

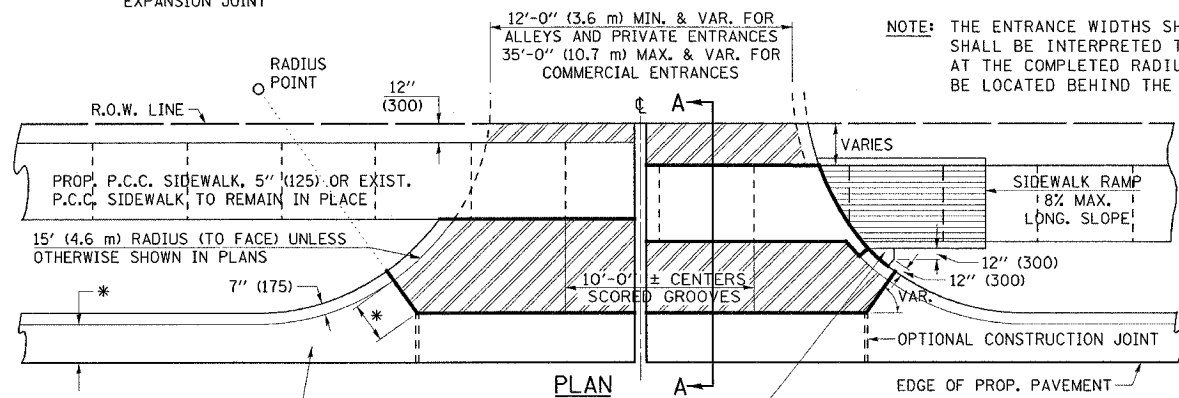


DETAIL OF P.C. CONCRETE DRIVEWAYS

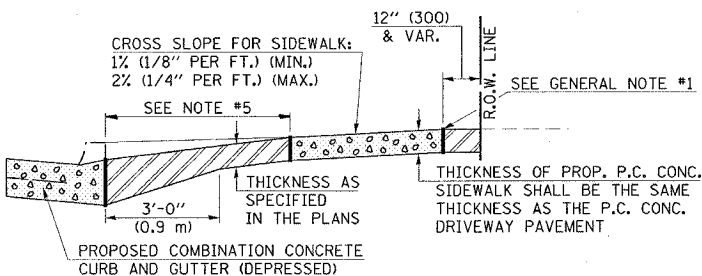
LEGEND

-  PROP. P.C. CONCRETE DRIVEWAY PAVEMENT
-  LONGITUDINAL CURB EXPANSION JOINT

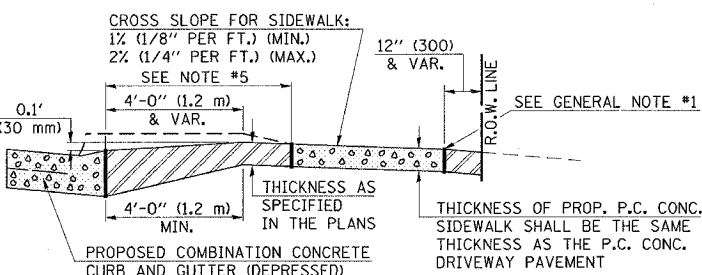


P.C. CONCRETE SIDEWALK, 5" (125) SHALL BE POURED BETWEEN THE CURB OF THE DRIVEWAY AND SIDEWALK RAMP, WHERE IN THE OPINION OF THE ENGINEER, CONSTRUCTION WILL CREATE A MOWING PROBLEM (I.E. POINTED AREAS ADJACENT TO HIGH CURBS).

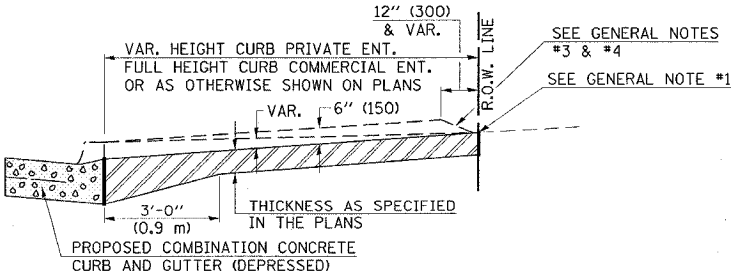
* WIDTH OF PROPOSED GUTTER FLAG



SECTION A-A WITH POSITIVE GRADE



SECTION A-A WITH NEGATIVE GRADE



SECTION A-A WITHOUT SIDEWALK

NOTE: SIDEWALK RAMPS SHALL BE CONSTRUCTED AT COMMERCIAL ENTRANCES. AT PRIVATE ENTRANCES RAMPS SHALL USUALLY BE NEEDED WHERE NARROW BOULEVARDS OR RADII GREATER THAN 15' (4.6 m) ARE CONSTRUCTED. THE BACK CURB RAMPS AT PRIVATE ENTRANCES MAY BE ELIMINATED IF GRADING CAN BE ACCOMPLISHED WITHOUT THE CURB.

