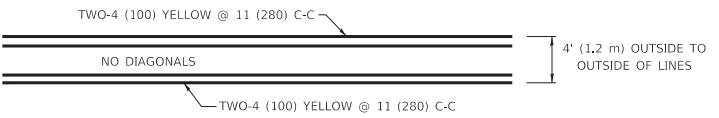
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

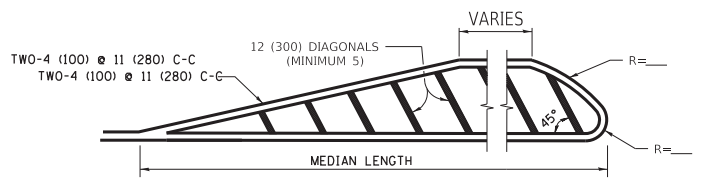


TYPICAL CROSSWALK MARKING

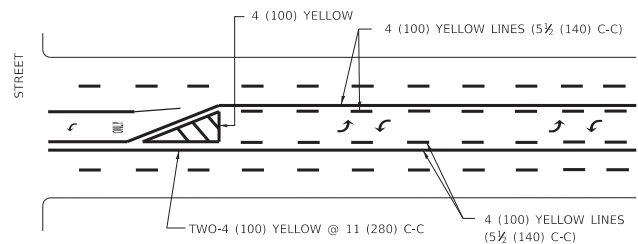
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



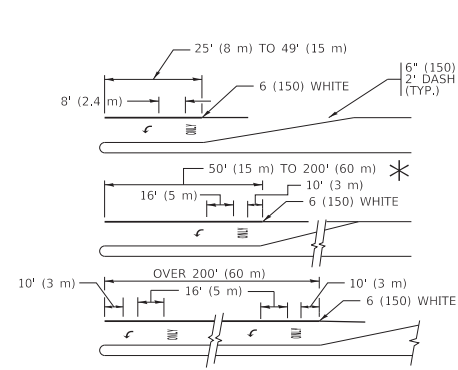
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE



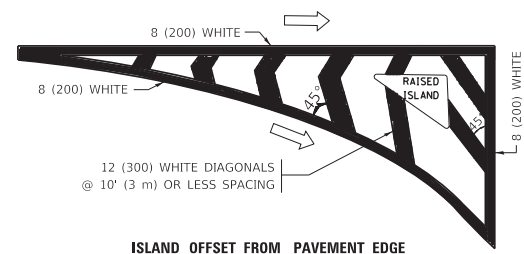
MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING



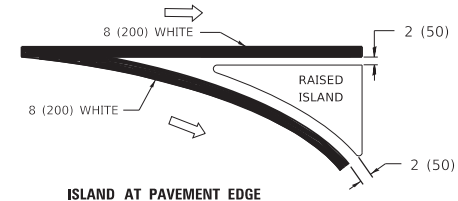
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

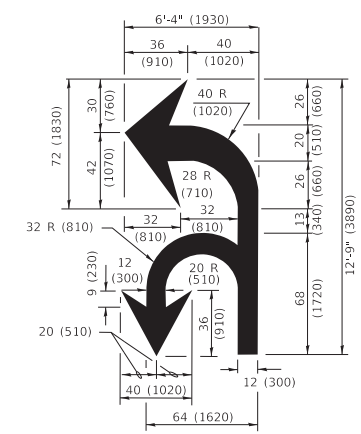
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".



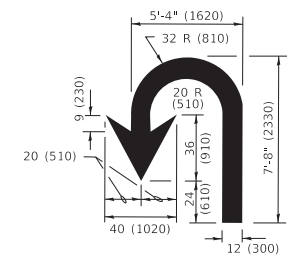
ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE: FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES "RR" IS 6' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ² EACH) "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

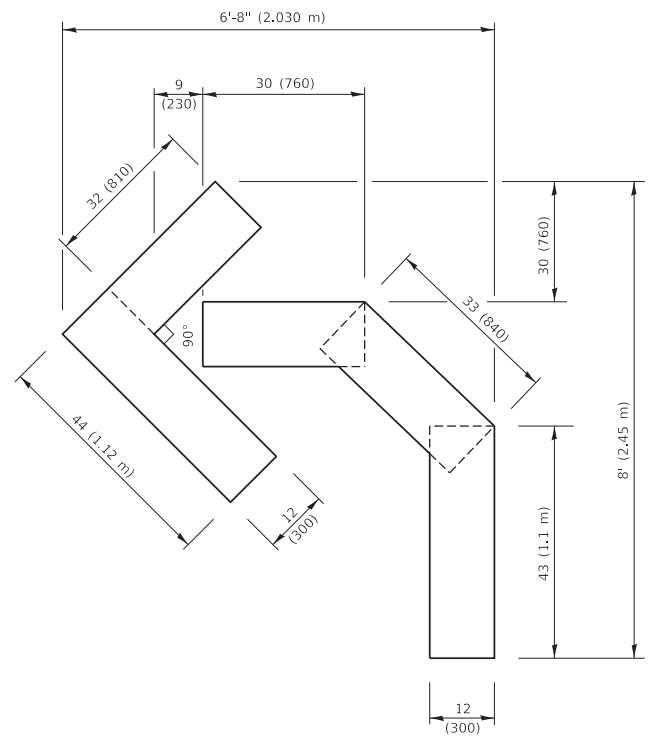
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PLOT DATE = 3/4/2019	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
		REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

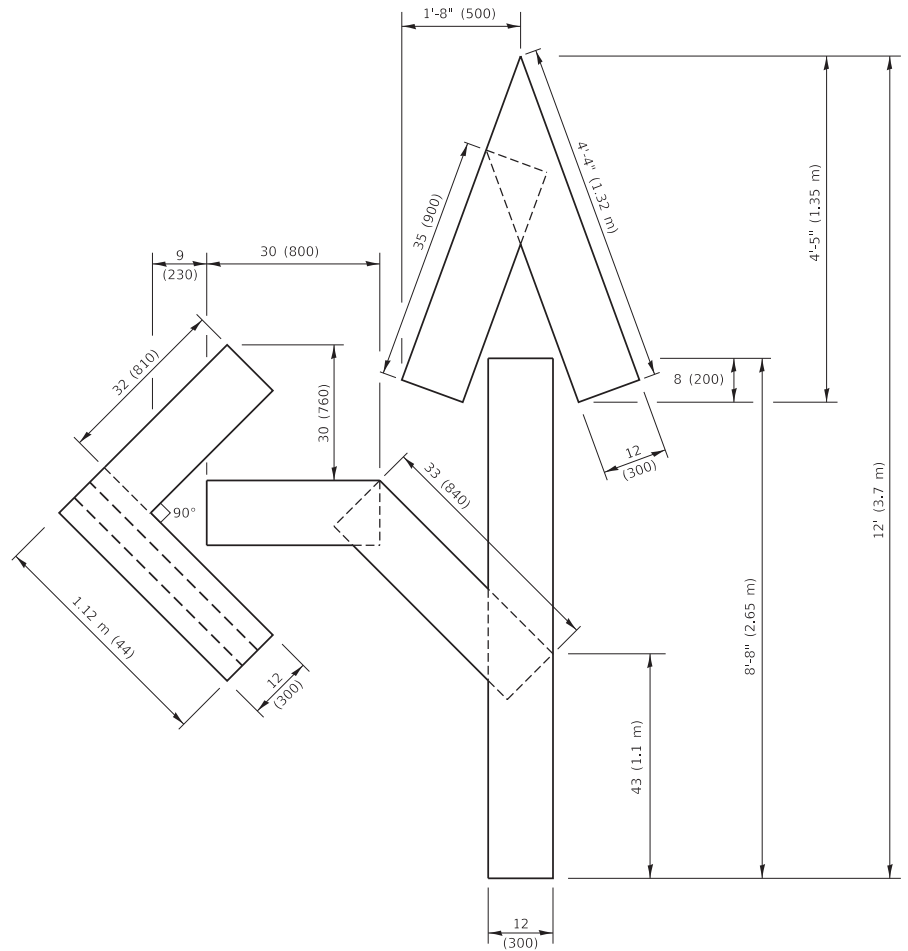
DISTRICT ONE	
TYPICAL PAVEMENT MARKINGS	
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	20-00085-00-RS	DuPage	37	35
TC-13		CONTRACT NO. 61J32		
ILLINOIS FED. AID PROJECT				



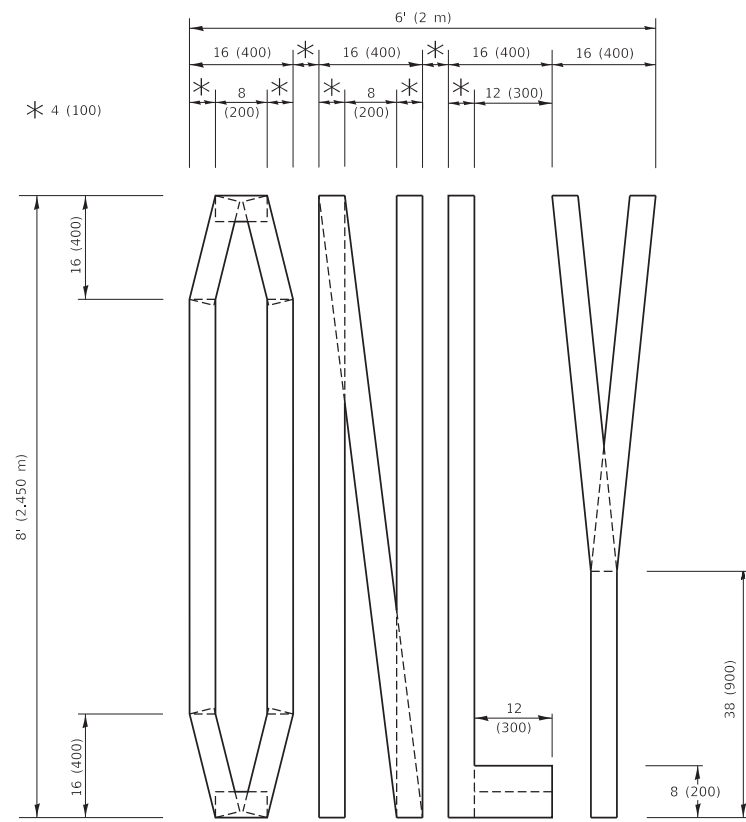
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



