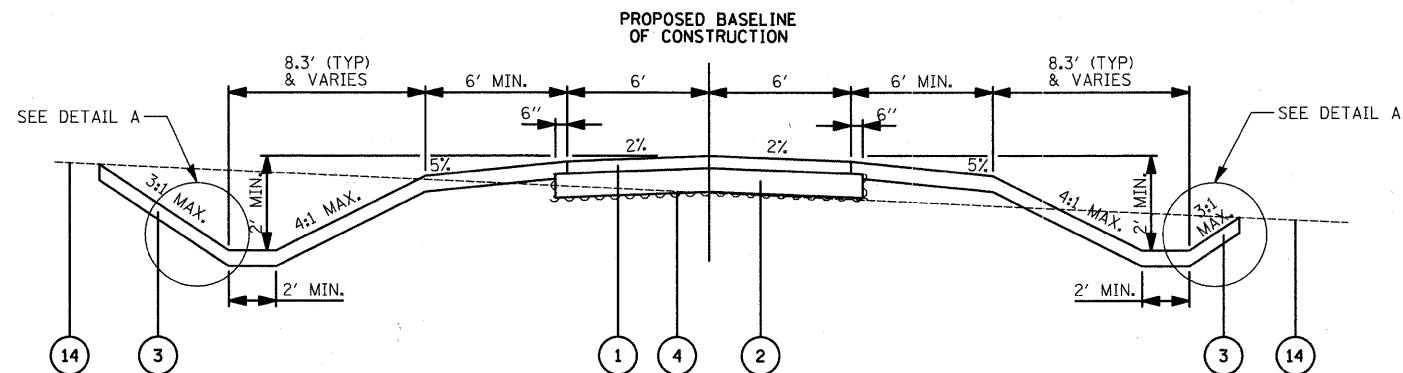
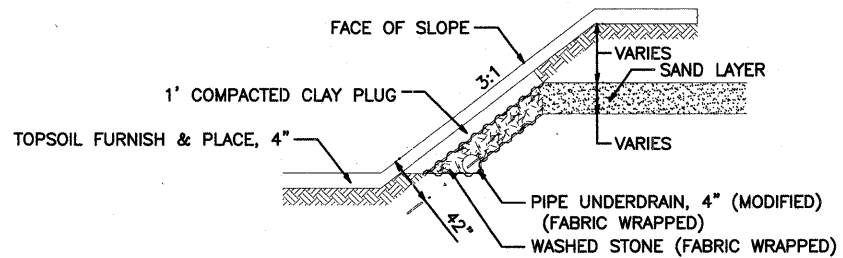


DATE	
BY	
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PLOTTED	
TEMPLATE	
AREAS CHECKED	
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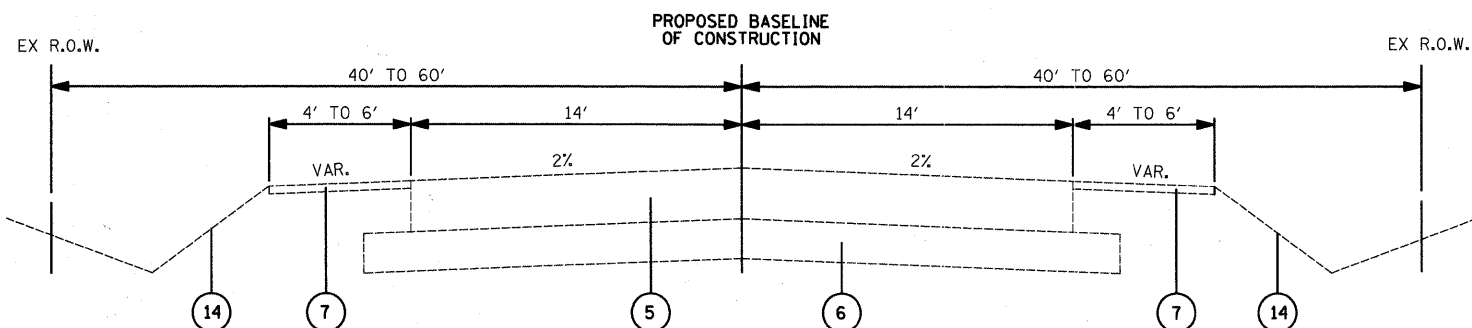


