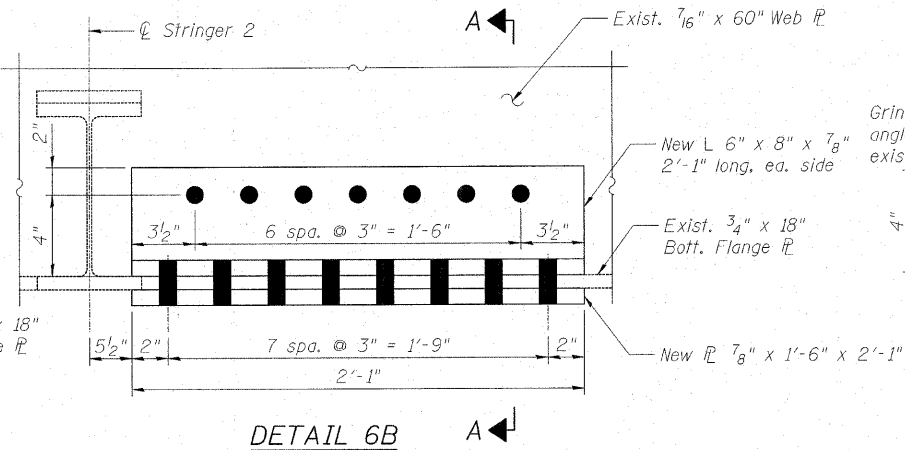
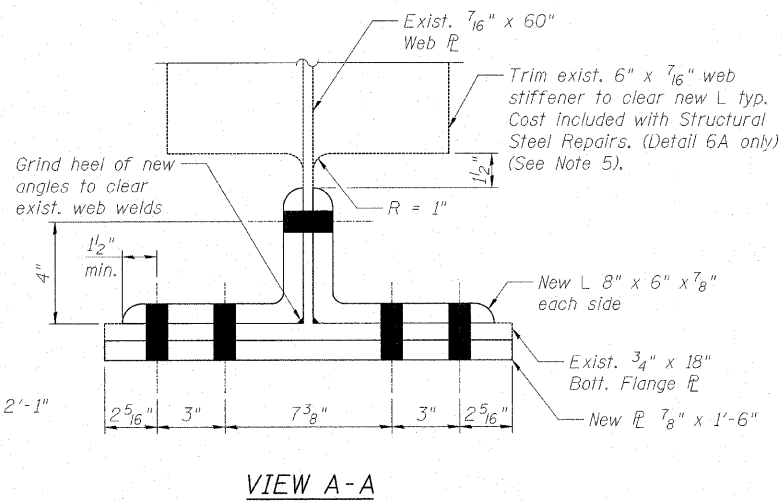