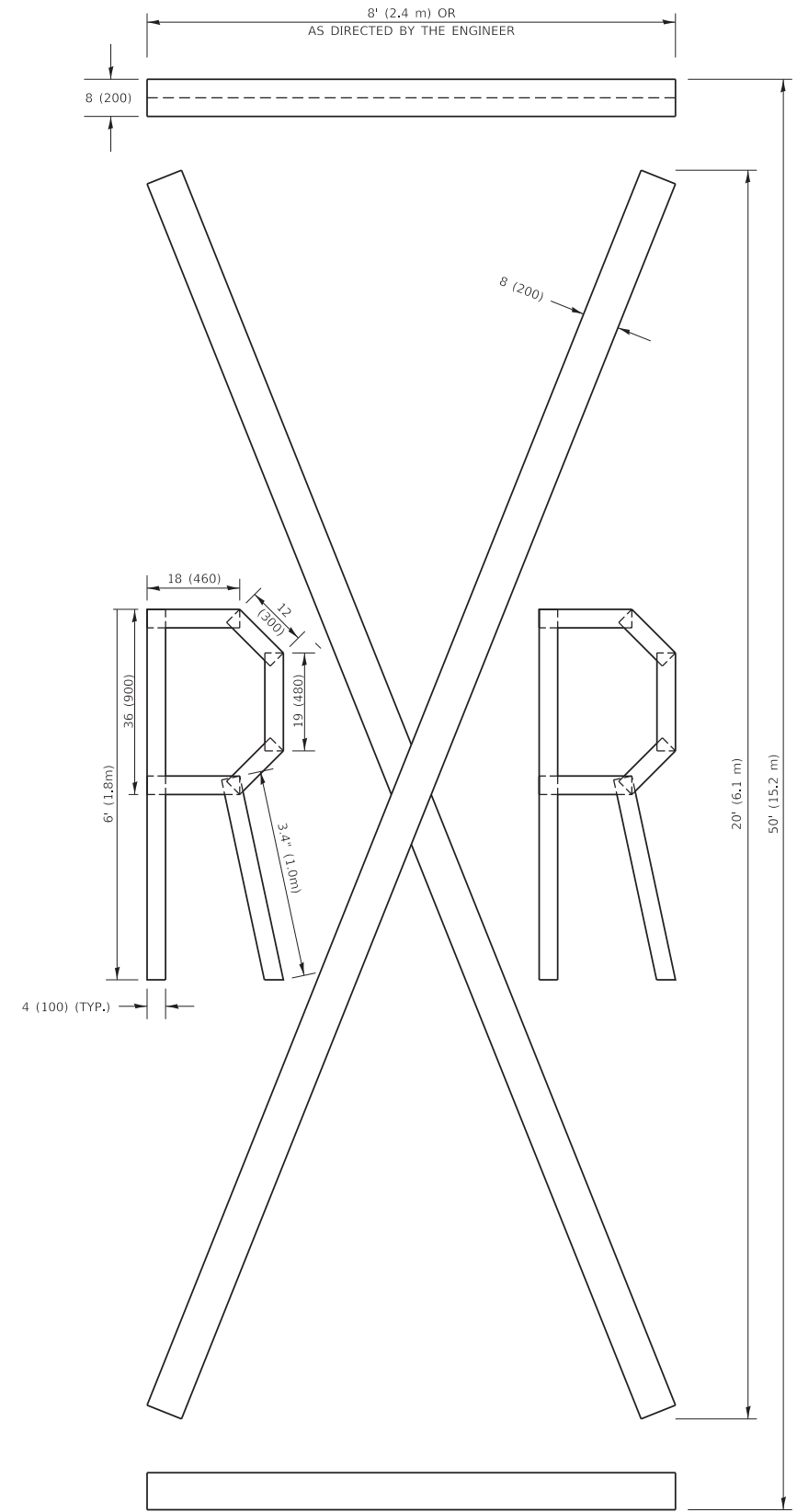
QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.