PROPOSED TYPICAL SECTION
STA 114+68.8 TO STA 125+75.0 - BIKE PATH

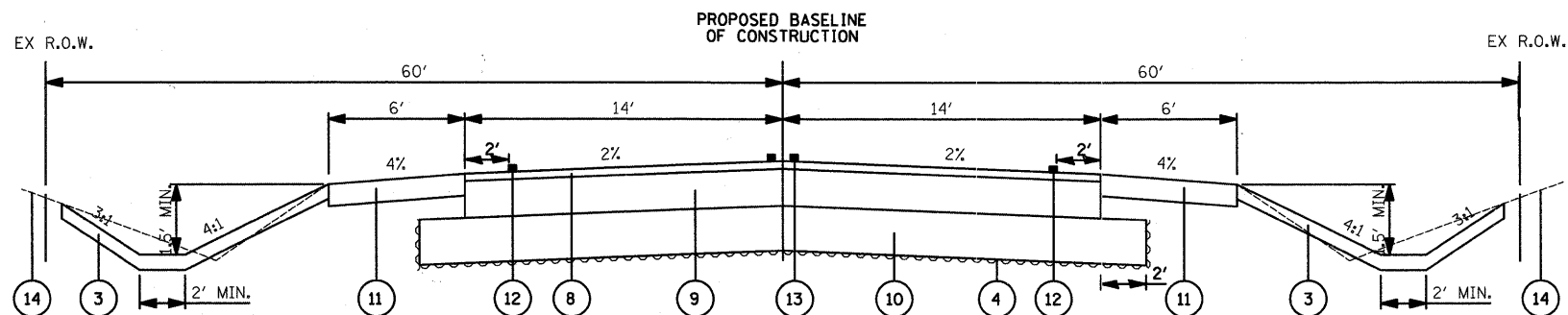


**PIPE UNDERDRAIN, 4" (MODIFIED) -
DETAIL A**

SEE BIKE PATH PLAN AND PROFILE FOR LOCATIONS



EXISTING TYPICAL SECTION
STA 265+42.4 TO STA 284+40.1 - GILMER ROAD



PROPOSED TYPICAL SECTION
STA 273+35.0 TO STA 274+98.0 - GILMER ROAD

LEGEND

- 1 PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 3"
- 2 PROPOSED AGGREGATE BASE COURSE, TYPE B, 6"
- 3 PROPOSED TOPSOIL FURNISH AND PLACE, 4"
- 4 PROPOSED GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- 5 EXISTING HOT-MIX ASPHALT COURSE, THICKNESS VARIES FROM 13" TO 13-1/4" (REFERENCE SOIL EXPLORATION AND ANALYSIS REPORT PREPARED BY MIDLAND STANDARD ENGINEERING & TESTING, INC. DATED APRIL 1, 2009)
- 6 EXISTING GRANULAR BASE COURSE, THICKNESS VARIES FROM 5-1/4" TO 10" (REFERENCE SOIL EXPLORATION AND ANALYSIS REPORT PREPARED BY MIDLAND STANDARD ENGINEERING & TESTING, INC. DATED APRIL 1, 2009)
- 7 EXISTING AGGREGATE SHOULDER, 2" (REFERENCE SOIL EXPLORATION AND ANALYSIS REPORT PREPARED BY MIDLAND STANDARD ENGINEERING & TESTING, INC. DATED APRIL 1, 2009)
- 8 PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"
- 9 PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 10"
- 10 PROPOSED AGGREGATE SUBGRADE, 12"
- 11 PROPOSED AGGREGATE SHOULDERS, TYPE B, 6"
- 12 WHITE EDGE LINE THERMOPLASTIC PAVEMENT MARKING LINE 5"
- 13 DOUBLE YELLOW CENTERLINE 11" (C-C) THERMOPLASTIC PAVEMENT MARKING LINE 4"
- 14 EXISTING GROUND LINE

NOTE:

THE CONTRACTOR SHOULD BE PREPARED TO CONTROL THE GROUNDWATER DURING THE CONSTRUCTION PERIOD. ACCORDING TO THE SOIL EXPLORATION AND ANALYSIS REPORT PREPARED BY MIDLAND STANDARD ENGINEERING & TESTING, INC. DATED APRIL 1, 2009 BECAUSE OF THE GROUNDWATER CONDITIONS AND THE DEPTH TO PROPOSED TUNNEL SUBGRADE, EXCAVATION CONDITIONS ARE EXPECTED TO BE RELATIVELY DIFFICULT. SOIL STRATA WITH A RELATIVELY LARGE AMOUNT OF SAND AND GRAVEL WAS ENCOUNTERED DURING BORING THAT WILL ALLOW GROUNDWATER TO TRAVEL RELATIVELY QUICKLY WHEN EXPOSED IN EXCAVATIONS. THIS, IN TURN, WILL CONTRIBUTE TO SLOUGHING OF EXCAVATION SIDEWALLS AND UNSTABLE SUBGRADE CONDITIONS.

A WORKING PLATFORM MAY BE NECESSARY TO MAINTAIN THE INTEGRITY OF THE EXPOSED BEARING SOIL FROM SOFTENING AND DISTURBANCE. THIS SHALL BE ACCOMPLISHED BY PLACING A MUD MAT 8"-INCH THICK LAYER OF OPEN GRADED AGGREGATE. THIS PROTECTIVE SUBGRADE SOIL LAYER SHALL BE PLACED SOON AFTER EXCAVATION AND APPROVAL OF BEARING SOIL.

POROUS GRANULAR EMBANKMENT, SUBGRADE (PGES) HAS BEEN PROVIDED FOR SOILS THAT ARE UNSTABLE WHEN WET. THE NEED FOR REMOVAL AND REPLACEMENT WITH PGES WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE ENGINEER (BY USE OF A CONE PENETROMETER IN CONJUNCTION WITH THE IDOT SUBGRADE MANUAL). IF UNSTABLE SOILS ARE ENCOUNTERED, THE SOILS SHALL BE REMOVED AND REPLACED WITH PGES. IF UNSTABLE SOILS ARE NOT ENCOUNTERED, THEN THE QUANTITY WILL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE THE CONTRACTOR.

NO SOIL SHALL BE USED TO BACKFILL THE FOOTINGS, WHETHER THE SOIL IS UNSTABLE OR NOT, PGES SHALL BE USED.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS
FULL-DEPTH PAVEMENT	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2" (IL-9.5 mm)	4% @ 50 Gyr.
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 10"	4% @ 50 Gyr.
BIKE PATH	
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 3" (IL-9.5 mm)	4% @ 50 Gyr.
TEMPORARY PAVEMENT	
TEMP PAVEMENT (HMA BINDER IL-19.0, N50, 8")	4% @ 50 Gyr.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG-70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

USER NAME = pk	DESIGNED - PK	REVISED -
FILE NAME = ...02 TYP & SCHED\2130_TYP.dgn	DRAWN - JRR	REVISED -
PLOT SCALE = 1/8" = 1'	CHECKED - RTM	REVISED -
PLOT DATE = 12/13/2010	DATE - 12/17/2010	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

N.T.S.

TYPICAL SECTIONS
FORT HILL TRAIL BIKE PATH UNDERPASS

SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3602	08-F3000-07-BT	LAKE	60	4
CONTRACT NO. 63422				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				