

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2 Y-B-I	DU PAGE	34	1
FED. ROAD DIST. NO.	ILLINOIS CONTRACT NO.		60D82	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

FAP 347 / ILLINOIS ROUTE 38
OVER KRESS CREEK
SECTION 2 Y-B-I
BOX CULVERT SLAB OVERLAY
PROJECT: *ESP-0347 (021)*
DU PAGE COUNTY
C-91-116-08

FOR INDEX OF SHEETS SEE SHEET NUMBER 2

THIS IMPROVEMENT IS LOCATED
IN WINFIELD TOWNSHIP

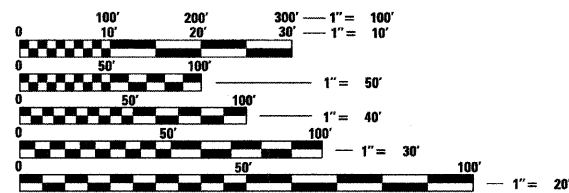
TRAFFIC DATA

2005 ADT - 23700
POSTED SPEED LIMIT - 45 MPH

IMPROVEMENT LOCATION
SN: 022-0150

IL 38 OVER KRESS CREEK

SN:022-0150
DOBLE BARREL 9'-0" x 9'-9"
CONCRETE BOX CULVERT



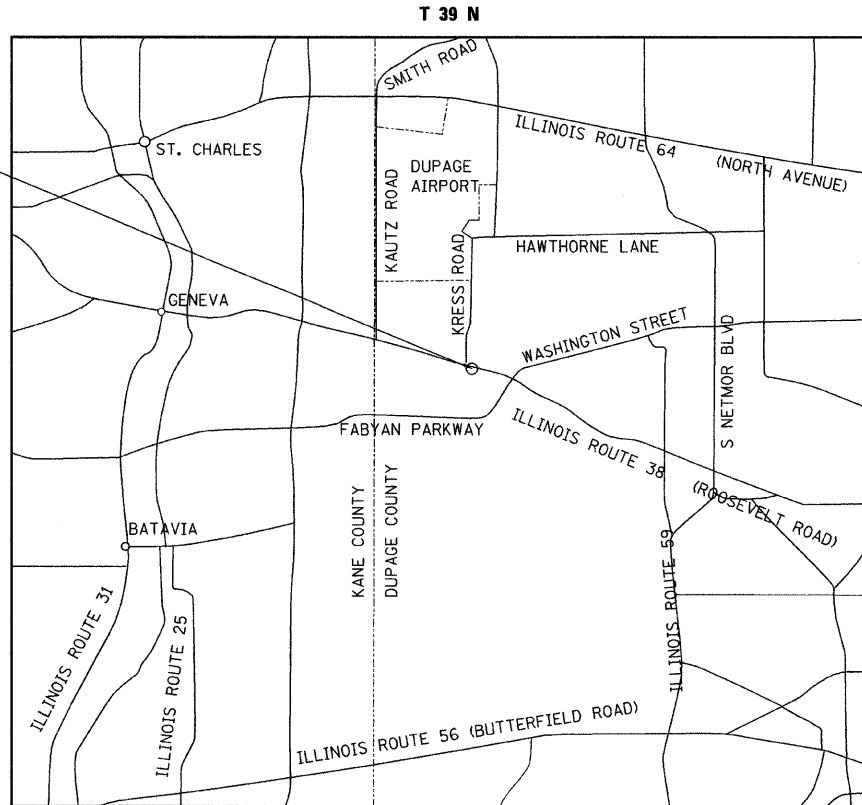
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

Clorba Group, Inc.

DESIGN FIRM
REGISTRATION NUMBER
184-001016

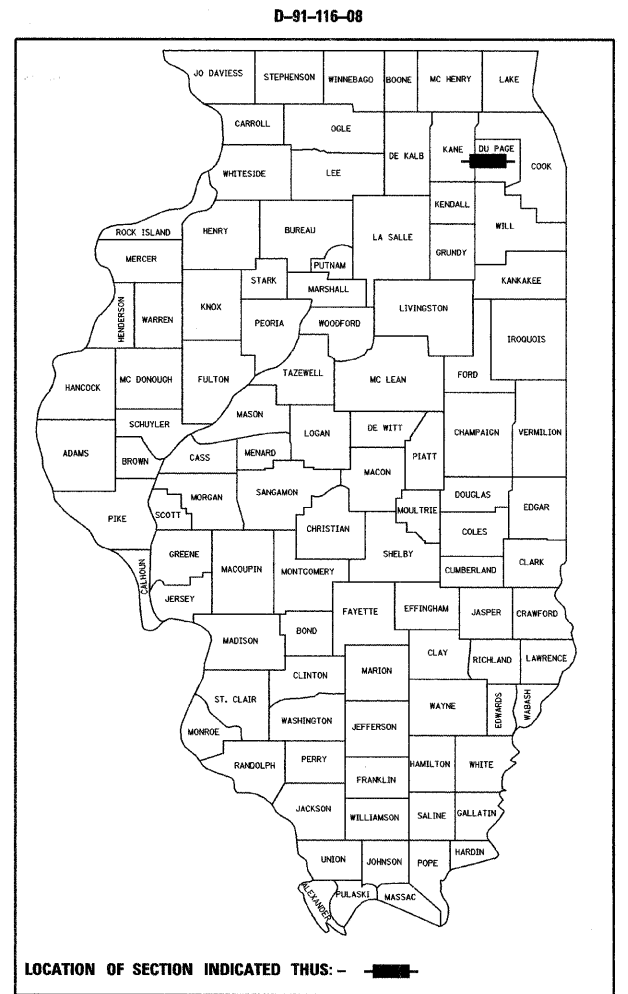
CONSULTING ENGINEERS
SUITE 402, 5507 NORTH CUMBERLAND AVE
CHICAGO, ILLINOIS 60656 ☎ (773) 775-4009



WINFIELD TOWNSHIP

LOCATION MAP
1" = 5000'

GROSS AND NET LENGTH OF PROJECT = 986 FT = 0.19 MI



LOCATION OF SECTION INDICATED THIS: - [black bar] -



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED JANUARY 8, 2009

Diana M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

March 12, 2009
Charles G. Ingersoll
ENGINEER OF DESIGN AND ENVIRONMENT

March 12, 2009
Christine M. Reed
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

DISTRICT 1 DESIGN PLAN PREPARATION ENGINEER: K. ENG (847) 705-4247

CONTRACT NO. 60D82

INDEX OF SHEETS

SHEET NO	DESCRIPTION
1	TITLE
2	INDEX OF SHEETS, GENERAL NOTES AND STATE STANDARDS
3	SUMMARY OF QUANTITIES
4	ALIGNMENT & CONTROL POINTS
5	EXISTING CONDITIONS AND REMOVAL PLAN
6	PROPOSED ROADWAY PLAN
7-8	TRAFFIC CONTROL AND PROTECTION - STAGE 1
9-10	TRAFFIC CONTROL AND PROTECTION - STAGE 2
11-12	PAVEMENT MARKING PLAN
13-26	TEMPORARY SIGNAL PLANS
27	GENERAL PLAN AND ELEVATION
28	TEMPORARY CONCRETE BARRIER
29	DETAILS
30	BUTT JOINT AND HMA TAPER DETAILS (BD-32)
31	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS (TC-10)
32	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT) (TC-11)
33	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)
34	ARTERIAL ROAD INFORMATION SIGN (TC-22)

STATE STANDARDS

000001-05	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
515001-03	NAME PLATE FOR BRIDGES
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
606301-04	PC CONCRETE ISLANDS AND MEDIAN
630001-08	STEEL PLATE BEAM GUARDRAIL
630301-05	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
631032-04	TRAFFIC BARRIER TERMINAL, TYPE 6A
635006-03	REFLECTOR AND TERMINAL MARKER PLACEMENT
635011-02	REFLECTOR MARKER AND MOUNTING DETAILS
701301-03	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701431-05	LANE CLOSURE, MULTILANE, UNDIV. WITH CROSSOVER, FOR SPEEDS > 45 MPH TO 55 MPH
701701-06	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-01	TRAFFIC CONTROL DEVICES
704001-05	TEMPORARY CONCRETE BARRIER
805001-01	ELECTRICAL SERVICE INSTALLATION DETAILS
857001-01	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
880001-01	SPAN WIRE MOUNTED SIGNAL AND FLASHING LIGHT BEACON INSTALLATION

GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- WHEN ARTIFICIAL LIGHTING IS USED IN NIGHT OPERATIONS THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OUTSIDE THE PAVEMENT MARKING LIMITS SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- TWO WEEKS PRIOR TO PLACING PERMANENT PAVEMENT MARKINGS, CONTACT MS. CORA MATHIS, AREA TRAFFIC FIELD ENGINEER AT (815) 485-6475.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TRAFFIC CONTROL DEVICES.
- CHANGEABLE MESSAGE SIGNS SHALL BE PLACED IN ADVANCE OF THE PROJECT LIMITS AT LEAST ONE WEEK PRIOR TO LANE CLOSURE AND TEMPORARY TRAFFIC SIGNAL OPERATIONS.

COMMITMENTS

NONE

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

OPERATIONS	MIXTURE TYPE	AC TYPE	PERCENT AIR VOIDS
ROADWAY RESURFACING	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5 mm), 1 1/2"	PG 64-22	4% @ 70 GYR
	LEVELING BINDER (MACHINE METHOD), N70 (IL-9.5mm), 3/4" MIN.	PG 64-22*	4% @ 70 GYR
HOT-MIX ASPHALT MEDIAN	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5 mm), 1 1/2"	PG 64-22	4% @ 70 GYR
	HOT-MIX ASPHALT BASE COURSE, (HMA BINDER IL 19 mm), 2 1/2"	PG 64-22*	4% @ 70 GYR
TEMPORARY PAVEMENT, 10"	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5 mm), 1 1/2"	PG 64-22	4% @ 50 GYR
	HOT-MIX ASPHALT BINDER COURSE, IL 19.0 mm, N50, 8 1/2"	PG 64-22*	4% @ 50 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

- WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22.

FILE NAME = N:\PROJ\3329\3329_29\Design\ProposedPlan\3329_29-02-index.dgn



Ciorba Group, Inc.
CONSULTING ENGINEERS
5507 North Cumberland Avenue, Suite 402
Chicago, Illinois 60656
Tel. 773.775.4009 Fax 773.775.4014

USER NAME = mready	DESIGNED - JCC	REVISED -
	DRAWN - JCC	REVISED -
PLOT SCALE = 1:8000' / IN.	CHECKED - MJL	REVISED -
PLOT DATE = 2/11/2009	DATE - 12/30/07	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAP 347 / ILLINOIS ROUTE 38 (ROOSEVELT ROAD)
OVER KRESS CREEK
INDEX OF SHEETS, STATE STANDARDS, & GENERAL NOTES**

F.A.P. RTE. 347	SECTION 2 Y-B-I	COUNTY	TOTAL SHEETS 34	SHEET NO. 2
SCALE: SHEET NO. OF SHEETS STA. TO STA.			CONTRACT NO. 608B2	
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT				

SUMMARY OF QUANTITIES			URBAN 100% FED. TOTAL QUANTITY	CONSTRUCTION CODE	
CODE NO.	DESCRIPTION	UNIT		ROADWAY 1000-2A	BRIDGE Y007
20200100	EARTH EXCAVATION	CU YD	120	120	
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	240	240	
25000210	SEEDING, CLASS 2A	ACRE	0.1	0.1	
25100630	EROSION CONTROL BLANKET	SQ YD	470	470	
31101600	SUB-BASE GRANULAR MATERIAL, TYPE B 8"	SQ YD	350	350	
35501288	HOT-MIX ASPHALT BASE COURSE, 2 1/2"	SQ YD	220	220	
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	95	95	
40600300	AGGREGATE (PRIME COAT)	TON	2	2	
40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	60	60	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	65	65	
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	125	125	
42001300	PROTECTIVE COAT	SQ YD	182		182
44000156	HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"	SQ YD	650	650	
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1,010	1,010	
44003100	MEDIAN REMOVAL	SQ FT	3,120	3,120	
44003510	MEDIAN REMOVAL PARTIAL DEPTH	SQ FT	203		203
48101500	AGGREGATE SHOULDERS, TYPE B 6"	SQ YD	220	220	
50300255	CONCRETE SUPERSTRUCTURE	CU YD	3.3		3.3
50300260	BRIDGE DECK GROOVING	SQ YD	149		149
60260100	INLETS TO BE ADJUSTED	EACH	2	2	
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	125	125	
60608300	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12	FOOT	990	990	
* 63000000	STEEL PLATE BEAM GUARD RAIL, TYPE A	FOOT	63	63	
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6A	EACH	4	4	
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	4	4	
63200310	GUARDRAIL REMOVAL	FOOT	420	420	
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	4	1	3
67100100	MOBILIZATION	L SUM	1	0.2	0.8
70101800	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	0.2	0.8
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	90	18	72
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	8	8	

* SPECIALTY ITEM

SUMMARY OF QUANTITIES			URBAN 100% FED. TOTAL QUANTITY	CONSTRUCTION CODE	
CODE NO.	DESCRIPTION	UNIT		ROADWAY 1000-2A	BRIDGE Y007
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	4,450	4,450	
70400100	TEMPORARY CONCRETE BARRIER	FOOT	375	375	
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	238	238	
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	75	75	
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	13,100	13,100	
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	380	380	
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	65	65	
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	30	30	
* 78100300	REPLACEMENT REFLECTOR	EACH	180	180	
* 78200450	MONODIRECTIONAL GUARDRAIL REFLECTORS	EACH	8	8	
* 78200530	BARRIER WALL MARKERS, TYPE C	EACH	25	25	
* 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	4	4	
78300100	PAVEMENT MARKING REMOVAL	SQ FT	4,800	4,800	
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	30	30	
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2	2	
89000100	TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1	1	
X0322050	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL	EACH	180	180	
X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	52	52	
X0322489	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 1/2 INCHES	SQ YD	149		149
X0325239	TEMPORARY PAVEMENT 10"	SQ YD	160	160	
X0325305	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	8		8
X0325775	WET REFLECTIVE TEMPORARY TAPE, TYPE III, 4 INCH	FOOT	12,000	12,000	
X0325837	WET REFLECTIVE TEMPORARY TAPE TYPE III, 6 INCH	FOOT	410	410	
X0325841	WET REFLECTIVE TEMPORARY TAPE, TYPE III, 24 INCH	FOOT	65	65	
X0325842	WET REFLECTIVE TEMPORARY TAPE, TYPE III, LETTERS AND SYMBOLS	SQ FT	73	73	
Z0001700	APPROACH SLAB REPAIR (FULL DEPTH)	SQ YD	1		1
Z0006204	BRIDGE DECK HYDRO-SCARIFICATION 1/2"	SQ YD	414		414
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	0.2	0.8
Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	7.3		7.3
Z0030250	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2	2	
Z0030350	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2	2	

FILE NAME = N:\PROJ\3329\3329_2\Design\ProposedPlan\3329_2-9-03-road.dgn



Ciorba Group, Inc.
CONSULTING ENGINEERS
5507 North Cumberland Avenue, Suite 402
Chicago, Illinois 60656
Tel. 773.775.4009 Fax 773.775.4014

USER NAME = wjanoaster	DESIGNED - JCC	REVISED -
PLOT SCALE = 1:8000' / IN.	DRAWN - JCC	REVISED -
PLOT DATE = 1/13/2009	CHECKED - MJL	REVISED -
	DATE - 12/30/07	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAP 347 / ILLINOIS ROUTE 38 (ROOSEVELT ROAD)
OVER KRESS CREEK
SUMMARY OF QUANTITIES**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

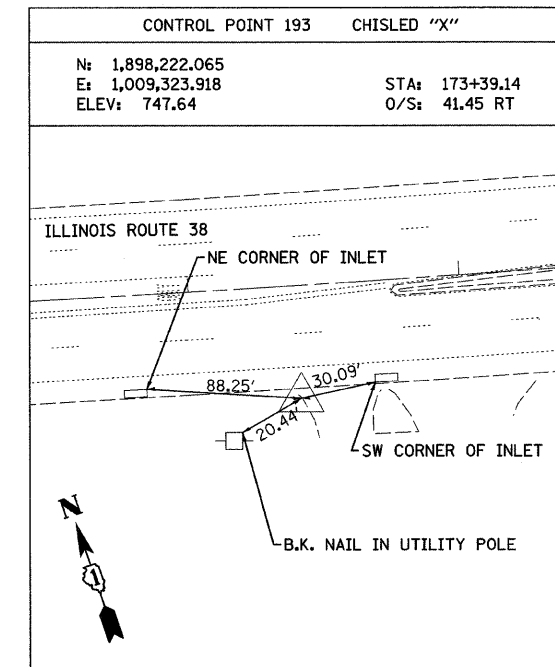
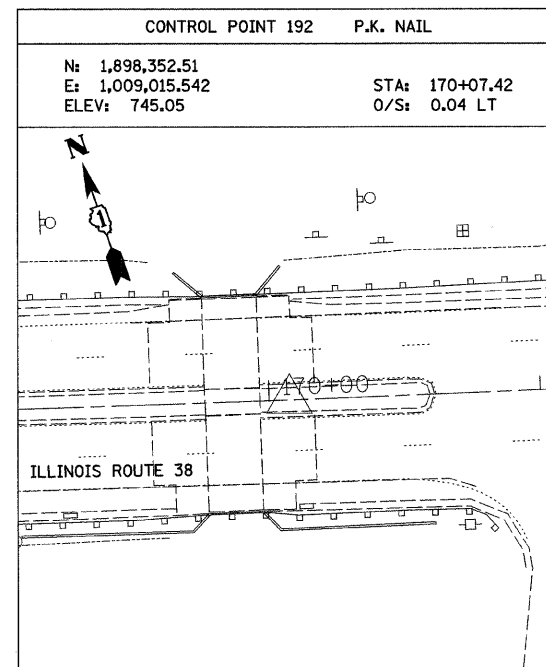
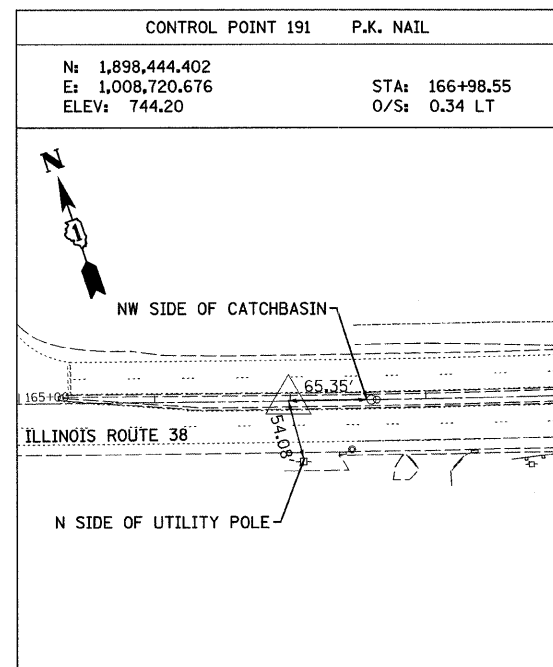
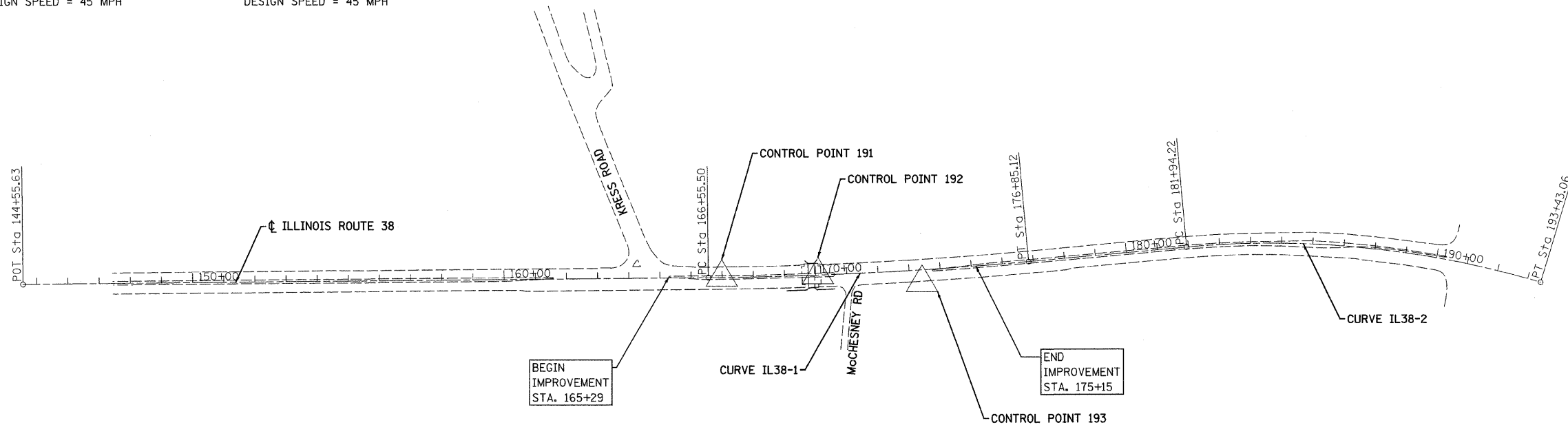
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2 Y-B-1	DU PAGE	34	3
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT			CONTRACT NO. 60D82	

EXIST. CURVE IL38-1
 PI STA. = 171+70.59
 $\Delta = 4^{\circ} 38' 00''$ (LT)
 $D = 0^{\circ} 27' 00''$
 $R = 12,732.40'$
 $T = 515.09'$
 $L = 1,029.62'$
 $E = 10.41'$
 P.C. STA. = 166+55.50
 P.T. STA. = 176+85.12
 DESIGN SPEED = 45 MPH

EXIST. CURVE IL38-2
 PI STA. = 187+75.76
 $\Delta = 21^{\circ} 56' 29''$ (RT)
 $D = 1^{\circ} 54' 35''$
 $R = 3,000.00'$
 $T = 581.55'$
 $L = 1,148.84'$
 $E = 55.85'$
 P.C. STA. = 181+94.22
 P.T. STA. = 193+43.06
 DESIGN SPEED = 45 MPH

ILLINOIS ROUTE 38 CENTERLINE			
DESCRIPTION	NORTHING	EASTING	
P.O.T.	144+55.63	1,899,142.397	1,006,589.128
P.C.	166+55.50	1,898,457.416	1,008,679.637
P.T.	176+85.12	1,898,176.708	1,009,669.964
P.C.	181+94.22	1,898,057.787	1,010,164.974
P.T.	193+43.06	1,897,584.649	1,011,204.173

BENCHMARK
 RAILROAD SPIKE IN NORTH FACE OF THIRD UTILITY
 POLE EAST OF STRUCTURE * 022-0150 ELEV. 749.66



FILE NAME = n:\proj\3329\3329_25\design\proposals\plan\3329_28-64-01\plan.dgn

Ciorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60656
 Tel 773.775.4009 Fax 773.775.4014

USER NAME = w\lancaster	DESIGNED - JCC	REVISED -
PLOT SCALE = 200.0000' / IN.	DRAWN - JCC	REVISED -
PLOT DATE = 1/13/2009	CHECKED - MJL	REVISED -
	DATE - 12/30/07	REVISED -

