

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|-----------|----------|--------------------|-----------|
| 39 | (50-5B)-1 | LASALLE | 42 | 1 |
| | | ILLINOIS | CONTRACT NO. 66L74 | |

P-93-019-20
D-93-036-21

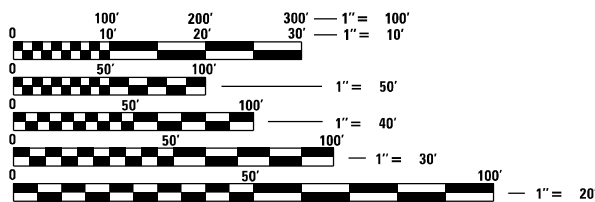
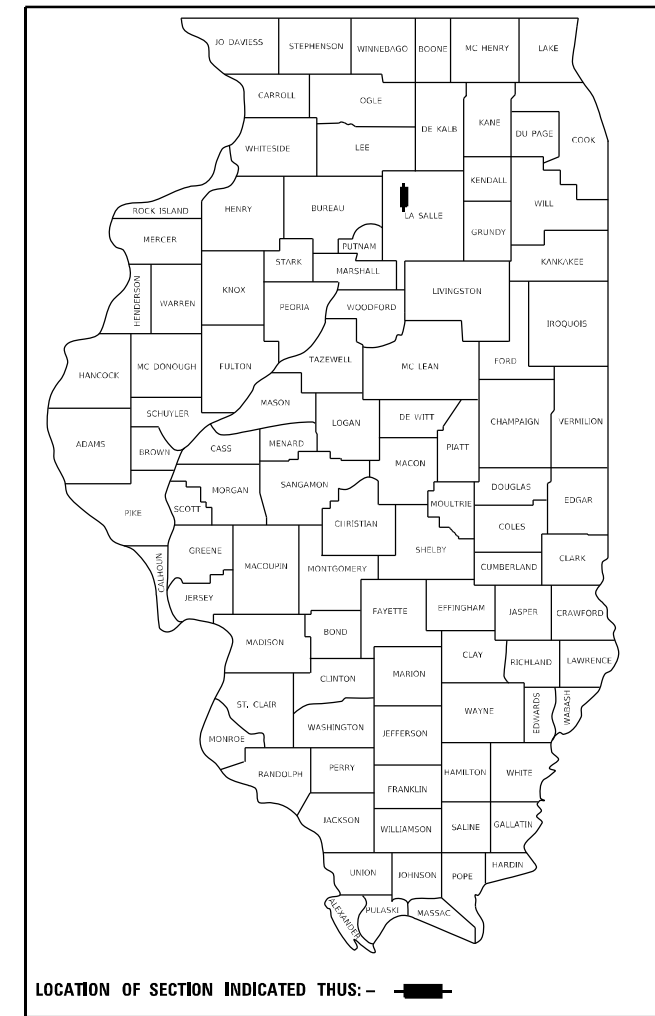
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PROPOSED HIGHWAY PLANS

FAI ROUTE 39 (I-39)
SECTION 50(5A&5B)BJR,BRR
PROJECT: NHPP-4WVR(541)
BRIDGE REPAIR
LASALLE COUNTY

C-93-070-21



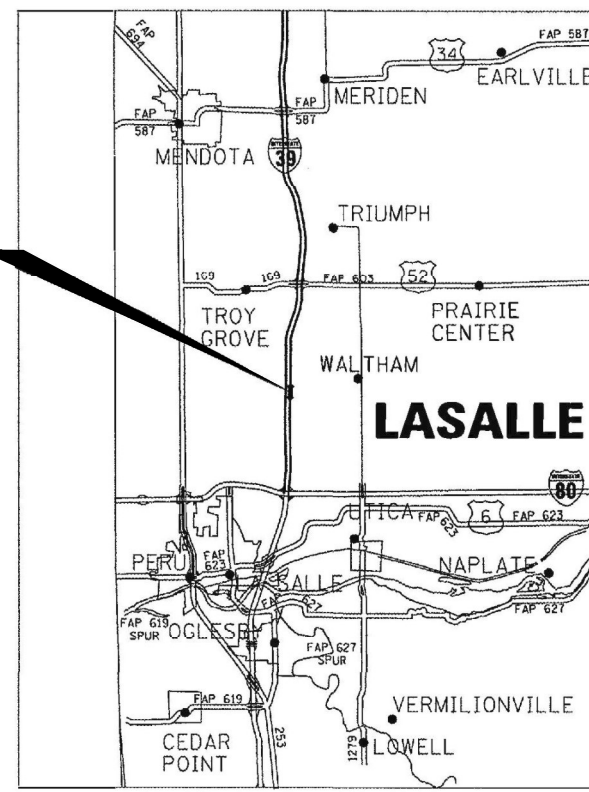
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS
1-800-892-0123
OR 811

PROJECT ENGINEER JACOB OYIER P.E.
UNIT CHIEF STEPHENIE CARMIN

CONTRACT NO. 66L74

PROJECT LOCATION
SN 050-0174
SN 050-0173
FAI 39 (I-39) NORTHBOUND
& SOUTHBOUND
OVER TOMAHAWK CREEK
3.5 MI NORTH OF I-80



N
LOCATION MAP
NOT TO SCALE

GROSS LENGTH = 136 FT. = 0.03 MILE
NET LENGTH = 136 FT. = 0.03 MILE

FUNCTIONAL CLASSIFICATION
INTERSTATE
2019 ADT = 19700
MU = 37.6%, SU = 5.5%, PV = 56.9%

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
SUBMITTED December 17, 20 21
[Signature] REGIONAL ENGINEER
February 4, 20 22
[Signature] ENGINEER OF DESIGN AND ENVIRONMENT
February 4, 20 22
[Signature] DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

HIGHWAY STANDARDS

| | |
|-----------|--|
| 000001-08 | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS |
| 001001-02 | AREAS OF REINFORCEMENT BARS |
| 001006 | DECIMAL OF AN INCH AND OF A FOOT |
| 483001-06 | PCC SHOULDER |
| 610001-09 | SHOULDER INLET WITH CURB |
| 701101-05 | OFF-ROAD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE |
| 701106-02 | OFF-ROAD OPERATIONS, MULTILANE, MORE THAN 15' AWAY |
| 701301-04 | LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS |
| 701400-11 | APPROACH TO LANE CLOSURE, FREEWAY EXPRESSWAY |
| 701402-12 | LANE CLOSURE, FREEWAY/EXPRESSWAY, WITH BARRIER |
| 701426-09 | LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≥ 45 MPH |
| 701428-01 | TRAFFIC CONTROL, SETUP AND REMOVAL, FREEWAY/EXPRESSWAY |
| 701901-08 | TRAFFIC CONTROL DEVICES |
| 704001-08 | TEMPORARY CONCRETE BARRIER |
| 782006-01 | GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS |

GENERAL NOTES

EXCEPT AS NOTED ON THE PLANS, PAVEMENT GRADES SHOWN ARE AT THE TOP OF PAVEMENT SURFACES.
 THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED IN CALCULATING PLAN QUANTITIES:

| | | |
|-----------------------------|----|----------------------------|
| SHORT TERM PAVEMENT MARKING | 10 | FT / 100 FT OF APPLICATION |
|-----------------------------|----|----------------------------|

MEMBERS OF J.U.L.I.E. KNOWN TO BE WITHIN THE LIMITS OF THE IMPROVEMENT ARE: NONE
 NON-MEMBERS OF J.U.L.I.E. KNOWN TO BE WITHIN THE LIMITS OF THE IMPROVEMENT ARE: NONE

COMMITMENTS

NONE 09/17/2021

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DISTRICT THREE
 AS BUILT INFORMATION

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DISTRICT THREE

 SUPERVISING CONSTRUCTION FIELD ENGINEER

PREPARED BY: _____
 DISTRICT STUDIES & PLANS ENGINEER

 RESIDENT ENGINEER / TECHNICIAN

DATE: _____

START & END DATES
 OF CONSTRUCTION: _____

EXAMINED BY: _____
 DISTRICT CONSTRUCTION ENGINEER

INSPECTORS: _____

 DISTRICT MATERIALS ENGINEER

 DISTRICT OPERATIONS ENGINEER

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| | | |
|---|-------------------|-----------|
| USER NAME = mjverheyen | DESIGNED - CRS | REVISED - |
| | DRAWN - CRS | REVISED - |
| PLOT SCALE = 20' 00" = 1" = 20' 00" / in. | CHECKED - DB/RJT | REVISED - |
| PLOT DATE = 12/10/2021 | DATE - 12/10/2021 | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**HIGHWAY STANDARDS,
 GENERAL NOTES AND COMMITMENTS**

SCALE: N.T.S. SHEET 1 OF 1 SHEETS STA. 1210+35.63 TO STA. 1222+53.23

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|-----------|---------|---------------------------|--------------------|
| 39 | (50-5B)-1 | LASALLE | 41 | 2 |
| | | | | CONTRACT NO. 66L74 |
| | | | ILLINOIS FED. AID PROJECT | |

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | CONSTRUCTION CODE |
|----------|--|--------|----------------|----------------------|
| | | | | 90% FED 10% STATE |
| | | | | BRIDGE |
| | | | | 0047 RURAL |
| 31102100 | SUBBASE GRANULAR MATERIAL, TYPE C 4" | SQ YD | 254 | 254 |
| 44004250 | PAVED SHOULDER REMOVAL | SQ YD | 3139 | 3139 |
| 48300500 | PORTLAND CEMENT CONCRETE SHOULDERS 10" | SQ YD | 3139 | 3139 |
| 50102400 | CONCRETE REMOVAL | CU YD | 16.2 | 16.2 |
| 50300255 | CONCRETE SUPERSTRUCTURE | CU YD | 16.2 | 16.2 |
| 50800205 | REINFORCEMENT BARS, EPOXY COATED | POUND | 2,550 | 2,550 |
| 50800515 | BAR SPLICERS | EACH | 24 | 24 |
| 52000110 | PREFORMED JOINT STRIP SEAL | FOOT | 178.0 | 178.0 |
| 58700300 | CONCRETE SEALER | SQ FT | 5,131 | 5,131 |
| 59000200 | EPOXY CRACK INJECTION | FOOT | 306 | 306 |
| 60260100 | INLETS TO BE ADJUSTED | EACH | 4 | 4 |
| 67000400 | ENGINEER'S FIELD OFFICE, TYPE A | CAL MO | 5 | 5 |

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| | | |
|----------------------------|-------------------|-----------|
| USER NAME = mjverheyen | DESIGNED - CRS | REVISED - |
| | DRAWN - CRS | REVISED - |
| PLOT SCALE = 2.0000' / in. | CHECKED - DB/RJT | REVISED - |
| PLOT DATE = 12/10/2021 | DATE - 12/10/2021 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: N.T.S. SHEET 1 OF 4 SHEETS STA. 1210+35.63 TO STA. 1222+53.23

| F.A.I RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|------------|---------|---------------------------|--------------|
| 39 | (50-58)F-1 | LASALLE | 41 | 3 |
| CONTRACT NO. 66L74 | | | ILLINOIS FED. AID PROJECT | |

| |
|----------------------|
| CONSTRUCTION CODE |
| 90% FED 10% STATE |
| BRIDGE |
| 0047 |
| RURAL |

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | |
|------------|--|--------|----------------|--------|
| 67100100 | MOBILIZATION | L SUM | 1 | 1 |
| 70100207 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701402 | EACH | 2 | 2 |
| 70103815 | TRAFFIC CONTROL SURVEILLANCE | CAL DA | 14 | 14 |
| 70107025 | CHANGEABLE MESSAGE SIGN | CAL DA | 158 | 158 |
| 70300100 | SHORT TERM PAVEMENT MARKING | FOOT | 1037 | 1037 |
| 70300150 | SHORT TERM PAVEMENT MARKING REMOVAL | SQ FT | 347 | 347 |
| 70301120 | TEMPORARY PAVEMENT MARKING - LINE 4" - EPOXY | FOOT | 11,610 | 11,610 |
| 70400100 | TEMPORARY CONCRETE BARRIER | FOOT | 1100 | 1100 |
| 70400200 | RELOCATE TEMPORARY CONCRETE BARRIER | FOOT | 1025 | 1025 |
| 70600250 | IMPACT ATTENUATORS, TEMPORARY, (NON-REDIRECTIVE), TEST LEVEL 3 | EACH | 2 | 2 |
| 70600350 | IMPACT ATTENUATORS, RELOCATE, (NON-REDIRECTIVE), TEST LEVEL 3 | EACH | 2 | 2 |
| * 78008310 | POLYUREA PAVEMENT MARKING TYPE II - LINE 4" | FOOT | 3433 | 3433 |
| * 78011025 | GROOVING FOR RECESSED PAVEMENT MARKING 5" | FOOT | 3433 | 3433 |
| * 78011045 | GROOVING FOR RECESSED PAVEMENT MARKING 9" | FOOT | 68 | 68 |

*= SPECIALTY ITEM

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| | | |
|----------------------------|-------------------|-----------|
| USER NAME = mjverheyen | DESIGNED - CRS | REVISED - |
| PLOT SCALE = 2,0000' / in. | DRAWN - CRS | REVISED - |
| PLOT DATE = 12/10/2021 | CHECKED - DB/RJT | REVISED - |
| | DATE - 12/10/2021 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: N.T.S. SHEET 2 OF 4 SHEETS STA. 1210+35.63 TO STA. 1222+53.23

| | | | | |
|---------------------------|-----------|---------|--------------------|-----------|
| F.A.J. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 39 | (50-58)-1 | LASALLE | 41 | 4 |
| | | | CONTRACT NO. 66L74 | |
| ILLINOIS FED. AID PROJECT | | | | |

CONSTRUCTION
CODE
90% FED
10% STATE
BRIDGE
0047
RURAL

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | |
|------------|--|-------|----------------|--------|
| * 78100100 | RAISED REFLECTIVE PAVEMENT MARKER | EACH | 8 | 8 |
| * 78200005 | GUARDRAIL REFLECTORS, TYPE A | EACH | 58 | 58 |
| * 78200011 | BARRIER WALL REFLECTORS, TYPE C | EACH | 86 | 86 |
| 78300202 | PAVEMENT MARKING REMOVAL - WATER BLASTING | SQ FT | 5049 | 5049 |
| X0322215 | CLEANING BRIDGE SCUPPERS AND DOWNSPOUTS | EACH | 6 | 6 |
| X0325748 | ACRYLIC COATING | SQ YD | 67 | 67 |
| X0325749 | FIBER WRAP | SQ FT | 480 | 480 |
| X0326649 | LINEAR DELINEATOR PANELS, 6 INCH | EACH | 4 | 4 |
| X0327809 | LINEAR DELINEATOR PANELS, 4 INCH | EACH | 16 | 16 |
| * 78004640 | PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - LINE 8" | FOOT | 68 | 68 |
| X5030250 | BRIDGE DECK GROOVING (LONGITUDINAL) | SQ YD | 703 | 703 |
| X5870015 | BRIDGE DECK CONCRETE SEALER | SQ FT | 11,536 | 11,536 |
| X7200201 | WIDTH RESTRICTION SIGNING | L SUM | 1 | 1 |
| Z0012148 | BRIDGE DECK SCARIFICATION 3" | SQ YD | 536 | 536 |

*= SPECIALTY ITEM

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| USER NAME = mjverheyen | DESIGNED - CRS | REVISED - |
| PLOT SCALE = 2.0000' / in. | DRAWN - CRS | REVISED - |
| PLOT DATE = 12/10/2021 | CHECKED - DB/RJT | REVISED - |
| | DATE - 12/10/2021 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: N.T.S. SHEET 3 OF 4 SHEETS STA. 1210+35.63 TO STA. 1222+53.23

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|---------|--------------------|-----------|
| 39 | (50-58)-1 | LASALLE | 41 | 5 |
| | | | CONTRACT NO. 66L74 | |
| ILLINOIS FED. AID PROJECT | | | | |

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | CONSTRUCTION CODE |
|----------|--|-------|----------------|----------------------|
| | | | | 90% FED 10% STATE |
| | | | | BRIDGE |
| | | | | 0047 RURAL |
| Z0012152 | BRIDGE DECK SCARIFICATION 3 1/2" | SQ YD | 556 | 556 |
| Z0012170 | BRIDGE DECK MICROSILICA CONCRETE OVERLAY, 3 1/4" | SQ YD | 556 | 556 |
| Z0012174 | BRIDGE DECK MICROSILICIA CONCRETE OVERLAY, 3 3/4" | SQ YD | 556 | 556 |
| Z0012754 | STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES) | SQ FT | 444 | 444 |
| Z0012755 | STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES) | SQ FT | 33 | 33 |
| Z0016001 | DECK SLAB REPAIR (FULL DEPTH, TYPE I) | SQ YD | 10.9 | 10.9 |
| Z0016002 | DECK SLAB REPAIR (FULL DEPTH, TYPE II) | SQ YD | 26.4 | 26.4 |
| Z0029090 | DIAMOND GRINDING (BRIDGE SECTION) | SQ YD | 1076 | 1076 |
| Z0043800 | PRECAST PRESTRESSED CONCRETE I-BEAM REPAIR | SQ FT | 67 | 67 |
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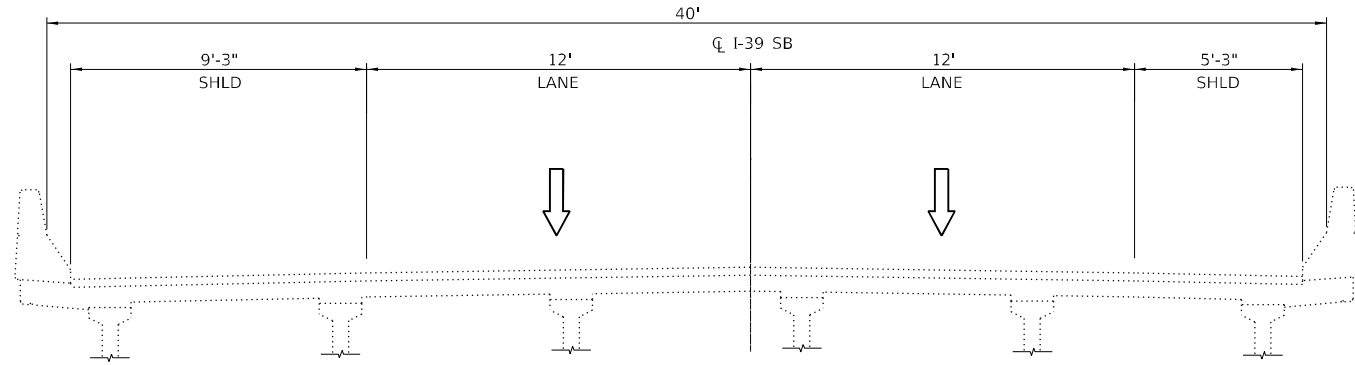


| | | |
|----------------------------|-------------------|-----------|
| USER NAME = mjverheyen | DESIGNED - CRS | REVISED - |
| | DRAWN - CRS | REVISED - |
| PLOT SCALE = 2.0000' / in. | CHECKED - DB/RJT | REVISED - |
| PLOT DATE = 12/10/2021 | DATE - 12/10/2021 | REVISED - |

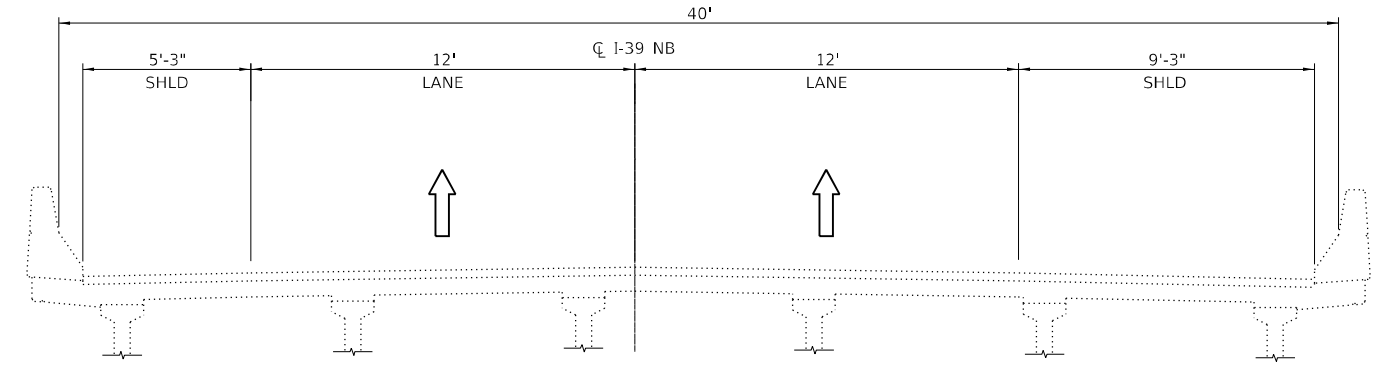
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| SUMMARY OF QUANTITIES | |
|-----------------------|---------------------|
| SCALE: N.T.S. | SHEET 4 OF 4 SHEETS |
| STA. 1210+35.63 | TO STA. 1222+53.23 |

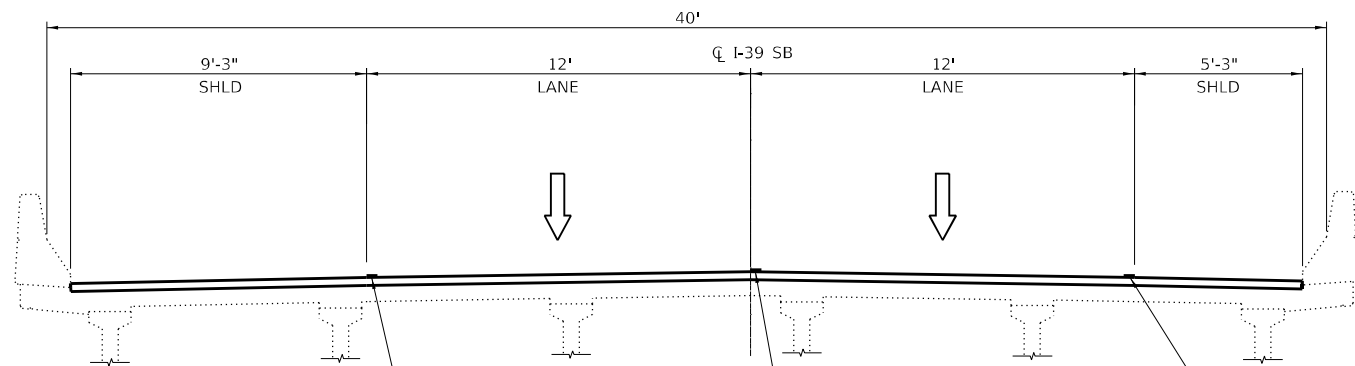
| F.A.I RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|------------|---------|---------------------------|--------------|
| 39 | (50-58)F-1 | LASALLE | 41 | 6 |
| CONTRACT NO. 66L74 | | | ILLINOIS FED. AID PROJECT | |



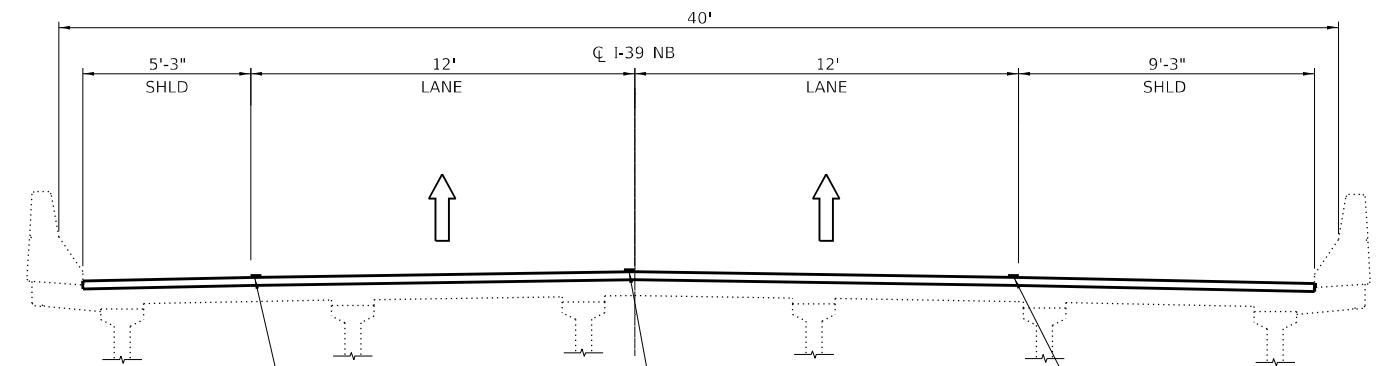
EXISTING TYPICAL SECTION
I - 39 SOUTHBOUND
(LOOKING NORTH)



EXISTING TYPICAL SECTION
I - 39 NORTHBOUND
(LOOKING NORTH)



PROPOSED TYPICAL SECTION
I - 39 SOUTHBOUND
(LOOKING NORTH)



PROPOSED TYPICAL SECTION
I - 39 NORTHBOUND
(LOOKING NORTH)

POLYUREA PAVEMENT MARKING
TYPE II - 4" (SOLID WHITE) AND
GROOVING FOR RECESSED
PAVEMENT MARKING 5"

PREFORMED PLASTIC PAVEMENT MARKING
TYPE D - LINE 8", CONTRAST (DASHED WHITE)
AND GROOVING FOR RECESSED
PAVEMENT MARKING 9"

POLYUREA PAVEMENT MARKING
TYPE II - 4" (SOLID YELLOW) AND
GROOVING FOR RECESSED
PAVEMENT MARKING 5"

POLYUREA PAVEMENT MARKING
TYPE II - 4" (SOLID YELLOW) AND
GROOVING FOR RECESSED
PAVEMENT MARKING 5"

PREFORMED PLASTIC PAVEMENT MARKING
TYPE D - LINE 8", CONTRAST (DASHED WHITE)
AND GROOVING FOR RECESSED
PAVEMENT MARKING 9"

POLYUREA PAVEMENT MARKING
TYPE II - 4" (SOLID WHITE) AND
GROOVING FOR RECESSED
PAVEMENT MARKING 5"

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| PLOT SCALE = 6,0000' / in. | DRAWN - MJV | REVISED - |
| PLOT DATE = 1/21/2022 | CHECKED - DB/RJT | REVISED - |
| | DATE - 01/21/2022 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS

SCALE: N.T.S. SHEET 1 OF 1 SHEETS STA. 1210+35.63 TO STA. 1222+53.23

| F.A.J. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|---------|--------------|-----------|
| 39 | 50(5A & 5B)BJR,BRR | LASALLE | 42 | 7 |
| CONTRACT NO. 66L74 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

| PAVEMENT MARKING SCHEDULE | 78008310 | | X2700027 | 78300202 | 70300100 | 70300150 | 78011025 | 78011045 | 78100100 |
|---------------------------|---|---------------------------------|--|---|--------------------------------|---|--|--|---|
| | POLYUREA PAVEMENT MARKING TYPE II - LINE 4" | | PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - LINE 8", CONTRAST | PAVEMENT MARKING REMOVAL - WATER BLASTING | SHORT TERM PAVEMENT MARKING | SHORT TERM PAVEMENT MARKING REMOVAL | GROOVING FOR RECESSED PAVEMENT MARKING 5" | GROOVING FOR RECESSED PAVEMENT MARKING 9" | RAISED REFLECTIVE PAVEMENT MARKER |
| LOCATION | YELLOW LANE LINE (FOOT) | WHITE LANE LINE (FOOT) | WHITE DOTTED LINE (FOOT) | (SQ FT) | (FOOT) | (SQ FT) | (FOOT) | (FOOT) | (EACH) |
| NORTHBOUND | 700.1 | 1016.4 | 34.0 | 589.2 | 518.4 | 173.4 | 1716.5 | 34.0 | 6.0 |
| SOUTHBOUND | 700.1 | 1016.4 | 34.0 | 589.2 | 518.4 | 173.4 | 1716.5 | 34.0 | 6.0 |
| TOTAL = | 3433 | | 68 | 1179 | 1037 | 347 | 3433 | 68 | 12 |

| MAINTENANCE OF TRAFFIC | 70400100 | 70400200 | 78200011 | 70600250 | 70600350 |
|------------------------|-------------------------------|---|---------------------------------------|--|---|
| | TEMPORARY CONCRETE BARRIER | RELOCATE TEMPORARY CONCRETE BARRIER | BARRIER WALL REFLECTORS, TYPE C | IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE) TEST LEVEL 3 | IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE) TEST LEVEL 3 |
| LOCATION | (FOOT) | (FOOT) | (EACH) | (EACH) | (EACH) |
| STAGE 1 | | | | | |
| NORTHBOUND | 550.0 | | 22 | 1 | |
| SOUTHBOUND | 550.0 | | 22 | 1 | |
| STAGE 2 | | | | | |
| NORTHBOUND | | 512.5 | 21 | | 1 |
| SOUTHBOUND | | 512.5 | 21 | | 1 |
| TOTAL = | 1100 | 1025 | 86 | 2 | 2 |

| GUARDRAIL SCHEDULE | 78200005 | X0326649 | X0327809 |
|--------------------|------------------------------------|--|--|
| | GUARDRAIL REFLECTORS, TYPE A | LINEAR DELINEATOR PANELS, 6 INCH | LINEAR DELINEATOR PANELS, 4 INCH |
| LOCATION | (EACH) | (EACH) | (EACH) |
| NORTHBOUND | 29.0 | 2.0 | 8.0 |
| SOUTHBOUND | 29.0 | 2.0 | 8.0 |
| TOTAL = | 58 | 4 | 16 |

| PRE-STAGING SHOULDER | 31102100 | 48300500 | 44004250 | 60260100 |
|----------------------|--|--|---------------------------|--------------------------|
| | SUBBASE GRANULAR MATERIAL, TYPE C 4" | PORTLAND CEMENT CONCRETE SHOULDERS 10" | PAVED SHOULDER REMOVAL | INLETS TO BE ADJUSTED |
| LOCATION | (SQ YD) | (SQ YD) | (SQ YD) | (EACH) |
| NORTHBOUND | 200.0 | 1570.5 | 1570.5 | 2.0 |
| SOUTHBOUND | 53.6 | 1567.7 | 1567.7 | 2.0 |
| TOTAL = | 254 | 3139 | 3139 | 4 |

| MAINTENANCE OF TRAFFIC TEMPORARY PAVEMENT MARKING | 70301120 | | X7030005 |
|--|---|------------------------------|---------------------------------------|
| | TEMPORARY PAVEMENT MARKING - LINE 4" - EPOXY | | TEMPORARY PAVEMENT MARKING REMOVAL |
| LOCATION | YELLOW LANE LINE (FOOT) | WHITE LANE LINE (FOOT) | (SQ FT) |
| STAGE 1 | | | |
| NORTHBOUND | 1016.6 | 2097.0 | 1037.9 |
| SOUTHBOUND | 1016.6 | 2097.0 | 1037.9 |
| STAGE 2 | | | |
| NORTHBOUND | 700.4 | 1990.3 | 896.9 |
| SOUTHBOUND | 700.4 | 1990.3 | 896.9 |
| TOTAL = | 11610 | | 3870 |