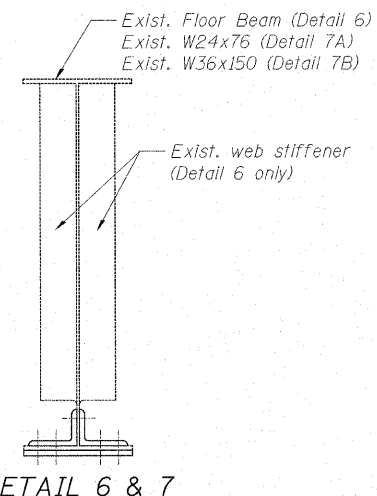
DETAIL 6A



DETAIL 6B



VIEW A-A



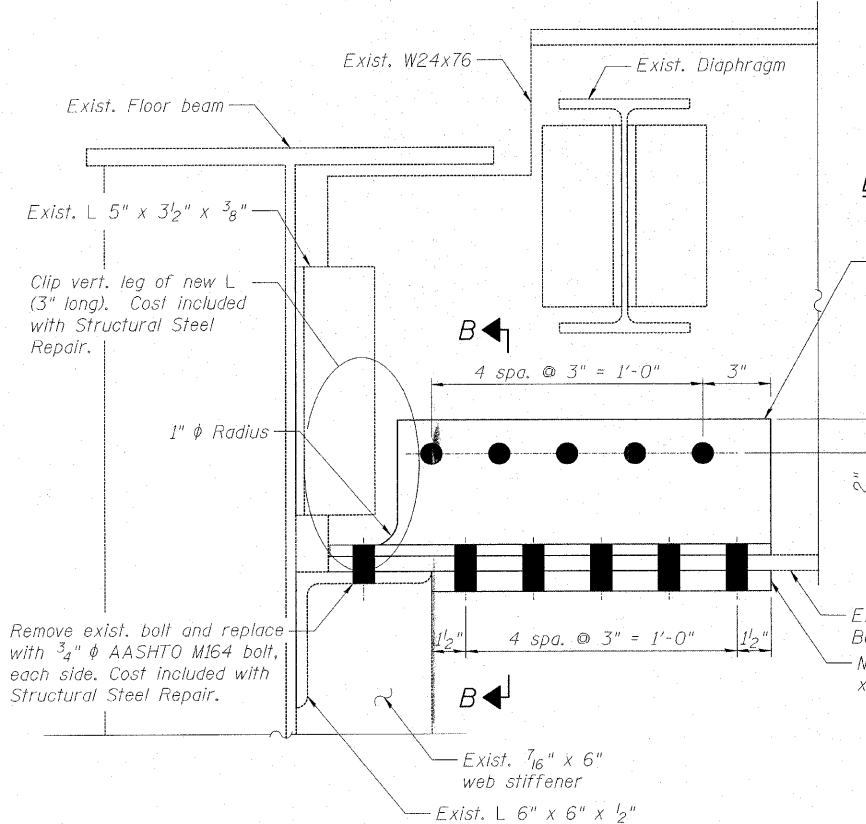
DETAIL 6 & 7

NOTES

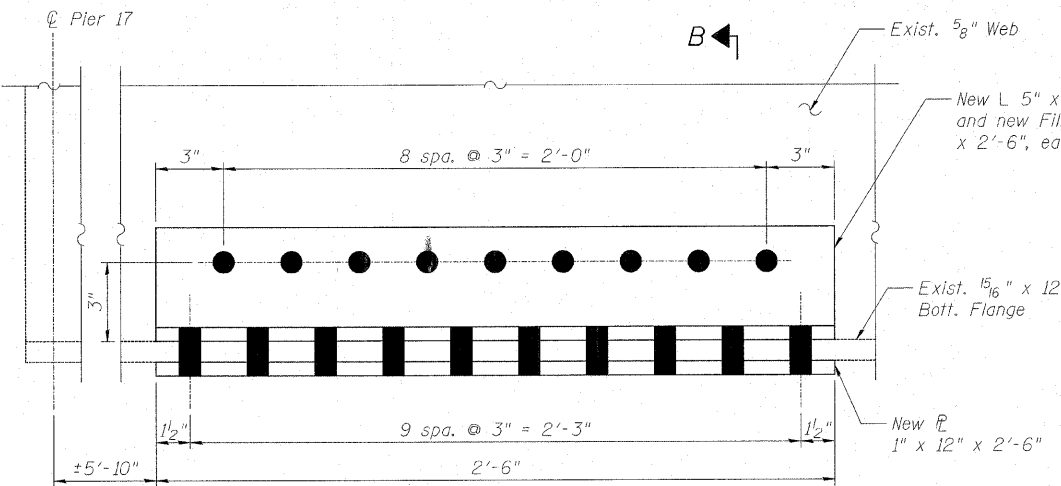
1. Cost of field drilling to be included in the cost of Structural Steel Repairs.
2. All holes to be field drilled $\frac{15}{16}$ " ϕ for $\frac{7}{8}$ " ϕ AASHTO M164/ASTM A325 bolts.
3. See Sheet S-3 and S-12 of S-35 for additional notes.
4. The repair plate and shape lengths shown are anticipated based on existing plan data. Longer repair plates and shapes may be required based on field conditions.
5. Procedure for cutting existing web stiffener:
 - A. Cut existing vertical stiffener and along web as shown, with a 1" R min. at web. The minimum distance from cut to face of web shall be the larger of $\frac{1}{4}$ " or web plate weld size, with removal of remaining material by grinding as described below. The cut shall be made parallel to the web without angling the cut towards the web. Equipment and method of cutting shall be approved by the Engineer. Any method of removal to be used shall ensure that no damage is done to the existing web, vertical stiffener or welds connecting these elements. Cutting shall be done in a manner such that the paint on the opposite face of the web is not damaged. If damage occurs, the damaged area shall be repainted at the contractor's expense and procedures shall be modified to prevent damage at subsequent removal locations.

NOTE 5 CONT'D:

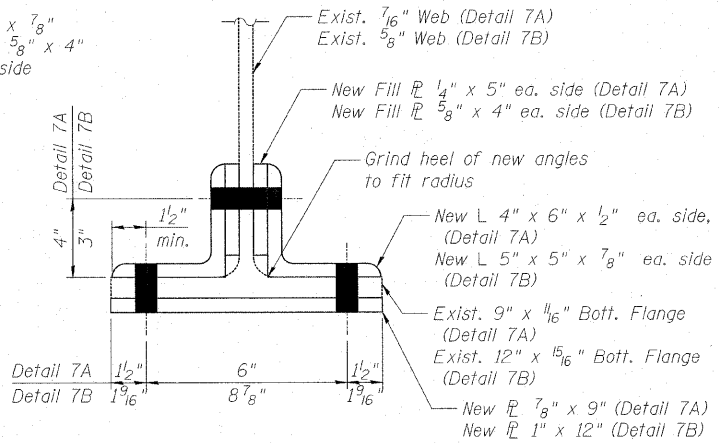
- B. Remove material between cut and web by grinding and grind smooth at web surface and cut end stiffener. Web ϕ surfaces and cut end of stiffener shall have a roughness average (Ra) of 250 μ in. or less. Grinding equipment shall be approved by the Engineer. The grinding operation should not gouge the girder web ϕ .
 - C. The web surface at the modification shall be inspected using dye penetrant or magnetic particle (MT) methods. Any cracks found shall be identified and reported to the Bureau of Bridges and Structures for further disposition.
 - D. The exposed steel surfaces shall be cleaned and painted using an aluminum epoxy mastic primer according to Article 506.05 of the Standard Specifications.
 - E. Cost shall be included with Structural Steel Repair.
6. For additional requirements, See Special Provision for Structural Steel Repair and Structural Steel Removal



DETAIL 7A

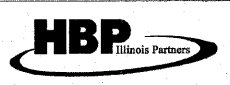


DETAIL 7B



VIEW B-B

USER NAME = Imuelter	DESIGNED - TL	REVISED -
PLOT SCALE = NTS	DRAWN - LAM	REVISED -
PLOT DATE = 2/9/2011 5:42:37 PM	CHECKED - BLU	REVISED -
	DATE - 1/20/2011	REVISED -



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STEEL REPAIR DETAILS 6
WESTBOUND I-80 OVER DES PLAINES RIVER
STRUCTURE NO. 099-0057
SHEET NO. S-18 OF S-35 SHEETS

F.A.I. RTE. 80	SECTION 99(4&4)-IHS-3	COUNTY WILL	TOTAL SHEETS 203	SHEET NO. 127
CONTRACT NO. 60M66				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				