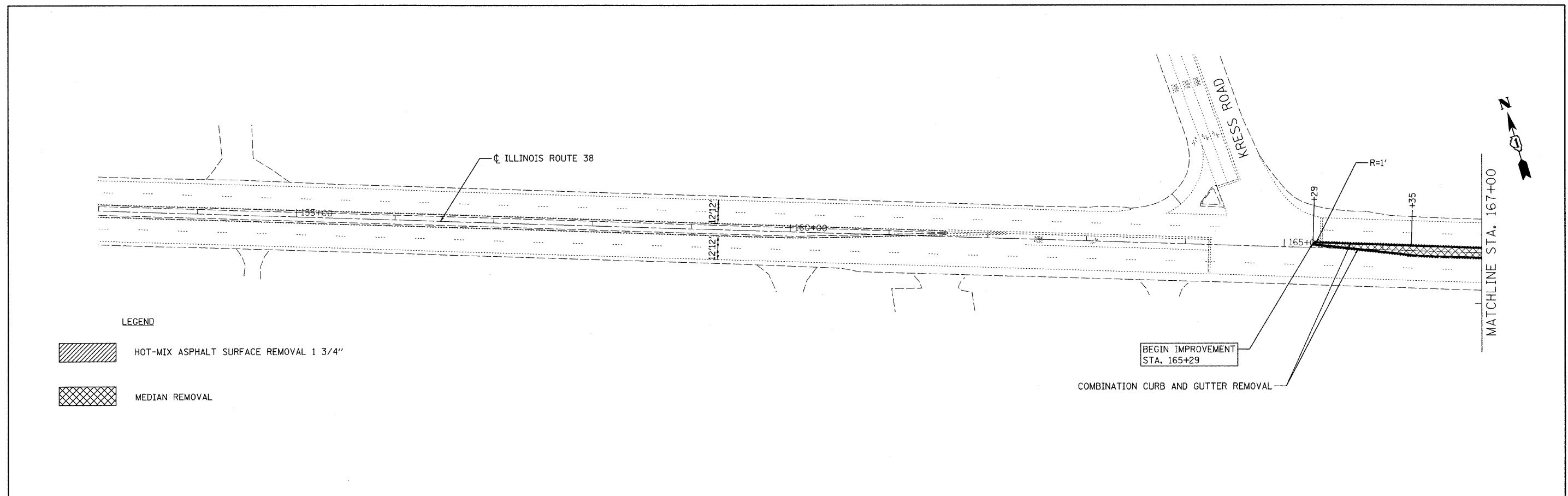
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAP 347 / ILLINOIS ROUTE 38 (ROOSEVELT ROAD)
 OVER KRESS CREEK
 ALIGNMENT AND CONTROL POINTS**

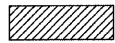

SCALE: 1"=200' SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE. 347	SECTION 2 Y-B-I	COUNTY	TOTAL SHEETS 34	SHEET NO. 4
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

CONTRACT NO. 60D82



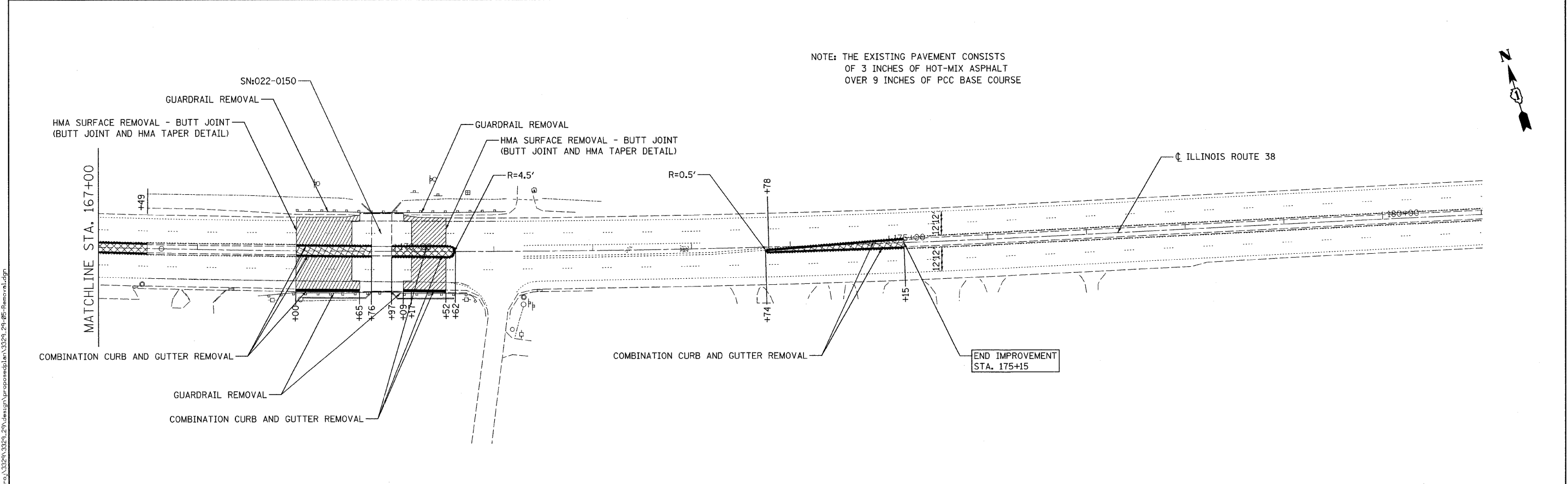
LEGEND

-  HOT-MIX ASPHALT SURFACE REMOVAL 1 3/4"
-  MEDIAN REMOVAL

BEGIN IMPROVEMENT
STA. 165+29

COMBINATION CURB AND GUTTER REMOVAL

NOTE: THE EXISTING PAVEMENT CONSISTS
OF 3 INCHES OF HOT-MIX ASPHALT
OVER 9 INCHES OF PCC BASE COURSE



FILE NAME = n:\pro\3329\3329.dgn

Ciorba Group, Inc.
CONSULTING ENGINEERS
5507 North Cumberland Avenue, Suite 402
Chicago, Illinois 60656
Tel. 773.775.4000 Fax 773.775.4014

USER NAME = wlcaster	DESIGNED - JCC	REVISED -
PLLOT SCALE = 50.0000' / IN.	DRAWN - JCC	REVISED -
PLLOT DATE = 1/13/2009	CHECKED - MJL	REVISED -
	DATE - 12/30/07	REVISED -

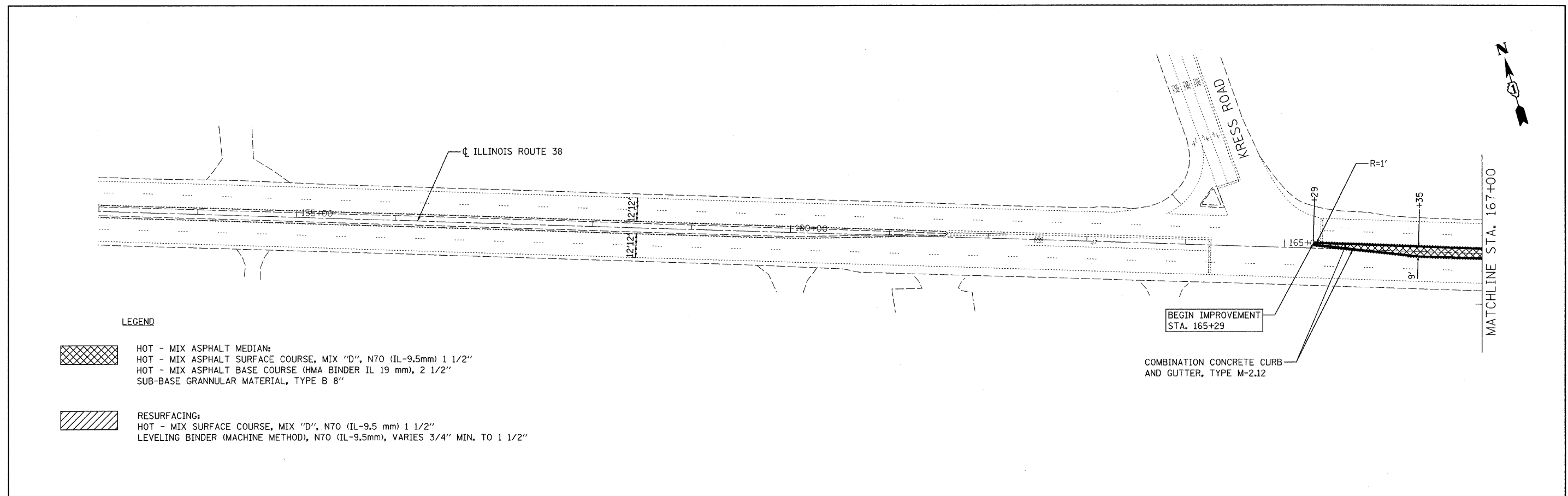
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAP 347 / ILLINOIS ROUTE 38 (ROOSEVELT ROAD)
OVER KRESS CREEK
EXISTING CONDITIONS AND REMOVAL PLAN**

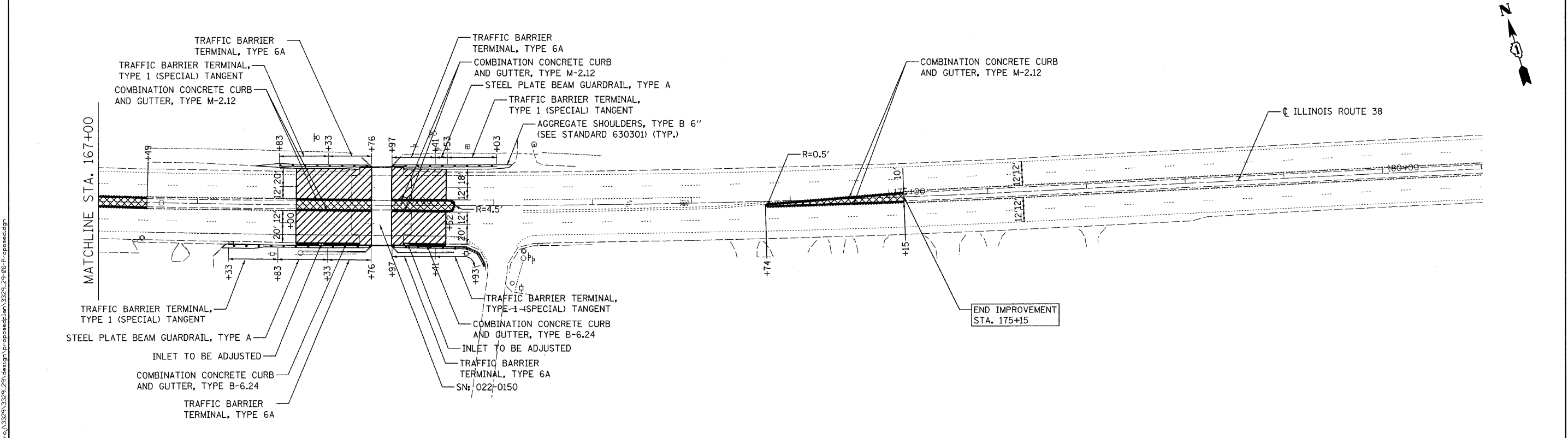
SCALE: 1"=50' SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE. 347	SECTION 2 Y-B-I	COUNTY	TOTAL SHEETS 34	SHEET NO. 5
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

CONTRACT NO. 60D82



NOTE: SEE PROFILE GRADE ON SHEET NO. 27



Ciorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60650
 Tel: 773.775.4099 Fax: 773.775.4014

USER NAME = wlanester	DESIGNED - JCC	REVISIONS -
PLOT SCALE = 50,0000' / IN.	DRAWN - JCC	REVISIONS -
PLOT DATE = 1/13/2009	CHECKED - MJL	REVISIONS -
	DATE - 12/30/07	REVISIONS -

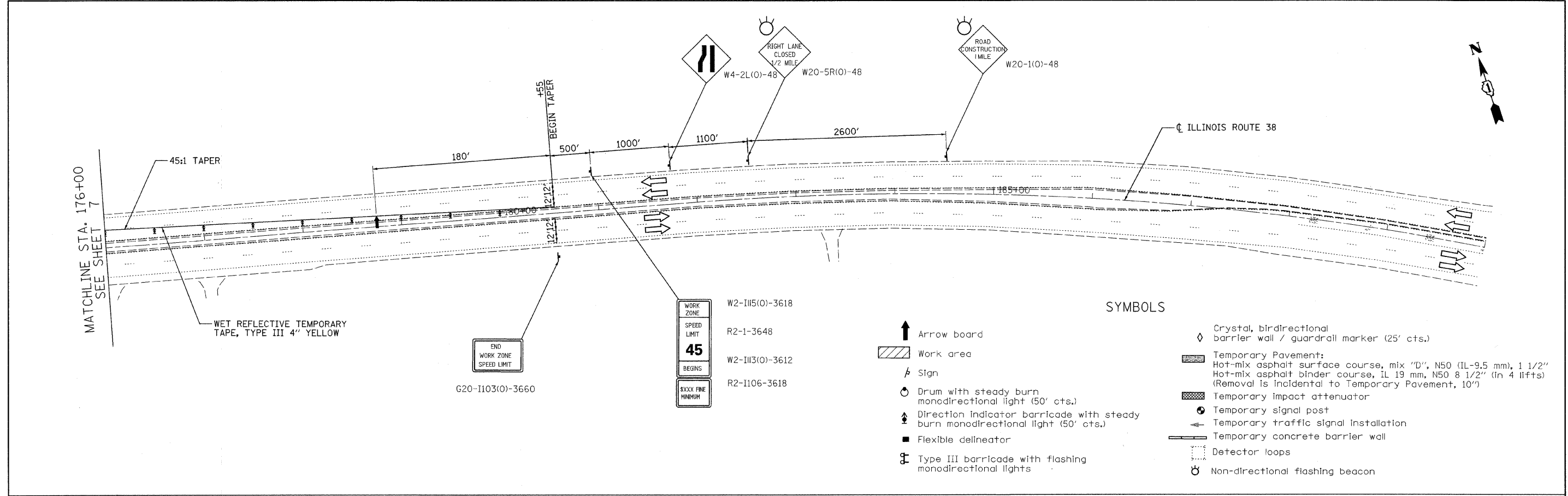
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAP 347 / ILLINOIS ROUTE 38 (ROOSEVELT ROAD)
 OVER KRESS CREEK
 PROPOSED ROADWAY PLAN**

SCALE: 1"=50' SHEET NO. OF SHEETS STA. TO STA.

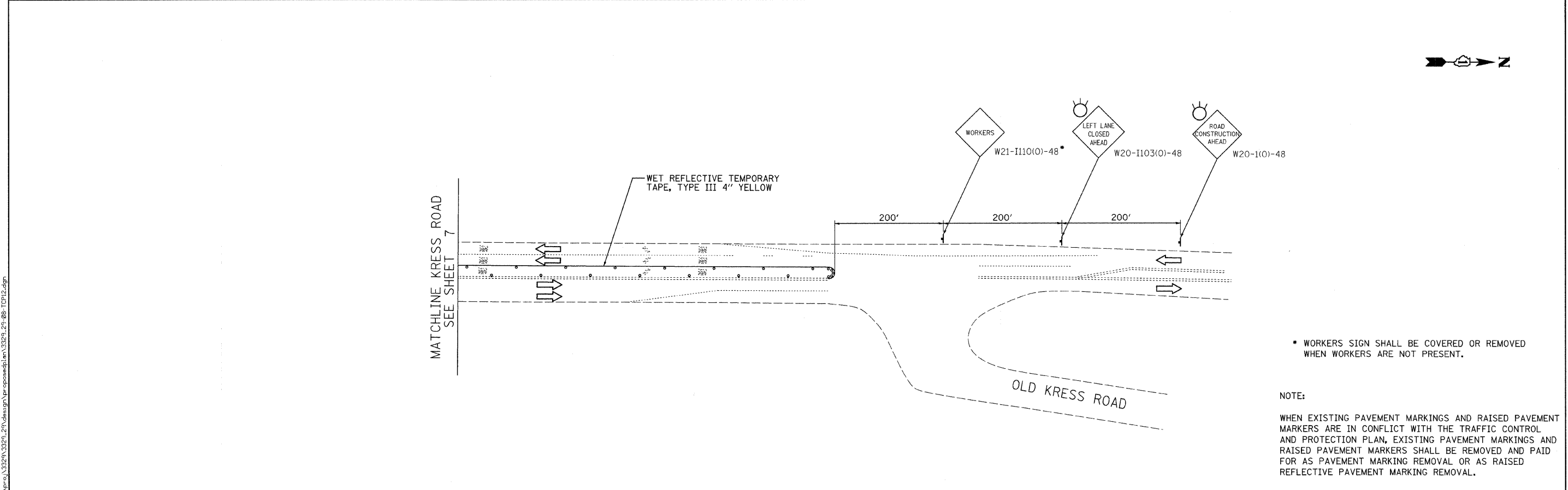
F.A.P. RTE. 347	SECTION 2 Y-B-I	COUNTY	TOTAL SHEETS 34	SHEET NO. 6
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		CONTRACT NO. 60D82		

FILE NAME = n:\p\ro\3329\3329_20\design\proposed\en\3329_20_06-Proposed.dgn



SYMBOLS

- ↑ Arrow board
- ▨ Work area
- ♠ Sign
- ⊙ Drum with steady burn monodirectional light (50' cts.)
- ↑ Direction Indicator barricade with steady burn monodirectional light (50' cts.)
- Flexible delineator
- ⊕ Type III barricade with flashing monodirectional lights
- ◇ Crystal, bidirectional barrier wall / guardrail marker (25' cts.)
- ▨ Temporary Pavement: Hot-mix asphalt surface course, mix "D", N50 (IL-9.5 mm), 1 1/2" Hot-mix asphalt binder course, IL 19 mm, N50 8 1/2" (in 4 lifts) (Removal is incidental to Temporary Pavement, 10")
- ▨ Temporary impact attenuator
- ⊙ Temporary signal post
- ↑ Temporary traffic signal installation
- ▬ Temporary concrete barrier wall
- Detector loops
- ⊙ Non-directional flashing beacon



* WORKERS SIGN SHALL BE COVERED OR REMOVED WHEN WORKERS ARE NOT PRESENT.

NOTE:
WHEN EXISTING PAVEMENT MARKINGS AND RAISED PAVEMENT MARKERS ARE IN CONFLICT WITH THE TRAFFIC CONTROL AND PROTECTION PLAN, EXISTING PAVEMENT MARKINGS AND RAISED PAVEMENT MARKERS SHALL BE REMOVED AND PAID FOR AS PAVEMENT MARKING REMOVAL OR AS RAISED REFLECTIVE PAVEMENT MARKING REMOVAL.

FILE NAME = n:\pro\3320\3320.dwg

Ciorba Group, Inc.
CONSULTING ENGINEERS
5507 North Cumberland Avenue, Suite 402
Chicago, Illinois 60650
Tel: 773.775.4009 Fax: 773.775.4014

USER NAME = wlanaster	DESIGNED - JCC	REVISED -
PLOT SCALE = 50.0000' / IN.	DRAWN - JCC	REVISED -
PLOT DATE = 1/13/2009	CHECKED - MJL	REVISED -
	DATE - 12/30/07	REVISED -

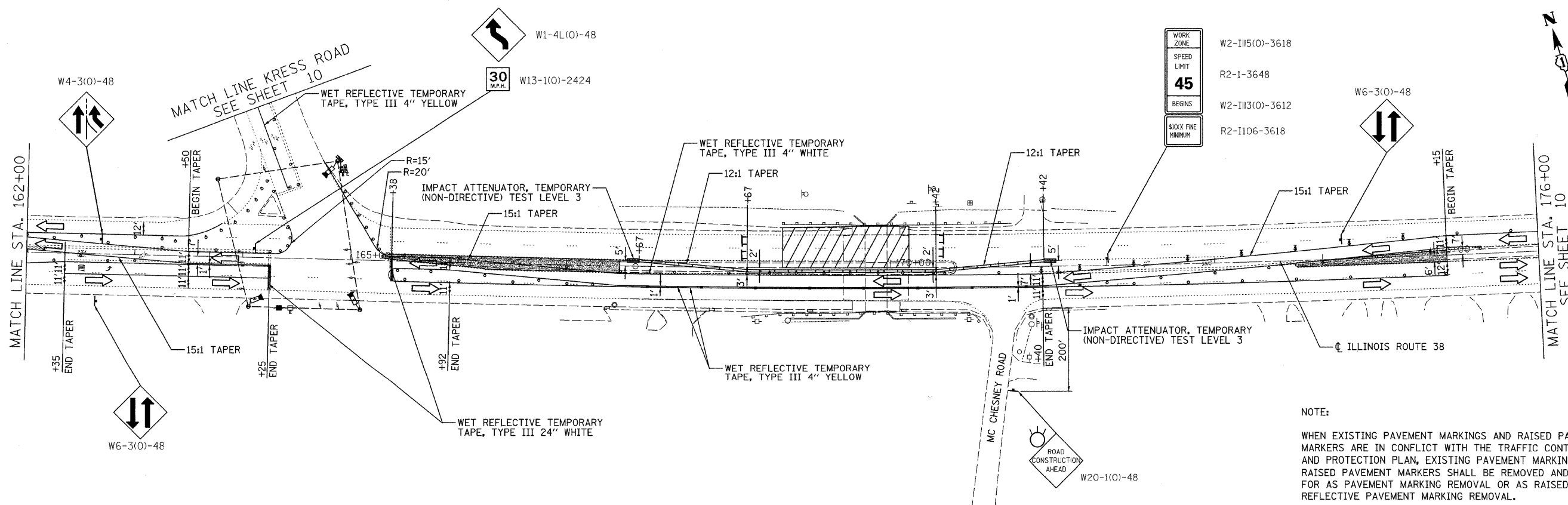
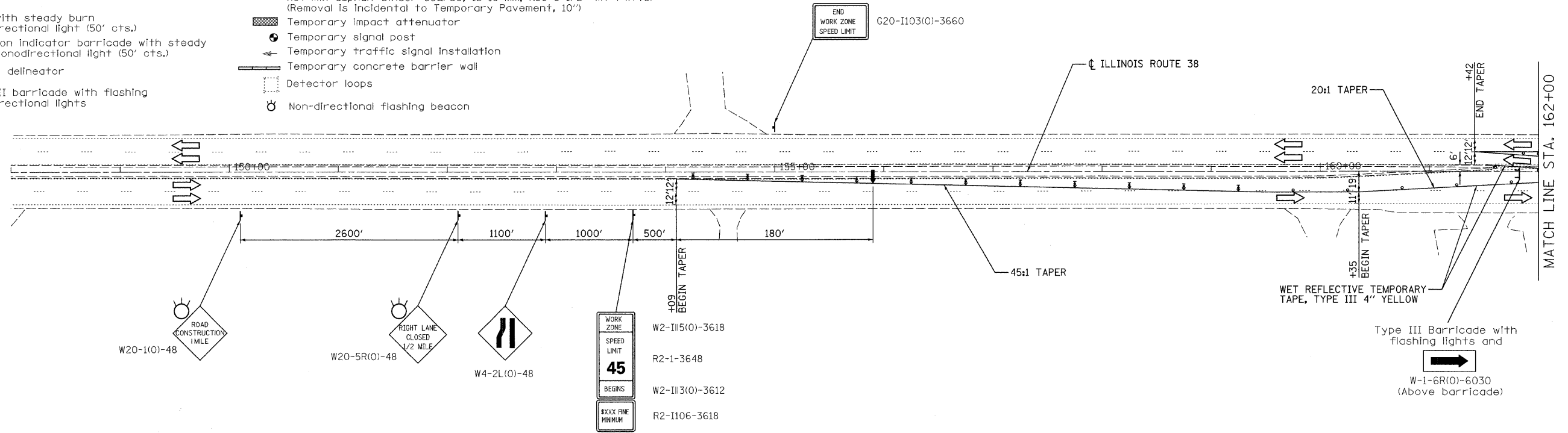
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAP 347 / ILLINOIS ROUTE 38 (ROOSEVELT ROAD)
OVER KRESS CREEK
TRAFFIC CONTROL AND PROTECTION - STAGE 1**

F.A.P. RTE. 347	SECTION 2 Y-B-I	COUNTY DU PAGE	TOTAL SHEETS 34	SHEET NO. 8
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT				

SYMBOLS

- ↑ Arrow board
- ▨ Work area
- ♣ Sign
- Drum with steady burn monodirectional light (50' cts.)
- ↑ Direction indicator barricade with steady burn monodirectional light (50' cts.)
- Flexible delineator
- ⊕ Type III barricade with flashing monodirectional lights
- ◇ Crystal, bidirectional barrier wall / guardrail marker (25' cts.)
- Temporary Pavement:
Hot-mix asphalt surface course, mix "D", N50 (IL-9.5 mm), 1 1/2"
Hot-mix asphalt binder course, IL 19 mm, N50 8 1/2" (In 4 lifts)
(Removal is incidental to Temporary Pavement, 10")
- Temporary impact attenuator
- Temporary signal post
- Temporary traffic signal installation
- Temporary concrete barrier wall
- Detector loops
- Non-directional flashing beacon



NOTE:
WHEN EXISTING PAVEMENT MARKINGS AND RAISED PAVEMENT MARKERS ARE IN CONFLICT WITH THE TRAFFIC CONTROL AND PROTECTION PLAN, EXISTING PAVEMENT MARKINGS AND RAISED PAVEMENT MARKERS SHALL BE REMOVED AND PAID FOR AS PAVEMENT MARKING REMOVAL OR AS RAISED REFLECTIVE PAVEMENT MARKING REMOVAL.

