

Bench Mark: KAM #5 Chiseled "X" on the north sidewalk, north of the east approach slab of the 103rd Street Bridge. Elevation 616.078.  
 Existing Structure: S.N. 016-2127, Built as F.A.I. Route 57, Section 068-2122-B at Station 617+95.79 in 1963.  
 Bituminous Wearing Surface was added at an unknown Date.  
 A 7'-10 1/2" Chain Link Fence Inside each Handrail and Under Deck Lighting Were Added at Unknown Dates.  
 Superstructure: 3 Span Continuous Rolled Steel Beams and Reinforced Slabs, Non Composite in all Spans.  
 Substructure: Closed Abutments and Column Piers on Spread Footings.  
 Length = 171'-1" (Back to Back Abutments), Width = 67'-0" (Out to Out Deck).  
 Contractor to remove the existing deck and replace with new composite deck.  
 Traffic is to be Maintained During the Rehabilitation Utilizing Stage Construction.

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	FEET	SHEET	SHEET NO. / 32 SHEETS
F.A.I. 57	*	COOK	75	29	
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

Contract 60366  
 ★ 2122B

STATION 5+47.40  
 REBUILT BY  
 STATE OF ILLINOIS  
 F.A.I. RT. 57 SECTION 068-2122-B  
 LOADING HS 20  
 STR. NO. 016-2127  
**NAME PLATES**  
 See Std. 515001

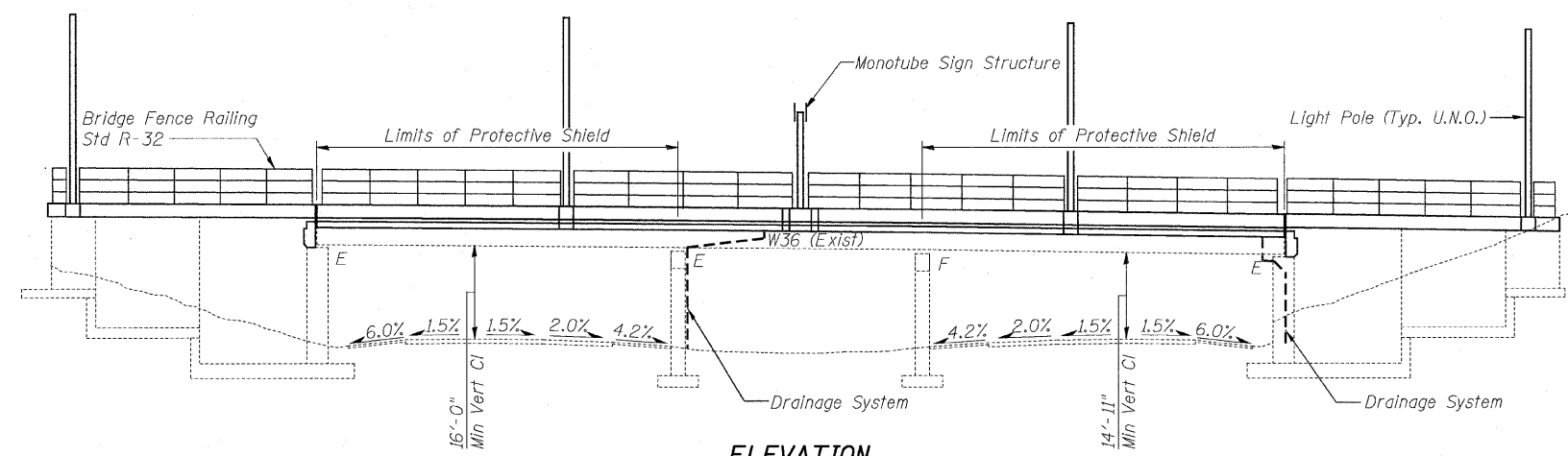
**DESIGN SPECIFICATIONS**  
 AASHTO 2002 Standard Specifications for Highway Bridges

**LOADING HS20-44**  
 Allow 50#/sq ft for future wearing surface.

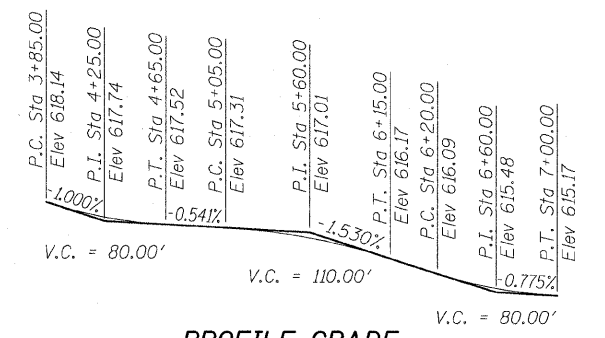
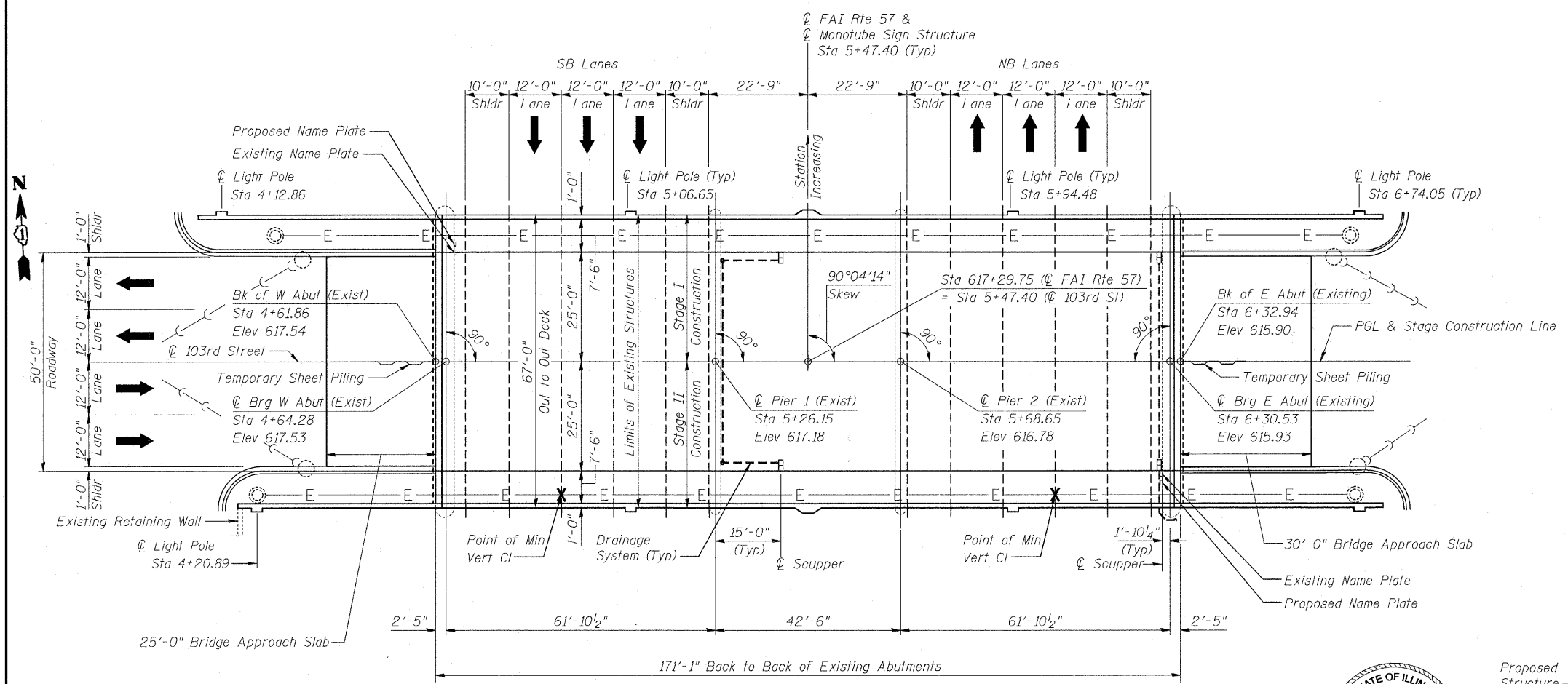
**DESIGN STRESSES**  
 New Construction (Field Units)  
 $f'_c = 3,500$  psi  
 $f_y = 60,000$  psi (Reinforcement)  
 $f_y = 36,000$  psi (Structural Steel) (M270 Grade 36)  
 Existing Construction (Field Units)  
 $f'_c = 1400$  psi (Without Earth Pressure)  
 $f'_c = 800$  psi (With Earth Pressure)  
 $f_s = 20,000$  psi (Reinforcement)  
 $f_y = 36,000$  psi (Structural Steel)

**SEISMIC DATA**  
 Seismic Performance Category (SPC) = A  
 Bedrock Acceleration Coefficient (A) = 0.04  
 Site Coefficient (S) = 1.0

**Notes:**  
 UNO = Unless Noted Otherwise



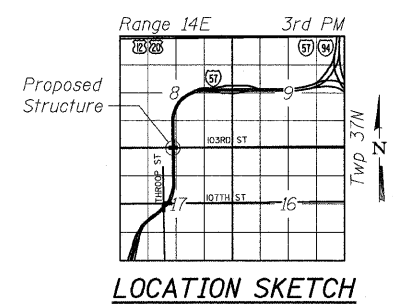
The Name Plates shall be attached to both abutments adjacent to the existing name plates according to Section 515 of the Standard Specifications except that the Name Plate shall be surface mounted using vandal-proof screws.



DESIGNED	NDR
CHECKED	KFA
DRAWN	RD
CHECKED	DSE
DATE	JAN 16, 2009

**APPROVED**  
 FOR STRUCTURAL ADEQUACY ONLY

*Ralph E. Anderson*  
 ENGINEER OF BRIDGES AND STRUCTURES



Illinois Firm Registration No. 184-00533

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**GENERAL PLAN**  
 103RD STREET OVER  
 FAI 57 (DAN RYAN-WEST LEG)  
 FAI RTE 57 SECTION 2122B  
 COOK COUNTY  
 STA 5+47.40  
 STRUCTURE NO 016-2127

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