

CONSTRUCTION PLANS FOR GALESBURG MUNICIPAL AIRPORT

GALESBURG, KNOX COUNTY, ILLINOIS

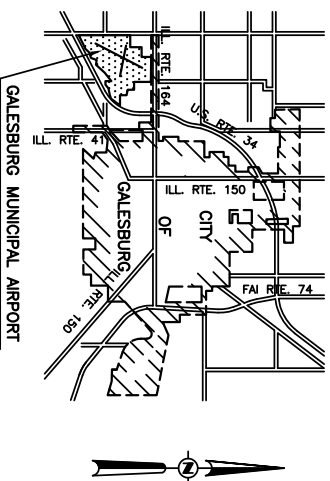
APPLY POROUS FRICTION COURSE (PFC)
TO A PORTION OF RUNWAY 3-21

SUMMARY OF QUANTITIES				
ITEM NO.	DESCRIPTION	UNIT	AS BID QUANTITY	AS BUILT QUANTITY
AR150530	TRAFFIC MAINTENANCE	L. SUM	1	
AR401110	PAVEMENT PREPARATION	L. SUM	1	
AR401655	BUTT JOINT CONSTRUCTION	SQ. YD.	1,629	
AR402620	POROUS FRICTION COURSE 5/8"	SQ. YD.	6,712	
AR603510	BITUMINOUS TACK COAT	GAL.	2,014	
AR620520	PAVEMENT MARKING-WATERBORNE	SQ. FT.	8,930	

ILLINOIS PROJECT NO. GBG-4053
AIP PROJECT NO. 3-17-0047-B14

JANUARY 14, 2011

INDEX OF SHEETS	
NO.	DESCRIPTION
1	COVER SHEET
2	IMPROVEMENT AND SAFETY PLAN
3	TYPICAL SECTION AND SPECIAL DETAILS
4	RUNWAY 3-21 PLAN AND PROFILE
5	RUNWAY MARKING LAYOUT



VICINITY MAP



EXPIRES 11/30/2011
ENGINEER'S SEAL

PLANS PREPARED BY :
HUTCHISON ENGINEERING, INC.
JACKSONVILLE, ILLINOIS

SUBMITTED - *Gary L. Hutchison*
DATE - *January 6, 2011*

CITY OF GALESBURG, ILLINOIS

APPROVED - *[Signature]*
DATE - *1-7-2011*
DIRECTOR OF PUBLIC WORKS

APPROVED - *[Signature]*
DATE - *1-7-2011*
CITY ENGINEER

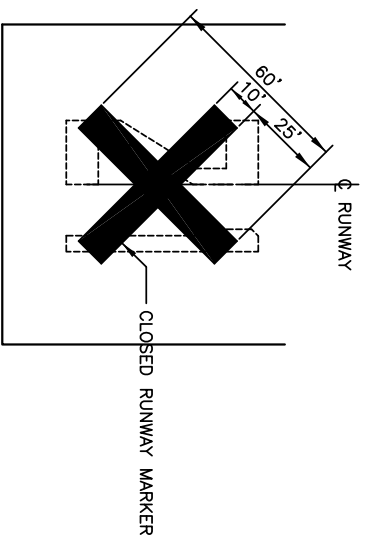
CONSTRUCTION PROCEDURE NOTES

1. ALL CONTRACTOR EMPLOYEES WILL PARK THEIR PERSONAL VEHICLES IN THE AREA DESIGNATED BY THE AIRPORT MANAGER FOR USE AS A VEHICLE PARKING AREA. THE CONTRACTOR WILL BE RESPONSIBLE FOR TRANSPORTING EMPLOYEES TO THE PROPOSED CONSTRUCTION SITE. ONLY AUTHORIZED CONTRACTOR VEHICLES WILL BE ALLOWED ON THE AIR OPERATIONS AREA OF THE AIRPORT AND ON THE CONSTRUCTION SITE.
2. FLAGS WILL BE REQUIRED ON ALL CONTRACTOR'S VEHICLES AND EQUIPMENT USED ON THE AIRPORT OPERATIONS AREA AND CONSTRUCTION SITE. THE FLAGS SHALL BE THREE (3) FOOT SQUARE CHECKERED FLAGS (INTERNATIONAL ORANGE AND WHITE) DISPLAYED IN FULL VIEW ABOVE THE VEHICLE.
3. THE CONTRACTOR WILL USE THE DESIGNATED HAUL ROUTE AND EQUIPMENT STORAGE AREA SHOWN ON THIS SHEET. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE DESIGNATED HAUL ROUTE AND EQUIPMENT STORAGE AREA FOR THE DURATION OF THE PROJECT. ANY DAMAGE TO EXISTING PAVEMENTS USED TO HAUL MATERIAL TO THE CONSTRUCTION SITE WILL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE TO THE SATISFACTION OF THE AIRPORT MANAGER AND THE RESIDENT ENGINEER.
4. THE CONTRACTOR SHALL FURNISH, MAINTAIN, AND REMOVE ALL BARRICADES REQUIRED TO DELINEATE THE WORK AREA AND KEEP AIRCRAFT FROM ENCRoACHING INTO SAID WORK AREA. THIS REQUIREMENT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
5. CONSTRUCTION ACTIVITY CONTROL AND OPERATION AREA PROTECTION SHALL BE ACCOMPLISHED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR NO. 150/5370-2E.
6. THE MAXIMUM CONSTRUCTION EQUIPMENT HEIGHT UTILIZED ON THE AIRPORT FOR THIS PROJECT SHALL BE 30'.
7. THIS PROJECT CONSISTS OF WORK TO APPLY POROUS FRICTION COURSE (PFC) TO A PORTION OF RUNWAY 3-21.
8. THE CONTRACTOR SHALL COORDINATE A METHOD OF DISABLING THE RUNWAY 3-21 EDGE LIGHTS WITH THE AIRPORT MANAGEMENT SO AS TO KEEP THE LIGHTS INOPERABLE WHILE THE RUNWAY IS CLOSED TO AIR TRAFFIC. THE COST OF DISABLING AND RE-ENERGIZING THE EDGE LIGHT CIRCUIT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
9. THE UNICOM FREQUENCY AT THE AIRPORT IS 123.0 MHZ. THE CONTRACTOR SHALL FURNISH A FLAGMAN WITH A RADIO IN HIS POSSESSION TO MONITOR AIR TRAFFIC ON THIS FREQUENCY AND DIRECT HAULING EQUIPMENT ACROSS RUNWAY 10-28 DURING CONSTRUCTION. NO VEHICLE, EQUIPMENT OR PERSON SHALL CROSS AN ACTIVE RUNWAY WITHOUT RECEIVING CLEARANCE TO CROSS FROM THE FLAGMAN. VEHICLE DRIVERS SHALL ALSO CONFIRM BY PERSONAL OBSERVATION THAT NO AIRCRAFT IS APPROACHING WHEN GIVEN CLEARANCE TO CROSS.
10. PRIOR TO THE OPERATION OF ANY CONSTRUCTION VEHICLES WITHIN THE AIR OPERATIONS AREA, IT WILL BE REQUIRED THAT THE CONTRACTOR RECEIVE TRAINING FROM THE AIRPORT MANAGEMENT REGARDING PROCEDURES FOR VEHICLE OPERATIONS TO ENSURE AIRCRAFT SAFETY DURING CONSTRUCTION. IT WILL BE REQUIRED THAT THE CONTRACTOR RECEIVE PERMISSION FROM THE AIRPORT MANAGEMENT IN ADVANCE OF ANY ACTIVITY SPECIFIC TO THE DAY, TIME AND EXACT PURPOSE OF ANY NEED TO ENTER THE AIRPORT OPERATIONS AREA.
11. THIS PROJECT WILL DISTURB LESS THAN 1 ACRE OF LAND.

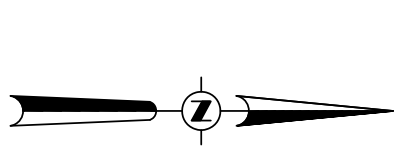
RECOMMENDED CONSTRUCTION SEQUENCE

1. INSTALL CLOSED RUNWAY MARKERS AT THE ENDS OF RUNWAY 3-21 TO CLOSE THE RUNWAY DURING THE CONSTRUCTION PERIOD. DISABLE THE RUNWAY 3-21 EDGE LIGHTING CIRCUIT FOR THE DURATION OF THE CONSTRUCTION PERIOD. COORDINATE WITH THE AIRPORT MANAGEMENT TO ARRANGE FOR SHUTDOWN OF THE RUNWAY 3-21 INSTRUMENT LANDING SYSTEM COMPONENTS AND NAVAIDS BY THE FAA AIRWAYS FACILITY UNIT FOR THE CONSTRUCTION PERIOD DURATION.
2. BARRICADE OFF TAXIWAY A AND RUNWAY 3-21 SOUTH OF RUNWAY 10-28 DENNING CONSTRUCTION PROJECT.
3. WORK SHALL BE ACCOMPLISHED ON RUNWAY 3-21 LEAVING THE PORTION OF TAXIWAY A LOCATED NORTH OF RUNWAY 10-28 OPEN FOR AIRCRAFT TO ACCESS RUNWAY 10-28 AND ITS PARALLEL TAXIWAY B. RUNWAY 10-28 SHALL REMAIN OPEN FOR USE BY AIR TRAFFIC DURING THIS CONSTRUCTION PROJECT.
4. THE CONTRACTOR MAY USE THE SECTION OF TAXIWAY A LOCATED SOUTH OF RUNWAY 10-28 AS HIS HAUL ROUTE PROVIDED THAT ANY PAVEMENT MARKING THAT IS OBTSCURED BY TRACKED BITUMINOUS TACK COAT SHALL BE REMARKED BY THE CONTRACTOR AT HIS OWN EXPENSE. THE CONTRACTOR IS ENCOURAGED TO INSTRUCT HIS HAULING PERSONNEL TO NOT DRIVE ON THE PAVEMENT CENTERLINE MARKING.
5. WORK SHALL BE SEQUENCED SO AS NOT TO ALLOW THE CONTRACTOR'S HAULING EQUIPMENT TO DRIVE ON THE FINISHED SURFACE OF THE POROUS FRICTION COURSE. OPEN RUNWAY 3-21 UPON COMPLETION OF WORK AND MARKING.

CLOSED RUNWAY MARKER DETAIL



1. CLOSED RUNWAY MARKER SHALL BE YELLOW IN COLOR.
2. CLOSED RUNWAY MARKER SHALL BE PAINTED PLYWOOD, PLASTIC SHEETING WEIGHTED DOWN WITH SANDBAGS, OR OTHER APPROVED MATERIALS ACCEPTABLE TO THE ENGINEER.
3. CONTRACTOR SHALL MAINTAIN MARKERS AS DIRECTED BY THE RESIDENT ENGINEER.
4. THE COST OF FURNISHING, PLACING, MAINTAINING, AND REMOVING CLOSED RUNWAY MARKERS SHALL BE PAID FOR UNDER ITEM AR150530 TRAFFIC MAINTENANCE.



LEGEND

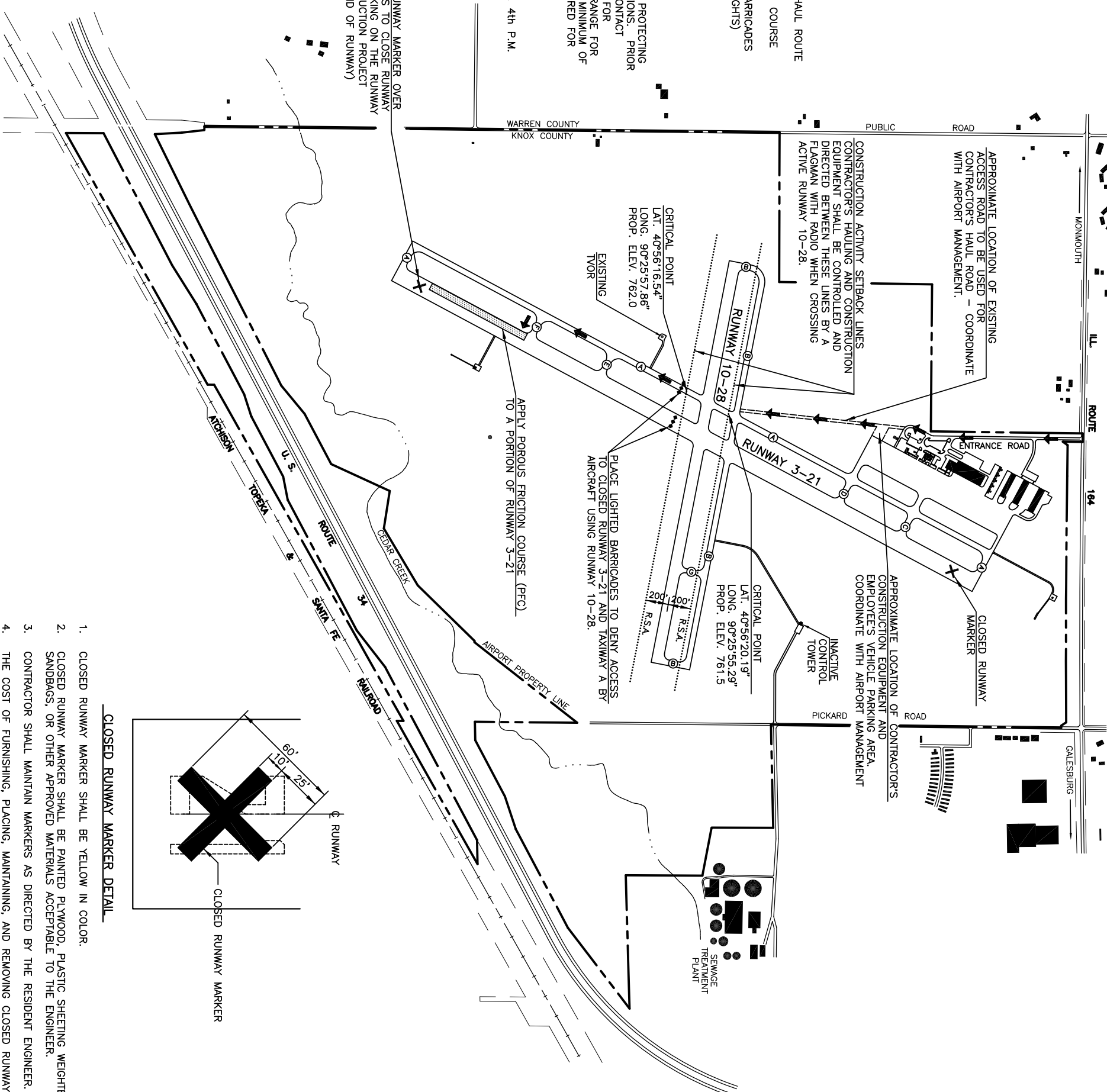
- CONTRACTOR'S ACCESS/HAUL ROUTE
- APPLY POROUS FRICTION COURSE
- LOCATION OF LIGHTED BARRICADES (USING FLASHING RED LIGHTS) AT 15'± CTRS.

GENERAL NOTES

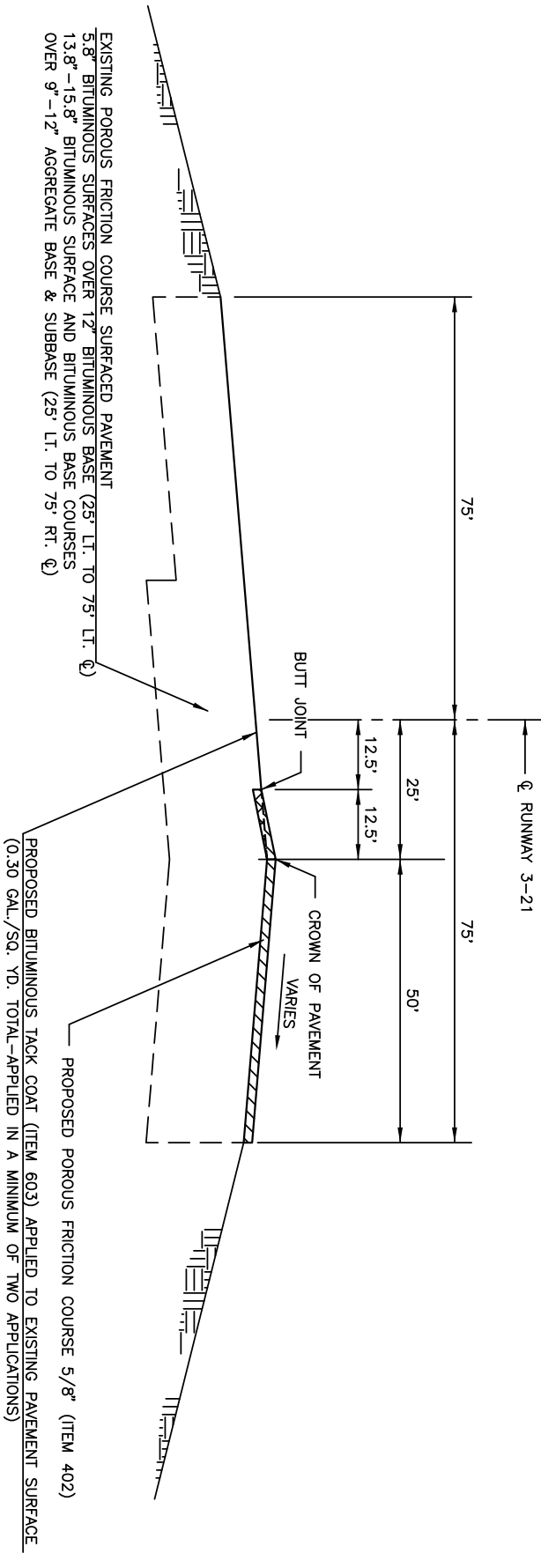
1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY FROM CONSTRUCTION OPERATIONS. PRIOR TO STARTING WORK, THE CONTRACTOR SHALL CONTACT JULLIE (JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS) PHONE 1-800-892-0123 TO ARRANGE FOR LOCATION OF UTILITIES IN THE WORK AREA. A MINIMUM OF FORTY-EIGHT HOURS ADVANCE NOTICE IS REQUIRED FOR NON-EMERGENCY WORK.

COUNTY — KNOX
 CITY — GALESBURG
 TOWNSHIP — T. 11 N., R. 1 E. 4th P.M.
 SECTION NO. — 18 & 19

PLACE CLOSED RUNWAY MARKER OVER RUNWAY NUMERALS TO CLOSE RUNWAY 3-21 WHEN WORKING ON THE RUNWAY FOR THIS CONSTRUCTION PROJECT (TYPICAL EACH END OF RUNWAY)



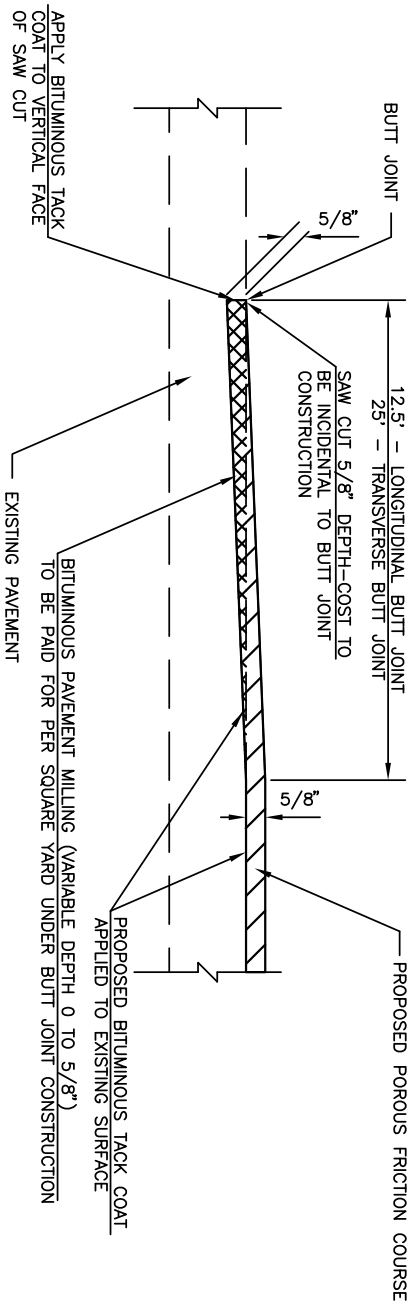
GALESBURG MUNICIPAL AIRPORT GALESBURG, ILLINOIS	
IMPROVEMENT AND SAFETY PLAN	
HUTCHINSON ENGINEERING, INC. JACKSONVILLE, ILLINOIS	
DRAWN BY: R.L.R.	ILL. PROJ. NO. 686-4053
DATE: JANUARY, 2011	AIP PROJ. NO. 3-17-0047-814



RUNWAY 3-21 TYPICAL SECTION
NOT TO SCALE

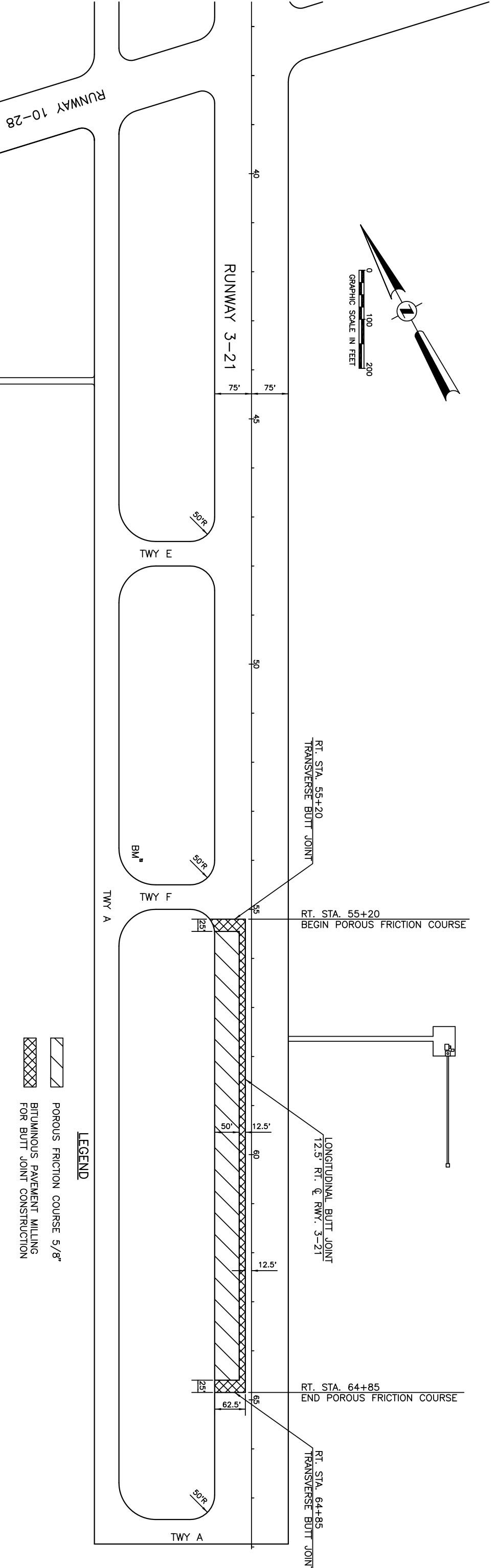
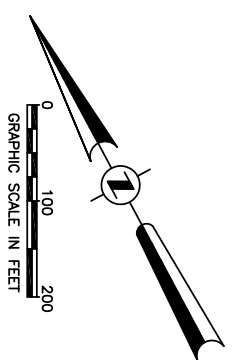
GENERAL NOTES

1. ANY SOD WHICH HAS ENCRACHPED UPON THE PAVEMENT EDGE SHALL BE BLADED BACK PRIOR TO THE PROPOSED POROUS FRICTION COURSE APPLICATION. THE PAVEMENT SURFACE SHALL BE SWPT TO REMOVE ALL LOOSE MATERIALS BEFORE THE APPLICATION OF BITUMINOUS TACK COAT. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR PAVEMENT PREPARATION.
2. ANY DAMAGE TO THE EXISTING SOD BEYOND THE EDGE OF PAVEMENT RESULTING FROM THE BLADING OF THE SOD OFF THE PAVEMENT SURFACE SHALL BE FILLED WITH DIRT, GRADED, FERTILIZED AND SEEDED TO RESTORE THE SOD SHOULDERS TO THEIR ORIGINAL CONDITION. ANY LOCALLY AVAILABLE 12-12-12 FERTILIZER MAY BE USED APPLIED AT A RATE OF 1 POUND OF FERTILIZER PER EACH 160 SQUARE FEET OF DISTURBED OR RESEEDING AREA. SEED USED SHALL BE ALTA FESCUE OR KENTUCKY 31, APPLIED AT THE RATE OF 1 POUND OF PURE LIVE SEED PER EACH 436 SQUARE FEET OF RESEEDING AREA. THIS WORK WILL NOT BE PAID FOR SEPARATELY AND SHALL BE CONSIDERED INCIDENTAL TO PAVEMENT PREPARATION.
3. A BITUMINOUS TACK COAT SHALL BE APPLIED TO THE EXISTING PAVEMENT PRIOR TO PLACING THE POROUS FRICTION COURSE. THE APPLICATION RATE SHOWN ON THE TYPICAL SECTION WAS USED TO CALCULATE THE PLAN QUANTITY. THE EXACT AMOUNT OF BITUMINOUS TACK COAT TO BE USED SHALL BE DETERMINED BY THE PROJECT ENGINEER AT THE TIME OF CONSTRUCTION.
4. THE POROUS FRICTION COURSE SHALL BE ABUTTED TO THE EXISTING PAVEMENT ON RUNWAY 3-21 AS SHOWN ON THE BITUMINOUS BUTT JOINT DETAIL. THE EXISTING PAVEMENT CONFINED WITHIN THE BITUMINOUS MILLING AREA WILL BE REMOVED IN ACCORDANCE WITH THE DETAIL FROM 0 TO 5/8" DEPTH AT THE BUTT JOINT LOCATION. FILLING OF THE MILLED AREA WILL BE MADE USING POROUS FRICTION COURSE MIX AND WILL BE PAID FOR PER SQUARE YARD FOR POROUS FRICTION COURSE.
5. ALL AREAS DISTURBED BY THE CONTRACTOR INCLUDING THE CONTRACTOR'S PARKING AREA, HAUL ROADS, ETC. SHALL BE REGRADED AND RESEDED AND/OR RESTORED TO THEIR ORIGINAL CONDITION. THIS RESTORATION SHALL BE ACCOMPLISHED BY THE CONTRACTOR AT HIS OWN EXPENSE UPON COMPLETION OF THIS IMPROVEMENT PROJECT.

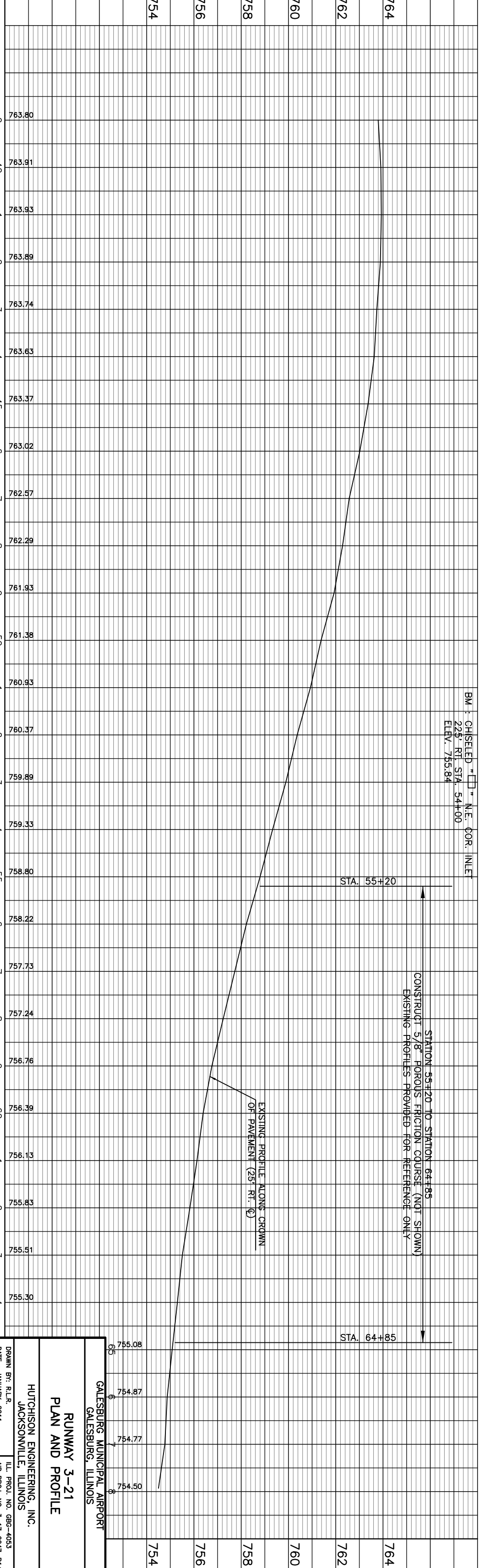


BITUMINOUS BUTT JOINT CONSTRUCTION DETAIL
NOT TO SCALE

GALESBURG MUNICIPAL AIRPORT GALESBURG, ILLINOIS
TYPICAL SECTIONS AND SPECIAL DETAILS
HUTCHISON ENGINEERING, INC. JACKSONVILLE, ILLINOIS
DRAWN BY: R.L.R. DATE: JANUARY, 2011
ILL. PROJ. NO. 686-4053 AIP PROJ. NO. 3-17-0047-814



- LEGEND**
- POROUS FRICTION COURSE 5/8"
 - BITUMINOUS PAVEMENT MILLING FOR BUTT JOINT CONSTRUCTION



BM : CHISELED "□" N.E. COR. INLET
 225' RT. STA. 54+00
 ELEV. 755.84

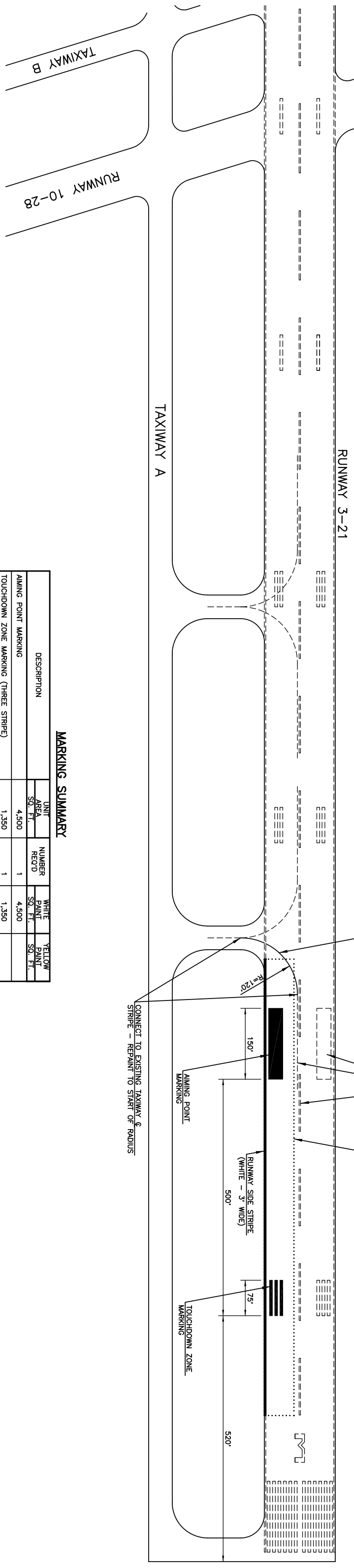
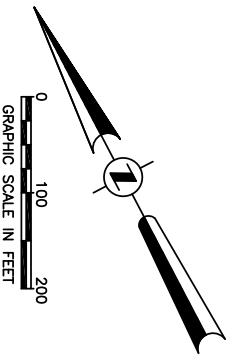
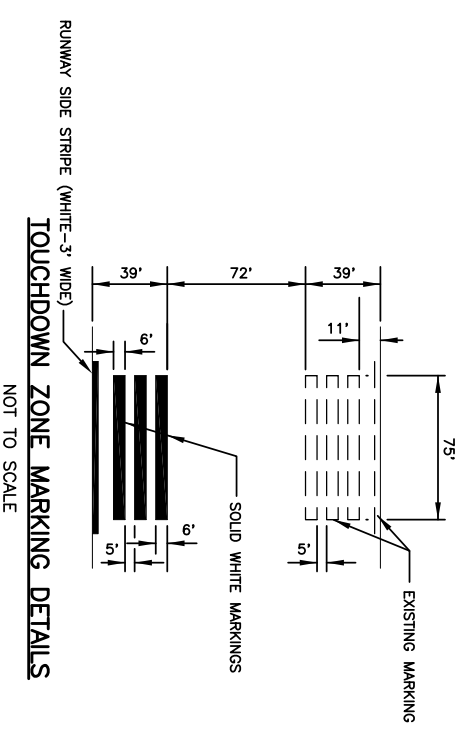
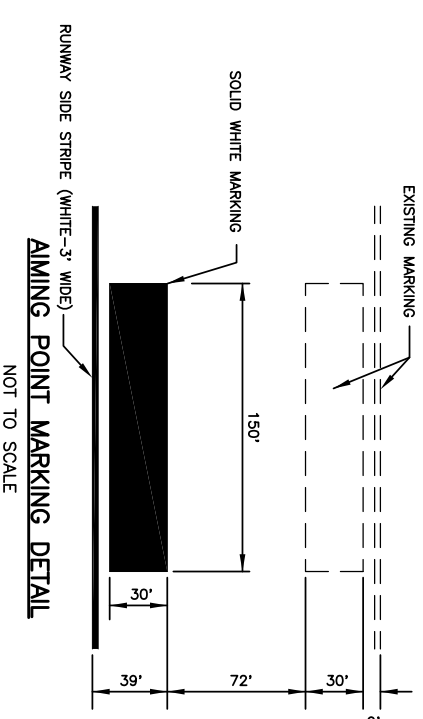
STATION 55+20 TO STATION 64+85
 CONSTRUCT 5/8" POROUS FRICTION COURSE (NOT SHOWN)
 EXISTING PROFILES PROVIDED FOR REFERENCE ONLY

EXISTING PROFILE ALONG CROWN
 OF PAVEMENT (25' RT. C/L)

GALESBURG MUNICIPAL AIRPORT
 GALESBURG, ILLINOIS

RUNWAY 3-21
PLAN AND PROFILE

HUTCHISON ENGINEERING, INC.
 JACKSONVILLE, ILLINOIS
 ILL. PROF. NO. 086-4053
 DATE : JANUARY, 2011
 AIP PROJ. NO. 3-17-0047-814



MARKING SUMMARY

DESCRIPTION	UNIT	WHITE PAINT		YELLOW PAINT	
		AREA SQ. FT.	NUMBER REQ'D	AREA SQ. FT.	NUMBER REQ'D
AIMING POINT MARKING		4,500	1	4,500	
TOUCHDOWN ZONE MARKING (THREE STRIPE)		1,350	1	1,350	
RUNWAY SIDE STRIPE		2,895	1	2,895	
TAXIWAY CENTERLINE STRIPE		185		185	
TOTALS		8,745		8,745	185

NOTES

1. ALL RUNWAY MARKING SHALL BE SOLID MARKING AND WHITE IN COLOR.
2. ALL TAXIWAY MARKING SHALL BE SOLID MARKING AND YELLOW IN COLOR.
3. ALL PAVEMENT MARKING SHALL CONSIST OF TWO APPLICATIONS OF PAINT AND ONE APPLICATION OF GLASS BEADS (REFLECTIVE MEDIA). SEE THE SPECIAL PROVISIONS FOR THE APPLICATION RATES.
4. TAXIWAY MARKING SHALL BE INTERRUPTED AT THE RUNWAY MARKING COMPONENTS FOR SIDE STRIPES, ETC. (I.E.-TAXIWAY MARKING SHALL NOT BE PAINTED ACROSS THE RUNWAY MARKING COMPONENTS)

MARKING LAYOUT

GALESBURG MUNICIPAL AIRPORT
GALESBURG, ILLINOIS

HUTCHISON ENGINEERING, INC.
JACKSONVILLE, ILLINOIS

DRAWN BY: R.L.R.
DATE: JANUARY, 2011
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