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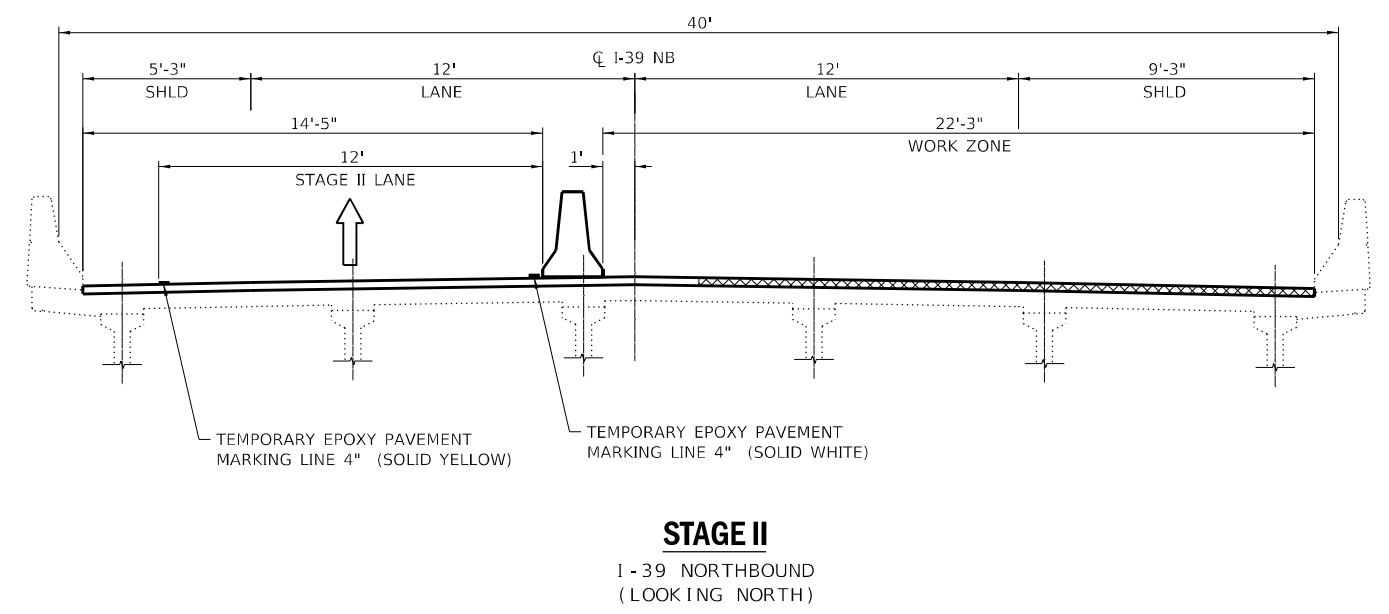
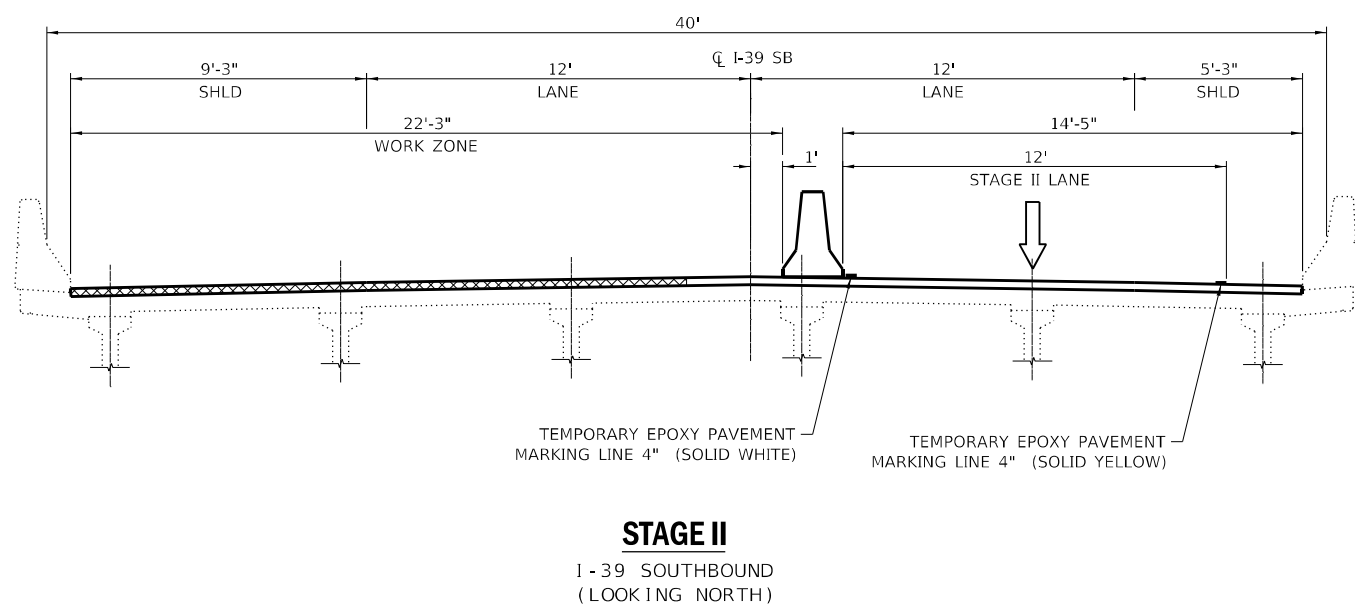
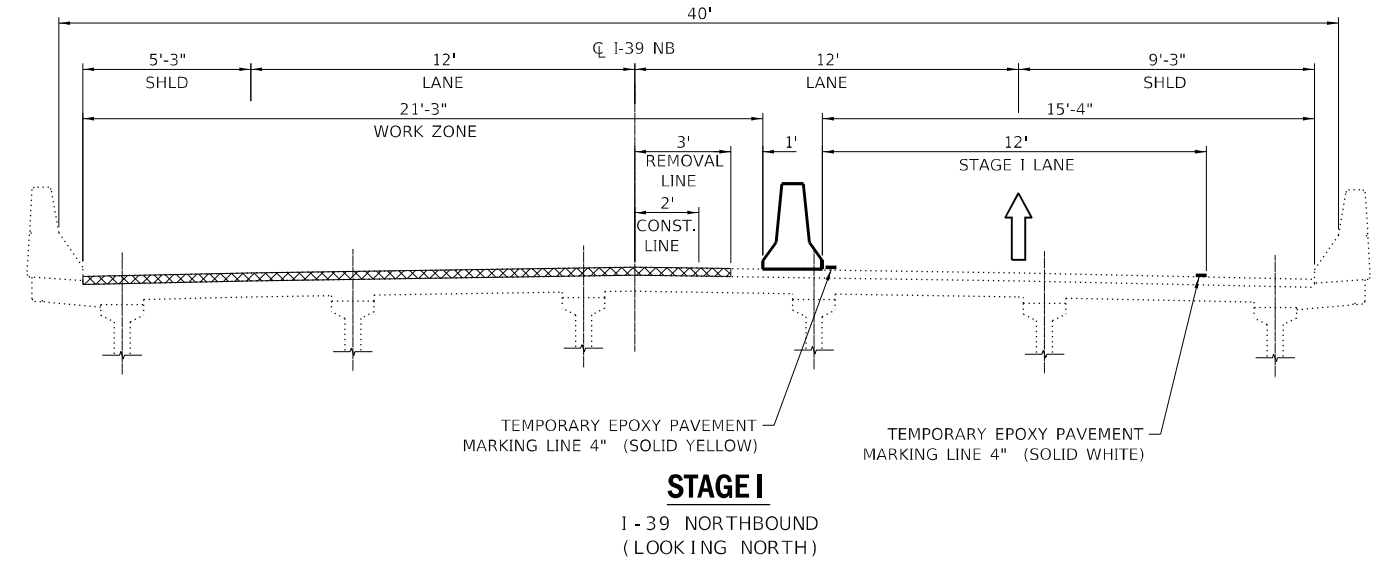
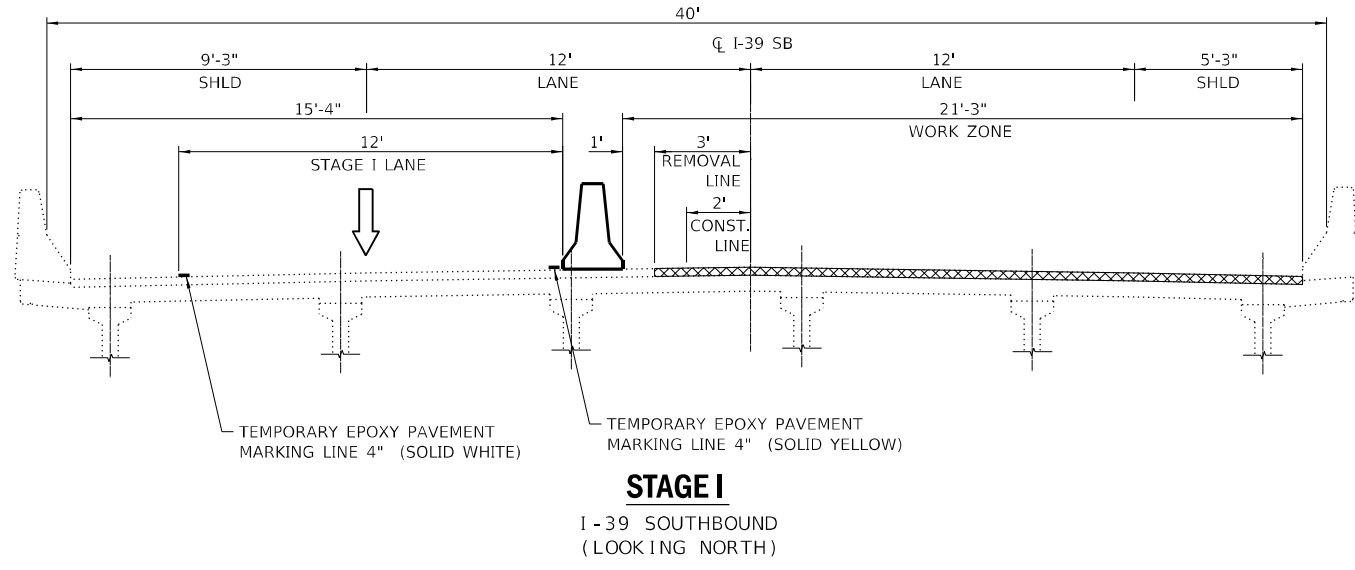
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| | DRAWN - MJV | REVISED - |
| PLOT SCALE = 100,0000' / in. | CHECKED - DB/RJT | REVISED - |
| PLOT DATE = 1/21/2022 | DATE - 01/21/2022 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CONSTRUCTION STAGING
SCHEDULE OF QUANTITIES**

SCALE: N.T.S. SHEET 1 OF 1 SHEETS STA. 1210+35.63 TO STA. 1222+53.23

| F.A.J RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|--------------------|---------|------------------|--------------|
| 39 | 50(5A & 5B)BJR,BRR | LASALLE | 42 | 8 |
| CONTRACT NO. 66L74 | | | | |
| ILLINOIS | | | FED. AID PROJECT | |



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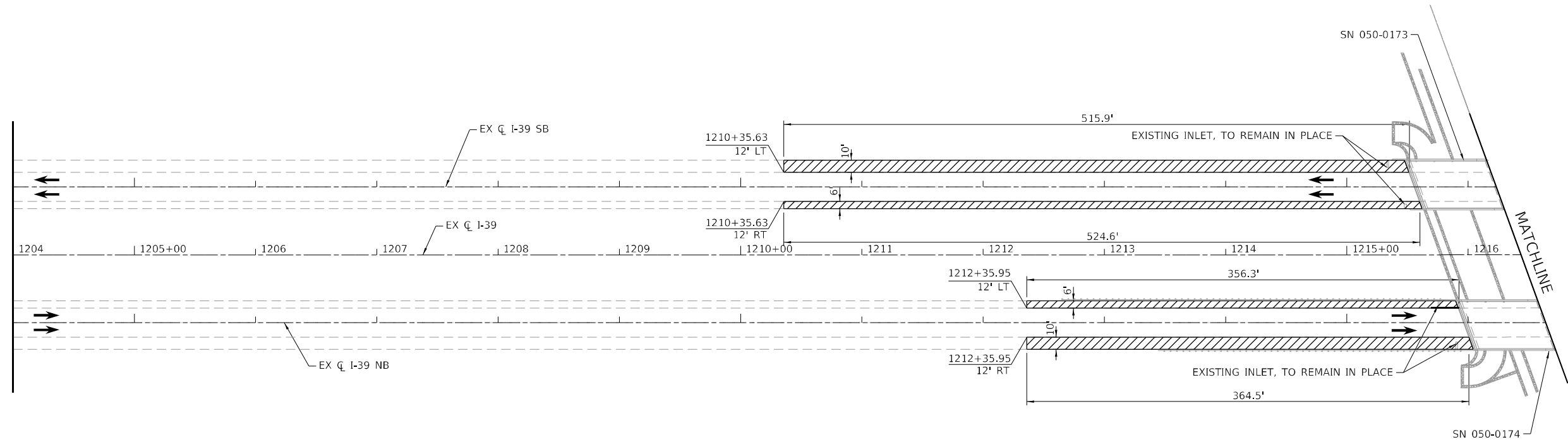
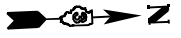
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| PLOT DATE = 1/21/2022 | DATE - 01/21/2022 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

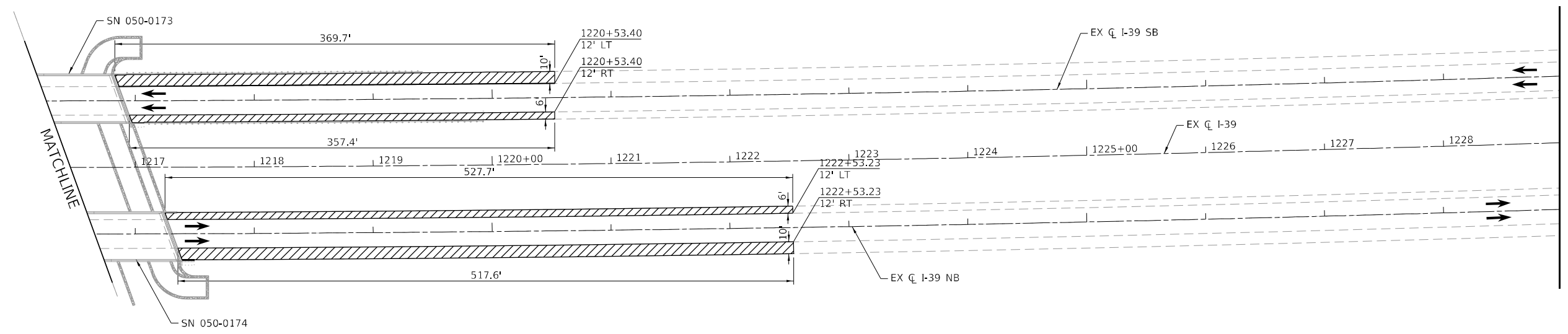
**STAGING AND TRAFFIC CONTROL
TYPICAL SECTIONS**

SCALE: N.T.S. SHEET 1 OF 1 SHEETS STA. 1210+35.63 TO STA. 1222+53.23

| F.A.J. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|---------|--------------|-----------|
| 39 | 50(5A & 5B)BJR,BRR | LASALLE | 42 | 9 |
| CONTRACT NO. 66L74 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



NOTES:
 EXISTING INLETS ARE TO REMAIN IN PLACE AND FIELD ADJUSTED TO MAINTAIN EXISTING 1/2"/FT SLOPE WHERE NECESSARY
 SEE STANDARD 610001-09, 420001-10, AND 483001-06 FOR FURTHER DETAIL



LEGEND

PAVED SHOULDER REMOVAL AND PROPOSED PCC SHOULDER 10"

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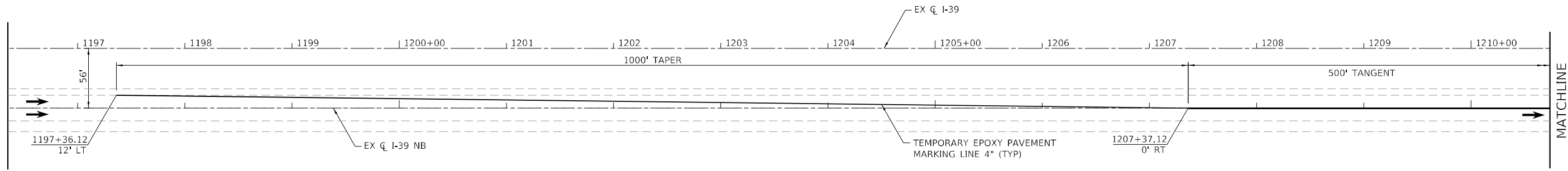
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| PLOT DATE = 1/21/2022 | DATE - 01/21/2022 | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC
 PRE-STAGING DETAILS**

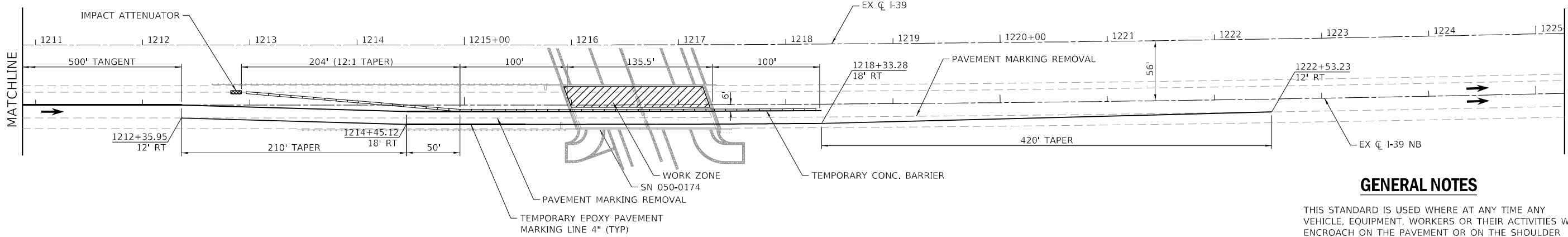
SCALE: 50 SHEET 1 OF 3 SHEETS STA. 1210+35.63 TO STA. 1222+53.23

| F.A.J. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|---------|--------------|-----------|
| 39 | 50(5A & 5B)BJR,BRR | LASALLE | 42 | 10 |
| CONTRACT NO. 66L74 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



NOTES:
 TRAFFIC CONTROL AND PROTECTION IS SHOWN FOR THE NORTHBOUND LANES,
 WHEN TRAFFIC CONTROL AND PROTECTION IS REQUIRED IN THE SOUTHBOUND LANES,
 THE TRAFFIC CONTROL AND PROTECTION SHALL BE SIMILAR THROUGH A 180° ROTATION
 ABOUT THE CENTERLINE OF F.A.I. 39

OFFSETS ARE MEASURED FROM EXISTING CENTERLINE OF NB OR SB PAVEMENT
 OFFSETS TO THE TEMPORARY CONCRETE BARRIER ARE MEASURED TO THE TRAFFIC FACE OF BARRIER



GENERAL NOTES

THIS STANDARD IS USED WHERE AT ANY TIME ANY VEHICLE, EQUIPMENT, WORKERS OR THEIR ACTIVITIES WILL ENCROACH ON THE PAVEMENT OR ON THE SHOULDER WITHIN 24" OF THE EDGE OF PAVEMENT FOR DAYLIGHT OPERATION EXCEEDING ONE DAY AND WHERE TEMPORARY CONCRETE BARRIER IS UTILIZED.

THIS DETAIL MUST ALWAYS BE USED IN COMBINATION WITH STANDARD 701400 AND 701402.

TEMPORARY CONCRETE BARRIER SHALL BE ACCORDING TO STANDARD 704001.

LEGEND

| | |
|--|----------------------------|
| | WORK ZONE |
| | TEMPORARY CONCRETE BARRIER |
| | IMPACT ATTENUATOR |

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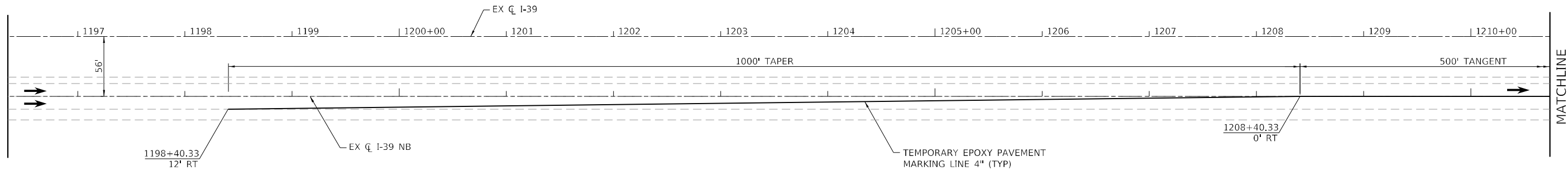
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| PLOT DATE = 1/21/2022 | DATE - 01/21/2022 | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC
 STAGE I**

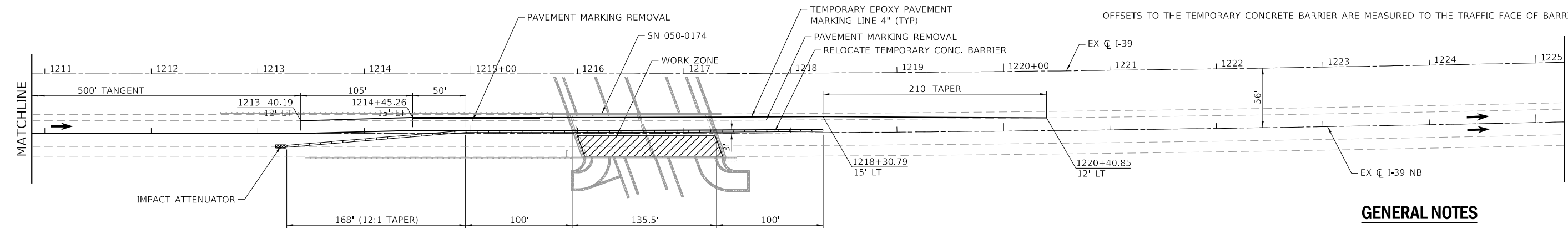
SCALE: 50 SHEET 2 OF 3 SHEETS STA. 1210+35.63 TO STA. 1222+53.23

| F.A.J. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|---------|--------------|-----------|
| 39 | 50(5A & 58)BJR,BRR | LASALLE | 42 | 11 |
| CONTRACT NO. 66L74 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



NOTES:
 TRAFFIC CONTROL AND PROTECTION IS SHOWN FOR THE NORTHBOUND LANES,
 WHEN TRAFFIC CONTROL AND PROTECTION IS REQUIRED IN THE SOUTHBOUND LANES,
 THE TRAFFIC CONTROL AND PROTECTION SHALL BE SIMILAR THROUGH A 180° ROTATION
 ABOUT THE CENTERLINE OF F.A.I. 39

OFFSETS ARE MEASURED FROM EXISTING CENTERLINE OF NB OR SB PAVEMENT
 OFFSETS TO THE TEMPORARY CONCRETE BARRIER ARE MEASURED TO THE TRAFFIC FACE OF BARRIER



GENERAL NOTES

THIS STANDARD IS USED WHERE AT ANY TIME ANY VEHICLE, EQUIPMENT, WORKERS OR THEIR ACTIVITIES WILL ENCROACH ON THE PAVEMENT OR ON THE SHOULDER WITHIN 24" OF THE EDGE OF PAVEMENT FOR DAYLIGHT OPERATION EXCEEDING ONE DAY AND WHERE TEMPORARY CONCRETE BARRIER IS UTILIZED.

THIS DETAIL MUST ALWAYS BE USED IN COMBINATION WITH STANDARD 701400.

TEMPORARY CONCRETE BARRIER SHALL BE ACCORDING TO STANDARD 704001.

LEGEND

- WORK ZONE
- TEMPORARY CONCRETE BARRIER
- IMPACT ATTENUATOR

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 PLOT DATE: 1/21/2022



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| PLOT DATE = 1/21/2022 | DATE - 01/21/2022 | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC
 STAGE II**

SCALE: 50 SHEET 3 OF 3 SHEETS STA. 1210+35.63 TO STA. 1222+53.23

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|---------|--------------|-----------|
| 39 | 50(5A & 5B)BJR,BRR | LASALLE | 42 | 12 |
| CONTRACT NO. 66L74 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

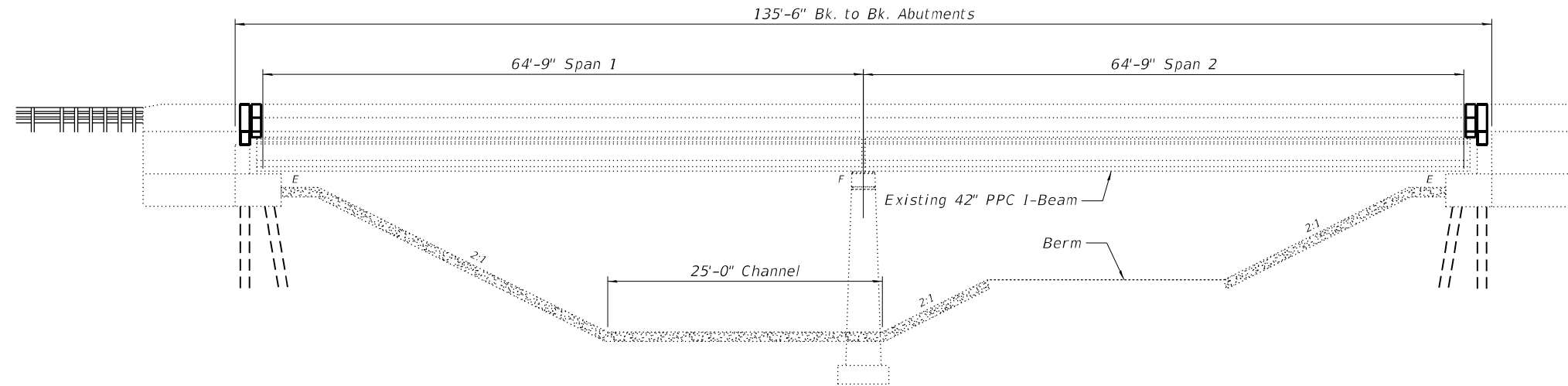
Existing Structure: S.N. 050-0174 (NB) was built in 1979. Structure consists of two-span continuous PPC I-Beams, skewed 20°00'00". Each span is 64'-9", and the back to back of abutments length is 135'-6". The out-to-out width of the superstructure is 42'-0". Abutments are reinforced concrete stub abutments supported by steel H-piles. The solid wall pier is supported on a spread footing in rock. In 2005 the bituminous concrete layer was removed and replaced with a microsilica overlay.

Traffic to be maintained using staged construction.

No salvage.

SCOPE OF WORK

1. Scarify existing microsilica overlay
2. Partial depth deck repair
3. Concrete repairs to parapets, abutments, and piers
4. PPC I Beam repairs
5. Remove and reconstruct expansion joints at north and south abutments and install new preformed joint strip seal
6. Place 3/4 inch Microsilica concrete overlay on bridge deck
7. Apply protective coat to bridge deck and approach slabs
8. Perform bridge deck grooving
9. Apply concrete sealer to substructure
10. Existing name plate to be removed, cleaned, and re-installed at the same location. Cost included with concrete removal.

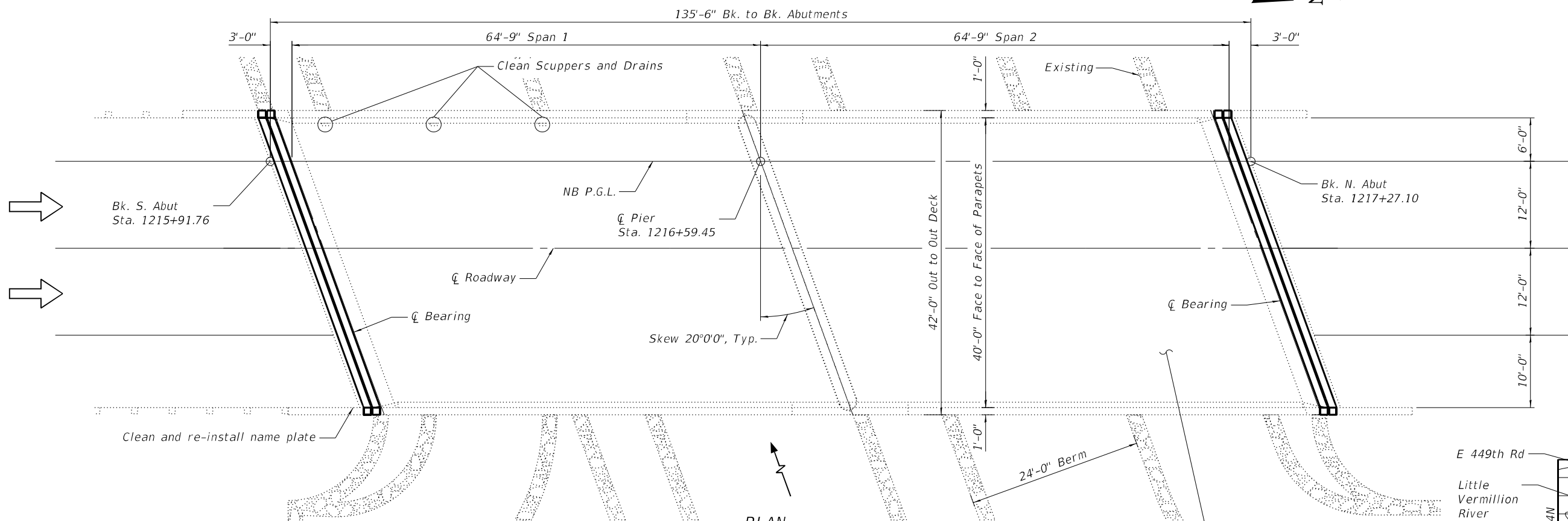


ELEVATION

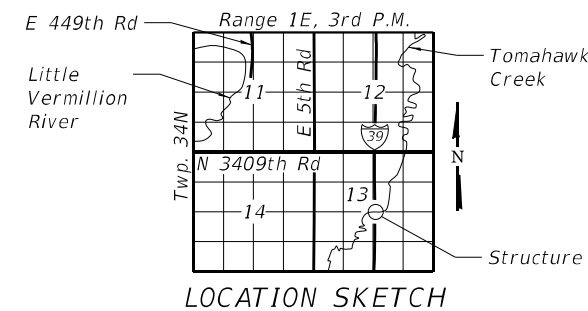
DESIGN STRESSES

FIELD UNITS

$f'_c = 3,500$ psi
 $f_y = 60,000$ psi (Reinforcement)



PLAN



LOCATION SKETCH

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| | CHECKED - DTS | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**GENERAL PLAN AND ELEVATION
 STRUCTURE NO. 050-0174**

SHEET S1 OF S15 SHEETS

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|--------------------|---------|--------------|-----------|
| 39 | 50(5A & 5B)BJR,BRR | LaSALLE | 42 | 13 |
| CONTRACT NO. 66L74 | | | | |

ILLINOIS FED. AID PROJECT

GENERAL NOTES

1. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
2. Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50° F.
3. Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
4. Reinforcement bars designated (E) shall be epoxy coated.
5. Bridge deck concrete sealer shall be applied to the new overlay, reconstructed transverse expansion joint concrete and inside faces and top faces of the parapets adjacent to the joints.
6. Concrete sealer shall be applied to all exposed concrete areas of the abutments and piers.
7. Prior to pouring the new concrete deck: all heavy or loose rust, loose mill scale, and other loose potentially detrimental foreign material shall be removed from the surface in contact with concrete. Removal shall be accomplished by methods that will not damage the PPC Beam and the cost will be included with the pay item covering removal of the existing concrete.
8. The Contractor shall use extreme care during concrete removal operations so the existing PPC beams are not damaged. If the beams are damaged due to the Contractor's operations, they shall be repaired to the satisfaction of the Engineer at no expense to the Department.
9. Removal of the existing expansion joint steel, anchor studs, expansion material shall not be paid for separately but shall be included in the cost of Concrete Removal.
10. All exposed concrete edges shall have a 3/4" chamfer, except where shown otherwise.
11. Expansion joint shall be fabricated to conform to the existing cross slopes of the bridge.
12. The full depth of the existing microsilica overlay shall be completely removed as part of the scarification. None of the existing microsilica overlay shall be allowed to remain in place. The scarification shall not expose the top mat of deck reinforcement. In the event that the top mat is exposed, the Contractor shall contact the Engineer.
13. Acrylic coating shall be placed over Fiber Wrap repairs. Two coats shall be applied.

INDEX OF SHEETS

- S1. General Plan and Elevation
- S2. General Notes, Total Bill of Material and Index of Sheets
- S3. Stage Construction Details
- S4. Temporary Concrete Barrier for Staged Construction
- S5. Deck Removal
- S6. Deck Repairs
- S7. Deck Reconstruction Plan
- S8. Deck Reconstruction Details
- S9. Parapet Repairs
- S10. Prefomed Joint Strip Seal Details
- S11. PPC Beam Repair Details 1
- S12. PPC Beam Repair Details 2
- S13. Abutment Repair Details
- S14. Pier Repair Details
- S15. Bar Splicer Assembly and Mechanical Splicer Details

TOTAL BILL OF MATERIAL

| ITEM | UNIT | SUPER | SUB | TOTAL |
|--|---------|-------|-------|-------|
| Concrete Removal | Cu. Yd. | 8.1 | - | 8.1 |
| Concrete Superstructure | Cu. Yd. | 8.1 | - | 8.1 |
| Reinforcement Bars, Epoxy Coated | Pound | 1,380 | - | 1,380 |
| Bar Splicers | Each | 12 | - | 12 |
| Prefomed Joint Strip Seal | Foot | 88.5 | - | 88.5 |
| Concrete Sealer | Sq. Ft. | - | 2,576 | 2,576 |
| Epoxy Crack Injection | Foot | - | 152 | 152 |
| Cleaning Bridge Scuppers and Downspouts | Each | 3 | - | 3 |
| Acrylic Coating | Sq. Yd. | 41 | - | 41 |
| Fiber Wrap | Sq. Ft. | 366 | - | 366 |
| Bridge Deck Grooving (Longitudinal) | Sq. Yd. | 352 | - | 352 |
| Bridge Deck Concrete Sealer | Sq. Ft. | 5,259 | - | 5,259 |
| Bridge Deck Scarification 3" | Sq. Yd. | 556 | - | 556 |
| Bridge Deck Microsilica Concrete Overlay, 3 1/4" | Sq. Yd. | 556 | - | 556 |
| Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches) | Sq. Ft. | 88 | 44 | 132 |
| * Structural Repair of Concrete (Depth Greater Than 5 Inches) | Sq. Ft. | 9 | - | 9 |
| Deck Slab Repair (Full Depth, Type I) | Sq. Yd. | 0.4 | - | 0.4 |
| Deck Slab Repair (Full Depth, Type II) | Sq. Yd. | 1.2 | - | 1.2 |
| Diamond Grinding (Bridge Section) | Sq. Yd. | 520 | - | 520 |
| Precast Prestressed Concrete I-Beam Repair | Sq. Ft. | 47 | - | 47 |

* Quantity includes a contingency (above the amount shown in the bill of material) to account for uncertainties associated with the condition of the existing structure. Actual repair areas will be determined by the Engineer in the field.