All dimensions are in inches (millimeters) unless otherwise shown.

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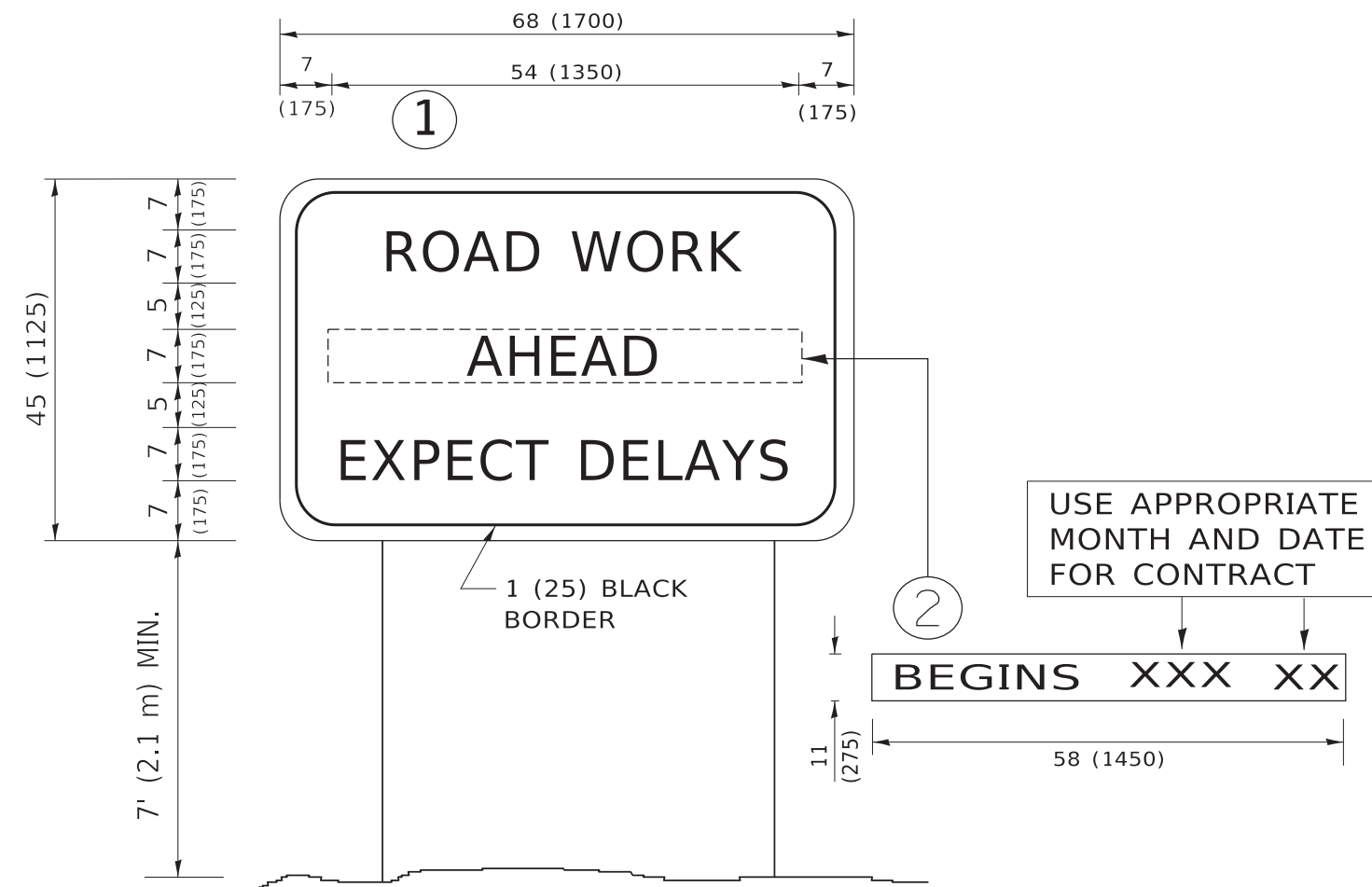
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DRAWN -	REVISIONS - E. GOMEZ 08-28-00	
PLOT SCALE = 50.0068 ' / in.	CHECKED -	REVISED - E. GOMEZ 08-28-00
PLOT DATE = 3/4/2019	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	20-00085-00-RS	DuPage	37	36
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NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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	DRAWN -	REVISED - R. MIRS 12-11-97
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ARTERIAL ROAD INFORMATION SIGN	
SCALE: NONE	SHEET 1 OF 1 SHEETS
STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7260	20-00085-00-RS	DuPage	37	37
TC-22			CONTRACT NO. 61J32	
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