NOTE: THE ENTRANCE WIDTHS SHOWN ON THE PLANS SHALL BE INTERPRETED TO BE THE WIDTHS AT THE COMPLETED RADIUS, WHICH MAY BE LOCATED BEHIND THE R.O.W. LINE.

GENERAL NOTES

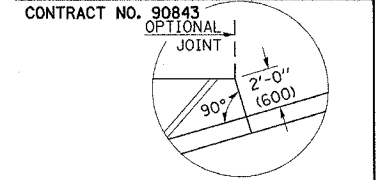
1. THIS LONGITUDINAL CURB EXPANSION JOINT SHALL BE PLACED ONLY WHERE THE PROPOSED P.C. CONCRETE SIDEWALK OR DRIVEWAY PAVEMENT MEETS AN EXISTING CONCRETE ENTRANCE. THE P.C.C. DRIVEWAY PAVEMENT WILL BE POURED MONOLITHIC WITH THE P.C.C. SIDEWALK WHEN THE DISTANCE BETWEEN THE SIDEWALK AND RIGHT-OF-WAY IS LESS THAN OR EQUAL TO 2' (0.6 m). THE LONGITUDINAL EXPANSION JOINT BETWEEN THE DRIVEWAY PAVEMENT AND SIDEWALK WILL NOT BE NEEDED UNLESS THE DISTANCE EXCEEDS 2' (0.6 m).
2. THE LONGITUDINAL CURB EXPANSION JOINT SHALL CONFORM TO SECTION 1051 OF THE STANDARD SPECS.
3. DRIVEWAYS THAT WILL MEET EXISTING OR PROPOSED CURB NEAR THE R.O.W. LINE SHALL HAVE THE CURB CONSTRUCTED FULL HEIGHT TO THE R.O.W. LINE.
4. DRIVEWAYS THAT WILL BE CONSTRUCTED WITH FULL HEIGHT CURBS AND NOT MEETING EXISTING OR PROPOSED CURBS NEAR THE R.O.W. LINE SHALL HAVE THE CURB SLOPED AS SHOWN ABOVE.
5. DEPRESSED CURB SHALL BE BUILT ONLY AT PRIVATE DRIVES WITH NO SIDEWALK RAMPS.
6. THE ENTRANCE GRADES WILL BE AS SHOWN ON THE STATION CROSS SECTIONS AND AS DIRECTED BY THE ENGINEER.
7. THE P.C. CONCRETE DRIVEWAY PAVEMENT SHALL BE CONSTRUCTED WITH SCORED GROOVES, AS SPECIFIED IN ARTICLE 424.06 OF THE STANDARD SPECIFICATIONS, AT APPROXIMATELY EVERY OTHER JOINT, EITHER IN THE EXISTING SIDEWALK OR THE PROPOSED SIDEWALK. THE PROPOSED P.C. CONCRETE SIDEWALK SHALL BE CONSTRUCTED PRIOR TO CONSTRUCTING THE P.C. CONC. DRIVEWAY PAVEMENT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
8. THE COMBINATION CONCRETE CURB AND GUTTER SHALL BE CONSTRUCTED AS SHOWN AND WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER LIN. FT. (METER) FOR COMBINATION CONCRETE CURB AND GUTTER OF THE TYPE SPECIFIED IN THE PLANS AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
9. THE P.C. CONCRETE SIDEWALK SHALL BE CONSTRUCTED AS SHOWN ABOVE AND WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQ. FT. (M²) FOR P.C. CONCRETE SIDEWALK OF THE THICKNESS SPECIFIED IN THE PLANS AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE EXTRA THICKNESS AS SHOWN ABOVE OR THE SEQUENCE OF CONSTRUCTION AS SPECIFIED.
10. BEFORE A CHANGE IN THE METHOD OF CONSTRUCTION IS ALLOWED, THE REQUEST SHALL BE SUBMITTED IN WRITING AND MUST BE APPROVED BY THE ENGINEER.
11. THE VARIABLE HEIGHT CURB ADJACENT TO THE P.C. CONCRETE DRIVEWAY PAVEMENT SHALL BE CONSTRUCTED MONOLITHIC WITH AND INCLUDED IN THE AREA OF THE P.C. CONCRETE DRIVEWAY PAVEMENT OF THE THICKNESS SPECIFIED IN THE PLANS. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQ. YD. (M²) FOR P.C. CONCRETE DRIVEWAY PAVEMENT OF THE THICKNESS SPECIFIED IN THE PLANS, INCLUDING THE MONOLITHIC CURBS AS SPECIFIED, THE SCORED GROOVES, THE LONGITUDINAL CURB EXPANSION JOINTS, AND THE ADDITIONAL THICKNESS REQUIRED ADJACENT TO THE DEPRESSED COMBINATION CONCRETE CURB AND GUTTER, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

		REVISIONS	
DESIGNED	NAME	DATE	REVISIONS
DESIGNED	J.M.H.	04/83	NAME
CHECKED	F.M.S.	04/83	D.L.P.
CADD NO.	C-1.14		K.A.G.