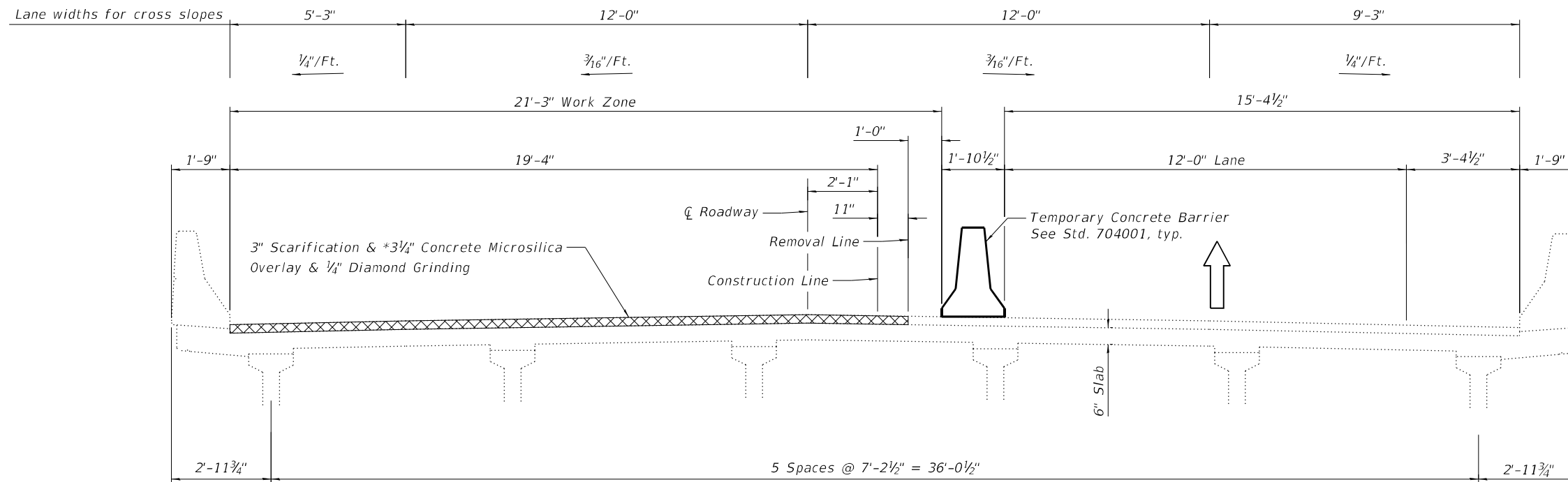
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| | PLOT DATE = 01/21/2022 | CHECKED - DTS | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| | |
|--|---------------|
| GENERAL NOTES, TOTAL BILL OF MATERIAL AND INDEX OF SHEETS | |
| STRUCTURE NO. 050-0174 | |
| SHEET S2 | OF S15 SHEETS |

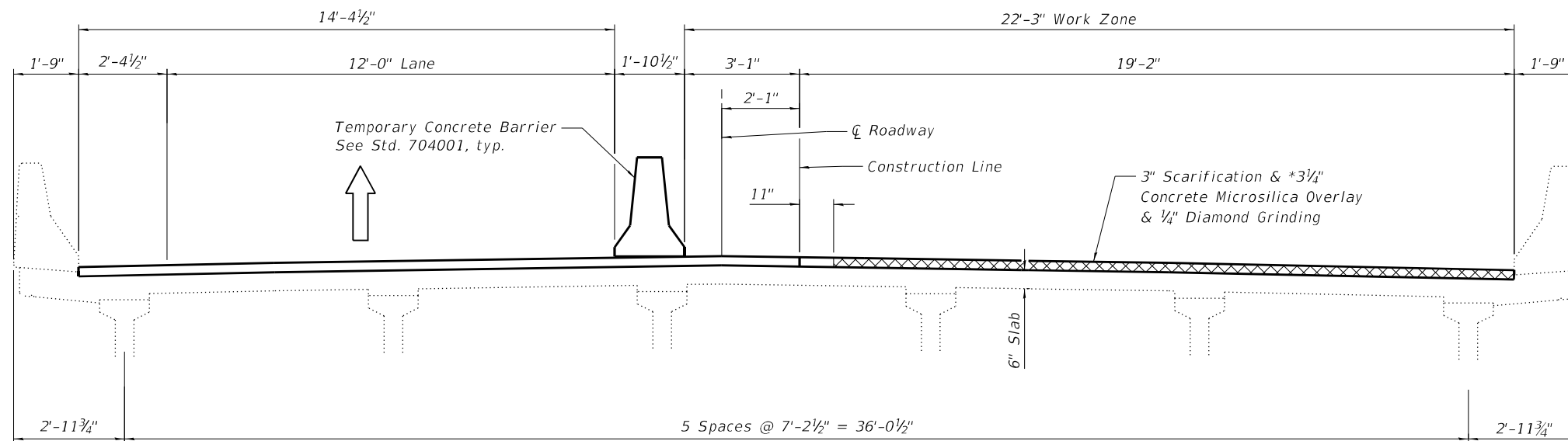
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| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 39 | 50(5A & 5B)BJR,BRR | LaSALLE | 42 | 14 |
| CONTRACT NO. 66L74 | | | | |
| | | ILLINOIS | FED. AID PROJECT | |



STAGE I CONSTRUCTION AND REMOVAL

(Looking North)

*Prior to grinding



STAGE II CONSTRUCTION AND REMOVAL

(Looking North)

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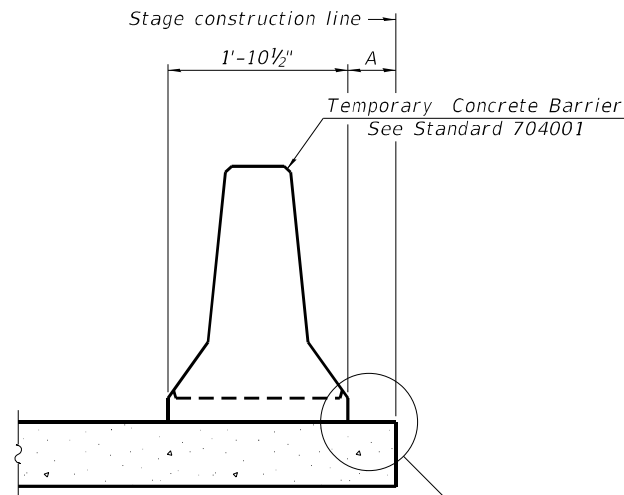
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| PLOT DATE = 01/21/2022 | CHECKED - DTS | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGE CONSTRUCTION DETAILS
STRUCTURE NO. 050-0174

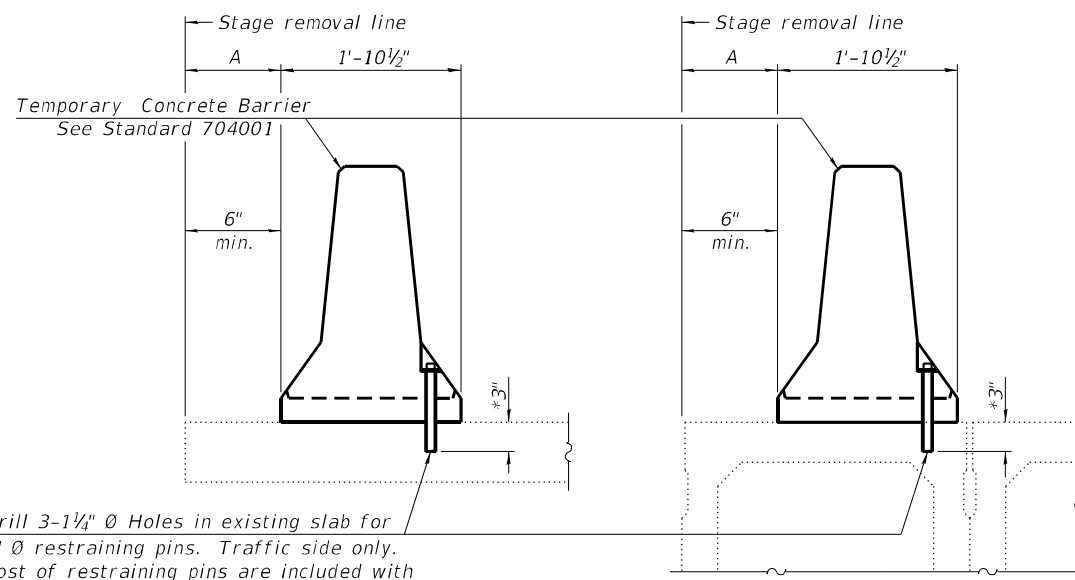
SHEET S3 OF S15 SHEETS

| FAI RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|---------|--------------|-----------|
| 39 | 50(5A & 5B)BJR,BRR | LaSALLE | 42 | 15 |
| CONTRACT NO. 66L74 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

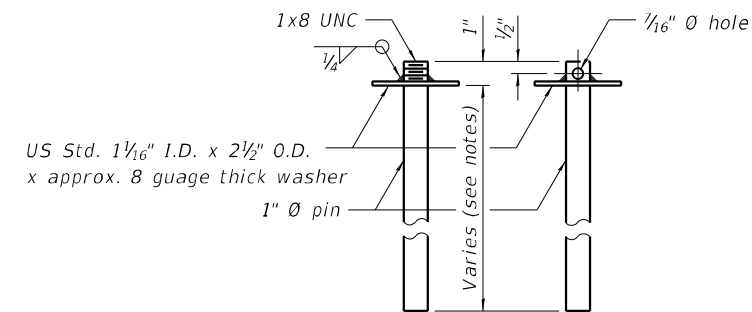
NEW SLAB OR NEW DECK BEAM OR NEW OVERLAY



Drill 3-1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

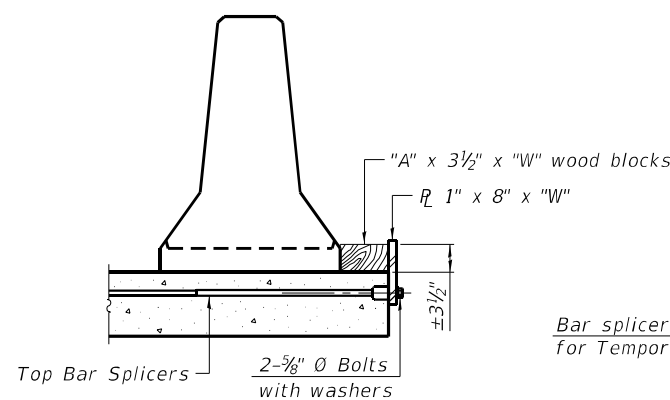
EXISTING DECK BEAM



RESTRAINING PIN

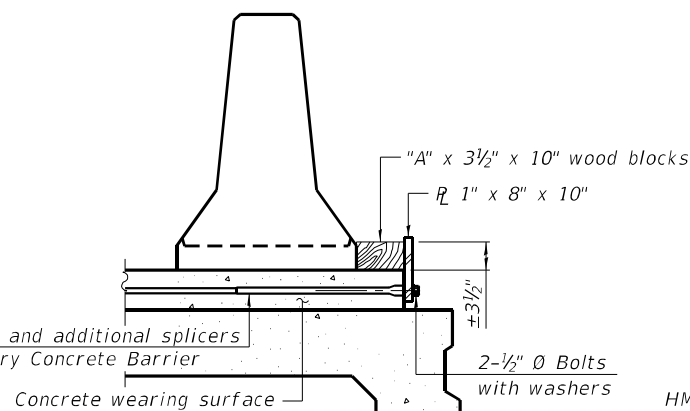
* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

SECTIONS THRU SLAB OR DECK BEAM

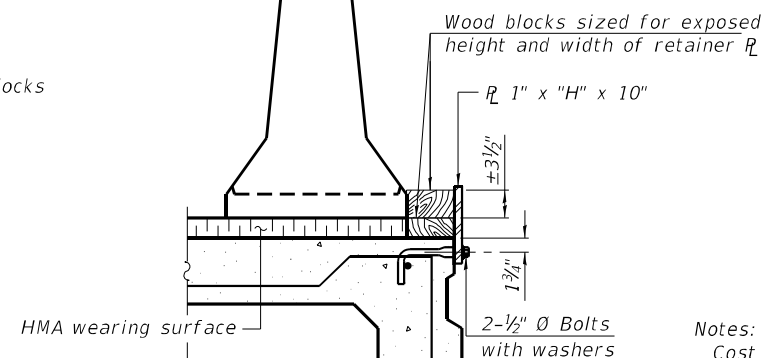


DETAIL I

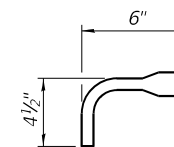
Bar splicers and additional splicers for Temporary Concrete Barrier



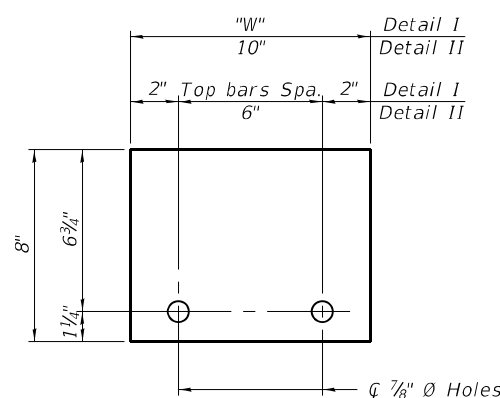
DETAIL II



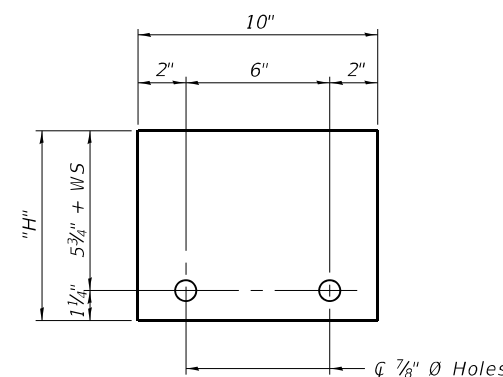
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER R 1" x 8" x "W"
(Detail I and II)



STEEL RETAINER R 1" x "H" x 10"
(Detail III)

Notes:
 Cost of retainer assembly is included with Temporary Concrete Barrier.
 A retainer assembly shall be located at the approximate \bar{C} of each temporary concrete barrier.
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.
 When the 'A' dimension is less than 1 1/2', the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.
 Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.
 Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

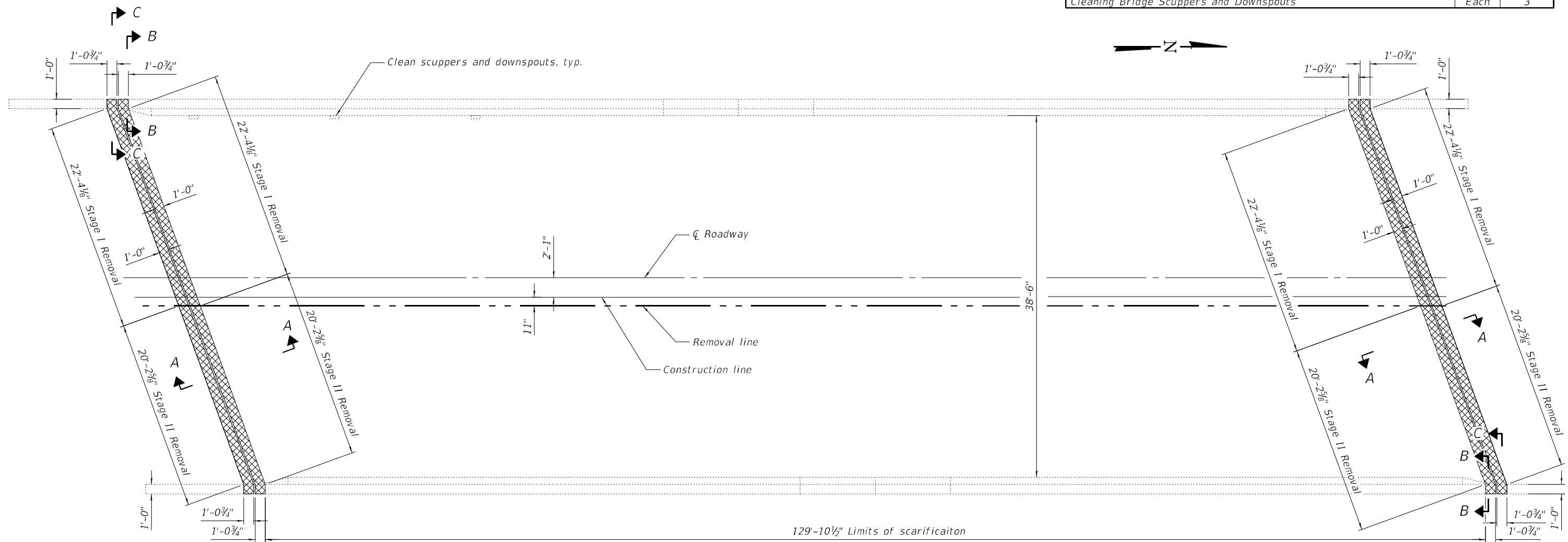
TEMPORARY CONCRETE BARRIER FOR STAGED CONSTRUCTION
 STRUCTURE NO. 050-0174

SHEET S4 OF S15 SHEETS

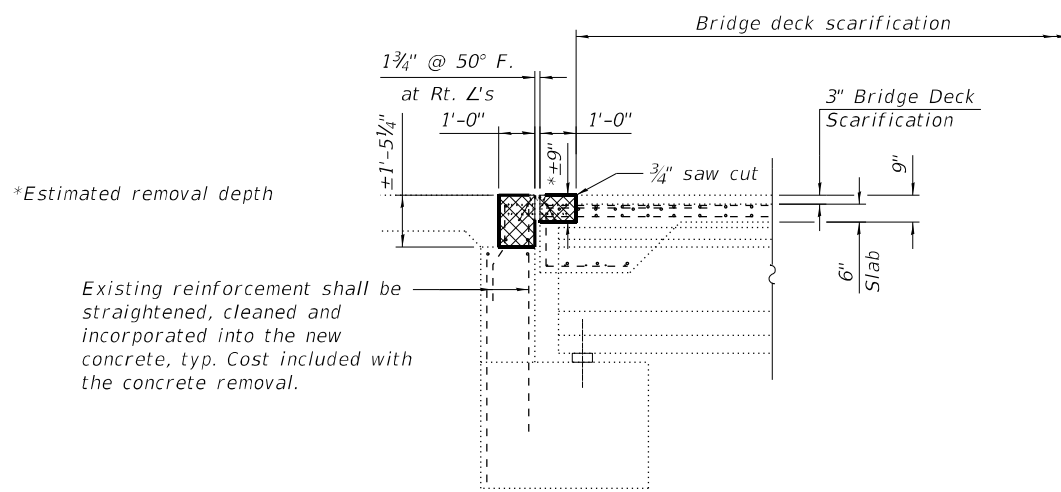
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| FAI RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 39 | 50(5A & 5B)BJR,BRR | LaSALLE | 42 | 16 |
| CONTRACT NO. 66L74 | | | | |
| ILLINOIS | | FED. AID PROJECT | | |

BILL OF MATERIAL

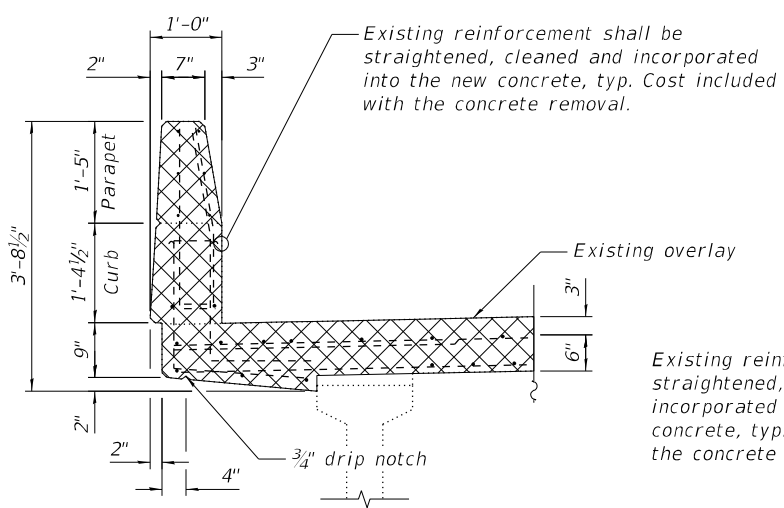
| Item Description | Unit | Quantity |
|---|---------|----------|
| Concrete Removal | Cu. Yd. | 8.1 |
| Bridge Deck Scarification 3" | Sq. Yd. | 556 |
| Cleaning Bridge Scuppers and Downspouts | Each | 3 |



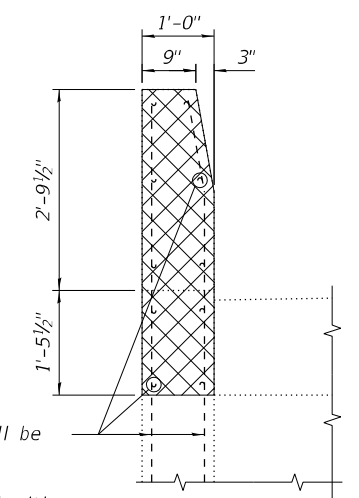
PLAN



SECTION A-A
(At Rt L's)



SECTION B-B



SECTION C-C

*Estimated removal depth

Existing reinforcement shall be straightened, cleaned and incorporated into the new concrete, typ. Cost included with the concrete removal.

LEGEND



MODEL: Default
FILE NAME: V:\1736\active\173630088\structural\drawing\sheet\0500174-66174-005-Deck-01.dgn

| | | | |
|--|------------------------|----------------|-----------|
| | USER NAME = | DESIGNED - CRS | REVISED - |
| | PLOT SCALE = N/A | CHECKED - DTS | REVISED - |
| | PLOT DATE = 01/21/2022 | DRAWN - CRS | REVISED - |
| | | CHECKED - DTS | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DECK REMOVAL
STRUCTURE NO. 050-0174

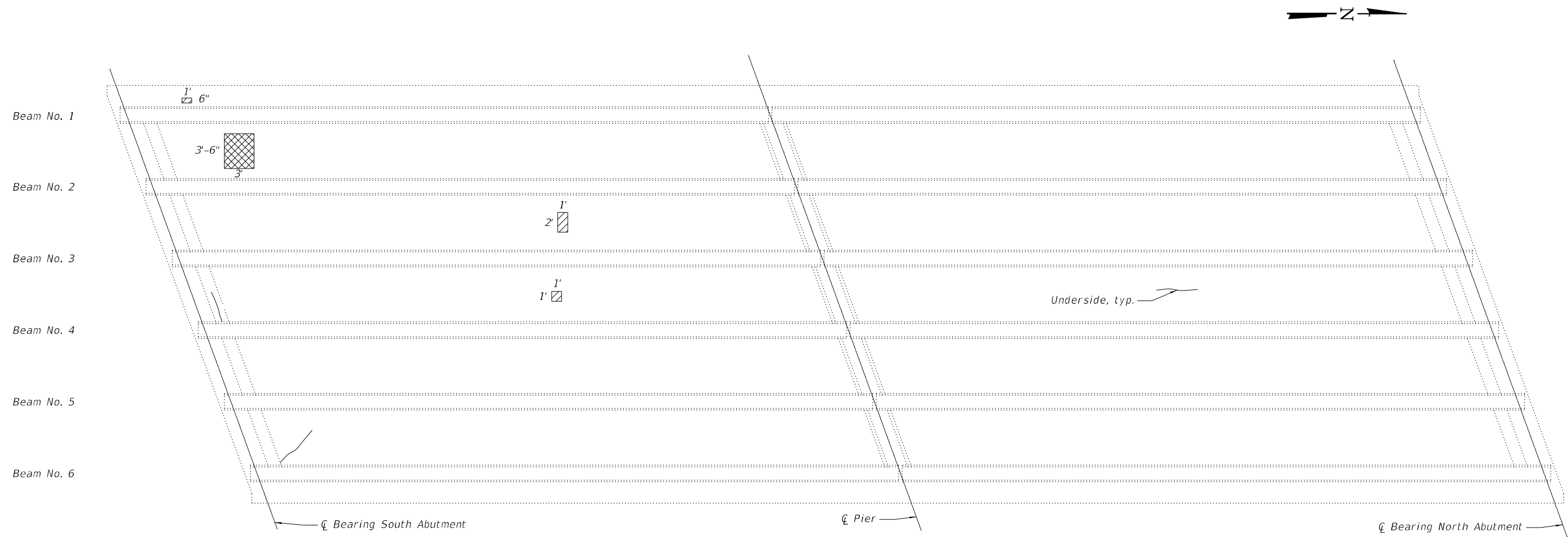
SHEET S5 OF S15 SHEETS

| | | | | |
|--------------------|----------------------------|----------------|-----------------|--------------|
| F.A.I. RTE. 39 | SECTION 50(5A & 5B)BJR,BRR | COUNTY LaSALLE | TOTAL SHEETS 42 | SHEET NO. 17 |
| CONTRACT NO. 66L74 | | | | |

ILLINOIS FED. AID PROJECT



BILL OF MATERIAL

| Item Description | Unit | Quantity |
|--|---------|----------|
| Deck Slab Repair (Full Depth, Type I) | Sq. Yd. | 0.4 |
| Deck Slab Repair (Full Depth, Type II) | Sq. Yd. | 1.2 |



PLAN

LEGEND

-  Deck Slab Repair (Full Depth, Type I)
-  Deck Slab Repair (Full Depth, Type II)

NOTES:

- Deck repairs areas are estimated based on visual inspection and will be paid for as specified in the Special Provisions. Actual repair areas and locations shall be determined by the Engineer and shown on As-built plans. Engineer shall sound deck after scarification.

MODEL: Default
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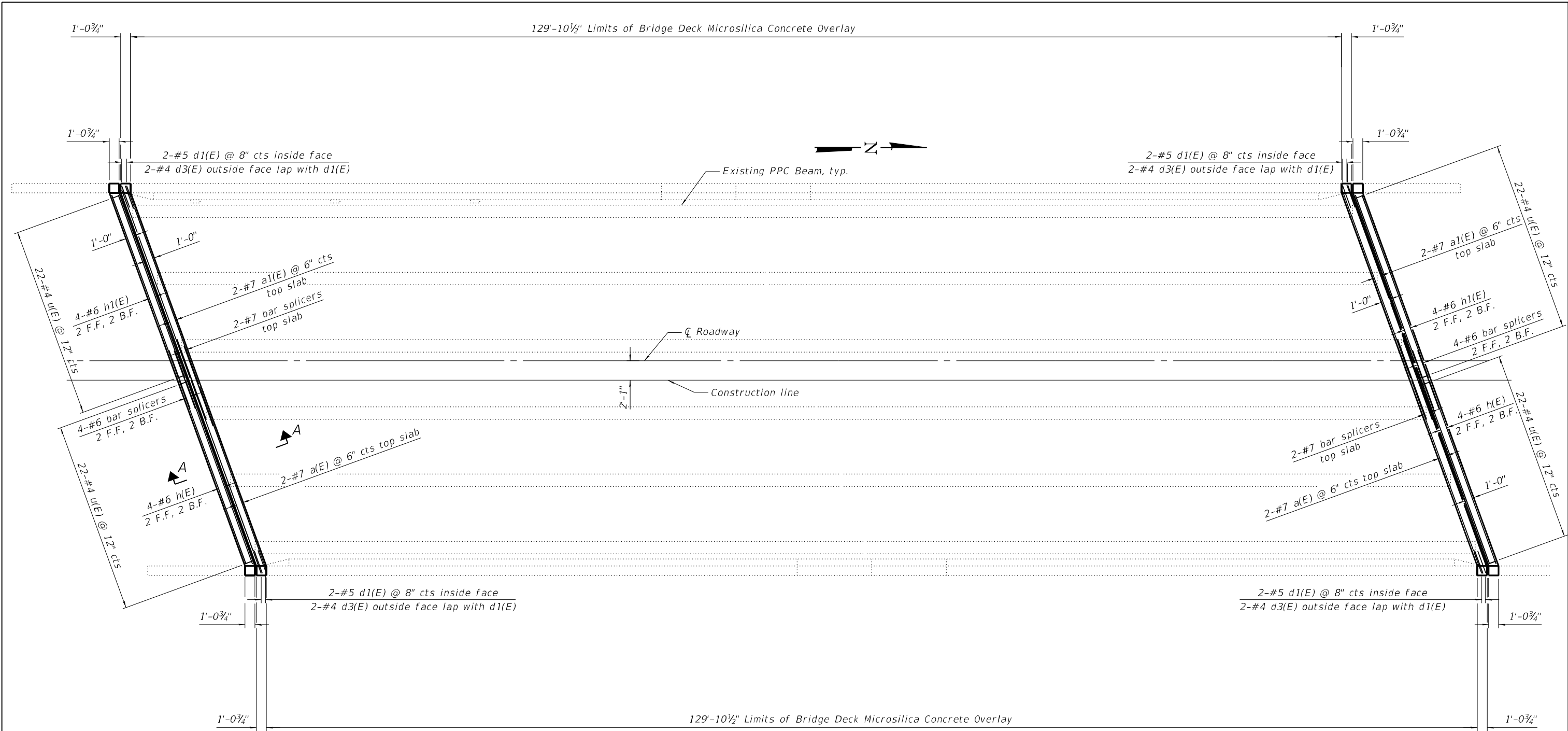
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|------------------------|----------------|-----------|
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| PLOT SCALE = N/A | DRAWN - CRS | REVISED - |
| PLOT DATE = 01/21/2022 | CHECKED - DTS | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DECK REPAIRS
STRUCTURE NO. 050-0174**

SHEET S6 OF S15 SHEETS

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|--------------------|----------|------------------|-----------|
| 39 | 50(5A & 5B)BJR,BRR | LaSALLE | 42 | 18 |
| CONTRACT NO. 66L74 | | | | |
| | | ILLINOIS | FED. AID PROJECT | |



PLAN

NOTES:

1. See sheet S8 for section A-A.
2. For bridge deck final cross section see sheet S3.
3. For North and South Transverse Joint Removal and Reconstruction, see sheets S5 and S8.

MODEL: Default
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|------------------------|----------------|-----------|
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| CHECKED - DTS | REVISED - | |
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| PLOT DATE = 01/21/2022 | CHECKED - DTS | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

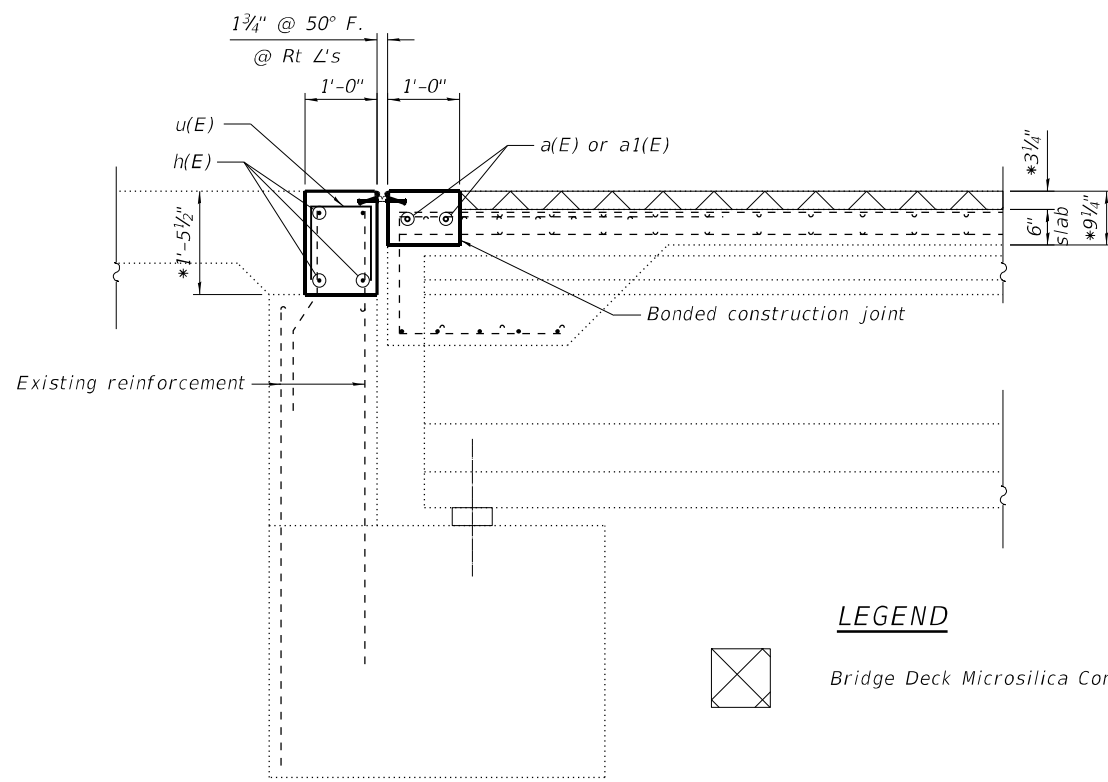
DECK RECONSTRUCTION PLAN
STRUCTURE NO. 050-0174

SHEET S7 OF S15 SHEETS

| | | | | |
|--------------------|--------------------|----------|------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 39 | 50(5A & 5B)BJR,BRR | LaSALLE | 42 | 19 |
| CONTRACT NO. 66L74 | | | | |
| | | ILLINOIS | FED. AID PROJECT | |

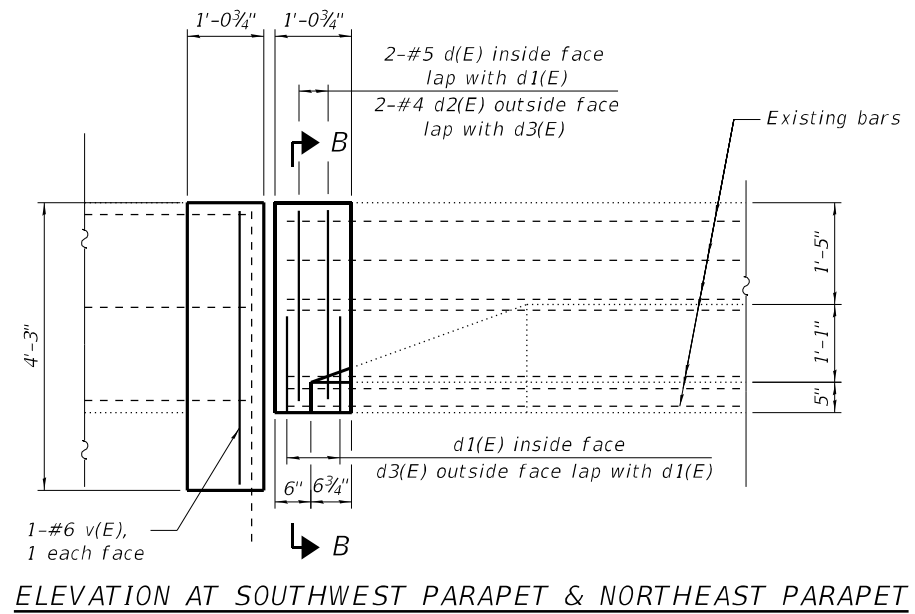
**SUPERSTRUCTURE
BILL OF MATERIAL**

| Bar | No. | Size | Length | Shape |
|--|-----|---------|----------|-------|
| a(E) | 4 | #7 | 21'-11" | — |
| a1(E) | 4 | #7 | 22'-1" | — |
| d(E) | 8 | #5 | 3'-0" | ⌋ |
| d1(E) | 8 | #5 | 3'-3" | ⌋ |
| d2(E) | 8 | #4 | 3'-0" | ⌋ |
| d3(E) | 8 | #4 | 4'-2" | ⌋ |
| h(E) | 8 | #6 | 20'-10" | — |
| h1(E) | 8 | #6 | 21'-0" | — |
| u(E) | 88 | #4 | 2'-10" | ⌊ |
| v(E) | 8 | #6 | 3'-11" | — |
| Item | | Unit | Quantity | |
| Concrete Superstructure | | Cu. Yd. | 8.1 | |
| Bridge Deck Grooving (Longitudinal) | | Sq. Yd. | 352 | |
| Reinforcement Bars, Epoxy Coated | | Pound | 1,380 | |
| Bridge Deck Concrete Sealer | | Sq. Ft. | 5,259 | |
| Bridge Deck Microsilica Concrete Overlay, 3/4 Inches | | Sq. Yd. | 556 | |
| Diamond Grinding (Bridge Section) | | Sq. Yd. | 520 | |

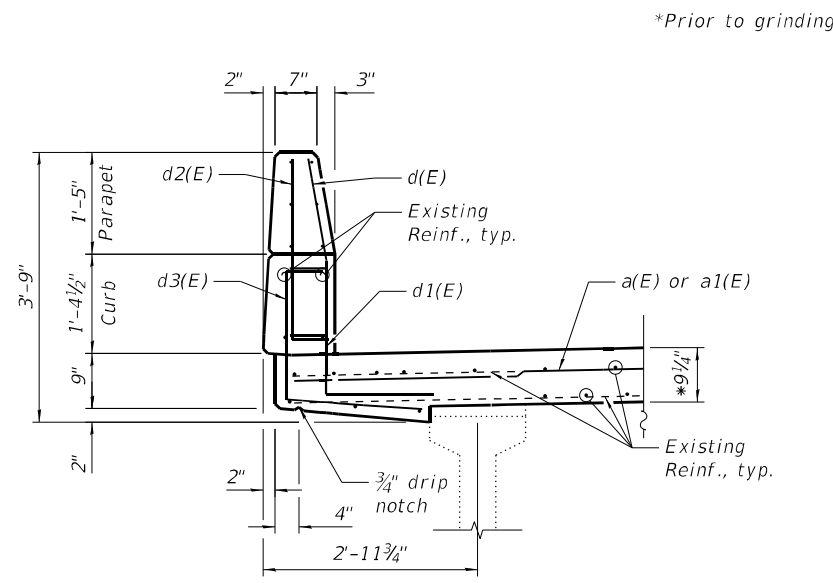


SECTION A-A

LEGEND
 Bridge Deck Microsilica Concrete Overlay

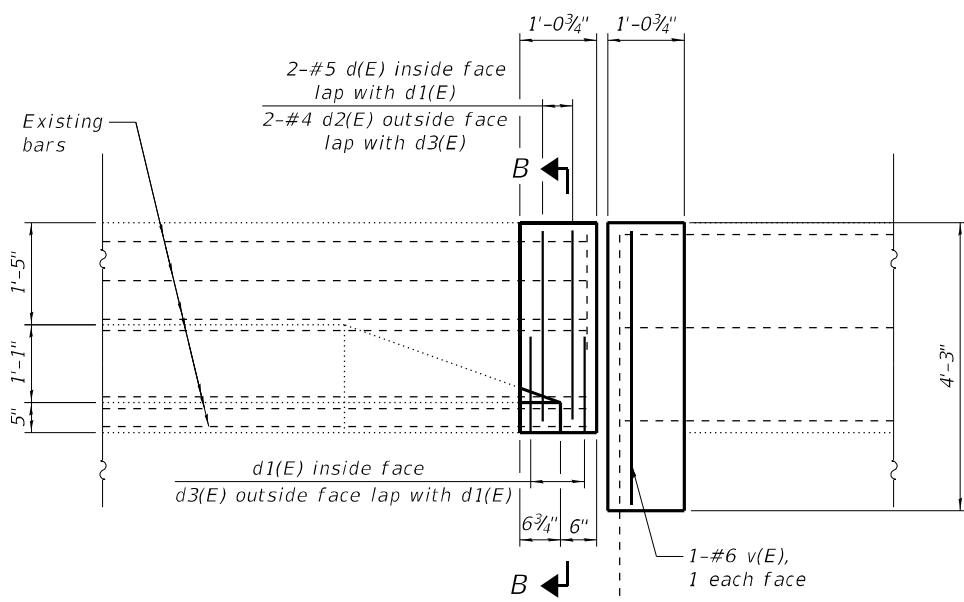


ELEVATION AT SOUTHWEST PARAPET & NORTHEAST PARAPET

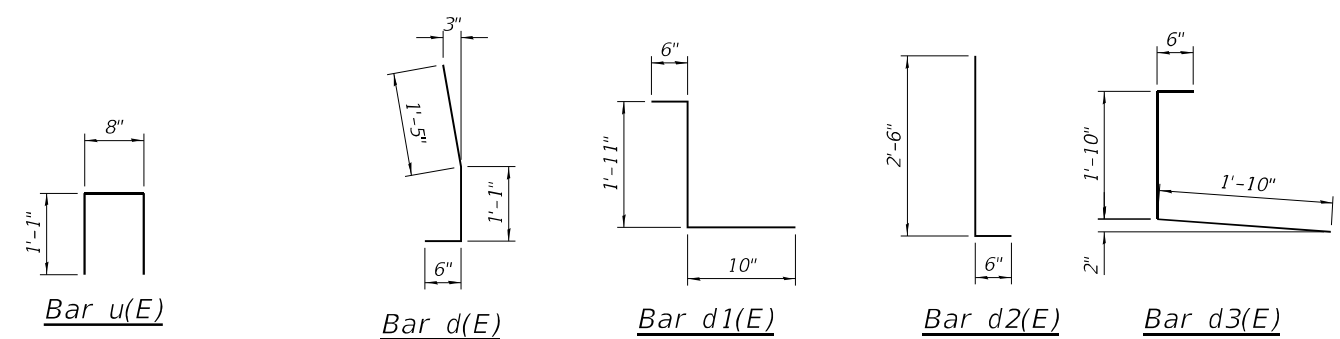


SECTION B-B

*Prior to grinding



ELEVATION AT NORTHWEST PARAPET & SOUTHEAST PARAPET



- NOTES:**
1. Space reinforcement to avoid studs in strip seal joint.
 2. Diamond Grinding (Bridge Section) within the limits of the bridge shall be measured for payment.

MODEL: Default
FILE NAME: V:\1736\active\173630088\structural\drawing\sheet\0500174-66174-008-Deck-04.dgn



| | | |
|------------------------|----------------|-----------|
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| PLOT SCALE = N/A | CHECKED - DTS | REVISED - |
| PLOT DATE = 01/21/2022 | DRAWN - CRS | REVISED - |
| | CHECKED - DTS | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DECK RECONSTRUCTION DETAILS
STRUCTURE NO. 050-0174**

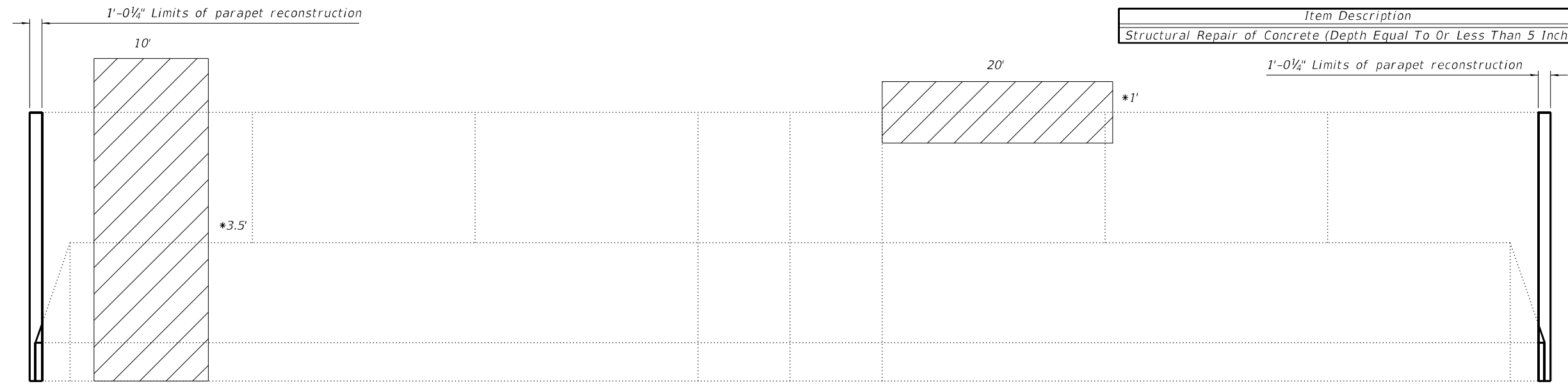
SHEET S8 OF S15 SHEETS

| | | | | |
|--------------------|--------------------|---------|--------------|-----------|
| F&I RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
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| CONTRACT NO. 66L74 | | | | |

ILLINOIS FED. AID PROJECT

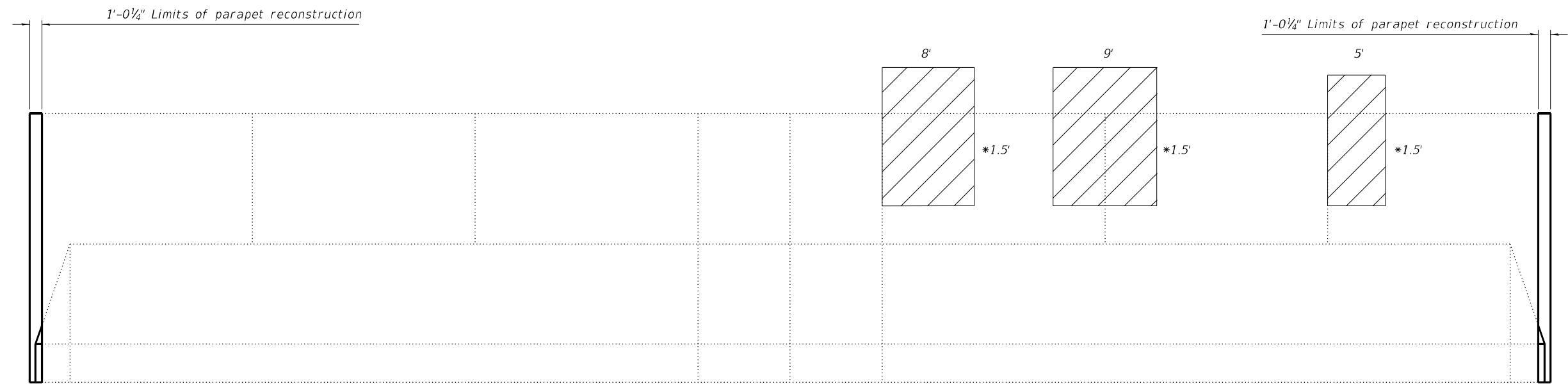
BILL OF MATERIAL

| Item Description | Unit | Quantity |
|--|---------|----------|
| Structural Repair of Concrete (Depth Equal To Or Less Than 5 Inches) | Sq. Ft. | 88 |




ELEVATION OF WEST PARAPET
(Looking West at West Parapet)

* Extends to top of parapet



ELEVATION OF EAST PARAPET
(Reflected View of East Parapet)

LEGEND

 Structural Repair of Concrete
(Depth Equal to or Less Than 5')

MODEL: Default
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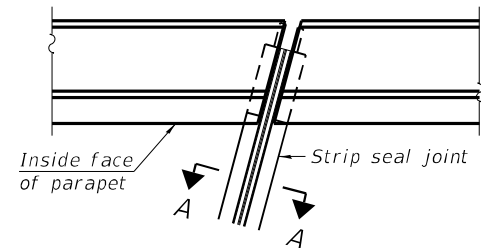
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| PLOT SCALE = N/A | DRAWN - CRS | REVISED - |
| PLOT DATE = 01/21/2022 | CHECKED - DTS | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

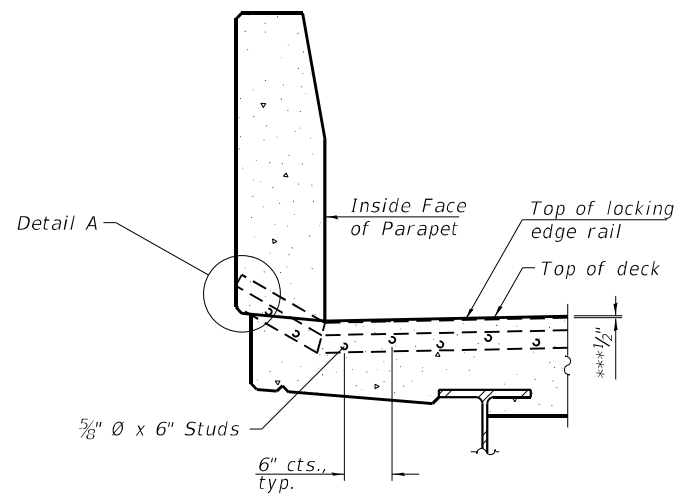
**PARAPET REPAIRS
STRUCTURE NO. 050-0174**

SHEET S9 OF S15 SHEETS

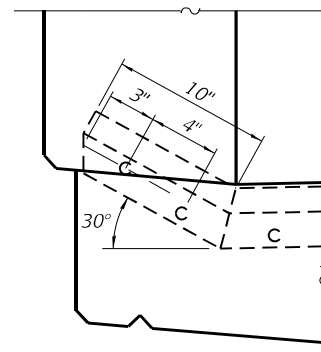
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|---------|--------------|-----------|
| 39 | 50(5A & 5B)BJR,BRR | LaSALLE | 42 | 21 |
| CONTRACT NO. 66L74 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



FOR SKEWS $\leq 30^\circ$
PLAN AT PARAPET

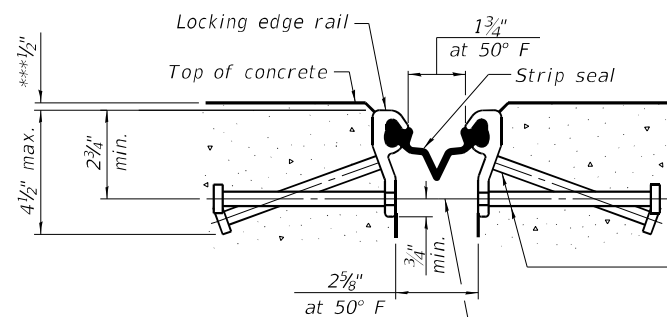


SECTION AT PARAPET



DETAIL A

*** Prior to grinding



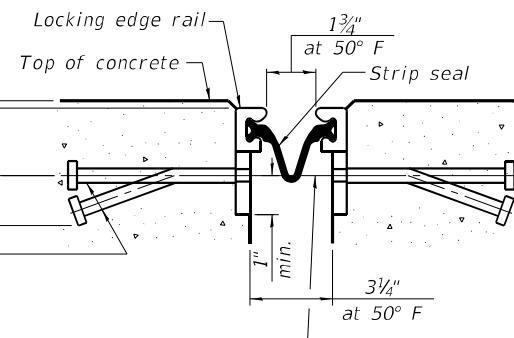
SHOWING ROLLED RAIL JOINT

* $\frac{5}{8}$ " \varnothing x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

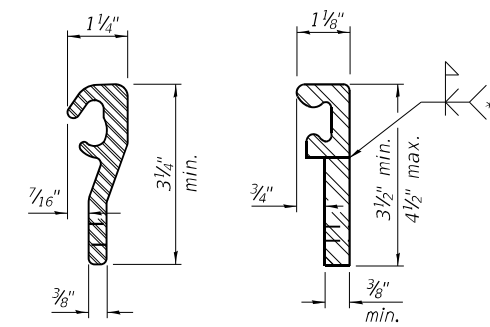
$\frac{3}{8}$ " \varnothing threaded rods in $\frac{1}{16}$ " \varnothing holes at ± 4 "-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

SECTION A-A

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



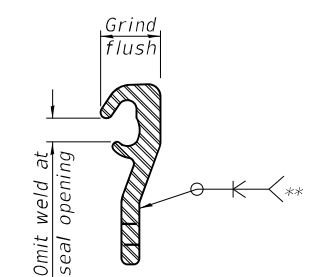
SHOWING WELDED RAIL JOINT



LOCKING EDGE RAILS
ROLLED (EXTRUDED) RAIL WELDED RAIL

LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

| Item | Unit | Total |
|----------------------------|------|-------|
| Preformed Joint Strip Seal | Foot | 88.5 |

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of $\frac{1}{4}$ ". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the $4\frac{1}{2}$ " maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.

The manufacturer's recommended installation methods shall be followed.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

The Maximum space between locking edge rail segments shall be $\frac{3}{16}$ " and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

Cost of parapet sliding plates, embedded plates, and anchorage studs included with Preformed Joint Strip Seal.

The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.

MODEL: Default
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| | | |
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| PLOT DATE = 01/21/2022 | DRAWN - CRS | REVISED - |
| | CHECKED - DTS | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL DETAILS
STRUCTURE NO. 050-0174



SHEET S10 OF S15 SHEETS

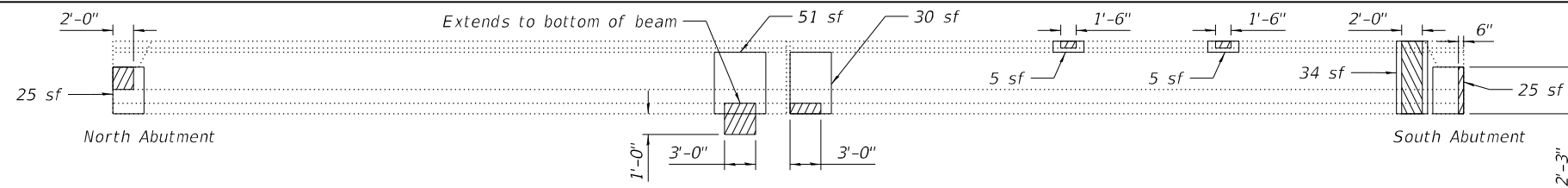
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| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 39 | 50(5A & 5B)BJR,BRR | LaSALLE | 42 | 22 |
| CONTRACT NO. 66L74 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

BILL OF MATERIAL

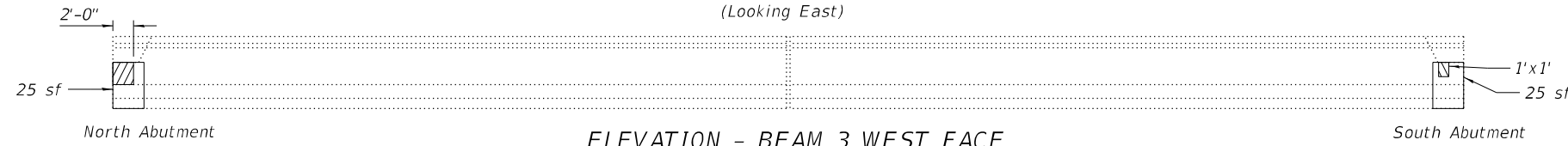
| Item Description | Unit | Quantity |
|--|---------|----------|
| Acrylic Coating | Sq. Yd. | 41 |
| Fiber Wrap | Sq. Ft. | 366 |
| Precast Prestressed Concrete I-Beam Repair | Sq. Ft. | 47 |

LEGEND

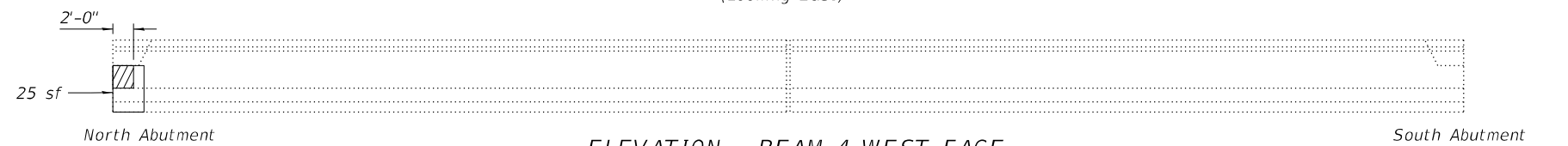
-  Precast Prestressed Concrete I-Beam Repair
-  Fiber Wrap Repair and Acrylic Coating



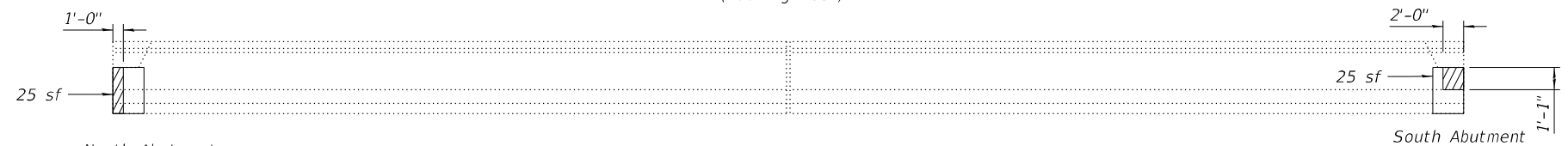
ELEVATION - BEAM 1 WEST FACE
(Looking East)



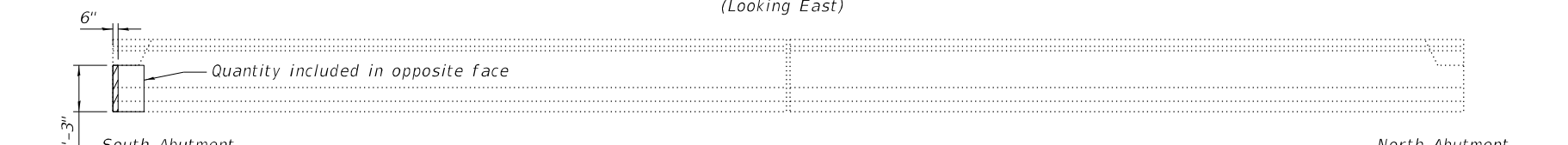
ELEVATION - BEAM 3 WEST FACE
(Looking East)



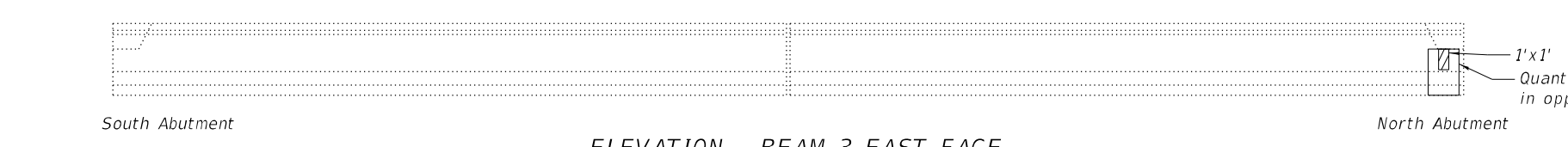
ELEVATION - BEAM 4 WEST FACE
(Looking East)



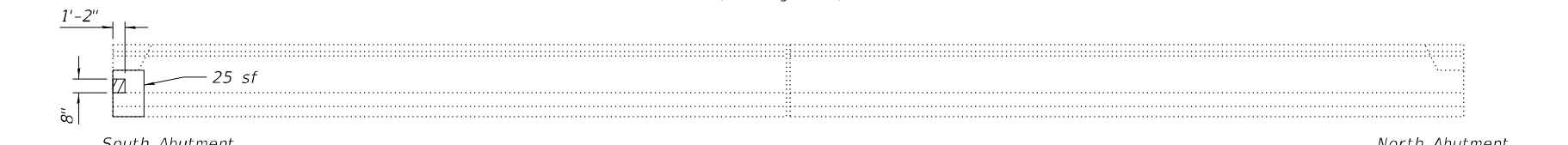
ELEVATION - BEAM 6 WEST FACE
(Looking East)



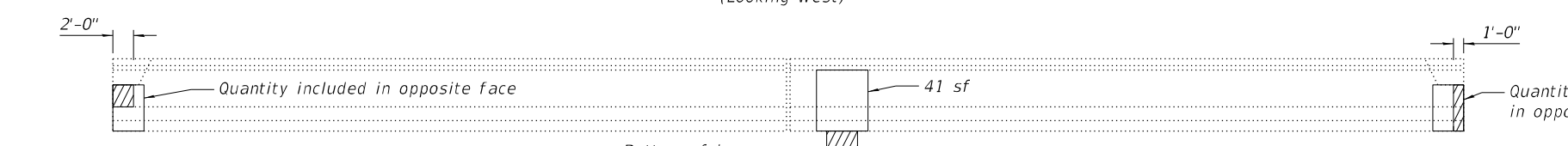
ELEVATION - BEAM 1 EAST FACE
(Looking West)



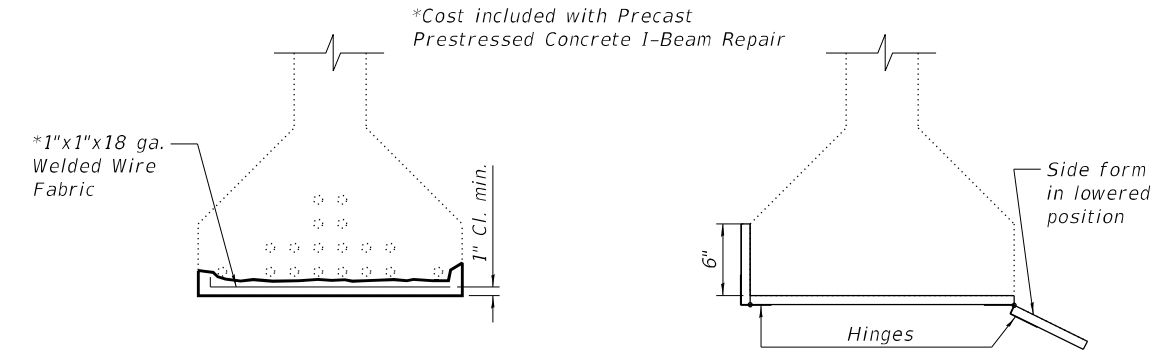
ELEVATION - BEAM 3 EAST FACE
(Looking West)



ELEVATION - BEAM 5 EAST FACE
(Looking West)

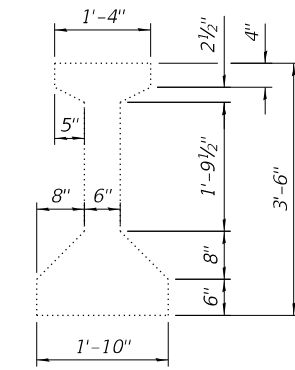


ELEVATION - BEAM 6 EAST FACE
(Looking West)



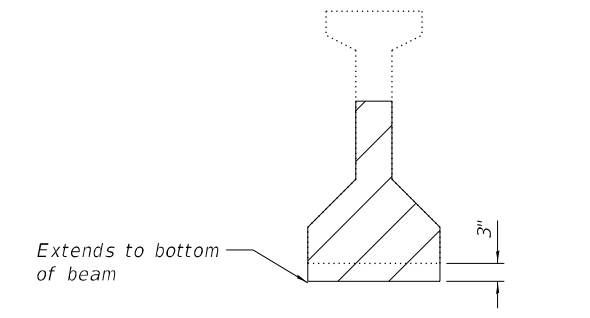
ESTIMATED REPAIR DETAIL AT BEAM 6
NORTH ABUTMENT

SUGGESTED FORM DETAIL

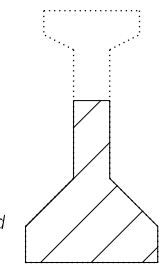


42" PPC BEAM

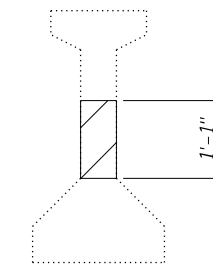
(Dimensions for reference when vertical dimension is omitted from elevation view)



