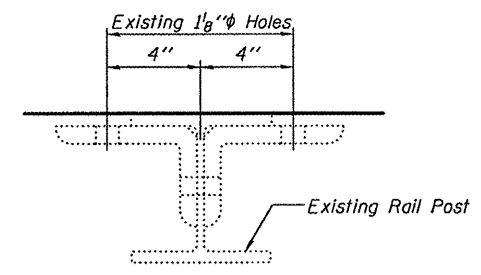


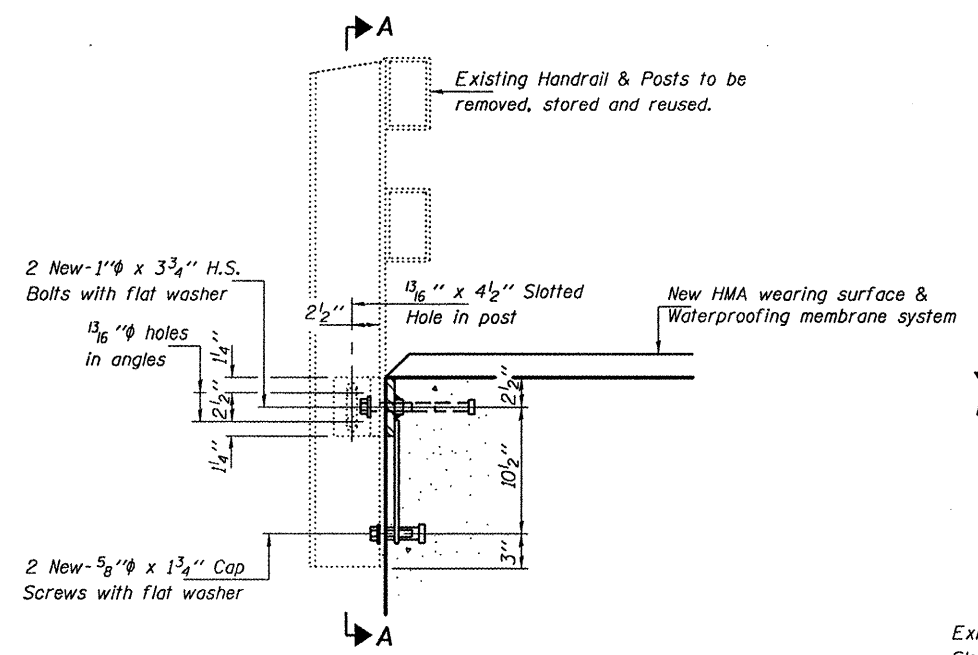
ANCHOR DEVICE

* Threaded areas shall be plugged or blocked off during casting of beam.

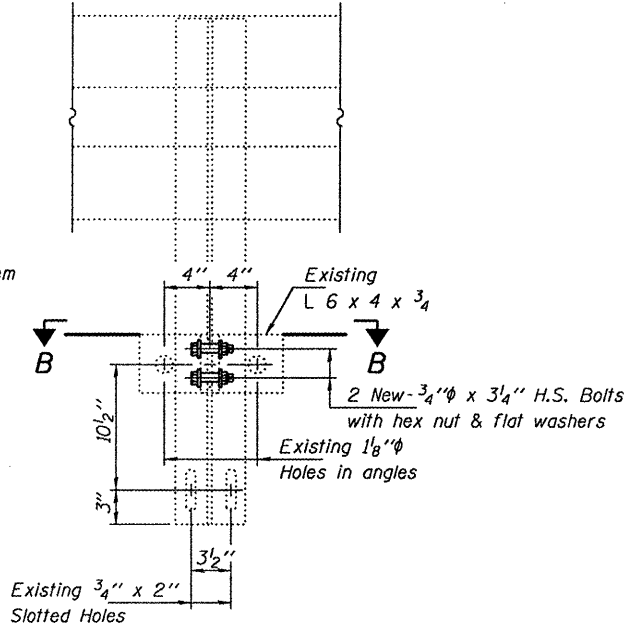
** Whenever the lower insert assemblies interfere with g₁ bars the #3 bars shall be cut and adjusted in order to allow raising or lowering of the lower inserts. Maximum adjustment not to exceed 1/2".



SECTION B-B



SECTION AT RAIL POST



SECTION A-A

NOTES

Bolts, cap screws, and nuts shall conform to the requirements of ASTM designation A 307 except for high strength bolts nuts and washers noted which shall conform to AASHTO M 164.

All bolts, nuts, cap screws, washers and lock washers shall be galvanized according to AASHTO M 232.

Anchor devices shall be galvanized after shop fabrication according to AASHTO M 111 and ASTM A 385. Cost of anchorage devices is included with the cost of Precast Concrete Bridge Slab.

For multi-span bridges, sufficient 1/4" galvanized steel shims of the dimensions shown shall be provided to align rail between adjacent spans.

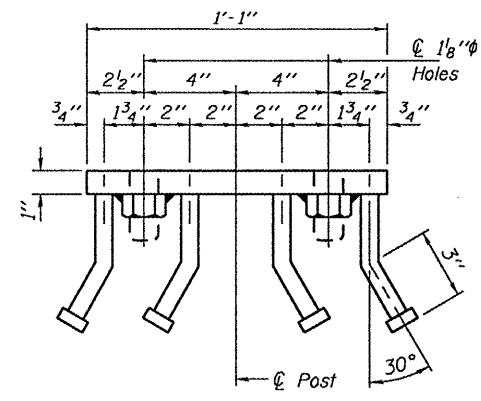
The lower portion of the post flange in contact with concrete shall receive two coats of asphalt paint conforming to Section 1060.07 Type II or place new 1/8" fabric bearing pad between the post and concrete. Fabric bearing pads shall meet the requirements of Article 1082.01 of the Standard Specifications.

The 1" high strength bolts connecting the angles to the concrete shall be tightened to a snug fit and given an additional 1/8 turn. The 5/8" cap screws in bottom of posts shall be tightened to snug fit only.

Removal and re-erection of the existing railing shall be accomplished in a manner that will avoid scratching, denting or other damage that may affect the durability or appearance of the railing.

The length paid for will be overall length along the rail from end to end, in place, at the location of re-erection.

This work will be paid for at the contract unit price per foot for Removing and Re-erection Existing Railing, which price shall include removal, temporary storage, re-erection, asphalt paint or new bearing pads, shims and all new hardware required to satisfactorily complete the work.



VIEW C-C

| | | | | | | | | | | |
|-----------------|---|-------------------------|---|-------------------------------------|---------------------------|---------|----------|--------------|-----------|--|
| DESIGNED MKC | EXAMINED <i>James F. [Signature]</i> | DATE - JANUARY 28, 2011 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | RAIL DETAILS SN 079-0401 | ILL. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| CHECKED IJL | ACTING ENGINEER OF STRUCTURAL SERVICES | | | | 4 | 218-HOF | RANDOLPH | 7 | 5 | |
| DRAWN baliva | PASSED <i>[Signature]</i> | | | | CONTRACT NO. 76E61 | | | | | |
| CHECKED MKC IJL | ACTING ENGINEER OF BRIDGES AND STRUCTURES | | | | ILLINOIS FED. AID PROJECT | | | | | |
| | | | | | SHEET NO. 3 OF 3 SHEETS | | | | | |