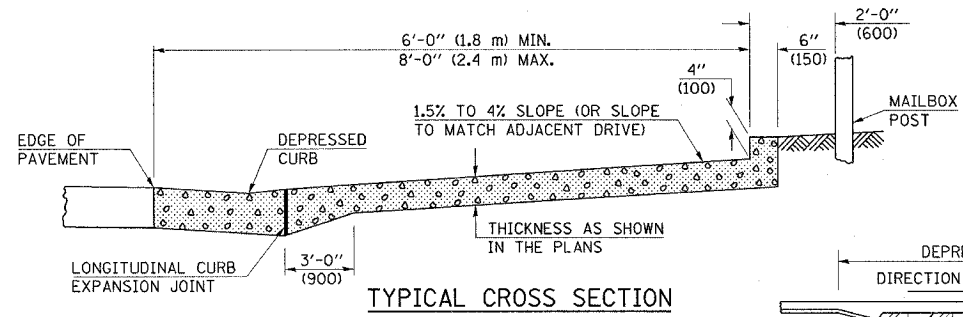
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

TYPICAL DETAIL OF MAILBOX TURNOUT IN CURB AND GUTTER SECTION

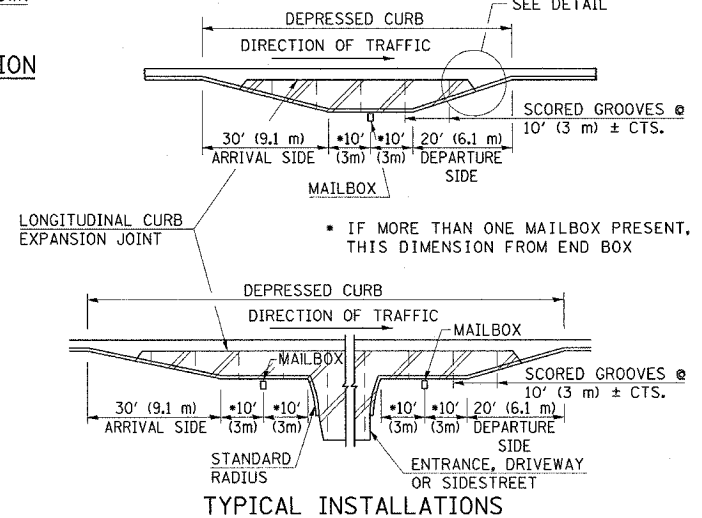
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
711	115(BY,BY-1)BR	VERMILION	93	68
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	



TYPICAL CORNER DETAIL



TYPICAL CROSS SECTION



TYPICAL INSTALLATIONS

GENERAL NOTES

1. THE LONGITUDINAL CURB EXPANSION JOINT SHALL CONFORM TO SECTION 1051 OF THE STANDARD SPECIFICATIONS.
2. THE MAILBOX TURNOUT CROSS SLOPE WILL BE AS SHOWN ABOVE, AS SHOWN ON THE STATION CROSS SECTIONS, OR AS DIRECTED BY THE ENGINEER.
3. THE MAILBOX TURNOUT SHALL BE CONSTRUCTED WITH SCORED GROOVES AT APPROXIMATELY 10' (3 m) CENTERS. IN THE EVENT THERE IS EXISTING OR PROPOSED SIDEWALK PRESENT, THESE SCORED GROOVES SHALL BE PLACED IN LINE WITH EVERY OTHER JOINT IN THE ADJACENT SIDEWALK.
4. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (M²) FOR P.C. CONCRETE DRIVEWAY PAVEMENT OF THE THICKNESS SPECIFIED IN THE PLANS WHICH PRICE SHALL INCLUDE THE LONGITUDINAL CURB EXPANSION JOINT, MONOLITHIC CURB AS SHOWN, SCORED GROOVES, AND THE ADDITIONAL THICKNESS REQUIRED ADJACENT TO THE DEPRESSED COMBINATION CONCRETE CURB AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

	NAME	DATE	REVISIONS	
DESIGNED	J.H.M.	6/88	NAME	DATE
CHECKED	F.M.S.	6/88	D.L.P.	10/96
CADD NO.	A-1.11		K.A.G.	06/03

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
ROADWAY DETAILS
DRAWN BY DIST. 5
CHECKED BY BWC
DATE 5/03