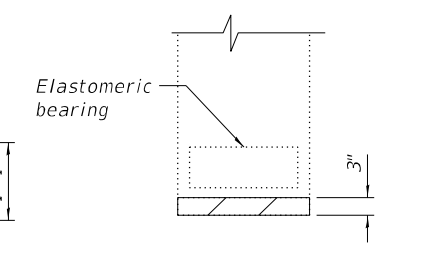
BEAM 6 NORTH ABUTMENT



BEAM 1 SOUTH ABUTMENT



BEAM 6 SOUTH ABUTMENT



PLAN - BOTTOM BEAM 6
NORTH ABUTMENT

MODEL: Default
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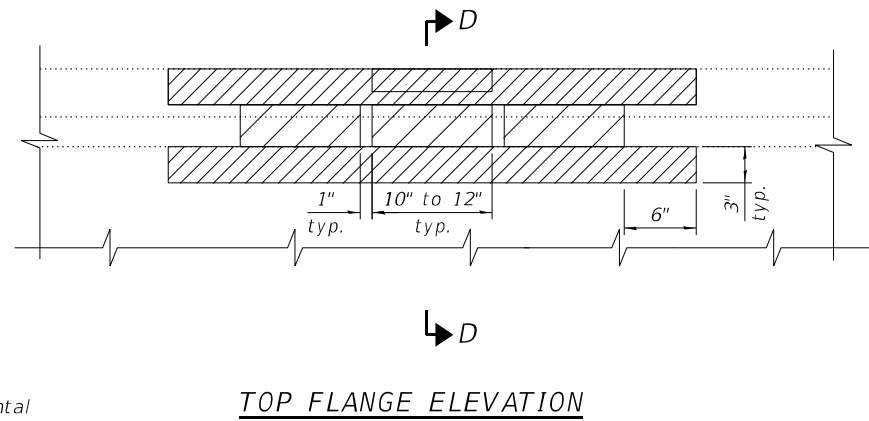
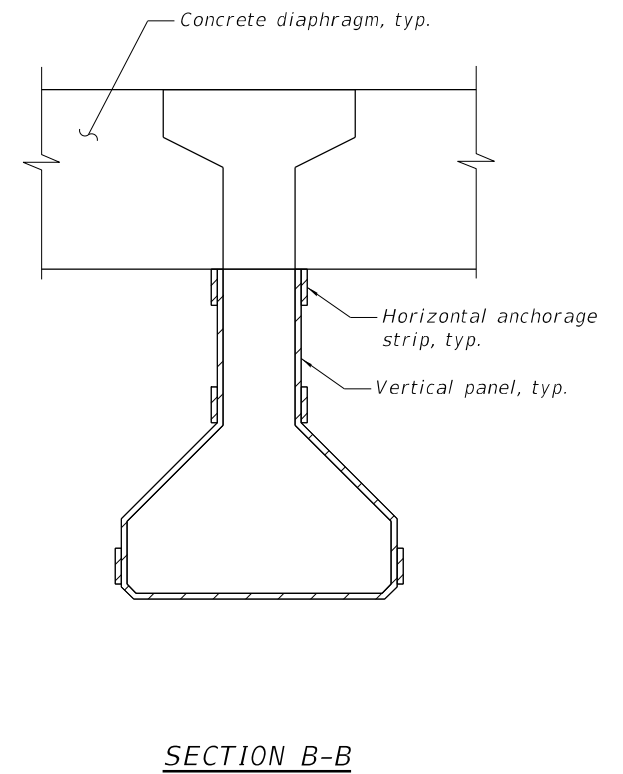
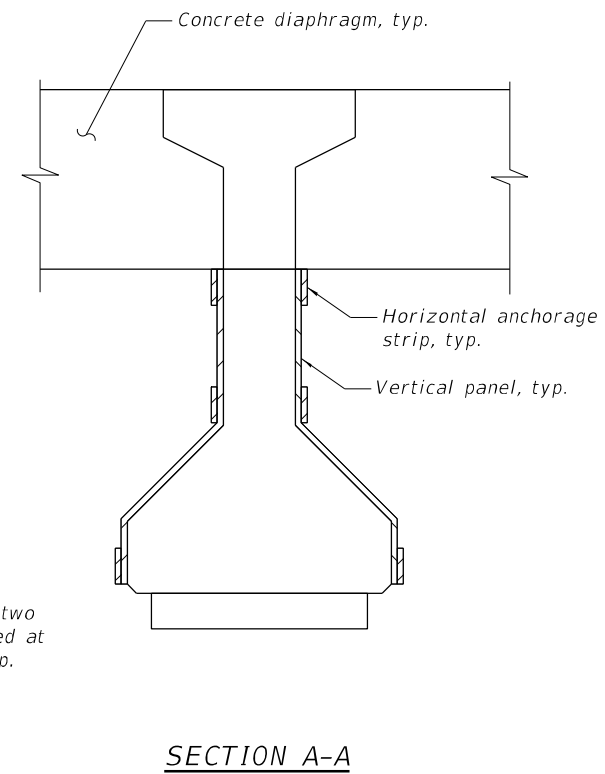
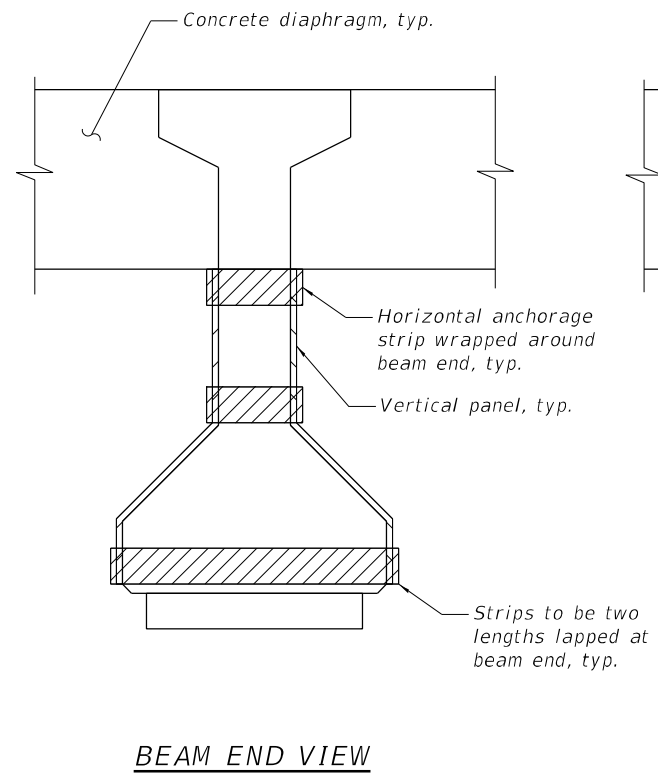
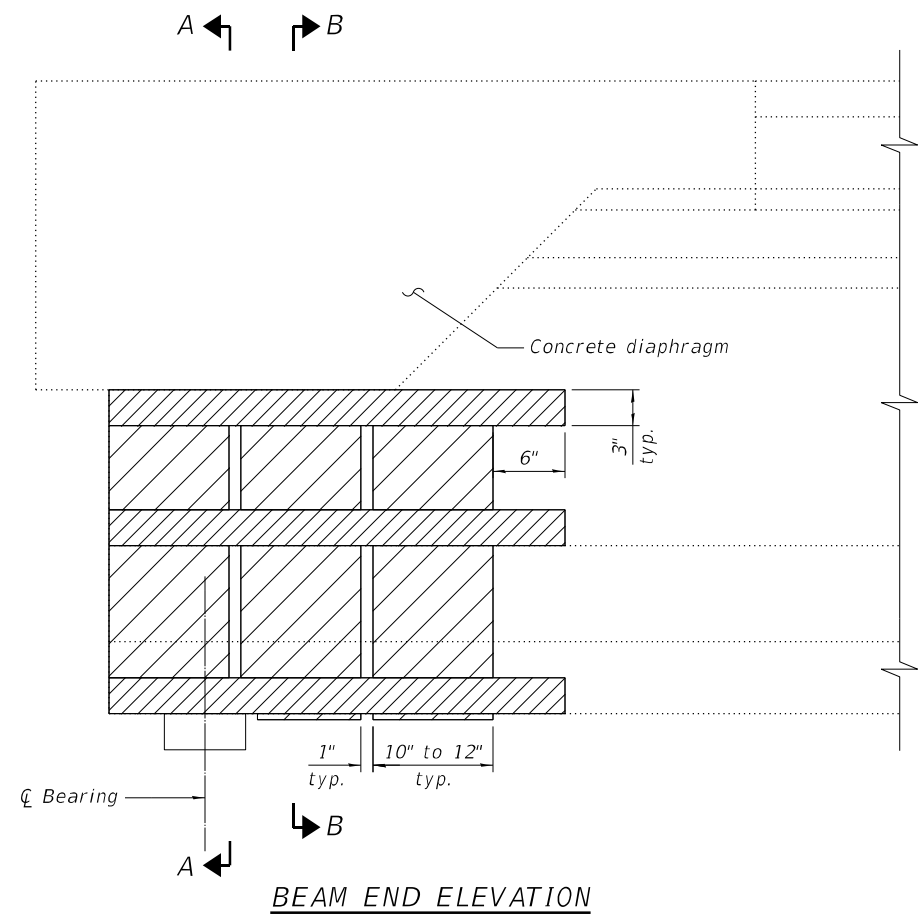
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| PLOT SCALE = N/A | CHECKED - DTS | REVISED - |
| PLOT DATE = 01/21/2022 | DRAWN - CRS | REVISED - |
| | CHECKED - DTS | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PPC BEAM REPAIR DETAILS - 1
STRUCTURE NO. 050-0174

SHEET S11 OF S15 SHEETS

| | | | | |
|---------------------------|--------------------|---------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 39 | 50(5A & 5B)BJR,BRR | LaSALLE | 42 | 23 |
| CONTRACT NO. 66L74 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

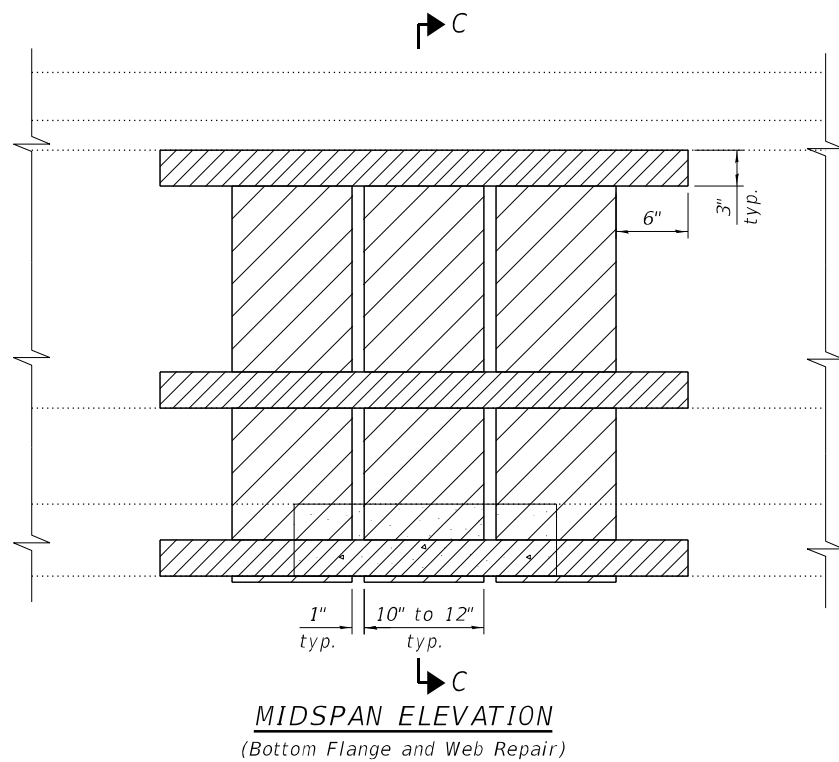
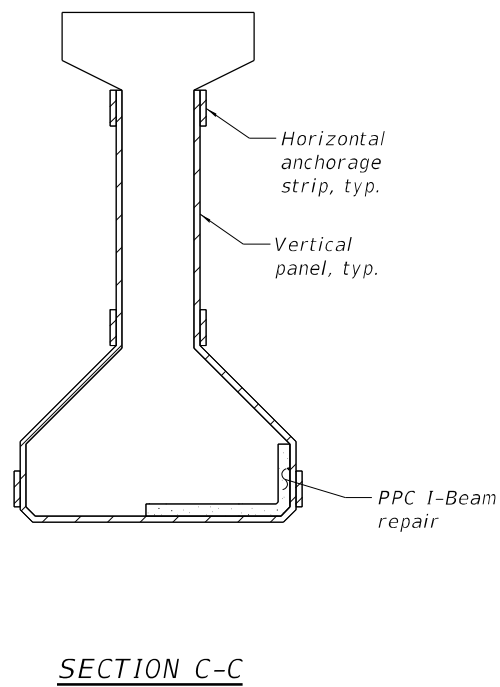


LEGEND

- Horizontal anchorage strip
- Vertical panel
- PPC I-Beam repair

NOTES:

1. Vertical panels must be between 10" and 12". The space between each vertical panel shall be 1". Vertical panels shall be extended beyond the repair zone by a minimum of 3". Vertical panels located above the bearing location shall be placed in two pieces as shown in Section A-A. At locations in front of the bearing, the vertical panels shall be one continuous strip wrapping beneath of the bottom flanges as shown in Section B-B.
2. Horizontal anchorage strips shall be 3" wide and extend a minimum of 6" beyond the vertical panels. The horizontal anchorage strips shall be placed on the top of the vertical panels.
3. Spalled concrete and exposed reinforcement shall be repaired according to the special provision "PRECAST PRESTRESSED CONCRETE I-BEAM REPAIR".
4. All cracks greater than 0.007 in. shall be injected with epoxy according to Section 590 of the Standard Specifications for Road and Bridge Construction.
5. Acrylic coating shall be placed over fiber wrap repairs. Two coats shall be applied.
6. See special provision for "FRP STRENGTHENING FOR PPC I-BEAM REPAIRS".



MODEL: Default
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| | | |
|------------------------|----------------|-----------|
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| CHECKED - DTS | REVISED - | |
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| PLOT DATE = 01/21/2022 | CHECKED - DTS | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

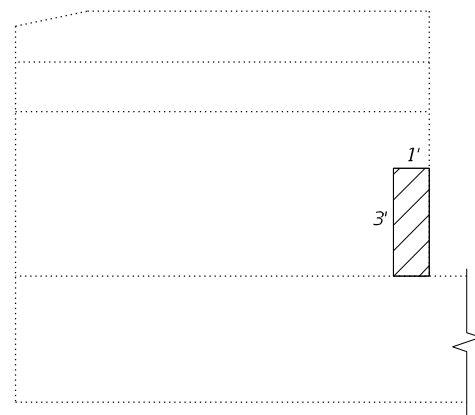
PPC BEAM REPAIR DETAILS - 2
STRUCTURE NO. 050-0174

SHEET S12 OF S15 SHEETS

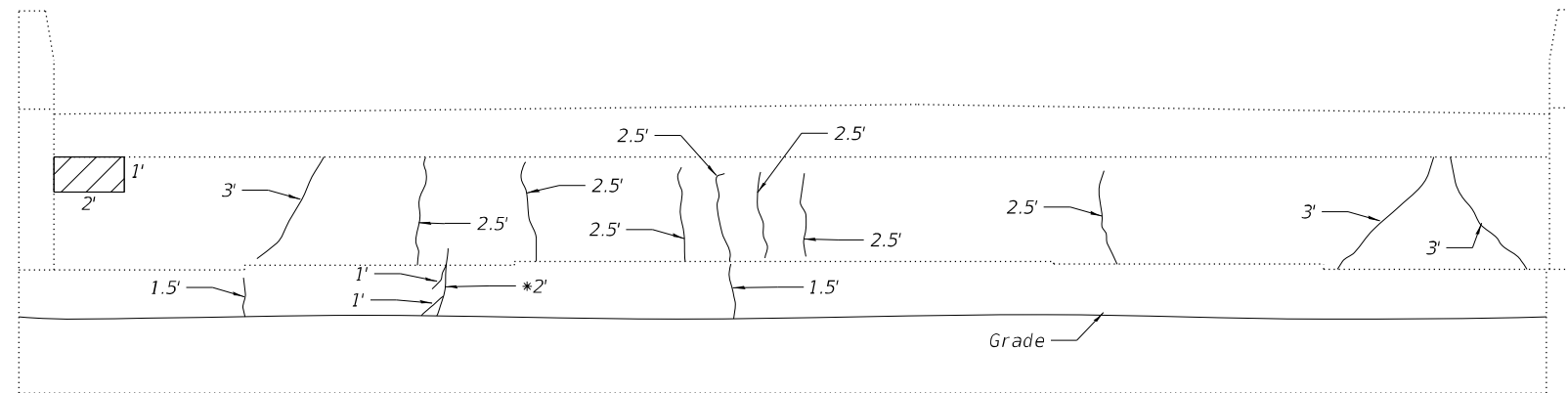
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| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 39 | 50(5A & 5B)BJR,BRR | LaSALLE | 42 | 24 |
| CONTRACT NO. 66L74 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

BILL OF MATERIAL

| Item Description | Unit | Quantity |
|--|---------|----------|
| Concrete Sealer | Sq. Ft. | 1,043 |
| Epoxy Crack Injection | Foot | 55 |
| Structural Repair of Concrete (Depth Equal To Or Less Than 5 Inches) | Sq. Ft. | 26 |

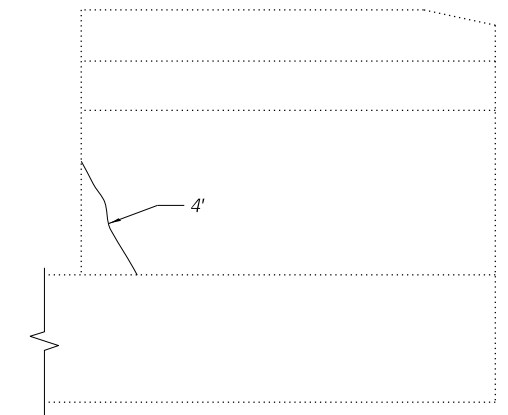


WINGWALL ELEVATION
(Looking West)



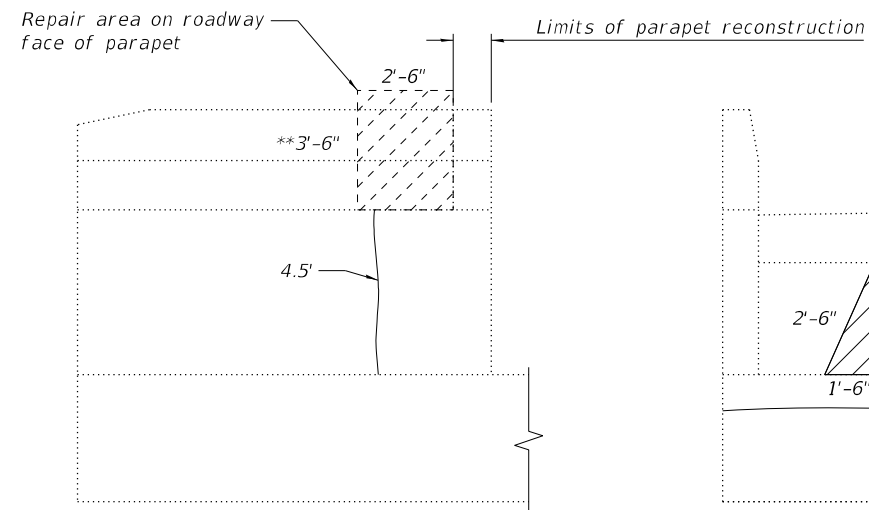
SOUTH ABUTMENT ELEVATION
(Looking South)

* Extends onto bearing seat

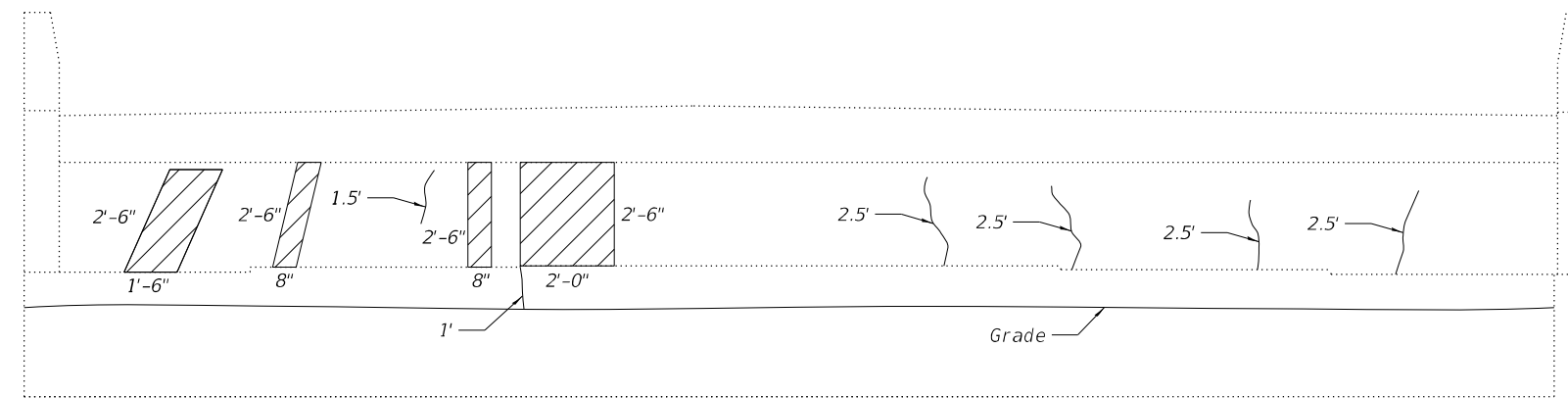


WINGWALL ELEVATION
(Looking East)

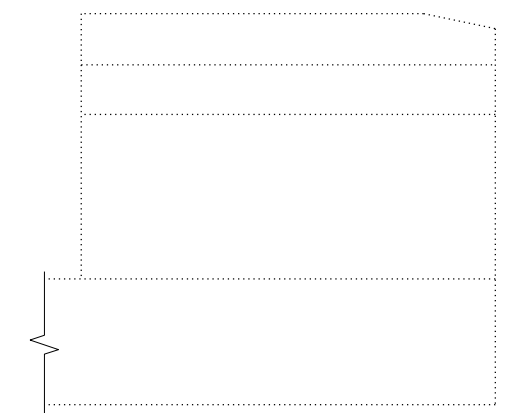
** Extends onto top of parapet



WINGWALL ELEVATION
(Looking East)



NORTH ABUTMENT ELEVATION
(Looking North)



WINGWALL ELEVATION
(Looking West)

LEGEND

- Epoxy Crack Injection
- Structural Repair of Concrete (Depth Equal to or Less Than 5')

MODEL: Default
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| PLOT SCALE = N/A | CHECKED - DTS | REVISED - |
| PLOT DATE = 01/21/2022 | DRAWN - CRS | REVISED - |
| | CHECKED - DTS | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ABUTMENT REPAIR DETAILS
STRUCTURE NO. 050-0174**

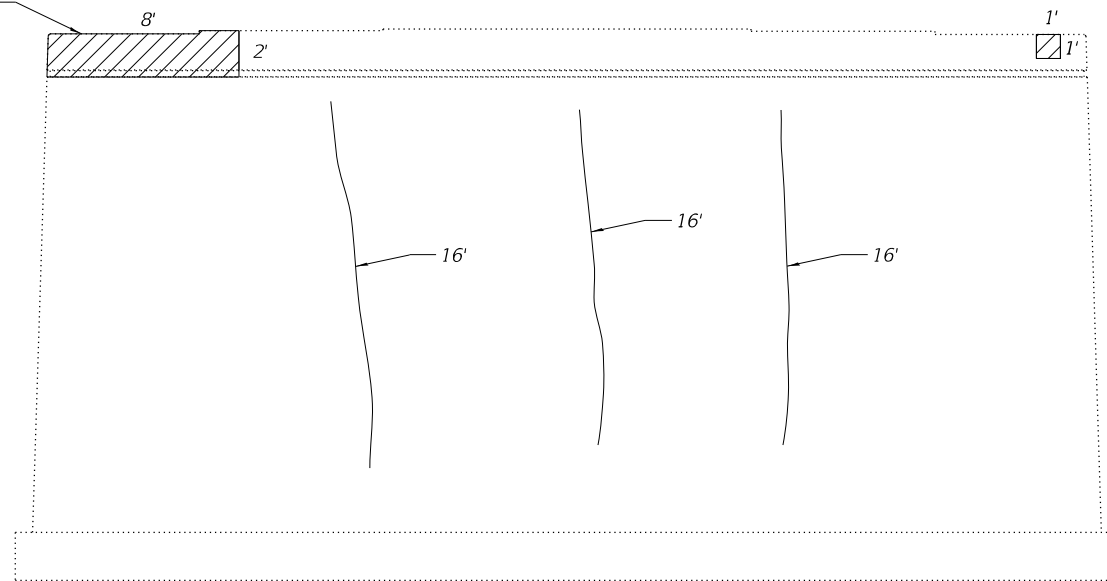
SHEET S13 OF S15 SHEETS

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|--------------------|----------|------------------|-----------|
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| CONTRACT NO. 66L74 | | | | |
| | | ILLINOIS | FED. AID PROJECT | |

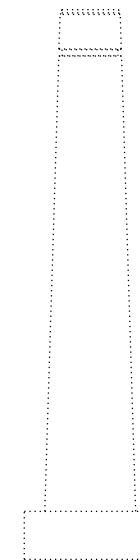
BILL OF MATERIAL

| Item Description | Unit | Quantity |
|--|---------|----------|
| Concrete Sealer | Sq. Ft. | 1,533 |
| Epoxy Crack Injection | Foot | 97 |
| Structural Repair of Concrete (Depth Equal To Or Less Than 5 Inches) | Sq. Ft. | 18 |

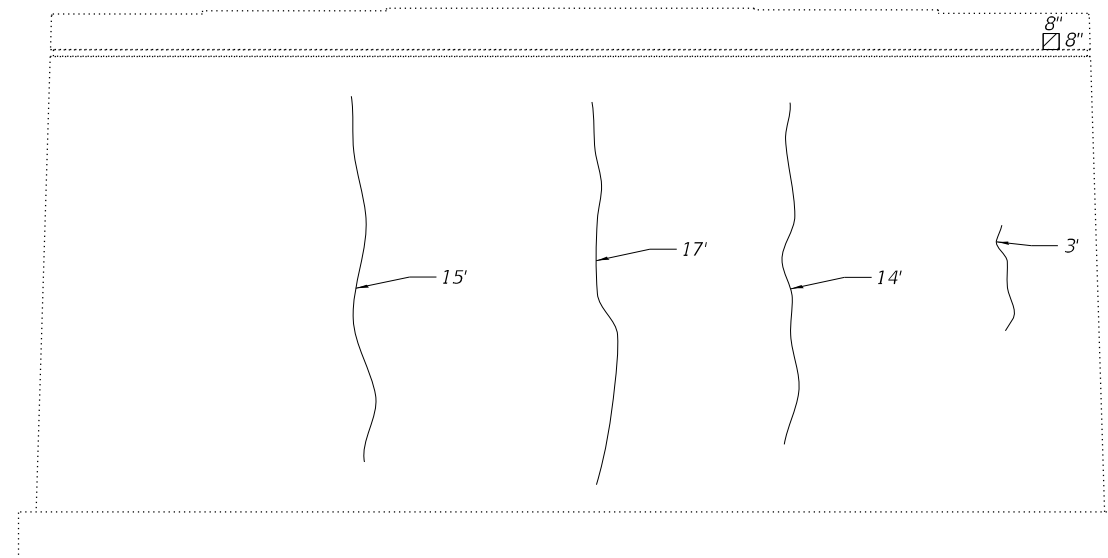
Notify Engineer if removal exceeds 6 inches in depth or stability of beam becomes a concern.



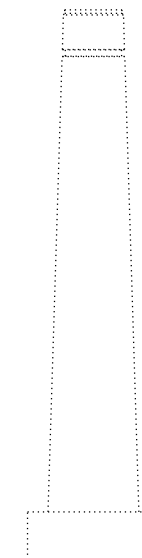
PIER ELEVATION - SOUTH FACE
(Looking North)



PIER ELEVATION - EAST FACE
(Looking West)



PIER ELEVATION - NORTH FACE
(Looking South)



PIER ELEVATION - WEST FACE
(Looking East)

LEGEND

- Epoxy Crack Injection
- Structural Repair of Concrete (Depth Equal to or Less Than 5')

MODEL: Default
FILE NAME: V:\1736\active\173630088\structural\drawing\sheet\0500174-66L74-014-Pier_Repairs.dgn



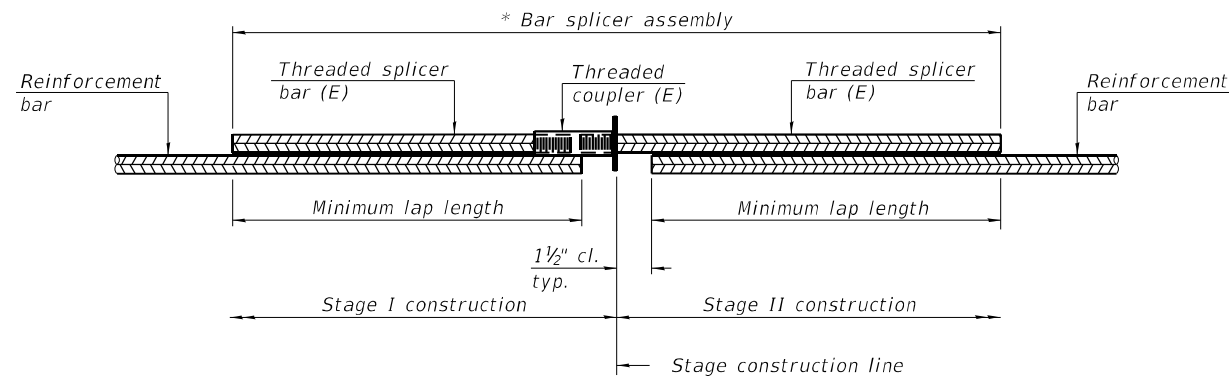
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|------------------------|----------------|-----------|
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| | CHECKED - DTS | REVISED - |
| PLOT SCALE = N/A | DRAWN - CRS | REVISED - |
| PLOT DATE = 01/21/2022 | CHECKED - DTS | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PIER REPAIRS DETAILS
STRUCTURE NO. 050-0174**

SHEET S14 OF S15 SHEETS

| FAI RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|--------------------|----------|------------------|-----------|
| 39 | 50(5A & 5B)BJR,BRR | LaSALLE | 42 | 26 |
| CONTRACT NO. 66L74 | | | | |
| | | ILLINOIS | FED. AID PROJECT | |

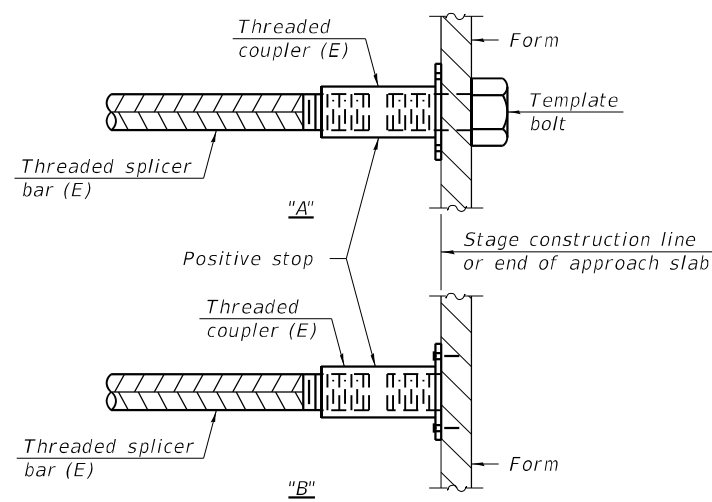


STANDARD BAR SPLICER ASSEMBLY PLAN
 (All components shall be provided from one supplier)

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

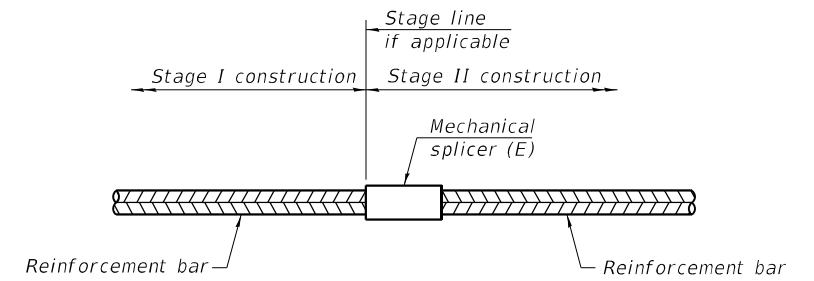
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

| Location | Bar size | No. assemblies required | Minimum lap length |
|----------------|----------|-------------------------|--------------------|
| South Backwall | #6 | 4 | 4'-0" |
| South Deck | #7 | 2 | 4'-2" |
| North Backwall | #6 | 4 | 4'-0" |
| North Deck | #7 | 2 | 4'-2" |



INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

| Location | Bar size | No. assemblies required |
|----------|----------|-------------------------|
| | | |
| | | |
| | | |
| | | |

Notes:
 Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

MODEL: Default
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| PLOT DATE = 01/21/2022 | CHECKED - DTS | REVISED - |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
 STRUCTURE NO. 050-0174

SHEET S15 OF S15 SHEETS

| | | | | |
|---------------------------|----------------------------|----------------|--------------------|--------------|
| F.A.I. RTE. 39 | SECTION 50(5A & 5B)BJR,BRR | COUNTY LaSALLE | TOTAL SHEETS 42 | SHEET NO. 27 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 66L74 | |

Existing Structure:

S.N. 050-0173 (SB) was built in 1979. Structure consists of two-span continuous PPC I-Beams, skewed 20°00'00". Each span is 64'-9", and the back to back of abutments length is 135'-6". The out-to-out width of the superstructure is 42'-0". Abutments are reinforced concrete stub abutments supported on steel H-piles. The solid wall pier is supported on a spread footing in rock. In 2005 the bituminous concrete layer was removed and replaced with a microsilica overlay.

Traffic to be maintained using staged construction.

Salvage: None

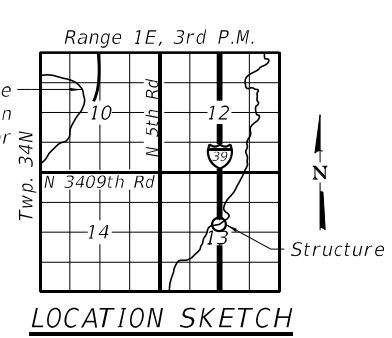
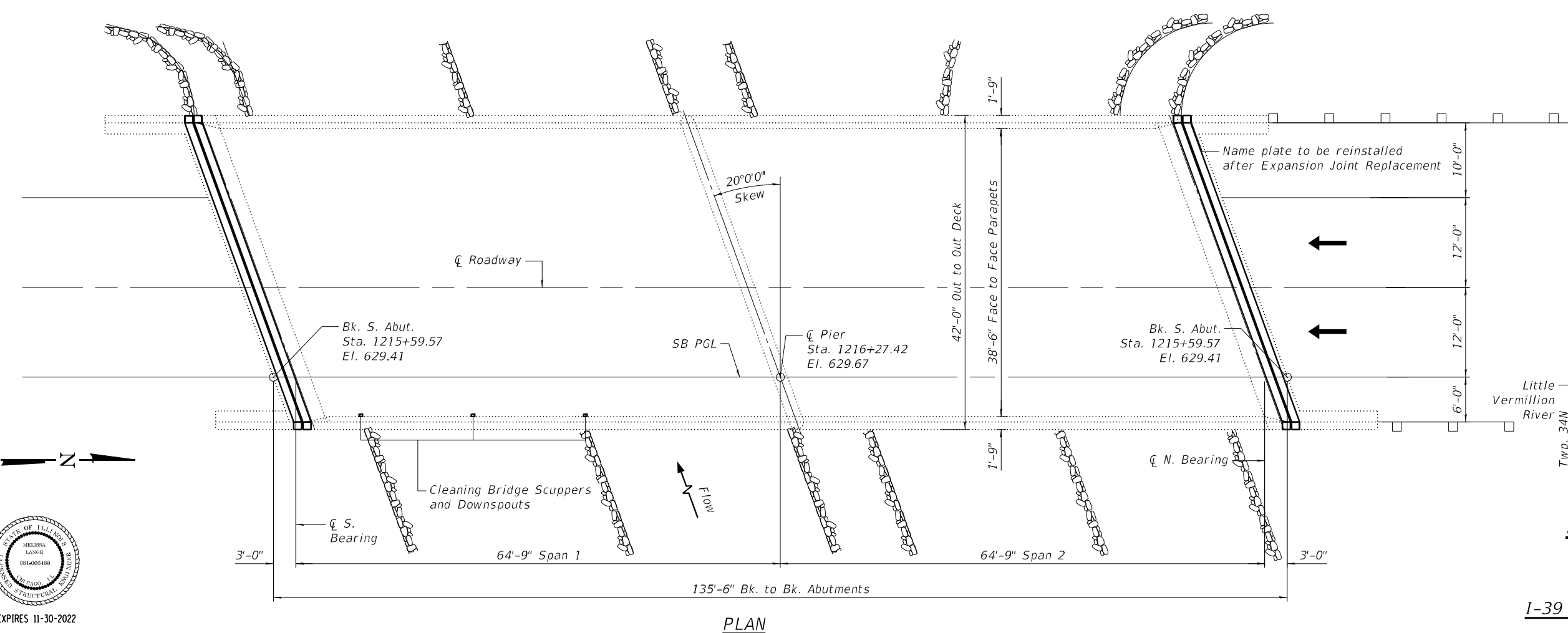
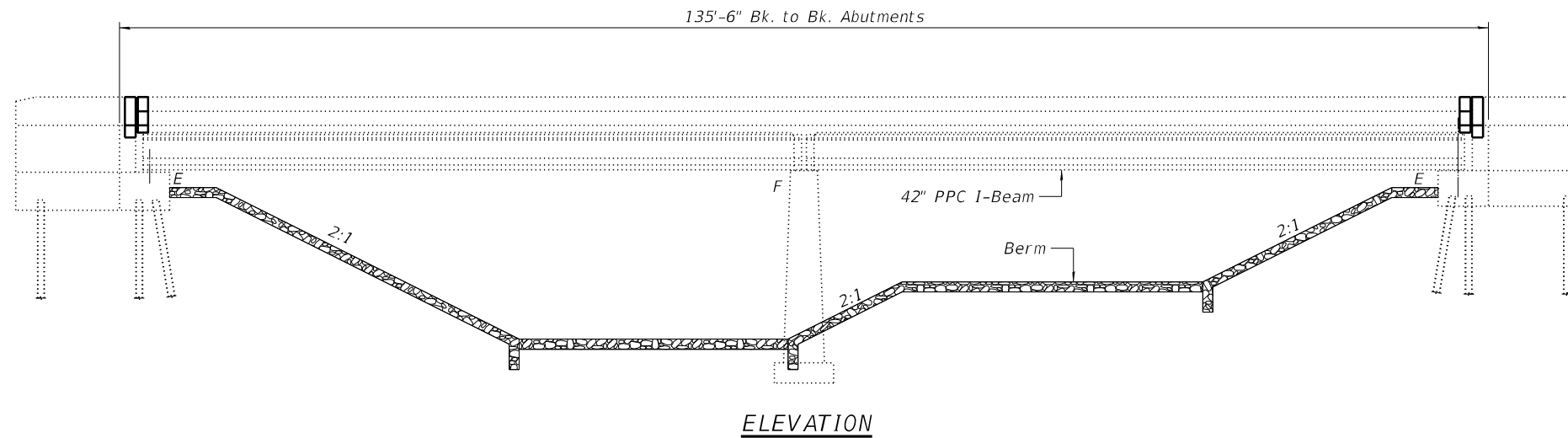
SCOPE OF WORK

1. Scarify existing microsilica overlay
2. Partial and Full Depth Deck Repairs
3. Concrete repairs to parapets, abutments and pier
4. PPC I-Beam Repairs
5. Apply concrete sealer to substructure
6. Remove and reconstruct expansion joints at north and south abutments and install new preformed joint strip seal
7. Place 3¾" microsilica concrete overlay on bridge Deck
8. Apply Bridge Grooving (Longitudinal) and Diamond Grinding (Bridge Section)
9. Apply bridge deck concrete sealer on deck

DESIGN STRESSES

FIELD UNITS

f'c = 3,500 psi
fy = 60,000 psi (Reinforcement)



F.A.I. 39
I-39 OVER TOMAHAWK CREEK
SECTION (50-5B)I-1
LASALLE COUNTY
SN 050-0173 (SB)

| | | | | | | | | | | |
|------------------------------|------------------------------|--|------------------------|---|--|------------------------|--------------------|---------------------------|-----------------|--------------|
| MODEL: Default FILE NAME: | USER NAME = mlange | DESIGNED - C. SEDLACKO CHECKED - M. LANGE | REVISED - REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | GENERAL PLAN & ELEVATION STRUCTURE NO. 050-0173 | F.A.I. RTE. 39 | SECTION (50-5B)I-1 | COUNTY LASALLE | TOTAL SHEETS 42 | SHEET NO. 28 |
| | PLOT SCALE = 16,0000 * / in. | DRAWN - C. SEDLACKO CHECKED - M. LANGE | REVISED - REVISED - | | | SHEET S1 OF S14 SHEETS | CONTRACT NO. 66L74 | ILLINOIS FED. AID PROJECT | | |

GENERAL NOTES

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Joint Openings shall be adjusted according to Article 520.04 of the Std. Specifications when the deck is poured at an ambient temperature other than 50°F.

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

Reinforcement bars designated (E) shall be epoxy coated.

Bridge deck concrete sealer shall be applied to the new overlay, reconstructed transverse expansion joint concrete and inside faces and top faces of the parapets adjacent to the joints.

Concrete sealer shall be applied to all exposed concrete areas of the abutments and piers.

Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

The Contractor shall use extreme care during concrete removal operations so the existing PPC beams are not damaged. If the beams are damaged due to the Contractor's operations they shall be repaired to the satisfaction of the Engineer at no expense to the Department.

Removal of the existing expansion joint steel, anchor studs, expansion material shall not be paid for separately but shall be included in the cost of Concrete Removal.

All exposed concrete edges shall have a 0¾" chamfer, except where shown otherwise.

Expansion joint shall be fabricated to conform to the existing cross slopes of the bridge.

The full depth of the existing microsilicia overlay shall be completely removed as part of the scarification. None of the existing microsilicia overlay shall be allowed to remain in place. The scarification shall not expose the top mat of deck reinforcement. In the event that the top mat is exposed., the Contractor shall contact the Engineer.

Acrylic coating shall be placed over fiber wrap repairs. Two coats shall be applied.

INDEX OF SHEETS

- S1. General Plan and Elevation
- S2. General Data
- S3. Stage Construction Details
- S4. Temporary Concrete Barrier for Staged Construction
- S5. Deck Removal
- S6. Underside Deck Repairs
- S7. Deck Reconstruction Plan
- S8. Deck Reconstruction Details
- S9. Parapet Repairs
- S10. Performed Joint Strip Seal Details
- S11. PPC Beam Repair Details
- S12. Abutment Repair Details
- S13. Pier Repair Details
- S14. Bar Splicer Assembly and Mechanical Splicer Details

TOTAL BILL OF MATERIAL

| ITEM | UNIT | SUPER | SUB | TOTAL |
|--|---------|-------|-------|-------|
| Concrete Removal | Cu. Yd. | 8.1 | | 8.1 |
| Concrete Superstructure | Cu. Yd. | 8.1 | | 8.1 |
| Reinforcement Bars, Epoxy Coated | Pound | 1,170 | | 1,170 |
| Bar Splicers | Each | 12 | | 12 |
| Performed Joint Strip Seal | Foot | 89 | | 89 |
| Concrete Sealer | Sq. Ft. | | 2,555 | 2,555 |
| Epoxy Crack Injection | Foot | 6 | 148 | 154 |
| Cleaning Bridge Scuppers and Downspouts | Each | 3 | | 3 |
| Acrylic Coating | Sq. Yd. | 26 | | 26 |
| Fiber Wrap | Sq. Ft. | 114 | | 114 |
| Bridge Deck Grooving (Longitudinal) | Sq. Yd. | 351 | | 351 |
| Bridge Deck Concrete Sealer | Sq. Ft. | 6,277 | | 6,277 |
| Bridge Deck Scarification 3½" | Sq. Yd. | 556 | | 556 |
| Bridge Deck Microsilica Concrete Overlay 3¾" | Sq. Yd. | 556 | | 556 |
| Structural Repair of Concrete (Depth Equal to or Less than 5 inches) | Sq. Ft. | 260 | 52 | 312 |
| Structural Repair of Concrete (Depth Greater than 5 inches) | Sq. Ft. | | 23.5 | 23.5 |
| Deck Slab Repair (Full Depth, Type I) | Sq. Yd. | 10.5 | | 10.5 |
| Deck Slab Repair (Full Depth, Type II) | Sq. Yd. | 25.2 | | 25.2 |
| | | | | |
| Diamond Grinding (Bridge Section) | Sq. Yd. | 556 | | 556 |
| Precast Prestressed Concrete I-Beam Repair | Sq. Ft. | 20 | | 20 |

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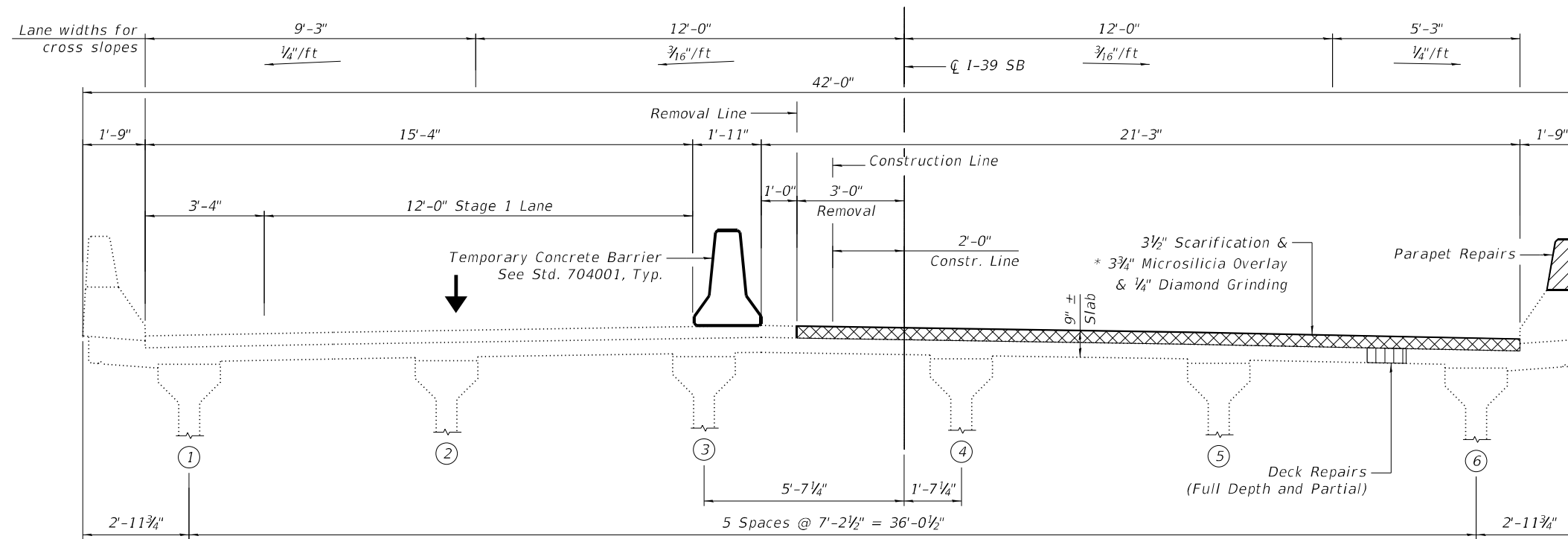
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| PLOT DATE = 1/21/2022 | DRAWN - C. SEDLACKO | REVISED - |
| | CHECKED - M. LANGE | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GGENERAL DATA
STRUCTURE NO. 050-0173**

SHEET 52 OF 514 SHEETS

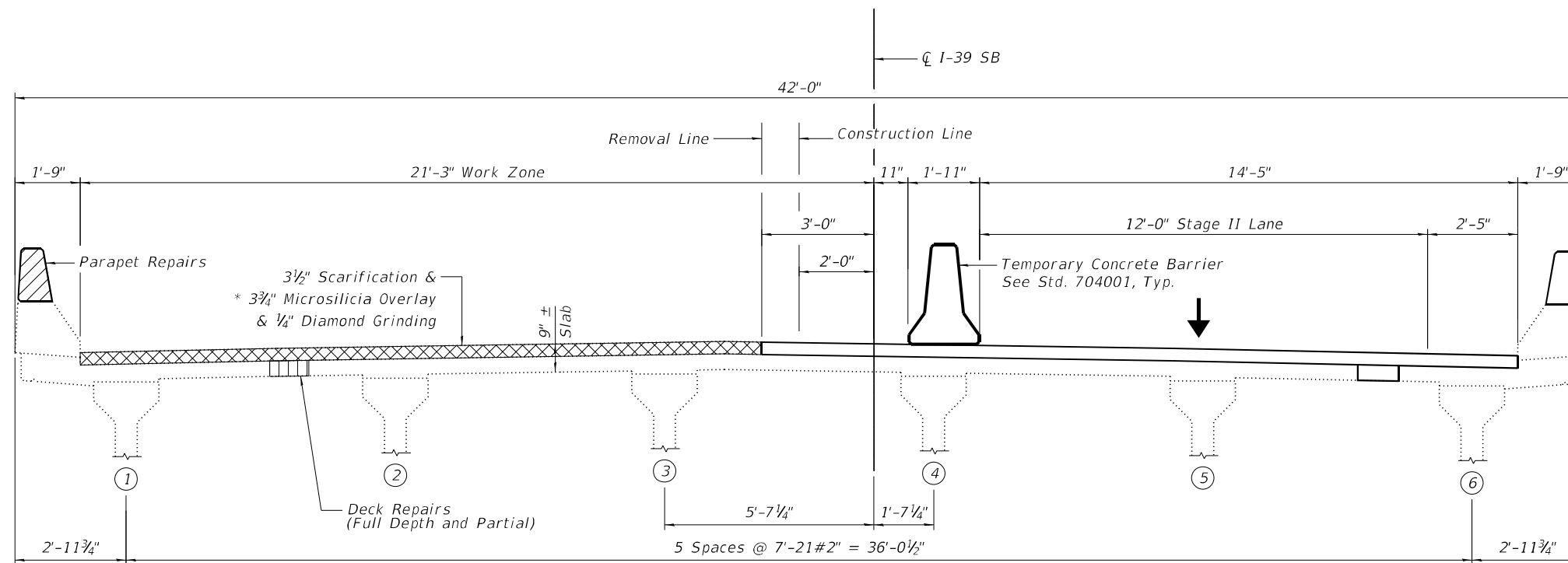
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| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 39 | (50-5B)-1 | LaSALLE | 42 | 29 |
| CONTRACT NO. 66L74 | | | | |
| ILLINOIS | | | FED. AID PROJECT | |



STAGE I CONSTRUCTION AND REMOVAL

I-39 Southbound (Looking North)

Note:
* prior to 1/4" Diamond Grinding



STAGE II CONSTRUCTION AND REMOVAL

I-39 Southbound (Looking North)

MODEL: Default
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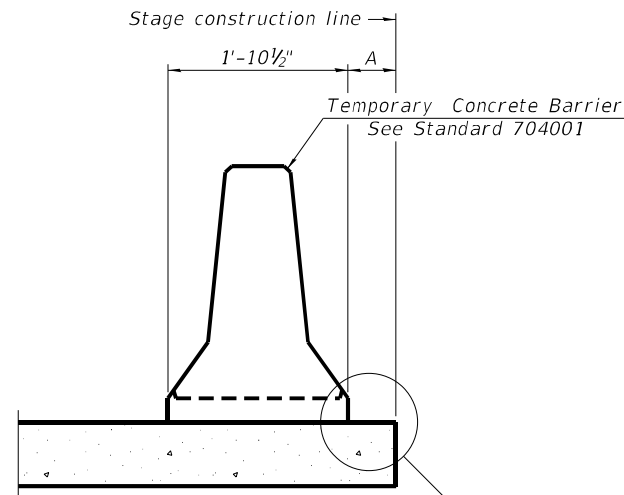
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| PLOT DATE = 1/21/2022 | CHECKED - M. LANGE | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGE CONSTRUCTION DETAILS
STRUCTURE NO. 050-0173

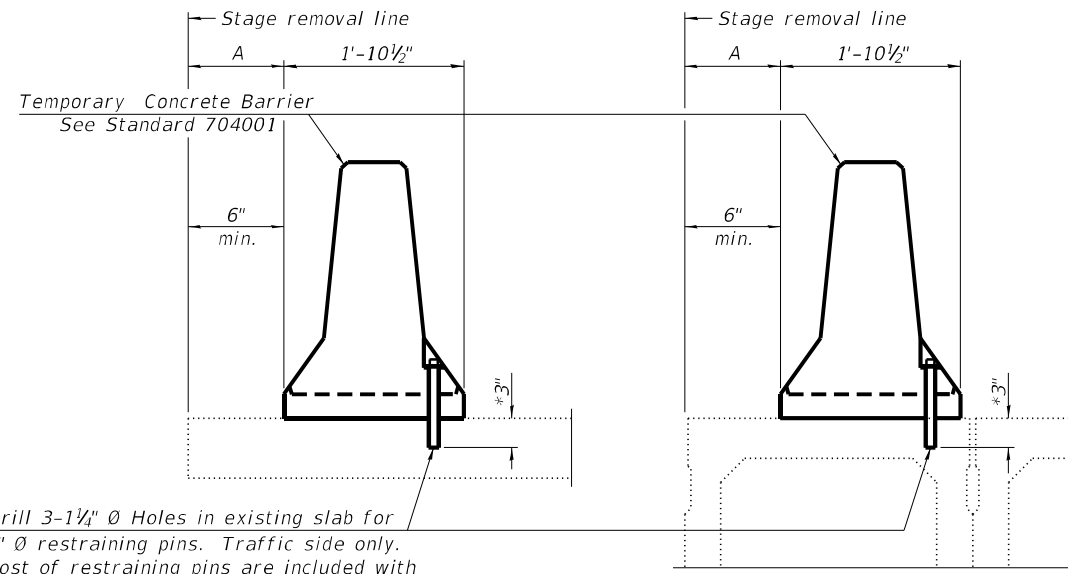
SHEET 53 OF 514 SHEETS

| | | | | |
|---------------------------|-----------|---------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 39 | (50-5B)-1 | LaSALLE | 42 | 30 |
| CONTRACT NO. 66L74 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM



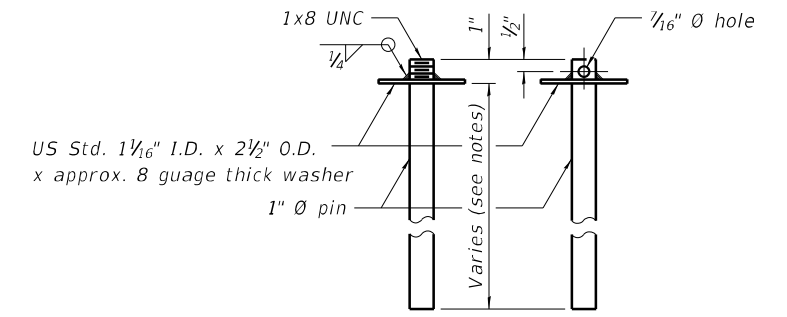
Drill 3-1 1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

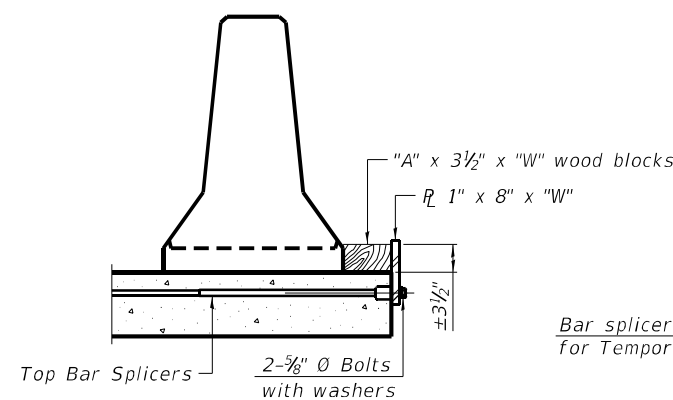
* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

EXISTING DECK BEAM

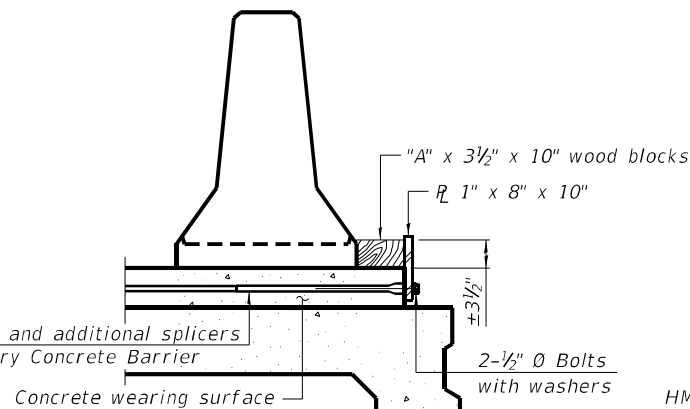
SECTIONS THRU SLAB OR DECK BEAM



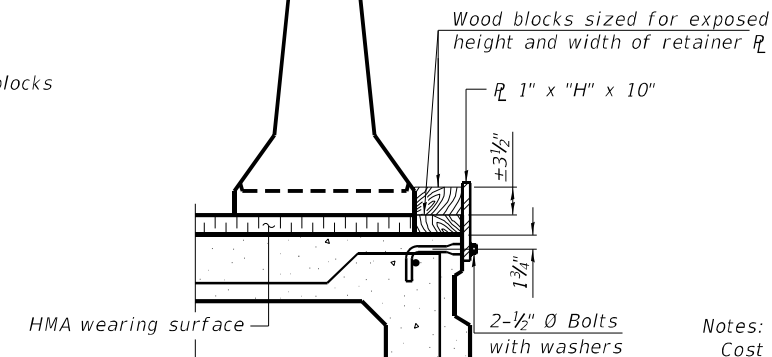
RESTRAINING PIN



DETAIL I

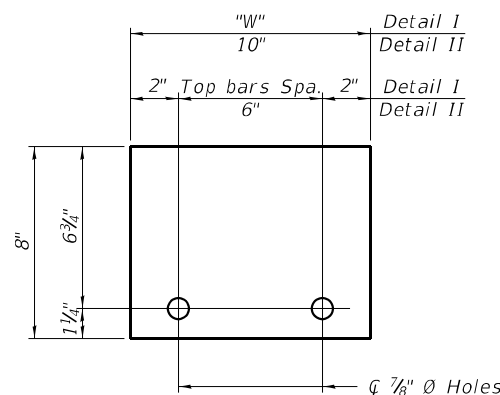


DETAIL II

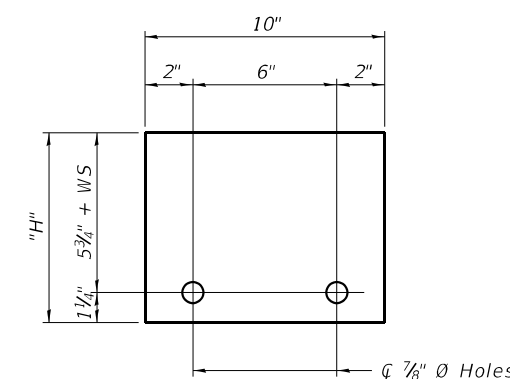


DETAIL III

BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER R 1" x 8" x "W"
(Detail I and II)



STEEL RETAINER R 1" x "H" x 10"
(Detail III)

Notes:
 Cost of retainer assembly is included with Temporary Concrete Barrier.
 A retainer assembly shall be located at the approximate \bar{c} of each temporary concrete barrier.
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.
 When the 'A' dimension is less than 1 1/2', the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

- Detail I - Installation for a new bridge deck or bridge slab.
- Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.
- Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

R-27 2-17-2017

MODEL: Default
FILE NAME:



ENGINEERING RESOURCE ASSOCIATES
 USER NAME = mlange
 PLOT SCALE = 20,0000 * / in.
 PLOT DATE = 1/21/2022

DESIGNED - C. SEDLACKO
 CHECKED - M. LANGE
 DRAWN - C. SEDLACKO
 CHECKED - M. LANGE

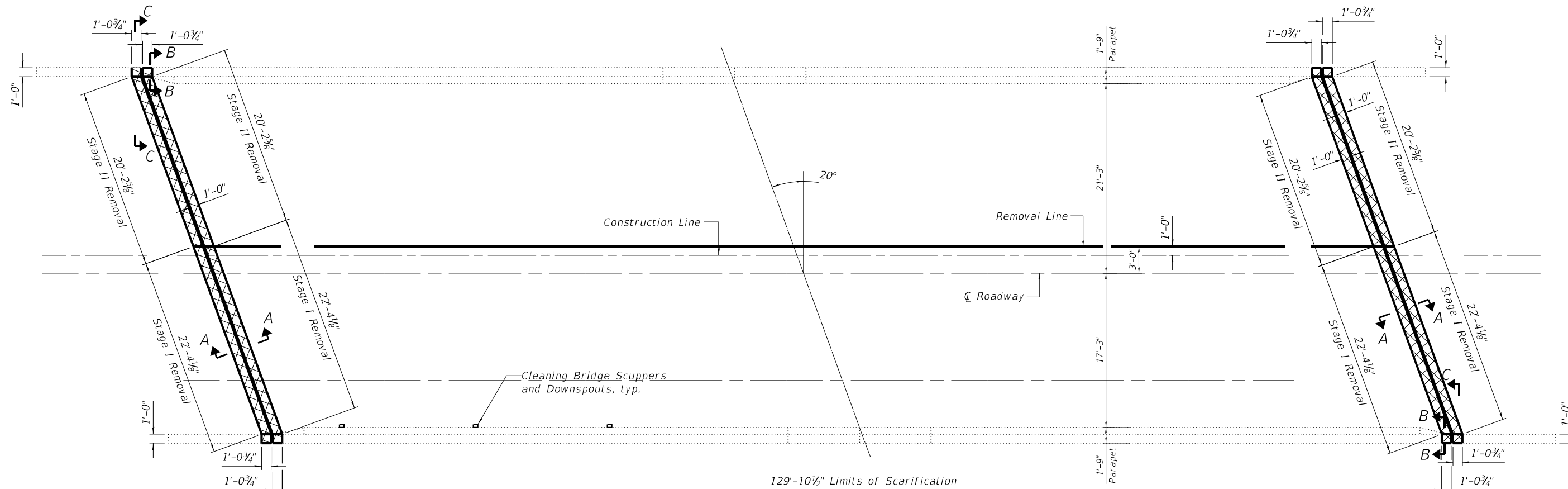
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

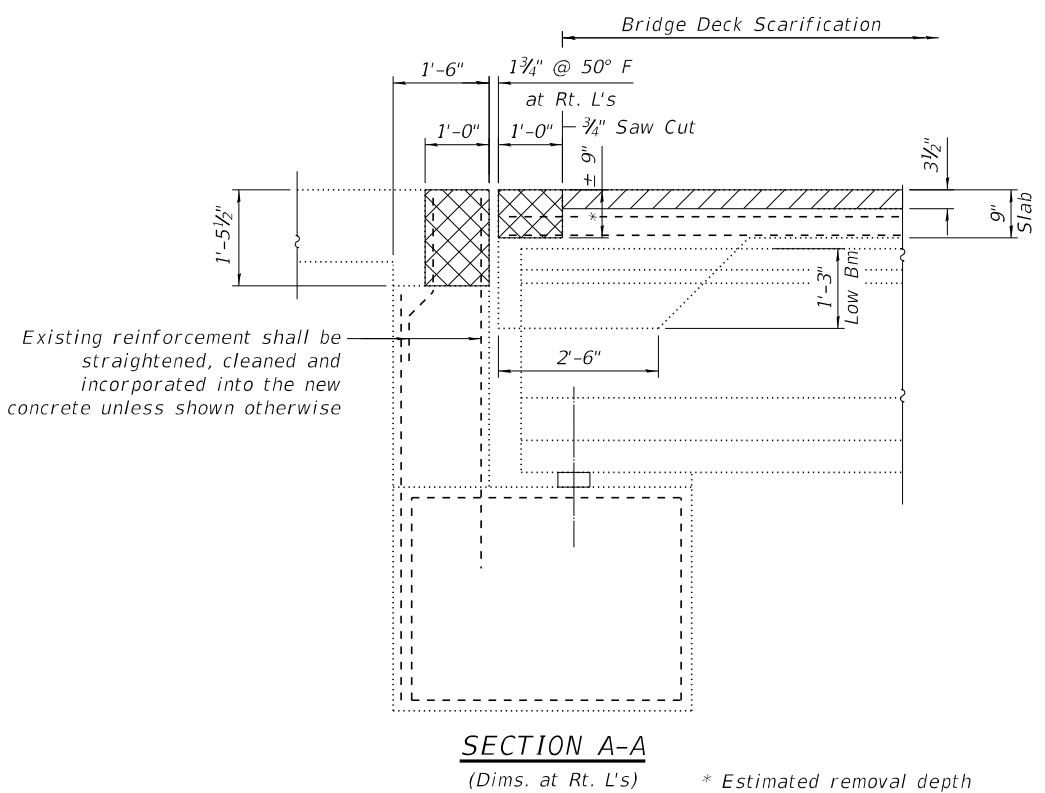
TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION
 STRUCTURE NO. 050-0173

SHEET 54 OF 514 SHEETS

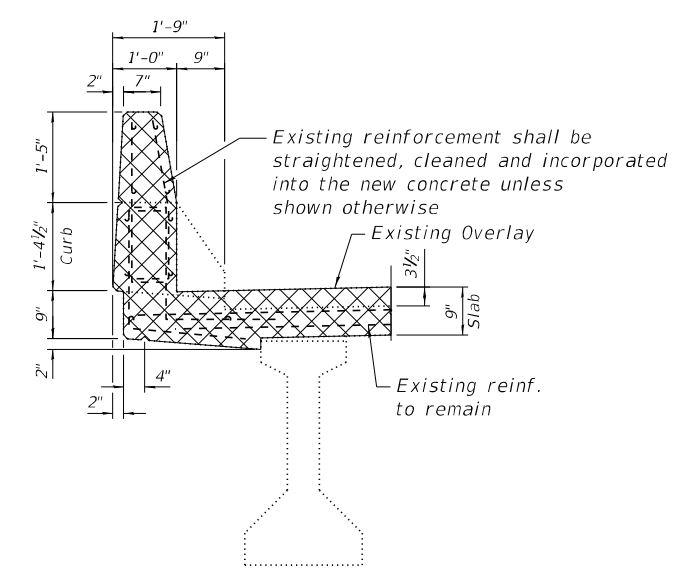
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| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 39 | (50-5B)-1 | LaSALLE | 42 | 31 |
| CONTRACT NO. 66L74 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



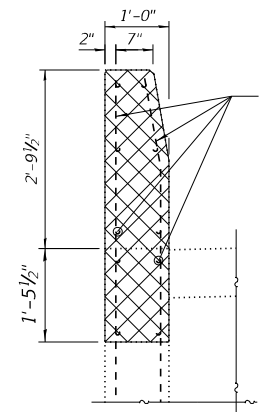
PLAN



SECTION A-A
(Dims. at Rt. L's) * Estimated removal depth



SECTION B-B



SECTION C-C

LEGEND

| | |
|--|----------------------|
| | Concrete Removal |
| | Bridge Scarification |

TOTAL BILL OF MATERIAL

| Item Description | Unit | Quantity |
|---|---------|----------|
| Concrete Removal | Cu. Yd. | 8.1 |
| Bridge Deck Scarification 3 1/2" | Sq. Yd. | 556 |
| Cleaning Bridge Scuppers and Downspouts | Each | 3 |

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FILE NAME:

ENGINEERING RESOURCE ASSOCIATES

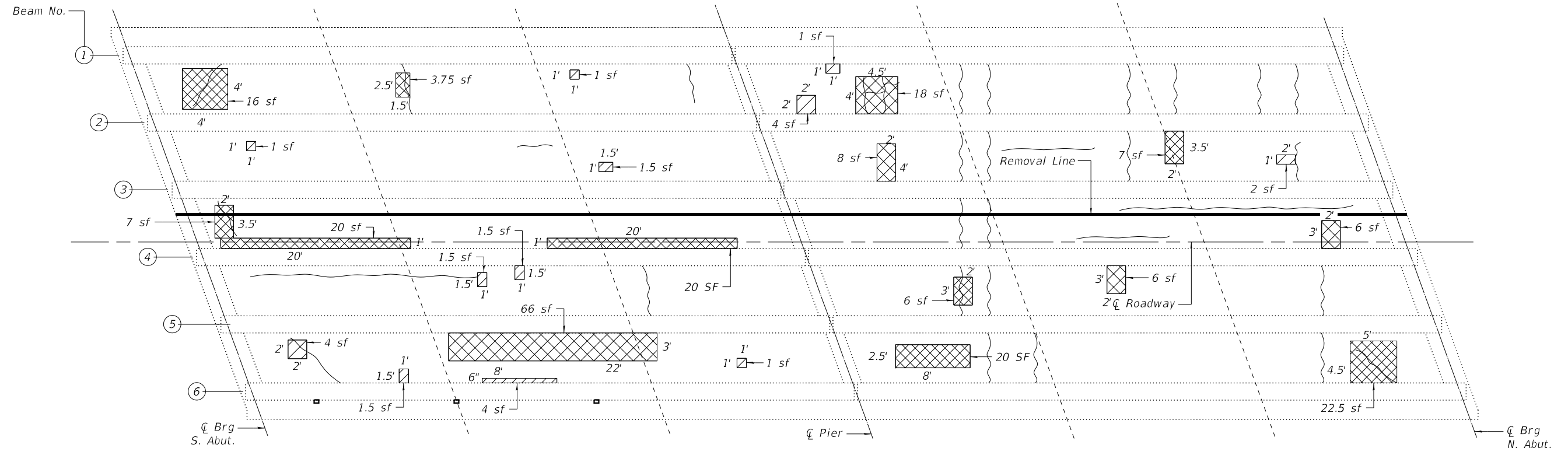
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| PLOT DATE = 1/21/2022 | DRAWN - C. SEDLACKO | REVISED - |
| | CHECKED - M. LANGE | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DECK REMOVAL
STRUCTURE NO. 050-0173**

SHEET 55 OF 514 SHEETS

| | | | | |
|---------------------------|-----------|---------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 39 | (50-5B)-1 | LaSALLE | 42 | 32 |
| CONTRACT NO. 66L74 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



PLAN →

LEGEND

- Full-Depth (Type I)
- Full-Depth (Type II)

Notes:
 1. Deck repairs area are estimated based on visual inspection and will be paid for as specified in the Special Provisions. Actual repair areas and location shall be determined by the Engineer and shown on As-built plans. Engineer shall sound deck after scarification.