FILE NAME = n:\proj\3323\3323_21\design\proposed\om\3323_29-89-TCP21.dgn

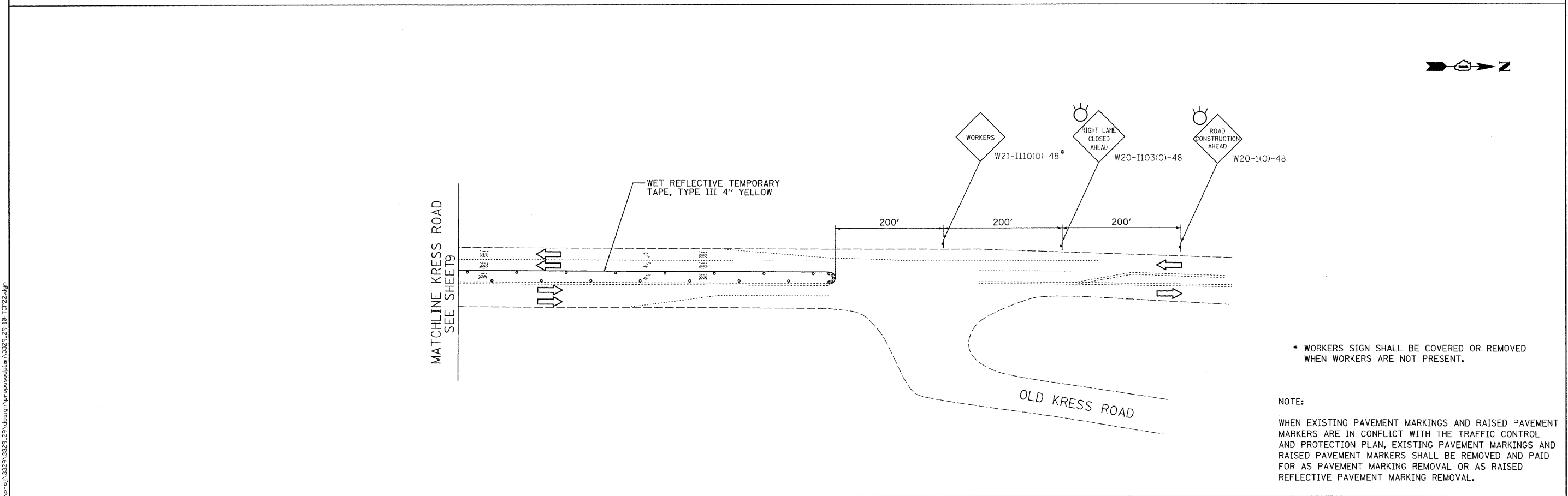
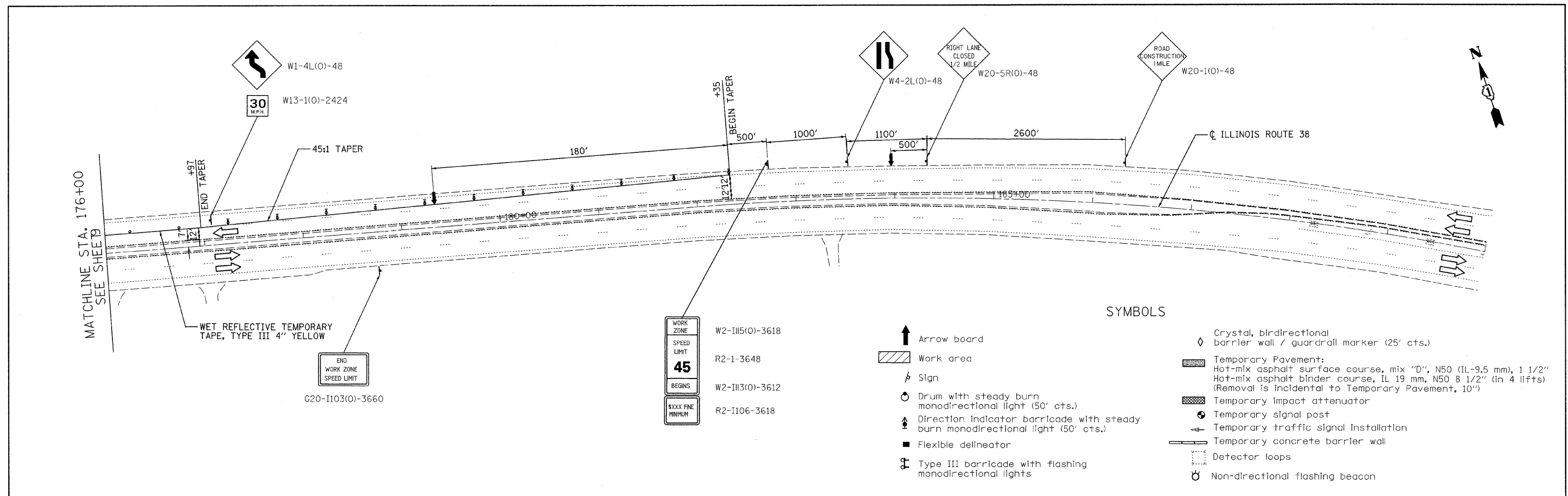
Ciorba Group, Inc.
CONSULTING ENGINEERS
5507 North Cumberland Avenue, Suite 402
Chicago, Illinois 60656
Tel 773.775.4009 Fax 773.775.4014

USER NAME = wjancaster	DESIGNED - JCC	REVISED -
PLOT SCALE = 50.0000' / IN.	DRAWN - JCC	REVISED -
PLOT DATE = 1/13/2009	CHECKED - MJL	REVISED -
	DATE - 12/30/07	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAP 347 / ILLINOIS ROUTE 38 (ROOSEVELT ROAD)
OVER KRESS CREEK
TRAFFIC CONTROL AND PROTECTION - STAGE 2**

F.A.P. RTE. 347	SECTION 2 Y-B-I	COUNTY	TOTAL SHEETS 34	SHEET NO. 9
SCALE: SHEET NO. OF SHEETS STA. TO STA.		CONTRACT NO. 60D82		
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT				



* WORKERS SIGN SHALL BE COVERED OR REMOVED WHEN WORKERS ARE NOT PRESENT.

NOTE:
WHEN EXISTING PAVEMENT MARKINGS AND RAISED PAVEMENT MARKERS ARE IN CONFLICT WITH THE TRAFFIC CONTROL AND PROTECTION PLAN, EXISTING PAVEMENT MARKINGS AND RAISED PAVEMENT MARKERS SHALL BE REMOVED AND PAID FOR AS PAVEMENT MARKING REMOVAL OR AS RAISED REFLECTIVE PAVEMENT MARKING REMOVAL.

FILE NAME = c:\p\proj\3329\3329_21\design\proposed\plan\3329_29-18-TCP22.dgn

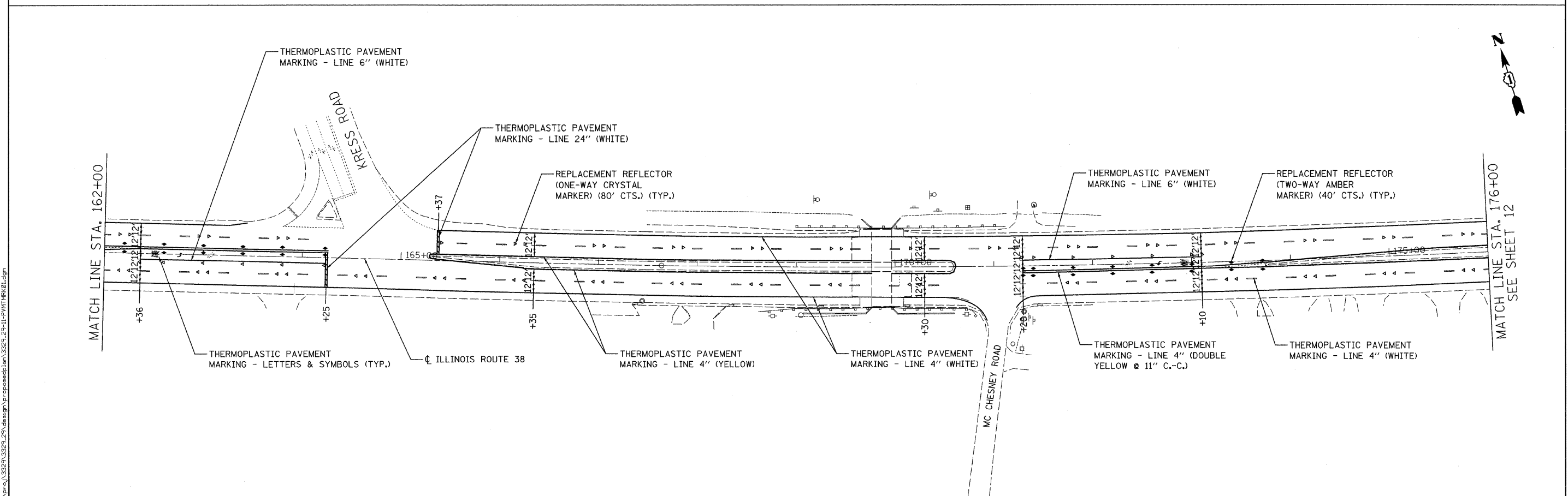
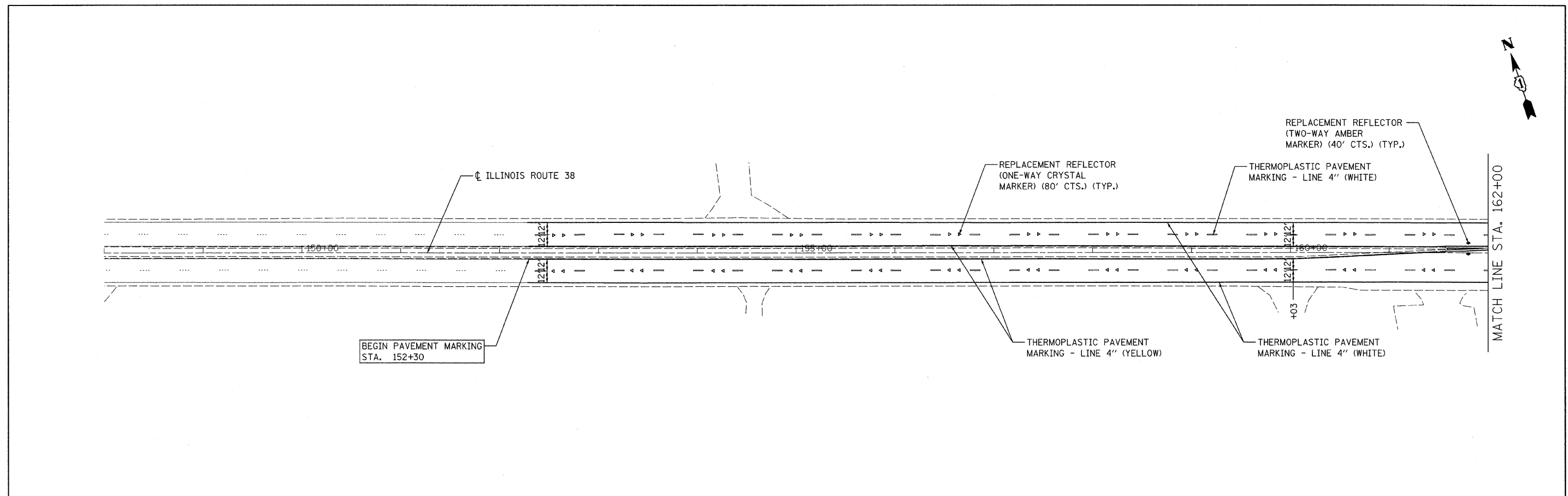
Ciorba Group, Inc.
CONSULTING ENGINEERS
5507 North Cumberland Avenue, Suite 402
Chicago, Illinois 60656
Tel. 773.775.4009 Fax 773.775.4014

USER NAME = wlancastr	DESIGNED - JCC	REVISED -
PLOT SCALE = 50.0000' / IN.	DRAWN - JCC	REVISED -
PLOT DATE = 1/13/2009	CHECKED - MJL	REVISED -
	DATE - 12/30/07	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAP 347 / ILLINOIS ROUTE 38 (ROOSEVELT ROAD)
OVER KRESS CREEK
TRAFFIC CONTROL AND PROTECTION - STAGE 2**

F.A.P. RTE. 347	SECTION 2 Y-B-1	COUNTY DU PAGE	TOTAL SHEETS 34	SHEET NO. 10
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



FILE NAME = n:\proj\332\3320_2\1\design\proposals\3320_25-11-PW\TRK01.dgn

Ciorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60660
 Tel. 773.775.4009 Fax 773.775.4014

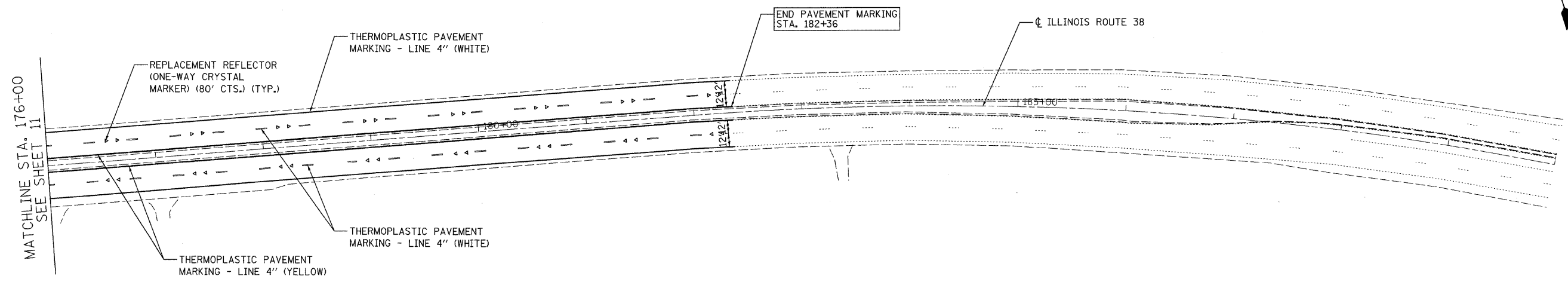
USER NAME = wlancoaster	DESIGNED - JCC	REVISED -
PLLOT SCALE = 50.0000' / IN.	DRAWN - JCC	REVISED -
PLLOT DATE = 1/13/2009	CHECKED - MJL	REVISED -
DATE - 12/30/07	REVISED -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAP 347 / ILLINOIS ROUTE 38 (ROOSEVELT ROAD)
 OVER KRESS CREEK
 PAVEMENT MARKING PLAN**

F.A.P. RTE. 347	SECTION 2 Y-B-I	COUNTY DU PAGE	TOTAL SHEETS 34	SHEET NO. 11
SCALE:		SHEET NO. OF SHEETS STA. TO STA.		FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT

MATCH LINE STA. 176+00
 SEE SHEET 12



FILE NAME = n:\area\3329\3329_29\design\proposals\3329_29-12_PVM\TKR02.dgn

Ciorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60656
 Tel. 773.775.4009 Fax 773.775.4014

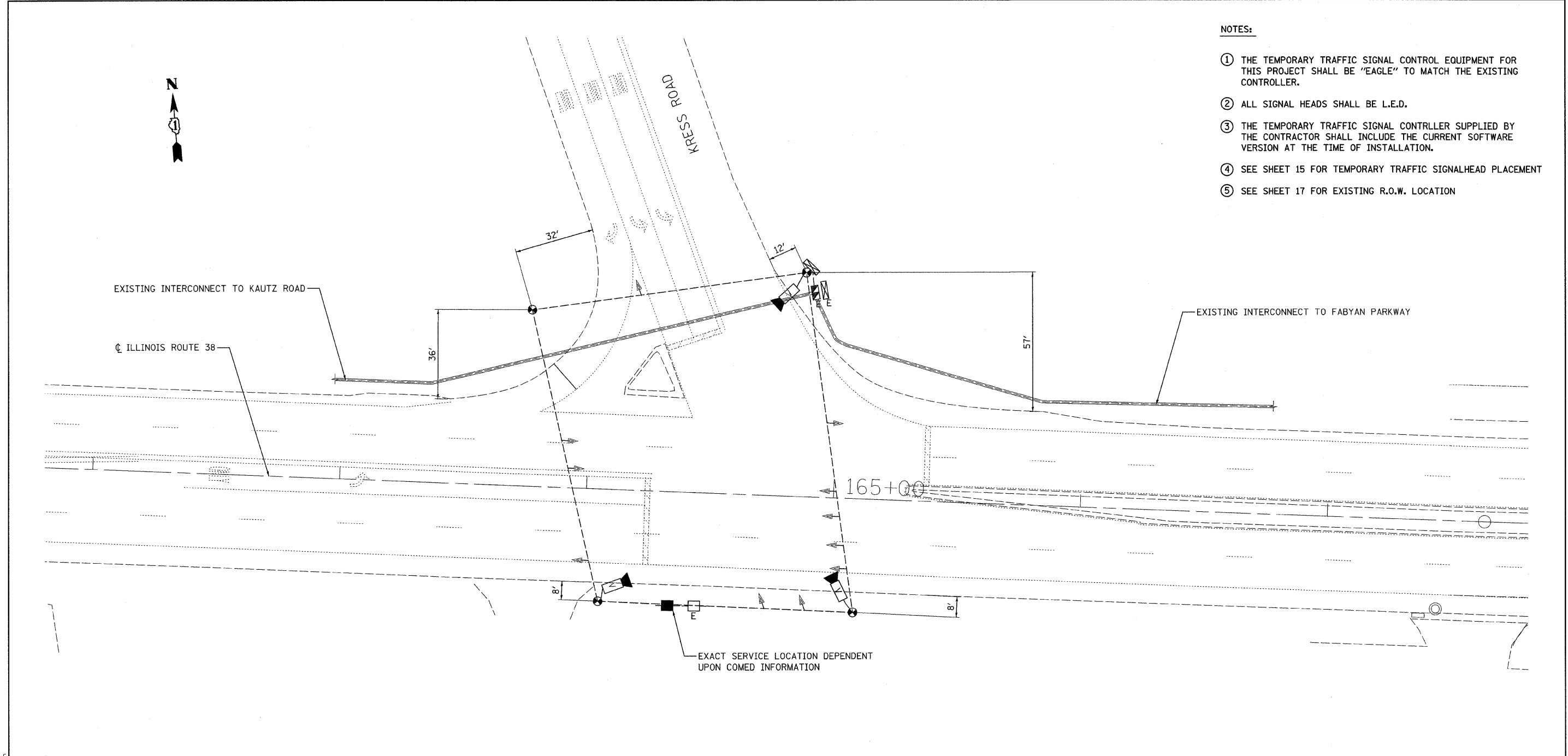
USER NAME = wjancoster	DESIGNED - JCC	REVISED -
PLOT SCALE = 50.0000' / IN.	DRAWN - JCC	REVISED -
PLOT DATE = 1/13/2009	CHECKED - MJL	REVISED -
	DATE - 12/30/07	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAP 347 / ILLINOIS ROUTE 38 (ROOSEVELT ROAD)
 OVER KRESS CREEK
 PAVEMENT MARKING PLAN**

F.A.P. RTE. 347	SECTION 2 Y-B-I	COUNTY	TOTAL SHEETS 34	SHEET NO. 12
SCALE:		SHEET NO. OF SHEETS		STA. TO STA.
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT				

CONTRACT NO. 60D82



- NOTES:**
- ① THE TEMPORARY TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "EAGLE" TO MATCH THE EXISTING CONTROLLER.
 - ② ALL SIGNAL HEADS SHALL BE L.E.D.
 - ③ THE TEMPORARY TRAFFIC SIGNAL CONTRLLER SUPPLIED BY THE CONTRACTOR SHALL INCLUDE THE CURRENT SOFTWARE VERSION AT THE TIME OF INSTALLATION.
 - ④ SEE SHEET 15 FOR TEMPORARY TRAFFIC SIGNALHEAD PLACEMENT
 - ⑤ SEE SHEET 17 FOR EXISTING R.O.W. LOCATION

TEMPORARY TRAFFIC SIGNAL LEGEND

TEMPORARY TRAFFIC SIGNAL HEAD	▶	TEMPORARY PEDESTRIAN PUSHBUTTON DETECTOR	⊙	TEMPORARY SERVICE INSTALLATION	■
SPAN WIRE MOUNTED ORIGINAL LOCATION	▶	MICROWAVE VEHICLE SENSOR	◻	EXISTING SERVICE	E ◻
TEMPORARY TRAFFIC SIGNAL HEAD	▶	EMERGENCY VEHICLE SYSTEM DETECTOR	▲	TEMPORARY PEDESTRIAN SIGNAL HEAD, BRACKET MOUNTED	■
SPAN WIRE MOUNTED SECONDARY LOCATION	▶	CONFIRMATION BEACON	●	TELEPHONE CONNECTION	⊠
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7M) MINIMUM	⊙	COMMON TRENCH	CT	G.S. CONDUIT IN TRENCH OR PUSHED	---
TEMPORARY CONTROLLER CABINET	⊠	UNIT DUCT	UD	VIDEO VEHICLE SENSOR	◻▶
EXISTING CONTROLLER CABINET	E ⊠	HANDHOLE	◻	CLOSED CIRCUIT TV	◻▶
TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE	---	HEAVY DUTY HANDHOLE	H	UN-INTERRUPTABLE POWER SUPPLY (UPS)	U
		DOUBLE HANDHOLE	◻		
		EXISTING DOUBLE HANDHOLE	E ◻		

FILE NAME = n:\proj\3325\3325_20\design\temp\signalplan\3325_20_13-tempsignalplan.dgn

Ciorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60650
 Tel. 773.775.4009 Fax 773.775.4014

USER NAME = wlcaster	DESIGNED - JCC	REVISED -
PLLOT SCALE = 20.0000' / IN.	DRAWN - JCC	REVISED -
PLLOT DATE = 1/13/2009	CHECKED - MJL	REVISED -
	DATE - 12/30/07	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAP 347 / ILLINOIS ROUTE 38 (ROOSEVELT ROAD)
 OVER KRESS CREEK
 TEMPORARY TRAFFIC SIGNAL PLAN**

SCALE: 1"=20'

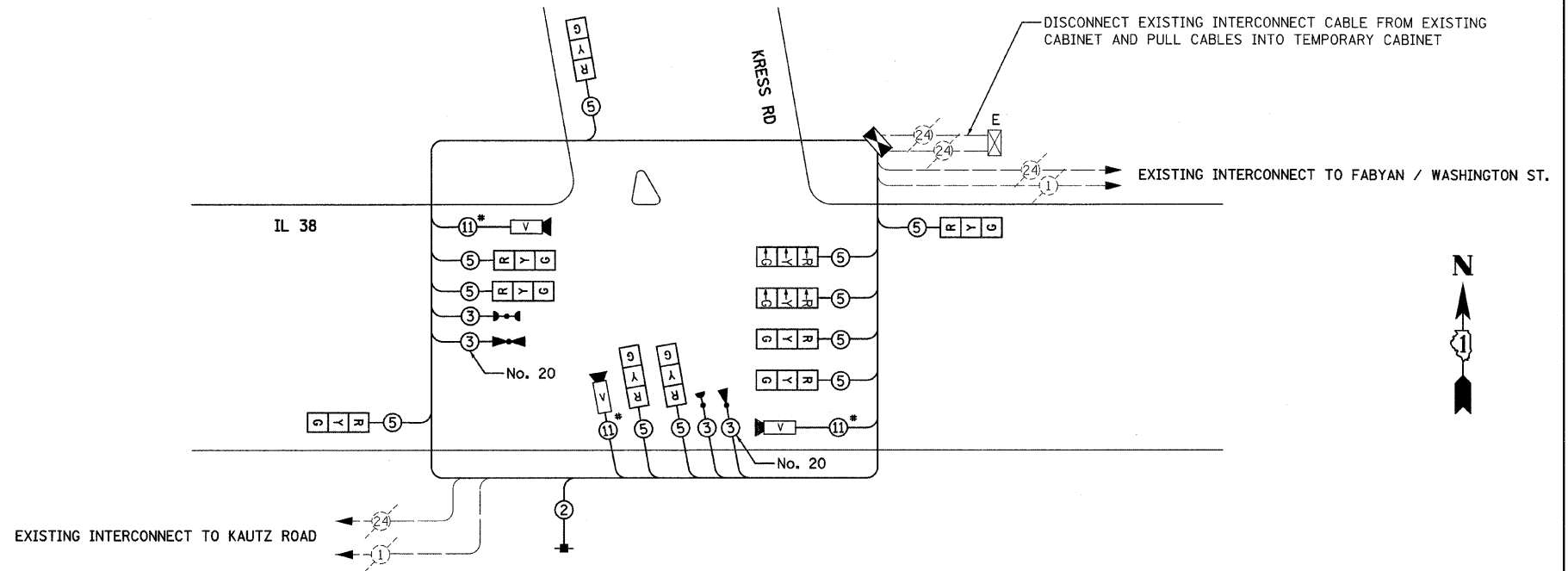
SHEET NO.	OF SHEETS	STA.	TO STA.
-----------	-----------	------	---------

F.A.P. RTE. 347	SECTION 2 Y-B-1	COUNTY	TOTAL SHEETS 34	SHEET NO. 13
FED. ROAD DIST. NO. - ILLINOIS		FED. AID PROJECT		

CONTRACT NO. 60D82

NOTES FOR TEMPORARY TRAFFIC SIGNALS

1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR, EVP WILL BE PAID FOR SEPARATELY.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS1 OR TS2 CABINET, ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT. A REPRESENTATIVE OF THE TRAFFIC SIGNAL CONTROLLER/CABINET VENDOR/SUPPLIER MUST BE PRESENT AT THE TRAFFIC SIGNAL TURN ON.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE 12" (300mm), HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATED HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS, EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
7. 24" WHITE STOP BAR TO BE INSTALLED AFTER THE INSTALLATION AND IMPLEMENTATION OF THE TEMPORARY TRAFFIC SIGNALS.
8. THE VIDEO CAMERA VENDOR/SUPPLIER REPRESENTATIVE WILL ASSIST THE CONTRACTOR IN THE EQUIPMENT SETUP/PLACEMENT OF CAMERAS AND WILL BE PRESENT AT THE TRAFFIC SIGNAL TURN ON.



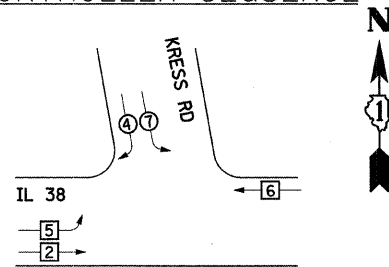
* NOTE: OR AS SPECIFIED BY CAMERA VENDOR

SUMMARY OF QUANTITIES			
ITEM	DESCRIPTION	UNIT	QUANTITY
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INST.	EACH	2
89000100	TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1

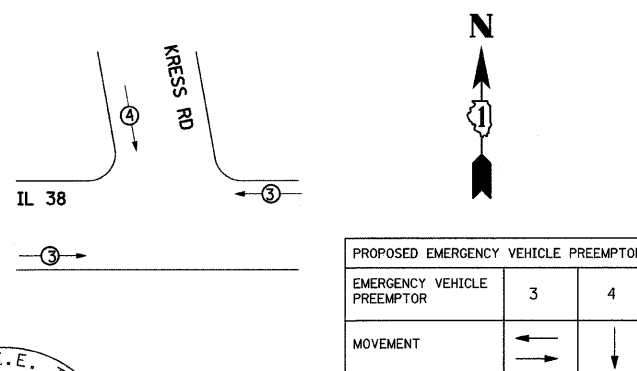
TEMPORARY CABLE DIAGRAM LEGEND

- [R] TEMPORARY TRAFFIC SIGNAL SECTION OR PEDESTRIAN SIGNAL SECTION 12" (300mm)
- [C] TEMPORARY CONTROLLER CABINET
- [S] TEMPORARY SERVICE INSTALLATION
- (5) INDICATES NUMBER OF CONDUCTORS IN CABLE, ALL CONDUCTORS TO BE NUMBER 14 AWG WIRE UNLESS OTHERWISE NOTED.
- [V] EMERGENCY VEHICLE LIGHT DETECTOR
- [B] CONFIRMATION BEACON
- [P] PEDESTRIAN PUSHBUTTON DETECTOR
- [D] VEHICLE DETECTOR, INDUCTION LOOP
- [M] 12" (300mm) PEDESTRIAN SIGNAL SECTION
- [M] MICROWAVE VEHICLE SENSOR
- [V] VIDEO DETECTOR SENSOR
- [C] CLOSED CIRCUIT TV
- [T] TELEPHONE CONNECTION

CONTROLLER SEQUENCE



EMERGENCY VEHICLE PREEMPTION SEQUENCE



LEGEND

- [S] SINGLE ENTRY PHASE
- [D] DUAL ENTRY PHASE
- [OL] OVERLAP
- [P] PEDESTRIAN PHASE
- * NUMBER REFERS TO ASSOCIATED PHASE

PHASE DESIGNATION

NOTE:

EQUIPMENT GROUND CONDUCTOR (GREEN COLOR CODED) SPLICE TO FRAME AND COVER IS REQUIRED FOR ALL HANDHOLES OR DOUBLE HANDHOLES THAT CARRY SIGNAL CABLES AND SERVICE CABLES.

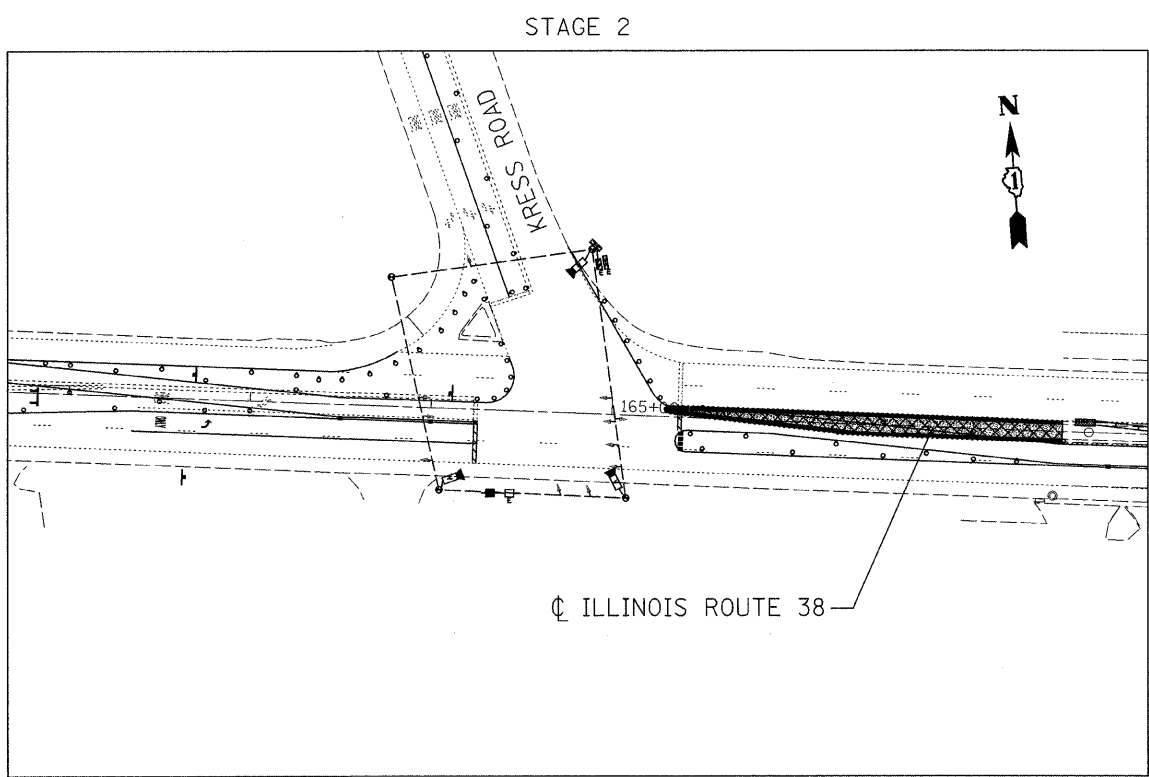
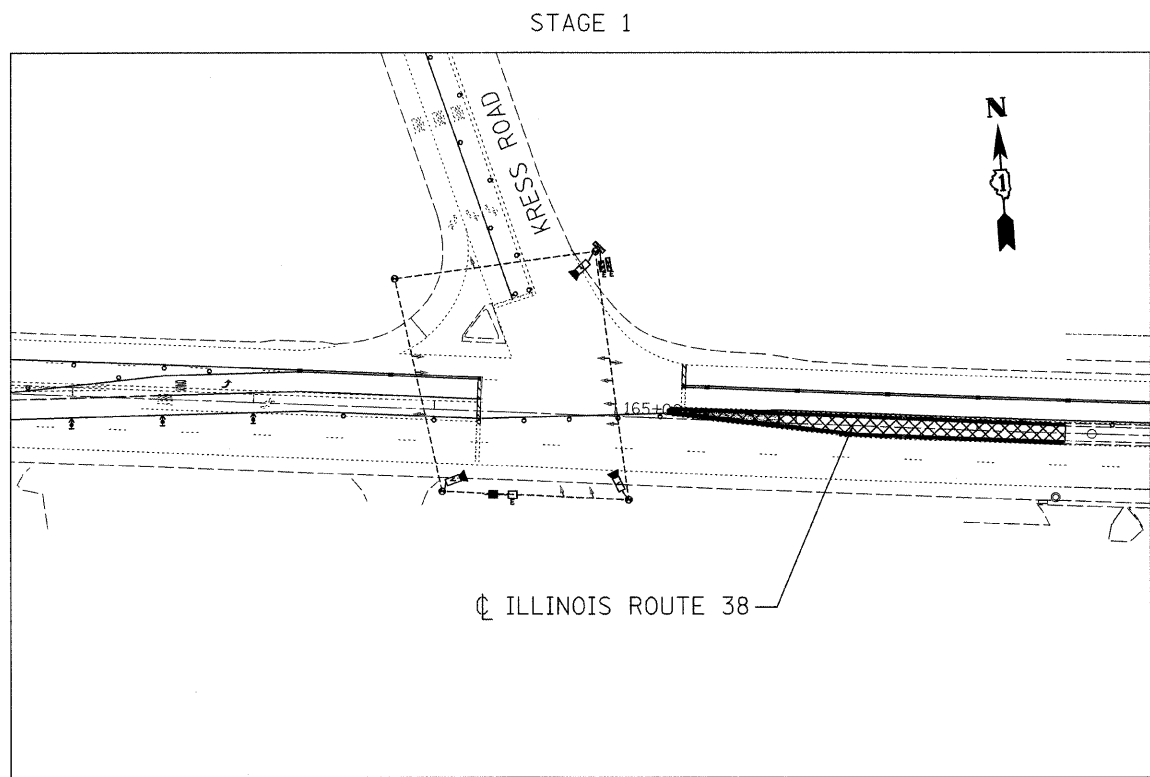
I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	WATTAGE LED	% OPERATION	
SIGNAL (RED)	9	135	17	0.50	76.5
(YELLOW)	9	135	25	0.25	56.25
(GREEN)	9	135	15	0.25	33.75
ARROW	6	135	12	0.10	7.2
PED. SIGNAL		90	25	1.00	
CONTROLLER	1	100	100	1.00	100.0
ILLUM. SIGN		84		0.05	
VIDEO CAMERA	3		45	1.00	135

ENERGY COSTS TO: TOTAL = 405.25
 ILLINOIS DEPARTMENT OF TRANSPORTATION
 201 WEST CENTER COURT
 SCHAMBURG, ILLINOIS 60196-1096
 ENERGY SUPPLY CONTACT: KATHY NYSTROM
 PHONE: (847) 816-5489
 COMPANY: COM. ED.

FOUNDATION (DEPTH)	FT. (m)	CABLE SLACK	FT. (m)	ALL FOUNDATIONS	FT. (m)
TYPE A - POST	4 (1.2)	HANDHOLE	6.5 (2.0)		3.5 (1.0)
D - CONTROLLER	4 (1.2)	DOUBLE HANDHOLE	13 (4.0)	MAST ARM (L) POLE	20'±L-2"±
E - M. ARM POLE	2 (0.6)	SIGNAL POST	2 (0.6)		(6m±L-0.6m)±
24" (600mm)	10 (3.0)	CONTROLLER CAB.	1 (0.5)	BRACKET MOUNTED	13 (4.0)
30" (750mm)	15 (4.6)	FIBER OPTIC	13 (4.0)	PED. PUSHBUTTON	4 (1.2)
		ELECTRIC SERVICE	1 (0.5)	ELECTRIC SERVICE	13.5 (4.1)
		GROUND CABLE	1 (0.5)	SERVICE TO GROUND	13.5 (4.1)
				POST MOUNTED	6 (1.8)



48 - HOURS BEFORE DIGGING



FILE NAME = n:\puroj\3329\3329_29\design\proposals\3329_27-15-temporary-traffic-signals-stage1.dgn



Ciorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60656
 Tel. 773.775.4009 Fax 773.775.4014

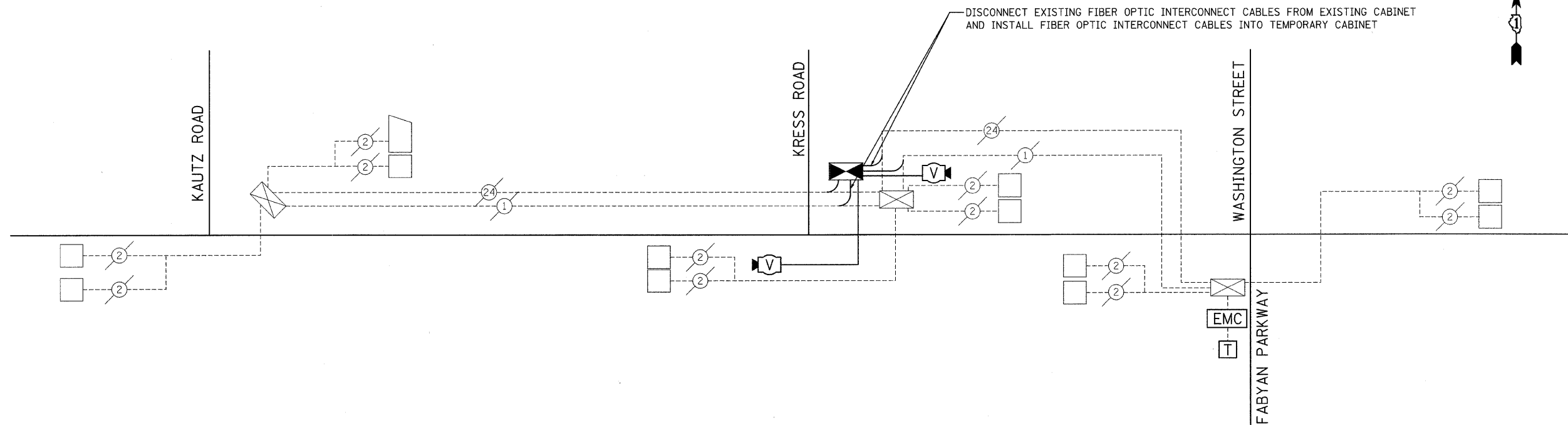
USER NAME = wlenoester	DESIGNED - JCC	REVISED -
DRAWN - JCC	REVISIONS	
CHECKED - MJL	REVISIONS	
DATE - 12/30/07	REVISIONS	
PLLOT SCALE = 20.0000' / IN.		
PLLOT DATE = 1/13/2009		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION










FAP 347 / ILLINOIS ROUTE 38 (ROOSEVELT ROAD)
OVER KRESS CREEK
TEMPORARY TRAFFIC SIGNALS - STAGE CONFIGURATIONS

SCALE: 1"=20' SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2 Y-B-I		34	15
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT			CONTRACT NO. 60B82	



SYMBOLS

-  PROPOSED TEMPORARY INTERSECTION CONTROLLER
-  EXISTING INTERSECTION CONTROLLER
-  EXISTING MASTER CONTROLLER
-  EXISTING TELEPHONE CONNECTION
-  EXISTING INTERSECTION & SAMPING (SYSTEM) DETECTOR LOOP
-  TEMPORARY VIDEO DETECTION SYSTEM TO BE USED FOR SAMPLING (SYSTEM) DETECTOR
-  INTERCONNECT CABLE - NO. 62.5/125 12F FIBER OPTIC CABLE
-  EXISTING TRACER CABLE 1/C
-  EXISTING LOOP DETECTOR CABLE - 2/C TWISTED, SHIELDED

NOTE:

THE CONTRACTOR WILL BE RESPONSIBLE FOR DISCONNECTING THE EXISTING FIBER OPTIC INTERCONNECT CABLE FROM THE EXISTING TRAFFIC SIGNAL CABINET; REMOVING IT FROM THE CONDUIT TO THE DOUBLE HANDHOLE; PULLING ANY NEEDED FIBER OPTIC INTERCONNECT SYSTEM CABLE SLACK TO THE LOCATION; CONNECTING IT TO THE TEMPORARY TRAFFIC SIGNAL CABINET; AND RE-INSTALLING THE FIBER OPTIC INTERCONNECT CABLE BACK INTO THE PERMANENT CABINET. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PROTECTION OF THE CABLE.

THIS WORK WILL BE INCIDENTAL TO THE PAY ITEMS TEMPORARY TRAFFIC SIGNAL INSTALLATION AND MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION.

ANY DAMAGE TO THE EXISTING INTERCONNECT SYSTEM OR THE EXISTING TRAFFIC SIGNAL INSTALLATION WILL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER AT THE CONTRACTOR'S EXPENSE.

FILE NAME = i:\proj\3329\3329_20\design\proposed\em\3329_20-16-interconnect.dgn



Ciorba Group, Inc.
CONSULTING ENGINEERS
5507 North Cumberland Avenue, Suite 402
Chicago, Illinois 60656
Tel 773.775.4009 Fax 773.775.4014

USER NAME = wlancoaster	DESIGNED - JCC	REVISED -
PLOT SCALE = 50.0000' / IN.	DRAWN - JCC	REVISED -
PLOT DATE = 1/13/2009	CHECKED - MJL	REVISED -
	DATE - 12/30/07	REVISED -

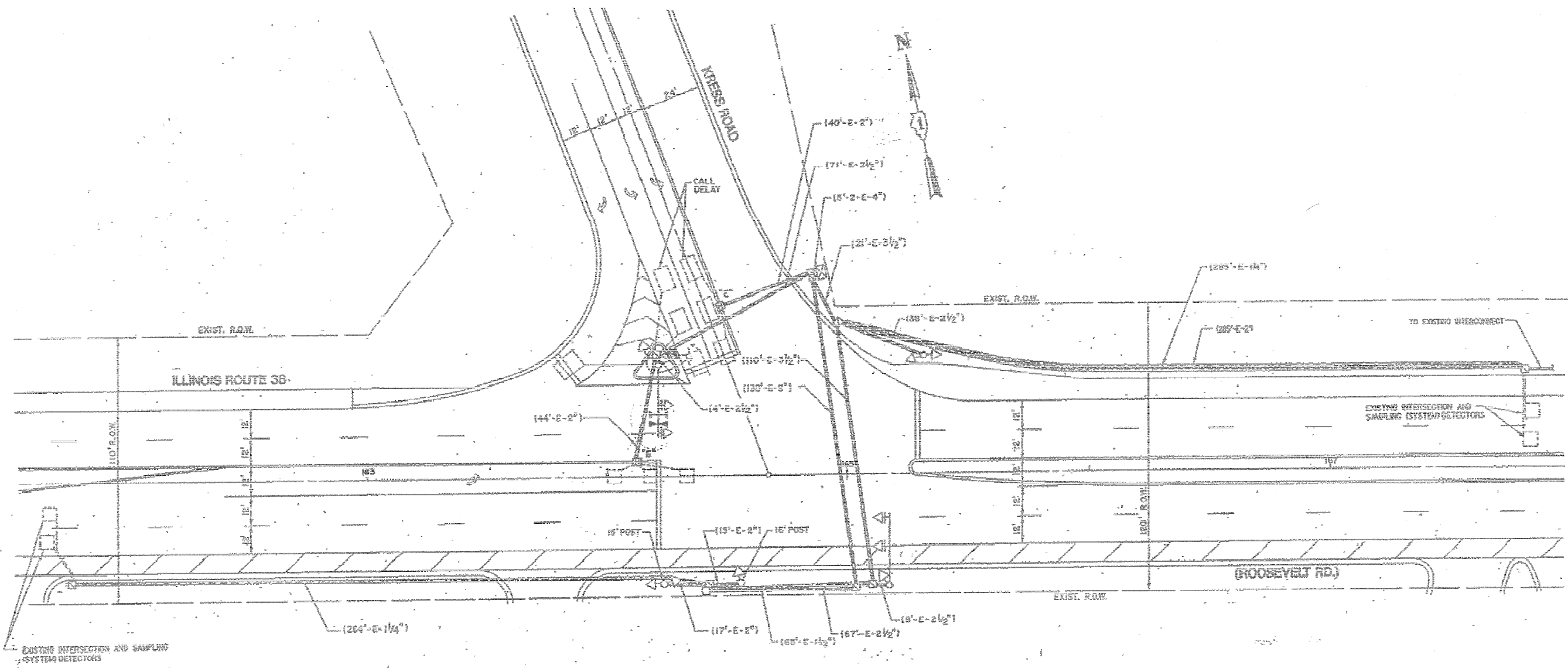
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAP 347 / ILLINOIS ROUTE 38 (ROOSEVELT ROAD)
OVER KRESS CREEK
TEMPORARY INTERCONNECT SCHEMATIC**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE. 347	SECTION 2 Y-B-I	COUNTY DU PAGE	TOTAL SHEETS 34	SHEET NO. 16
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT				

CONTRACT NO. 60DB2



TRAFFIC SIGNAL LEGEND

	EXISTING	PROPOSED		EXISTING	PROPOSED
DOUBLE SIGNAL			CONTROLLER		
O.S. CONDUIT IN TRENCH OR PISHED			SERVICE INSTALLATION		
EMERGENCY VEHICLE SYSTEM DETECTOR			SIGNAL HEAD		
CONFIRMATION BEADDER			SIGNAL HEAD WITH BACKPLATE		
COMMON TRENCH			SIGNAL HEAD, PEDESTRIAN		
ATTACHED TO STRUCTURE		ATS	SIGNAL POST		
UNIT DETCT			WEST ARM ASSEMBLY AND POLE, STEEL		
PEDESTRIAN PUSHBUTTON DETECTOR			HANHOLE		
DETECTOR LOOP			HEAVY DUTY HANHOLE		
			JUNCTION BOX, ATTACHED TO STRUCTURE		

FOR INFORMATION ONLY

FILE NAME: c:\p\proj\3329\3329_20\design\proposed\en\3329_20-17_signalplan.dgn

Ciorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402
 Chicago, Illinois 60656
 Tel. 773.775.4009 Fax 773.775.4014

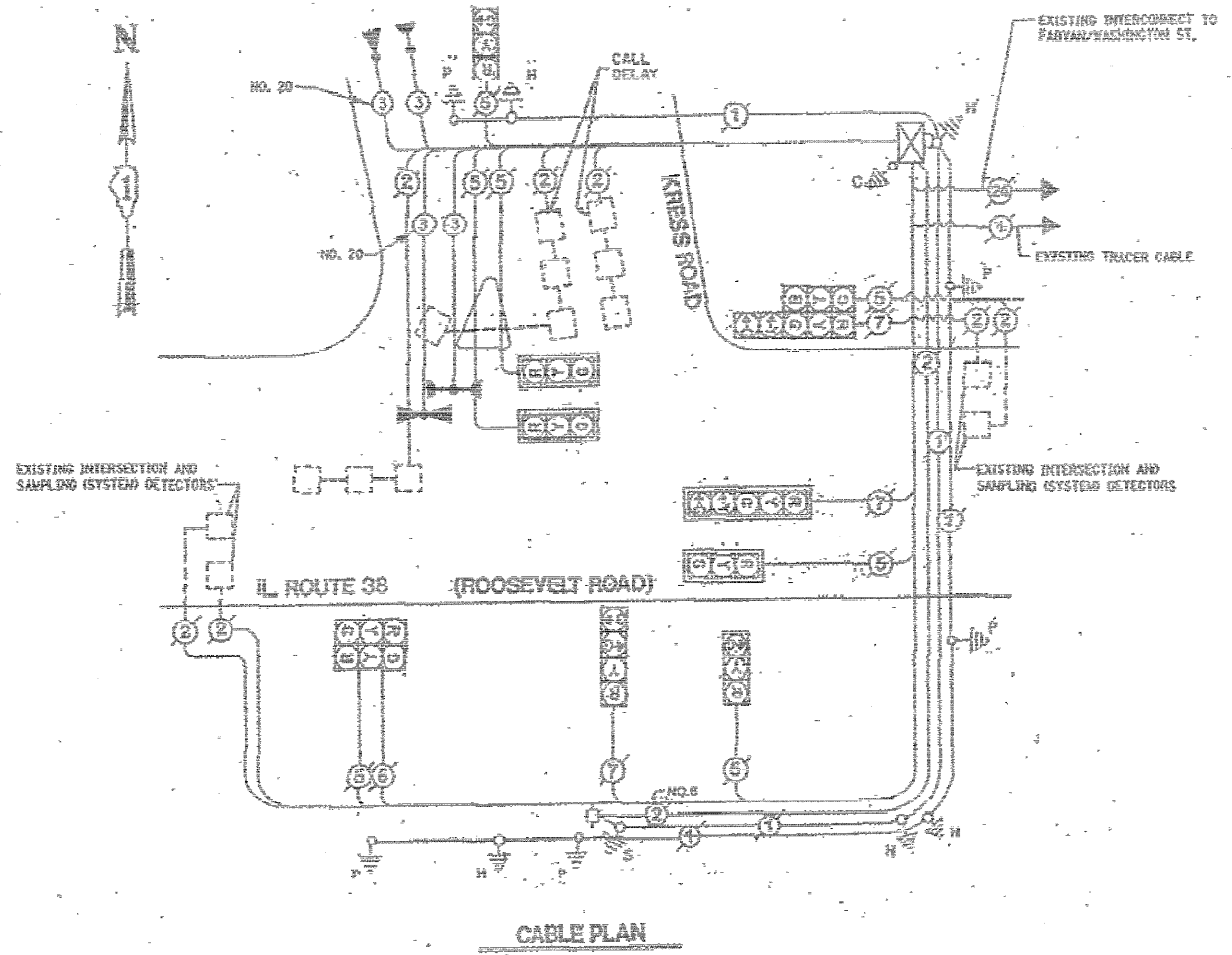
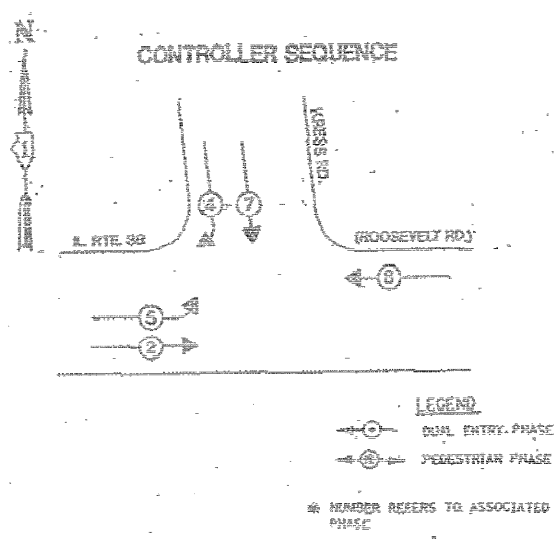
USER NAME = wlancastr	DESIGNED - JCC	REVISED -
PLOT SCALE = 20,0000' / IN.	DRAWN - JCC	REVISED -
PLOT DATE = 1/13/2009	CHECKED - MJL	REVISED -
	DATE - 12/30/07	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAP 347 / ILLINOIS ROUTE 38 (ROOSEVELT ROAD)
 OVER KRESS CREEK
 EXISTING TRAFFIC SIGNAL PLAN**

SCALE: 1"=20'
 SHEET NO. OF SHEETS STA. TO STA.

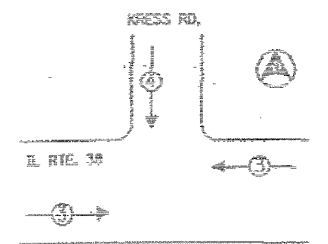
F.A.P. RTE. 347	SECTION 2 Y-B-I	COUNTY	TOTAL SHEETS 34	SHEET NO. 17
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT		CONTRACT NO. 60D82		



CABLE PLAN LEGEND

- EXISTING**
- 4" ROUND TRAFFIC SIGNAL SECTION
 - 12" ROUND TRAFFIC SIGNAL SECTION
 - 12" ROUND PEDESTRIAN SIGNAL SECTION
 - 12" ROUND PEDESTRIAN SIGNAL SECTION
 - CONTROLLER CABINET
 - ⊕ SERVICE INSTALLATION
 - VEHICLE DETECTOR, INDUCTION LOOP
 - MAGNETIC DETECTOR
 - EMERGENCY VEHICLE LIGHT DETECTOR
 - CONFIRMATION BEACON
 - PULSATION DETECTOR
 - ⊙ DENOTES NUMBER OF CONDUCTORS. ALL CABLE NO. 14 EXCEPT AS INDICATED. ALL LOOP DETECTOR CABLE TO BE SHIELDED.
 - SIGNAL FACE WITH BACKPLATE "PM" INDICATES PROGRAMMED HEAD
 - GROUND ROD AT MASTHEAD OR CONTROLLER
 - GROUND ROD AT POLE OR MAST ARM POLE
 - GROUND ROD AT ELECTRIC SERVICE INSTALLATION
 - GROUND ROD EXISTING TO BE REUSED
 - GROUND CABLE IN CONDUIT, NO. 6 SOLID COPPER WIREDS
 - ② NO. 62.5/125 MM 12F & 5M 12F, FIBER OPTIC CABLE
 - ① NO. 14 10 TRACER CABLE
- NOTE:** ALL NEW GROUND RODS SHALL BE 3/4" X 10'-0" LONG COPPER CLAD. THE COST SHALL BE INCIDENTAL TO THE COST OF INSTALLATION.

EMERGENCY VEHICLE PREEMPTION SEQUENCE



PROPOSED EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	←	↓

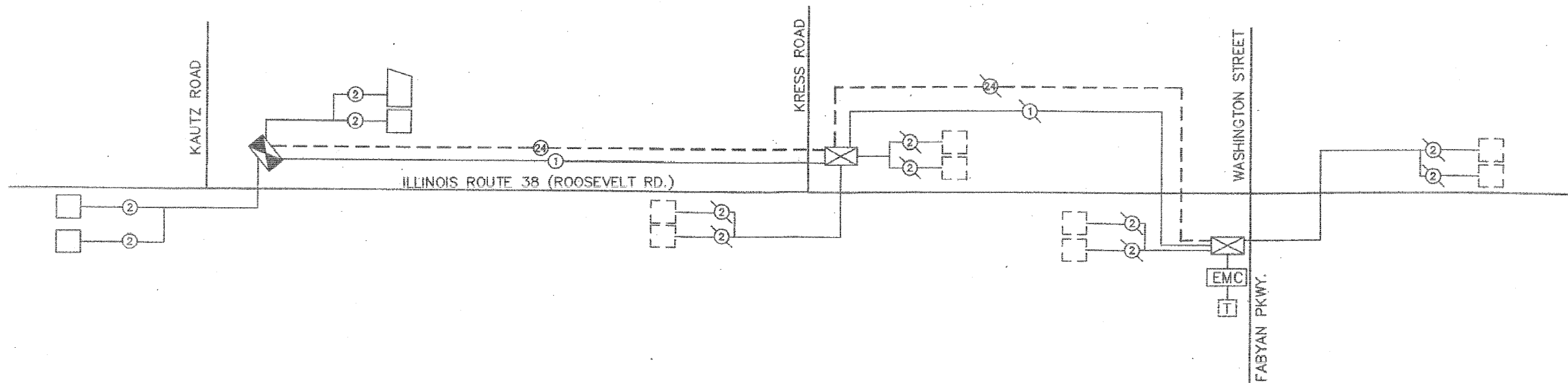
SCHEDULE OF QUANTITIES

- 2 EACH LIGHT DETECTOR
- 1 EACH LIGHT DETECTOR AMPLIFIER
- 204 FOOT ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14 3/C
- 204 FOOT ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 20 3/C, TWISTED, SHIELDED
- 1 EACH MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION

THE LIGHT DETECTORS AND LIGHT DETECTOR AMPLIFIER FOR THIS PROJECT SHALL BE OPTICOM.

FOR INFORMATION ONLY

FILE NAME: m:\proj\3329\3329_23\design\proposed\plan\3329_23-18-esignal.dgn



INTERCONNECT SCHEDULE OF QUANTITIES

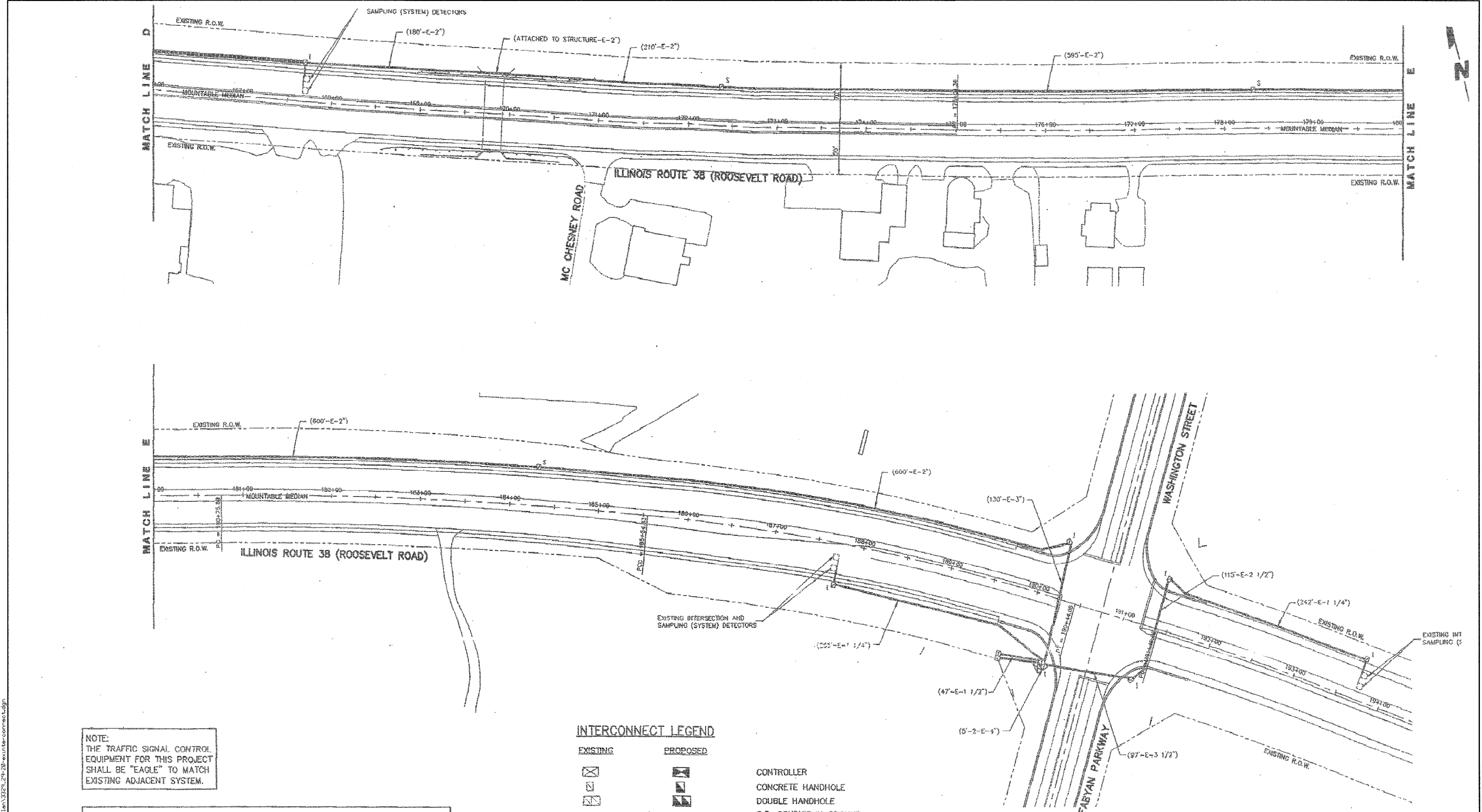
QUANTITY	UNIT	ITEM
0.5	EACH	TRAFFIC CONTROL & PROTECTION, STANDARD 701201
0.5	EACH	TRAFFIC CONTROL & PROTECTION, STANDARD 701406
0.5	EACH	TRAFFIC CONTROL & PROTECTION, STANDARD 701701
2900	FOOT	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL
1350	FOOT	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL
7	EACH	HEAVY DUTY HANDHOLE
1	EACH	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION
2900	FOOT	TRENCH AND BACKFILL FOR ELECTRICAL WORK
1	EACH	DRILL EXISTING HANDHOLE
5234	FOOT	ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1/C
5263	FOOT	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM12F
1	L SUM	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM

INTERCONNECT SCHEMATIC LEGEND

- PROPOSED INTERSECTION CONTROLLER
- EXISTING INTERSECTION CONTROLLER
- PROPOSED MASTER CONTROLLER
- EXISTING MASTER CONTROLLER
- MASTER MASTER CONTROLLER
- PROPOSED INTERSECTION & SAMPLING (SYSTEM) DETECTORS
- EXISTING INTERSECTION & SAMPLING (SYSTEM) DETECTORS
- INTERCONNECT CABLE - NO. 62.5/125 MM 12F & SM 12F FIBER OPTIC CABLE
- INTERCONNECT CABLE - NO. 18 3 PAIR TWISTED, SHIELDED
- LOOP DETECTOR CABLE - 2/C TWISTED, SHIELDED
- EXISTING INTERCONNECT CABLE - NO. 62.5/125 MM 12F FIBER OPTIC CABLE
- EXISTING INTERCONNECT CABLE NO. 18 3 PAIR TWISTED, SHIELDED
- EXISTING LOOP DETECTOR CABLE - 2/C TWISTED, SHIELDED
- TELEPHONE CONNECTION
- PROPOSED TRACER CABLE NO. 14 1/C
- EXISTING INTERSECTION LOOP DETECTORS AND PROPOSED SAMPLING (SYSTEM) DETECTORS
- EXISTING TELEPHONE CONNECTION
- EXISTING TRACER CABLE 1/C (AS SPECIFIED)
- EXISTING SAMPLING (SYSTEM) DETECTORS
- PROPOSED SAMPLING (SYSTEM) DETECTORS
- EXISTING SAMPLING (SYSTEM) DETECTORS

FOR INFORMATION ONLY

FILE NAME = i:\proj\3329\3329_2\design\proposed\en\3329_2\15-axinterconnect.dgn



NOTE:
THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "EAGLE" TO MATCH EXISTING ADJACENT SYSTEM.

RESTORATION OF WORK AREA, RESTORATION OF TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDING IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

INTERCONNECT LEGEND

EXISTING	PROPOSED	
		CONTROLLER
		CONCRETE HANDHOLE
		DOUBLE HANDHOLE
		G.S. CONDUIT IN GROUND
		DETECTOR LOOP
		UNIT DUCT
		SYSTEM
		INTERSECTION
		UD
		IP

FOR INFORMATION ONLY

FILE NAME = n:\pro\3329\3329_2\design\proposals\en\3329_29-28-exinter-connect.dgn

Ciorba Group, Inc.
CONSULTING ENGINEERS
5507 North Cumberland Avenue, Suite 402
Chicago, Illinois 60656
Tel. 773.775.4009 Fax 773.775.4014

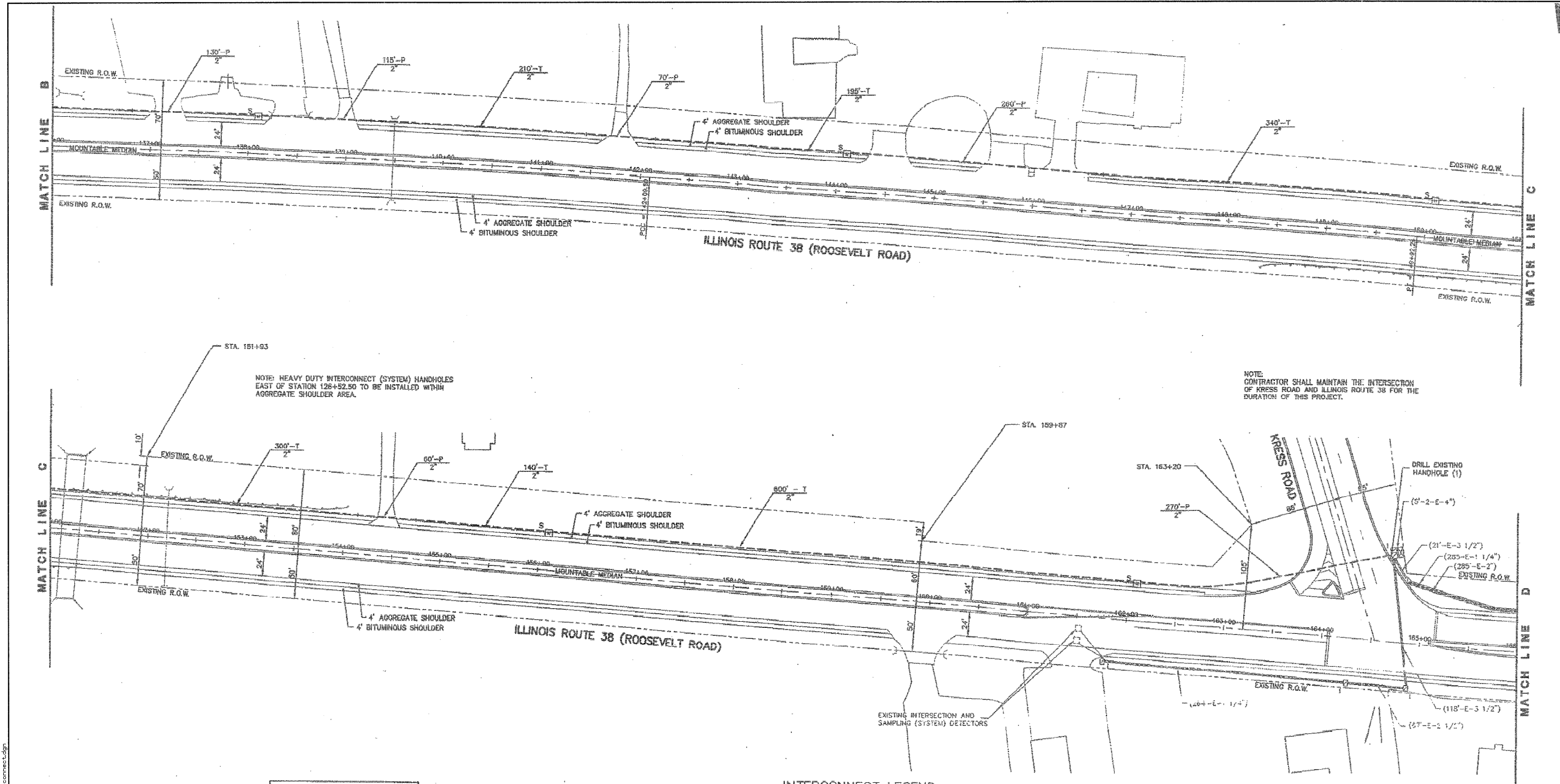
USER NAME = wlancaster	DESIGNED - JCC	REVISED -
PLOT SCALE = 20.0000' / IN.	DRAWN - JCC	REVISED -
PLOT DATE = 1/13/2009	CHECKED - MJL	REVISED -
	DATE - 12/30/07	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAP 347 / ILLINOIS ROUTE 38 (ROOSEVELT ROAD)
OVER KRESS CREEK
EXISTING INTERCONNECT PLAN**

SCALE: 1"=20'

F.A.P. RTE. 347	SECTION 2 Y-B-I	COUNTY	TOTAL SHEETS 34	SHEET NO. 20
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT		CONTRACT NO. 60D82		



NOTE: HEAVY DUTY INTERCONNECT (SYSTEM) HANDHOLES EAST OF STATION 128+52.50 TO BE INSTALLED WITHIN AGGREGATE SHOULDER AREA.

NOTE: CONTRACTOR SHALL MAINTAIN THE INTERSECTION OF KRESS ROAD AND ILLINOIS ROUTE 38 FOR THE DURATION OF THIS PROJECT.

NOTE:
THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "EAGLE" TO MATCH EXISTING ADJACENT SYSTEM.

RESTORATION OF WORK AREA. RESTORATION OF TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

INTERCONNECT LEGEND

EXISTING	PROPOSED	
		CONTROLLER
		CONCRETE HANDHOLE
		DOUBLE HANDHOLE
		G.S. CONDUIT IN GROUND
		DETECTOR LOOP
		UNIT DUCT
		SYSTEM
		INTERSECTION

FOR INFORMATION ONLY

FILE NAME = n:\proj\3320\3320_20\design\proposals\plan\3320_20-21-interconnect.dgn

Ciorba Group, Inc.
CONSULTING ENGINEERS
5507 North Cumberland Avenue, Suite 402
Chicago, Illinois 60656
Tel. 773.775.4009 Fax 773.775.4014

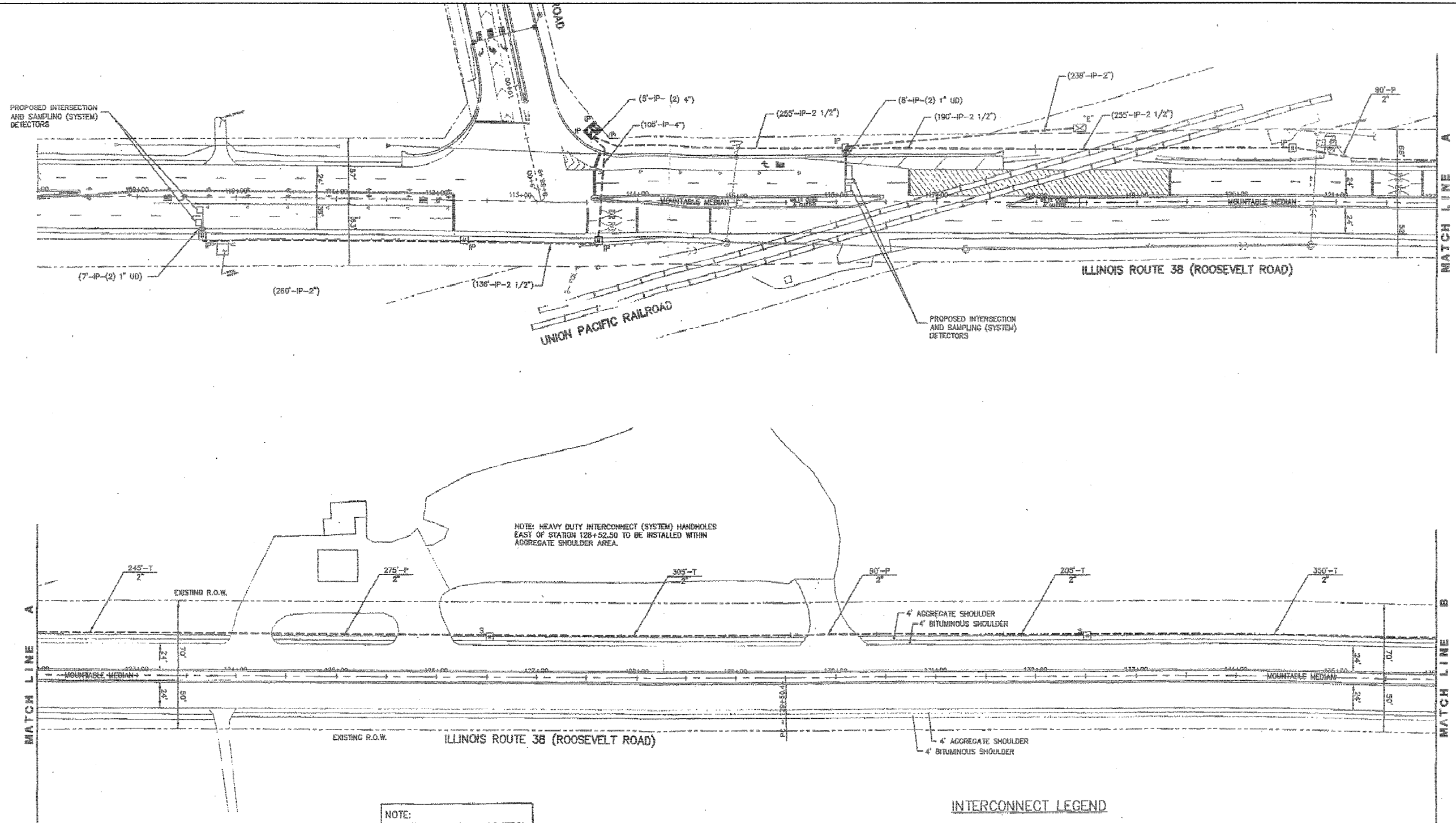
USER NAME = wlcaster	DESIGNED - JCC	REVISED -
PLOT SCALE = 20,0000' / IN.	DRAWN - JCC	REVISED -
PLOT DATE = 1/13/2009	CHECKED - MJL	REVISED -
	DATE - 12/30/07	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAP 347 / ILLINOIS ROUTE 38 (ROOSEVELT ROAD)
OVER KRESS CREEK
EXISTING INTERCONNECT PLAN**

SCALE: 1"=20' SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE. 347	SECTION 2 Y-B-I	COUNTY DU PAGE	TOTAL SHEETS 34	SHEET NO. 21
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT				
CONTRACT NO. 60D82				



NOTE: HEAVY DUTY INTERCONNECT (SYSTEM) HANDHOLES EAST OF STATION 128+52.50 TO BE INSTALLED WITHIN AGGREGATE SHOULDER AREA.

NOTE:
THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "EAGLE" TO MATCH EXISTING ADJACENT SYSTEM.

RESTORATION OF WORK AREA, RESTORATION OF TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDING IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

INTERCONNECT LEGEND

EXISTING	PROPOSED	
		CONTROLLER
		CONCRETE HANDHOLE
		DOUBLE HANDHOLE
		G.S. CONDUIT IN GROUND
		DETECTOR LOOP
		UNIT DUCT
		SYSTEM
		INTERSECTION

FOR INFORMATION ONLY

FILE NAME = n:\pro\3328\3328_2\design\proposed\01\3328_22-ecinterconnect.dgn

Ciorba Group, Inc.
CONSULTING ENGINEERS
5507 North Cumberland Avenue, Suite 402
Chicago, Illinois 60656
Tel: 773.775.4009 Fax: 773.775.4014

USER NAME = wjancoster	DESIGNED - JCC	REVISED -
PLOT SCALE = 20.0000' / IN.	DRAWN - JCC	REVISED -
PLOT DATE = 1/13/2009	CHECKED - MJL	REVISED -
	DATE - 12/30/07	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAP 347 / ILLINOIS ROUTE 38 (ROOSEVELT ROAD)
OVER KRESS CREEK
EXISTING INTERCONNECT PLAN**

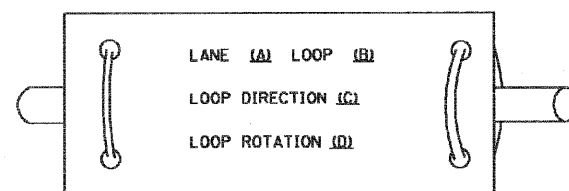
SCALE: 1"=20' SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE. 347	SECTION 2 Y-B-1	COUNTY	TOTAL SHEETS 34	SHEET NO. 22
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 60D82	

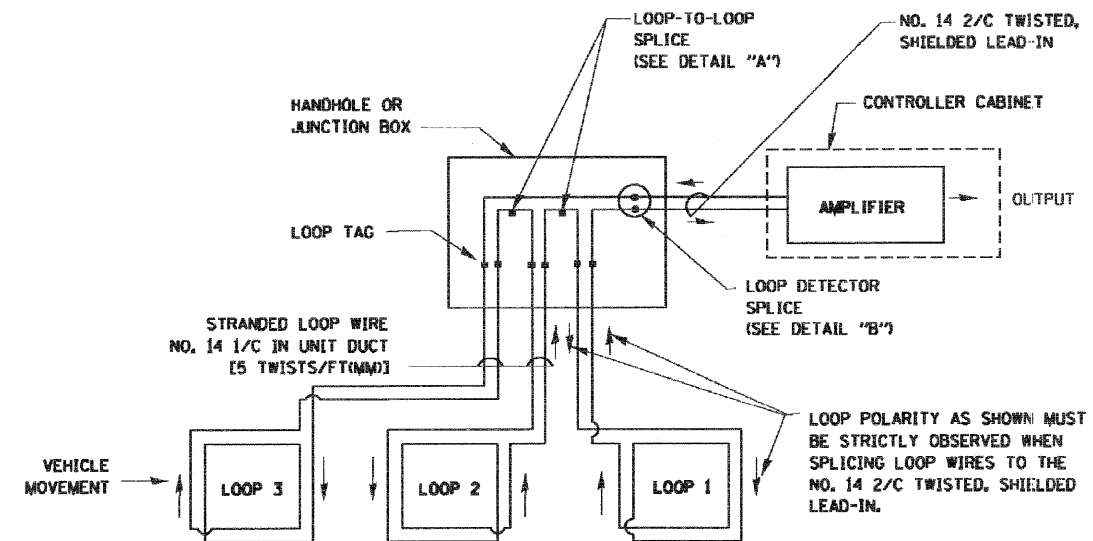
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE UNIT DUCT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). UNIT DUCT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

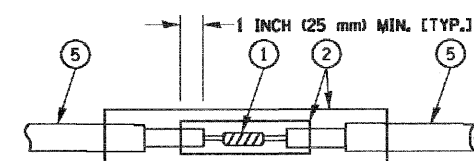


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

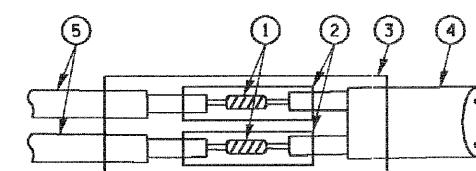


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm), IF IN CONCRETE. THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

- ① WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- ② WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- ③ WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- ④ NO. 14 2/C TWISTED, SHIELDED CABLE.
- ⑤ LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

FILE NAME =	USER NAME = goglionob	DESIGNED - D.A.D.	REVISED - 11-12-01
W:\d\stata\22x34\ts085.dgn		DRAWN - R.W.P.	REVISED - BUR. TRAFFIC 01-01-02
	PLOT SCALE = 50.0000 "/ IN.	CHECKED - D.A.Z.	REVISED -
	PLOT DATE = 1/4/2000	DATE - 05-30-00	REVISED -

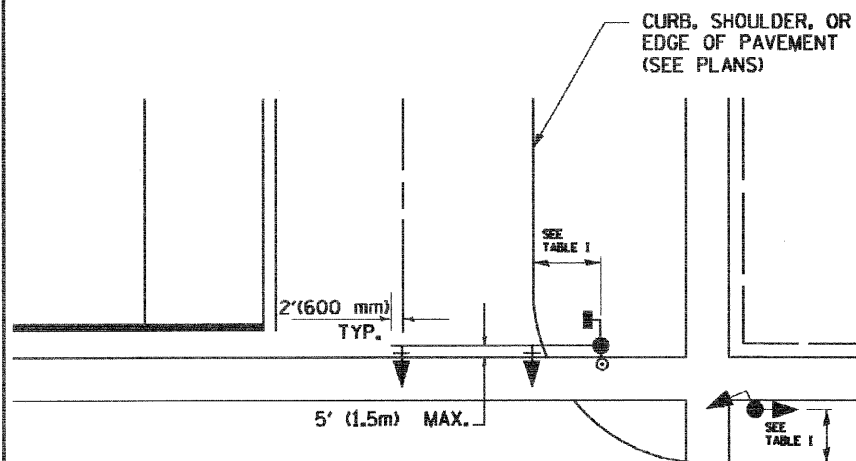
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS
SCALE: NONE SHEET NO. 1 OF 4 SHEETS STA. TO STA.

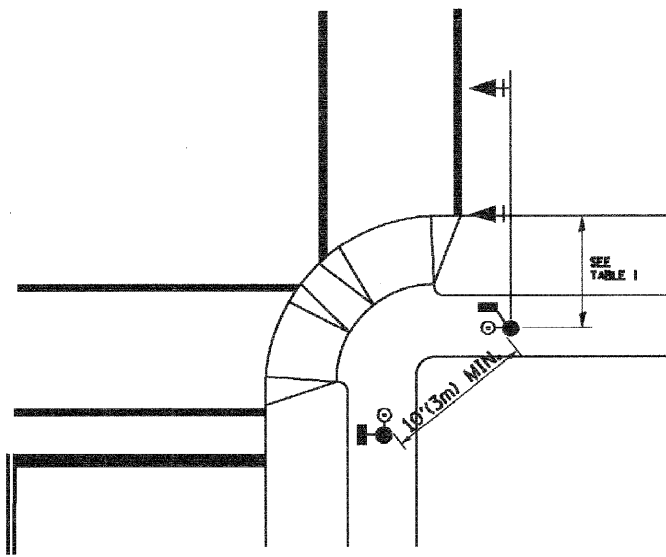
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2 Y-B-1		34	23
TS-05		CONTRACT NO.	60D82	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

TRAFFIC SIGNAL MAST ARM AND POST

MAST ARM MOUNTED SIGNAL IN PROPOSED & FUTURE SIDEWALK AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNAL AND PUSHBUTTON DETECTOR



PEDESTRIAN SIGNAL PUSHBUTTON



RECOMMENDED PUSHBUTTON LOCATIONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHALL BE IN ACCORDANCE WITH THE CURRENT MUTCD (SEE NOTE 1). TO MEET MUTCD REQUIREMENTS, PEDESTRIAN SIGNAL PUSHBUTTONS MAY HAVE TO BE MOUNTED ON A SEPARATE POST.

NOTES:

- AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS WITH PEDESTRIAN ACTUATION, EACH PUSHBUTTON SHALL ACTIVATE BOTH THE WALK INTERVAL AND THE ACCESSIBLE PEDESTRIAN SIGNALS.

AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS, PUSHBUTTONS SHOULD CLEARLY INDICATE WHICH CROSSWALK SIGNAL IS ACTUATED BY EACH PUSHBUTTON. PUSHBUTTONS AND TACTILE ARROWS SHOULD HAVE HIGH VISUAL CONTRAST (SEE THE DEPARTMENT OF JUSTICE'S AMERICANS WITH DISABILITIES ACT STANDARDS FOR ACCESSIBLE DESIGN, 1991). TACTILE ARROWS SHOULD POINT IN THE SAME DIRECTION AS THE ASSOCIATED CROSSWALK. AT CORNERS OF SIGNALIZED LOCATIONS WITH ACCESSIBLE PEDESTRIAN SIGNALS WHERE PEDESTRIAN PUSHBUTTONS ARE PROVIDED, THE PUSHBUTTONS SHOULD BE SEPARATED BY THE DISTANCE OF AT LEAST 10 FT (3m). THIS ENABLES PEDESTRIANS WHO HAVE VISUAL DISABILITIES TO DISTINGUISH AND LOCATE THE APPROPRIATE PUSHBUTTON.

PUSHBUTTONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHOULD BE LOCATED AS FOLLOWS:
 A: ADJACENT TO A LEVEL ALL-WEATHER SURFACE TO PROVIDE ACCESS FROM A WHEELCHAIR, AND WHERE THERE IS AN ALL WEATHER SURFACE, WHEELCHAIR ACCESSIBLE ROUTE TO THE RAMP.
 B: WITHIN 5 FT (1.5m) OF THE CROSSWALK EXTENDED.
 C: WITHIN 10 FT (3m) OF THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
 D: PARALLEL TO THE CROSSWALK TO BE USED (SEE MUTCD FIGURE 4E-2).
 E: NORMAL PEDESTRIAN PUSHBUTTON MOUNTING HEIGHT SHOULD BE 3.5 FT (1.05m) ABOVE ADJACENT SIDEWALK
- PEDESTRIAN SIGNAL FACES SHALL BE MOUNTED WITH THE BOTTOM OF THE HOUSING NOT LESS THAN 8 FT (2.4m) NOR MORE THAN 10 FT (3.0m) ABOVE THE SIDEWALK LEVEL AND SO THERE IS A PEDESTRIAN INDICATION IN THE LINE OF PEDESTRIANS' VISION WHICH PERTAINS TO THE CROSSWALK BEING USED.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, NOT MOUNTED OVER A ROADWAY, SHALL BE AT LEAST 10 FT (3.0m) BUT NOT MORE THAN 15 FT (4.5m) ABOVE THE SIDEWALK OR, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE HIGHWAY IF NO SIDEWALKS EXIST.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, MOUNTED OVER A ROADWAY, SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001 AND 877006. (16 FT (5m) MIN., 18 FT (5.5m) MAX., FROM HIGHEST POINT OF PAVEMENT)

PEDESTRIAN SIGNAL POST

PEDESTRIAN SIGNAL HEAD AND PEDESTRIAN PUSHBUTTON DETECTOR LOCATION

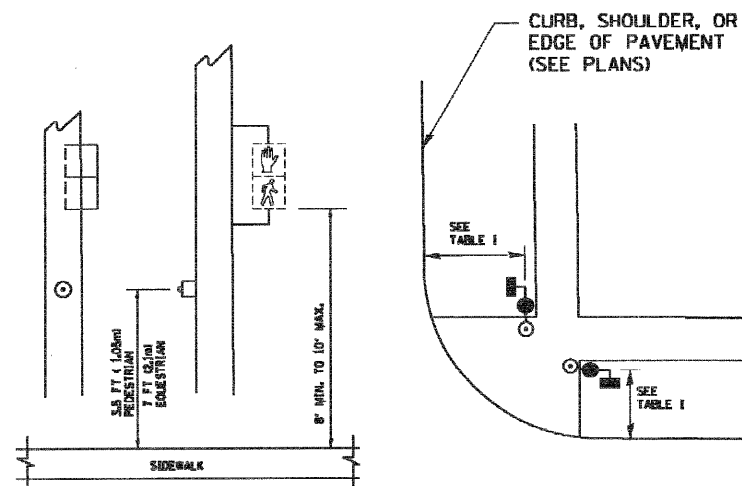
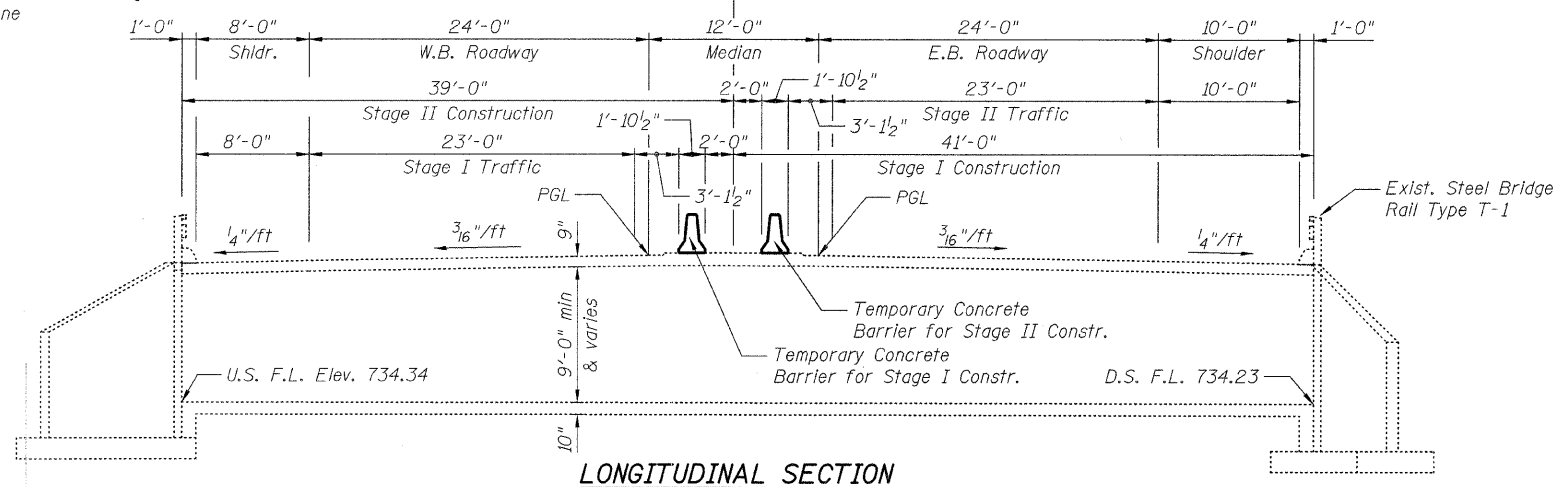


TABLE I

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MIN. DIST. FROM BACK OF CURB)	SHOULDER/NON-CURBED AREA (MIN. DIST. FROM EDGE OF PAVEMENT)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN PUSHBUTTON	SEE NOTE 1	SEE NOTE 1

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

Benchmark: R.R. spike in the north face of the 3rd utility pole east of structure #022-0150. Elev. 749.66
 Existing Structure: Structure 022-0150 built in 1983 as a 2 cell concrete box culvert with overall length of 80 ft. Traffic is to be maintained utilizing stage construction. One lane in each direction will be provided during construction.
 Salvage: None



LONGITUDINAL SECTION

INDEX OF SHEETS

- S1. General Plan
- S2. Temporary Concrete Barrier
- S3. Details

SCOPE OF WORK

1. Hydroscarify slab and approach slab surface
2. Full depth slab repairs
3. Remove and replace concrete median
4. Place Latex Concrete Overlay
5. Sawcut Groove slab surface
6. Full-Depth Repairs on Approach Pavements
7. Resurface Approach Pavements (See Roadway Plans)

GENERAL NOTES

Plan dimensions and details relative to existing plans are subject to routine variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished based upon the unit price bid for the work.

No future wearing surface will be allowed.

Hydro-Scarification of the approach slabs will be paid for as "Bridge Deck Hydro-Scarification 1/2"

DESIGN SPECIFICATIONS

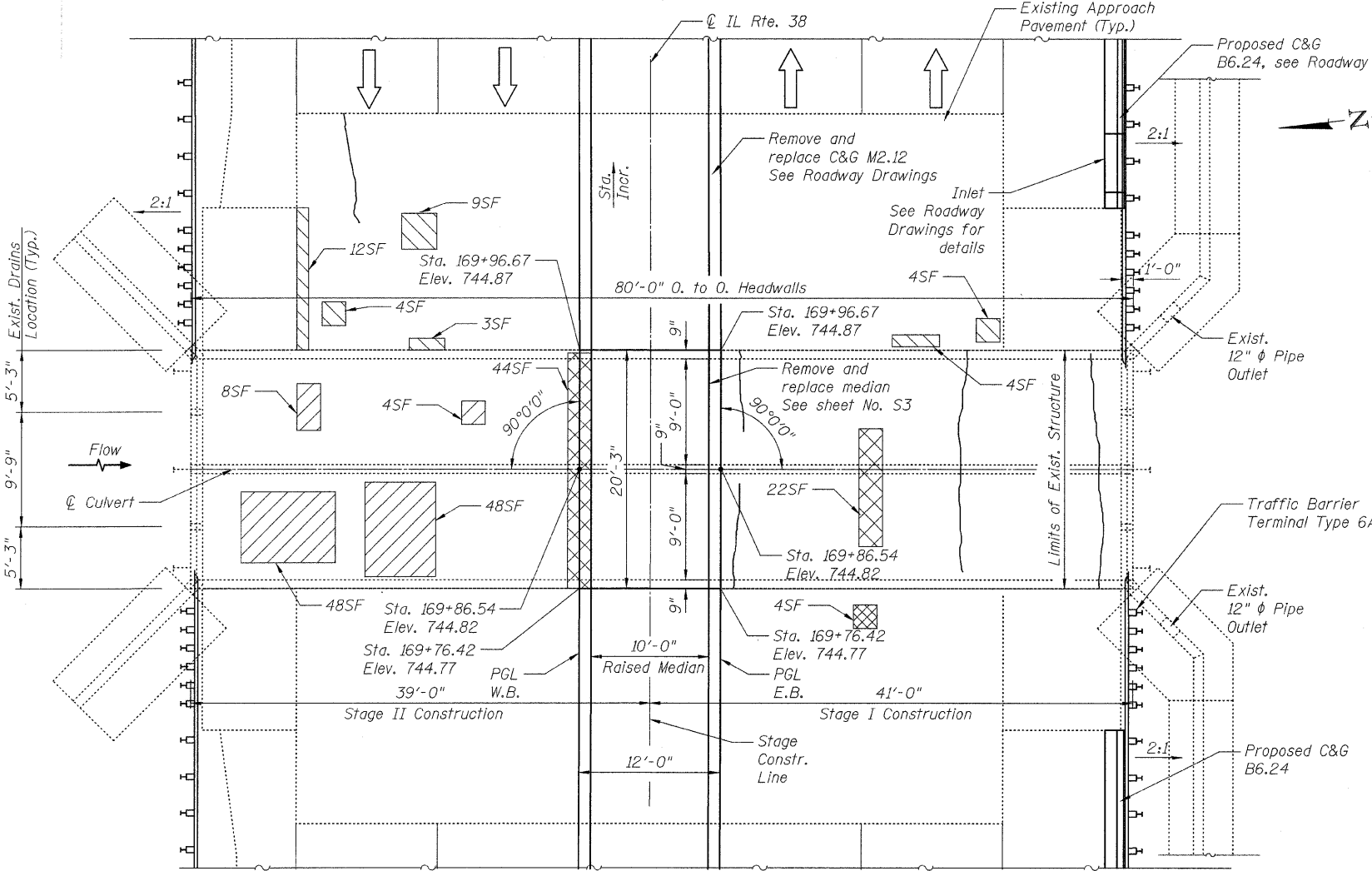
2002 AASHTO Standard Specifications

DESIGN STRESSES

FIELD UNITS
 f'c = 3,500 psi
 fy = 60,000 psi (Reinforcement)

LAST DELAMINATION SURVEY

November 8, 2007

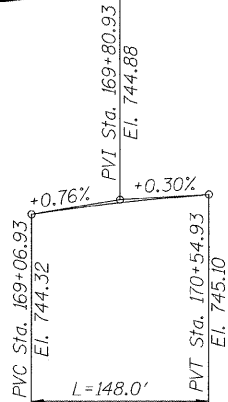


PLAN LEGEND

- Crack (For information only)
- Approach Slab Repair (Full Depth)
- Partial depth repair (For information only)
- Deck Slab Repair (Full depth, Type II)
- Partial depth Approach Pavement Patches (For information only)

HORIZONTAL CURVE

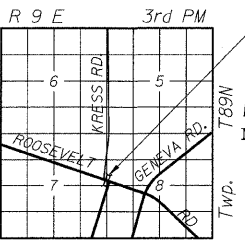
PC = Sta. 166+55.50
 PT = Sta. 176+85.12
 $\Delta = 4^{\circ}38'00''$
 $D = 0^{\circ}27'00''$
 $T = 515.09'$
 $L = 1029.62'$
 $E = 10.41$
 $R = 12,732.40'$



PROFILE GRADE



DATE: 1/13/2009
 SEAL EXPIRES: 11/30/2010



BILL OF MATERIALS

Item	Unit	Quantity
Protective Coat	Sq. Yd.	182
Median Removal Partial Depth	Sq. Ft.	203
Concrete Superstructure	Cu. Yd.	3.3
Bridge Deck Grooving	Sq. Yd.	149
* Bridge Deck Latex Concrete Overlay, 2 1/2 inches	Sq. Yd.	149
* Structural Repair of Concrete (Depth Equal to or Less than 5 inches)	Sq. Ft.	8
* Approach Slab Repair (Full Depth)	Sq. Yd.	1.0
* Bridge Deck Hydro-Scarification 1/2"	Sq. Yd.	414
* Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	7.3

* Special Provisions

GENERAL PLAN
 IL RTE. 38 OVER KRESS CREEK
 STA. 169+84.54
 S.N. 022-0150

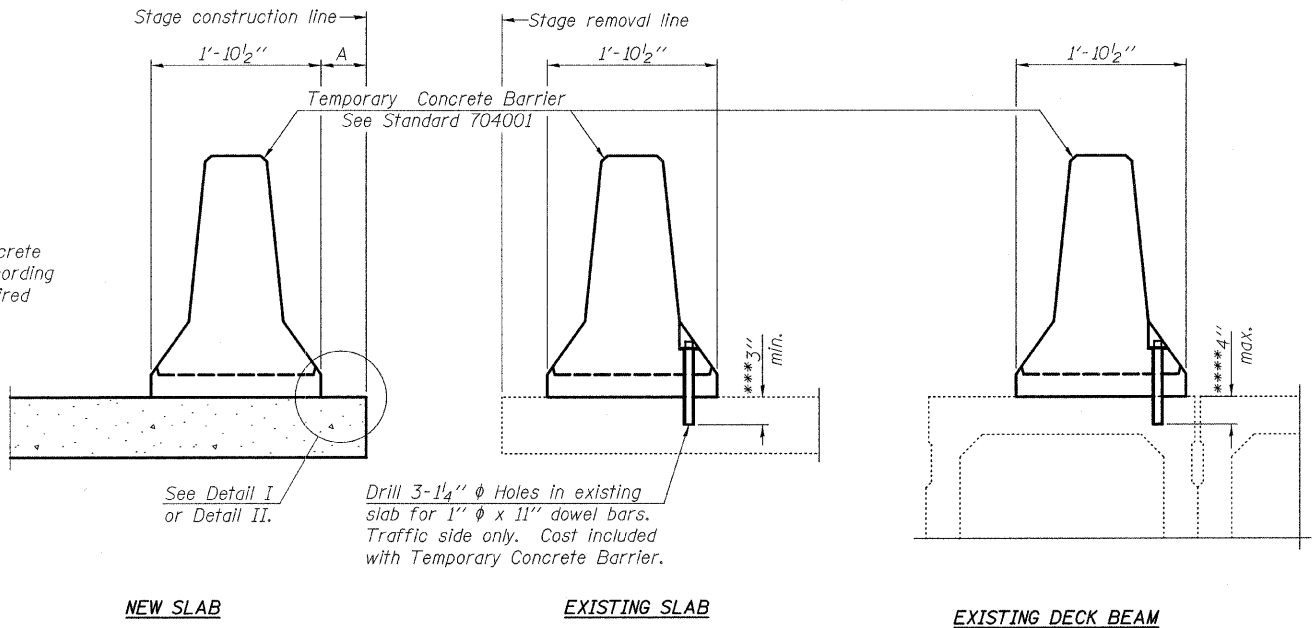
DESIGNED	B. Sauter
CHECKED	E. Mroczek
DRAWN	R. Danley
CHECKED	B. Sauter

Ciorba Group, Inc.
 CONSULTING ENGINEERS
 5507 North Cumberland Avenue, Suite 402 Chicago, Illinois 60656
 Tel. 773.775.4009 Fax 773.775.4014 Email chicago@ciorba.com

SHEET NO. S1	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	347	2 Y-B-I	DU PAGE	34	27
S3 SHEETS	CONTRACT NO. 60D82				
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT		

1/13/2009 11:52:25 AM \\PROJ\3329_25\Design\Structural\CAD\Sheet\3329_25_01.dwg

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".

NEW SLAB

EXISTING SLAB

EXISTING DECK BEAM

SECTIONS THRU SLAB OR DECK BEAM

NOTES

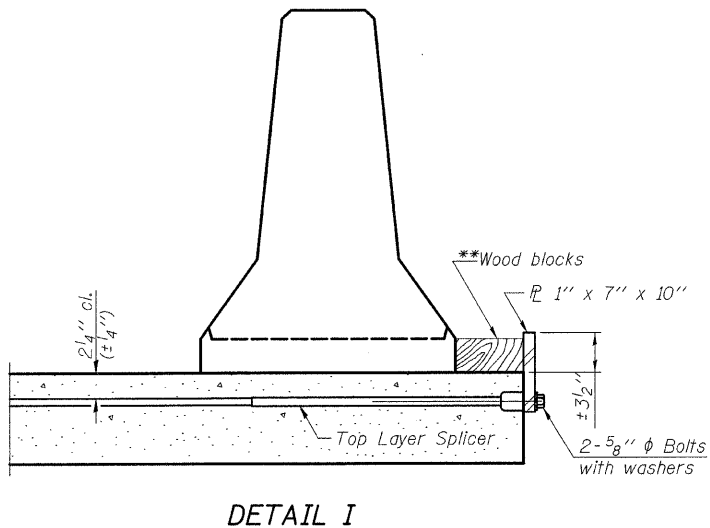
Detail I - With Bar Splicer or Couplers:
Connect one (1) 1"x7"x10" steel \bar{P} to the top layer of couplers with 2-5/8" ϕ bolts screwed to coupler at approximate \bar{C} of each barrier panel.

Detail II - With Extended Reinforcement Bars:
Connect one (1) 1"x7"x10" steel \bar{P} to the concrete slab or concrete wearing surface with 2-5/8" ϕ Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate \bar{C} of each barrier panel.

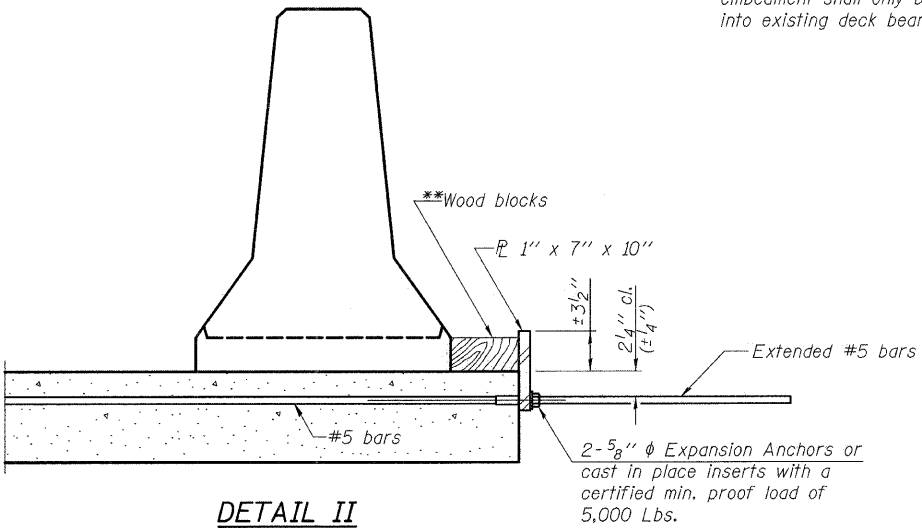
Cost of anchorage is included with Temporary Concrete Barrier. The 1" x 7" x 10" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

*** Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

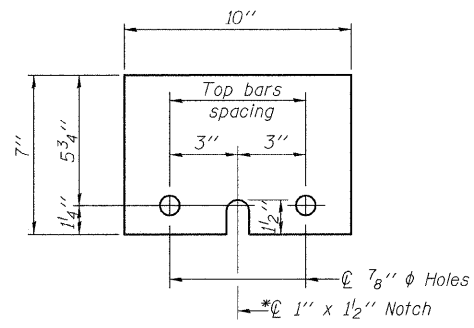
**** If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



DETAIL I



DETAIL II



STEEL RETAINER \bar{P} 1" x 7" x 10"

* Required only with Detail II

**Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

TEMPORARY CONCRETE BARRIER
FOR STAGE CONSTRUCTION
IL RTE. 38 OVER KRESS CREEK
STA. 169+84.54
S.N. 022-0150

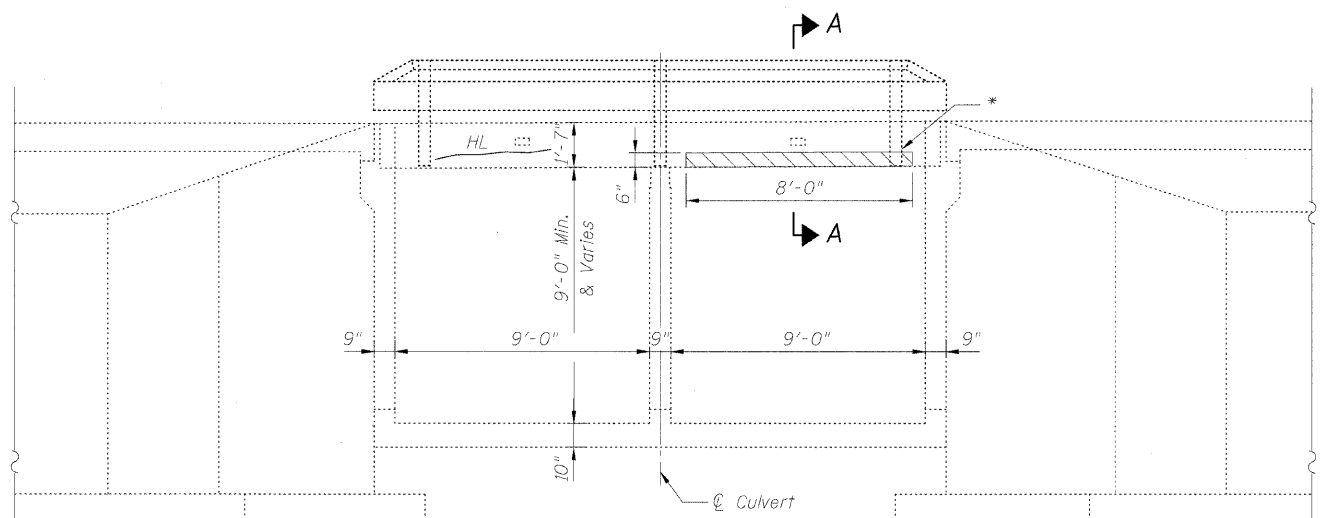
DESIGNED	B. Sauter
CHECKED	E. Mroczek
DRAWN	R. Danley
CHECKED	B. Sauter

Ciorba Group, Inc.
CONSULTING ENGINEERS
5507 North Cumberland Avenue, Suite 402 Chicago, Illinois 60656
Tel. 773.775.4009 Fax 773.775.4014 Email chicago@ciorba.com

SHEET NO. S2	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	347	2 Y-B-I	DU PAGE	34	28
S3 SHEETS	CONTRACT NO. 60D82				
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT		

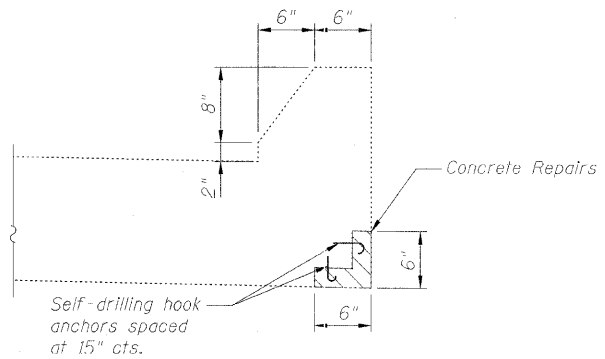
1/13/2009 rdenley na\pco\3329\3329_29\design\structural\ced\ah\3329_29_02 Temp.Conc.Barrier.sht

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



SOUTH HEADWALL REPAIRS

*-The existing rail post shall be removed during the repairs and the anchor devices shall be reused. The existing steel rail tubing shall remain in place during the work. Cost included with Structural Repair of Concrete.



SECTION A-A

LEGEND

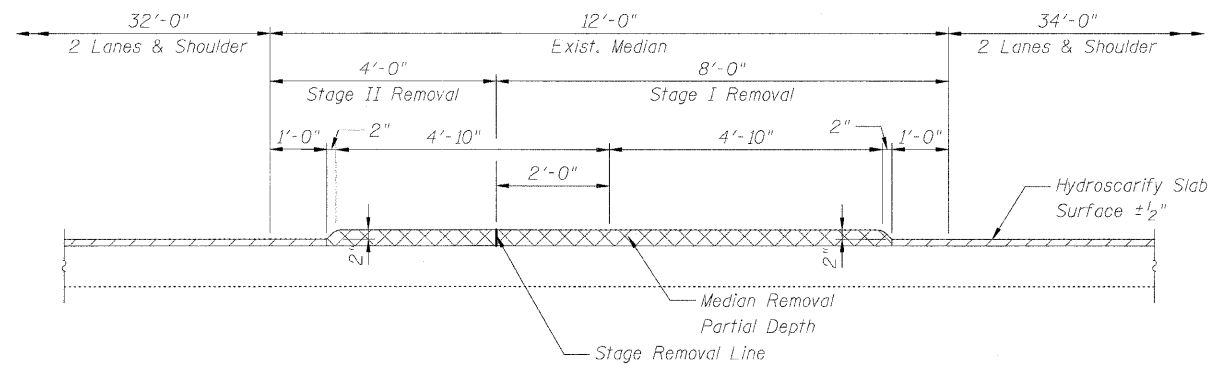
- HL Hairline Crack (No Repairs)
- Structural Repair of Concrete
- Hydroscarify Slab Surface
- Median Removal Partial Depth
- Concrete Superstructure

NOTES:

1. Repairs shall include but not be limited to the areas shown. The actual areas to be repaired will be determined by engineer at the time of construction and marked on as-built plans.
2. The contractor is ultimately responsible for the means and methods to assure the complete stability of the structural members during construction.
3. Protective coat to be applied to roadway, median and inside faces of curbs.
4. The repair concrete shall be securely anchored to the headwall using self drilling anchors. Cost included with Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches).

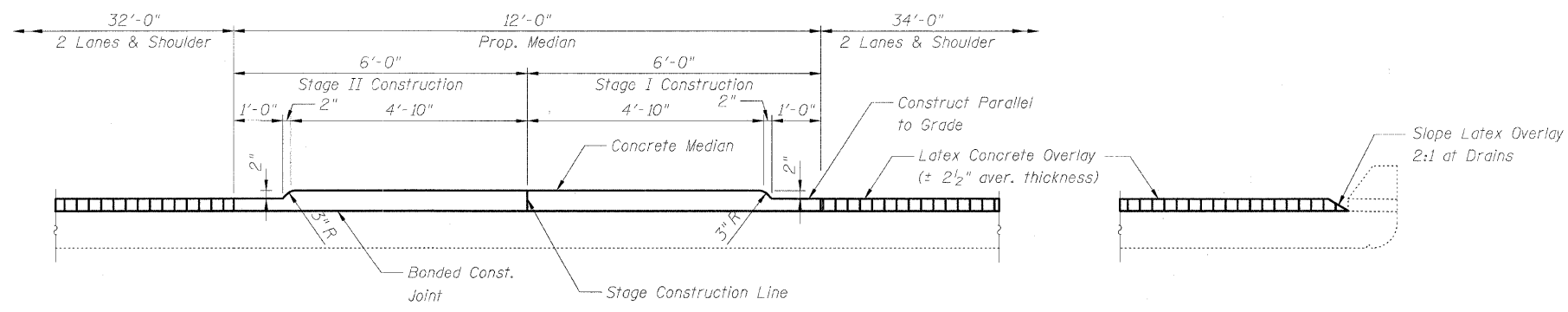
BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Concrete Superstructure	Cu.Yd.	3.3
Protective Coat	Sq. Yd.	182
Median Removal Partial Depth	Sq. Ft.	203
Structural Repair of Concrete (Depth Equal to or Less than 5 inches)	Sq. Ft.	8.0



SECTION THRU EXIST. MEDIAN

Looking East



SECTION THRU PROP. MEDIAN

Looking East

SECTION AT DRAINS

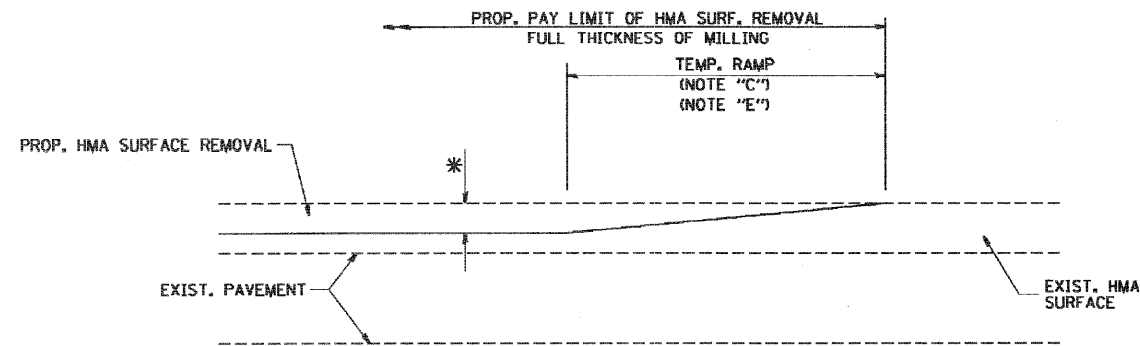
DETAILS
IL RTE. 38 OVER KRESS CREEK
STA. 169+84.54
S.N. 022-0150

DESIGNED	B. Sauter
CHECKED	E. Mroczek
DRAWN	R. Danley
CHECKED	B. Sauter

Ciorba Group, Inc.
CONSULTING ENGINEERS
5507 North Cumberland Avenue, Suite 402 Chicago, Illinois 60656
Tel. 773.775.4009 Fax 773.775.4014 Email chicago@ciorba.com

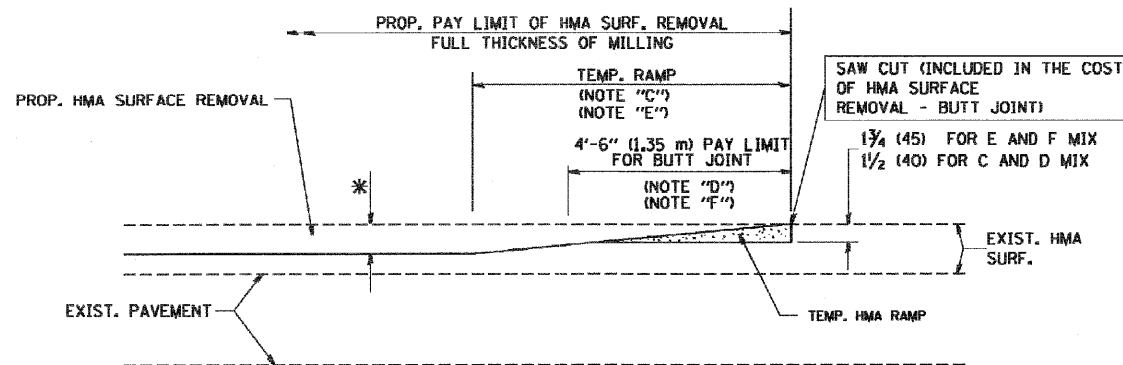
SHEET NO. S3	F.A.P. RTE. 347	SECTION 2 Y-B-I	COUNTY DU PAGE	TOTAL SHEETS 31	SHEET NO. 29
S3 SHEETS			CONTRACT NO. 60D82		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

1/13/2009 r-danley N:\PROJ\3329\29\Design\Structural\CAD\Sheet\3329_29_03_Detailed.sht



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

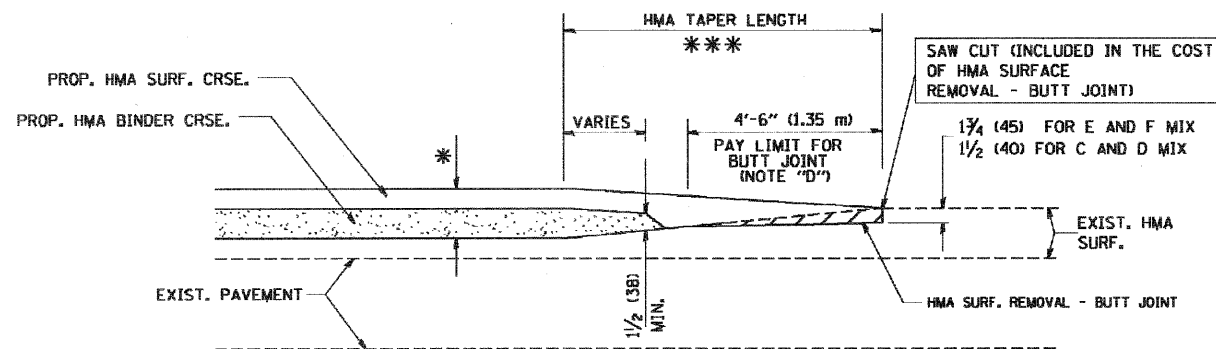
OPTION 1



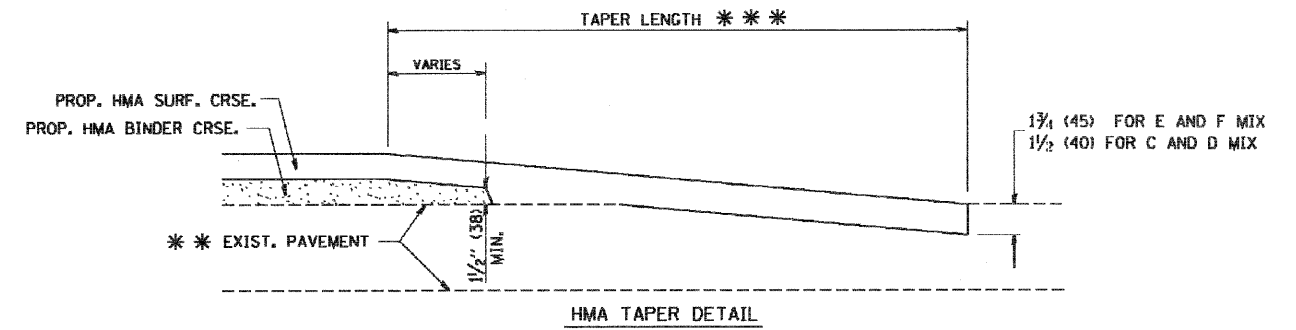
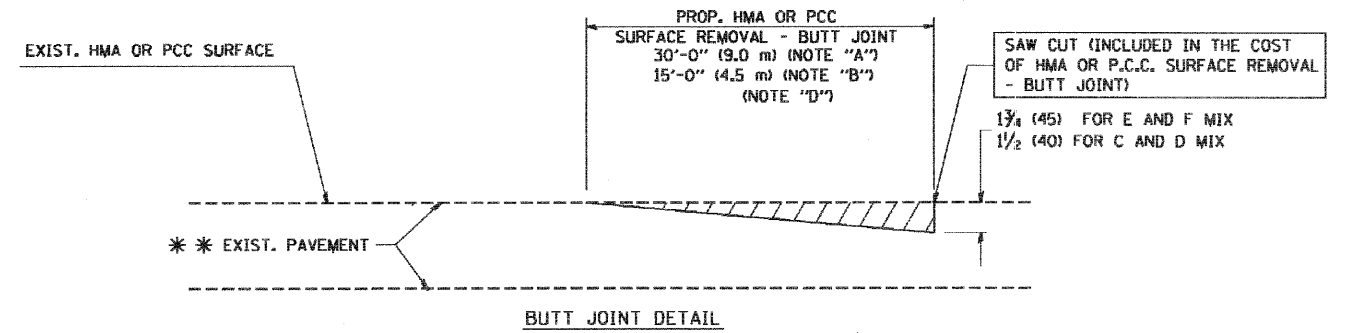
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT AND HMA TAPER
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

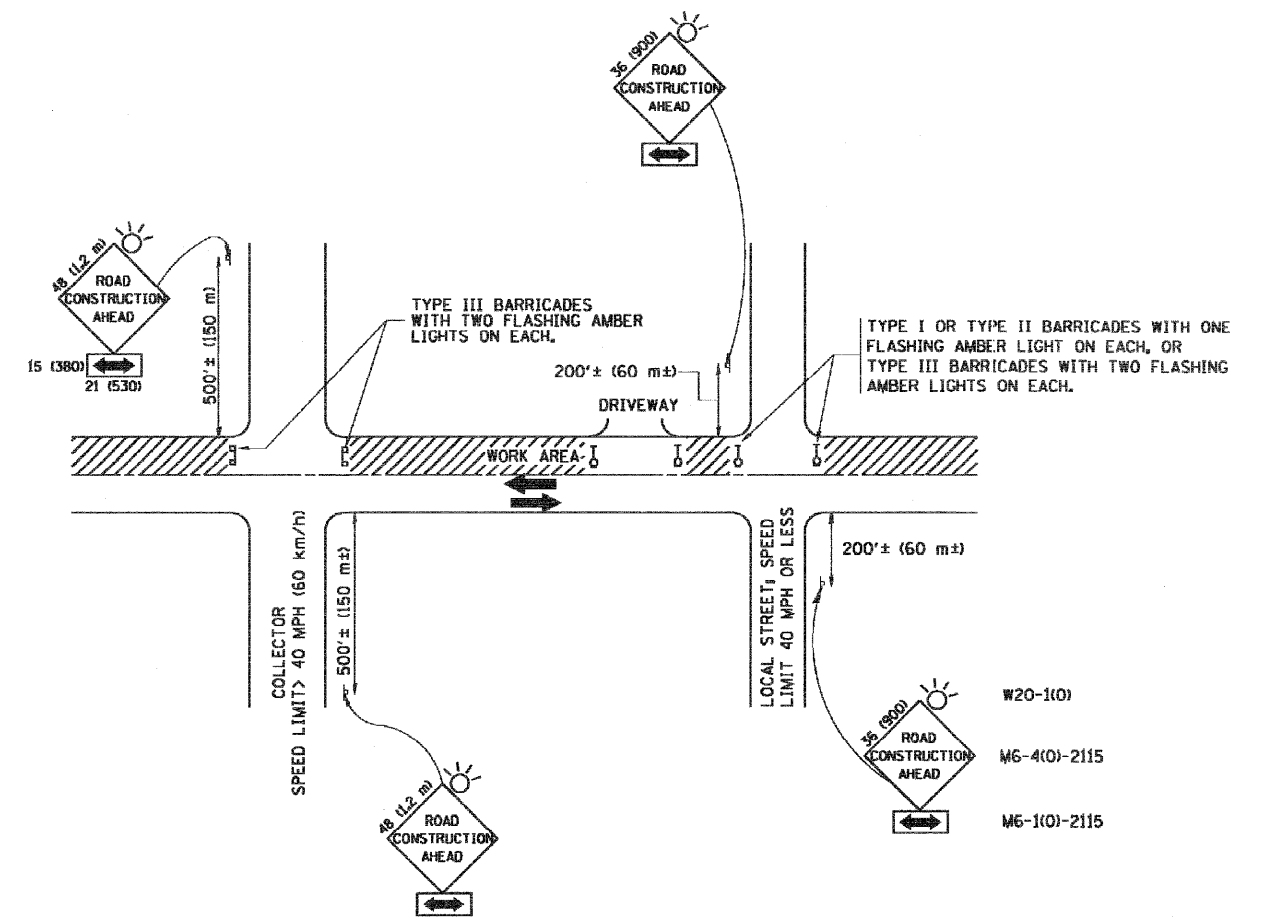
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = goglionobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
W:\dotststd\22x34\bd32.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000 "/ IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2000	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2 Y-B-I		34	30
BD40-05 BD32		CONTRACT NO.	60D82	
FED. ROAD DIST. NO. 1 ILLINOIS/FED. AID PROJECT				



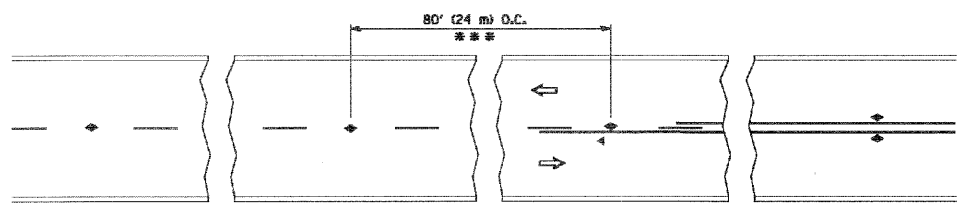
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS**
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:**
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 70150L, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.**
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.**

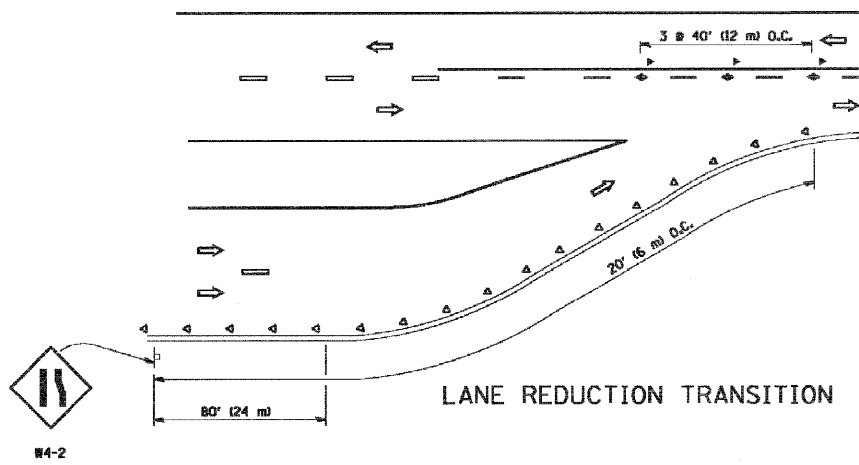
All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME =	USER NAME = gogianobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
W:\dists\d22x34\1018.dgn		DRAWN -	REVISED - A. HOUSEH 03-06-96		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	347	2 Y-B-I		34	31	
		PLOT SCALE = 50,000' / IN.	REVISED - A. HOUSEH 10-15-96				TO STA.		TC-10		DU PAGE		
		PLOT DATE = 1/4/2000	REVISED - T. RAMMACHER 01-06-00								CONTRACT NO.	60D82	

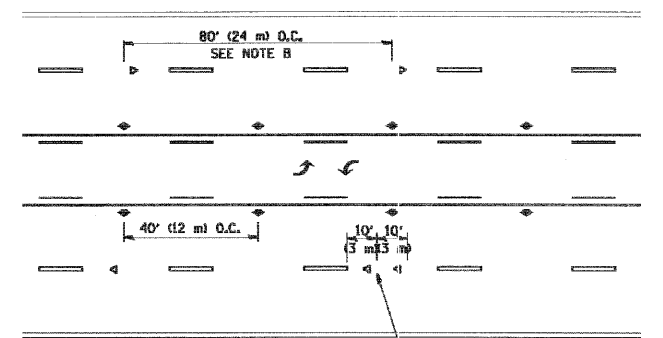


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

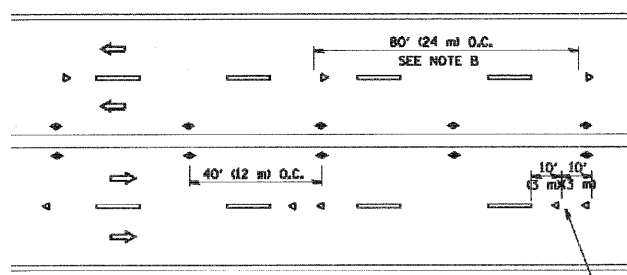
TWO-LANE/TWO-WAY



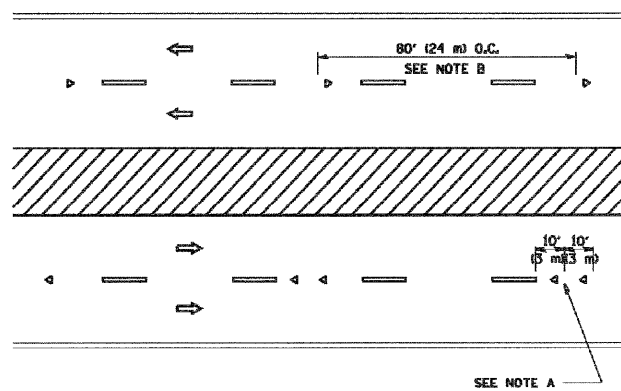
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

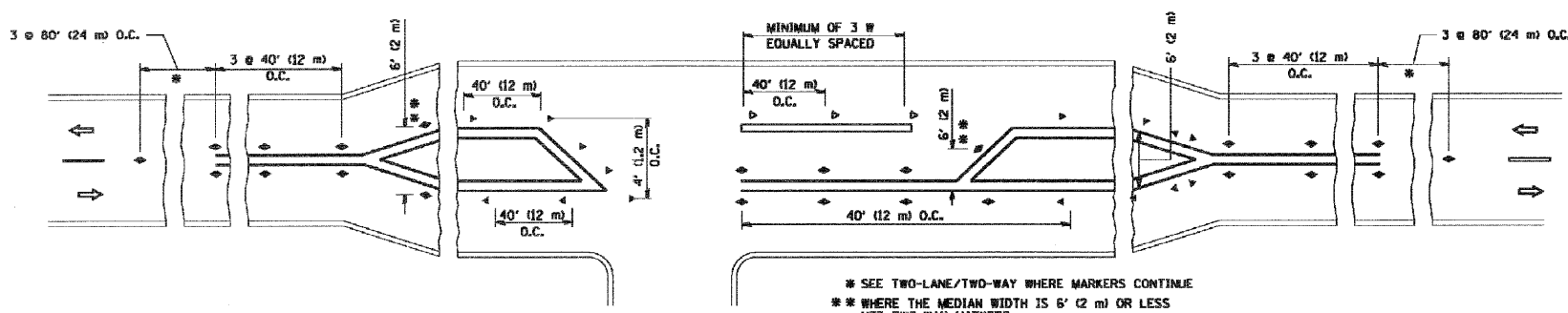
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◀ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

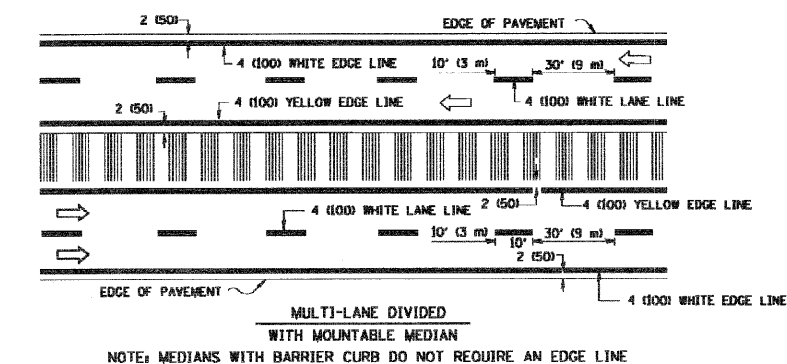
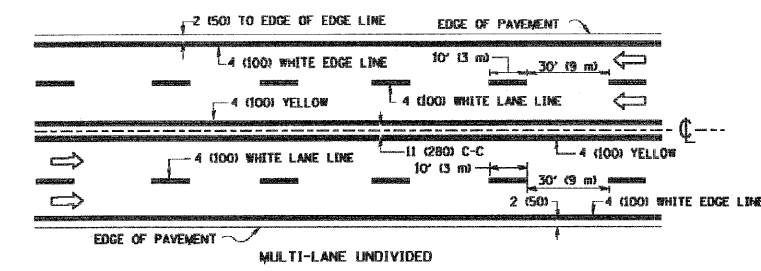
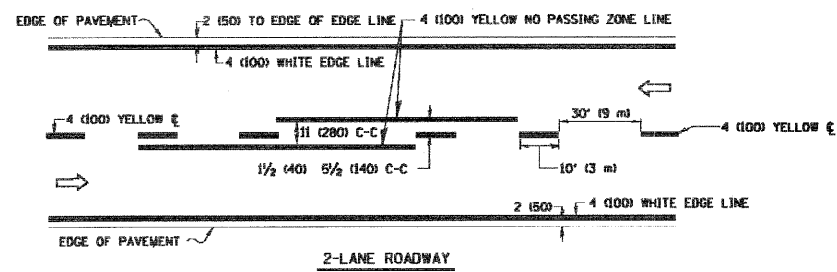


LEFT TURN

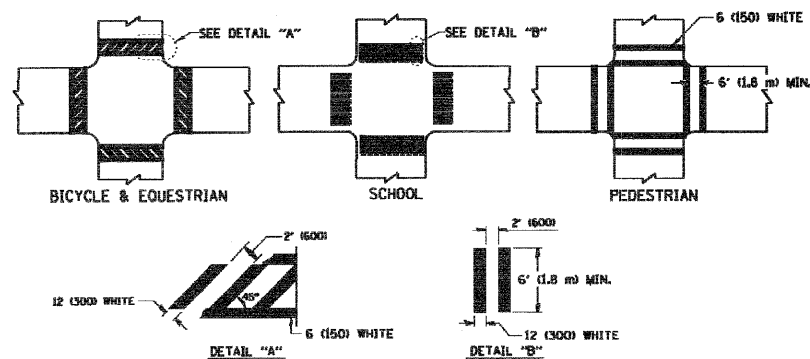
* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

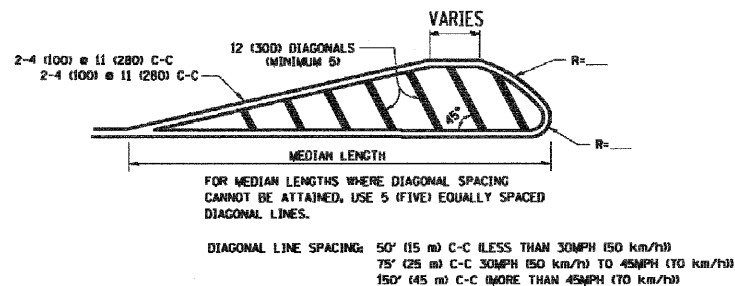
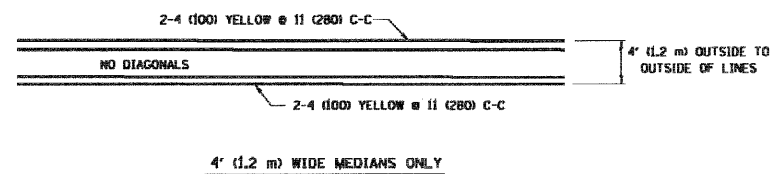
FILE NAME = W:\diststd\22x34\vol1\dgn	USER NAME = goglienobt	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			F.A.P. RTE. 347	SECTION 2 Y-B-1	COUNTY	TOTAL SHEETS 34	SHEET NO. 32
	PLOT SCALE = 50,000' / IN.	DRAWN -	REVISED - T. RAMMACHER 03-12-99		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TD STA.	TC-11	CONTRACT NO. 60D82		
	PLOT DATE = 1/4/2000	CHECKED -	REVISED - T. RAMMACHER 01-06-00									
		DATE -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							



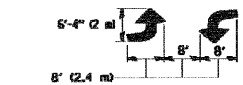
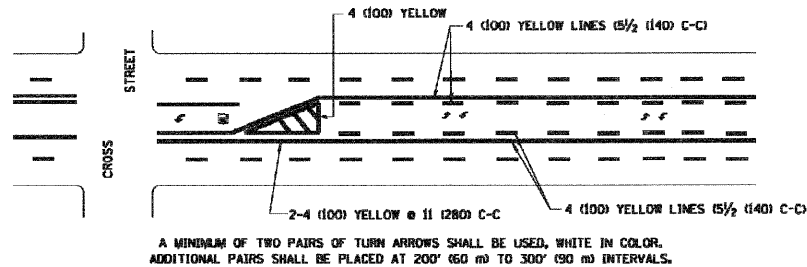
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

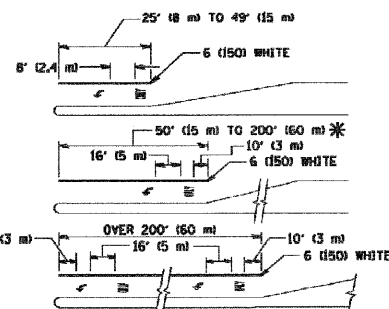


MEDIANS OVER 4' (1.2 m) WIDE



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

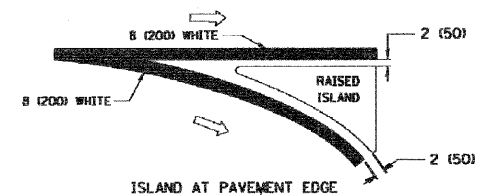
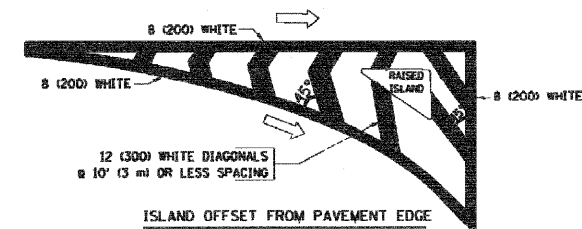


FULL SIZE LETTERS 6' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" (INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY").

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

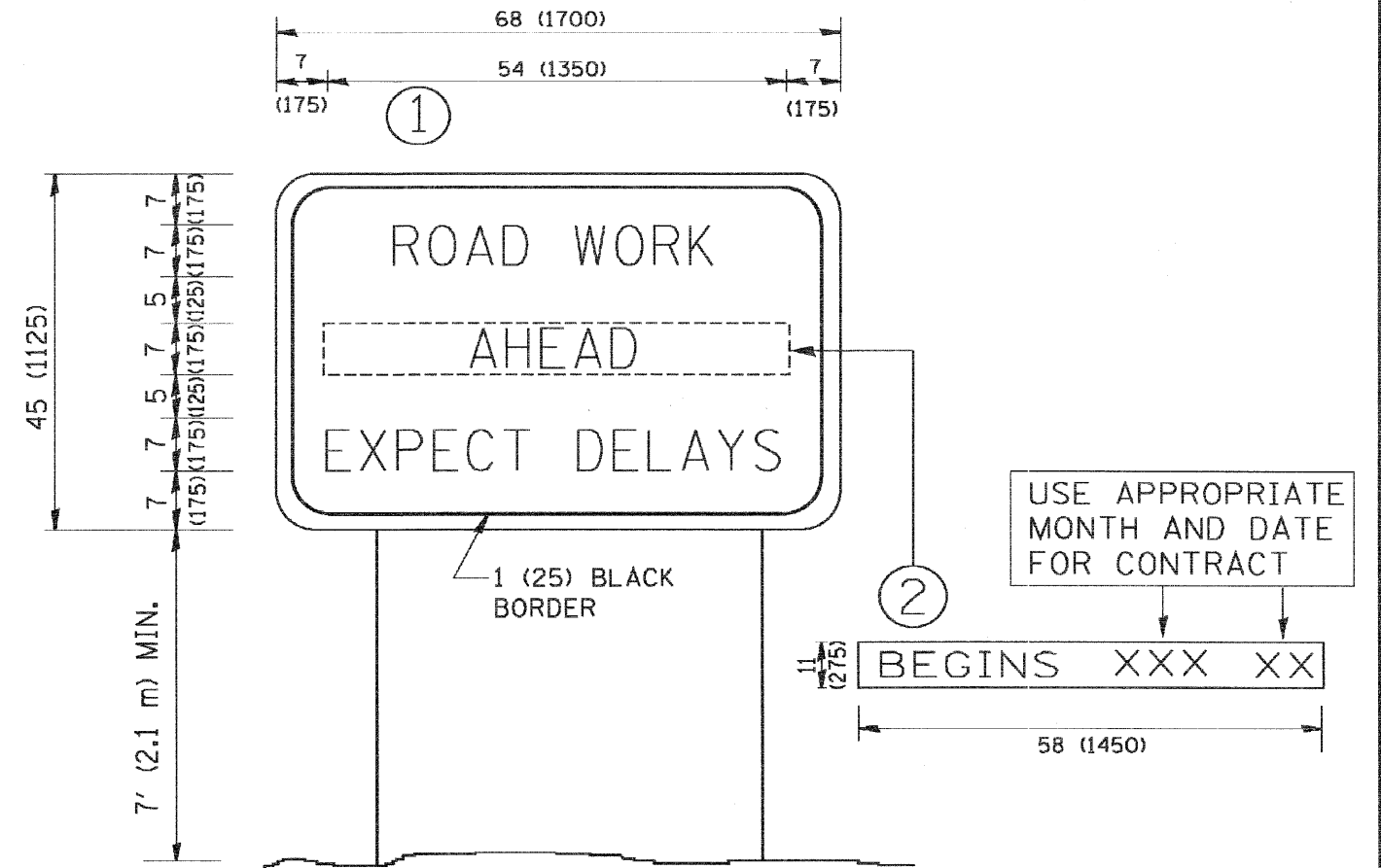


TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS 18" (4.6m)	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL.
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = goglionobt	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
W:\distsstd\22x34\to22.dgn		DRAWN -	REVISED - R. MIRS 12-11-97			347	2 Y-B-I	DU PAGE	34	34
		CHECKED -	REVISED - T. RAMMACHER 02-02-99			TC-22		CONTRACT NO. 60082		
		DATE -	REVISED - C. JUCIUS 01-31-07			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT