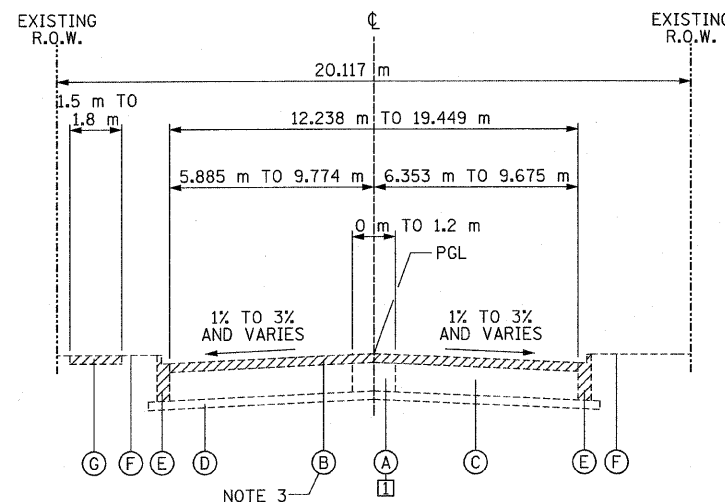


F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	1919.2B	COOK	119	9
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

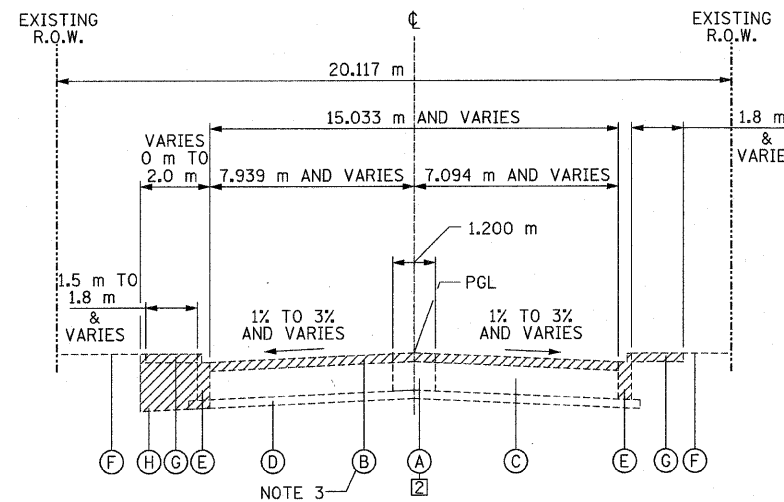
CONTRACT 60371



EXISTING TYPICAL SECTION

115TH STREET
LOOKING EAST
(STA. 2+261.000 TO STA. 2+317.460
AND STA. 2+331.831 TO STA. 2+354.498)
BACK OF WEST ABUTMENT STA. 2+354.404

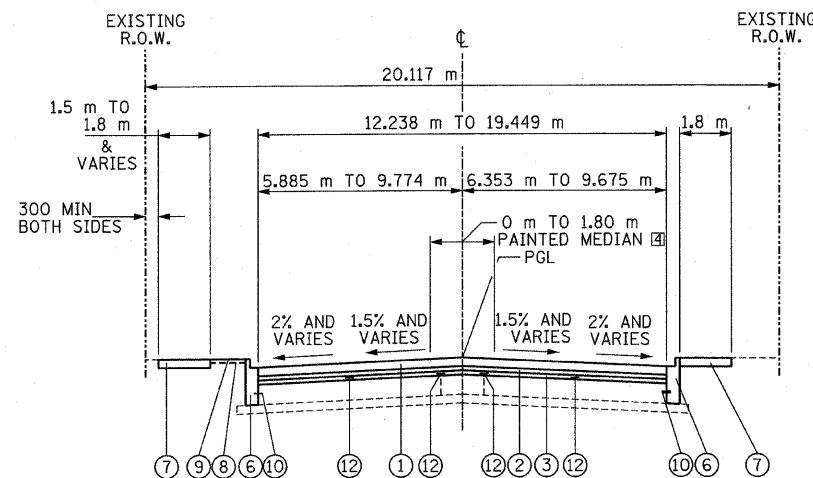
1] STA. 2+295.049 TO STA. 2+324.914 AND
STA. 2+345.927 TO 2+348.402



EXISTING TYPICAL SECTION

115TH STREET
LOOKING EAST
(STA. 2+317.460 TO STA. 2+324.914)

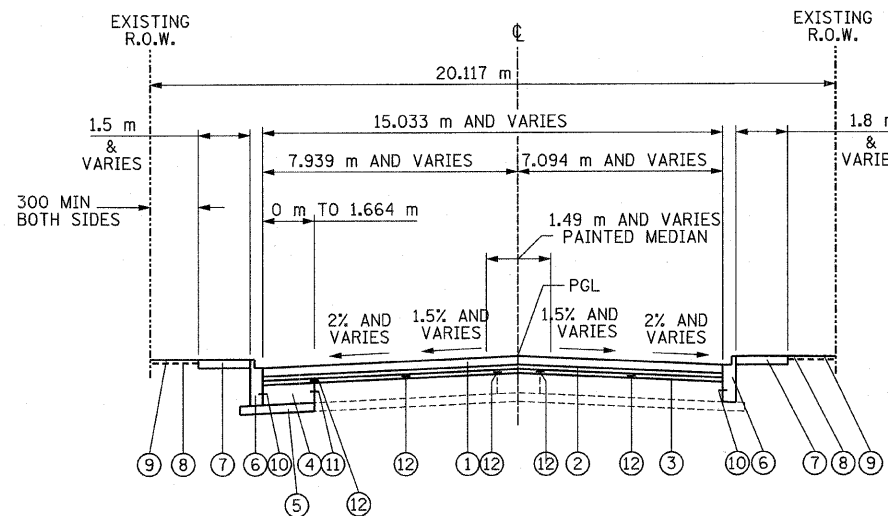
2] STA. 2+317.460 TO STA. 2+324.914



PROPOSED TYPICAL SECTION

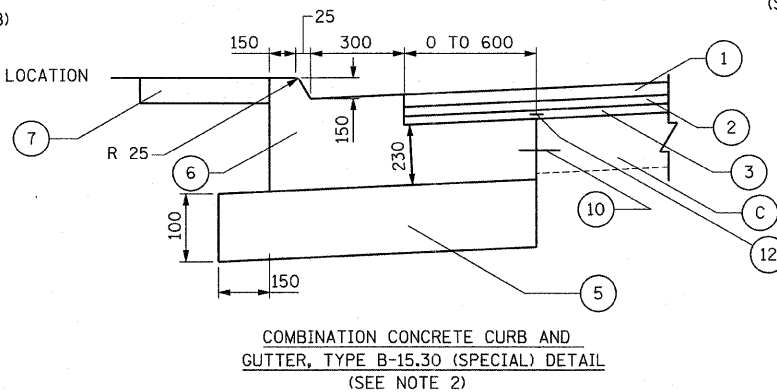
115TH STREET
LOOKING EAST
(STA. 2+261.000 TO STA. 2+317.460
AND STA. 2+331.831 TO STA. 2+354.498)
BACK OF WEST ABUTMENT STA. 2+354.404

4] SEE PAVEMENT MARKING PLAN SHEET FOR LOCATION



PROPOSED TYPICAL SECTION

115TH STREET
LOOKING EAST
(STA. 2+317.460 TO STA. 2+331.831)



COMBINATION CONCRETE CURB AND
GUTTER, TYPE B-15.30 (SPECIAL) DETAIL
(SEE NOTE 2)

EXISTING TYPICAL SECTION LEGEND

- (A) CONCRETE MEDIAN (HAS BEEN SCARIFIED AND OVERLAID WITH HOT-MIX ASPHALT)
- (B) HOT-MIX ASPHALT BINDER AND SURFACE COURSE, 60 mm AND VARIES +/- 20 mm PAID AS HMA SURFACE REMOVAL, 60 mm
- (C) PORTLAND CEMENT CONCRETE BASE 230 AND VARIES
- (D) STABILIZED SUBBASE, 100
- (E) COMBINATION CURB AND GUTTER, TYPE B-15.30
- (F) TOPSOIL AND SODDING - TO BE REPLACED, AS REQUIRED
- (G) PORTLAND CEMENT CONCRETE SIDEWALK
- (H) EARTH EXCAVATION
- (Hatched) REMOVAL ITEM
- (Dashed) EXISTING ITEM

PROPOSED TYPICAL SECTION LEGEND

- 1] HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 38
- 2] POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 (57 TO 206) (WHERE REQUIRED DUE TO RAISE IN PROFILE)
- 3] LEVELING BINDER (MACHINE METHOD), N70 (19 TO 57)
- 4] PORTLAND CEMENT CONCRETE BASE COURSE, 230
- 5] SUBBASE GRANULAR MATERIAL, TYPE B, 100
- 6] COMBINATION CONCRETE C & G, TYPE B-15.30
- 7] PORTLAND CEMENT CONCRETE SIDEWALK, 125
- 8] TOPSOIL FURNISH AND PLACE, 100 - AS REQUIRED
- 9] SODDING, SALT TOLERANT - AS REQUIRED
- 10] NO. 25 EPOXY COATED DEFORMED TIE BARS @ 600 mm C-C, 600 mm LONG, DRILLED AND GROUTED (WHERE REQUIRED). COST INCLUDED IN COMB. CONC. C & G, TYPE B-15.30
- 11] NO. 25 EPOXY COATED DEFORMED TIE BARS @ 600 mm C-C, 600 mm LONG, DRILLED AND GROUTED (WHERE REQUIRED). COST INCLUDED IN PORTLAND CEMENT CONC. BASE COURSE, 230
- 12] STRIP REFLECTIVE CRACK CONTROL TREATMENT (SEE NOTE 1)

- (Solid) PROPOSED ITEM
- (Dashed) EXISTING ITEM

NOTES:

1. ADDITIONAL CRACK CONTROL TREATMENT LOCATIONS MAY BE ADDED AS DIRECTED BY THE ENGINEER.
2. AREAS REQUIRING WIDENING 600 mm OR LESS SHOULD BE POURED MONOLITHICALLY WITH THE PROPOSED CURB AND GUTTER (SEE SPECIAL DETAIL). COMBINATION CONCRETE CURB AND GUTTER TYPE B-15.30 (SPECIAL) WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE MEASURED AND PAID FOR AT THE CONTRACT UNIT PRICE FOR COMBINATION CONCRETE CURB AND GUTTER, TYPE B-15.30, IN ACCORDANCE WITH SECTION 606 OF THE STANDARD SPECIFICATIONS.
3. VARIABLE DEPTH MILLING WILL BE PAID FOR AS HOT-MIX ASPHALT SURFACE REMOVAL 60 mm IN SQUARE METERS. WHEN MILLING THE EXISTING HOT-MIX ASPHALT SURFACE, A MINIMUM OF 38 mm HOT-MIX ASPHALT LAYER SHOULD BE LEFT OVER THE EXISTING PCC PAVEMENT, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. IF EXISTING HOT-MIX ASPHALT SURFACE, WHICH REMAINS AFTER MILLING IS LESS THAN 38 mm, ALL OF THE EXISTING HOT-MIX ASPHALT SURFACING SHALL BE REMOVED TO THE TOP OF THE CONCRETE PAVEMENT.
4. FOR DETAILS ON BRIDGE APPROACH PAVEMENT (SPECIAL) AND BRIDGE APPROACH PAVEMENT CONNECTOR (PCC) SPECIAL, SEE BRIDGE APPROACH PAVEMENT DETAILS SHEETS.
5. CROSS SLOPES WILL TRANSITION FROM ROADWAY CROSS SLOPES TO BRIDGE CROSS SLOPES AT THE BEGINNING AND END OF THE ROADWAY IMPROVEMENTS.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
115TH STREET OVER I-57
DRAWN BY: PRH
CHECKED BY: JMH
DATE: JANUARY 16, 2009

COLLINS ENGINEERS
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ILLINOIS PROFESSIONAL DESIGN FIRM LICENSE NO. 184-000993