TOTAL BILL OF MATERIAL

| Item Description | Unit | Quantity |
|--|---------|----------|
| Deck Slab Repair (Full Depth, Type I) | Sq. Yd. | 10.5 |
| Deck Slab Repair (Full Depth, Type II) | Sq. Yd. | 25.2 |

MODEL: Default
FILE NAME:



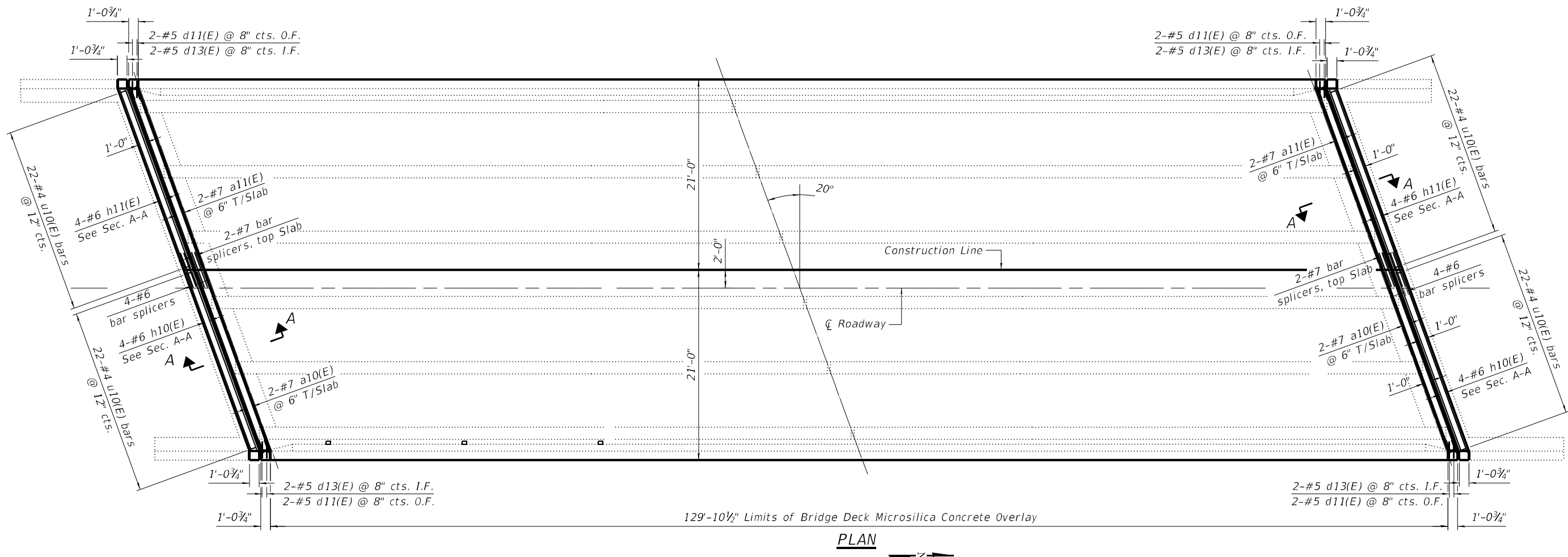
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|-------------------------------|------------------------|-----------|
| USER NAME = mlange | DESIGNED - C. SEDLACKO | REVISED - |
| | CHECKED - M. LANGE | REVISED - |
| PLOT SCALE = 1/2,0000 * / in. | DRAWN - C. SEDLACKO | REVISED - |
| PLOT DATE = 1/21/2022 | CHECKED - M. LANGE | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**UNDERSIDE DECK REPAIRS
STRUCTURE NO. 050-0173**

SHEET 56 OF 514 SHEETS

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|---------|--------------|-----------|
| 39 | (50-5B)-1 | LaSALLE | 42 | 33 |
| CONTRACT NO. 66L74 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



PLAN

Notes:

1. See Sheet Sx for Section A-A.
2. For bridge deck final cross section slopes, see sheet S3.
3. For North and South Transverse Joint Removal and Reconstruction, see sheets S5 and S8.

MODEL: Default
FILE NAME:



ENGINEERING
RESOURCE ASSOCIATES

| | | |
|------------------------------|------------------------|-----------|
| USER NAME = mlange | DESIGNED - C. SEDLACKO | REVISED - |
| | CHECKED - M. LANGE | REVISED - |
| PLOT SCALE = 12,0000 * / in. | DRAWN - C. SEDLACKO | REVISED - |
| PLOT DATE = 1/21/2022 | CHECKED - M. LANGE | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DECK RECONSTRUCTION PLAN
STRUCTURE NO. 050-0173

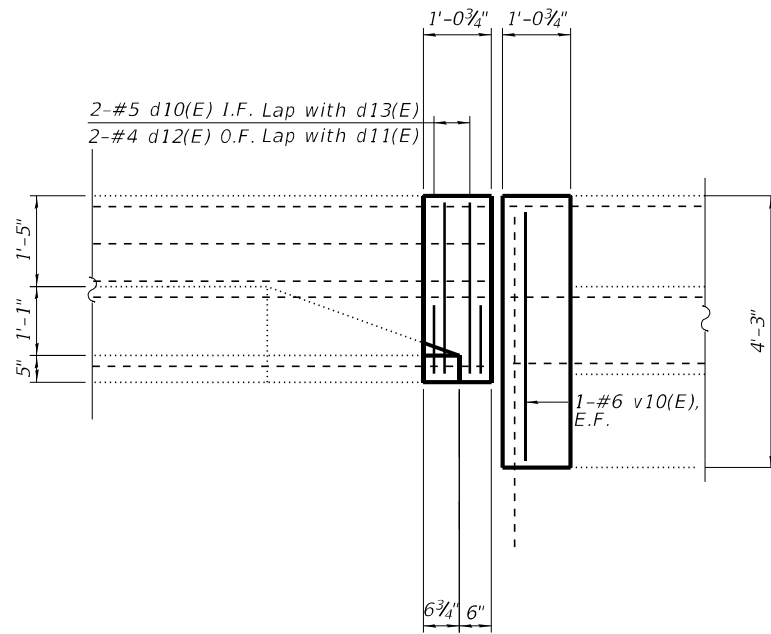
SHEET 57 OF 514 SHEETS

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-----------|---------|--------------|-----------|
| 39 | (50-5B)-1 | LaSALLE | 42 | 34 |
| CONTRACT NO. 66L74 | | | | |

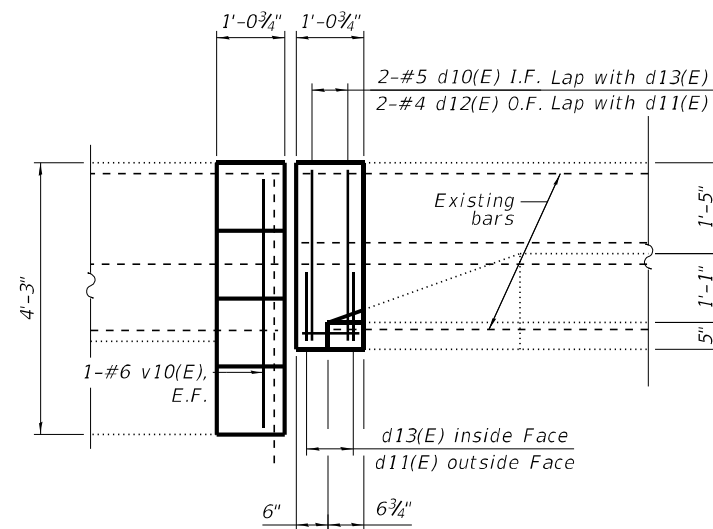
ILLINOIS FED. AID PROJECT

**SUPERSTRUCTURE
BILL OF MATERIAL**

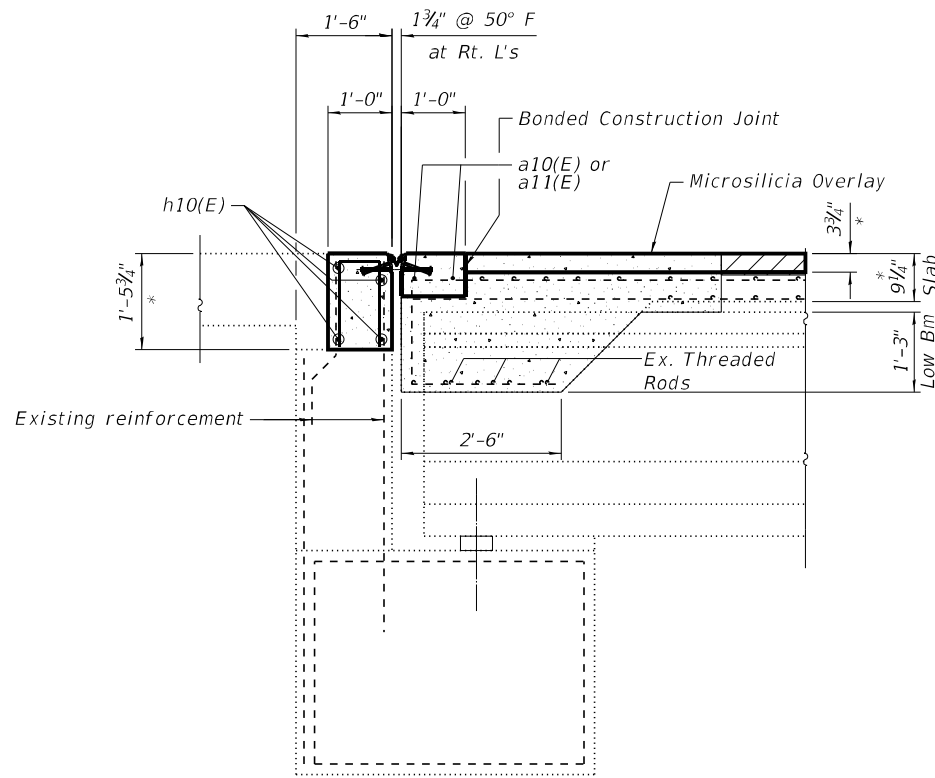
| Bar | No. | Size | Length | Shape |
|--|---------|----------|--------|-------|
| a10(E) | 4 | #7 | 21'-3" | — |
| a11(E) | 4 | #7 | 21'-4" | — |
| d10(E) | 8 | #5 | 3'-0" | ⌒ |
| d11(E) | 8 | #4 | 4'-2" | ⌒ |
| d12(E) | 8 | #4 | 3'-0" | ⌒ |
| d13(E) | 8 | #5 | 3'-9" | ⌒ |
| h10(E) | 8 | #6 | 21'-3" | — |
| h11(E) | 8 | #6 | 21'-4" | — |
| u10(E) | 88 | #4 | 2'-10" | ⌒ |
| v10(E) | 8 | #6 | 3'-11" | — |
| Item | Unit | Quantity | | |
| Concrete Superstructure | Cu. Yd. | 8.1 | | |
| Reinforcement Bars, Epoxy Coated | Lbs. | 1,170 | | |
| Bar Splicers | Each | 12 | | |
| Bridge Deck Grooving (Longitudinal) | Sq. Yd. | 351 | | |
| Bridge Deck Concrete Sealer | Sq. ft. | 6,277 | | |
| Bridge Deck Microsilica Concrete Overlay, 3/4" | Sq. Yd. | 556 | | |
| Diamond Grinding (Bridge Section) | Sq. Yd. | 556 | | |



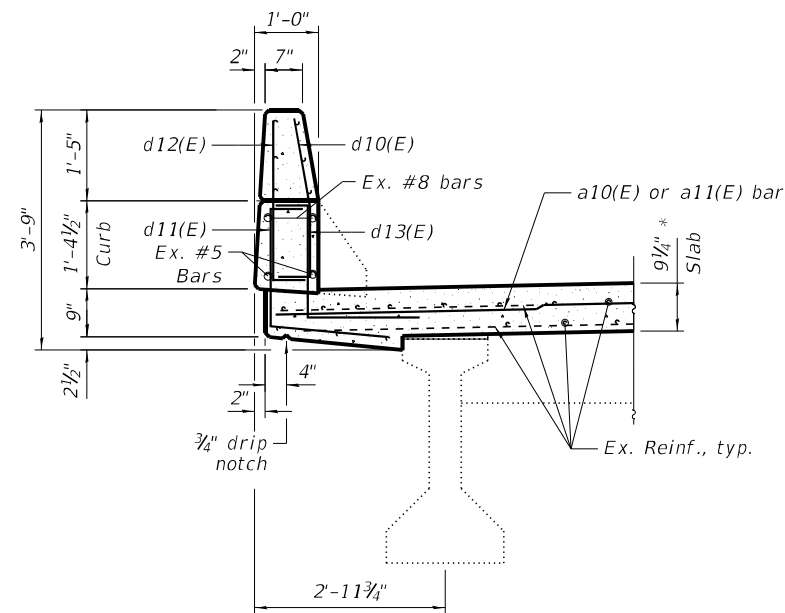
ELEVATION AT SOUTHWEST PARAPET & NORTHEAST PARAPET



ELEVATION AT NORTHWEST PARAPET & SOUTHEAST PARAPET

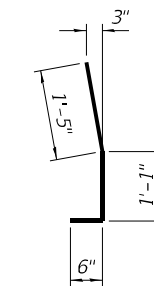


SECTION A-A
(Dims. at Rt. L's)

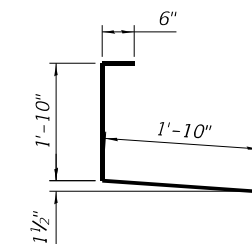


SECTION B-B

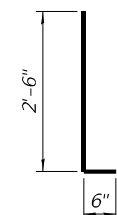
* Prior to 1/4" Diamond Grinding



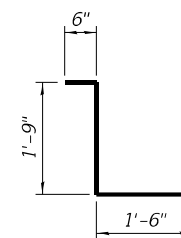
BAR d10(E)



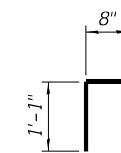
BAR d11(E)



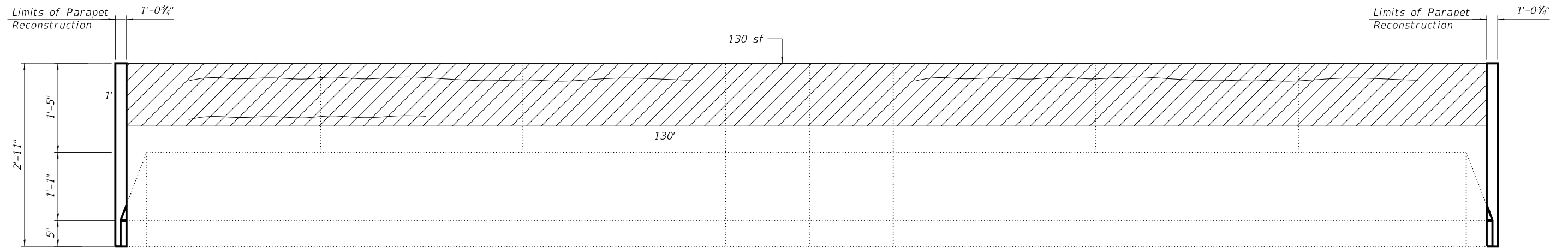
BAR d12(E)



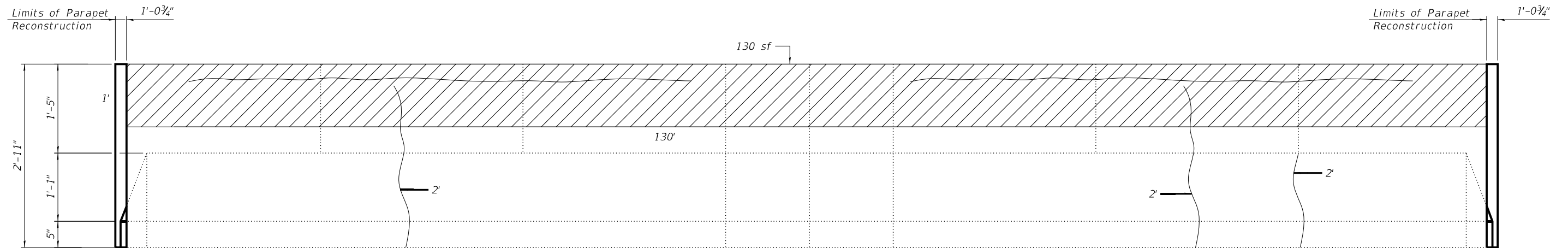
BAR d13(E)



BAR u10(E)



ELEVATION OF WEST PARAPET
(Looking West at West Parapet)



ELEVATION OF EAST PARAPET
(Looking East at East Parapet)

BILL OF MATERIAL

| Item Description | Unit | Quantity |
|--|---------|----------|
| Epoxy Crack Injection | Foot | 6 |
| Structural Repair of Concrete (Depth Equal to or Less than 5 inches) | Sq. Ft. | 260 |

MODEL: Default
FILE NAME:



ENGINEERING
RESOURCE ASSOCIATES

USER NAME = mlange
DESIGNED - C. SEDLACKO
CHECKED - M. LANGE
DRAWN - C. SEDLACKO
CHECKED - M. LANGE

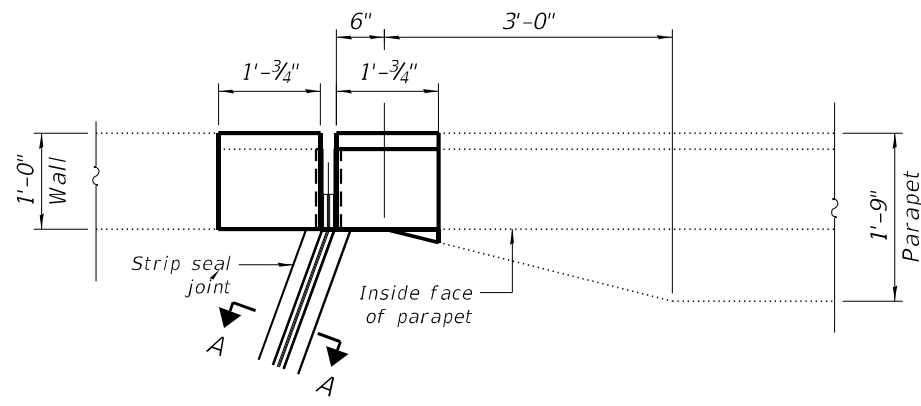
REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

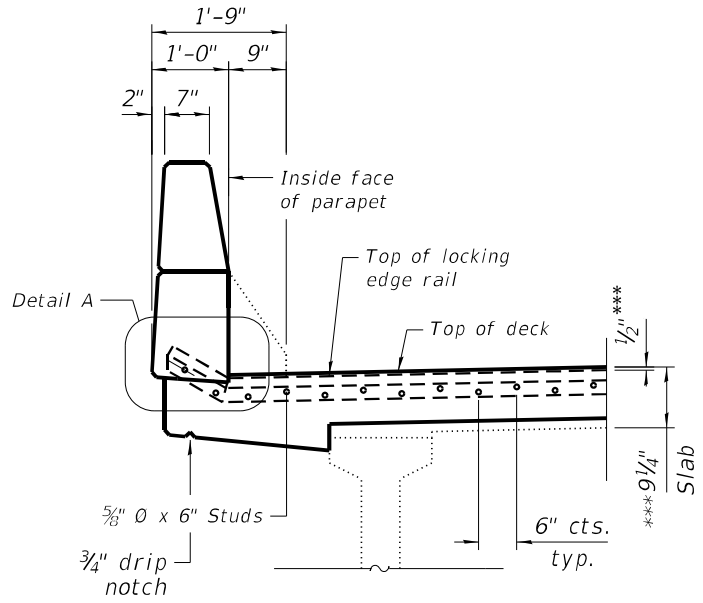
PARAPET REPAIRS
STRUCTURE NO. 050-0173

SHEET 59 OF 514 SHEETS

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|---------|--------------|-----------|
| 39 | (50-5B)-1 | LaSALLE | 42 | 36 |
| CONTRACT NO. 66L74 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

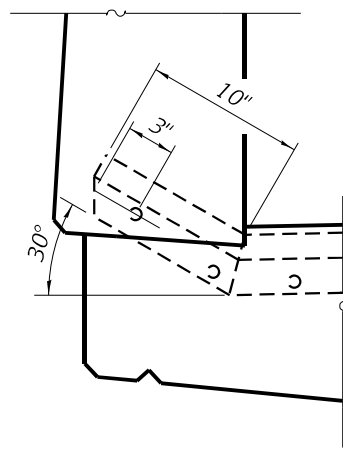


PLAN AT PARAPET

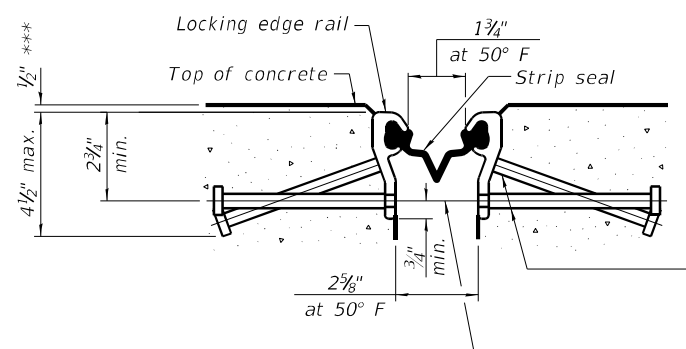


SECTION AT PARAPET

*** prior to diamond grinding



DETAIL A

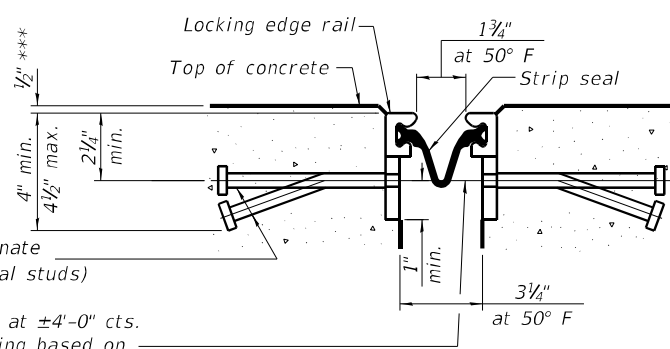


SHOWING ROLLED RAIL JOINT

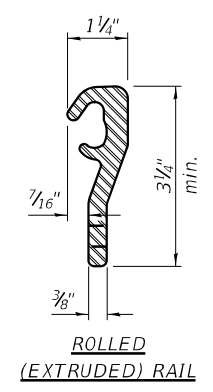
* 5/8" ϕ x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)
 3/8" ϕ threaded rods in 1/16" ϕ holes at $\pm 4'-0"$ cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

SECTION A-A

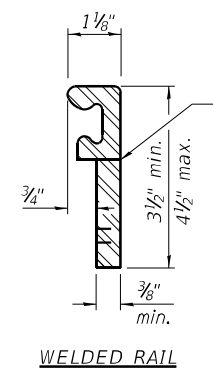
* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



SHOWING WELDED RAIL JOINT



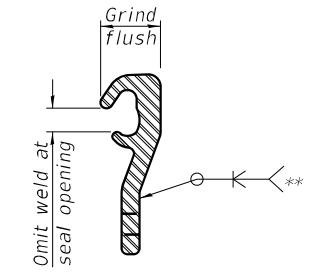
ROLLED (EXTRUDED) RAIL



WELDED RAIL

LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

| Item | Unit | Total |
|----------------------------|------|-------|
| Preformed Joint Strip Seal | Foot | 89 |

Notes:
 The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.
 The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4 1/2" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.
 The manufacturer's recommended installation methods shall be followed.
 All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.
 The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.
 Cost of parapet sliding plates, embedded plates, and anchorage studs included with Preformed Joint Strip Seal.
 39" constant slope barrier shown, 44" constant slope barrier similar as noted.
 The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.

MODEL: Default
 FILE NAME:



USER NAME = mlange
 PLOT SCALE = 20,0000 * / in.
 PLOT DATE = 1/21/2022

DESIGNED - C. SEDLACKO
 CHECKED - M. LANGE
 DRAWN - C. SEDLACKO
 CHECKED - M. LANGE

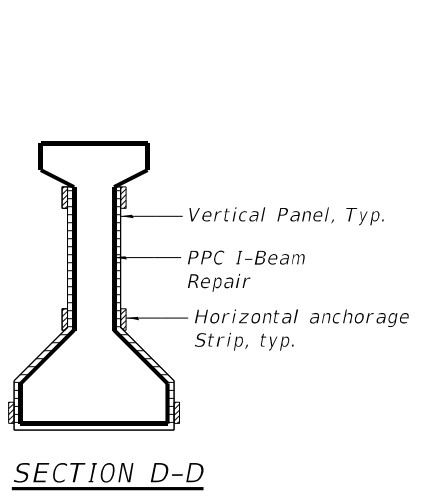
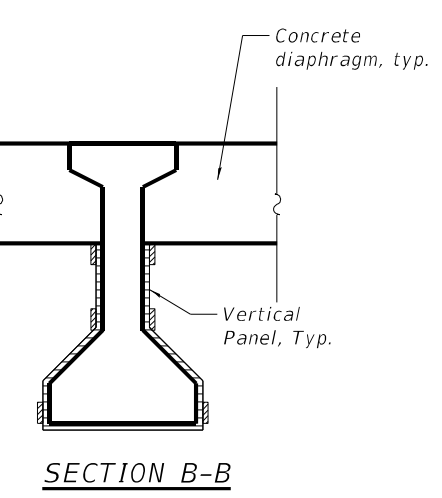
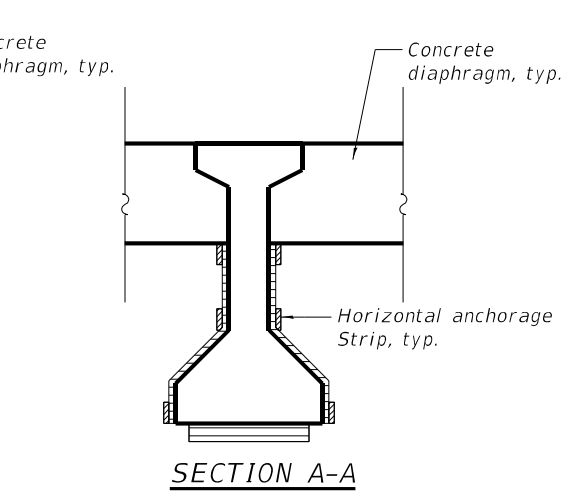
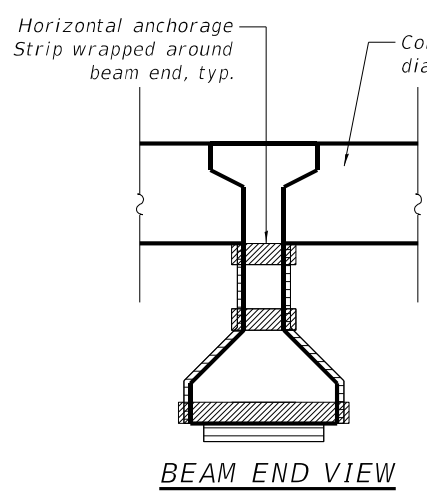
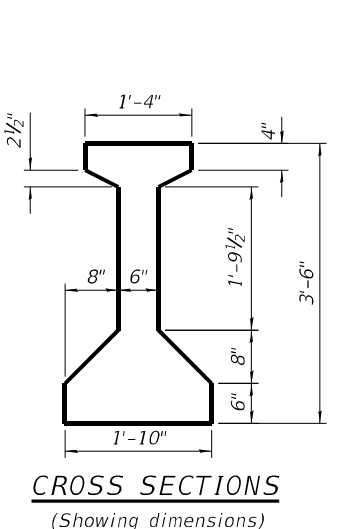
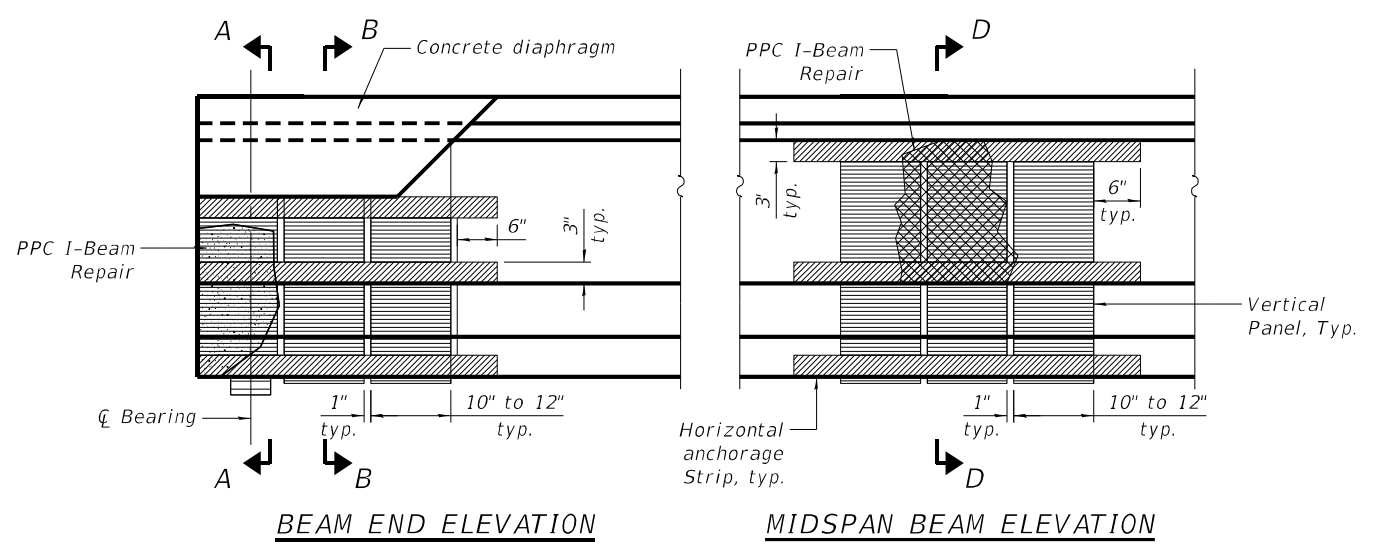
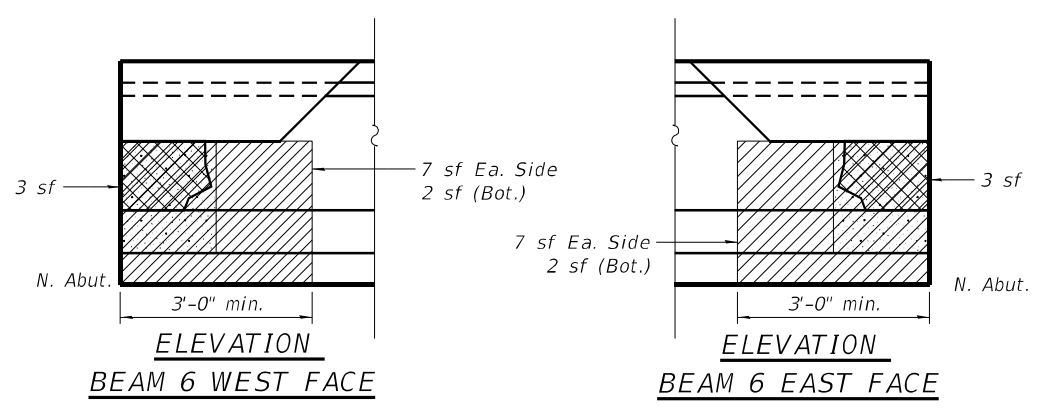
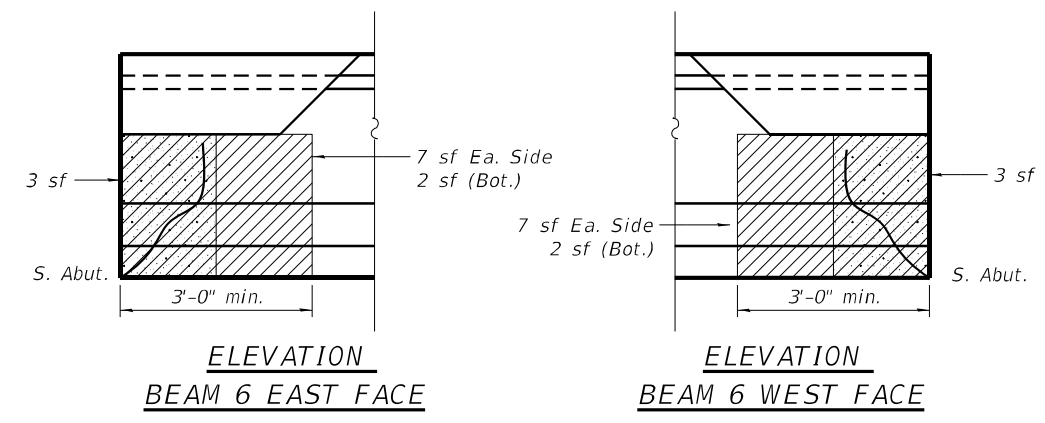
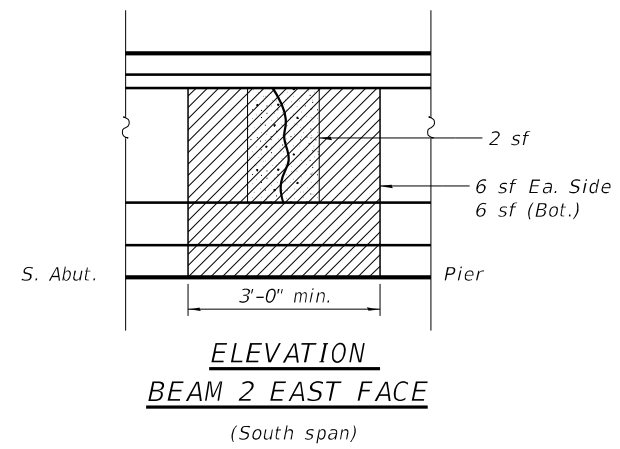
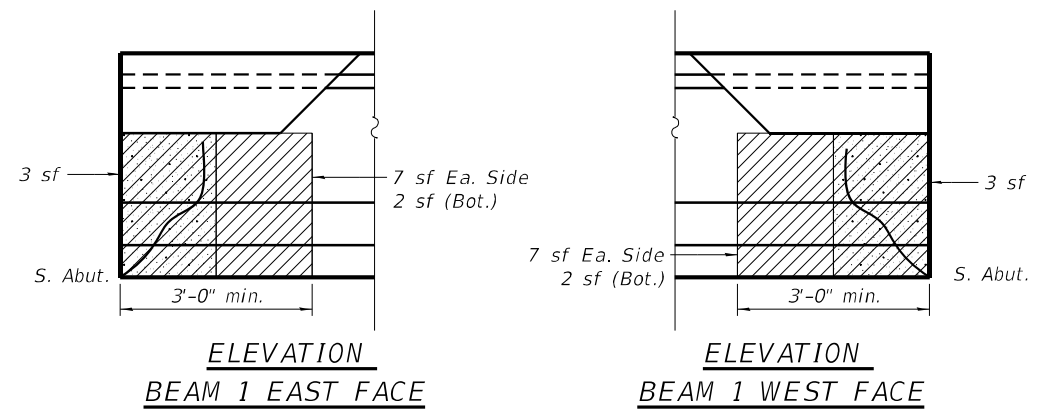
REVISED -
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 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PREFORMED JOINT STRIP SEAL
 STRUCTURE NO. 050-0173**

SHEET 510 OF 514 SHEETS

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------|---------|--------------|-----------|
| 39 | (50-5B)-1 | LaSALLE | 42 | 37 |
| CONTRACT NO. 66L74 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



- Notes:**
- Vertical panels must be between 10" and 12". The space between each vertical panel shall be 1". Vertical panels shall be extended beyond the repair zone by a minimum of 3". Vertical panels located above the bearing location shall be placed in two pieces as shown in Section A-A. At locations in front of the bearing, the vertical panels shall be one continuous strip wrapping beneath of the bottom flanges as shown in Section B-B.
 - Horizontal anchorage strips shall be 3" wide and extend a minimum of 6" beyond the vertical panels. The horizontal anchorage strips shall be placed on the top of their vertical panels.
 - Spalled concrete and exposed reinforcement shall be repaired according to the special provision "PRECAST PRESTRESSED CONCRETE I-BEAM REPAIR".
 - All cracks greater than 0.007 in. shall be injected with epoxy according to Section 590 of the Standard Specifications for Road and Bridge Construction.
 - Acrylic coating shall be placed over fiber wrap repairs. Two coats shall be applied.
 - See special provision for "FRP STRENGTHENING FOR PPC I-BEAM REPAIRS".

LEGEND

- Precast Prestressed Concrete I-Beam Repair
- Crack - Epoxy Crack Injection
- Fiber Wrap Repair and Acrylic Coating
- Delaminated

BILL OF MATERIAL

| Item Description | Unit | Quantity |
|--|---------|----------|
| Acrylic Coating | Sq. Yd. | 26 |
| Fiber Wrap | Sq. Ft. | 114 |
| Precast Prestressed Concrete I-Beam Repair | Sq. Ft. | 20 |

MODEL: Default
FILE NAME:
ER ENGINEERING RESOURCE ASSOCIATES

| | | |
|-------------------------------|------------------------|-----------|
| USER NAME = mlange | DESIGNED - C. SEDLACKO | REVISED - |
| PLOT SCALE = 1/2,0000 * / in. | CHECKED - M. LANGE | REVISED - |
| PLOT DATE = 1/21/2022 | DRAWN - C. SEDLACKO | REVISED - |
| | CHECKED - M. LANGE | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

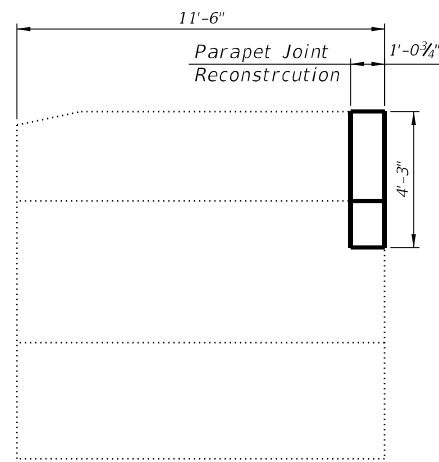
PPC BEAM REPAIR DETAILS
STRUCTURE NO. 050-0173

SHEET 511 OF 514 SHEETS

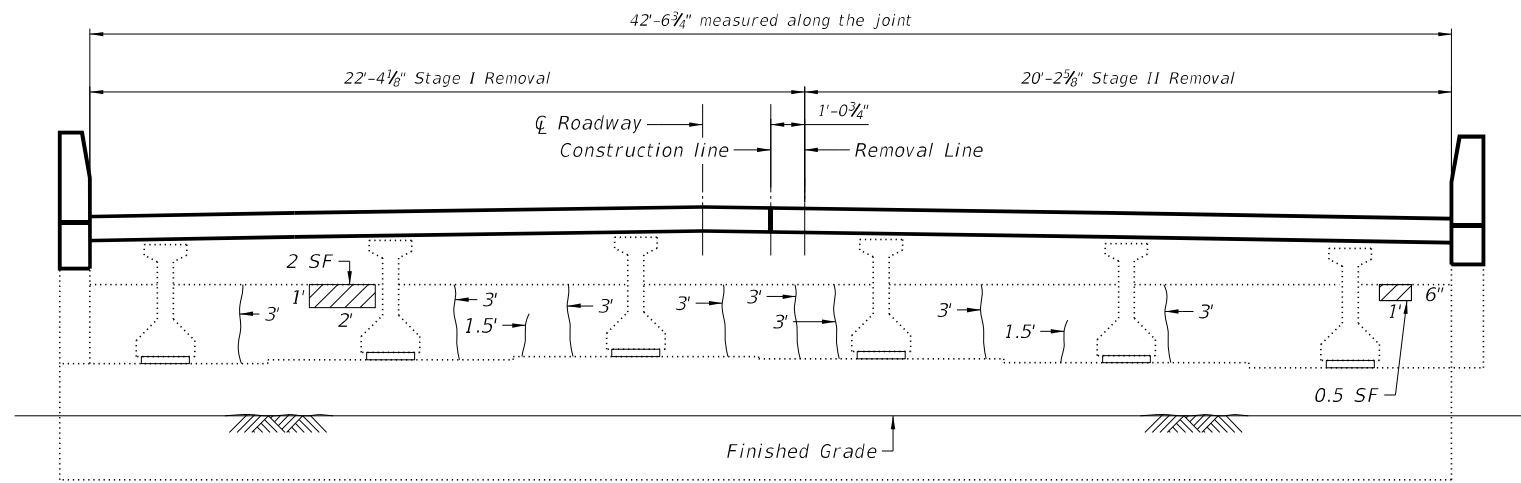
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|---------------------------|-----------|---------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 39 | (50-5B)-1 | LaSALLE | 42 | 38 |
| CONTRACT NO. 66L74 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

BILL OF MATERIAL

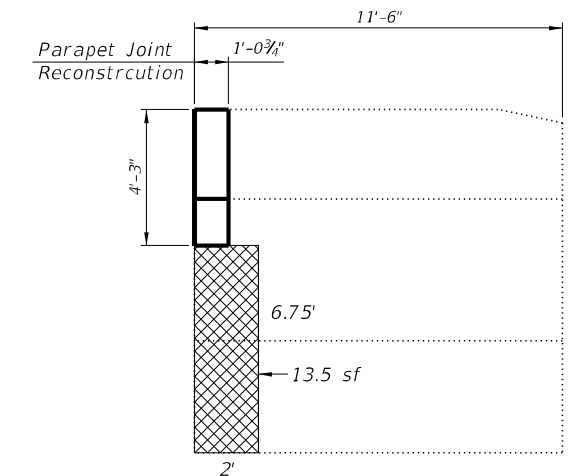
| Item Description | Unit | Quantity |
|--|---------|----------|
| Concrete Sealer | Sq. Ft. | 1017 |
| Epoxy Crack Injection | Foot | 47 |
| Structural Repair of Concrete (Depth Equal to or Less than 5 inches) | Sq. Ft. | 16 |
| Structural Repair of Concrete (Depth Greater than 5 inches) | Sq. Ft. | 23.5 |



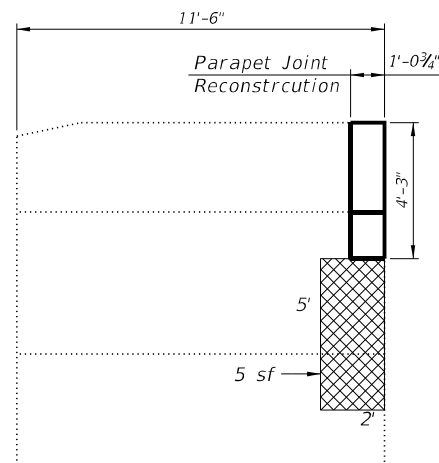
WINGWALL ELEVATION
(Looking West)



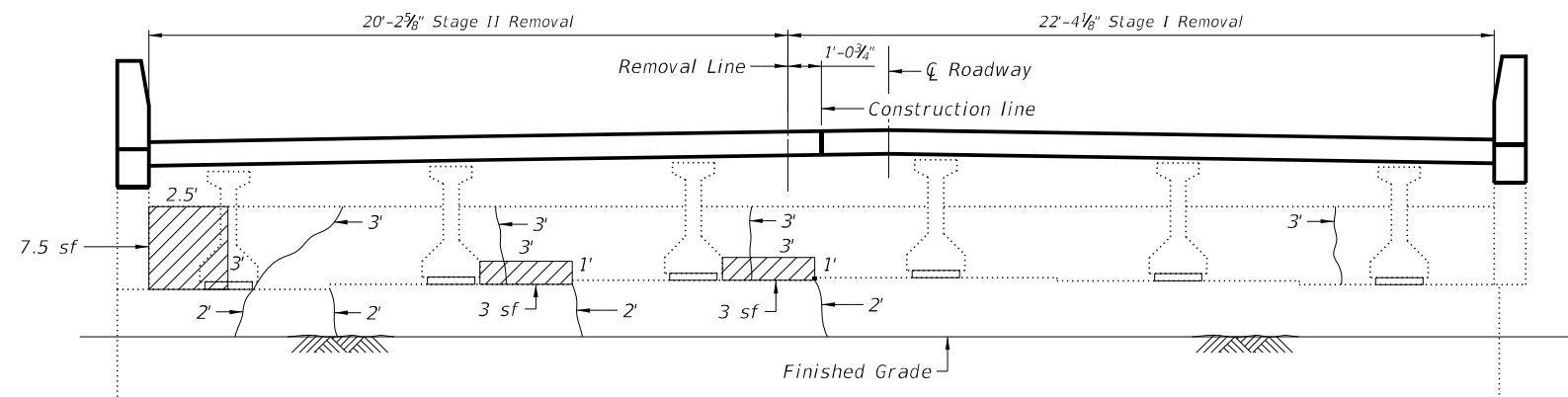
SOUTH ABUTMENT ELEVATION
(dimensions are measured along the skew)



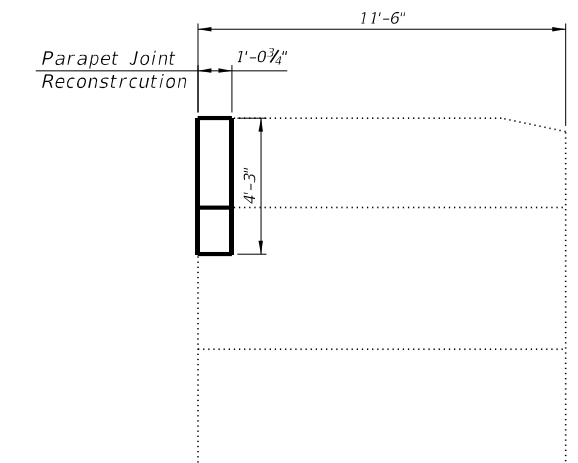
WINGWALL ELEVATION
(Looking East)



WINGWALL ELEVATION
(Looking East)



NORTH ABUTMENT ELEVATION
(dimensions are measured along the skew)



WINGWALL ELEVATION
(Looking West)

LEGEND

- Epoxy Crack Injection
- Structural Repair of Concrete (Depth Equal to or Less than 5 Inches)
- Structural Repair of Concrete (Depth Greater than 5 Inches)

MODEL: Default
FILE NAME:



| | | |
|---|------------------------|-----------|
| USER NAME = mlange | DESIGNED - C. SEDLACKO | REVISED - |
| PLOT SCALE = 6:0,0000 '"/> <td>CHECKED - M. LANGE</td> <td>REVISED -</td> | CHECKED - M. LANGE | REVISED - |
| PLOT DATE = 1/21/2022 | DRAWN - C. SEDLACKO | REVISED - |
| | CHECKED - M. LANGE | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

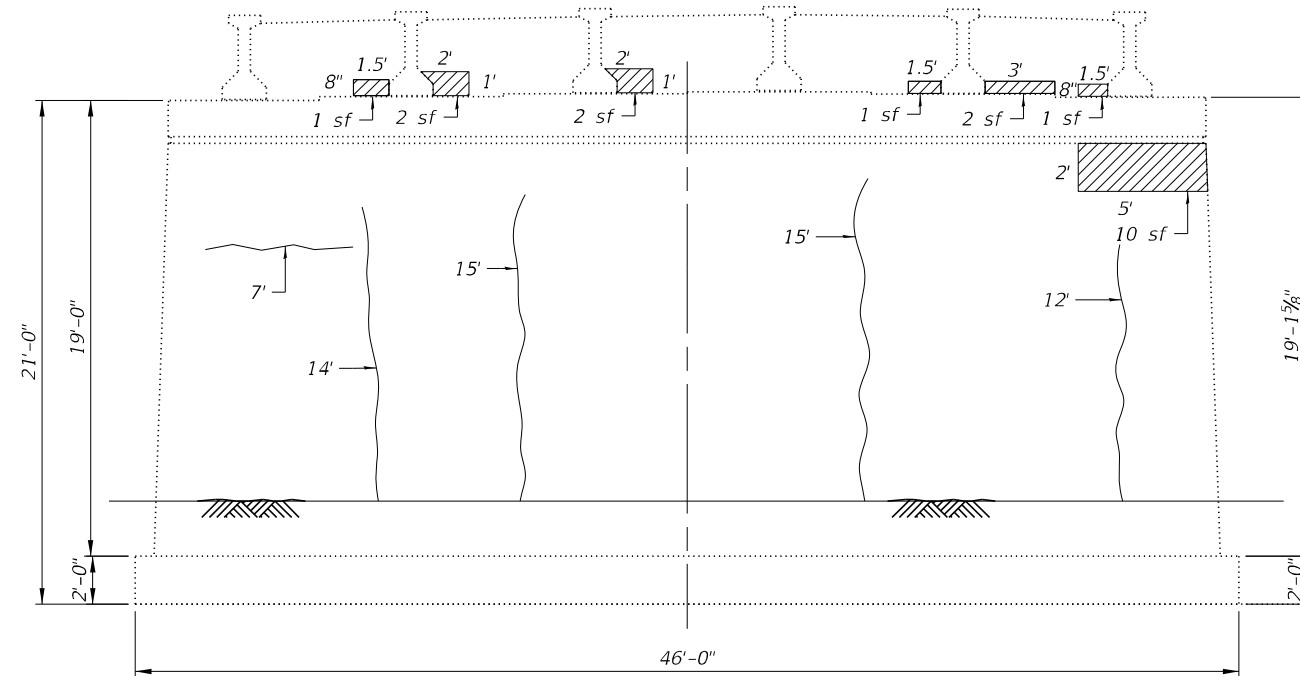
**ABUTMENT REPAIR DETAILS
STRUCTURE NO. 050-0173**

SHEET 512 OF 514 SHEETS

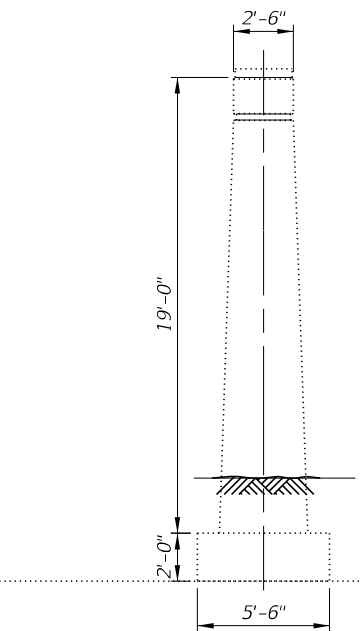
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|---------------------------|-----------|---------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 39 | (50-5B)-1 | LaSALLE | 42 | 39 |
| CONTRACT NO. 66L74 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

BILL OF MATERIAL

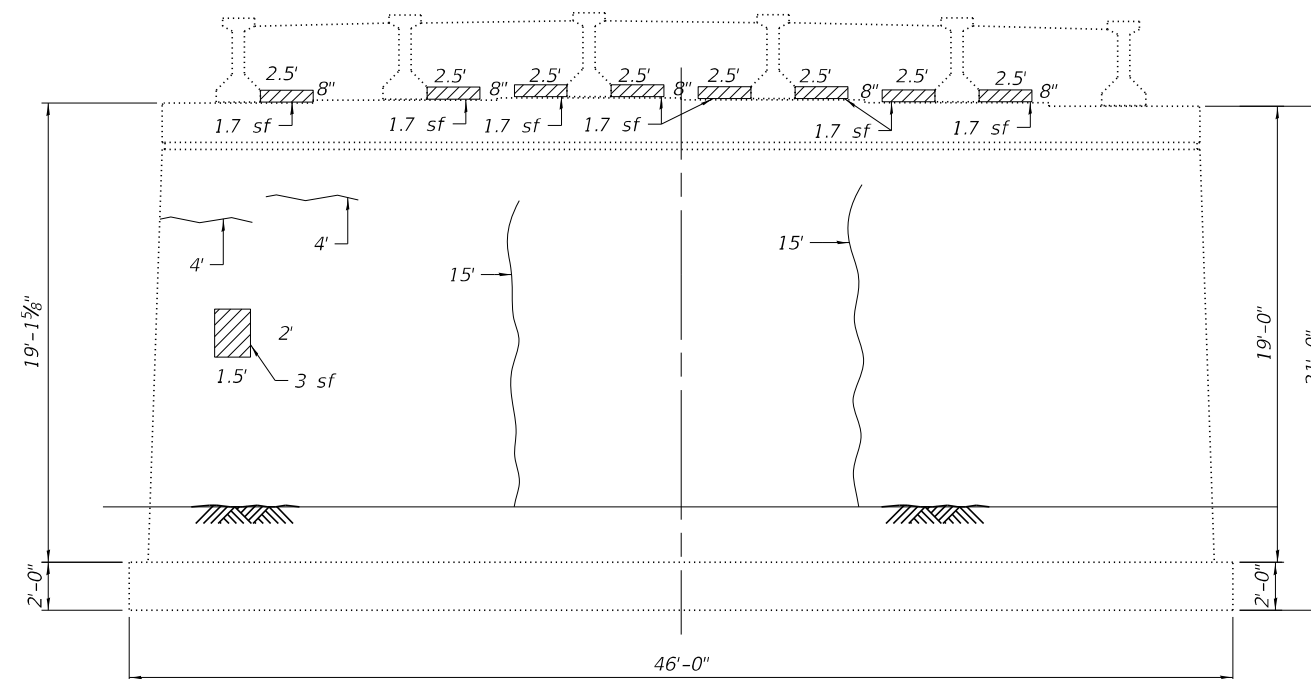
| Item Description | Unit | Quantity |
|--|---------|----------|
| Concrete Sealer | Sq. Ft. | 1,538 |
| Epoxy Crack Injection | Foot | 101 |
| Structural Repair of Concrete (Depth Equal to or Less than 5 inches) | Sq. Ft. | 35.6 |



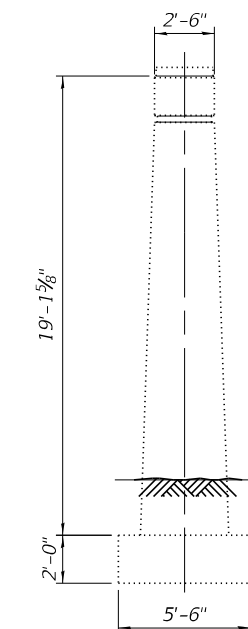
PIER ELEVATION - SOUTH FACE
(Looking North)



PIER ELEVATION - EAST FACE
(Looking West)



PIER ELEVATION - NORTH FACE
(Looking South)



PIER ELEVATION - WEST FACE
(Looking East)

LEGEND

- Epoxy Crack Injection
- Structural Repair of Concrete (Depth Equal to or Less than 5")

MODEL: Default
FILE NAME:



| | | |
|--|------------------------|-----------|
| USER NAME = mlange | DESIGNED - C. SEDLACKO | REVISED - |
| | CHECKED - M. LANGE | REVISED - |
| PLOT SCALE = 8:0,0000 '"/> <td>DRAWN - C. SEDLACKO</td> <td>REVISED -</td> | DRAWN - C. SEDLACKO | REVISED - |
| PLOT DATE = 1/21/2022 | CHECKED - M. LANGE | REVISED - |

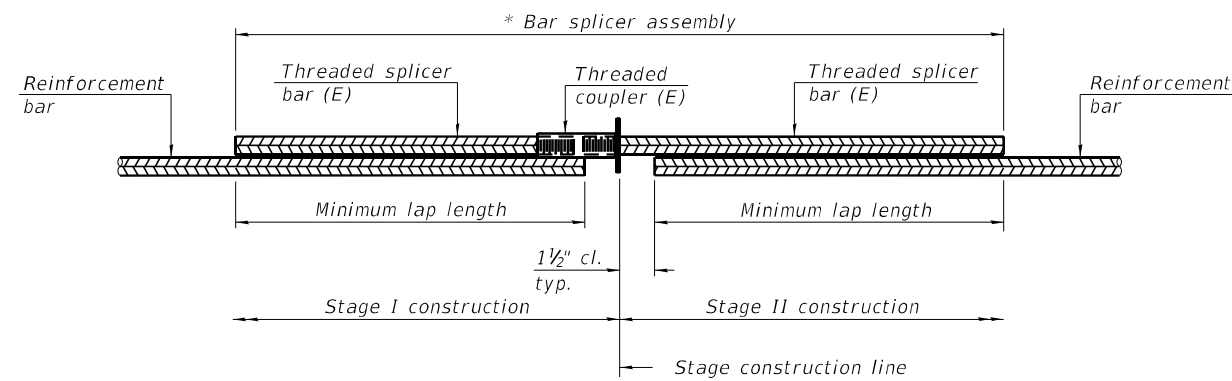
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PIER REPAIR DETAILS
STRUCTURE NO. 050-0173**

SHEET 513 OF 514 SHEETS

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-----------|---------|--------------|-----------|
| 39 | (50-5B)-1 | LaSALLE | 42 | 40 |
| CONTRACT NO. 66L74 | | | | |

ILLINOIS FED. AID PROJECT

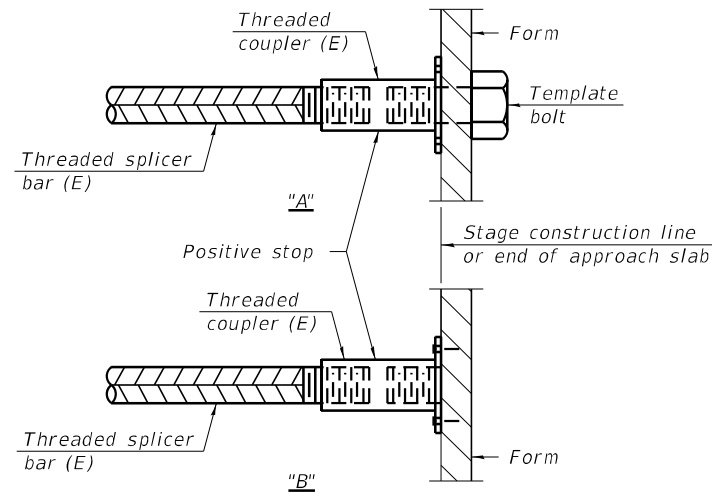


STANDARD BAR SPLICER ASSEMBLY PLAN
 (All components shall be provided from one supplier)

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

| Location | Bar size | No. assemblies required | Minimum lap length |
|----------------|----------|-------------------------|--------------------|
| South Backwall | #6 | 4 | 4'-0" |
| South Deck | #7 | 2 | 4'-2" |
| North Backwall | #6 | 4 | 4'-0" |
| North Deck | #7 | 2 | 4'-2" |

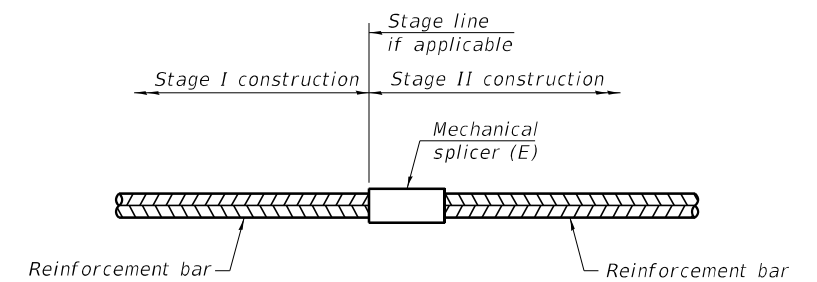


INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.

"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

| Location | Bar size | No. assemblies required |
|----------|----------|-------------------------|
| | | |
| | | |
| | | |
| | | |

Notes:

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars.

Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

1-1-2020

MODEL: Default
 FILE NAME:



ENGINEERING RESOURCE ASSOCIATES
 USER NAME = mlange
 PLOT SCALE = 20,0000 * / in.
 PLOT DATE = 1/21/2022

DESIGNED - C. SEDLACKO
 CHECKED - M. LANGE
 DRAWN - C. SEDLACKO
 CHECKED - M. LANGE

REVISED -
 REVISED -
 REVISED -
 REVISED -

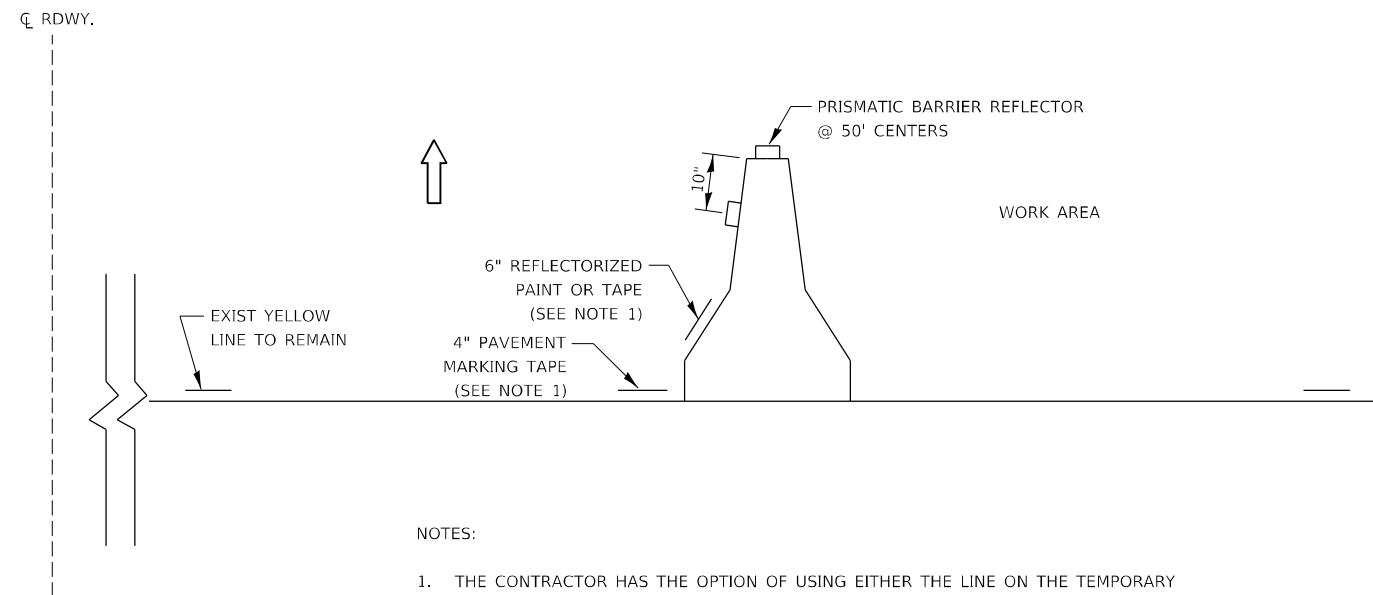
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
 STRUCTURE NO. 050-0173

SHEET 514 OF 514 SHEETS

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-----------|---------|--------------|-----------|
| 39 | (50-5B)-1 | LaSALLE | 42 | 41 |
| CONTRACT NO. 66L74 | | | | |

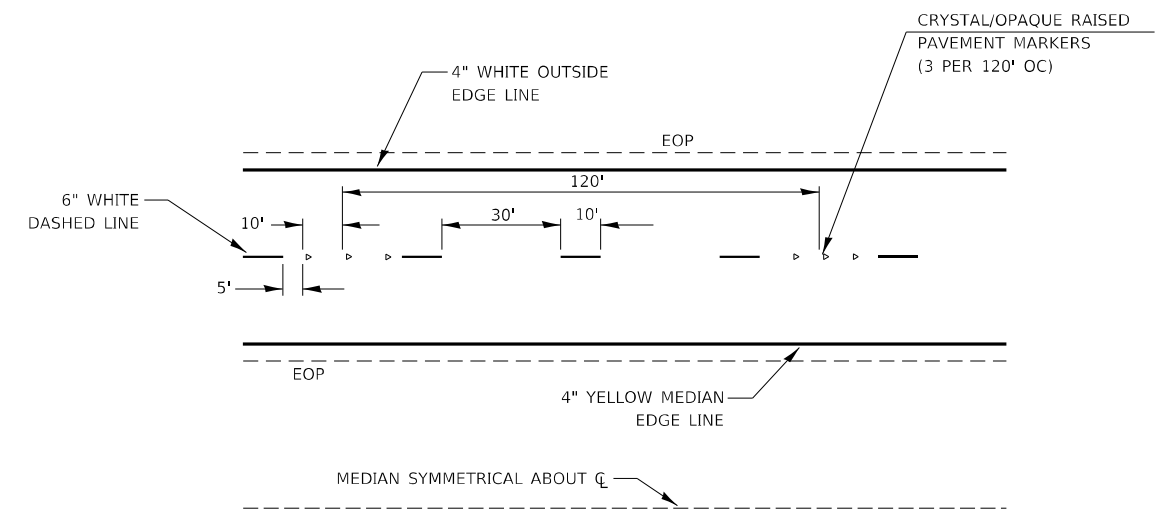
ILLINOIS FED. AID PROJECT



NOTES:

1. THE CONTRACTOR HAS THE OPTION OF USING EITHER THE LINE ON THE TEMPORARY CONCRETE BARRIER OR ON THE PAVEMENT.
2. THE COLOR OF THE REFLECTORS AND PAVEMENT/BARRIER MARKING LINE WILL VARY WITH STAGING AND SHALL MATCH THE EXISTING LINE IN THE WORK AREA.
3. THE COST OF THE REFLECTORS AND THE PAVEMENT/BARRIER MARKING LINE IS INCLUDED IN THE COST OF THE TEMPORARY CONCRETE BARRIER.

TRAFFIC CONTROL DETAIL
FOR TEMPORARY CONCRETE BARRIER



TYPICAL PAVEMENT MARKINGS

MODEL: D:\p4\h...
FILE NAME: V:\1725\acth\17253008\cl\h\drawings\std\0266174-std-std-1.dgn



| | | |
|------------------------------|-------------------|-----------|
| USER NAME = cstanch | DESIGNED - MJV | REVISED - |
| | DRAWN - MJV | REVISED - |
| PLOT SCALE = 100,0000' / in. | CHECKED - DB/RJT | REVISED - |
| PLOT DATE = 1/21/2022 | DATE - 01/21/2022 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 3
STANDARD DETAILS**

SCALE: N.T.S. SHEET 1 OF 1 SHEETS STA. 1210+35.63 TO STA. 1222+53.23

| F.A.J. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------------|---------|--------------|-----------|
| 39 | 50(5A & 5B)BJR,BRR | LASALLE | 42 | 42 |
| CONTRACT NO. 66L74 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |